

BARANOF INTERBAY STORAGE

Early Design Guidance Package

West Design Review Board: Meeting Date 02/03/2016

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DPD PROJECT # 3022095



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Aerial Photograph

SITE

1634 15th Ave W.
Interbay, Seattle
WA 98119

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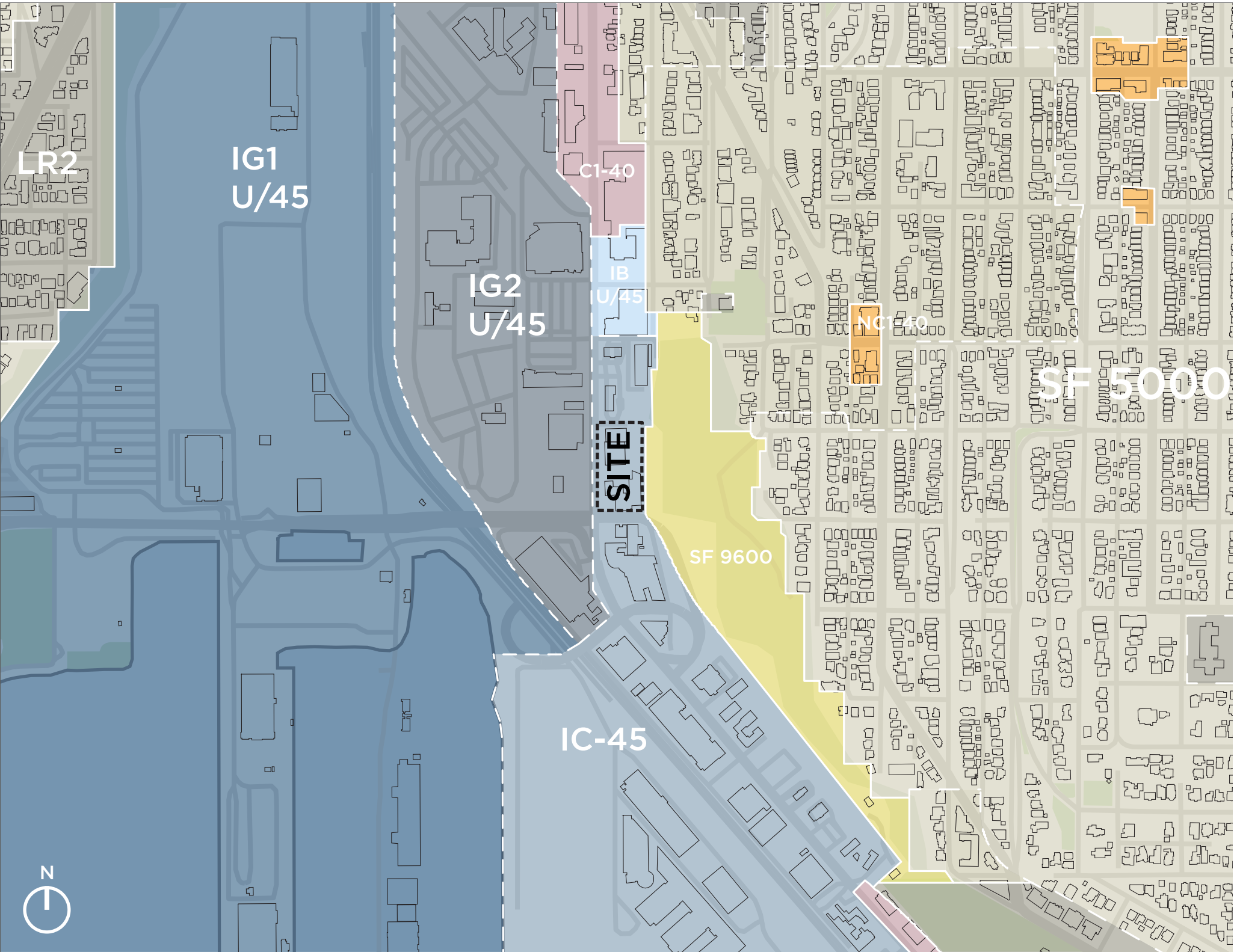
DEVELOPMENT OBJECTIVE

The proposed project at 1634 15th Avenue West consists of two independent, four-story structures consisting of approximately 207,108 gross square feet of self-storage space and 76 parking spaces.

The site was identified as a candidate for self-storage to serve the growing storage needs as multi-family housing increases within this area. The site itself abuts the Magnolia Bridge and Queen Anne Green Belt, which offers a natural visual buffer between it and the residential zone at the crest of the hillside. The area itself as an industrial/commercial zone is suited to the use and the development additionally allows increased density and infill along 15th Ave. W. The goal of this project is to serve the growing need for inner city storage demands, while provide a contextual facility that blends with the growing and transitioning Interbay area. Its location offers ease of access from residents within the urban core.

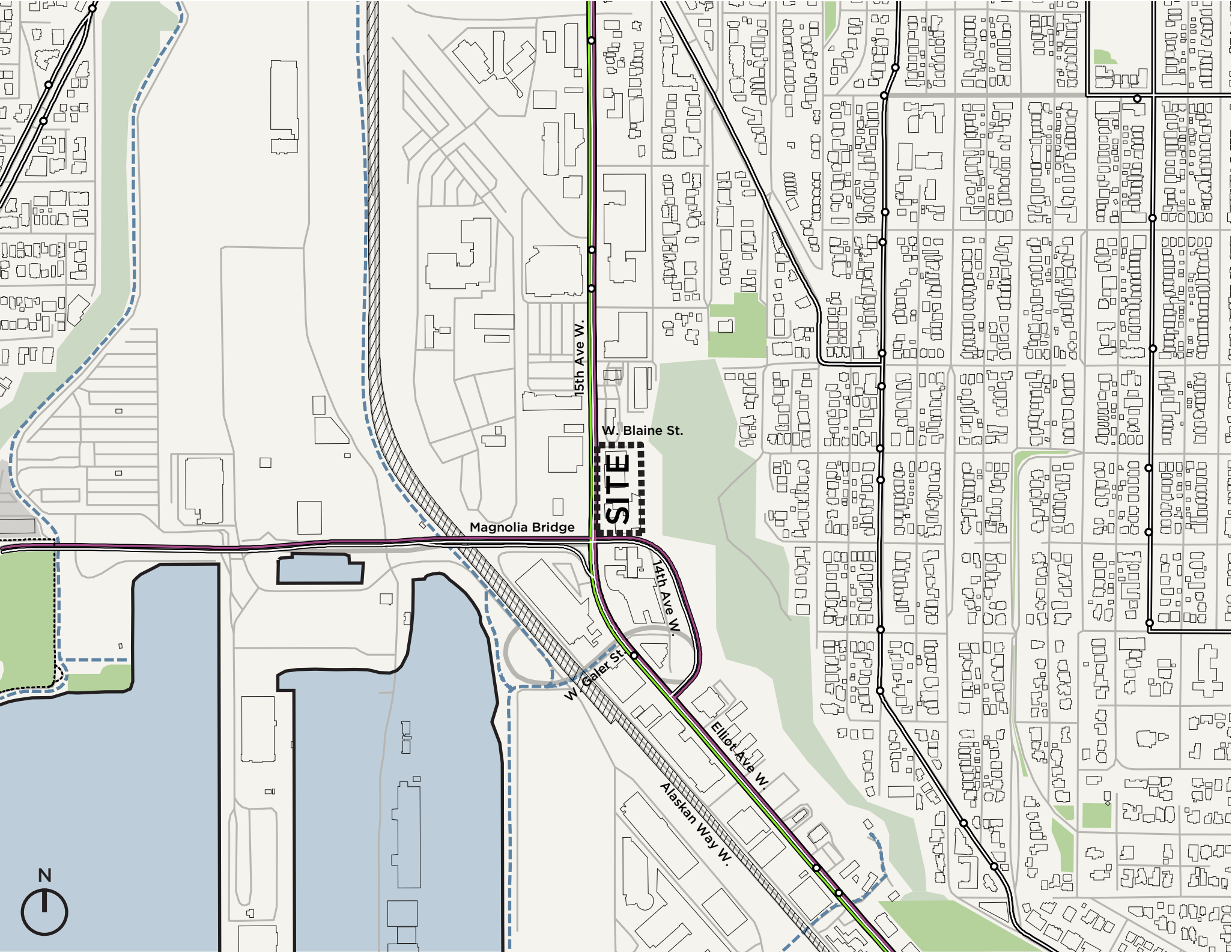


2.0 SITE CONTEXT AND URBAN DESIGN ANALYSIS



SURROUNDING ZONING CLASSIFICATION

| MAP LEGEND | |
|------------|-------------------------|
| | INDUSTRIAL GENERAL 1 |
| | INDUSTRIAL GENERAL 2 |
| | INDUSTRIAL COMMERCIAL |
| | INDUSTRIAL BUFFER |
| | SINGLE FAMILY 9600 |
| | SINGLE FAMILY 5000 |
| | LOWRISE |
| | COMMERCIAL |
| | NEIGHBORHOOD COMMERCIAL |



SURROUNDING TRANSIT MAP

MAP LEGEND

- TRANSIT WAY
(Seattle Transit Classification)
- MAJOR TRANSIT STREET
(Seattle Transit Classification)
- BUS ROUTE
- BUS STOP
- BICYCLE TRAIL
- BICYCLE-FRIENDLY ROADWAY
- RAILWAY AREA



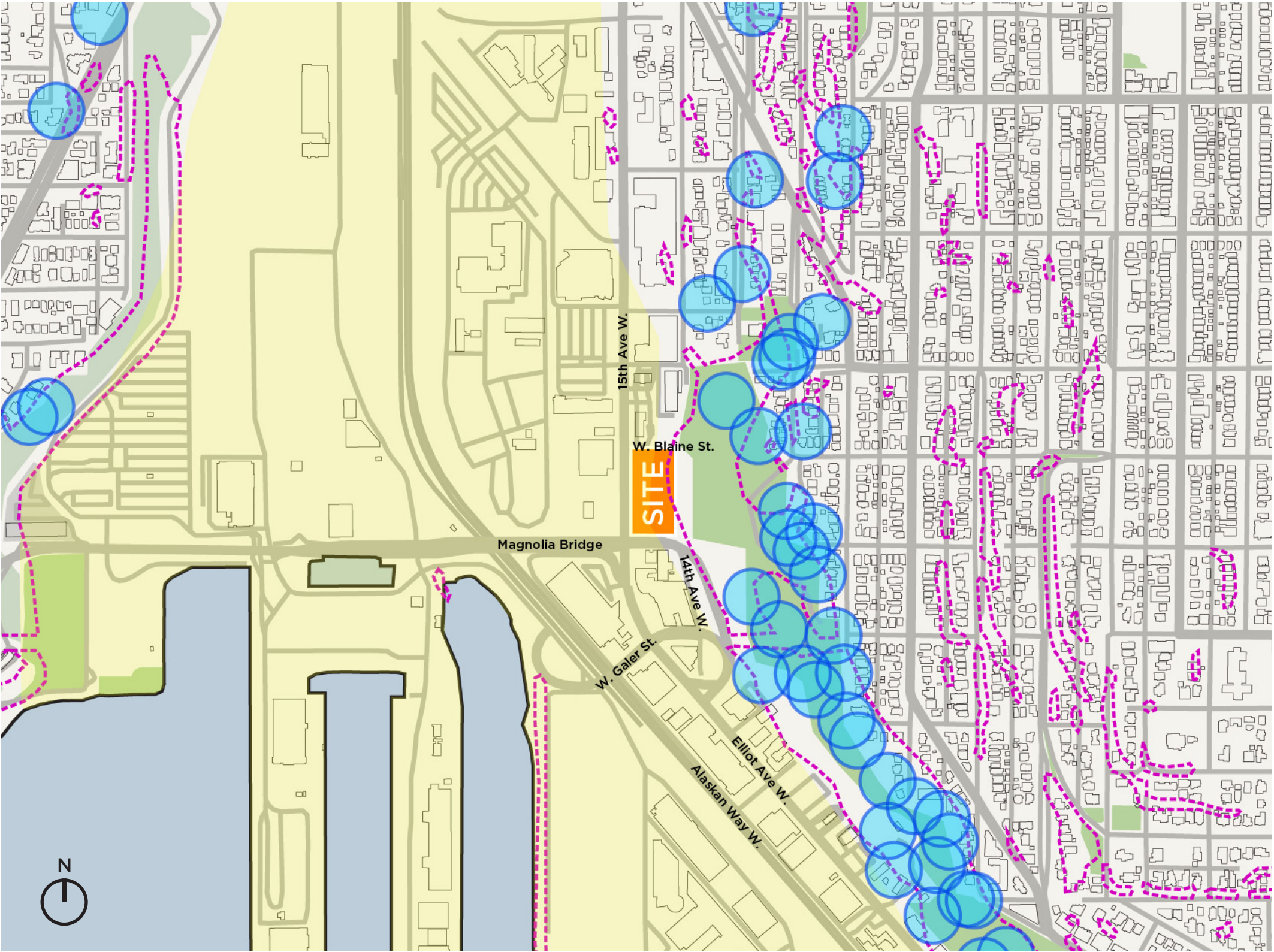
2.0 SITE CONTEXT AND URBAN DESIGN ANALYSIS



SURROUNDING MAJOR BUILDING TYPES

MAP LEGEND

- PROJECT SITE
- RETAIL
- MIXED USE
- MULTI-FAMILY RESIDENTIAL
- RESTAURANT
- RECREATIONAL
- MEDICAL FACILITY
- OFFICE BUILDING
- STORAGE FACILITY
- GOVERNMENT
- INDUSTRIAL



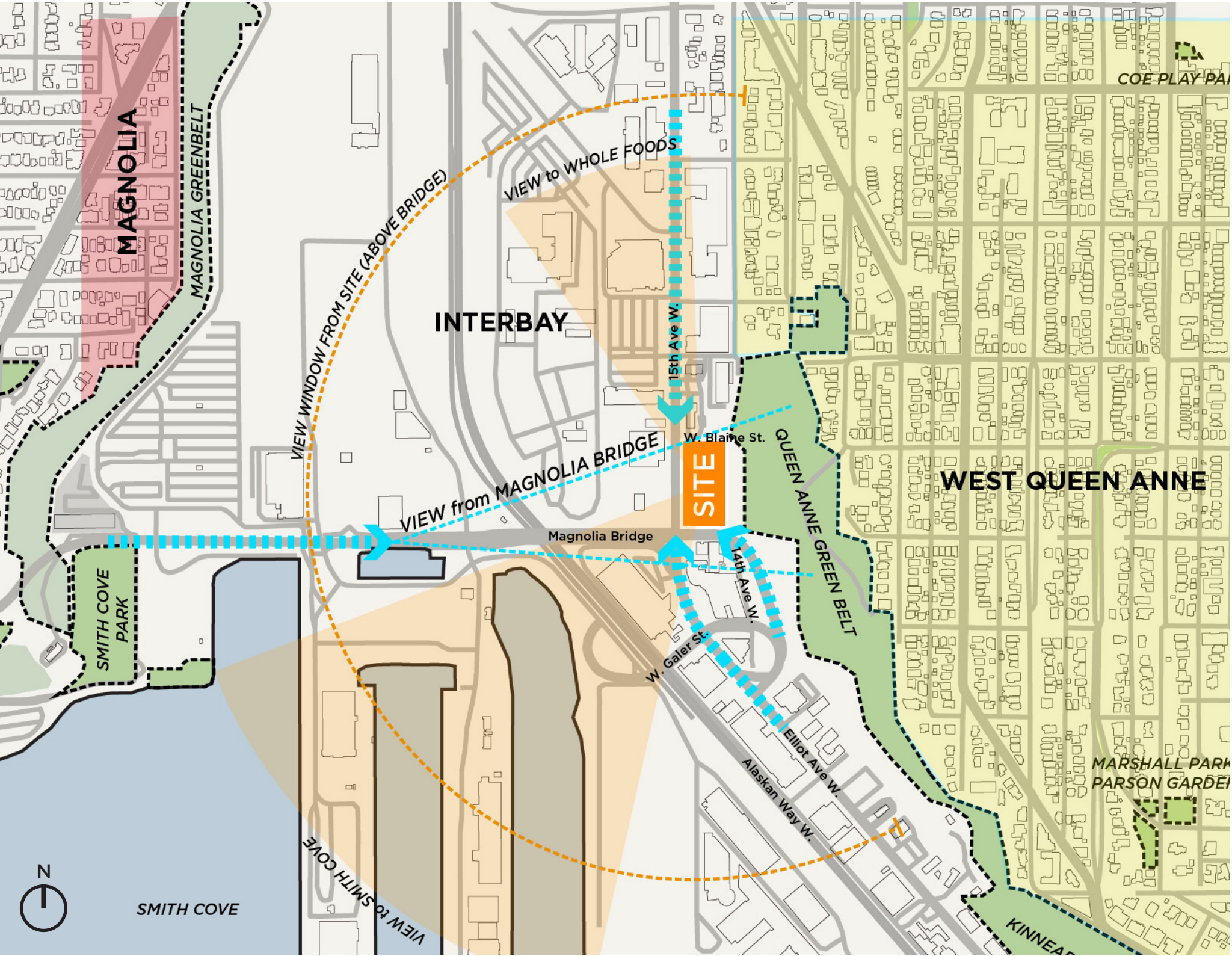
ENVIRONMENTAL CRITICAL AREAS

MAP LEGEND

- PROJECT SITE
- LIQUEFACTION AREAS
- STEEP SLOPE AREAS
- KNOWN SLIDE AREAS



2.0 SITE CONTEXT AND URBAN DESIGN ANALYSIS



VIEWS, GREEN SPACES, AND NEIGHBORHOODS

MAP LEGEND

- PROJECT SITE
- INTERBAY
- MAGNOLIA
- WEST QUEEN ANNE
- GREEN SPACES
- APPROACHING VIEWS



DESIGN CUES NARRATIVE

Views from the project are limited due site adjacencies. Magnolia Bridge to the South limits views toward Smith Cove while the greenbelt hillside to the East, along the backside of the site, completely blocks all views to and from the site. Views toward the site are much more prevalent both coming across Magnolia Bridge and approaching North on 15th Ave W. toward Ballard.

Project Response: Scale the building appropriately, especially adjacent to the bridge. Vary the facade along 15th Ave W. to create visual interest.

Site access is limited, requiring a need for below grade as well as surface level parking. Clearance under the bridge onto W. Garfield St. could present an access challenge.

Project Response: Consider secure surface parking at the back of the site and provide parking garage access off of W. Blaine St. Align W. Garfield St access with bridge piers and coordinate with Seattle City Light regarding pole locations.

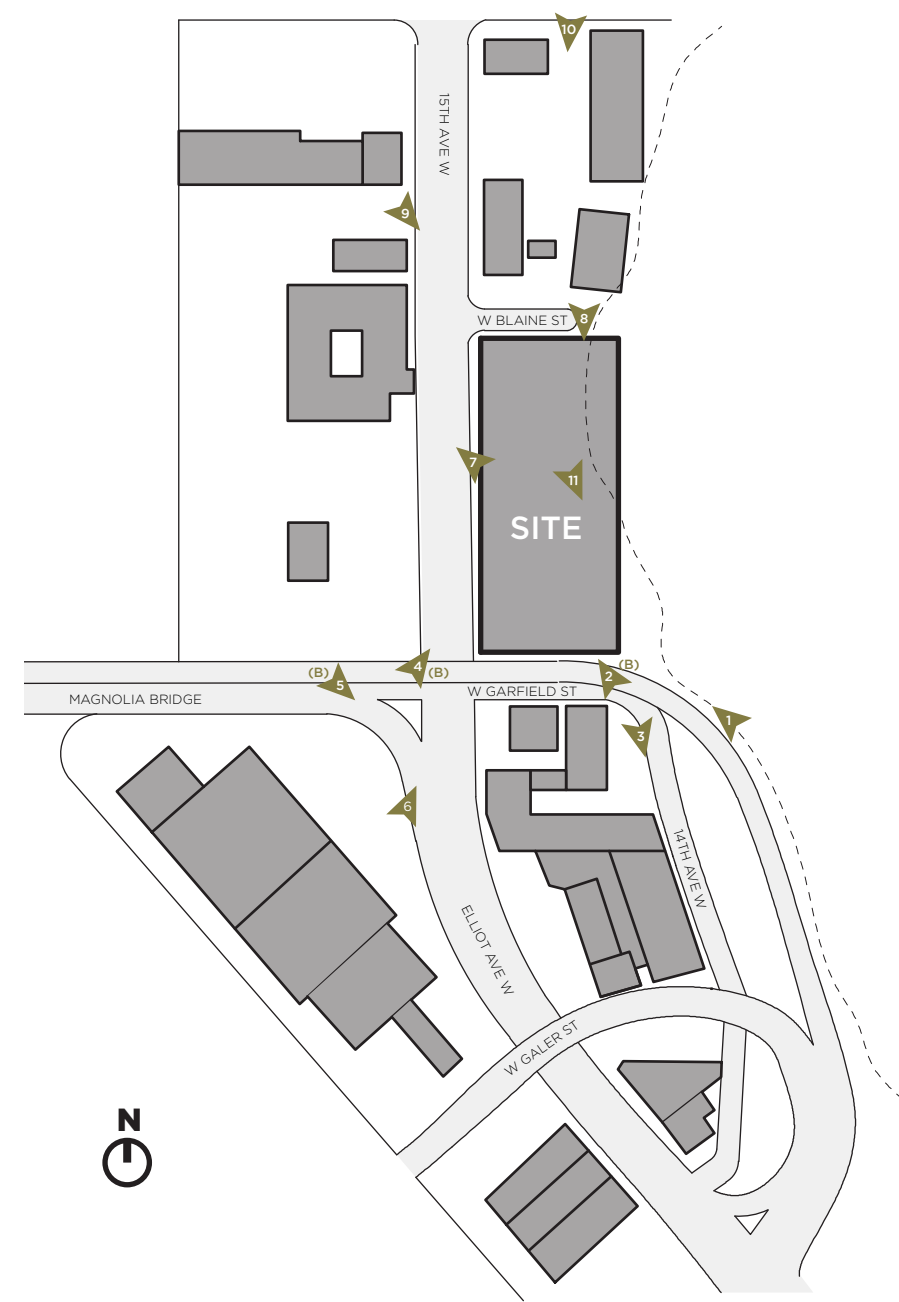
The industrial character of the Interbay area suggests a specific palette of colors, materials, forms, etc. The unique location of the site along a main arterial through Interbay allows an opportunity to exemplify a uniquely 'industrial-tech' feel.

Project Response: Concrete, glass, metal panel, and corrugated siding will act as the primary materials along with hints of color. Examples should be taken from the Work Lofts adjacent and the Fire Station and Whole Foods to the North.

The adjacent Queen Anne Green Belt to the east provides a substantial vegetation buffer between the residential zone and site.

Project Response: The site slope and green belt reduce the available developable area, however, aid in influencing the access, massing and form of the project while preserving views to hillside.

2.0 SITE CONTEXT AND URBAN DESIGN ANALYSIS



1 View adjacent Garfield St. looking toward 15th Ave



2 View from bridge, looking Northwest into site.



3 View down 14th Ave W.



④ View from bridge, looking Northeast into site



⑤ View from bridge, looking SouthEast down Elliot Ave W.



⑥ View looking North up 15th Ave W.



⑦ View looking accross from site, North up 15th Ave W.



⑧ View looking down the alley in the back of the site



9 View looking Southeast toward site

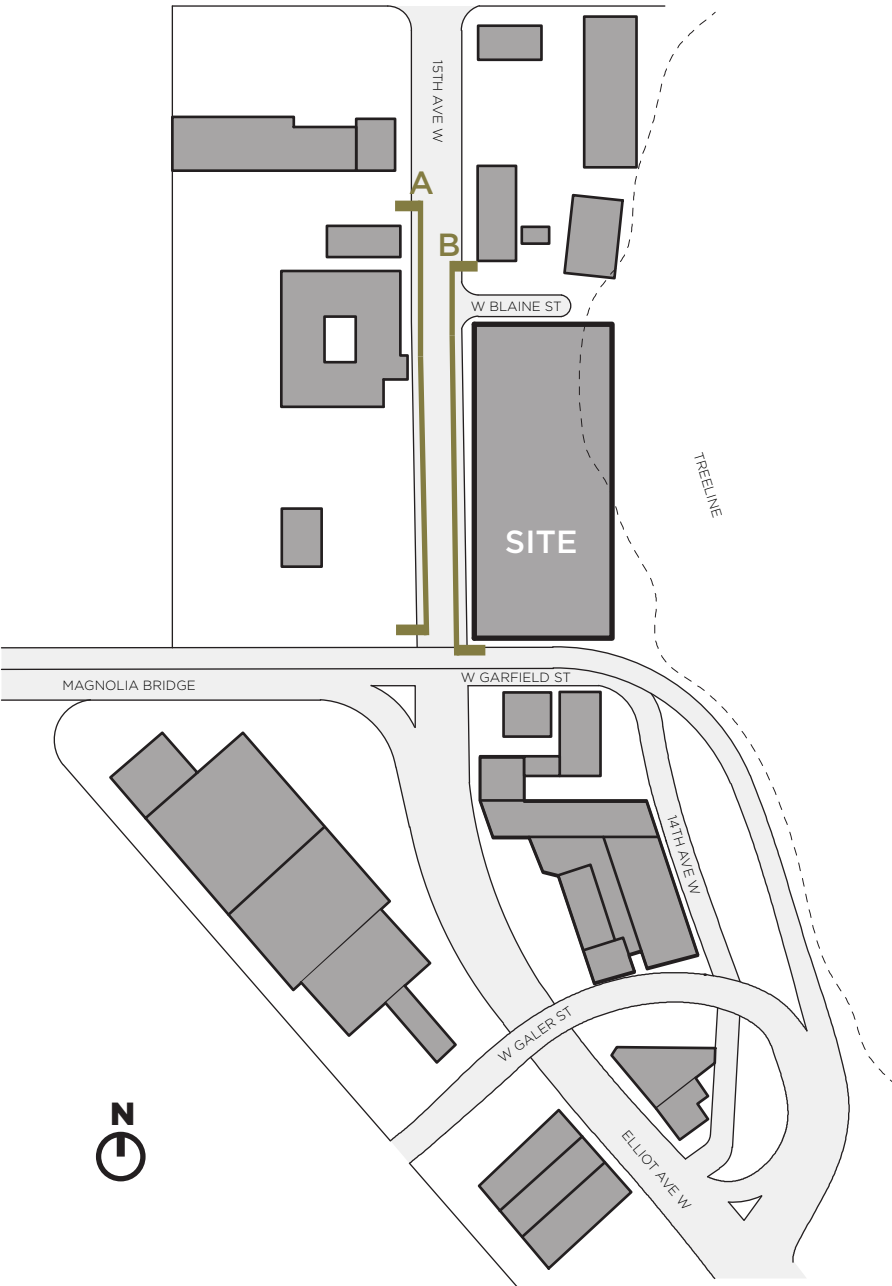


10 View looking South toward site



11 View looking SouthEast at the back of site, at property line jog (existing retaining wall)

2.0 SITE CONTEXT AND URBAN DESIGN ANALYSIS



Site Elevation A



Site Elevation B



3.0 DESIGN GUIDELINES

CS1. Natural Systems and Site Features

Citywide Guideline: Use natural systems and features of the site and its surroundings as a starting point for project design.

The site development and building placement responds to the natural features and existing topography presented by the Queen Anne Green Belt. In order to most preserve the existing trees the project seeks to develop primarily within the existing developed area.

CS2. Urban Pattern and Form

Citywide Guideline: Strengthen the most desirable forms, characteristics, and patterns of the streets, block faces, and open spaces into the surrounding area.

The preferred building option seeks to reduce the overall massing of the building by separating them into two independent structures - hence reducing the monolithic presence along 15th Avenue W. The separation between the buildings further allows for the preservation of views to the Queen Anne Green Belt.

CS3. Architectural Context and Character

Citywide Guideline: Contribute to the architectural character of the neighborhood.

With the project site located along an industrial/commercial corridor, the building design seeks to respond to the existing industrial character along 15th Avenue W., while acknowledging the evolving and ever-changing uses as sites are redeveloped; particularly for multi-family housing. To that degree, the architectural character, massing and preliminary material choices seek to respond to cues of neighboring developments such as the Work Loft; a mixed use multifamily building across the street.

PL1. Connectivity

Citywide Guideline: Complement and contribute to the network of open spaces around the site and the connections among them.

In lieu of siting access and parking to the back (east) edge of the site, the separation of the buildings in the proposed scheme seeks to provide a break in the massing and introduce open space between the buildings for parking, sidewalks, enlarged entry walk at the lobby and space for vegetation. This gesture additionally allows for a pedestrian experience through the reduced scale of the massing, introduction of entry canopies and setting the building back from the sidewalk for inclusion of landscape planting strips.

PL2. Walkability

Citywide Guideline: Create a safe and comfortable walking environment that is easy to navigate and well-connected to existing pedestrian walkways and features.

By locating the access and parking at the center of the site it allows for increased visibility, as well as greater safety and security of pedestrians or motorists visiting the building. Below grade parking access has been further located off Blaine St. to reduce the cross pollination of motorists and pedestrians coming to and from the site or along the public way. The selection and placement of materials has been carefully scrutinized in order to emphasize the internal corners of each building flanking the surface parking adjacent to the lobby, and thereby enhancing the overall experience of navigating to the primary entry.

PL3. Street-Level Interaction

Citywide Guideline: Encourage human interaction and activity at the street-level with clear connections to building entries and edges.

While the majority of visitors to the site and building will come by car, the building seeks to locate the lobby near the public way to improve interaction along 15th Ave. W. Building signage and strategic placement of increased glazing areas seek to clearly identify entries to the building, which additionally are protected by overhead pedestrian canopies. The project is further sited to the westerly edge to preserve the wooded hillside that visually screens the adjacent residential zone up the hill, offering privacy from 15th Ave. W.

PL4. Active Transportation

Citywide Guideline: Incorporate design features that facilitate active forms of transportation such as walking, bicycling, and use of transit.

As a storage facility, the site will be primarily served by motorists and it is anticipated to have limited bicycle or transit visitors. However, as the site is located along a major transit corridor, sidewalks have been incorporated into the site development in order to address any future increased pedestiran and bicycle access that may arise.

DC1. Project Uses and Activities

Citywide Guideline: Optimize the arrangement of uses and activities on site.

The division of the building mass serves to allow separation of vehicle access dependent on parking durations. The central at-grade parking is situated to serve short term users to the site and ease of access to the lobby. Below grade parking access has been positioned off W. Blaine St. to provide separation of uses and to reduce overburdening the central space between the buildings. The building design additionally seeks to contain loading activities within the buildings themselves by introducing both a loading bay and full loading drive-thru. Further service activities are housed within these loading areas. Such areas will additionally provide covered and secure areas for bicycles.

DC2. Architectural Concept

Citywide Guideline: Develop an architectural concept that will result in a unified and functional design that fits well on the site and within its surroundings.

The building placement responds to the topography, green belt and ideal access points. The building is further split to reduce the perceived massing while maintaining clear edges at the North and South. The building façade utilizes glazing panels with offset interior program to allow for window fenestration along the primary facades. Blank walls exist at the lower levels for security purposes and have been addressed through the use of vegetative panels to add visual interest and a stronger pedestrian engagement. The overall façade treatment seeks to emulate and blend with the neighborhood, while additionally striving for material selections that align with new and future developments.

DC3. Open Space Concept

Citywide Guideline: Integrate open space design with the design of the building so that each complements the other.

The development’s program is such that it maintains a high level of service features; office, loading and movement of storage. Service elements have been located furthest from the street, while the higher engaged program elements such as the office have been located at the street frontage. These moves seek to allow for pedestrian engagement and opportunities for locating pedestrian friendly functions such as canopies, enlarged walks or plazas, sidewalks and parking at the frontage. These gestures further support the development objectives of maintaining the Queen Anne Green Belt and natural buffer between the site and residential neighborhood above.

DC4. Exterior Elements and Finishes

Use appropriate and high quality elements and finishes for the building and its open spaces. The building hosts a selection of material products that are compatible with a rain screen envelope system. These materials are intended to be highly durable and low maintenance. Signage will be an integral inclusion in the design and play a strong roll in branding the development.

4.0 SITE ANALYSIS

ZONING DATA

Zoning Designation: Industrial Commercial-45 (IC-45)
Zoning Overlays: None
Urban Village Overlay: Ballard-Interbay-Northend Manufacturing Industrial

SEATTLE MUNICIPAL CODE

Development Standards:
The standards for development within the IC zone are outlined in Seattle Municipal Code (SMC) Chapter 23.50.

Structure Height:
Within the IC-45 zone, structure heights are limited to 45 feet. Extra FAR and/or FAR bonuses are not available in the IC zones.

Floor Area Limits:
Per SMC Section 23.50.028 – Floor area limits, the base and maximum floor area ratio (FAR) for the site is 2.5.

Setback Requirements:
Assuming that the space between the subject site and the residentially-zoned lots to the east would be considered an alley, a ten-foot setback would be required on the east side of the site, and half of the alley width may be counted as part of the required setback, per SMC Section 23.50.32 – Industrial Commercial—Setback requirements.

Screening and Landscaping:
Per SMC 23.50.038 – Industrial Commercial – Screening and landscaping, because the proposed development will create more than 4,000 new square feet of a non-residential use, the required landscaping must achieve a “Green Factor” score of 0.30 or greater (such landscaping includes large shrubs or other perennials and trees, as well as green roofs and high soil depths). Street trees must also be provided. Blank segments of facades that are 60 feet wide and greater, and within 20 feet of the street lot line, must be screened with a hedge, trellis, and/or vining plant, or other landscaping. Screening is required in all surface parking areas; in surface parking areas for more than 50 cars, interior landscaping is also required, and view obscuring screening may be required in surface parking areas adjacent to residential zones. Outdoor storage and outdoor loading berths adjacent to residential zones must also be screened with view obscuring screening.

Light and Glare Standards:
Per SMC Section 23.50.046 – Industrial Buffer and Industrial Commercial—Light and glare standards, exterior lighting must be shielded and directed away from lots in adjacent residential zones, and additional requirements (possibly including a glare diagram) may apply if the proposed development includes a significant percentage of highly reflective façade material.

PART II: SITE DEVELOPMENT INFO

1.

Please describe the existing site, including location, existing uses and/or structures, topographical or other physical features, etc.

***Project Response:** The existing project site consists of four (4) separate tax parcels totaling 2.016 acres. The majority of the site is generally flat, with the exception of the northeast corner, which features approximately 40 percent slopes that rise approximately 25 feet in elevation to the eastern property line. The project area is located on the east side of 15th Ave W between W. Garfield Street to W. Blaine Street. Existing structures on site consists of a two story wood frame self-storage facility, a two story CMU block building that contains a distillery and a furniture consignment store, and a single story CMU building with miscellaneous office users and a ceramic art gallery.*
2.

Please indicate the site’s zoning and any other overlay designations, including applicable Neighborhood Specific Guidelines.

***Project Response:** The site is zoned Industrial Commercial – 45 (IC-45). No zoning overlay or Neighborhood Specific Guidelines apply for the subject site. The Urban Village Overlay is Ballard-Interbay-Northend (Manufacturing Industrial).*
3.

Please describe neighborhood development and uses, including adjacent zoning, physical features, existing architectural and siting patterns, views, community landmarks, etc.

***Project Response:** The surrounding neighborhood development and uses consist of early/mid-century low-rise structures and newer commercial buildings and uses. Existing uses in the vicinity include a Brown Bear car wash to the North (IC-45), work loft units and vacant property to the West (IG2 U/45), building supply retailer to the South (IC-45) and unimproved right of way (14th Ave W) and vacant single family to the East (SR 9600). The subject site is separated from the SR 9600 zone by a significant topographical slope. A majority of the existing and recently constructed buildings are sited in a North/South orientation with frontage toward 15th Ave W, as 15th Ave W is a main North/South arterial that connects the Magnolia and Ballard neighborhoods with downtown Seattle and vice versa. There are territorial views from the subject property, views from the SR 9600 zone east of the site will not be impacted by this development as the maximum building height is 45 feet. The elevation of the existing homes in the SR 9600 zone is significantly higher (approximately 200 feet). The average elevation of the subject site where the new proposed buildings will be sited is 18 feet.*
4.

Please describe the applicant’s development objectives, indicating types of desired uses, structure height (approx), number of residential units (approx), amount of commercial square footage (approx), and number of parking stalls (approx). Please also include potential requests for departure from development standards.

***Project Response:** The proposed use will be a self-storage facility, the current proposal consist of two separate buildings known as building A (South end) and building B (North end). Each building will have one story below grade. For building B parking will be provided below grade and building A will have one level of below grade self-storage. Building A is proposed with five stories of self-storage units, approximately 102,331 gross square footage of unit storage. Building B is proposed with three stories of self-storage units, approximately 33,150 gross square footage of unit storage. There will be 70 parking stalls located on the first level and below grade in building B, and 16 surface parking stalls between buildings A and B to support the self-storage use.*



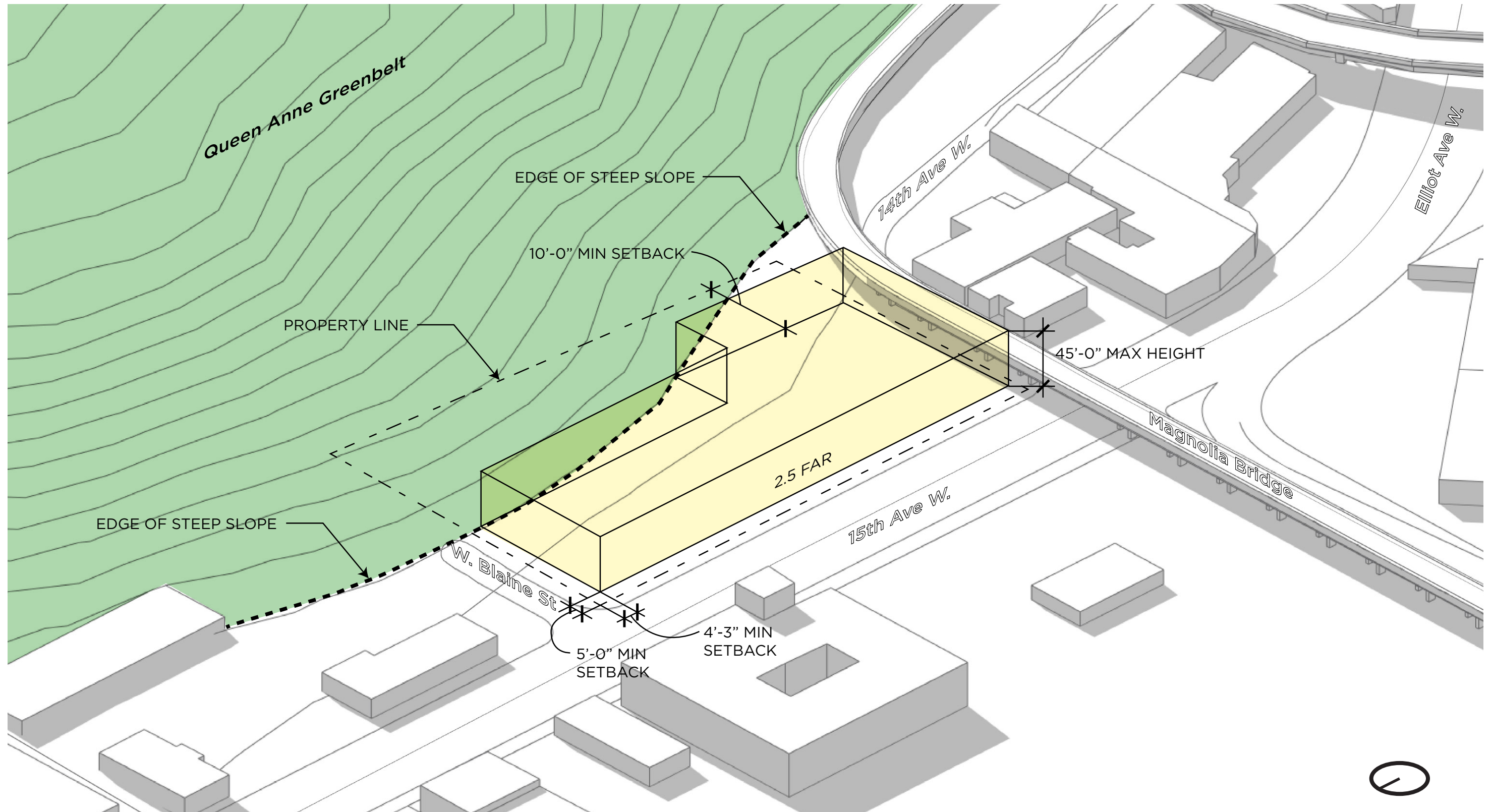
① View from above, looking west, down the slope adjacent to the site.
*site/project not visible from residential zone

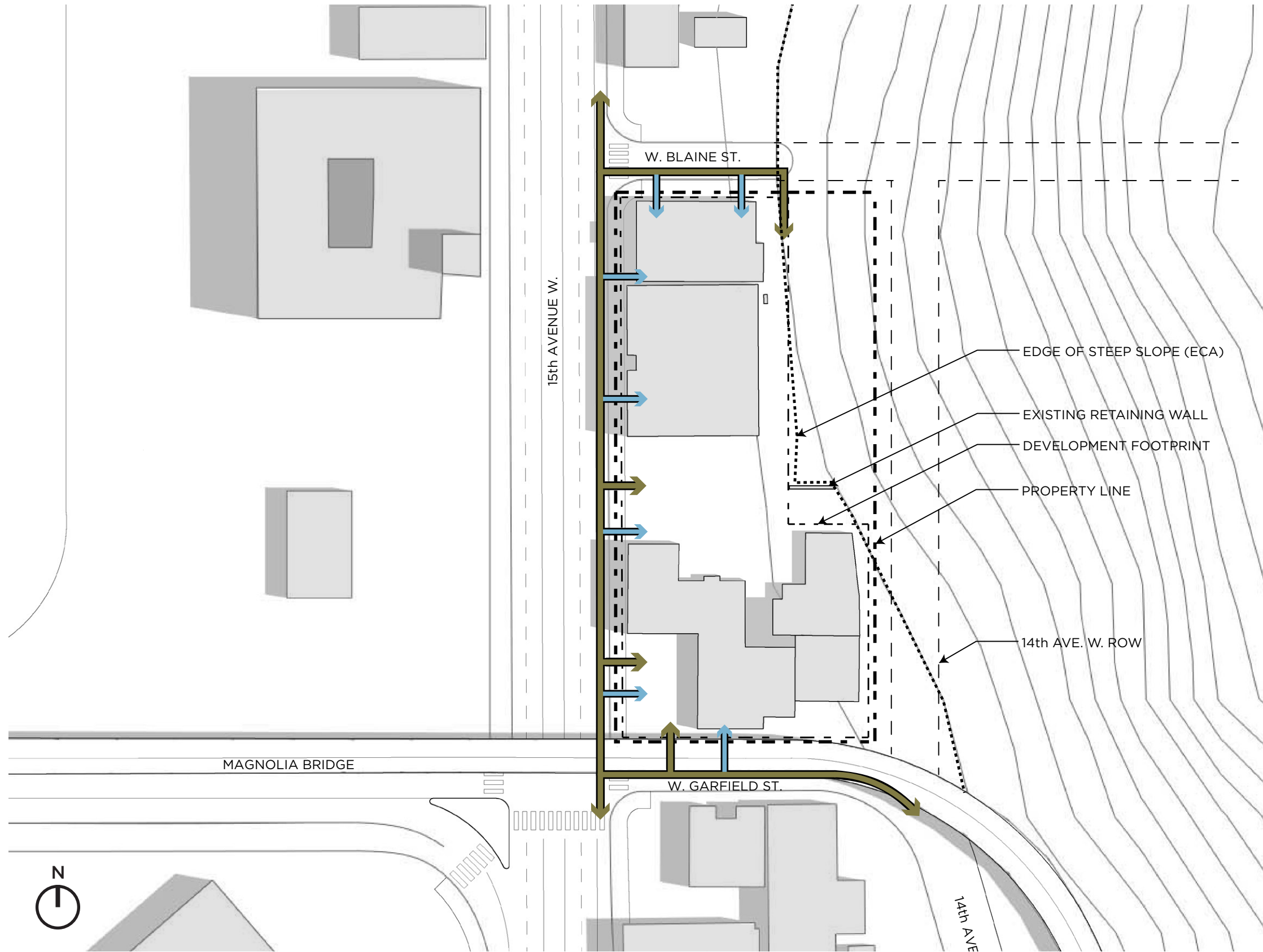
② View from above, looking south, down the slope adjacent to the site.
*site/project not visible from residential zone



Aerial Site Plan

4.0 SITE ANALYSIS





ACCESS OPPORTUNITIES

MAP LEGEND

- Pedestrian Access
- Vehicular Access
- Edge of Steep Slope (ECA)

4.0 SITE ANALYSIS



Existing South-building, South elevation



Existing South-building, North elevation



Existing North and middle-buildings, West elevations



Existing middle-building, South elevation



Existing North-building, North elevation



Existing North and middle-buildings, East elevation (back alley)

Boundary & Topographic Survey

A portion of the SW 1/4 of the SW 1/4 of Section 24, Township 25 N., Range 3 E., W.M.
King County, Washington

Scale: 1" = 30'

14TH AVE W

15TH AVE W

W BLAINE ST

W GARFIELD ST

Parcel A

Parcel B

Parcel C

Parcel D

REPLAT OF BLOCKS 7 & 8, JAMES ADDITION AND BLOCK 132, SEATTLE TIDE LANDS

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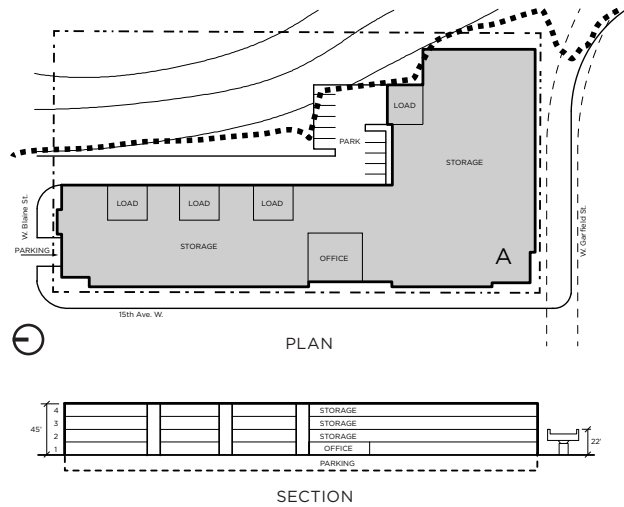
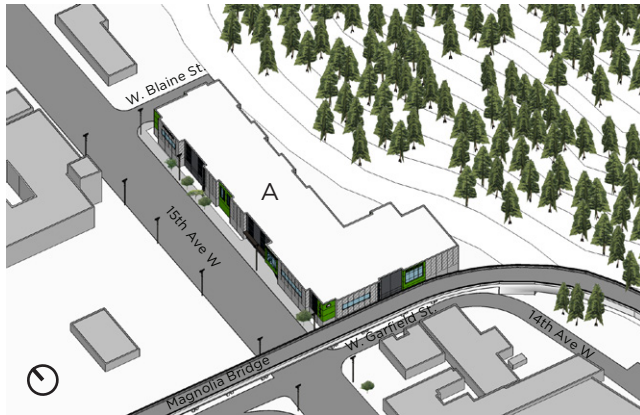
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There are no trees of significant size on the site. There are however street trees along 15th Ave W as follows - (1) 10" deciduous, (1) 12" deciduous, (1) 14" deciduous, (1) 16" deciduous, and (1) 18" deciduous. Along the eastern boundary of the building site resides the heavily wooded area known as Queen Anne Greenbelt.

| SHEET TITLE: | | REVISION | | DATE | | BY | |
|---|--|---|--|--|--|----|--|
| BOUNDARY & TOPOGRAPHIC SURVEY CONTOUR ENGINEERING, L.L.C. CIVIL ENGINEERS-SURVEYORS-LAND PLANNERS 1706 S. W. 10th St., Suite 1115 Ft. Lauderdale, FL 33316 Phone: (954) 561-5451 Fax: (954) 561-5454 Email: info@contoureng.com | | | | | | | |
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| DRAWN BY: LN SURVEYORS S. MOORE, PL CHECKED: SW SEC 24 T 25N R 3E W 1/4 DATE: SEP 23, 2015 PROJECT: 15-184 DWG NAME: 15-184B | |  | | CLIENT: BARTLEY HOLDINGS 1717 McKinney Ave., Suite 700 Dallas, TX 75202 CONTACT: Michael Chen | | | |
| SHEET SHT-NO | | REV. | | | | | |
| 1 OF 2 | | | | | | | |

5.0 ARCHITECTURAL CONCEPTS

CONCEPT 01



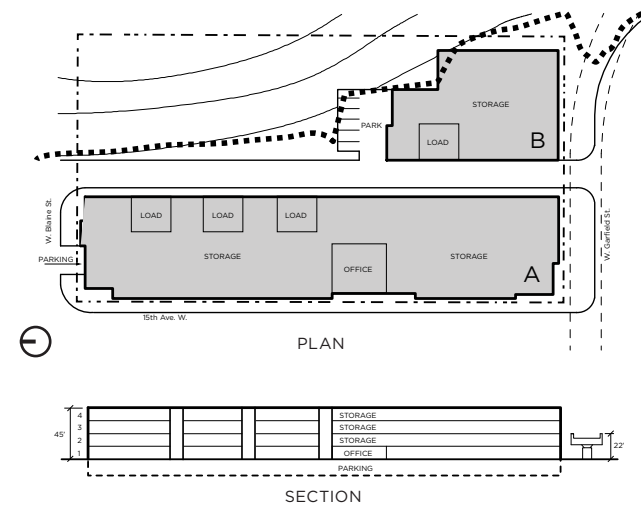
PROS

- Maximum FAR and developable storage space
- Allows for at grade temporary parking
- Least construction cost of all options

CONS

- Mass of building along 15th Ave. W and Garfield St. overpowers context
- Limited open space opportunities at frontage
- Access through rear alley difficult to see
- Access to below grade parking shared with loading access
- Limited *sense of entry* for pedestrians and motorists
- Building blocks views of greenbelt

CONCEPT 02



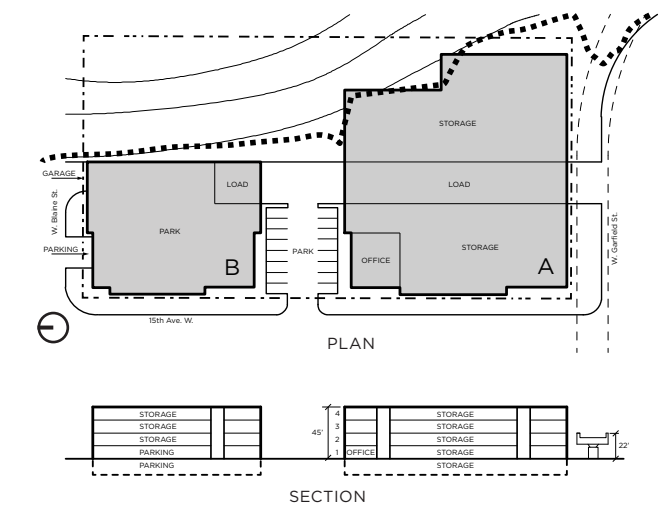
PROS

- Mass of building along Garfield St. is minimized
- Drive through alley reduced access congestion through site
- Near maximum FAR and developable storage space

CONS

- Mass of building along 15th Ave. W and Garfield St. overpowers context
- Higher construction costs than Concept 1
- Limited open space opportunities at frontage
- Access through rear alley difficult to see
- Limited *sense of entry* for pedestrians and motorists
- Building blocks views of greenbelt

CONCEPT 03 (preferred)



PROS

- Mass of buildings is reduced along 15th Ave. W. and fits with context
- Separation of building allows view of greenbelt
- Centralized surface parking and entry easily identifiable
- Reduced massing and separation allow for centralized open space
- Lobby located adjacent surface parking and frontage
- Loading function enclosed with thru access
- Temporary surface parking and below grade parking access separated

CONS

- Least FAR and developable storage space
- Higher construction cost than Concept 1
- Building B nears bottom of slope to achieve developable storage space

5.0 CONCEPT 01



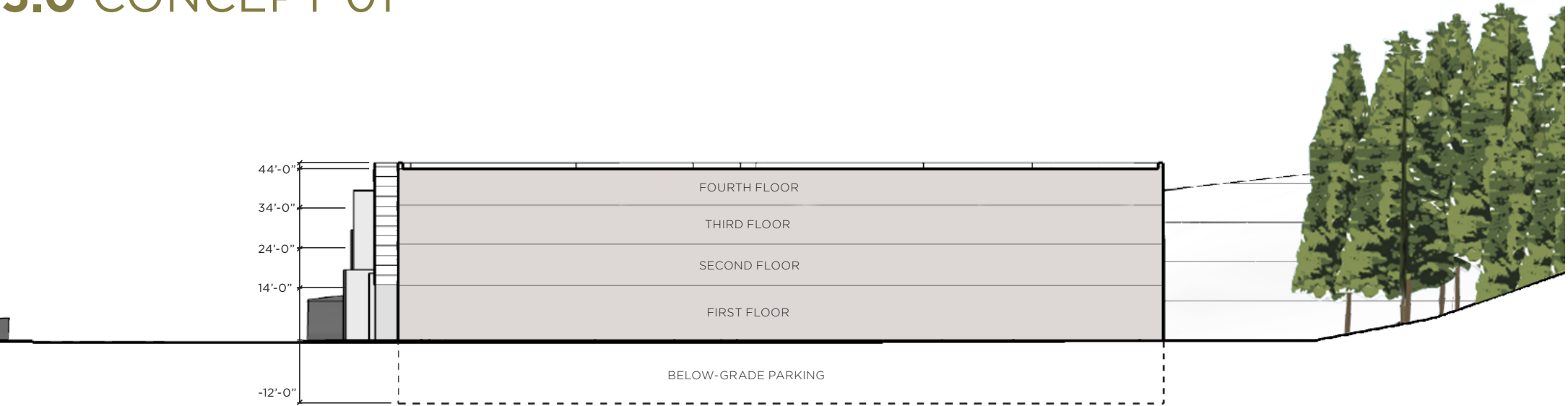
5.0 CONCEPT 01



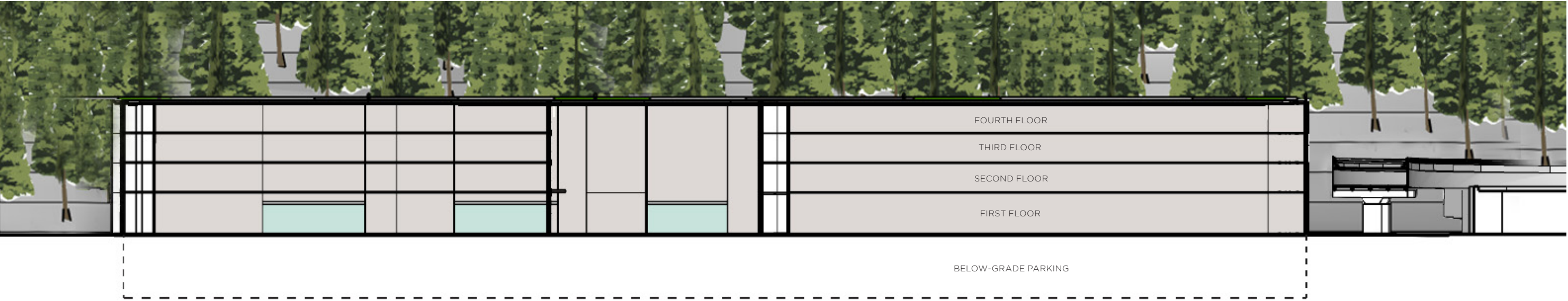
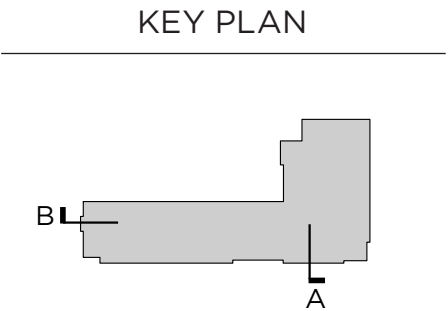
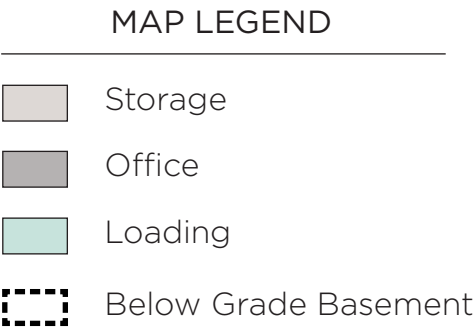
5.0 CONCEPT 01



5.0 CONCEPT 01



BUILDING SECTION A

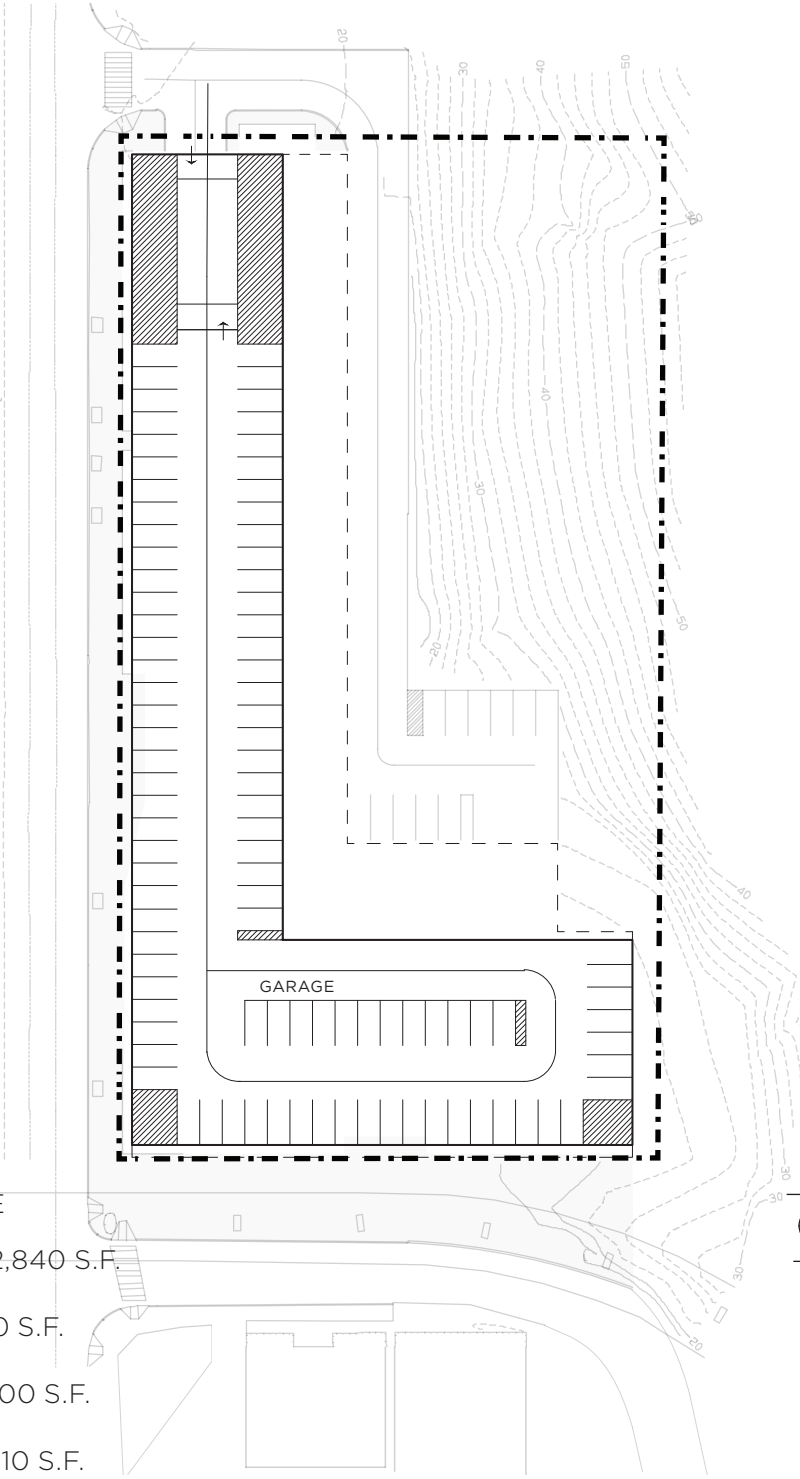


BUILDING SECTION B

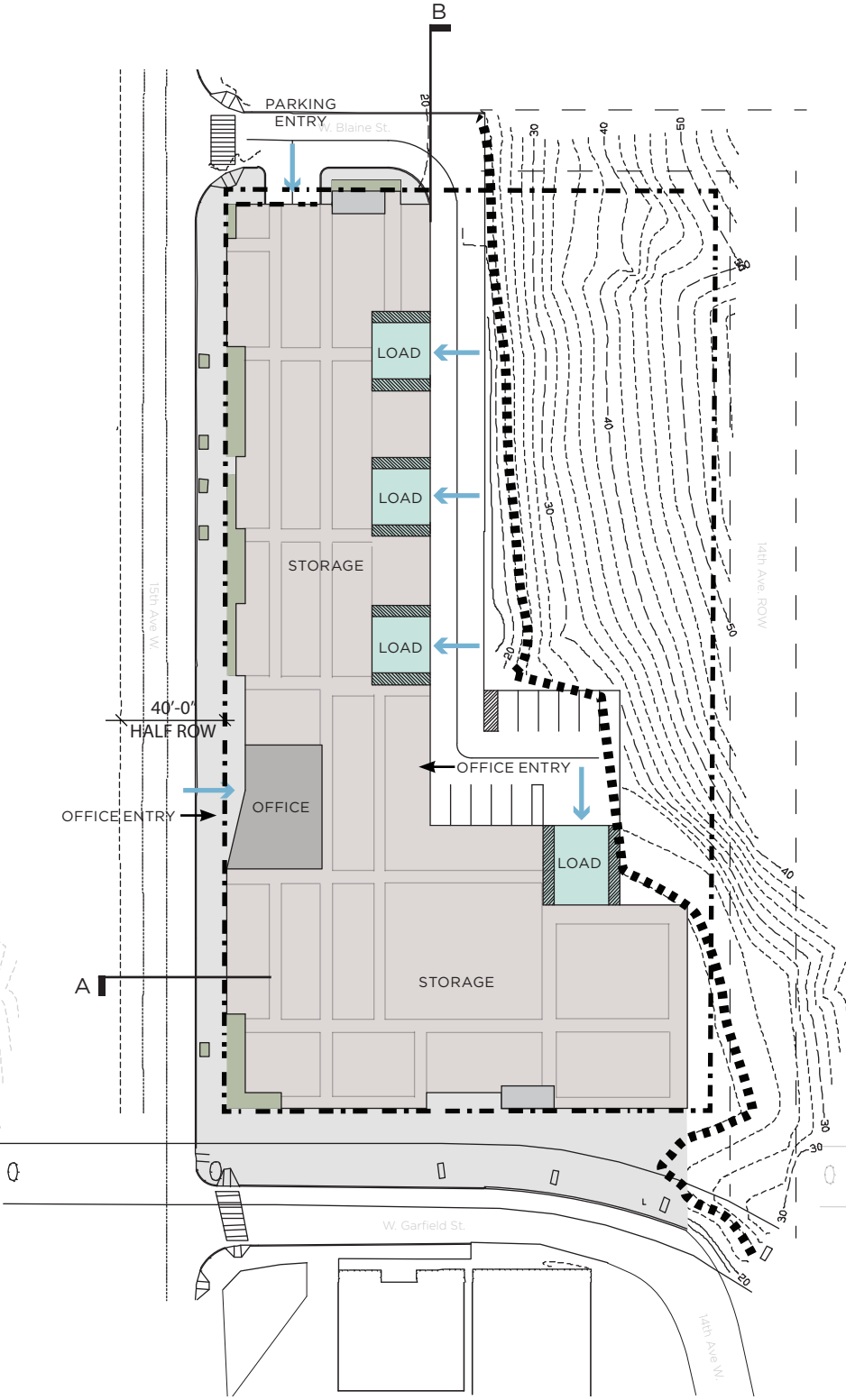
5.0 CONCEPT 01



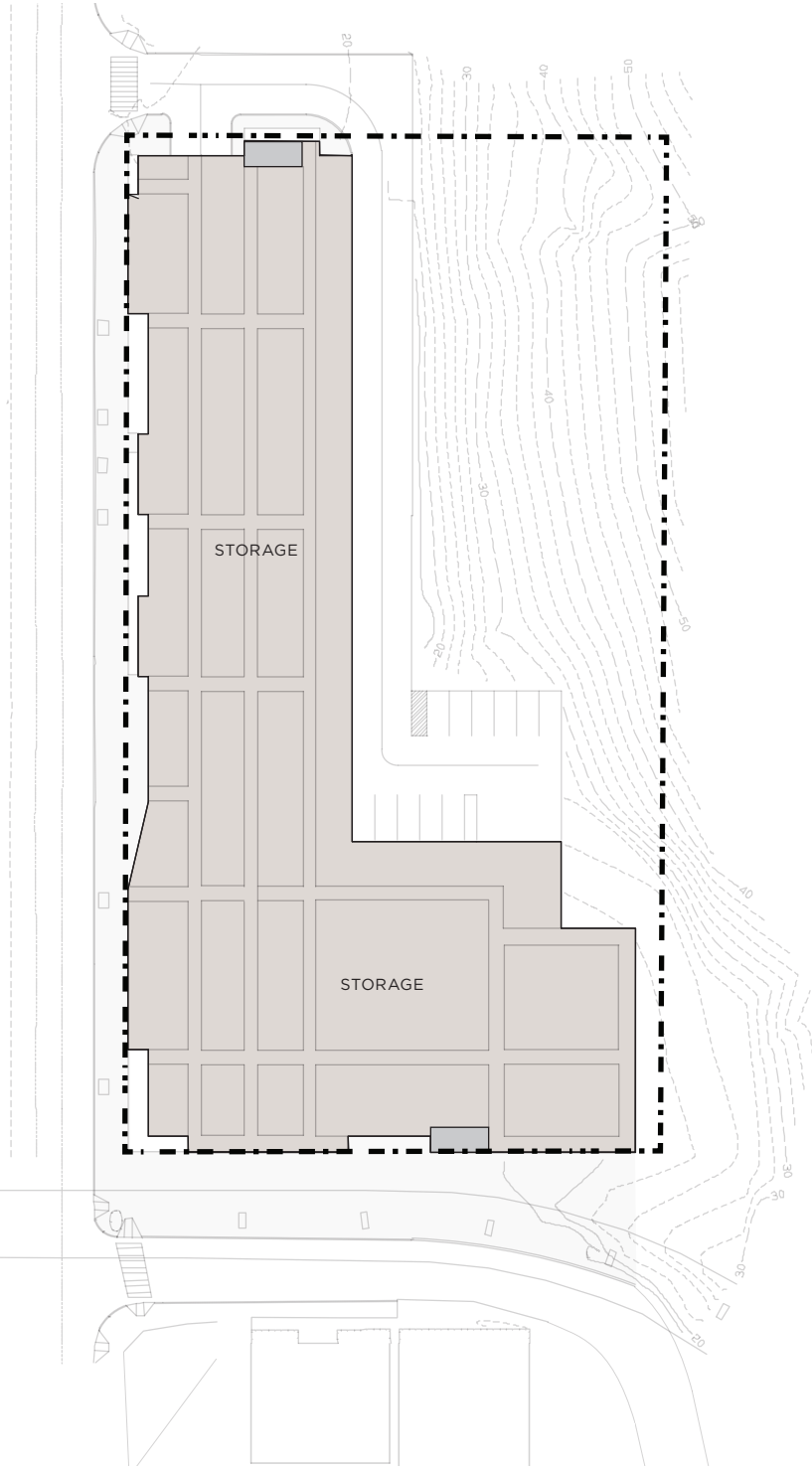
- ENTRY
- EDGE OF STEEP SLOPE
- STORAGE 182,840 S.F.
- OFFICE 2,000 S.F.
- LOADING 3,900 S.F.
- PARKING 39,110 S.F.
SURFACE SPACES: 11
GARAGE SPACES: 96



BASEMENT FLOOR



FIRST FLOOR



2ND-4TH FLOORS



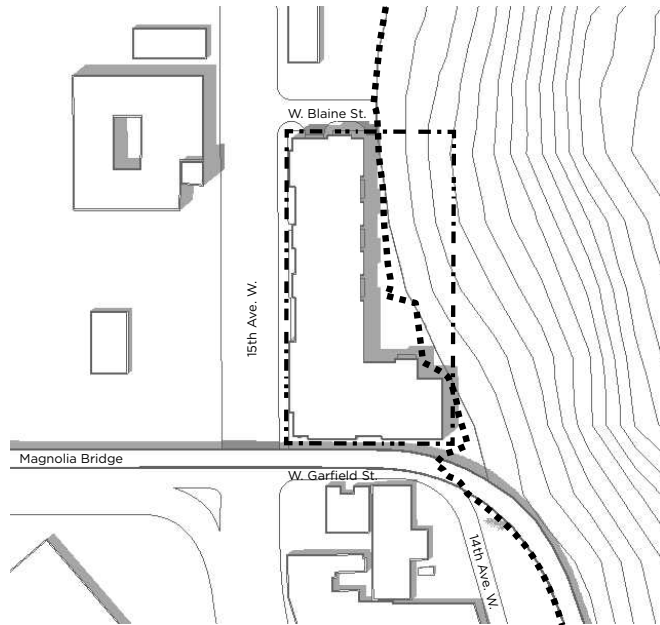
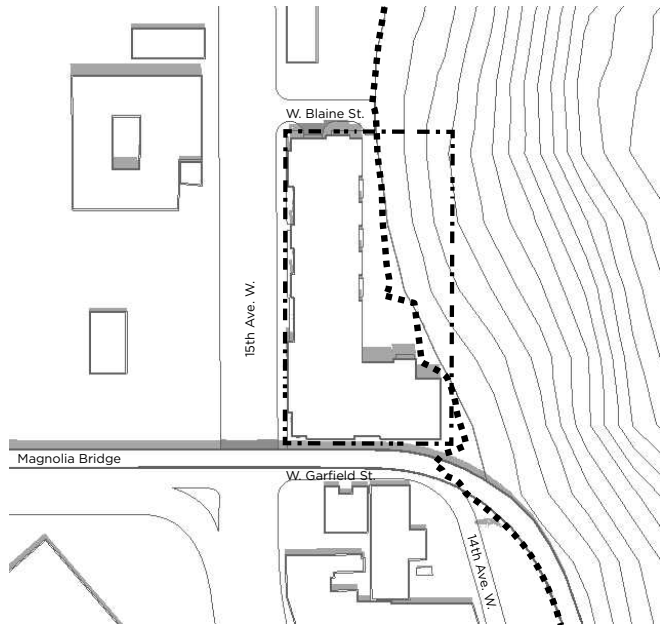
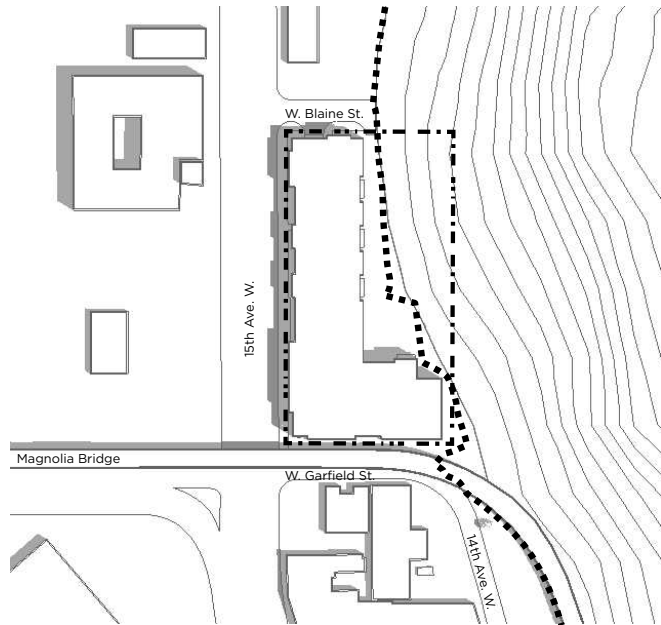
5.0 CONCEPT 01

SUMMER
SOLSTICE

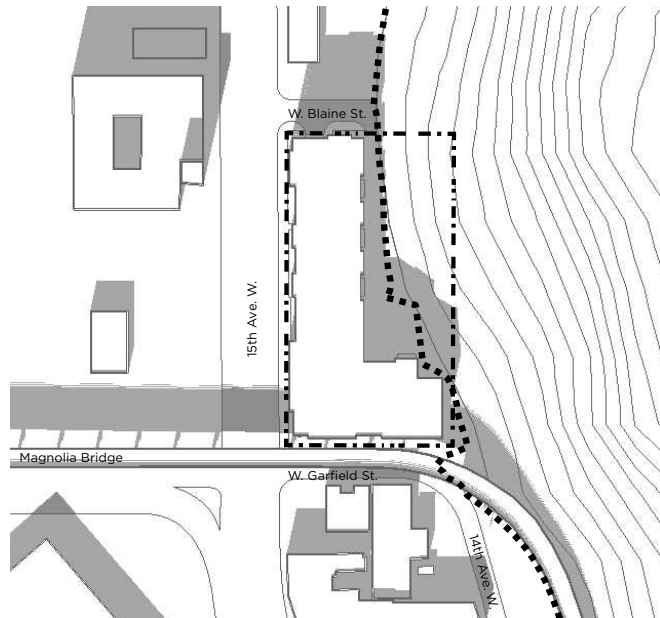
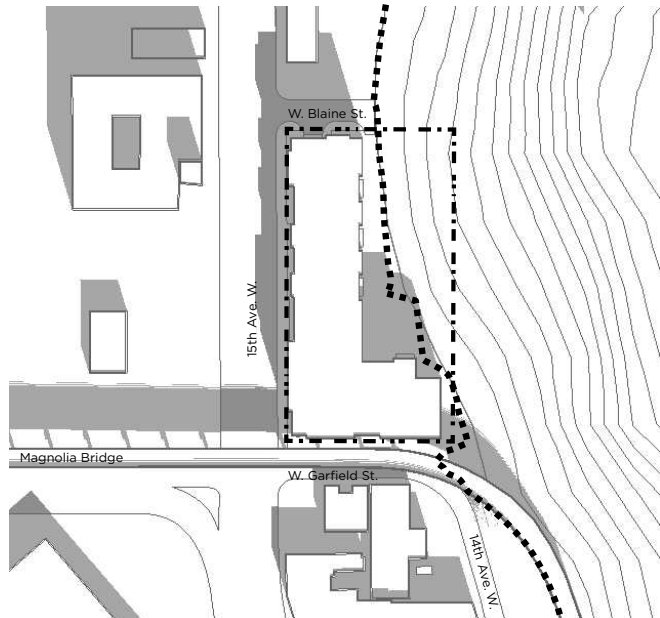
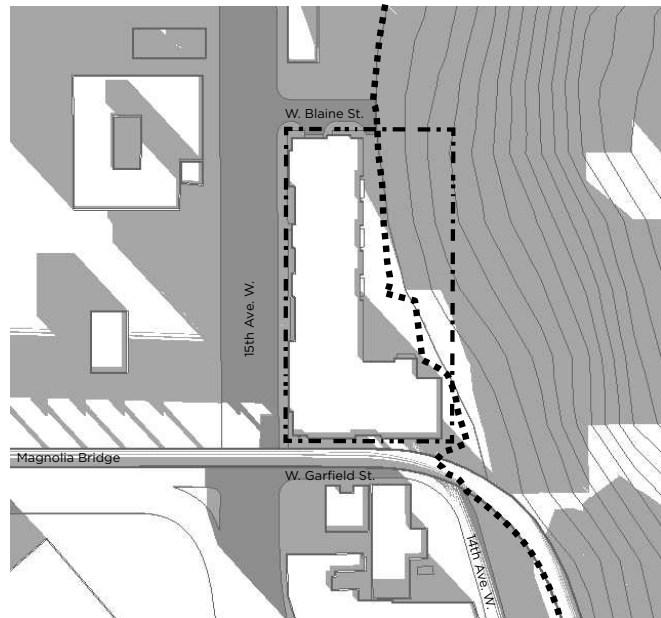
10:00 AM

12:00 PM

2:00 PM



WINTER
SOLSTICE



SUN & SHADOW STUDIES

The shadows for Concept 01 have no negative impacts on surrounding developments or uses.

Note: The shadow studies assume 45'-0" tall structures for both Building A and B - in compliance with Seattle Land-Use code.

- PROPERTY LINE
- EDGE OF STEEP SLOPE

5.0 CONCEPT 02



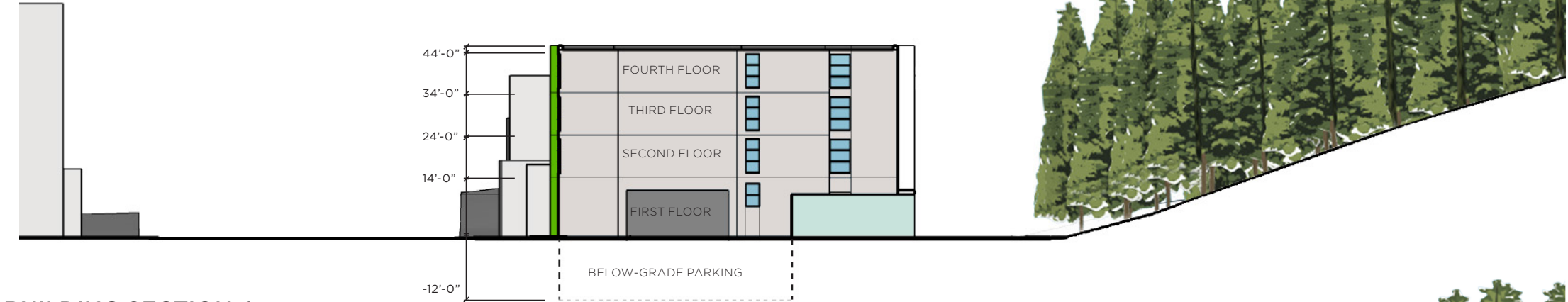
5.0 CONCEPT 02



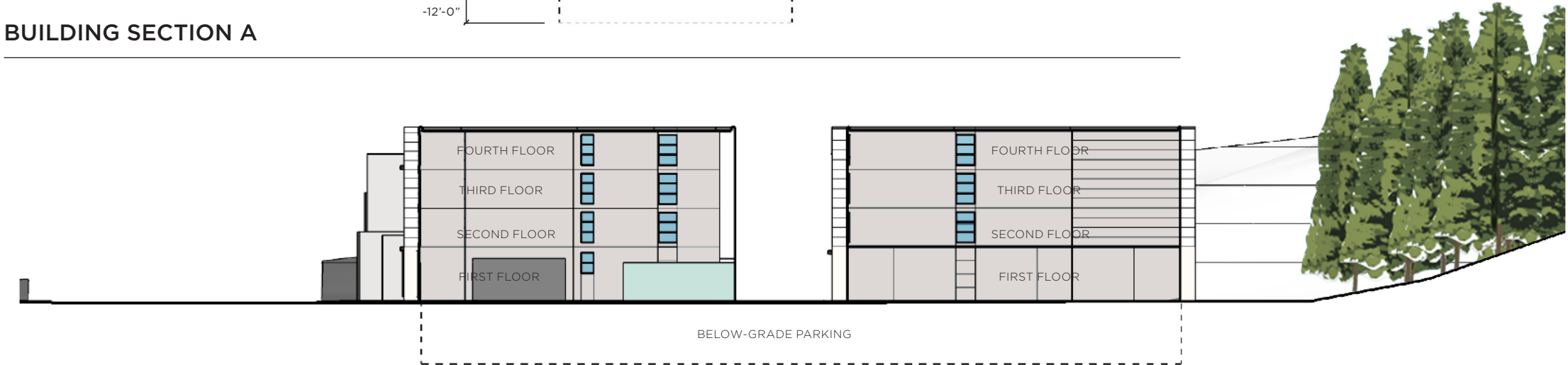
5.0 CONCEPT 02



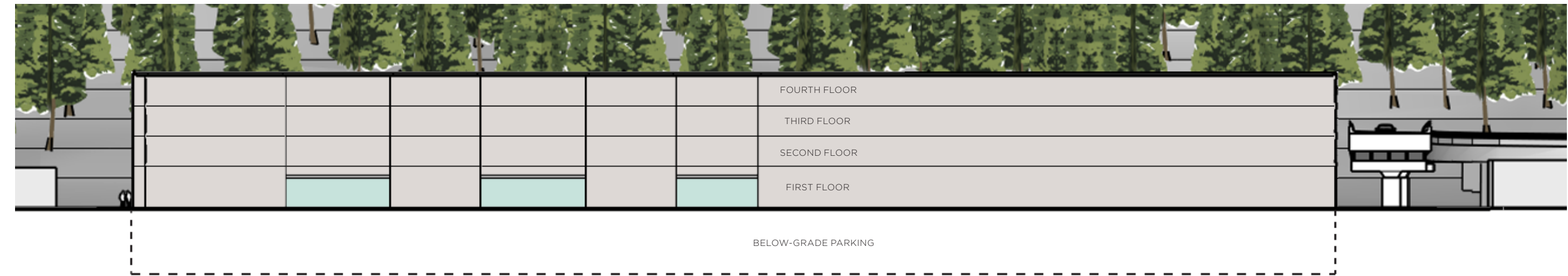
5.0 CONCEPT 02



BUILDING SECTION A



BUILDING SECTION B

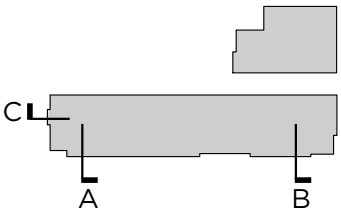


BUILDING SECTION C

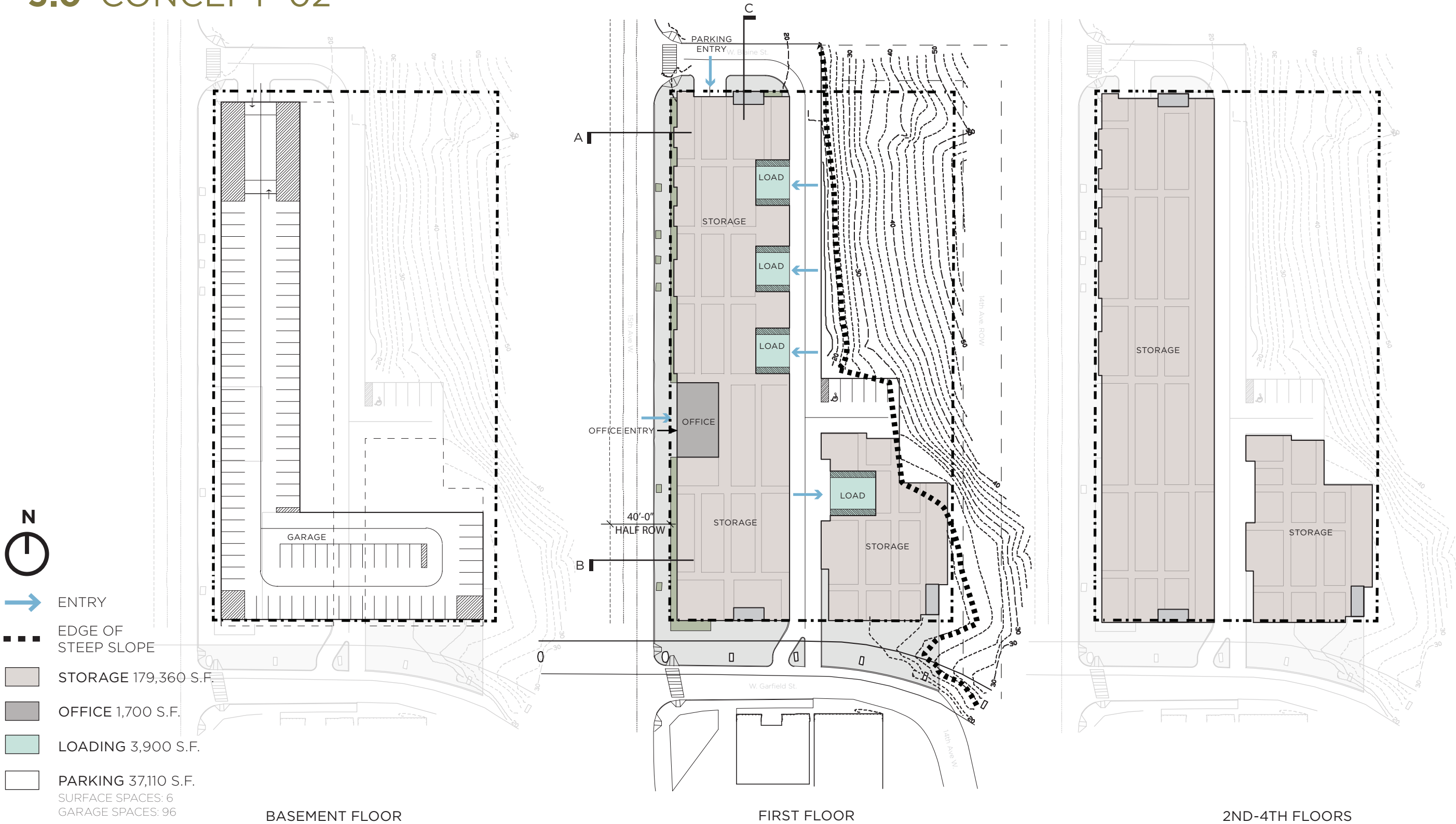
MAP LEGEND

- Storage
- Office
- Loading
- Below Grade Basement

KEY PLAN



5.0 CONCEPT 02



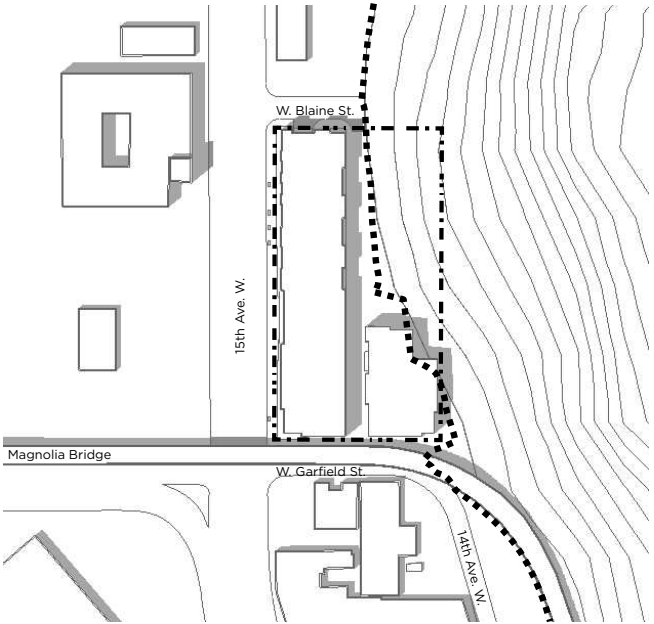
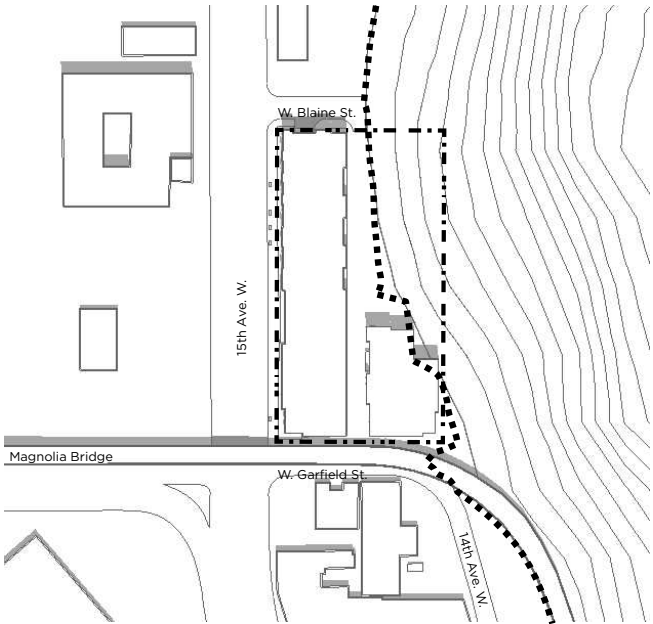
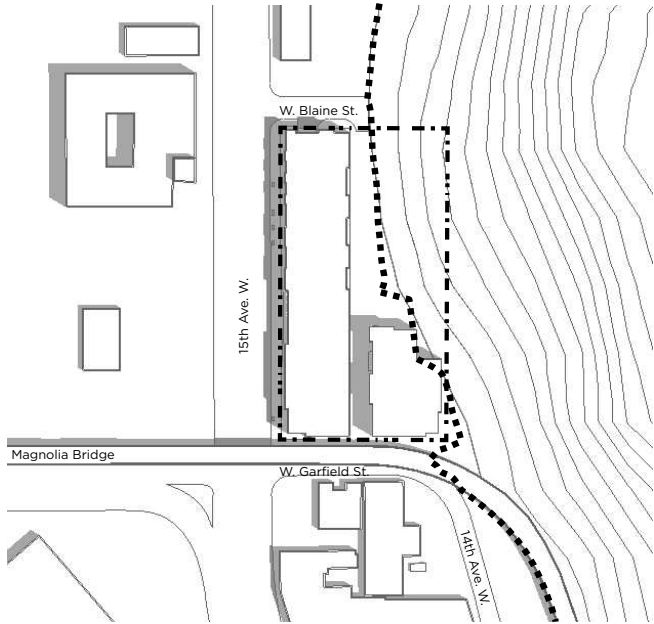
5.0 CONCEPT 02

SUMMER
SOLSTICE

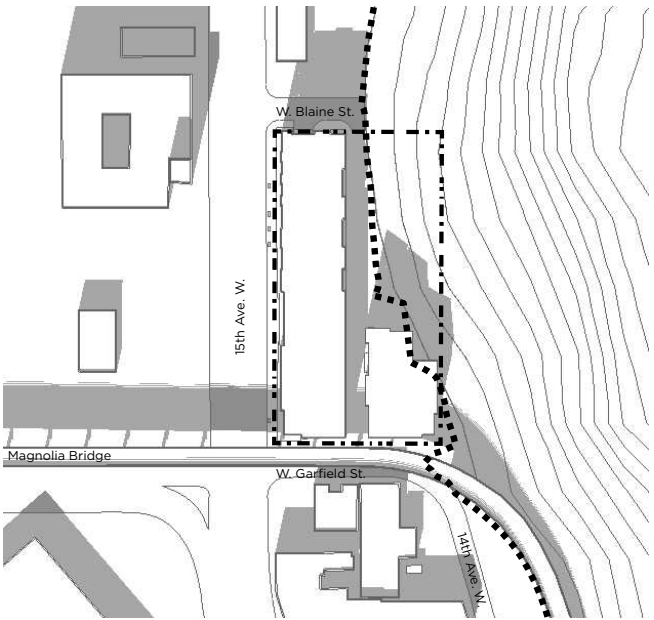
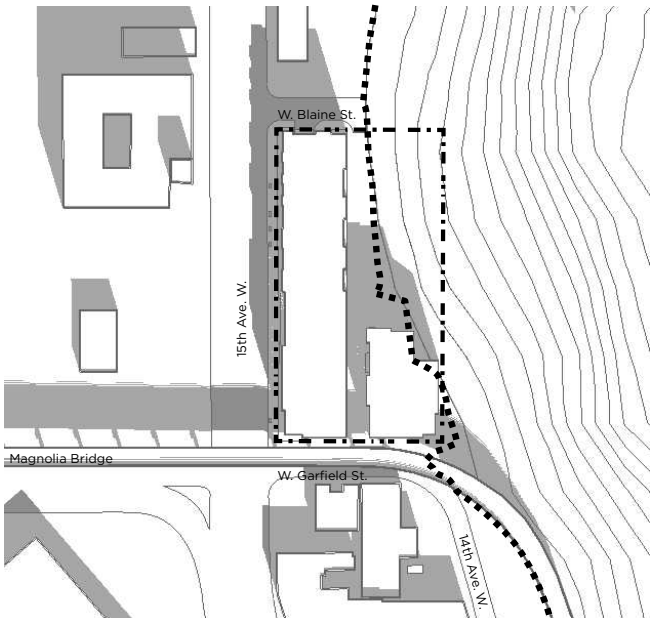
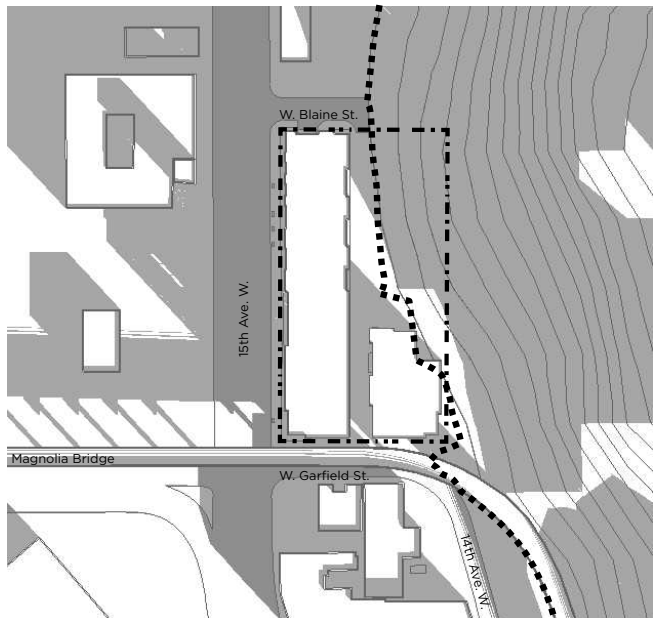
10:00 AM

12:00 PM

2:00 PM



WINTER
SOLSTICE



SUN & SHADOW STUDIES

The shadows for Concept 02 have no negative impacts on surrounding developments or uses.

Note: The shadow studies assume 45'-0" tall structures for both Building A and B - in compliance with Seattle Land-Use code.

- PROPERTY LINE
- EDGE OF STEEP SLOPE

5.0 CONCEPT 03 (PREFERRED)



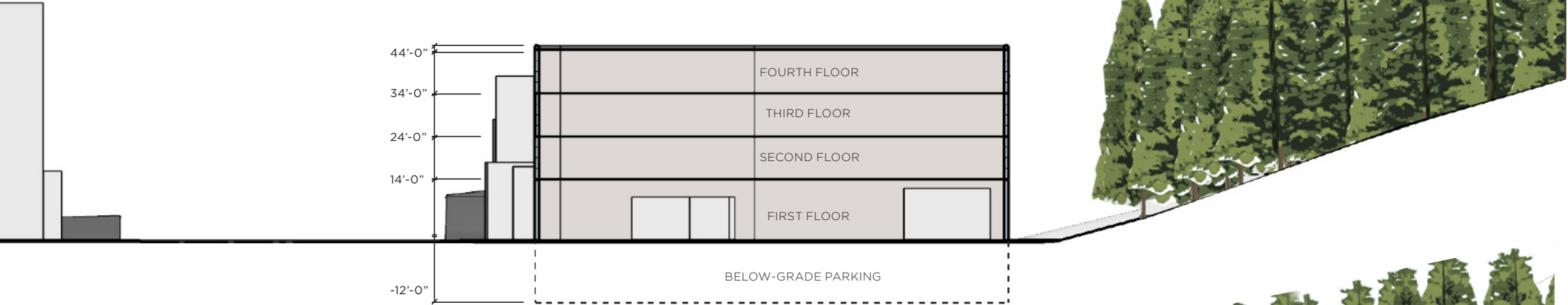
5.0 CONCEPT 03 (PREFERRED)



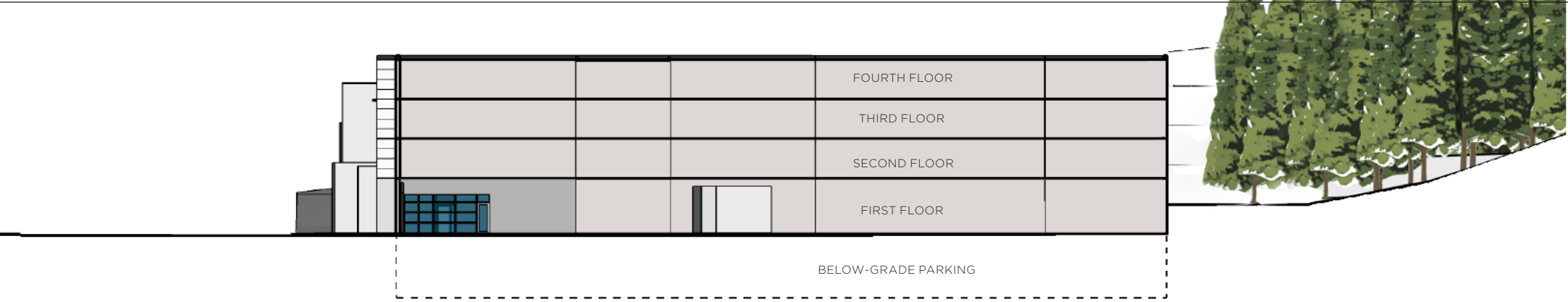
5.0 CONCEPT 03 (PREFERRED)



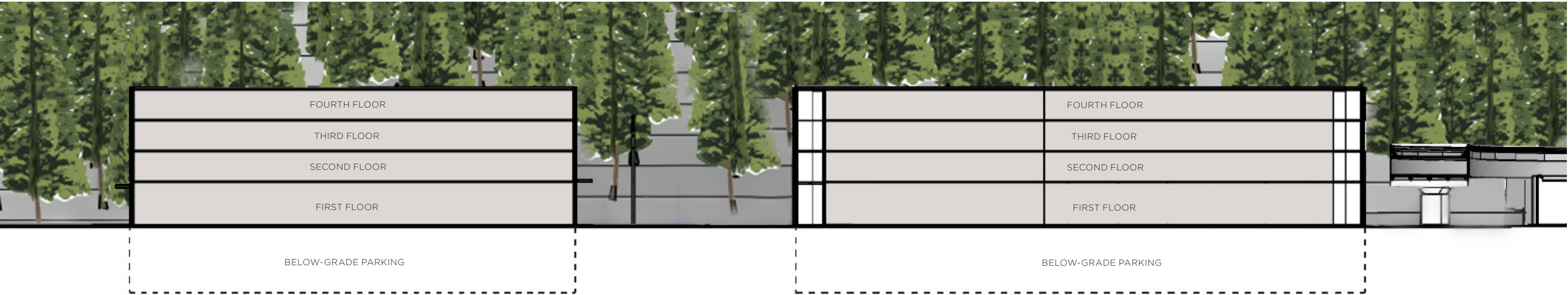
5.0 CONCEPT 03 (PREFERRED)



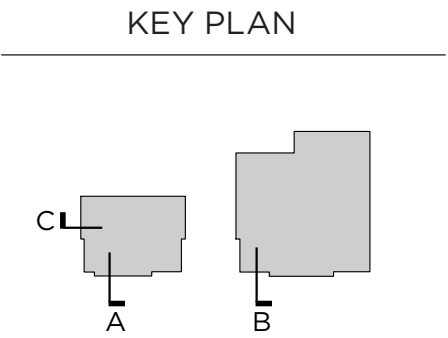
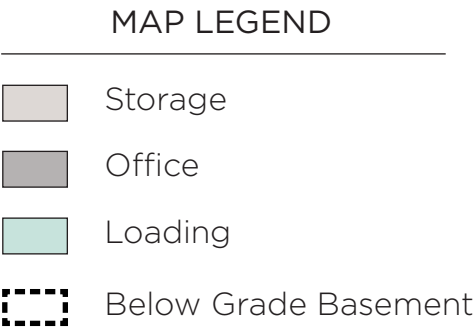
BUILDING SECTION A



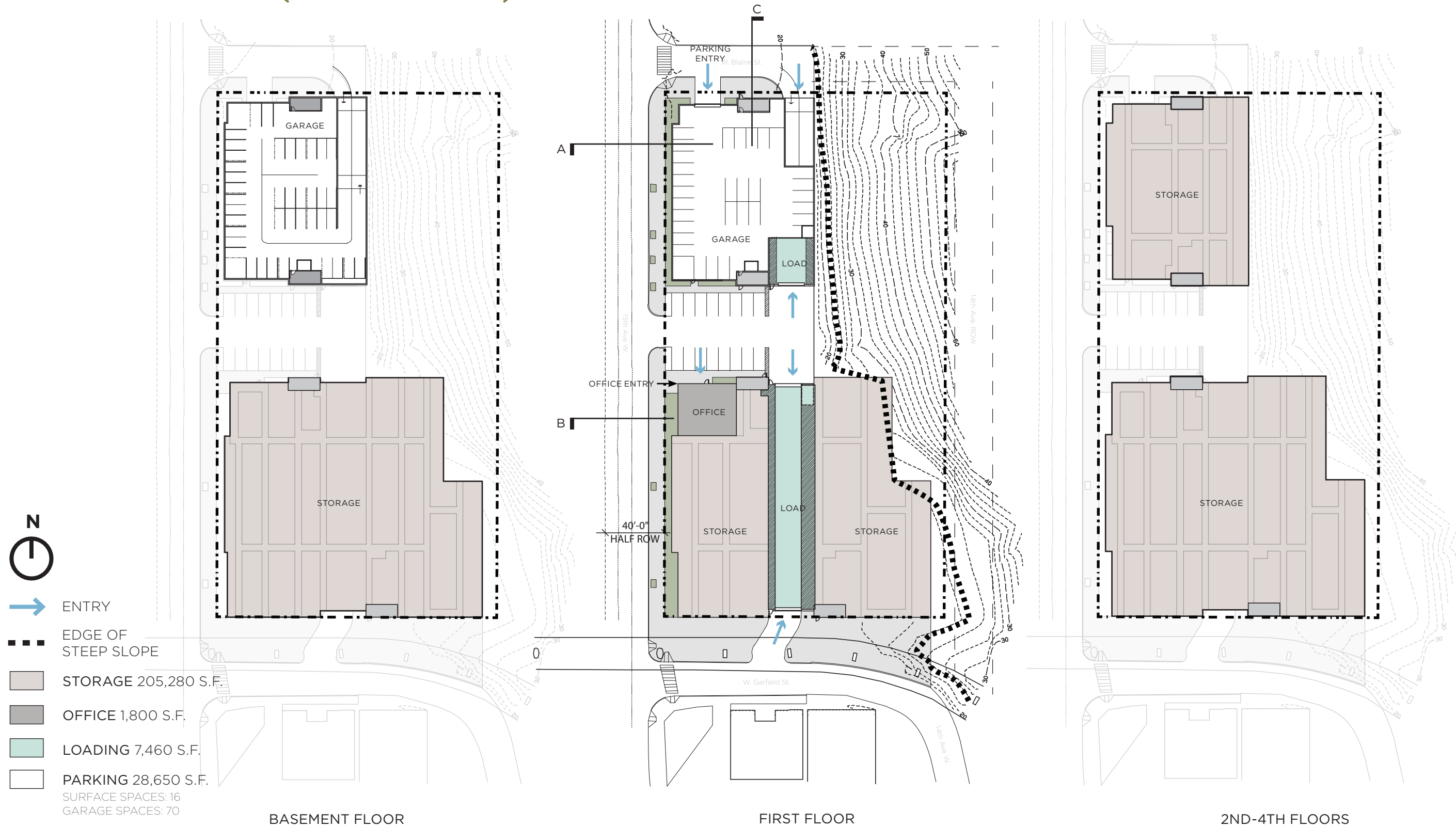
BUILDING SECTION B



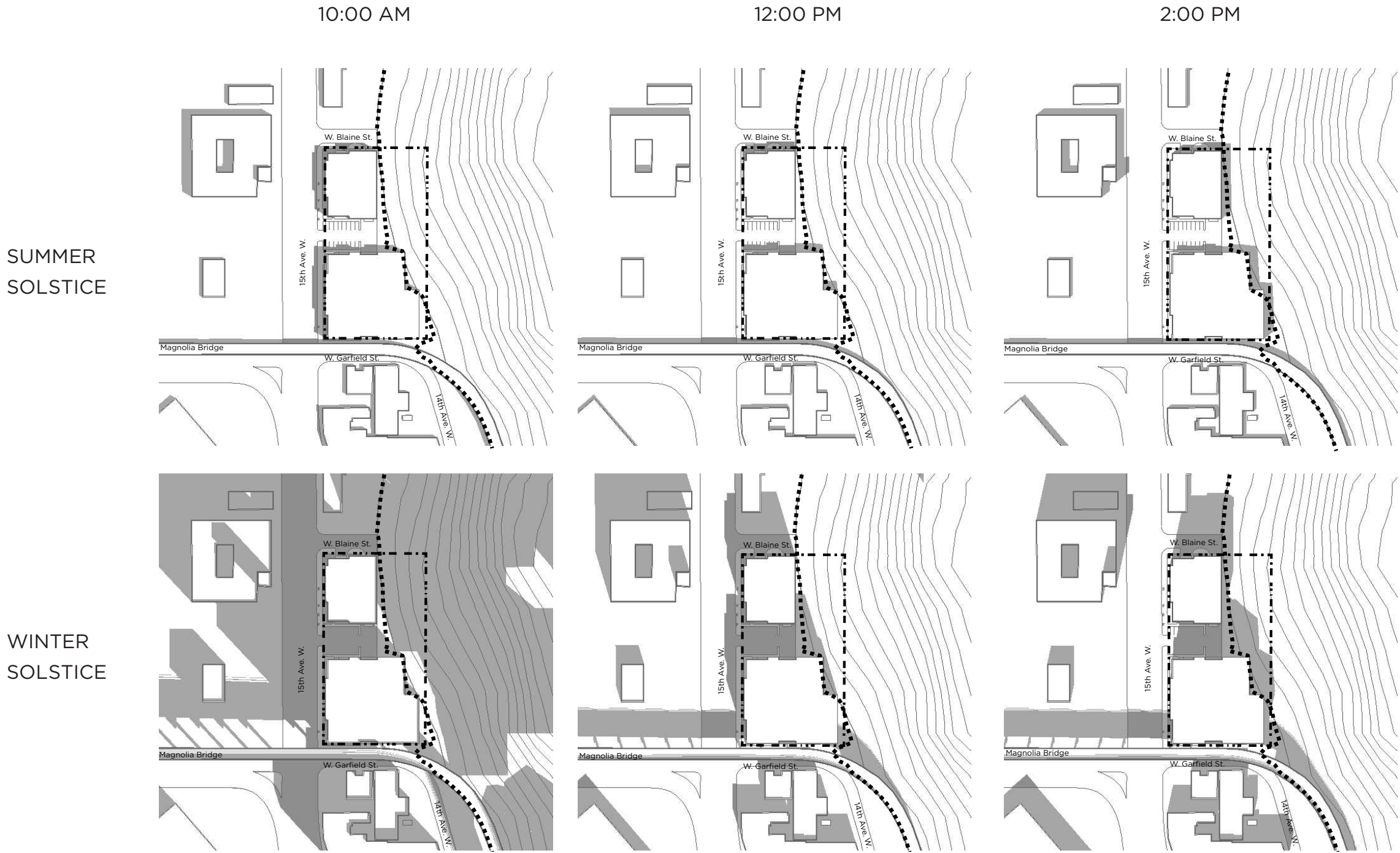
BUILDING SECTION C



5.0 CONCEPT 03 (PREFERRED)



5.0 CONCEPT 03 (PREFERRED)



SUN & SHADOW STUDIES

The shadows for Concept 03 have no negative impacts on surrounding developments or uses.

Note: The shadow studies assume 45'-0" tall structures for both Building A and B - in compliance with Seattle Land-Use code.

- PROPERTY LINE
- EDGE OF STEEP SLOPE

6.0 RELATIVE PROJECT EXPERIENCE







For more information please contact:

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