



2ND AND VIRGINIA

1931 2ND AVENUE EARLY DESIGN GUIDANCE | MARCH 5, 2019 | DPD #3033067-EG

KENGO KUMA // ANKROM MOISAN ARCHITECTS // BERGER PARTNERSHIP // PACIFIC VIRGINIA

Contents

SECTION 01 / PROJECT INFORMATION 04

SECTION 02 / SITE 05

SECTION 03 / LANDMARK 09

SECTION 04 / ZONING 12

SECTION 05 / CONTEXT ANALYSIS 16
Street Characteristics 18
Neighborhood Uses 19
Changing Fabric 20
Nodes & Landmarks 22
Views & Orientation 24
Streetscapes 26

SECTION 06 / DESIGN GUIDELINES 28
DRB Design Guidelines 29
Landmark Preservation Board Priorities 31

SECTION 07 / PUBLIC OUTREACH. 32

SECTION 08 / DESIGN CONCEPTS. 34
Tower Location 35
Concept Overview 36
Concept 1: Reveal 39
Concept 2: Shift 53
Concept 3 (Preferred): Gradation 67
Drop-off Study 85
Shadow Studies 86

SECTION 09 / LANDSCAPE 89

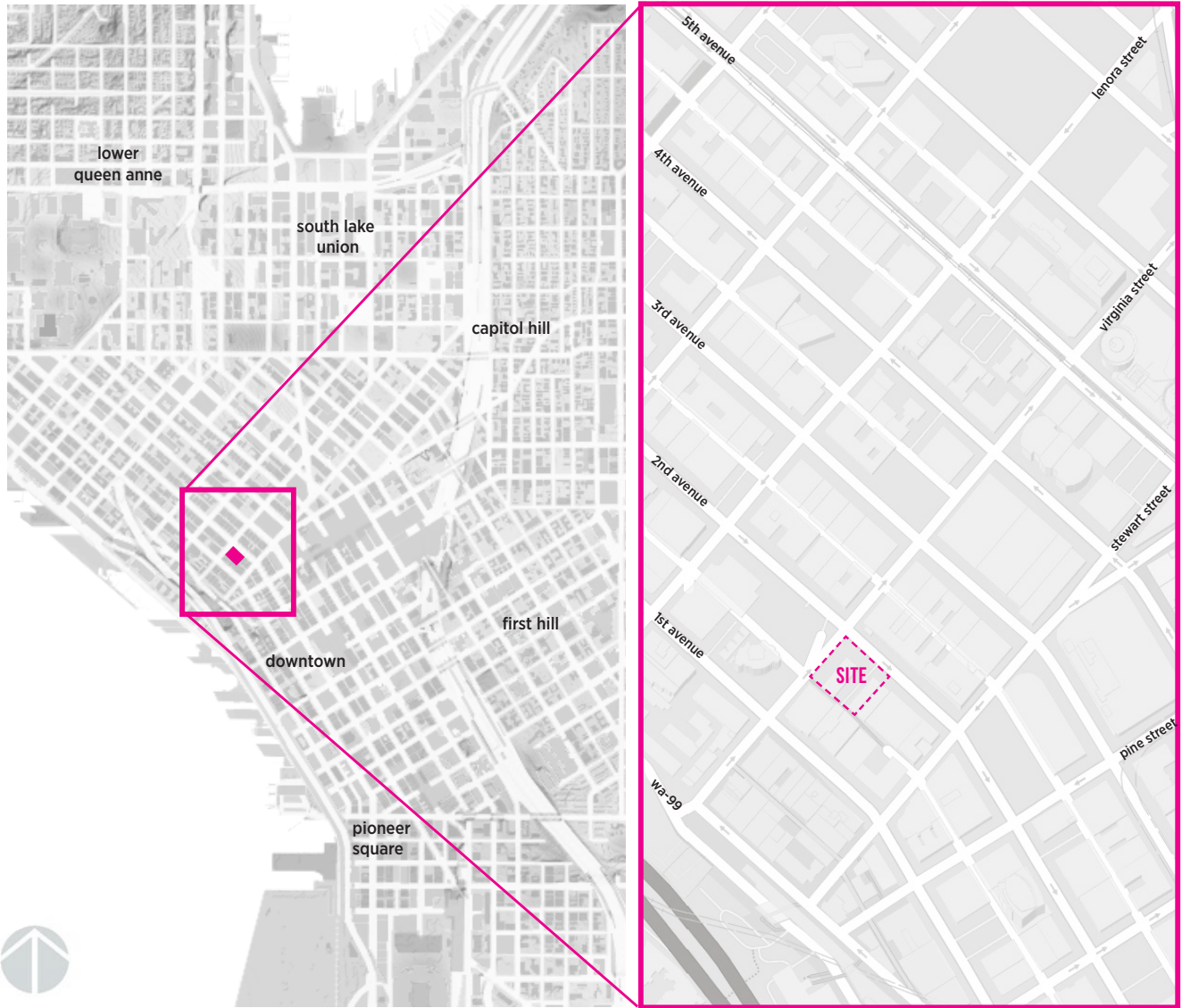
SECTION 10 / DEPARTURES. 95

01 PROJECT INFORMATION

Proposal 1

A new mixed-use hotel, residential, and co-working building will be located at the corner of 2nd Avenue and Virginia Street in downtown Seattle. The building will be a vibrant addition to this key intersection and will take design cues from the existing landmark on site and the urban patterns of buildings and pedestrian experiences that make up the Downtown Retail Core of Seattle.

- The proposed development consists of the following:
- One 42-story structure of approximately 487,100 sq. ft. above ground
 - 2 levels of co-work, and 6,700 sf of retail
 - 10 levels of hotel with 237 keys
 - 27 levels of residential with 222 condo units
 - Separate lobbies for hotel and residential uses
 - 5 levels of below-grade parking (approx. 250 stalls)
 - 2 loading spaces will be accessed through the Alley



02 SITE

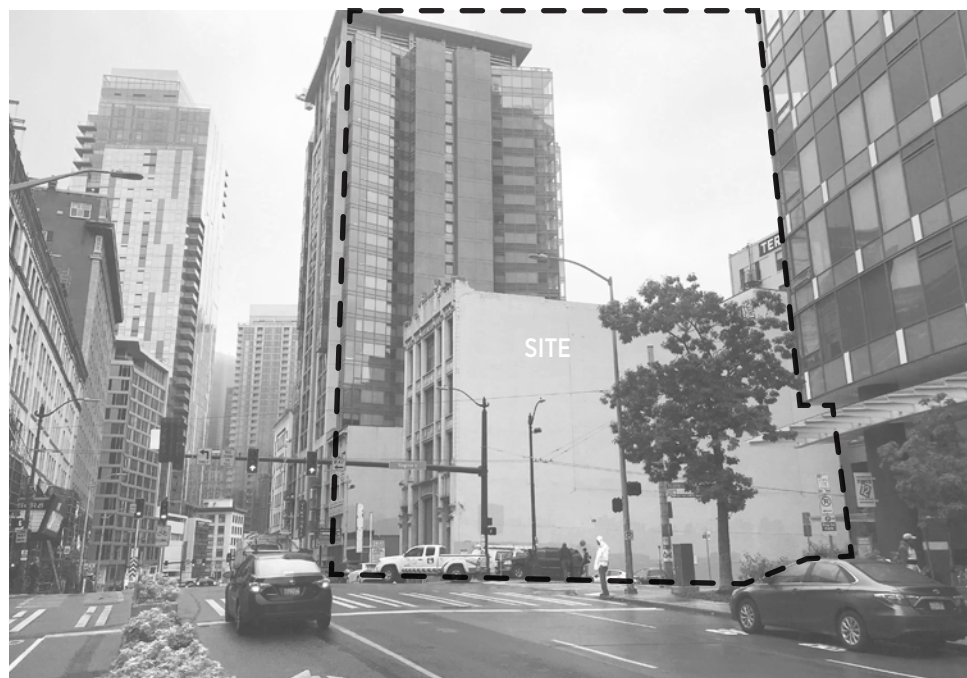
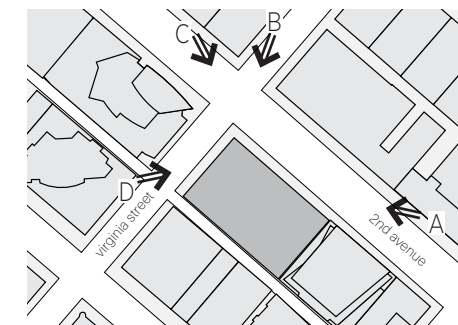
Street Photographs 2



A. View towards West



B. View towards SouthWest



C. View towards SouthEast



D. View Towards West

SITE

2 Existing Survey

Site Impressions:

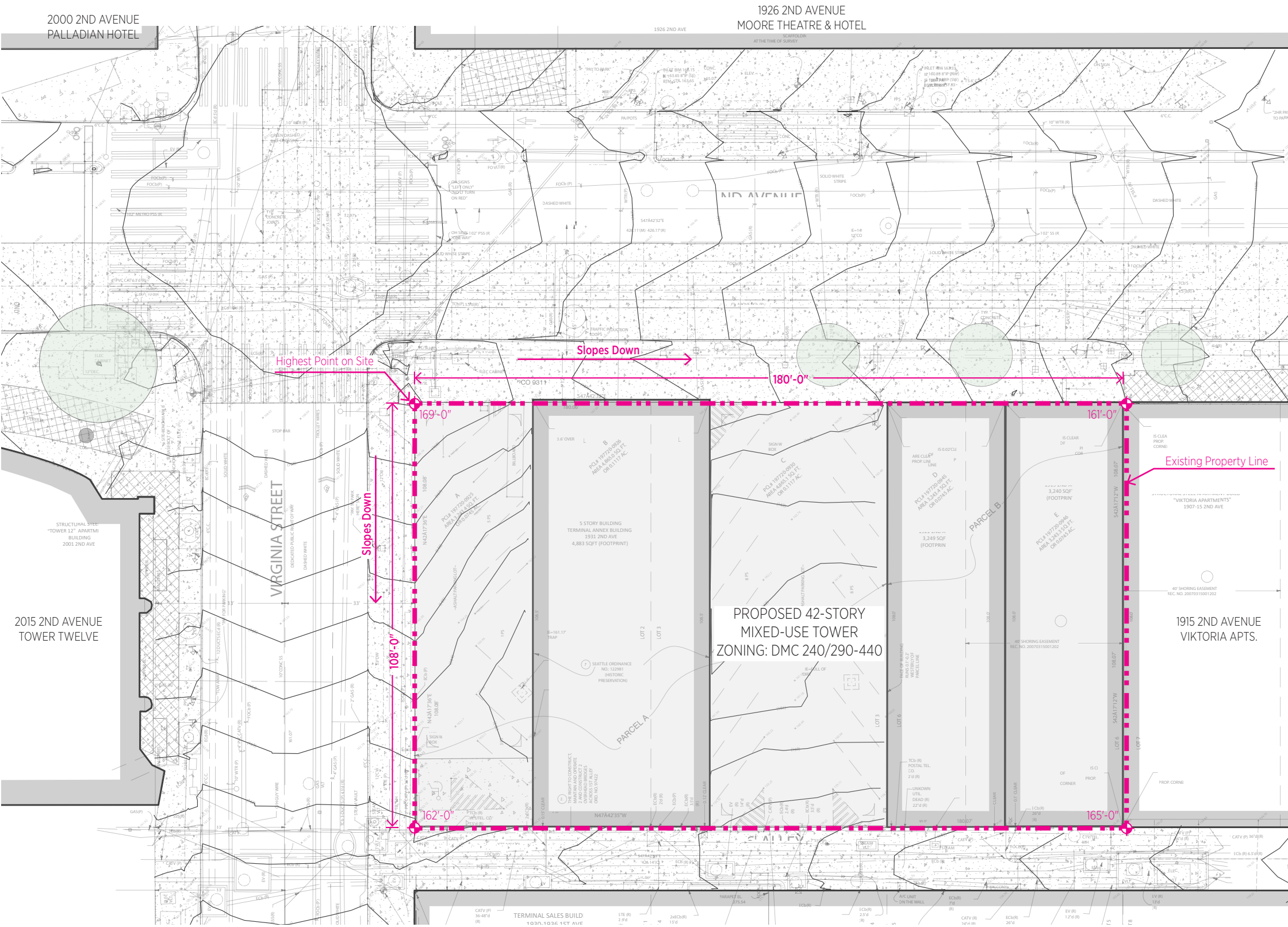
Address: 1931 2nd Avenue, Seattle WA. 98101
Parcel: 1977200925-26, 1977200930, 1977200945-46
Lot Area: 19,440 sf

Existing Buildings: Three buildings currently exist on the lots; the Terminal Sales Annex Building, a 4-story office building, and a 1-story office building.

Topography: The site slopes from approximately +169' on the highest NE side at the corner of 2nd and Virginia to +155' on the lowest SW side in the corner (1' total slope). Average grade = 165'-0"

- Site Characteristics:**
- Highest point in downtown
 - Views to Puget Sound down Virginia Street and at elevation above Terminal Sales Building
 - 2nd Avenue is a one-way street headed south.
 - Virginia Street is a one-way street headed east.
 - Intersection is home to 3 Landmarked Buildings: Terminal Sales Annex, Palladian Hotel, Moore Theatre & Hotel

Street Classification:
2nd Avenue is a Principal Transit Street, Class I Pedestrian
Virginia Street is Minor Arterial, Class II Pedestrian



03 LANDMARK

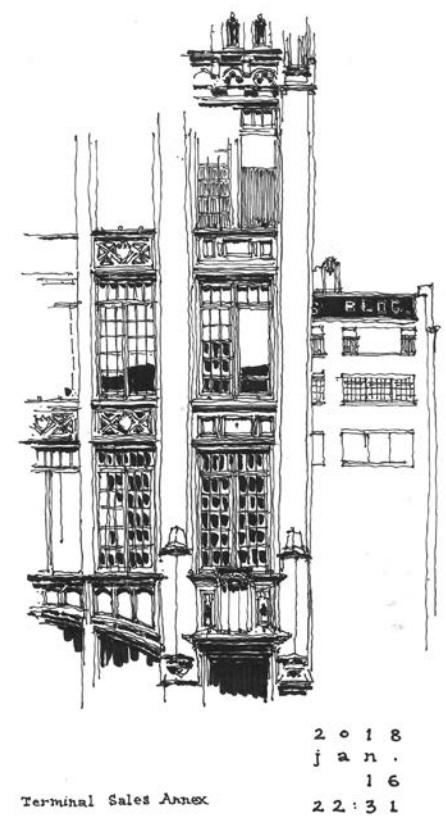
LANDMARK

3 Terminal Sales Annex

The Terminal Sales Annex building is located in Seattle’s Belltown neighborhood (1931 2nd Avenue) as a designated Seattle Landmark. The building was designed in the distinctive Collegiate Gothic Revival style by architect’s Bebb & Gould. Charles Bebb was a leading innovator in the use of architectural terracotta in the Seattle area.

The landmark is a five-story (+72’) reinforced structure that was designed as an infill building and currently is flanked by two on-grade parking lots. It holds a corner with other landmark structures, including the Palladian Hotel and Moore Theatre & Hotel.

This project aims to integrate the Terminal Sales Annex into a cohesive design which seeks inspiration from the landmark itself as well as the local surroundings.





Historic Photograph



Photograph of Current Conditions

The team has met with the Architectural Review Committee (ARC) on two separate occasions to review massing, integration of the landmark and overall scale. The ARC reacted positively to the preferred massing presented in this packet and advised the team to proceed to Early Design Guidance.

ARC Meeting 1 - 06.29.2018

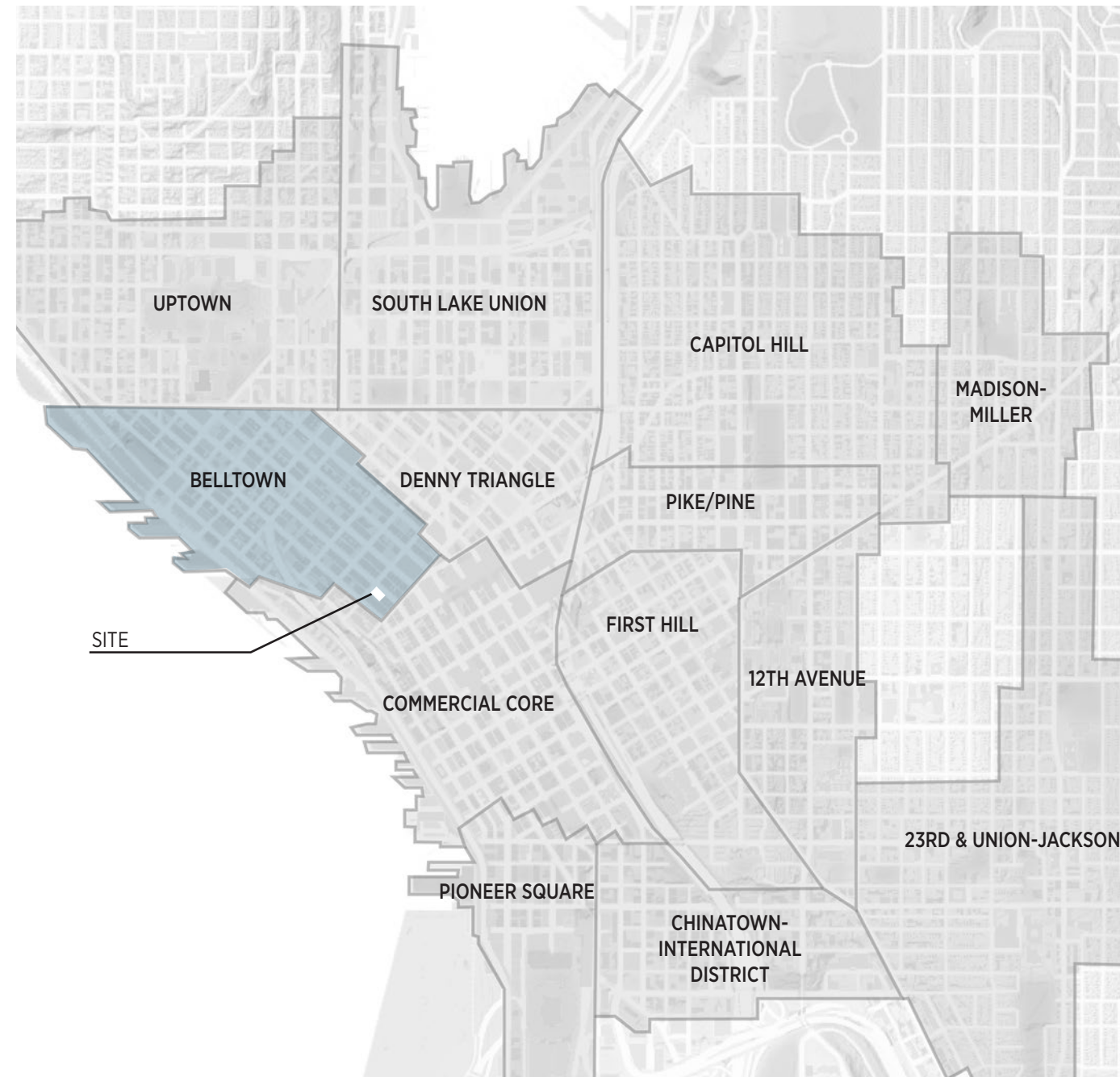
The ARC requested further study of the integration of the landmark building.

ARC Meeting 2 - 8.31.2018

Reviewed the preferred option.
The ARC stated that they recommend the preferred option move forward into Early Design Guidance.

04 ZONING

Zoning and Overlay Designations



Zoning Summary



ZONING

4 Zoning Summary

EXISTING LOT INFORMATION

- Address: 1931 2nd Avenue, Seattle WA. 98101
- Parcel: 1977200925-26, 1977200930, 1977200945-46
- Lot Area: 19,440 sf
- Existing Buildings: Three buildings currently exist on the lots; the Terminal Sales Annex Building, a 4-story office building, and a 1-story office building.
- Street Classification: 2nd Avenue is a Principal Transit Street, Class I Pedestrian Street and requires property line facade and street-level uses. Virginia Street is Minor Arterial, Class II Pedestrian.
- Topography: The site slopes from approximately +169' on the highest NE side at the corner of 2nd and Virginia to +155' on the lowest SW side in the corner (1' total slope). Average grade = 165'-0"

ZONE

- DMC 240-290/440 (Downtown Mixed Commercial)
- Overlay: BELLTOWN URBAN CENTER VILLAGE

ALLOWED USES

- Include Retail, Hotel, Residential and Office
 - For Residential Use, Common recreation area equal to 5% of the total gross floor area in residential use is required for more than 20 dwelling units, 50% of which must be outdoors

HEIGHT LIMIT

- Base Height = 240' for commercial uses
- Maximum Height = 440' if any of the following are met:
 - All portion of a structure above 240ft are residential only
 - At least 25 percent of the gross floor area of all structures on a lot is in residential use
 - Min 1.5 FAR dedicated to eating & drinking establishments &/or retail sales and service is provided
- Rooftop Features:
 - Parapets allowed to extend 4' above height limit.
 - Stair Penthouses, Common Rec Areas, Eating & Drinking Establishments, Mechanical Equipment & Mech Screening allowed to extend 15' above height limit.
 - Elevator Penthouses up to 25' above height limit.

FLOOR PLATE

- Average plates limited to 10,700sf for tower up to 440ft. Maximum floor plate is 11,500 sf.
- Tower width not to exceed 120ft above 85ft along 2nd Avenue.
- Tower Modulation applies to facades within 15ft of the property line:
 - No limits to un-modulated building facade widths below 85ft.
 - From 85'-160', max un-modulated facade width = 155'.
 - From 161'-240', max un-modulated facade width = 125'.
 - From 241'-500', max un-modulated facade width = 100'.

FAR

- Base FAR 5, Maximum FAR 8 (Maximum is achieved through payments for childcare, TDR, etc.).
- The following are not included in chargeable floor area, Street-level uses, Residential use, Floor area below grade
- 3.5% indoor gsf shall be deducted in computing chargeable gross floor area as an allowance for Mechanical Equipment.
 - Mechanical equipment located on the roof of a structure shall not be calculated as part of the total gross floor area of the structure.

DMC STREET FACADE & SETBACKS

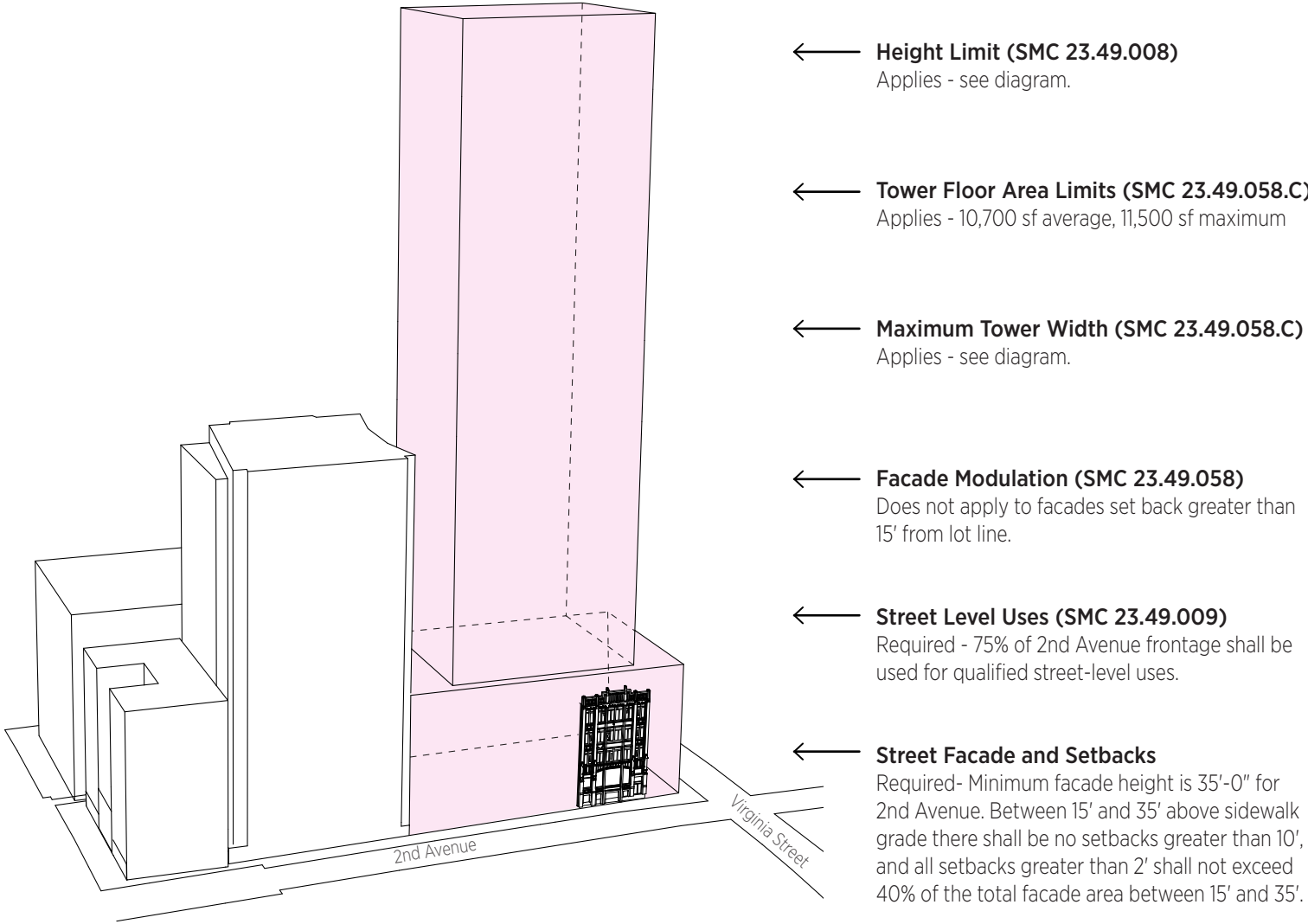
- Minimum facade height on Virginia Street = 25'
- Minimum facade height on 2nd Avenue = 35'
- No setback limits up to an elevation of 15' feet above sidewalk
- On 2nd Avenue, between 15' and 35' above sidewalk grade there shall be no setbacks except for the following:
 - The max setback shall be 10'
 - The total area of the portion of the facade between +15' - +35' at the street property line that is set back more than 2' shall not exceed 40% of the total facade area between +15' - +35'
 - No setback deeper than 2' shall be wider than 20'
 - The facade of the structure shall return to within 2' of the street property line between each setback for a min. of 10'

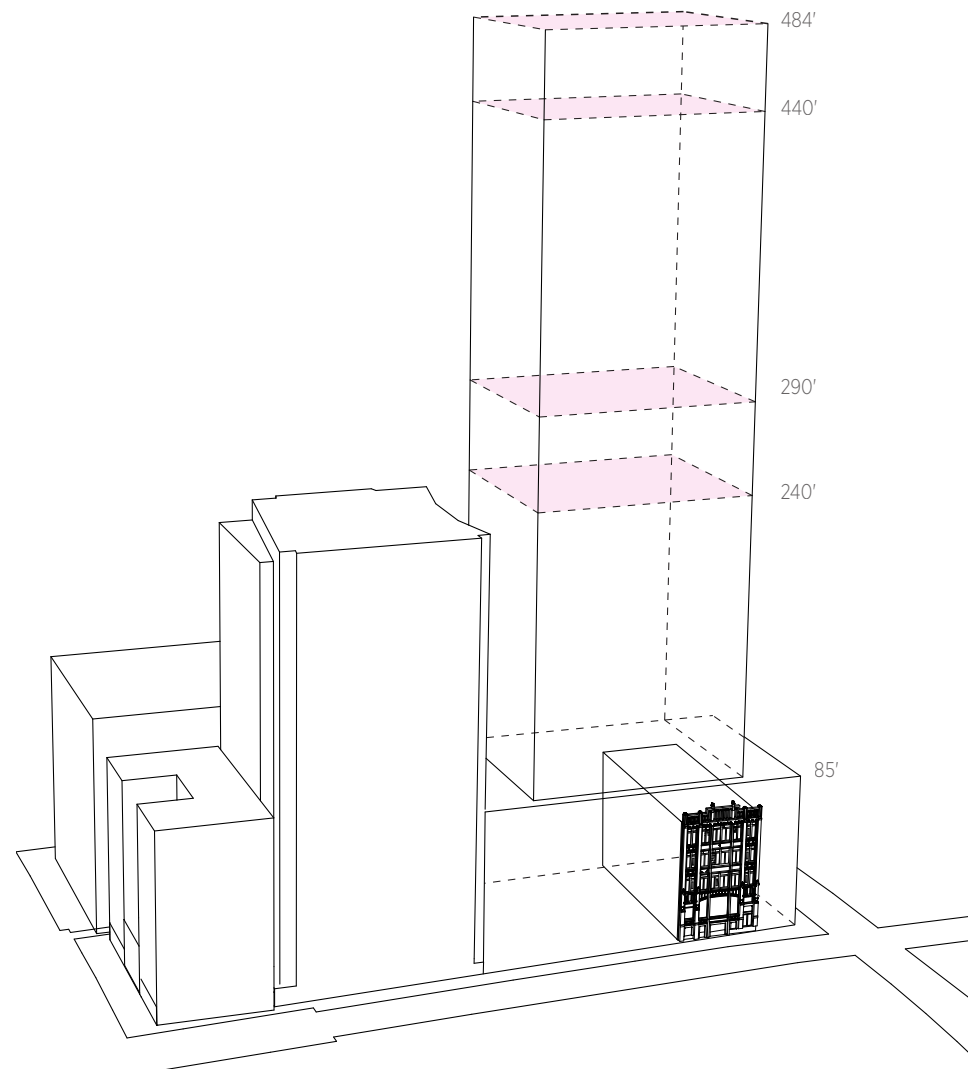
STREET, SIDEWALK & GROUND LEVEL REQUIREMENTS

- One or more of the following uses qualify as required street-level uses, and are required at street level for at least 75% of the 2nd Avenue frontage (including): General sales and services; Retail sales, Eating and drinking establishments. Required uses shall be located within 10' of the lot-line.
- Continuous overhead weather protection (8' wide) shall be required for new development along the entire street frontage except along portions of facade that are located farther than 5' from property line
- On 2nd Avenue, min 60% of the street level facade shall be transparent (between 2' and 8'). Virginia Street = 30%
- Street trees are required on all streets. Trees shall be planted according to SDOT tree planting standards.
- Sidewalk Width: 2nd Avenue = 15'. Virginia St. = no requirements
- Alley width= 20' min right of way. A dedication equal to half the difference between the current alley right-of-way width and minimum right-of-way width shall be required (2').

PARKING & LOADING

- No parking, either long-term or short-term, is required for uses on lots in Downtown zones.
- Off-street loading: 3 loading berths req'd 10' wide, 14' clear vertical, length 35' (25' if approved by Planning Director)
- Bicycle parking is required as specified in subsection 23.54.015.K.





SMC 23.49.008 - Structural Height

Zone: DMC 240-290/440

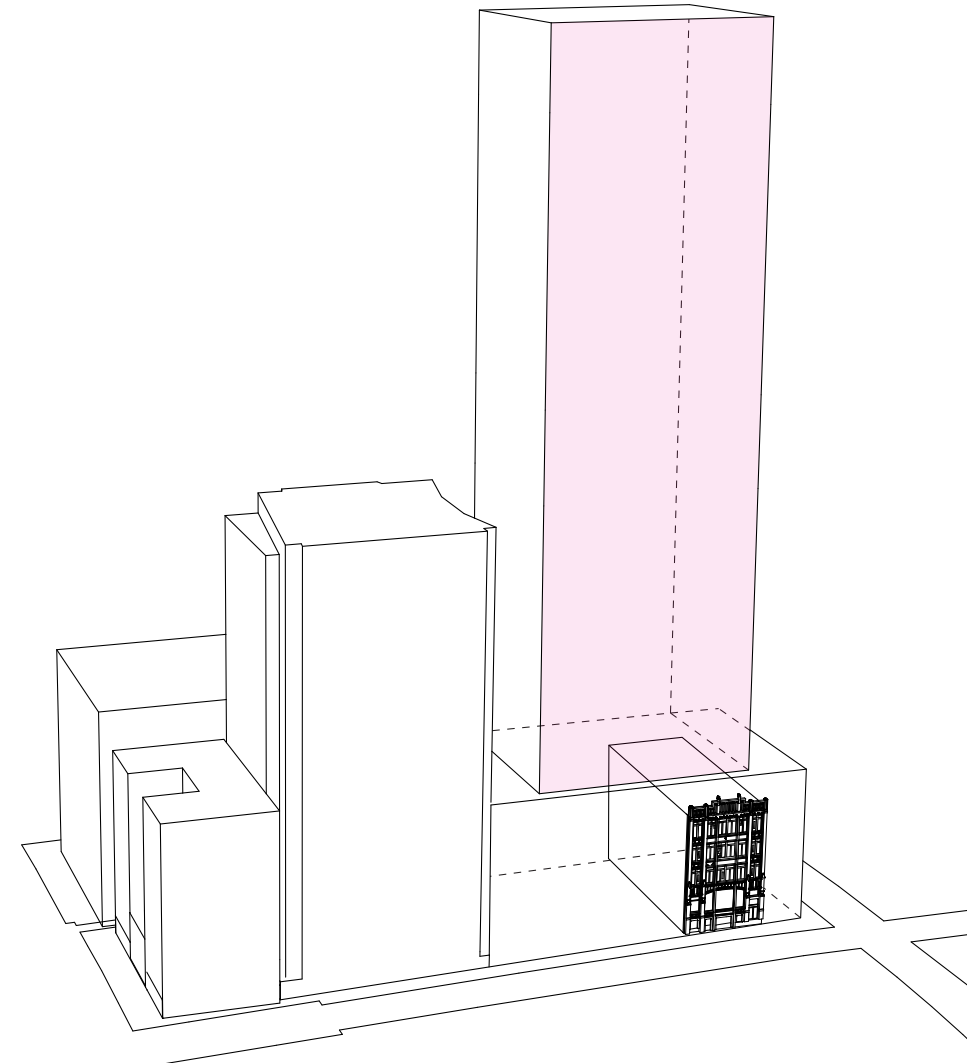
484' - 10% Height Allowance for common recreation area and mechanical - facades above 400' may not enclose an area greater than 9,000 sf

440' - Maximum residential height limit (HALA)

290' - Base height limit for portions in residential use

240' - Maximum non-residential height limit

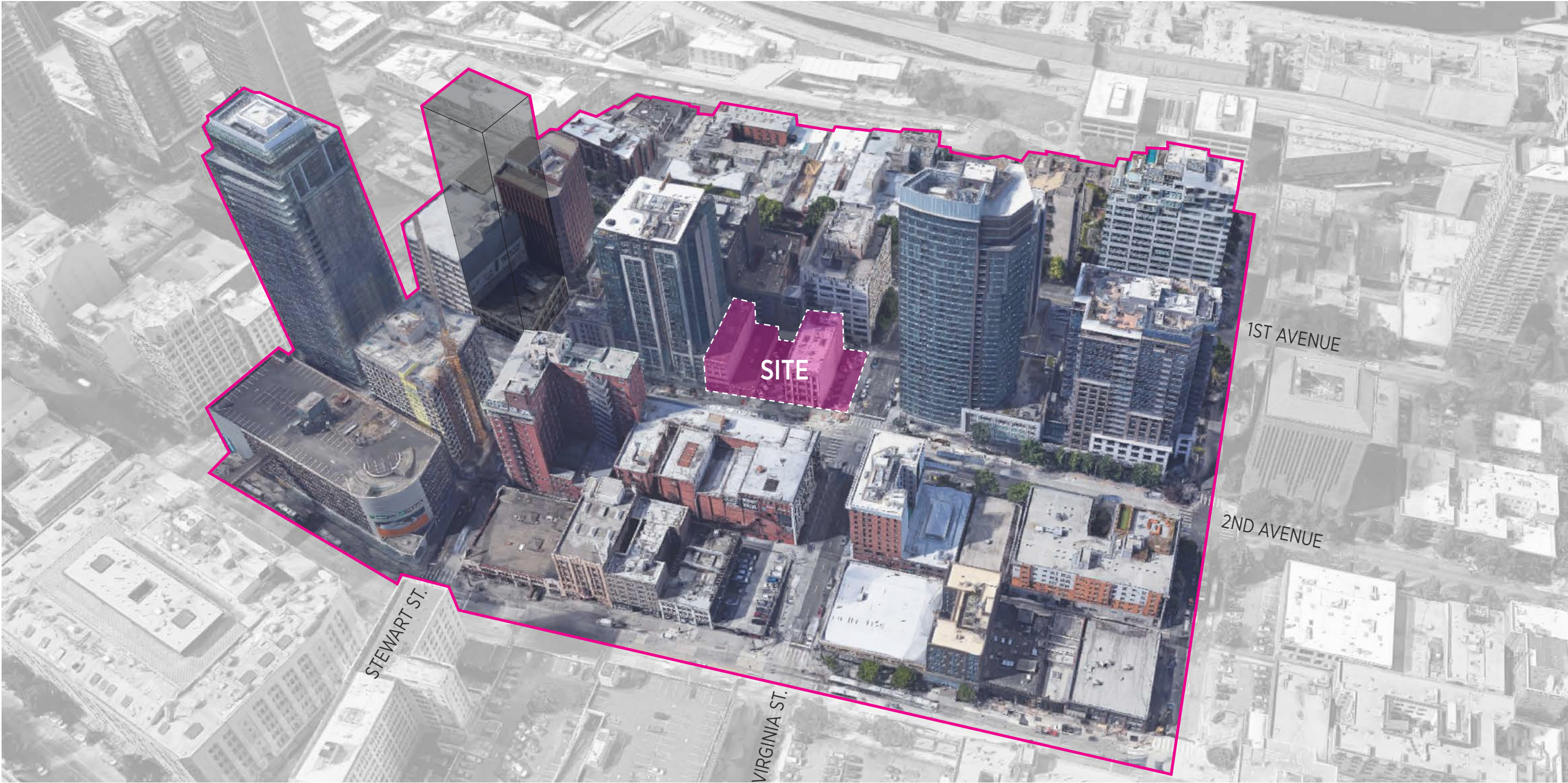
85' - Maximum height for unlimited length of un-modulated facade within 15' of street lot line



SMC 23.49.058 - Maximum Tower Width

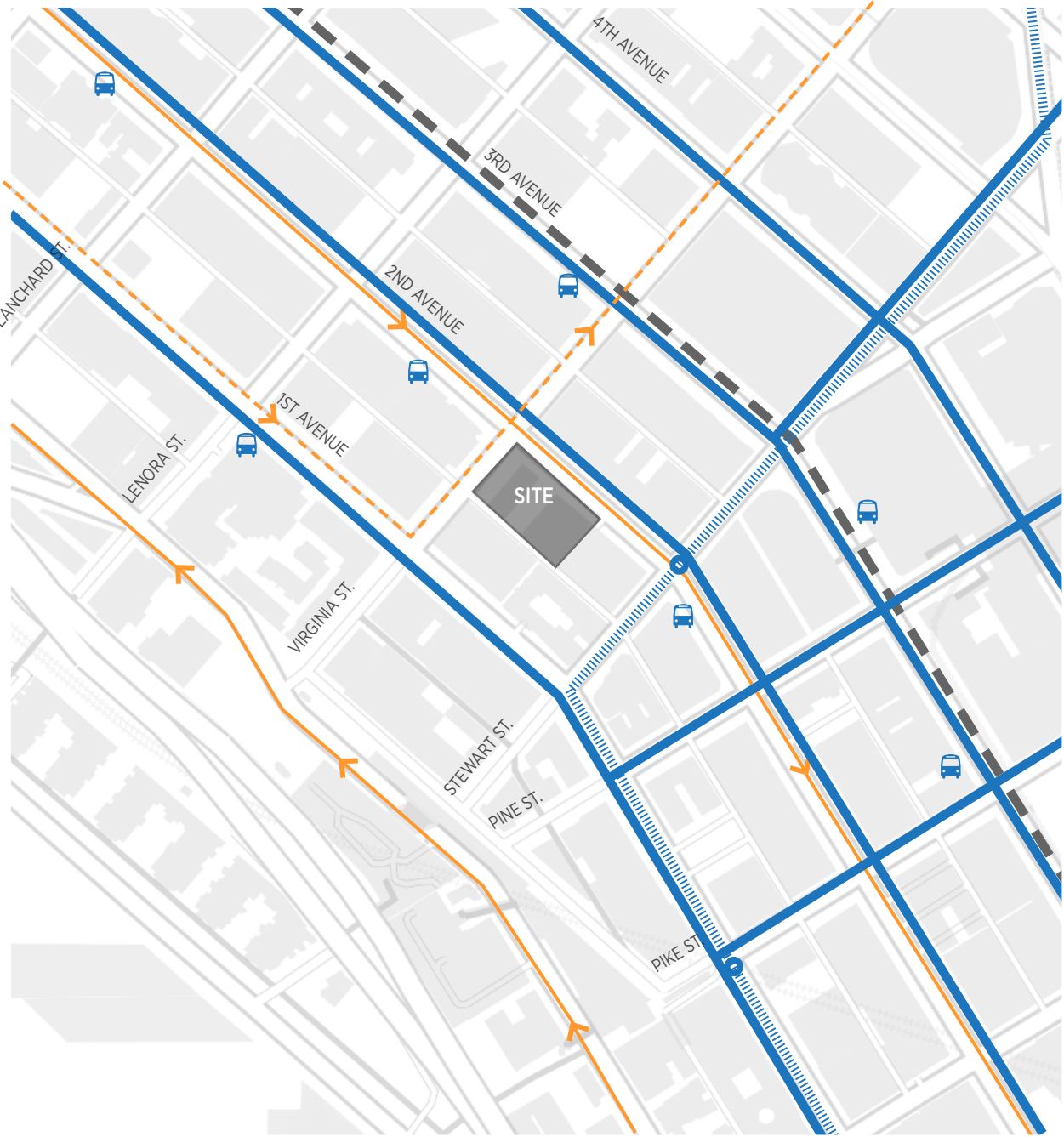
In DMC zones, the maximum facade width for portions of a building above 85 feet along the general north/south axis of a site (parallel to the Avenues) shall be 120 feet or 80 percent of the width of the lot, whichever is less.

05 CONTEXT ANALYSIS



5 Street Characteristics

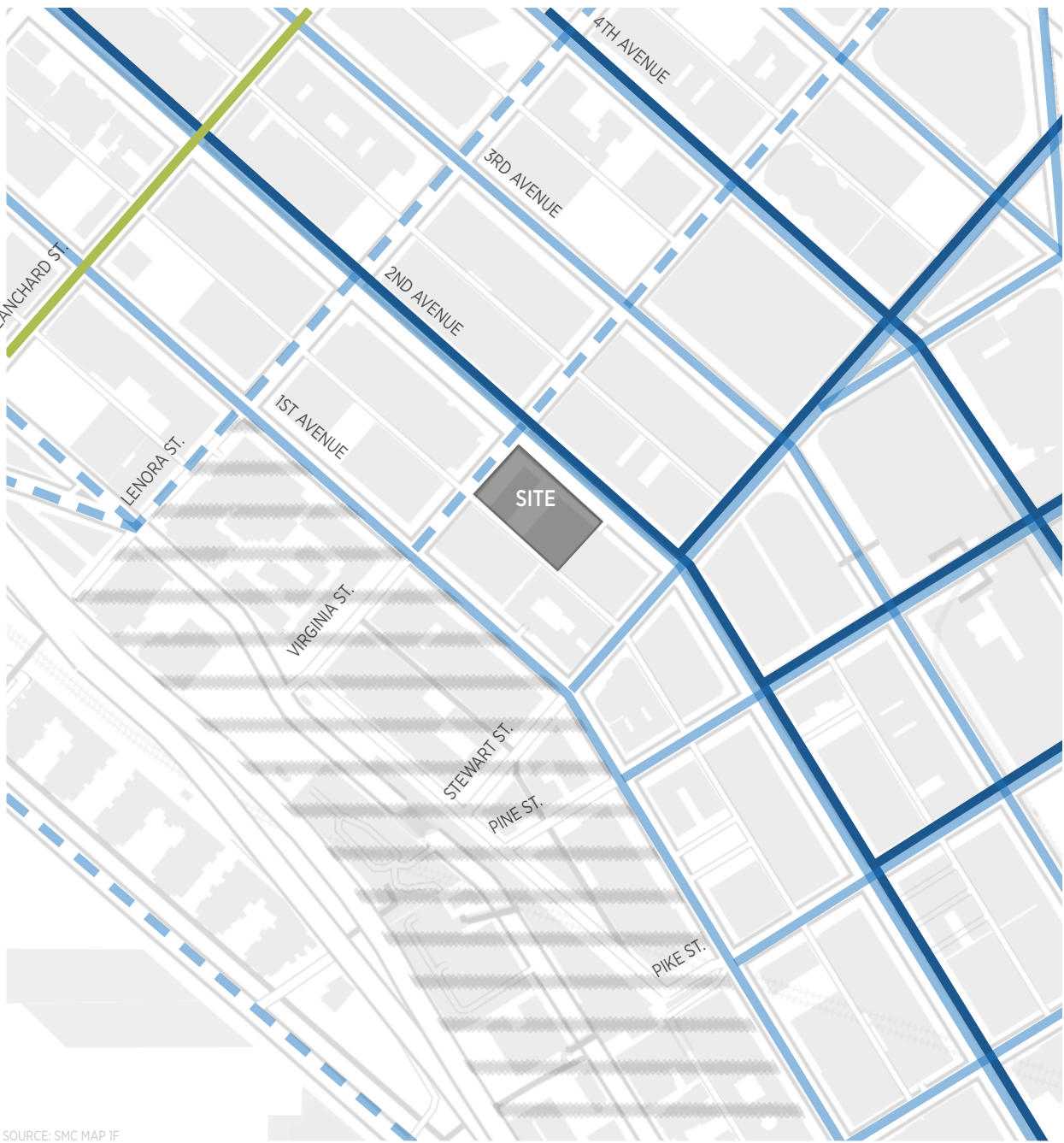
Public Transit



- Bike Lane
- Sharrow
- Bus Route
- Major Bus Stops
- Future Streetcar Station
- Future Streetcar Line
- Bus Rapid Transit



Pedestrian Street Classification



SOURCE: SMC MAP 1F

- Class I Pedestrian
- Class II Pedestrian
- Principal Arterial
- Neighborhood Green St.

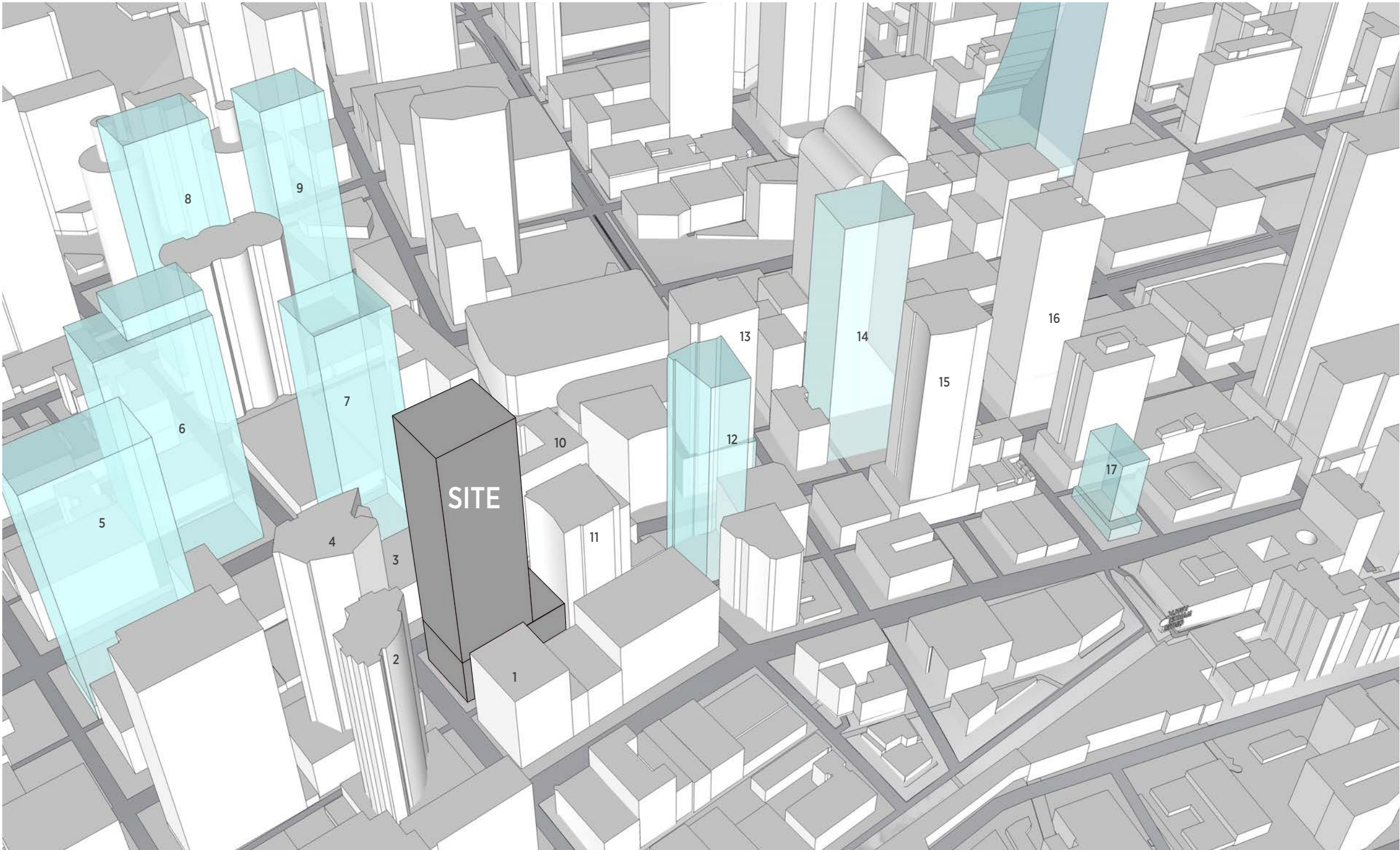


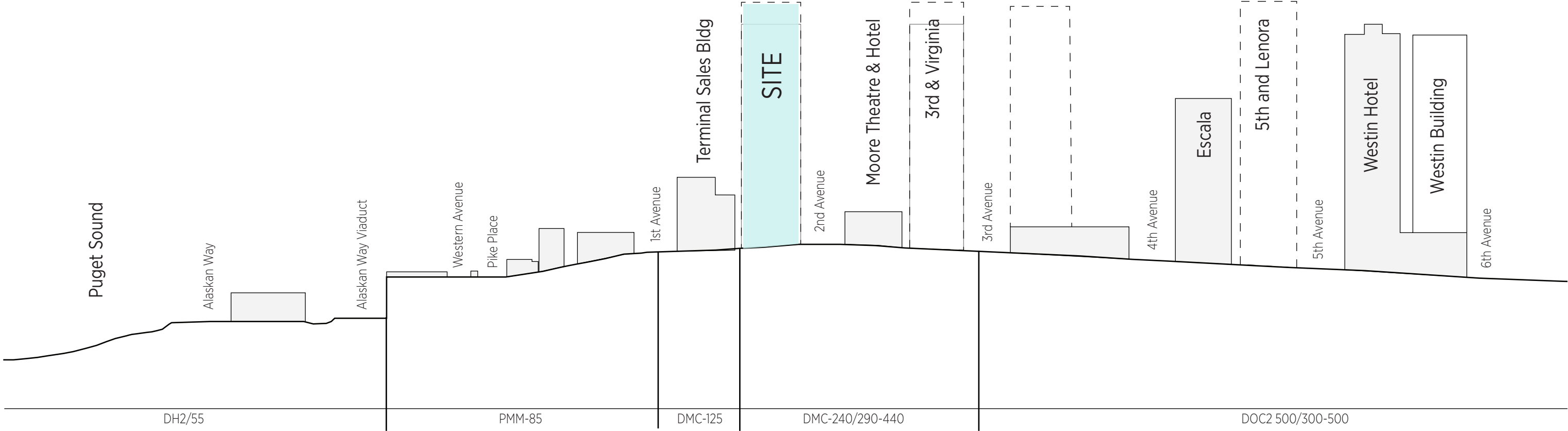
- Bar/Nightlife
- Restaurants
- Office
- Residential
- Retail
- Hotel

5 Changing Fabric

Current and Existing Neighbors

- 1. Terminal Sales Building, Office (132 ft)
- 2. One Pacific Tower, Condominium (259 ft)
- 3. Moore Theatre & Hotel (73 ft)
- 4. Tower Twelve, Condominium (440 ft)
- 5. [MUP 3018686](#) 3rd and Lenora, Future Residential & Office (360 ft)
- 6. [MUP 3026416](#) 3rd and Virginia, 2000 3rd Avenue , Future Residential (484 ft)
- 7. [MUP 3023678](#) 1927 3rd Avenue, Future Residential (484 ft)
- 8. [MUP 3019699](#) 5th and Virginia, Future Hotel & Residential (525 ft)
- 9. [MUP 3018037](#) 1903 5th Avenue, Future Hotel & Residential (500 ft)
- 10. Josephinum, Residential (170 ft)
- 11. Viktoria Apartments, Residential (265 ft)
- 12. [MUP 3016702](#) 2nd and Stewart, Future Residential (400 ft)
- 13. [MUP 3014773](#) 2nd and Pine, Residential, (400 ft)
- 14. [MUP 3032531](#) 1516 2nd Avenue (484 ft)
- 15. 1521 2nd Avenue, Condominium (400 ft)
- 16. 2nd and Pike Residential (400 ft)
- 17. [MUP 3028428](#) 1st and Pike, Future Hotel (160 ft)





CONTEXT ANALYSIS

5 Historic Landmarks



1. Palladian Hotel, 2000 2nd Ave. - Designated Landmark



2. Moore Theatre & Hotel, 1932 2nd Ave - Designated Landmark



3. Josephinum, 1902 2nd Ave - Designated Landmark



6. Terminal Sales Building, 1932 1st Ave - Designated Landmark



4. Haddon Hall, 1921 3rd Ave.



5. Doyle Building, 119 East Pine St. - Designated Landmark



7. Guiry Hotel 2104 1st Ave - Designated Landmark



8. Alaska Trade Building, 1917 1st Ave - Designated Landmark



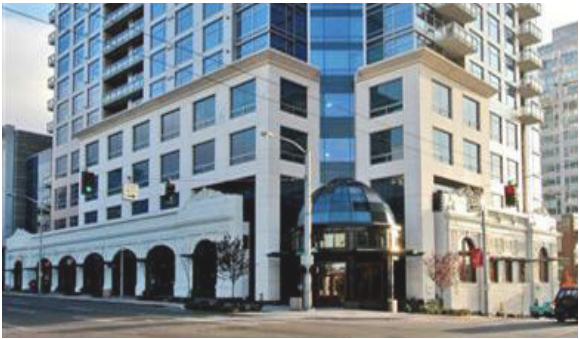
A. Viktoria Apartments, 1915 2nd Avenue (240 ft)



B. Tower Twelve, 2015 2nd Avenue (440')



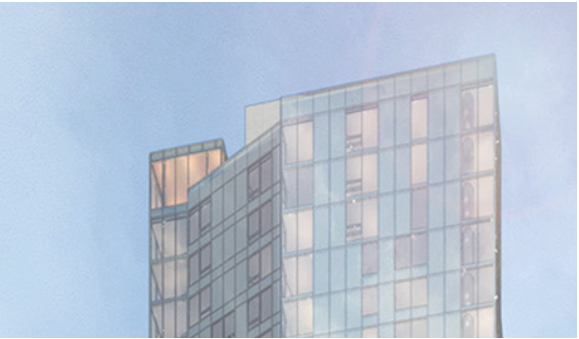
C. Second and Pine (440') [MUP 3014773](#)



D. 1521 Second Avenue (240')



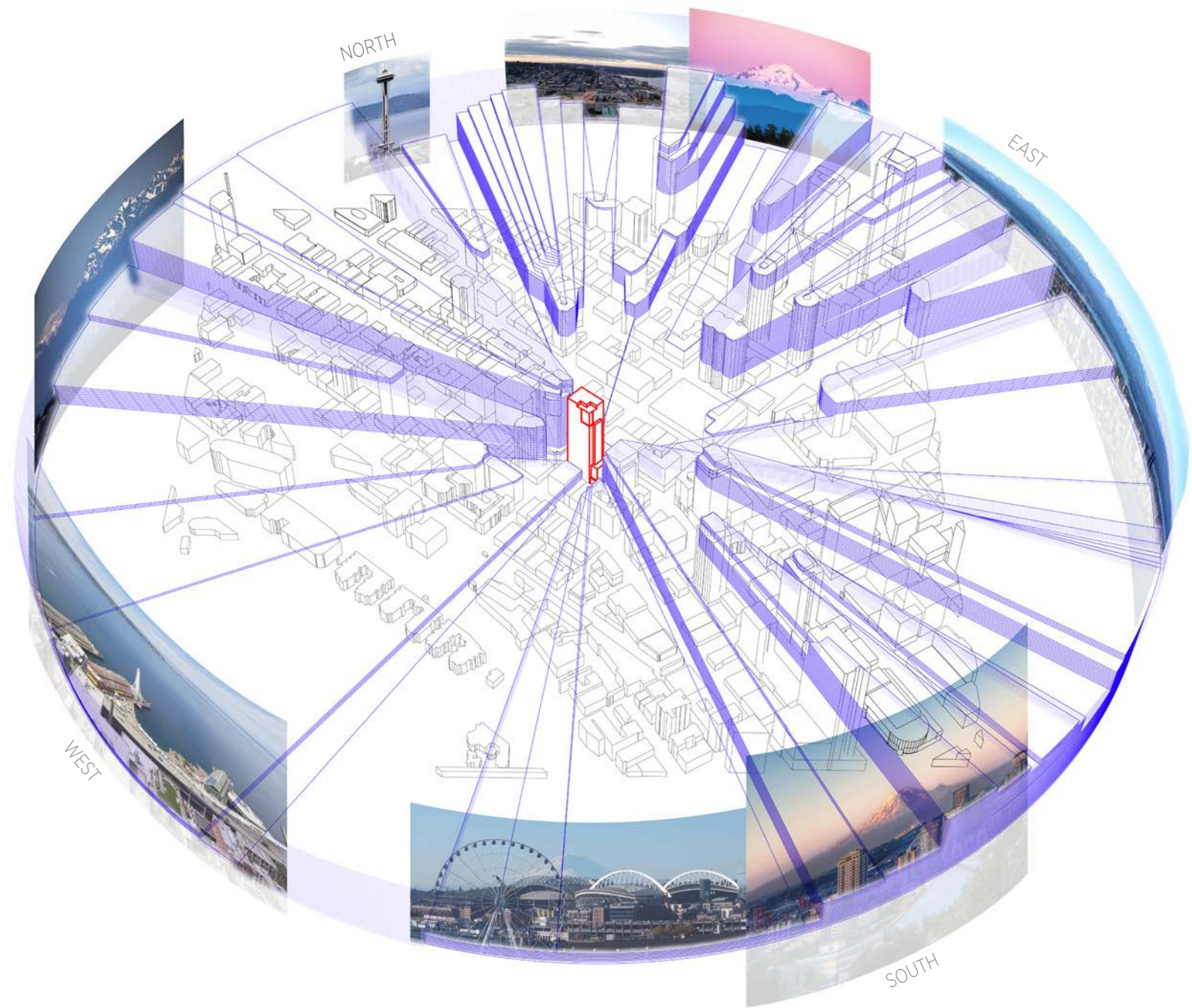
E. 1521 Second Avenue (440')

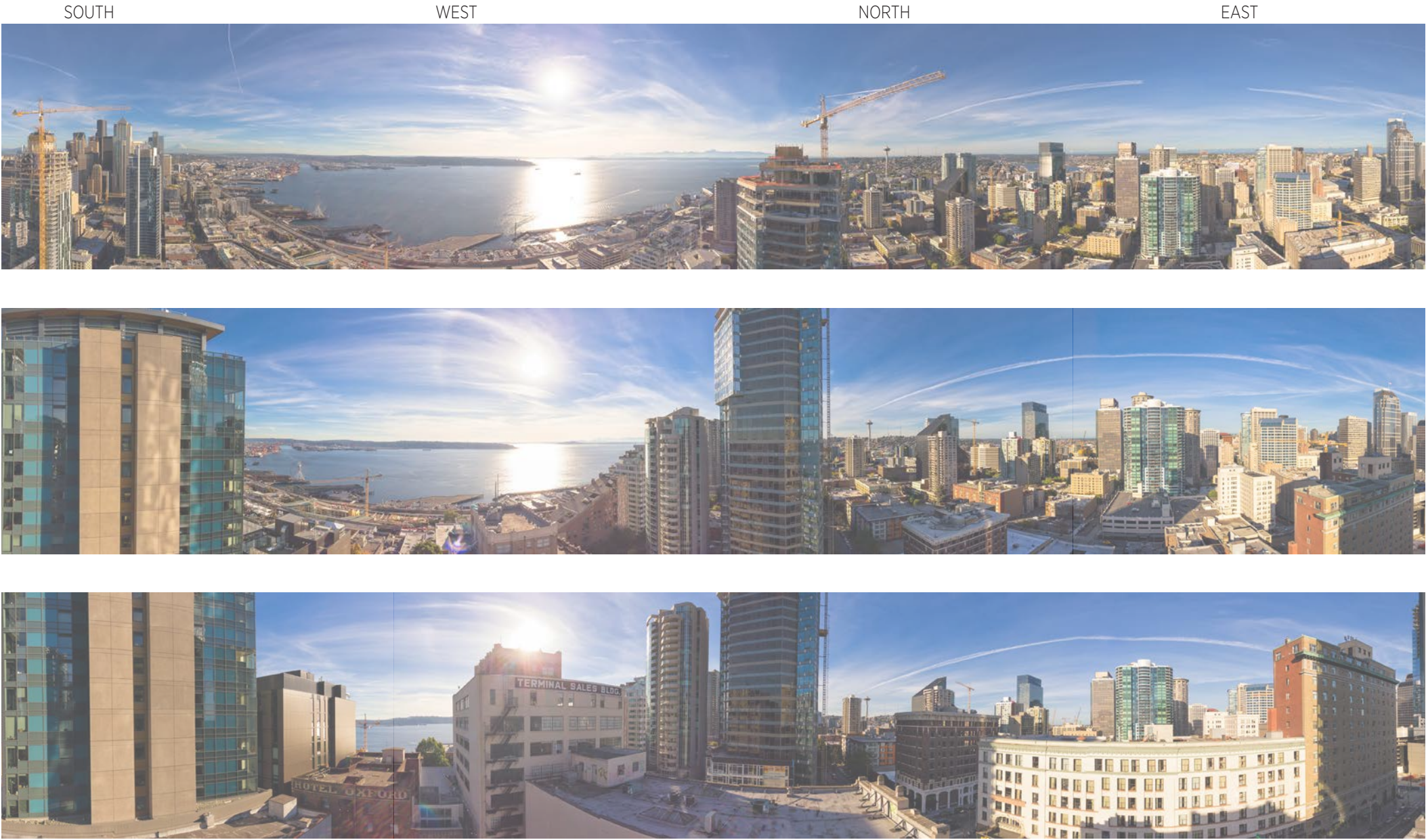
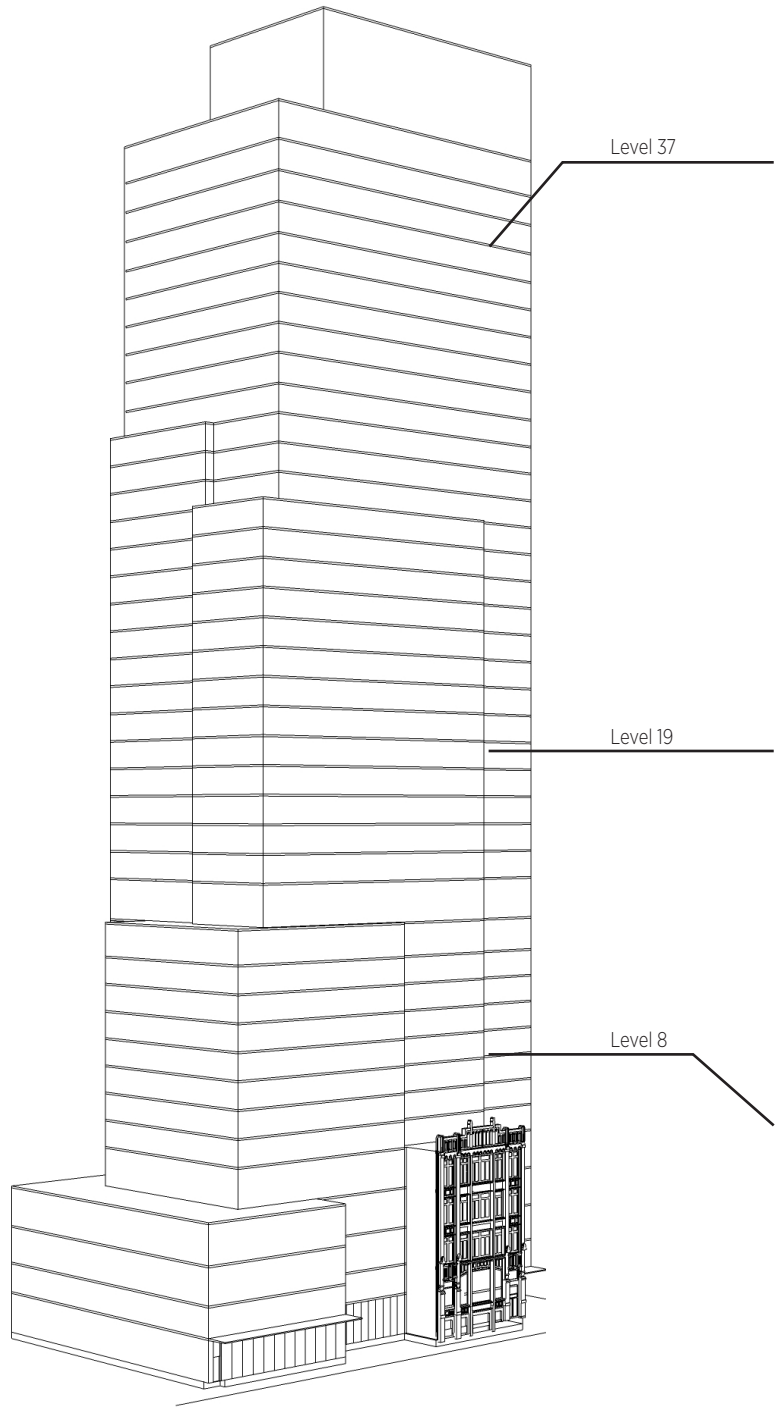


F. Second and Stewart (400') [MUP 3016702](#)



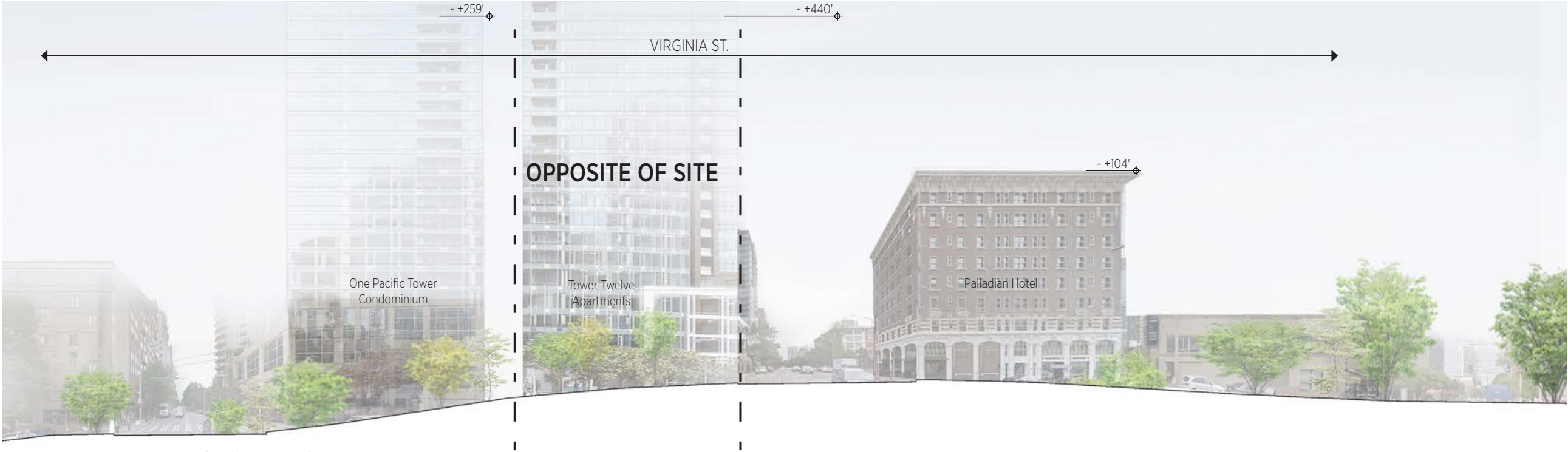
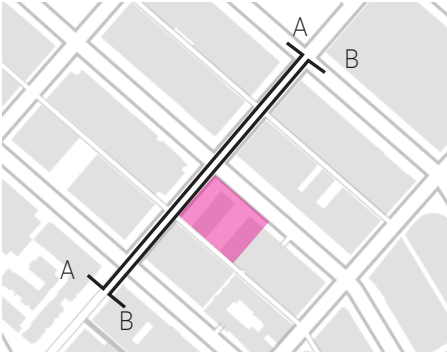
5 Views/Orientation





CONTEXT ANALYSIS

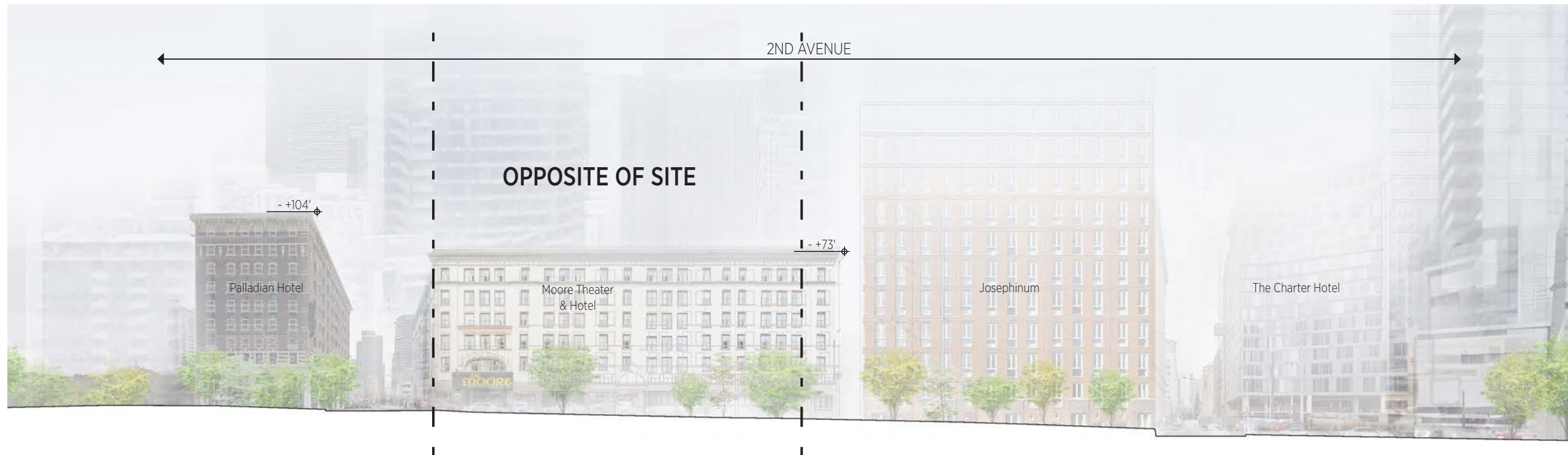
5 Streetscapes



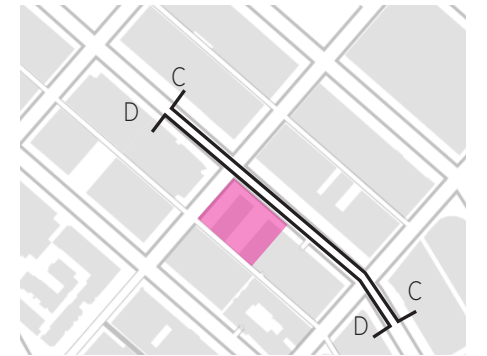
Section A-A, Virginia St., looking North



Section B-B, Virginia St., looking South



Section C-C, 2nd Ave., looking East



Section D-D, 2nd Ave., looking West

06 DESIGN GUIDELINES

Guidelines for Downtown Development 6

The following Guidelines for Downtown Development are relevant to the design of the preferred alternative for this project.

A - SITE PLANNING AND MASSING

A-1 Respond to the physical environment.

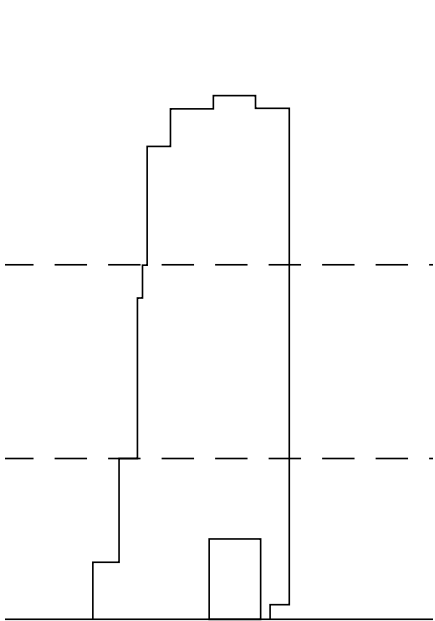
Develop an architectural concept and compose the building's massing in response to geographic conditions and patterns of urban form found beyond the immediate context of the building site.

A-2 Enhance the skyline.

Design the upper portion of the building to promote visual interest and variety in the downtown skyline.

RESPONSE

The proposed tower is designed around a concept of transitioning from the existing Bebb & Gould's Terminal Sales Annex (TSA) to the new high-rise living and hotel tower. This transition is apparent in the new tower's telescoping form, its ground level setbacks and its matching material palette, each of which serve to transitions from the past (the TSA) to the future (the new tower).



B - ARCHITECTURAL EXPRESSION

B-3 Reinforce the positive urban form & architectural attributes.

Consider the predominant attributes of the immediate neighborhood and reinforce desirable siting patterns, massing arrangements, and streetscape characteristics of nearby development.

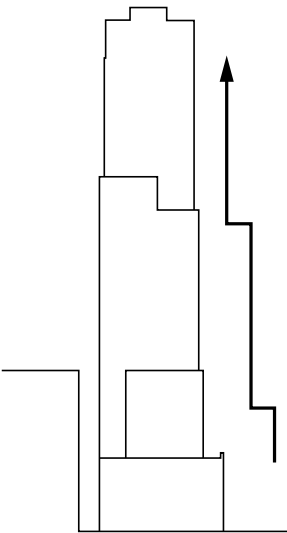
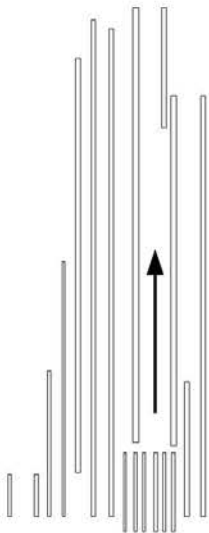
B-2 Create a transition in bulk and scale.

Compose the massing of the building to create a transition to the height, bulk, and scale of development in neighboring or nearby less intensive zones.

RESPONSE

The proposed tower is designed to celebrate the Terminal Sales Annex landmark and reinforce the strong Gothic and art deco heritage of Seattle's downtown stepped massings. The base of the building is scaled to relate to the passing pedestrian, while completing a corner of landmarks. The architectural language of the tower will reflect the best features of the surrounding neighborhood buildings - highly transparent, use of regulating lines to express verticality, and contextual harmony.

Low volumes at the street level complement the adjacent Landmark buildings creating a transition in scale to increasingly slender masses of surrounding buildings. A voluntary setback from the Terminal Sales Building Annex helps transition the scale along 2nd Avenue.



6 Guidelines for Downtown Development

C - THE STREETSCAPE

C-1 Promote pedestrian interaction.

Spaces for street level uses should be designed to engage pedestrians with the activities occurring within them. Sidewalk-related spaces should be open to the general public and appear safe and welcoming.

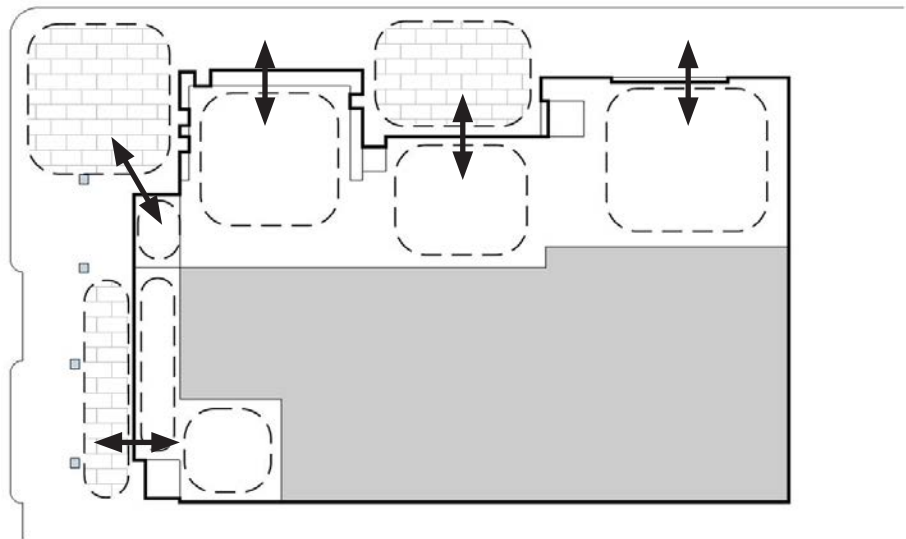
C-4 Reinforce building entries.

To promote pedestrian comfort, safety, and orientation, reinforce the building's entry.

RESPONSE

There are large setbacks and a high degree of transparency to highlight the major entrances and serve as places for pedestrian interaction: the 2nd Avenue cafe plaza, the corner plaza hotel entry and the Virginia Street drop-off and vehicle entry.

The highly activated ground floor is punctuated with several entries on 2nd Avenue. Transitioning to the steep slope of Virginia Street, the open plaza invites patrons into the main hotel entry. As the slope increases, a protected drop-off engages the building and doubles as a unique residential entry which also activates the alley.



D - PUBLIC AMENITIES

D-1 Provide inviting and usable open space.

Design public open spaces to promote a visually pleasing, safe, and active environment for workers, residents, and visitors. Views and solar access from the principal area of the open space should be especially emphasized.

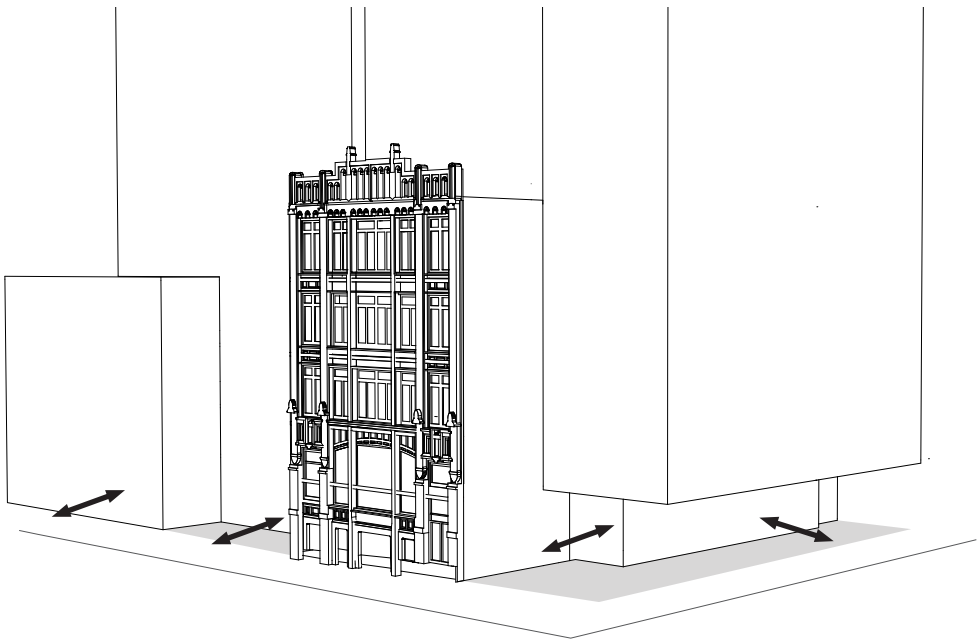
D-6 Design for personal safety & security.

Design the building and site to enhance the real and perceived feeling of personal safety and security in the immediate area.

RESPONSE

Plaza's create inviting moments before transitioning to the interior.

There is a high degree of transparency and functional lighting at the street level to enhance the security around the building. Clear signage and safe waiting areas for transportation will also make the site a safer environment.



LANDMARK PRESERVATION BOARD PRIORITIES

- 1 Set Tower Back from Landmark

Setting the tower back highlights the Landmark Building by giving it necessary breathing room. This allows the Landmark to maintain its presence in the neighborhood.
- 2 Express Volume of TSA through Building

The design should imply the sense of the building in the function of the space.
- 3 Match Existing Levels of Landmark

Floors levels should remain the same behind the Landmark. These elevations are important at the windows.
- 4 Retain Existing Entry Door of Landmark

The Landmark building should retain its existing entry.
- 5 Create Active Uses within Landmark

There should be active uses filling the space within the Landmark.
- 6 Tower Expression should draw inspiration from Landmark

There is opportunity to reference the existing massing and embrace what came before. This can be done by using the Terminal Sales Annex to set up the alignments of the tower above.
- 5 Landmark to Complete Corner of Landmark Structures

An open corner exposes the landmark to the corner which is defined by other existing landmarks, including Palladian Hotel and Moore Theater & Hotel.

07 PUBLIC OUTREACH

Site Walk 7

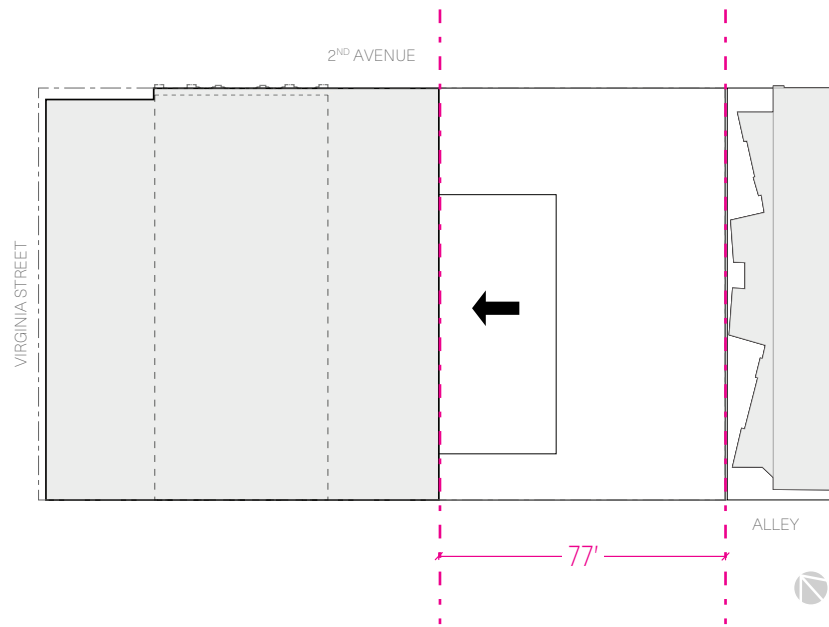


The design team held a site walk on Saturday, August 25th, 2018. The goal of this community meeting was to share development of the design with the area locals and provide an opportunity to collect their feedback. Posters were hung to advertise the event with access to a project hot-line.

Topics broached by attendees focused on a variety of themes, including the site programming, design, property safety, temporary interim uses and curb cuts. The neighborhood supported restaurants and music uses on the ground floors.

The community also mentioned the current congestion in the alley. Due to these concerns, the design team is proposing a drop-off along Virginia Street in lieu of congesting the alley even more with hotel patrons.

08 DESIGN CONCEPTS

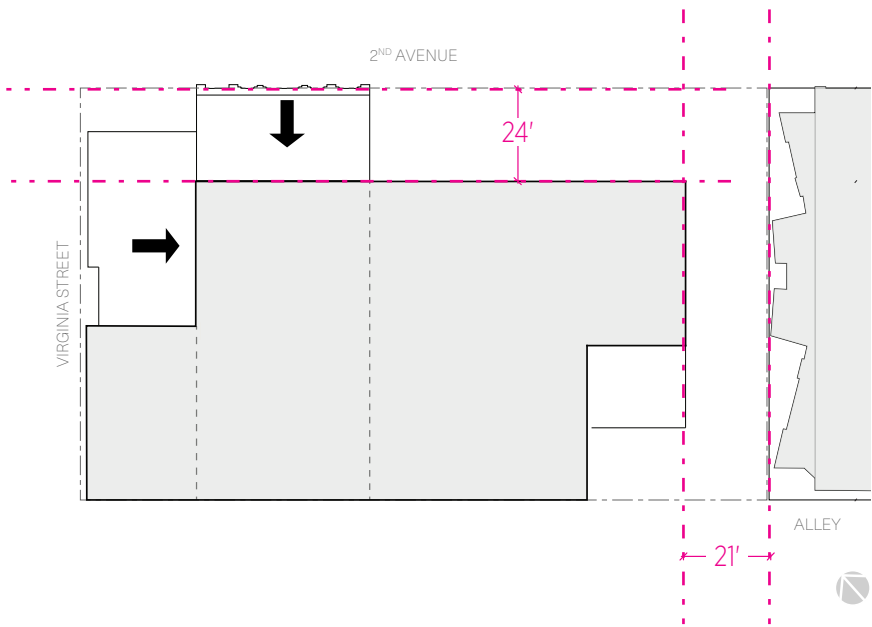


Concept 1 / Typical Tower Location (Code-Compliant)

The typical tower location maximizes tower spacing with Viktoria Apartments. The tower is pushed to the corner to respond to the height of Tower Twelve, creating a gateway on Virginia Street. This tower placement does not meet the priorities of the Landmark Preservation Board.

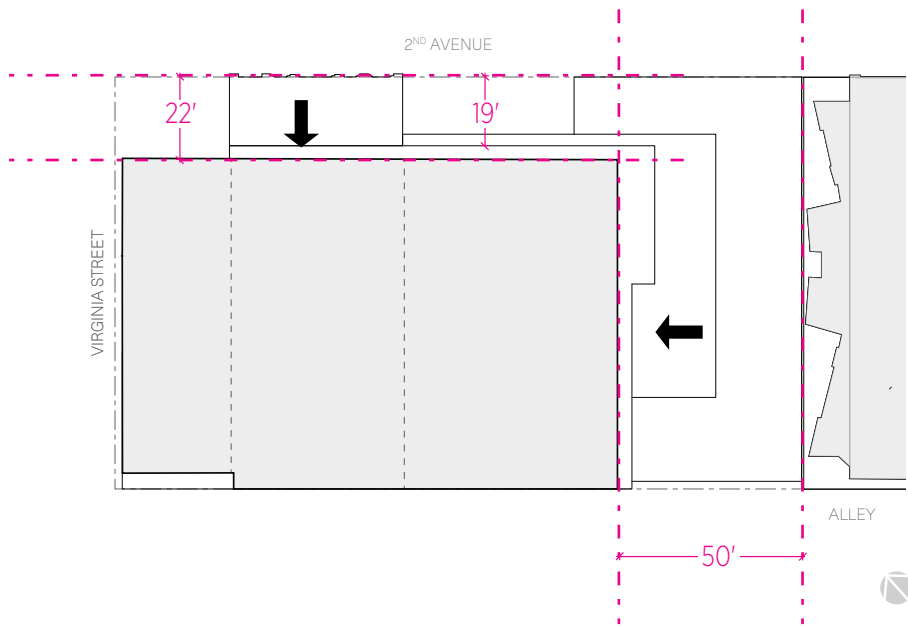
Tower Spacing Does Not Apply

SMC 23.49.058.F.1.d The 80' tower spacing requirement does not apply in this condition since Viktoria was developed prior to the 2006 Land Use Code.



Concept 2 / Reduce Impact on Landmark

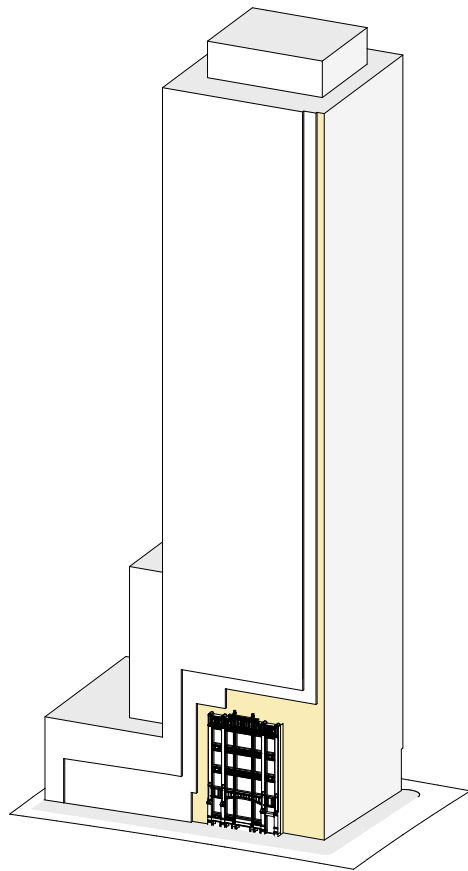
To better respond to the Landmark structure, the tower mass is pushed back from the Landmark and the corner opens up to further express the Landmark structure. This shift in the tower to express the Landmark lessens the tower spacing between Viktoria Apartments.



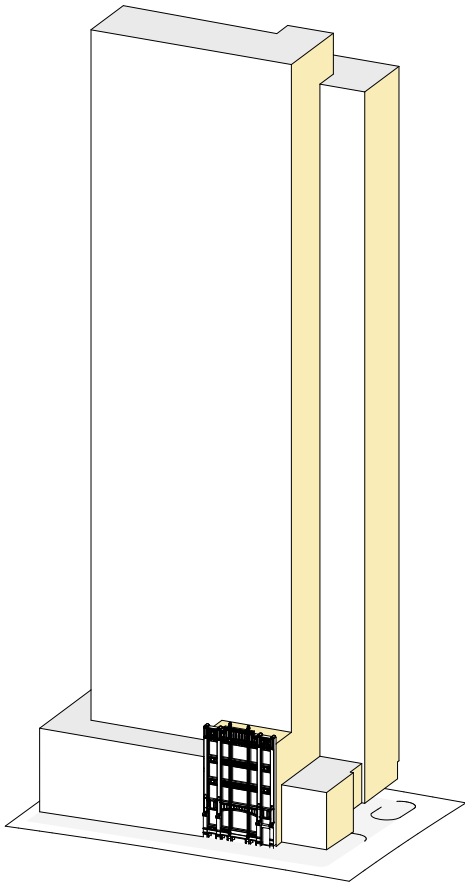
Concept 3 / Reduce Impact on Landmark and Tower Spacing

To combine options 1 and 2, the preferred tower placement steps the tower massing back away to find a balance between the Landmark and maintaining a good setback from Viktoria Apartments. It keeps the setback from the Landmark and continues the open corner concept to the ground level. This tower placement is most in line with the priorities of the Landmark Preservation Board.

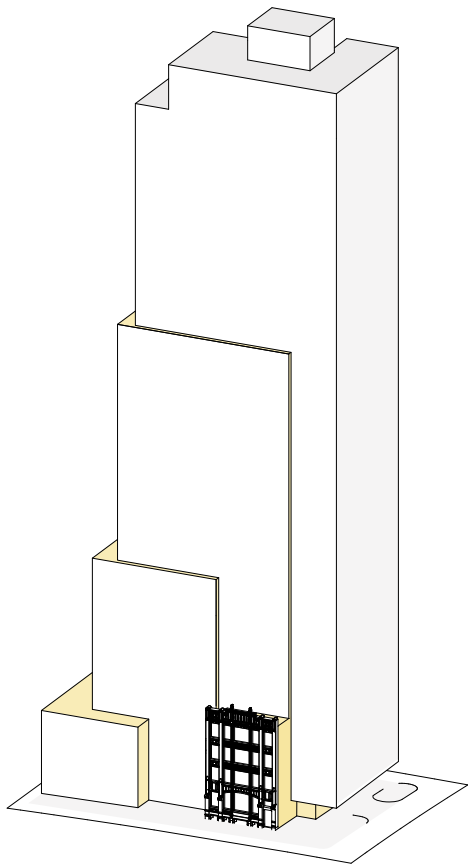
8 Massing Options Overview



CONCEPT 1: REVEAL
(CODE COMPLIANT)



CONCEPT 2: SHIFT



CONCEPT 3: GRADATION
(PREFERRED)

Massing Option Overview 8

- Good●
- Better●●
- Best●●●

| | CONCEPT 1 (CODE-COMPLIANT) | CONCEPT 2 | CONCEPT 3 (PREFERRED) |
|--|-------------------------------|-----------|--------------------------|
| DRB Design Guidelines | | | |
| A-1 Respond to the physical environment | ● | ● | ●●● |
| A-2 Enhance the skyline | ● | ● | ●●● |
| B-2 Create a transition in bulk and scale | ● | ●● | ●●● |
| B-3 Reinforce the positive urban form & architectural attributes | ● | ●● | ●●● |
| C-1 Promote pedestrian interaction | ● | ● | ●●● |
| C-6 Develop the alley facade | ● | ● | ●●● |
| D-1 Provide inviting and usable open space | ● | ● | ●●● |
| D-6 Design for Personal Safety & Security | ●● | ●● | ●●● |
| Landmark Preservation Board Priorities | | | |
| 01 Set tower back from Landmark | ● | ●● | ●●● |
| 02 Express volume of Landmark through the building | ● | ●● | ●●● |
| 03 Match existing levels of Landmark | ●●● | ●●● | ●●● |
| 04 Retain existing entry door of Landmark | ●●● | ●●● | ●●● |
| 05 Create active uses within Landmark | ●●● | ●●● | ●●● |
| 06 Tower expression should draw inspiration from Landmark | ● | ● | ●●● |
| 07 Landmark to complete corner | ● | ●● | ●●● |

This page intentionally left blank.

DESIGN CONCEPTS

8 Concept 1

Reveal (Code-Compliant)

Massing Concept 1 works within the existing zoning envelope. A simple volume is determined by providing more space for the neighboring Viktoria Apartments Building. The concept takes cues from contemporary massings in the neighborhood.

There is no opportunity for setback at the podium/street level to promote pedestrian interaction and public open space at a heavily -trafficked intersection. The Landmark's Board has also requested a substantial setback to provide a distinct relationship with the landmark.

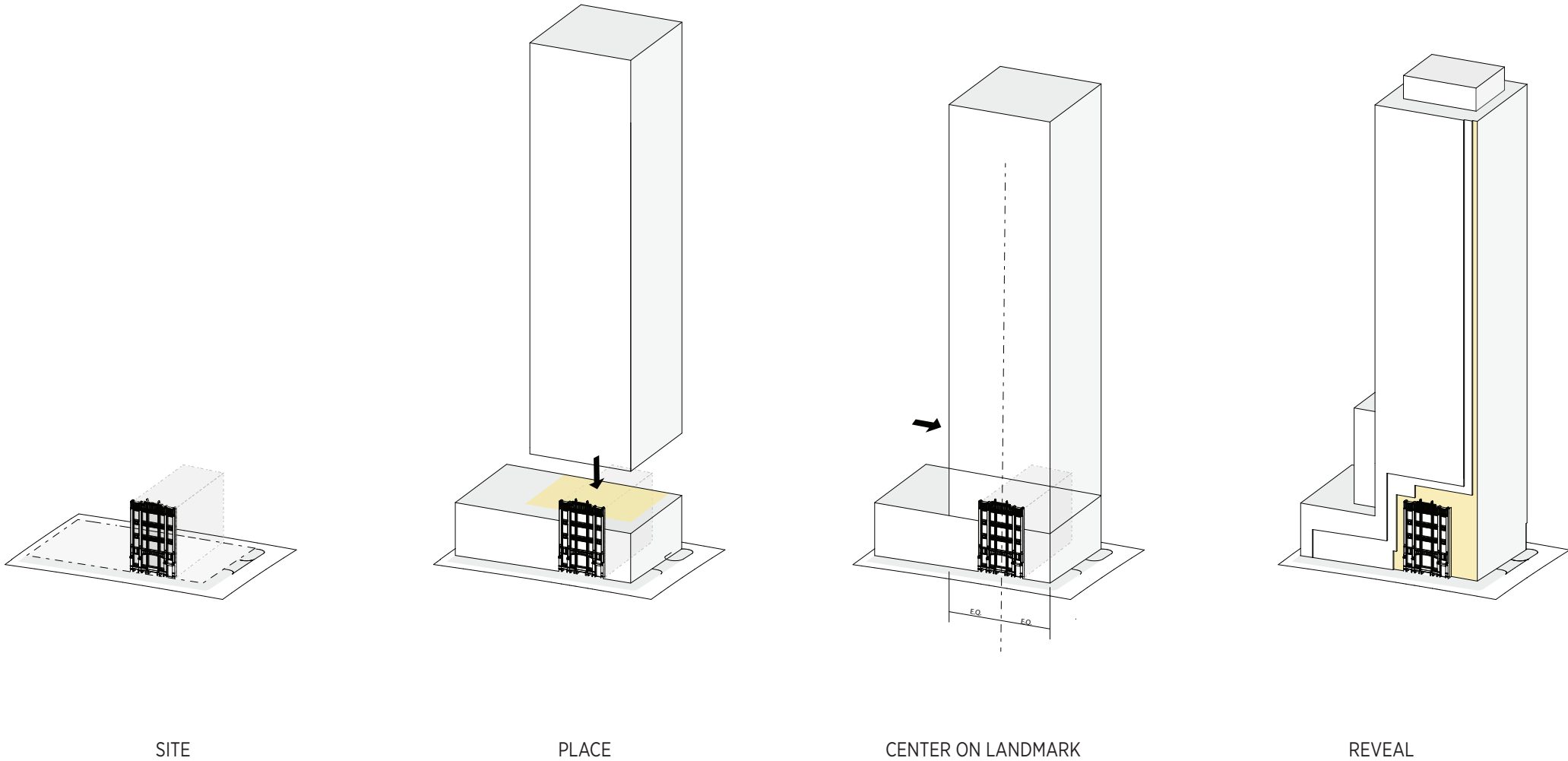
While Massing Concept 1 follows the zoning envelope and provides more space for Viktoria, it presents major challenges highlighting the landmark building and meeting desired design guidelines.

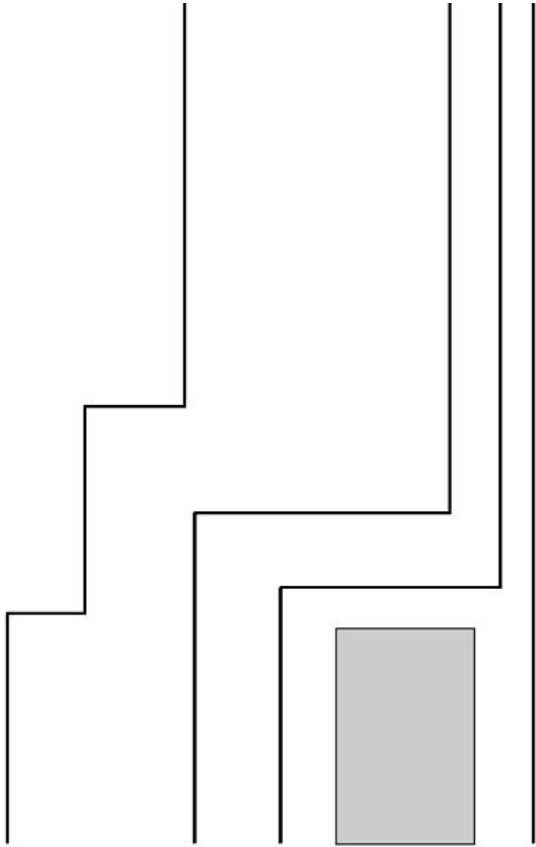
Design Guidelines:

- **A-1** Respond to the Physical Environment
- **D-6** Design for Personal Safety & Security

Landmark Preservation Board Priorities :

- **03** Match existing levels of Landmark
- **04** Retain existing entry door of Landmark
- **05** Create active uses within Landmark



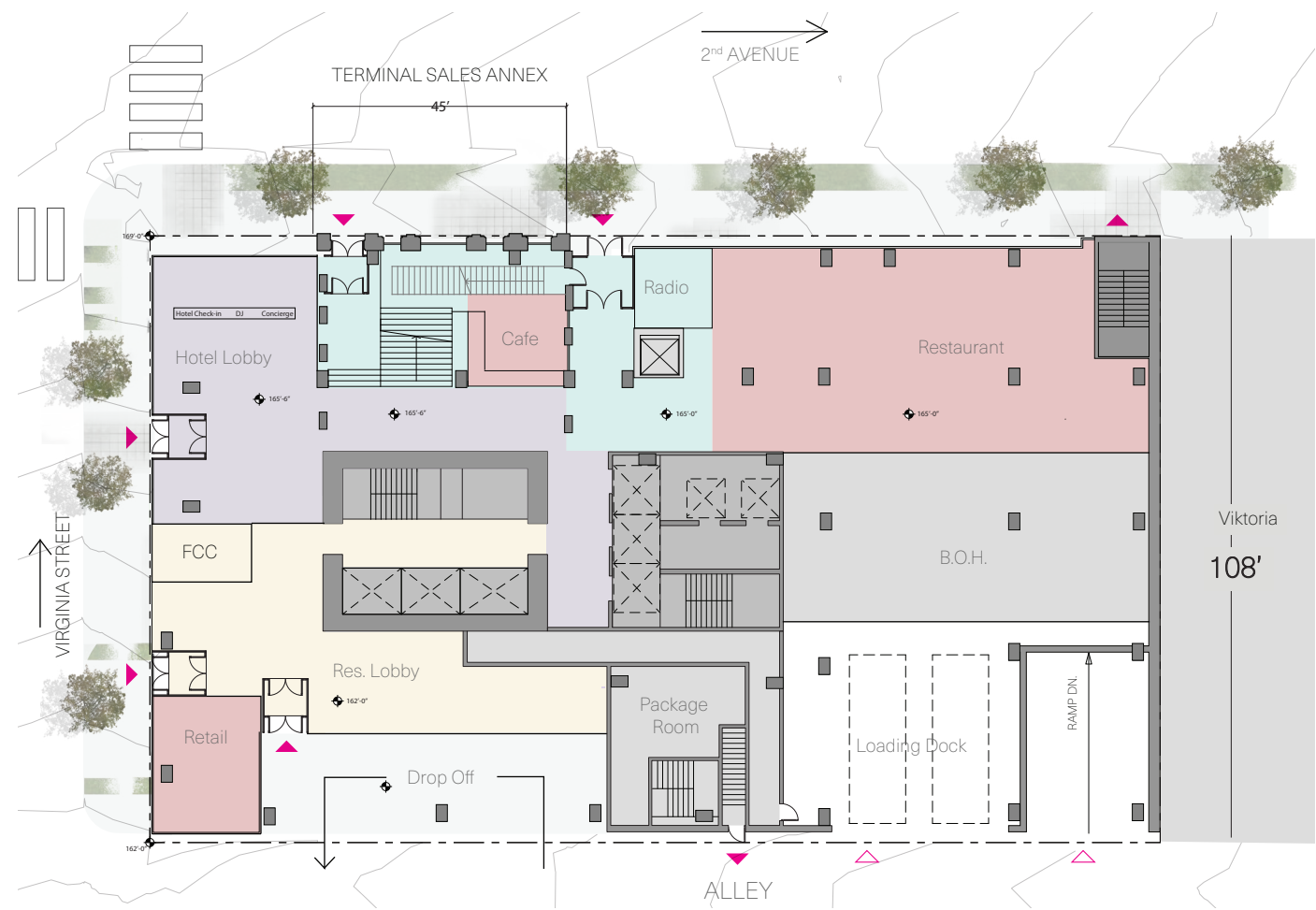


A reveal between past and future. As wood grains flow around a knot in a tree, the new building flows around the historic building. Knots form where branches are connected to the tree trunk, similar to how the historic building is connected to the new tower.



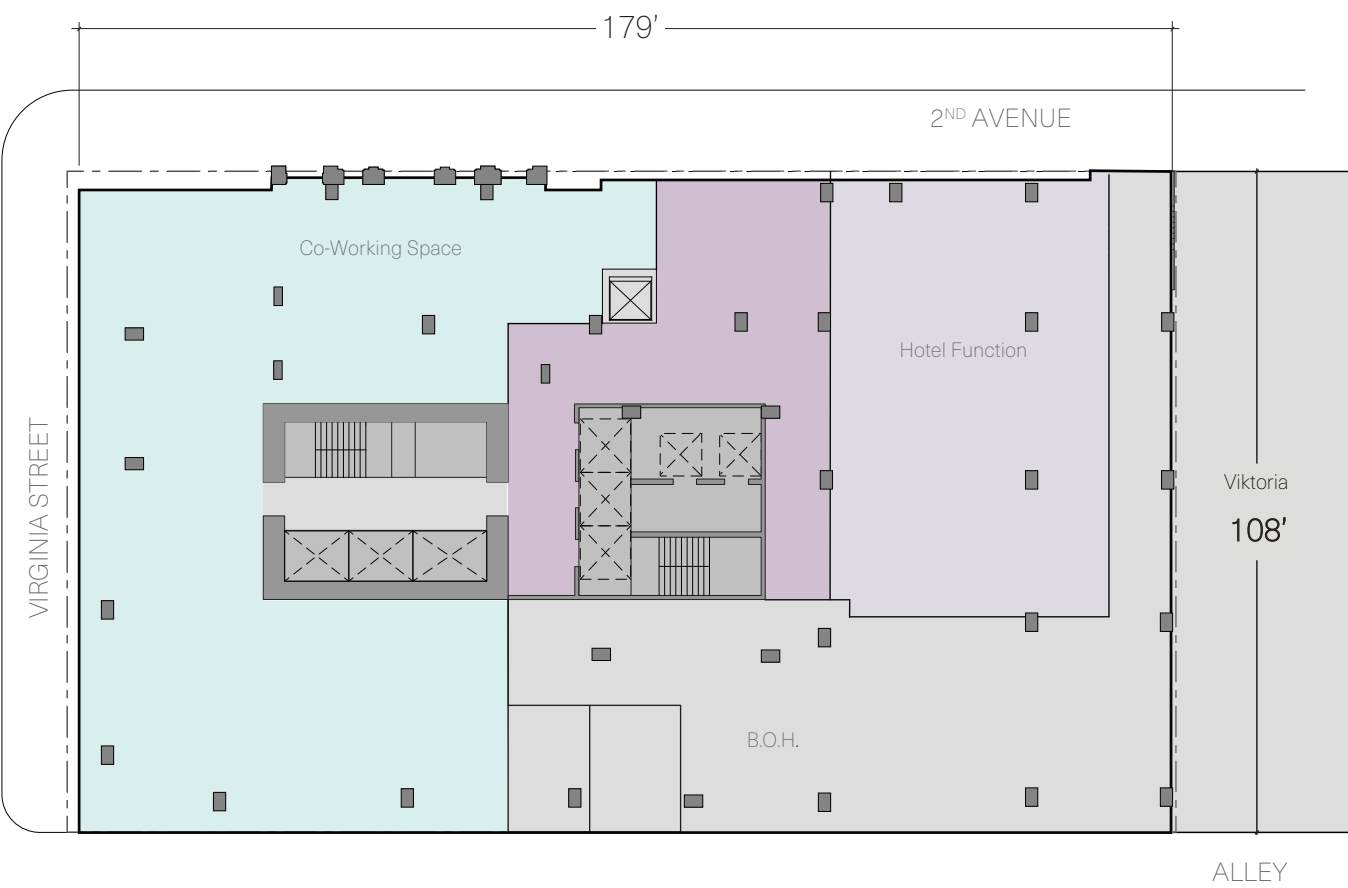
DESIGN CONCEPTS

8 Concept 1 - Plans

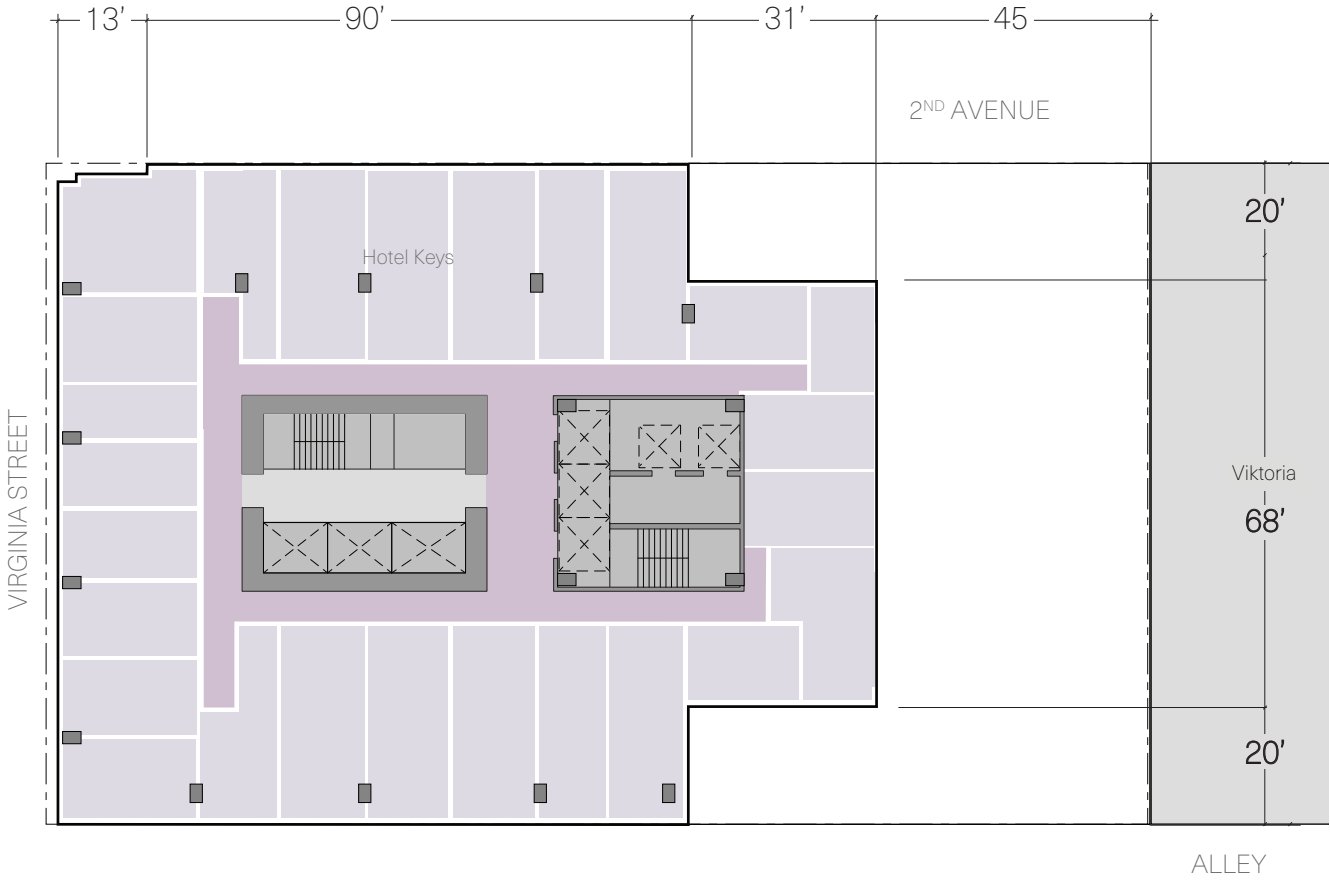


Ground Floor Plan

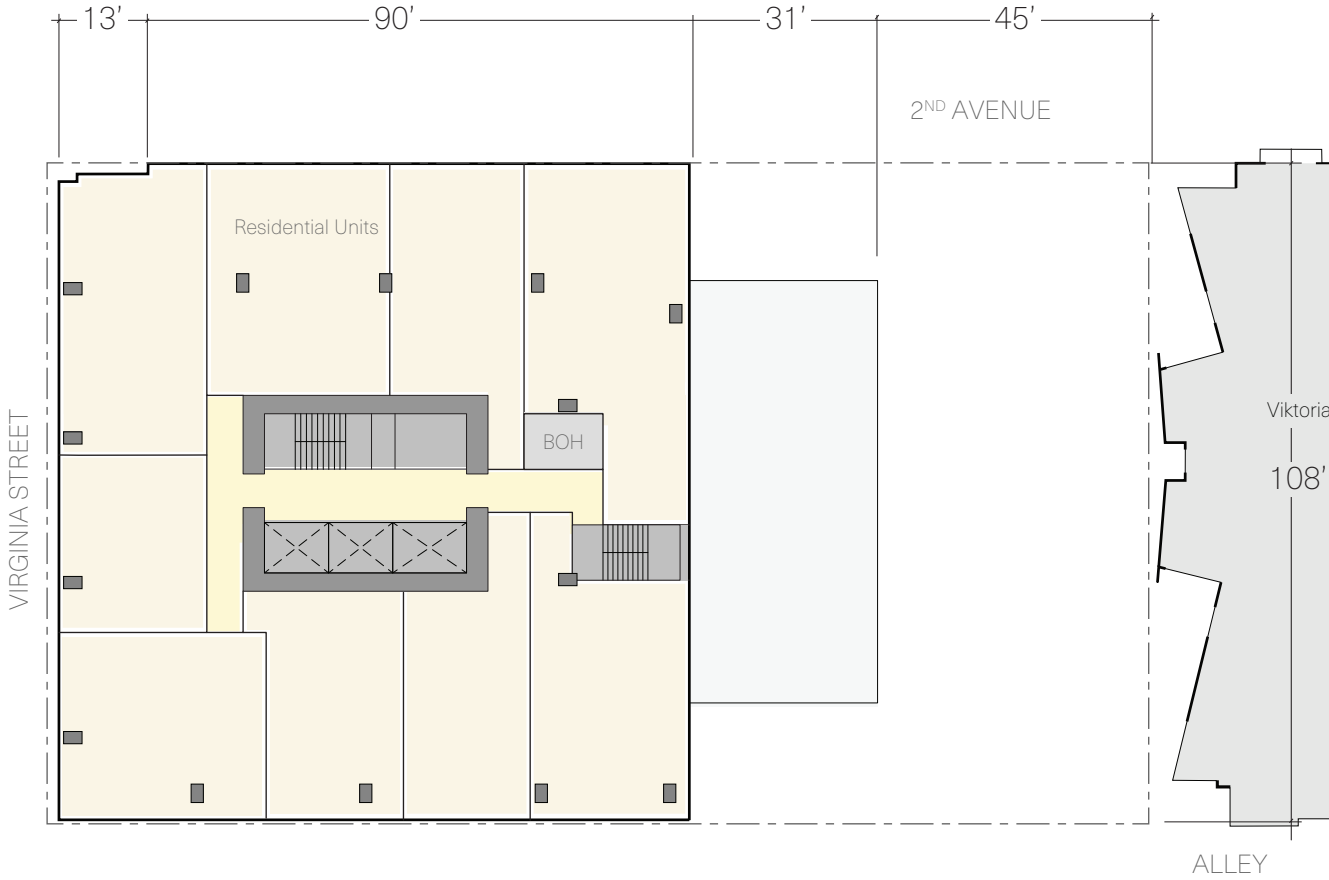
- COWORK
- HOTEL
- BOH
- RESIDENCE
- RESTAURANT
- PEDESTRIAN ENTRY
- VEHICULAR ENTRY



Podium Floor (L3)



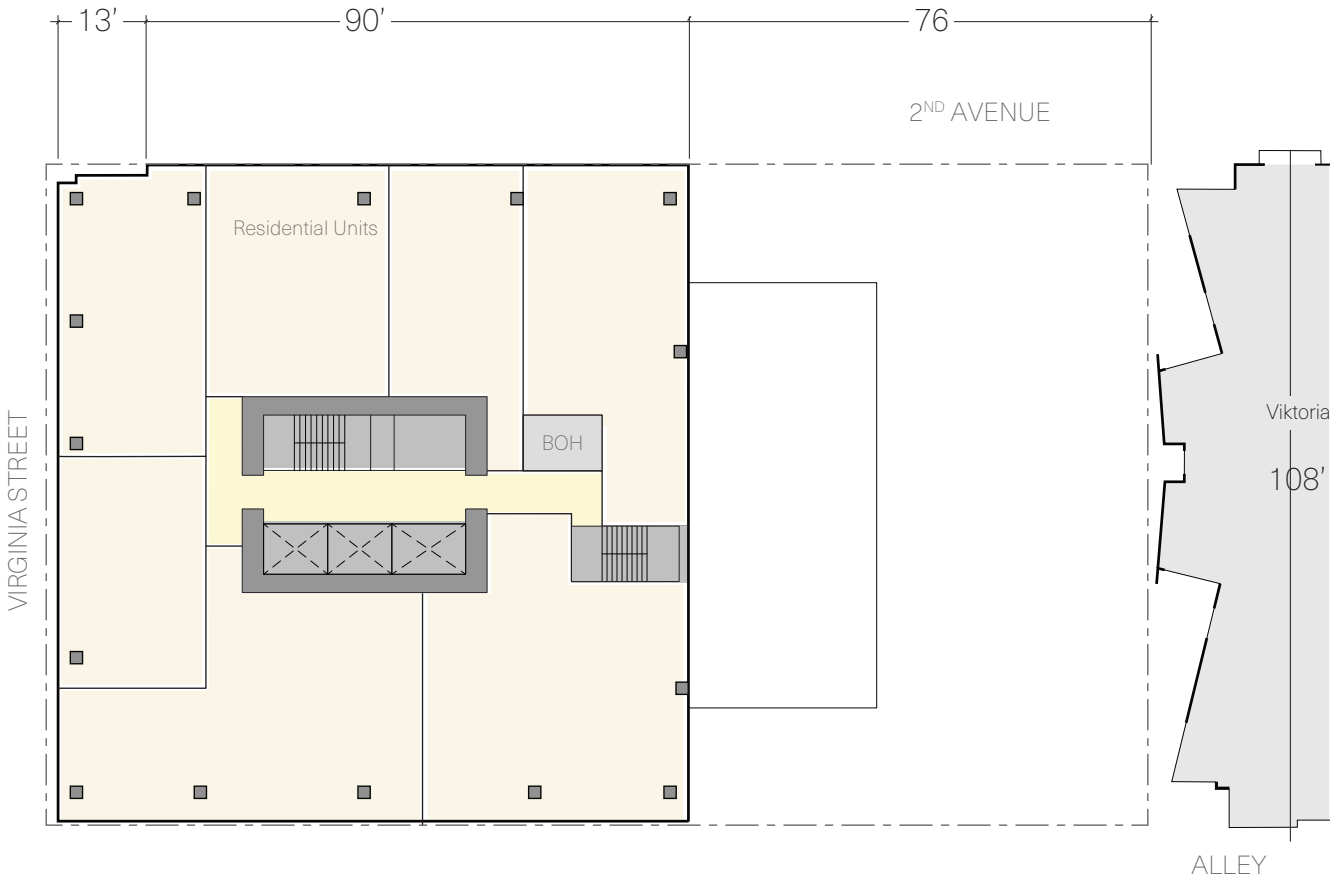
Typical Hotel Floor (L5-12)



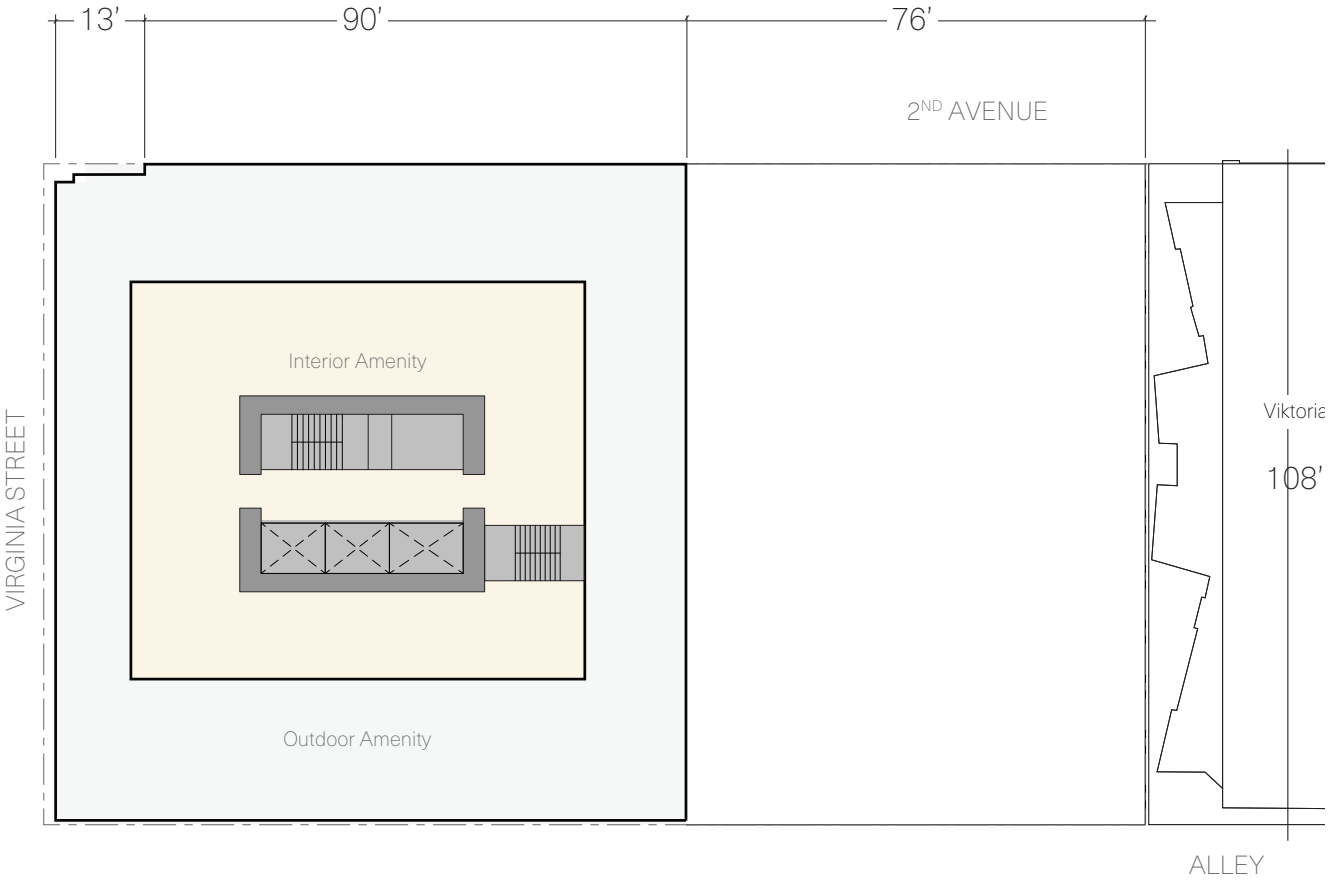
Residential Floor (L16-27)

DESIGN CONCEPTS

8 Concept 1 - Plans

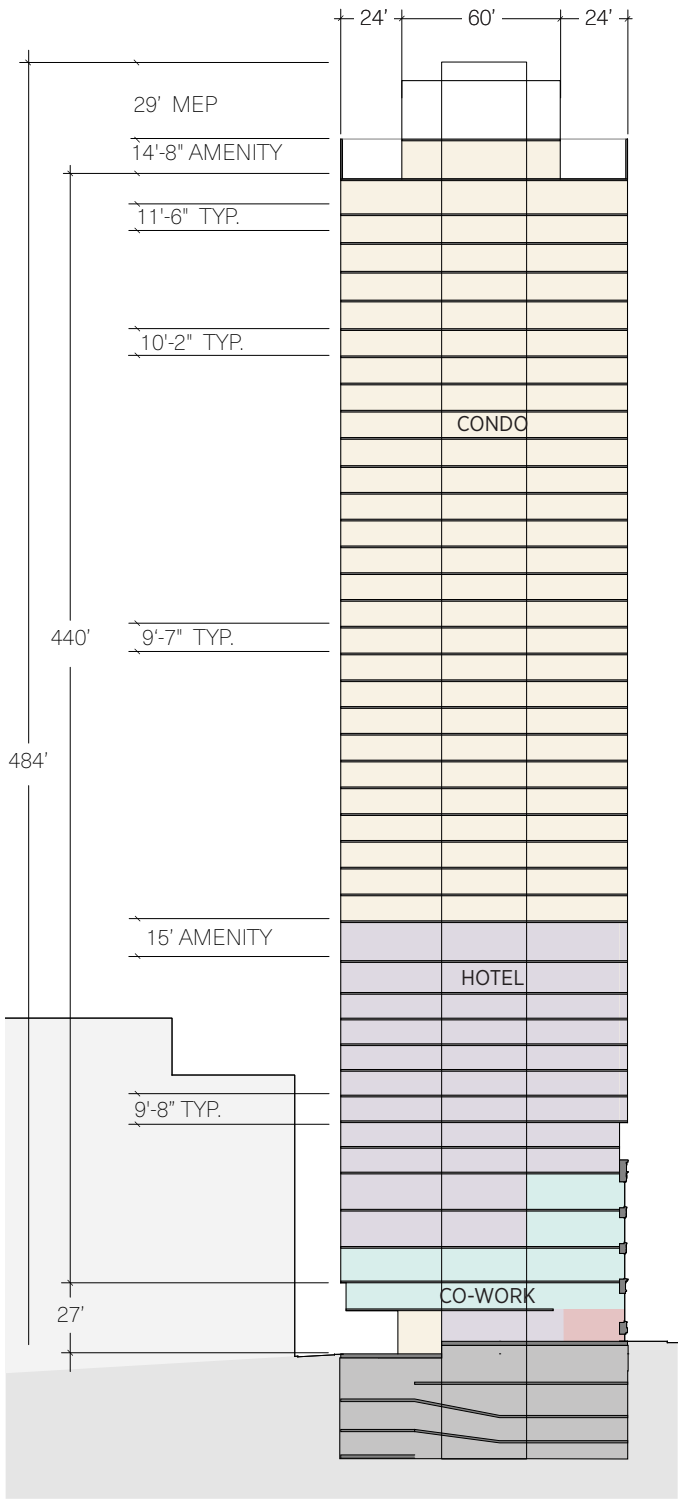


Residential Floor (L31-37)

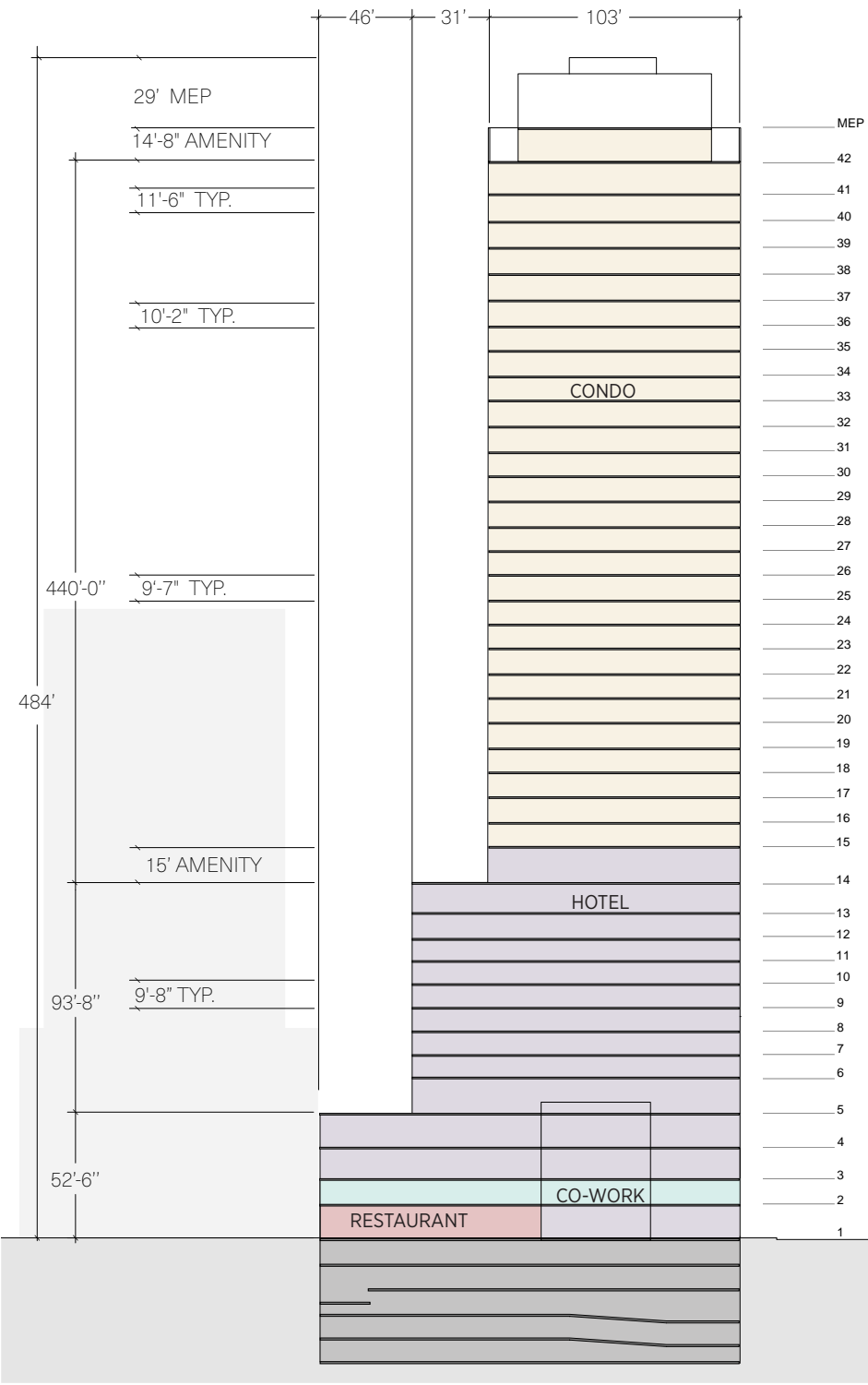


Rooftop Amenity Floor (L42)

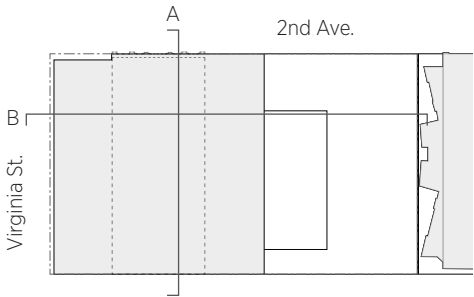
- COWORK
- HOTEL
- BOH
- RESIDENCE
- RESTAURANT
- PEDESTRIAN ENTRY
- VEHICULAR ENTRY



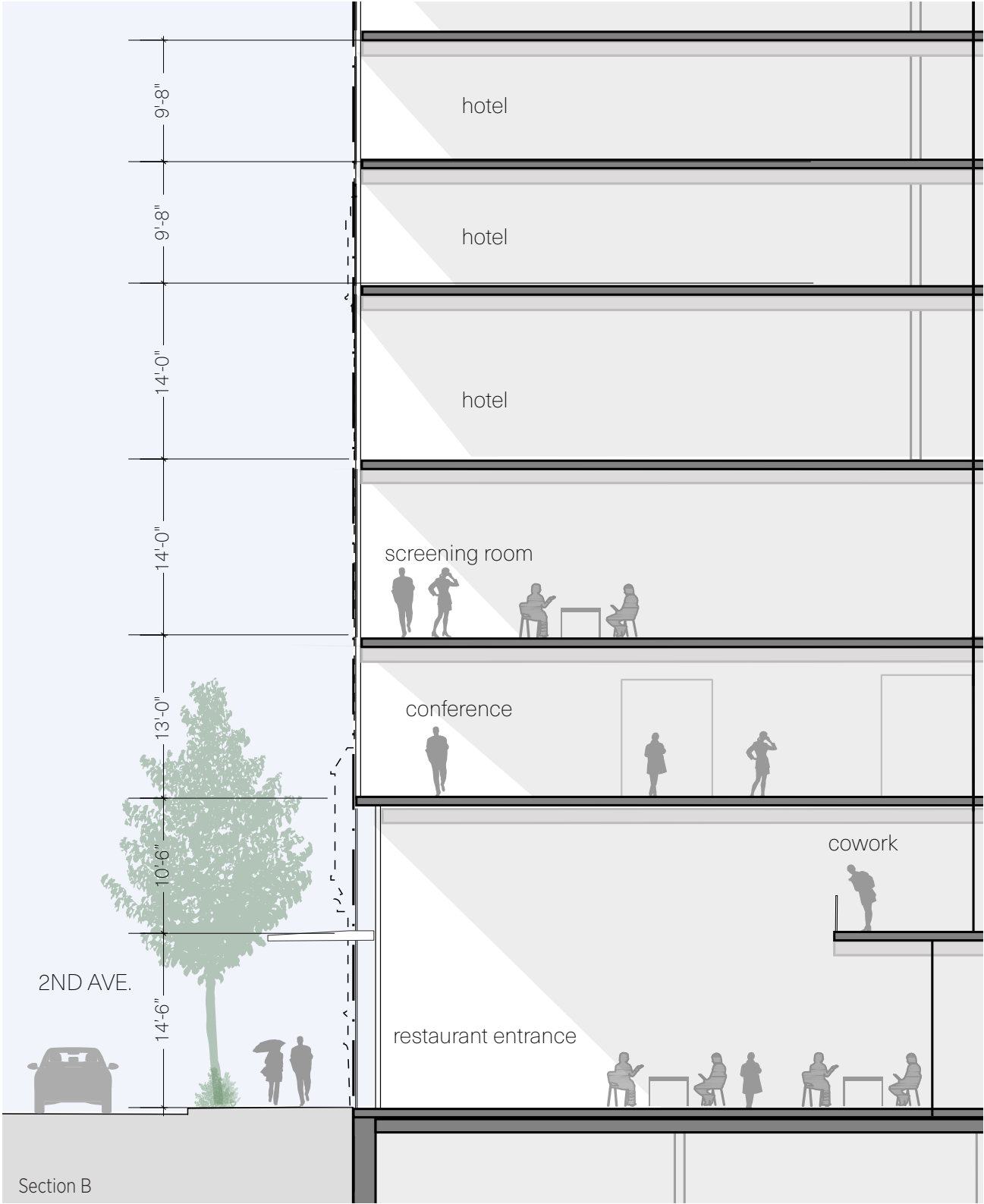
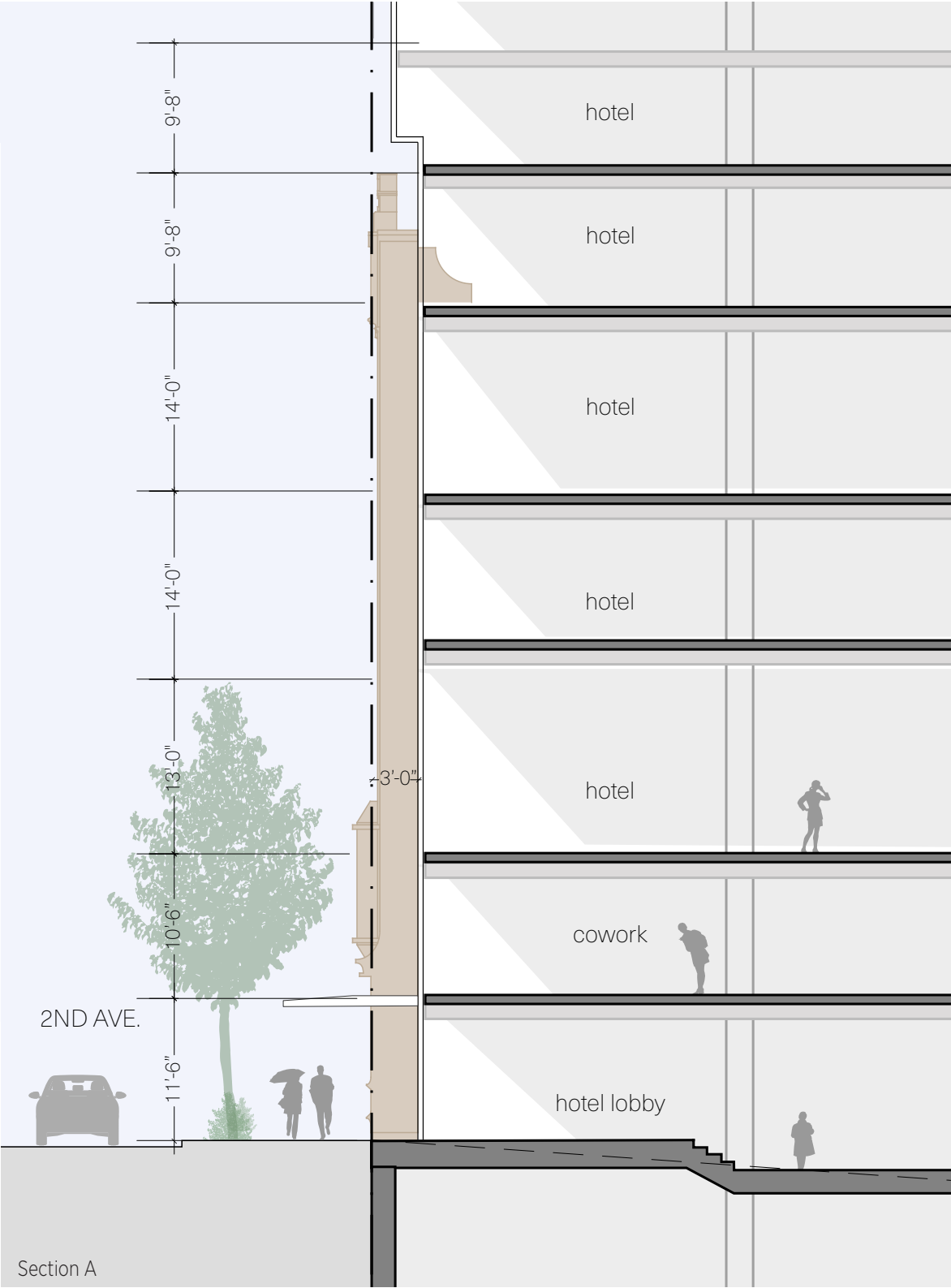
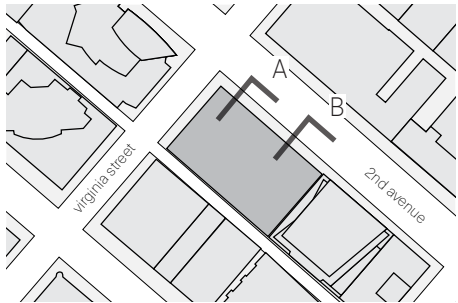
Section A



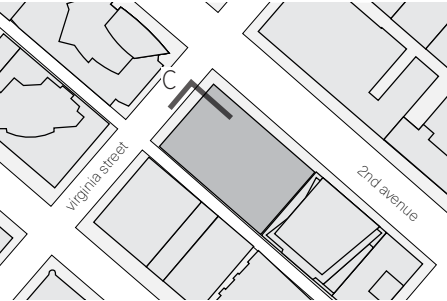
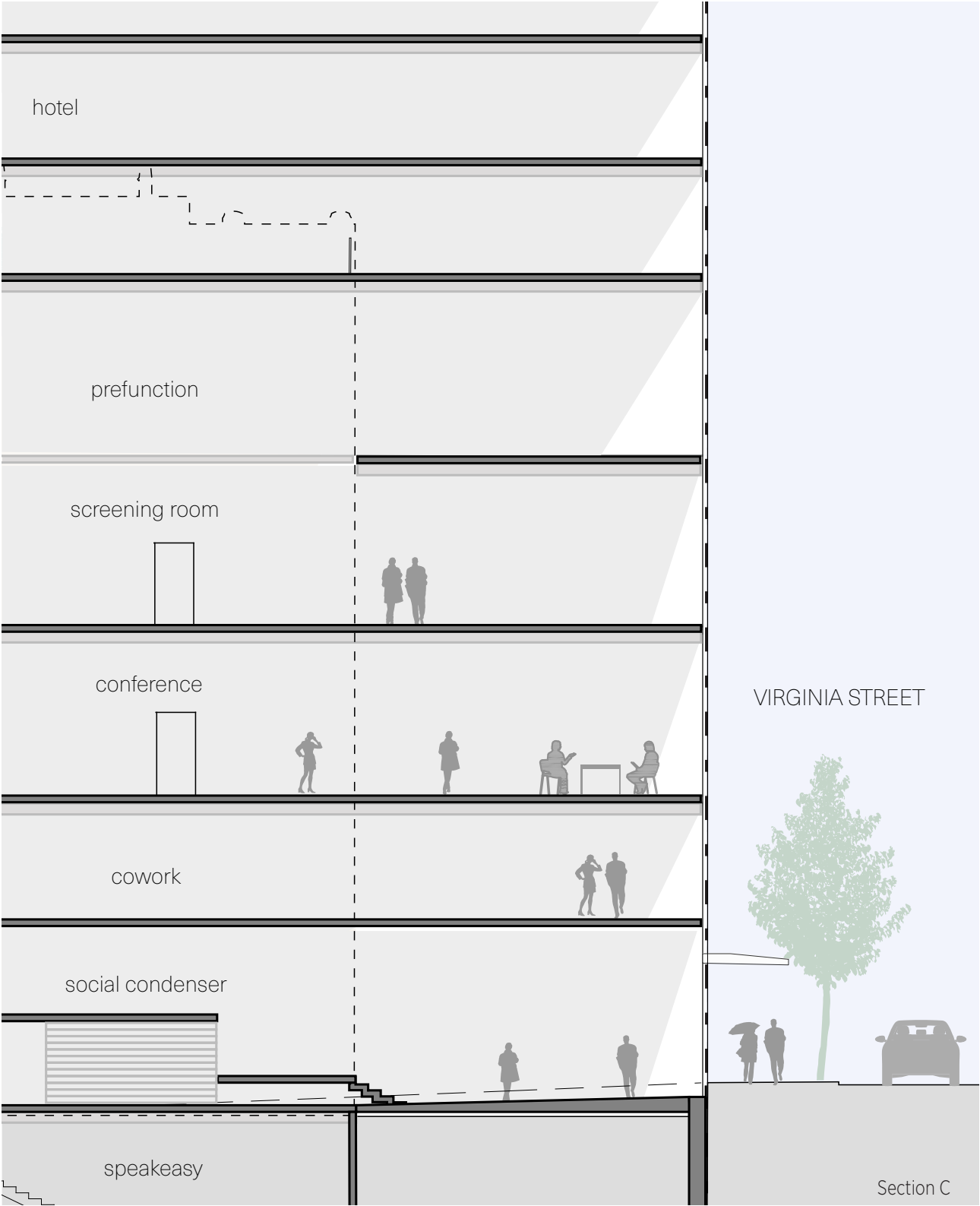
Section B



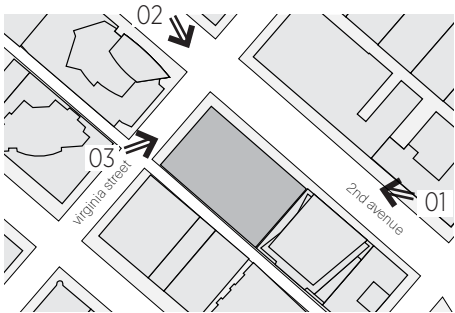
8 Concept 1 - Street Sections



Concept 1 - Street Sections 8

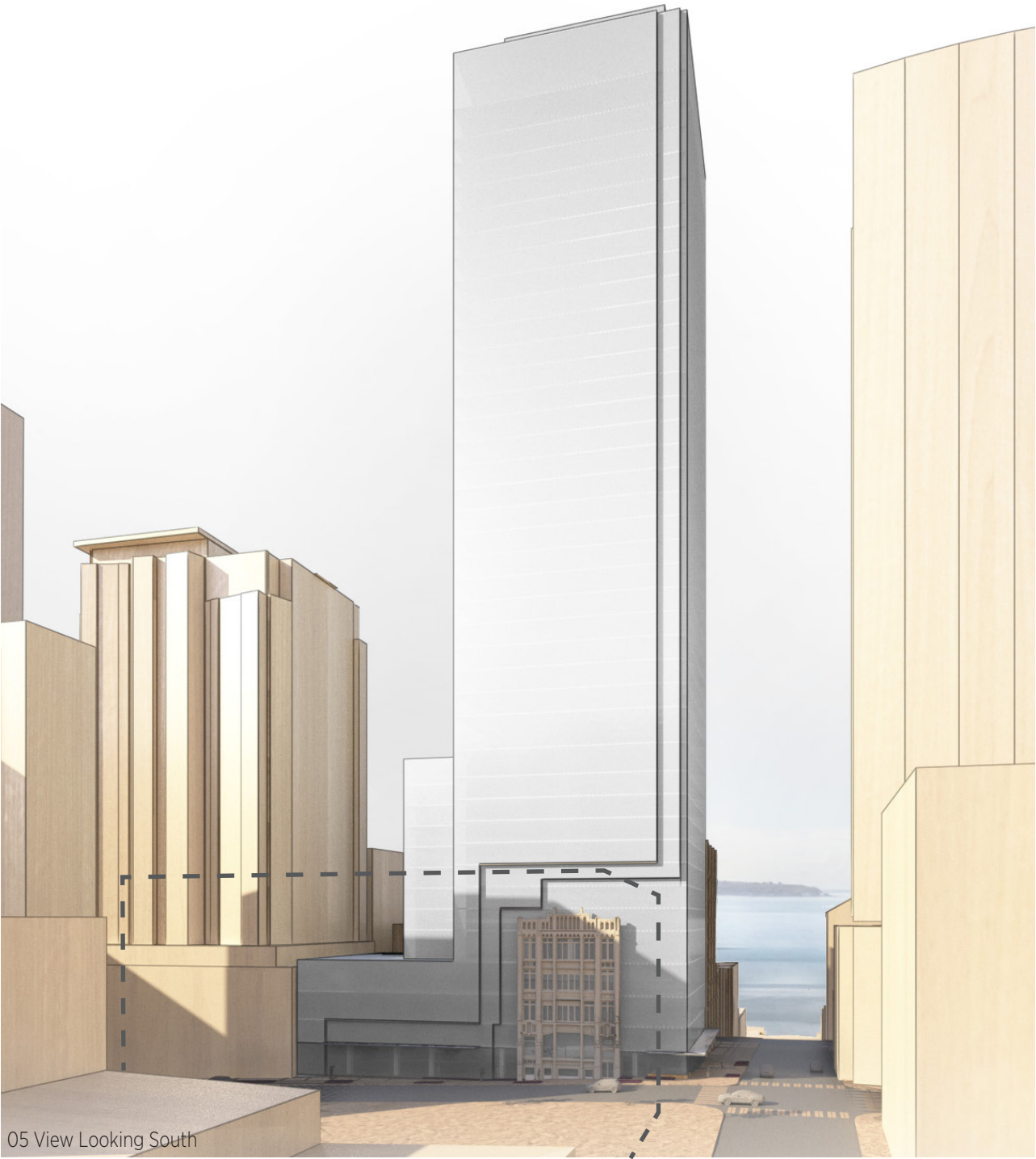
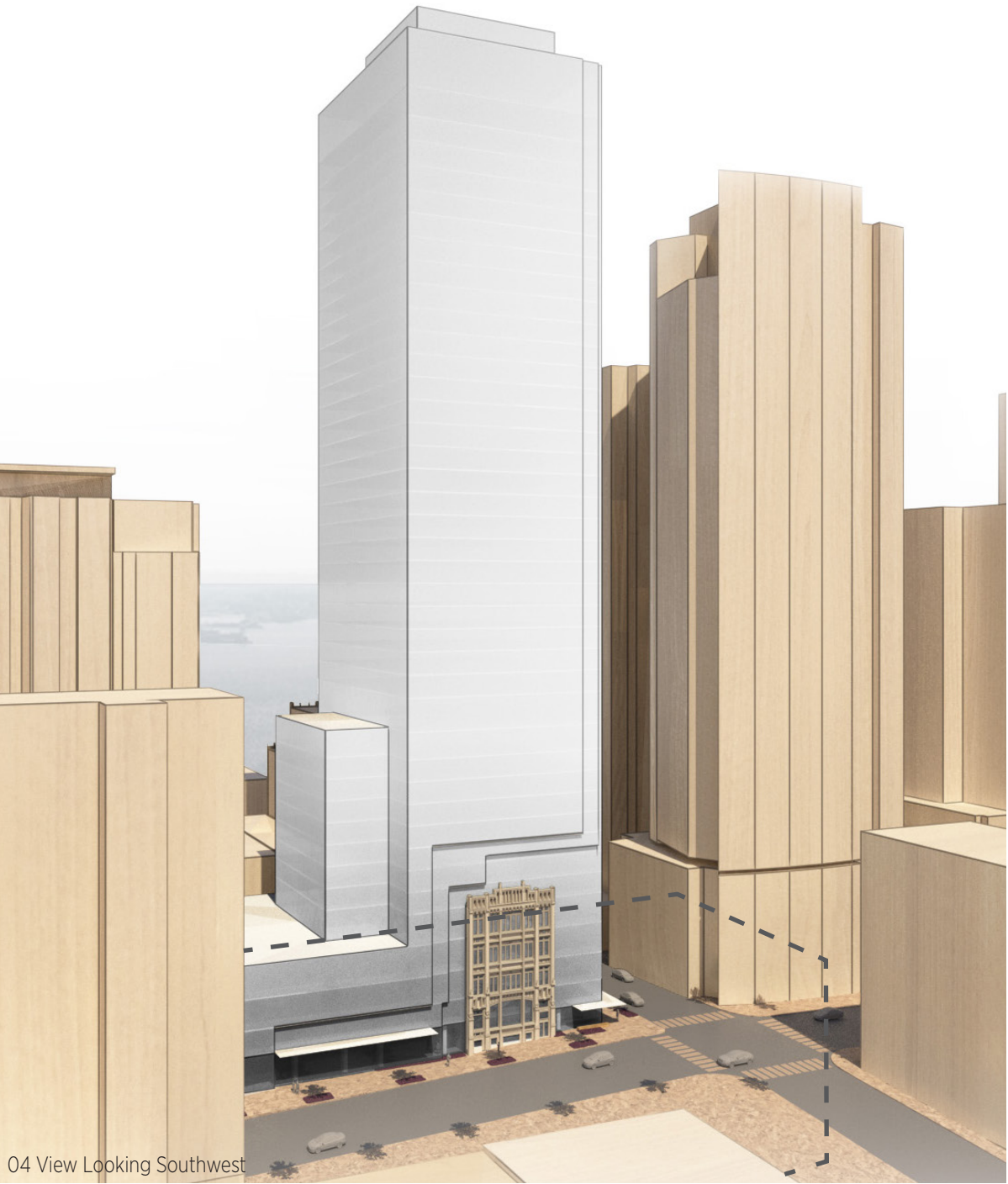
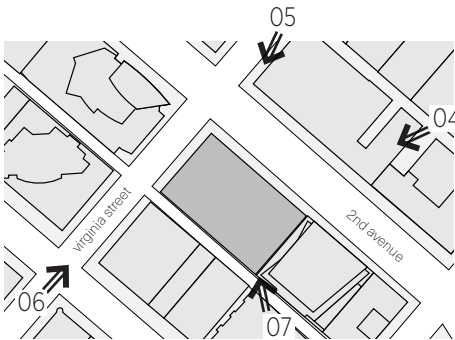


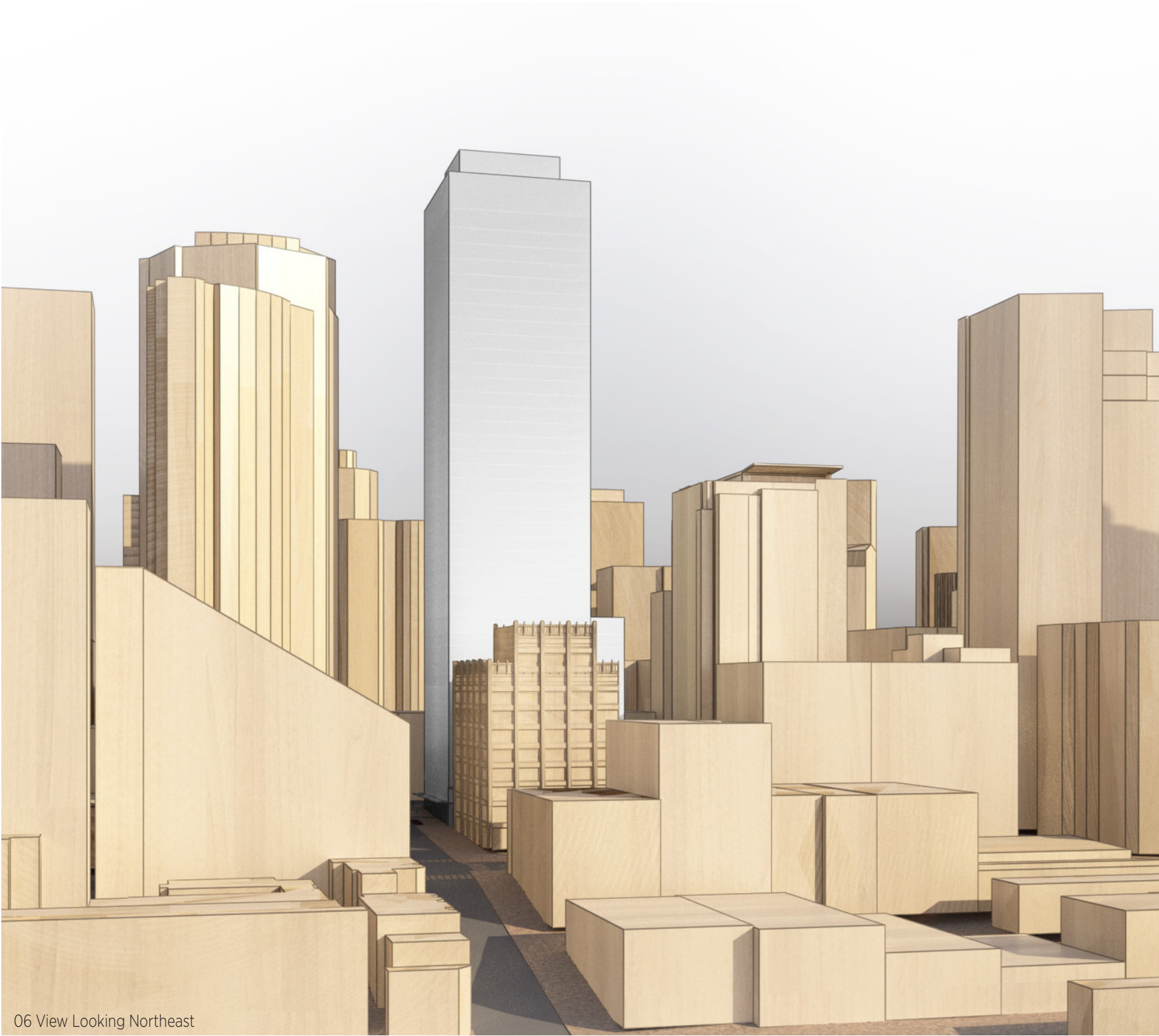
8 Concept 1 - Renderings



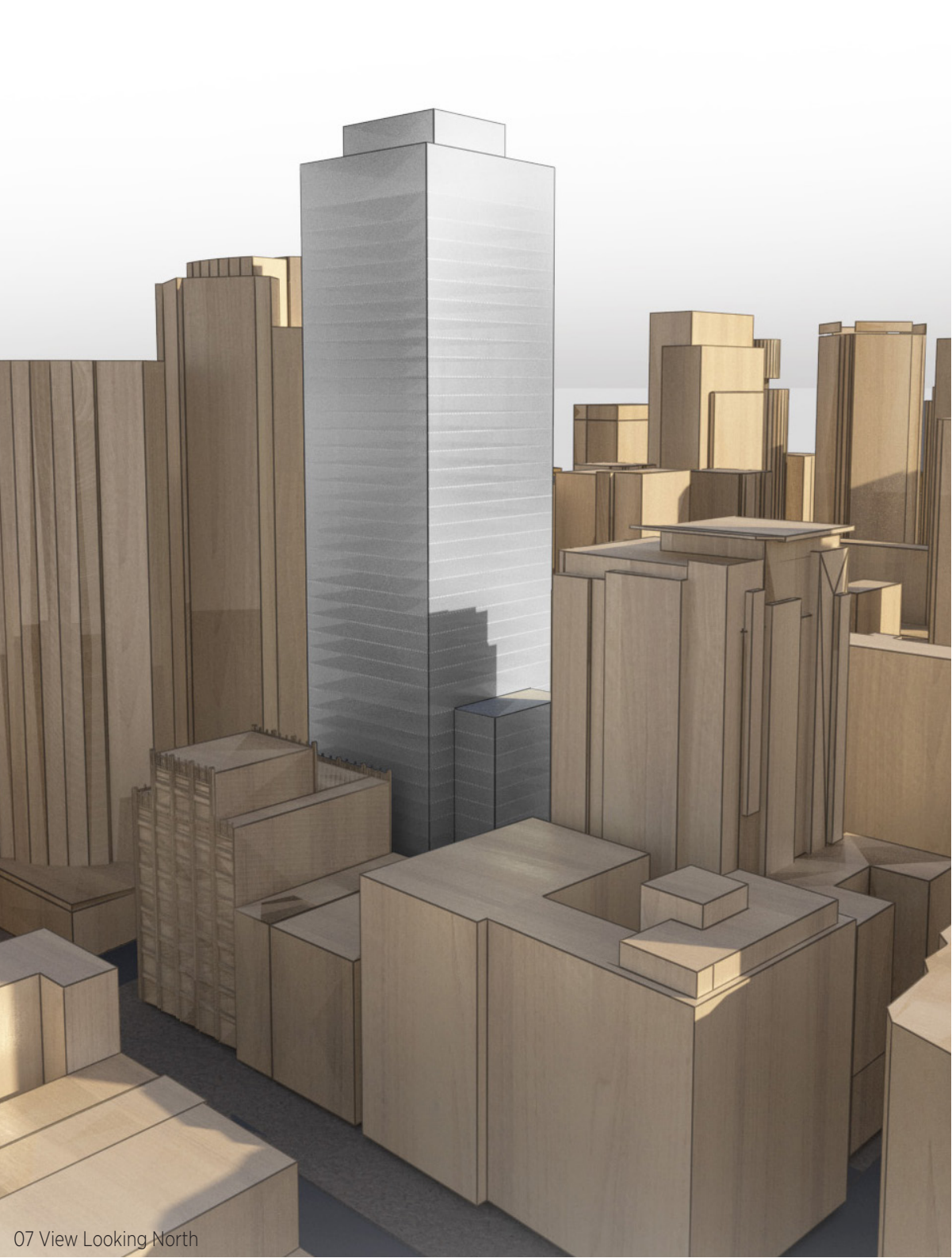


8 Concept 1 - Renderings





06 View Looking Northeast



07 View Looking North

This page intentionally left blank.

DESIGN CONCEPTS

8 Concept 2

Shift

Massing Concept 2 follows previous Landmark Presentation Board approvals by setting the tower mass back 23 feet away from 2nd Avenue, uncovering the landmark building. The lowered podium provides a transition in scale between the Moore Theater across the street and the adjacent taller buildings.

A shift in the tower creates two thinner volumes to emphasize verticality and open up the corner following the edge of the landmark building. The facade design further expresses this verticality with more solid materials to contrast with nearby towers and orient people as they move through the city.

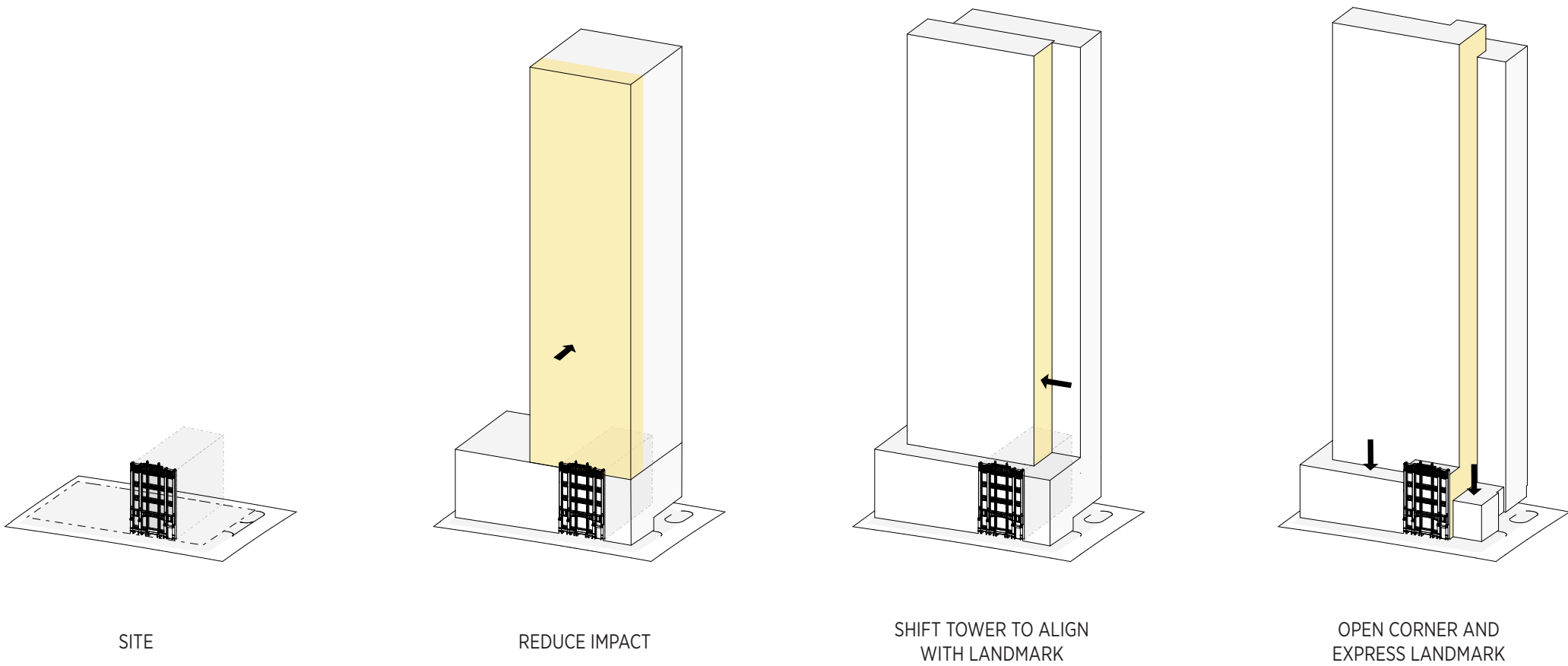
- Some of the challenges of Massing Concept 2 include:
- Increased tower width and minimized tower spacing from Viktoria Apartments by setting back from the Landmark
 - The tower massing lacks a distinct relationship to the landmark or city fabric.
 - There is no massing setback at the street level for open public space or reinforced entries.

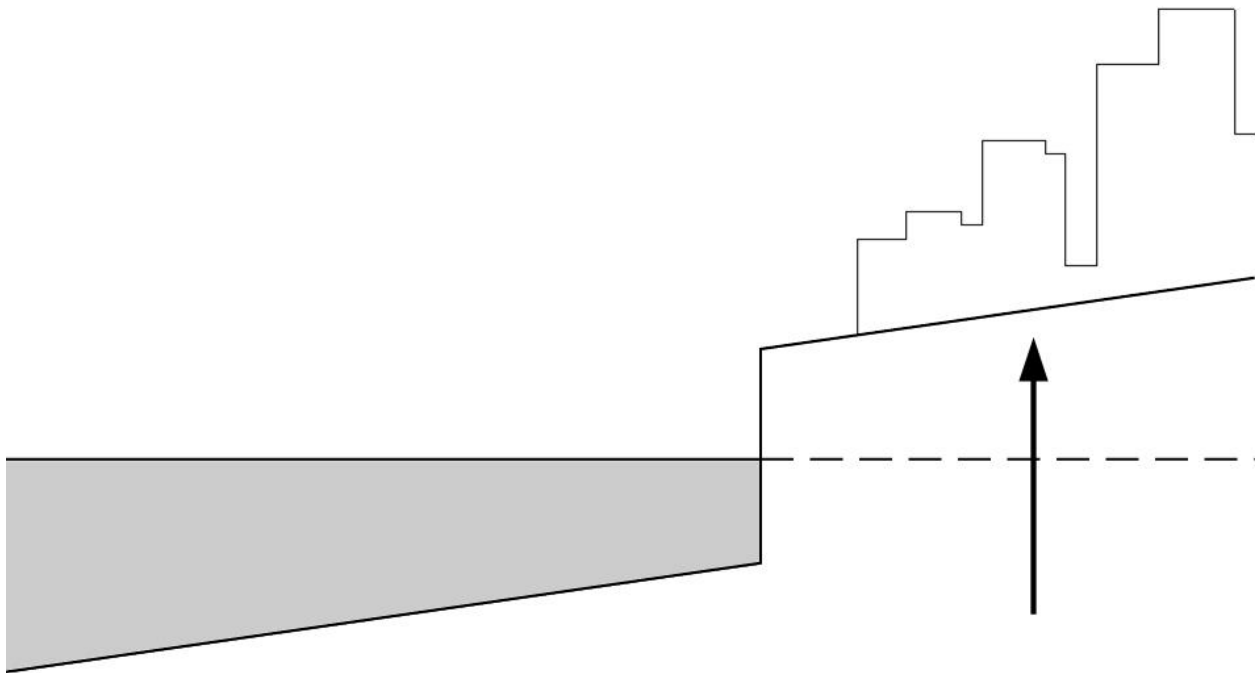
Design Guidelines:

- **B-2** Create a Transition in Bulk and Scale
- **B-3** Reinforce Positive Urban Form & Arch. Attributes
- **D-6** Design for Personal Safety & Security

Landmark Preservation Board Priorities:

- **01** Set Tower Back from Landmark
- **03** Match existing levels of Landmark
- **04** Retain existing entry door of Landmark
- **05** Create active uses within Landmark





Shift to Reveal Past and Future

As the horizontal line of the ocean's surface abruptly shifts upward to reveal the land under Seattle and support the city built upon it, the new tower shifts to the side to align with the existing building. The shifted tower form reveals new vertical proportions, new corners and new views out.



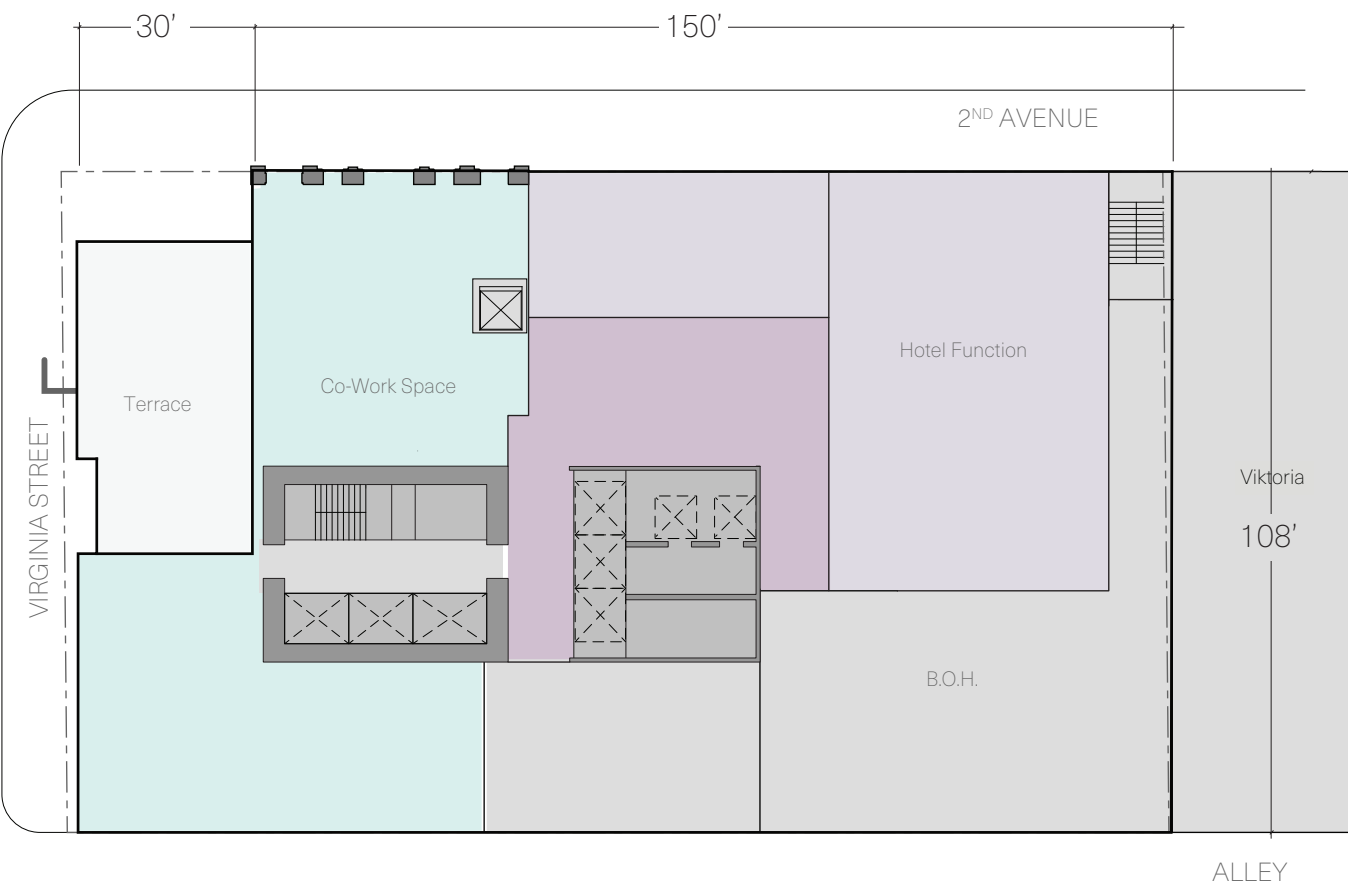
DESIGN CONCEPTS

8 Concept 2 - Plans

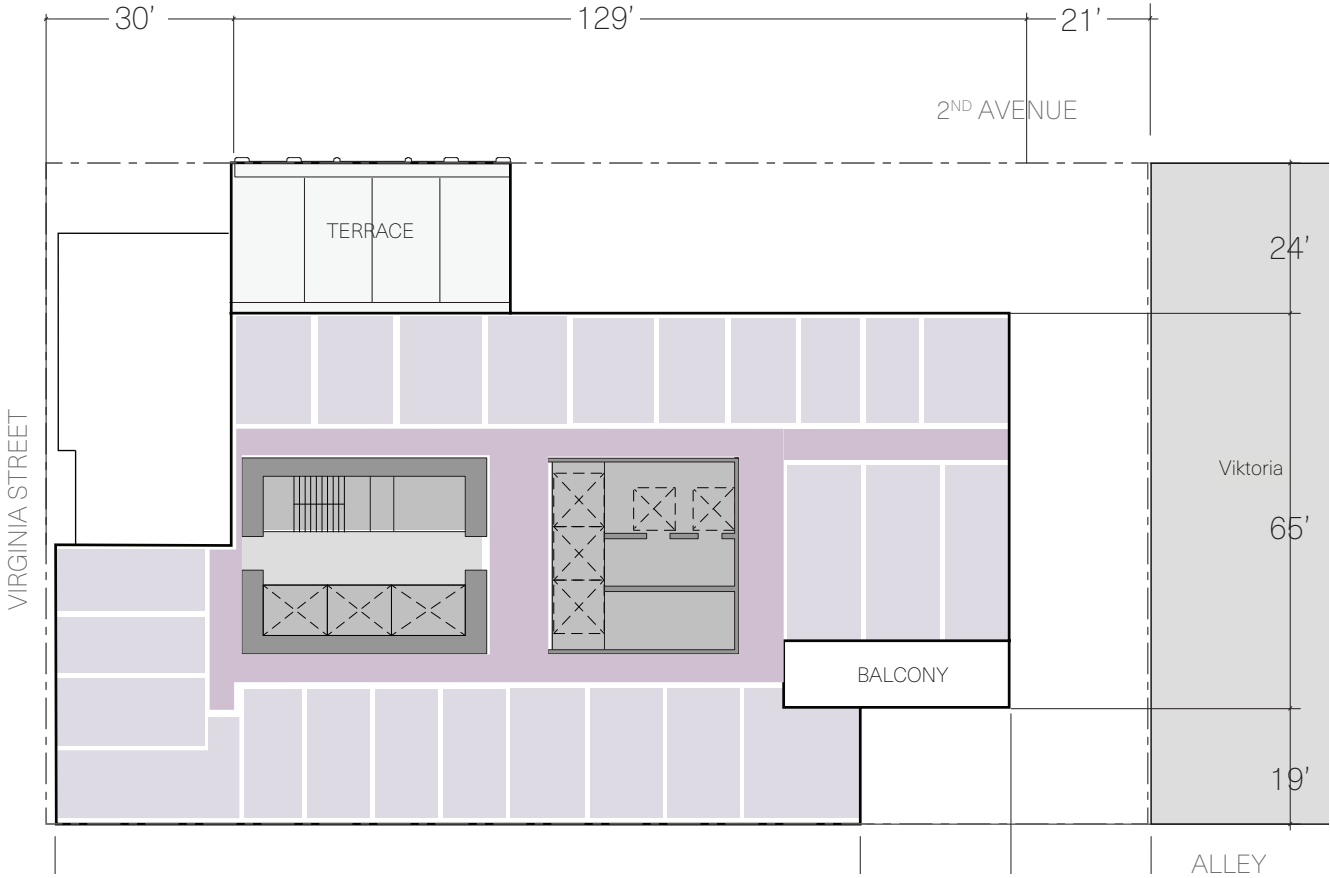


Ground Floor Plan

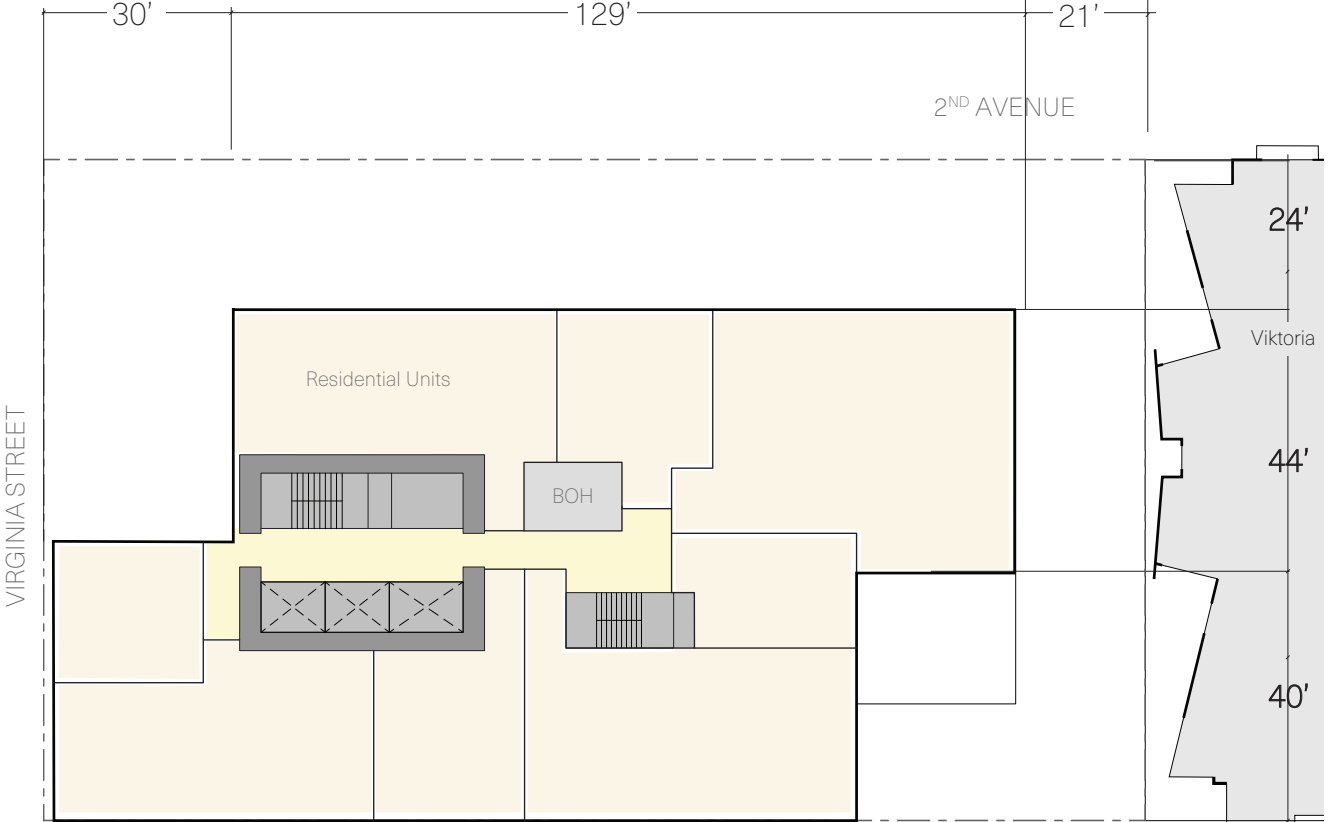
- COWORK
- HOTEL
- BOH
- RESIDENCE
- RESTAURANT
- PEDESTRIAN ENTRY
- VEHICULAR ENTRY



Podium Floor (L3)



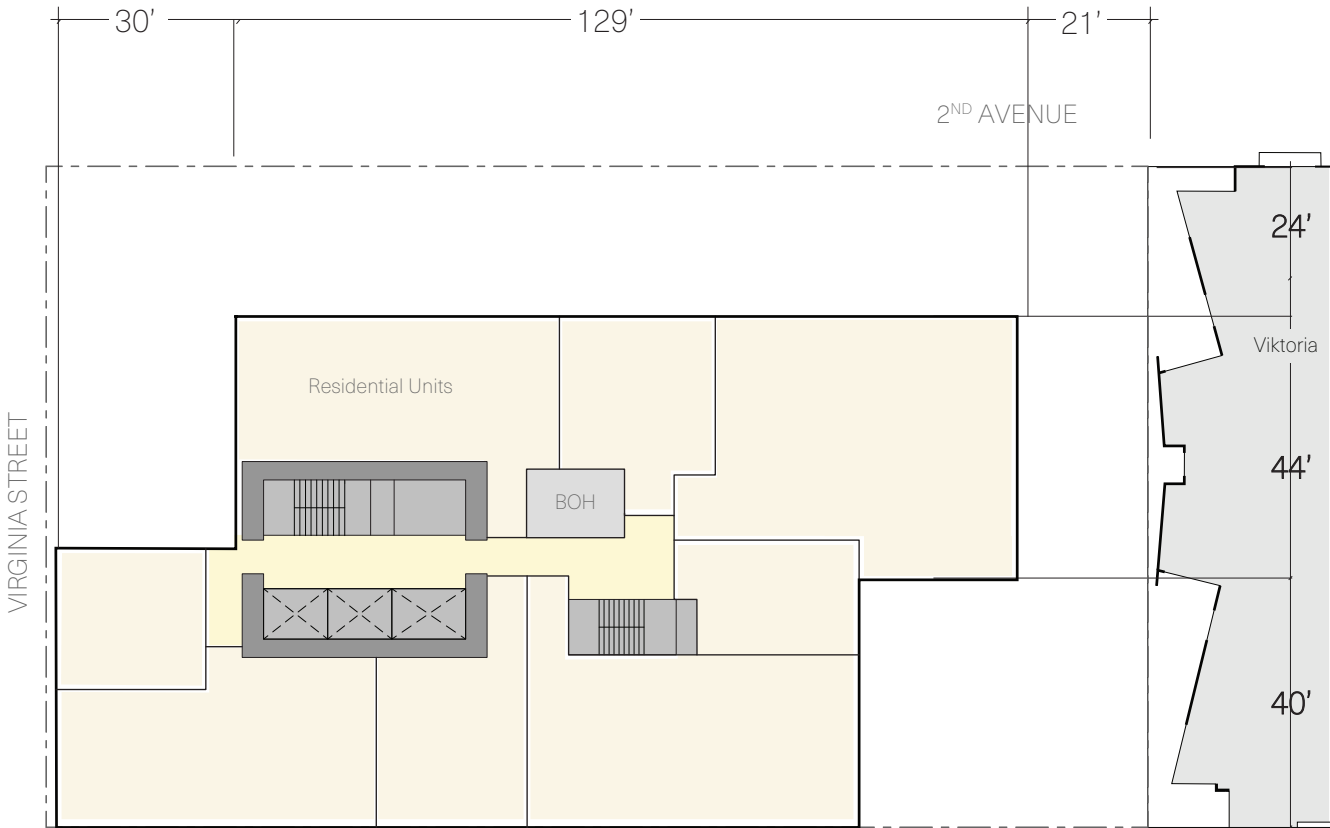
Typical Hotel Floor (L5-12)



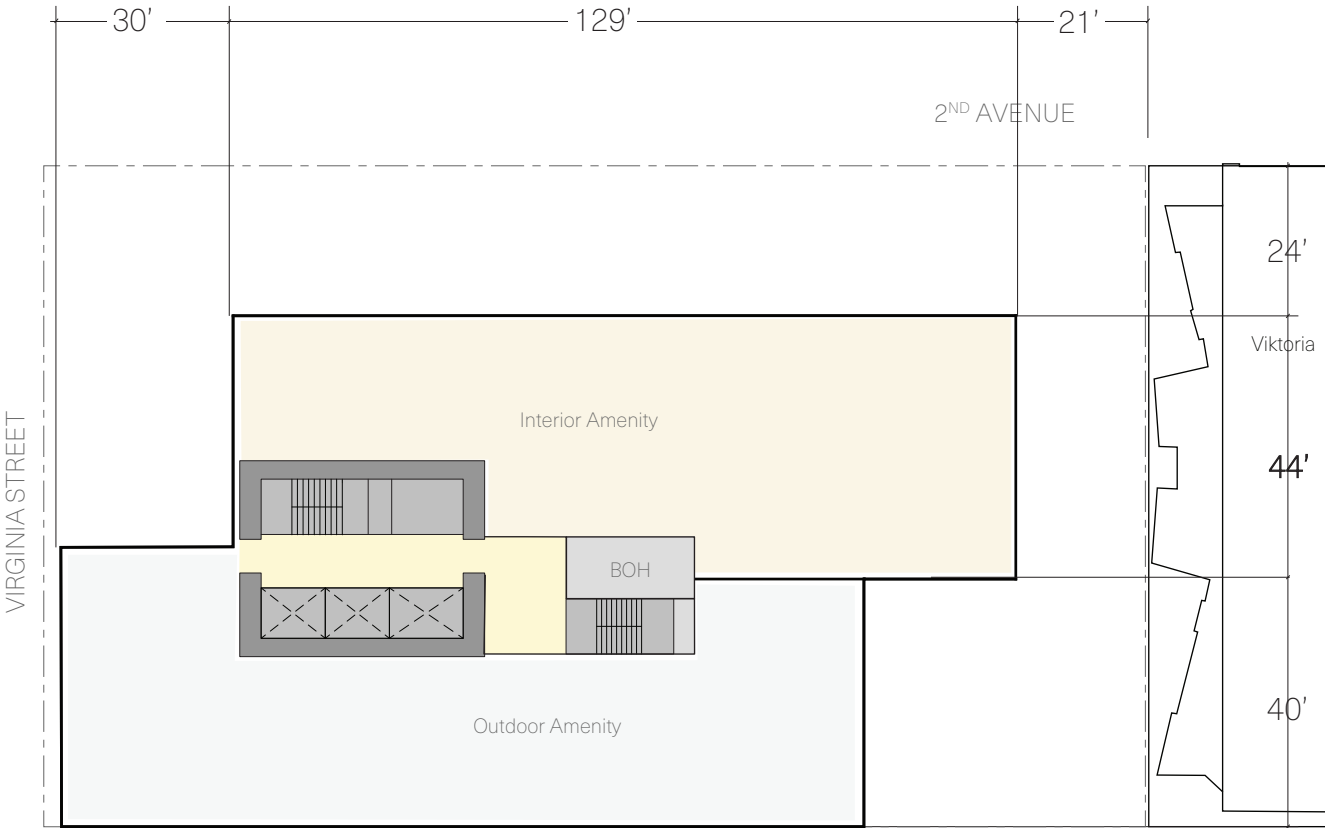
Residential Floor (L16-27)

DESIGN CONCEPTS

8 Concept 2 - Plans



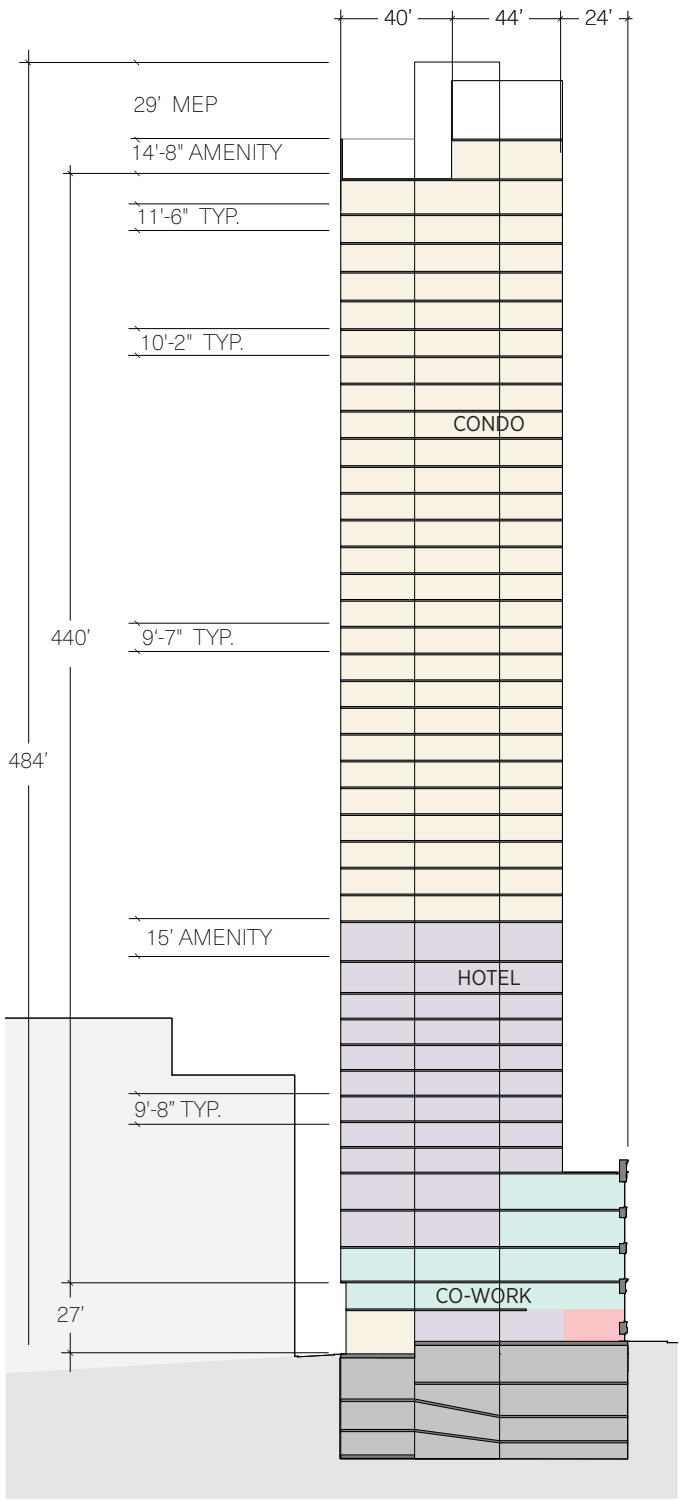
Typical Residential Floor (L21-27)



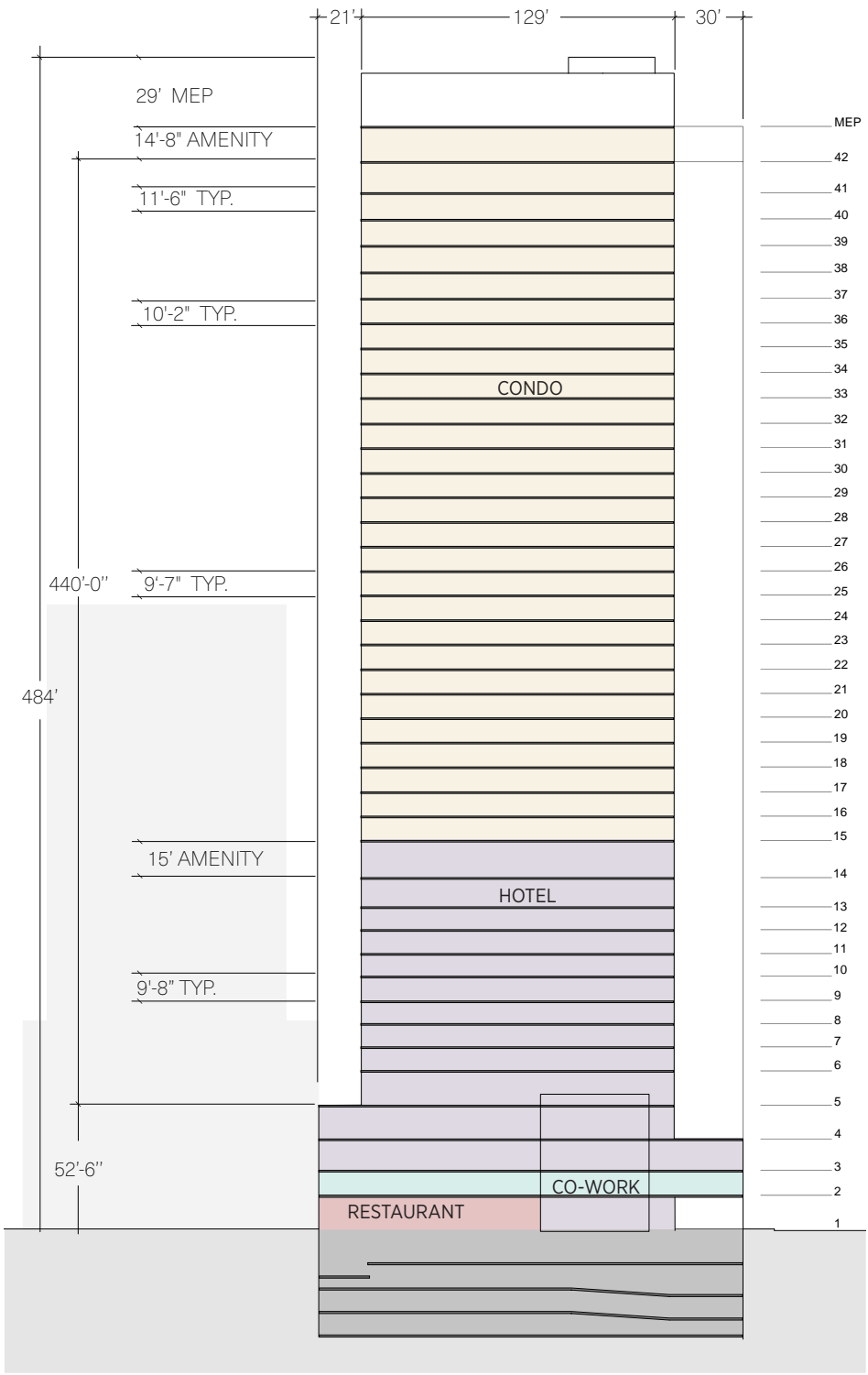
Rooftop Amenity Floor (L42)



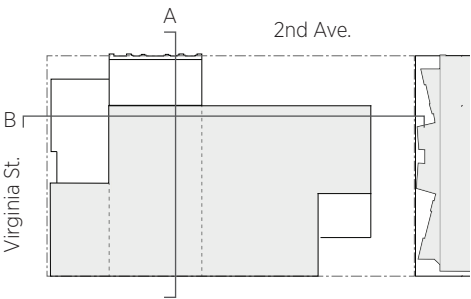
- COWORK
- HOTEL
- BOH
- RESIDENCE
- RESTAURANT



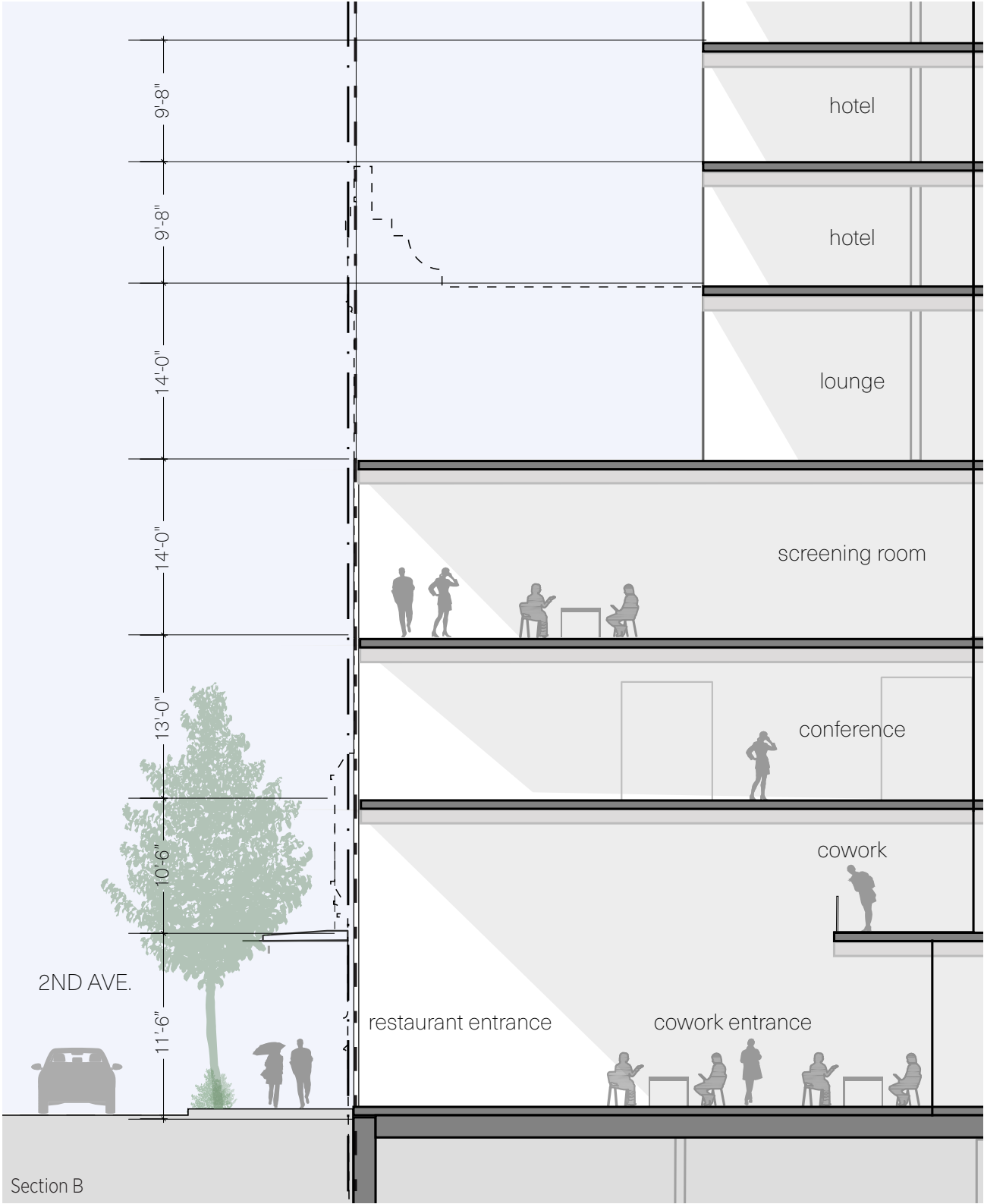
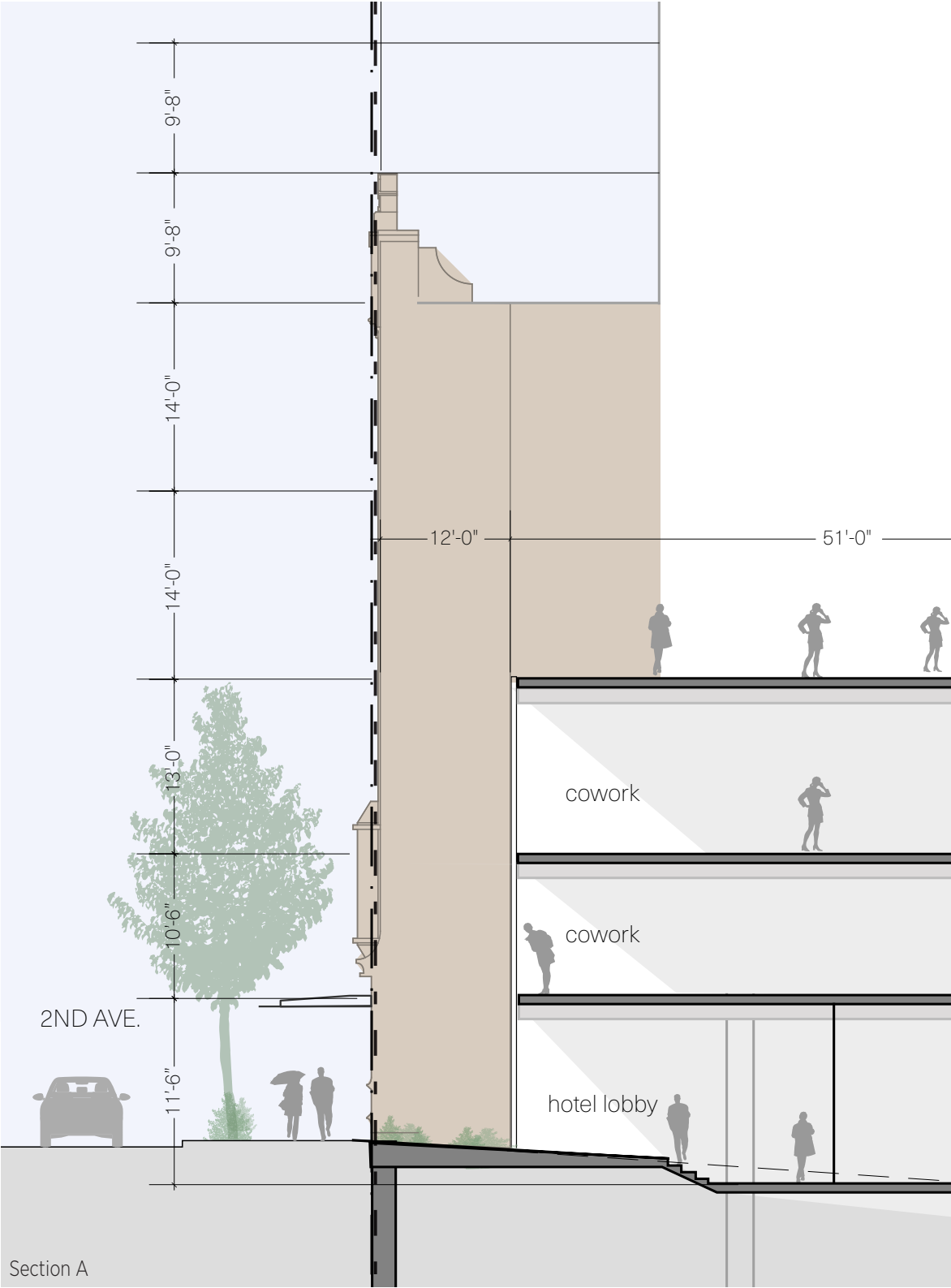
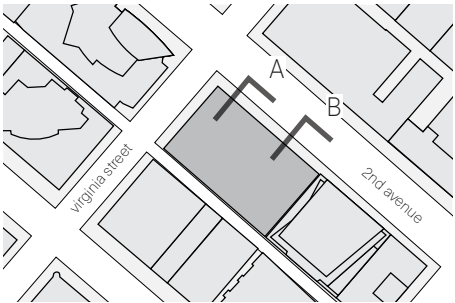
Section A



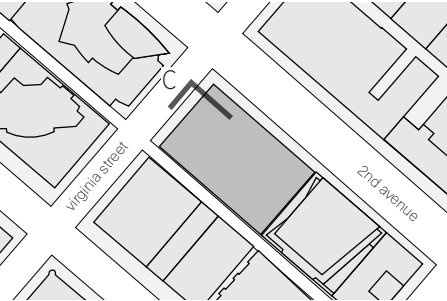
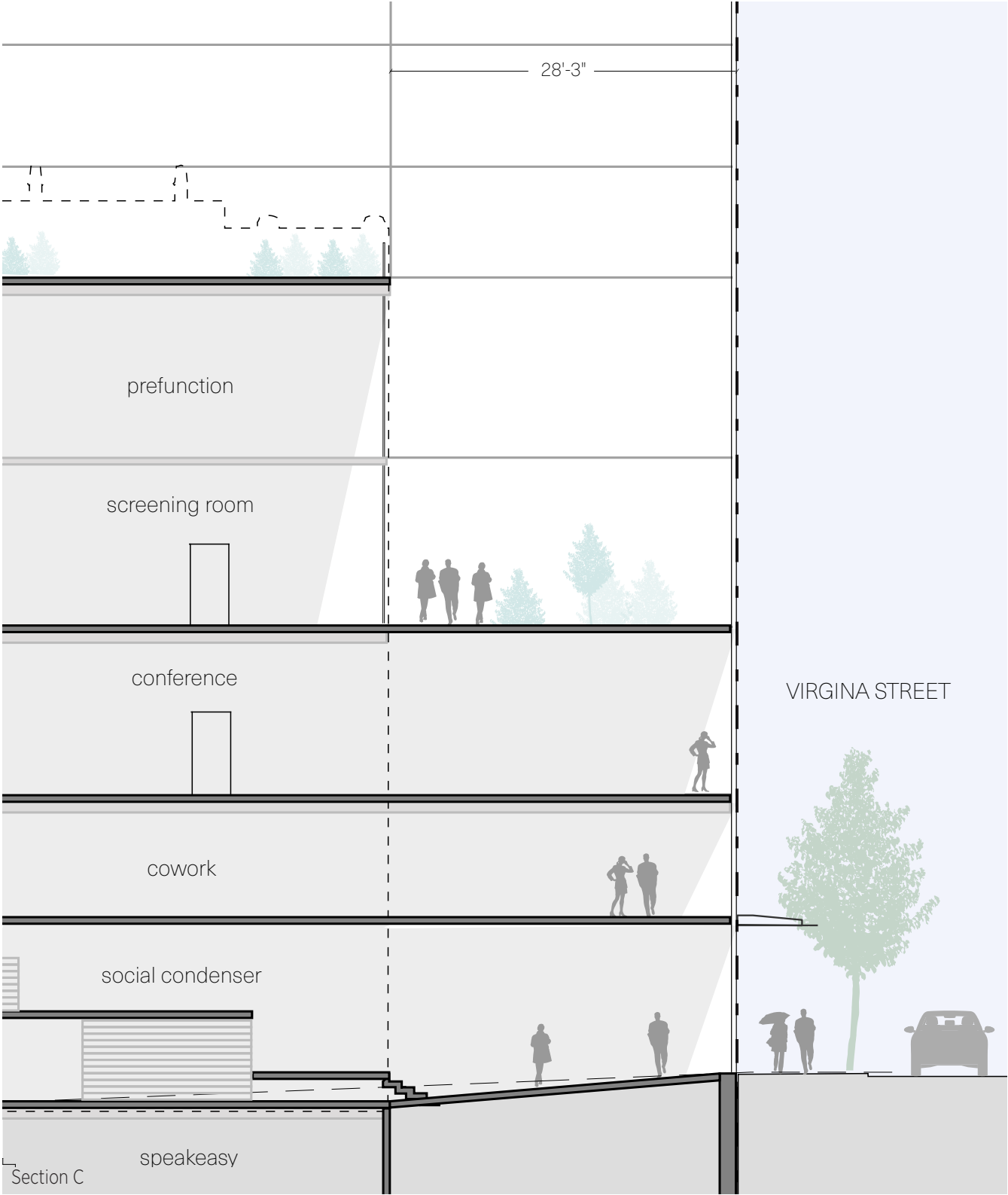
Section B



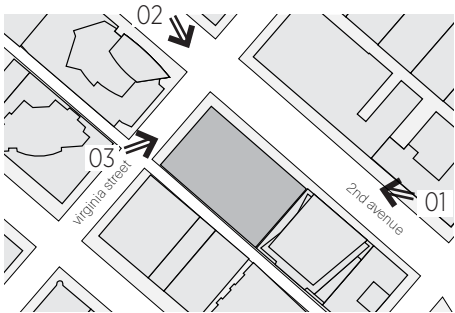
8 Concept 2 - Street Sections

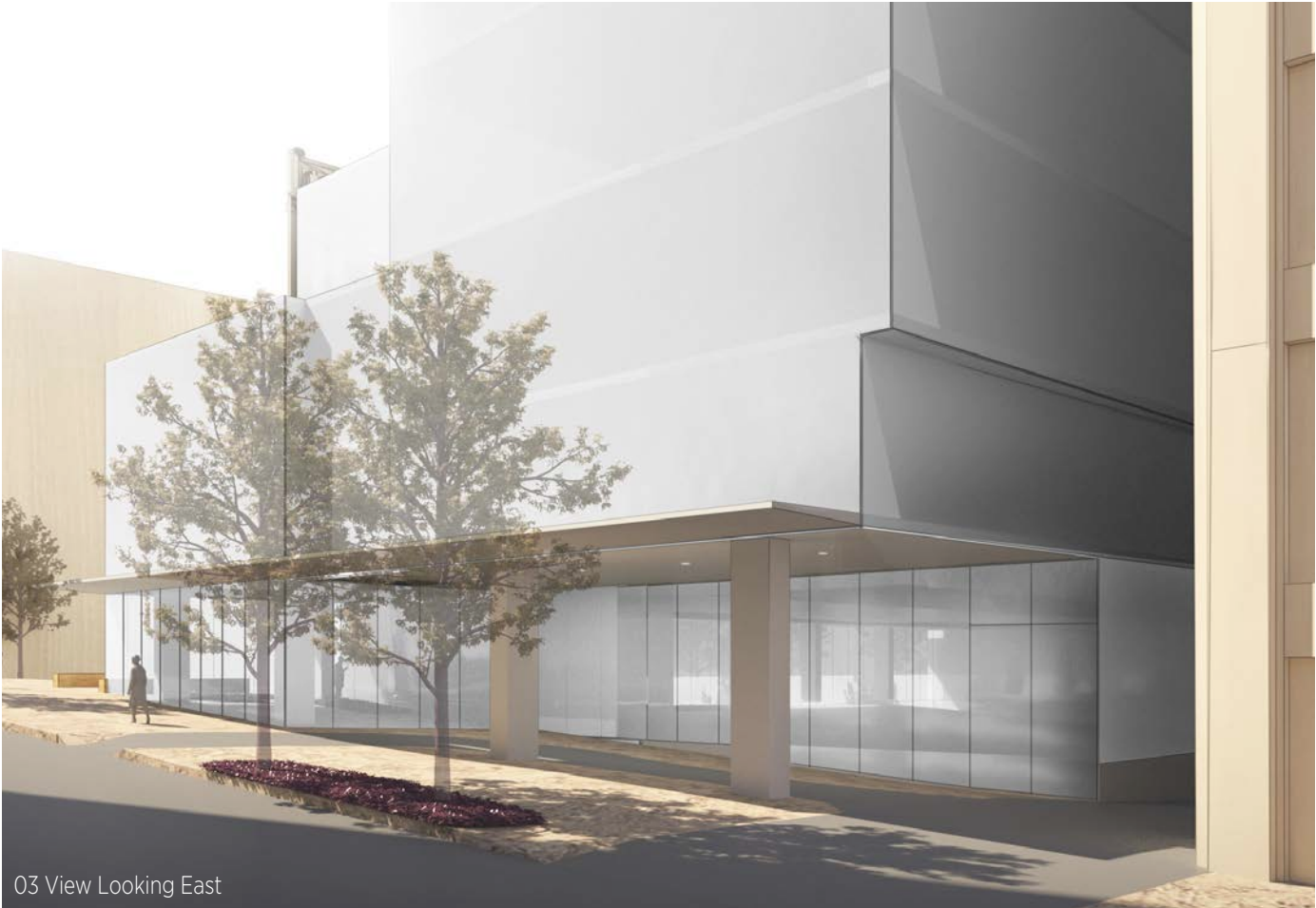


Concept 2 - Street Sections 8

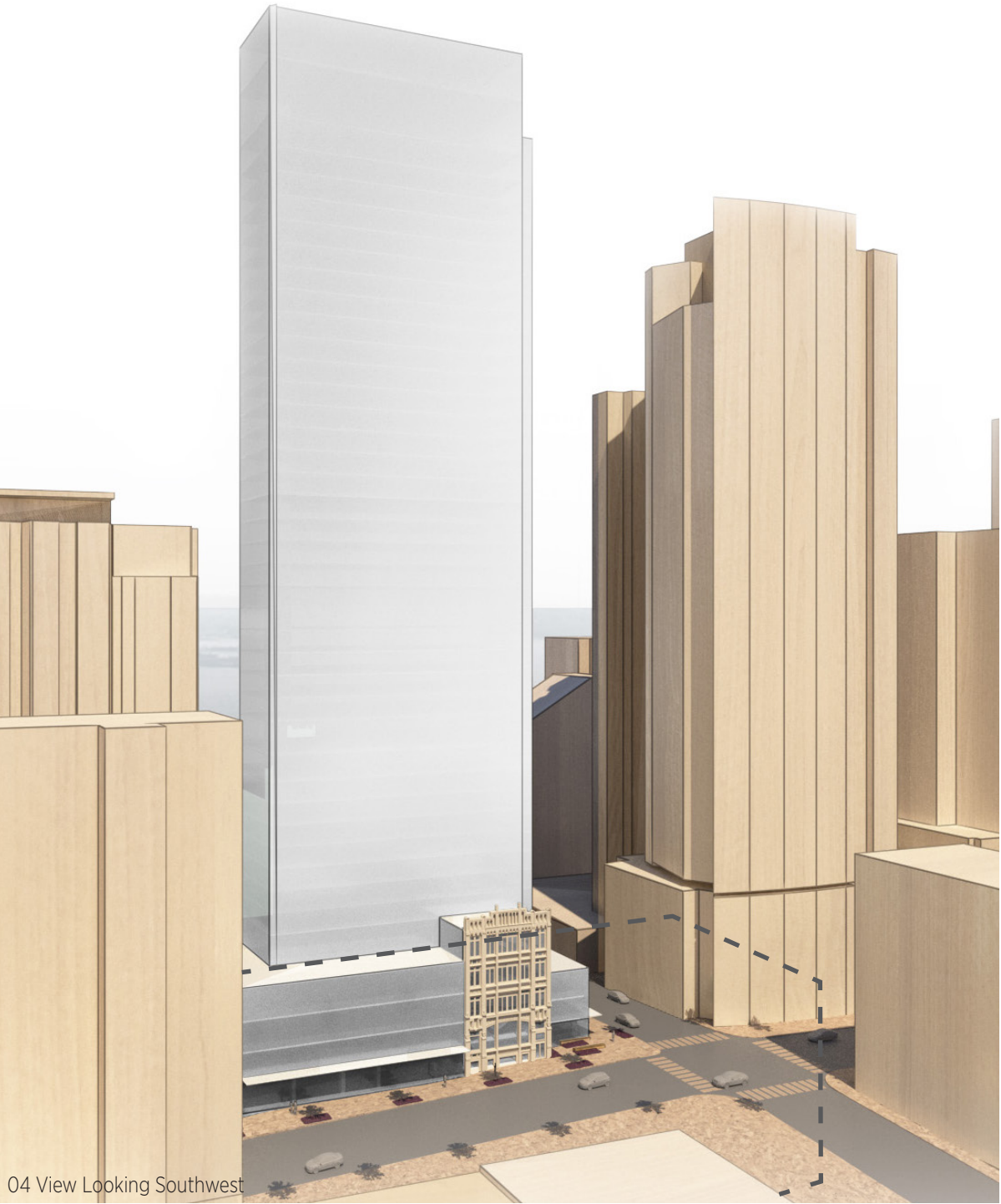
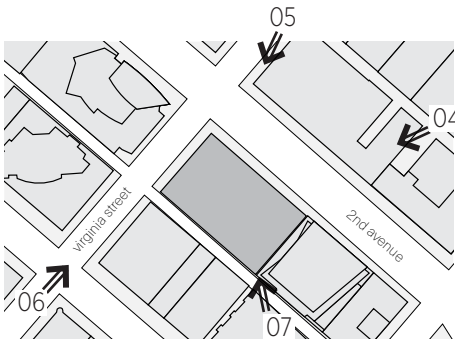


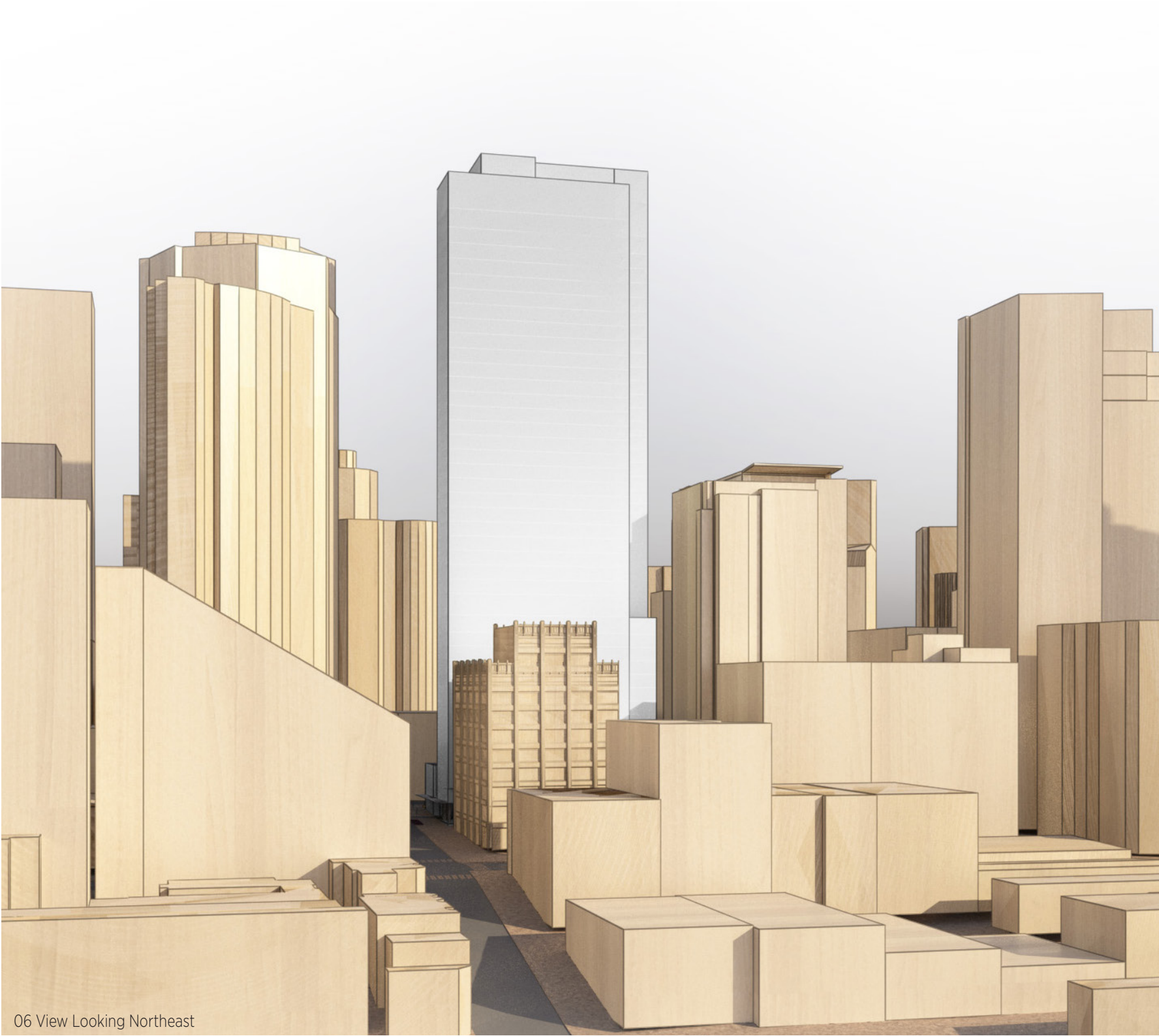
8 Concept 2 - Renderings



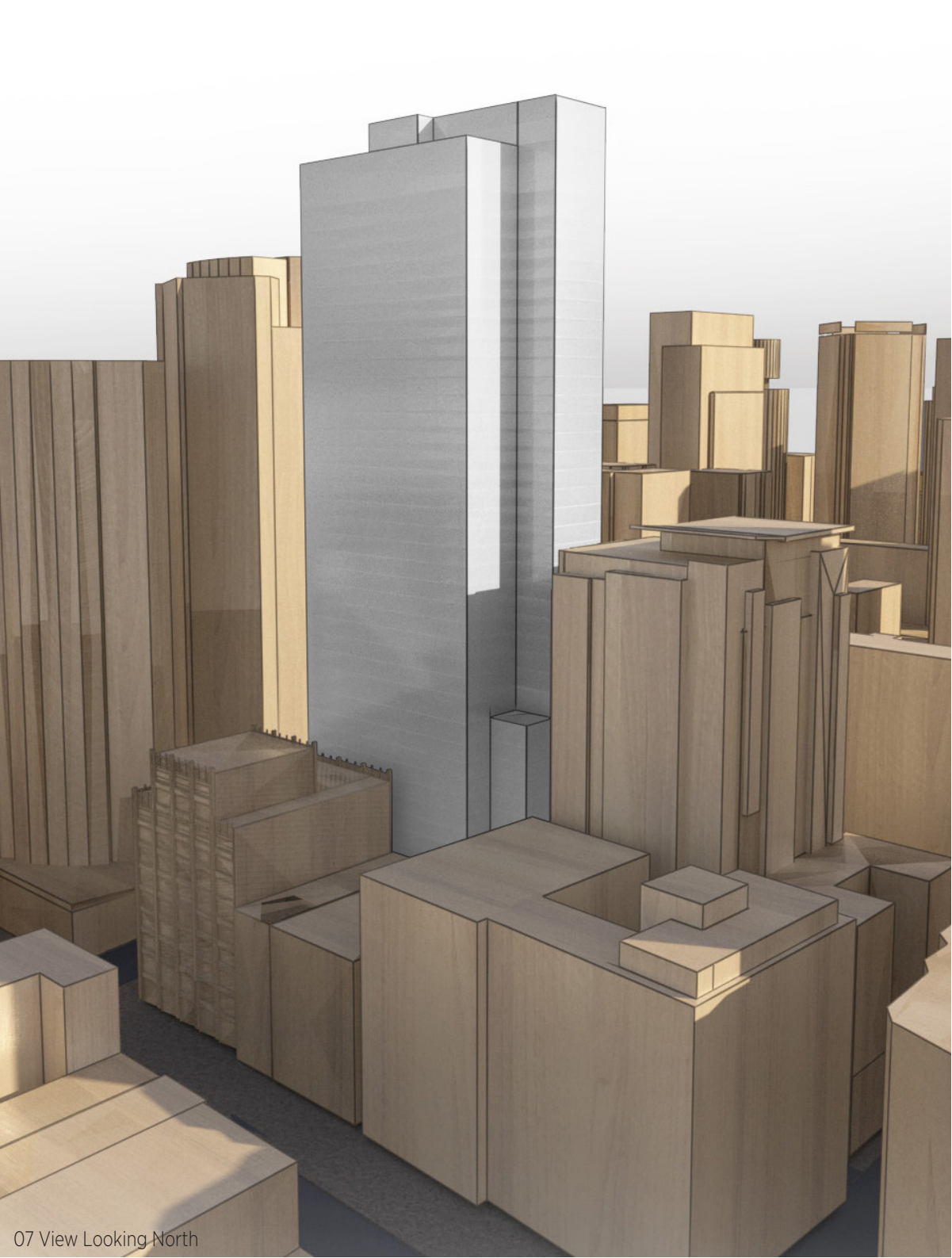


8 Concept 2 - Renderings





06 View Looking Northeast



07 View Looking North

This page intentionally left blank.

DESIGN CONCEPTS

8 Preferred Concept

Gradation

Telescoping volumes of the preferred concept reinforce the architectural context while finding a balance between necessary tower spacing and breathing room for the landmark building. The stepped massing takes cues from the strong Gothic and art deco heritage of Seattle to create a well-proportioned and unified massing which will enhance the skyline.

Embracing what came before, the landmark building sets up the alignments for the stepped massing and provides inviting and usable open plazas flanking the Terminal Sales Annex at the ground level. The shifts at ground level reinforce many design guideline goals of this project.

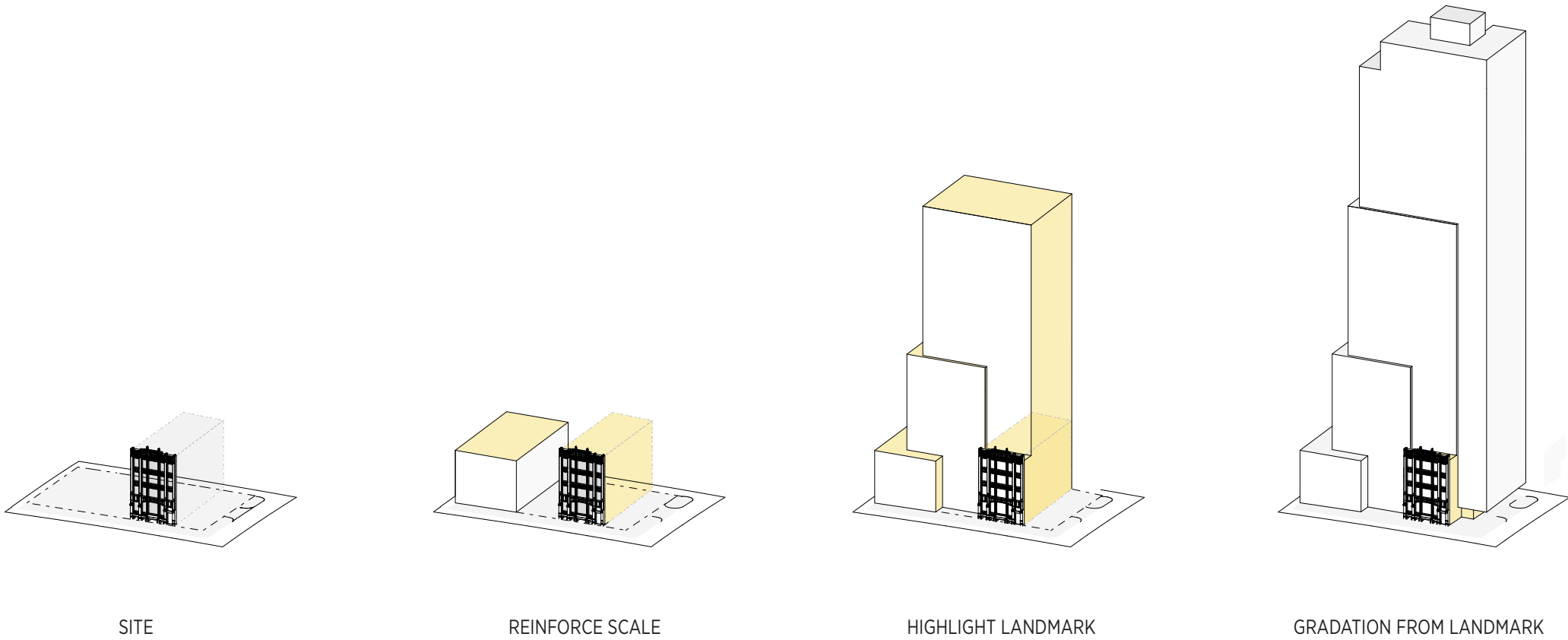
The preferred concept combines the best features of Massing Concepts 1 and 2. The massing moves allow transition in scale and appropriate spacing from both Viktoria Apartments and 2nd Avenue.

Design Guidelines:

- **A-1** Respond to the Physical Environment
- **A-2** Enhance the skyline
- **B-2** Create a transition in bulk and scale
- **B-3** Reinforce the Positive Urban Form & Architectural Attributes of the Immediate Area
- **C-1** Promote Pedestrian Interaction
- **C-4** Reinforce Building Entries
- **D-1** Provide inviting and usable open space
- **D-6** Design for personal safety and security

Landmark Preservation Board Priorities:

- **01** Set Tower Back from Landmark
- **02** Express Volume of Landmark
- **03** Match Existing Levels of Landmark
- **04** Retain existing entry door of Landmark
- **05** Create Active Uses within Landmark
- **06** Tower Expression from Landmark
- **07** Landmark to complete Corner





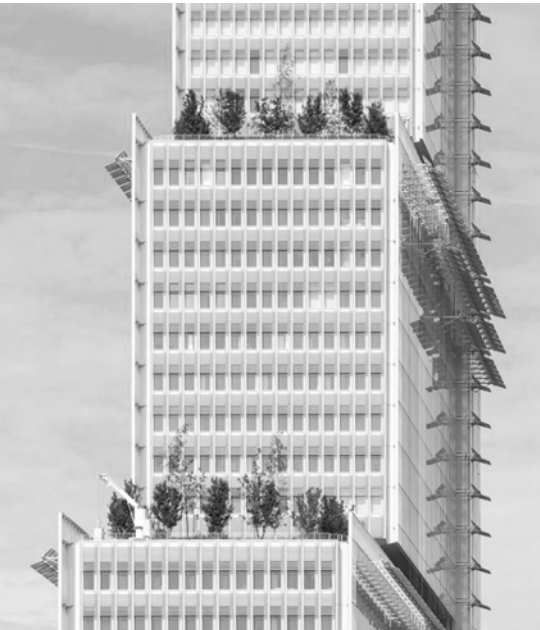
future (skyline)



past (streetscape)

Gradation from Past to Future

Starting down at the street level where the remains of Seattle's past exists in many historic buildings, the new tower rises upward from the historic TSA landmark in a series of graded steps into the skyline where it positions itself amongst the new towers that form the future of Seattle. Bebb & Gould's use of terracotta on the Terminal Sales Annex was innovative and new to the city. Each step holds this forward-thinking approach of the Terminal Sales Annex, to a more innovative skyline reflecting the change of the city.

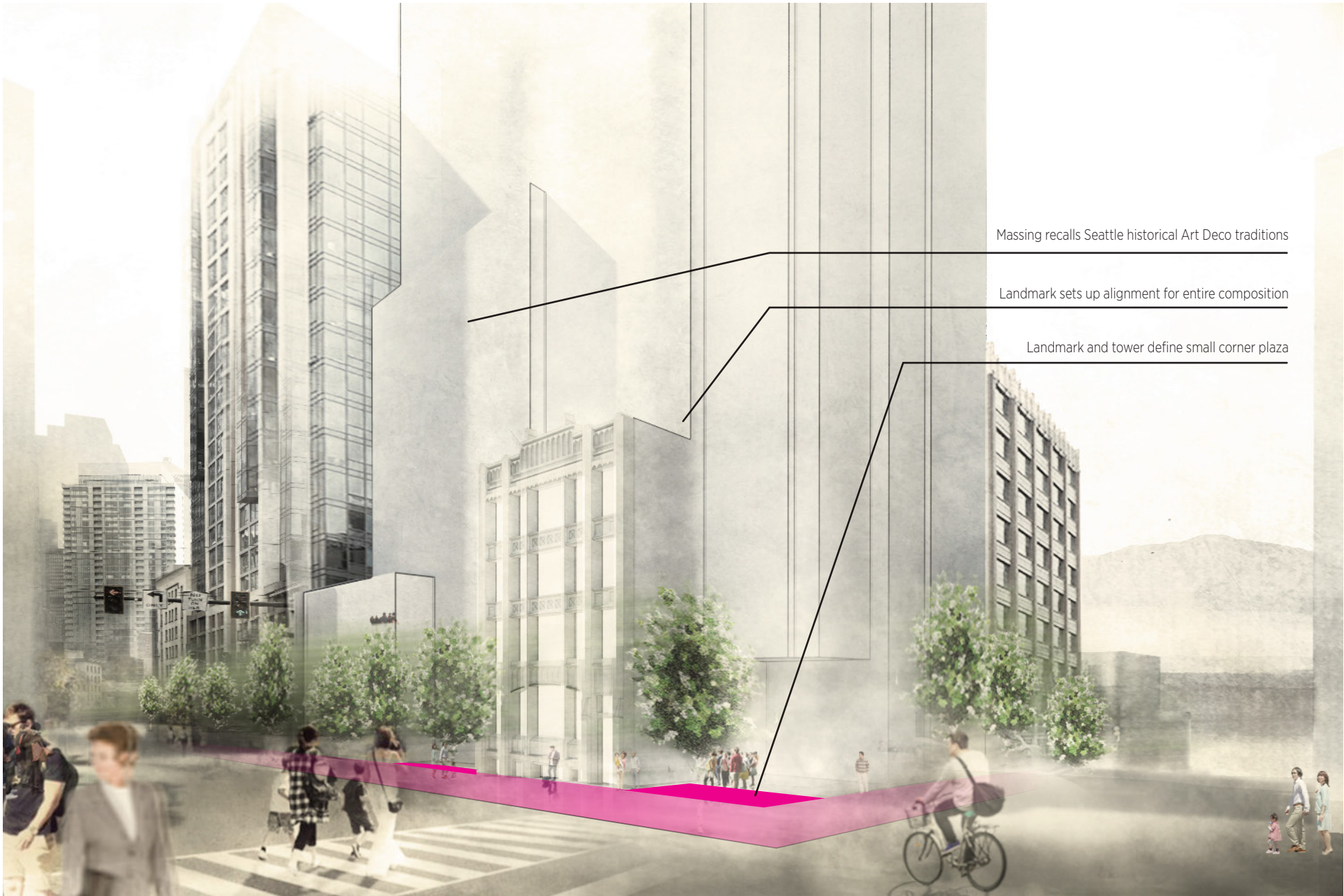
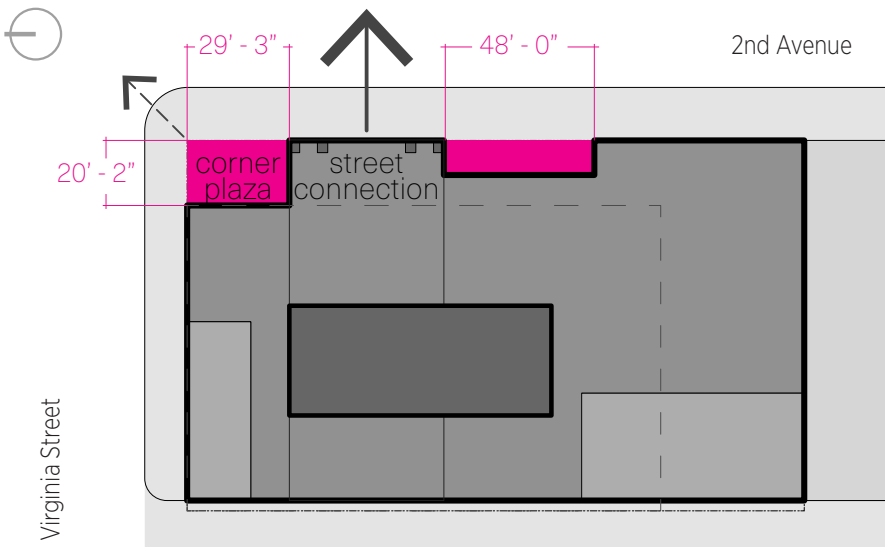


DESIGN CONCEPTS

8 ARC 1 Comments

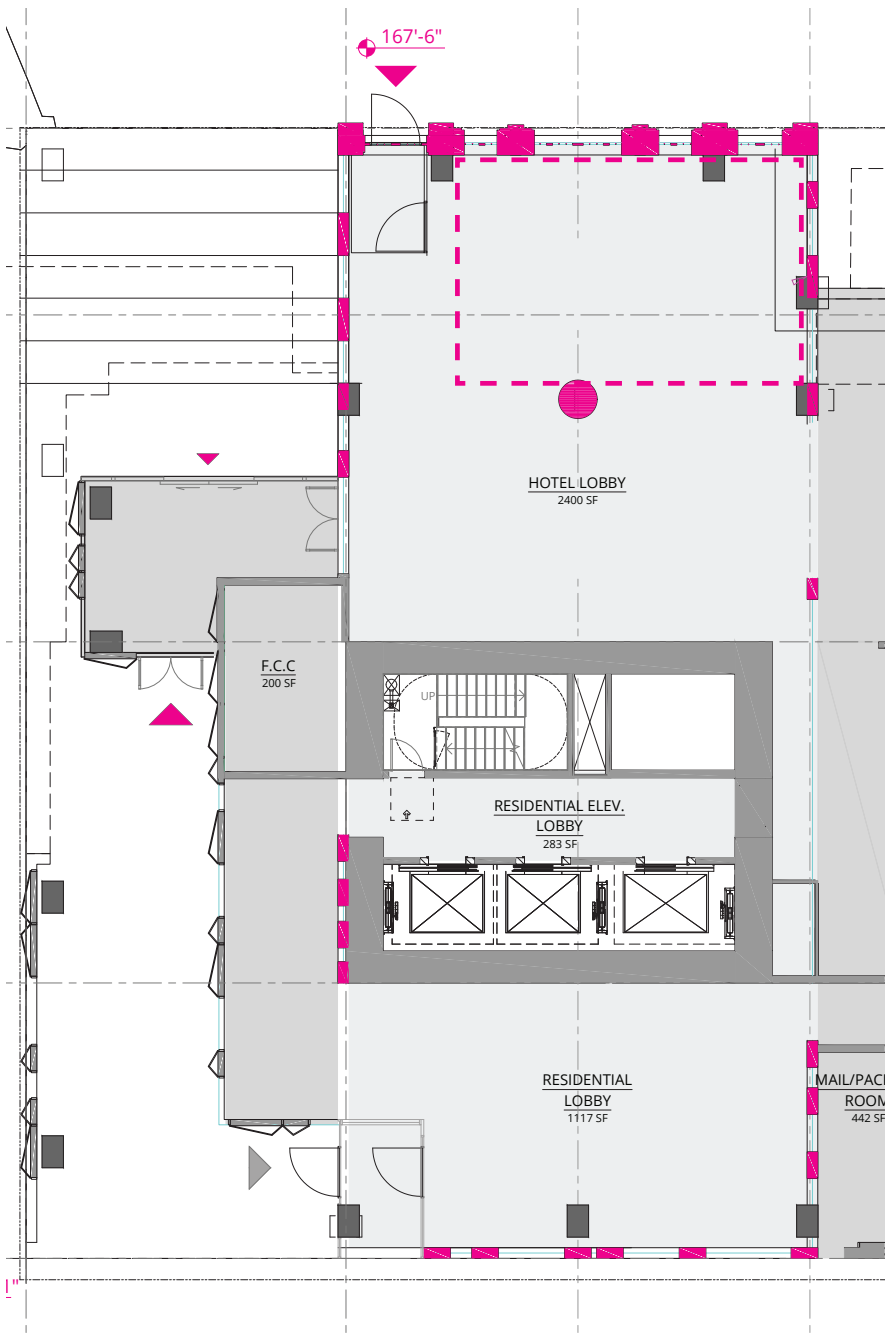
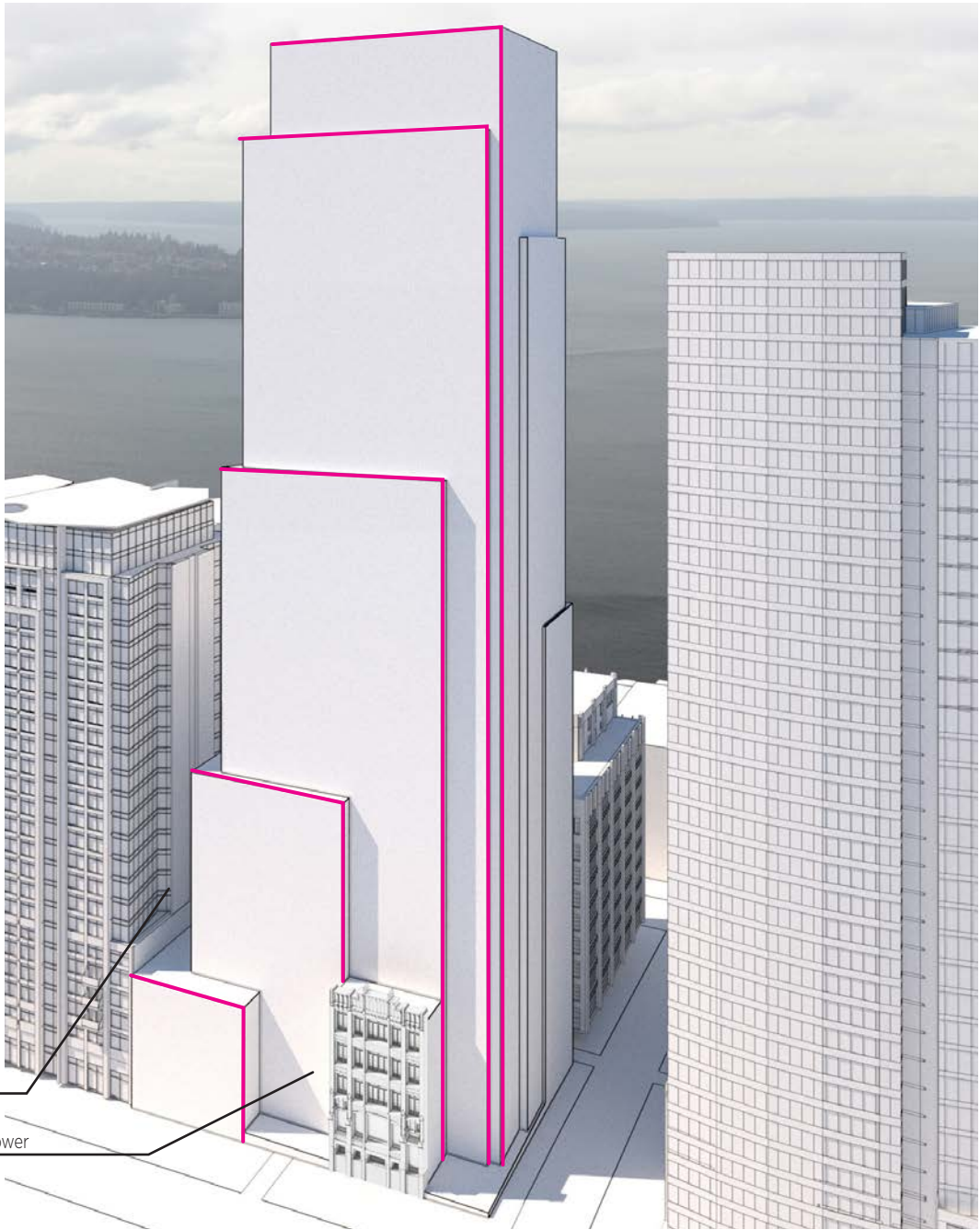
ARC Meeting 1 / June 29, 2018
The team presented early concepts to the ARC and received the following comments. We have incorporated these changes into the EDG design.

- 1 **Massing:**
Main argument is how this is an improvement beyond 2015, what are we doing beyond that
- 2 **Terminal Sales Annex Volume Expression:**
Imply the sense of the building in the function of the space
- 3 **Existing Floor Levels**
Elevation is more about elevation at windows, need to look at ways to keep floor level
- 4 **Integration of Hotel Clare Comments:**
Outdoor Corner is a step forward in exposing the landmark
Study Relationship at the Ground
- 5 **Architectural Style:**
Appreciates the reference to art deco massing and embracing what came before"
References to Northern Life Tower is pleasing, sculpting at the top of tower reflects some of these qualities



ARC Meeting 2 / August 31, 2018
The preferred massing has deviated slightly from the one presented to the ARC, but the over-arching concepts of massing remaining the same.

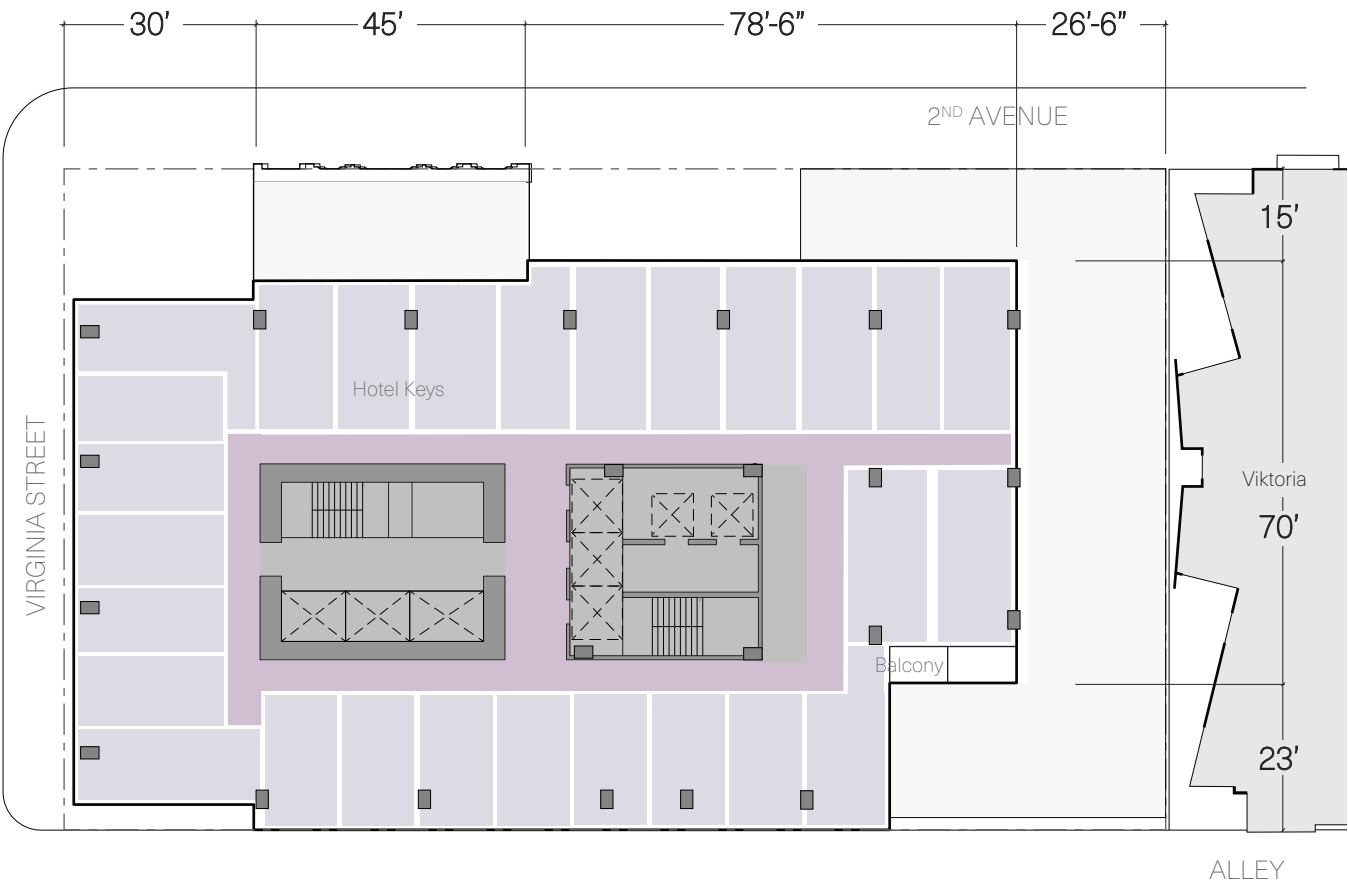
- 1 **Massing:**
Believes the massing concept is on the right track and is appropriate to move forward to Early Design Guidance
- 2 **Vertical Alignment:**
Appreciates the Terminal Sales Annex setting up alignments of the tower above
Applauds approach for it being the growth point as it relates to the ground level pedestrian experience.
- 3 **Integration of the Terminal Sales Annex:**
Intrigued by the experiential integration of the landmark building.
- 4 **Party Walls**
Explore how to treat the return party walls.



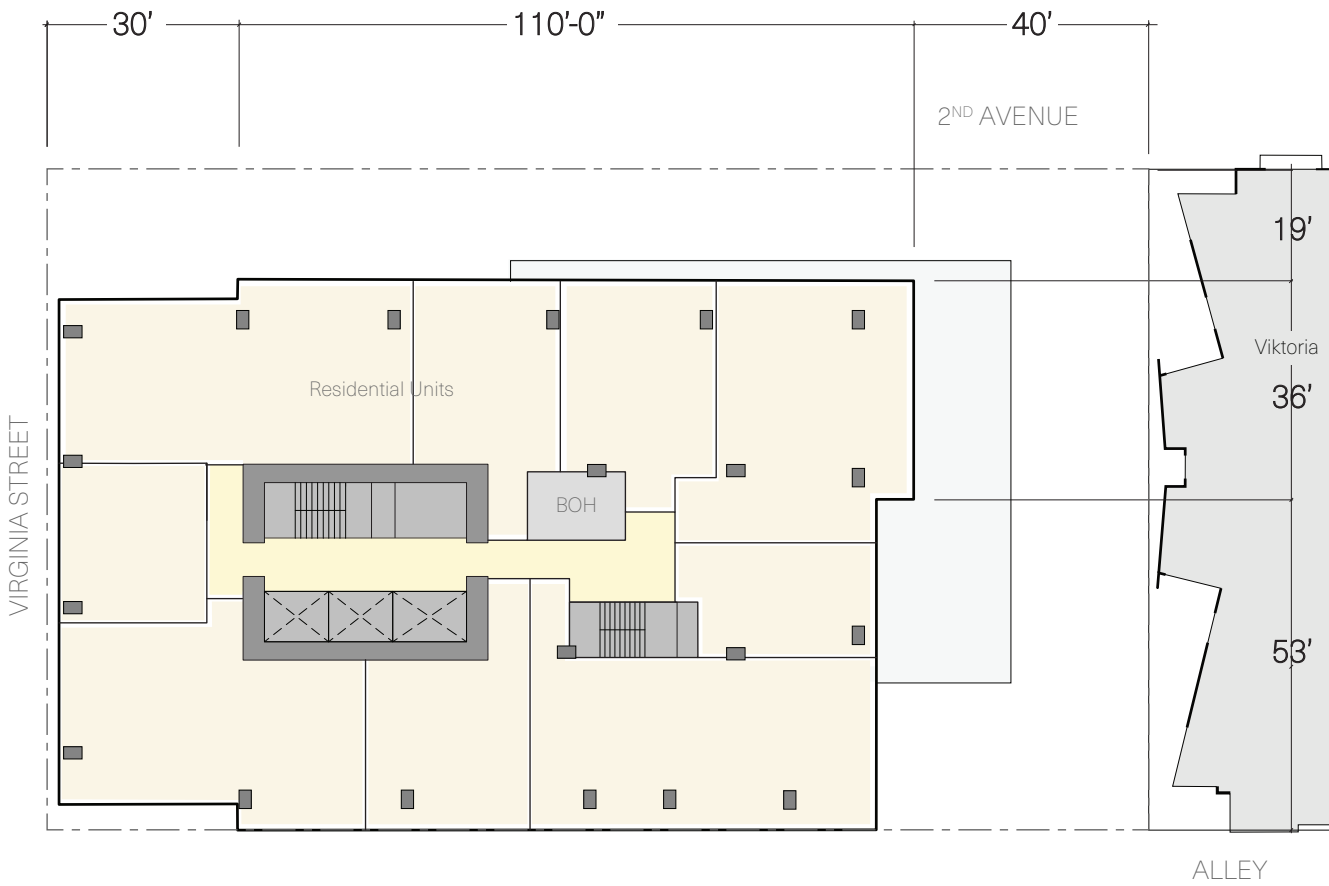
DESIGN CONCEPTS

8 Preferred Concept - Plans



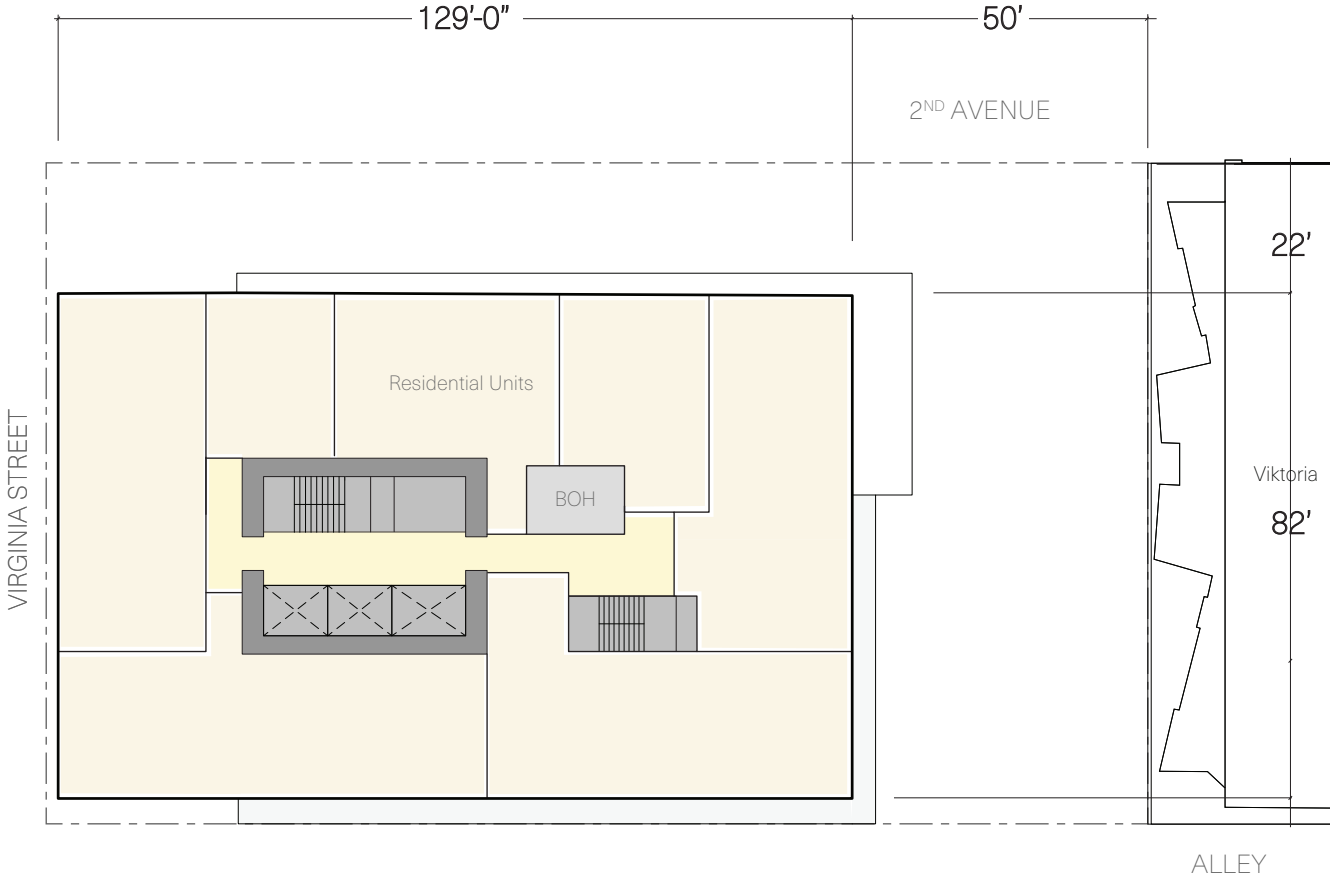


Typical Hotel Floor (L5-12)

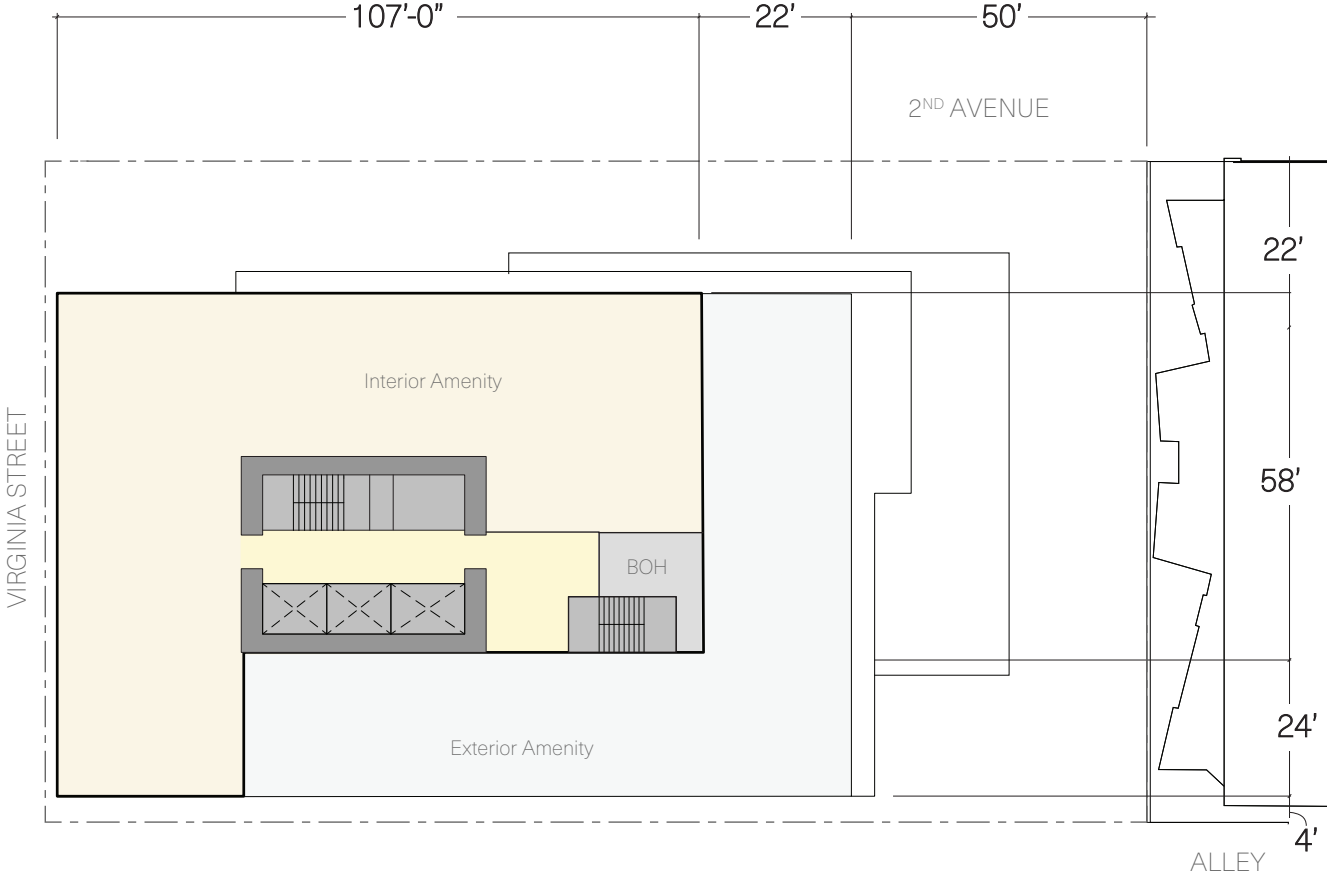


Residential Floor (L16-27)

8 Preferred Concept - Plans

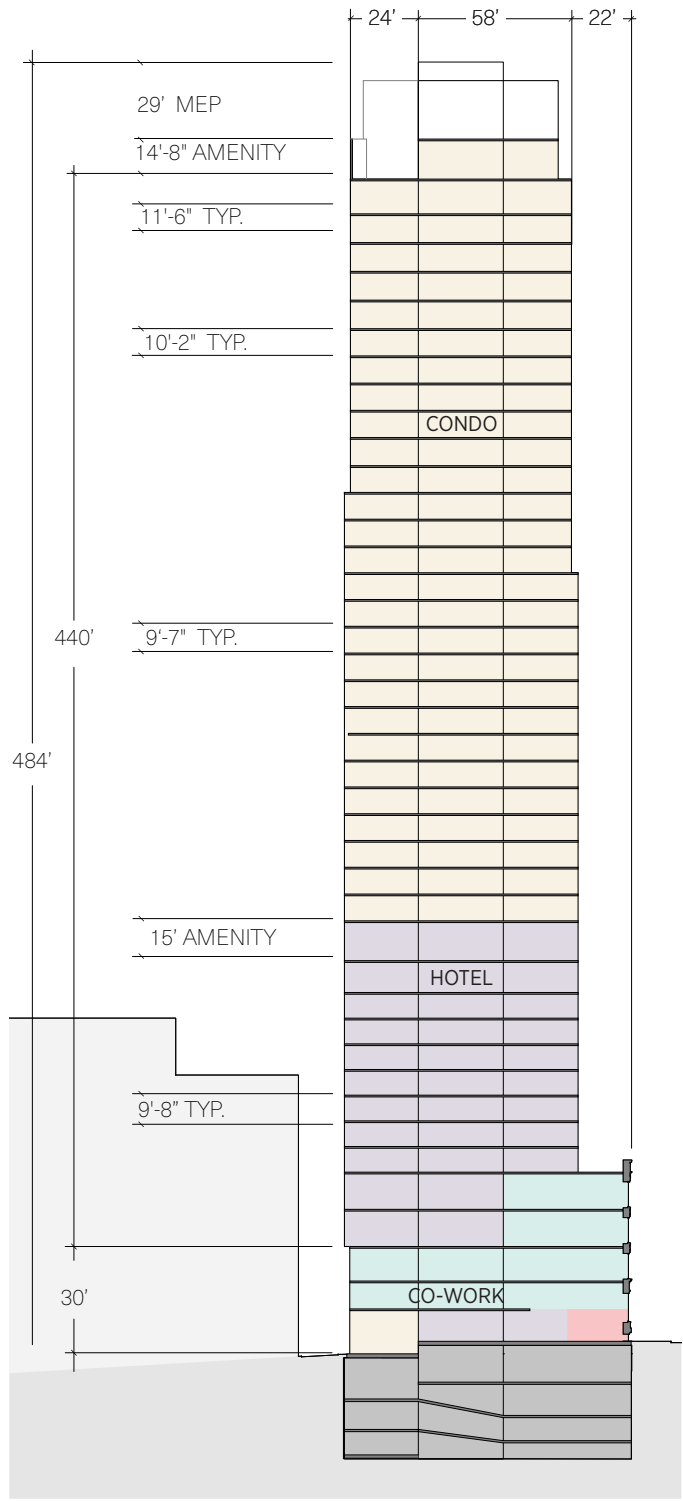


Residential Floor (L31-37)

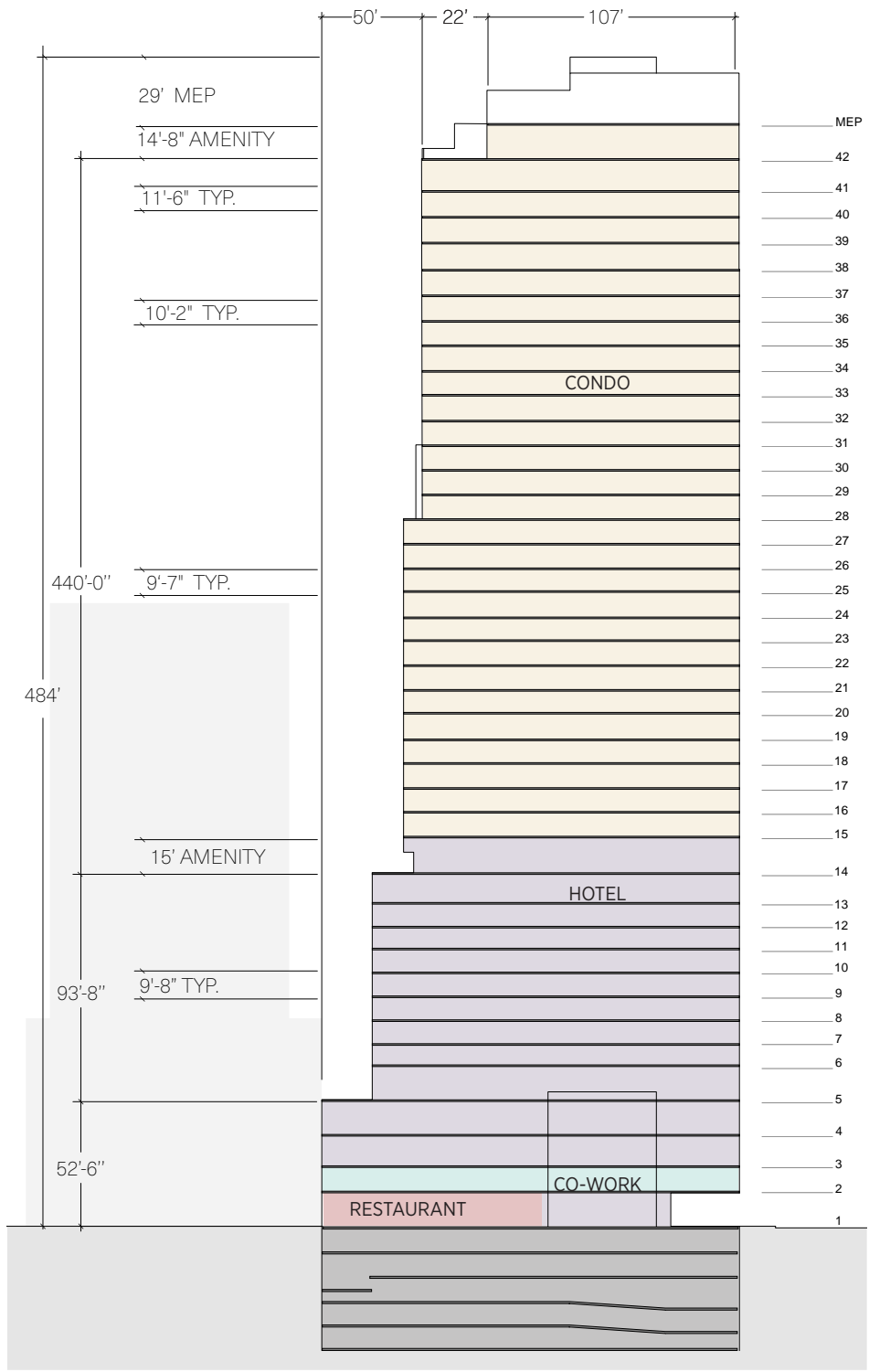


Rooftop Amenity Floor (L42)

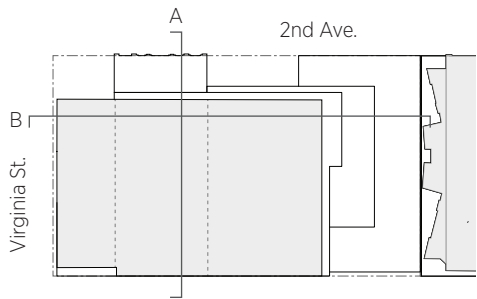
- COWORK
- HOTEL
- BOH
- RESIDENCE
- RESTAURANT
- PEDESTRIAN ENTRY
- VEHICULAR ENTRY



Section A

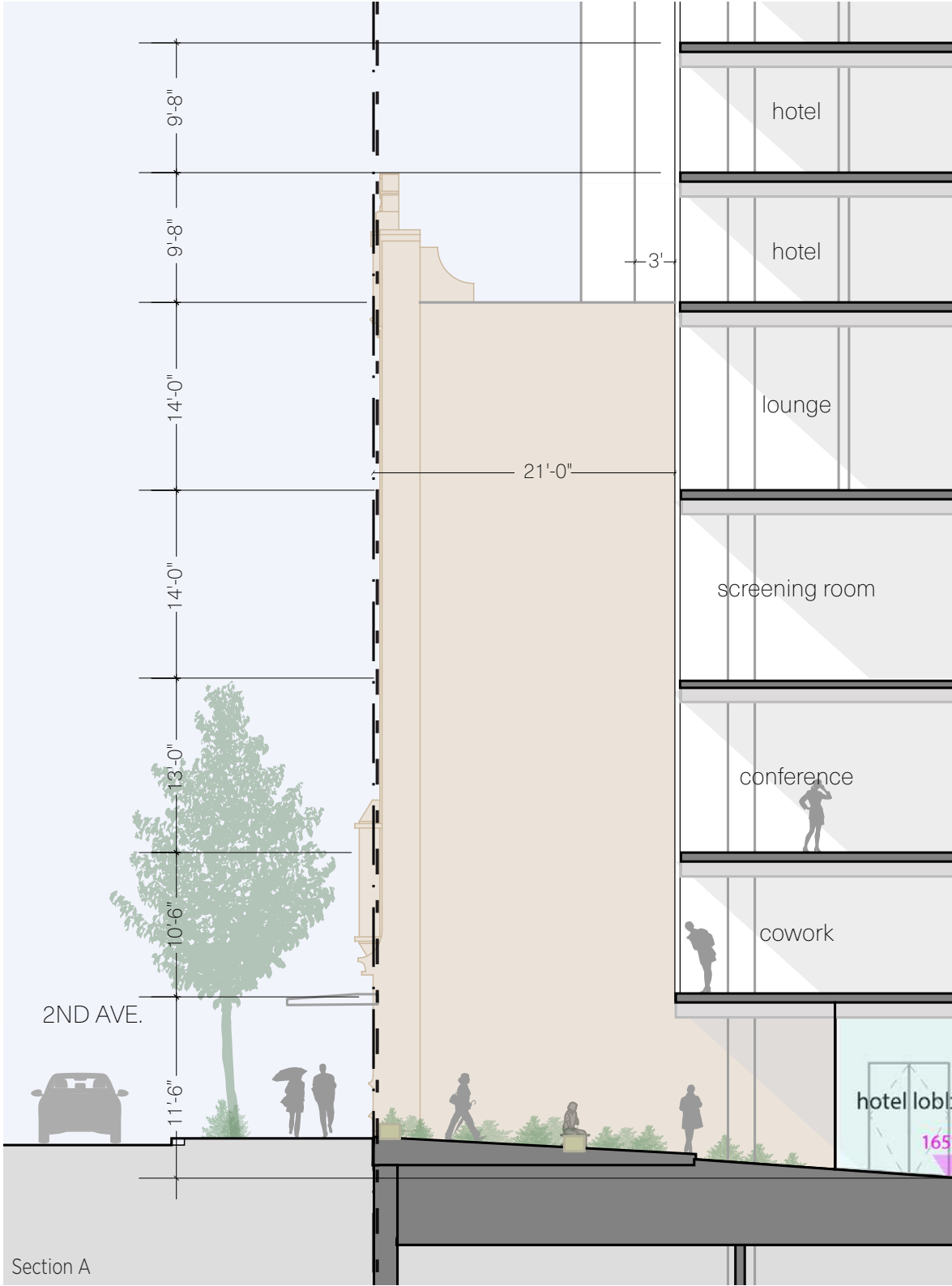
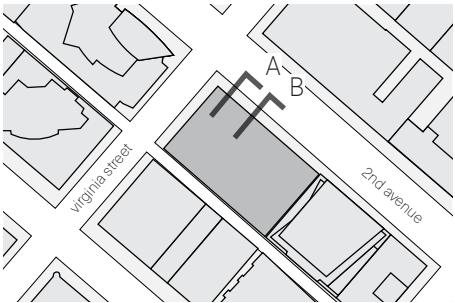


Section B

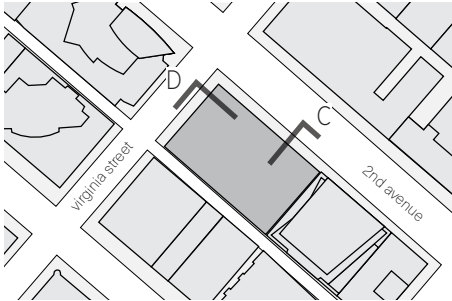
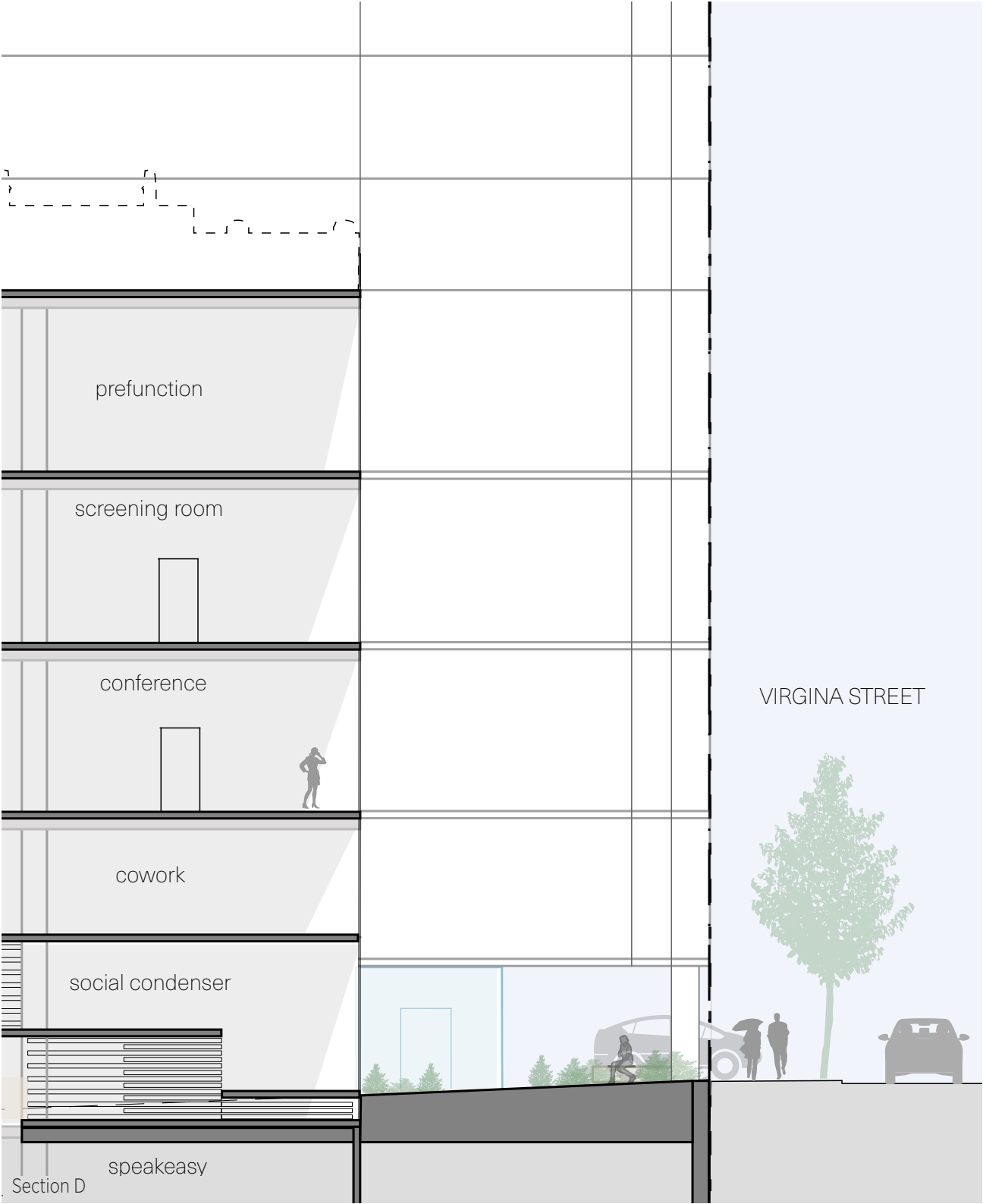
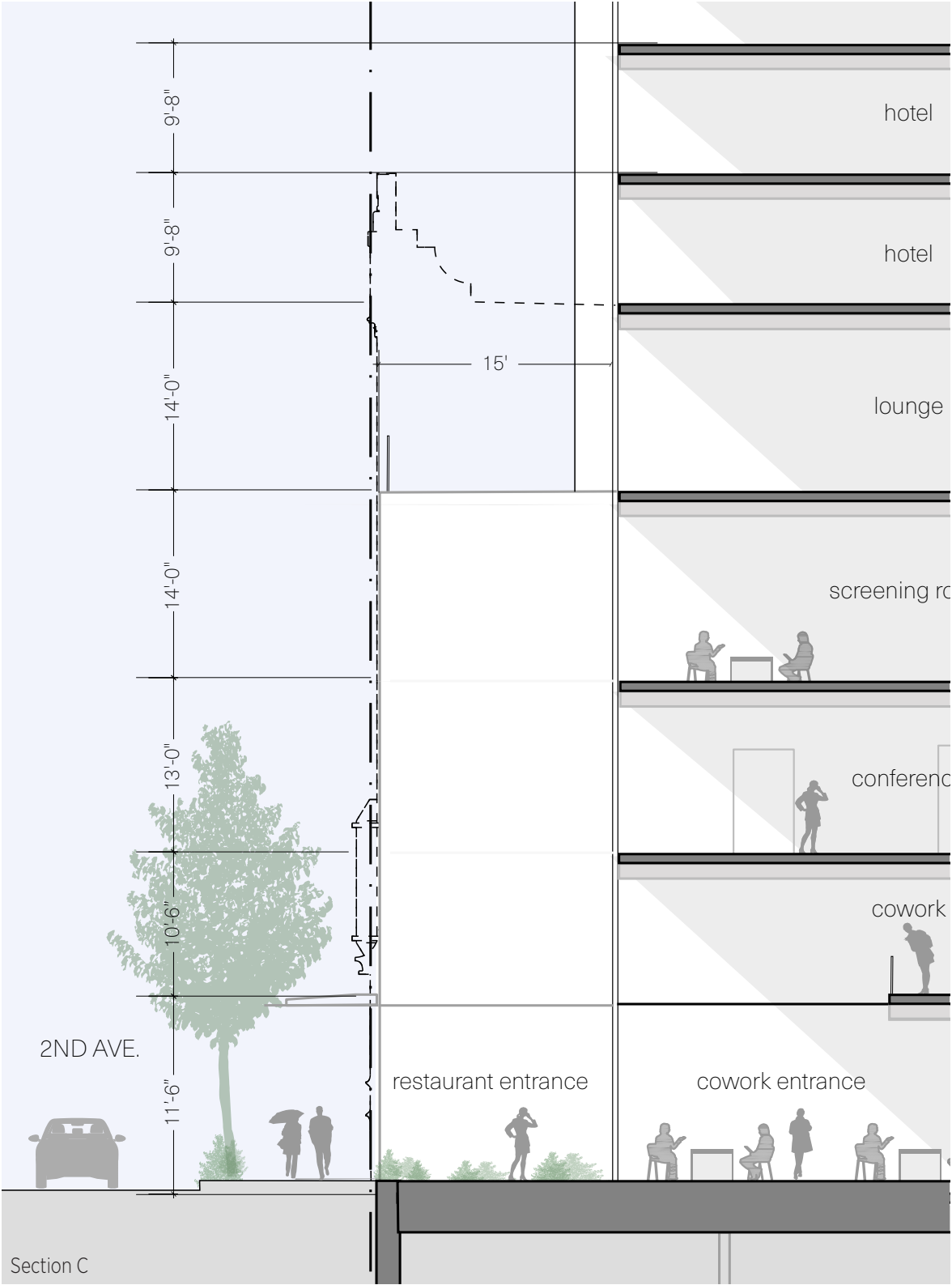


DESIGN CONCEPTS

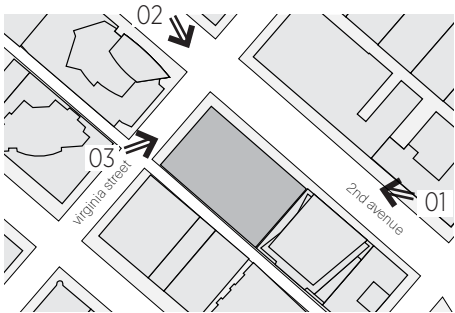
8 Preferred Concept - Street Sections



Preferred Concept - Street Sections 8



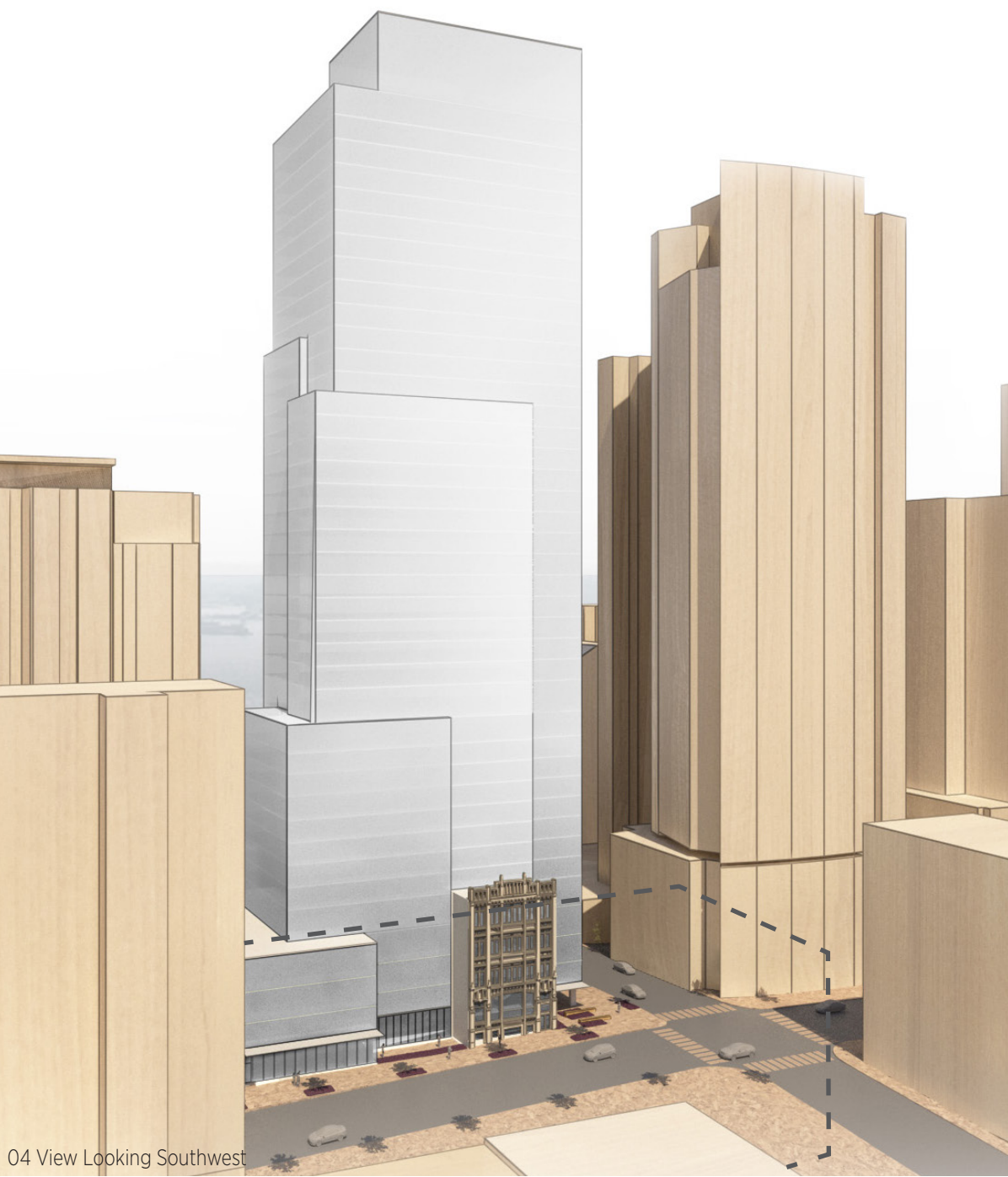
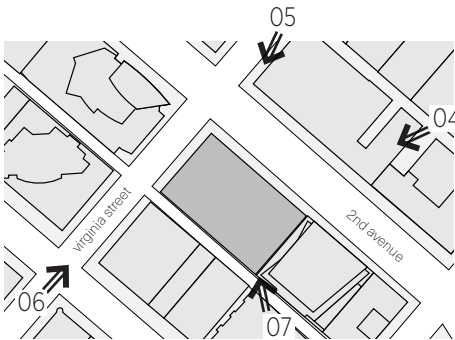
8 Preferred Concept - Renderings



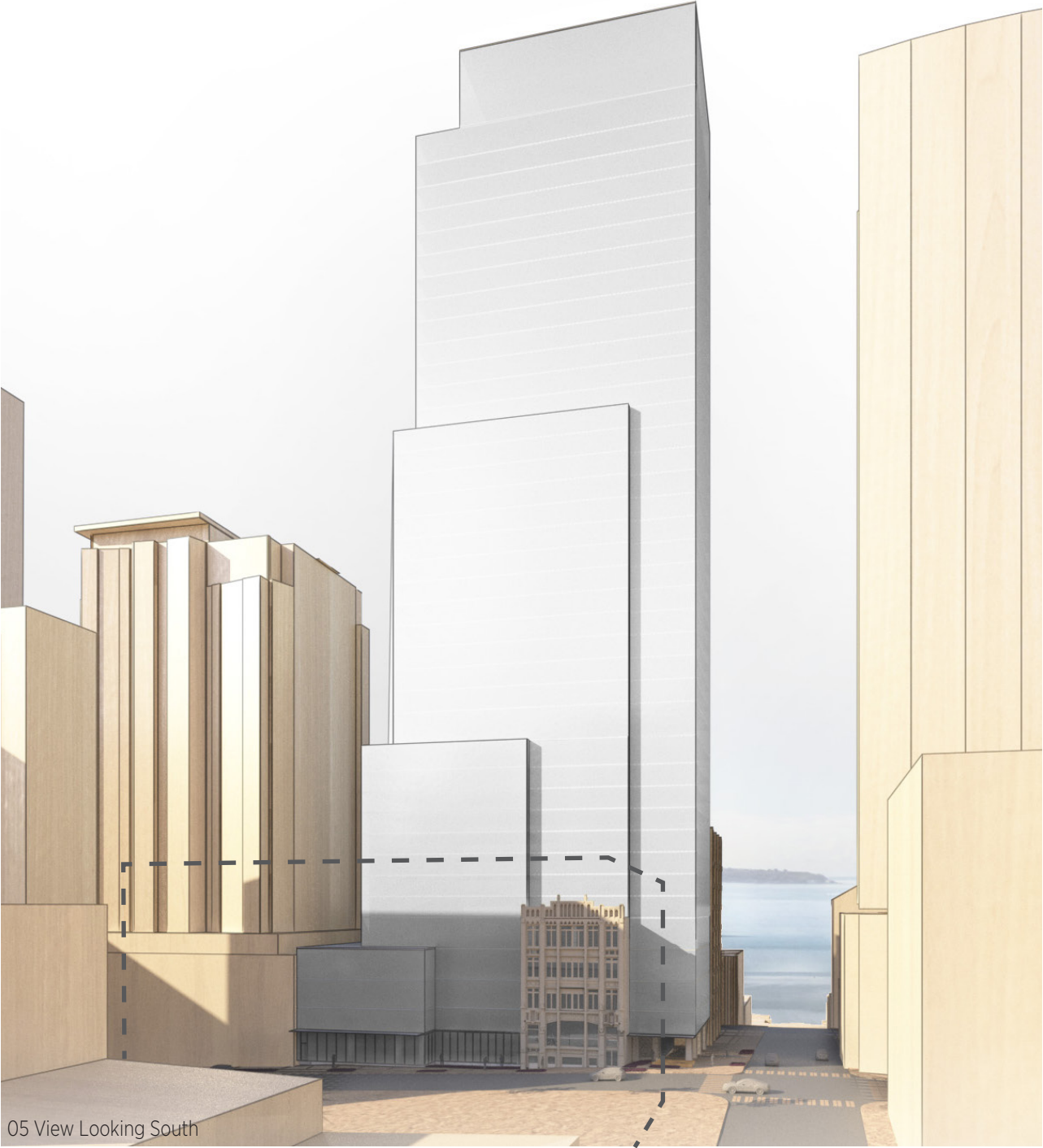
01 View Looking Northwest



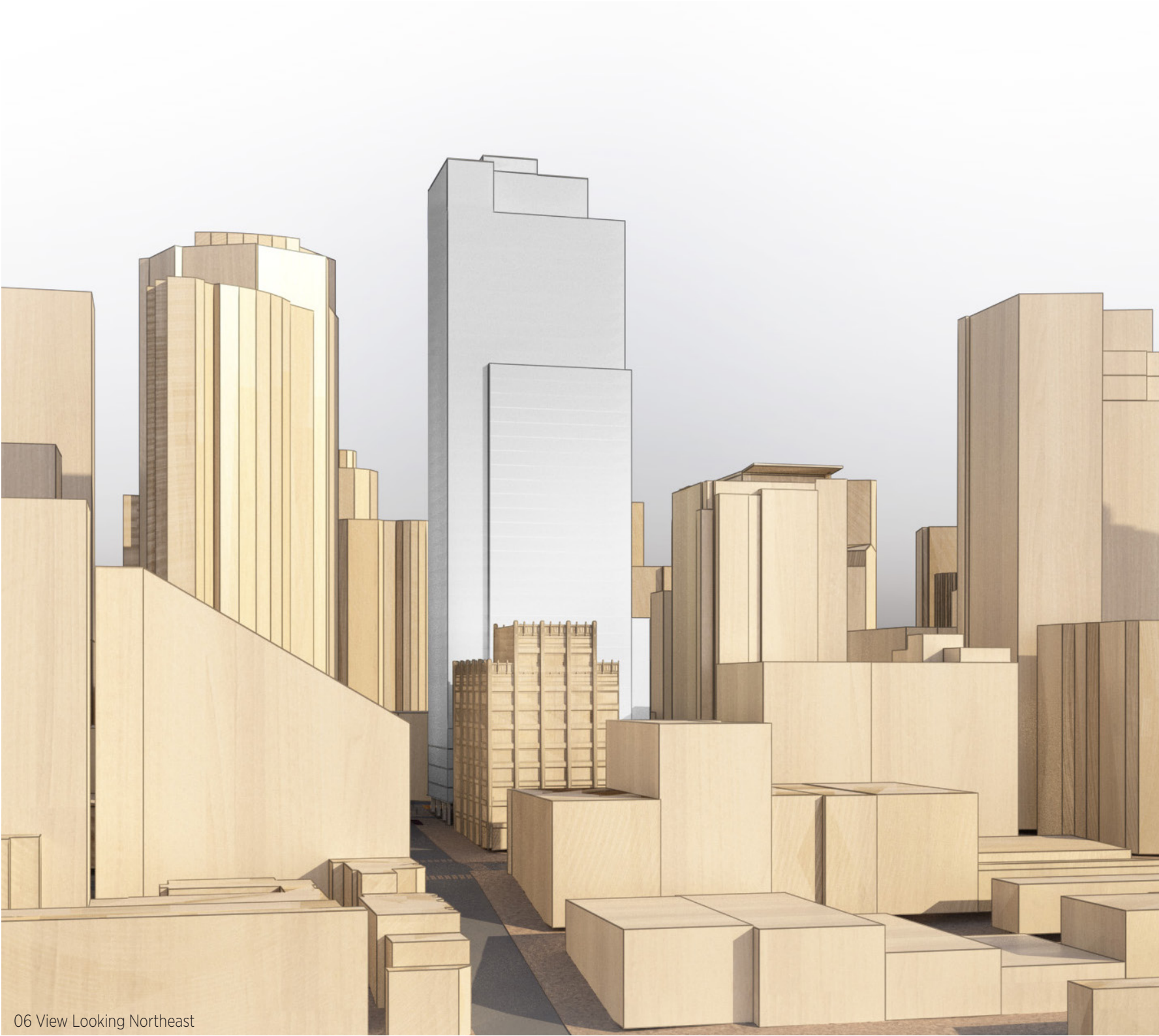
8 Preferred Concept - Renderings



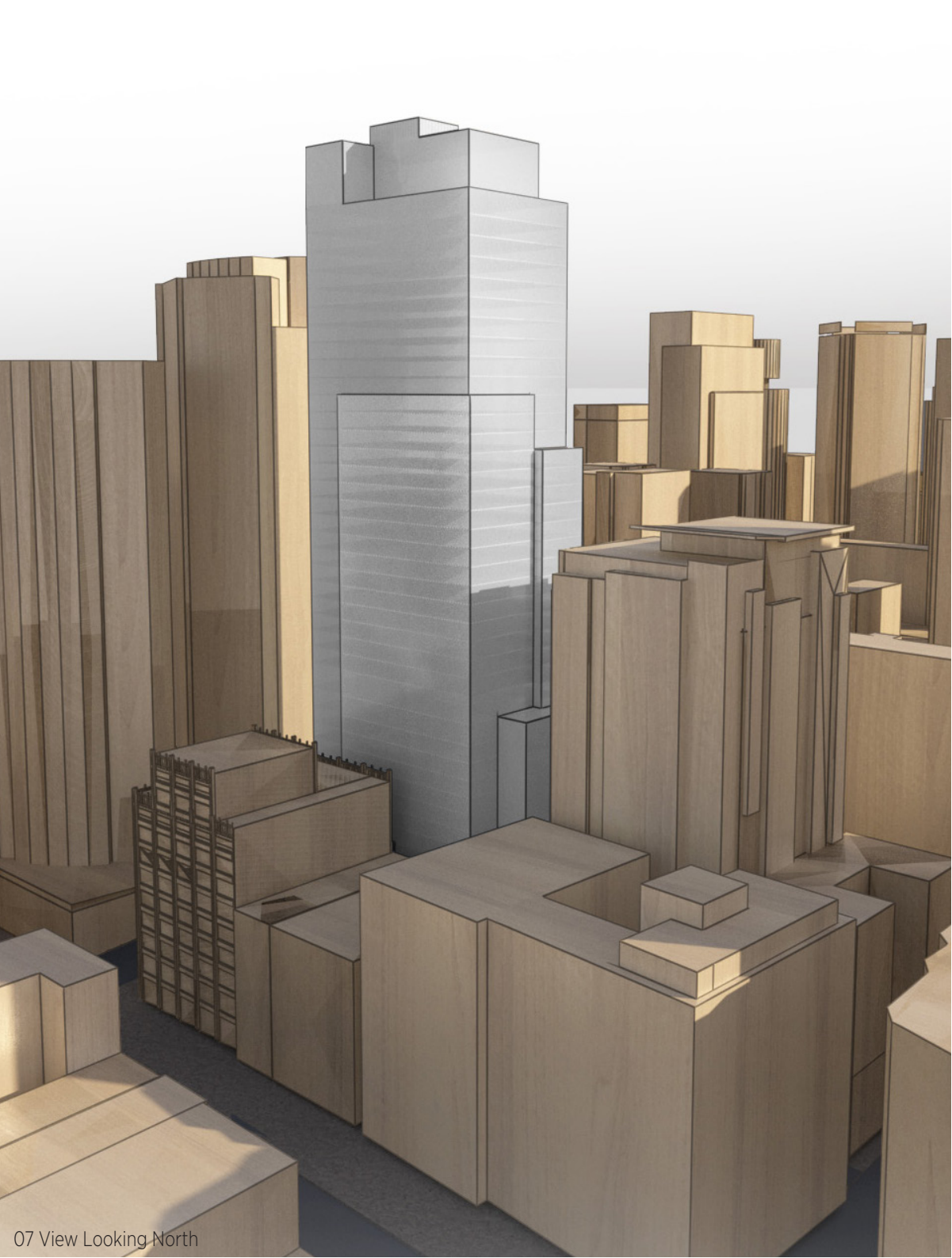
04 View Looking Southwest



05 View Looking South



06 View Looking Northeast



07 View Looking North

8 Preferred Concept - Renderings





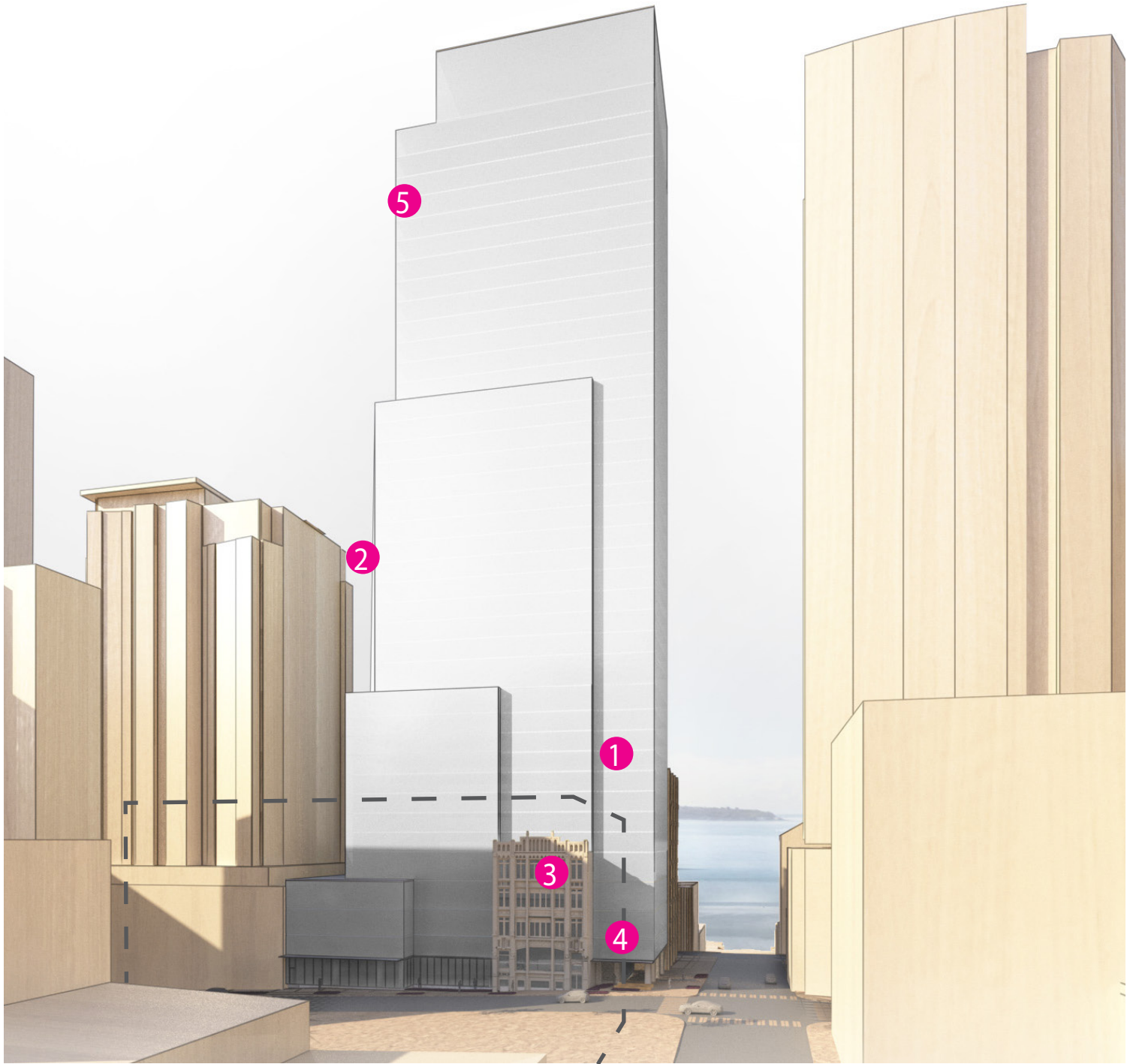
08 View Looking East

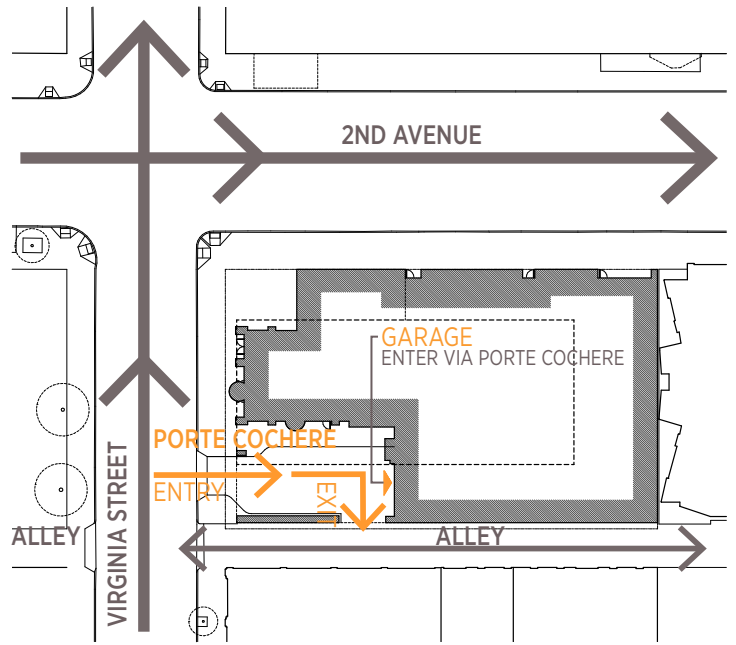
DESIGN CONCEPTS

8 Preferred Option - ARC

The Architectural Review Committee supported the preferred massing option to highlight the landmark building on site.

- 1 Terminal Sales Annex sets up alignment for the entire composition/
- 2 Setbacks give the landmark "breathing room" while still providing adequate tower spacing for Viktoria Tower.
- 3 The experience of the landmark building is implied in the function of the space.
- 4 The landmark and tower define a small corner plaza to activate the corner.
- 5 The massing recalls Seattle historical Art Deco traditions and embraces what came before.

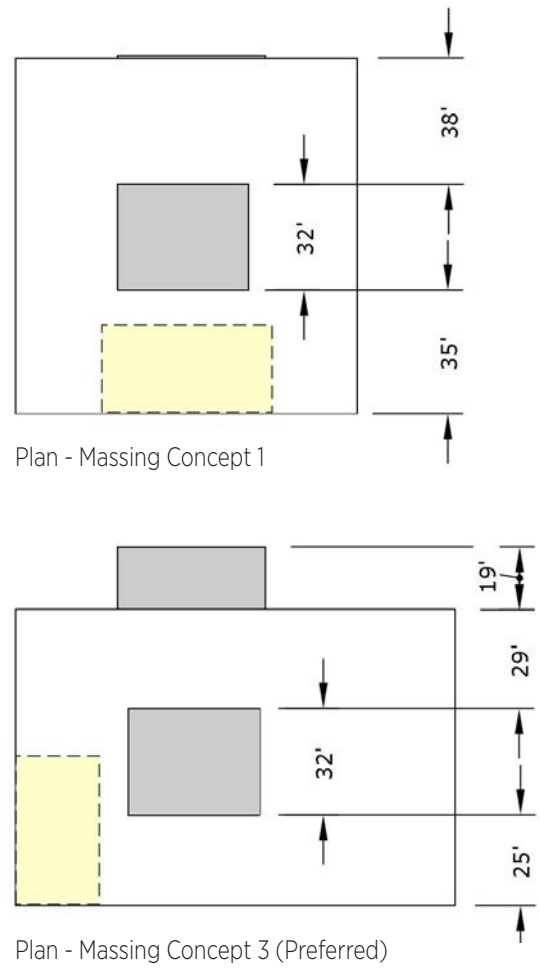




Approved MUP - Hotel Clare

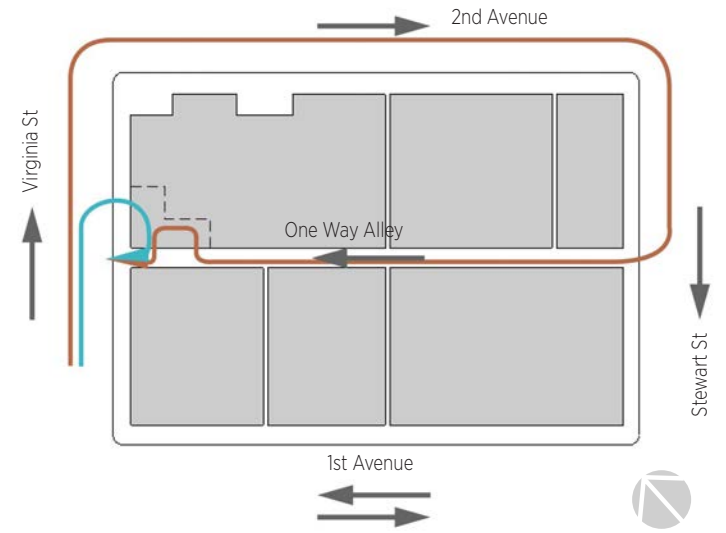
The current approved MUP set on this site uses the existing curb cut on Virginia Street to carry traffic through the site to avoid further congesting the alley and minimizing curb cuts. This addresses Design Guideline *E1 Minimize Curb Cut Impacts*.

Landmarks Preservation Board approved this design on: 6/15/2016
Downtown Design Review Board approved this design on: 6/2/2015



Tower Core Position at Ground Level

Massing Concept 3 sets the tower mass 22' back from 2nd Avenue to leave space around the Landmark. This pushes the tower's structural core toward the alley, leaving 10' less space for vehicle maneuvering relative to Massing Concept 1. Therefore, the vehicle drop-off is located on the Virginia side of the core, where there is more space.



Vehicle Access & Wayfinding

Without a Virginia Street drop-off, a visiting vehicle would need to enter the one-way alley off of Stewart Street, on the opposite side of the block as the tower, and then drive the length of the alley to the new building. Signage along Stewart Street would be required.

Our community outreach recognized the already congested alleys in downtown Seattle. By shifting those cars to Virginia Street, we do not further strain the alley. Also, by having cars pull on to our property off of Virginia Street it doesn't disrupt traffic flow on Virginia.

8 Sun/Shadow Studies - Summer Solstice



Sun/Shadow Analysis - Winter Solstice 8



8 Sun/Shadow Studies - Equinox



09 LANDSCAPE

LANDSCAPE

9 Street Level Concept

Street level design is driven by fluid transitions between streetscape, plazas, and interior spaces.

ROW, 2nd Ave Plaza, Porte Cochere

Right of Way

The ROW concept carries design moves including planting, seating and paving across the sidewalk creating continuity from plaza spaces into the public realm. The ROW concept acknowledges that there will be high demand for ride share services and access from curbside to main building entries and provides frequent pedestrian cut throughs.

Existing utilities and trees and requirements for sidewalk widths and planting strips set the framework for new tree locations and planter locations.

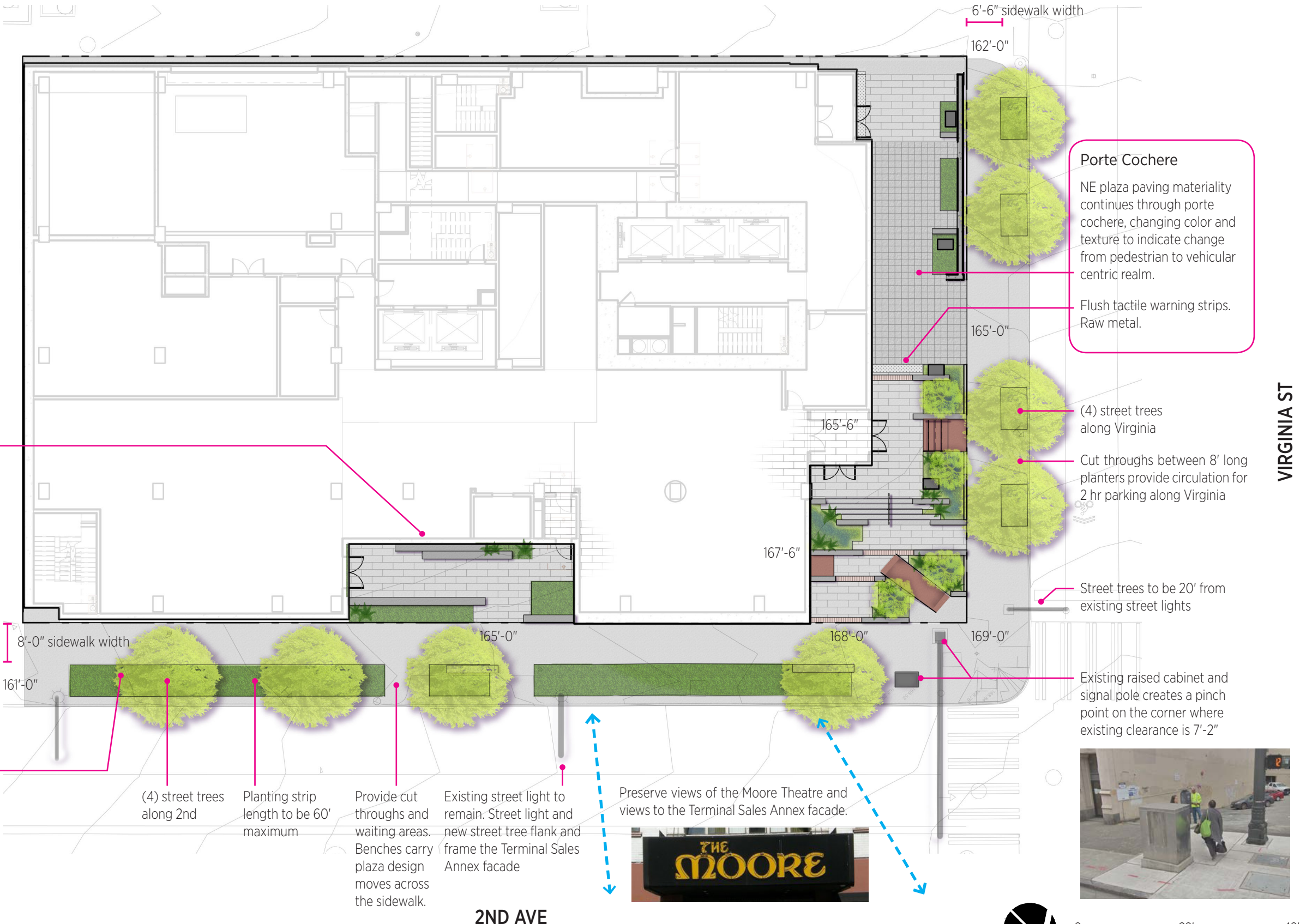
2nd Ave Plaza

Stepping seat benches carry material from NE corner and provide indoor/outdoor continuity.



Pavement pattern to carry through from the NE corner plaza

Planting to be as lush and wild as possible, typical at street level



2nd Ave

SDCI

Map A: Not within the Denny Triangle, no landscape in ROW required
Map 1C: Sidewalk Widths: 15' wide
Map 1B: Principle Transit Street
Map 1F: Pedestrian Street Classifications: Class 1 Pedestrian Street. Street Trees required.

SDOT

Seattle ROW Improvements Manual:
Step out zones required adjacent to parking/loading/transit: 24" from face of curb.
Step out zones not required adjacent to travel lanes.
Planting area 8' minimum length, 60' maximum length
6'-8' of planting zone (includes step out zones) to be provided from face of curb
8'-10' pedestrian clear space required (no furniture, poles, signs, cafes)

STD Plan 030: Minimum tree clearances, CL of tree to CL of element:

- 30' to extension of cross street curb at intersection
- 20' to utility pole with or without existing light
- 5' to underground facility
- 7'-6" to driveway or alley
- 3'-6" to face of curb
- 2' to edge of sidewalk
- 5' to hydrant, water meter, CB, manhole, sewer, etc.

Virginia St

SDCI

Map A: Not within the Denny Triangle, no landscape in ROW required
Map 1C: Sidewalk Widths: 12' wide* **
Map 1B: Minor Arterial
Map 1F: Pedestrian Street Classifications: Class 2 Pedestrian Street. Street Trees required.

SDOT

Seattle ROW Improvements Manual:
Step out zones required adjacent to parking/loading/transit: 24" from face of curb.**
Step out zones not required adjacent to travel lanes.
Planting area 8' minimum length, 60' maximum length.**
6'-8' of planting zone (includes step out zones) to be provided from face of curb.*
8'-10' pedestrian clear space required (no furniture, poles, signs, cafes).*

STD Plan 030: Minimum tree clearances, CL of tree to CL of element:

- 30' to extension of cross street curb at intersection
- 20' to utility pole with or without existing light
- 5' to underground facility
- 7'-6" to driveway or alley
- 3'-6" to face of curb
- 2' to edge of sidewalk
- 5' to hydrant, water meter, CB, manhole, sewer, etc.

Code Conflicts and Proposed Deviations

* Within a 12' wide ROW it is not possible to provide a 6' planting zone and 8' pedestrian clear space (14' total). Proposed streetscape: 5' planting zone and 6'-6" pedestrian clear space.

**Given the tight 12' ROW and required street trees, the proposed streetscape does not provide step out zones adjacent to the 2 hour parking on Virginia St. Instead, the minimum length planting zone (8') is utilized to provide pedestrian cut throughs.



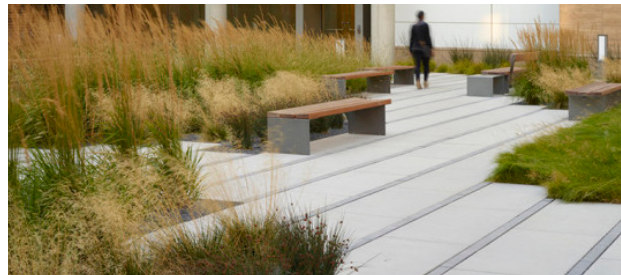
LANDSCAPE

9 NE Plaza Concept

Setting back the Virginia Street edge provides space for ADA accessible grades to flow to pedestrian building entries as they step down with sidewalk grade. This setback space creates an opportunity for a widened pedestrian realm, indoor/outdoor visual access, and interesting, permeable corner gathering space.

The plaza creates visual interest from the sidewalk and from inside the building while providing a welcoming area to travel through or gather. Permeability to the streets and building edges is a primary driver of this space.

Paving pattern is a linear play off the 2x2 Seattle standard sidewalk grid, and flows from the building entries into the sidewalk grid and porte cochere. Paving, stone, and metal elements float over lush planting and bridge between interior and exterior spaces.



Character: PAVING RHYTHM, PLANTING



Character: 'FLOATING' CHATTERED STONE STAIRS



Character: INDOOR / OUTDOOR ROOM

ELEMENTS IN THE SPACE MAY INCLUDE

- benches of reclaimed timbers, concrete, or boulders •



- corten edging •



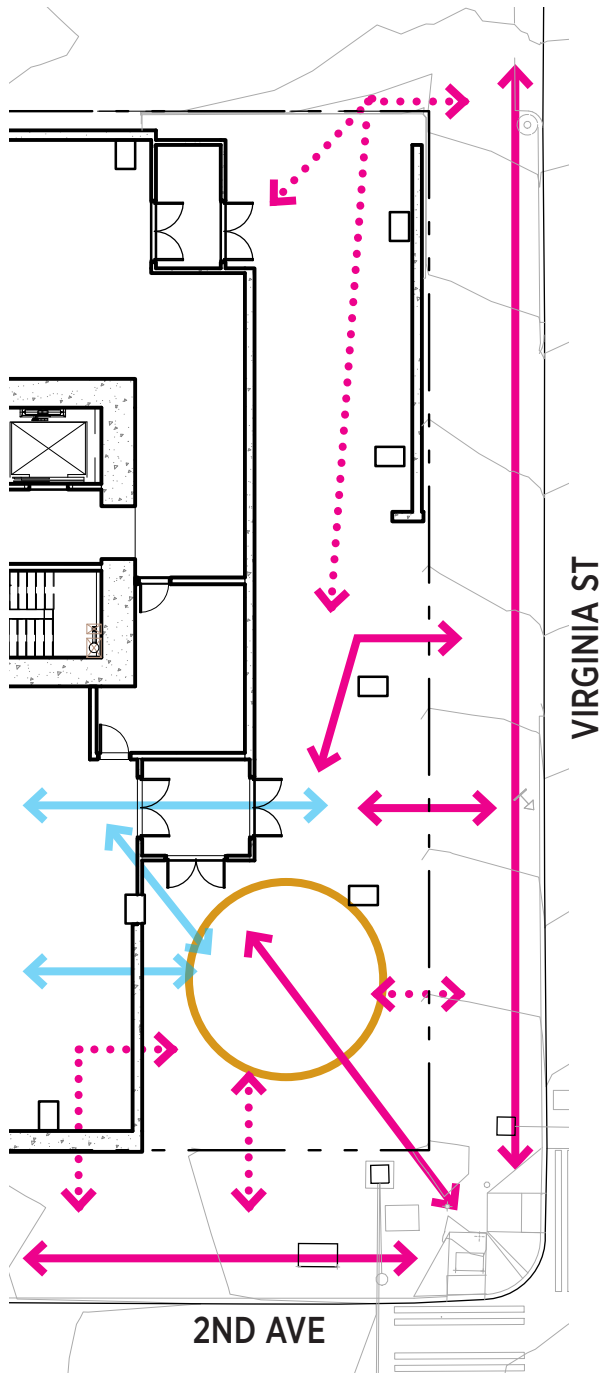
- bioretention areas •



- underlit glass elements •



- metal 'bridging' elements •



Tower Terrace Concept 9

5th, 6th, 14th, and 42nd Floors

The tower carries various occupiable and visually accessible terraces. The concept for these spaces is flexible and modular, anchored with green edges and views of the City and Sound. A family of design language will be used to create design continuity throughout the project. Stormwater management will be integrated throughout.



42nd Floor Terrace

Tall wind screens will protect this area for condo residents. A difficult area for plants to thrive, this terrace will focus on hardscape moves accented with simple, rugged plant species and visual appeal from interior spaces while creating outdoor rooms for hosting different groups simultaneously.

14th Floor Terrace

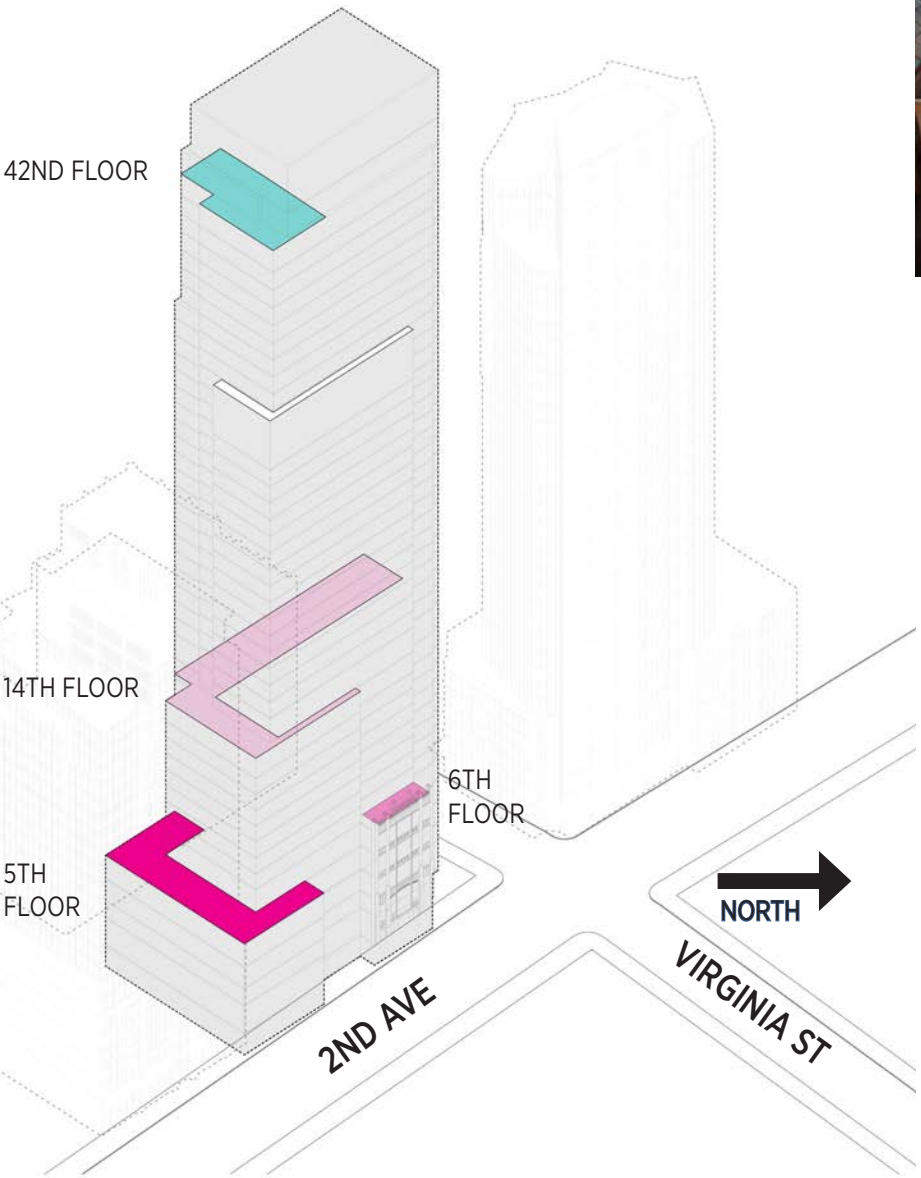
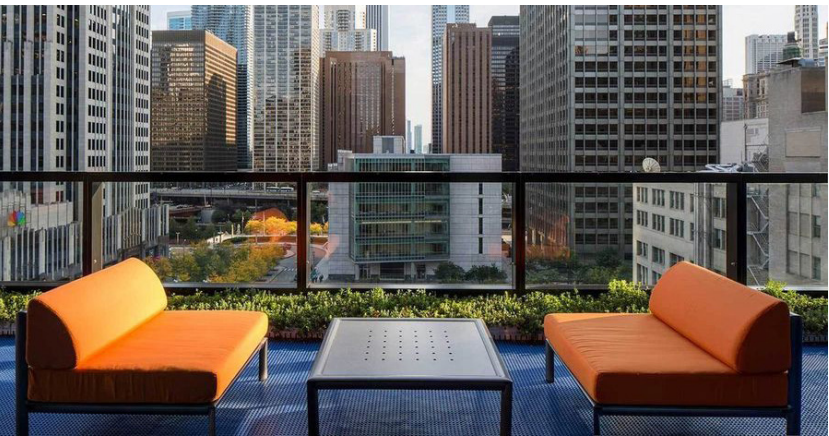
The bar and pool terrace, this area is very modular for ease of hosting hotel events of various sizes. Edges will be lushly planted. Walls will be operable to create an indoor/outdoor experience.

6th Floor Terrace

Hotel room terraces provide outdoor respite for occupants while continuing the design language of the NE corner when viewed from above.

5th Floor Terrace

Accessible to staff, this terrace includes a productive space for the kitchen and visually accessible landscape for hotel tenants.



This page intentionally left blank.

10 DEPARTURES

DEPARTURES

10 Departure 1

Street Facade, Landscaping, and
Street Setback Requirements
SMC 23.49.056

"Along 2nd Avenue, facades between 15 and 35 feet above sidewalk grade shall be located within 2-feet of the street lot line, except as permitted below:

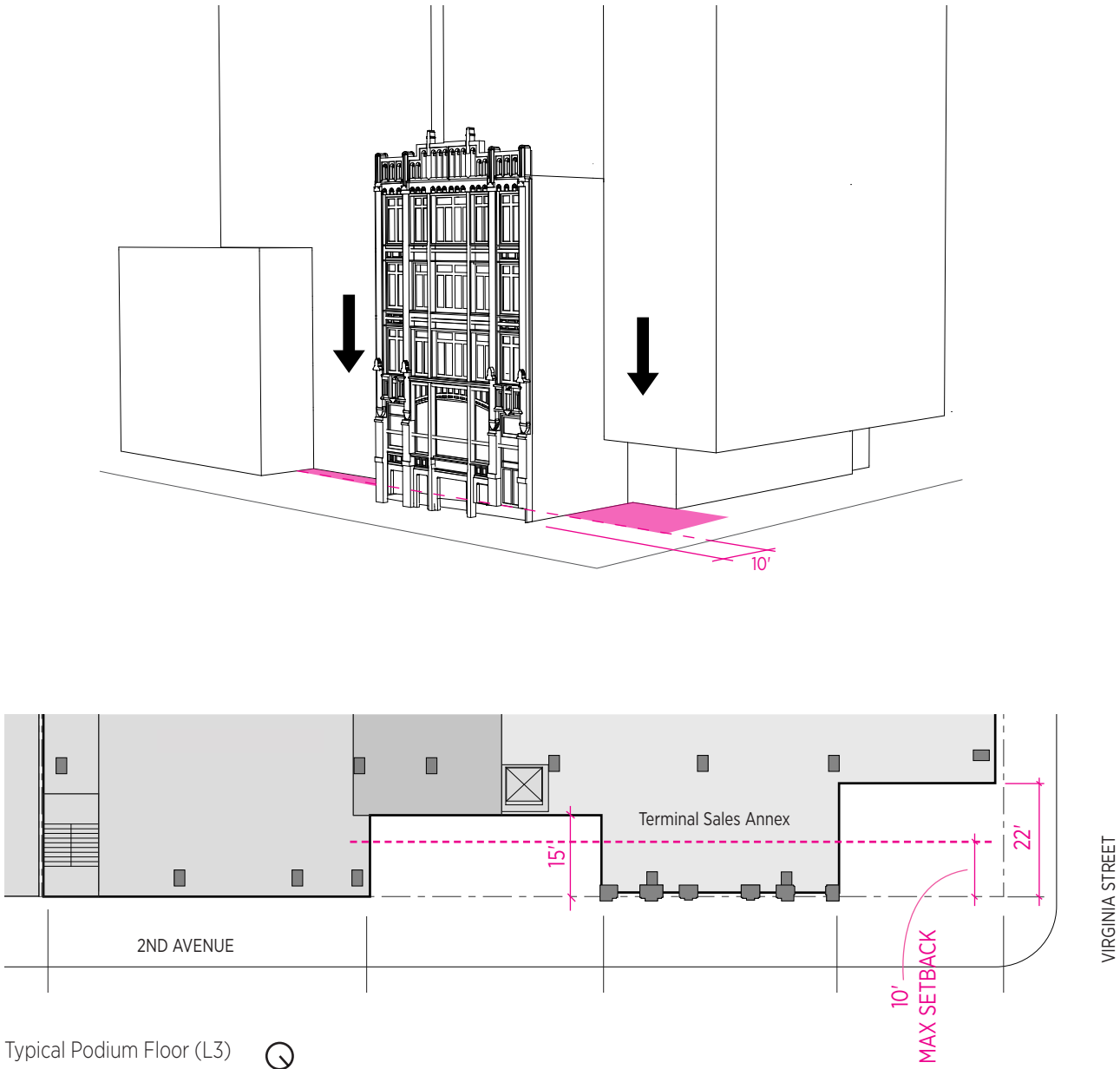
- Maximum setback is 10 feet"

REQUEST:
A departure is requested to setback the facades surrounding the Terminal Sales Annex (TSA) greater than 10'. We are requesting 15' on the south and 22' on the north.

JUSTIFICATION:
South of the TSA: The #1 priority of the Landmarks Preservation Board (LPB) is to set back the new tower from the landmark structure. In conversations with the ARC over the years, a 15' setback is a reasonable starting point for tower setback from the Landmark. In this location, which supports our overall design concept, the tower form comes down the ground thus creating the 15' setback.
North of the TSA: As the tower begins to telescope and step back from the Landmark, the setback dimensions transition from 15' to 22' at the corner. This tower setback allows visibility to the Terminal Sales Annex and creates a grand open space to denote entry and respite for pedestrians.

RELEVANT DESIGN GUIDELINES:
B-2 Create a Transition in Bulk and Scale
B-3 Reinforce Positive Urban Form & Arch. Attributes
D-1 Provide Inviting and Usable Open Space

LANDMARK PRESERVATION BOARD PRIORITIES:
1. Set tower back from Landmark
2. Express volume of Landmark through the building
4. Retain existing entry door of Landmark
6. Tower expression should draw inspiration from TSA
7. Landmark to complete corner



Departure 2 **10**

Street Facade, Landscaping, and
Street Setback Requirements
SMC 23.49.056

"Along 2nd Avenue, facades between 15 and 35 feet above sidewalk grade shall be located within 2-feet of the street lot line, except as permitted below:

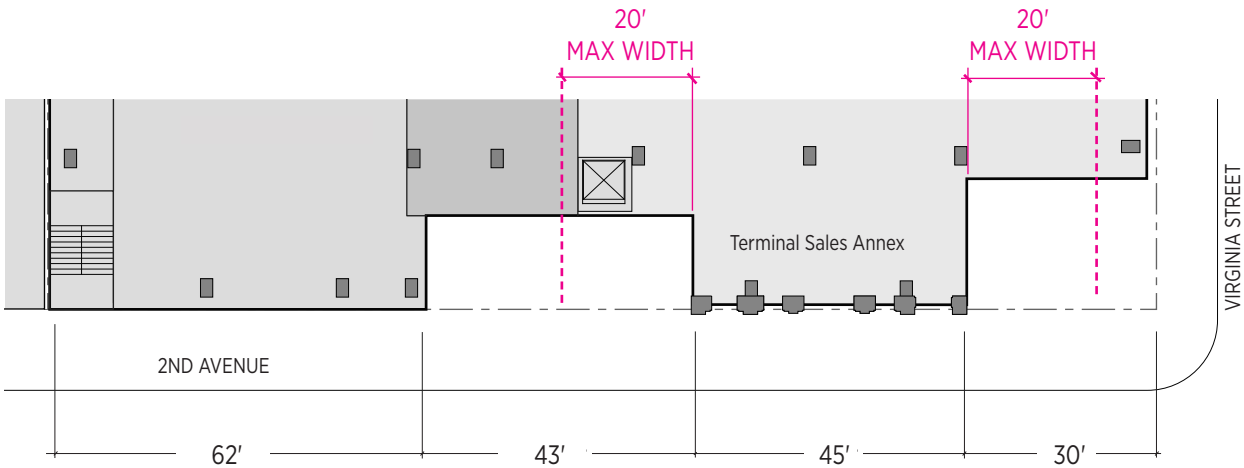
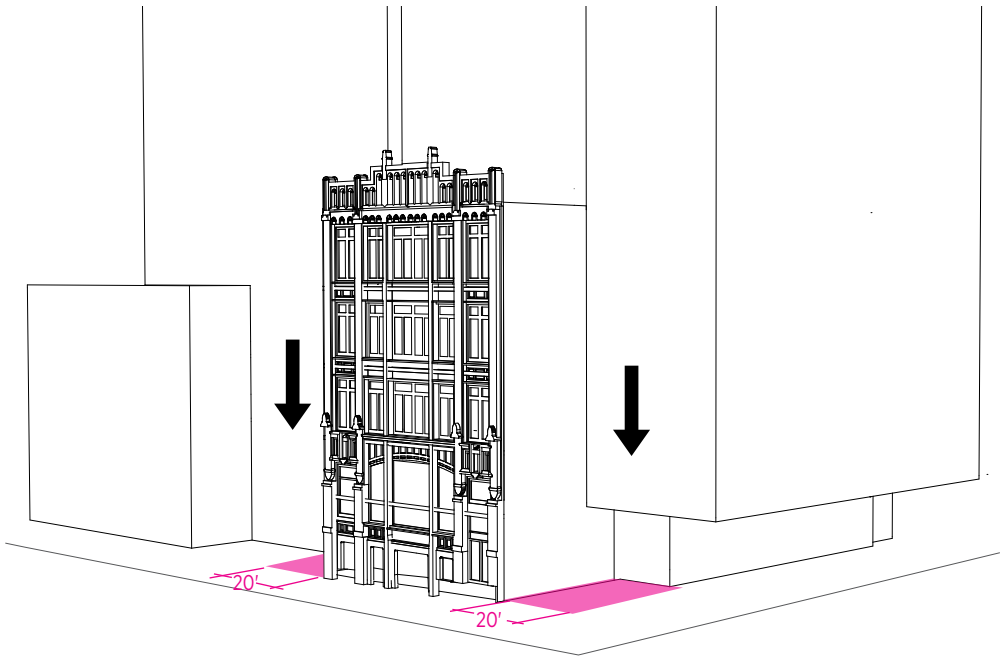
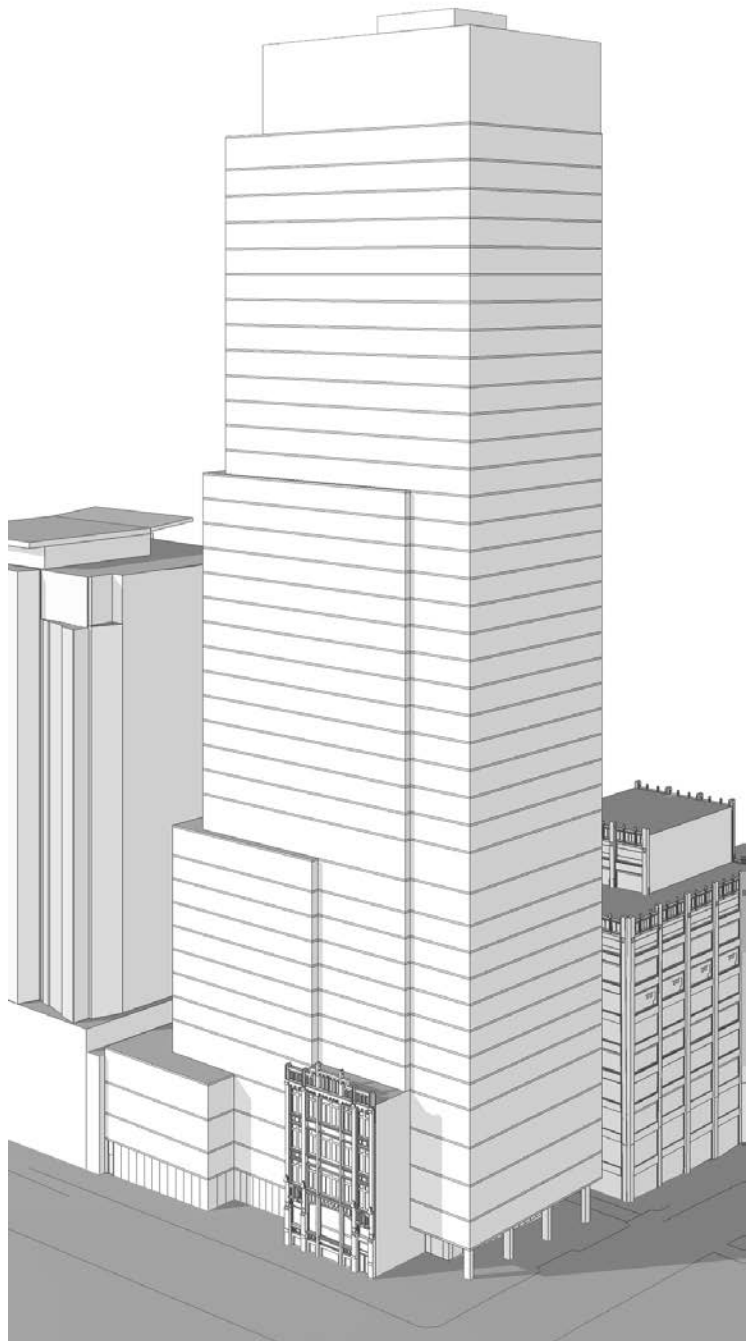
- **No setback deeper than deeper than 2-feet shall be wider than 20-feet"**


REQUEST:
A departure is requested to increase width of setback to relate to the TSA on the south, while opening the corner to Landmark on the North. We are requesting 43' on the south and 30' on the north.

JUSTIFICATION:
South of the TSA: The width of the setback on the south of TSA is directly related to the width of the TSA itself. The TSA is approximately 45' wide and the width of the setback is approximately 43'. This width also is directly related to the proportions of the tower's telescoping form concept.
North of the TSA: To create a corner open space and for the Landmark to 'front' the corner, a greater setback is required. This corner open area reveals the lower edge of the tower as it descends from above, hovering just above ground at the main entry.

RELEVANT DESIGN GUIDELINES:
A-1 Respond to the physical environment
B-2 Create a transition in bulk and scale
D-1 Provide inviting and usable open space

LANDMARK PRESERVATION BOARD PRIORITIES:
1. Set tower back from Landmark
2. Express volume of Landmark through the building
4. Retain existing entry door of Landmark
6. Tower expression should draw inspiration from TSA
7. Landmark to complete corner



Typical Podium Floor (L3) 

DEPARTURES

10 Departure 3

Maximum Tower Width
SMC 23.49.058C.2

"In DMC zones, the maximum facade width for portions of a building above 85 feet along the general north/south axis of a site (parallel to the Avenues) shall be 120 feet or 80 percent of the lot measured on the Avenue, whichever is less"

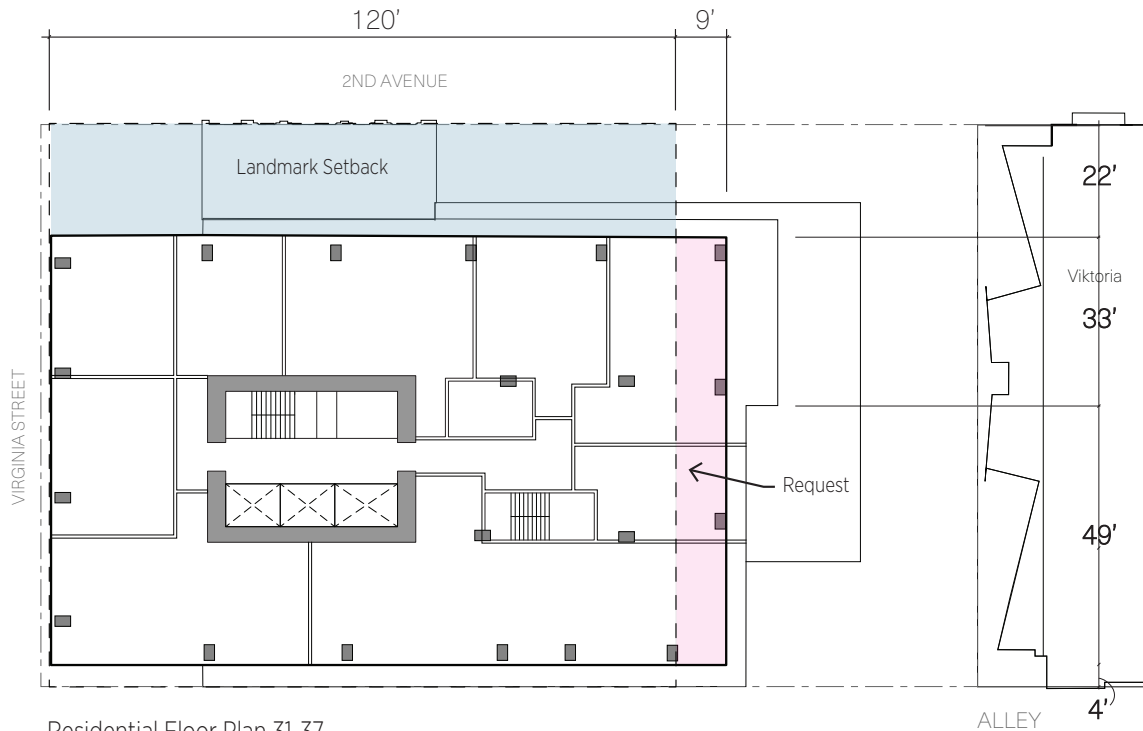
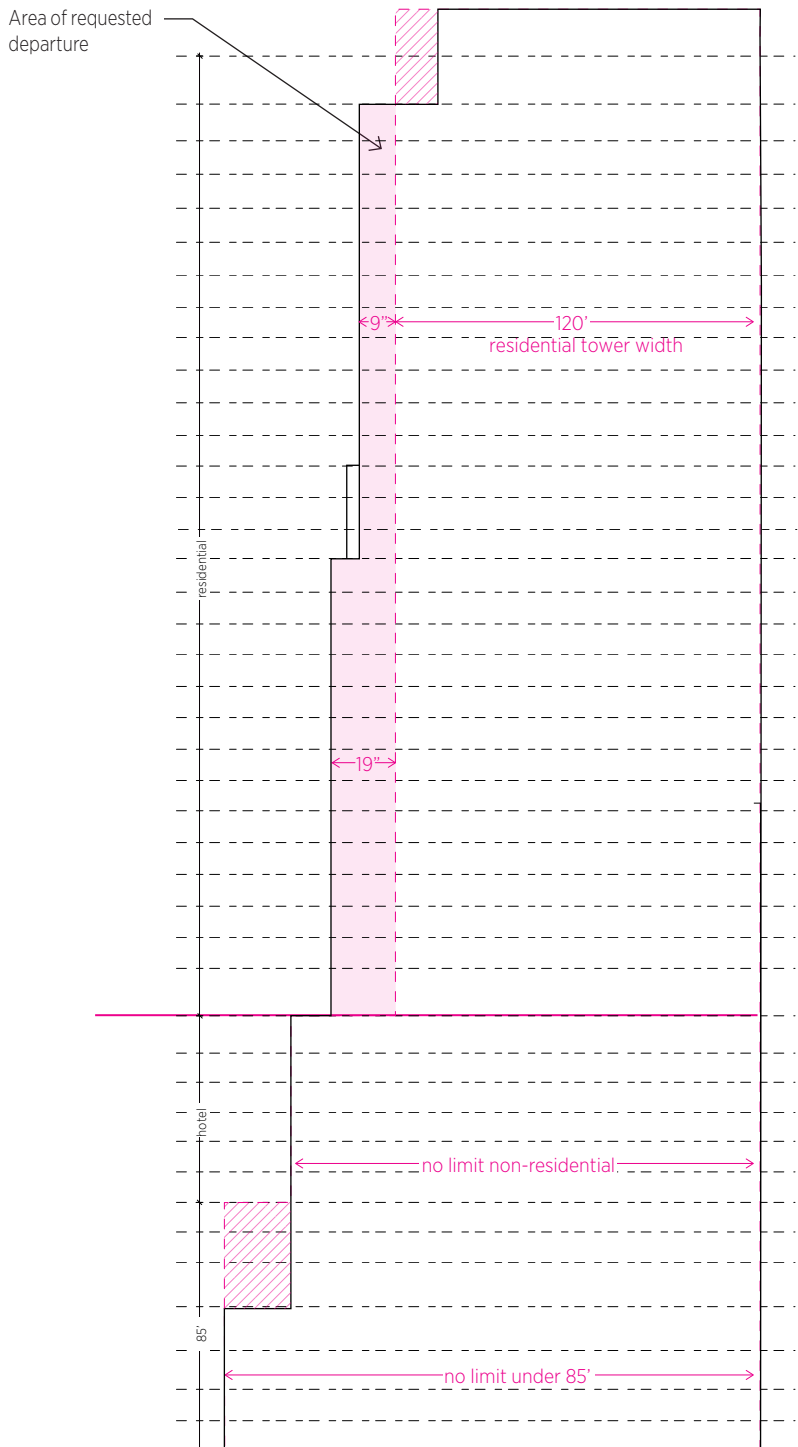
REQUEST:
A departure is requested to extend the residential portion of the tower by 19' at the widest section.

JUSTIFICATION:
The current maximum facade width is not ideal for the historic landmark on site. When the tower sets back from the landmark, it immediately creates a more rectangular tower which does not allow an average tower plate of 10,700sf without increasing the overall width.

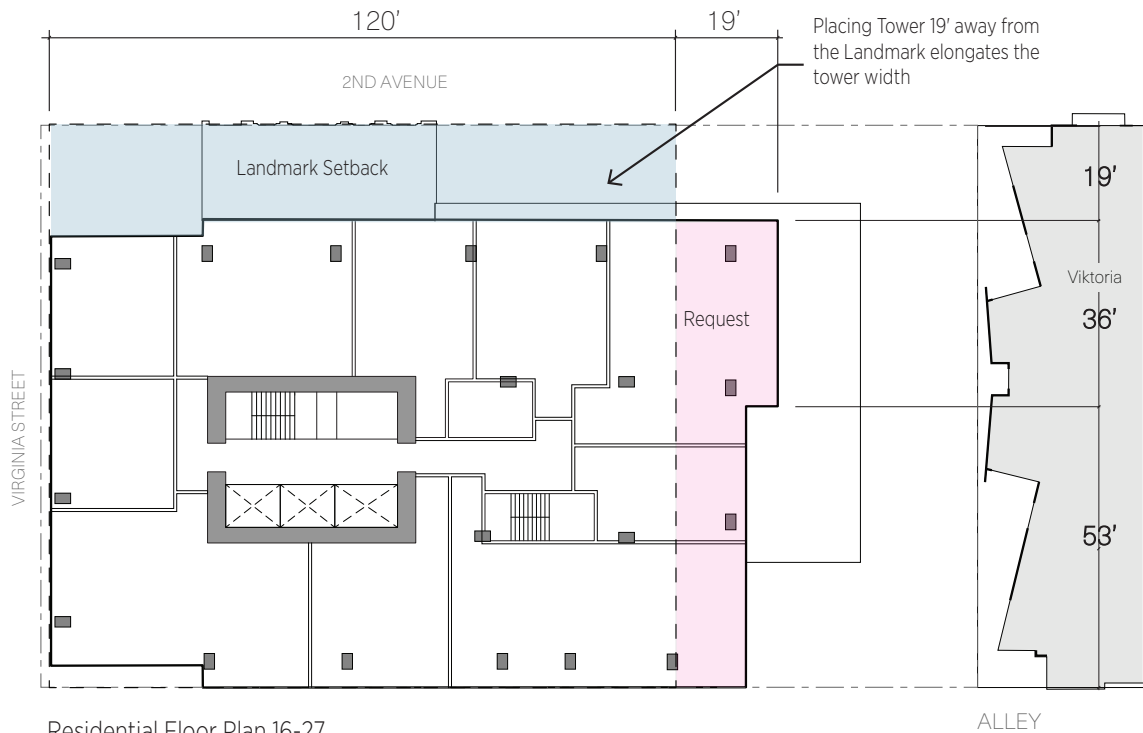
By setting the entire tower back from the landmark by 19' (it ranges from 19'-22') it essentially widens the tower by 19' to create an average residential footprint of 10,700sf.

RELEVANT DESIGN GUIDELINES:

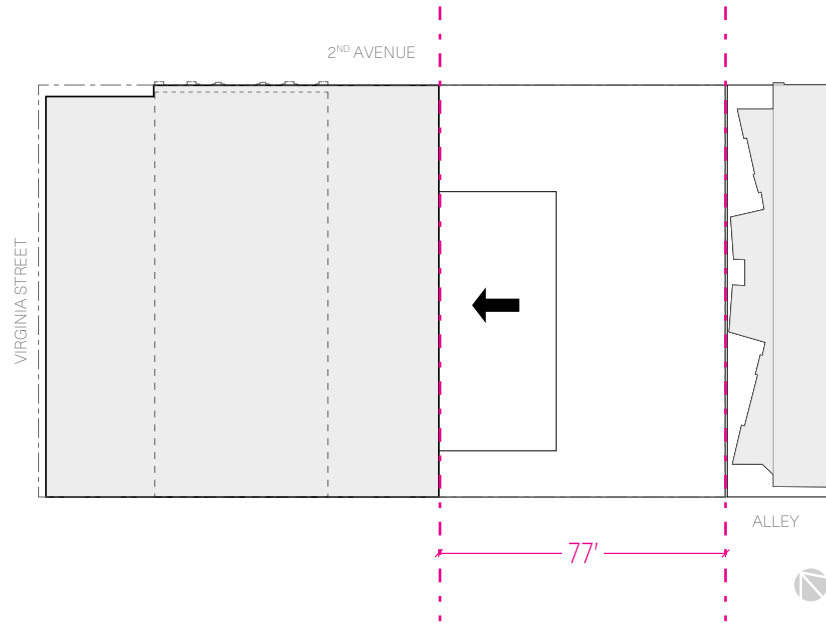
- B-1 Respond to the Neighborhood Context
- B-2 Create a Transition in Bulk and Scale
- B-3 Reinforce Positive Urban Form & Arch. Attributes



Residential Floor Plan 31-37

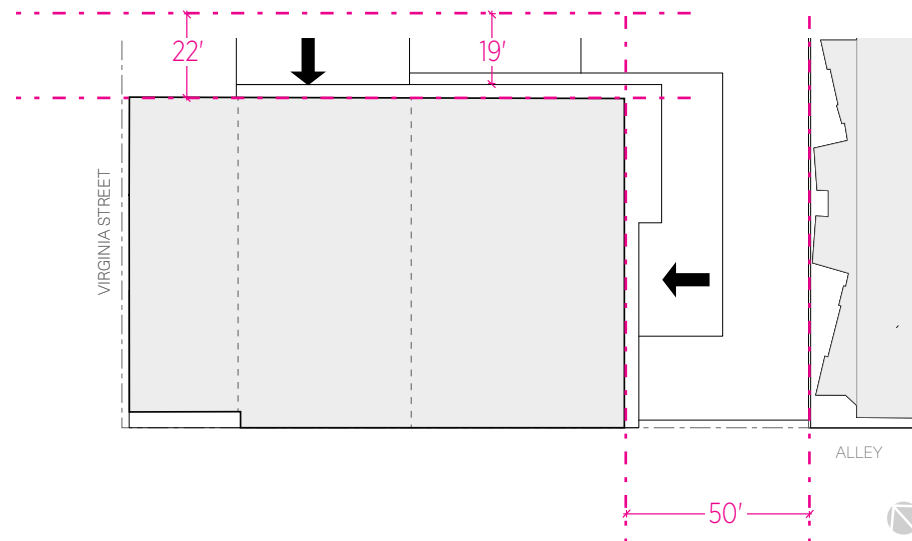


Residential Floor Plan 16-27



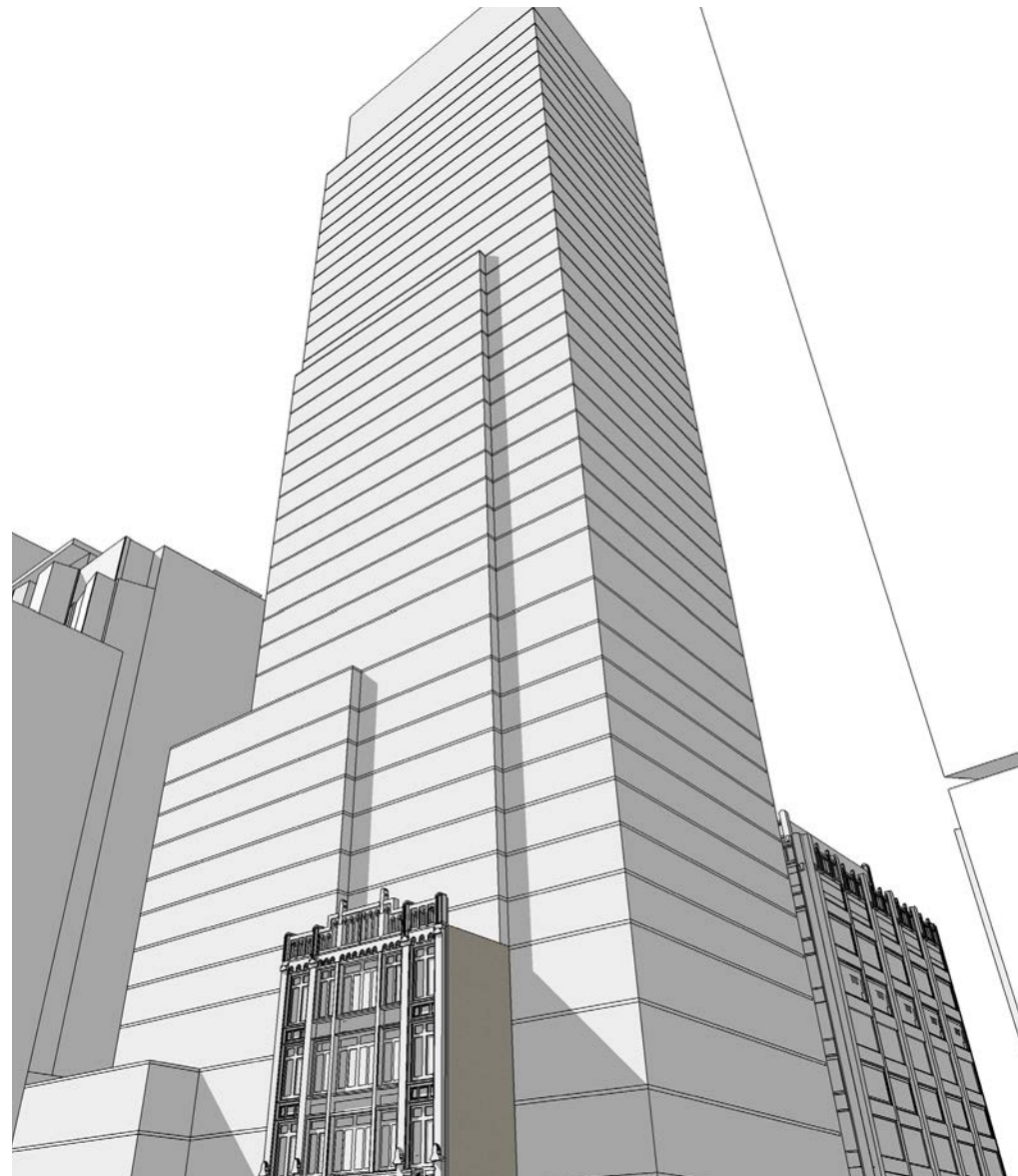
Concept 1 (Code-Compliant)

A code-compliant scheme which adheres to the 120' maximum facade width does not set back from the Landmark.

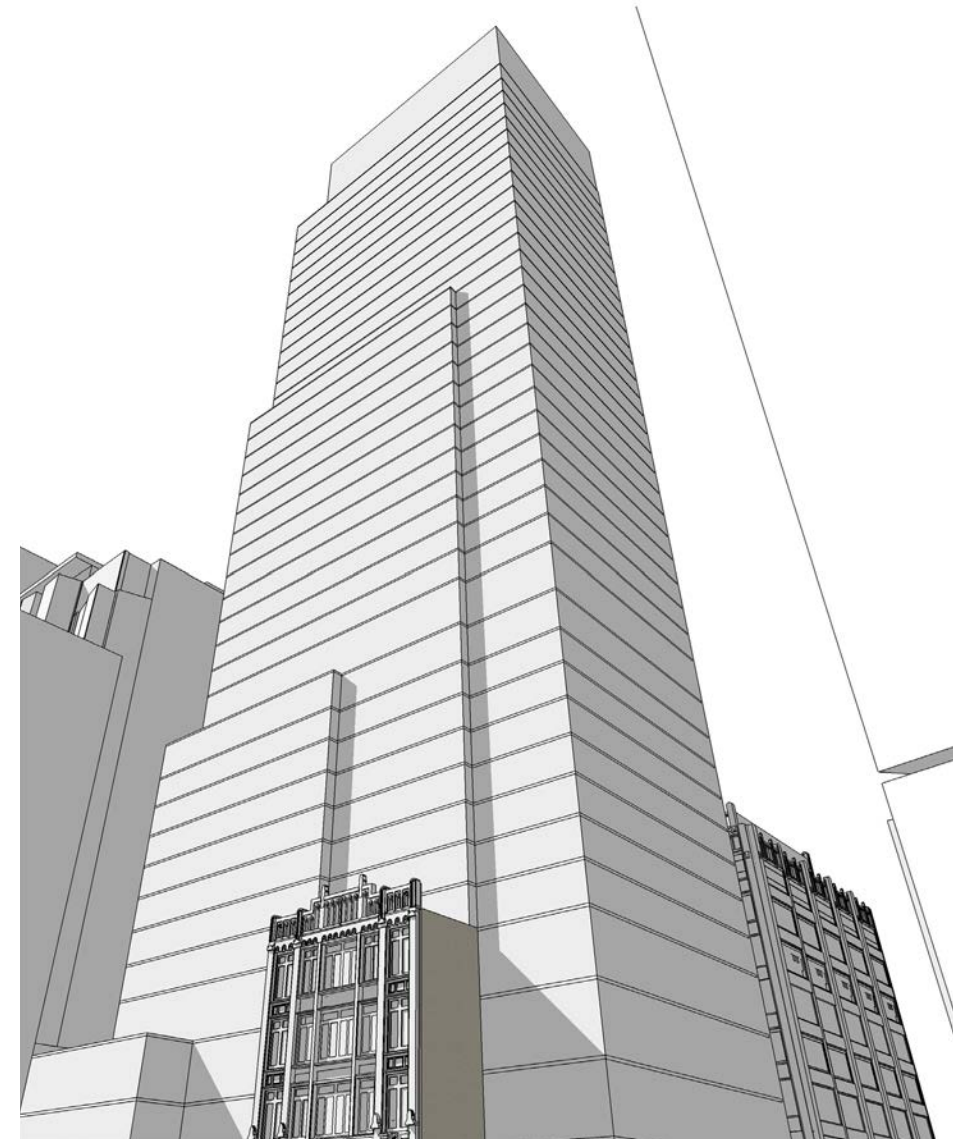


Concept 3 Preferred

The preferred scheme creates a setback between 19' and 24' from face of Landmark to face of residential tower. In doing this, it essentially widens the tower by a 1-to-1 ratio.



Code Compliant Tower Width



Proposed Tower Width

This page intentionally left blank.

