

3039 SW AVALON WAY

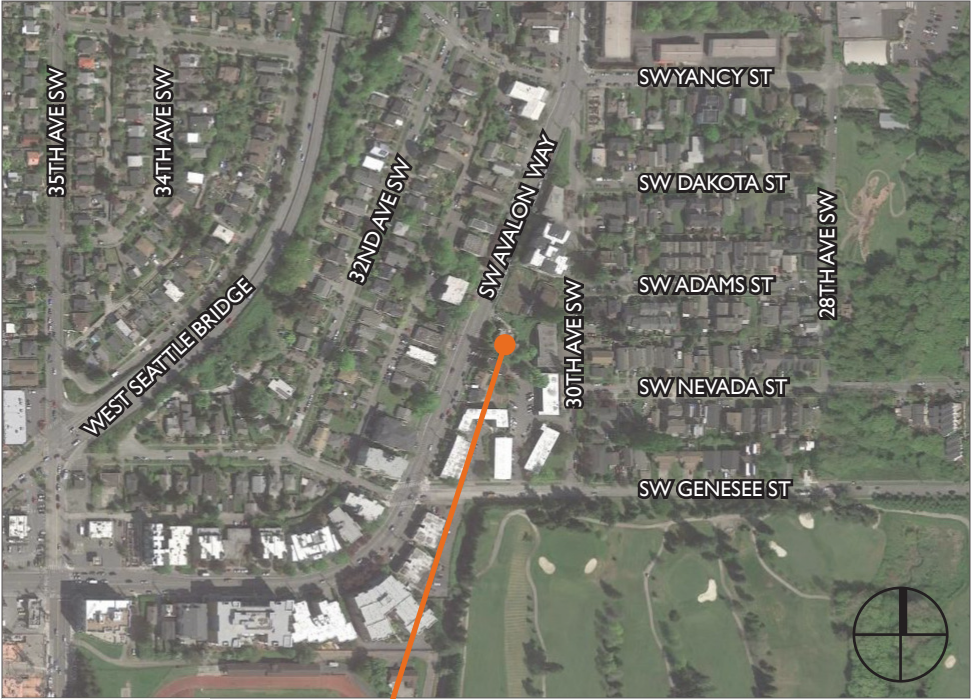


EARLY DESIGN GUIDANCE
FEBRUARY 18, 2016
DPD #3022717

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Seattle, WA 98104
206.933.1150
www.nkarch.com



PROJECT INTRODUCTION



SITE LOCATION

ADDRESS: 3039 SW AVALON WAY
DPD PROJECT #: 3022717
OWNER: Avalon Apartments LLC

APPLICANT: Nicholson Kovalchick Architects
CONTACT: Steve Fischer

DEVELOPMENT OBJECTIVES

The project is a seven-story apartment building containing approximately 71 residential units. Parking for approximately 21 vehicles will be located in a below grade parking garage, which is accessed via a ramp from Avalon Way. The existing one-story residential structure on site will be demolished. The approximate sizes of the proposed building and its individual uses are as follows:

Number of Residential Units:	Approximately 71
Number of Parking Stalls:	Approximately 21, partially below grade
Area of Residential Levels:	Approximately 45,900 square feet
Area of Parking Levels:	Approximately 6,600 square feet
Total Building Area:	Approximately 52,500 square feet

EXISTING SITE

The site is located at the mid block on east side of SW Avalon Way between SW Genesee St. and SW Yancy St. The site is 12,340 sf and measures approximately 120' wide by 108' deep. It slopes significantly down to the southeast, with the steepest slope near the southeast corner of the site. An alley right-of-way dead ends at the southeast corner of the site, however the alley is not improved and is used parking for neighboring properties. There is an existing 2-story duplex on the site, currently in poor condition. Several small trees/large bushes line the south and east property lines, and there are two larger trees near the southeast corner of the site.

ZONING AND OVERLAY DESIGNATION

The site is zoned Midrise, and is located in the West Seattle Junction Hub Urban Village and a Frequent Transit Area. The West Seattle Design Guidelines are applicable to this site.

NEIGHBORING DEVELOPMENT

The site is located in a transitional neighborhood in the valley between Delridge and Alaska Junction. The neighborhood consists of a mix of single family and low-rise residential buildings, with an increasing number of newer mid-rise residential and mixed use buildings. Most new mid-rise development has been concentrated along SW Avalon Way, which is a moderately busy arterial street connecting to the West Seattle Bridge to the north and the Junction to the west. Several bus routes, including Rapidride Line C run through the neighborhood, providing frequent transit service to downtown and the rest of West Seattle.

The neighborhood slopes down to the East, providing views of Downtown and to the northeast. Nearby neighborhood amenities include Dragonfly Park, Delridge Playfield and West Seattle Stadium.

The properties immediately adjacent to the site are all zoned Midrise (MR). The properties to the south and east are developed with 2-3 residential buildings. The property to the north contains a small, abandoned church, and is likely to be redeveloped in the near future. The properties across Avalon Way are a mix of low-rise residential buildings.

ZONING SUMMARY

PARCEL #: 9297300725
ZONING: MR
OVERLAYS: West Seattle Junction Hub Urban Village
Frequent Transit
LOT AREA: 12,340 SF

23.45.504 PERMITTED USES

Permitted outright: Residential

23.45.510 FLOOR AREA RATIO

Base FAR: 3.2
Maximum FAR: 4.25
Maximum FAR per sustainable design and affordability incentives (SMC 23.45.516, SMC 23.45.526, SMC 23.58A.014)

23.45.514 STRUCTURE HEIGHT

Allowed Maximum Structure Height:

- Base Height: 60'-0"
- Maximum bonus height per incentives: 75'-0"
- 4' additional allowed for parapets: 79'-0"
- 15' additional allowed for stair penthouse: 90'-0"
- 16' additional allowed for elevator penthouse: 91'-0"

Increase from base height limit to maximum requires compliance with incentive provisions for affordable housing and sustainable design.

23.86.006 STRUCTURE HEIGHT MEASUREMENT

The height of a structure is the difference between the elevation of the highest point of the structure not excepted from applicable height limits and the average grade level ('average grade level' means the average of the elevation of existing lot grades at the midpoints, measured horizontally, of each exterior wall of the structure or at the midpoint of each side of the smallest rectangle that can be drawn to enclose the structure).

23.45.518 SETBACK REQUIREMENTS

Front and side setback from street lot lines:

- 7' average, 5' minimum
- No setback required if a courtyard abuts street, and the courtyard is minimum 30% width of abutting street frontage or 20' whichever is greater, and minimum 20' deep measured from street lot line

Rear setback: 15'

Side setback from interior lot line:

- For portions 42' high or less, 7' average setback and 5' minimum setback
- For portions higher than 42', 10' average setback and 7' minimum setback

Additional setbacks:

- Cornices, eaves, gutters, roofs and other forms of weather protection

may project into required setbacks and separations a maximum of 4' if they are no closer than 3' to any lot line

- Unenclosed decks and balconies may project a maximum of 4' into required setbacks if each one is no closer than 5' to any lot line, no more than 20' wide, and separated from other decks and projections by a distance equal to one-half the width of the projection

23.45.522 AMENITY AREA

Required: 5% of gross floor area in residential use

General requirements:

- All units shall have access to private or common amenity area
- No more than 50% of the amenity area may be enclosed, and this enclosed area shall be provided as common amenity area
- No minimum horizontal dimension for private amenity areas, except 10' at non-street side lot lines

Requirements for apartments, rowhouses, and townhouses:

- No common amenity area shall be less than 250 sf in area, and common amenity areas shall have a minimum horizontal dimension of 10'
- Min. 50% of common amenity area at ground level shall be landscaped
- Seating, lighting, outdoor protection, art, etc. shall be provided
- Common amenity area req'd at ground level will be accessible to all units

23.45.524 LANDSCAPING REQUIREMENTS

Green Factor score minimum 0.5 required

23.45.526 LEED, BUILT GREEN, AND EVERGREEN SUSTAINABLE DEVELOPMENT STANDARDS

Projects seeking additional height and FAR must meet LEED Silver or Built Green 4-Star rating.

23.45.528 STRUCTURAL WIDTH & DEPTH

For lots larger than 9,000 SF:

- Maximum Structure Width: 150'
- Maximum Structure Depth: 75% of Lot Depth

Lot Depth of an irregularly-shaped lot is determined by dividing the lot area by the lot frontage.

23.54.015 REQUIRED PARKING

- Required parking in multi-family zones in urban villages in frequent transit areas: None
- Bicycle long-term parking: 1 per 4 units

23.45.536 PARKING LOCATION, ACCESS, AND SCREENING

- Parking may be located in a structure provided that no portion of a garage that is higher than 4' above grade shall be closer to a street lot line than any part of the first floor of the structure

- Alley access to parking is not required where the abutting alley is not improved.

23.54.030 PARKING SPACE STANDARDS

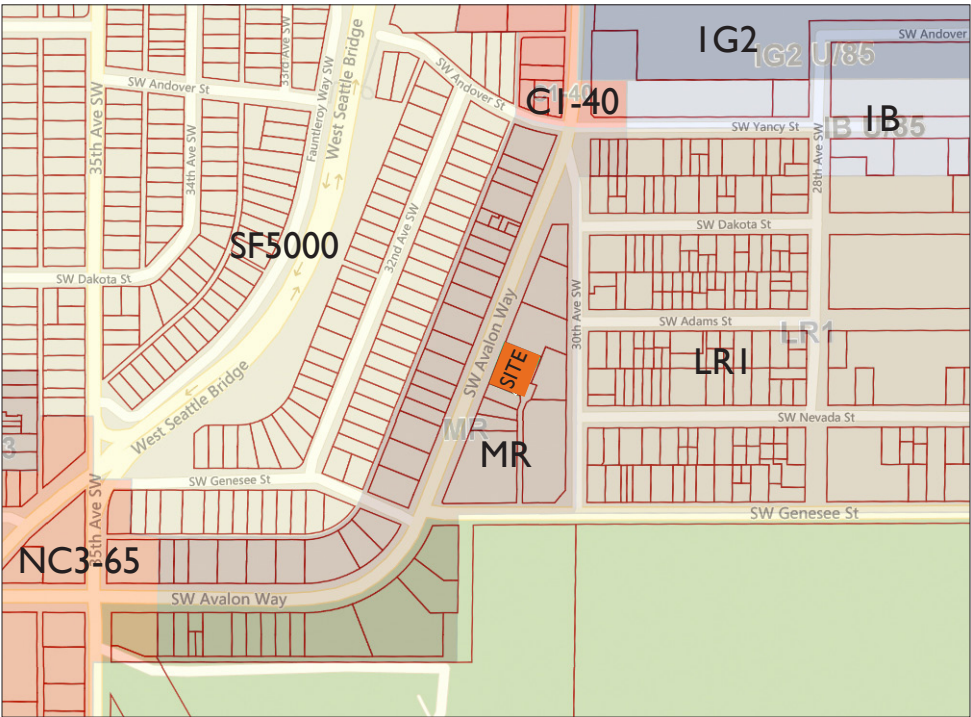
- Driveway width min. 10' for driveways serving 30 spaces or fewer for one or two-way traffic
- Driveway slope maximum 15%

Steeper driveway slope may be approved by DPD where topographic conditions make 15% slope unfeasible.

23.54.040 SOLID WASTE & RECYCLABLE MATERIALS STORAGE AND ACCESS

50-100 units:

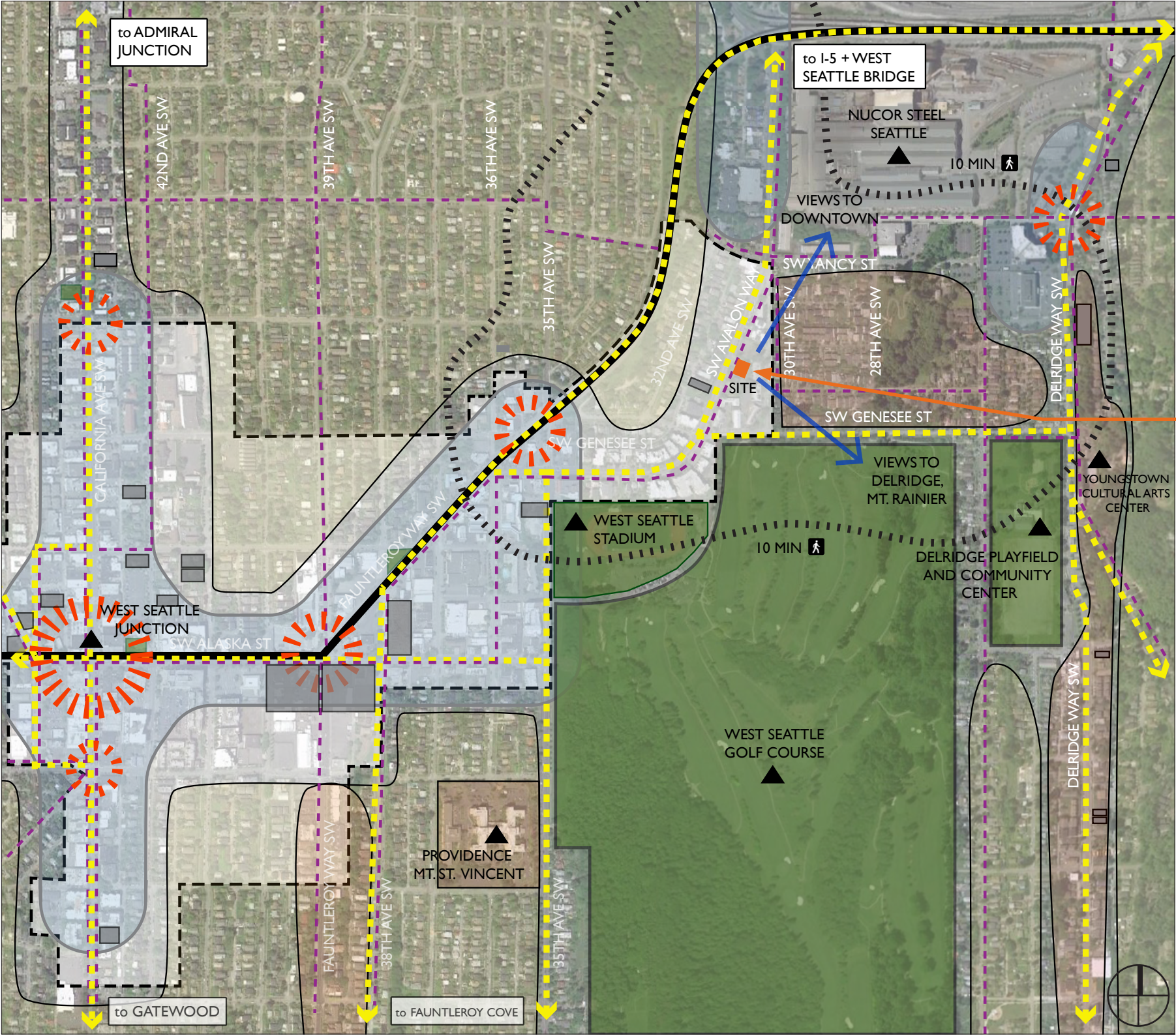
- 375 sf + 4 sf per unit in excess of 50
- Min. horizontal dimension of required storage space is 12'



DPD ZONING MAP

URBAN CONTEXT

CONSTRAINTS AND OPPORTUNITIES












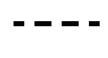


OPPORTUNITIES

- Walking distance to retail, neighborhood amenities and parks
- Close to several bus routes
- Opportunity for views of downtown to the northeast

CONSTRAINTS

- Moderate traffic on Avalon Way
- Unimproved, dead-end alley currently unusable, alley improvements would be cost prohibitive and negatively impact neighboring properties.
- Steep slope at back of site makes alley access unfeasible
- Overhead power lines on Avalon Way impact building massing facing street
- Future development on neighboring property to the north may block views

SITE LOCATION

KEY			
	SITE		NODE
	PARK		WALKSHED
	RECENT PROJECT		FREQUENT TRANSIT BUS ROUTE
	COMMERCIAL ZONING		BICYCLE ROUTE
	LOW-RISE ZONING		WEST SEATTLE JUNCTION HUB URBAN VILLAGE
	SINGLE-FAMILY ZONING		LANDMARK

SITE CONTEXT



The site context contains a mix of single family and low-rise residential buildings, with an increasing number of newer mid-rise residential and mixed use buildings. Most of the larger, recent developments are located along Avalon Way, increasing in density to the west, approaching Alaska Junction. There are also several parks and open spaces in the vicinity of the site.



① NOVA APARTMENTS



② VISTA POINT



③ WEST SEATTLE STADIUM



④ FUTURE TRINISIC MIXED-USE



⑤ RAPID RIDE ON AVALON/YANCY



⑥ THE EDGE APARTMENTS



⑦ THE VUE APARTMENTS



⑧ FUTURE APARTMENTS



⑨ SAUSALITO CONDOMINIUMS



⑩ FUTURE TOWNHOMES



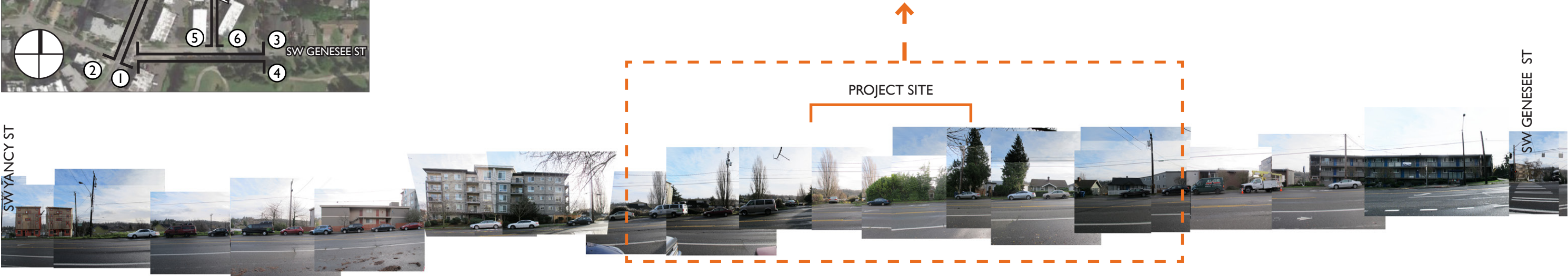
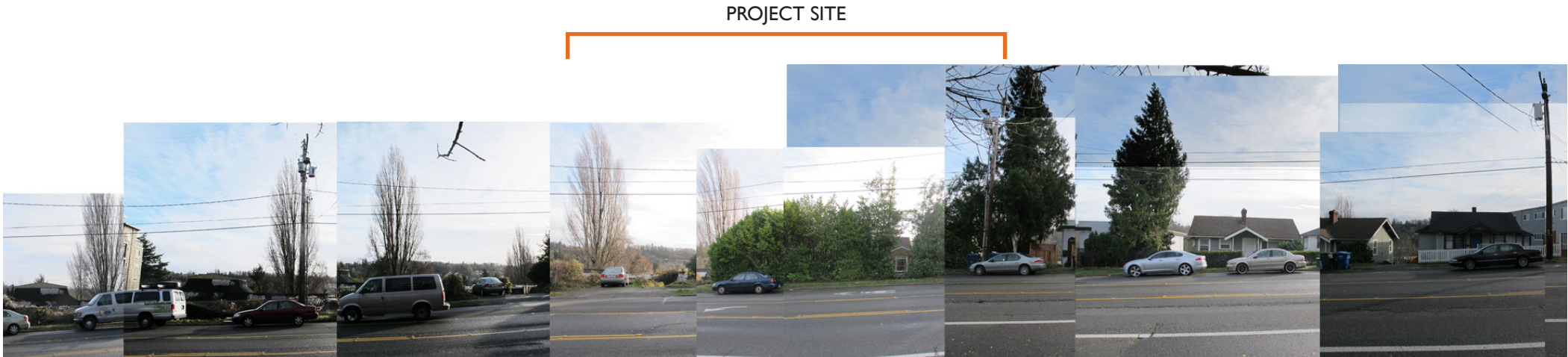
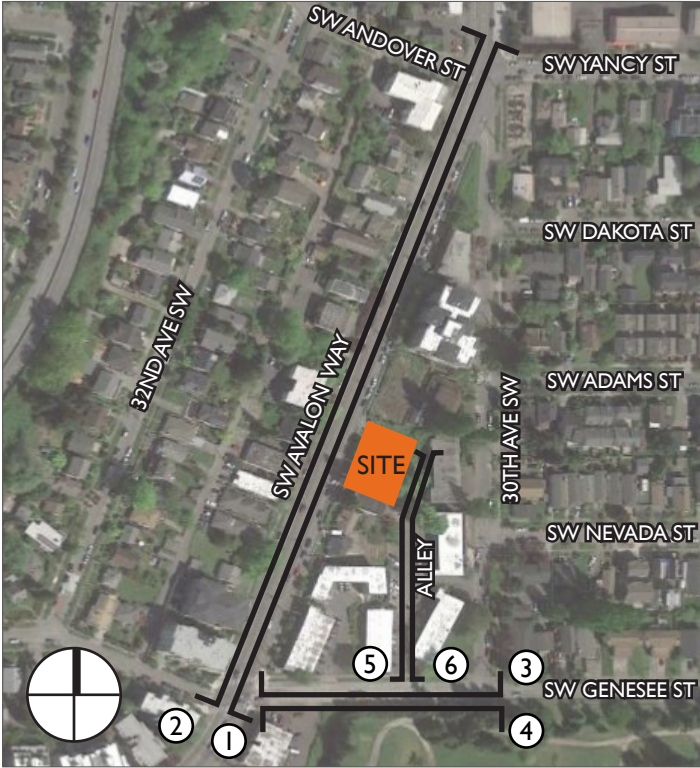
⑪ TOWNHOMES



⑫ LUNA PARK CAFE

STREETSCAPES

SW AVALON WAY



① SW AVALON WAY LOOKING EAST



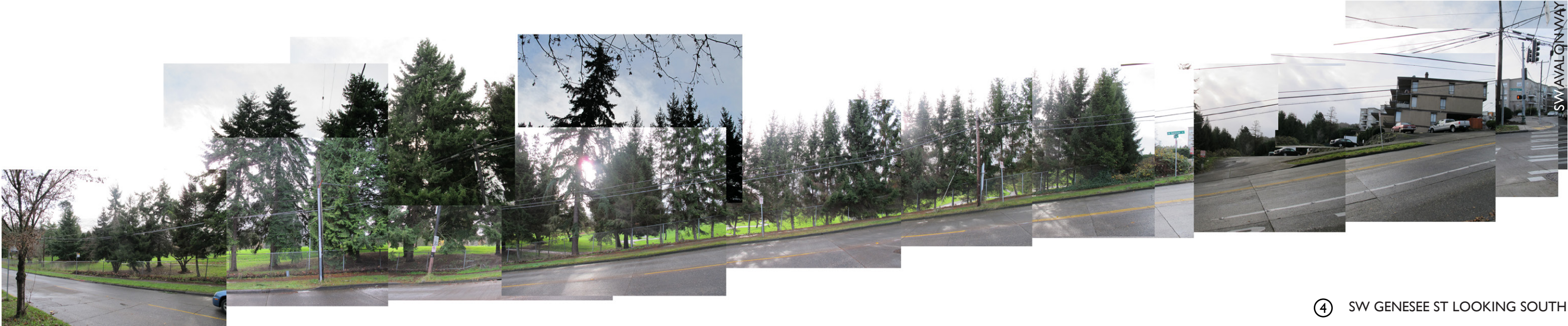
② SW AVALON WAY LOOKING WEST

STREETSCAPES

SW GENESEE ST



③ SW GENESEE ST LOOKING NORTH



④ SW GENESEE ST LOOKING SOUTH

STREETSCAPES

ALLEY



PROJECT SITE



⑤ ALLEY LOOKING WEST

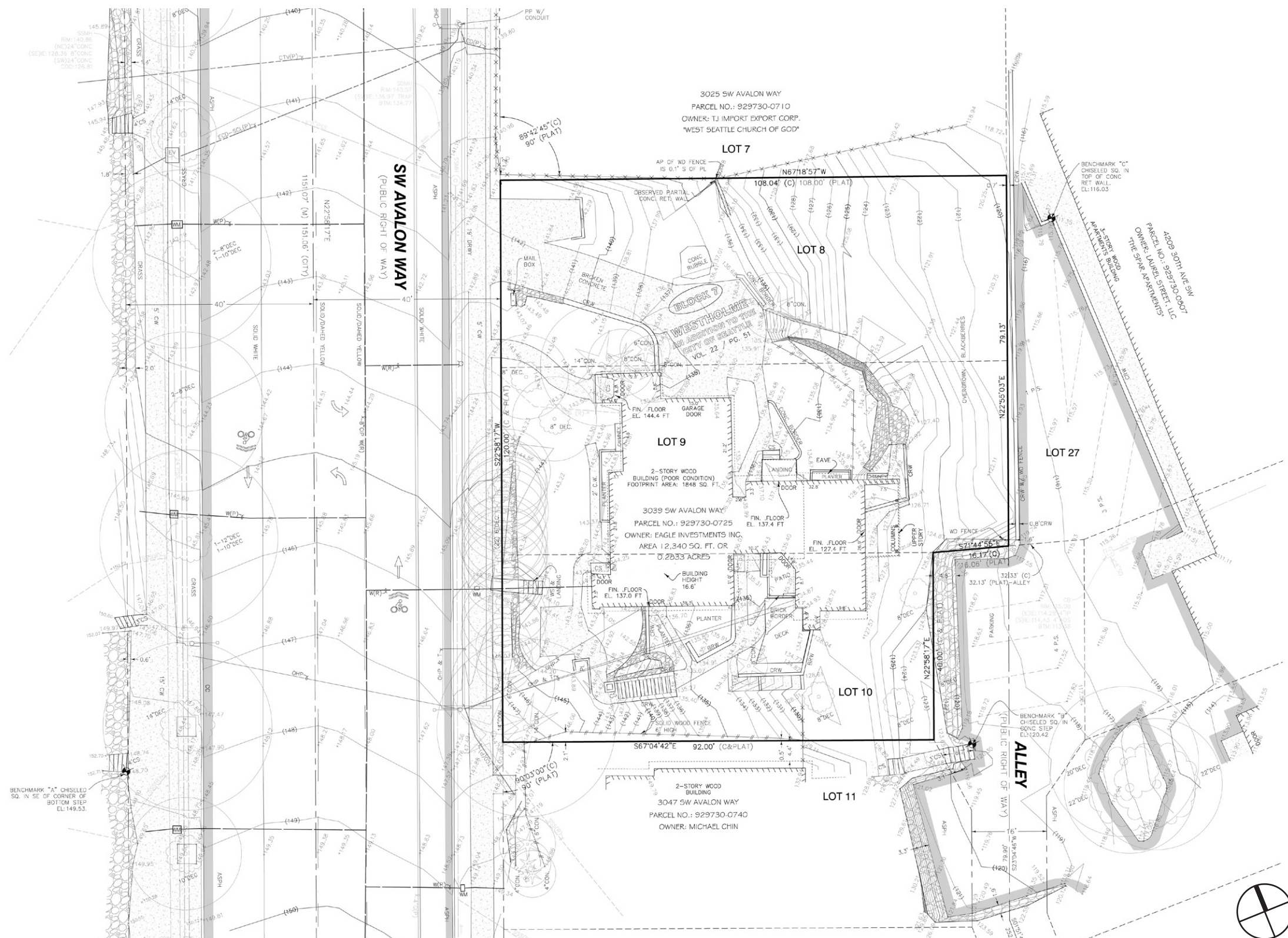
ACROSS FROM PROJECT SITE



⑥ ALLEY LOOKING WEST

EXISTING SITE

SURVEY

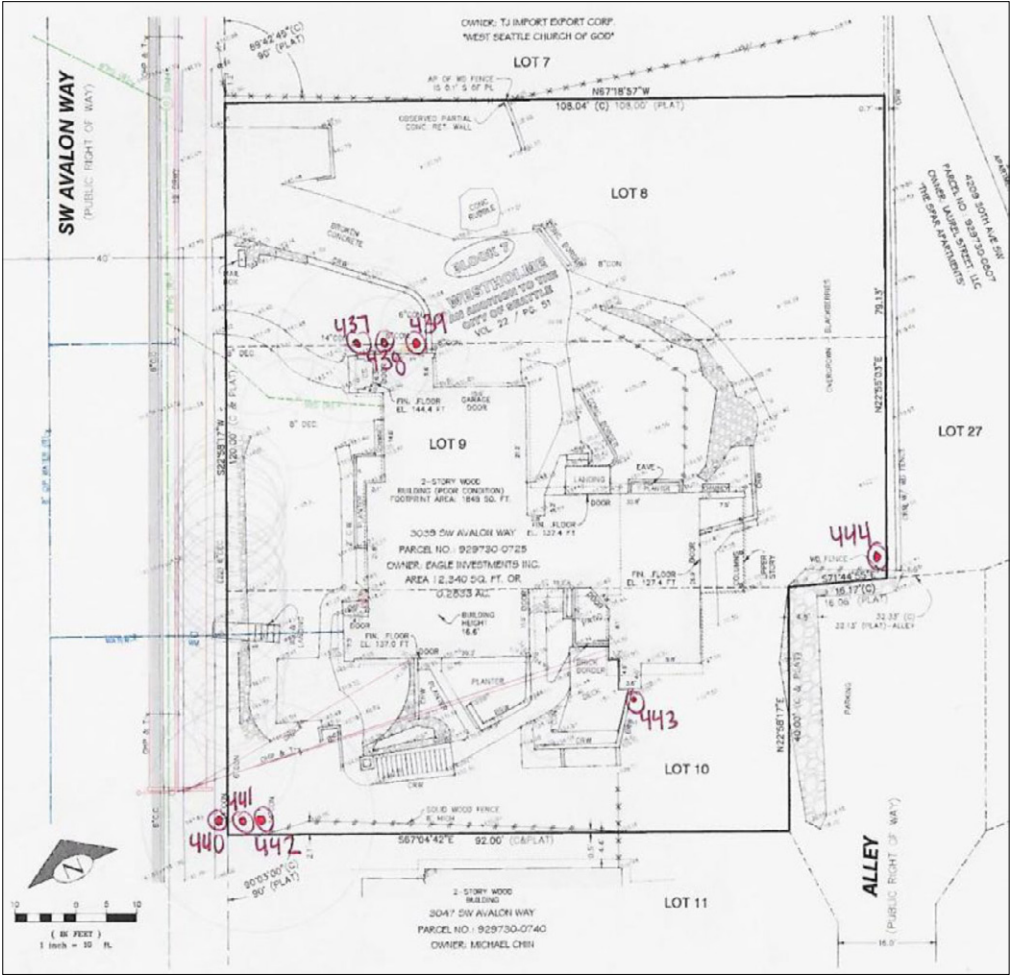


EXISTING SITE

ARBORIST'S TREE SURVEY

All of the existing trees on site have been surveyed and identified by an arborist. As indicated in the tree inventory table below, none of the existing trees are considered exception trees, and most are in poor condition. All existing trees on site are proposed to be removed. Several new trees will be planted as part of the site development. The new trees will be better integrated with the overall landscape design of the project and will be better situated for healthy growth than the existing trees. See landscape plans for proposed trees and plantings.

TREE SURVEY - SITE PLAN



3039 SW AVALON WAY - DPD #3022717

TREE INVENTORY

ABBREVIATED LEGEND--SEE GLOSSARY IN REPORT ATTACHMENTS FOR GREATER DETAIL																		
#1 Property: Whether the tree is on or off the Subject Property, or a Right-of-Way tree									#6 Limits of Disturbance: The boundary between the area of minimum protection around a tree and the allowable site disturbance as determined by a qualified professional.									
#2 Tree #: The unique tag number of each tree.																		
#3 Species:									#7 LCR: <i>Live Crown Ratio</i> - the amount of live canopy expressed as a % of the entire tree height									
									#8 Symmetry: General shape of canopy and weight distribution of the tree around the trunk.									
									#9 Foliage: General description of foliage density that indicates tree health and vigor.									
									#10 Crown Condition: The most important external indication of tree health and vigor.									
									#11 Trunk: Description of trunk condition or abnormalities if any.									
									#12 Root Collar: The base of the tree where the trunk flares into the roots--deformities or problems are noted here.									
									#13 Roots: Root problems are noted here.									
#4 DBH: Trunk diameter @ 4.5' above average ground level.									#14 Comments: Additional observations about the tree's condition.									
#5 Drip Line: The radius, the distance from the trunk to the furthest branch tips.									#15 Current Health Rating: A description of general health ranging from dead, dying, poor, fair, good, very good, to excellent.									
									#16 Status/Recommendation: This is an estimate of whether or not the tree is of sufficient health, vigor, and structure that it is worth consideration of retention.									

1	2	3	4	5	6 -- LIMITS OF DISTURBANCE				7	8	9	10	11	12	13	14	15	16
PROPERTY	TREE #	SPECIES	DBH	DRIP LINE	North	South	East	West	LCR	SYMMETRY	FOLIAGE	CROWN CONDITION	TRUNK	ROOT COLLAR	ROOTS	COMMENTS	CURRENT HEALTH RATING	STATUS / RECOMMENDATION
Subject property	437	POC/CI	14.4"	12'	NA	NA	NA	NA	25%	Major asymmetry	Sparse	Dead	Center rot	Base rot	Restricted		Dying	Remove for safety
Subject property	438	JC/Cj	6.4"	14'	NA	NA	NA	NA	90%	Major asymmetry	Average	Regenerating average	Forked at 6'	Exposed	Restricted		Poor	Remove for safety
Subject property	439	EH/la	6.1"	16'	NA	NA	NA	NA	80%	Major asymmetry	Thin	Weak	Kinked at 3'	NAD	Restricted	Base is approximately 4 inches north of building and against the brick wall to the east.	Poor	Remove for safety
Right of way	440	WRC/Tp	14.0"	12'	12'	South property line	12'	To sidewalk	55%	Major asymmetry	Dense	Utility prune	Kinked at 7', leans west over sidewalk and parking lane	NAD	Restricted		Fair	Potential to retain with tree protection measures
Subject property	441	WRC/Tp	25.5"	18'	18'	South property line	18'	To sidewalk	98%	Major asymmetry	Dense	Healthy	Kinked at 5'	NAD	Restricted	English ivy up tree 95%. Excessively side pruned for utility clearance.	Fair	Potential to retain with tree protection measures
Subject property	442	WRC/Tp	23.4"	22'	22'	South property line	22'	To sidewalk	98%	Minor asymmetry	Dense	Healthy	Straight	NAD	Restricted	English ivy up tree 95%.	Good	Potential to retain with tree protection measures
Subject property	443	Ap/Msp	13.5"	9'	NA	NA	NA	NA	35%	Major asymmetry	PBS/PSE	Regenerating weak	Center rot	Base rot	Rot, restricted	Clematis infestation up 100% of tree. Carpenter ant infestation and woodpecker activity. Poor past pruning practices.	Dying	Remove for safety
Subject property	444	BLM/Am	19.6, 17.9 & 12.8"	24'	NA	NA	NA	NA	95%	Minor asymmetry	PBS/PSE	Weak	Center rot	Base rot	Rot	Dead branches in canopy. Storm damage in canopy.	Dying	Remove for safety

EARLY DESIGN GUIDANCE

EXISTING SITE

SITE PLAN



EXISTING SITE

PHOTOS



① FRONT OF EXISTING BUILDING ON SITE LOOKING FROM AVALON WAY



② TREE AT BACK OF PROJECT SITE



③ LOOKING FROM THE ALLEY TO THE BACK OF THE SITE



④ FRONT OF PRJECT SITE LOOKING SOUTHEAST FROM AVALON WAY



⑤ LOOKING FROM SITE UP DRIVEWAY TOWARDS AVALON WAY



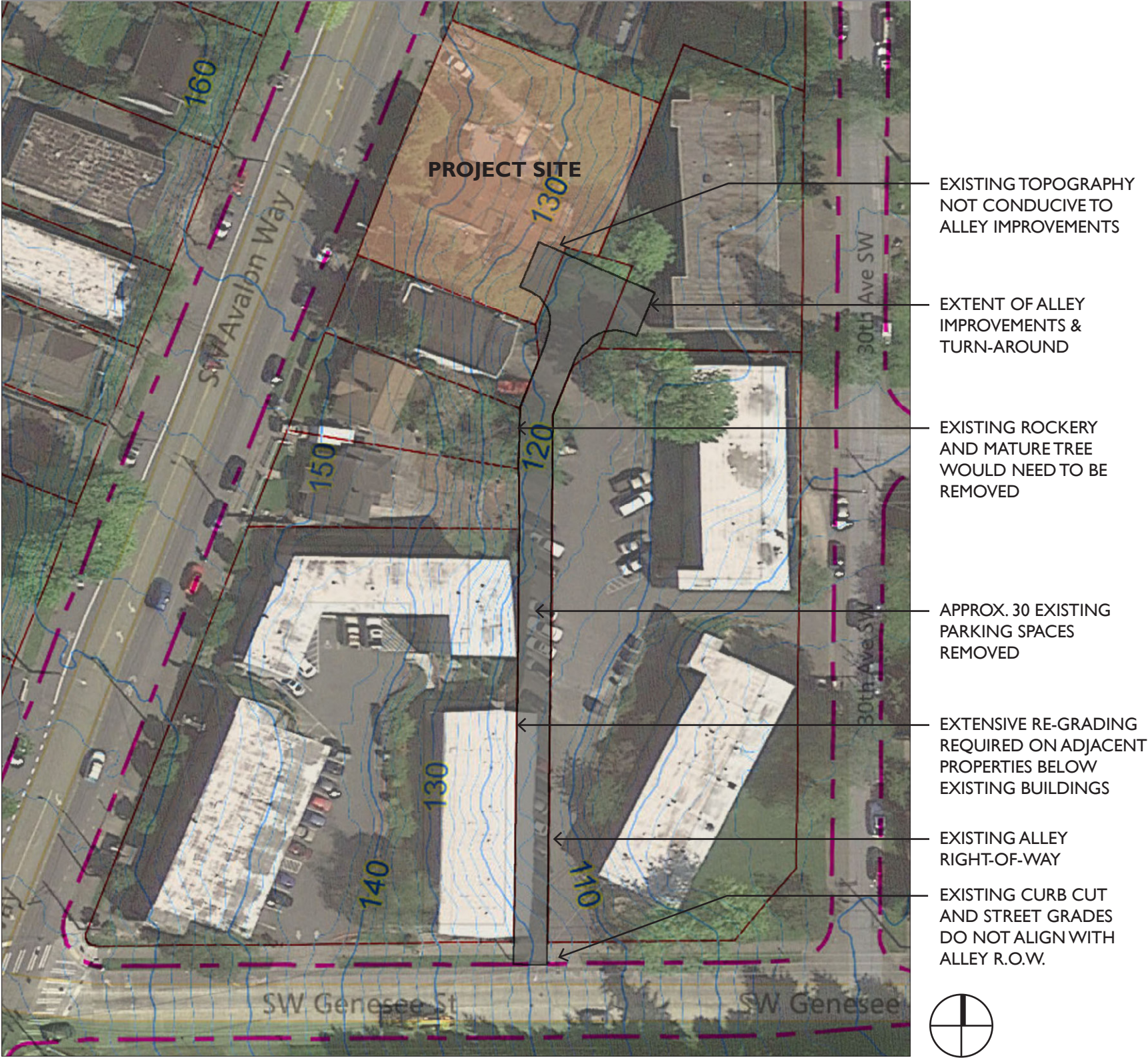
⑥ LOOKING EAST FROM SITE TO NEIGHBORING BUILDING

3039 SW AVALON WAY - DPD #3022717

EARLY DESIGN GUIDANCE

PARKING ACCESS ANALYSIS

ALLEY ACCESS & REQUIRED IMPROVEMENTS



NEGATIVE IMPACTS OF ALLEY IMPROVEMENTS

If the proposed project is required to use the existing alley right-of-way for parking or trash access, significant improvements would be required. These include paving the entire alley right-of-way per SDOT standards, and providing an SDOT-approved turn-around at the end adjacent to the project site. Construction of these improvements would require the permanent removal of approximately 30 existing parking spaces currently used by neighboring properties, as well as the loss of at least one mature tree.

Due to the extent of these impacts and the prohibitive cost of the improvements, the preferred massing provides all access from SW Avalon Way, eliminating the requirement for alley improvements and avoiding impacts to the neighboring properties.



ALLEY RIGHT-OF-WAY LOOKING NORTH FROM SW GENESEE ST.



ALLEY RIGHT-OF-WAY LOOKING SOUTH FROM BACK OF PROJECT SITE

DESIGN GUIDELINES

WEST SEATTLE JUNCTION NEIGHBORHOOD GUIDELINES

CS2 URBAN PATTERN AND FORM

Citywide Guideline: Strengthen the most desirable forms, characteristics, and patterns of the streets, block faces, and open spaces in the surrounding area.

West Seattle Junction Supplemental Guidance:

I. Streetscape Compatibility

Neighborhood Priority: Create a pedestrian-oriented streetscape:

- Reduce the scale of the street wall with residential bays and entries, reinforced with trees, lighting & landscaping.
- Provide recessed entries and small, ground-related open spaces.
- Provide outdoor power and water sources to facilitate maintenance

Response: The preferred massing option provides a larger front setback than required, allowing for ground-level residential units with street-facing entries and generous outdoor patios. The space allows for plentiful landscaping and street furnishings to create a comfortable pedestrian environment.

III. Height, Bulk, and Scale Compatibility

Neighborhood Priority: Create refined transitions in height, bulk and scale between the proposed development and the surrounding context.

- New buildings should use architectural methods including modulation, color, texture, entries, materials and detailing to break up the façade

Response: The proposed project is not located in an NC zone nor does it border on a less-intensive zone. However, the preferred massing uses subtle modulation to break down the massing and highlight the building entry. Setting the building back further from the street also helps to mitigate the impact of the building height on the street.

CS3 ARCHITECTURAL CONTEXT AND CHARACTER

Citywide Guideline: Contribute to the architectural character of the neighborhood.

West Seattle Junction Supplemental Guidance:

I. Architectural Context

- Facade Articulation: To make new, larger development compatible with the surrounding architectural context, facade articulation and architectural embellishment are important considerations in mixed-use and multifamily residential buildings.

Response: The neighborhood context consists of a mix of small and large residential buildings. As the block is zoned MR, it is likely that 6-7 story multifamily buildings of similar scale to the proposed project will become the predominant form of development on the block. The proposed massing sets a precedent for this type of development on the street, with a simple massing

punctuated by modulation that corresponds to significant building elements, such as the main entry.

PL2 WALKABILITY

Citywide Guideline: Create a safe and comfortable walking environment that is easy to navigate and well-connected to existing pedestrian walkways and features.

West Seattle Junction Supplemental Guidance:

I. Human Scale

Neighborhood Priority: Facades should contain elements that enhance pedestrian comfort and orientation while presenting features with visual interest that invite activity.

Response: The preferred option creates an active and comfortable pedestrian environment with private patios along the sidewalk and numerous pedestrian entries to maintain the human scale. The landscaping and building articulation at the street will further reinforce the human scale.

DC1 PROJECT USES AND ACTIVITIES

Citywide Guideline: Optimize the arrangement of uses and activities on site.

West Seattle Junction Supplemental Guidance:

I. Visual Impacts of Parking Structures

- Design of parking structures should improve the safety and appearance of parking uses in relation to the pedestrian environment.
- There should be no auto access from the principal street unless no feasible alternative exists. The parking portion of the structure should be compatible with the rest of the building and the surrounding streetscape.

Response: Alley access is not feasible on this site, as the existing alley is not improved and alley improvements would be unfeasible. Parking is located entirely within the building, located one level below the street. Access is from a single driveway along the north property line, where it will have the least impact on the building design and massing, and be away from the main pedestrian entry. Where the parking level is above grade at the rear of the building, the design will be integrated with the building above.

DC2 ARCHITECTURAL CONCEPT

Citywide Guideline: Develop an architectural concept that will result in a unified and functional design that fits well on the site and within its surroundings.

West Seattle Junction Supplemental Guidance:

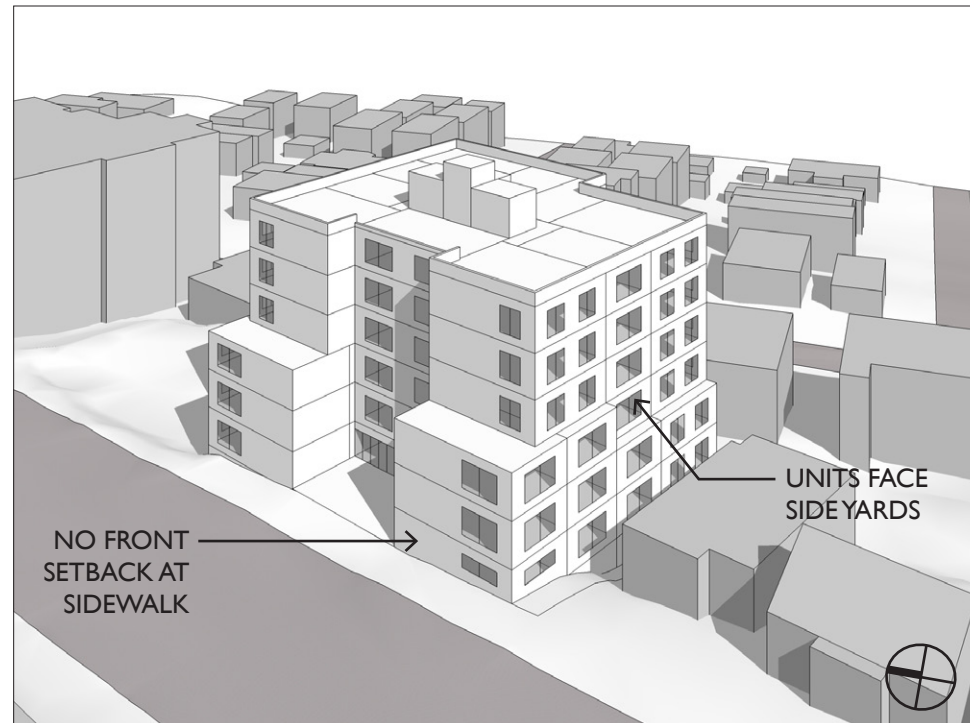
I. Architectural Concept and Consistency

- Multi-story buildings should consider ways to integrate a building's upper and lower levels.

- The use and repetition of architectural features building materials, textures and colors can help create unity in a structure.

Response: Of the three proposed massing options, the preferred option has the simplest massing to avoid creating a disconnect between the base and upper levels. Modulation is limited to keep the design simple and avoid an unnecessarily busy facade.

MASSING OPTIONS SUMMARY



OPTION A - CODE COMPLIANT

72 Units
0 Parking Spaces
4.16 FAR

Pros:

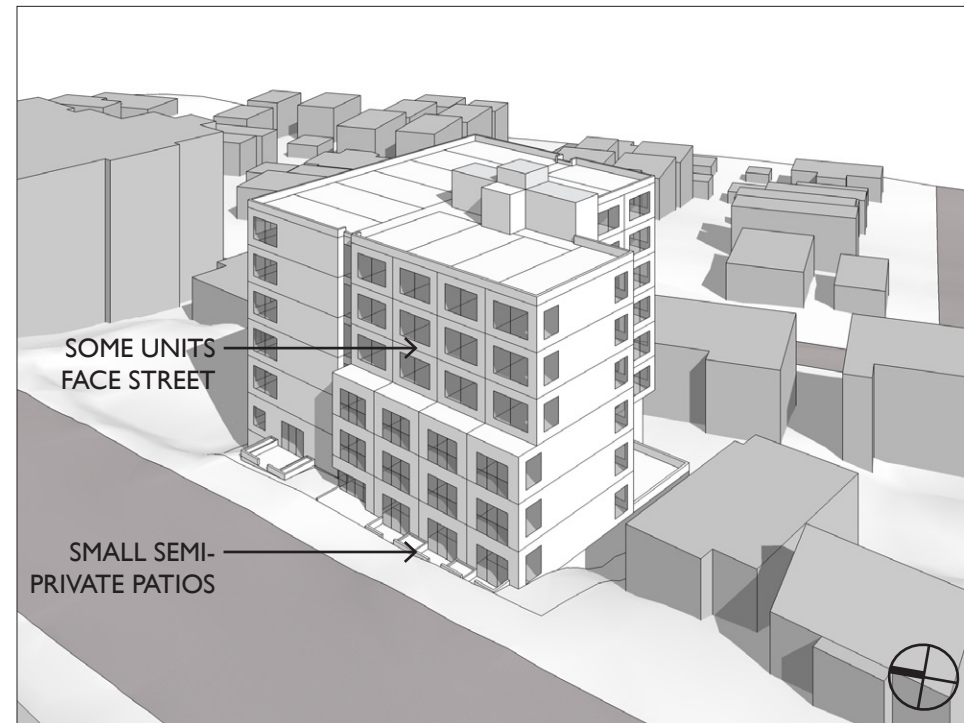
- Provides street-facing entry courtyard
- Takes advantage of views to the northeast

Cons:

- No Parking
- Zero-lot-line condition at front not appropriate for arterial street with narrow sidewalk, and not consistent with existing development patterns in the neighborhood
- Complex, messy massing for a relatively small building
- Most units face toward adjacent properties, impacting privacy
- Northeast views likely to be blocked by future development

Departures:

- None



OPTION B

77 Units
19 Parking Spaces
4.25 FAR

Pros:

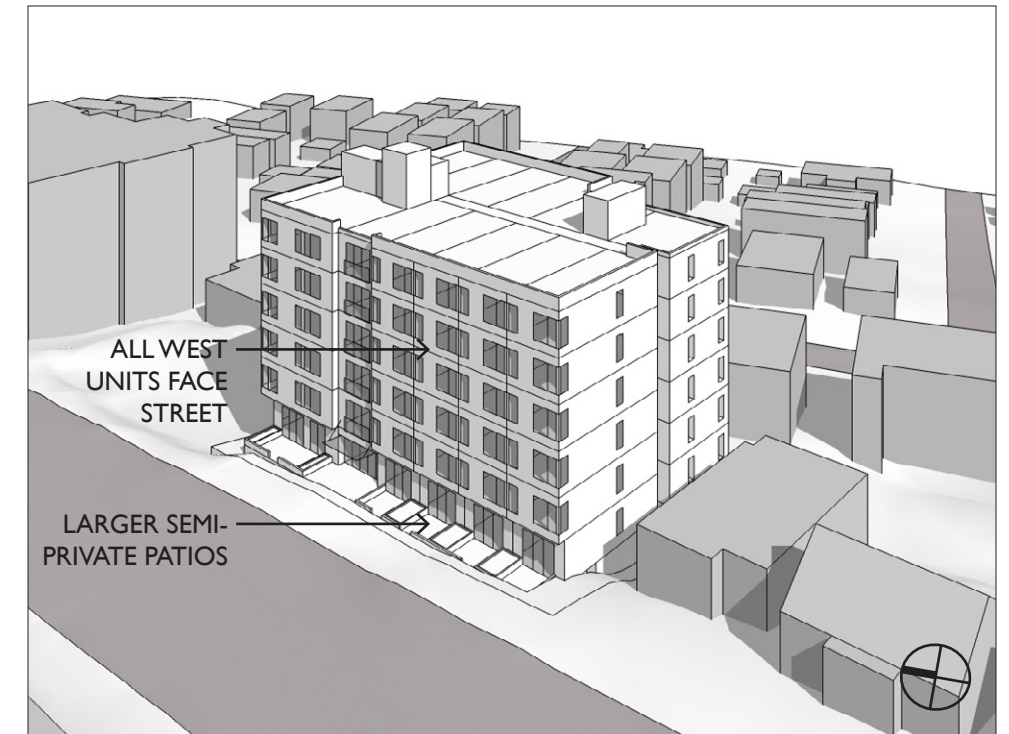
- More units facing street create stronger street presence for building
- Takes advantage of views to the northeast
- Alley access to parking

Cons:

- Very massive facing north and east
- Northeast view likely to be blocked by future development
- Upper level setbacks facing street create complex massing, not conducive to coherent facade design
- Improvements required for alley access will result in significant loss of parking and trees for neighboring properties
- 5' setback at street still too small to create desirable outdoor space and maintain unit privacy

Departures:

- Building Depth
- North side & rear setbacks



OPTION C - PREFERRED

71 Units
21 Parking Spaces
4.09 FAR

Pros:

- Provides large front setback down to ground level, allowing for better unit privacy, more landscaping and usable private patios
- Units oriented toward street & rear, away from neighboring buildings to south
- Least impacted by future development to the north
- Provides the most parking without any impact on neighbor's parking
- Clean, orderly massing, with modulation to accentuate main entry
- Code compliant setbacks at rear and south side where facing existing residential buildings

Cons:

- Parking access from street

Departures:

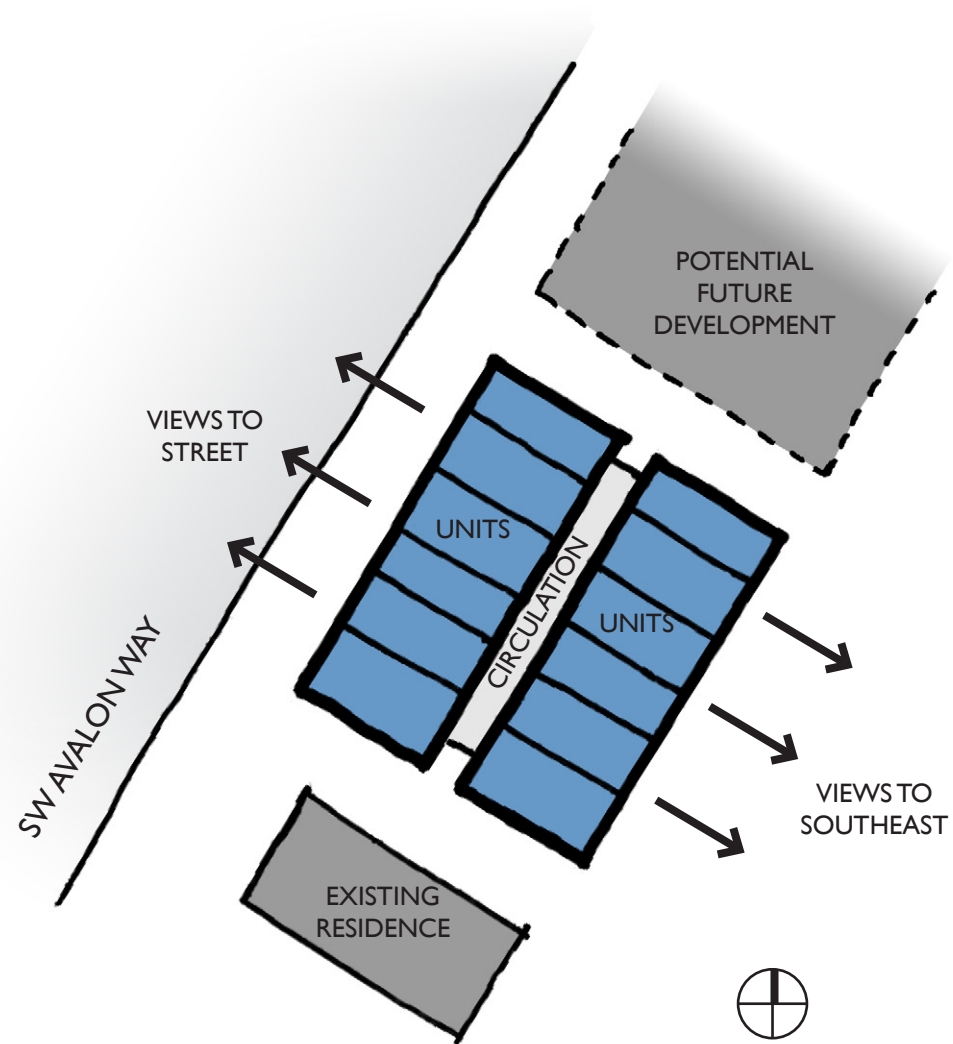
- North side setback
- Driveway sight triangle

MASSING CONCEPT

PREFERRED MASSING OPTION 3

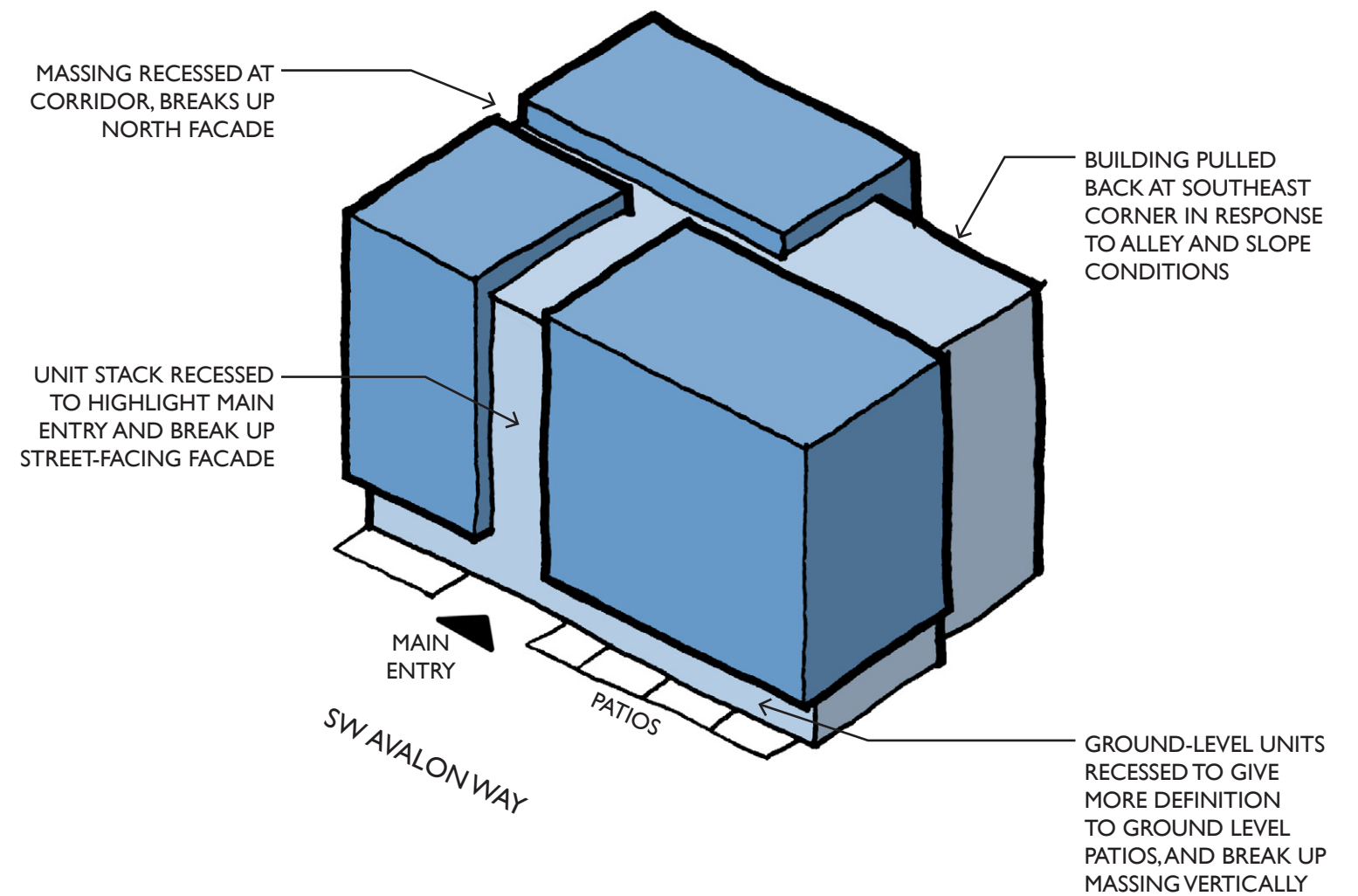
FLOOR PLAN

The preferred massing option divides the units into two blocks with a central corridor. One block faces the street and the other faces to the back of the site. All of the units have views to the street or to the southeast. This arrangement minimizes the impact on and from potential development to the north, and protects the privacy of existing and future neighboring properties.



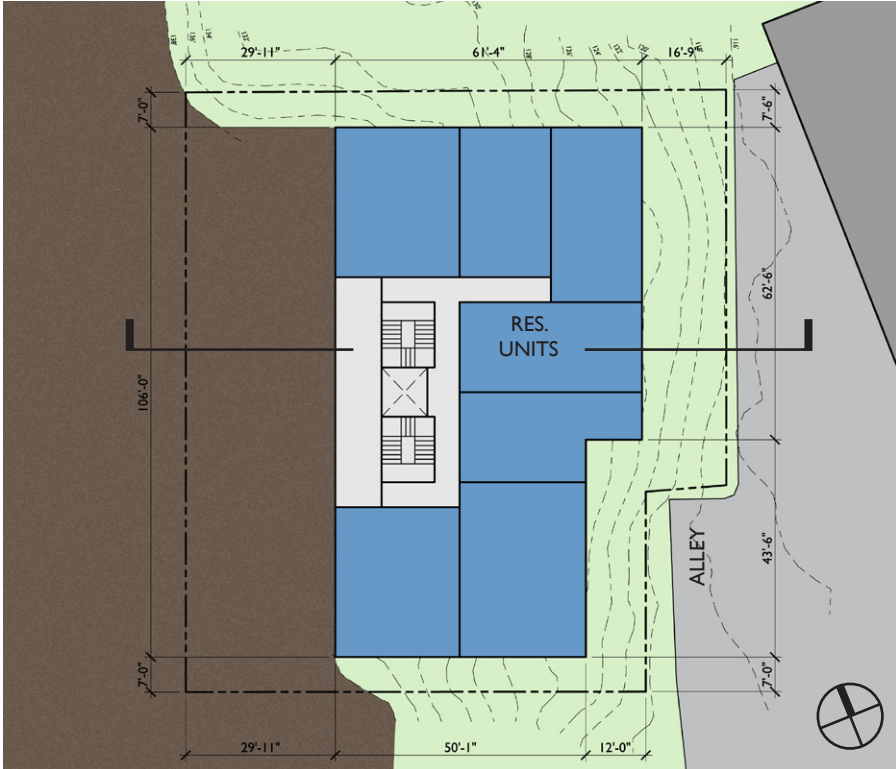
MASSING ARTICULATION

The two unit blocks are further articulated in the preferred option in response to unique site conditions and to break down massing facing the street.

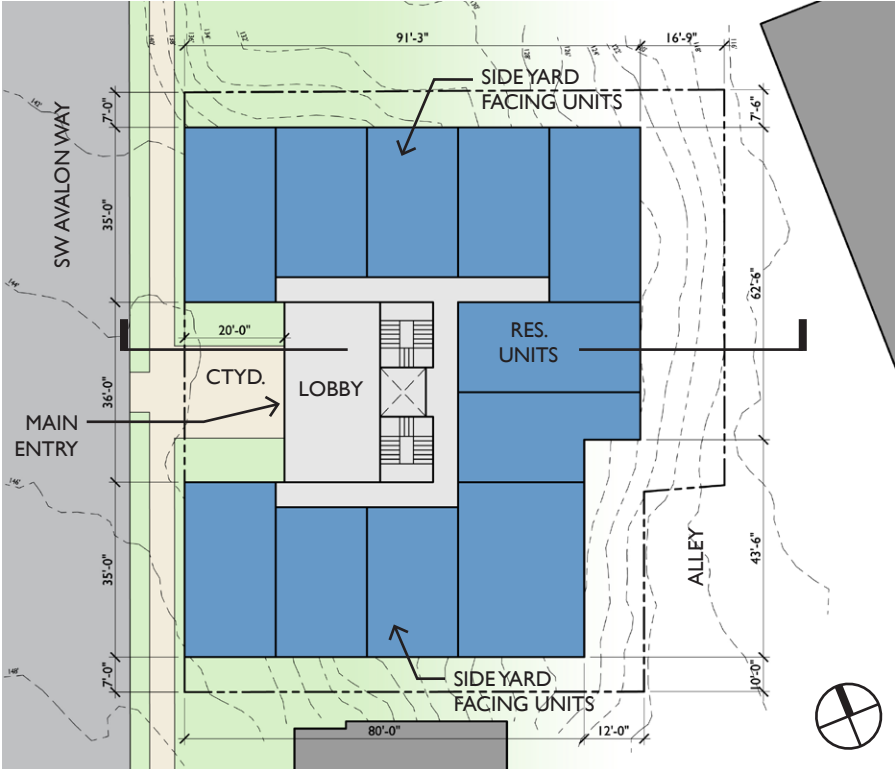


MASSING OPTION I

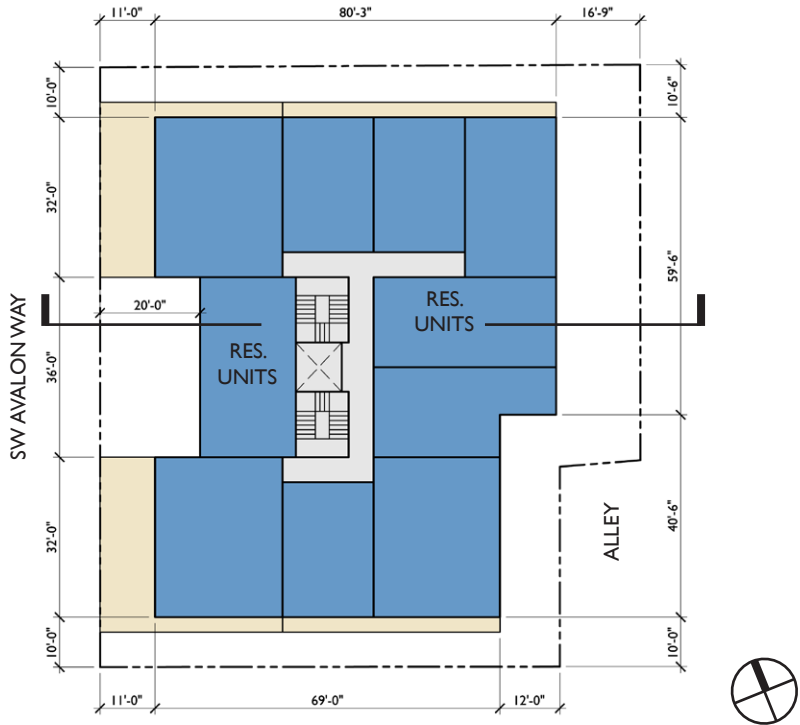
ZONING COMPLIANT OPTION



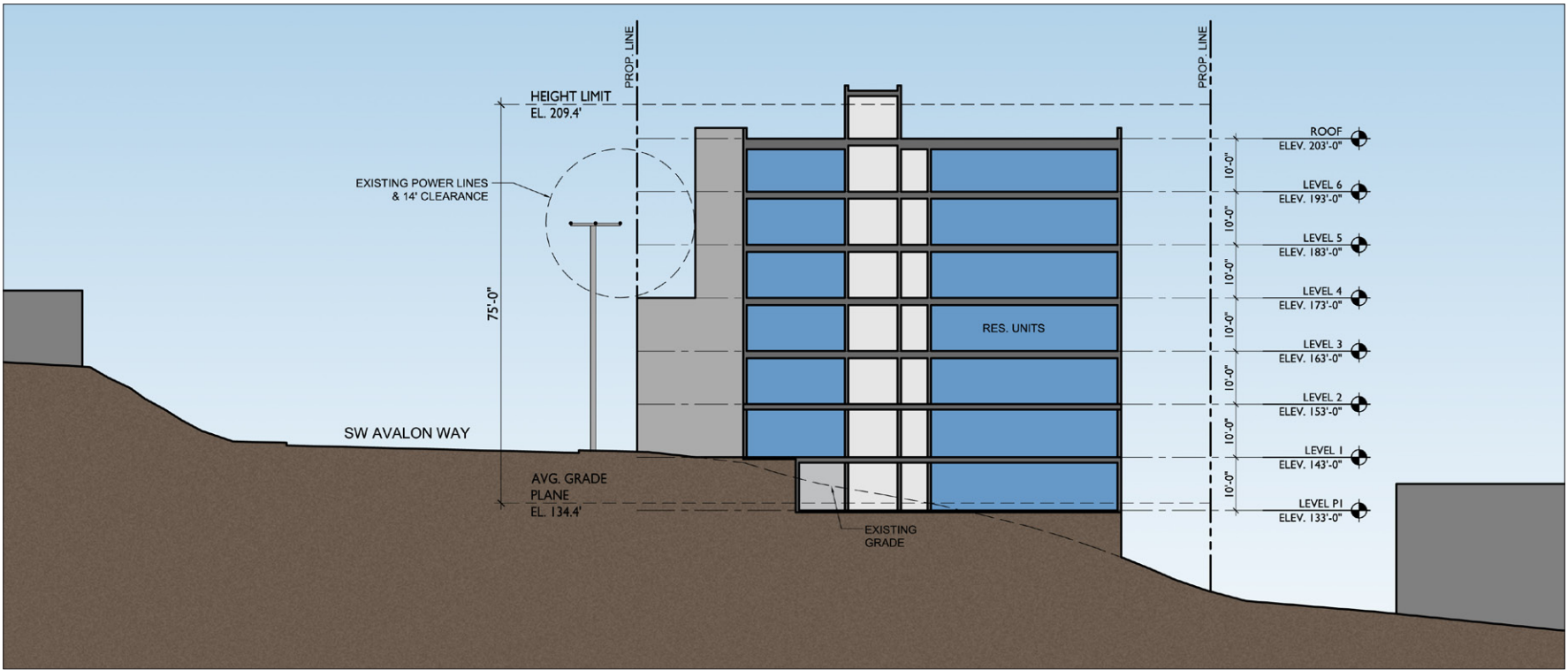
LEVEL B



LEVELS 1-3



LEVELS 4-6



SECTION - LOOKING NORTH

- RESIDENTIAL
- CIRCULATION
- PARKING
- OUTDOOR
DECKS

MASSING OPTION I

ZONING COMPLIANT OPTION



STREET-LEVEL VIEW FROM SOUTHWEST



AERIAL VIEW FROM NORTHWEST

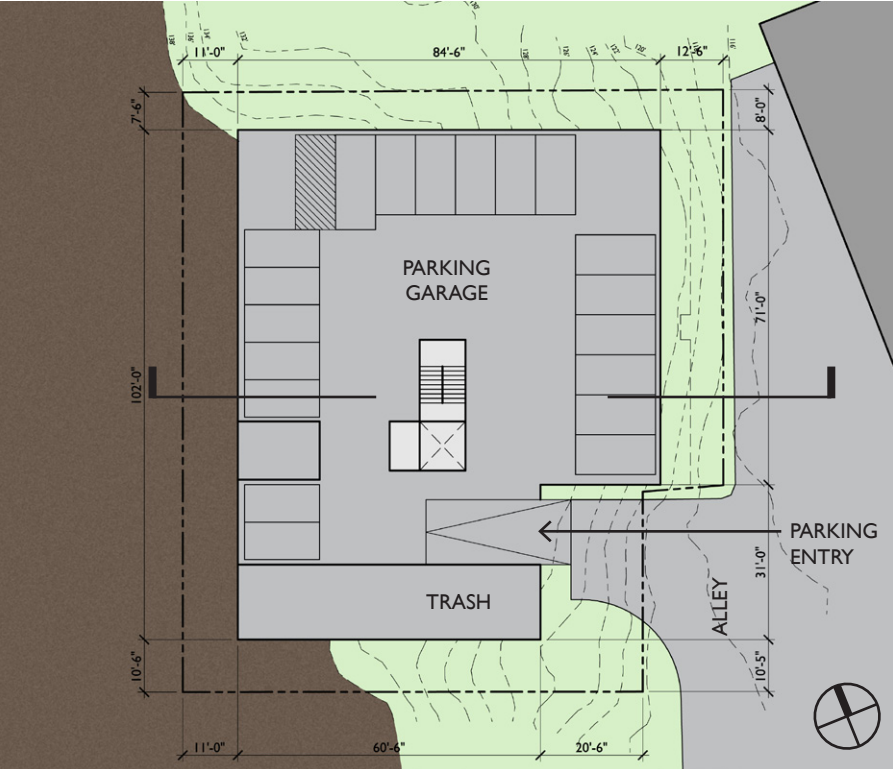


AERIAL VIEW FROM NORTHEAST

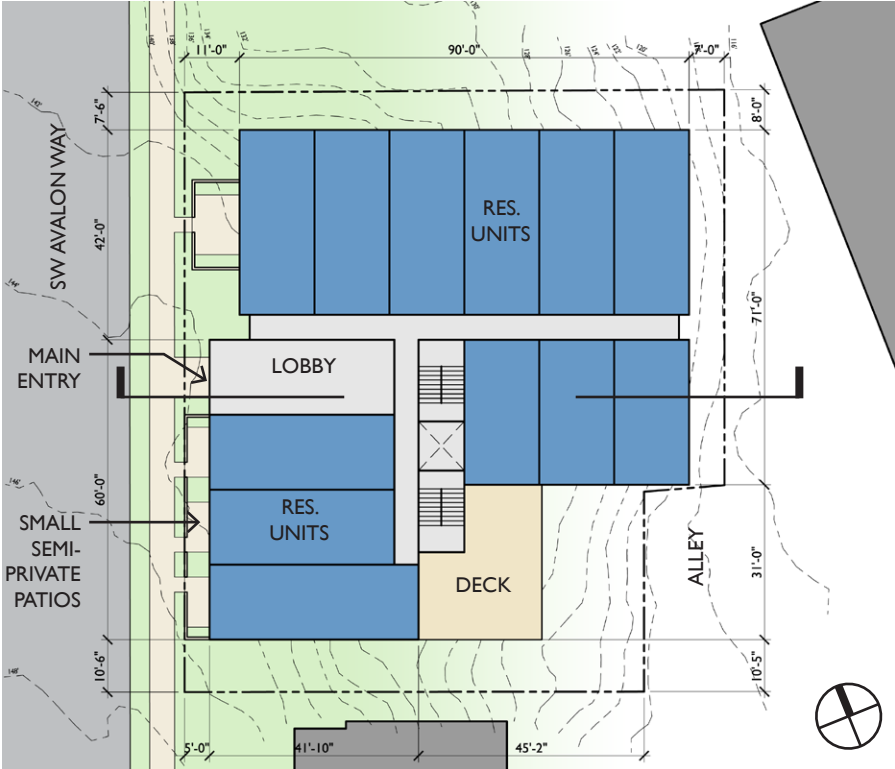


AERIAL VIEW FROM SOUTHEAST

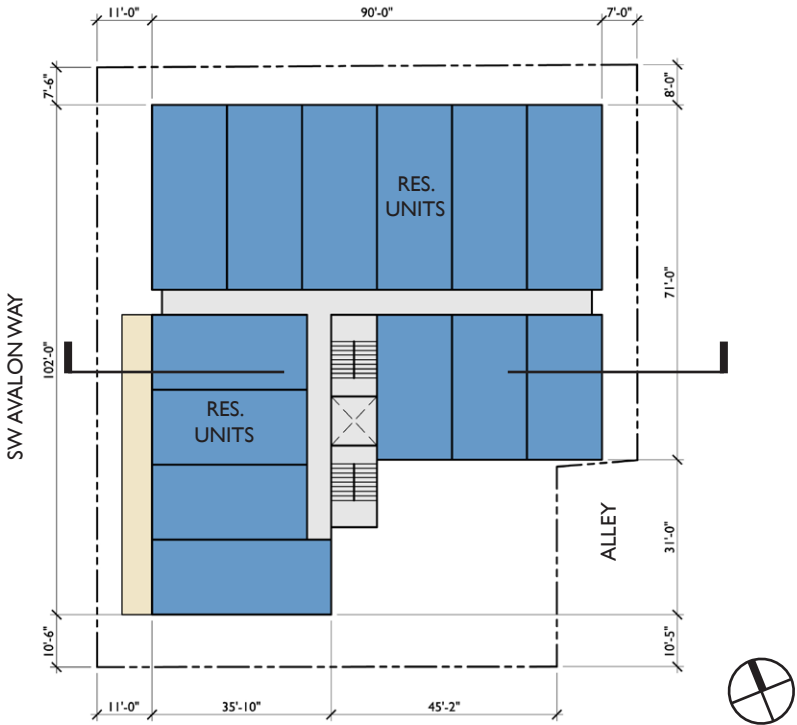
MASSING OPTION 2



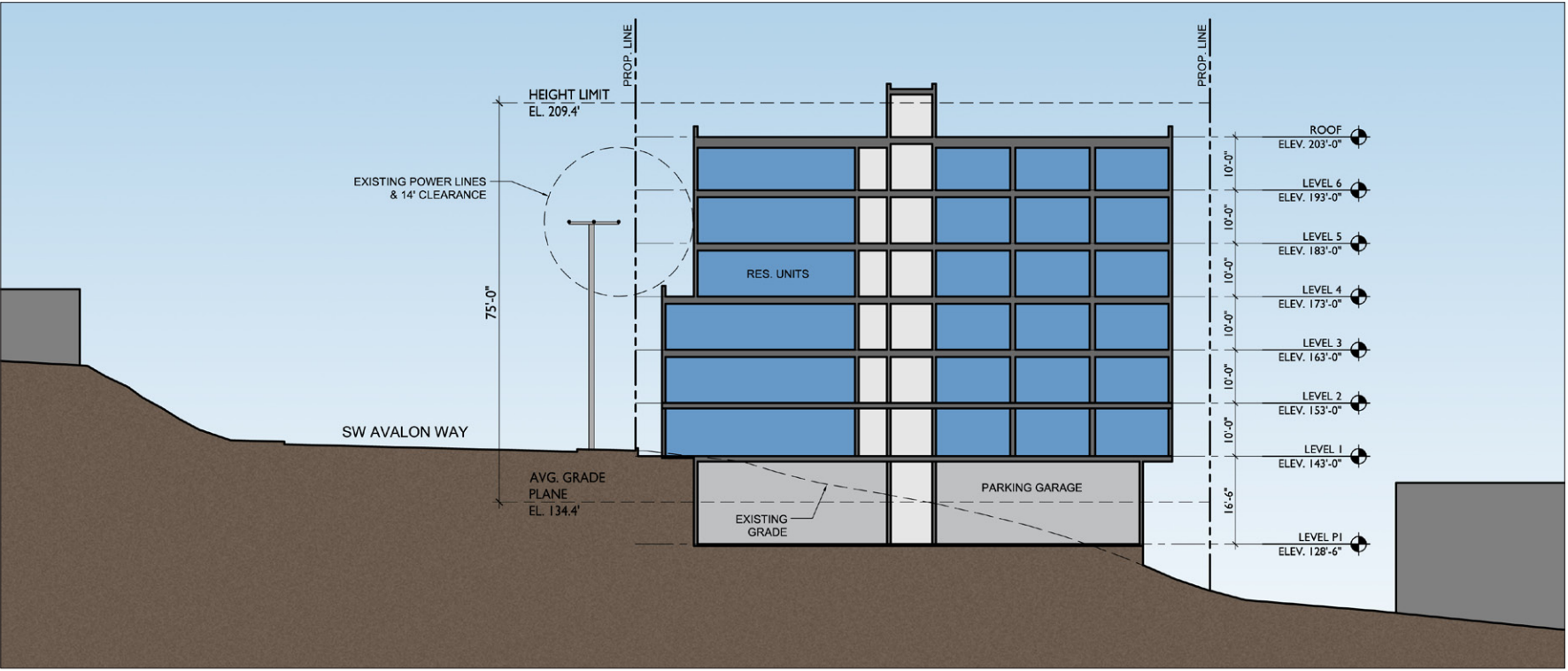
LEVEL P1



LEVELS 1-3



LEVELS 4-6

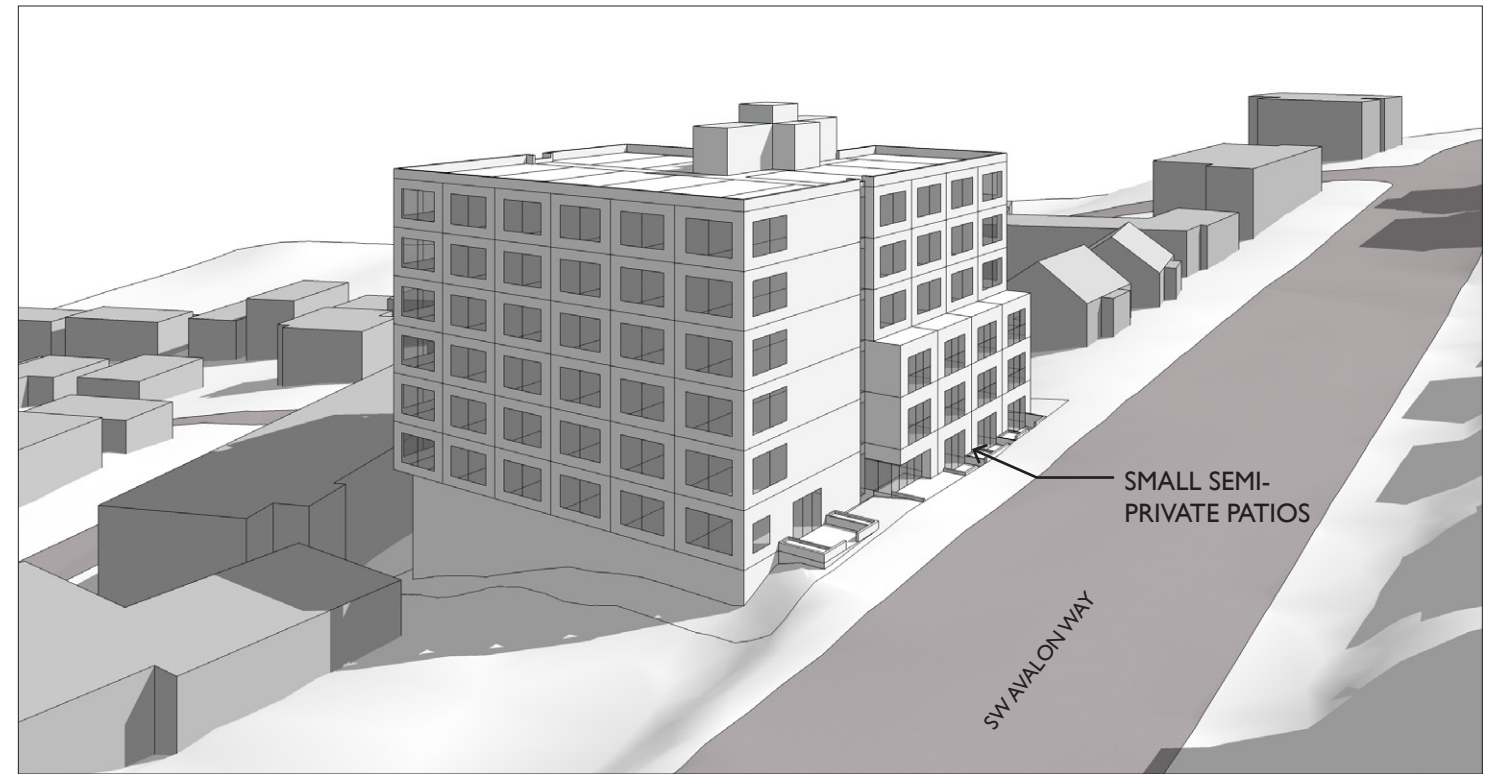


SECTION - LOOKING NORTH

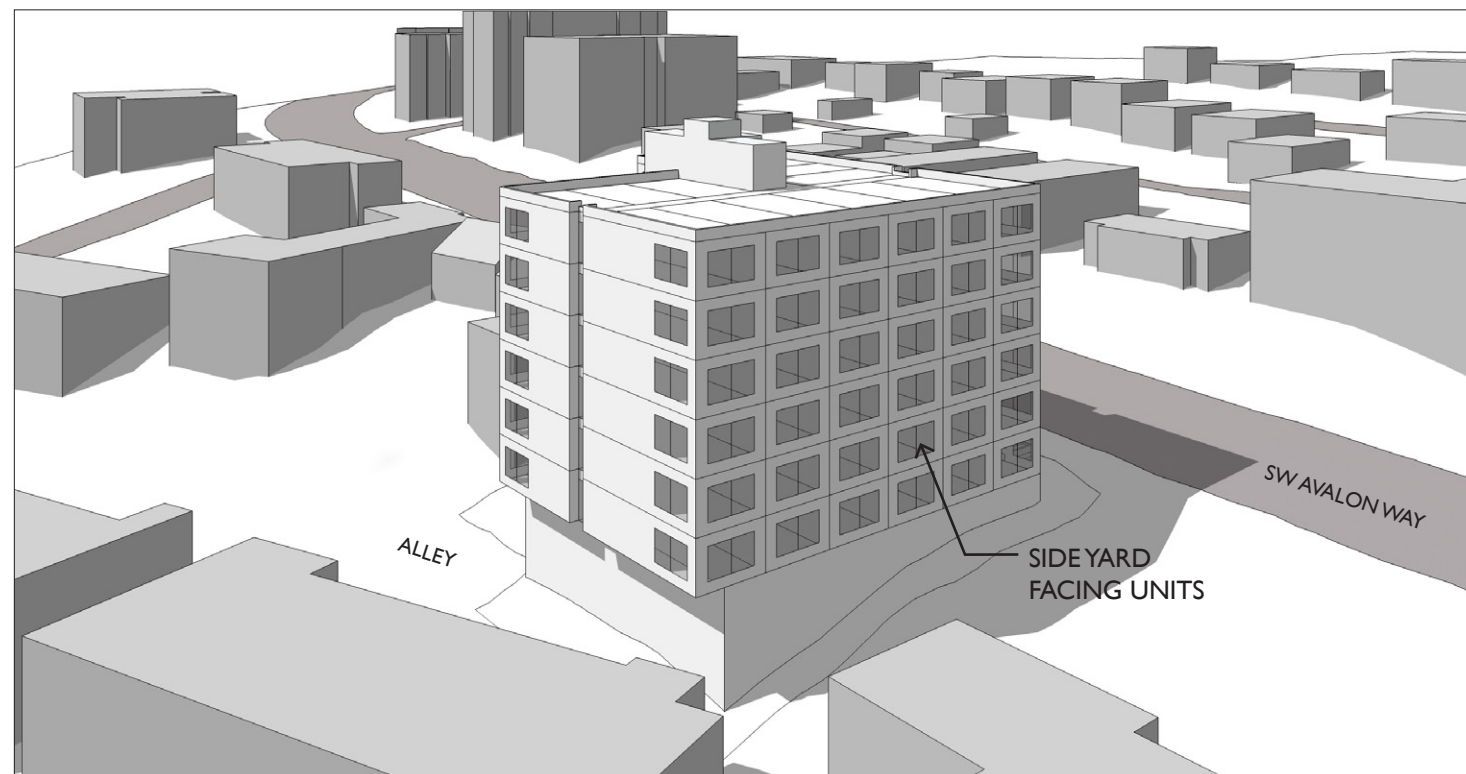
MASSING OPTION 2



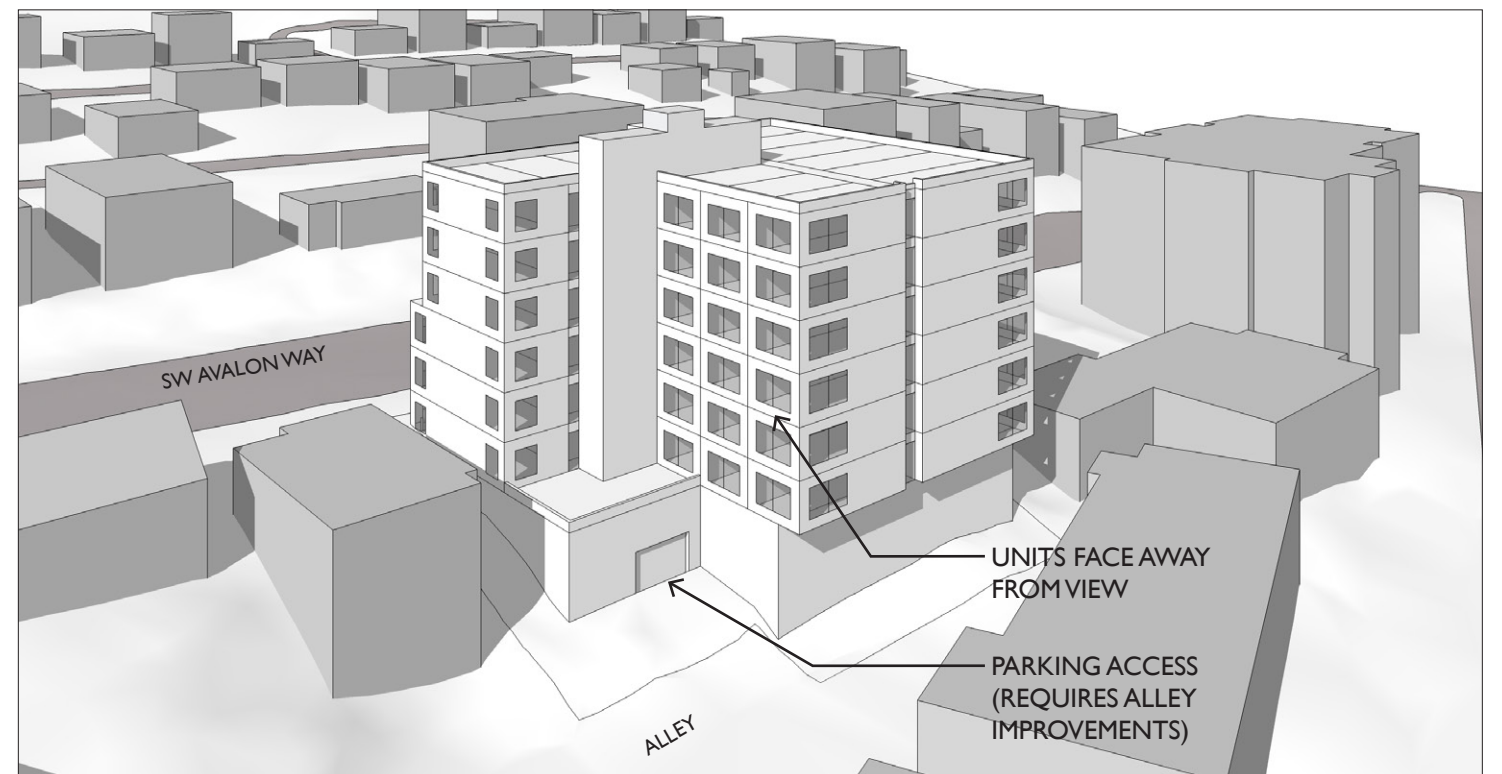
STREET-LEVEL VIEW FROM SOUTHWEST



AERIAL VIEW FROM NORTHWEST



AERIAL VIEW FROM NORTHEAST



AERIAL VIEW FROM SOUTHEAST

PREFERRED OPTION



- RESIDENTIAL
- CIRCULATION
- PARKING
- OUTDOOR
DECKS

MASSING OPTION 3

PREFERRED OPTION

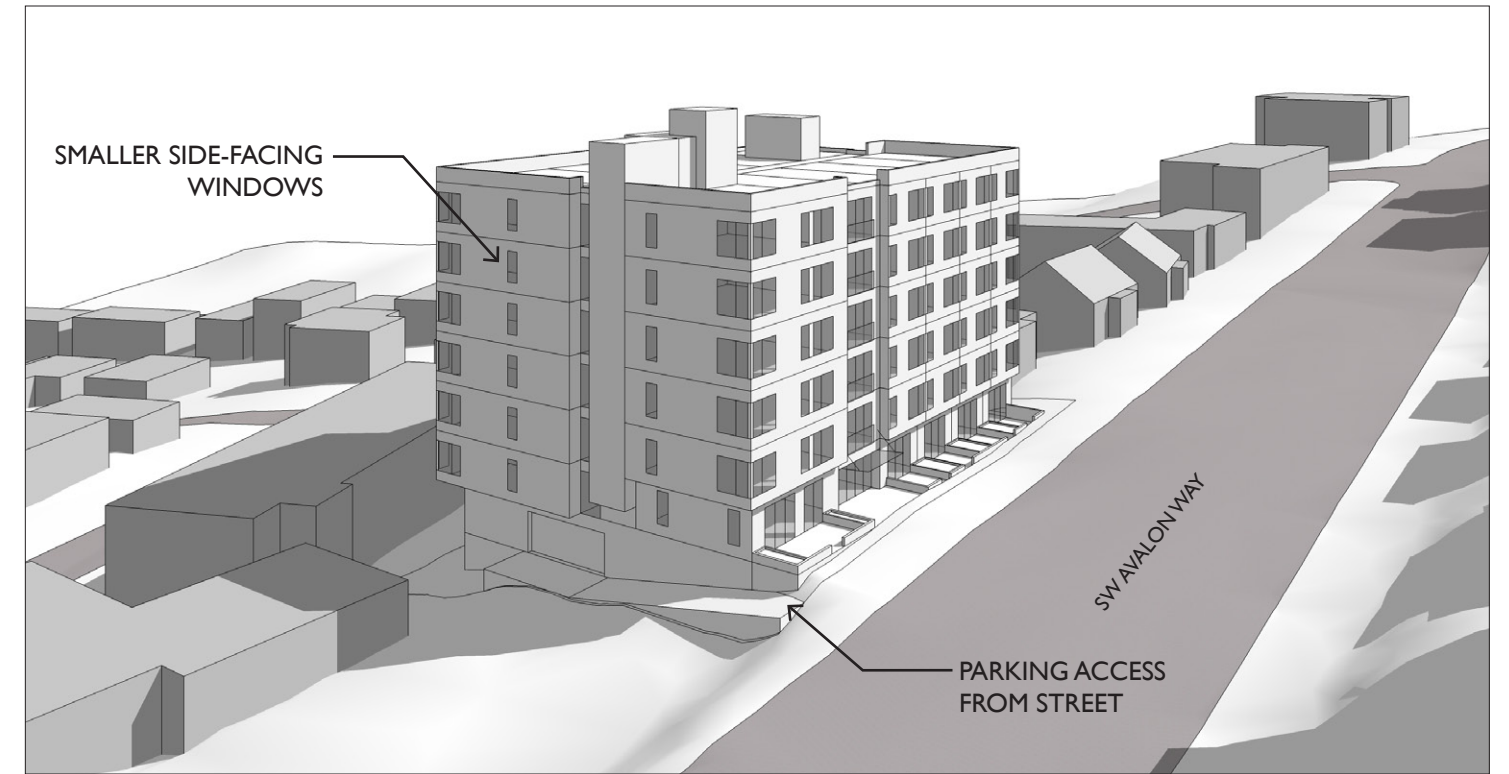


STREET-LEVEL VIEW FROM SOUTHWEST

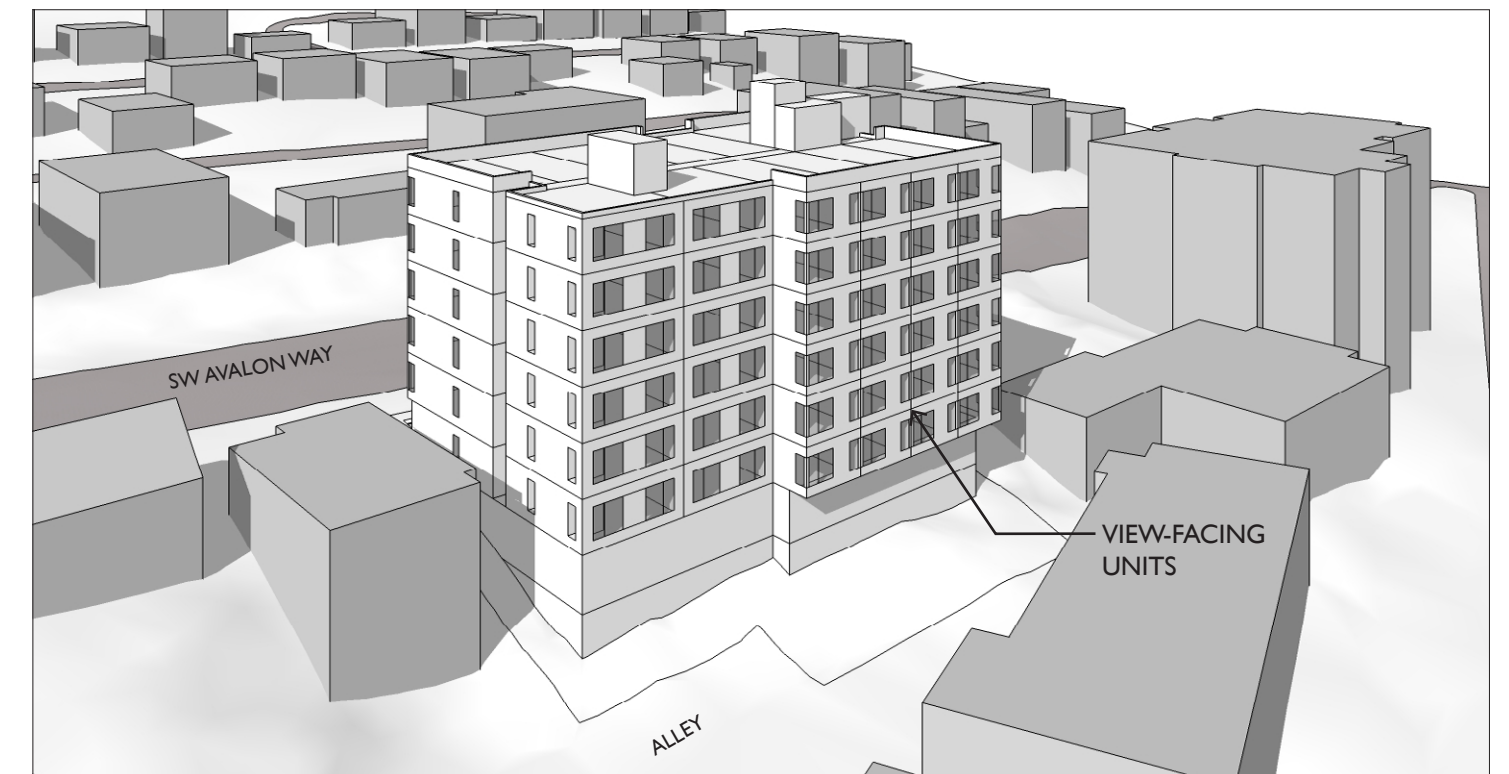


AERIAL VIEW FROM NORTHEAST

3039 SW AVALON WAY - DPD #3022717



AERIAL VIEW FROM NORTHWEST

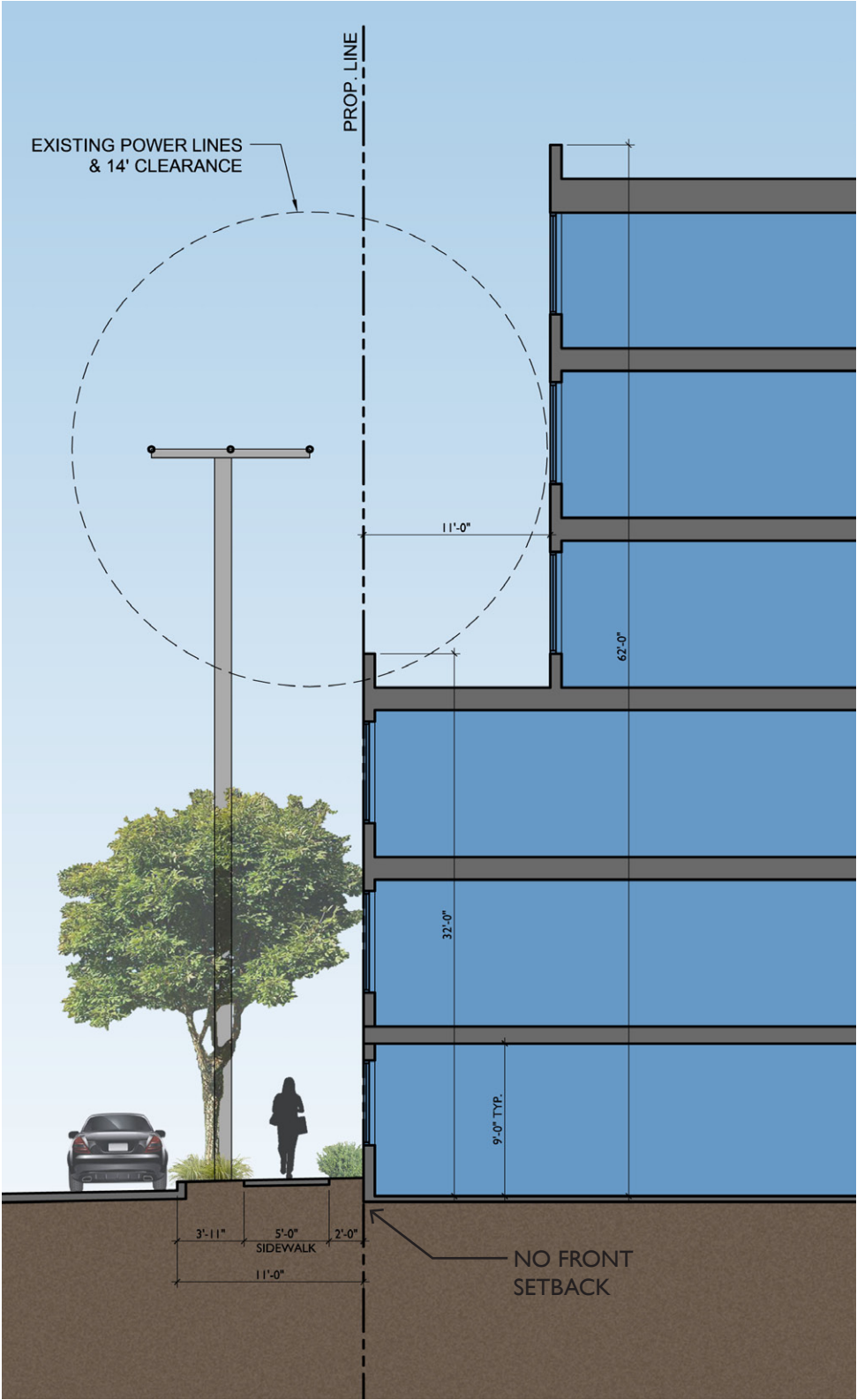


AERIAL VIEW FROM SOUTHEAST

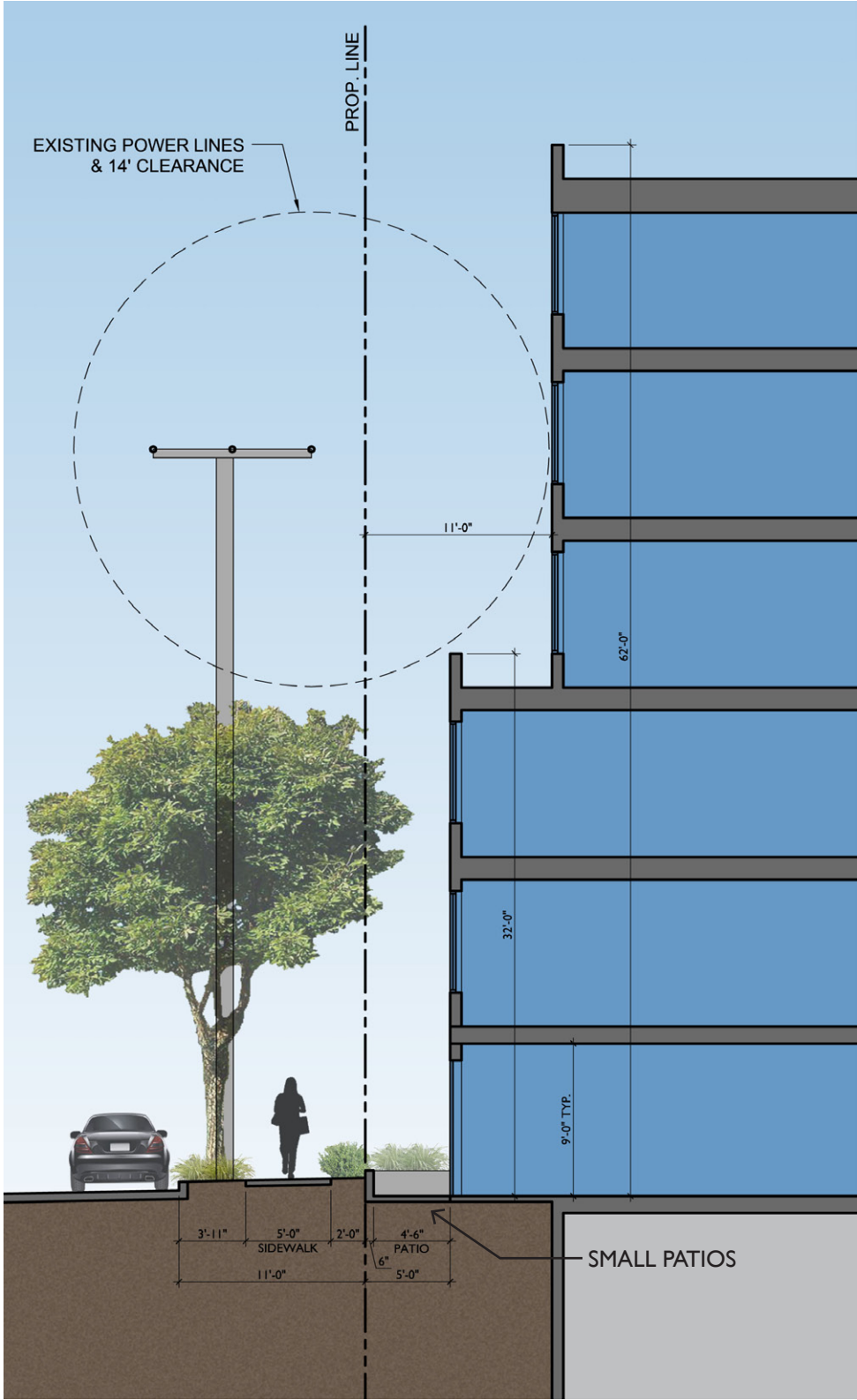
EARLY DESIGN GUIDANCE

STREETSCAPE SECTIONS

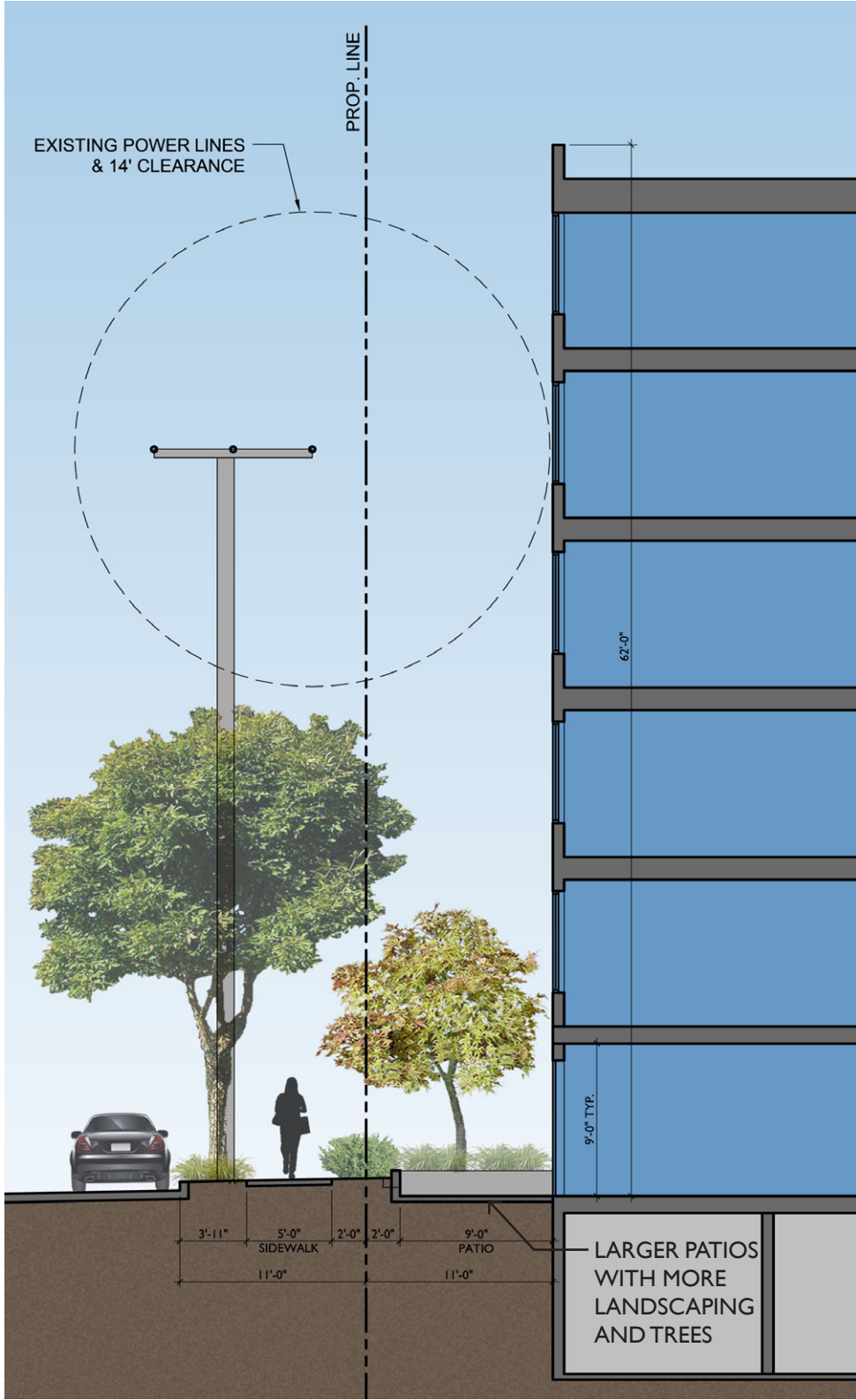
SW AVALON WAY



MASSING OPTION 1



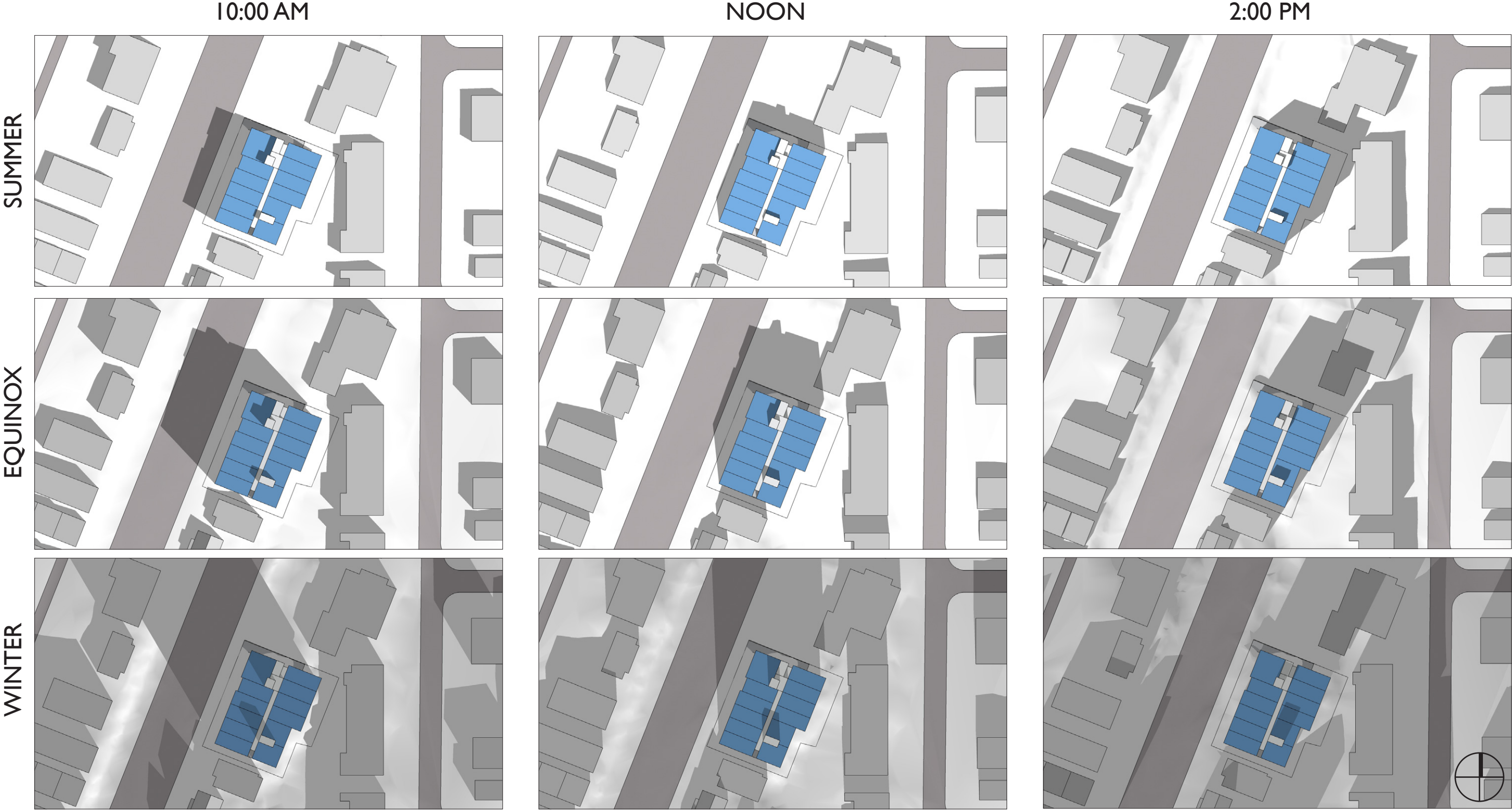
MASSING OPTION 2



MASSING OPTION 3

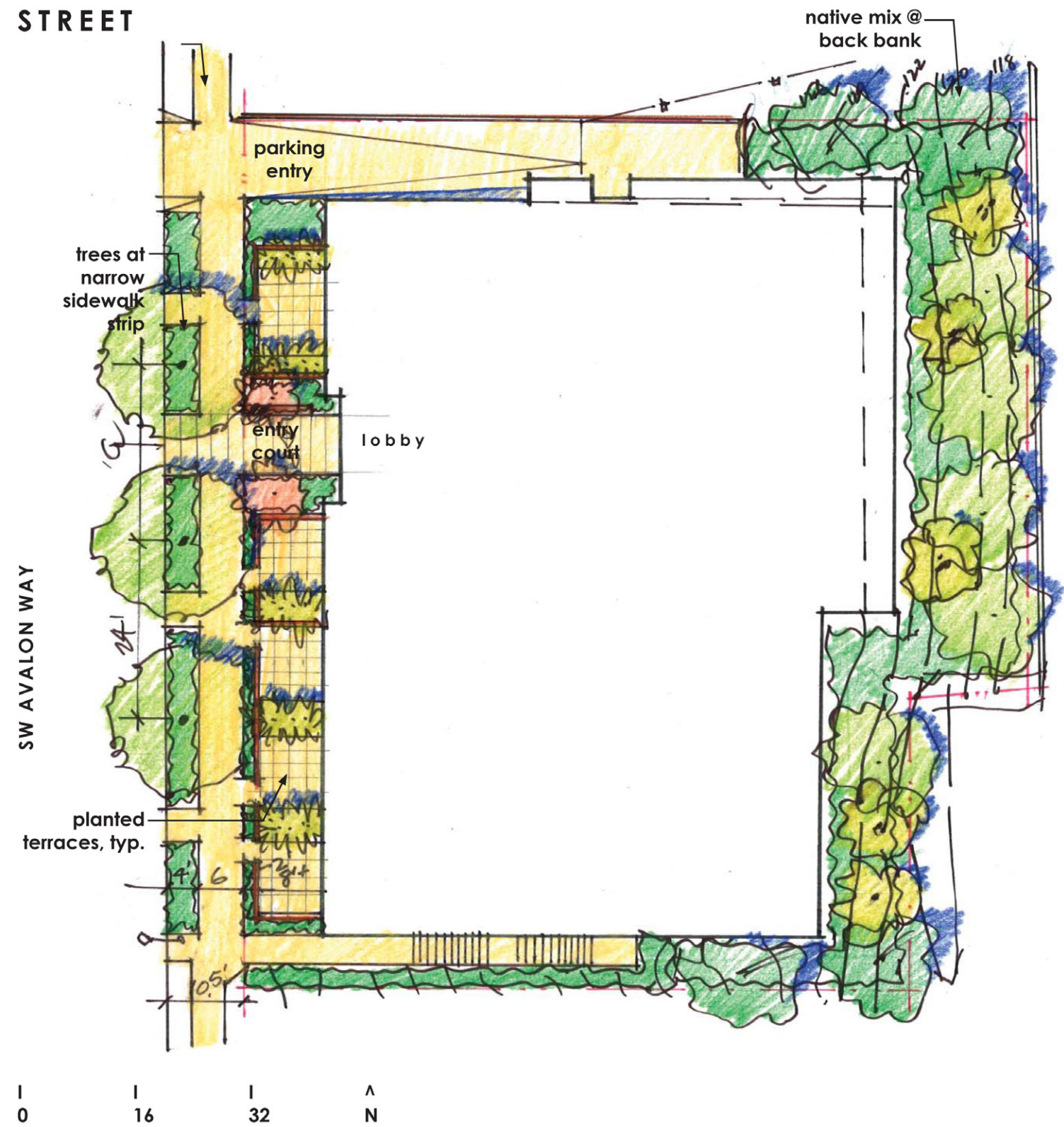
SHADING STUDY

W OPTION 3



LANDSCAPE CONCEPT

GROUND LEVEL



planted terraces



trees at narrow sidewalk strip



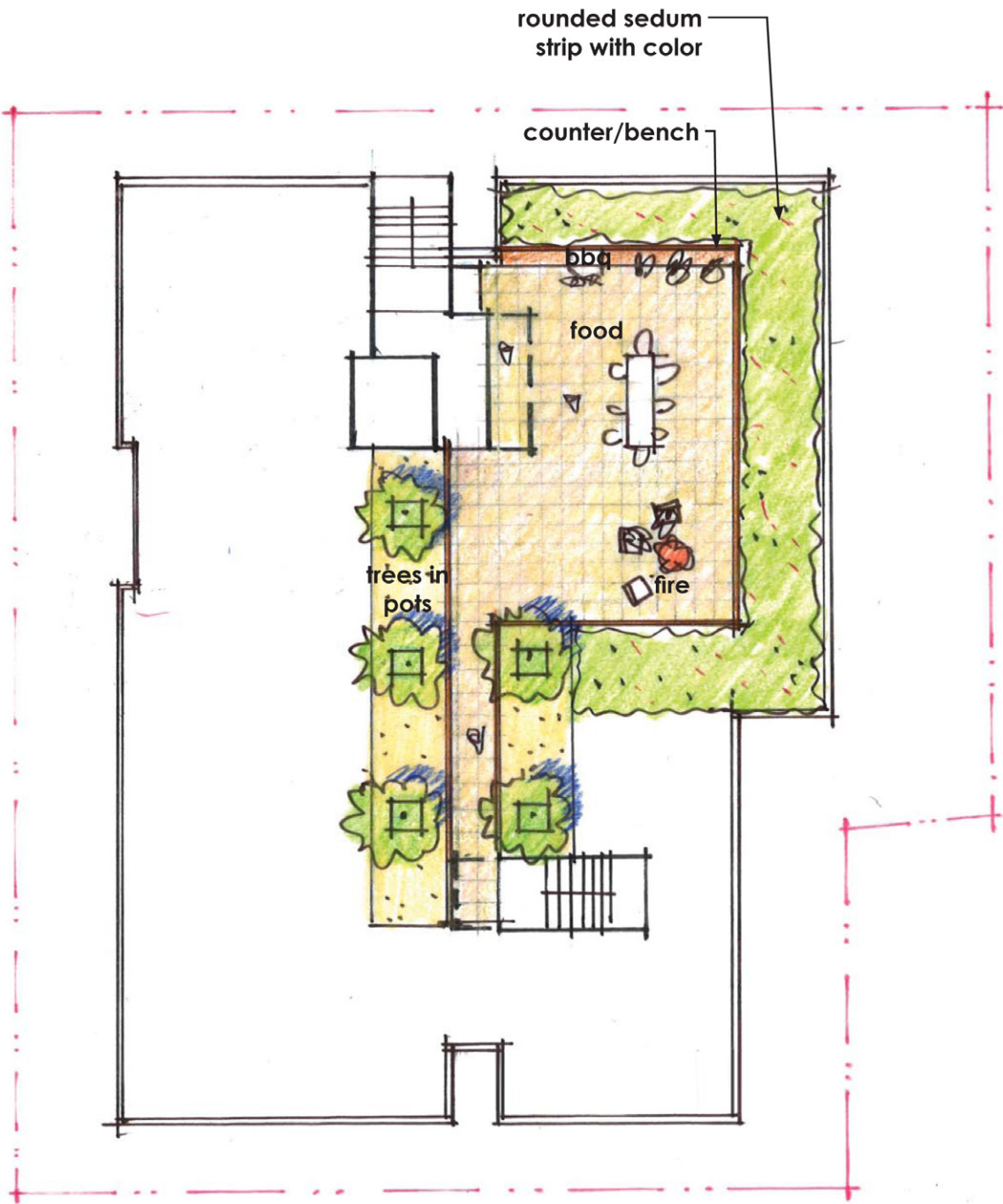
native mix at back bank

LANDSCAPE CONCEPT

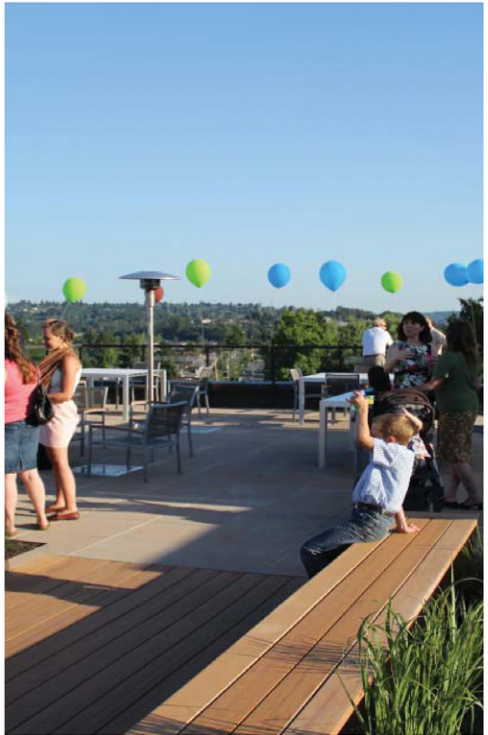
ROOF LEVEL

ROOF

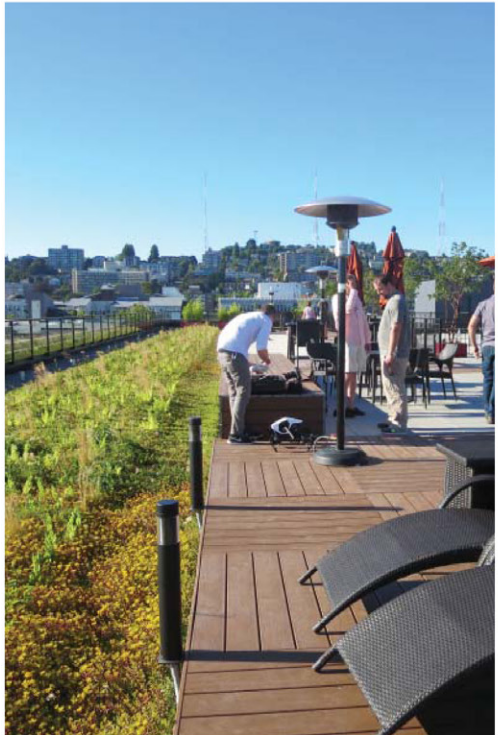
SW AVALON WAY



fun pots



bench



rounded sedum strip with color



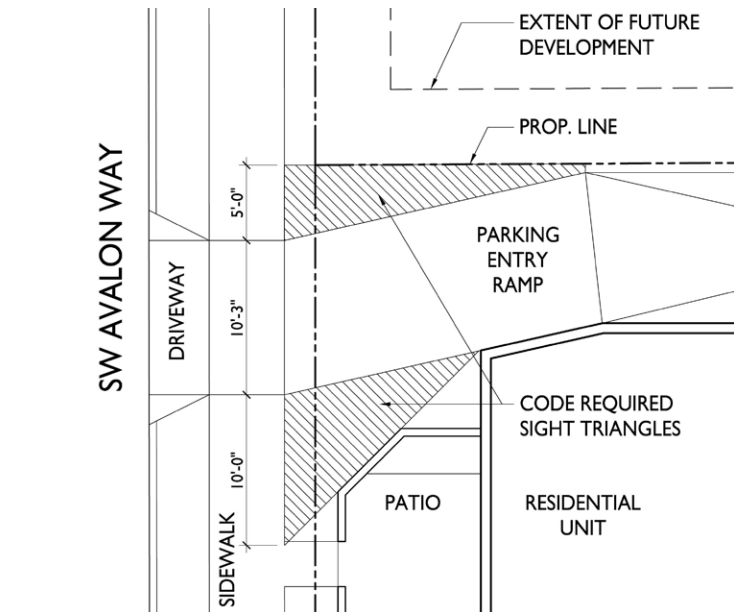
informal fire

DEPARTURE MATRIX

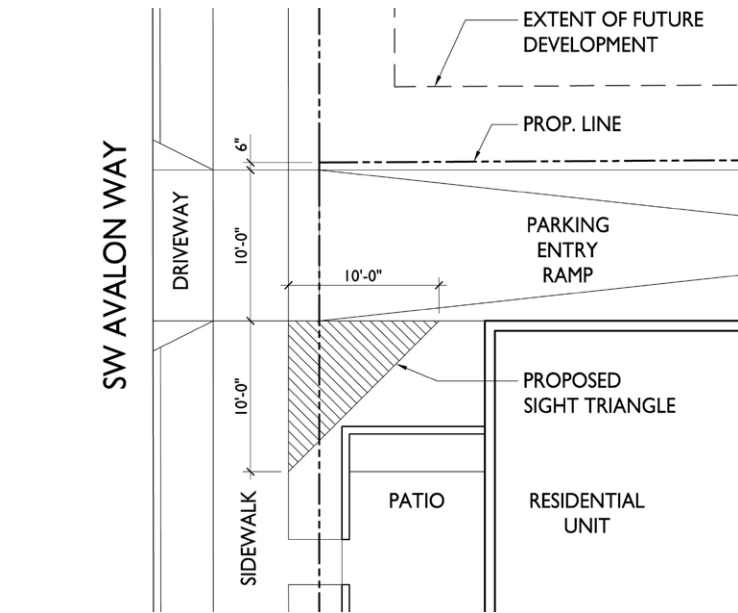
PREFERRED OPTION 3

MR ZONING CODE	REQUIREMENT	PROPOSED DESIGN	DEPARTURE RATIONALE	DESIGN REVIEW GUIDELINES
1) DRIVEWAY SIGHT TRIANGLE: 23.54.030.G.4.	For a driveway adjacent to a side lot line, the driveway shall start 5'-0" from the lot line. A 10'-0" wide sight triangle shall be provided on the opposite side.	EXTENT OF DEPARTURE: Allow driveway to meet the street at the north property line without separation. Maintain 10' x 10' triangle at south side of driveway. See Diagram	The design intent is to maximize landscaping and outdoor space and minimize the presence of the driveway. The proposed departure will allow for minimum disruption of the streetscape for the driveway. The code-required sight triangles would result in more site area being taken up by the driveway, giving it a greater presence on the streetscape and reducing the potential for landscaping. Furthermore, the corner of the building and the adjacent patio would need to be cut back to accommodate the angled driveway, resulting in an awkward massing corner at a highly visible location. The proposed departure keeps the building corner intact and allows for a cleaner, better-resolved design.	PL2 - Walkability DC1 - Project Uses and Activities
2) NORTH SIDE UPPER LEVEL SETBACK: SMC 23.45.518.B.	Below 42' above grade: 5'-0" Minimum, 7'-0" Average Above 42' above grade: 7'-0" Minimum, 10'-0" Average	PROPOSED: At all levels: 7'-6" Minimum 8'-3" Average EXTENT OF DEPARTURE: Compliant below 42' 1'-9" reduction of average setback above 42' See Diagram	Reducing the average setback for the upper levels of the north side of the building keeps the massing simple, avoiding an awkward step-back in the massing along the north side. The property to the north is currently vacant, and if redeveloped, it would likely be of the same scale as the proposed project, so the impact of a 1'-9" reduction on the property to the north would likely be minimal. Furthermore, the departure allows for the preferred massing to trade a small reduction in the north setback for a substantial increase in the front setback, where it will have a much larger impact on the character and functionality of the streetscape.	CS2 - Urban Pattern and Form PL2 - Walkability DC2 - Architectural Concept
3) SOUTH SIDE UPPER LEVEL SETBACK: SMC 23.45.518.B.	Below 42' above grade: 5'-0" Minimum, 7'-0" Average Above 42' above grade: 7'-0" Minimum, 10'-0" Average	PROPOSED: At all levels: 7'-5" Minimum 7'-5" Average EXTENT OF DEPARTURE: Compliant below 42' 2'-7" reduction of average setback above 42'	Similar to the departure requested for the north side setback, reducing the average setback for the upper levels of the south side of the building simplifies the massing. Furthermore, the departure allows for the preferred massing to trade a small reduction in the south setback for a substantial increase in the front setback, where it will have a much larger impact on the character and functionality of the streetscape.	CS2 - Urban Pattern and Form PL2 - Walkability DC2 - Architectural Concept

DEPARTURE I - DRIVEWAY SIGHT TRIANGLE



PLAN VIEW: REQUIRED DRIVEWAY SIGHT TRIANGLES

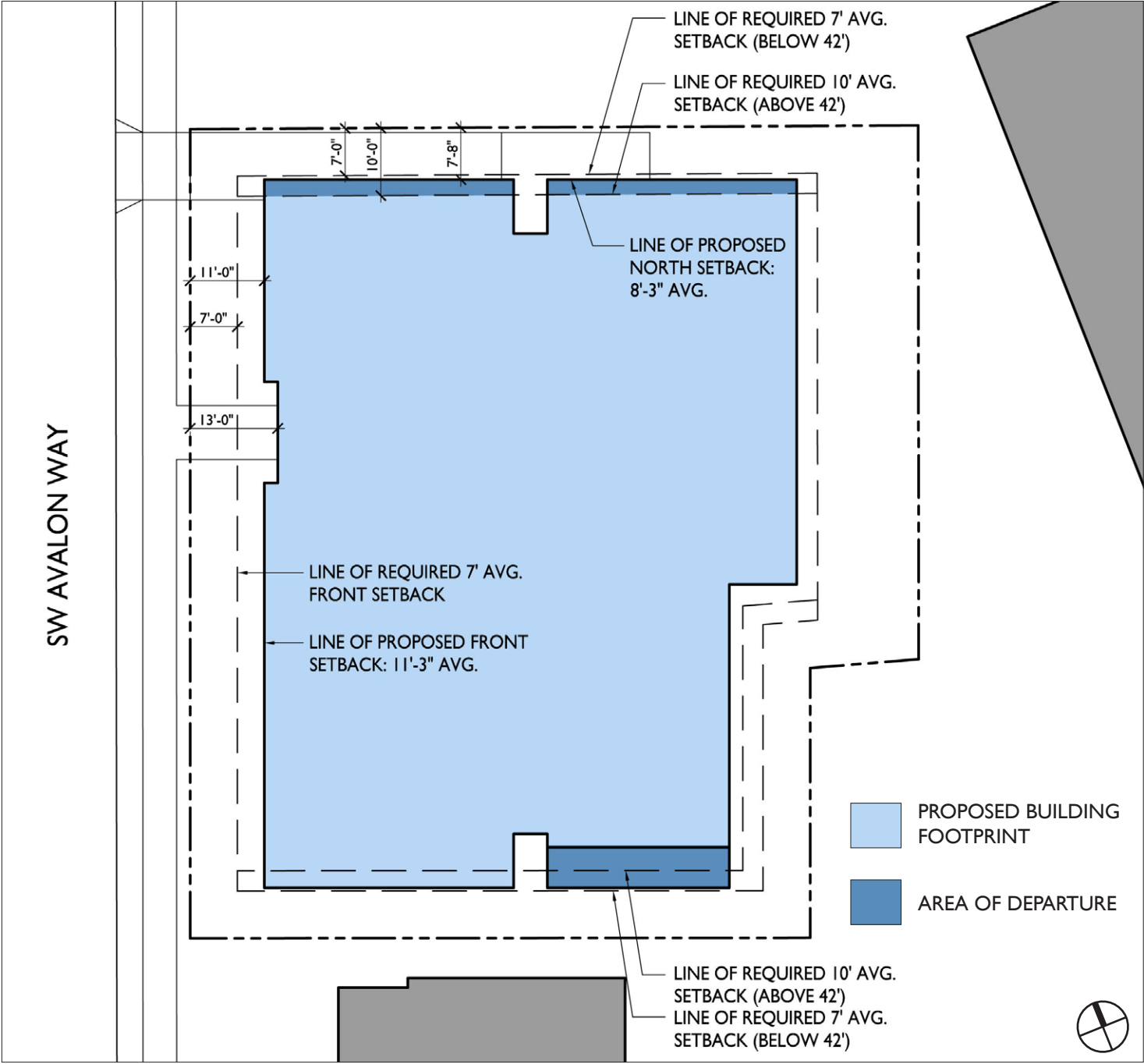


PLAN VIEW: PROPOSED DRIVEWAY SIGHT TRIANGLES

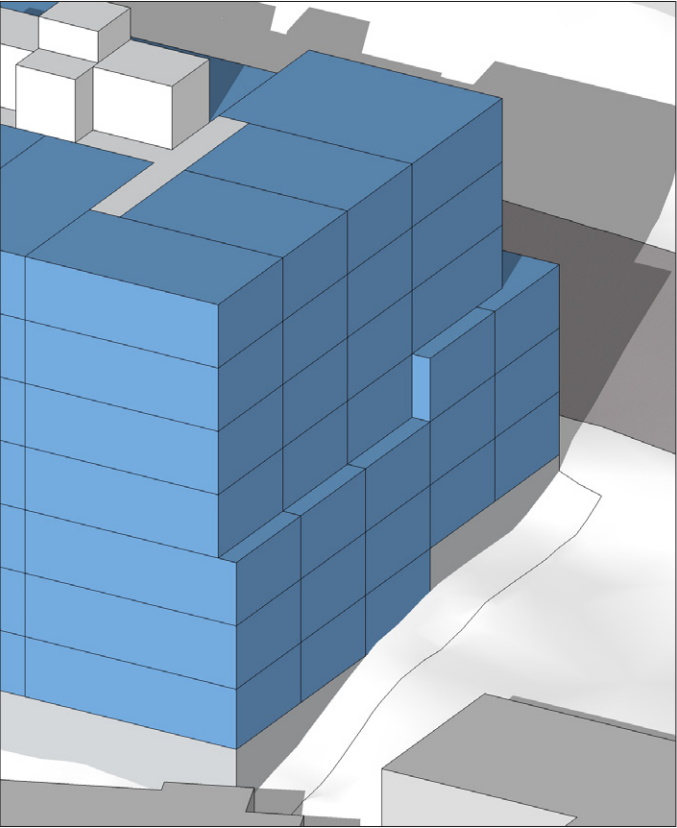
DEPARTURE DIAGRAMS

PREFERRED OPTION 3

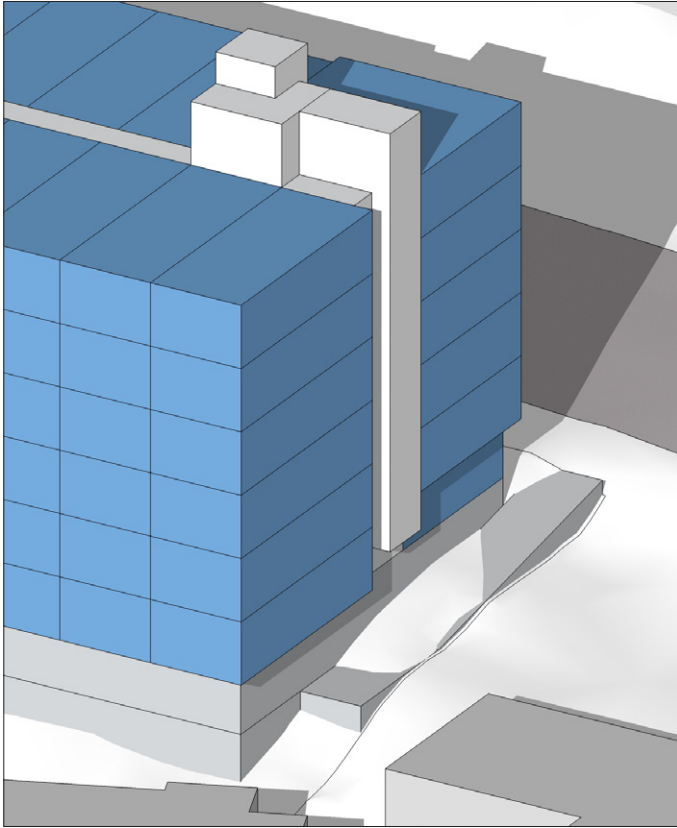
DEPARTURE 2 - SETBACKS



PLAN VIEW: PROPOSED SETBACKS



NORTHEAST VIEW: CODE COMPLIANT OPTION A



NORTHEAST VIEW: PROPOSED OPTION C