

2 AND U

1201 2nd Avenue - #3019177

2/3 Alley Vacation

Recommendations Meeting / Downtown Design Review Board / 04.05.2016

SKANSKA / **PICKARD CHILTON** / **SWIFT** COMPANY ^{LLC} / **GRAHAM BABA** ARCHITECTS

TABLE OF CONTENTS |

1	Site Context
4	Project Summary
5	Context Aerial
6	9-Block Radius
2	Early Design Guidance
8	Downtown DRB Design Guidance
3	Ground Plane
10	Site Plan
11	1st Avenue
12	Alley Plan
13	Terrace Level / 2nd Avenue Plan
14	Open Space - Materials
16	Open Space - Planting
18	Corner Plaza
20	Southeast Stoop
22	Central Plaza
24	Overlook Plaza
26	Southwest Corner
28	Mid-Block Stair
4	Podium, Plans + Elevations
32	University Street
33	1st Avenue
34	Seneca Street
35	2nd Avenue
36	Overall Site Plan
37	Level 1 Plan: 1st Avenue
38	Level 2 Plan: Central Plaza
39	Level 3 Plan: 2nd Avenue Lobby / Overlook Terrace
40	Level 4 Plan: Village Roofs
41	Site Sections
44	University Street - North Elevation
45	2nd Avenue - East Elevation
46	Seneca Street - South Elevation
47	1st Avenue - West Elevation
48	University Street - North Elevation
49	2nd Avenue - East Elevation
50	Seneca Street - South Elevation
51	1st Avenue - West Elevation
52	Soffit + Columns
55	Direct Solar Exposure Studies
56	Materials

5	Level 19
58	Site Plan
59	Materials + Planting
6	Tower / Rooftop
62	Scheme Development - EDG 2
63	Scheme Development - Current
64	Office Level Plans
65	Level 19 + Level 20 Plans
66	Levels 21-36 Plan
67	Scheme Development - Street Level Views
70	Scheme Development - Aerial Views
72	Scheme Development
7	Lighting
76	Plaza + Architectural Lighting
77	Site Fixtures + Family, Arts + Culture Spaces
78	Level 19
8	Proposed Departures
81	Proposed Departure #1 - Open Space Standards
82	Proposed Departure #2 - Overhead Weather Protection
86	Proposed Departure #3 - Minimum Facade Height
88	Proposed Departure #4 - Property Line Facades
90	Proposed Departure #5 - General Setbacks
94	Proposed Departure #6 - Blank Facade Limits
96	Proposed Departure #7 - Facade Transparency
97	Proposed Departure #8 - Facade Modulation
98	Proposed Departure #9 - Upper Level Width Limit
101	Proposed Departure #10 - Parking at Street Level
9	Appendices
ii	Background Information

PROJECT INFORMATION |

Property Address /	1201 2nd Avenue Seattle, WA 98101
DPD Project Number /	3019177
Owner / Applicant Name /	Samis Foundation Christian Gunter SCD 2U LLC 221 Yale Ave., Ste. 400 Seattle, WA 98109
Design Architect /	Pickard Chilton 980 Chapel Street New Haven, CT 06510 Nancy Clayton 203.786.8600 nclayton@pickardchilton.com
Architect /	Kendall / Heaton Associates Inc. 3050 Post Oak Boulevard, Suite 1000 Houston, TX 77056 Tom Milholland 713.877.1192 tmilholland@kendall-heaton.com
Landscape Architect /	Swift Company 3131 Western Avenue, Suite M423 Seattle, WA 98121 Barbara Swift 206.632.2038 barbara@swiftcompany.com
Retail Experience /	Graham Baba Architects 1507 Belmont Avenue, Suite 200 Seattle, WA 98122 Jim Graham 206.323.9932 jim@grahambaba.com



1

Site Context

OVERVIEW

Wide variety of privately owned, public open spaces and five pedestrian passages throughout the block, connecting 2nd Avenue, 1st Avenue, Seneca Street, and University Street.

- 48,500 SF Retail space (all levels)
- 671,000 SF of leasable Class A office space

476 parking spaces in below-grade garage. Garage and loading, access, and exit off Seneca Street. Site is bounded by 2nd Avenue, 1st Avenue, University Street, and Seneca Street.

DEVELOPMENT CONTEXT

Project includes the full eastern half of the block and 2/3 of western half of the block.

Diller Hotel is under separate ownership and will remain.

The southern 2/3rds of the public alley is petitioned to be vacated. Currently, the northern 1/3 of the alley remains a functioning public alley with a hammerhead turn-around easement provided as part of this project.

Eastern portion of site is zoned DOC1 U/450/U.

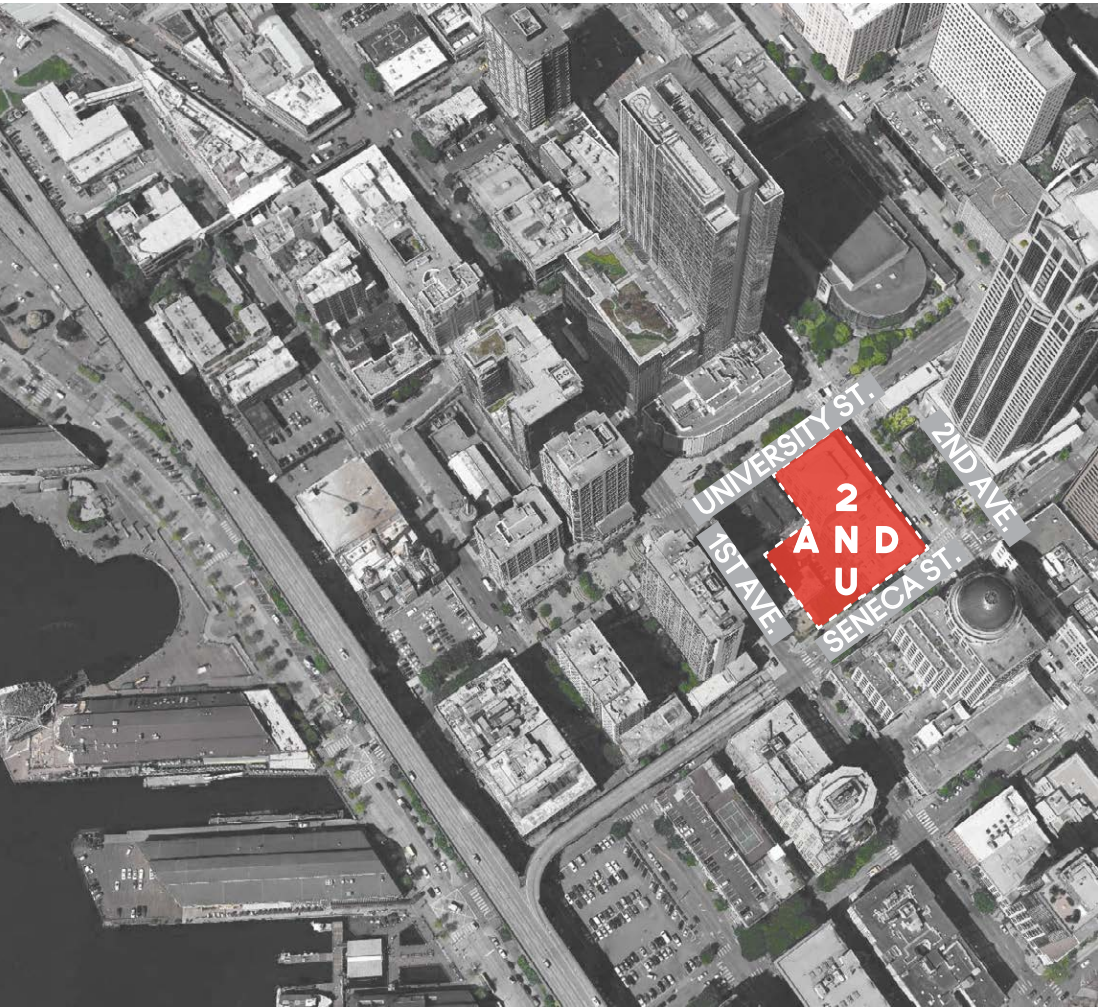
25,920 SF	property area, plus
1,280 SF	vacated alley,
27,200 SF	total eastern site area.
20	Max FAR
544,000 SF	resulting FAR.

Western portion of site is zoned DMC 240/290-400.

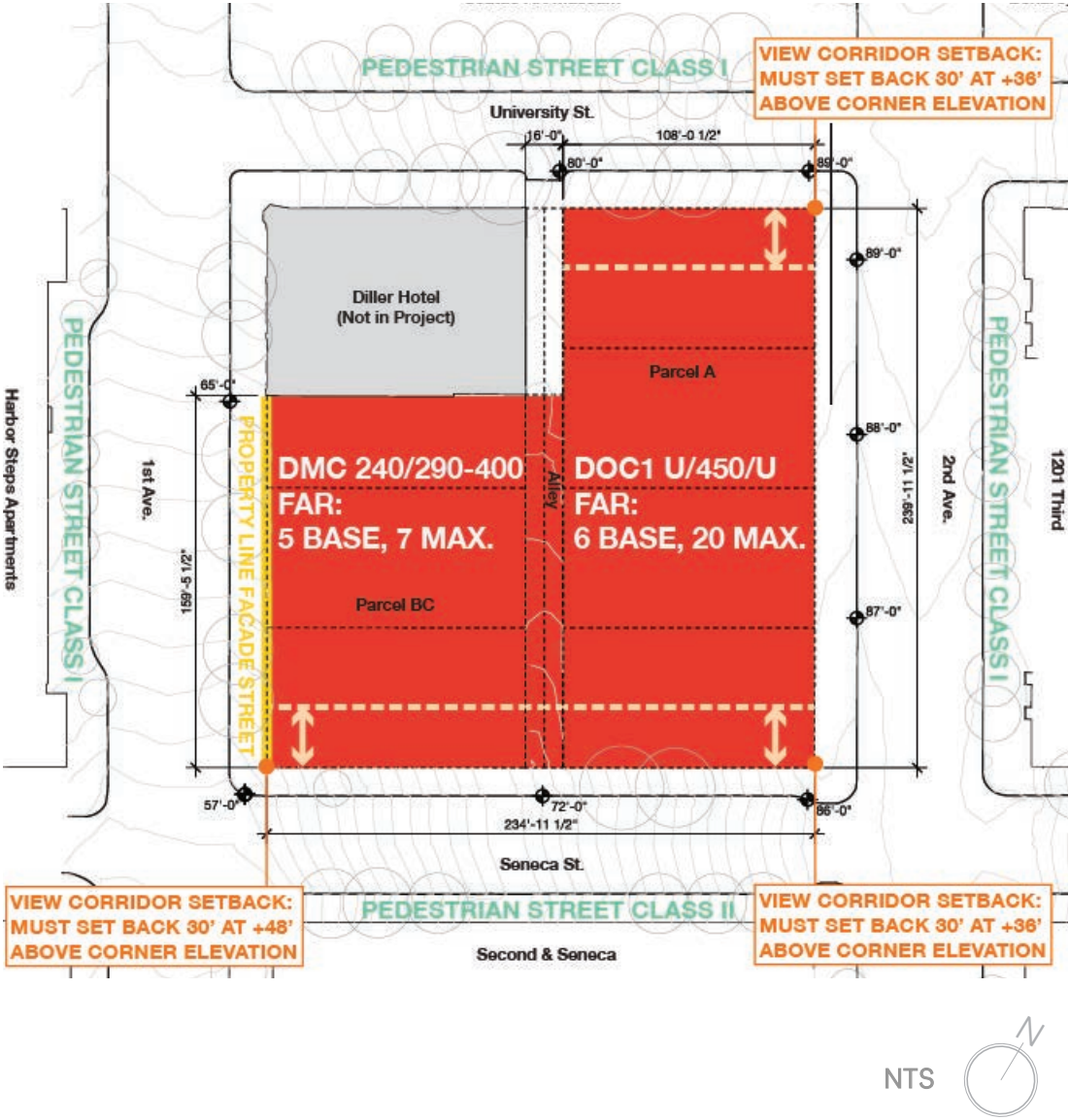
17,760 SF	property area, plus
1,280 SF	vacated alley,
19,040 SF	total eastern site area.
7	Max FAR
133,280 SF	resulting FAR.

46,240 SF	total site area
677,280 SF	FAR total
23,704 SF	3.5% MEP Bonus
700,984 SF	Adjusted Total FAR

CONTEXT MAP

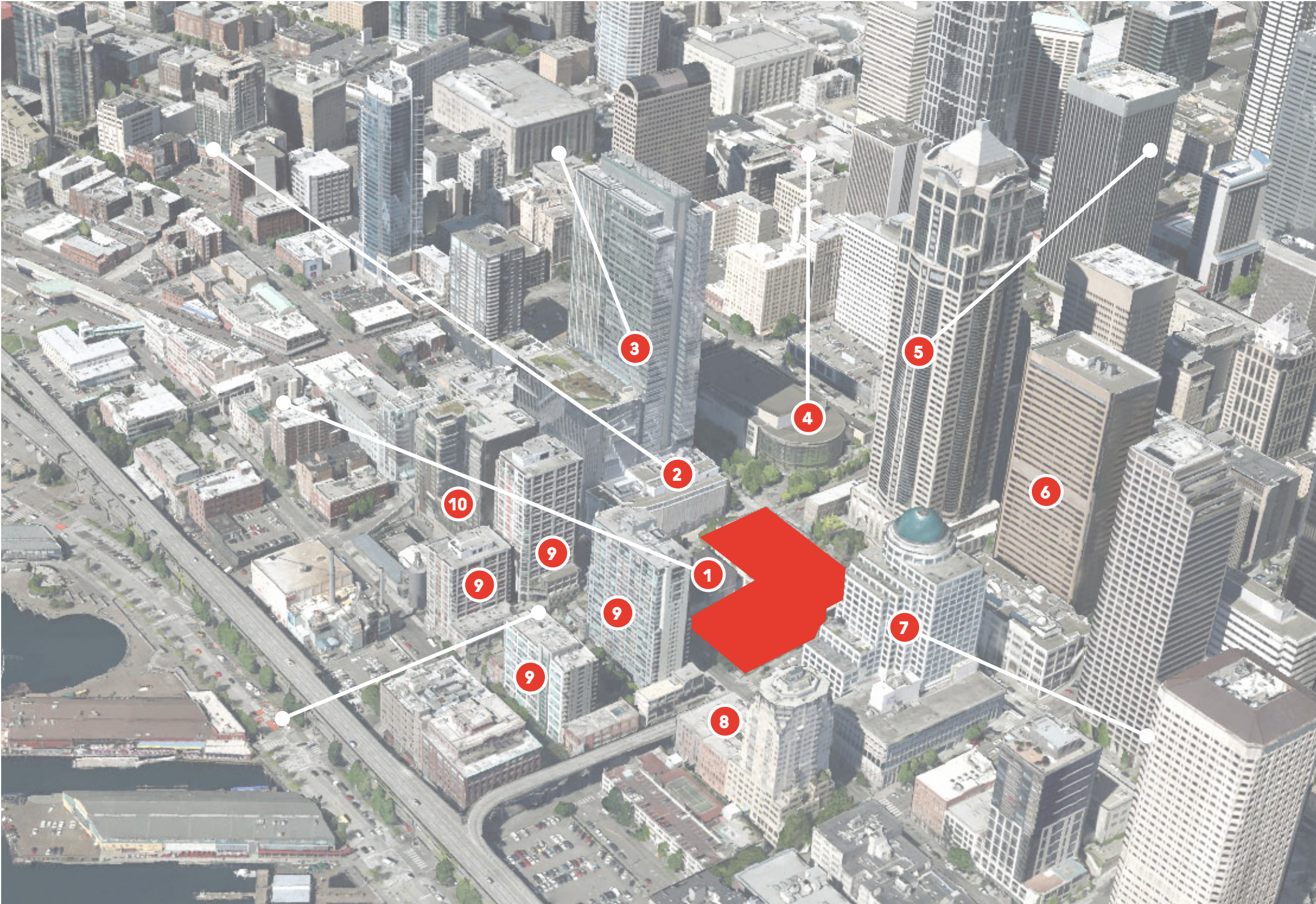


SITE ZONING PLAN



SITE CONTEXT

- 1 The Diller
Height: 4 Stories
- 2 Seattle Art Museum
Height: 92'-0"
- 3 Russell Investments Center
Height: 598'-0"
- 4 Benaroya Hall
Height: 76'-0"
- 5 1201 Third
Height: 772'-0"
- 6 1111 Third
Height: 454'-0"
- 7 Second & Seneca
Height: 315'-0"
- 8 1st & Seneca
Height: 5 Stories
- 9 Harbor Steps Apartments
Height: Varies
- 10 Four Seasons Hotel & Residences
Height: 240'-0"

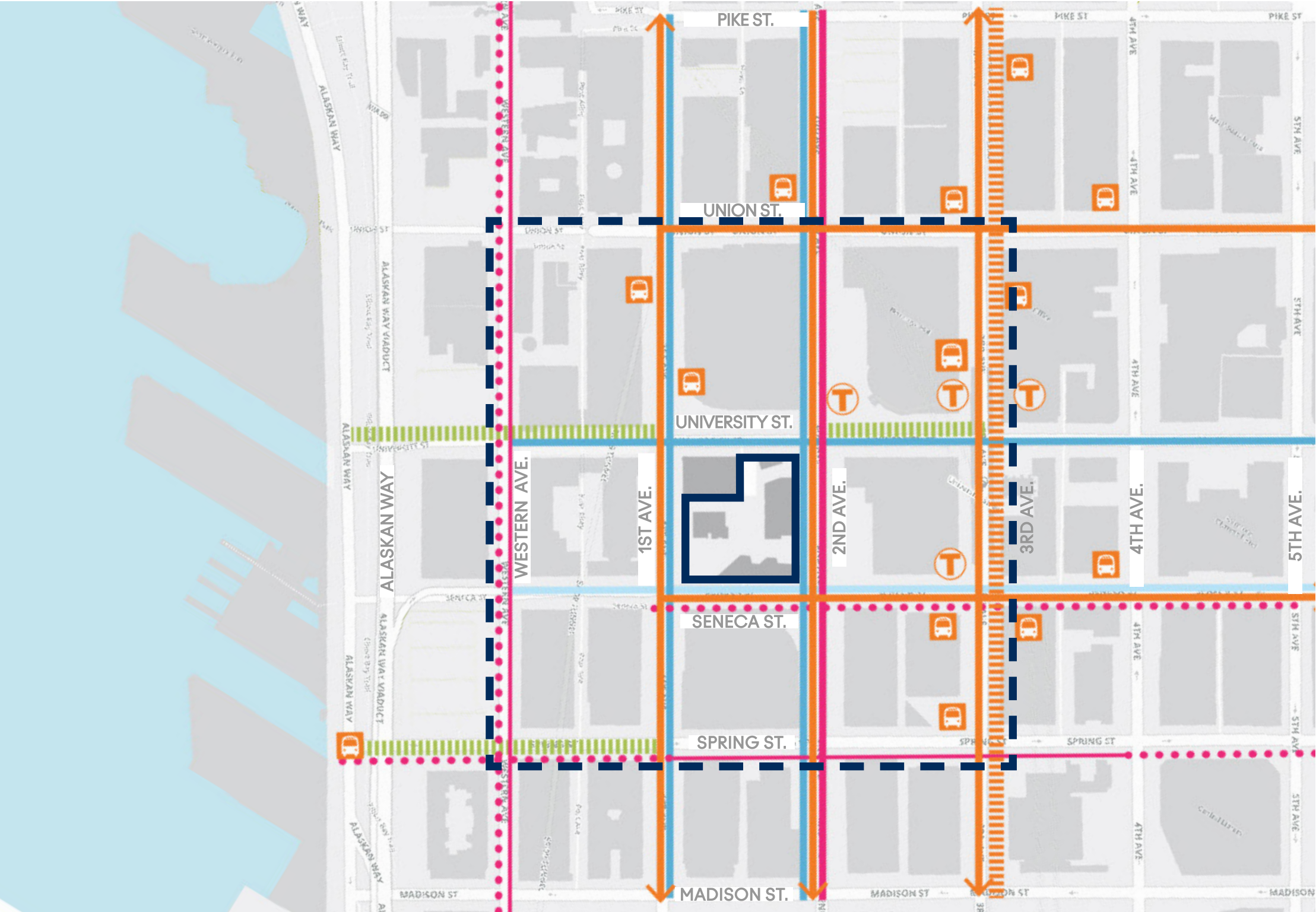


URBAN CONTEXT

- The location of the five generous shortcuts connect to the city’s growing transportation network.
- The routes add to the downtown hill climb corridors.
- Routes connect the waterfront and ferries to the bus tunnel. Busses, bikes, and street cars add to the network.

LEGEND

-  2+U Site
-  Nine-block zone
-  Pedestrian Street Class I
-  Pedestrian Street Class II
-  Green Street
-  Cycle Track
-  Bike Lane
-  Sharrow
-  Bus Route
-  Transit Tunnel
-  Bus Stop
-  Access to Transit Tunnel



2

Early Design Guidance

Comment	Page Reference
5a Board requests (indirectly) complete and clear floor plans for lower levels of the Village. Board endorsed SW corner, with a non-privatized Creative Commons.	Pages 10-13, 18-30, 36-40
5b Board agreed the public ramp to the overlook needs adjusting and widening to ensure it is welcoming to pedestrians at the corner, and that its directional sightlines (currently focused on a concrete core) are not pinched by the 'commons'. (D1)	Page 20
5c The Board agreed the ADA route from 1st Ave to the 'alley plaza' was not clear or intuitive, and the stated 'hillclimb assist' interior lobby/elevator should be more transparent and evident to all public users. Similar concern was expressed for the ADA and/or public route from the midblock on 2nd Ave to the 'alley plaza' level, which was not clear. (C1; D1)	Page 27
5d The Board supported the transparent corner retail and access stairs at the 1st Ave midblock, but not the height of the 'overlook dining', due to the shadows it casts on public steps, and the tall, unfriendly wall it presented to 1st Avenue. These stairs appear overly privatized, and the ephemeral perspectives shown were not clear or definitive enough to confirm this or other ground floor conditions. (C1; C3; D1)	Pages 26-27
5e The Board supported the large central court, and the implied degree of perimeter activating uses. The Board was skeptical the loading dock – no matter how programmed - would provide consistent activation, but supported that wall being transparent or translucent. The landscaping strips appear random and clutter the plaza for flexible uses. The Board requested detailed lighting studies to ensure the 'village' is safely but not glaringly well lit. (D1; D2; D3)	Pages 22-23
5f The Board supported the diagonal stairs from the northeast corner, and the substantial bike parking presence at the bottom, off the alley stub. A bike runnel on these stairs from the 2nd Ave cycle tracks was suggested. The Board supported the low, wide seating steps on the east side of the 'wedge' and encouraged they wrap onto the University frontage as well, to energize that important pedestrian corridor. (B1; C1; D1)	Pages 18-19
6a The Board regretted the absence of complete and clear elevations for the essential lower levels, but understood them enough to provide some guidance. The primary concern was that the lower levels exhibited an entirely different architectural character at odds with the tower columns and facades above. The Board strongly endorsed the design development of the 'V' columns, but recommended substantially more integration of the architectural elements above and below the soffit. (B4; C2)	Pages 32-35
6b The Board endorsed the 2-story height and relatively transparent storefront character shown on 1st Ave, and the setbacks which allow the structural 'V' columns to be in the round. (C1; C3; C4)	Pages 27 + 29

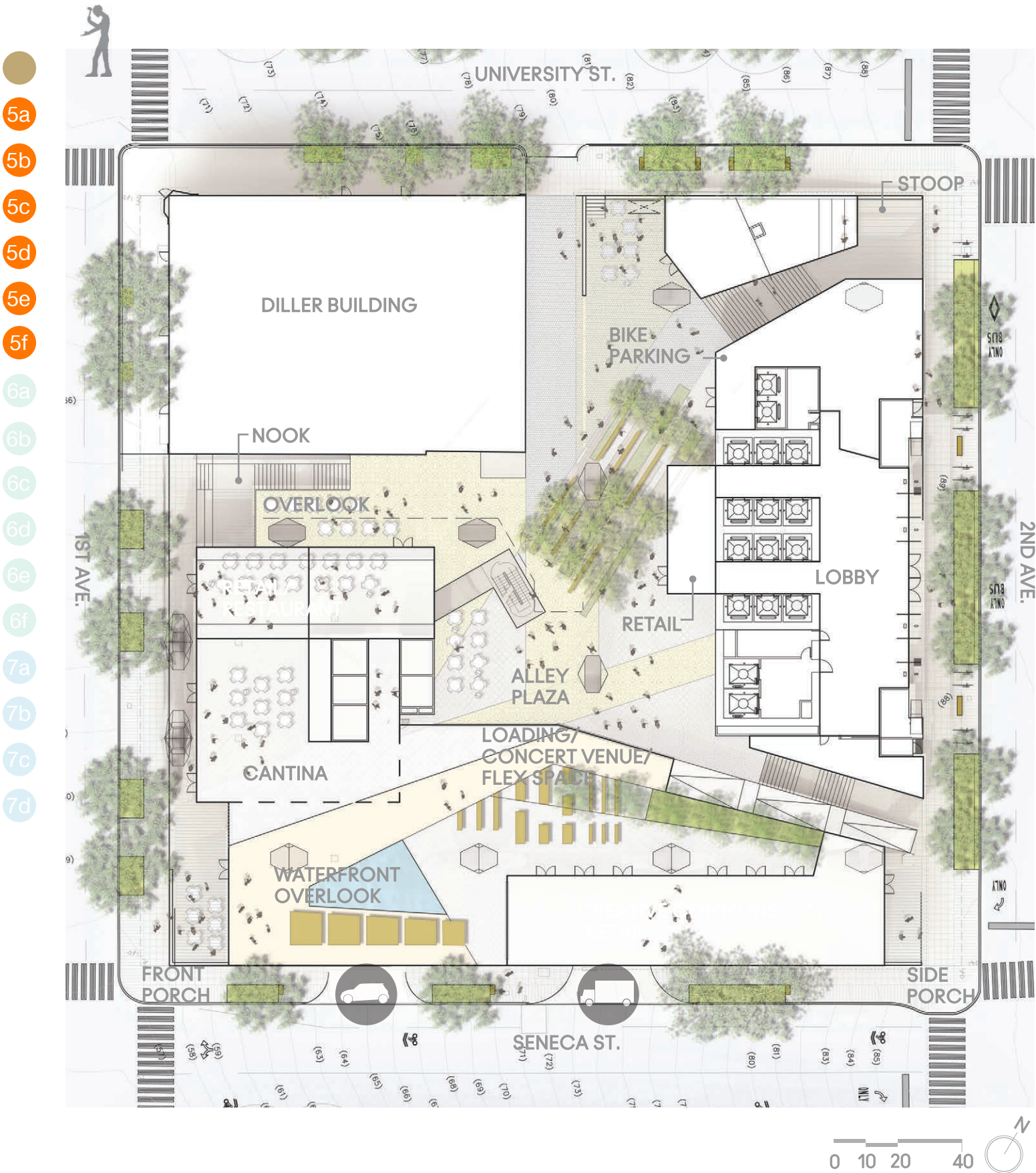
Comment	Page Reference
6c The Board supported the stepped street wall along Seneca and the high transparency at the corners, but was less certain about the translucent portion suggested at the mid-block loading zone; detailed study of façade materials, canopies and lighting options is needed. (C1; C3)	Pages 34 + 50
6d The Board tentatively supported the short street wall 'folly' of the 'SAM wedge' on University, and its large activating retail windows onto the street and the alley. (C2; C3)	Page 19
6e The Board supported the double-height, highly transparent and skylit lobby along 2nd Ave, but agreed the merging of the 'V' columns into the core elements above needed better resolution. Possibly the vertical core elements should be brought to ground. The Board strongly supported the three tall, transparent retail corners that wrap into the inner 'village' from the 2nd Ave sidewalk, and the publically populated roof deck above the lobby/retail, but questioned how the public and disabled will intuitively access them. (B1; B4; C1)	Pages 30 + 35
6f The Board agreed the large tower soffit will be highly visible to the public on adjacent streets and function as a sky for the 'village' below, and agreed neither of the two design studies shown was compelling. The rectangle and infill was too banal, and the dia-grid as too alien to the design language; further studies are required, including generous but not glaring night lighting for all portions and places under the soffit. (B4; D5; D6)	Pages 29, 52-53, 76-78
7a The Board supported the reduced tower height as a better fit into the downtown skyline, as trade-off for the enlarged office floor plates since EDG #1. The Board also supported the revised, symmetrical stairs and simpler modulation along the 2nd Ave elevation, and the more solid mass elements that anchor that façade to the street (also see comment 6E). (A2; B4)	Pages 44-51, 63
7b The Board supported the double-height horizontal 'notch' on the south façade that interlocks the two towers, and the west-facing corner balcony/ notches that modulate the tower every fifth floor. (B4)	Pages 71-73
7c The Board supported the structural system and trellis above the lower tower mass, but recommended it be raised to approximately align with the 'notch' along Seneca; this creates a stronger interlock and better proportions to the waterside elevation. (B2; B4)	Pages 50-51
7d The Board supported the structural system and symmetrical (except for the minor cut out) rooftop trellis, but agreed it appeared too short and compressed; the height should be raised approximately one more floor to mostly or fully conceal the equipment screen. (A2; B4)	Page 48-51

3

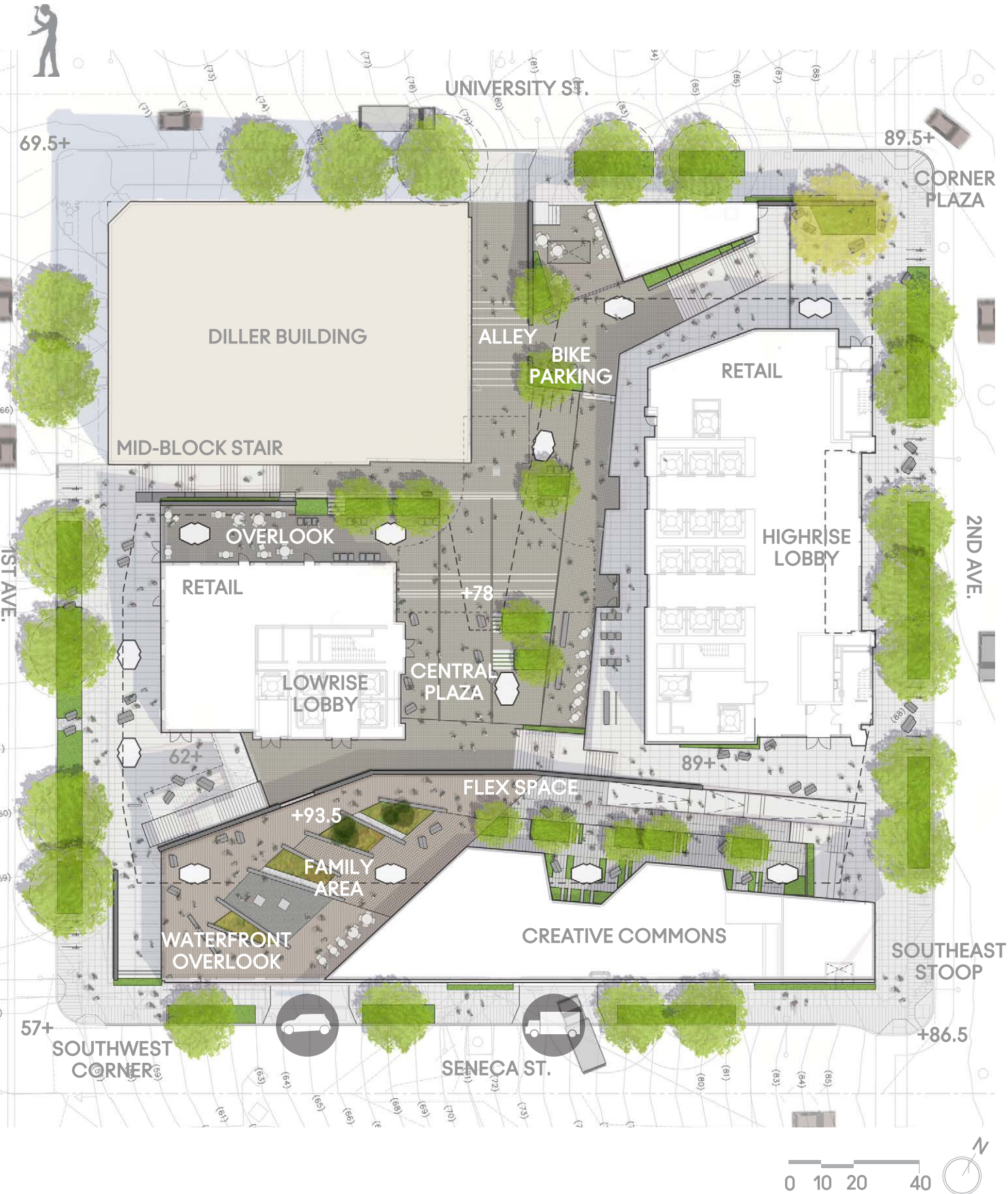
Ground Plane

GROUND PLANE | Site Plan

EDG 2 PLAN



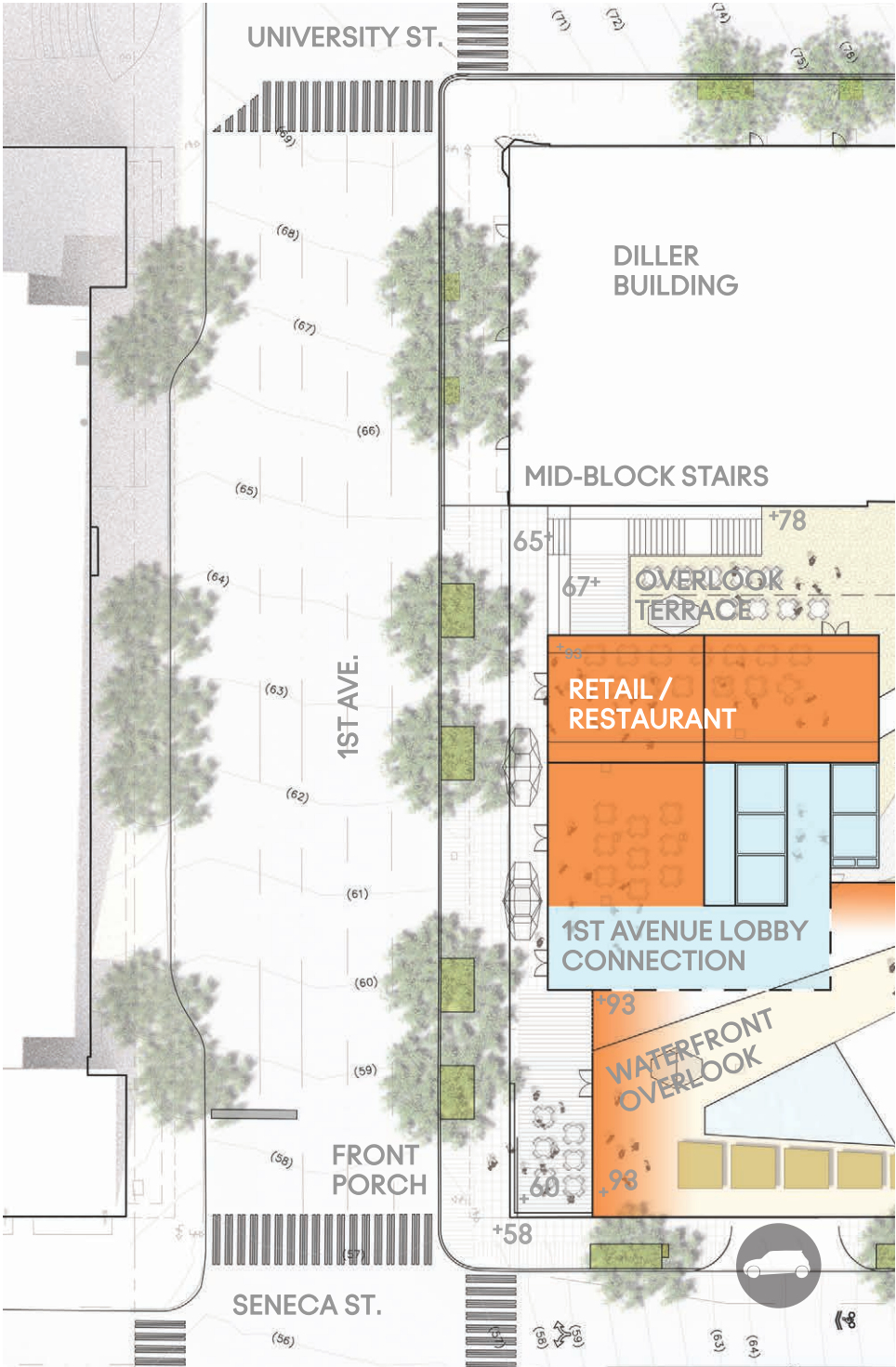
CURRENT PLAN



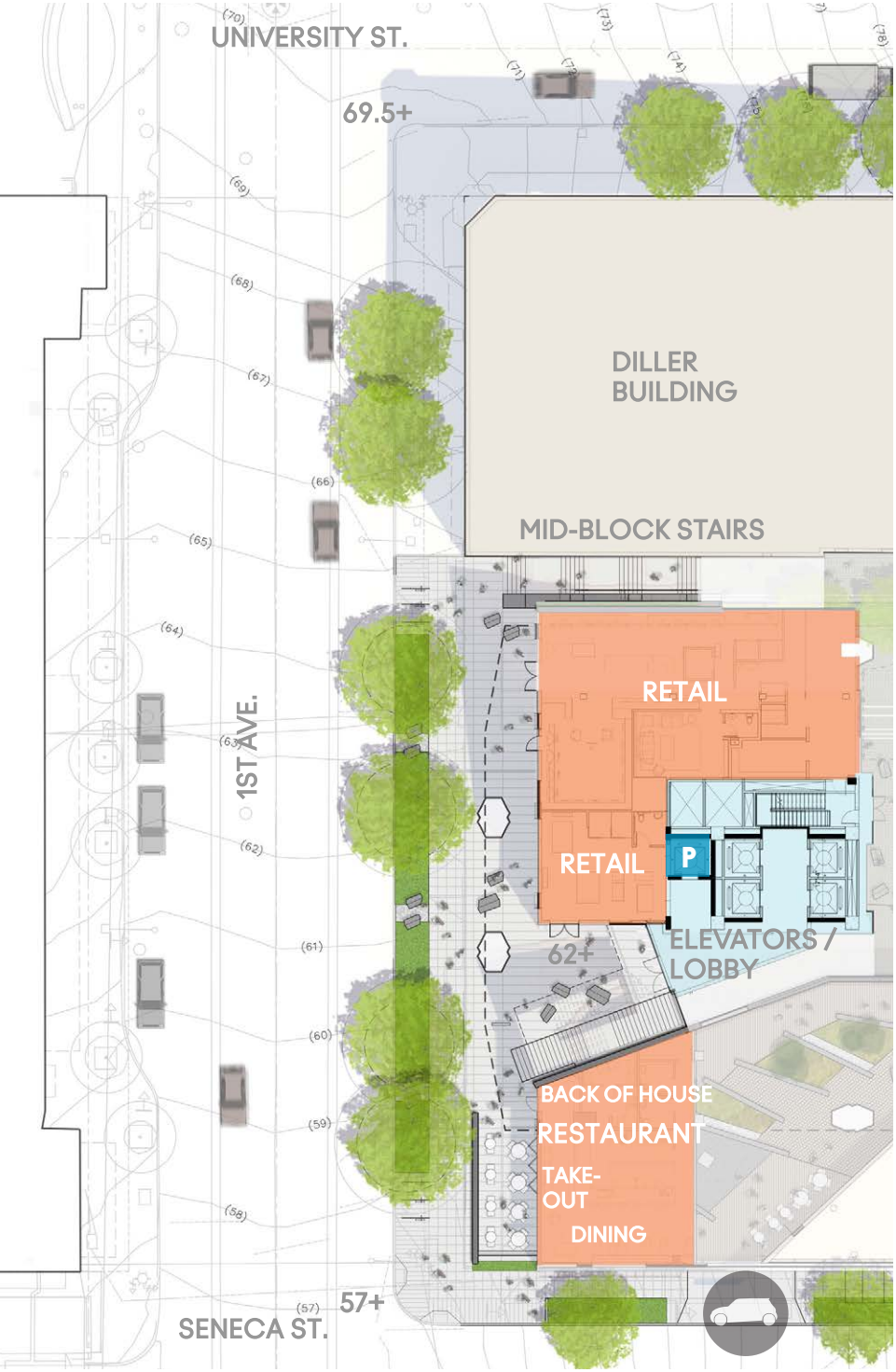
GROUND PLANE | 1st Avenue Plan

EDG 2 PLAN

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



CURRENT PLAN SEE PAGE 37 FOR COMPLETE ARCHITECTURAL FLOOR PLAN



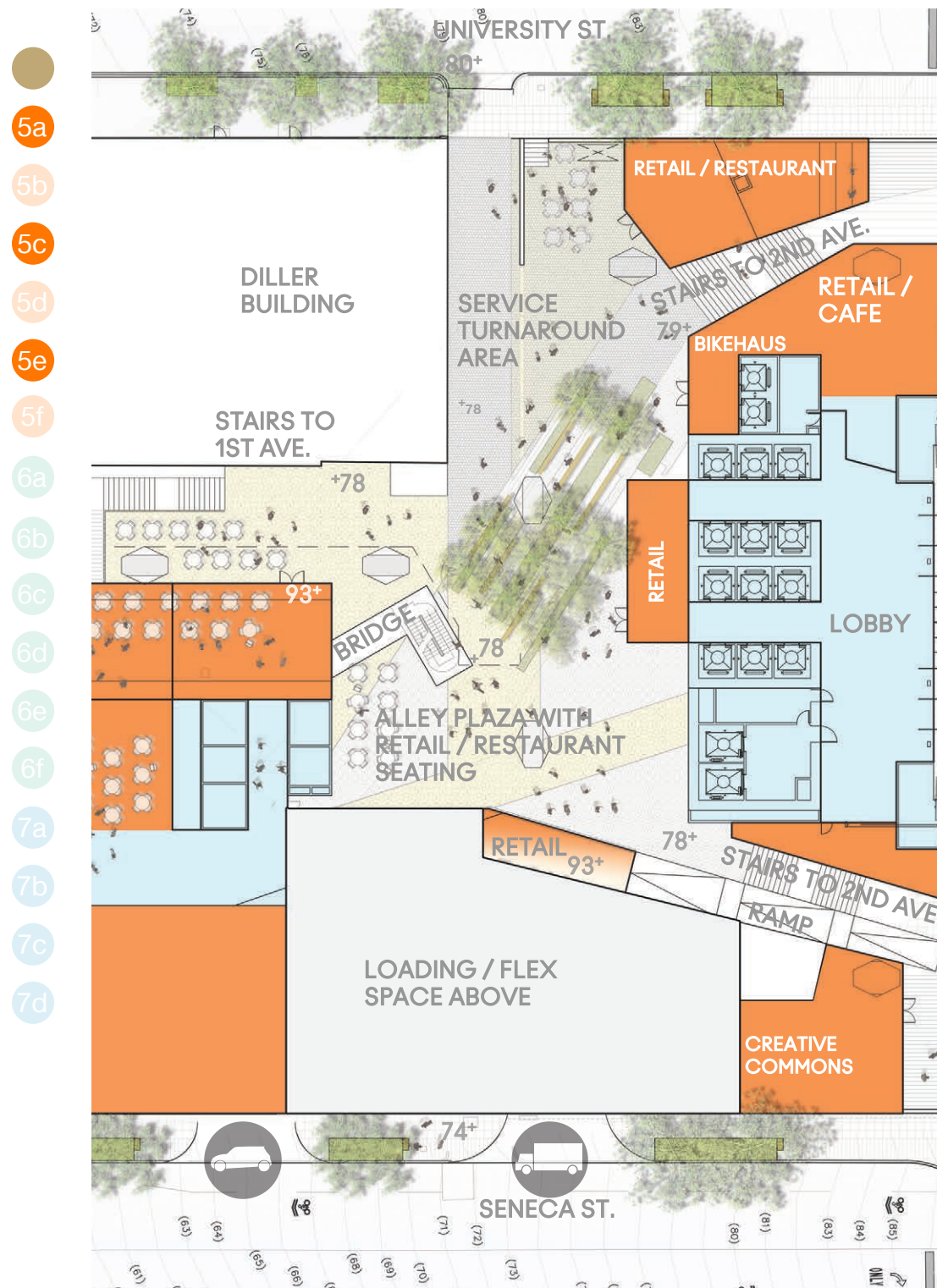
LEGEND

- Retail / Restaurant Space
- Lobby Space
- Arts / Cultural Space
- Publicly Accessible Elevator



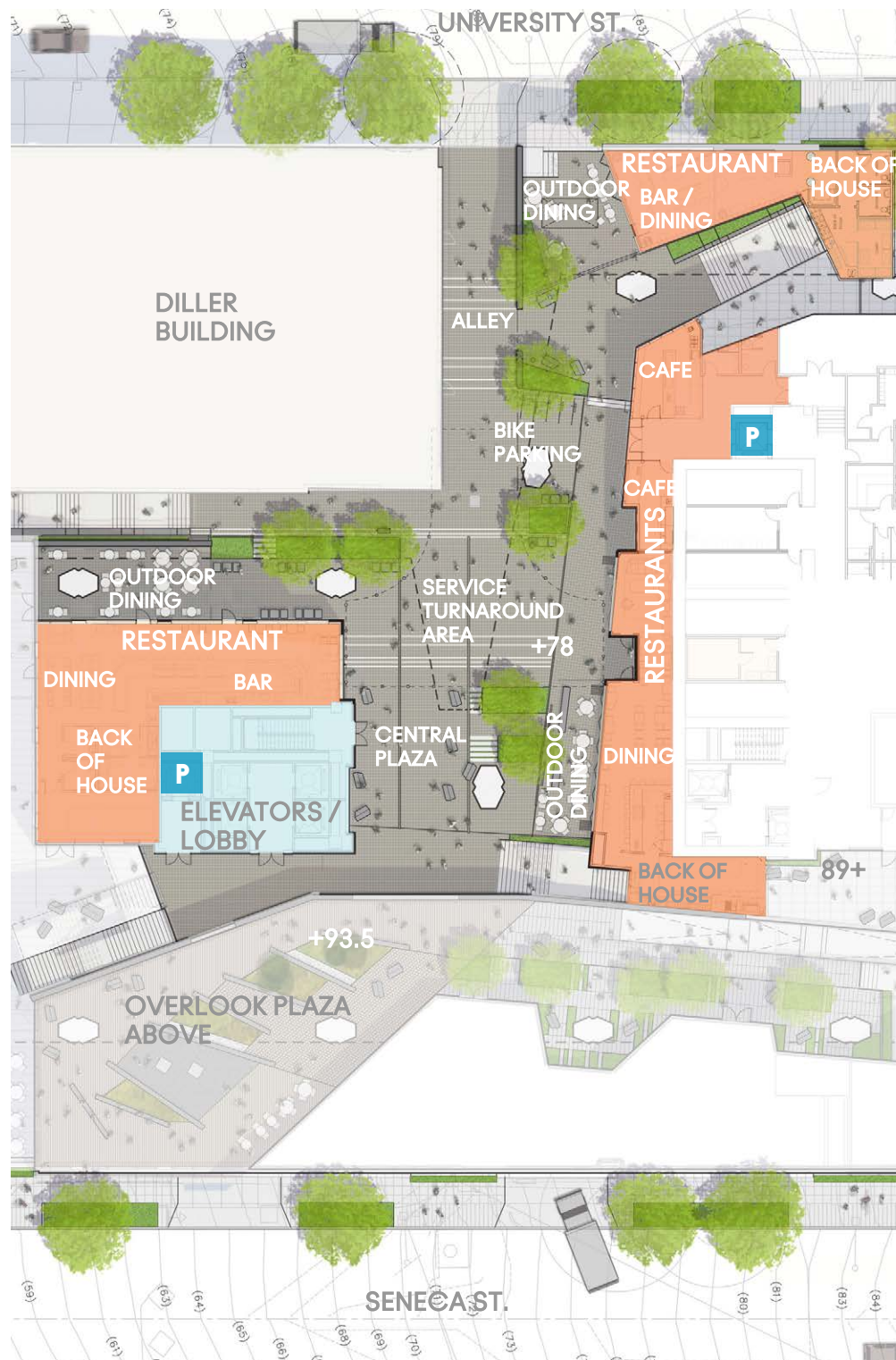
GROUND PLANE | Alley Plan

EDG 2 PLAN



CURRENT PLAN

SEE PAGE 38 FOR COMPLETE ARCHITECTURAL FLOOR PLAN



LEGEND

-  Retail / Restaurant Space
-  Lobby Space
-  Arts / Cultural Space
-  Publicly Accessible Elevator



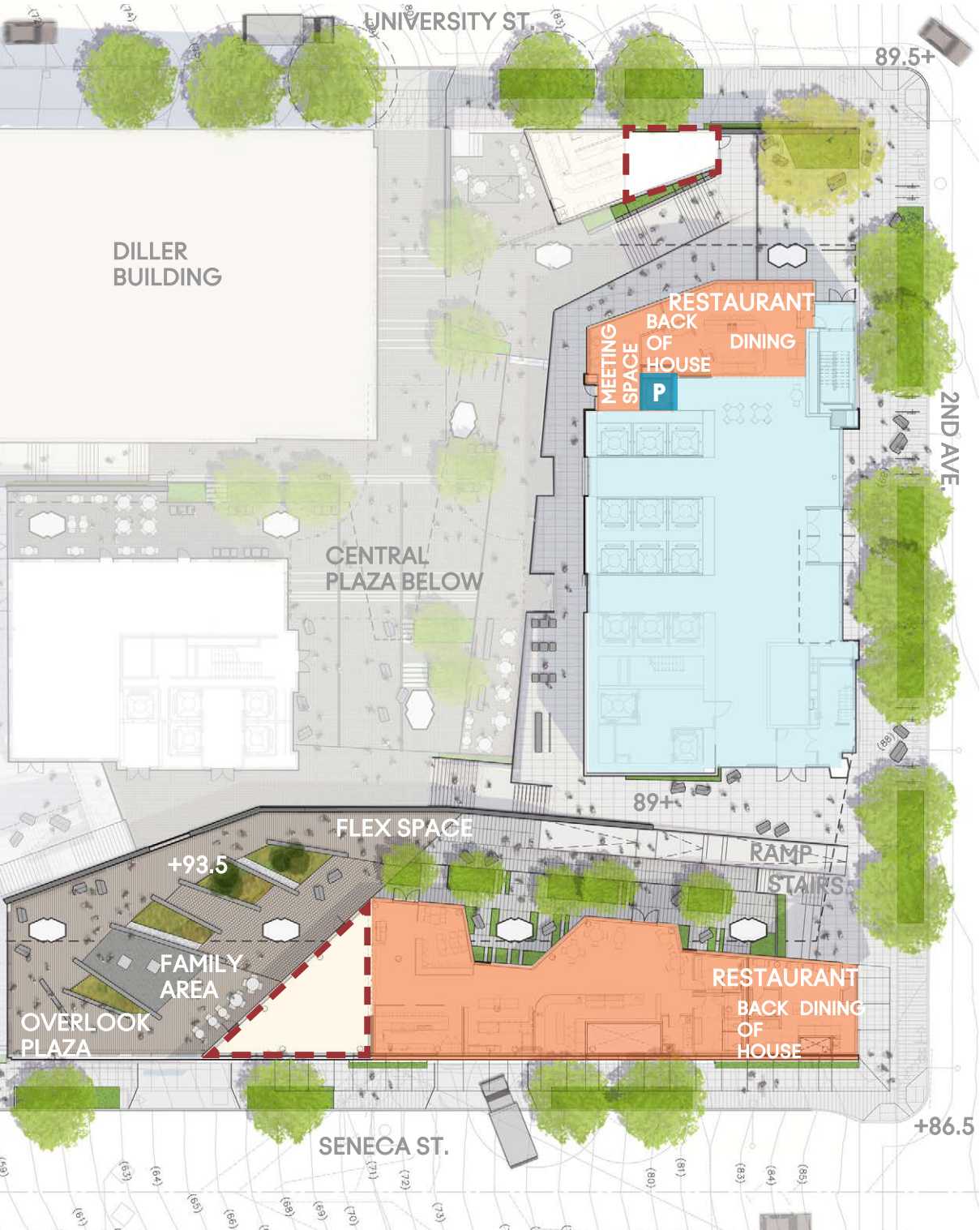
GROUND PLANE | Terrace Level / 2nd Avenue Plan

EDG 2 PLAN



CURRENT PLAN

SEE PAGE 39 FOR COMPLETE ARCHITECTURAL FLOOR PLAN

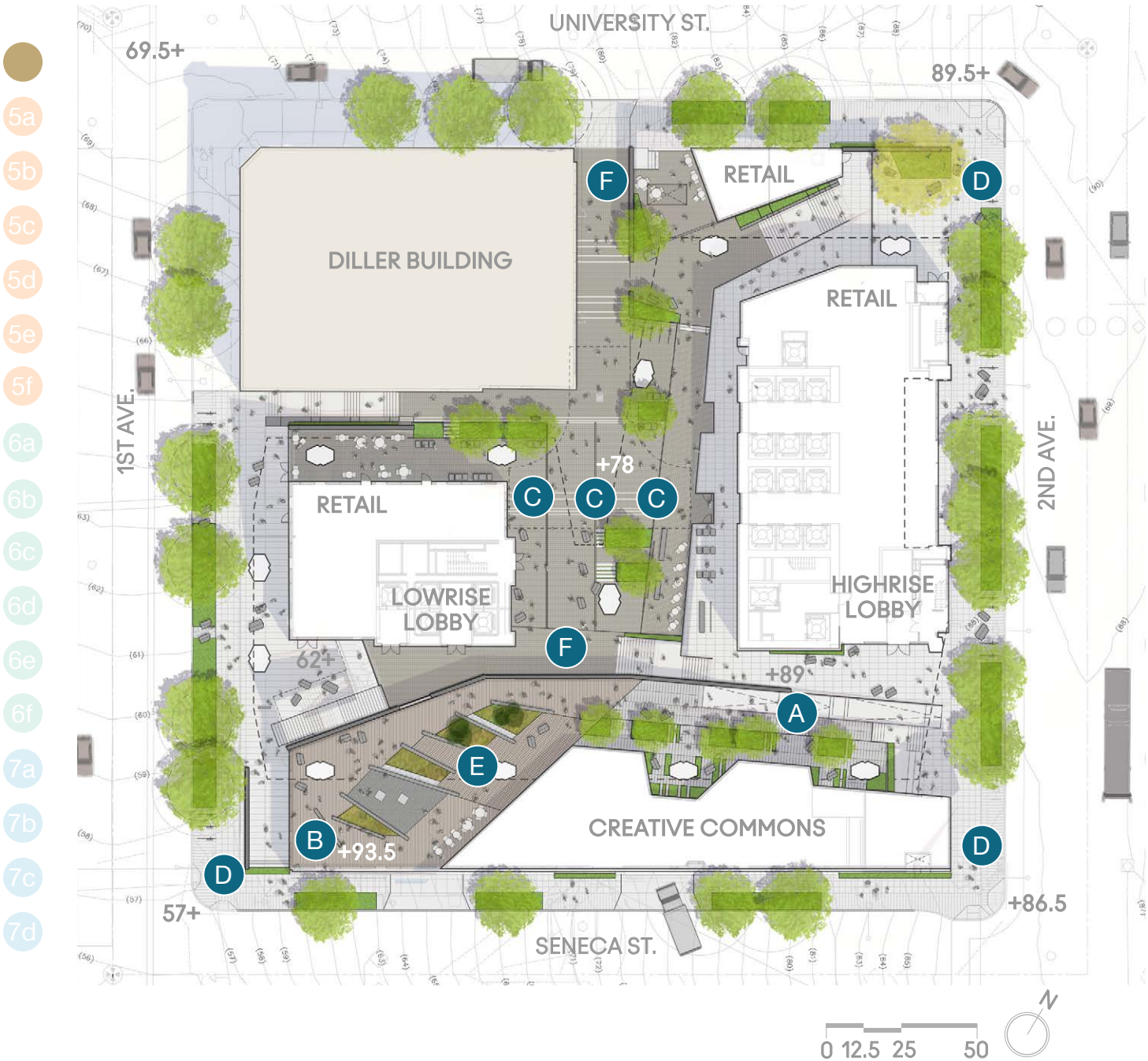


LEGEND

- Retail / Restaurant Space
- Lobby Space
- Arts / Cultural Space
- Publicly Accessible Elevator



CURRENT MATERIALS



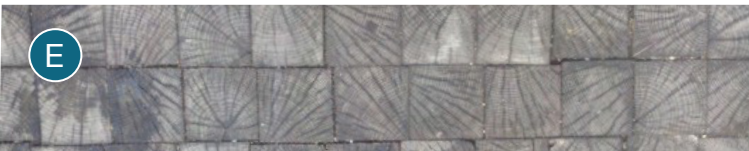
Precast Pavers and Steel Banding



Wood Decking



Decorative Cast Metal Trench Drain Grate



End Grain Wood Pavers



CIP Concrete Pavement



Cobble Paving



Custom Table and Chairs



Custom Table and Lean Rail



Custom Seat

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



GROUND PLANE | Open Space - Planting

CURRENT PLANTING



TREES

- Trees create scale, life, and constant change.
- Trees tell the story of the site ecology - exposed sun to shade.
- Trees create an alley plaza understory, a canopy on an overlook, and a humanized street.
- Street trees establish context, have large canopies, and seasonal interest.
- Trees create seasonal change and introduce nature.

LEGEND

- Building at Grade
- Building Above
- Site Furnishings
- Code Required Trees
- Trees Provided Beyond Code Requirements

CURRENT PLANTING



PLANTING

- Plants signal life, seasonal change, ecological conditions, and a commitment to care.
- Shrubs, groundcovers, and vines amplify the site character.
- Plants create movement, smell, and rich seasonal change.
- Plants intentionally opportunistically appear in small areas, eddies, and cracks.
- Vines climb walls—bringing scale, texture, pattern, and life to the city.

LEGEND

- Building at Grade
- - - Building Above
- Site Furnishings
- Code Required Planting
- Planting Provided Beyond Code Requirements



Western Maidenhair Fern



Fiveleaf Akebia



Pacific Mist Manzanita



Beesia



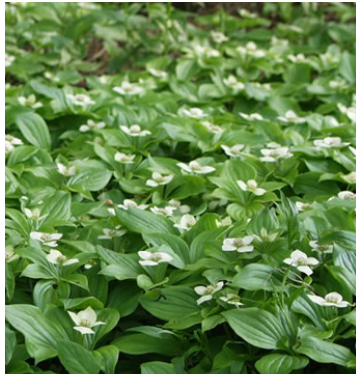
Deer Fern



Sandy Heather



Evergreen Clematis



Bunchberry Dogwood



Common Lady Fern



American Dune Grass



Beach Strawberry



Salal



Silvervein Creeper



Sweet Box



New Zealand Wind Grass



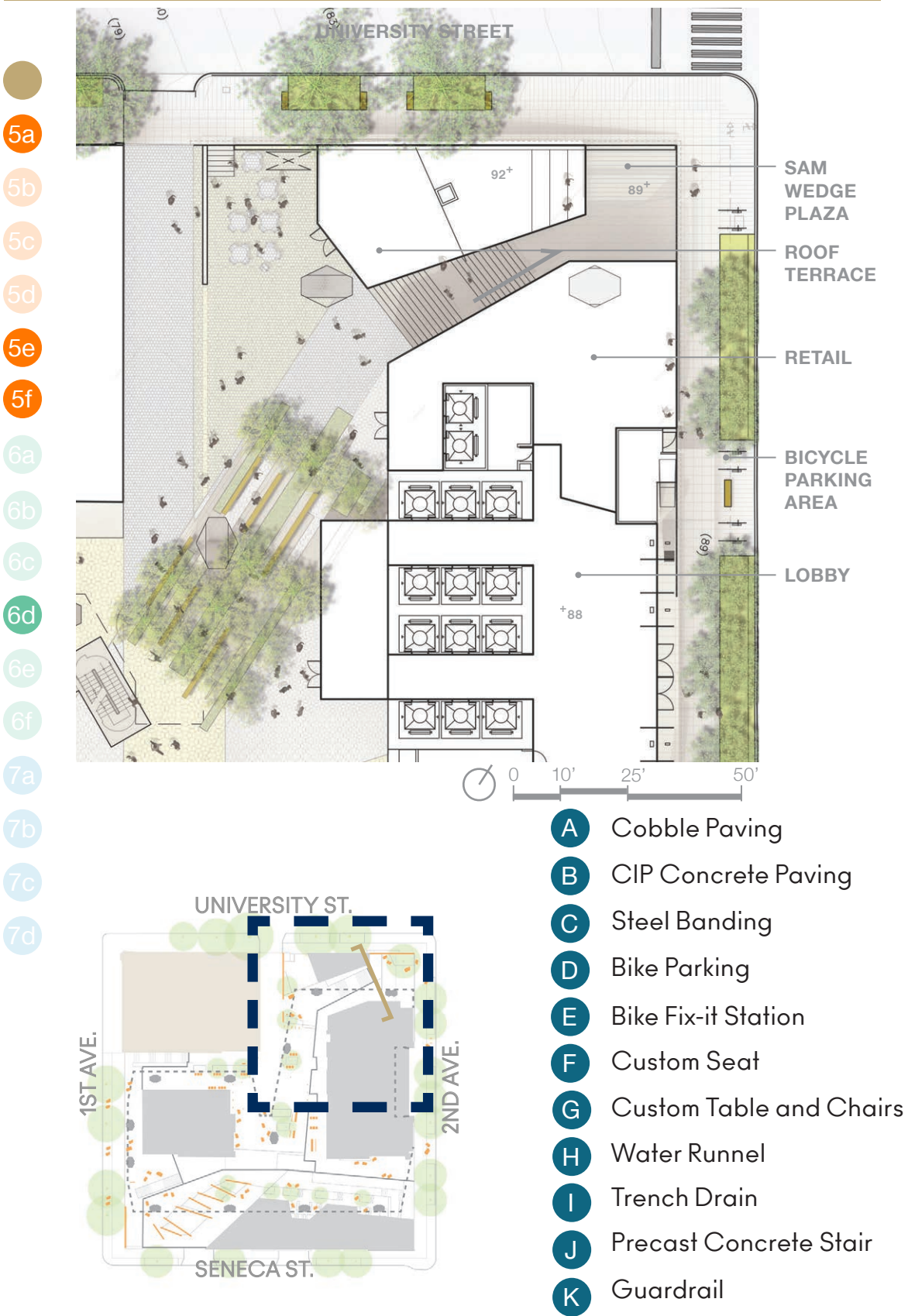
Everlow Yew



Himalayan Huckleberry

GROUND PLANE | Corner Plaza

EDG 2 PLAN

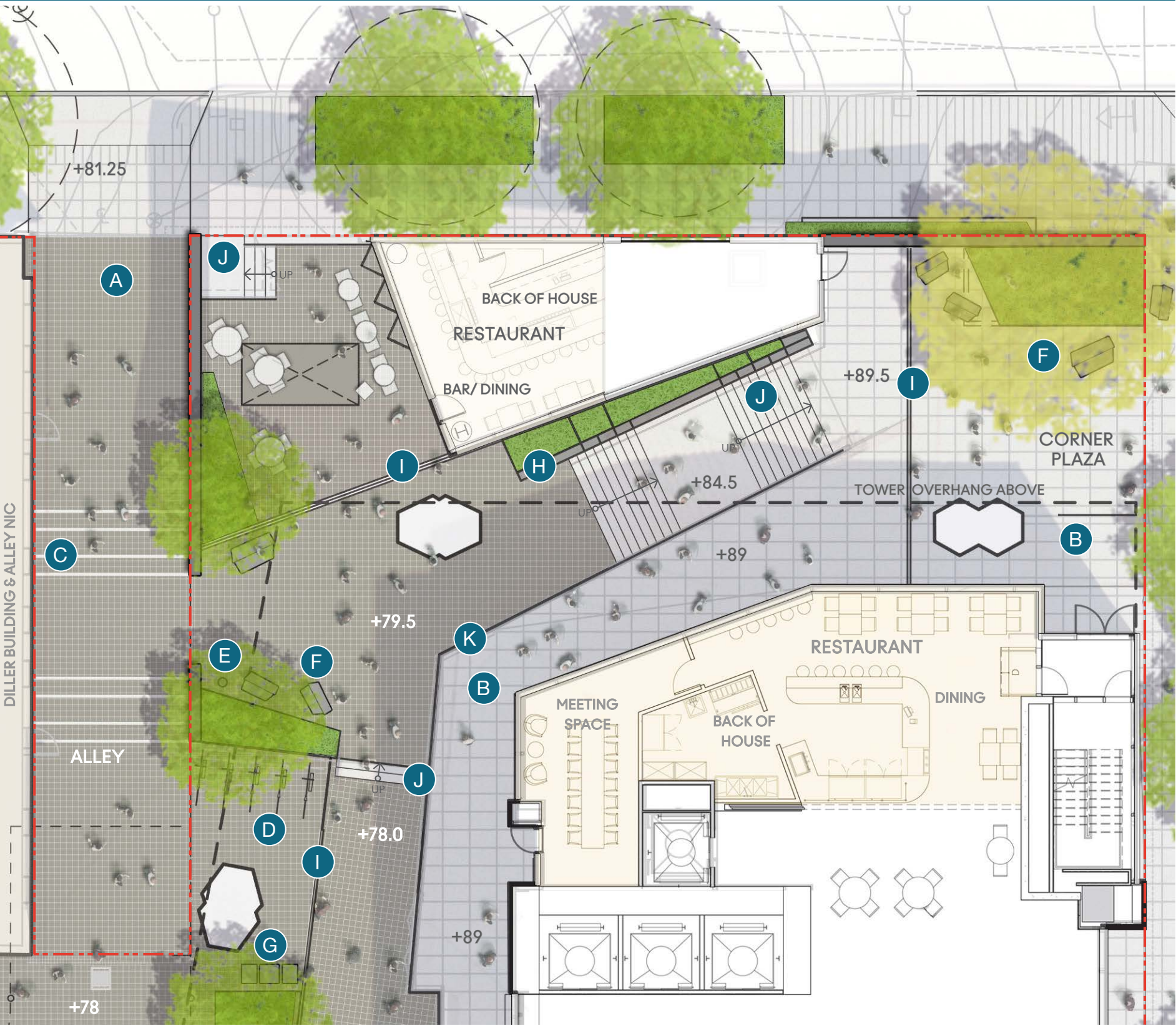


KEY PLAN (NTS)

LEGEND

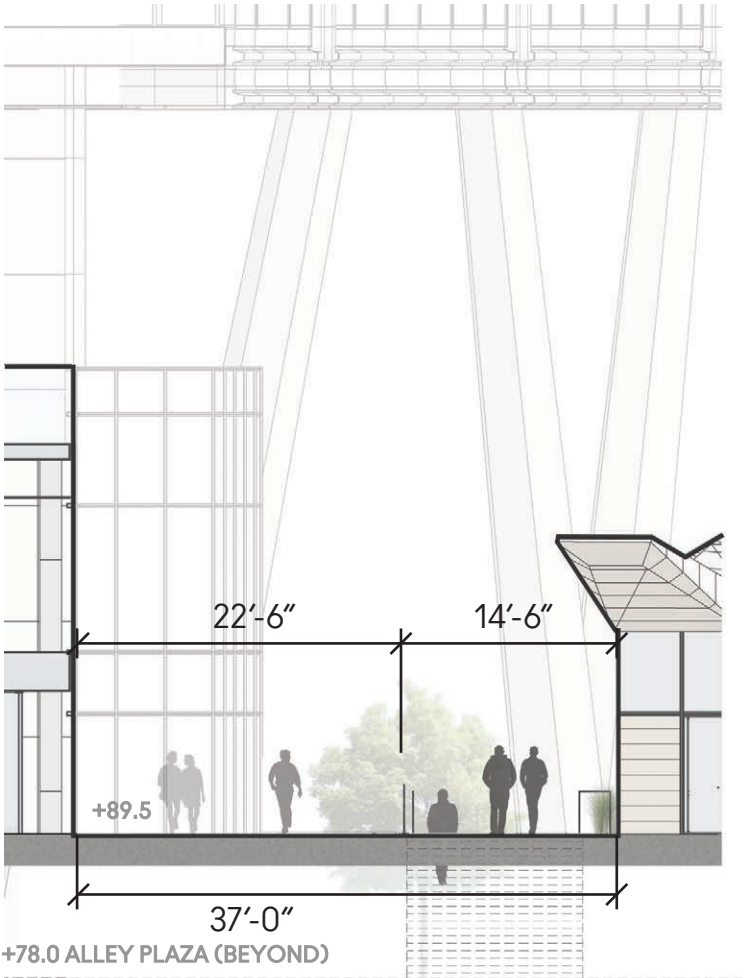
CURRENT PLAN

SEE PAGES 36-40 FOR COMPLETE ARCHITECTURAL FLOOR PLANS



GROUND PLANE | Corner Plaza

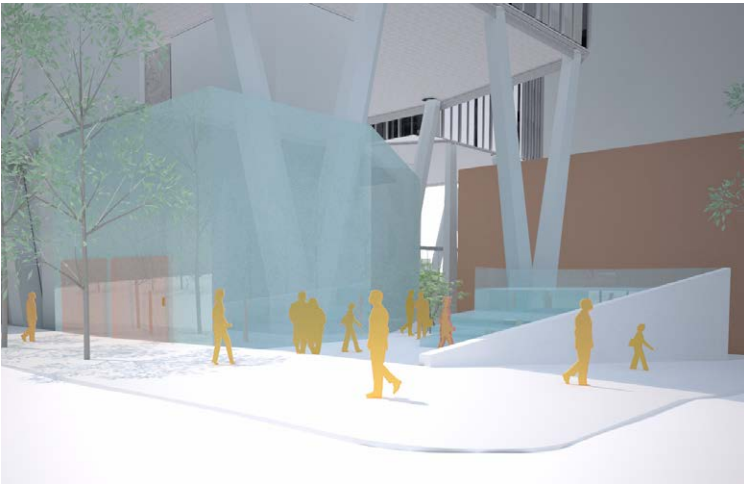
- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



Section at Corner Plaza stair (NTS)



EDG 2: CORNER PLAZA



CURRENT: CORNER PLAZA



VIEW TO ALLEY

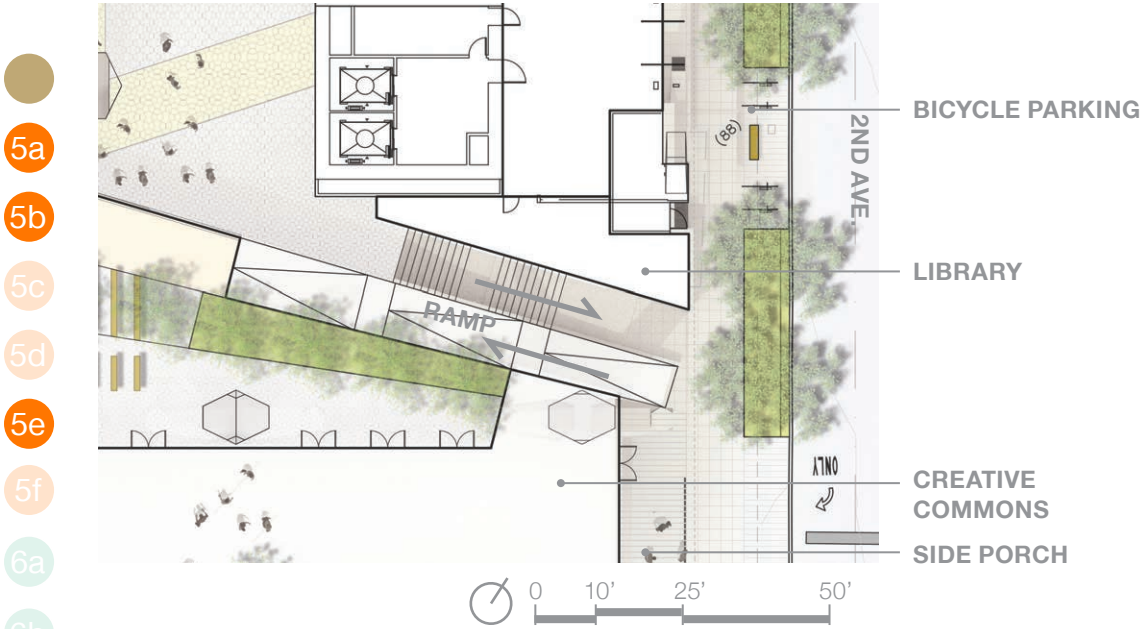


VIEW TO PLAZA



GROUND PLANE | Southeast Stoop

EDG 2 PLAN



- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



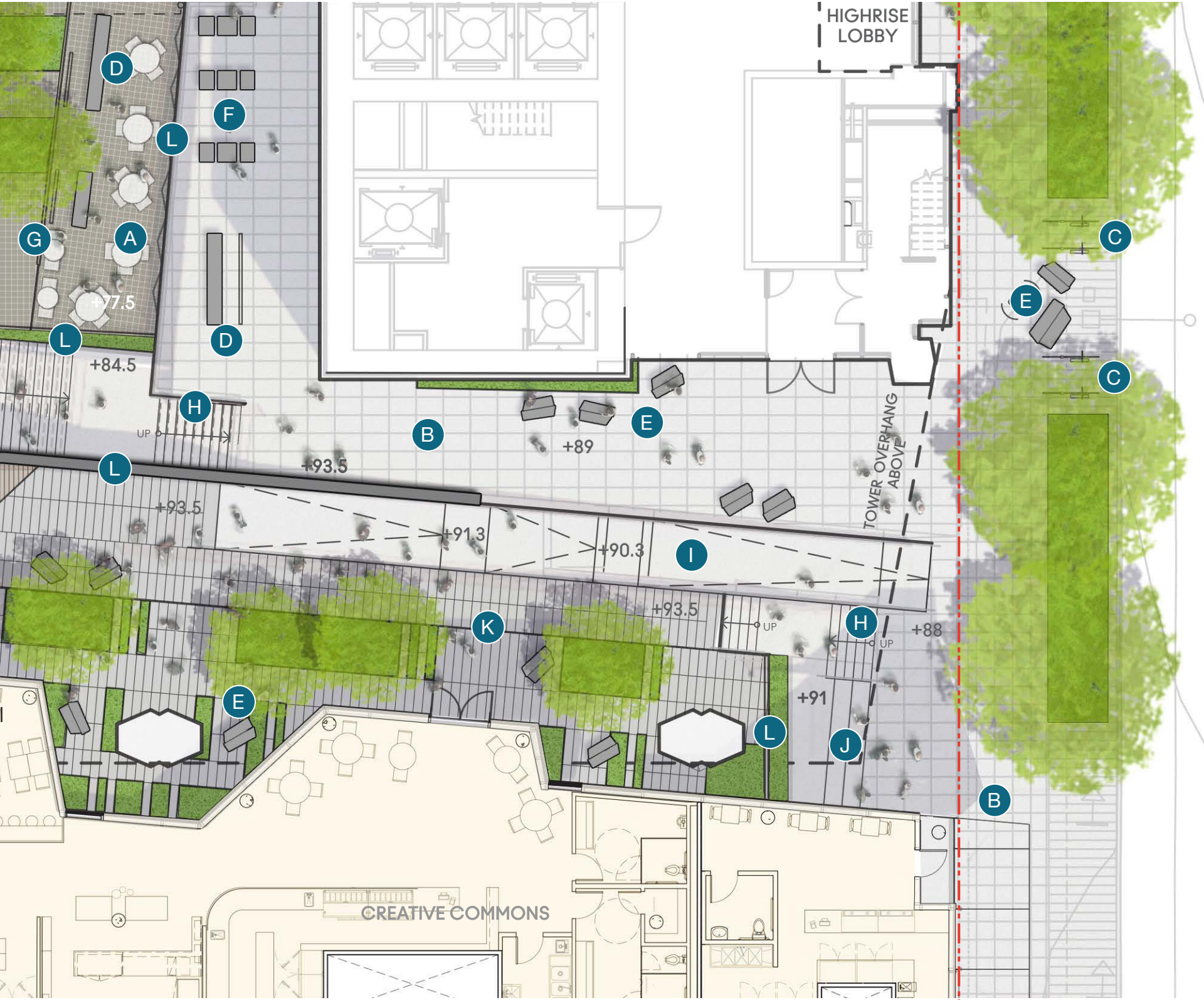
KEY PLAN (NTS)

- A Cobble Paving
- B CIP Concrete Paving
- C Bike Parking
- D Custom Table and Leanrail
- E Custom Seat
- F Custom Table and Chairs
- G Trench Drain
- H Precast Concrete Stair
- I ADA Ramp
- J CIP Concrete Seat Steps
- K Precast Concrete Pavers
- L Guardrail

LEGEND

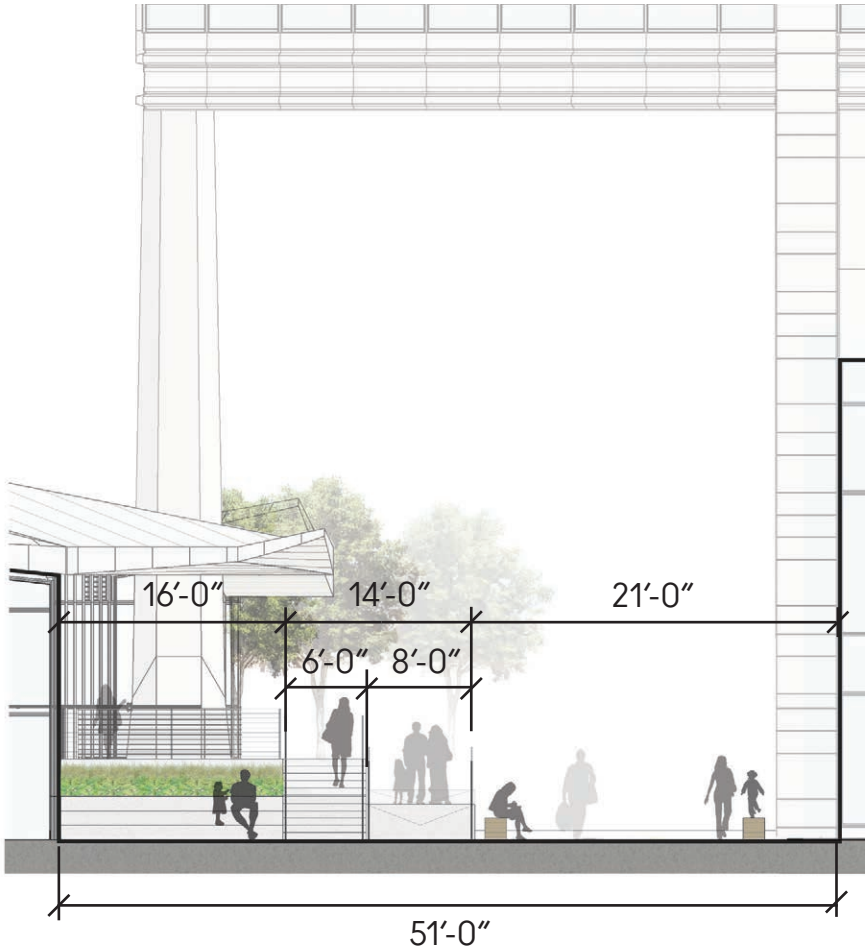
CURRENT PLAN

SEE PAGES 36-40 FOR COMPLETE ARCHITECTURAL FLOOR PLANS



GROUND PLANE | Southeast Stoop

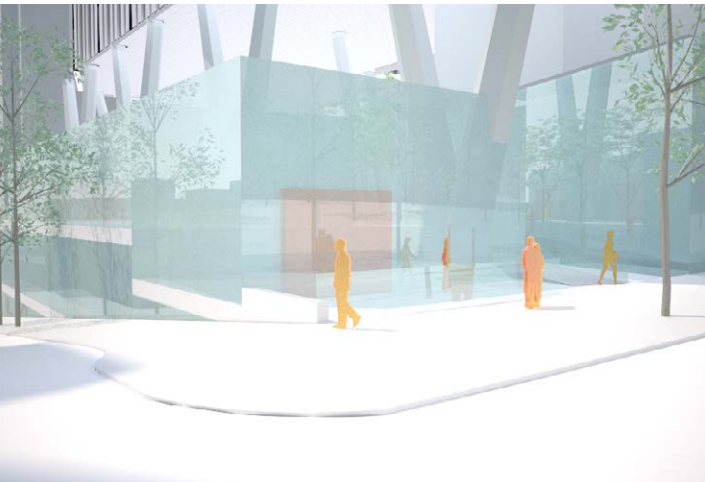
- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



Section at Southeast Stoop ramp and stair (NTS)



EDG 2: SOUTHEAST STOOP



CURRENT: SOUTHEAST STOOP



EDG 2: STAIR AND RAMP

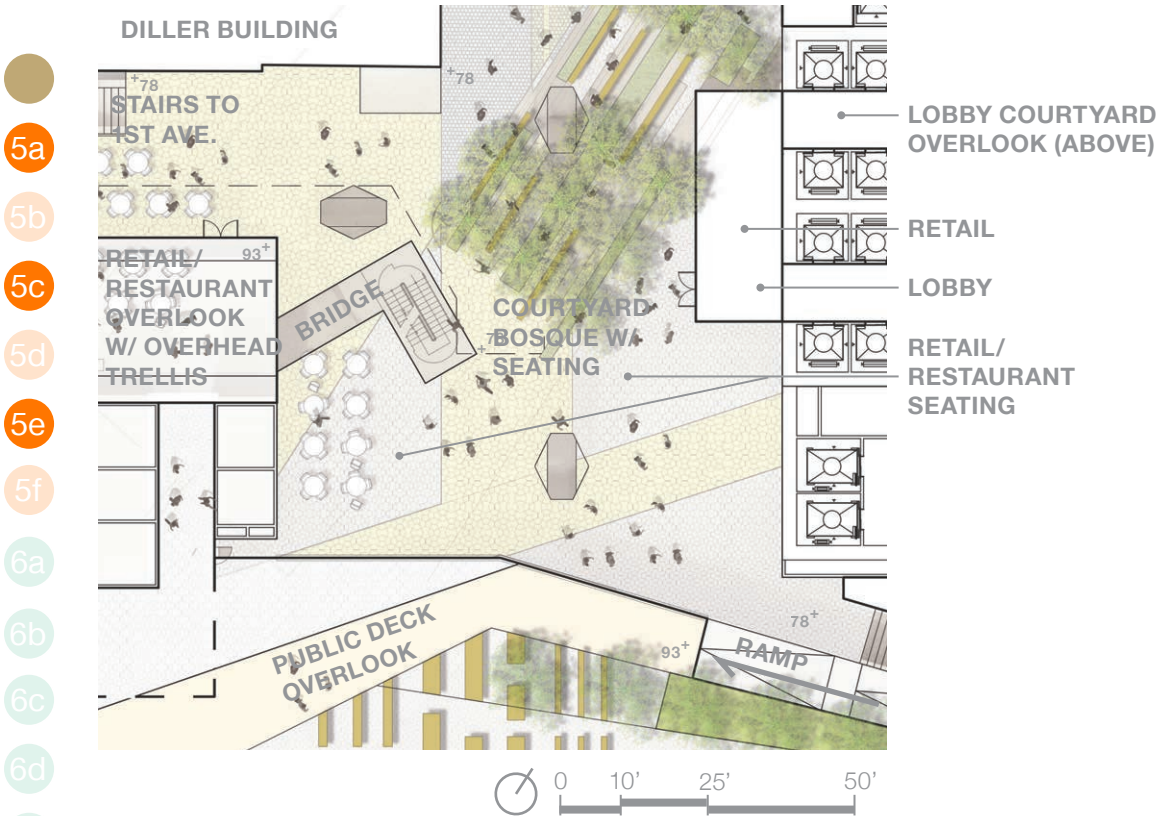


CURRENT: STAIR AND RAMP



GROUND PLANE | Central Plaza

EDG 2 PLAN



- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



KEY PLAN (NTS)

- A Cobble Paving
- B Precast Concrete Bands
- C Steel Banding
- E Custom Seat
- F Custom Table and Chairs
- G Trench Drain
- H Precast Concrete Stairs
- I Bollards
- J Guardrail

LEGEND

CURRENT PLAN

SEE PAGES 36-40 FOR COMPLETE ARCHITECTURAL FLOOR PLANS



GROUND PLANE | Central Plaza

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



EDG 2: PLAZA



CURRENT: CENTRAL PLAZA



EDG 2: PLAZA

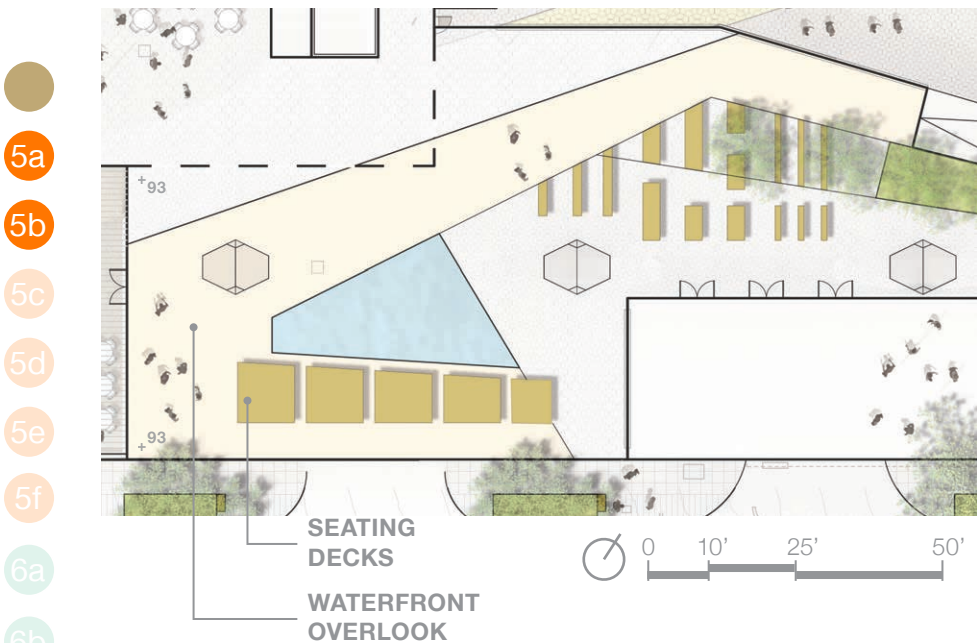


CURRENT: CENTRAL PLAZA



GROUND PLANE | Overlook Plaza

EDG 2 PLAN

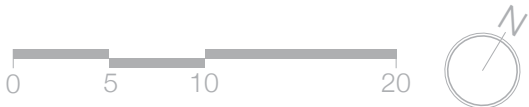


- A Wood Deck
- B Wood End Grain Pavers
- C Rubber Play Surface
- D Gravel Mulch
- E Custom Seat
- F Timber Bench
- G Trampoline
- H Guardrail

LEGEND

CURRENT PLAN

SEE PAGES 36-40 FOR COMPLETE ARCHITECTURAL FLOOR PLANS

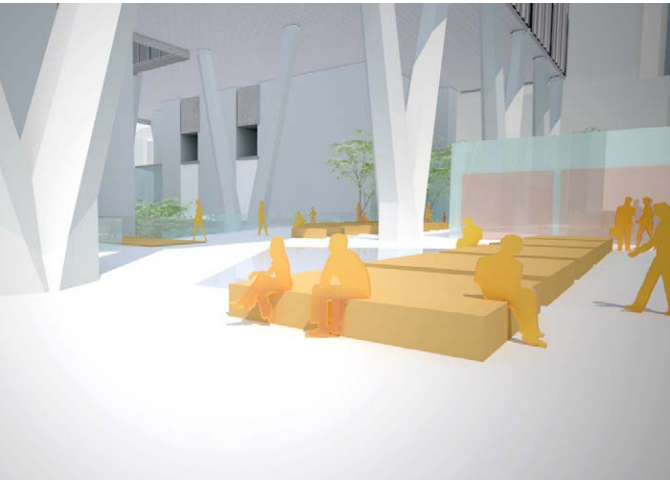


GROUND PLANE | Overlook Plaza

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



EDG 2: OVERLOOK PLAZA

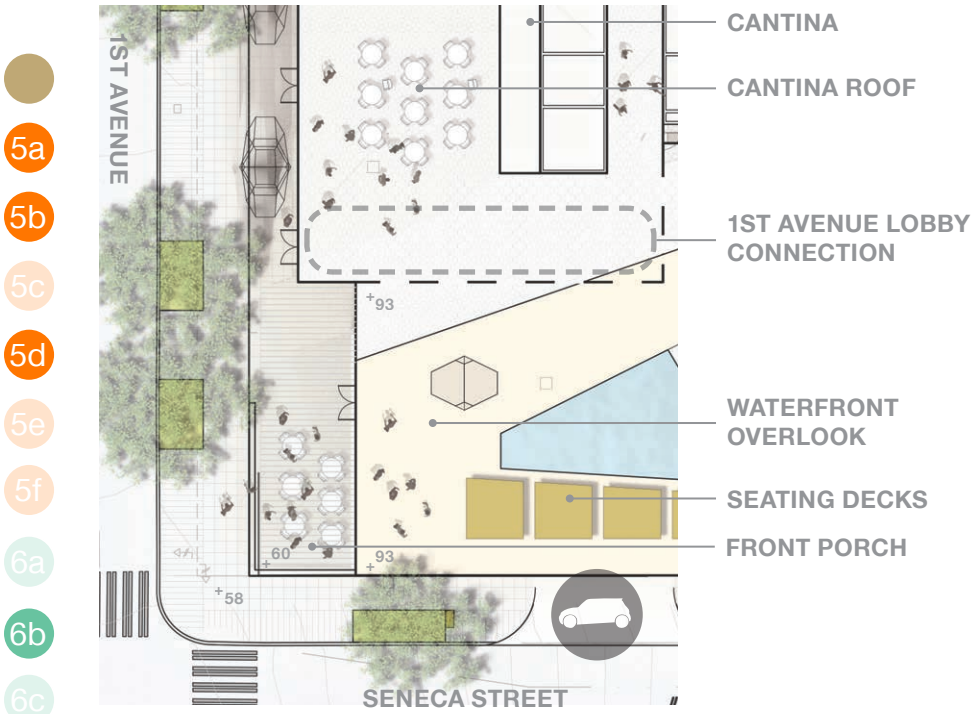


CURRENT: OVERLOOK PLAZA



GROUND PLANE | Southwest Corner

EDG 2 PLAN



- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



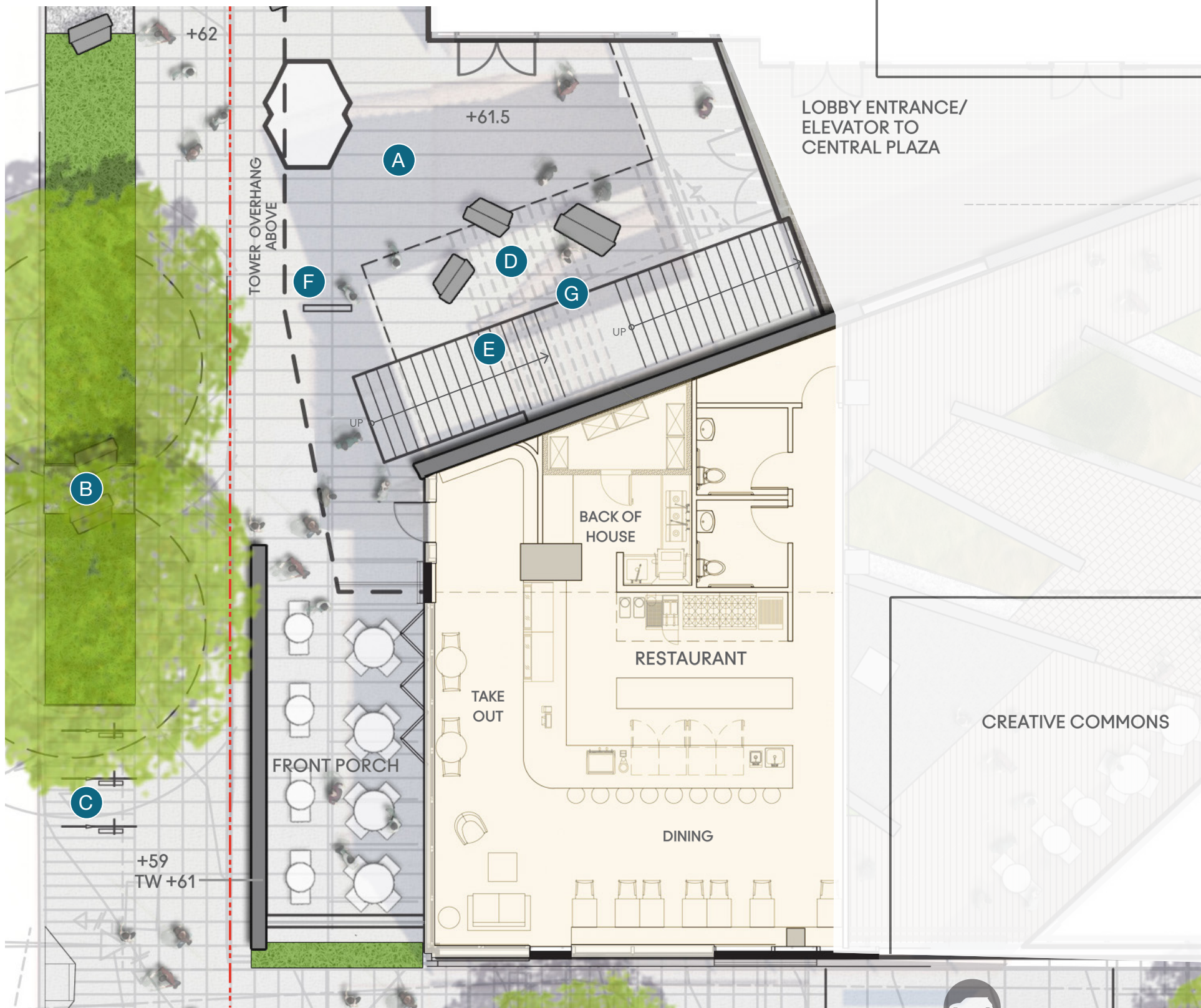
KEY PLAN (NTS)

- A CIP Concrete Paving
- B Gravel Mulch
- C Bike Parking
- D Custom Seat
- E Precast Concrete Stairs
- F Custom Sign Post
- G Guardrail

LEGEND

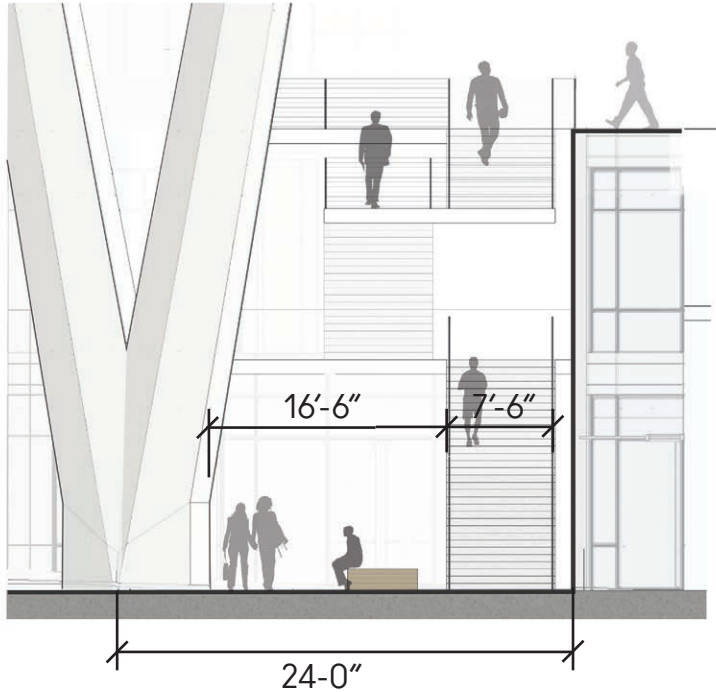
CURRENT PLAN

SEE PAGES 36-40 FOR COMPLETE ARCHITECTURAL FLOOR PLANS



GROUND PLANE | Southwest Corner

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



Section at the Southwest Corner stair (NTS)



EDG 2: VIEW FROM 2ND AVE



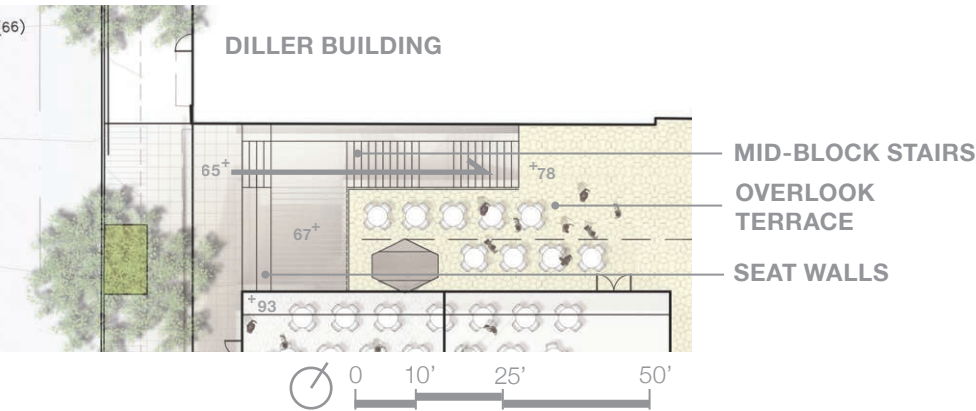
CURRENT: VIEW FROM 2ND AVE.



GROUND PLANE | Mid-Block Stair

EDG 2 PLAN

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d

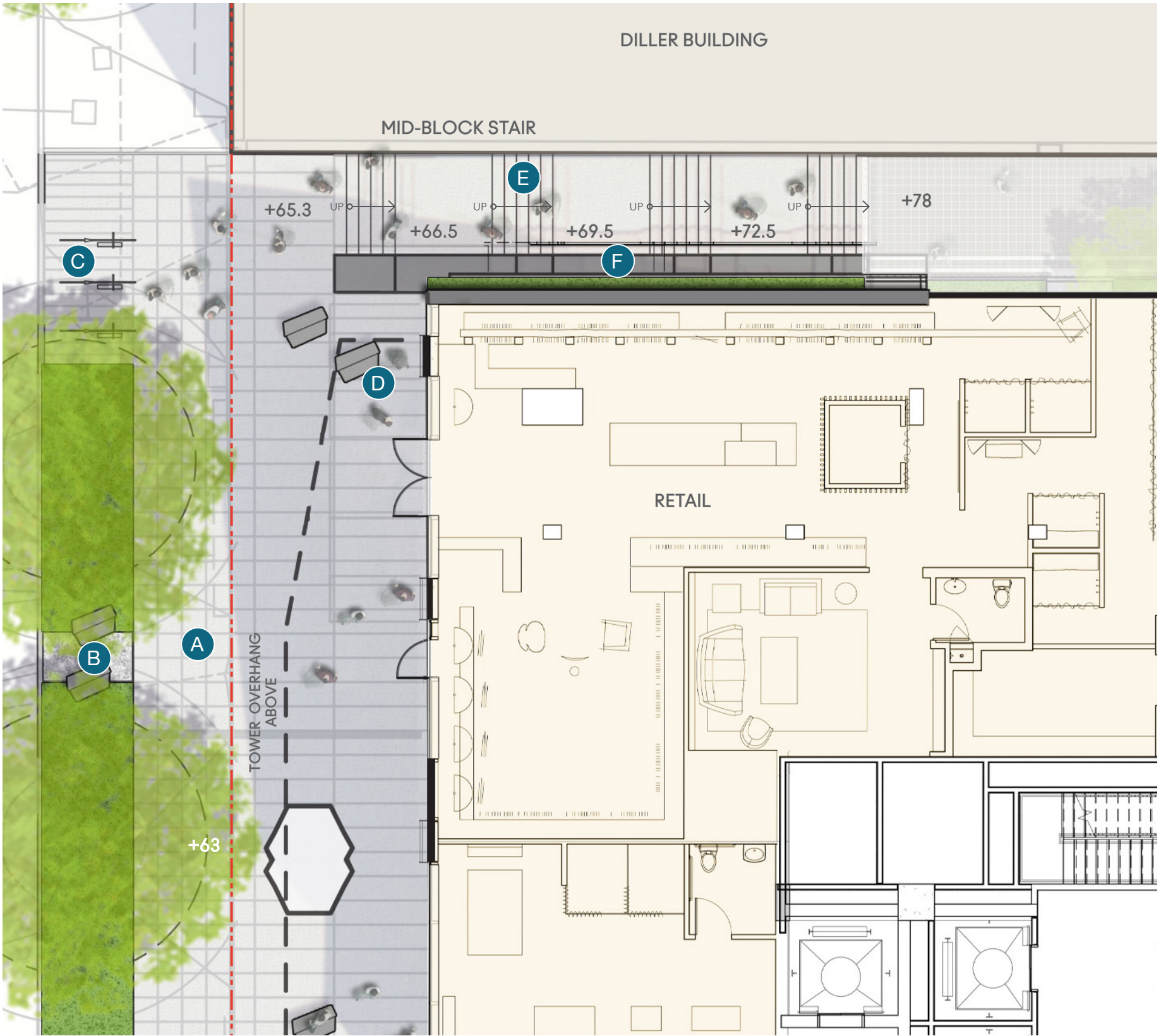


KEY PLAN (NTS)

- A CIP Concrete Paving
- B Gravel Mulch
- C Bike Parking
- D Custom Seat
- E Precast Concrete Stairs
- F Water Runnel

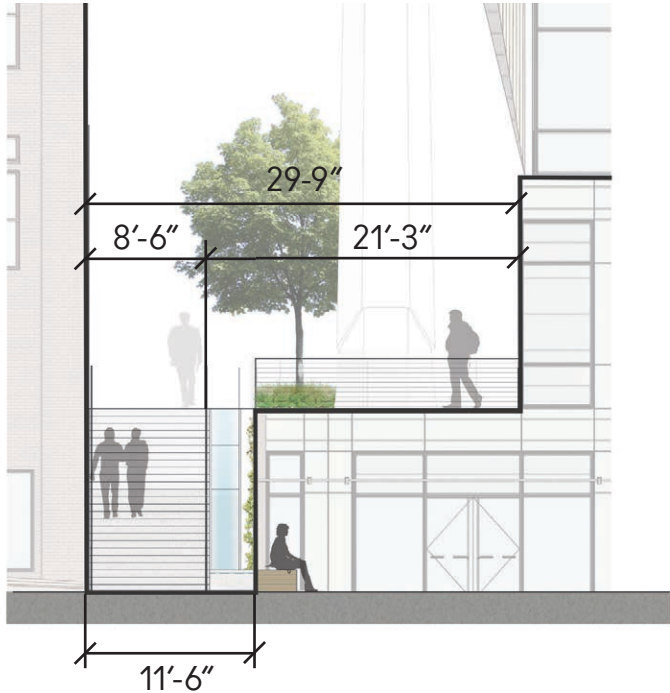
LEGEND

CURRENT PLAN SEE PAGES 36-40 FOR COMPLETE ARCHITECTURAL FLOOR PLANS



GROUND PLANE | Mid-Block Stair

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



Section at 1st Avenue Mid-Block Stair (NTS)



EDG 2: MID-BLOCK STAIR



CURRENT: MID-BLOCK STAIR



EDG 2: CENTRAL PLAZA



CURRENT: CENTRAL PLAZA



- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d

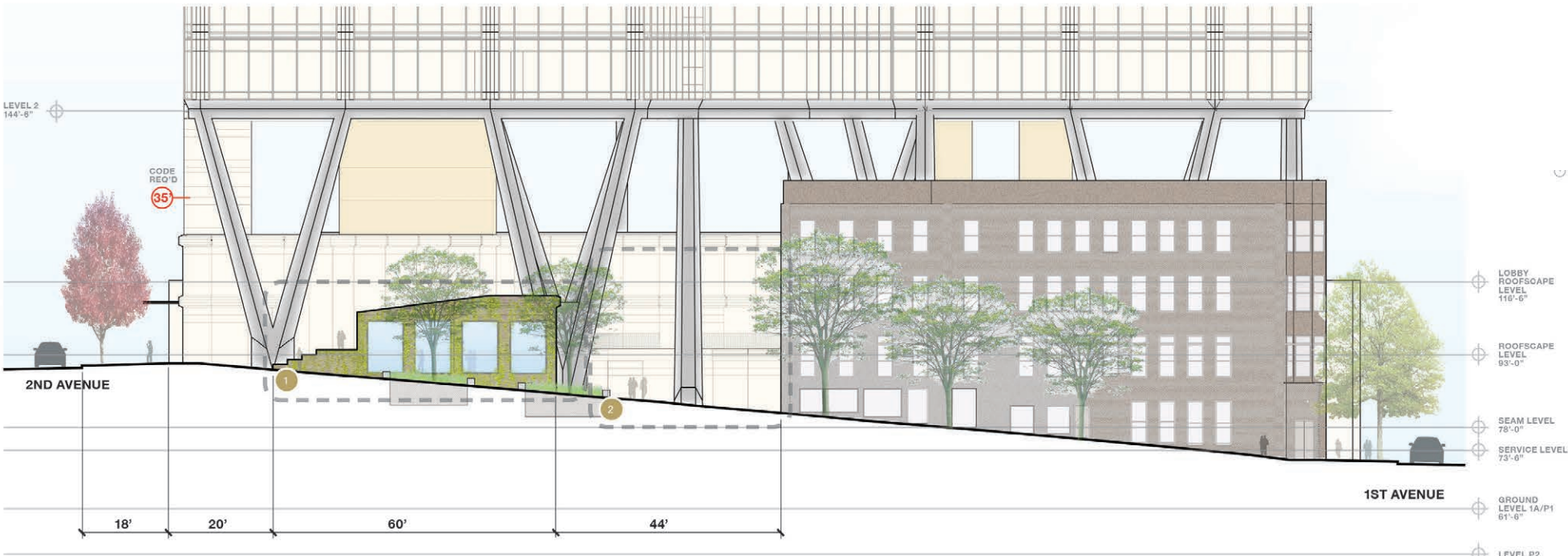


4

Podium, Plans & Elevations

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d

EDG 2



KEY PLAN (NTS)

MATERIAL LEGEND

- 1. PRECAST CONCRETE
- 2. CAST-IN-PLACE CONCRETE
- 3. VISION GLASS
- 4. TRANSLUCENT GLASS / SPANDREL
- 5. METAL PANEL
- 6. TERRA COTTA PANEL
- 7. STANDING SEAM METAL ROOF
- 8. WOOD

CURRENT

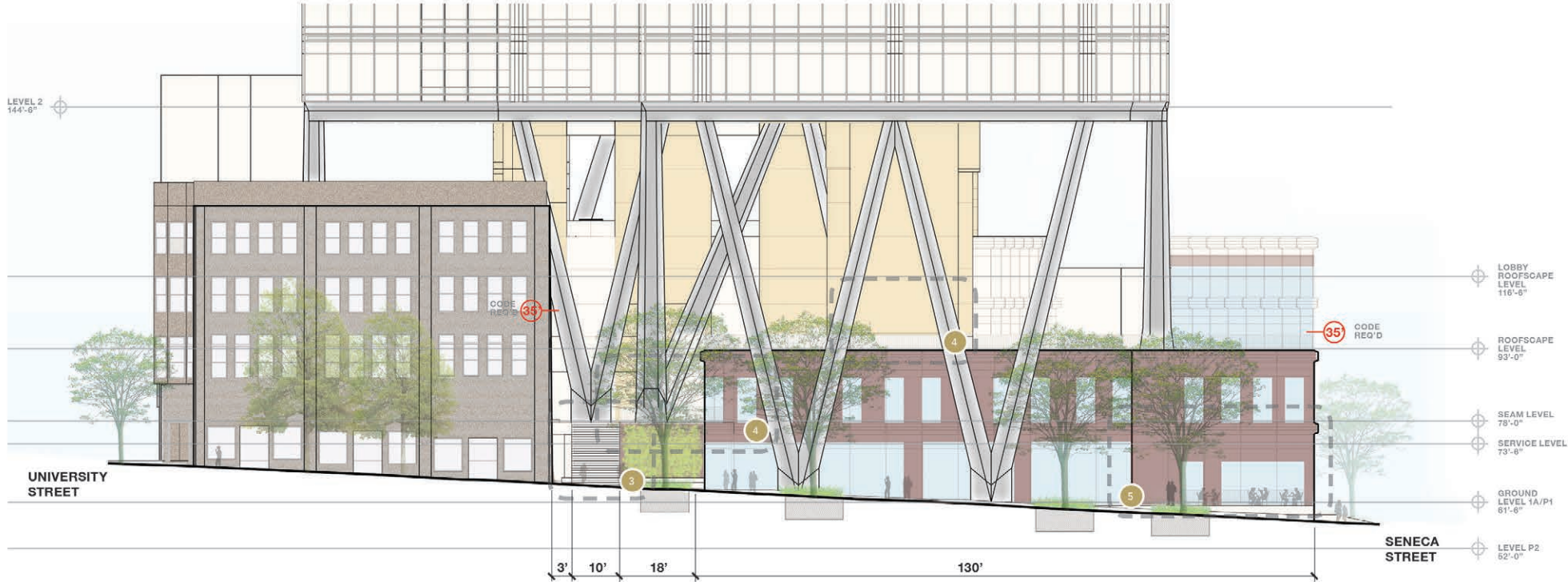


PODIUM, PLANS + ELEVATIONS | 1st Avenue

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d

EDG 2

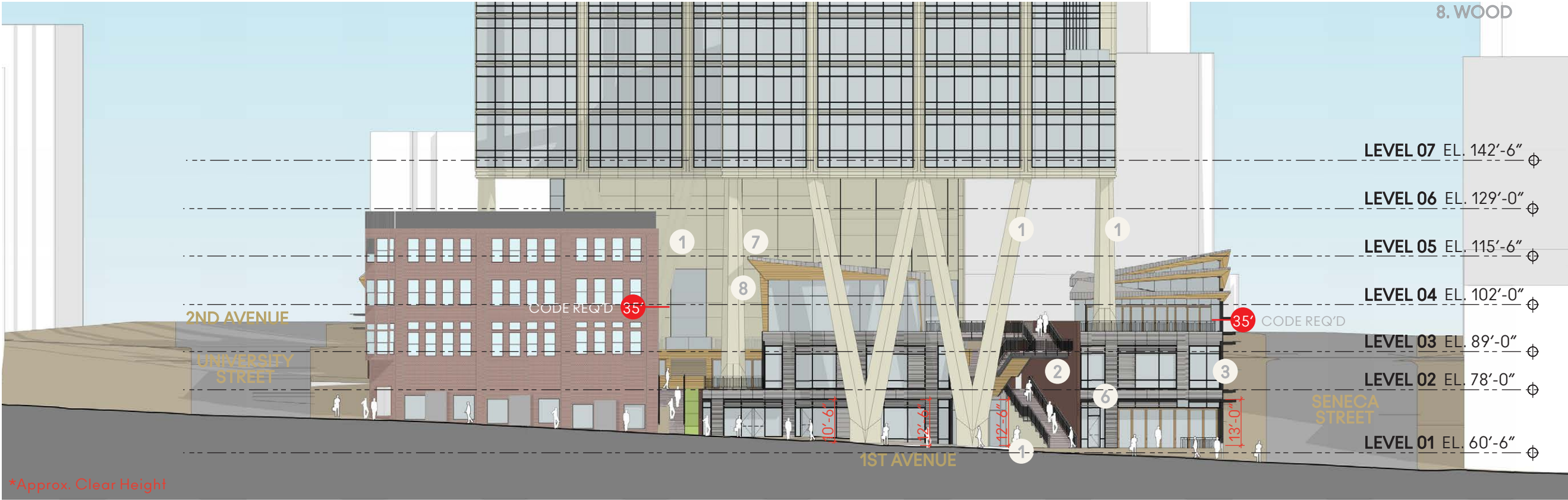
CURRENT



KEY PLAN (NTS)

MATERIAL LEGEND

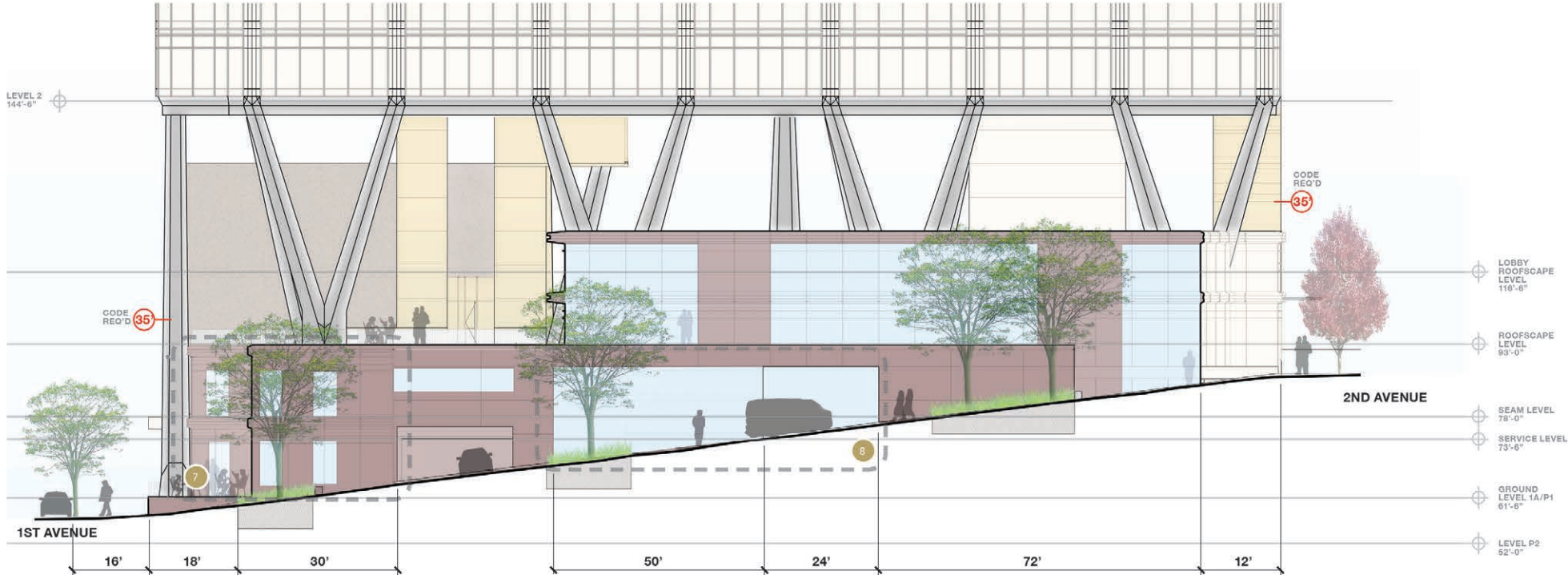
- 1. PRECAST CONCRETE
- 2. CAST-IN-PLACE CONCRETE
- 3. VISION GLASS
- 4. TRANSLUCENT GLASS / SPANDREL
- 5. METAL PANEL
- 6. TERRA COTTA PANEL
- 7. STANDING SEAM METAL ROOF
- 8. WOOD



- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d

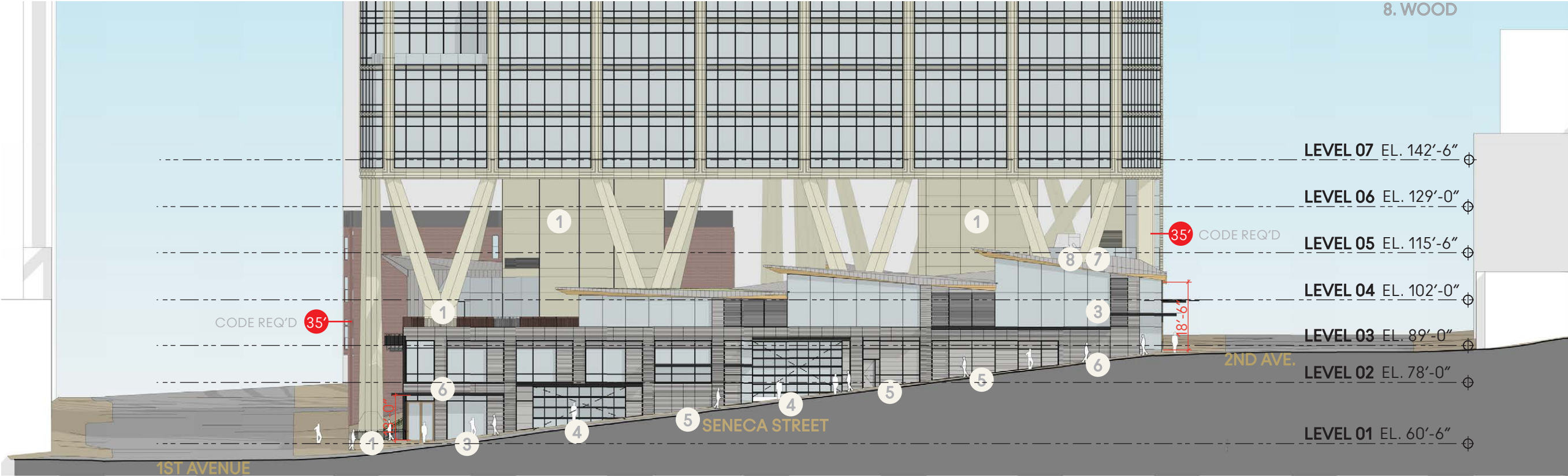
EDG 2

CURRENT



KEY PLAN (NTS)

- MATERIAL LEGEND**
- 1. PRECAST CONCRETE
 - 2. CAST-IN-PLACE CONCRETE
 - 3. VISION GLASS
 - 4. TRANSLUCENT GLASS / SPANDREL
 - 5. METAL PANEL
 - 6. TERRA COTTA PANEL
 - 7. STANDING SEAM METAL ROOF
 - 8. WOOD

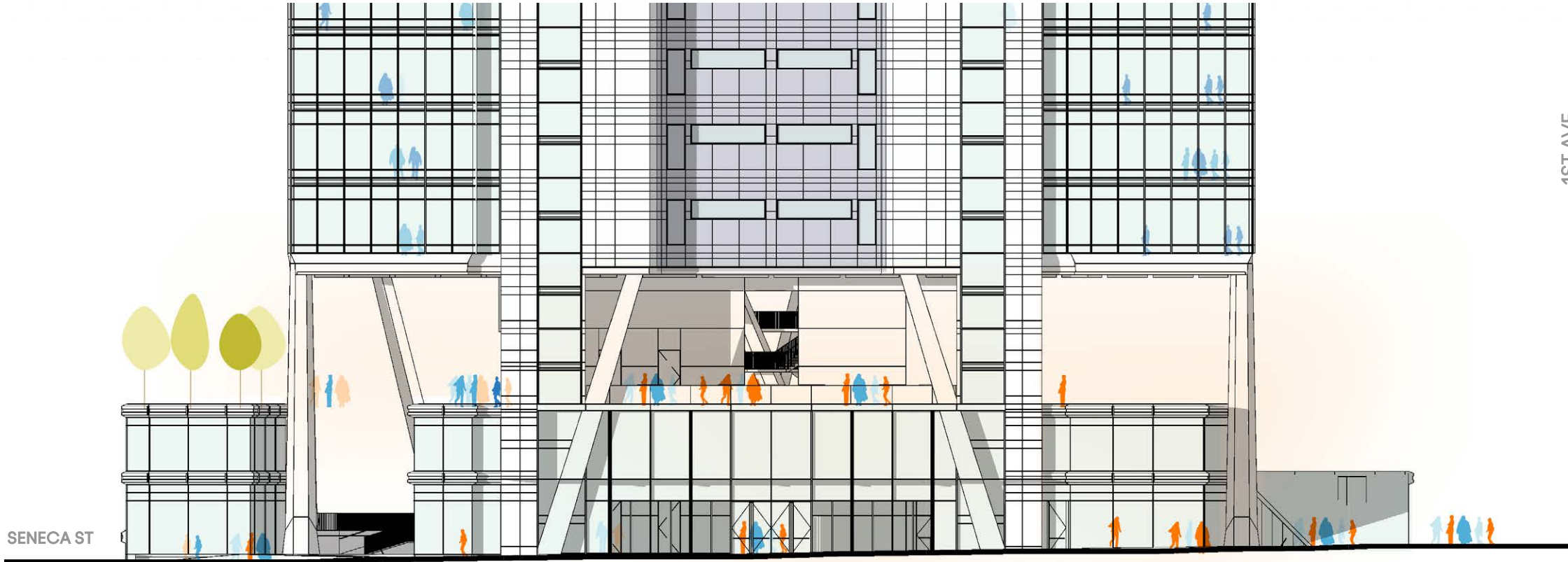


PODIUM, PLANS + ELEVATIONS | 2nd Avenue

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d

EDG 2

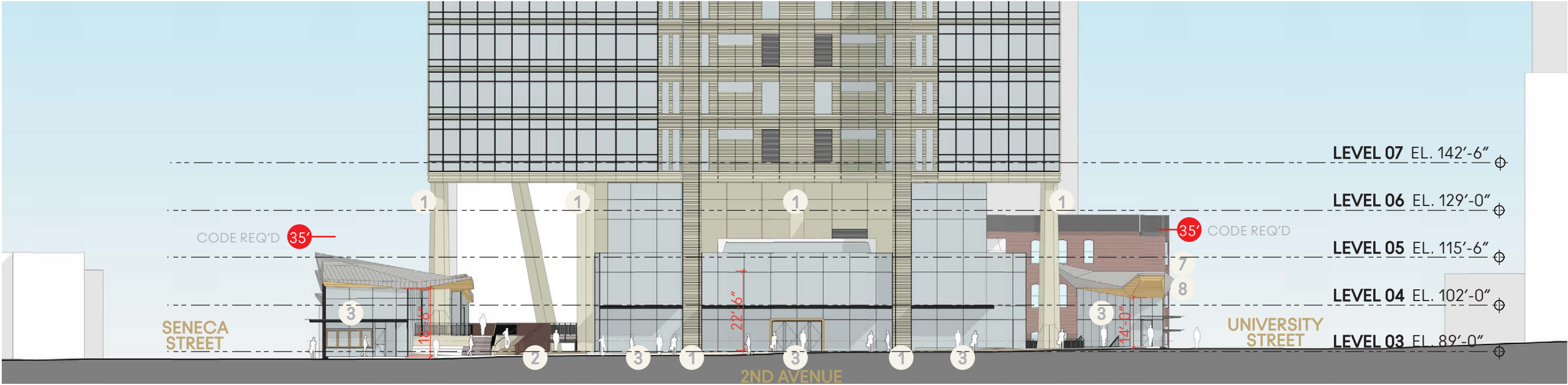
CURRENT



KEY PLAN (NTS)

MATERIAL LEGEND

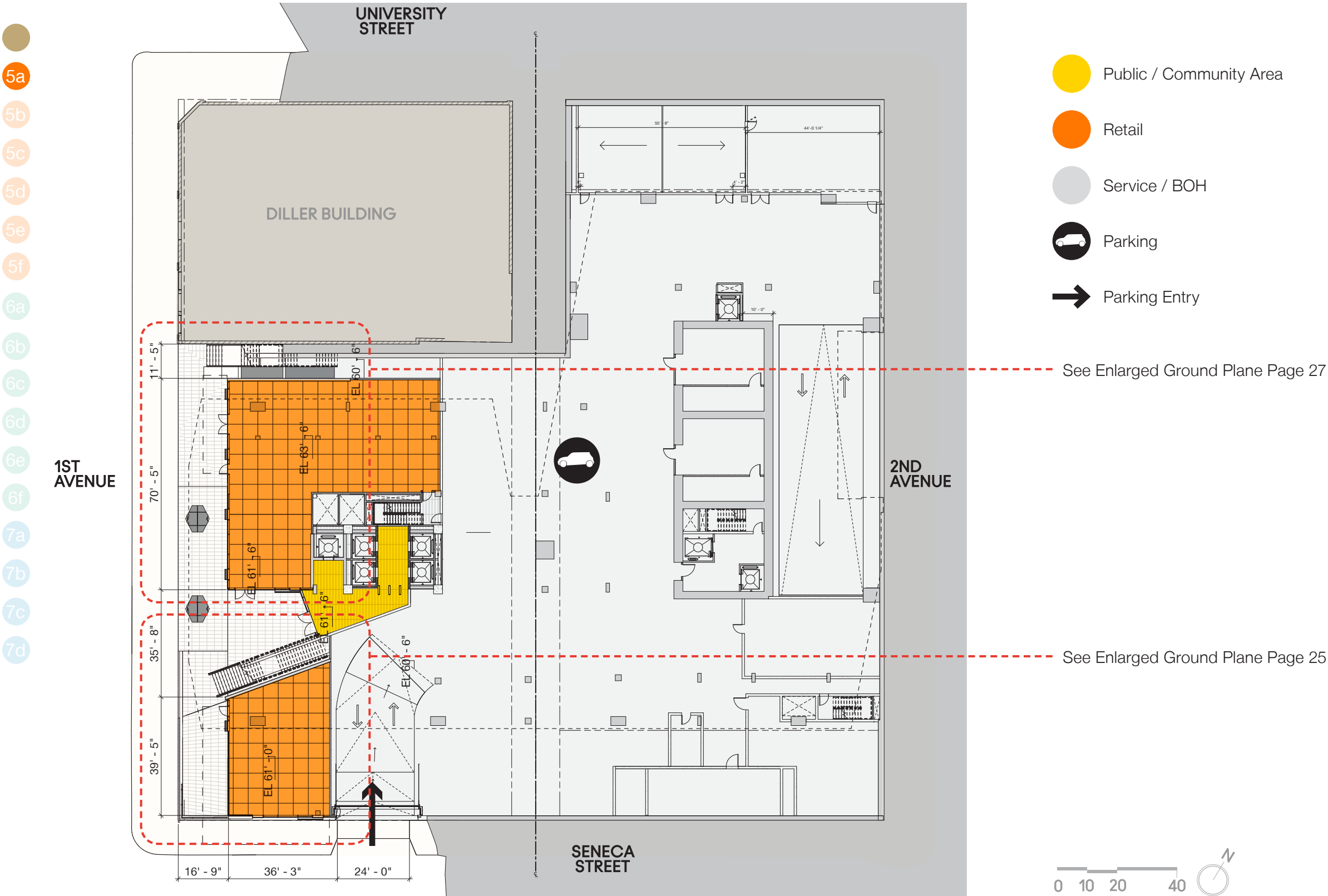
- 1. PRECAST CONCRETE
- 2. CAST-IN-PLACE CONCRETE
- 3. VISION GLASS
- 4. TRANSLUCENT GLASS / SPANDREL
- 5. METAL PANEL
- 6. TERRA COTTA PANEL
- 7. STANDING SEAM METAL ROOF
- 8. WOOD



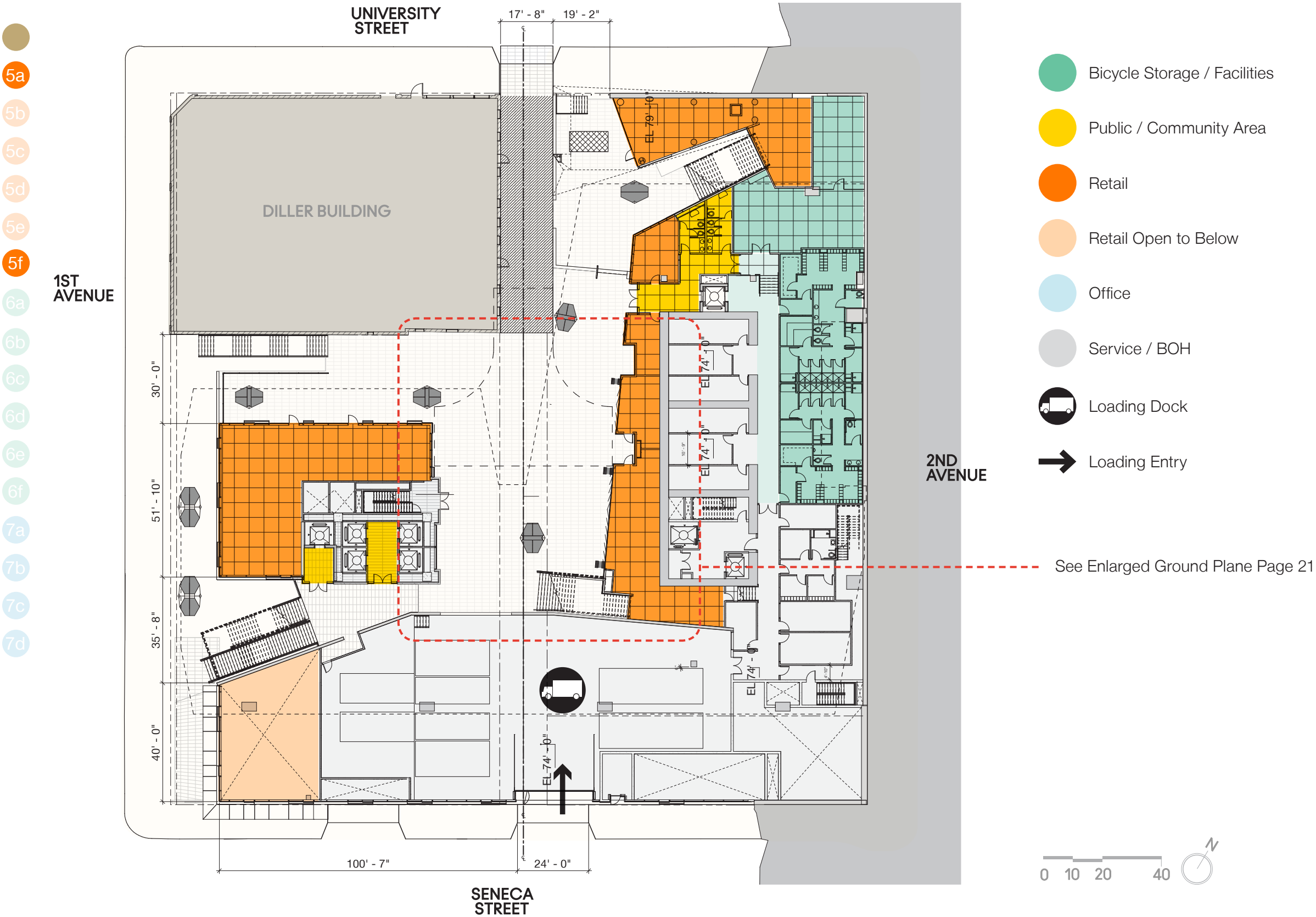
CURRENT PLAN



CURRENT PLAN



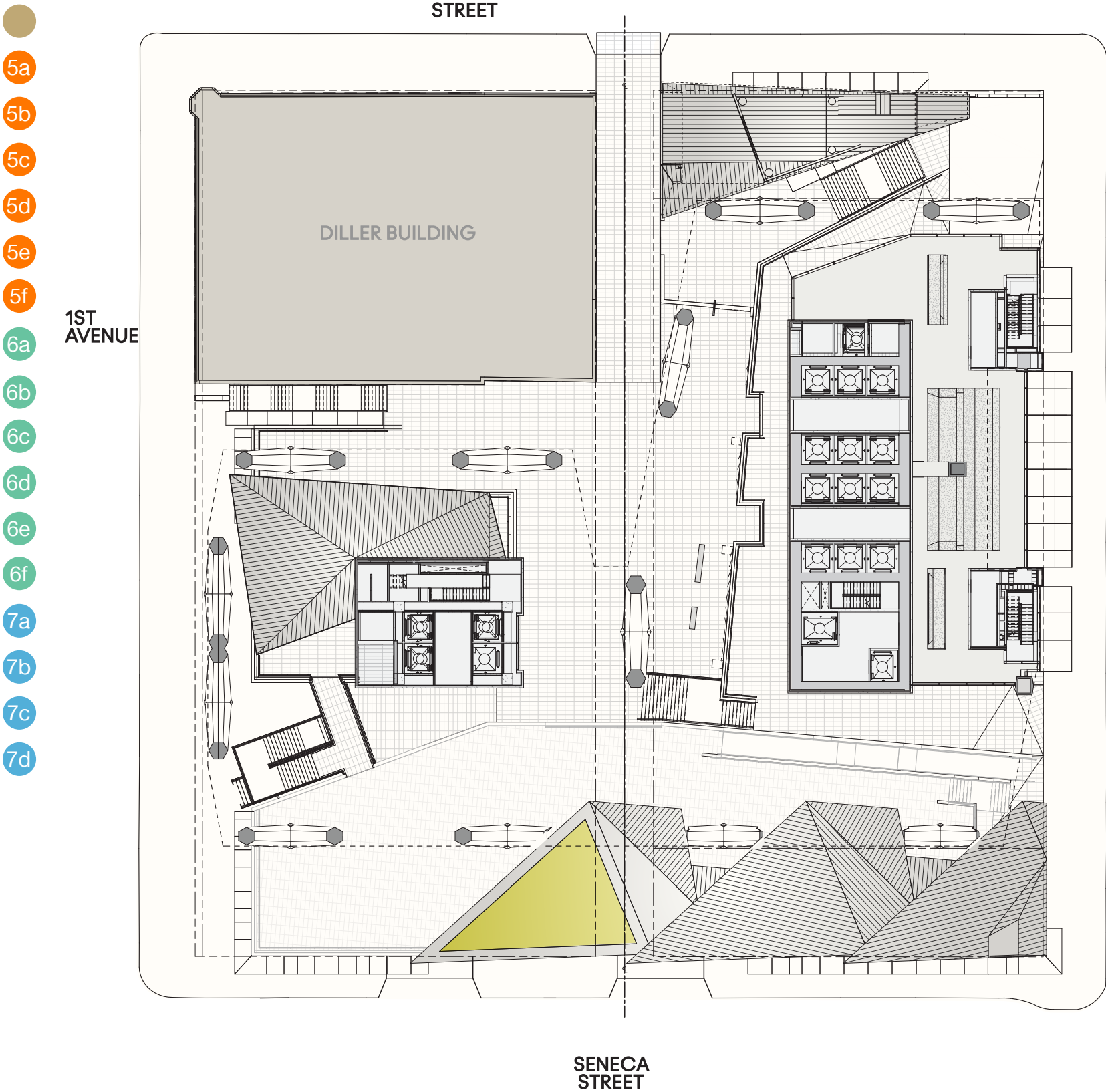
CURRENT PLAN



CURRENT PLAN



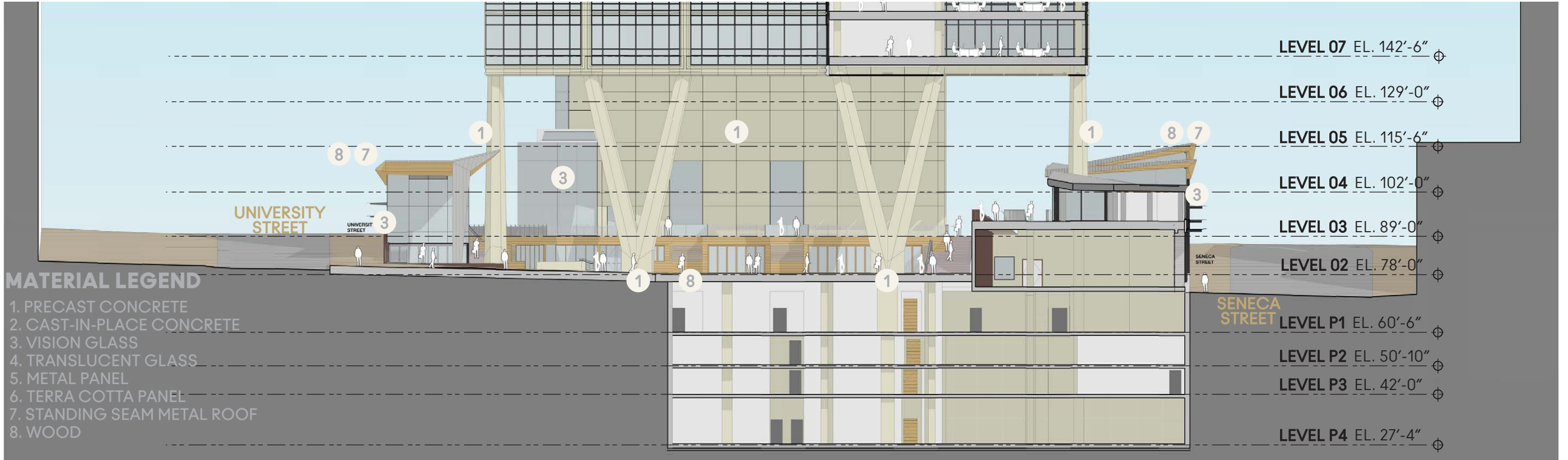
CURRENT PLAN



- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d

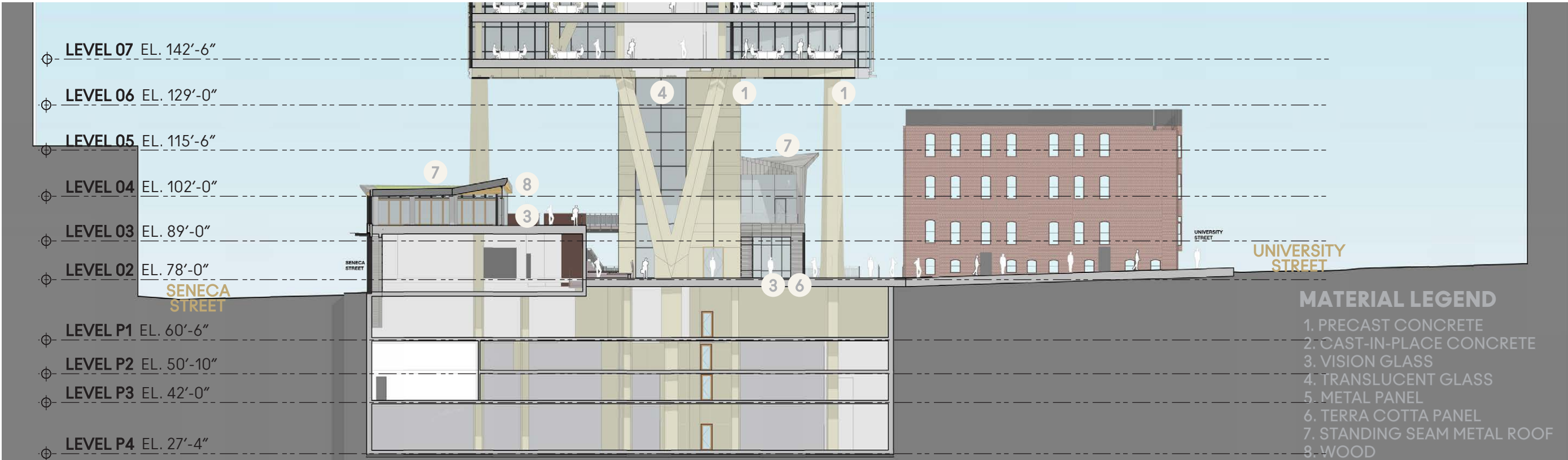


East Core Section

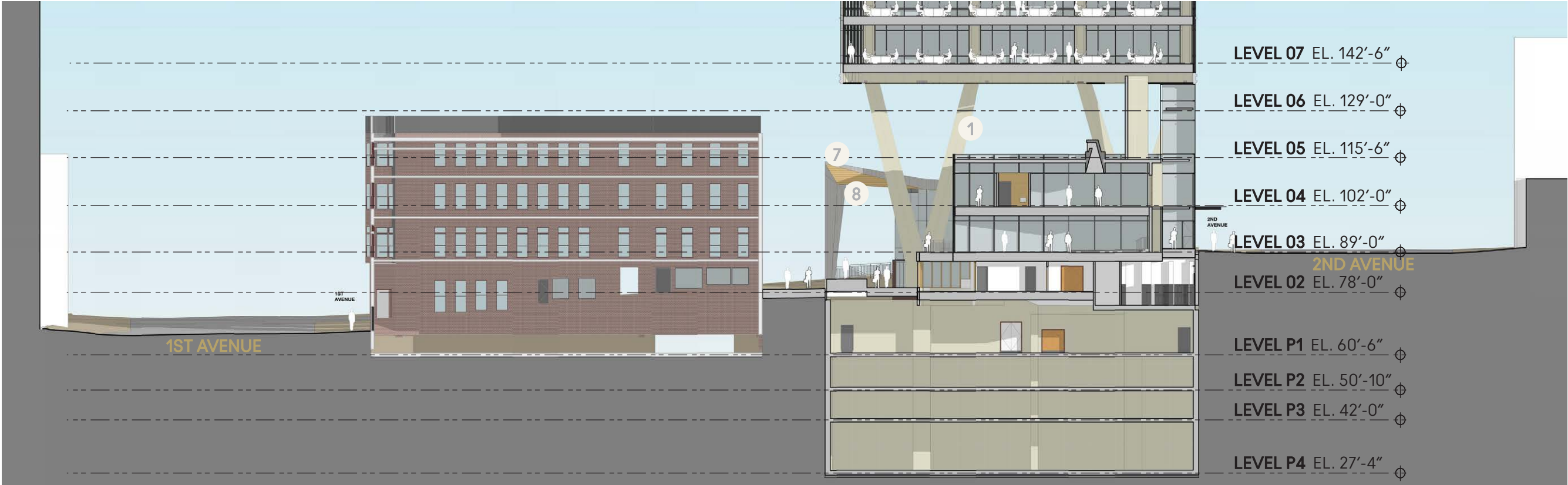


Plaza Section - Facing East

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



Plaza Section - Facing West

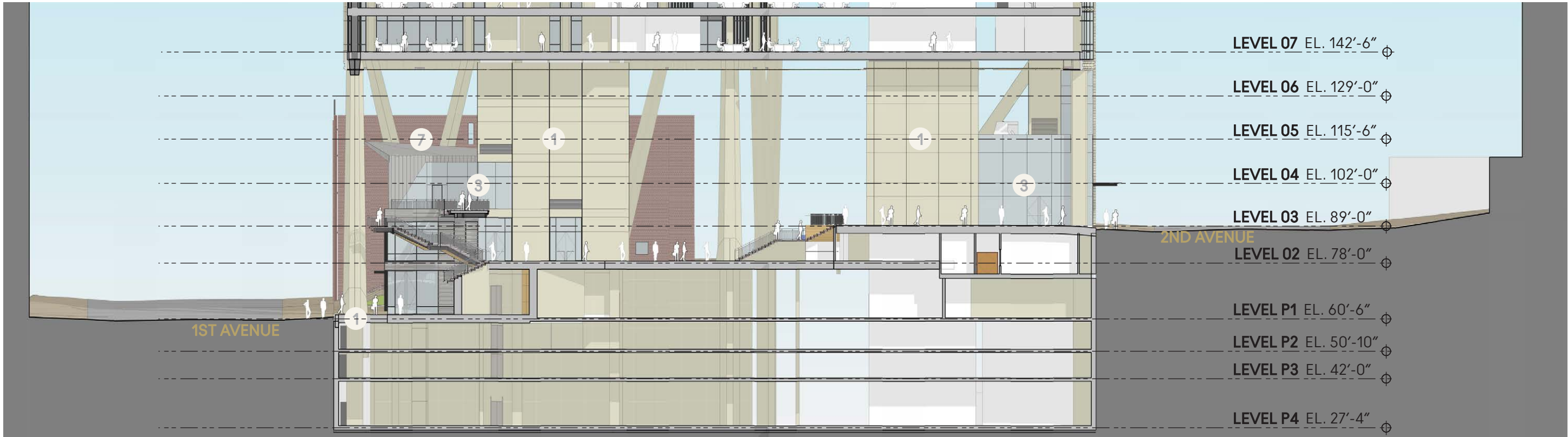


Lobby / Cafe Section - Facing North

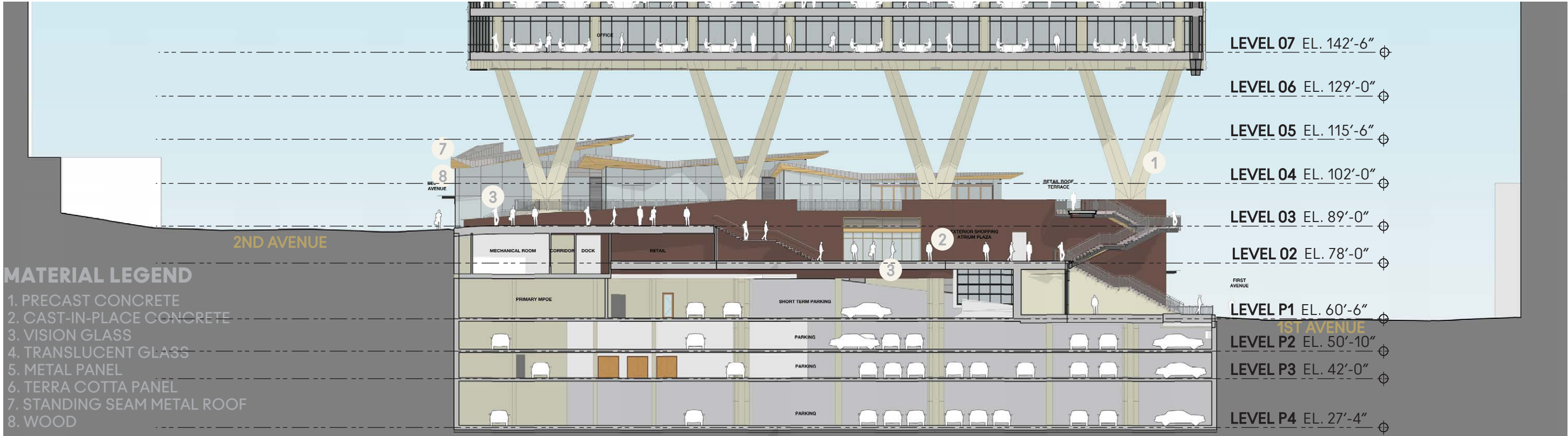
CURRENT

PODIUM, PLANS + ELEVATIONS | Site Sections

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



Plaza Section - Facing North

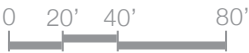
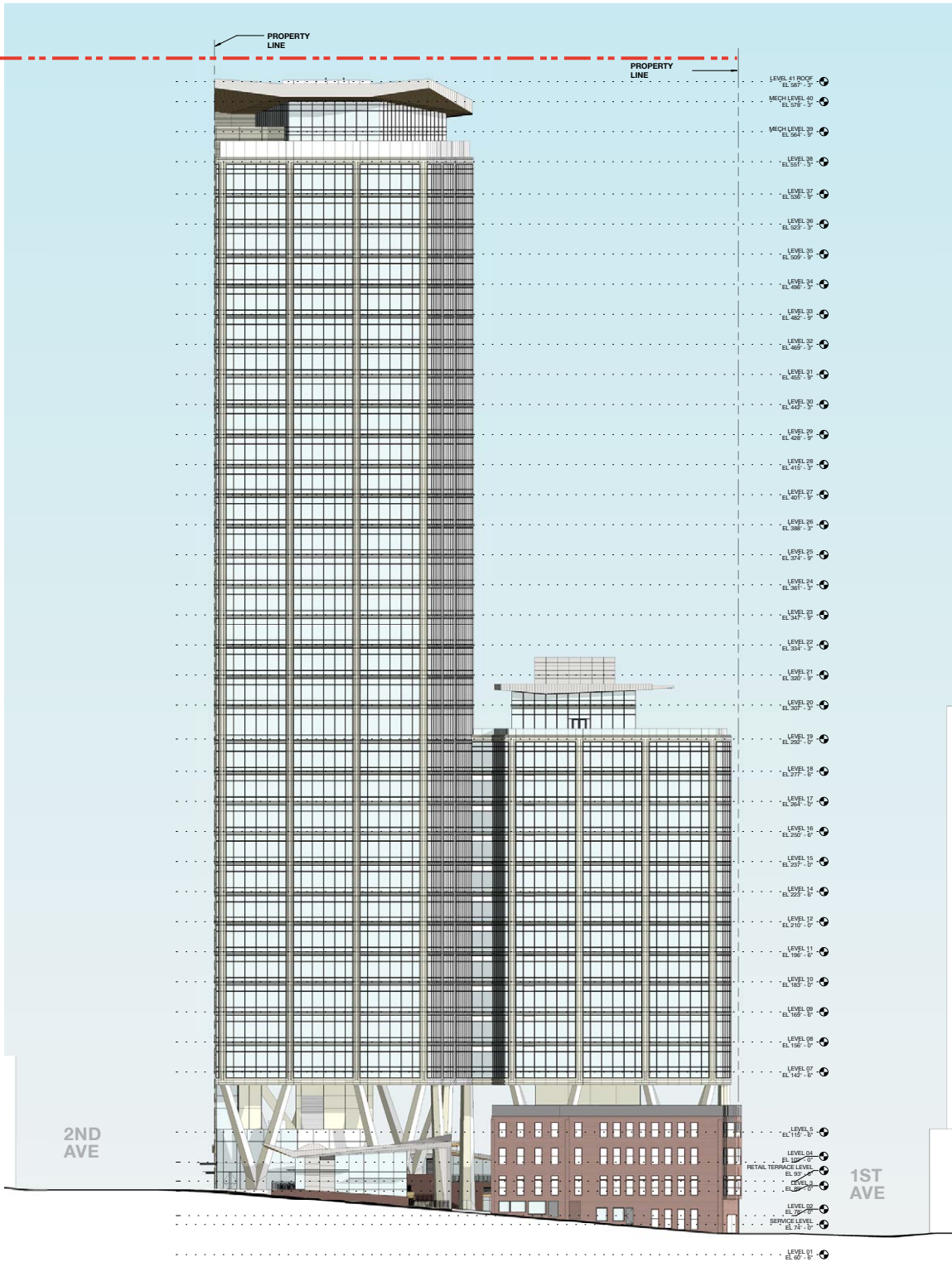
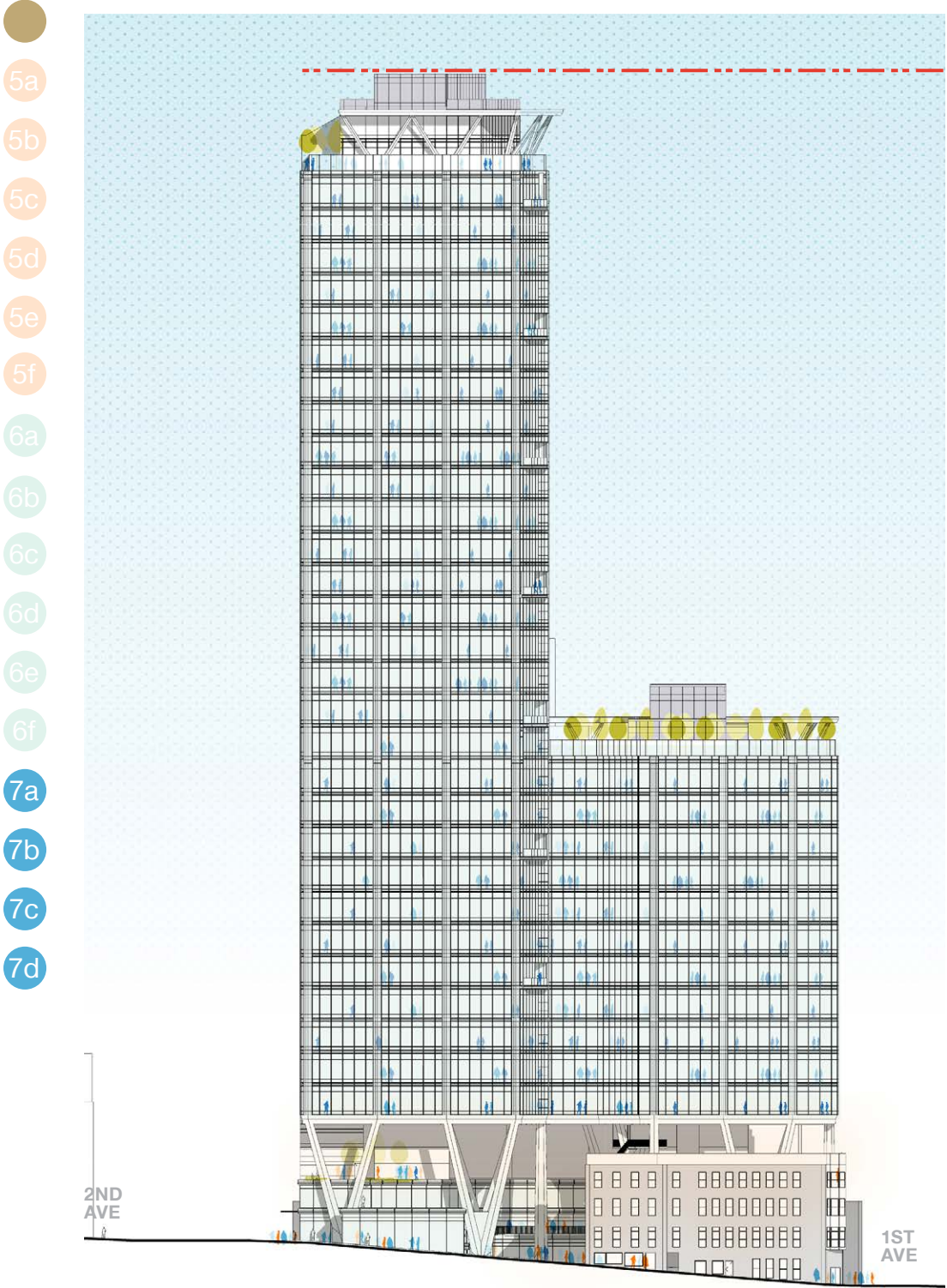


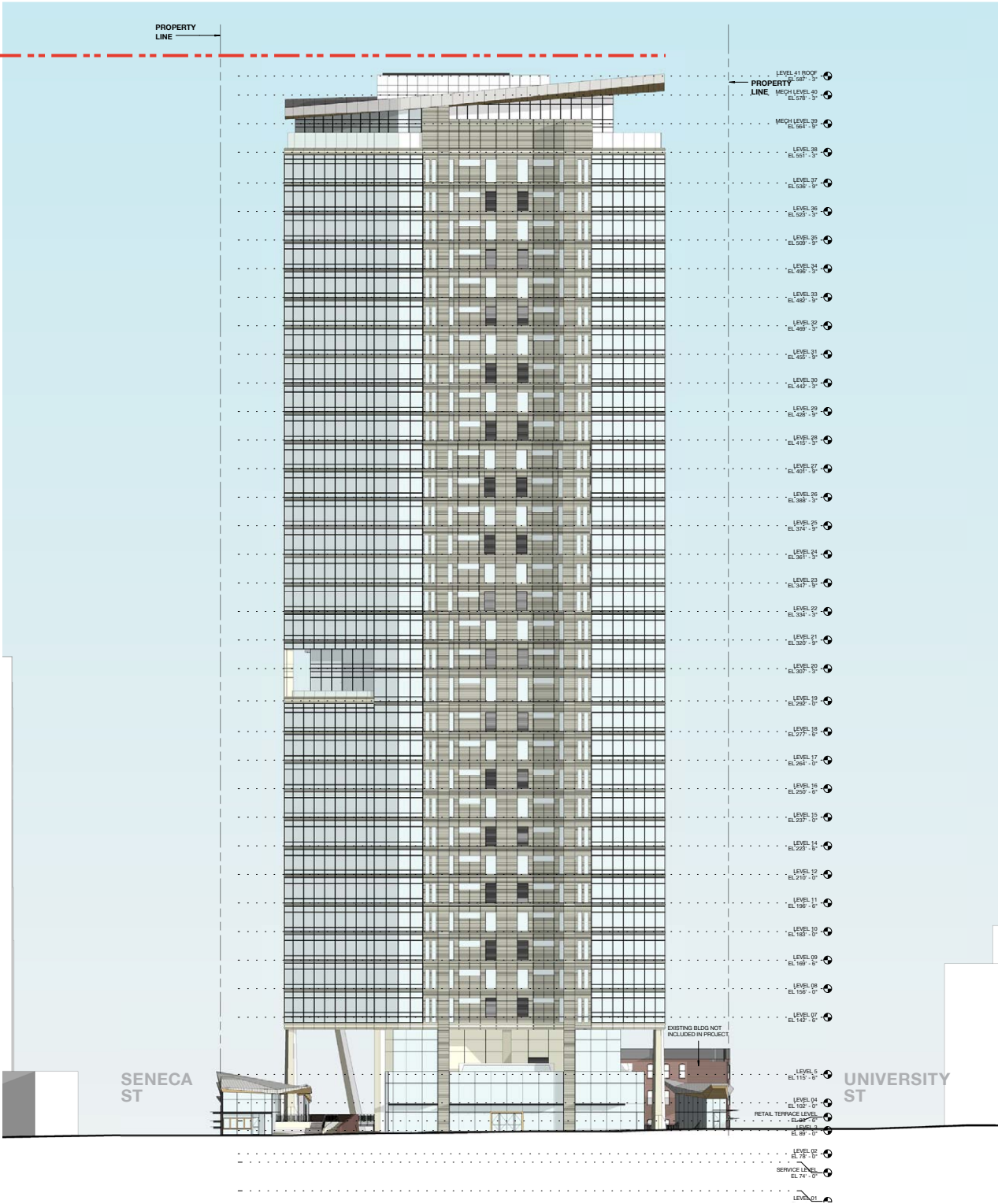
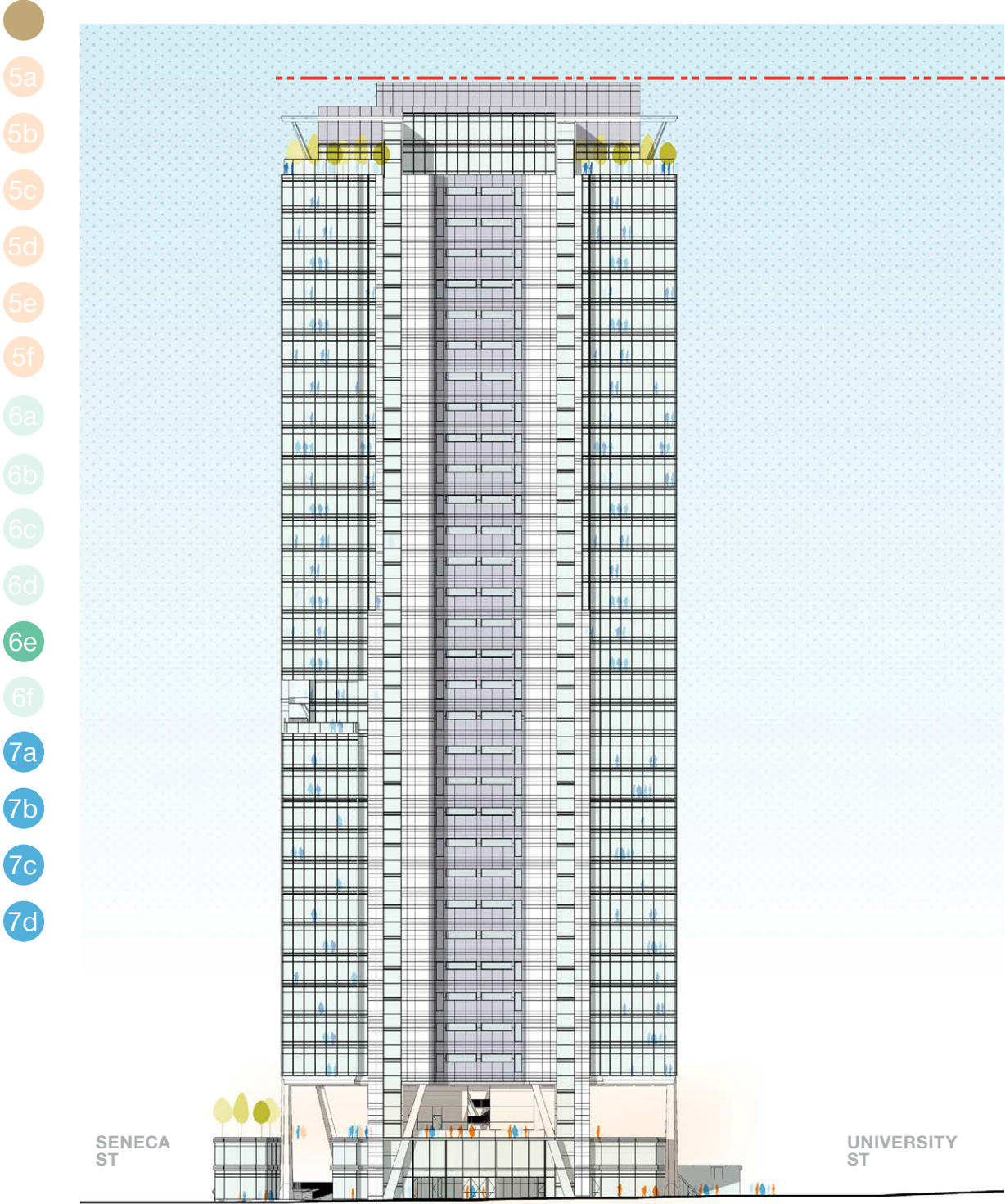
Plaza Section - Facing South

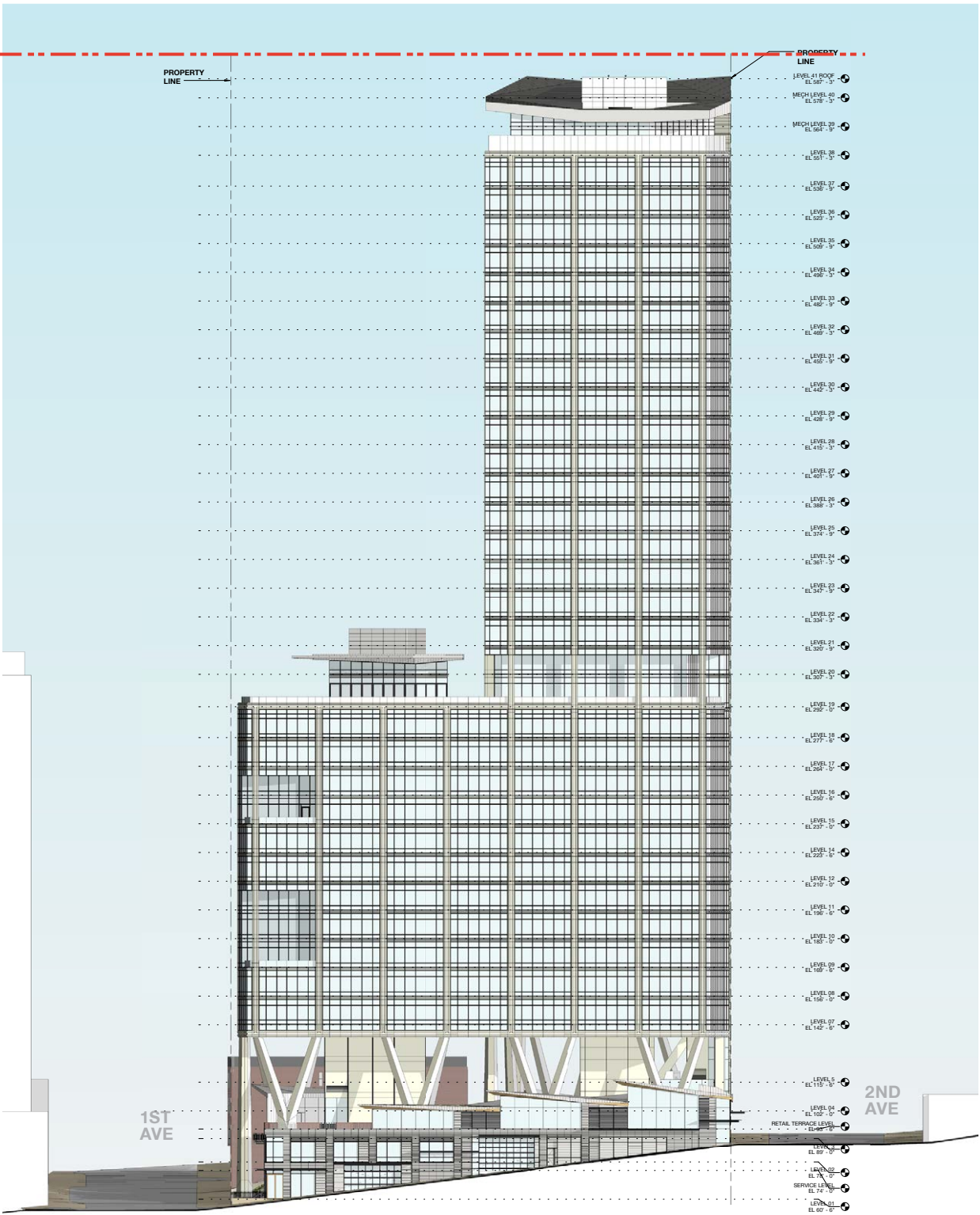
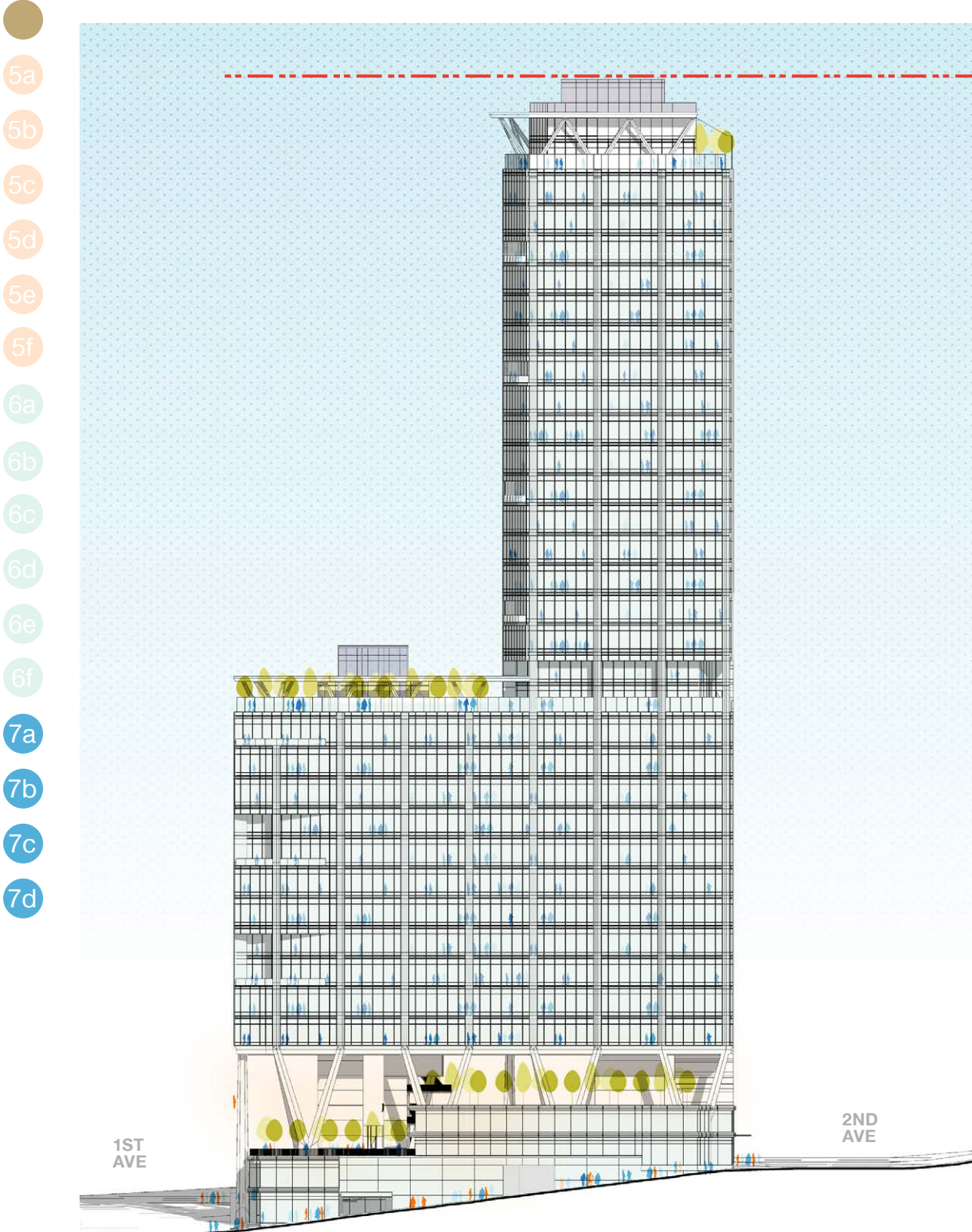
PODIUM, PLANS + ELEVATIONS | University Street - North Elevation

EDG 2 ELEVATION

CURRENT ELEVATION

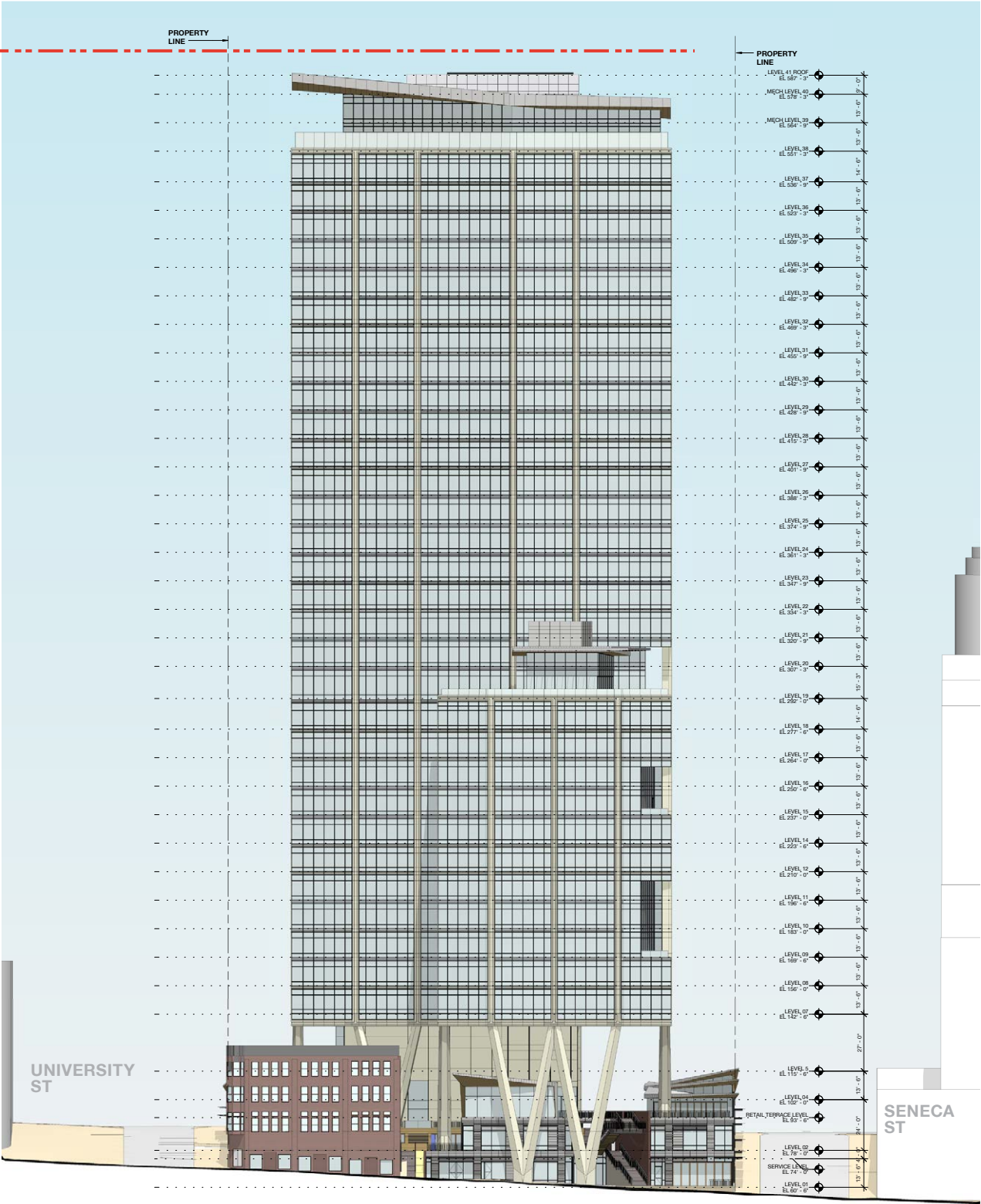
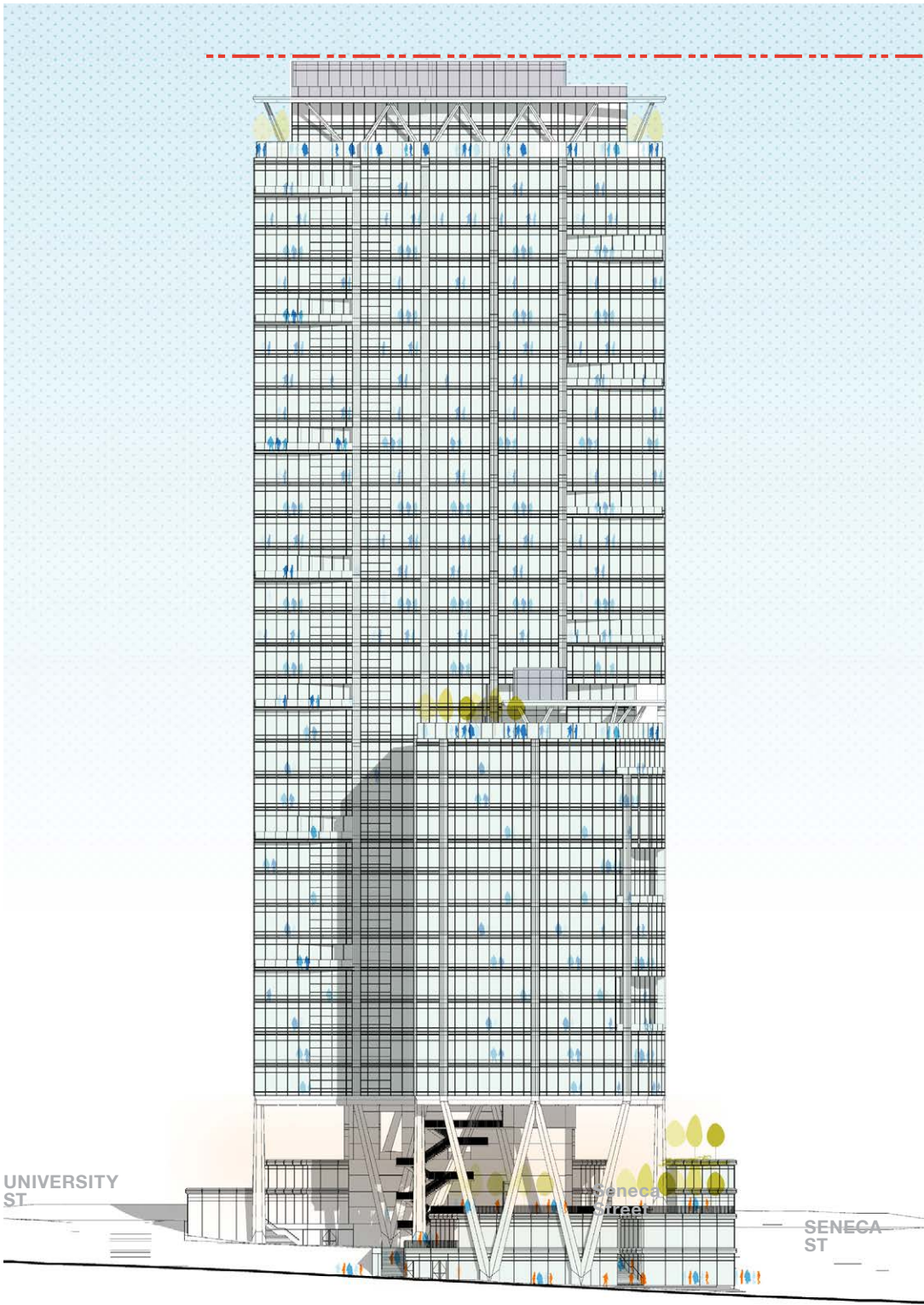


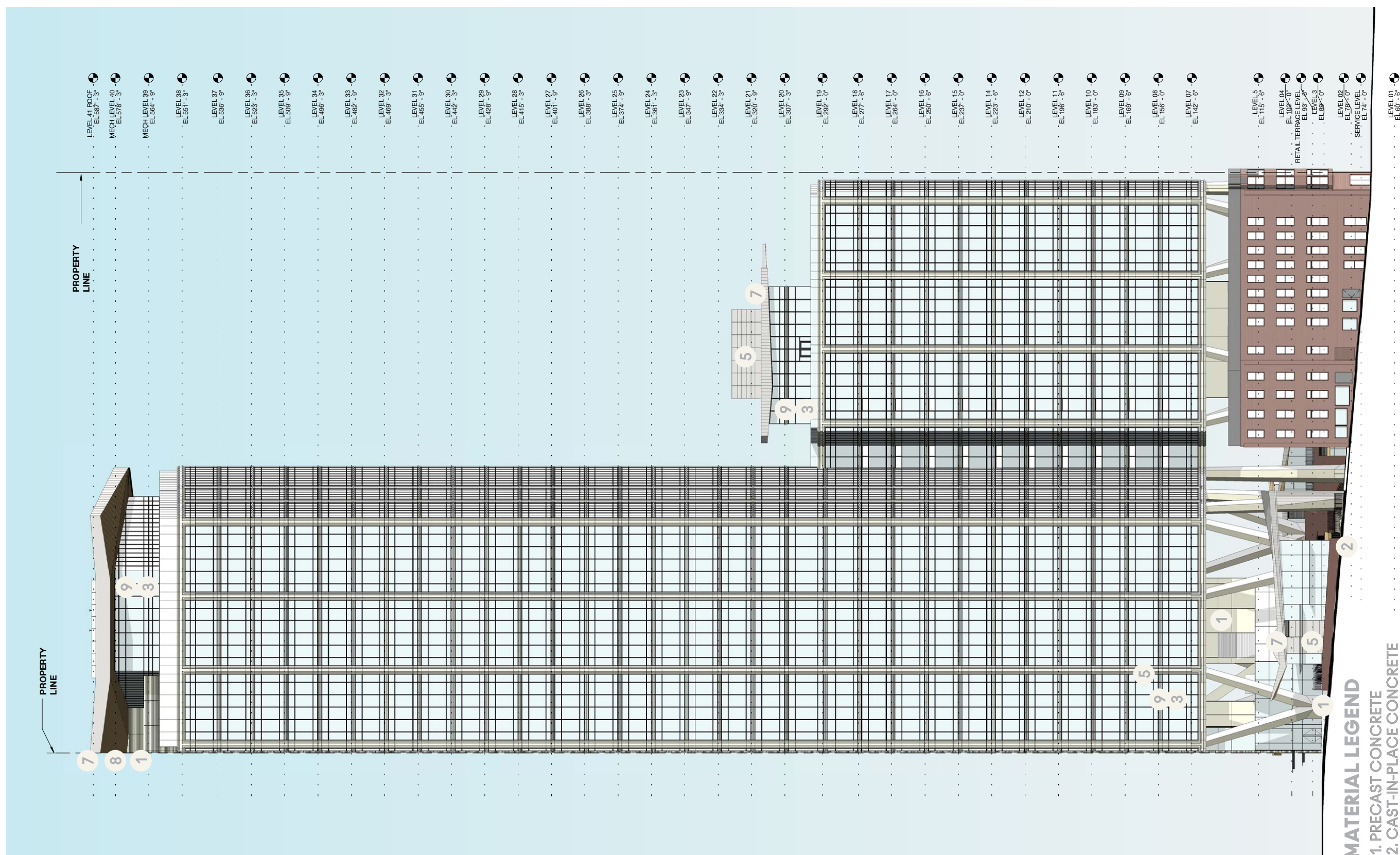




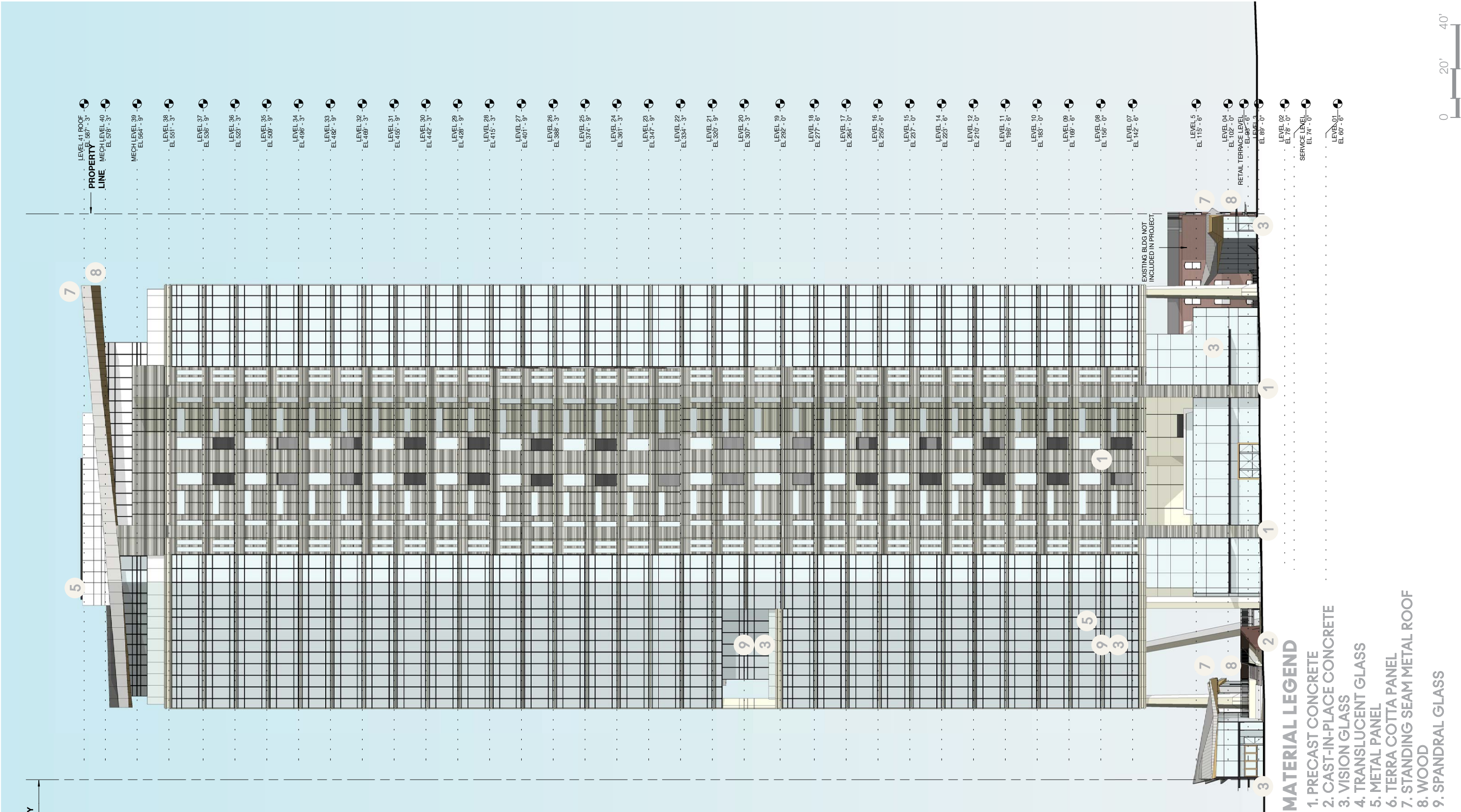
0 20' 40' 80'

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d

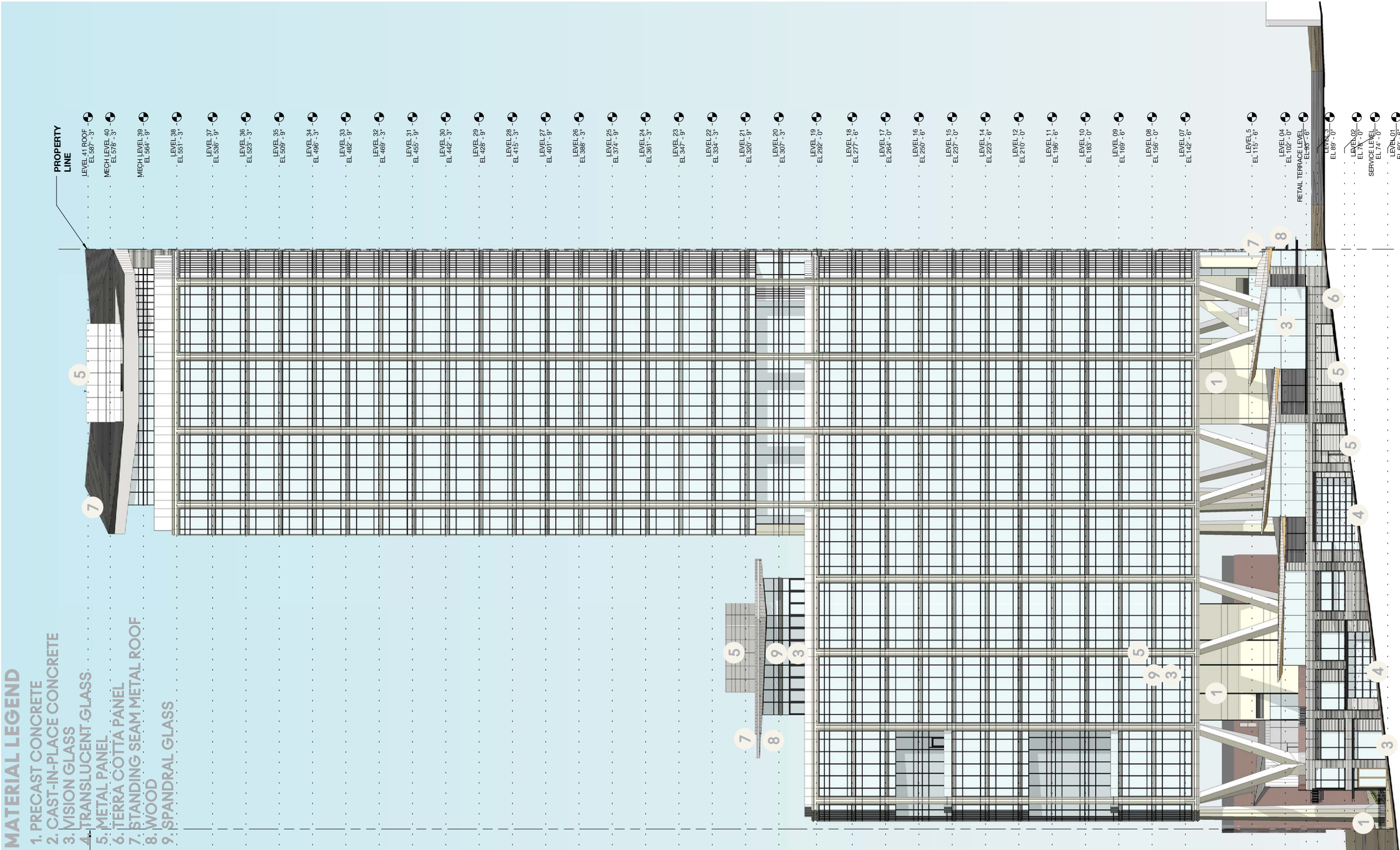




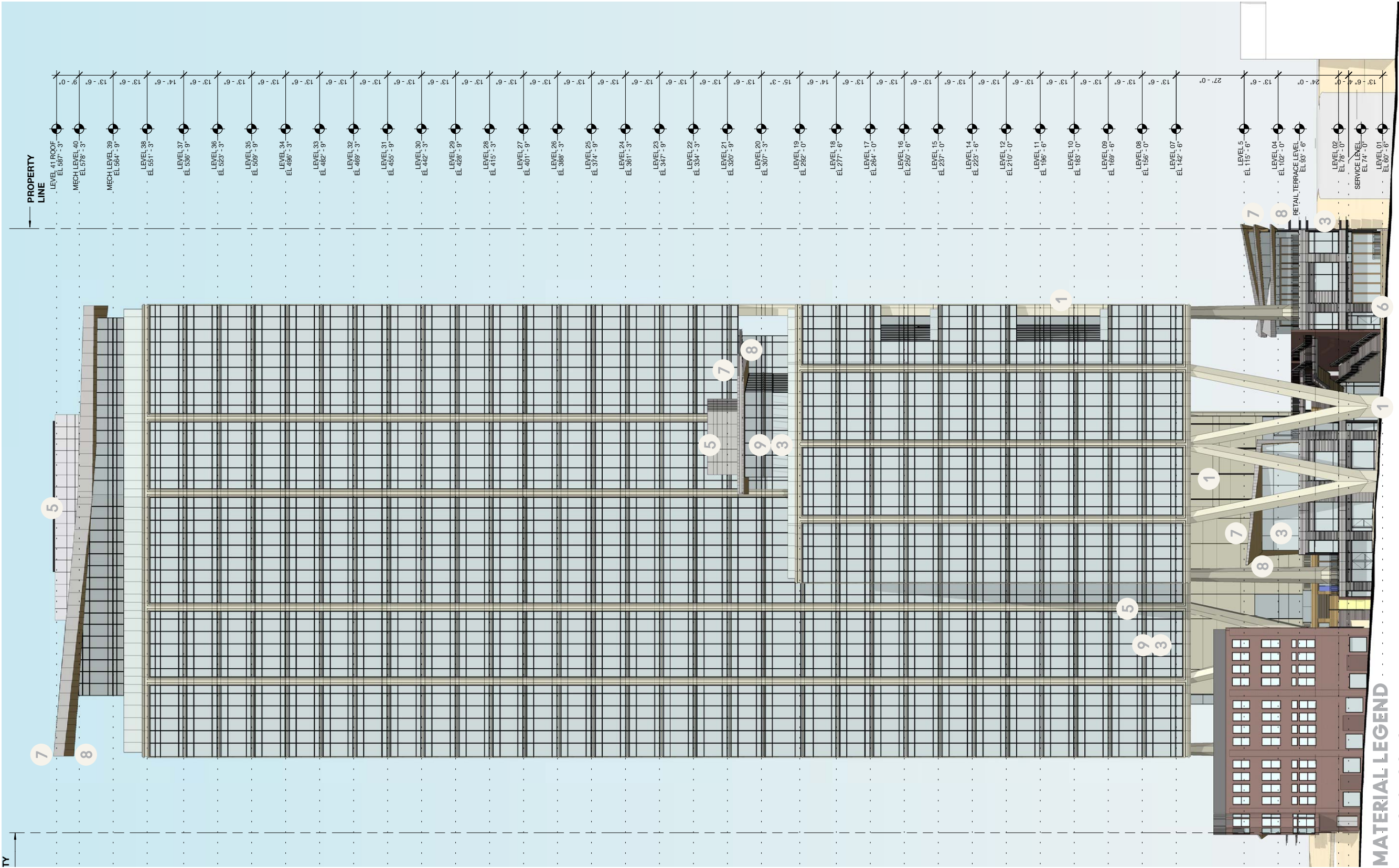
- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d

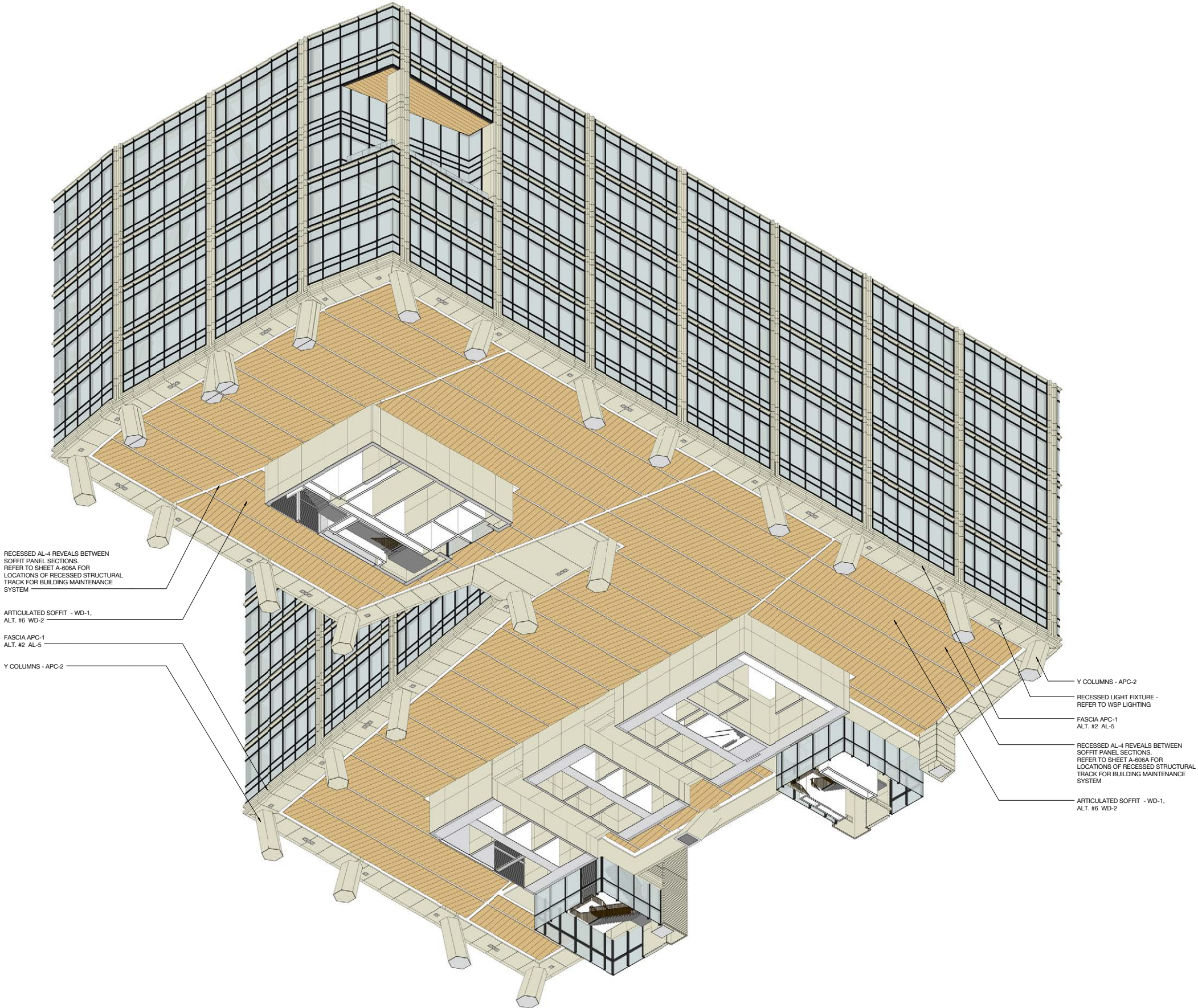


MATERIAL LEGEND

- 1. PRECAST CONCRETE
- 2. CAST-IN-PLACE CONCRETE
- 3. VISION GLASS
- 4. TRANSLUCENT GLASS
- 5. METAL PANEL
- 6. TERRA COTTA PANEL
- 7. STANDING SEAM METAL ROOF
- 8. WOOD
- 9. SPANDRAL GLASS

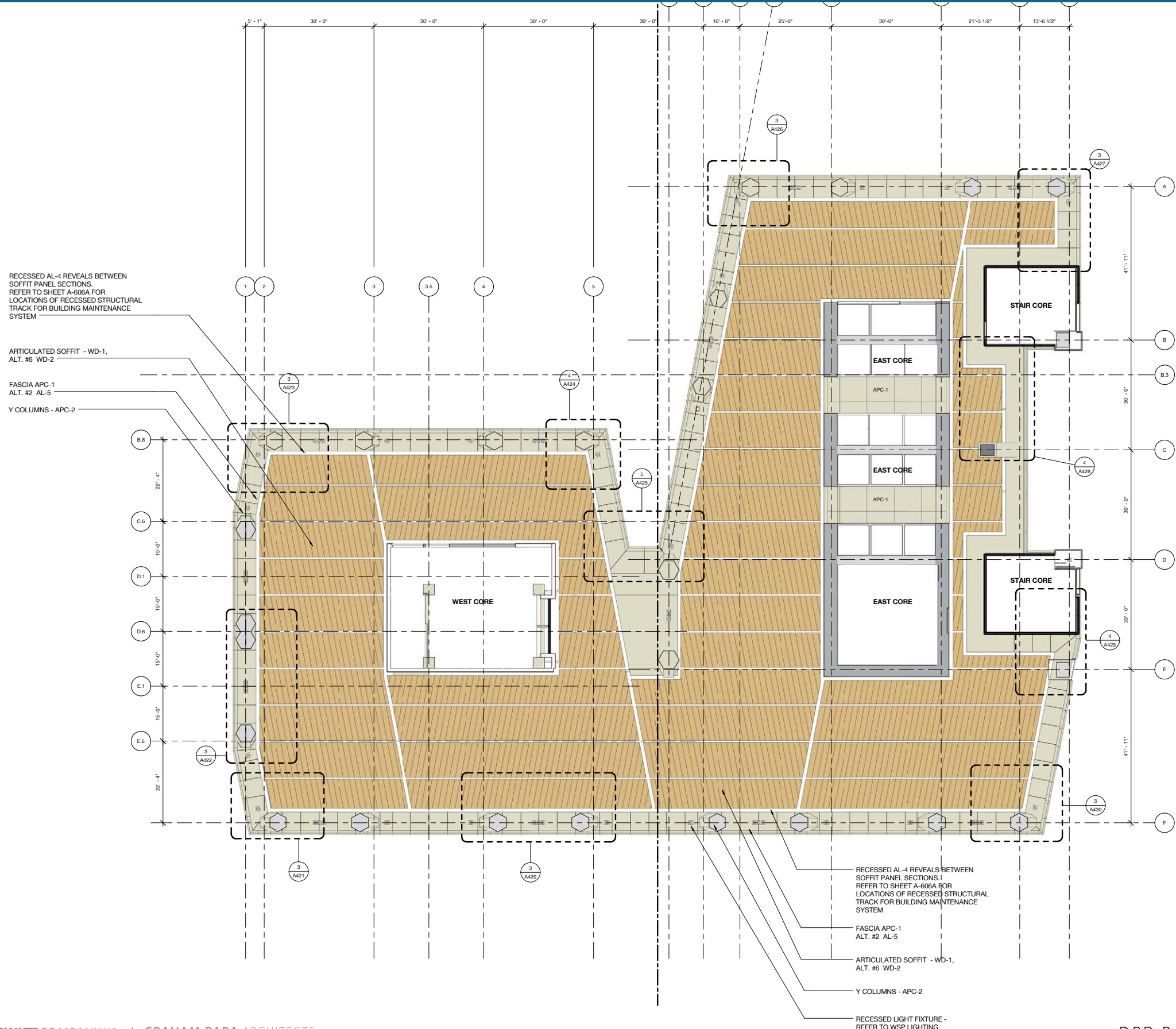
CURRENT AXONOMETRIC

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



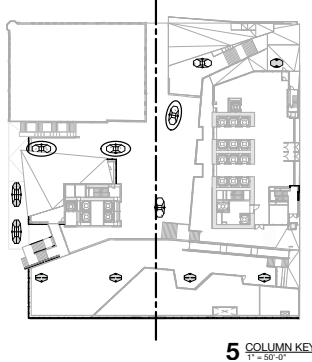
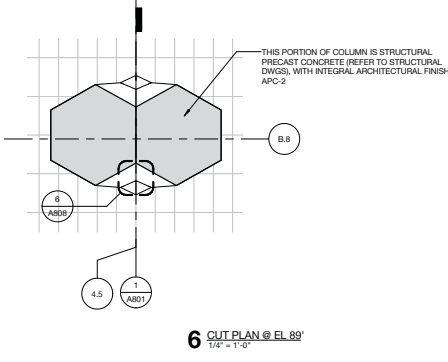
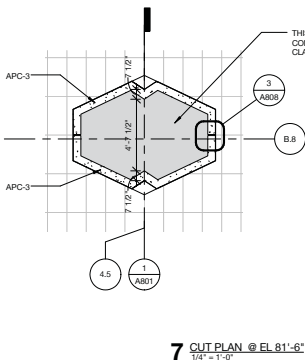
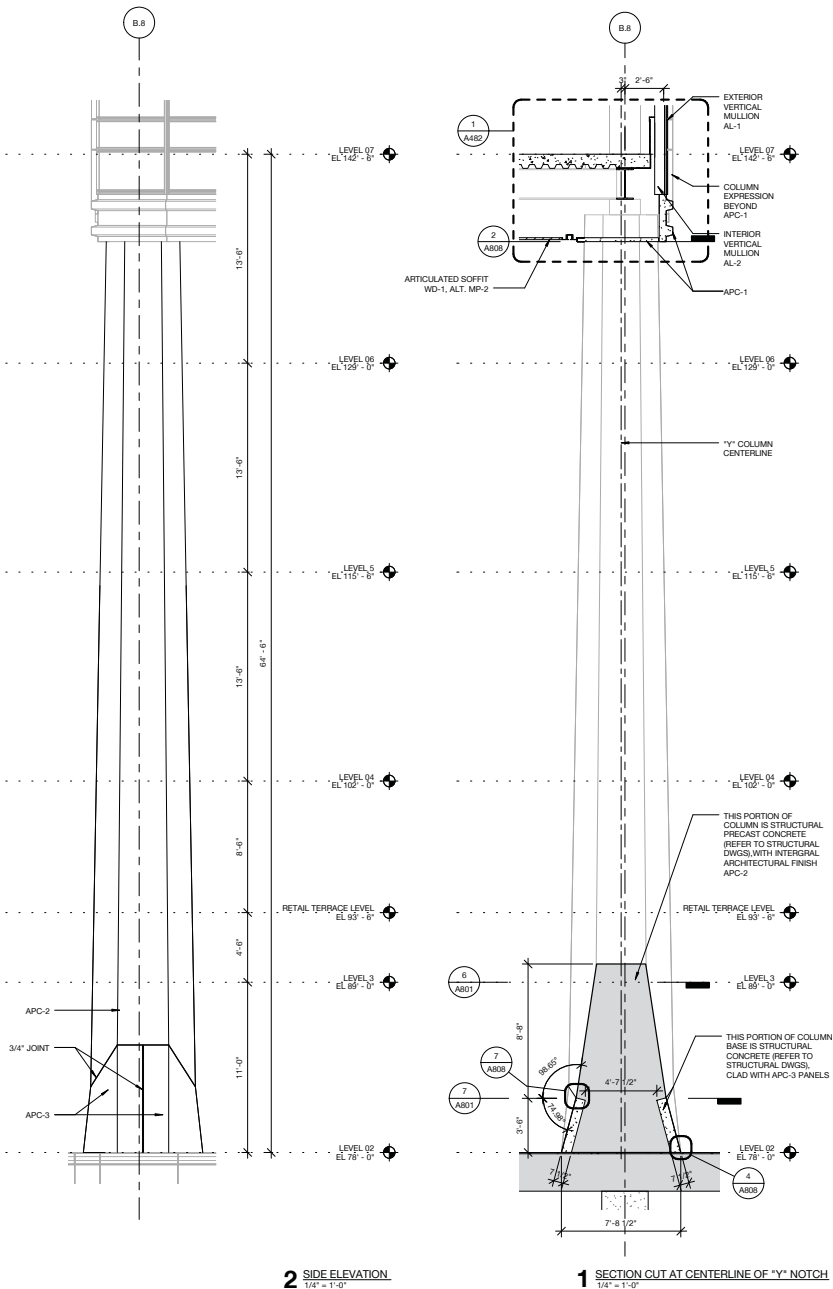
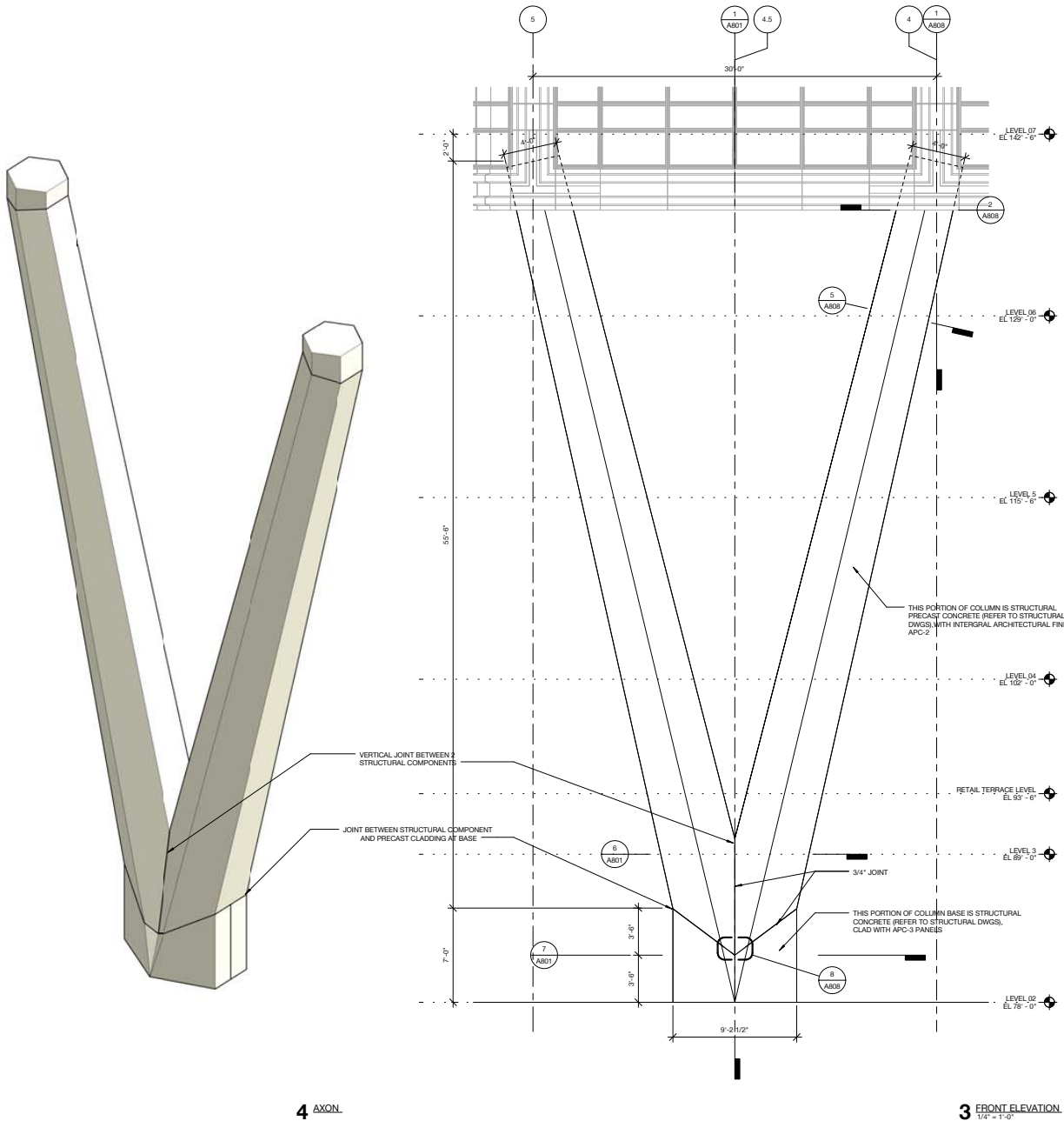
CURRENT PLAN

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



CURRENT DETAILS

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



5a

5b

5c

5d

5e

5f

6a

6b

6c

6d

6e

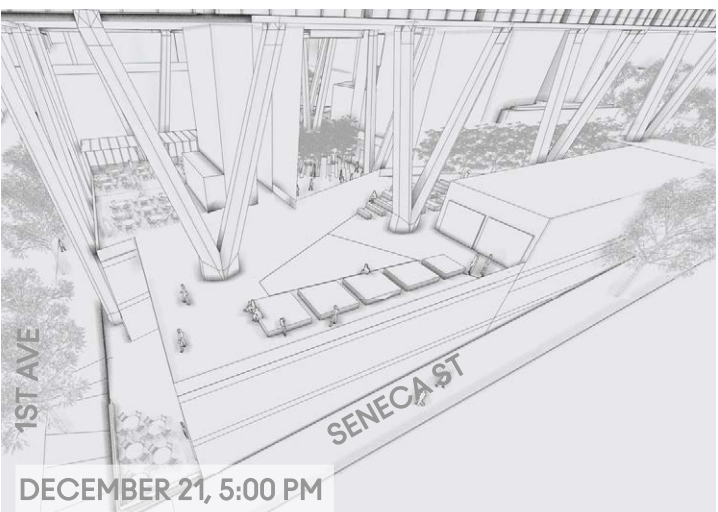
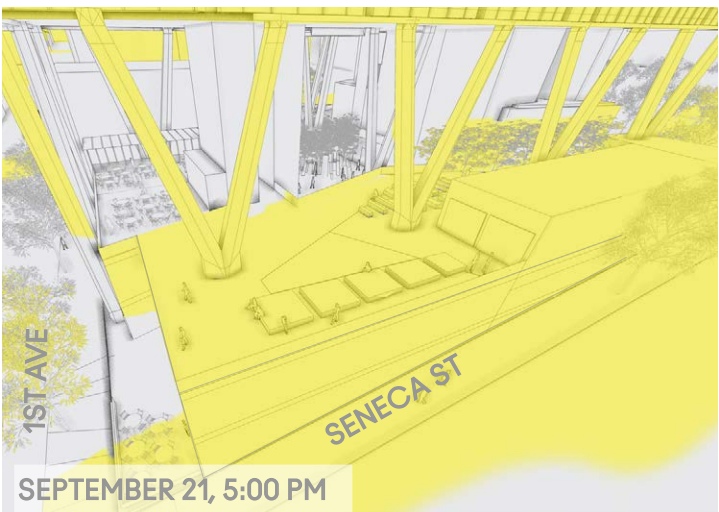
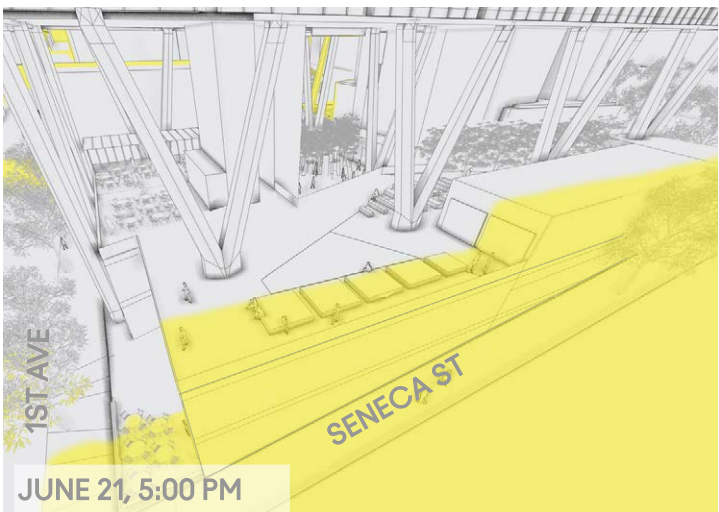
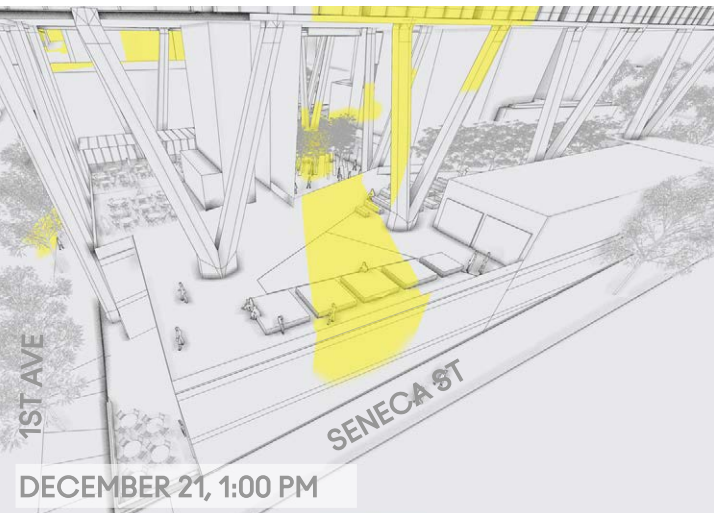
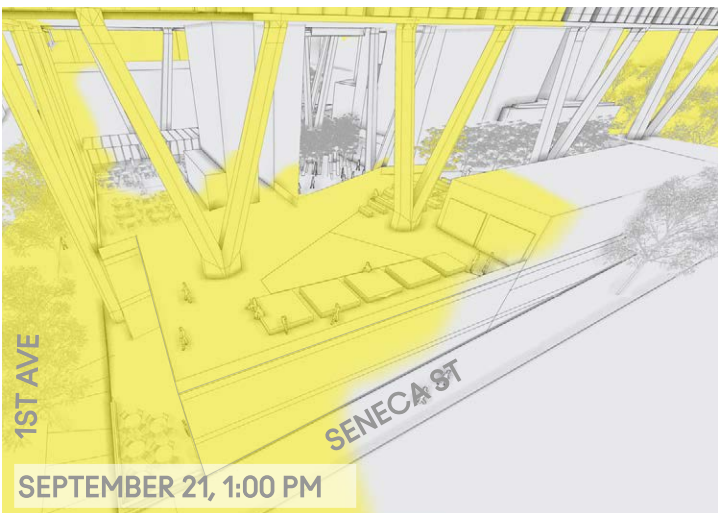
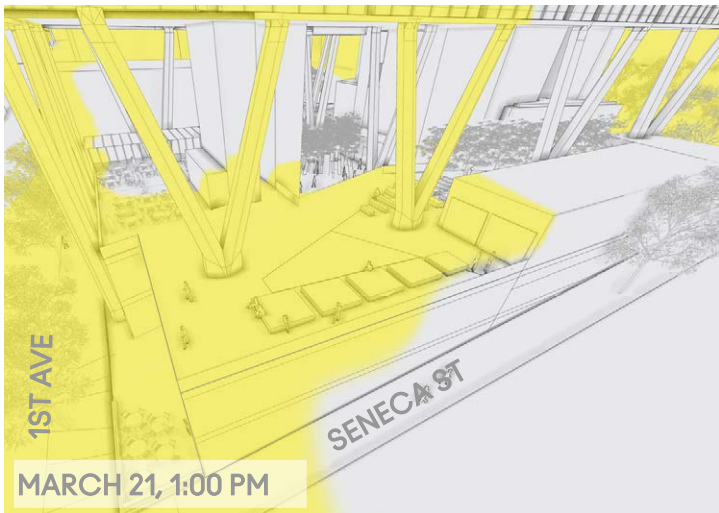
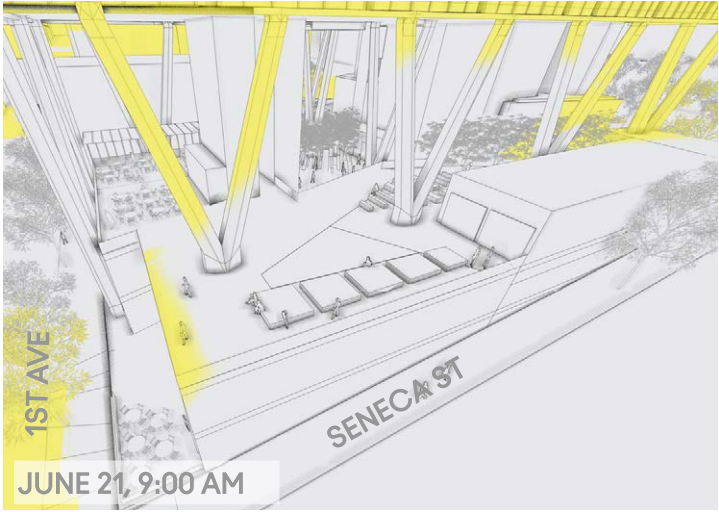
6f

7a

7b

7c

7d



Direct Solar Indirect/Ambient

CURRENT PALETTE

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d

MATERIAL LEGEND

- 1. PRECAST CONCRETE
- 2. CAST-IN-PLACE CONCRETE
- 3. VISION GLASS
- 4. TRANSLUCENT GLASS/SPANDREL
- 5. METAL PANEL
- 6. TERRA COTTA PANEL
- 7. STANDING SEAM METAL ROOF
- 8. WOOD



5

Level 19

CURRENT PLAN

5a

5b

5c

5d

5e

5f

6a

6b

6c

6d

6e


6f

7a

7b

7c

7d



UNIVERSITY ST.

1ST AVE.

2ND AVE.

SENECA ST.

KEY PLAN (NTS)

A

B

C

D

E

Precast Concrete Pavers
on pedestals

Precast Concrete Pavers
sand-set

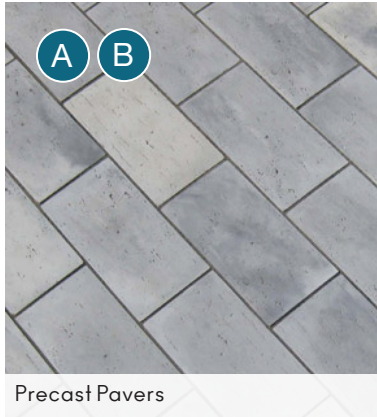
Wood Decking

Custom Wood and Metal Seating

Glass Guardrail



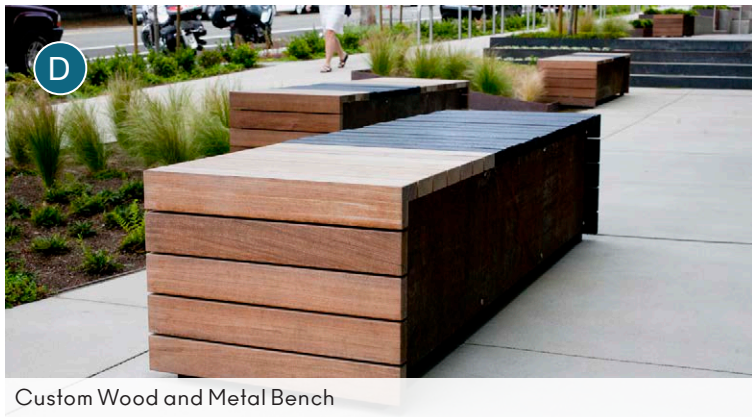
- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



Precast Pavers



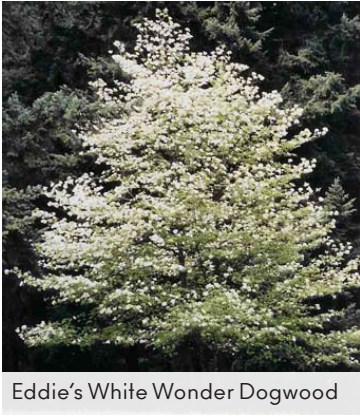
Wood Decking



Custom Wood and Metal Bench



Guardrail



Eddie's White Wonder Dogwood



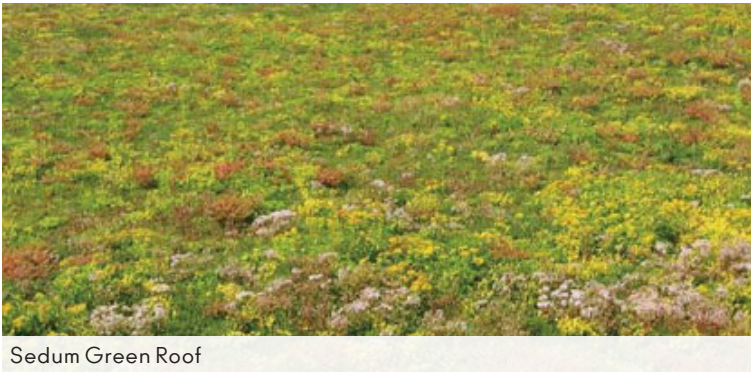
Shore Pine



American Dune Grass



New Zealand Wind Grass



Sedum Green Roof

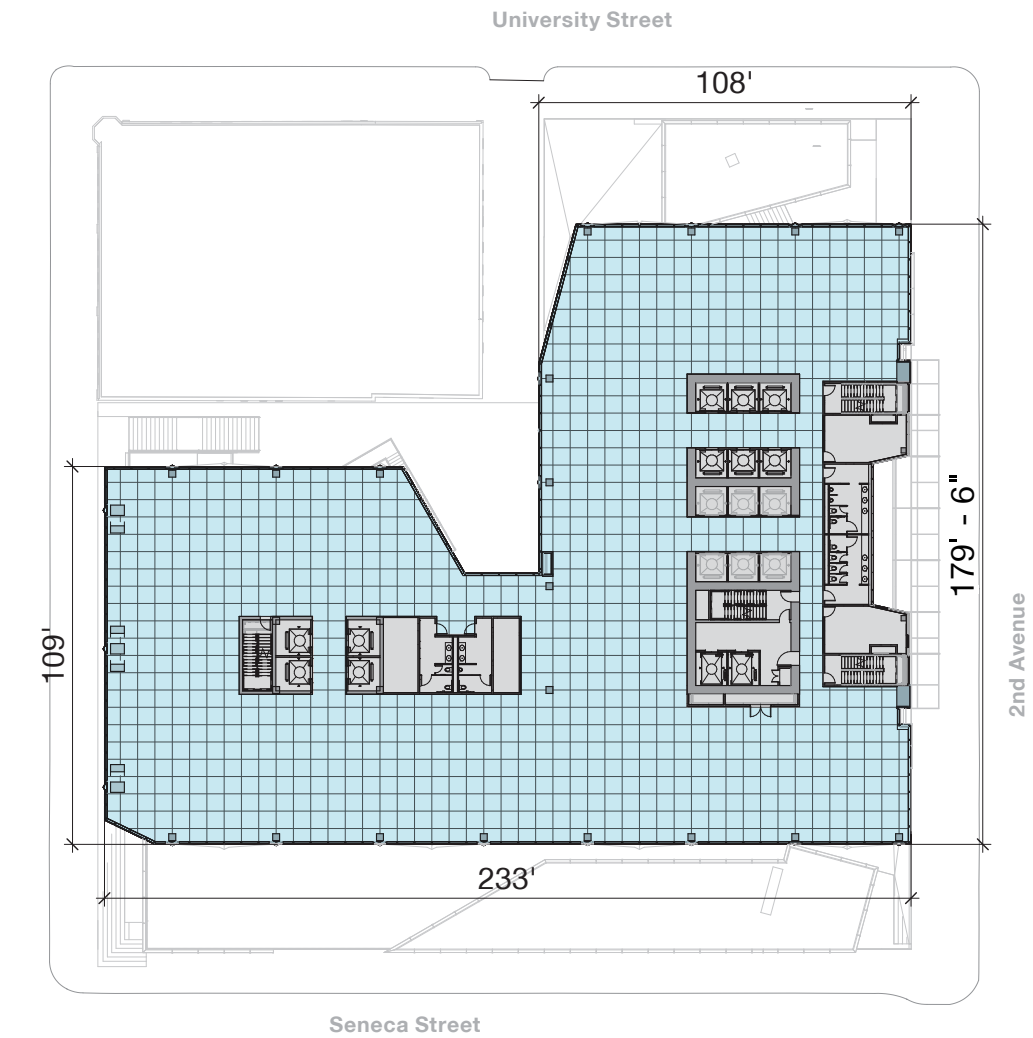
- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



6

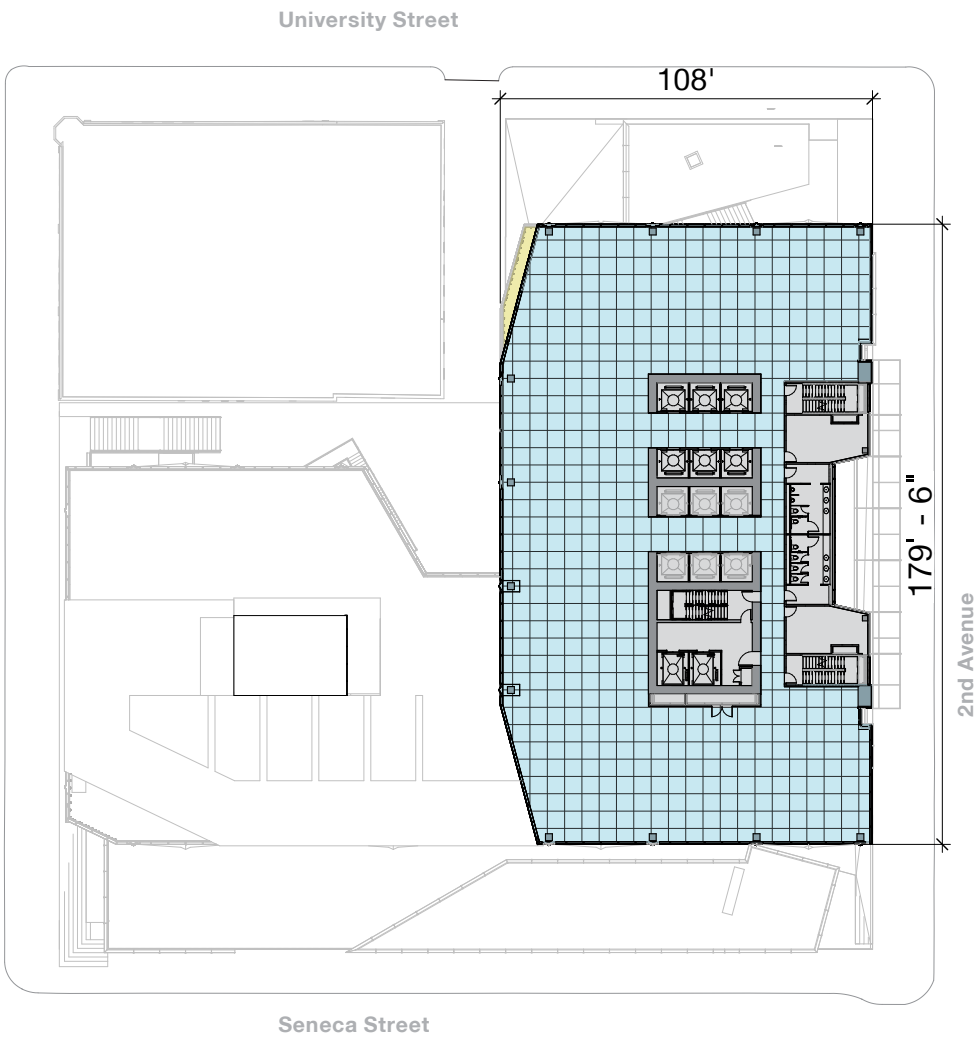
Tower / Rooftop

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



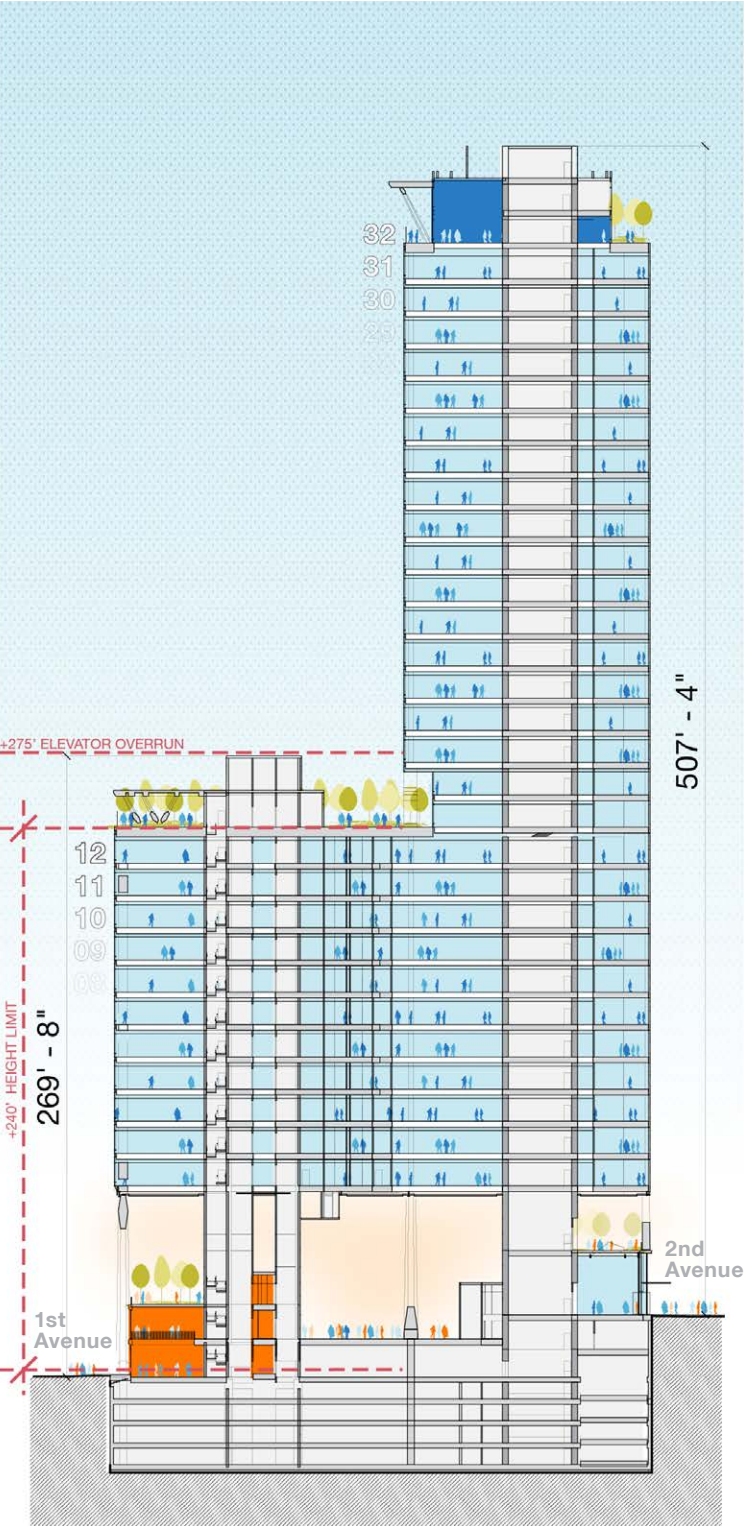
Low-rise Plan

- Office
- Service



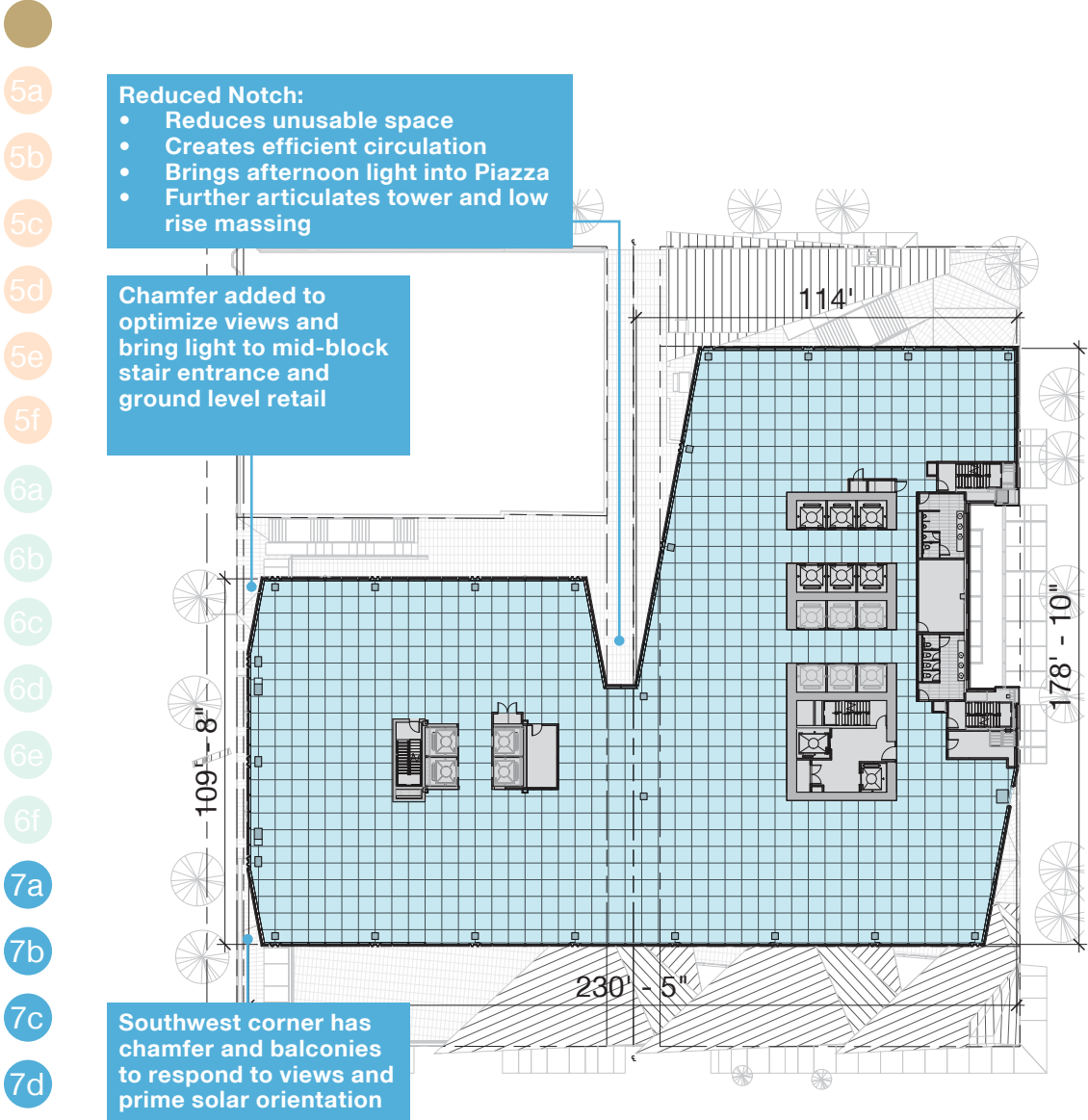
High-rise Plan

- Office
- Service



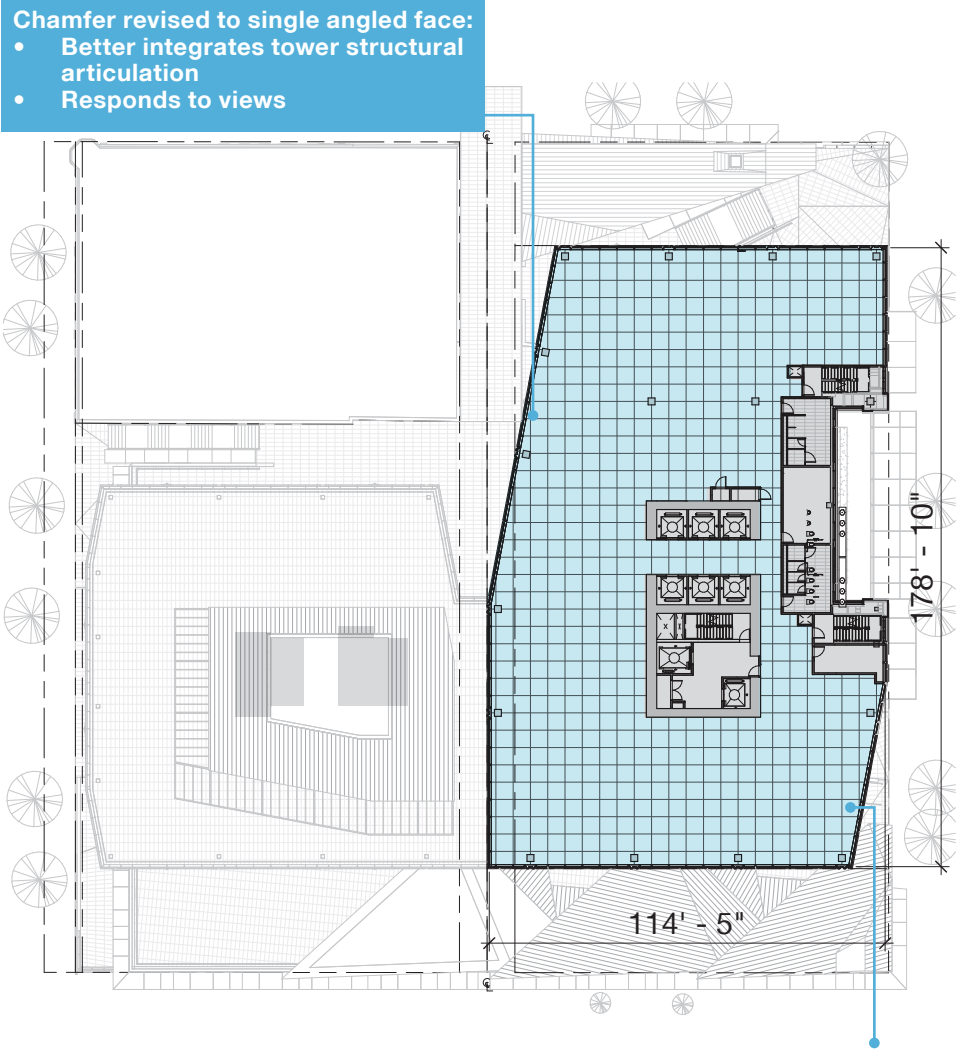
Section

CURRENT PLANS



Low-rise Plan

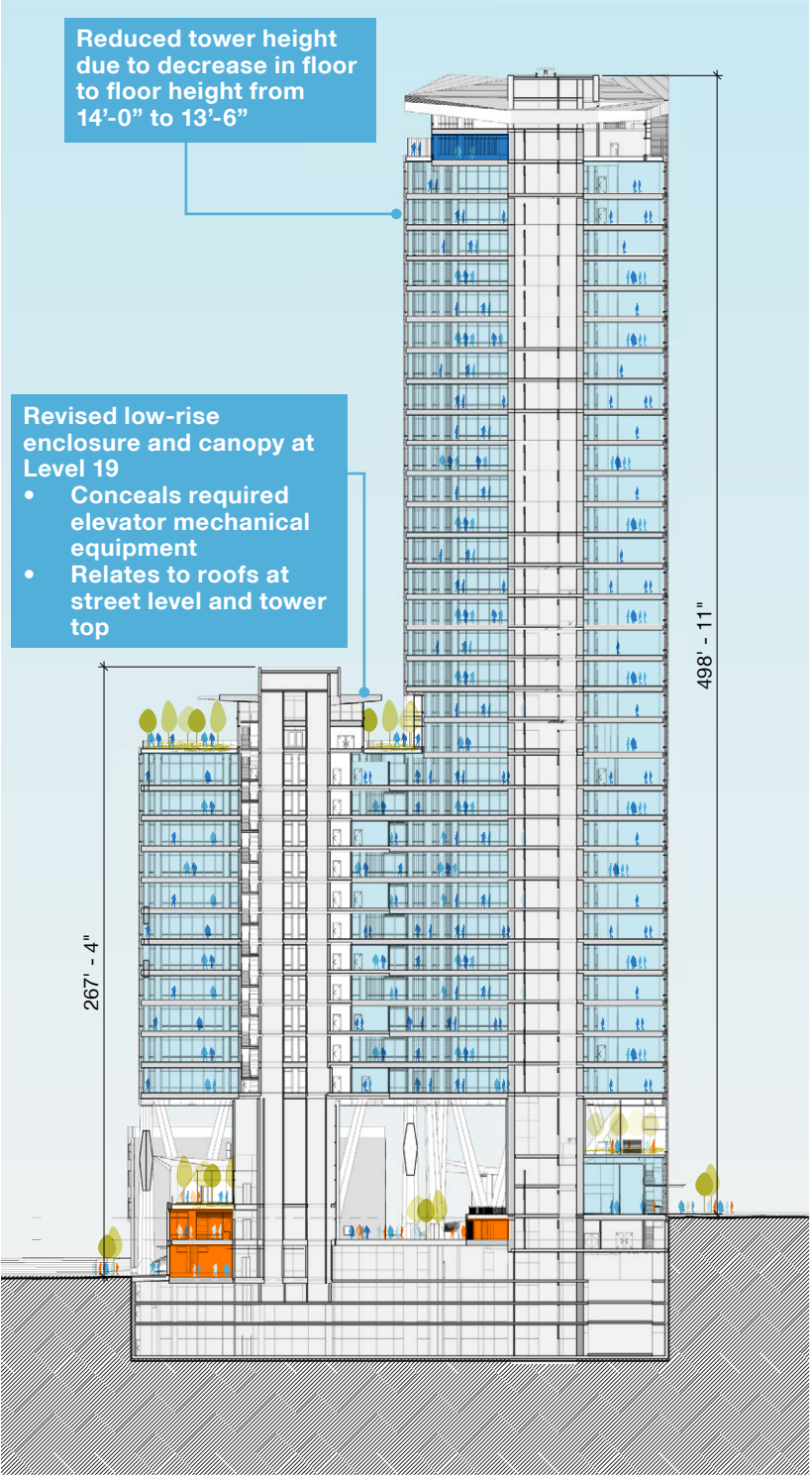
- Office
- Service



High-rise Plan

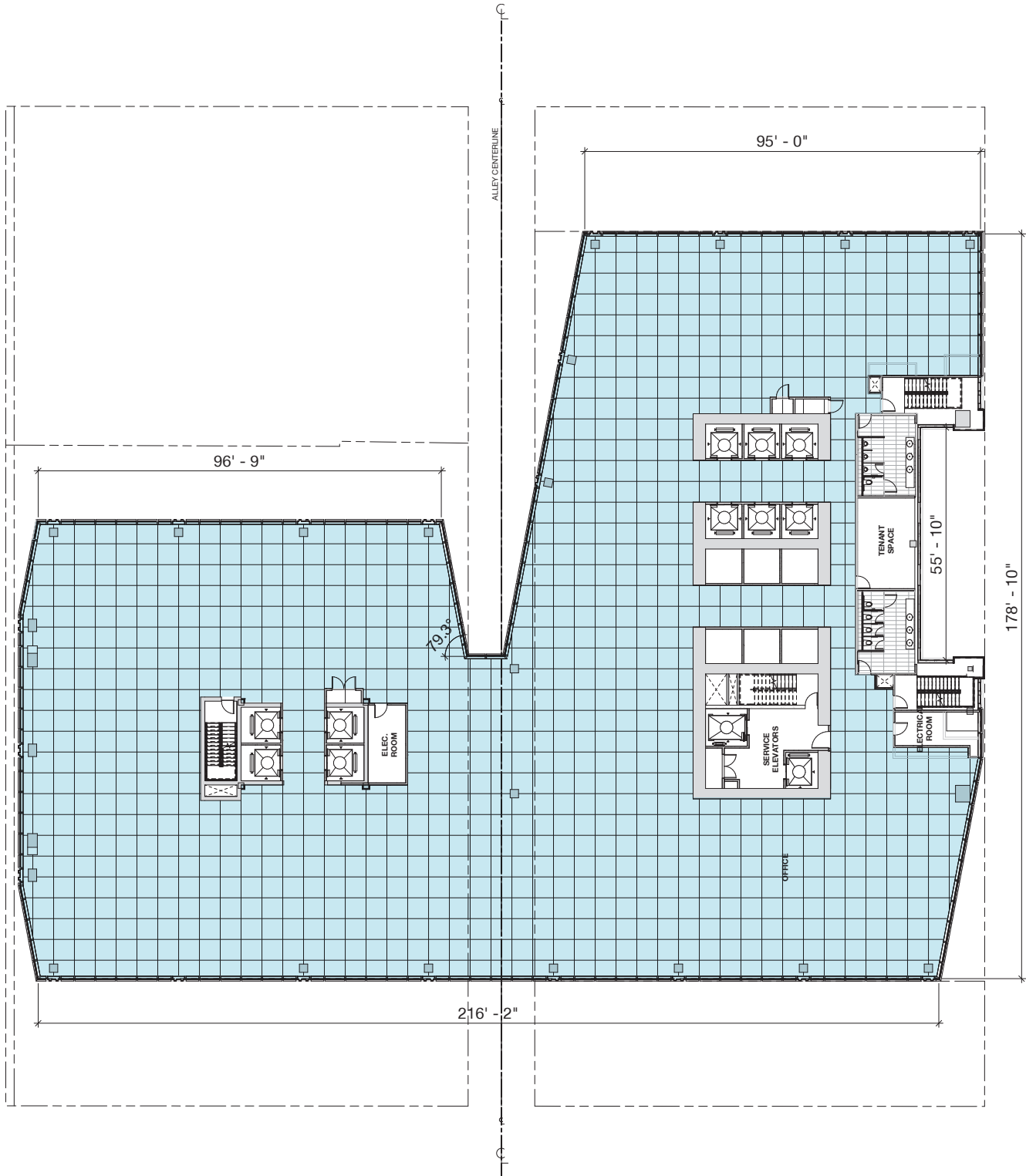
- Office
- Service

CURRENT SECTION

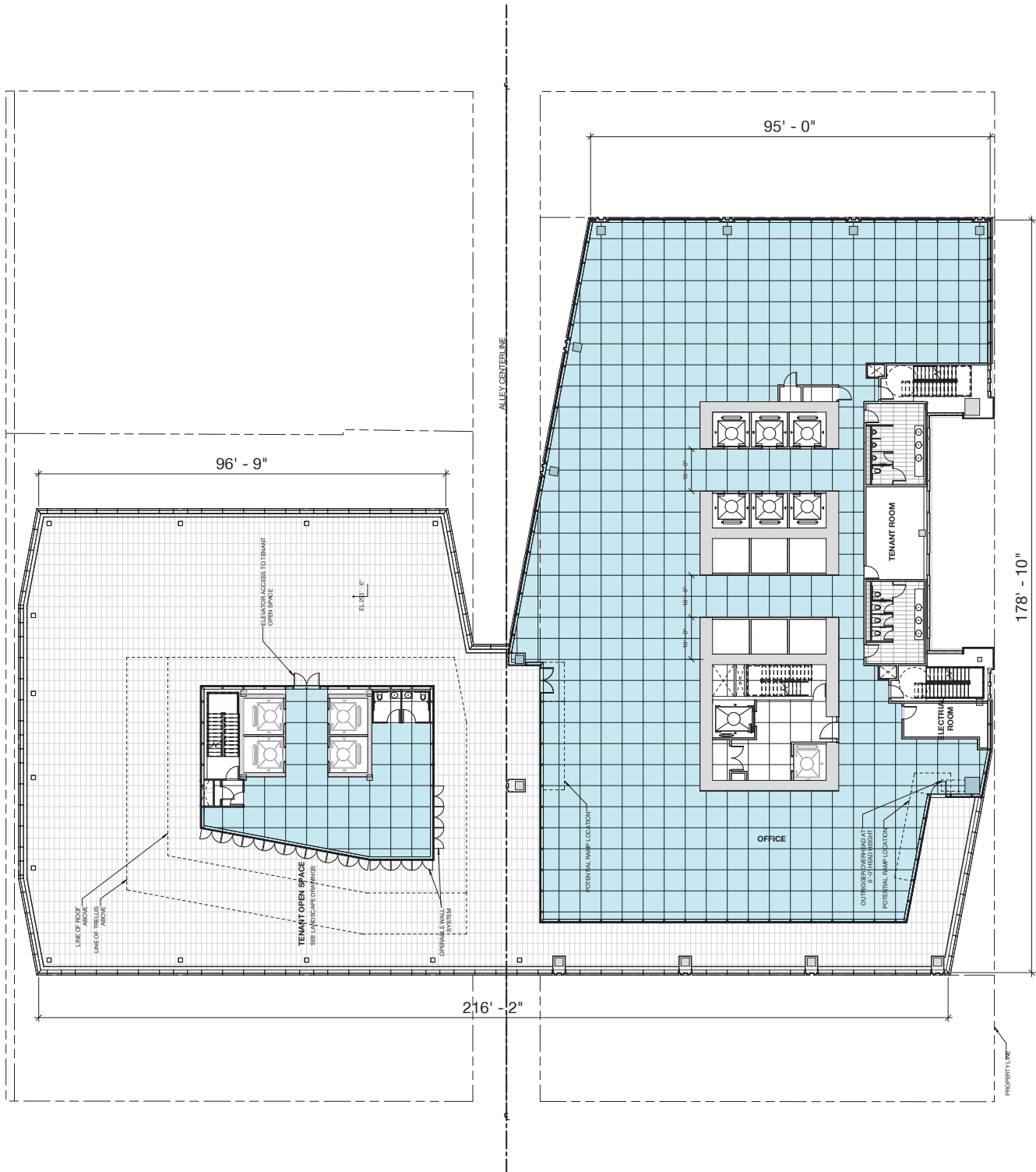


Section

-
- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



Levels 7-18 Plan

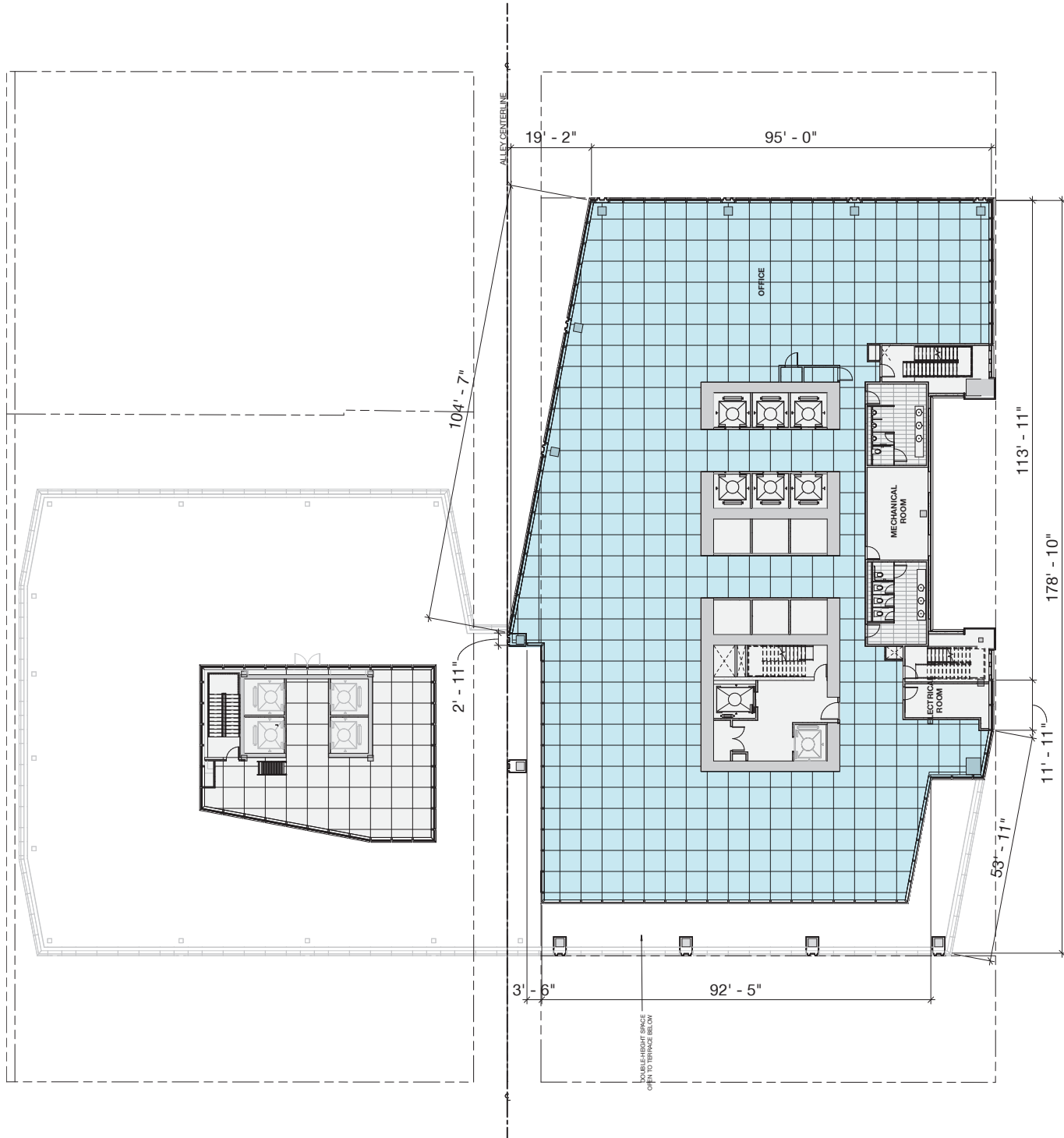


Level 19 Plan

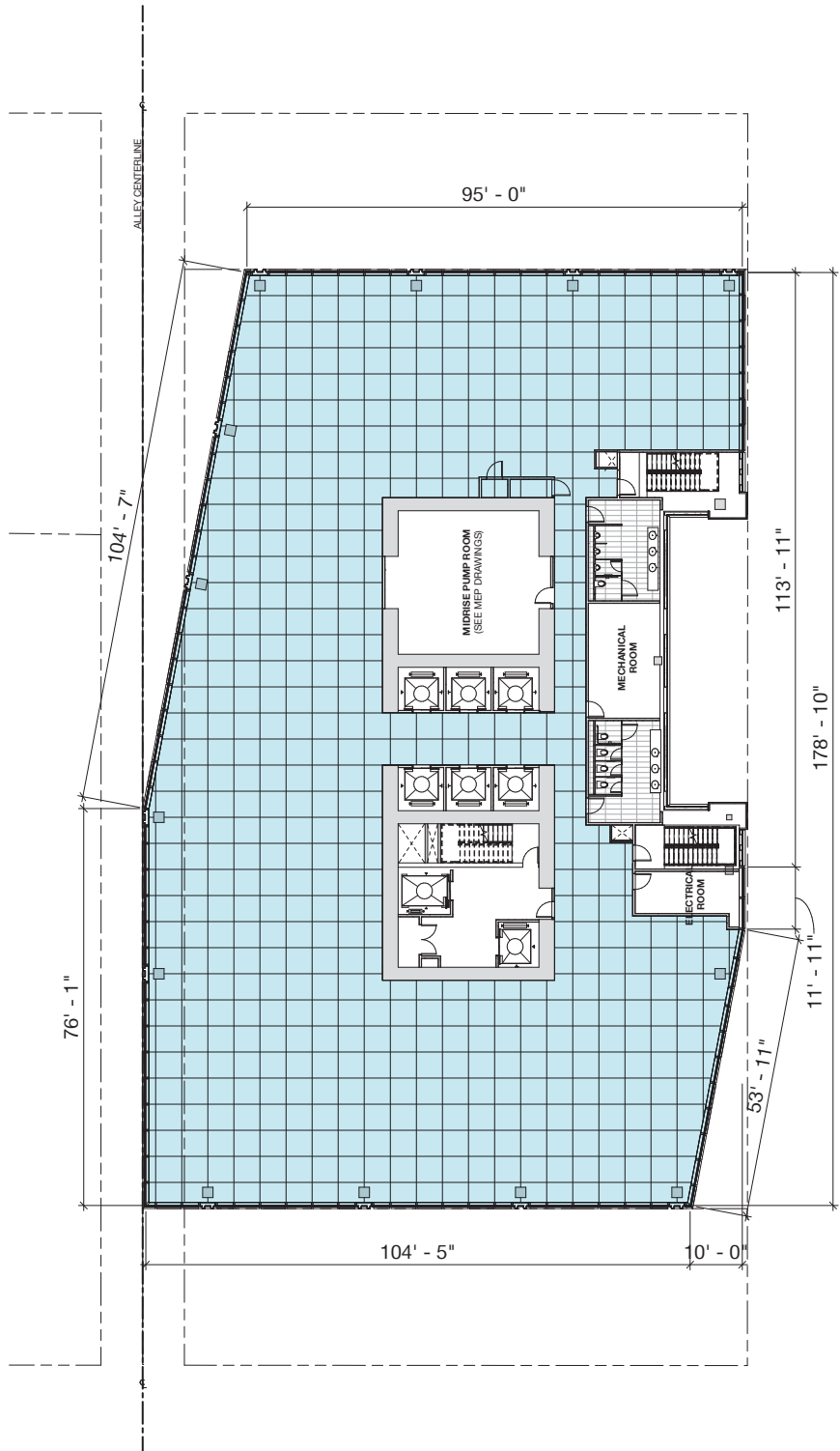


CURRENT PLANS

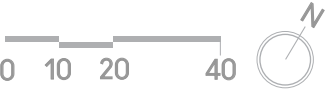
- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



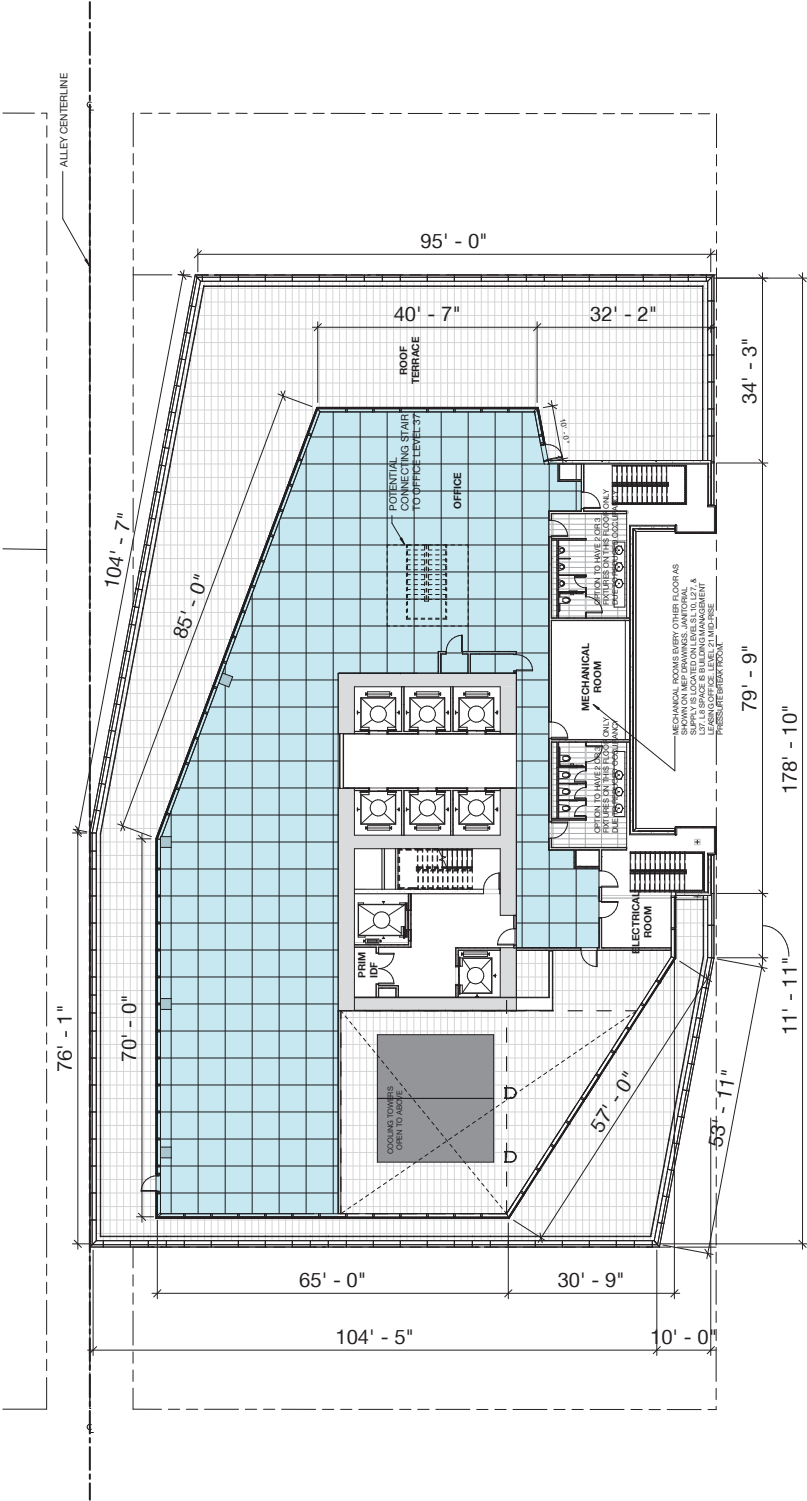
Level 20 Plan



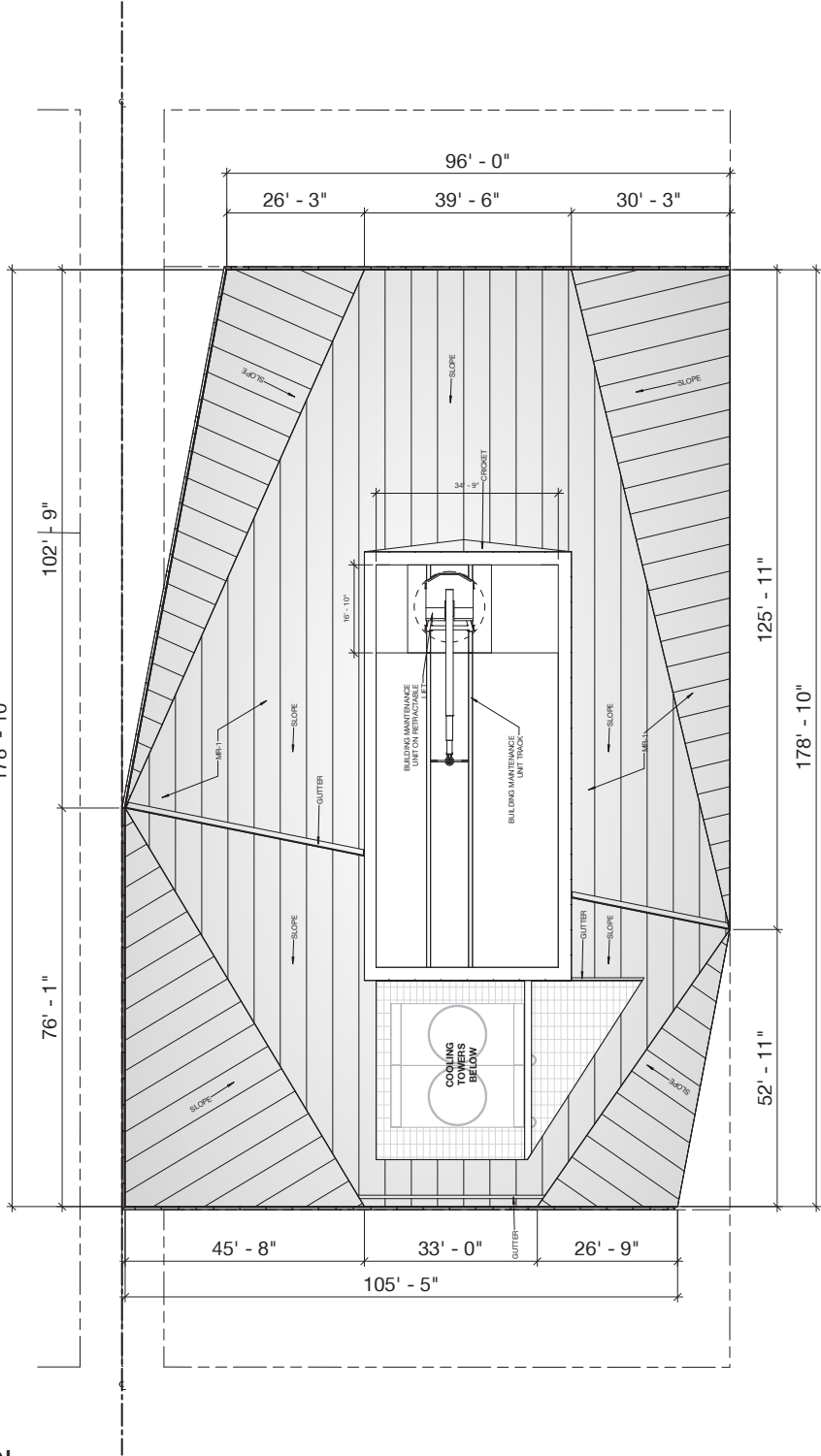
Levels 21-36 Plan



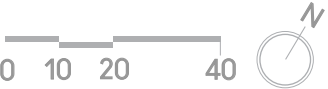
-
- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



Level 38 Plan

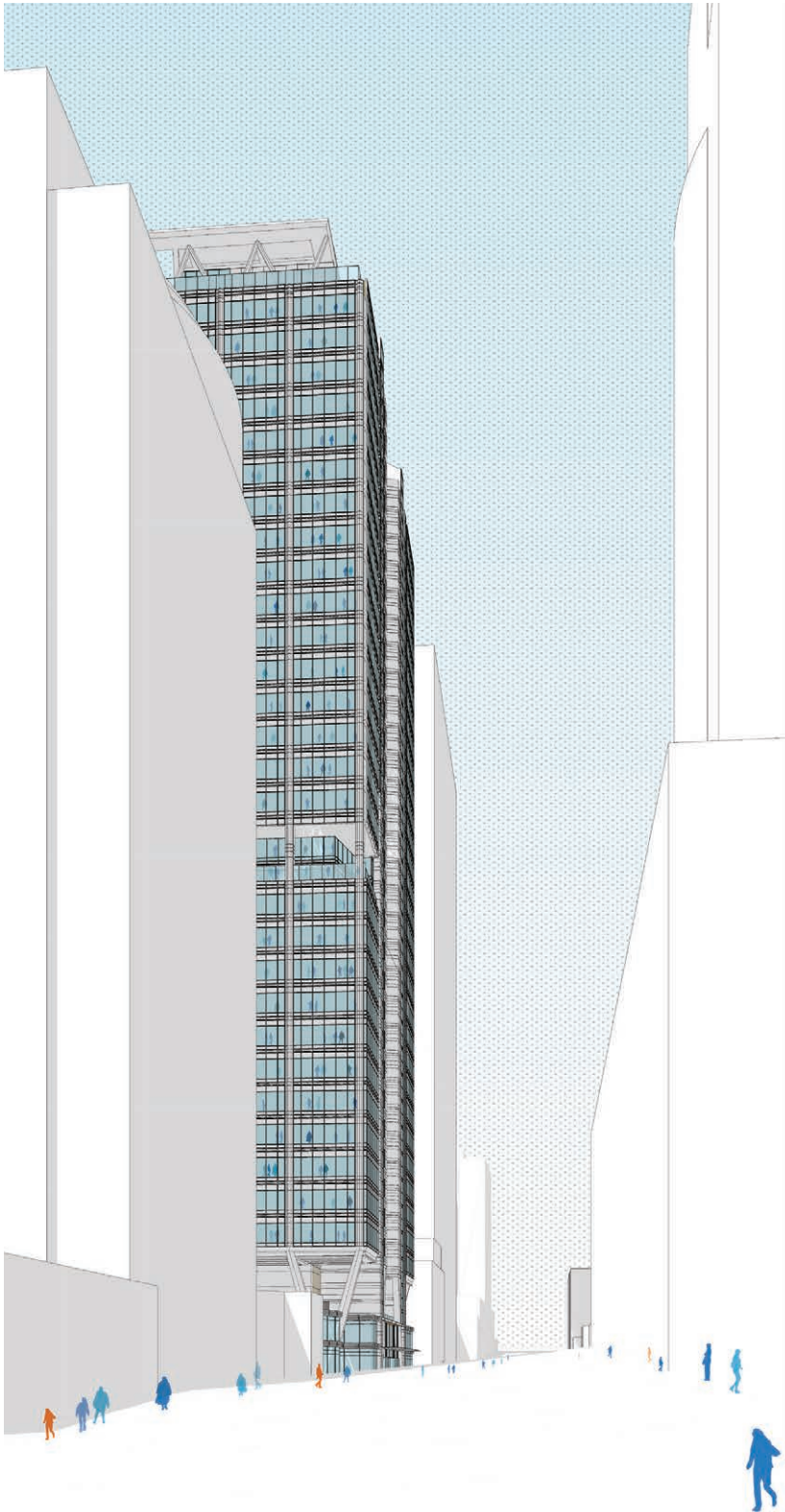


Roof Level Plan



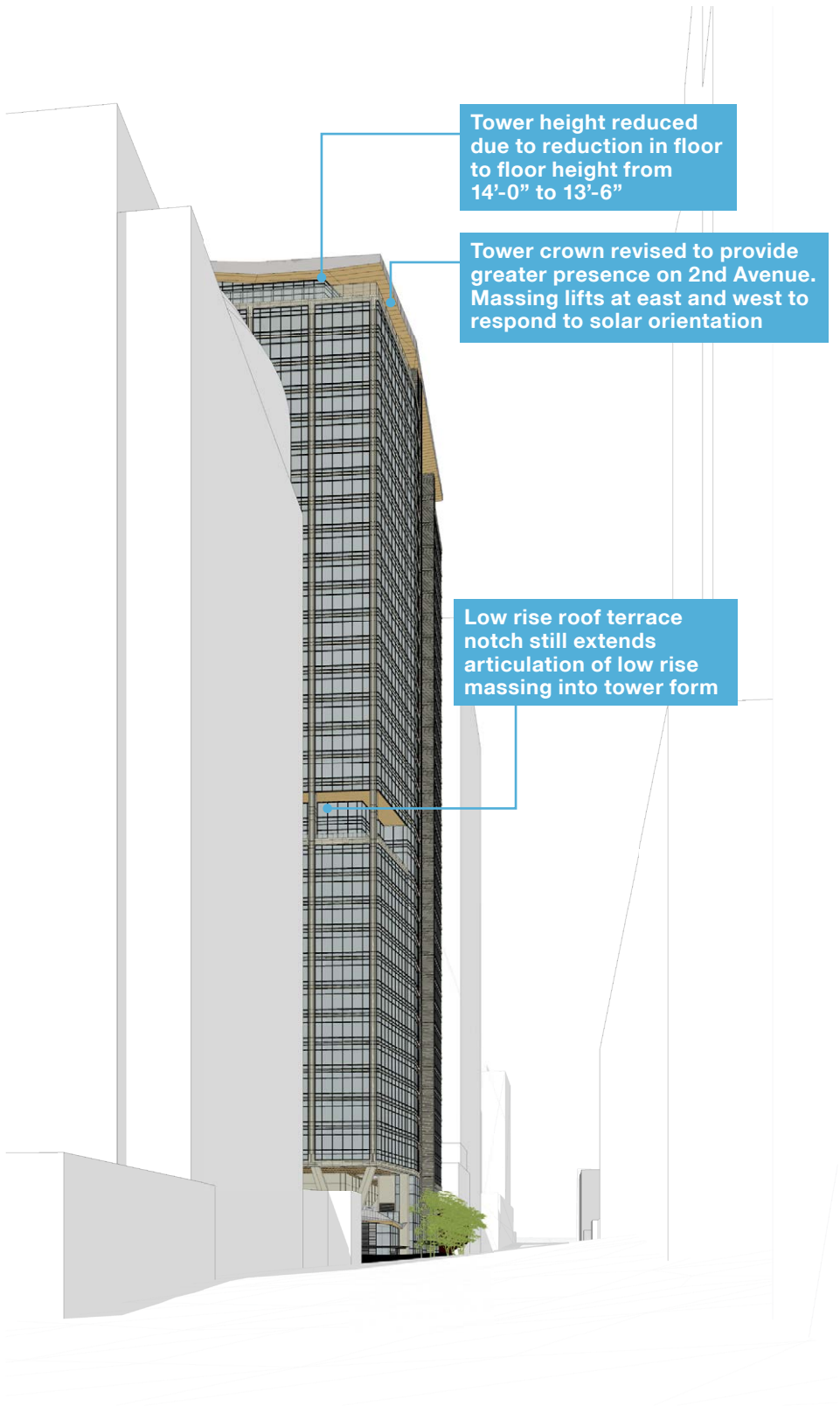
EDG2 VIEW

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



View from 2nd Avenue looking North

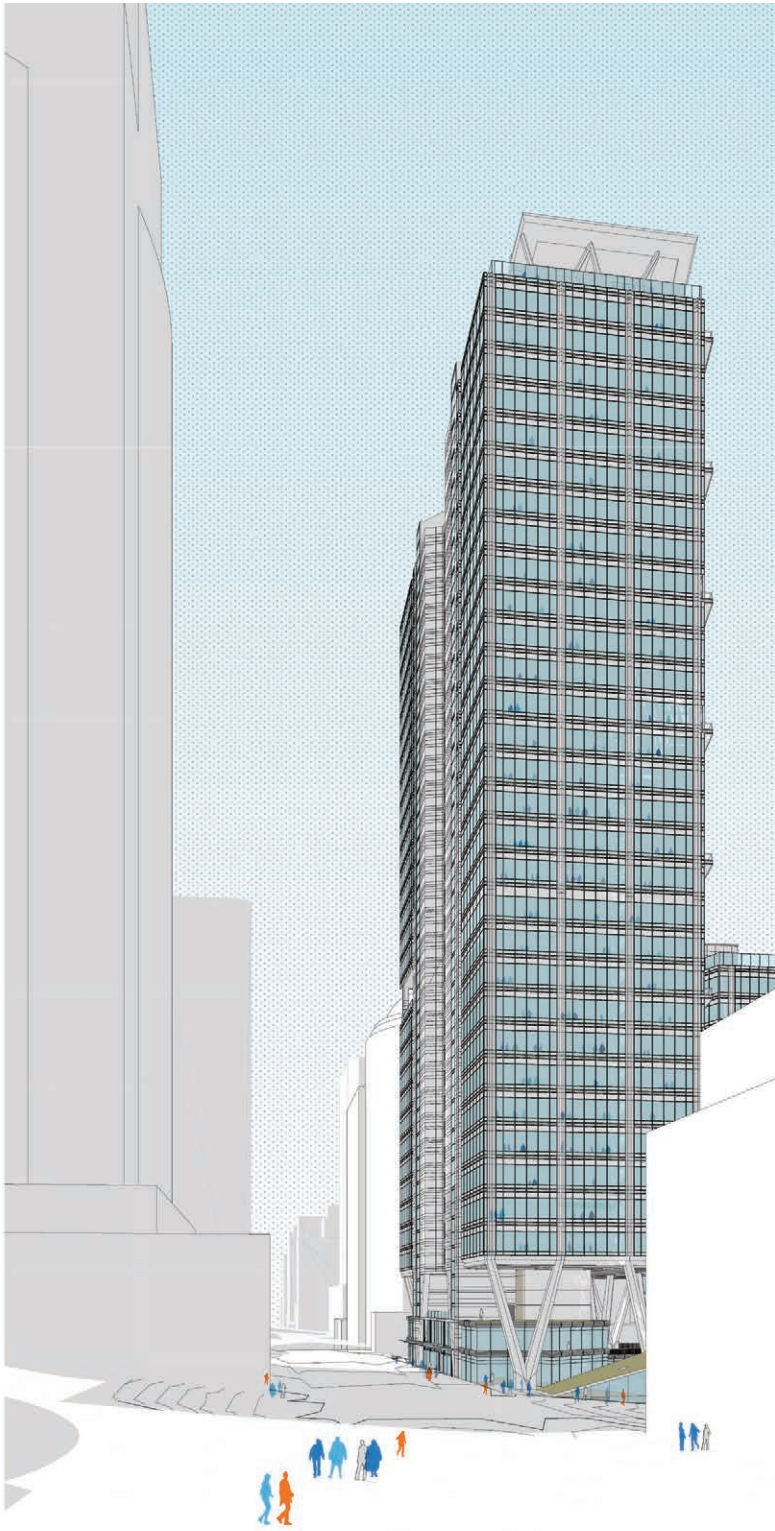
CURRENT VIEW



View from 2nd Ave. Looking North

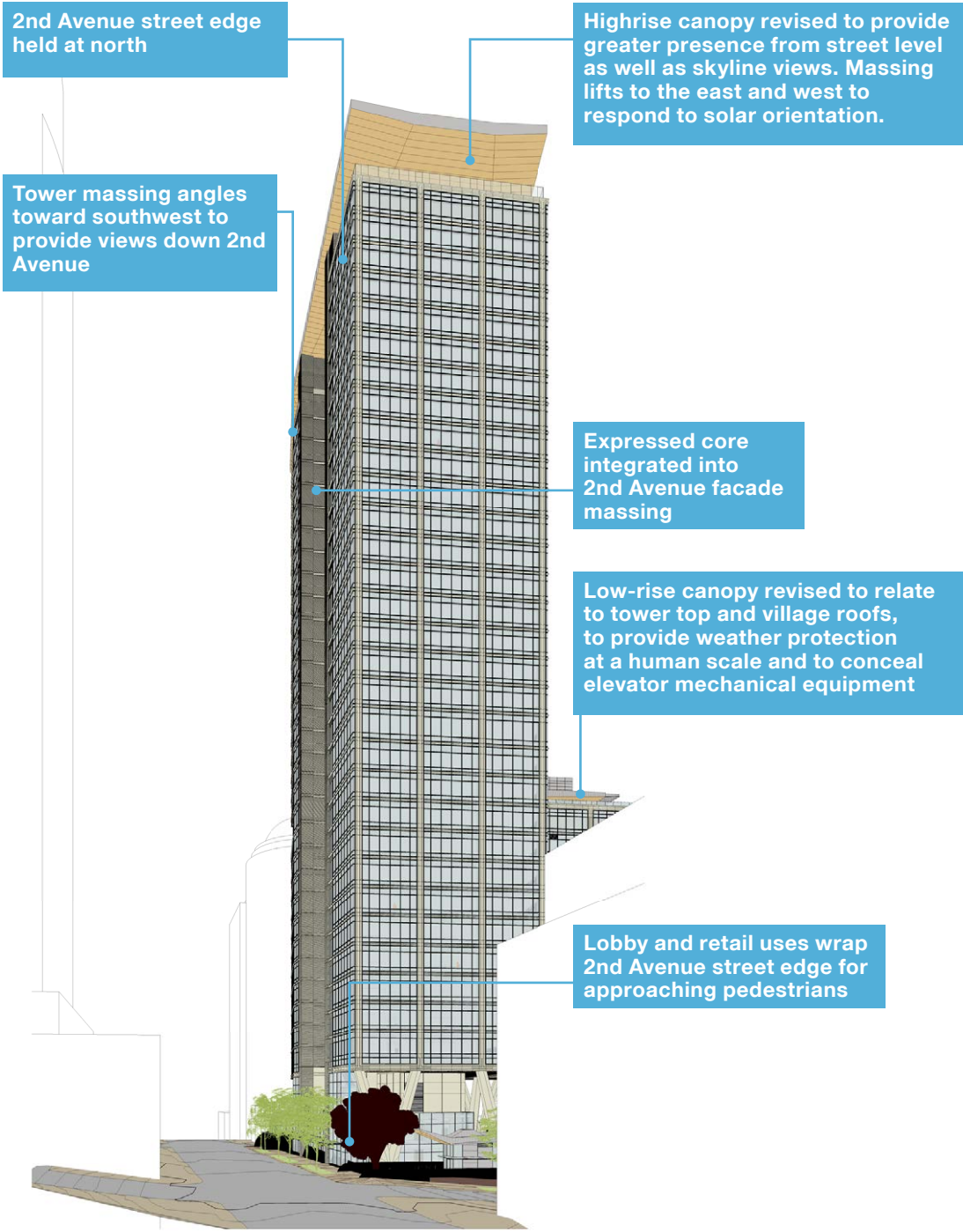
EDG2 VIEW

-
- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



View from 2nd Avenue looking South

CURRENT VIEW

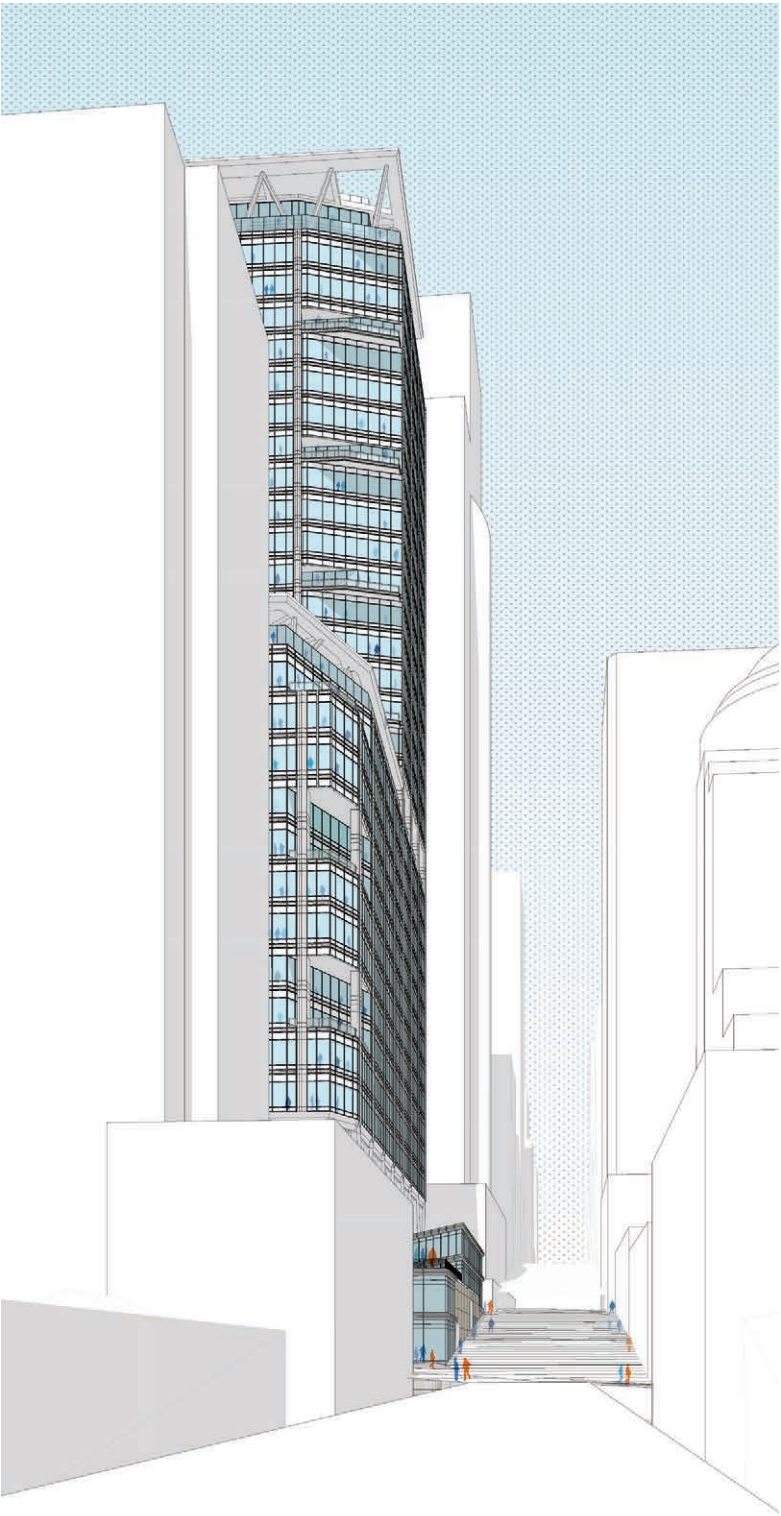


View from 2nd Ave. Looking South

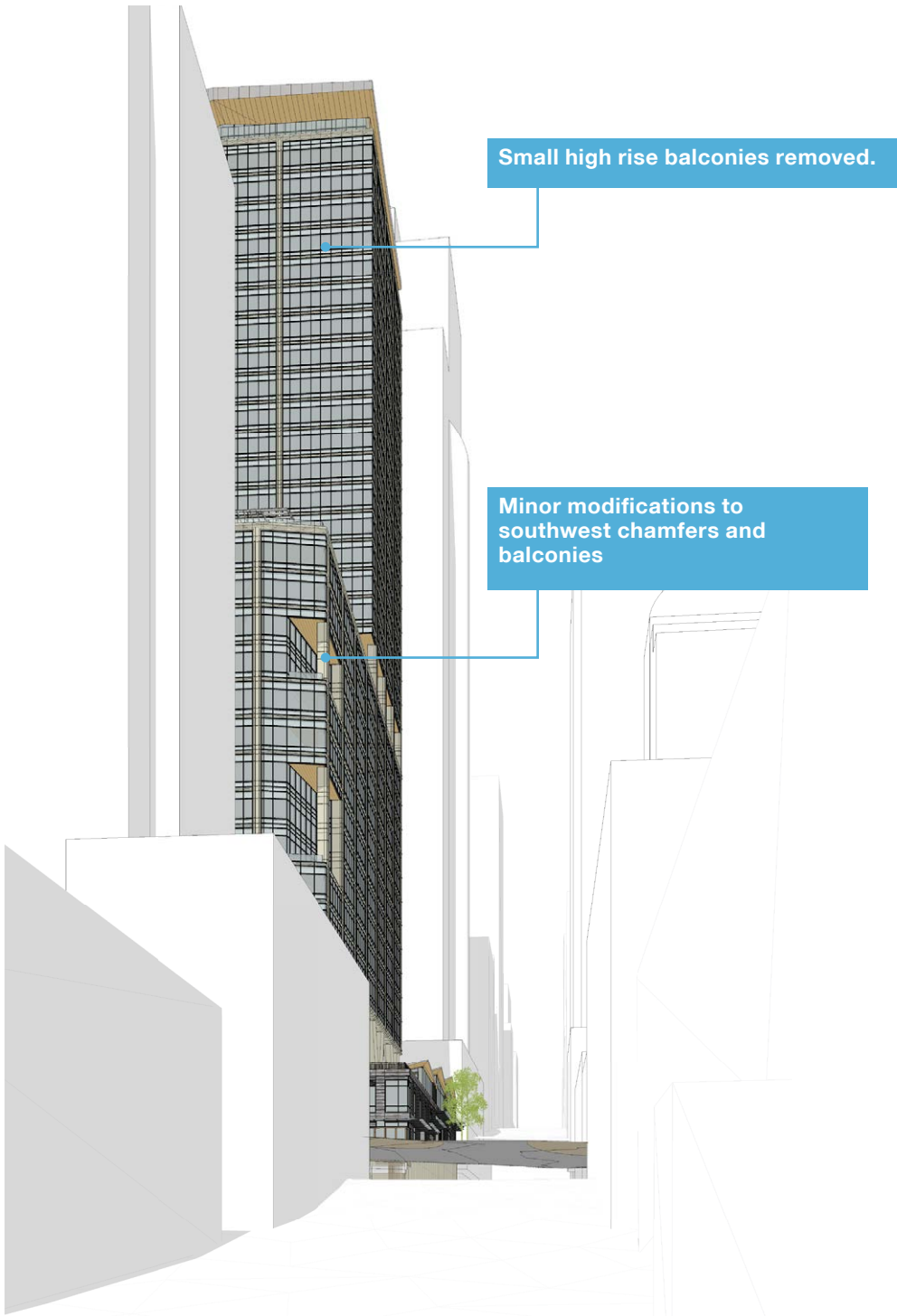
EDG2 VIEW

CURRENT VIEW

-
- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



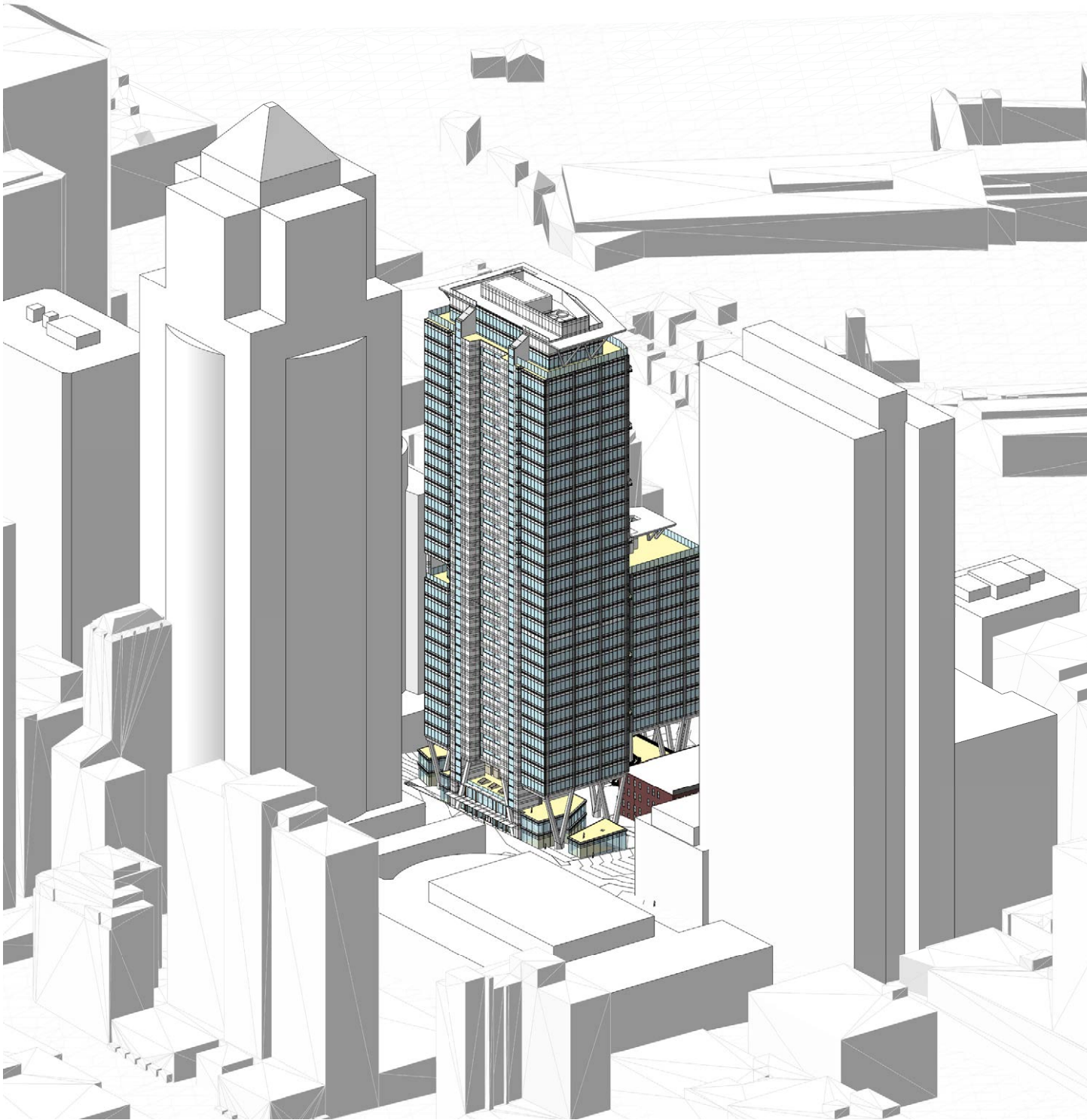
View from Seneca Street looking East



View from Seneca Street looking East

EDG2 VIEW

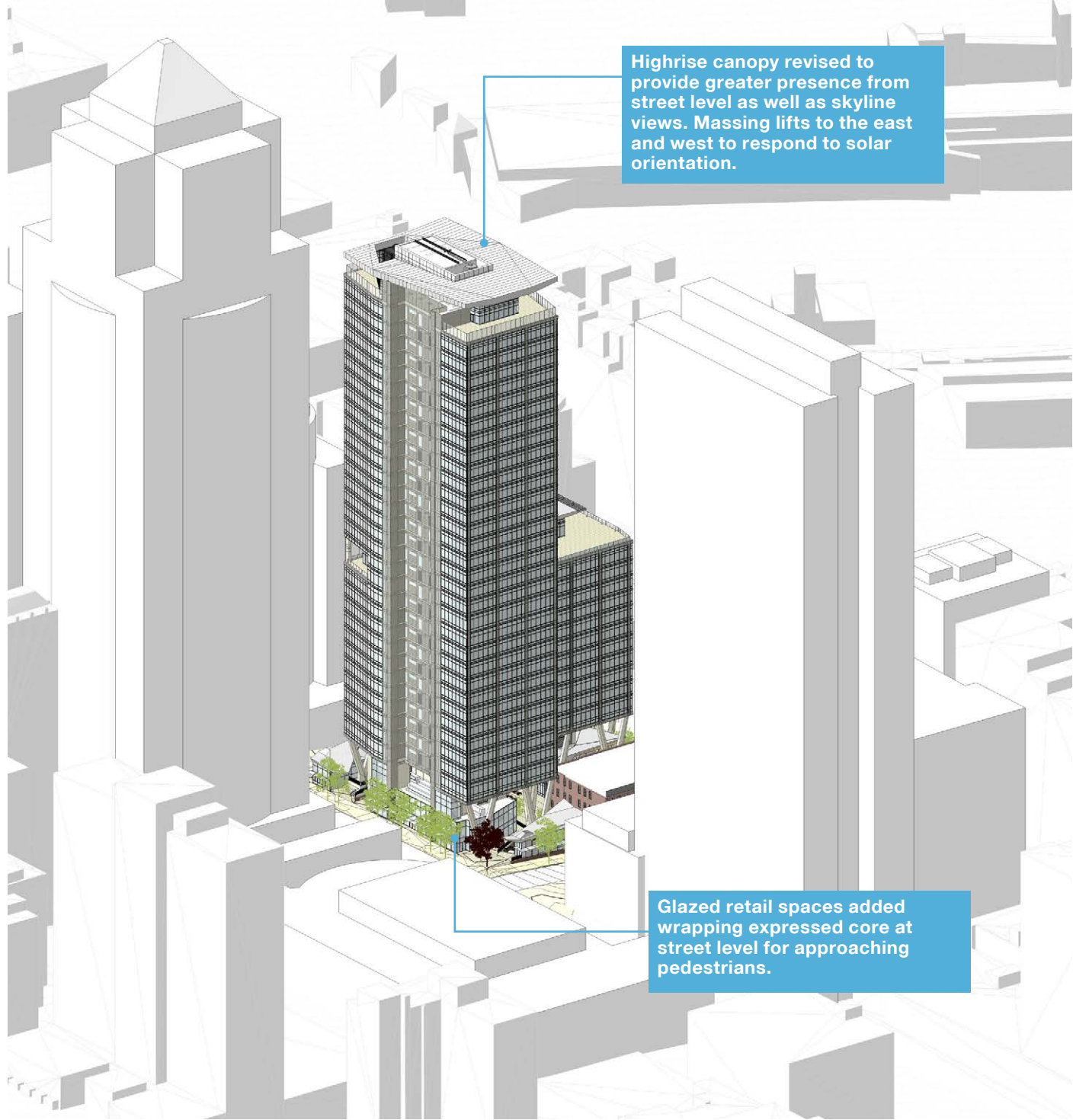
- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



Northeast Aerial

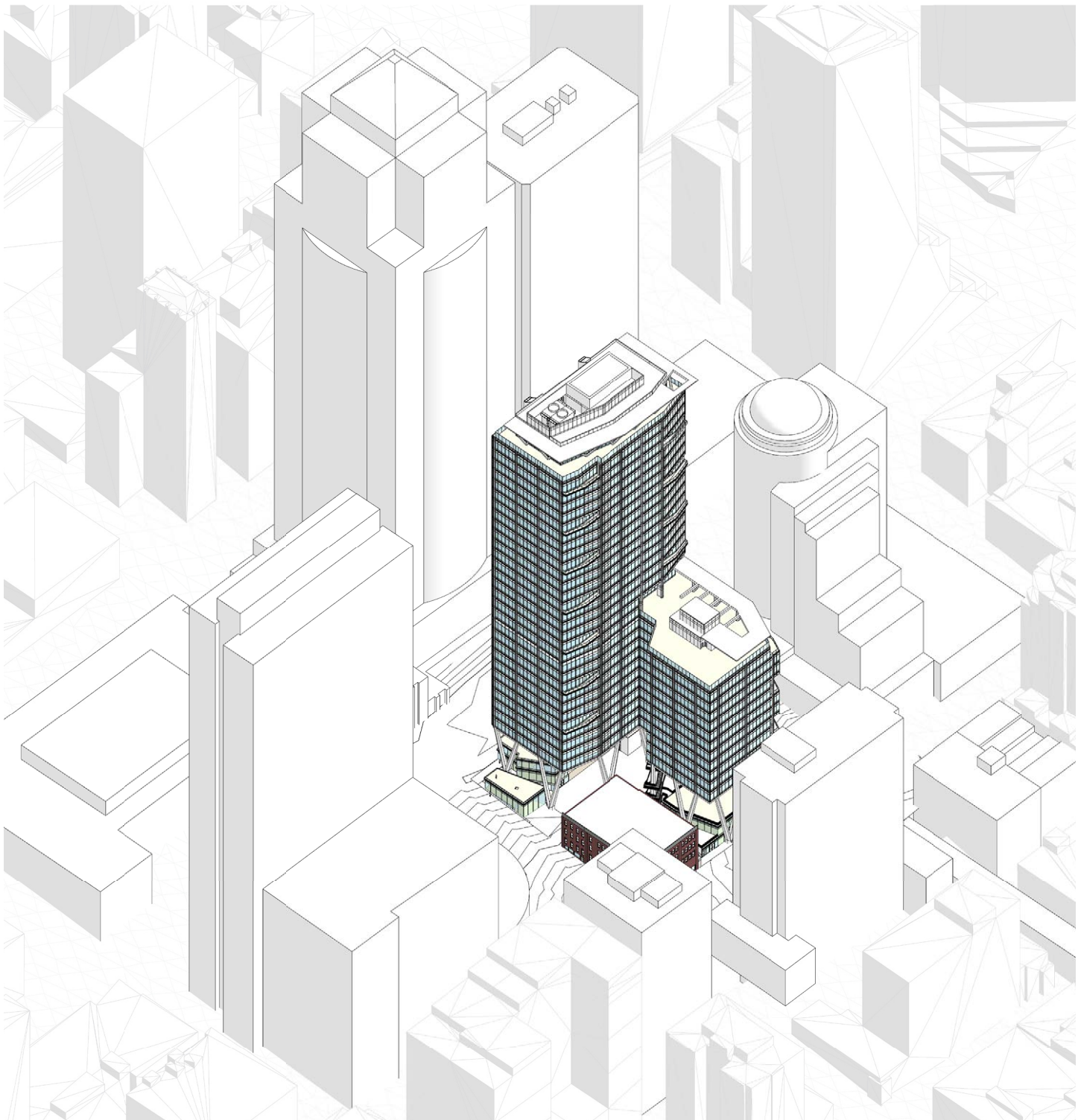
CURRENT VIEW

The stepped building form transitions between the DOC1 U/450/U and DMC 240/290-400 zone massings, and responds to similar stepping at the adjacent Russell Investments Center and Second & Seneca Building to create a transition in scale per Downtown Guideline B2, and to reinforce positive attributes of the immediate area per Downtown Guideline B3.

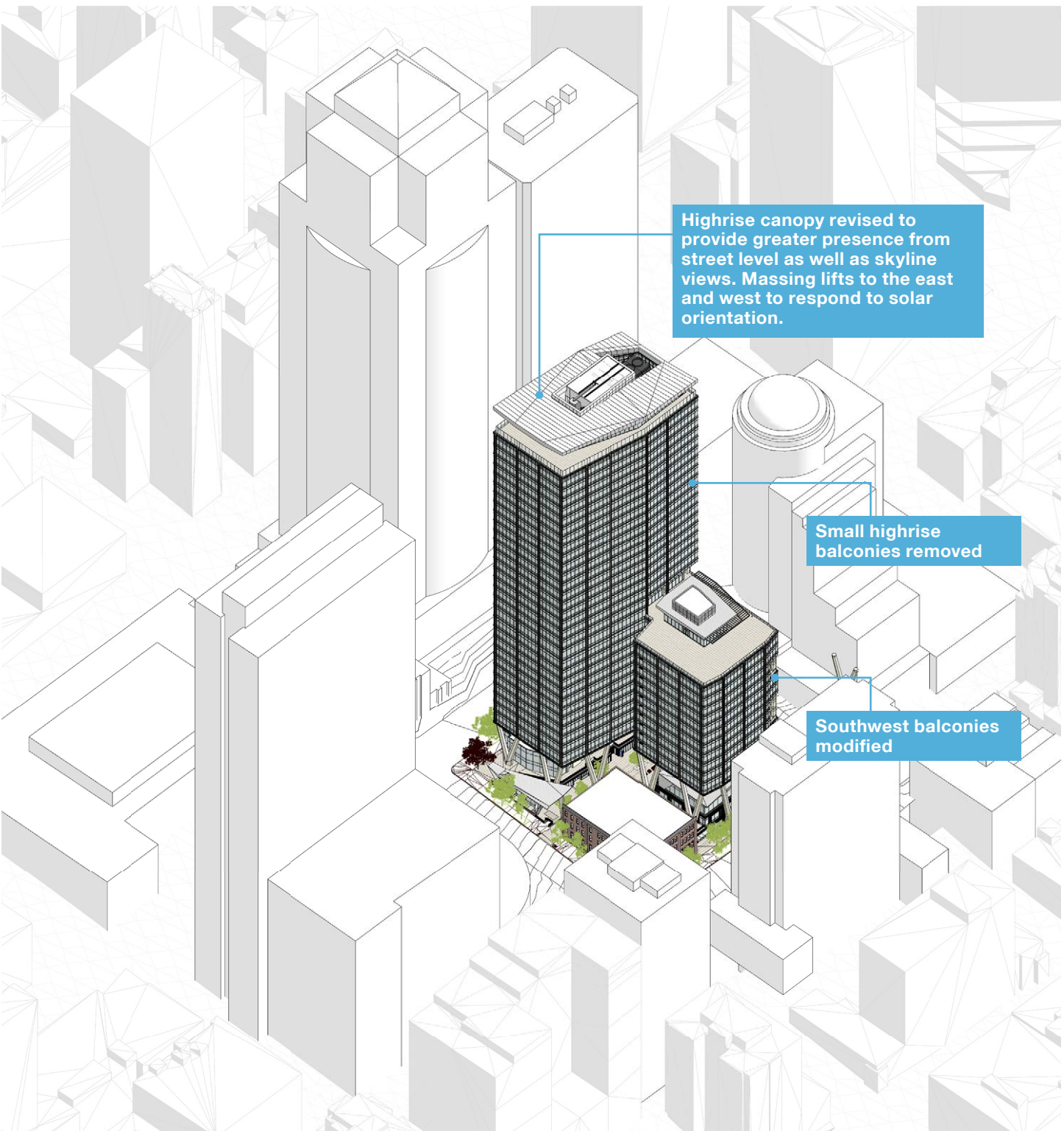


Northeast Aerial

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d

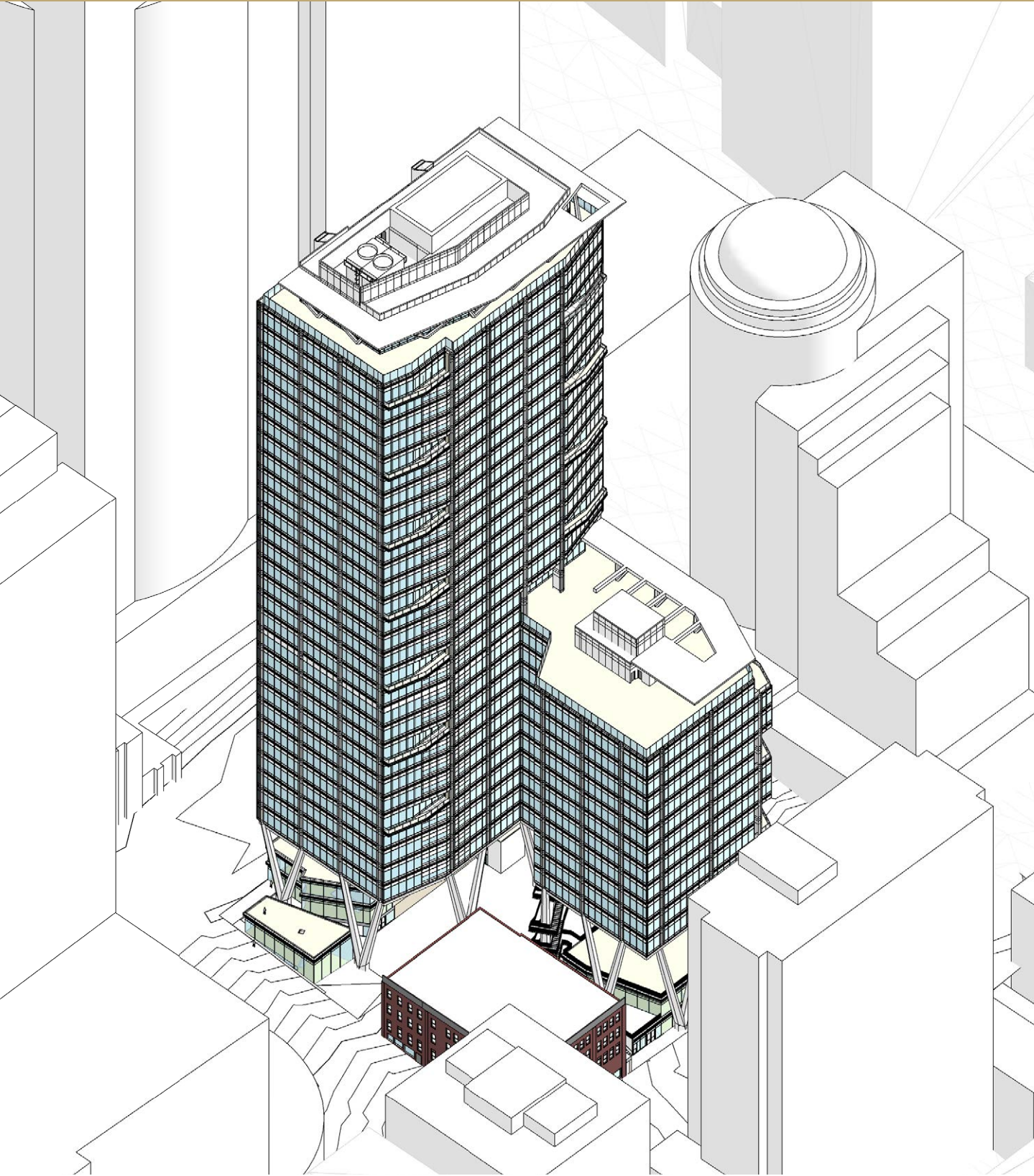


Northwest Aerial



Northwest Aerial

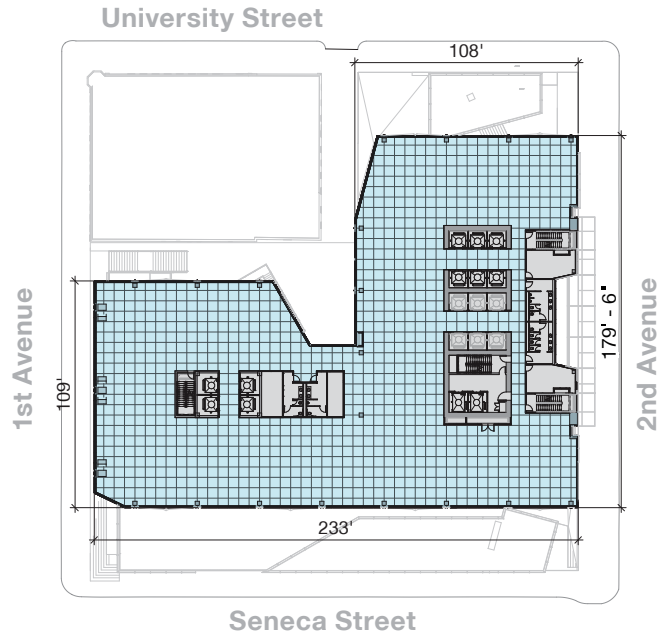
- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



View from Northwest



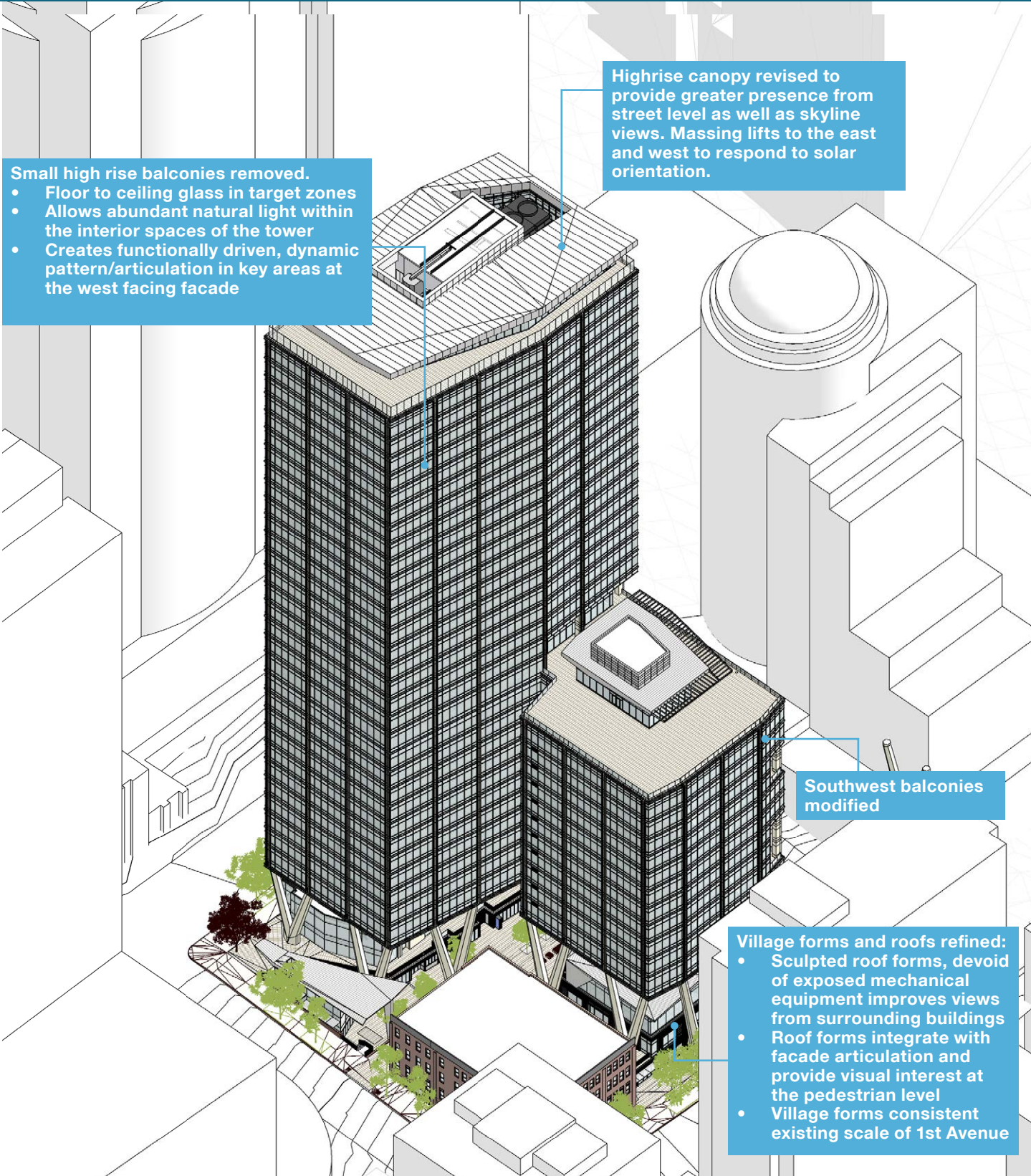
Southeast Tower Intersection



Low Rise Tower Plan

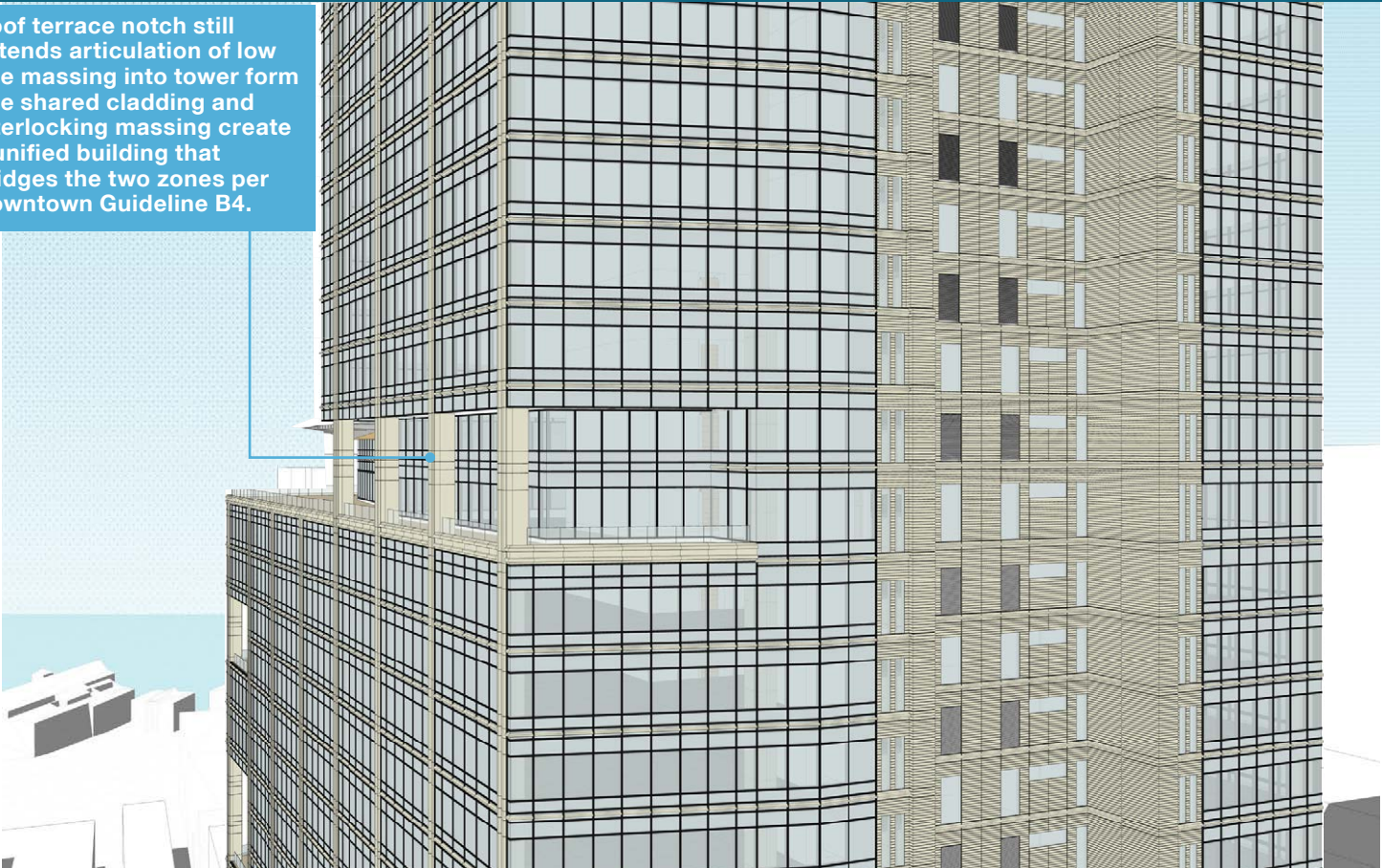
CURRENT

- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d

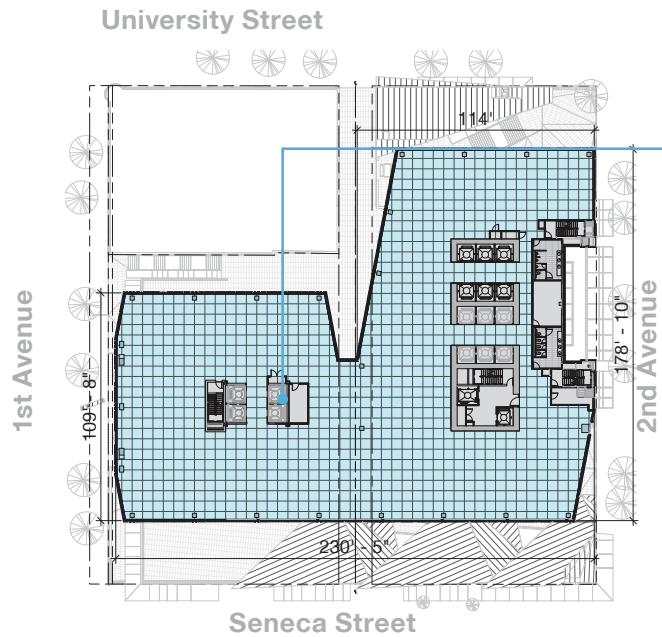


View from Northwest

- Roof terrace notch still extends articulation of low rise massing into tower form
- The shared cladding and interlocking massing create a unified building that bridges the two zones per Downtown Guideline B4.



Southeast Tower Intersection



- Notch revised (approximately 20 ft x 30 ft at widest point):
- Creates more efficient and usable space
 - Brings late afternoon and evening daylight into the Piazza below to provide inviting and usable open space per Downtown Guideline D1
 - Brings daylight into tenant office space and creates more occupied vision glass looking into the Piazza
 - Further articulates low rise and tower massing



7

Lighting

CURRENT

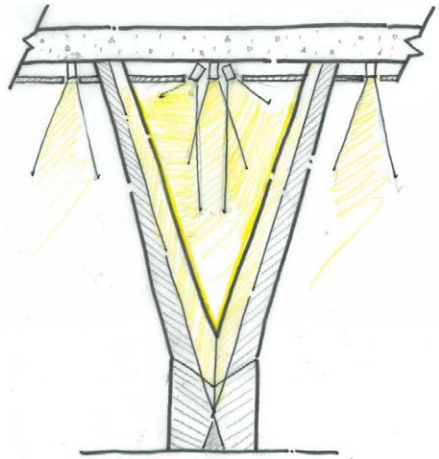
- 5a
- 5b
- 5c
- 5d
- 5e
- 5f
- 6a
- 6b
- 6c
- 6d
- 6e
- 6f
- 7a
- 7b
- 7c
- 7d



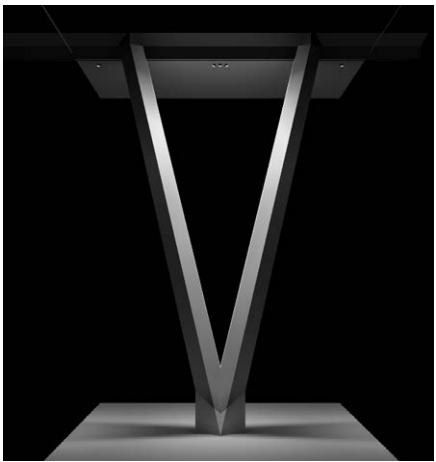
Plaza Level Lighting Plan

LIGHT LEVEL KEY

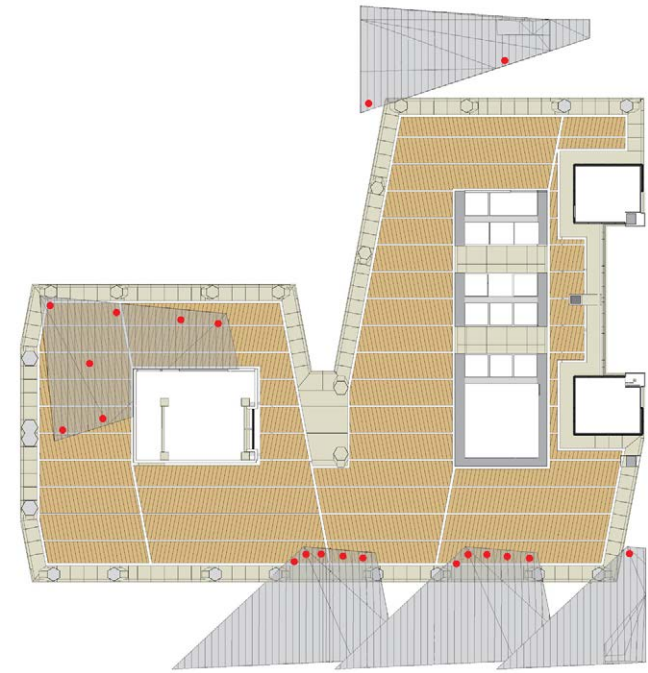
- | | |
|---|--------------------------------------|
| Stairs and Ramps
2.0 Foot-candles | Pedestrian Entries
4 Foot-candles |
| Plazas & Open Space Areas
2.0 Foot-candles | Features
5:1 Contrast Ratio |
| Vehicle Entries
4 Foot-candles | Existing Street Lights |



COLUMN LIGHTING SKETCH
Column Lighting Detail



COLUMN LIGHTING RENDERED



SOFFIT LIGHTING PLAN



SOFFIT LIGHTING RENDERED
Soffit Lighting Detail

CURRENT

5a

5b

5c

5d

5e

5f

6a

6b

6c

6d

6e

6f

7a

7b

7c

7d



LED Downlight mounted in awning structure to illuminate covered walkways



LED steplights mounted in concrete wall at alley entrance



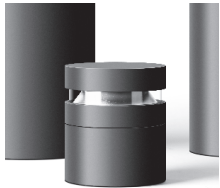
LED handrail downlight to illuminate stairs and ramps



In-grade LED grazing fixture to illuminate geologic wall feature



Exterior rated LED flexible tape mounted in extrusion at timber seating



In-grade mounted landscape lighting



Cafe string lights mounted from poles or structural armature over exterior amenity space

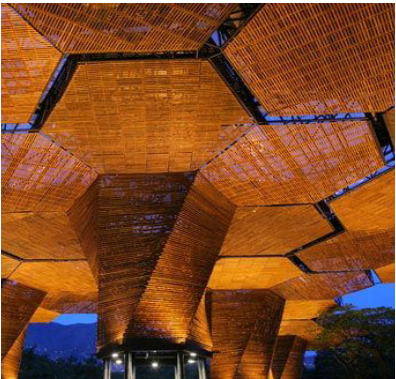
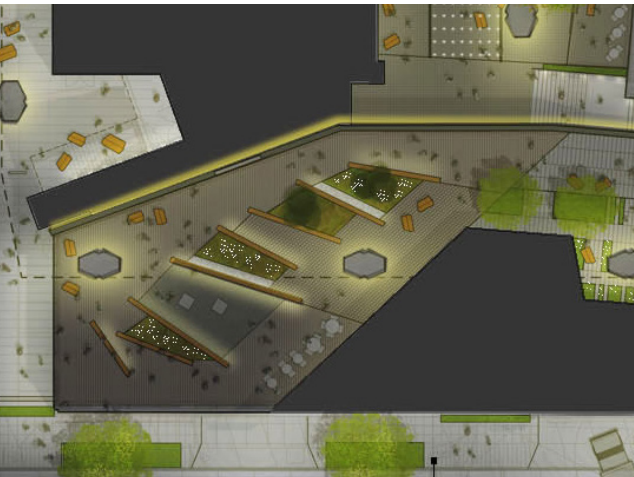


Surface mounted asymmetric uplight mounted to solid facade member to uplight tower soffits



Floodlight at village roof aimed at soffit for plaza illumination and mounted from plaza soffit to illuminate columns

Site Fixture Examples



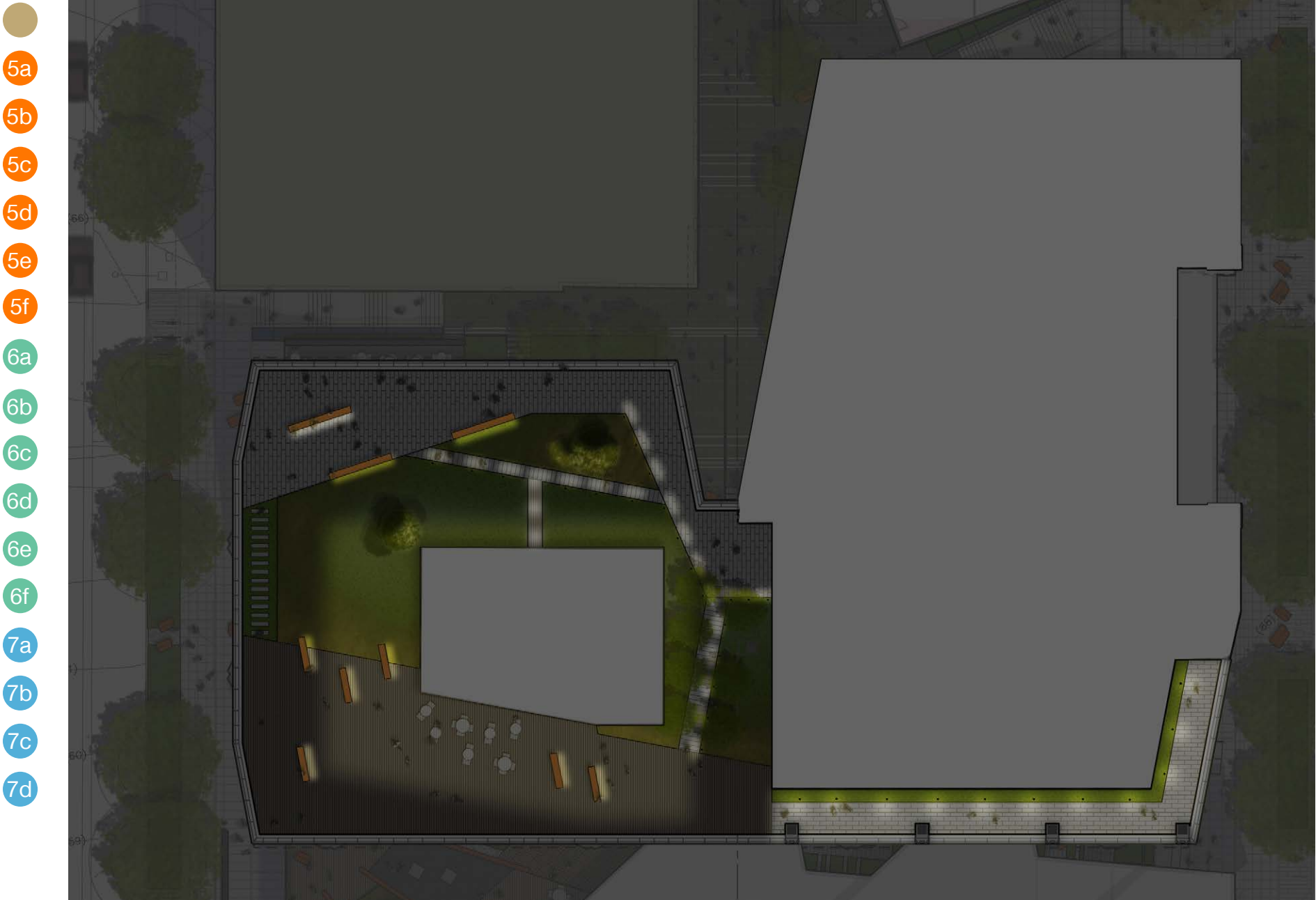
OVERLOOK PLAZA

ALLEY PLAZA

CREATIVE COMMONS

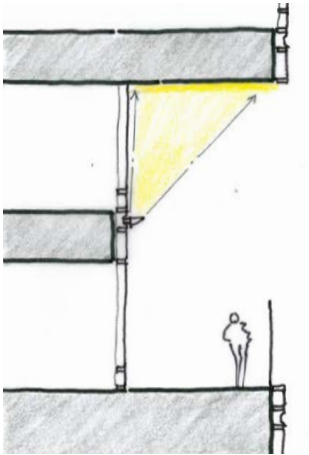
Overlook Plaza, Alley Plaza + Creative Commons

CURRENT

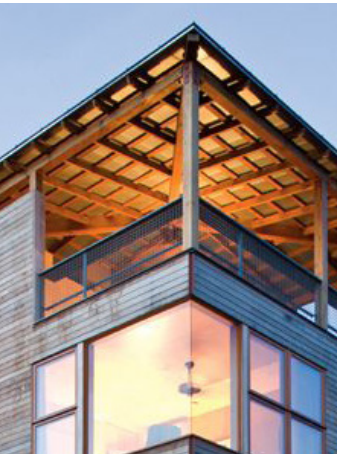


Level 19 Lighting Plan

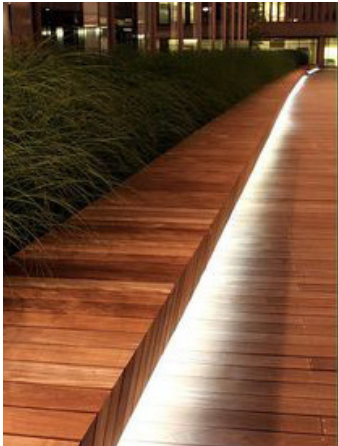
Level 19 Lighting Strategies



SOFFIT LIGHTING SKETCH



SOFFIT PRECEDENT IMAGES



BENCHES



PATHWAYS



LOW LEVEL LANDSCAPE



TREES

8

Proposed Departures

PROPOSED DEPARTURE #1 - OPEN SPACE STANDARDS

Code Citation & Requirement

SMC 23.49.016.C

C. Standards for Open Space. To satisfy this requirement, open space may be provided on-site or off-site, as follows:

1. Private Open Space. Private open space on the project site or on an adjacent lot directly accessible from the project site may satisfy the requirement of this section. Such space shall not be eligible for bonuses. Private open space shall be open to the sky and shall be consistent with the general conditions related to landscaping; seating and furnishings contained in the Downtown Amenity Standards. Private open space satisfying this requirement must be accessible to all tenants of the building and their employees.

Proposed Design Departure & Rationale

In the proposed design, 27.7% of the total required Open Space has OVERHEAD WEATHER PROTECTION.

The total OPEN SPACE is provided in 3 locations: 1,250 SF combined at Retail Village at EL+90 and EL+78 is OPEN TO SKY.

12,170 SF provided at Level 19, of which 3,715 SF is UNDER COVER OF OVERHEAD PROTECTION.

The eastern portion of the Level 19 terrace has overhead protection created by the double-height notch of the main highrise tower.

The western portion of the Level 19 terrace has overhead protection created by a canopy around the lowrise elevator and stair core that provides tenant access to and egress from the Open Space terrace.

The SMC 23 references that Private Open Space should be consistent with Downtown Amenity Standards.

The Downtown Amenity Standards for "Neighborhood Open Spaces permit up to 20 percent of the neighborhood open

Proposed Design Departure & Rationale (cont.)

space may be covered to accommodate activities that complement use of the space and make it more comfortable and usable, such as ... overhead weather protection."

The DAS for "Urban Plazas" permit "limited coverage may be appropriate to increase activity in the space and provide for the comfort of the users, while maintaining the overall character of the space as an extension of the outdoor public street environment."

The overhanging roof also provides visual screening of the elevator overrun for people below.

The proposed departure has the following benefits supporting the Design Guidelines:

A2 Enhance the Skyline

The overhanging roof helps screen the roof terrace elevator overrun from view from below.

B4 Design a Well-Proportioned and Unified Building

The form and materiality of the overhang visually relate to the retail village roof overhangs, and the overhang at the tower top, thus unifying the building at the mid-height level.

D1 Provide Inviting and Usable Open Space

The overhanging roof provides some shelter from wind, rain, and sun, to allow the space to be used in more swing seasons. It also screens the elevator overrun bulkhead from the user's view.

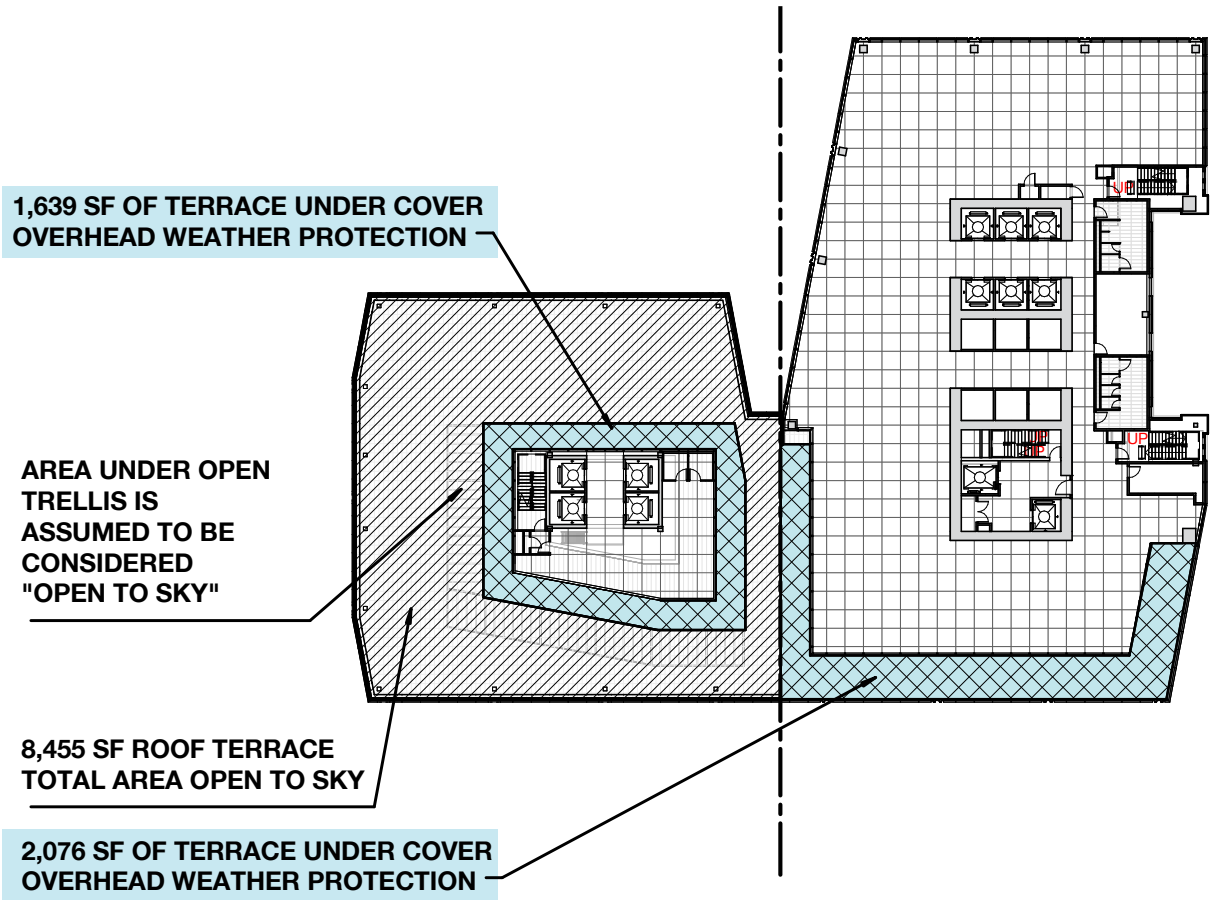
D3 Provide elements that define the Place

It visually connects the space to the open spaces at the retail village below, to create a sense of place.

Downtown Amenity Standards for Neighborhood Open Spaces" and "Urban Plazas"

Permit portions of the space to be covered to accommodate activities and make it more comfortable and usable, such as overhead weather protection.

Departure Diagram



1 / Level 19 Roof Terrace Plan

Area Not Open to Sky = Departure

670,278 GFA OFFICE / 1,000 = 671 GFA x 20 SF = 13,420 SF OPEN SPACE REQUIRED

1,250 SF	PROVIDED AT RETAIL VILLAGE
12,170 SF	PROVIDED AT LEVEL 19 LOW-RISE ROOF TERRACE
13,420 SF	TOTAL PROVIDED COMPLIES

DEPARTURE REQUESTED FOR AREA UNDER COVER OF OVERHEAD WEATHER PROTECTION

1,639 SF	UNDER OVERHANGING ROOF AT ELEVATOR CORE
2,076 SF	UNDER DOUBLE-HEIGHT ROOF OF TOWER ABOVE
3,715 SF	TOTAL AREA UNDER COVER

3,715 / 13,420 = **27.7% UNDER OVERHEAD WEATHER PROTECTION (DEPARTURE #01 REQUESTED)**

PROPOSED DEPARTURE #2 - OVERHEAD WEATHER PROTECTION

Code Citation & Requirement	Proposed Design Departure & Rationale (cont.)	Proposed Design Departure & Rationale (cont.)
<p>SMC 23.49.018 - Overhead Weather Protection and Lighting.</p> <p>A. <u>Continuous overhead weather protection shall be required for new development along the entire street frontage of a lot except along those portions of the structure facade that:</u></p> <ul style="list-style-type: none">1. are located farther than five (5) feet from the street property line or widened sidewalk on private property; or2. abut a bonused open space amenity feature; or3. are separated from the street property line or widened sidewalk on private property by a landscaped area at least two (2) feet in width; or4. are driveways into structures or loading docks. <p>B. <u>Overhead weather protection shall have a minimum dimension of eight (8) feet measured horizontally from the building wall or must extend to a line two (2) feet from the curb line, whichever is less.</u></p> <p>C. <u>The installation of overhead weather protection shall not result in any obstructions in the sidewalk area.</u></p> <p>D. <u>The lower edge of the overhead weather protection must be a minimum of ten (10) feet and a maximum of fifteen (15) feet above the sidewalk.</u></p> <p>E. <u>Adequate lighting for pedestrians shall be provided. The lighting may be located on the facade of the building or on the overhead weather protection.</u></p>	<p>The scale is also appropriate to the pavilion-scale of the northeast retail building.</p> <p>B1 Respond to Neighborhood Context The proposed reduction in width of overhead weather protection reinforces the desirable existing mature landscaping on University Street (a continuation of a Green Street corridor).</p> <p>D2 Enhance the Building with Landscaping The SDOT requirements for clearance around trees will allow the existing mature trees on University St to remain. This portion of University St is contiguous with other portions of a Green Street corridor.</p> <p>SDOT requirements for Street Trees Minimum 3 ft clear from centerline of trees. This requirement supersedes the overhead weather protection dimensional requirements.</p> <p>C5 Encourage overhead weather protection. As recommended in C5, the design at Univeristy St is related to “the scale of the space defined by the height and depth of the weather protection” and the pavilion-scale of the northeast retail building.</p> <p>2B. SENECA ST: Proposed to have 5 FT WIDE OVERHEAD WEATHER PROTECTION instead of 8 FT wide DUE TO CLOSENESS OF NEW TREES AND NARROW SIDEWALK PAVING WIDTH.</p> <p>On Seneca Street, new trees will be planted 9 ft from the property line, so the 8 ft canopy would be 1 ft from the centerline of the trunk. SDOT will require a min 3 ft radius canopy. The sidewalk paving width is typically 6 ft, so 8 ft canopies would extend beyond the walking surface and over the planting beds. The proposed 5 ft wide canopies, suggested by Land Use Comments, should allow the new trees to grow. This street frontage does not include building entries. The proposed design continues the 5 ft width of overhead weather protection along the entire facade due to the narrow sidewalk, and to unify the facade (the street</p>	<p>is steeply sloped, and the overhead protection must step in height multiple times, and for visual continuity, it is preferable to have a consistent width, rather than stepping in both plan and section).</p> <p>B1 Respond to Neighborhood Context The proposed reduction in width of overhead weather protection responds to the existing narrow width of the Seneca St sidewalk, by providing protection at a scale that is responsive to the walking and planting zones.</p> <p>D2 Enhance the Building with Landscaping The SDOT requirements for clearance around trees will allow the new trees on Seneca to thrive.</p> <p>SDOT requirements for Street Trees Minimum 3 ft clear from centerline of trees. This requirement supersedes the overhead weather protection dimensional requirements.</p> <p>C5 Encourage overhead weather protection. As recommended in C5, the design of protection along Seneca St is related to “the scale of the space defined by the height and depth of the weather protection” and the narrow sidewalk/planting zone.</p> <p>2C. SENECA ST: SHORT PORTIONS of the Overhead Protection are SLIGHTLY ABOVE OR BELOW THE MINIMUM 10 FT AND MAXIMUM 15 FT ABOVE SIDEWALK.</p> <p>On Seneca Street, due to the steep slope of 12.6%, the overhead protection must step multiple times over the length of the building frontage. In order to unify the facade and for visual continuity, the number of steps has been kept to six (6) stepping components that are integrated into the facade modulation, massing, program, and glazing.</p> <p>B4 Design a well-proportioned and unified building. The proposed stepping of the overhead weather protection is derived in close correlation to features noted in B4, such as the “relative sizes and shapes of distinct building</p>
<p>Proposed Design Departure & Rationale</p> <p>2A. UNIVERSITY ST: Proposed to have 5 FT WIDE OVERHEAD WEATHER PROTECTION instead of 8 FT wide DUE TO MATURE TREES.</p> <p>On University St., 8 ft wide overhead weather protection would conflict with mature tree canopies, and require significant pruning. The proposed 5 ft wide canopies, suggested by Land Use Comments, should clear the branches with minimal or no pruning. This street frontage does not include building entries.</p>		

PROPOSED DEPARTURE #2 - OVERHEAD WEATHER PROTECTION

Proposed Design Departure & Rationale (cont.)

volumes”, and “façade modulation and articulation; windows and fenestration patterns; ... garage entries”.

C5 Encourage overhead weather protection.

As recommended in C5, the design of protection along Seneca St is related to “the overall architectural concept of the building.”

2D. SECOND AVE: A 24 FT LONG PORTION of Overhead Protection near the corner of 2nd & Seneca is proposed to be 5 FT WIDE TO CONNECT WITH THE CONTIGUOUS PROTECTION AT SENECA.

At the southern corner at Seneca Street, 5 ft wide protection is proposed to match the proposed 5 ft wide protection at Seneca that wraps the corner, and to relate to the small scale of the retail pavilion, in comparison to the 8 ft protection at the main tower lobby entry.

B4 Design a well-proportioned and unified building.

The proposed reduction in depth at the corner of 2nd & Seneca, allows the proposed 5 ft deep canopies (due to Seneca St trees and sidewalk planter widths) to wrap consistently around the corner; this is responsive to attention to “corner features” as noted in B4.

C5 Encourage overhead weather protection.

As recommended in C5, the design of protection at this retail corner is related to “the scale of the space defined by the height and depth of the weather protection” and the pavilion-scale of the southeast retail building.

2E. SECOND AVE: AT THE MAIN LOBBY FRONTAGE, SEVERAL GAPS are proposed at the main structural articulation, and near the north egress stair. None of these gaps occur at entries.

On 2nd Avenue, 8 ft wide protection is provided at the main tower facade, with small breaks proposed in the overhead protection at the columns, to allow the facade articulation to continue to grade, and at the northeast end of the lobby near the exit stair, to focus pedestrian attention to the main tower entry.

Proposed Design Departure & Rationale (cont.)

C4 Reinforce building entries

The canopy length and placement at the central zone of the lobby focuses the pedestrian towards the lobby entrance, while de-emphasizing the egress stair at the north end of the lobby.

C2 Design facades of many scales

The main canopy massing reinforces the primary building lobby, while the small gaps allowing the verticality of the main tower structure to be legible to grade, adding “building modulation and articulated structural bays establish as frameworkd for composing facades” as recommended in C2.

C5 Encourage overhead weather protection.

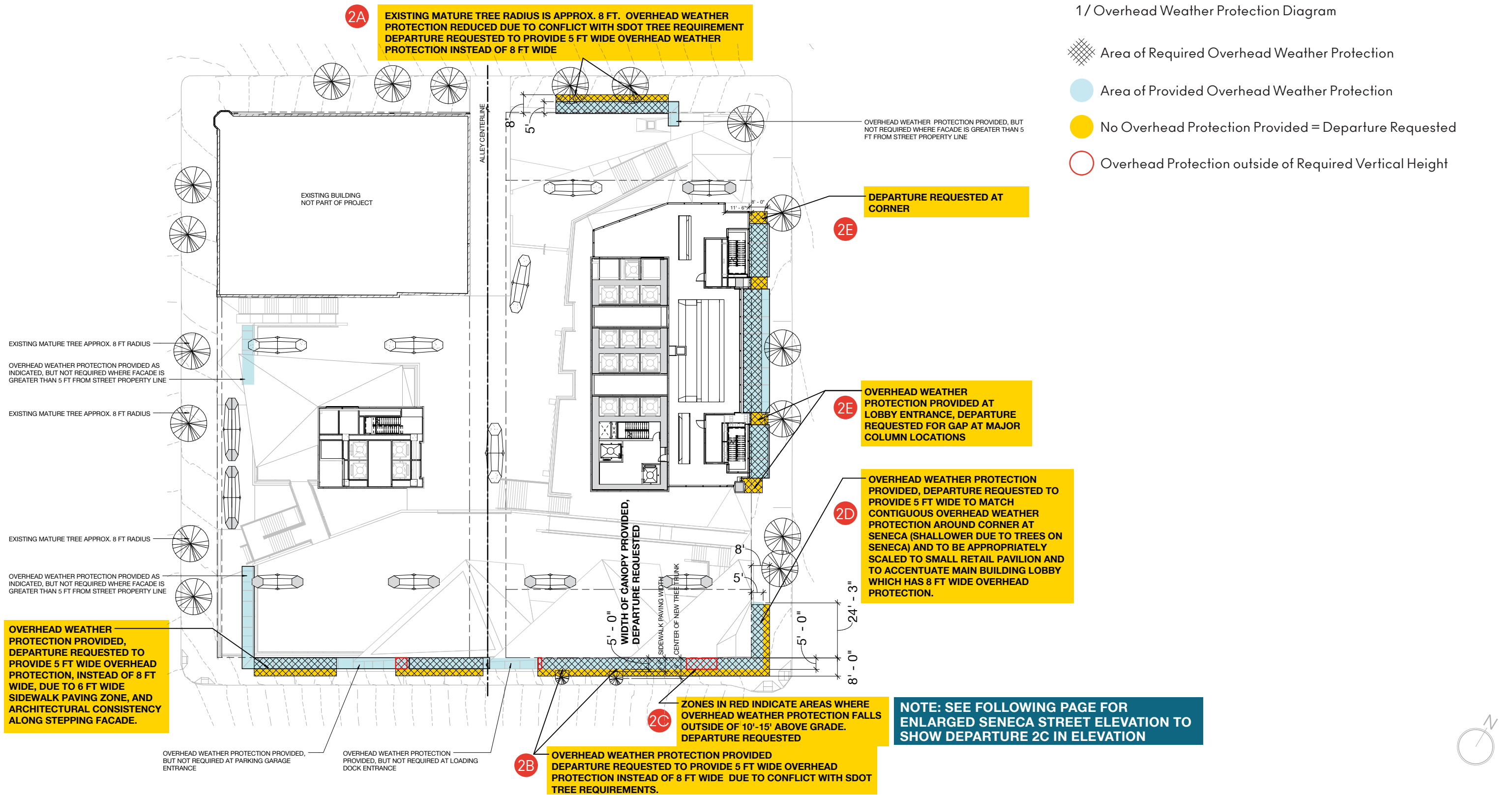
As recommended in C5, the design of the protection at the main lobby at 2nd Ave is related to “the overall architectural concept of the building; and uses occurring within the building (such as entries...)”

NOTE:
PLEASE SEE DEPARTURE DIAGRAMS ON FOLLOWING PAGES.

PROPOSED DEPARTURES |

PROPOSED DEPARTURE #2 - OVERHEAD WEATHER PROTECTION

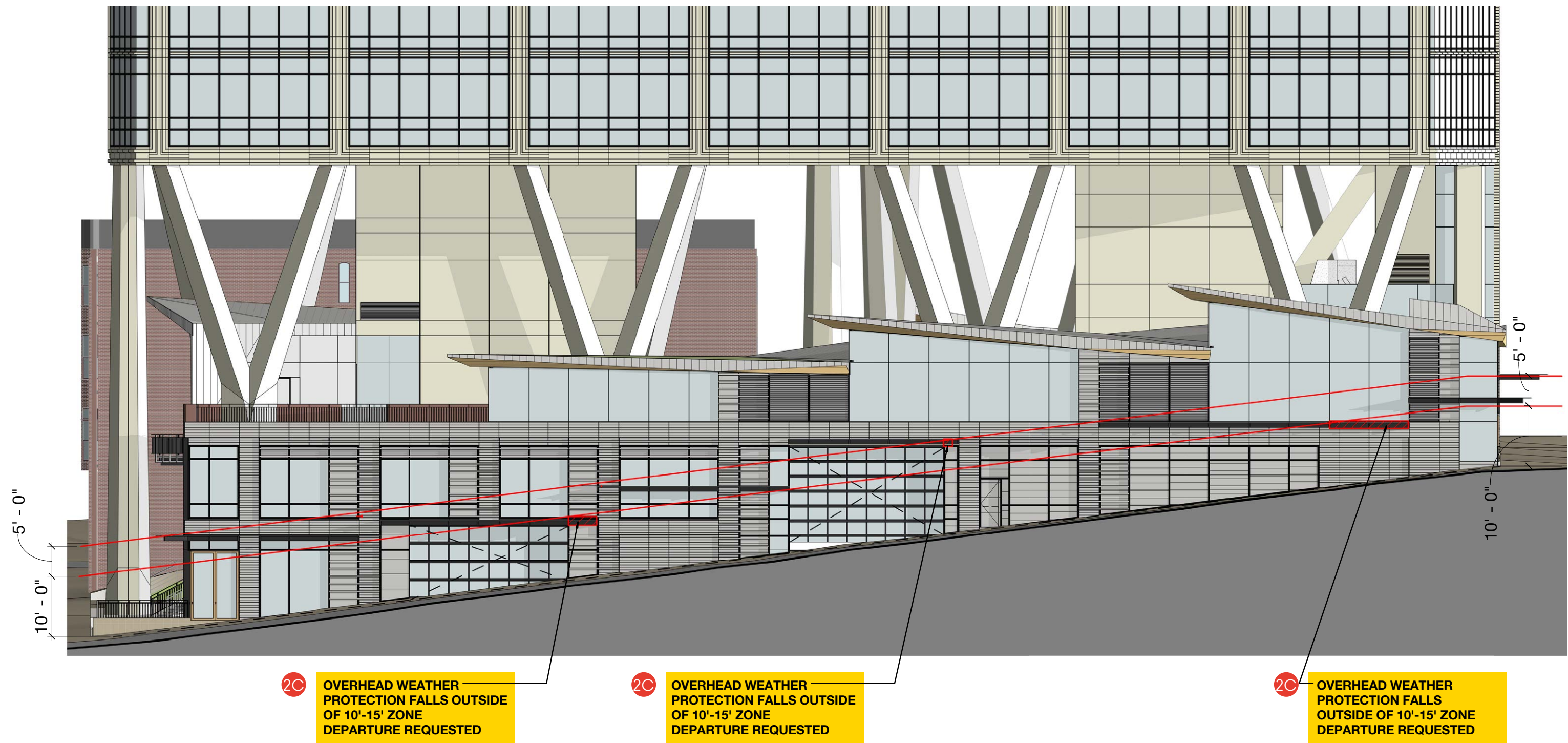
Departure Diagram



PROPOSED DEPARTURES |

PROPOSED DEPARTURE #2 - OVERHEAD WEATHER PROTECTION

Departure Diagram



2 / Overhead Weather Protection - Seneca Street

PROPOSED DEPARTURES |

PROPOSED DEPARTURE #3 - MINIMUM FACADE HEIGHTS

Code Citation & Requirement

SMC 23.49.056.A.1 / A.2 - Minimum Facade Heights

A. Minimum facade height

1. Minimum facade height(s) are prescribed in Table A for 23.49.056 and Exhibit A for 23.49.056, but minimum facade heights do not apply if all portions of the structure are lower than the elevation of the required minimum facade height.

Table A for 23.49.056
Minimum Façade Height

Streets requiring property line facades
DOC1, DOC2, DMC: 35 feet
Applicable Street: 1st Avenue

Class I pedestrian streets
DOC 1, DOC 2: 35 feet
DMC: 25 feet

Applicable Streets: 1st Avenue, 2nd Avenue, University Street

Class II pedestrian streets
DOC 1, DOC 2: 25 feet
DMC: 15 feet

Applicable Street: Seneca Street

Designated green streets
DOC1, DOC2, DMC: 25 feet
*Except as provided in subsection 23.49.056.A.2 regarding view corridor requirements.

Proposed Design Departure & Rationale

Portions of the retail village facades on all 4 frontages do not meet the minimum façade height, especially at the street corners where the higher avenue requirements wrap the corner, and at publicly accessible open spaces (Ref. MUP-016).

Proposed Design Departure & Rationale (cont.)

The proposed departure would allow a design for a series of retail massings with ramps and stepped elements that are lower than the required facade height. The resulting groundscape has unique site-specific benefits which reinforce the intent of the Design Guidelines.

The proposed departure has the following benefits supporting the Design Guidelines:

C1 Promote pedestrian interaction

Portions of the retail village roofs are lower than the minimum proscribed height, in order to allow more light into the mid block plaza and retail roof terraces, and to create a more welcoming presence that encourages pedestrians to access the roof terraces, overlooks and gardens.

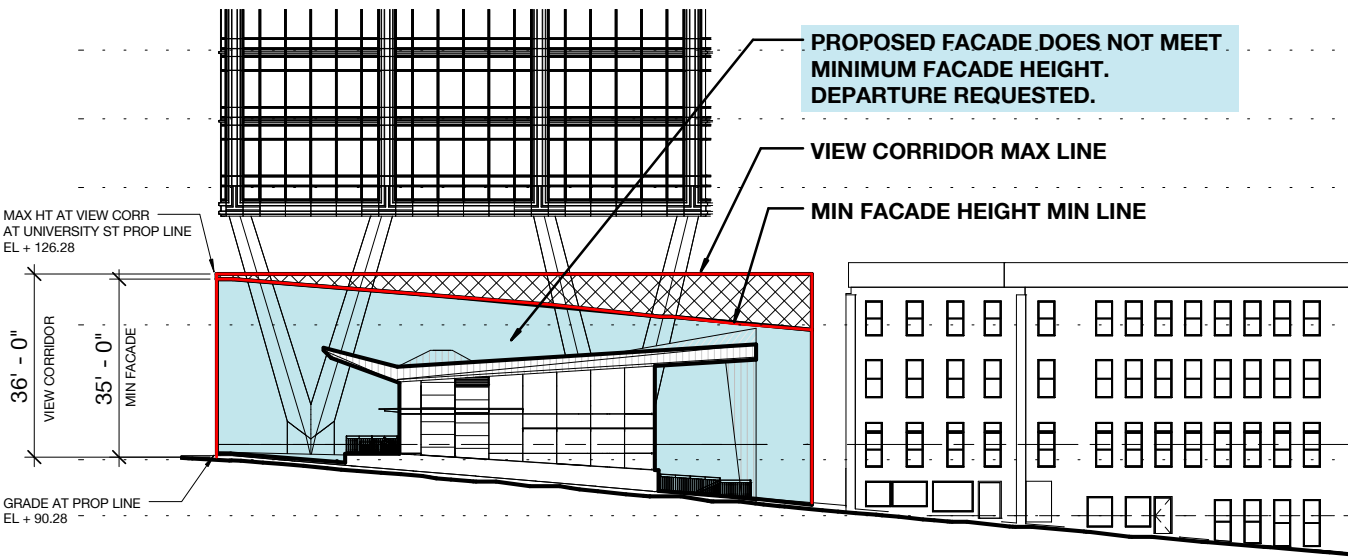
C2 Design facades of many scales

The more intimate height of the retail village creates a welcoming pedestrian scale at the base of the larger tower, and creates an interplay of scale with the larger Y columns supporting the tower lift.

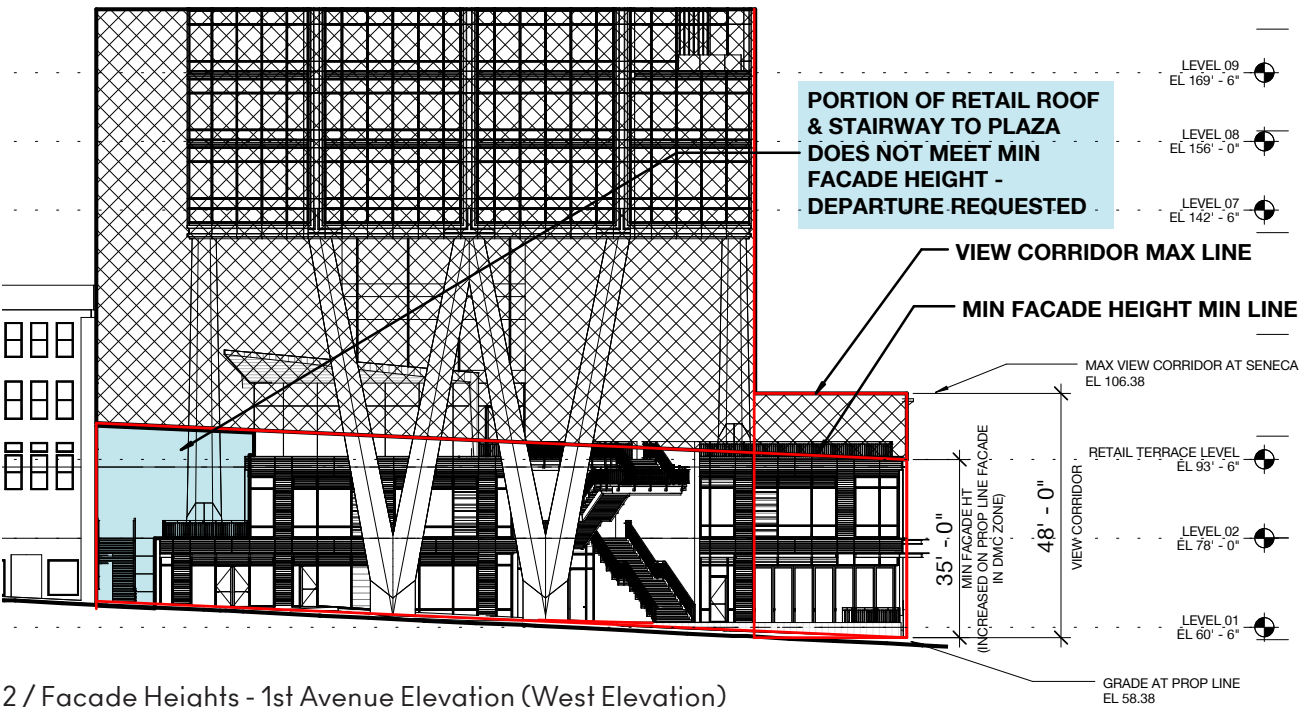
D3 Provide Elements that Define the Place

The unique groundscape design at the base of 2+U provides multi-level retail and lobby elements. Due to the 30 ft. grade change across the site, there are opportunities to access the roofscapes of the lower 1st Ave retail from grade at the 2nd Ave and upper Seneca portions of the site. To enhance that opportunity, a series of exterior ramps and steps are incorporated into the massing. This unique series of interlocking spaces and views creates a memorable, site-specific environment that encourages a wide variety of programmed and spontaneous activities.

Departure Diagrams



1 / Facade Heights - University Street Elevation (North Elevation)



2 / Facade Heights - 1st Avenue Elevation (West Elevation)

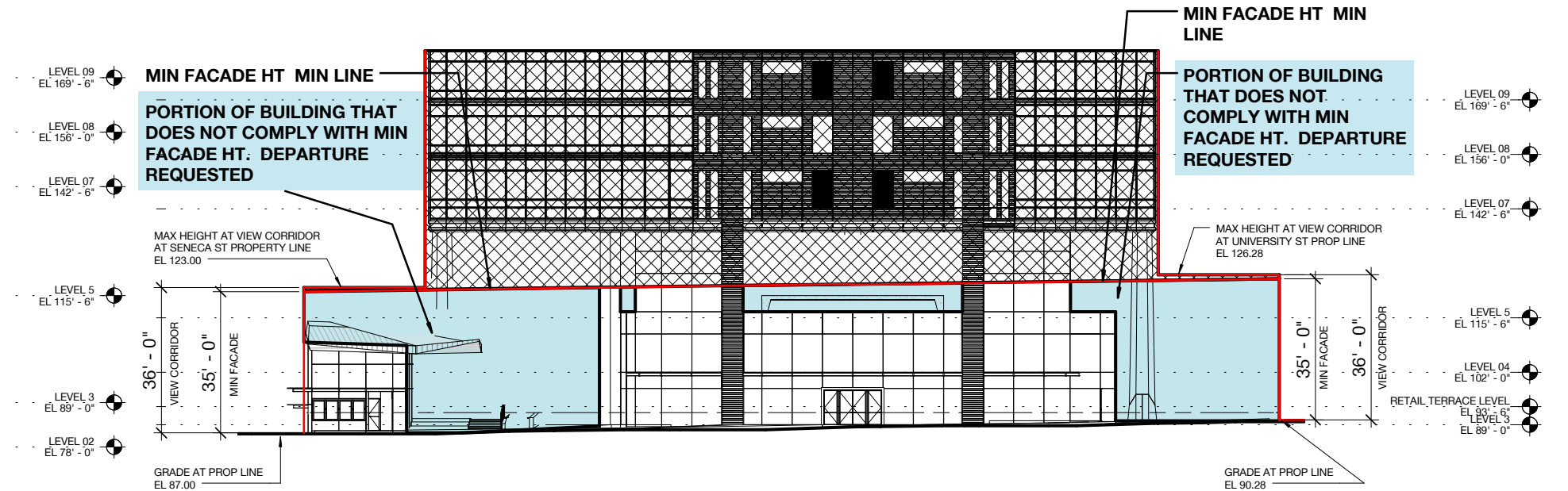
Area of Requested Departure

NOTE:
DEPARTURE DIAGRAMS CONTINUE ON FOLLOWING PAGE.

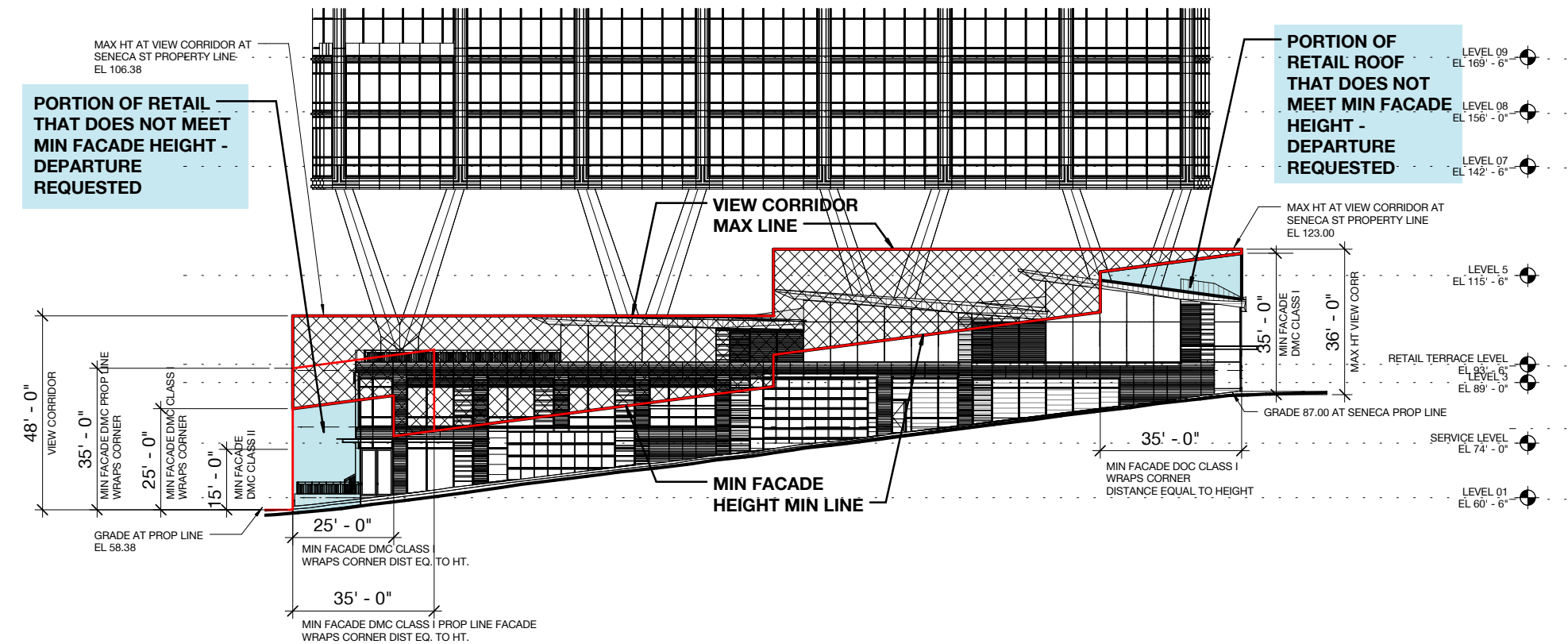
PROPOSED DEPARTURES |

PROPOSED DEPARTURE #3 - MINIMUM FACADE HEIGHTS

Departure Diagrams (Cont.)



3 / Facade Heights - 2nd Avenue Elevation (East Elevation)



4 / Facade Heights - Seneca Street Elevation (South Elevation)

Area of Requested Departure

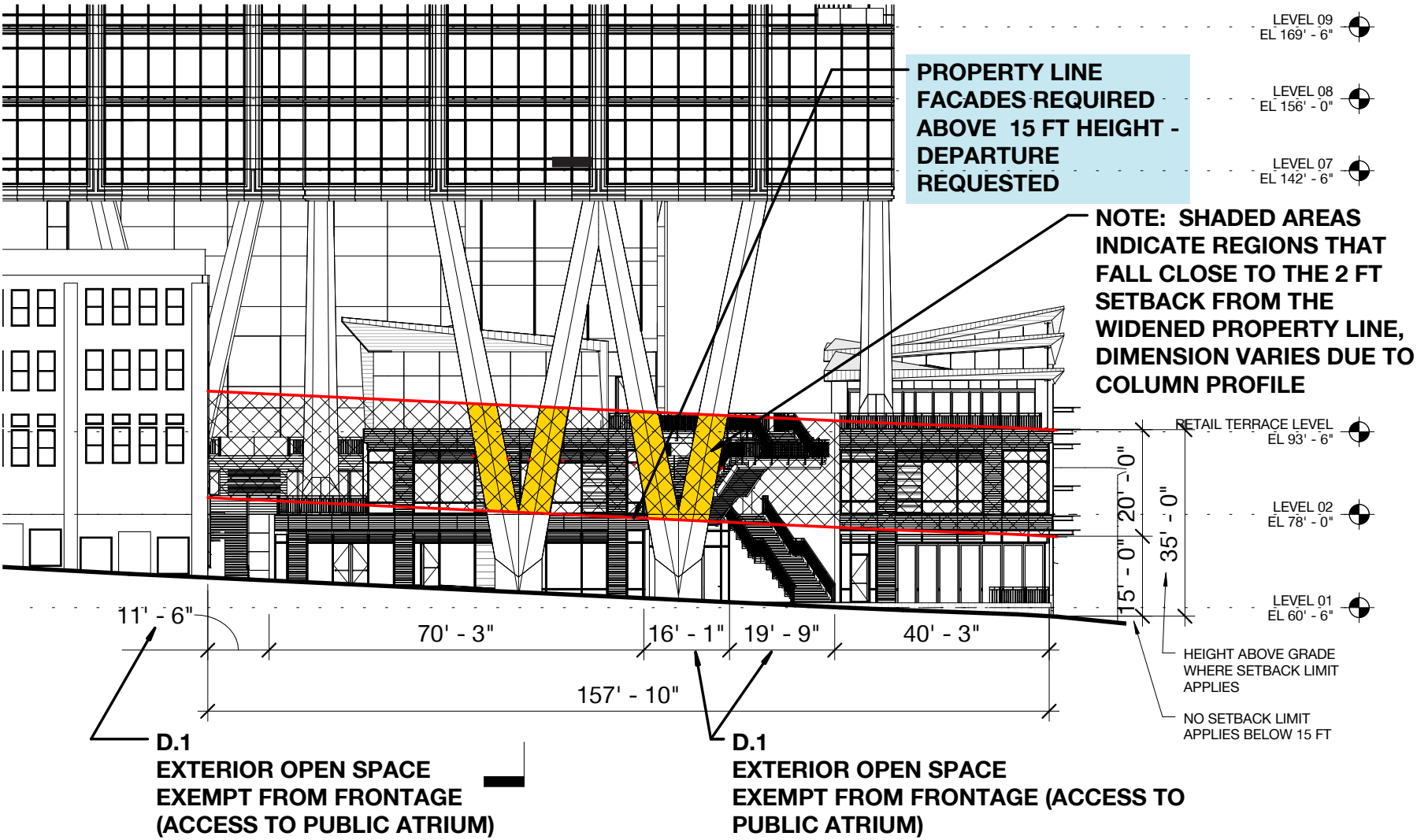
PROPOSED DEPARTURE #4 - PROPERTY LINE FACADES

Code Citation & Requirement	Code Citation & Requirement (cont.)	Proposed Design Departure & Rationale (cont.)
<p>SMC 23.49.056.B.1 - Property Line Facades</p> <p>B. Facade setback limits</p> <p>1. Setback limits for property line facades. The following setback limits apply to all streets designated on Map 1H as requiring property line facades, except as specified in subsection 23.49.056.B.1.d.</p> <p>a. The facades of structures 15 feet or less in height shall be located within 2 feet of the street lot line.</p> <p>b. Structures greater than 15 feet in height are governed by the following criteria:</p> <p>1) No setback limits apply up to an elevation of 15 feet above sidewalk grade.</p> <p>2) <u>Between the elevations of 15 and 35 feet above sidewalk grade, the facade shall be located within 2 feet of the street lot line, except that:</u></p> <p>a) <u>Any exterior public open space that satisfies the Downtown Amenity Standards, whether it receives a bonus or not, and any outdoor common recreation area required for residential uses, is not considered part of the setback.</u></p> <p>b) Setbacks between the elevations of 15 and 35 feet above sidewalk grade at the street lot line are permitted according to the following standards, as depicted in Exhibit B for 23.49.056:</p> <p>i. The maximum setback is 10 feet.</p> <p>ii. The total area of a facade that is set back more than 2 feet from the street lot line shall not exceed 40 percent of the total facade area between the elevations of 15 and 35 feet.</p> <p>iii. No setback deeper than 2 feet shall be wider than 20 feet, measured parallel to the street lot line.</p>	<p>iv. The facade of the structure shall return to within 2 feet of the street lot line between each setback area for a minimum of 10 feet. Balcony railings and other non-structural features or walls are not considered the facade of the structure.</p> <p>c) If sidewalk widening is required by Section 23.49.022, setback standards shall be measured to the line established by the new sidewalk width rather than the street lot line.</p> <p>Proposed Design Departure & Rationale</p> <p>In the proposed design, 81.5% of the façade along First Ave between 15 FT- 35 FT high is setback greater than the permitted 10 FT setback from the sidewalk widening line.</p> <p>The proposed design requires a departure to extend the street level setback up to the roof of the retail elements, which are greater than 15 ft. The tower above and its structural columns, meet the sidewalk widening property line, and create an urban scale arcade along 1st Ave as frontage for the retail spaces along 1st Ave. The proposed departure benefits pedestrians and supports the intent of the Design Guidelines.</p> <p>The proposed departure has the following benefits supporting the Design Guidelines:</p> <p>C2 Design a Facade of Many Scales</p> <p>A signature feature of 2+U is the large-scale structure that elevates the tower over the pedestrian-scale retail and groundscape components at the base, creating memorable juxtapositions and a rich variety of materials and scales.</p> <p>D3 Provide Elements that Define the Place</p> <p>The proposed design uses the tower’s larger, authentic structure to create a grand urban-scaled colonnade along 1st Ave, paired with autonomous, pedestrian-scaled retail forms. This repetition of columns defines a unique semi-covered space, allowing retail or dining to spill out and claim ownership over the widened sidewalks. The greater height permits more southwestern sun and daylight into the retail, and into outdoor</p>	<p>spaces at street level and on terraces above. The continuous tower overhang unifies the space better than the undulating modulations permitted as-of-right.</p> <p>C4 Reinforce Building Entries</p> <p>The urban colonnade also creates a visual presence for the 1st Ave office lobby while maintaining the Design Guideline goal of minimal office lobby frontage where Street Level Uses are required.</p> <p>C5 Encourage Overhead Weather Protection</p> <p>The urban colonnade also creates a large sheltered porch along 1st Ave.</p> <div><p>NOTE:</p><p>PLEASE SEE DEPARTURE DIAGRAMS ON FOLLOWING PAGE.</p></div>

PROPOSED DEPARTURES |

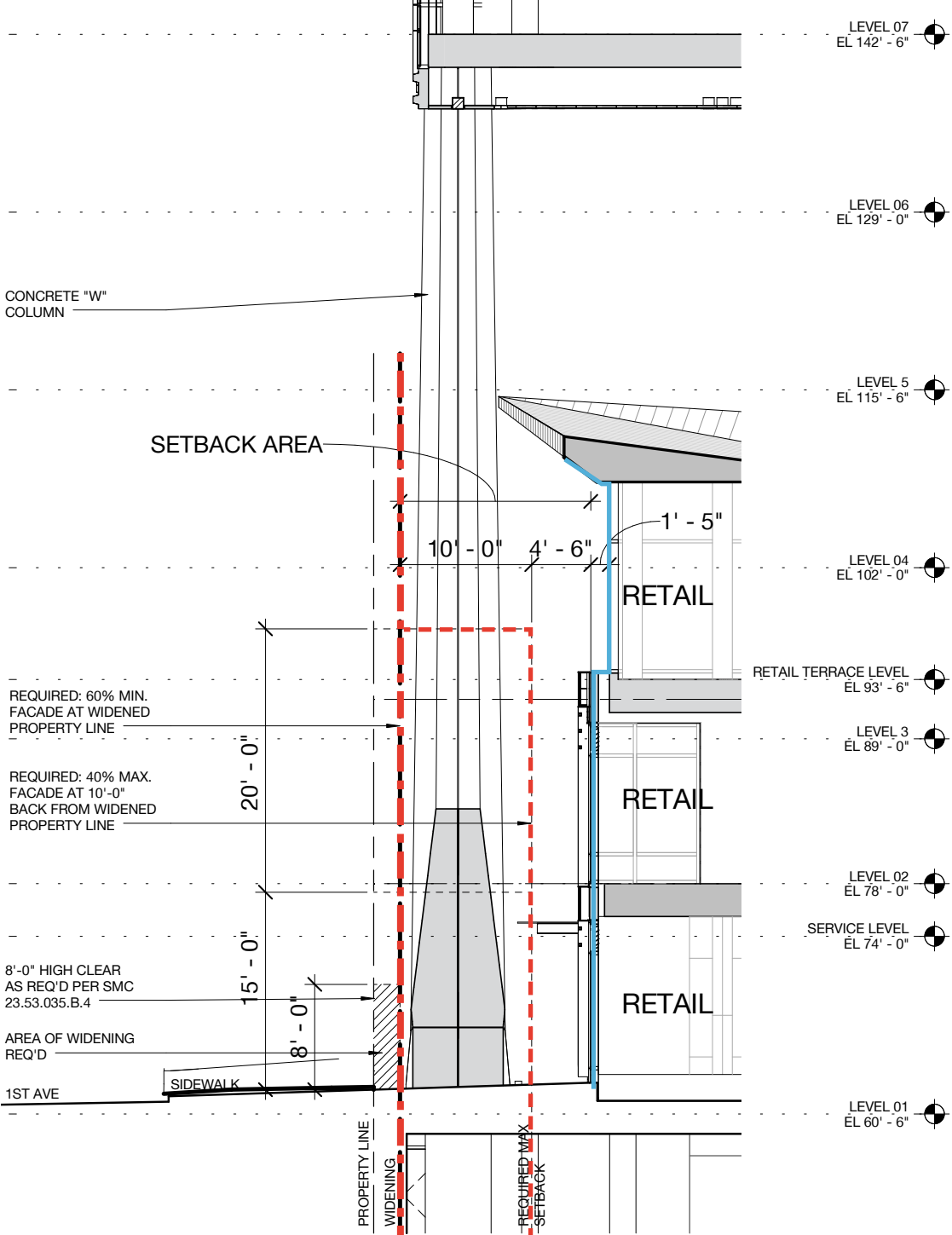
PROPOSED DEPARTURE #4 - PROPERTY LINE FACADES

Departure Diagrams



1 / Property Line Facade - 1st Avenue Elevation (West Elevation)

Regions that fall close to the 2' setback from widened property line, dimension varies due to column profile.



2 / 1st Avenue Sidewalk widening Diagram

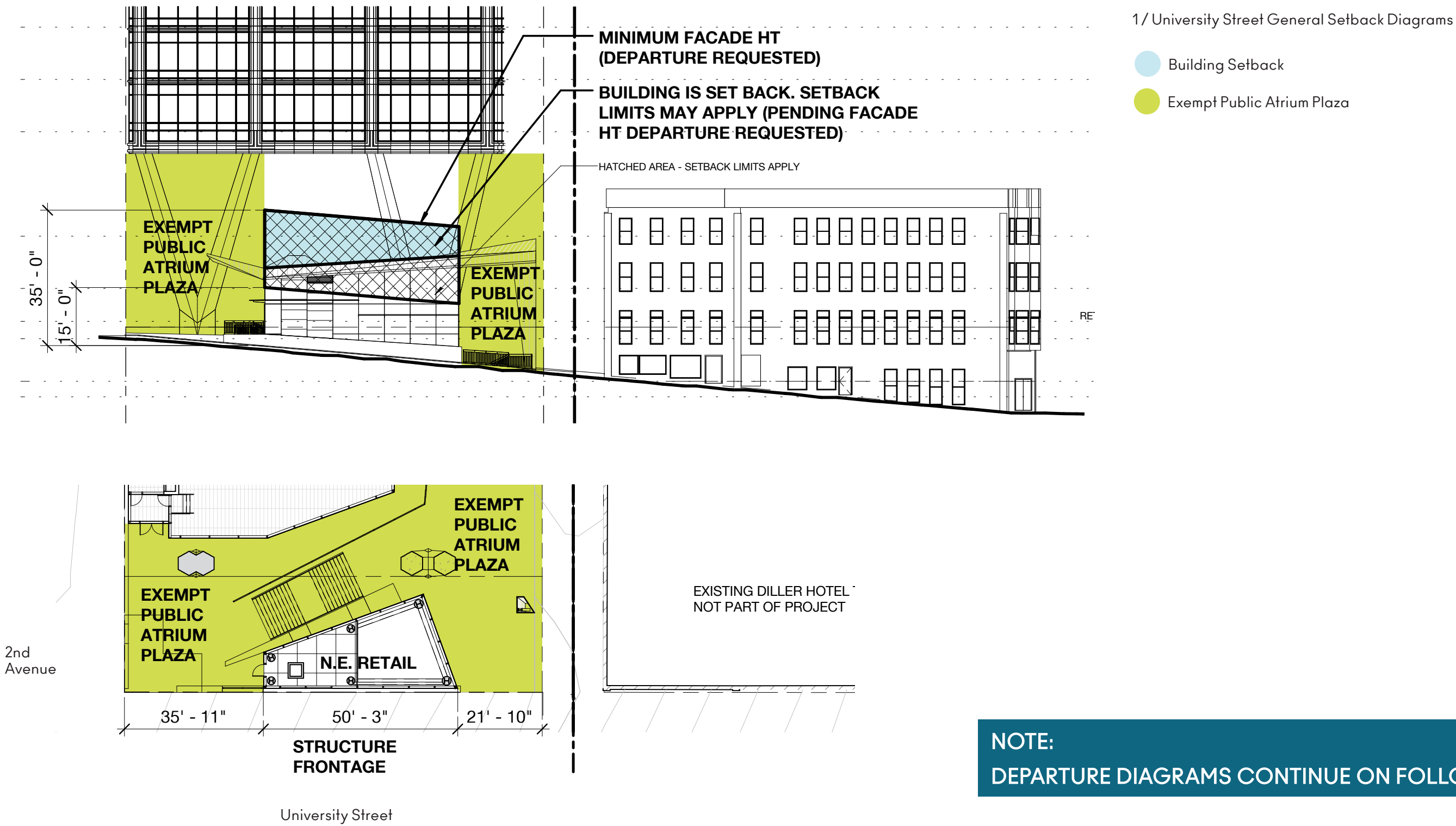
PROPOSED DEPARTURE #5 - GENERAL SETBACKS

Code Citation & Requirement	Code Citation & Requirement (cont.)	Proposed Design Departure & Rationale (cont.)
<p>SMC 23.49.056.B.2 - General Setbacks</p> <p>2. General Setback Limits. The following setback limits apply on streets not requiring property line facades, as shown on Map 1H:</p> <p>a. The portion of a structure subject to setback limits shall vary according to the structure height and required minimum facade height, as follows:</p> <p>1) Except as provided in subsection 23.49.056.B.2.a.3, if the structure is greater than 15 feet in height, the setback limits apply to the facade between an elevation of 15 feet above sidewalk grade and the minimum facade height established in subsection 23.49.056.A and illustrated in Exhibit C for 23.49.056.</p> <p>2) If the entire structure is 15 feet or less in height, the setback limits apply to the entire street-facing facade.</p> <p>3) If the minimum facade height is 15 feet, the setback limits apply to the portion of the street-facing facade that is 15 feet or less in height.</p> <p>b. The maximum area of all setbacks between the street lot line and facade along each street frontage of a lot shall not exceed the area derived by multiplying the averaging factor by the width of the street frontage of the structure along that street (see Exhibit D for 23.49.056). The averaging factor is five on Class I pedestrian streets and ten on Class II pedestrian streets and designated green streets.</p> <p>c. The maximum width, measured along the street lot line, of any setback area exceeding a depth of 15 feet from the street lot line shall not exceed 80 feet, or 30 percent of the lot frontage on that street, whichever is less. (See Exhibit D for 23.49.056.)</p> <p>d. The maximum setback of the facade from the street lot lines at intersections is 10 feet. The minimum distance the facade must conform to this limit is 20 feet along each street. (See Exhibit E for 23.49.056.)</p>	<p>e. Any exterior public open space that meets the Downtown Amenity Standards, whether it receives a bonus or not, and any outdoor common recreation area required for residential uses, is not considered part of a setback. (See Exhibit C for 23.49.056.)</p> <p>f. If a sidewalk is widened into the lot as a condition to development, setback standards shall be measured to the line established by the new sidewalk width rather than the street lot line.</p> <p>Proposed Design Departure & Rationale</p> <p>Note that following 23.49.056.B.2.e, the areas of the project that are Exterior Public Open Spaces are understood to be exempt from the requirements for General Setbacks. (See areas illustrated in diagrams, labelled as “Exempt Public Atrium Plaza”).</p> <p>As a result of the Chapter 23 exemption of Exterior Public Open Space, the areas requested for Departure are solely portions of University St, Second Ave, and Seneca St facades that meet the setback guidelines in plan, but which DO NOT MEET THE FULL HEIGHT OF THE SETBACK ZONE FROM 15FT UP TO MIN FACADE HEIGHT because the building ROOFS ARE LOWER THAN THE MINIMUM HEIGHT (this is tied to Departure #3 for Minimum Facade Height).</p> <p>All of the frontages (University St., Second Ave., or Seneca St.) comply with the maximum area of setbacks permitted with their averaging factor; since the major entries to the mid-block plaza and cross-block pedestrian routes are integral portions of the exempt Exterior Public Open Space sequence.</p> <p>The departure request is for the setbacks above the lower roofscapes of the retail and lobbies, which are an essential component of the unique urban village, and midblock plaza. The lowered roofs allow more daylight into the midblock plaza and the retail roof terrace levels. This request is tied to the departures for Minimum Facade Height.</p>	<p>The proposed departure has the following benefits supporting the Design Guidelines:</p> <p>C1 Promote pedestrian interaction</p> <p>Portions of the retail village roofs are lower than the minimum proscribed height, in order to allow more light into the mid block plaza and retail roof terraces, and to create a more welcoming presence that encourages pedestrians to access the roof terraces, overlooks and gardens.</p> <p>C2 Design facades of many scales</p> <p>The more intimate height of the retail village creates a welcoming pedestrian scale at the base of the larger tower, and creates an interplay of scale with the larger Y columns supporting the tower lift.</p> <p>D3 Provide Elements that Define the Place</p> <p>The unique groundscape design at the base of 2+U provides multi-level retail and lobby elements. Due to the 30 ft. grade change across the site, there are opportunities to access the roofscapes of the lower 1st Ave retail from grade at the 2nd Ave and upper Seneca portions of the site. To enhance that opportunity, a series of exterior ramps and steps are incorporated into the massing. This unique series of interlocking spaces and views creates a memorable, site-specific environment that encourages a wide variety of programmed and spontaneous activities.</p> <p>NOTE: PLEASE SEE DEPARTURE DIAGRAMS ON FOLLOWING PAGES.</p>

PROPOSED DEPARTURES |

PROPOSED DEPARTURE #5 - GENERAL SETBACKS

Departure Diagrams



PROPOSED DEPARTURE #5 - GENERAL SETBACKS

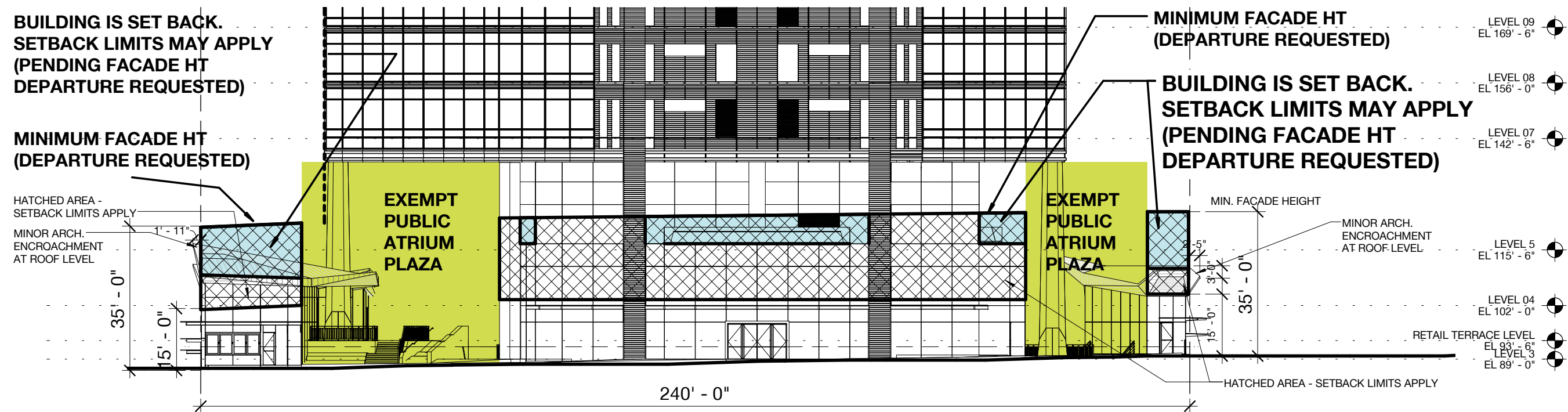
2 / Seneca Street General Setback Diagrams



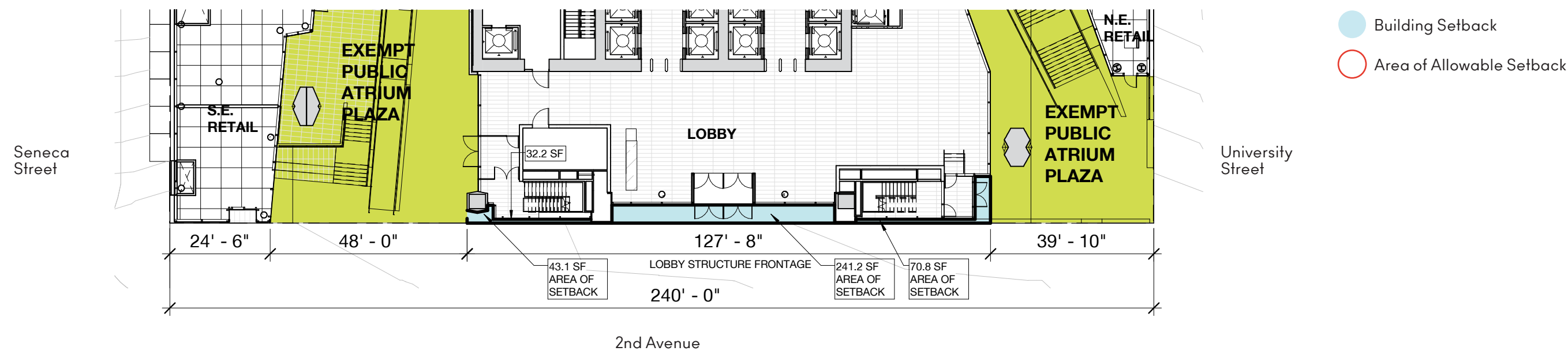
PROPOSED DEPARTURES |

PROPOSED DEPARTURE #5 - GENERAL SETBACKS

Departure Diagrams (cont.)



3 / 2nd Avenue General Setback Diagrams



PROPOSED DEPARTURE #6 - BLANK FACADE LIMITS

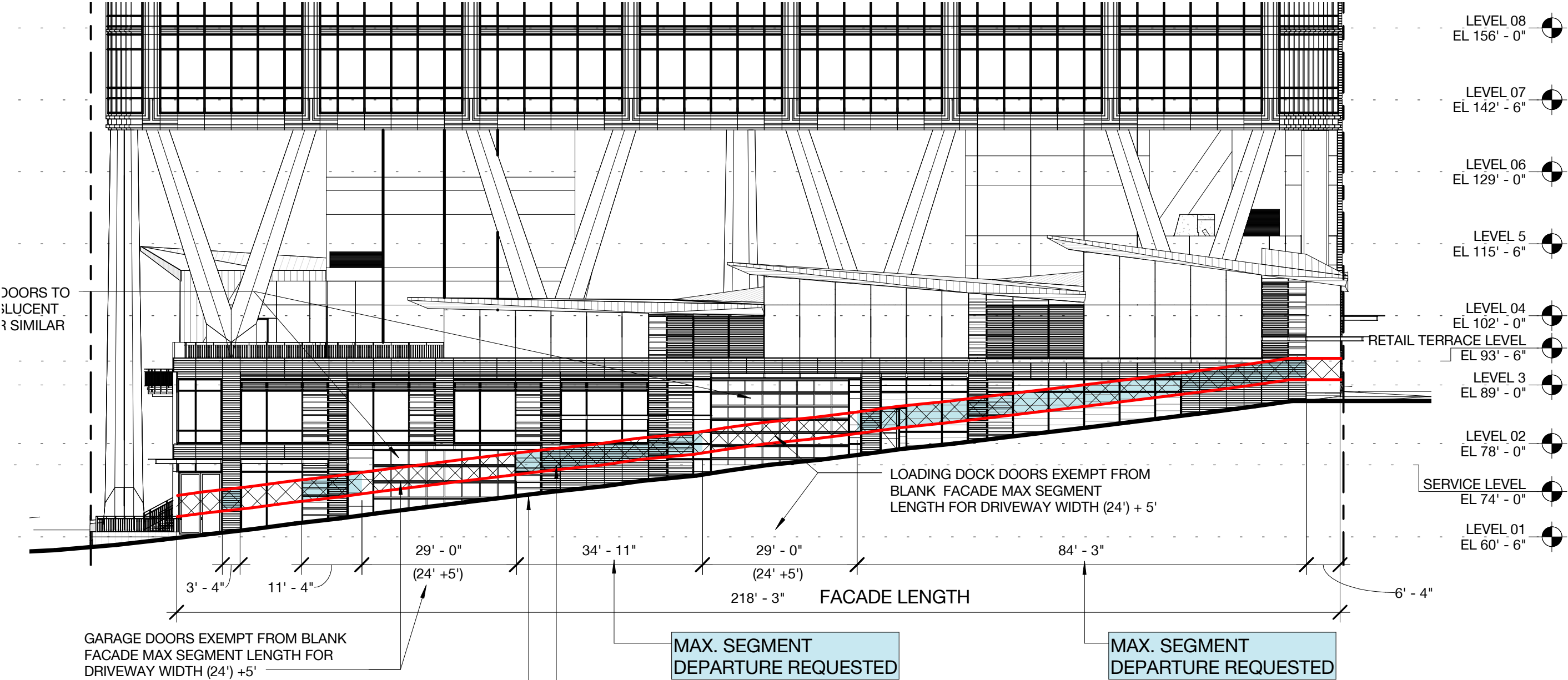
Code Citation & Requirement	Proposed Design Departure & Rationale	Proposed Design Departure & Rationale (cont.)
<p>SMC 23.49.056.D.2.a / D.2.c / D.3.a - Blank Facade Limits</p> <p>D. Blank Facade Limits.</p> <p>1. General Provisions.</p> <p>a. Blank facade limits apply to the area of the facade between 2 feet and 8 feet above the sidewalk, except that where the slope along the street frontage of the facade exceeds 7.5 percent, blank facade limits apply to the area of the facade between 4 feet and 8 feet above sidewalk grade.</p> <p>b. Any portion of a facade that is not transparent shall be considered to be a blank facade.</p> <p>c. Blank facade limits do not apply to portions of structures in residential use.</p> <p>3. Blank Facade Limits for Class II Pedestrian Streets.</p> <p>a. Blank façade segments shall be no more than 30 feet wide, except for garage doors, which may exceed 30 feet. Blank facade segment width may be increased to 60 feet if the Director in a Type I decision determines that the facade segment is enhanced by architectural detailing, artwork, landscaping, or similar features that have visual interest. <u>The width of garage doors shall be limited to the width of the driveway plus 5 feet.</u></p> <p>b. Any blank segments of the facade shall be separated by transparent areas at least 2 feet wide.</p> <p>c. The total of all blank facade segments, including garage doors, shall not exceed 70 percent of the street facade of the structure on each street frontage; or 75 percent if <u>the slope of the street frontage of the facade exceeds 7.5 percent.</u></p>	<p>On Seneca Street, two segments EXCEED THE MAXIMUM SEGMENT LENGTH of 30 FT.</p> <p>One segment is approximately 84'-3" long and the other segment is approximately 34'-11" long.</p> <p>Due to the creation of a midblock plaza with publicly accessible plaza and retail spaces, mechanical spaces and service spaces have limited area to ventilate. Portions of the Seneca St facade are dedicated to intake and exhaust louvers for the retail spaces, and the parking garage, which is located below grade to reduce visual impact.</p> <p>Although the length of the maximum segments exceeds the maximum, the overall percentage of blank facade does not exceed the maximum of 75% (Class II Street, slope steeper than 7.5%). The design emphasis has been on locating non-transparent elements closer together at the central portion of the block, across the street from the loading and parking dock entry for the Second & Seneca Building, in order to maximize the amount of transparent and non-blank facades at the corners near First and Second Avenues, which are Class I Pedestrian Streets.</p> <p>The proposed departure has the following benefits supporting the Design Guidelines:</p> <p>B3 Reinforce the positive urban form and architectural attributes of the immediate area</p> <p>The goal is to locate the required building, retail, and garage louvers in locations with the least pedestrian impact. Seneca St is a Class II pedestrian street, so locating louvers along the facade helps minimize the amount of louvers on the Class I pedestrian streets (2nd Ave, University St., and 1st Ave).</p> <p>Locating the active non-blank portions of the program at the</p>	<p>corners of the block near Second Ave and First Ave reinforces the architecture of the adjacent neighborhood, and locating the more service-oriented areas directly across the street from the service entries for the Second & Seneca Building also responds to the immediate context.</p> <p>E2 Integrate parking facilities</p> <p>The location of the parking garage fully below-grade reduces its visual impact, but results in the requirement to mechanically ventilate the garage via louvers.</p>

NOTE:
PLEASE SEE DEPARTURE DIAGRAM ON FOLLOWING PAGE.

PROPOSED DEPARTURES |

PROPOSED DEPARTURE #6 - BLANK FACADE LIMITS

Departure Diagram



SLOPE AT SENECA STREET (CLASS II):
12.6% > 7.5%

ZONE BETWEEN 4' - 8' ABOVE SIDEWALK, MAX 30 FT SEGMENT EXCEPT AT GARAGE DOORS
BLANK FACADE MAX SEGMENT LENGTH PROVIDED: **84.27 FT = DEPARTURE REQUESTED**
75% MAX. ALLOWABLE BLANK FACADE INCL GARAGE DOORS: 218.27 FT x .75 = **163.70 FT**
BLANK FACADE TOTAL LENGTH PROVIDED: **133.81 FT = COMPLIANT**
BLANK FACADE PERCENTAGE PROVIDED: **61.3% = COMPLIANT**

1/ Seneca Street Blank Facades

Areas of Blank Facades

PROPOSED DEPARTURE #7 - FACADE MODULATION

Code Citation & Requirement

SMC 23.49.058.C - Facade Modulation

3. The maximum length of a facade without modulation is prescribed in Table A for 23.49.058. This maximum length shall be measured parallel to each street lot line, and shall apply to any portion of a facade, including projections such as balconies, that is located within 15 feet of street lot lines.

Table A for 23.49.058
Modulation Requirements for DOC 1, DOC 2, and DMC Zones, Except DMC 160 Zone

Elevation Maximum length of unmodulated facade within 15 feet of street lot line

- 0 to 85 feet - No limit
- 86 to 160 feet - 155 feet
- 161 to 240 feet - 125 feet
- 241 to 500 feet - 100 feet
- Above 500 feet - 80 feet

4. Any portion of a facade exceeding the maximum length of facade prescribed on Table A for 23.49.058 shall be set back a minimum of 15 feet from the street lot line for a minimum distance of 60 feet before any other portion may be within 15 feet of the street lot line.

Proposed Design Departure & Rationale

In the proposed design, the notch width of 54'-4" is 6 ft. shorter than required. The notch depth complies with 15 ft. requirement (ref. MUP-013).

In addition to the 54' x 15' notch, the 2nd Ave. façade has additional modulation created by the change in material along the eastern façade. The central portion with architectural precast concrete panels, in contrast to the primary material of glass, is 79'-6" long. The façade also has a 53' long portion that angles away from the property line.

Proposed Design Departure & Rationale (cont.)

The proposed departure has the following benefits supporting the Design Guidelines:

A2 Enhance the Skyline

The overall central notch and its extension up to the tower crown creates a dynamic, sculptural massing that is legible from the immediate neighborhood as well as from more distant view points.

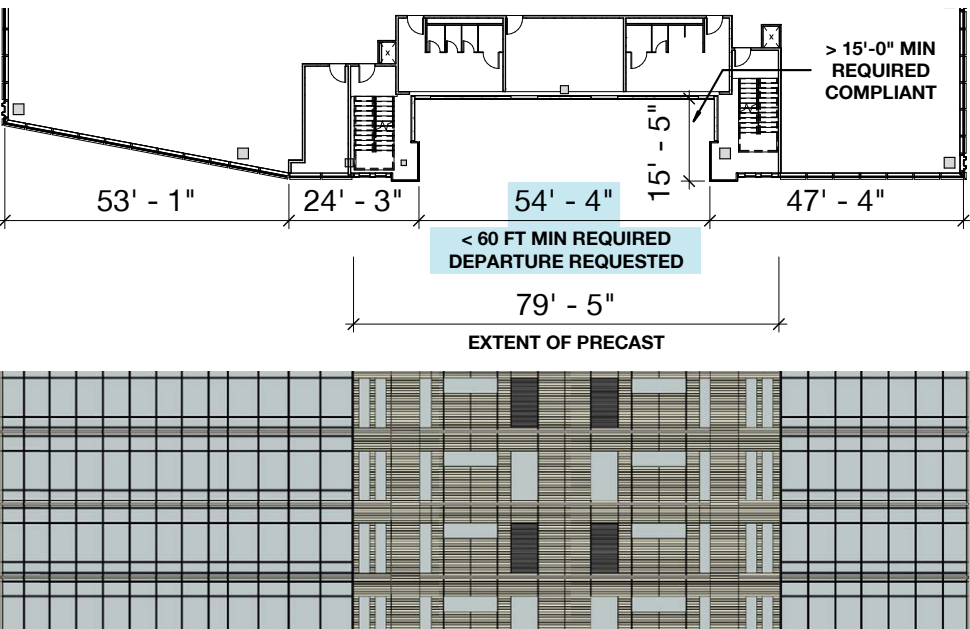
B2 Create a Transition in Bulk and Scale

The variety of planes on 2nd Ave., and the change in material from glass to architectural precast concrete panels, break the tower into several vertically soaring massing components, and a more sculptural composition, than would a simple code-compliant solution with 100' and 125' broad faces with orthogonal inset corners.

B4 Design a Well-Proportioned and Unified Building

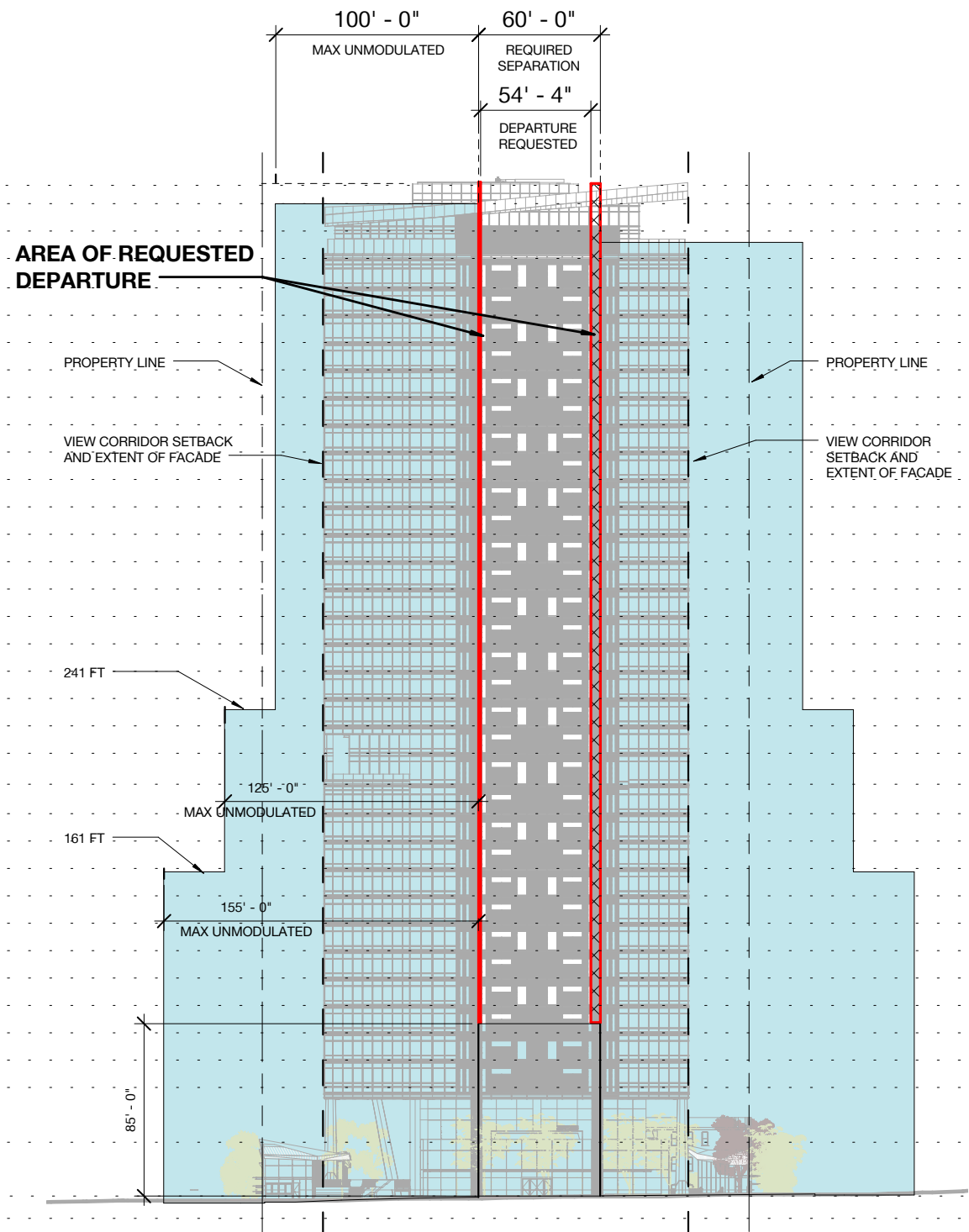
The departure allows an elegant facade modulation rhythm.

Departure Diagrams



1/ Tower Floor Plan & Enlarged Elevation - Facade Modulation

Departure Diagrams (cont.)



2/ 2nd Avenue Facade Modulation

PROPOSED DEPARTURES |

PROPOSED DEPARTURE #8 - UPPER-LEVEL WIDTH LIMIT

Code Citation & Requirement

SMC 23.49.058.D.1- Upper Level Width Limit

D. Upper-level width limit

- 1. On lots where the width and depth of the lot each exceed 200 feet, the maximum facade width for any portion of a structure above 240 feet shall be 145 feet along the general north/south axis of a site (parallel to the Avenues), and this portion of the structure shall be separated horizontally from any other portion of a structure on the lot above 240 feet by at least 80 feet at all points.

Proposed Design Departure & Rationale

The proposed scheme tower width above the 240' height is 180'-0" wide, which exceeds the 145' width by 35'-0". This tower width aligns with the view corridor setbacks (ref. MUP-012).

The proposed departure has the following benefits supporting the Design Guidelines:

B2 Create a Transition in Bulk and Scale

Even with the requested departure, the proposed maximum tower floor plate due to the view corridor setbacks (108' x 180'-0"), is small compared to adjacent downtown office towers in the vicinity, and creates a very small diagonal in terms of view blockage from all directions. The proposed tower massing results in fewer stories in the tower, which is sympathetic to the adjacent office buildings, keeping the tower as a "member of chorus" in the adjacent skyline, rather than being significantly taller as would be permitted by zoning with Unlimited Height.

B3 Reinforce the Positive Urban Form of Immediate Area

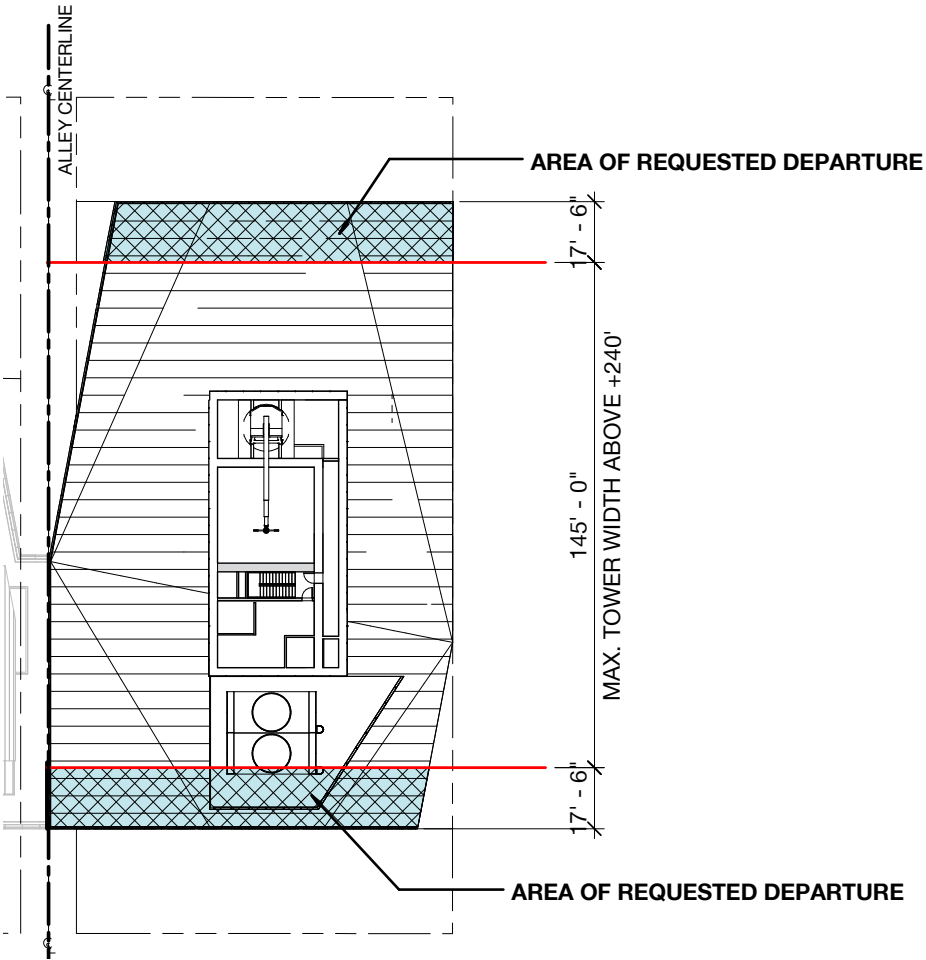
The required 30' view corridor setbacks maintain wide viewing angles from the east, in keeping with the spirit of the ordinance. The building massing and orientation are compatible with, and smaller than, the adjacent 2nd & Seneca Building, Russell Investments, and 1201 3rd Ave., all of which have towers greater than 145' wide.

Proposed Design Departure & Rationale (cont.)

B4 Design a Well-Proportioned and Unified Building

The proposed facade modulation reduces the visual mass of the already slender tower. The proposed streamlined and integrated vertical articulation would not be achievable with a 240' high, 180'-0" wide low rise base, stepping back to a narrow tower with 3 additional stories. The as-of-right zoning would result in a tiered-structure with a schism half-way up the tower massing, instead of an integrated and unified massing and facade.

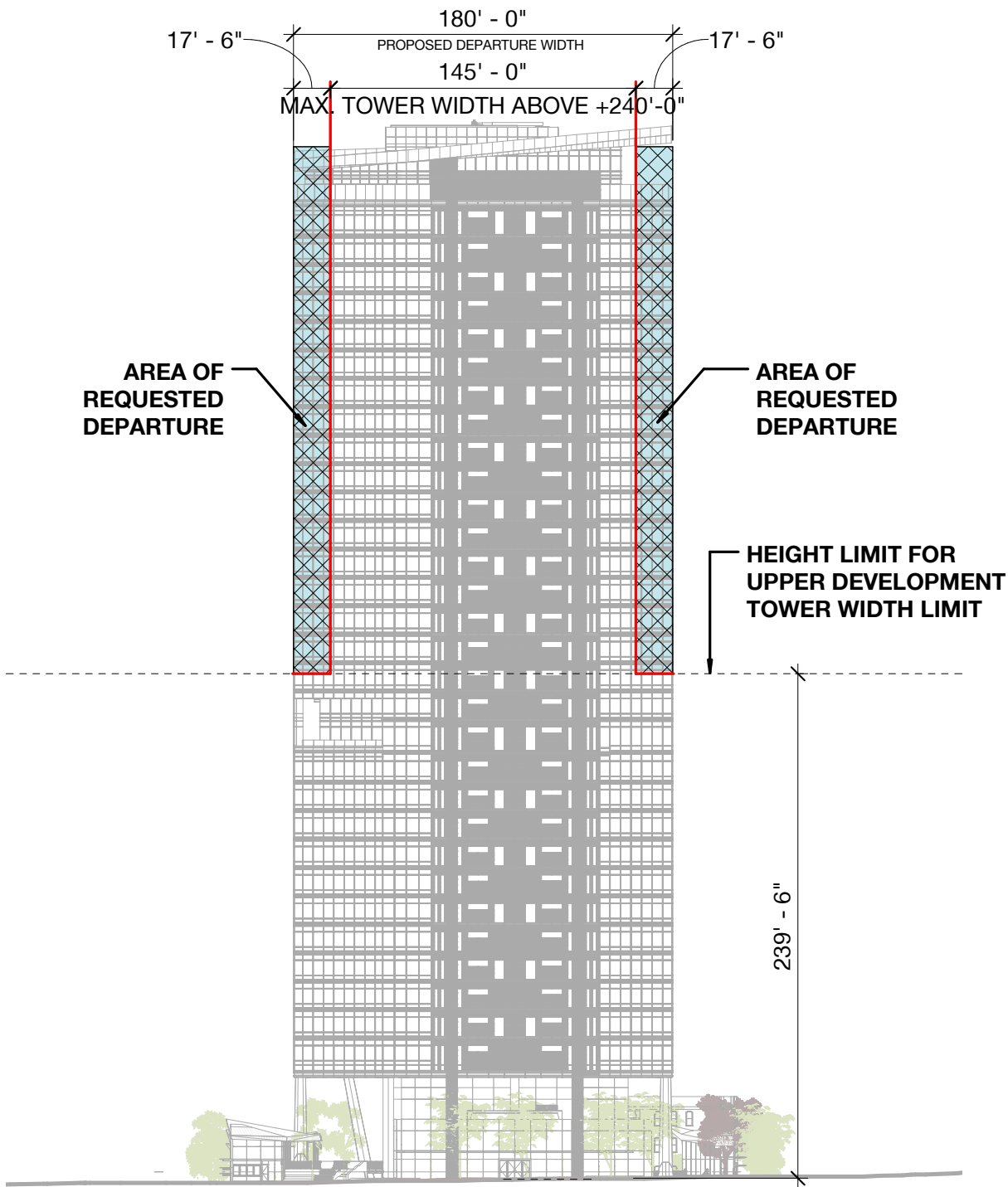
Departure Diagrams



1/ Upper Level Tower Plan

Area of Requested Departure

Departure Diagrams (cont.)



2/ 2nd Avenue Elevation - Tower Width

PROPOSED DEPARTURE #9 - PARKING AT STREET LEVEL

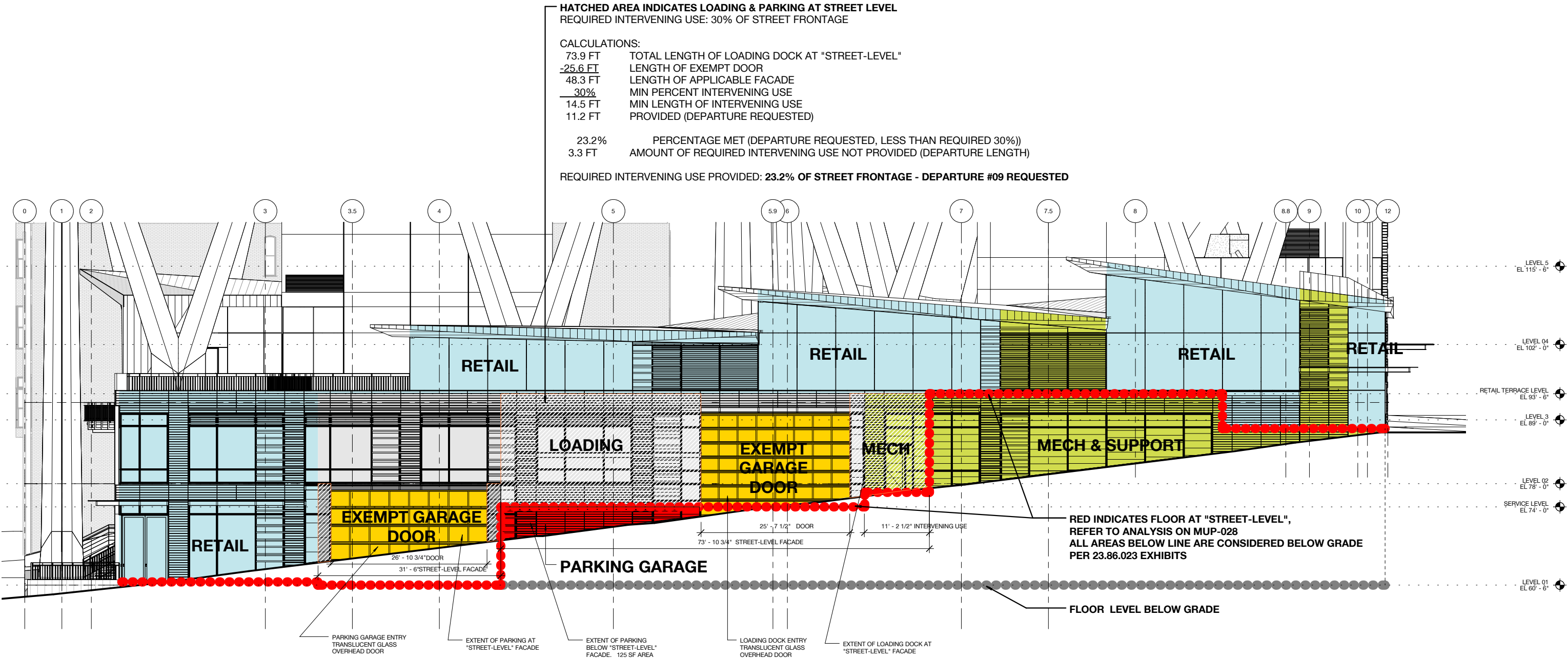
Code Citation & Requirement	Proposed Design Departure & Rationale	Proposed Design Departure & Rationale (cont.)
<p>SMC 23.49.019.B.1- <i>Parking at Street Level</i></p> <p>B. <i>Parking location within structures</i></p> <p>1. <i>Parking at street level</i></p> <p>a. <i>On Class I pedestrian streets and designated green streets, parking is not permitted at street level unless separated from the street by other uses, provided that garage doors need not be separated.</i></p> <p>b. <i>On Class II pedestrian streets, parking may be permitted at street level if:</i></p> <p><i>1) at least 30 percent of the street frontage of any street-level parking area, excluding that portion of the frontage occupied by garage doors, is separated from the street by other uses;</i></p> <p><i>2) the facade of the separating uses satisfies the transparency and blank wall standards for Class I pedestrian streets for the zone in which the structure is located;</i></p> <p><i>3) the portion of the parking, excluding garage doors, that is not separated from the street by other uses is screened from view at street level; and</i></p> <p><i>4) the street facade is enhanced by architectural detailing, artwork, landscaping, or similar visual interest features.</i></p>	<p>Short Term Parking: Due to the steep 12.6% grade at Seneca St., a small fraction of Short-term Parking has an exposed wall on Seneca St. This includes a 2'-6" length solid wall on one side of the exempt garage door and a 2'-2 3/4" length on the other. The remainder of garage wall is defined as below "street-level" per Exhibit 23.86.023 A "Street level facade". That triangular portion of below-street-level-facade is approximately 125 sf.</p> <p>Loading Dock: Loading has been identified during Zoning Review as "Parking" and has been determined to be subject to this code section. The total length of dock is at Street-level per 23.86.023 is 73.9 FT long. 30% of the street level dock (minus exempt doors) requires 14.5 FT of intervening use. The project provides 11.2 FT intervening use at the Gas Meter Room. This is 23.2% instead of the required 30%.</p> <p>Due to the creation of a midblock plaza with publicly accessible plaza and retail spaces, the loading dock and parking garage access occupy a narrow segment of built area at grade along Seneca Street.</p> <p>The parking has been located with full intervening use along First Ave, and as maximized below-grade as possible given the need for an entry door along the slope of Seneca St.</p> <p>The Loading Dock requires a small departure for reduced intervening use for the street-level portion of the truck area. The at-grade retail at the corners of 1st and 2nd Avenues at Seneca, and the plaza to the north, have restricted the at-grade area available for essential services. All other streets surrounding this block are Class I pedestrian streets, meaning that Seneca Street is the most appropriate to have a concentration of service program. Given these space constraints, and the steep grade of the street, the visual impact of the loading has been minimized.</p>	<p>The translucent glass loading dock doors, and the consistent facade treatment along the dock that corresponds to the solid cladding of adjacent retail, minimizes the visual presence of the dock.</p> <p>The proposed departure has the following benefits supporting the Design Guidelines:</p> <p>B3 Reinforce the positive urban form and architectural attributes of the immediate area. / C4 Service Uses The goal is to locate the required loading bay and short term parking in locations with the least pedestrian impact. Seneca St is a Class II pedestrian street whereas all other streets bounding the site are Class I pedestrian streets (2nd Ave, University St., and 1st Ave); additionally, the midblock plaza creates an alternative path for traversing the site in the east-west direction.</p> <p>C3 Provide Active-not blank-facades This facade has pedestrian-scale retail at either end and richly detailed wall surfaces boasting a variety of materials, creating interest for pedestrians. Wide, translucent glass garage doors at the loading dock and parking garage access punctuate the opaque surfaces.</p> <p>E3 Minimize the Presence of Service Areas The proposed substitution of TRANSLUCENT glass instead of Transparent Glass at the loading dock and garage entries meets the intent of this section, in order to screen these necessary areas from pedestrian view. While the glass could be transparent, the project team believes translucent glass meets the Guidelines intent better.</p>

NOTE:
PLEASE SEE DEPARTURE DIAGRAMS ON FOLLOWING PAGES.

PROPOSED DEPARTURES |

PROPOSED DEPARTURE #9 - PARKING AT STREET LEVEL

Departure Diagrams



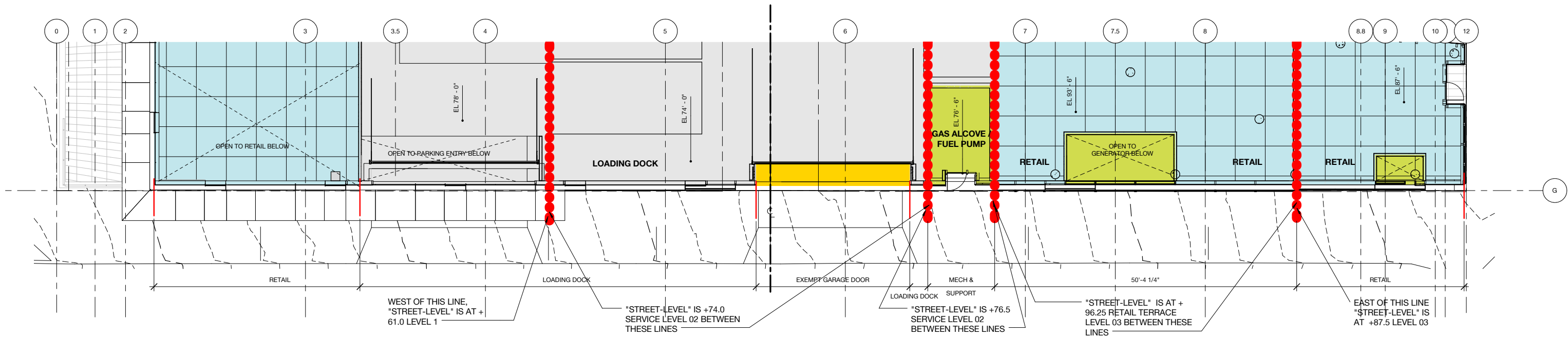
1/ Ground Enlarged South Elevation (Seneca Street) - Uses

- Retail
- Loading
- Exempt Garage Door
- Parking Garage
- Mech & Support

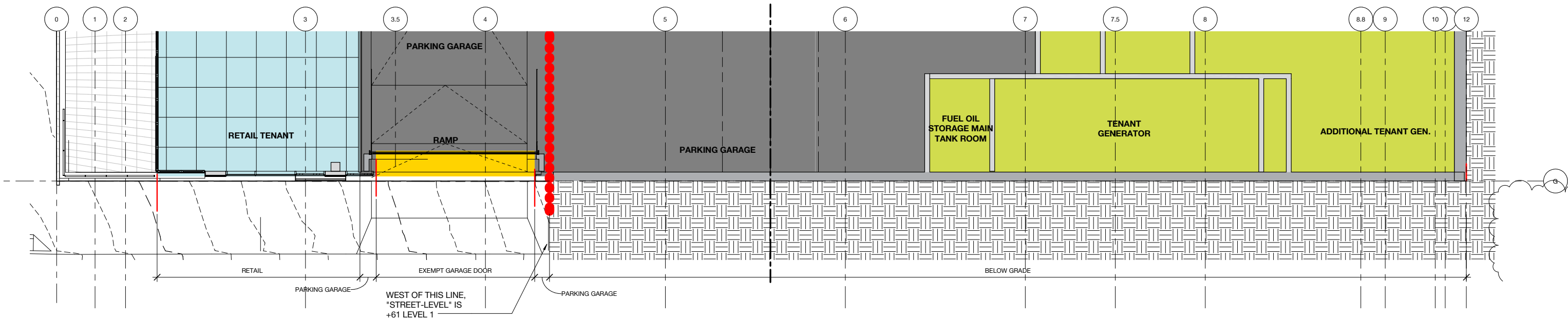
PROPOSED DEPARTURES |

PROPOSED DEPARTURE #9 - PARKING AT STREET LEVEL

Departure Diagrams (cont.)



2 / Level 2 Seneca Street Partial Plan



3 / Level 1 Seneca Street Partial Plan

- Retail
- Mech & Support
- Parking Garage
- Exempt Garage Door
- Loading

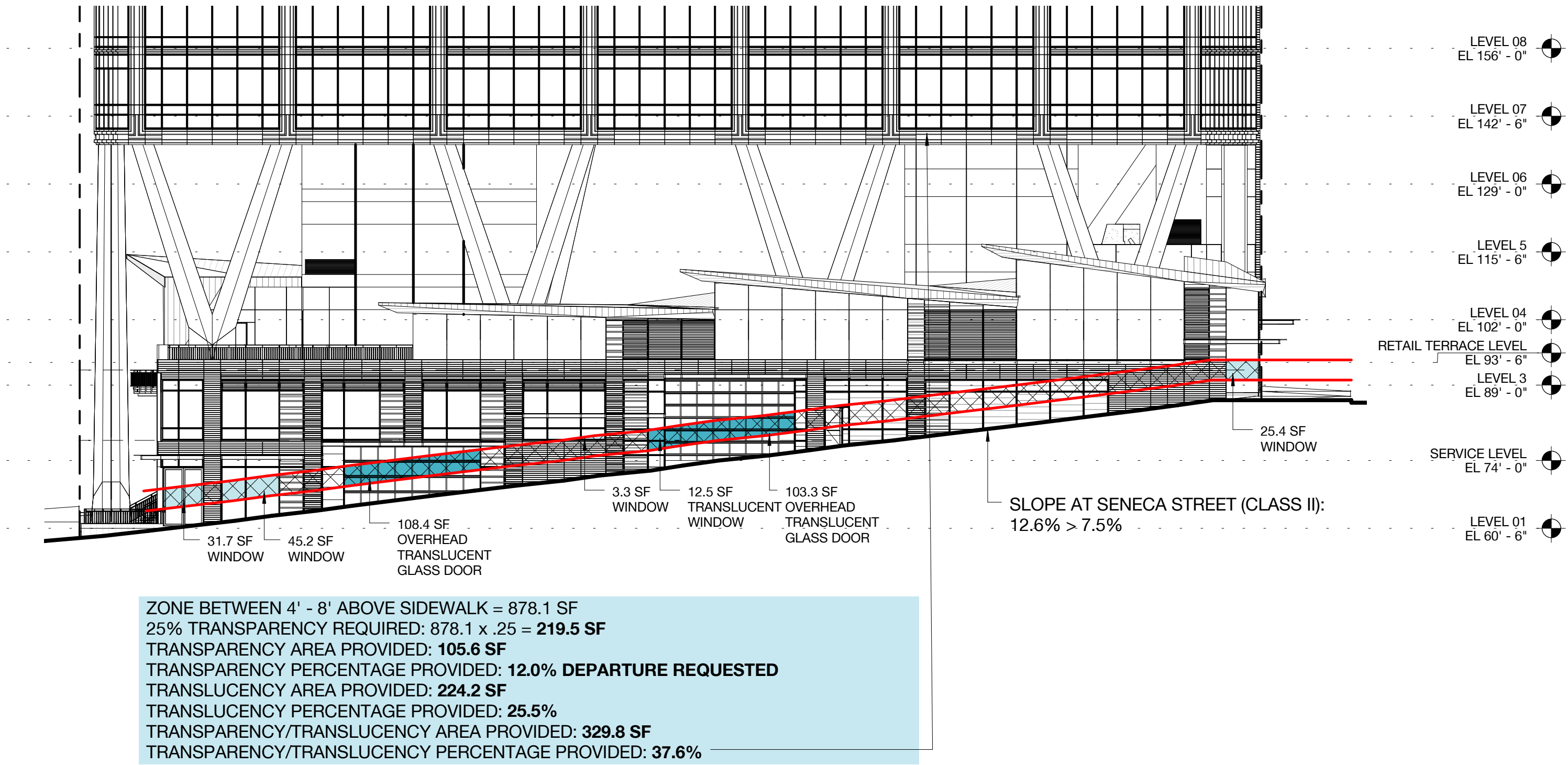
PROPOSED DEPARTURE #10 - FACADE TRANSPARENCY

Code Citation & Requirement	Proposed Design Departure & Rationale	Proposed Design Departure & Rationale (cont.)
<p>SMC 23.49.056.C.4.C Facade Transparency Requirements</p> <p>Facade transparency requirements apply to the area of the facade between 2 feet and 8 feet above the sidewalk, except that if the slope along the street frontage of the facade exceeds 7.5 Percent, the transparency requirements apply to the area of the facade between 4 feet and 8 feet above sidewalk grade. Only clear or lightly tinted glass in windows, doors, and display windows is considered to be transparent. Transparent areas shall allow views into the structure or into display windows from the outside.</p> <p>2. Facade transparency requirements do not apply to portions of structures in residential use.</p> <p>3. If the transparency requirements of this subsection 23.49.056.C are inconsistent with the glazing limits in the energy code, this subsection 23.49.056.C applies to the extent permitted by applicable law.</p> <p>4. Transparency requirements are as follows:</p> <p>A. Class i pedestrian streets and designated green streets: a minimum of 60 percent of the street level street-facing facade shall be transparent.</p> <p>B. Class ii pedestrian streets: a minimum of 30 percent of the street level street-facing facade shall be transparent.</p> <p>C. <u>Where the slope along the street frontage of the facade exceeds 7.5 Percent, the required amount of transparency shall be reduced to 50 percent on Class I Pedestrian Streets and designated green streets and 25 percent on Class II Pedestrian Streets.</u></p>	<p>The Facade Transparency Percentage is less than the 25% required for a Class II Pedestrian Street with a slope greater than 7.5%. TRANSLUCENT glass facade areas have been provided, such that the COMBINED TRANSLUCENT AND TRANSPARENT GLASS AREAS EXCEED THE 25% required area for this street condition.</p> <p>While the areas proposed as Translucent Glass could be Transparent Glass, the project team believes that TRANSLUCENT glass meets the DESIGN INTENT of the requirements for this class of street, while more effectively screening loading and mechanical spaces from pedestrian view, as recommended by E3, MINIMIZE THE PRESENCE OF SERVICE AREAS.</p> <p>Due to the creation of a midblock plaza with publicly accessible plaza and retail spaces and the exigencies of all the other streets surrounding this property being Class I pedestrian streets, the loading dock and parking garage access occupy a narrow segment of built area at grade along Seneca Street. Additionally, mechanical services occupy a segment of the at-grade portion of this facade. As such, opportunities for transparent areas to activate this facade are limited.</p> <p>At-grade retail with transparent glass has been provided at the corners of Seneca Street and 1st and 2nd Avenues, in compliance with the Zoning and Guidelines for Downtown Development, and to prioritize vision glass at the most visible and active pedestrian areas of the block.</p> <p>Translucent glass garage doors to the loading dock and parking garage have been provided as a means of adding interest and modulating the facade in the mid portion of the facade length, while minimizing the visual impact of service and parking spaces.</p> <p>The combined total translucent and transparent area covers 37.3% of the required area between 4 ft and 8 ft above the sidewalk.</p>	<p>The proposed departure has the following benefits supporting the Design Guidelines:</p> <p>B3 Reinforce the positive urban form and architectural attributes of the immediate area.</p> <p>The overall project planning follows the B3 recommendations to “locate parking and vehicle access away from entries, open space, and street intersections”. The goal is to locate the required loading bay and short term parking in locations with the least pedestrian impact. Seneca St is a Class II pedestrian street whereas all other streets bounding the site are Class I pedestrian streets (2nd Ave, University St., and 1st Ave); additionally, the midblock plaza creates an alternative path for traversing the site in the east-west direction.</p> <p>C2 Design Facade of Many Scales</p> <p>This facade has transparent-glazed pedestrian-scale retail at either end and richly detailed wall surfaces boasting a variety of materials, creating interest for pedestrians. Wide, translucent glass garage doors at the loading dock and parking garage access punctuate the opaque surfaces.</p> <p>D1 Provide Inviting & Usable Open Space</p> <p>The location of the loading, parking, and mechanical areas along Seneca St. allows for the creation of the midblock plaza, and through-block passages. The prioritization of these pedestrian-friendly spaces has been the key focus of the 2&U site planning strategy.</p> <p>E3 Minimize the Presence of Service Areas</p> <p>The proposed substitution of TRANSLUCENT glass instead of Transparent Glass at the loading dock and garage entries meets the intent of this section, in order to screen these necessary areas from pedestrian view. While the glass could be transparent, the project team believes translucent glass meets the Guidelines intent better.</p>
<div>NOTE: PLEASE SEE DEPARTURE DIAGRAM ON FOLLOWING PAGE.</div>		

PROPOSED DEPARTURES |

PROPOSED DEPARTURE #10 - FACADE TRANSPARENCY

Departure Diagram



1/ Seneca Street Facade Transparency

Transparent Facade Translucent Facade

	PROPOSED DEPARTURES TABLE / STANDARD	PROPOSED	RATIONALE	APPLICABLE DESIGN GUIDELINES
#01	23.49.016.C - OPEN SPACE STANDARDS			
	<p>C. Standards for Open Space. To satisfy this requirement, open space may be provided on-site or off-site, as follows:</p> <p>1. Private Open Space. Private open space on the project site or on an adjacent lot directly accessible from the project site may satisfy the requirement of this section. Such space shall not be eligible for bonuses. Private open space shall be open to the sky and shall be consistent with the general conditions related to landscaping; seating and furnishings contained in the Downtown Amenity Standards. Private open space satisfying this requirement must be accessible to all tenants of the building and their employees.</p>	<p>DEPARTURE REQUESTED, REF MUP-010</p> <p>27.7% of the total required Open Space has OVERHEAD WEATHER PROTECTION.</p> <p>The total OPEN SPACE is provided in 3 locations: 1,250 SF combined at Retail Village at EL+90 and EL+78 is OPEN TO SKY.</p> <p>12,170 SF provided at Level 19, of which 3,715 SF is UNDER COVER OF OVERHEAD PROTECTION.</p> <p>The eastern portion of the Level 19 terrace has overhead protection created by the double-height notch of the main highrise tower.</p> <p>The western portion of the Level 19 terrace has overhead protection created by a canopy around the lowrise elevator and stair core that provides tenant access to and egress from the Open Space terrace.</p>	<p>The SMC 23 references that Private Open Space should be consistent with Downtown Amenity Standards.</p> <p>The Downtown Amenity Standards for "Neighborhood Open Spaces" permit up to 20 percent of the neighborhood open space may be covered to accommodate activities that complement use of the space and make it more comfortable and usable, such as ... overhead weather protection."</p> <p>The DAS for "Urban Plazas" permit "limited coverage may be appropriate to increase activity in the space and provide for the comfort of the users, while maintaining the overall character of the space as an extension of the outdoor public street environment."</p> <p>The overhanging roof also provides visual screening of the elevator overrun for people below.</p>	<p>A2 Enhance the Skyline The overhanging roof helps screen the roof terrace elevator overrun from view from below.</p> <p>B4 Design a Well-Proportioned and Unified Building The form and materiality of the overhang visually relate to the retail village roof overhangs, and the overhang at the tower top, thus unifying the building at the mid-height level.</p> <p>D1 Provide Inviting and Usable Open Space The overhanging roof provides some shelter from wind, rain, and sun, to allow the space to be used in more swing seasons. It also screens the elevator overrun bulkhead from the user's view.</p> <p>D3 Provide elements that define the Place It visually connects the space to the open spaces at the retail village below, to create a sense of place.</p> <p>Downtown Amenity Standards for Neighborhood Open Spaces" and "Urban Plazas" permit portions of the space to be covered to accommodate activities and make it more comfortable and usable, such as overhead weather protection.</p>
#02a	23.49.018.B - OVERHEAD WEATHER PROTECTION (Depth from Building - University)			
	<p>23.49.018 - Overhead Weather Protection and Lighting.</p> <p>B. Overhead weather protection shall have a minimum dimension of eight (8) feet measured horizontally from the building wall or must extend to a line two (2) feet from the curb line, whichever is less.</p>	<p>DEPARTURE REQUESTED, REF MUP-019.</p> <p>UNIVERSITY ST: Proposed to have 5 FT WIDE OVERHEAD WEATHER PROTECTION instead of 8 FT wide DUE TO MATURE TREES.</p>	<p>On University St., 8 ft wide overhead weather protection would conflict with mature tree canopies, and require significant pruning. The proposed 5 ft wide canopies, suggested by Land Use Comments, should clear the branches with minimal or no pruning. This street frontage does not include building entries. The scale is also appropriate to the pavilion-scale of the northeast retail building.</p>	<p>B1 Respond to Neighborhood Context The proposed reduction in width of overhead weather protection reinforces the desirable existing mature landscaping on University Street (a continuation of a Green Street corridor).</p> <p>D2 Enhance the Building with Landscaping The SDOT requirements for clearance around trees will allow the existing mature trees on University St to remain. This portion of University St is contiguous with other portions of a Green Street corridor.</p> <p>SDOT requirements for Street Trees Minimum 3 ft clear from centerline of trees. This requirement supersedes the overhead weather protection dimensional requirements.</p> <p>C5 Encourage overhead weather protection. As recommended in C5, the design at Univeristy St is related to "the scale of the space defined by the height and depth of the weather protection" and the pavilion-scale of the northeast retail building.</p>

#02a	23.49.018.B - OVERHEAD WEATHER PROTECTION (Depth from Building - University)			
	23.49.018 - Overhead Weather Protection and Lighting. B. <u>Overhead weather protection shall have a minimum dimension of eight (8) feet measured horizontally from the building wall or must extend to a line two (2) feet from the curb line, whichever is less.</u>	DEPARTURE REQUESTED, REF MUP-019. UNIVERSITY ST: Proposed to have 5 FT WIDE OVERHEAD WEATHER PROTECTION instead of 8 FT wide DUE TO MATURE TREES.	On University St., 8 ft wide overhead weather protection would conflict with mature tree canopies, and require significant pruning. The proposed 5 ft wide canopies, suggested by Land Use Comments, should clear the branches with minimal or no pruning. This street frontage does not include building entries. The scale is also appropriate to the pavilion-scale of the northeast retail building.	B1 Respond to Neighborhood Context The proposed reduction in width of overhead weather protection reinforces the desirable existing mature landscaping on University Street (a continuation of a Green Street corridor). D2 Enhance the Building with Landscaping The SDOT requirements for clearance around trees will allow the existing mature trees on University St to remain. This portion of University St is contiguous with other portions of a Green Street corridor. SDOT requirements for Street Trees Minimum 3 ft clear from centerline of trees. This requirement supersedes the overhead weather protection dimensional requirements. C5 Encourage overhead weather protection. As recommended in C5, the design at Univeristy St is related to "the scale of the space defined by the height and depth of the weather protection" and the pavilion-scale of the northeast retail building.
#02b	23.49.018.B - OVERHEAD WEATHER PROTECTION (Depth from Building- Seneca)			
	23.49.018 - Overhead Weather Protection and Lighting. B. <u>Overhead weather protection shall have a minimum dimension of eight (8) feet measured horizontally from the building wall or must extend to a line two (2) feet from the curb line, whichever is less.</u>	DEPARTURE REQUESTED, REF MUP-019. SENECA ST: Proposed to have 5 FT WIDE OVERHEAD WEATHER PROTECTION instead of 8 FT wide DUE TO CLOSENESS OF NEW TREES AND NARROW SIDEWALK PAVING WIDTH.	Seneca St.: New trees will be planted 9 ft from the property line, so the 8 ft canopy would be 1 ft from the centerline of the trunk. SDOT will require a min 3 ft radius canopy. The sidewalk paving width is typically 6 ft, so 8 ft canopies would extend beyond the walking surface and over the planting beds. The proposed 5 ft wide canopies, suggested by Land Use Comments, should allow the new trees to grow. This street frontage does not include building entries. The proposed design continues the 5 ft width of overhead weather protection along the entire facade due to the narrow sidewalk, and to unify the facade (the street is steeply sloped, and the overhead protection must step in height multiple times, and for visual continuity, it is preferable to have a consistent width, rather than stepping in both plan and section).	B1 Respond to Neighborhood Context The proposed reduction in width of overhead weather protection responds to the existing narrow width of the Seneca St sidewalk, by providing protection at a scale that is responsive to the walking and planting zones. D2 Enhance the Building with Landscaping The SDOT requirements for clearance around trees will allow the new trees on Seneca to thrive. SDOT requirements for Street Trees Minimum 3 ft clear from centerline of trees. This requirement supersedes the overhead weather protection dimensional requirements. C5 Encourage overhead weather protection. As recommended in C5, the design of protection along Seneca St is related to "the scale of the space defined by the height and depth of the weather protection" and the narrow sidewalk/planting zone.
#02c	23.49.018.D - OVERHEAD WEATHER PROTECTION (Height above Sidewalk - Seneca)			
	23.49.018 - Overhead Weather Protection and Lighting. D. <u>The lower edge of the overhead weather protection must be a minimum of ten (10) feet and a maximum of fifteen (15) feet above the sidewalk.</u>	DEPARTURE REQUESTED, REF A-313. SENECA ST: SHORT PORTIONS of the Overhead Protection are SLIGHTLY ABOVE OR BELOW THE MINIMUM 10 FT AND MAXIMUM 15 FT ABOVE SIDEWALK.	Seneca St.: Due to the steep slope of 12.6%, the overhead protection must step multiple times over the length of the building frontage. In order to unify the facade and for visual continuity, the number of steps has been kept to six (6) stepping components that are integrated into the facade modulation, massing, program, and glazing.	B4 Design a well-proportioned and unified building. The proposed stepping of the overhead weather protection is derived in close correlation to features noted in B4, such as the "relative sizes and shapes of distinct building volumes", and "façade modulation and articulation; windows and fenestration patterns; ... garage entries". C5 Encourage overhead weather protection. As recommended in C5, the design of protection along Seneca St is related to "the overall architectural concept of the building."

#02d	23.49.018.B - OVERHEAD WEATHER PROTECTION (Depth from Building- Second Ave)			
	23.49.018 - Overhead Weather Protection and Lighting. B. Overhead weather protection shall have a minimum dimension of eight (8) feet measured horizontally from the building wall or must extend to a line two (2) feet from the curb line, whichever is less.	DEPARTURE REQUESTED, REF MUP-019. SECOND AVE: A 24 FT LONG PORTION of Overhead Protection near the corner of 2nd & Seneca is proposed to be 5 FT WIDE TO CONNECT WITH THE CONTIGUOUS PROTECTION AT SENECA.	Second Ave.: At the southern corner at Seneca, 5 ft wide protection is proposed to match the proposed 5 ft wide protection at Seneca that wraps the corner, and to relate to the small scale of the retail pavilion, in comparison to the 8 ft protection at the main tower lobby entry.	B4 Design a well-proportioned and unified building. The proposed reduction in depth at the corner of 2nd & Seneca, allows the proposed 5 ft deep canopies (due to Seneca St trees and sidewalk planter widths) to wrap consistently around the corner; this is responsive to attention to "corner features" as noted in B4. C5 Encourage overhead weather protection. As recommended in C5, the design of protection at this retail corner is related to "the scale of the space defined by the height and depth of the weather protection" and the pavilion-scale of the southeast retail building.
#02e	23.49.018.A - OVERHEAD WEATHER PROTECTION (Non-continuous - Second Ave)			
	23.49.018 - Overhead Weather Protection and Lighting. A. Continuous overhead weather protection shall be required for new development along the entire street frontage of a lot except along those portions of the structure facade that: 1.are located farther than five (5) feet from the street property line or widened sidewalk on private property; or 2.abut a bonused open space amenity feature; or 3.are separated from the street property line or widened sidewalk on private property by a landscaped area at least two (2) feet in width; or 4.are driveways into structures or loading docks.	DEPARTURE REQUESTED, REF MUP-019. SECOND AVE: At THE MAIN LOBBY FRONTAGE, SEVERAL GAPS are proposed at the main structural articulation, and near the north egress stair. None of these gaps occur at entries.	Second Ave: 8 ft wide protection is provided at the main tower facade, with small breaks proposed in the overhead protection at the columns, to allow the facade articulation to continue to grade, and at the northeast end of the lobby near the exit stair, to focus pedestrian attention to the main tower entry.	C4 Reinforce building entries The canopy length and placement at the central zone of the lobby focuses the pedestrian towards the lobby entrance, while de-emphasizing the egress stair at the north end of the lobby. C2 Design facades of many scales The main canopy massing reinforces the primary building lobby, while the small gaps allowing the verticality of the main tower structure to be legible to grade, adding "building modulation and articulated structural bays establish as frameworkd for composing facades" as recommended in C2. C5 Encourage overhead weather protection. As recommended in C5, the design of the protection at the main lobby at 2nd Ave is related to "the overall architectural concept of the building; and uses occurring within the building (such as entries...)"
#03	23.49.056.A.1 / A.2 - MINIMUM FAÇADE HEIGHTS			
	A. Minimum facade height 1. Minimum facade height(s) are prescribed in Table A for 23.49.056 and Exhibit A for 23.49.056, but minimum facade heights do not apply if all portions of the structure are lower than the elevation of the required minimum facade height. Table A for 23.49.056 Minimum Façade Height Streets requiring property line facades DOC1, DOC2, DMC: 35 feet <u>Class I pedestrian streets</u> <u>DOC 1, DOC 2: 35 feet</u> DMC: 25 feet <u>Class II pedestrian streets</u> <u>DOC 1, DOC 2: 25 feet</u> DMC: 15 feet Designated green streets DOC1, DOC2, DMC: 25 feet *Except as provided in subsection 23.49.056.A.2 regarding view corridor requirements.	DEPARTURE REQUESTED, REF MUP-016. PORTIONS OF THE RETAIL VILLAGE FACADES ON ALL 4 FRONTAGES do not meet the minimum facade height, especially at the street CORNERS where higher Avenue zoning requirements wrap the corner, and at PUBLICLY ACCESSIBLE OPEN SPACES.	The proposed departure would allow a design for a series of retail massings with ramps and stepped elements that are lower than the required facade height. The resulting landscape has unique site-specific benefits which reinforce the intent of the Design Guidelines	C1 Promote pedestrian interaction Portions of the retail village roofs are lower than the minimum proscribed height, in order to allow more light into the midblock plaza and retail roof terraces, and to create a more welcoming presence that encourages pedestrians to access the roof terraces, overlooks and gardens. C2 Design facades of many scales The more intimate height of the retail village creates a welcoming pedestrian scale at the base of the larger tower, and creates an interplay of scale with the larger Y columns supporting the tower lift. D3 Provide Elements that Define the Place The unique landscape design at the base of 2&U provides multi-level retail and lobby elements. Due to the 30 ft grade change across the site, there are opportunities to access the roofscapes of the lower 1st Ave retail from grade at the 2nd Ave and upper Seneca portions of the site. To enhance that opportunity, a series of exterior ramps and steps are incorporated into the massing. This unique series of interlocking spaces and views creates a memorable, site-specific environment that encourages a wide variety of programmed and spontaneous activities.

#04	23.49.056.B.1 - PROPERTY LINE FACADES			
	<p>B. Facade setback limits</p> <p>1. Setback limits for property line facades. The following setback limits apply to all streets designated on Map 1H as requiring property line facades, except as specified in subsection 23.49.056.B.1.d.</p> <p>a. The facades of structures 15 feet or less in height shall be located within 2 feet of the street lot line.</p> <p>b. Structures greater than 15 feet in height are governed by the following criteria:</p> <p>1) No setback limits apply up to an elevation of 15 feet above sidewalk grade.</p> <p><u>2) Between the elevations of 15 and 35 feet above sidewalk grade, the facade shall be located within 2 feet of the street lot line, except that:</u></p> <p><u>a) Any exterior public open space that satisfies the Downtown Amenity Standards, whether it receives a bonus or not, and any outdoor common recreation area required for residential uses, is not considered part of the setback.</u></p> <p>b) Setbacks between the elevations of 15 and 35 feet above sidewalk grade at the street lot line are permitted according to the following standards, as depicted in Exhibit B for 23.49.056:</p> <p>i. The maximum setback is 10 feet.</p> <p>ii. The total area of a facade that is set back more than 2 feet from the street lot line shall not exceed 40 percent of the total facade area between the elevations of 15 and 35 feet.</p> <p>iii. No setback deeper than 2 feet shall be wider than 20 feet, measured parallel to the street lot line.</p> <p>iv. The facade of the structure shall return to within 2 feet of the street lot line between each setback area for a minimum of 10 feet. Balcony railings and other nonstructural features or walls are not considered the facade of the structure.</p> <p><u>c) If sidewalk widening is required by Section 23.49.022, setback standards shall be measured to the line established by the new sidewalk width rather than the street lot line.</u></p>	<p>DEPARTURE REQUESTED. REF MUP-014,</p> <p>FIRST AVE:</p> <p>81.5% of the façade along First Ave between 15 FT-35 FT high is setback greater than the permitted 10 FT setback from the sidewalk widening line.</p>	<p>The proposed design requires a departure to extend the street level setback up to the roof of the retail elements, which are greater than 15 ft. The tower above and its structural columns, meet the sidewalk widening property line, and create an urban scale arcade along 1st Ave as frontage for the retail spaces along 1st Ave. The proposed departure benefits pedestrians and supports the intent of the Design Guidelines.</p>	<p>C2 Design a Facade of Many Scales</p> <p>A signature feature of 2&U is the large-scale structure that elevates the tower over the pedestrian-scale retail and groundscape components at the base, creating memorable juxtapositions and a rich variety of materials and scales.</p> <p>D3 Provide Elements that Define the Place</p> <p>The proposed design uses the tower’s larger, authentic structure to create a grand urban-scaled colonnade along 1st Ave, paired with autonomous, pedestrian-scaled retail forms. This repetition of columns defines a unique semi-covered space, allowing retail or dining to spill out and claim ownership over the widened sidewalks. The greater height permits more southwestern sun and daylight into the retail, and into outdoor spaces at street level and on terraces above. The continuous tower overhang unifies the space better than the undulating modulations permitted as-of-right.</p> <p>C4 Reinforce Building Entries</p> <p>The urban colonnade also creates a visual presence for the 1st Ave office lobby while maintaining the Design Guideline goal of minimal office lobby frontage where Street Level Uses are required.</p> <p>C5 Encourage Overhead Weather Protection</p> <p>The urban colonnade also creates a large sheltered porch along 1st Ave.</p>
#05	23.49.056.B.2 - GENERAL SETBACKS			
	<p>2. General Setback Limits. The following setback limits apply on streets not requiring property line facades, as shown on Map 1H:</p> <p>a. The portion of a structure subject to setback limits shall vary according to the structure height and required minimum facade height, as follows:</p> <p><u>1) Except as provided in subsection 23.49.056.B.2.a.3, if the structure is greater than 15 feet in height, the setback limits apply to the facade between an elevation of 15 feet above sidewalk grade and the minimum facade height established in subsection 23.49.056.A and illustrated in Exhibit C for 23.49.056.</u></p> <p>2) If the entire structure is 15 feet or less in height, the setback limits apply to the entire street-facing facade.</p> <p>3) If the minimum facade height is 15 feet, the setback limits apply to the portion of the street-facing facade that is 15 feet or less in height.</p> <p>b. The maximum area of all setbacks between the street lot line and facade along each street frontage of a lot shall not exceed the area derived by multiplying the averaging factor by the width of the street frontage of the structure along that street (see Exhibit D for 23.49.056). The averaging factor is five on Class I pedestrian streets and ten on Class II pedestrian streets and designated green streets.</p> <p>c. The maximum width, measured along the street lot line, of any setback area exceeding a depth of 15 feet from the street lot line shall not exceed 80 feet, or 30 percent of the lot frontage on that street, whichever is less. (See Exhibit D for 23.49.056.)</p> <p>d. The maximum setback of the facade from the street lot lines at intersections is 10 feet. The minimum distance the facade must conform to this limit is 20 feet along each street. (See Exhibit E for 23.49.056.)</p> <p><u>e. Any exterior public open space that meets the Downtown Amenity Standards, whether it receives a bonus or not, and any outdoor common recreation area required for residential uses, is not considered part of a setback. (See Exhibit C for 23.49.056.)</u></p> <p>f. If a sidewalk is widened into the lot as a condition to development, setback standards shall be measured to the line established by the new sidewalk width rather than the street lot line.</p>	<p>DEPARTURE REQUESTED. REF MUP-015.</p> <p>Note that following 23.49.056.B.2.e, the areas of the project that are Exterior Public Open Space are understood to be exempt from the requirements for General Setbacks. (See areas illustrated in diagrams).</p> <p>As a result of the Chapter 23 exemption of Exterior Public Open Space, the areas requested for Departure are solely portions of University St, Second Ave, and Seneca St facades that meet the setback guidelines in plan, but which DO NOT MEET THE FULL HEIGHT OF THE SETBACK ZONE FROM 15FT UP TO MIN FACADE HEIGHT because the building ROOFS ARE LOWER THAN THE MINIMUM HEIGHT (this is tied to Departure #3 for Minimum Facade Height).</p>	<p>All of the frontages (University St., Second Ave., or Seneca St.) comply with the maximum area of setbacks permitted with their averaging factor; since the major entries to the mid-block plaza and cross-block pedestrian routes are integral portions of the exempt Exterior Public Open Space sequence.</p> <p>The departure request is for the setbacks above the lower roofscapes of the retail and lobbies, which are an essential component of the unique urban village, and midblock plaza. The lowered roofs allow more daylight into the midblock plaza and the retail roof terrace levels. This request is tied to the departures for Minimum Facade Height.</p>	<p>C1 Promote pedestrian interaction</p> <p>Portions of the retail village roofs are lower than the minimum proscribed height, in order to allow more light into the midblock plaza and retail roof terraces, and to create a more welcoming presence that encourages pedestrians to access the roof terraces, overlooks and gardens.</p> <p>C2 Design facades of many scales</p> <p>The more intimate height of the retail village creates a welcoming pedestrain scale at the base of the larger tower, and creates an interplay of scale with the larger Y columns supporting the tower lift.</p> <p>D3 Provide Elements that Define the Place</p> <p>The unique groundscape design at the base of 2&U provides multi-level retail and lobby elements. Due to the 30 ft grade change across the site, there are opportunities to access the roofscapes of the lower 1st Ave retail from grade at the 2nd Ave and upper Seneca portions of the site. To enhance that opportunity, a series of exterior ramps and steps are incorporated into the massing. This unique series of interlocking spaces and views creates a memorable, site-specific environment that encourages a wide variety of programmed and spontaneous activities.</p>

#06	23.49.056.D.2.a / D.2.c / D.3.a - BLANK FAÇADE			
	<p>D. Blank Facade Limits.</p> <p>1. General Provisions.</p> <p>a. Blank facade limits apply to the area of the facade between 2 feet and 8 feet above the sidewalk, except that where the slope along the street frontage of the facade exceeds 7.5 percent, blank facade limits apply to the area of the facade between 4 feet and 8 feet above sidewalk grade.</p> <p>b. Any portion of a facade that is not transparent shall be considered to be a blank facade.</p> <p>c. Blank facade limits do not apply to portions of structures in residential use.</p> <p>3. Blank Facade Limits for Class II Pedestrian Streets.</p> <p>a. Blank facade segments shall be no more than 30 feet wide, except for garage doors, which may exceed 30 feet. Blank facade segment width may be increased to 60 feet if the Director in a Type I decision determines that the facade segment is enhanced by architectural detailing, artwork, landscaping, or similar features that have visual interest. The width of garage doors shall be limited to the width of the driveway plus 5 feet.</p> <p>b. Any blank segments of the facade shall be separated by transparent areas at least 2 feet wide.</p> <p>c. The total of all blank facade segments, including garage doors, shall not exceed 70 percent of the street facade of the structure on each street frontage; or 75 percent if the slope of the street frontage of the facade exceeds 7.5 percent.</p>	<p>DEPARTURE REQUESTED. REF MUP-018.</p> <p>SENECA ST:</p> <p>Two segments EXCEED THE MAXIMUM SEGMENT LENGTH of 30 FT.</p> <p>One segment is approximately 84'-3" long and the other segment is approximately 34'-11" long.</p>	<p>Due to the creation of a midblock plaza with publicly accessible plaza and retail spaces, mechanical spaces and service spaces have limited area to ventilate. Portions of the Seneca St facade are dedicated to intake and exhaust louvers for the retail spaces, and the parking garage, which is located below grade to reduce visual impact.</p> <p>Although the length of the maximum segments exceeds the maximum, the overall percentage of blank facade does not exceed the maximum of 75% (Class II Street, slope steeper than 7.5%). The design emphasis has been on locating non-transparent elements closer together at the central portion of the block, across the street from the loading and parking dock entry for the Second & Seneca Building, in order to maximize the amount of transparent and non-blank facades at the corners near First and Second Avenues, which are Class I Pedestrian Streets.</p>	<p>B3 Reinforce the positive urban form and architectural attributes of the immediate area.</p> <p>The goal is to locate the required building, retail, and garage louvers in locations with the least pedestrian impact. Seneca St is a Class II pedestrian street, so locating louvers along the facade helps minimize the amount of louvers on the Class I pedestrian streets (2nd Ave, University St., and 1st Ave).</p> <p>B3 Reinforce the positive urban form and architectural attributes of the immediate area.</p> <p>Locating the active non-blank portions of the program at the corners of the block near Second Ave and First Ave reinforces the architecture of the adjacent neighborhood, and locating the more service-oriented areas directly across the street from the service entries for the Second & Seneca Building also responds to the immediate context.</p> <p>E2 Integrate parking facilities</p> <p>The location of the parking garage fully below-grade reduces its visual impact, but results in the requirement to mechanically ventilate the garage via louvers.</p>
#07	23.49.058.C.3 - FAÇADE MODULATION			
	<p>3. The maximum length of a facade without modulation is prescribed in Table A for 23.49.058. This maximum length shall be measured parallel to each street lot line, and shall apply to any portion of a facade, including projections such as balconies, that is located within 15 feet of street lot lines.</p> <p>Table A for 23.49.058</p> <p>Modulation Requirements for DOC 1, DOC 2, and DMC Zones, Except DMC 160 Zone</p> <p>Elevation Maximum length of unmodulated facade within 15 feet of street lot line</p> <p>0 to 85 feet - No limit</p> <p>86 to 160 feet - 155 feet</p> <p>161 to 240 feet - 125 feet</p> <p>241 to 500 feet - 100 feet</p> <p>Above 500 feet - 80 feet</p> <p>4. Any portion of a facade exceeding the maximum length of facade prescribed on Table A for 23.49.058 shall be set back a minimum of 15 feet from the street lot line for a minimum distance of 60 feet before any other portion may be within 15 feet of the street lot line.</p>	<p>DESIGN DEPARTURE REQUESTED, REF MUP-013.</p> <p>Notch width 54'-4" is 6 FEET SHORTER THAN REQUIRED.</p> <p>Notch depth COMPLIES with 15 FT requirement.</p>	<p>In addition to the 54' x 15' notch, the 2nd Ave. façade has additional modulation created by the change in material along the eastern façade. The central portion with architectural precast concrete panels, in contrast to the primary material of glass, is 79'-6" long. The façade also has a 53' long portion that angles away from the property line.</p>	<p>A2 Enhance the Skyline</p> <p>The overall central notch and its extension up to the tower crown creates a dynamic, sculptural massing that is legible from the immediate neighborhood as well as from more distant view points.</p> <p>B2 Create a Transition in Bulk and Scale</p> <p>The variety of planes on 2nd Ave., and the change in material from glass to architectural precast concrete panels, break the tower into several vertically soaring massing components, and a more sculptural composition, than would a simple code-compliant solution with 100' and 125' broad faces with orthogonal inset corners.</p> <p>B4 Design a Well-Proportioned and Unified Building</p> <p>The departure allows an elegant facade modulation rhythm.</p>

#08	23.49.058.D.1 - UPPER-LEVEL WIDTH LIMIT			
	<p>D. Upper-level width limit</p> <p>1. On lots where the width and depth of the lot each exceed 200 feet, <u>the maximum facade width for any portion of a structure above 240 feet shall be 145 feet along the general north/south axis of a site (parallel to the Avenues),</u> and this portion of the structure shall be separated horizontally from any other portion of a structure on the lot above 240 feet by at least 80 feet at all points.</p>	<p>TOWER WIDTH 180'-0".</p> <p>DESIGN DEPARTURE REQUESTED: EXCEEDS MAXIMUM LENGTH</p> <p>REF SHEET MUP-012</p> <p>The preferred scheme tower width above the 240' height is 180'-0" wide, which EXCEEDS the 145' width by 35'-0". This tower width aligns with the view corridor setbacks.</p>		<p>B2 Create a Transition in Bulk and Scale</p> <p>Even with the requested departure, the proposed maximum tower floor plate due to the view corridor setbacks (108' x 180'-0"), is small compared to adjacent downtown office towers in the vicinity, and creates a very small diagonal in terms of view blockage from all directions. The proposed tower massing results in fewer stories in the tower, which is sympathetic to the adjacent office buildings, keeping the tower as a "member of chorus" in the adjacent skyline, rather than being significantly taller as would be permitted by zoning with Unlimited Height.</p> <p>B3 Reinforce the Positive Urban Form of Immediate Area</p> <p>The required 30' view corridor setbacks maintain wide viewing angles from the east, in keeping with the spirit of the ordinance. The building massing and orientation are compatible with, and smaller than, the adjacent Second & Seneca Building, Russell Investments, and 1201 3rd Ave., all of which have towers greater than 145' wide.</p> <p>B4 Design a Well-Proportioned and Unified Building</p> <p>The proposed facade modulation reduces the visual mass of the already slender tower. The proposed streamlined and integrated vertical articulation would not be achievable with a 240' high 180'-0" wide low rise base, stepping back to a narrow tower with 3 additional stories. The as-of-right zoning would result in a tiered-structure with a schism half-way up the tower massing, instead of an integrated and unified massing and facade.</p>
#09	23.49.019.B.1 - PARKING QUANTITY, LOCATION, AND ACCESS REQUIREMENTS, AND SCREENING AND LANDSCAPING OF PARKING AREAS			
	<p>B. Parking location within structures</p> <p>1. Parking at street level</p> <p>b. On Class II pedestrian streets, parking may be permitted <u>at street level</u> if:</p> <p><u>1) at least 30 percent of the street frontage of any street-level parking area, excluding that portion of the frontage occupied by garage doors, is separated from the street by other uses;</u></p> <p>2) the facade of the separating uses satisfies the transparency and blank wall standards for Class I pedestrian streets for the zone in which the structure is located;</p> <p>3) the portion of the parking, excluding garage doors, that is not separated from the street by other uses is screened from view at street level; and</p> <p>4) the street facade is enhanced by architectural detailing, artwork, landscaping, or similar visual interest features.</p>	<p>REF MUP-027. DESIGN DEPARTURE REQ.</p> <p>Short Term Parking:</p> <p>Due to the steep 12.6% grade at Seneca St., a small fraction of Short-term Parking has an exposed wall on Seneca St. This includes a 2'-6" length solid wall on one side of the exempt garage door and a 2'-2 3/4" length on the other. The remainder of garage wall is defined as below "street-level" per Exhibit 23.86.023 A "Street level facade". That triangular portion of below-street-level-facade is approximately 125 sf.</p> <p>Loading Dock:</p> <p>Loading has been identified during Zoning Review as "Parking" and has been determined to be subject to this code section. The total length of dock is at Street-level per 23.86.023 is 73.9 FT long. 30% of the street level dock (minus exempt doors) requires 14.5 FT of intervening use. The project provides 11.2 FT intervening use at the Gas Meter Room. This is 23.2% instead of the required 30%.</p>	<p>Due to the creation of a midblock plaza with publicly accessible plaza and retail spaces, the loading dock and parking garage access occupy a narrow segment of built area at grade along Seneca Street.</p> <p>The parking has been located with full intervening use along First Ave, and as maximized below-grade as possible given the need for an entry door along the slope of Seneca St.</p> <p>The Loading Dock requires a small departure for reduced intervening use for the street-level portion of the truck area. The at-grade retail at the corners of 1st and 2nd Avenues at Seneca, and the plaza to the north, have restricted the at-grade area available for essential services. All other streets surrounding this block are Class I pedestrian streets, meaning that Seneca Street is the most appropriate to have a concentration of service program. Given these space constraints, and the steep grade of the street, the visual impact of the loading has been minimized. The translucent glass loading dock doors, and the consistent facade treatment along the dock that corresponds to the solid cladding of adjacent retail, minimizes the visual presence of the dock.</p>	<p>B3 Reinforce the positive urban form and architectural attributes of the immediate area. / C4 Service Uses</p> <p>The goal is to locate the required loading bay and short term parking in locations with the least pedestrian impact. Seneca St is a Class II pedestrian street whereas all other streets bounding the site are Class I pedestrian streets (2nd Ave, University St., and 1st Ave); additionally, the midblock plaza creates an alternative path for traversing the site in the east-west direction.</p> <p>C3 Provide Active-not blank-facades</p> <p>This facade has pedestrian-scale retail at either end and richly detailed wall surfaces boasting a variety of materials, creating interest for pedestrians. Wide, translucent glass garage doors at the loading dock and parking garage access punctuate the opaque surfaces.</p> <p>E3 Minimize the Presence of Service Areas</p> <p>The proposed substitution of TRANSLUCENT glass instead of Transparent Glass at the loading dock and garage entries meets the intent of this section, in order to screen these necessary areas from pedestrian view. While the glass could be transparent, the project team believes translucent glass meets the Guidelines intent better.</p>

#10	23.49.056.C.4.c - FAÇADE TRANSPARENCY REQUIREMENTS			
	<p>Facade transparency requirements apply to the area of the facade between 2 feet and 8 feet above the sidewalk, except that if the slope along the street frontage of the facade exceeds 7.5 percent, the transparency requirements apply to the area of the facade between 4 feet and 8 feet above sidewalk grade. Only clear or lightly tinted glass in windows, doors, and display windows is considered to be transparent. Transparent areas shall allow views into the structure or into display windows from the outside.</p> <p>2. Facade transparency requirements do not apply to portions of structures in residential use.</p> <p>3. If the transparency requirements of this subsection 23.49.056.C are inconsistent with the glazing limits in the Energy Code, this subsection 23.49.056.C applies to the extent permitted by applicable law.</p> <p>4. Transparency requirements are as follows:</p> <p>a. Class I pedestrian streets and designated green streets: A minimum of 60 percent of the street level street-facing facade shall be transparent.</p> <p>b. Class II pedestrian streets: A minimum of 30 percent of the street level street-facing facade shall be transparent.</p> <p><u>c. Where the slope along the street frontage of the facade exceeds 7.5 percent, the required amount of transparency shall be reduced to 50 percent on Class I pedestrian streets and designated green streets and 25 percent on Class II pedestrian streets.</u></p>	<p>REF MUP-017. DESIGN DEPARTURE REQUESTED.</p> <p>The Facade Transparency Percentage is less than the 25% required for a Class II Pedestrian Street with a slope greater than 7.5%. TRANSLUCENT glass facade areas have been provided, such that the COMBINED TRANSLUCENT AND TRANSPARENT GLASS AREAS EXCEED THE 25% required area for this street condition.</p> <p>While the areas proposed as Translucent Glass could be Transparent Glass, the project team believes that TRANSPARENT glass meets the DESIGN INTENT of the requirements for this class of street, while more effectively screening loading and mechanical spaces from pedestrian view, as recommended by E3, MINIMIZE THE PRESENCE OF SERVICE AREAS.</p>	<p>Due to the creation of a midblock plaza with publicly accessible plaza and retail spaces and the exigencies of all the other streets surrounding this property being Class I pedestrian streets, the loading dock and parking garage access occupy a narrow segment of built area at grade along Seneca Street.</p> <p>Additionally, mechanical services occupy a segment of the at-grade portion of this facade. As such, opportunities for transparent areas to activate this facade are limited.</p> <p>At-grade retail with transparent glass has been provided at the corners of Seneca Street and 1st and 2nd Avenues, in compliance with the Zoning and Guidelines for Downtown Development, and to prioritize vision glass at the most visible and active pedestrian areas of the block.</p> <p>Translucent glass garage doors to the loading dock and parking garage have been provided as a means of adding interest and modulating the facade in the mid portion of the facade length, while minimizing the visual impact of service and parking spaces.</p> <p>The combined total translucent and transparent area covers 37.3% of the required area between 4 ft and 8 ft above the sidewalk.</p>	<p>B3 Reinforce the positive urban form and architectural attributes of the immediate area.</p> <p>The overall project planning follows the B3 recommendations to "locate parking and vehicle access away from entries, open space, and street intersections". The goal is to locate the required loading bay and short term parking in locations with the least pedestrian impact. Seneca St is a Class II pedestrian street whereas all other streets bounding the site are Class I pedestrian streets (2nd Ave, University St., and 1st Ave); additionally, the midblock plaza creates an alternative path for traversing the site in the east-west direction.</p> <p>C2 Design Facade of Many Scales</p> <p>This facade has transparent-glazed pedestrian-scale retail at either end and richly detailed wall surfaces boasting a variety of materials, creating interest for pedestrians. Wide, translucent glass garage doors at the loading dock and parking garage access punctuate the opaque surfaces.</p> <p>D1 Provide Inviting & Usable Open Space</p> <p>The location of the loading, parking, and mechanical areas along Seneca St. allows for the creation of the midblock plaza, and through-block passages. The prioritization of these pedestrian-friendly spaces has been the key focus of the 2&U site planning strategy.</p> <p>E3 Minimize the Presence of Service Areas</p> <p>The proposed substitution of TRANSLUCENT glass instead of Transparent Glass at the loading dock and garage entries meets the intent of this section, in order to screen these necessary areas from pedestrian view. While the glass could be transparent, the project team believes translucent glass meets the Guidelines intent better.</p>





Appendices

APPENDIX I | Existing Conditions Imagery

2ND AVENUE STREETSCAPE



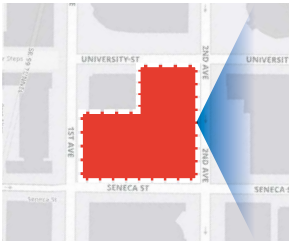
VIEW TO SITE - 2ND AVE



VIEW FROM SITE - 2ND AVE



Top Image: View to Site



Bottom Image: View from Site



APPENDIX I | Existing Conditions Imagery

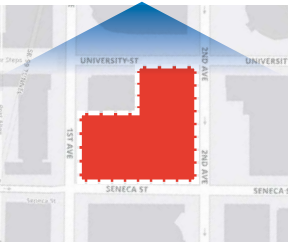
UNIVERSITY STREET STREETSCAPE



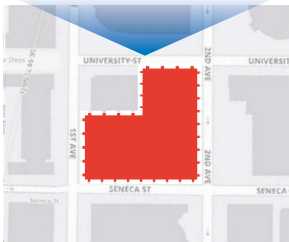
VIEW TO SITE - UNIVERSITY STREET



VIEW FROM SITE - UNIVERSITY STREET



Top Image: View to Site

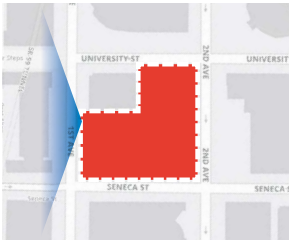


Bottom: View from Site



APPENDIX I | Existing Conditions Imagery

1ST AVENUE STREETSCAPE

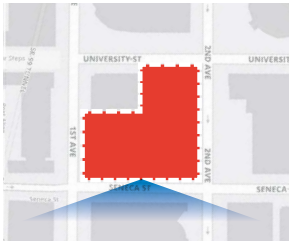
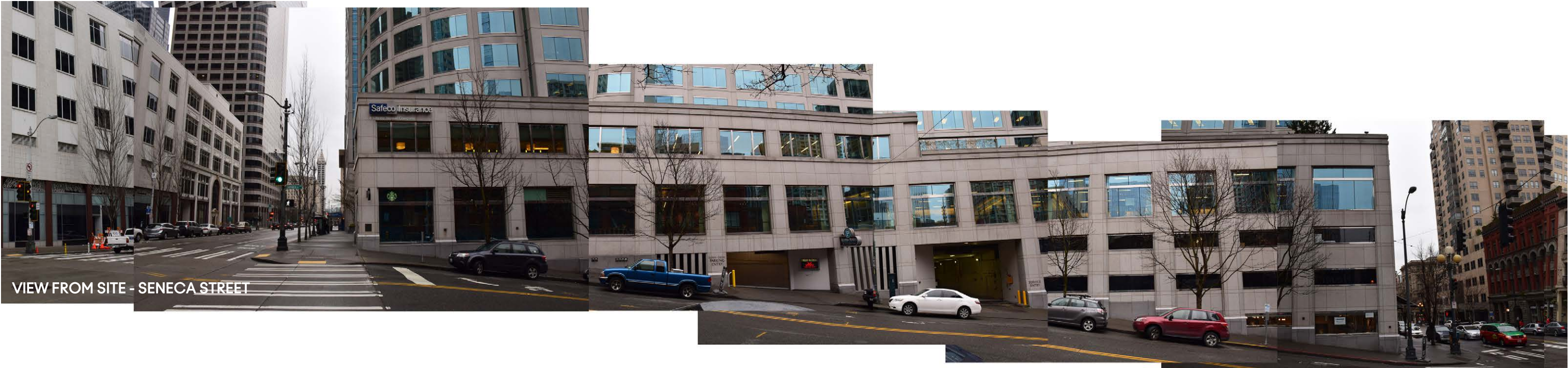


Top Image: View to Site
iv | DPD Project Number: 3019177

Bottom Image: View from Site

APPENDIX I | Existing Conditions Imagery

SENECA STREET STREETSCAPE



Top Image: View to Site

Bottom: View from Site

APPENDIX I | Existing Conditions Imagery

EXISTING ALLEY CONDITIONS

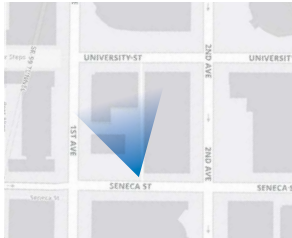
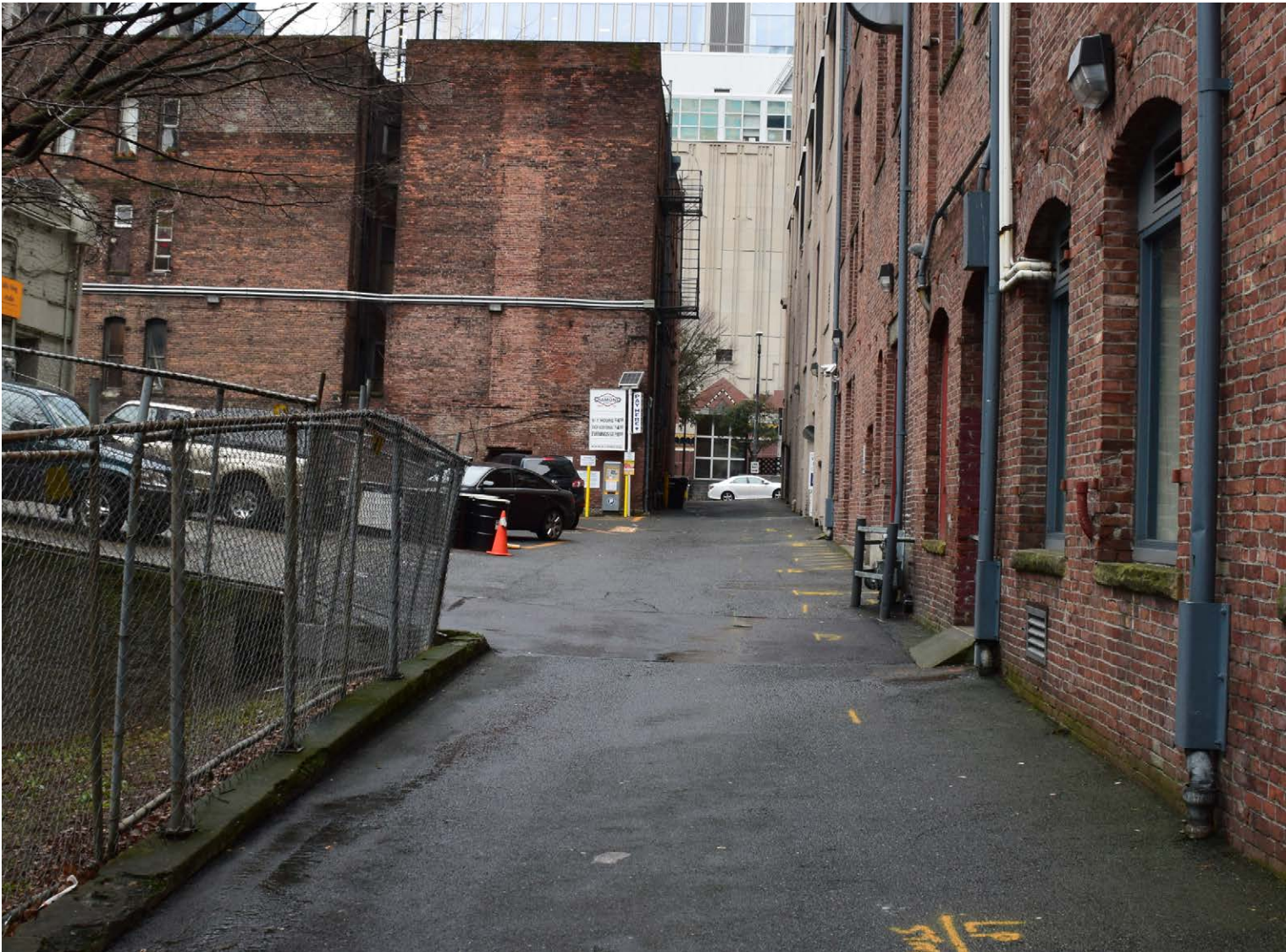


Image:
Alley View Facing North

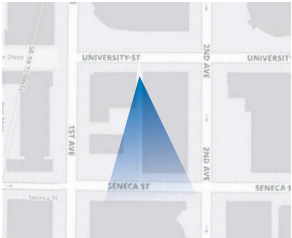
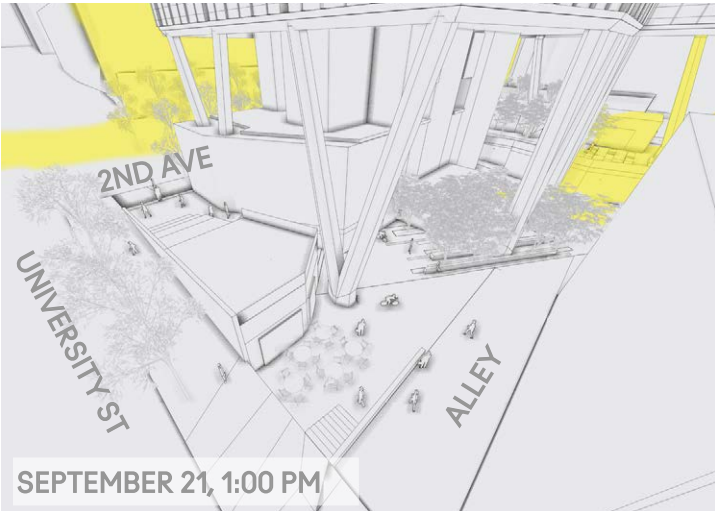
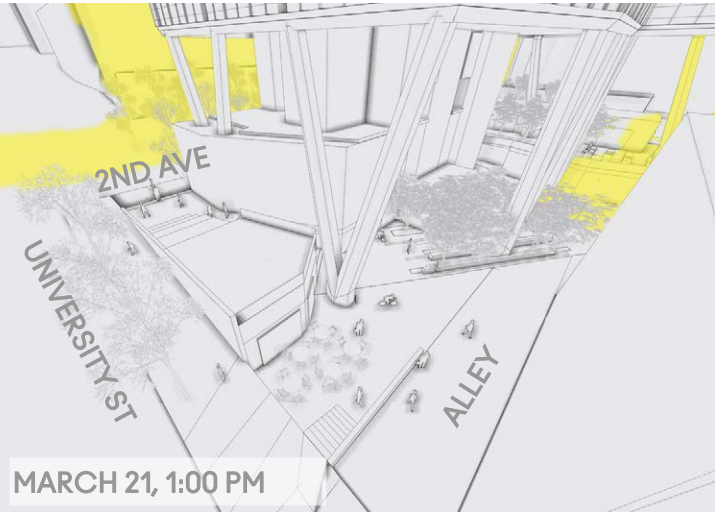


Image:
Alley View Facing South

APPENDIX I | Direct Solar Exposure Studies - SAM Wedge and Alley Plaza

Direct Solar Indirect/Ambient



SITE STRATEGIES CONSIDERED

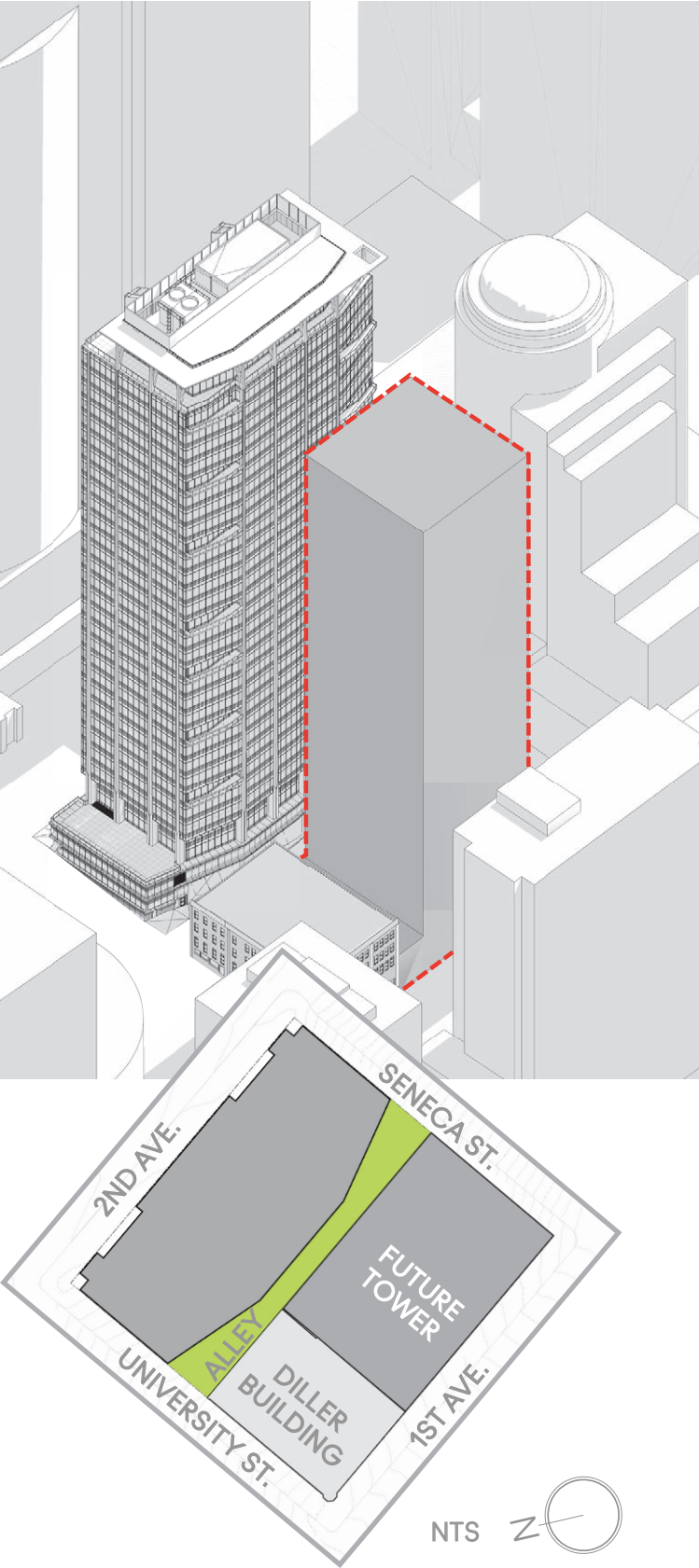
- 1 NO ALLEY VACATION:
- Commercial office tower on 2nd Avenue.
 - 400 ft Residential tower on 1st Avenue.
 - No public space created.
 - Reference MUP 3019178.
- 2 VACATION WITH NO LIFT:
- Typical market development.
 - L-shaped office tower with 2nd and 1st Avenue entries.
 - Ground level lobbies, retail, parking, and service.
 - No public space created.
- 3 ALLEY VACATION + LIFT:
- Tower lifted above ground plane.
 - Publicly-accessible space created with mid-block plaza and through-block passages.
 - Reference MUP 3019177.

LEGEND

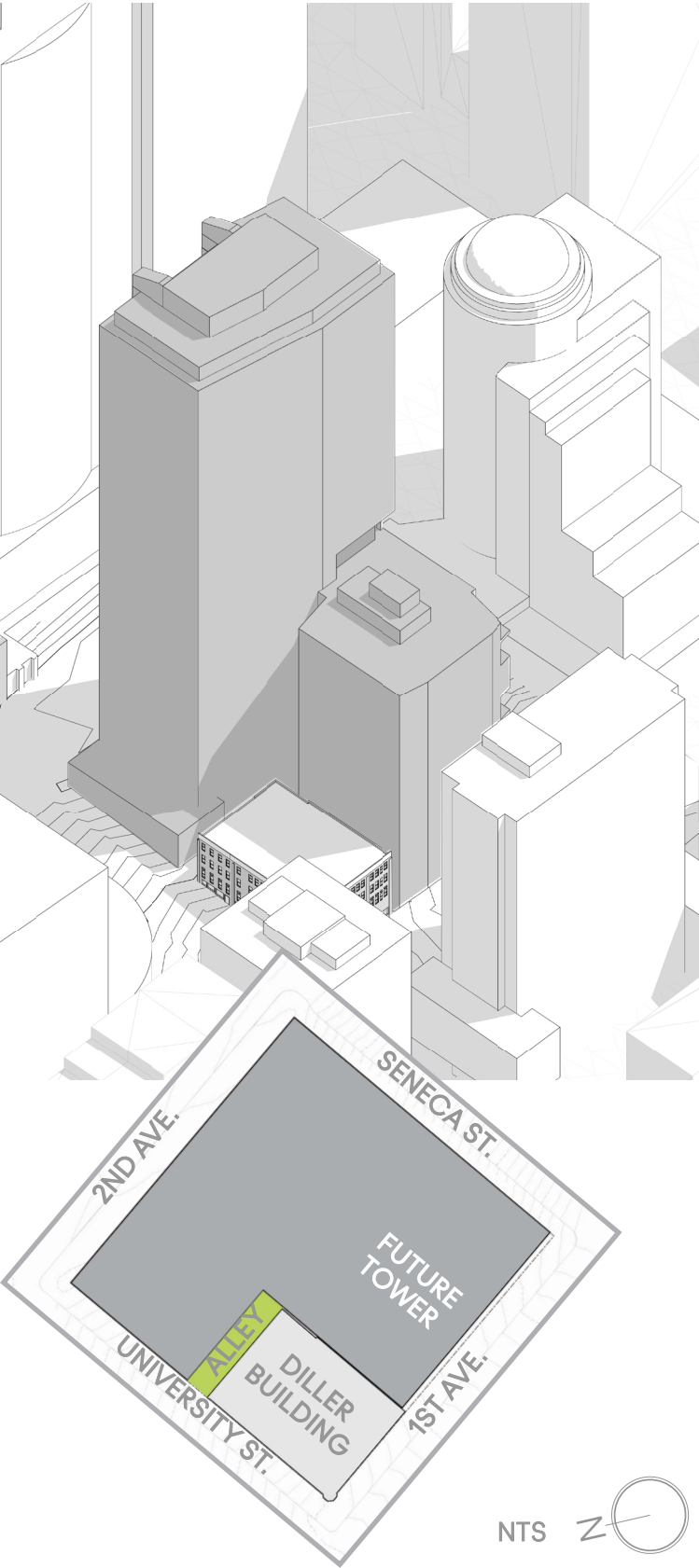
Open Space

Building Footprint

STRATEGY 1: NO ALLEY VACATION

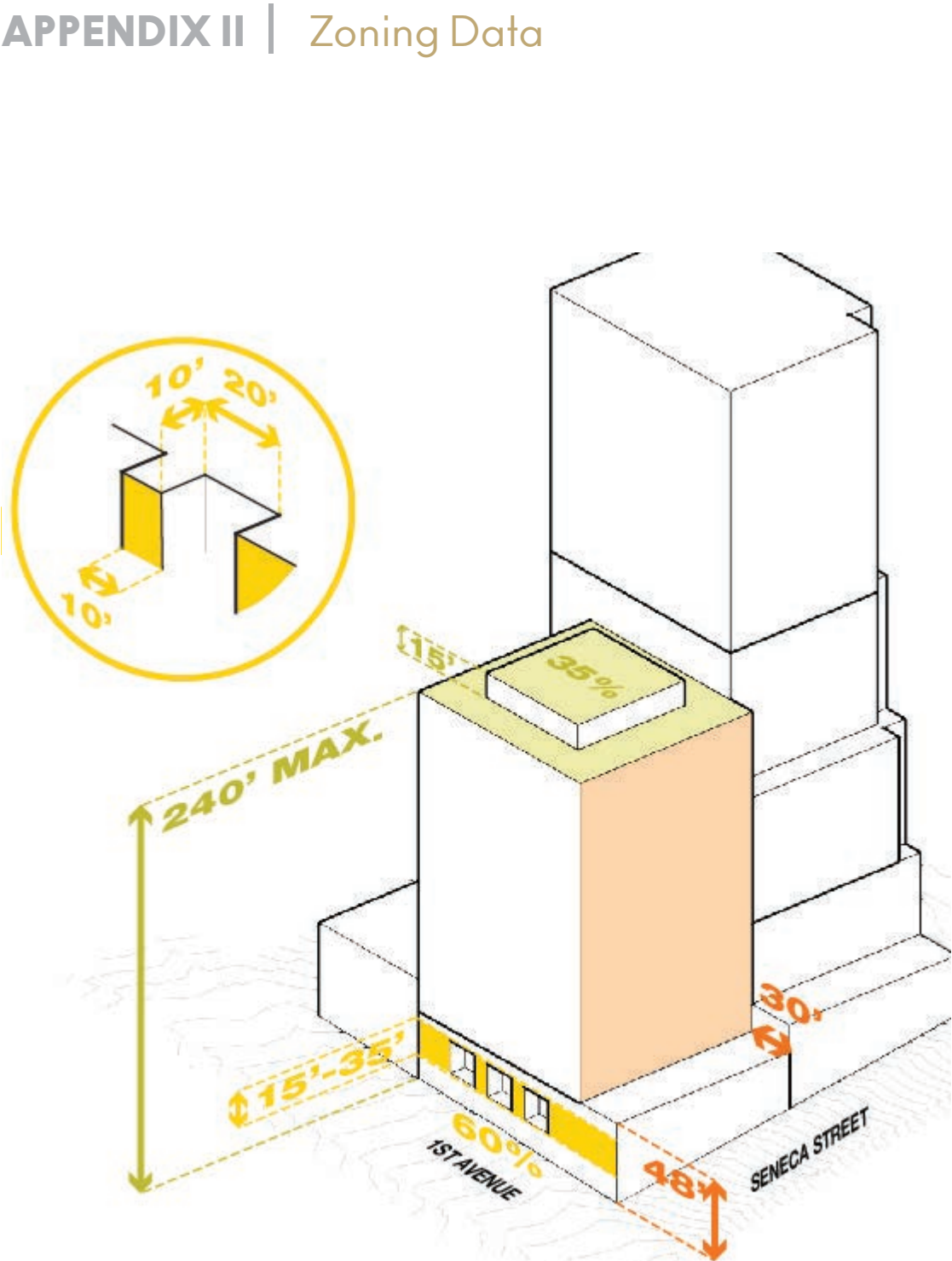


STRATEGY 2: 2/3 VACATION



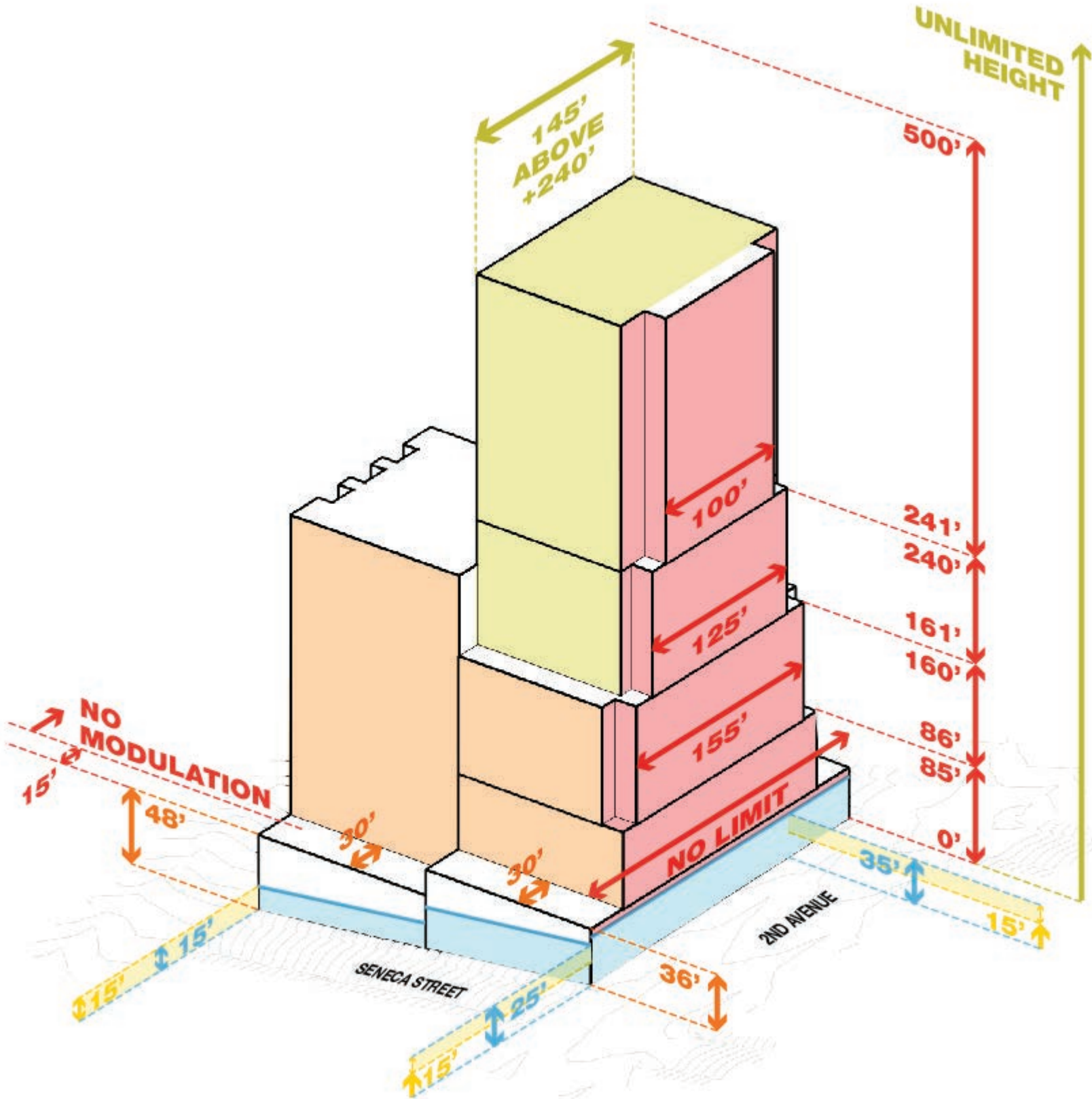
STRATEGY 3: 2/3 ALLEY VACATION + LIFT





ZONING ENVELOPE

- Maximum Tower Height + Width - Section 23.49.058.D.2
- Facade Modulation - Table 23.49.058A
- View Corridor Setbacks - Section 23.49.024
- Minimum Facade Heights - Section 23.49.056 Table A



ZONING CLASSIFICATION

DOC1U/460/U Downtown Office Core
DMC 240/290-400 Downtown Mixed Commercial

SITE AREA (INCLUDING VACATED ALLEY PORTIONS)

DOC1U/450/U: 27,200 sf
DMC 240/290-400: 19,040 sf

STREET CLASSIFICATION (MAP 1B)

1st Ave. & 2nd Ave.: Principal Transit Street
University St.: Minor Arterial
Seneca St.: Principal Arterial

SIDEWALK WIDENING (MAP 1C) & 23.49.022

1st Ave. & 2nd Ave.: 18’ (widening required at 1st Ave.)
University & Seneca: 12’ (no widening required)

VIEW CORRIDORS (MAP 1D) & 23.49.024

University St. & Seneca St.:
30 ft. view corridor setbacks per Section 23.49.024.
(Reference Zoning Envelope Diagram in appendix).

PEDESTRIAN STREET CLASSIFICATION (MAP 1F)

1st Ave., 2nd Ave. & University St.: Class I Pedestrian Streets
Seneca St.: Class II Pedestrian Street

STREET LEVEL USES REQUIRED (MAP 1G)

Street level uses required at 1st Ave. per 23.49.009.

PROPERTY LINE FACADES (MAP 1H)

Property line facades required at 1st Ave.

23.49.008.A.3 STRUCTURE HEIGHT

DOC1U/450/U: Unlimited
DMC 240/290-400: 240’ (plus permitted penthouses)

23.49.009 STREET LEVEL USES REQUIREMENTS

Required at 1st Ave. per Map 1G
A. Uses: includes general sales and services; retail sales; entertainment;
public atriums; eating and drinking establishments.
B.1. 75% min street frontage. 25% entrances or other. Frontage of
exterior public open space is not counted in street frontage.

23.49.011 FLOOR AREA RATIO

DOC1U/450/U: Base: 6 Max: 20
DMC 240/290-400: Base: 5 Max: 7

23.49.016 OPEN SPACE

20 sf required per 1,000 gsf office = approx.. 13,400 sf.

23.49.018 OVERHEAD WEATHER PROTECTION AND LIGHTING

Required along entire street frontage except as indicated in 23.49.018.

23.49.019 PARKING QUANTITY, LOCATION, AND ACCESS REQUIREMENTS

A. No long-term or short-term parking required. If provided:
35% min. small vehicles, min. 7’-6” x 15’
35% min. large vehicles, min. 8’-6” x 19’

E. Bicycle parking: approx. 100 bicycles required.
Office: 1 per 5,000 gsf up to 50, then 1 per 10,000 gsf
Retail: 1 per 5,000 gsf up to 50, then 1 per 10,000 gsf

F. Bike Commuter Shower Facilities:
3 showers per gender minimum required.

G. Off-street Loading:
Low & medium demand - 7 berths required.
Type I application pending for reduced quantity and
size of berths.

H. Access to Parking:
If the lot does not abut an alley & abuts more than 1 ROW, location is
determined by Director (Type I decision with DOT - application pending)
from ROW per Map 1B & Map 1F, most preferred according to ranking:
Rank 3: Class II Pedestrian St. - Principal arterial
 (Seneca St.)
Rank 4: Class I Pedestrian St. - Minor arterial
 (University St.)
Rank 5: Class I Pedestrian St. - Principal arterial
 (1st Ave. & 2nd Ave.)

23.49.056 ST. FACADE, LANDSCAPING, & ST. SETBACK REQ.

A. Min. Facade Ht.
Class I Pedestrian St (1st Ave., 2nd Ave. & University):
35’ ht.
Class II Pedestrian St (Seneca St.):
25’ in DOC1U/450/U; 15’ in DMC-240/290-400.

B-2. General Setback Limits Averaging Factor
Class I Pedestrian St (1st Ave., 2nd Ave. & University)
Factor 5
Class II Pedestrian St (Seneca St.):
Factor 10

e. Exterior public open space meeting Downtown Amenity
Standards, bonused or not, is not considered part of a setback.

C. Facade Transparency:
Class I Ped. St, slope <7.5%.: (1st Ave., 2nd Ave.) :
Min. 60% transparent between 2’ & 8’ above sidewalk.
Class I Ped St, slope >7.5%: (University St.):
Min. 50% transparent between 4’ & 8’ above sidewalk.
Class II Ped. St, slope >7.5%: (Seneca St.):
Min. 25% transparent between 4’ & 8’ above sidewalk.

D. Blank Facade elements:
Class I Ped. St, slope <7.5%.: (1st Ave., 2nd Ave.) :
Max. 40% blank between 2’ & 8’ above sidewalk.
Max. 15’ segment length.
Class I Ped St, slope >7.5%: (University St.):
Max. 50% blank between 4’ & 8’ above sidewalk.
Max. 15’ segment length.
Class II Ped. St, slope >7.5%: (Seneca St.):
Max. 75% blank between 4’ & 8’ above sidewalk.
Max. 30’ segment except garage (60’ with exceptions).

E. Street Trees: Required on 2nd Ave., University St., and Seneca St.

23.49.038 - LOTS LOCATED IN MORE THAN ONE (1) ZONE.

When a lot is located in more than one (1) zone, the regulations for each
zone shall apply to the portion of the lot located in that zone.

23.49.058 - DOC1, DOC2, & DMC UPPER-LEVEL DEVELOPMENT STANDARDS

B. Facade Modulation. Refer to diagram (appendix).
Required above 85’ for any portion within 15’ of street property line. No
modulation required for portions set back 15’ from street property line.

C. Upper-level width limit.
On lots where width & depth of lot each exceed 200’, maximum facade
width for any portion of building above 240’ shall be 145’ along north/
south axis of site (parallel to the Avenues).

EXISTING ALLEY NETWORK



APPENDIX III | Design Commission Vacation

EXPANDING OPEN SPACE

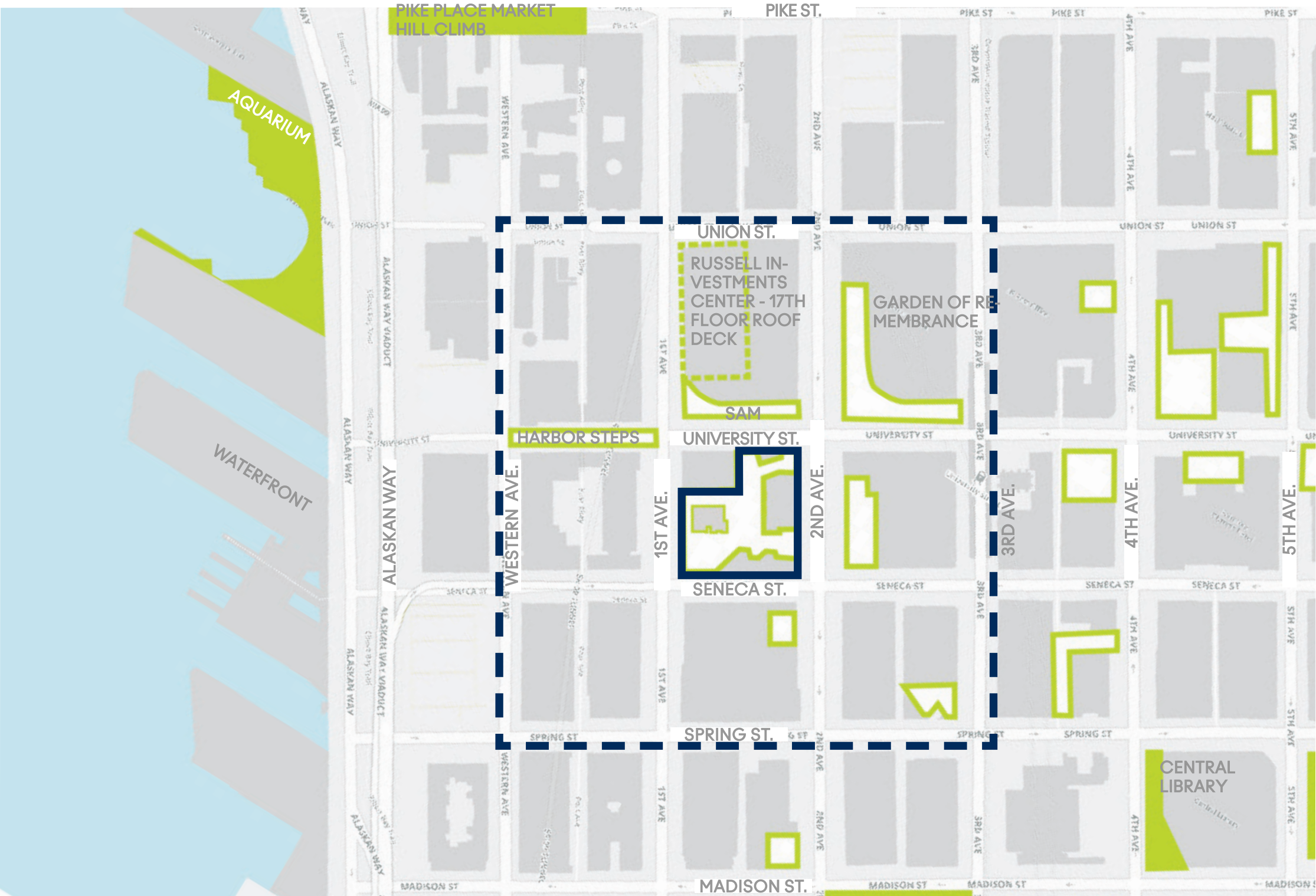
LEGEND

2+U Site

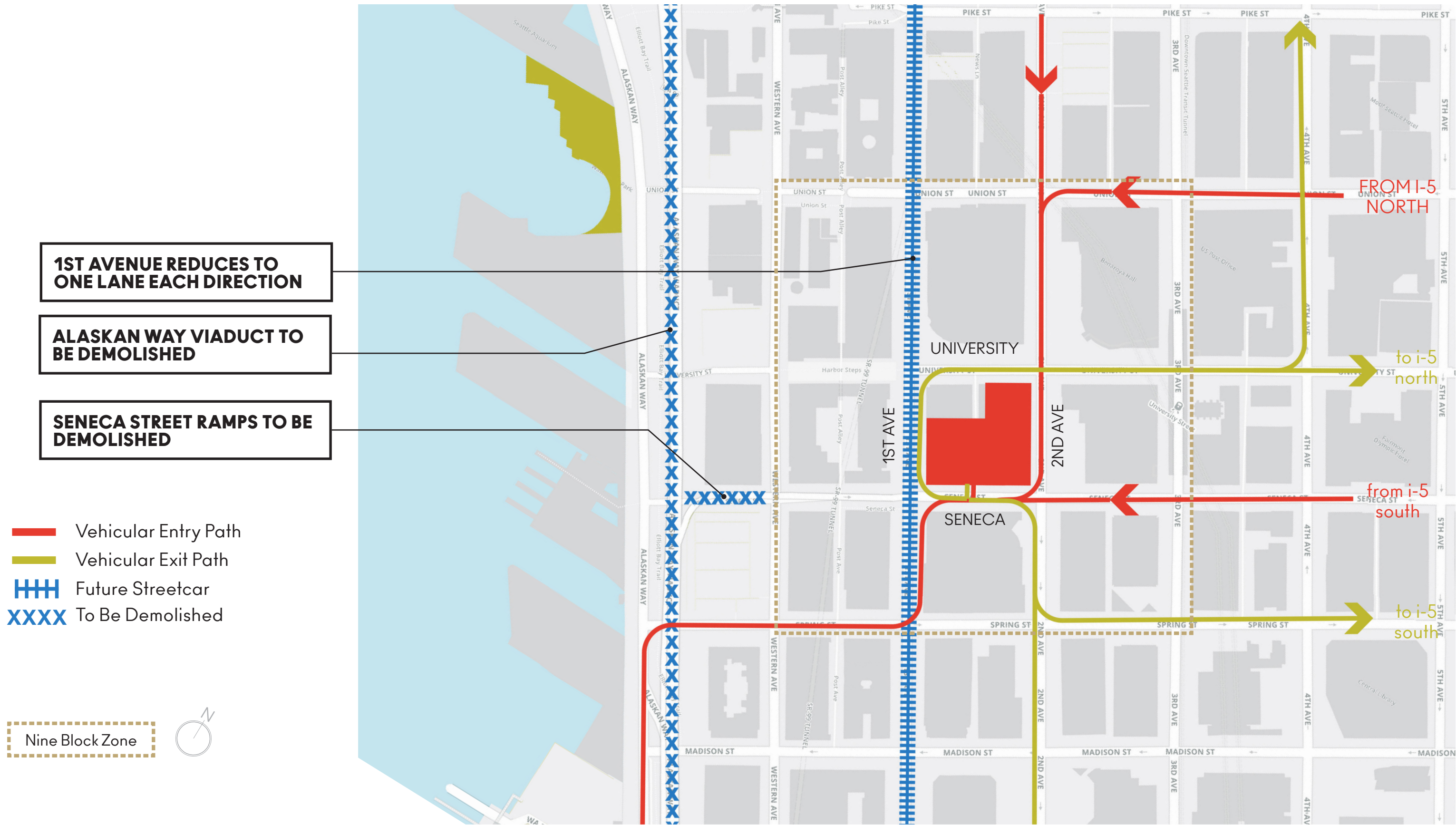
Nine-block zone

Public Open Space

Publicly Accessible Privately-Owned Open Space



FUTURE VEHICULAR ACCESS



URBAN MERIT SUMMARY

Unanimous approval of Urban Merit (8-0) with three Administrative Items (Follows):

PEDESTRIAN ACCESS / OPEN SPACE

The Commission supported the lift and asked the team to focus on creating entrances visible / inviting to the public and use landscaping to invite pedestrians into the site.

VEHICULAR CIRCULATION

The Commission supported the hammerhead design for pedestrian access as well as access for the Diller building. The team was asked to continue to study how the non-vacated alley will be managed during daytime hours.

LIGHT, AIR,+ VIEWS

The Commission values the design team’s attempt to bring light and air into the open space under the building through the lift and encouraged the team to think about how lighting can be used to create comfort throughout the day.

SERVICES + UTILITIES

The Commissioners understand the difficulty of site utility coordination, and asked the team to continue researching on-site utility vault locations, ideally outside of pedestrian sidewalks and pathways.

BUILDING MASS + SCALE

The Commissioners agree that the scale of the building in relation to the Diller Hotel works very well. They cautioned the team from labeling the project a “urban village” and encouraged use of “downtown urban center.” The Commissioners felt the project would serve as an opportunity to do something distinctive within the downtown core.

EQUITY

The Commission has stressed that equity is fundamentally important when reviewing projects and they will continue to evaluate every decision made through an equitable framework. Although the project team has provided a unique solution for creating public space, the commissioners highly encourage the project team to continue making equitable design decisions.

ADMINISTRATIVE ACTIONS / RESPONSES

ACTION 1

The proposed pedestrian access points along 1st Avenue should be redesigned to reinforce their role as access to and through the site, increasing their visibility, and presence along 1st Avenue.

RESPONSE

The pedestrian access points along 1st Avenue were redesigned to provide greater access, transparency, and visibility for pedestrians.

ACTION 2

Provide a conceptual landscape plan for the site. As part of this plan, please include information on how the ground plane will be treated and how planting will reinforce pedestrian routes in and through the site.

RESPONSE

A landscape plan was provided which included information on how the ground plane will be treated in terms of planting, materials, and pedestrian infrastructure to reinforce pedestrian routes through the site from the five entrance points.

ACTION 3

Provide additional details about the hammerhead design. Project design should show how the hammerhead relates to alley function, how pedestrian movement will be accommodated, and how it reinforces the open space network.

RESPONSE

Additional hammerhead diagrams were provided showing relationship to pedestrian movement, alley function, and open space network.

OUTCOME

The Commission approved the project team’s response to all three recommended actions during a subcommittee meeting on November 16, 2015.