

2220 E UNION STREET MIXED-USE BUILDING
DESIGN RECOMMENDATION (DPD 3019001) • FEBRUARY 2016



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Project Information

Name:

East Union Apartments

Address:

2220 E Union Street, Seattle WA

Owner:

East Union 22, LLC

Developer:

Lake Union Partners
T (206) 829-9452

Architect:

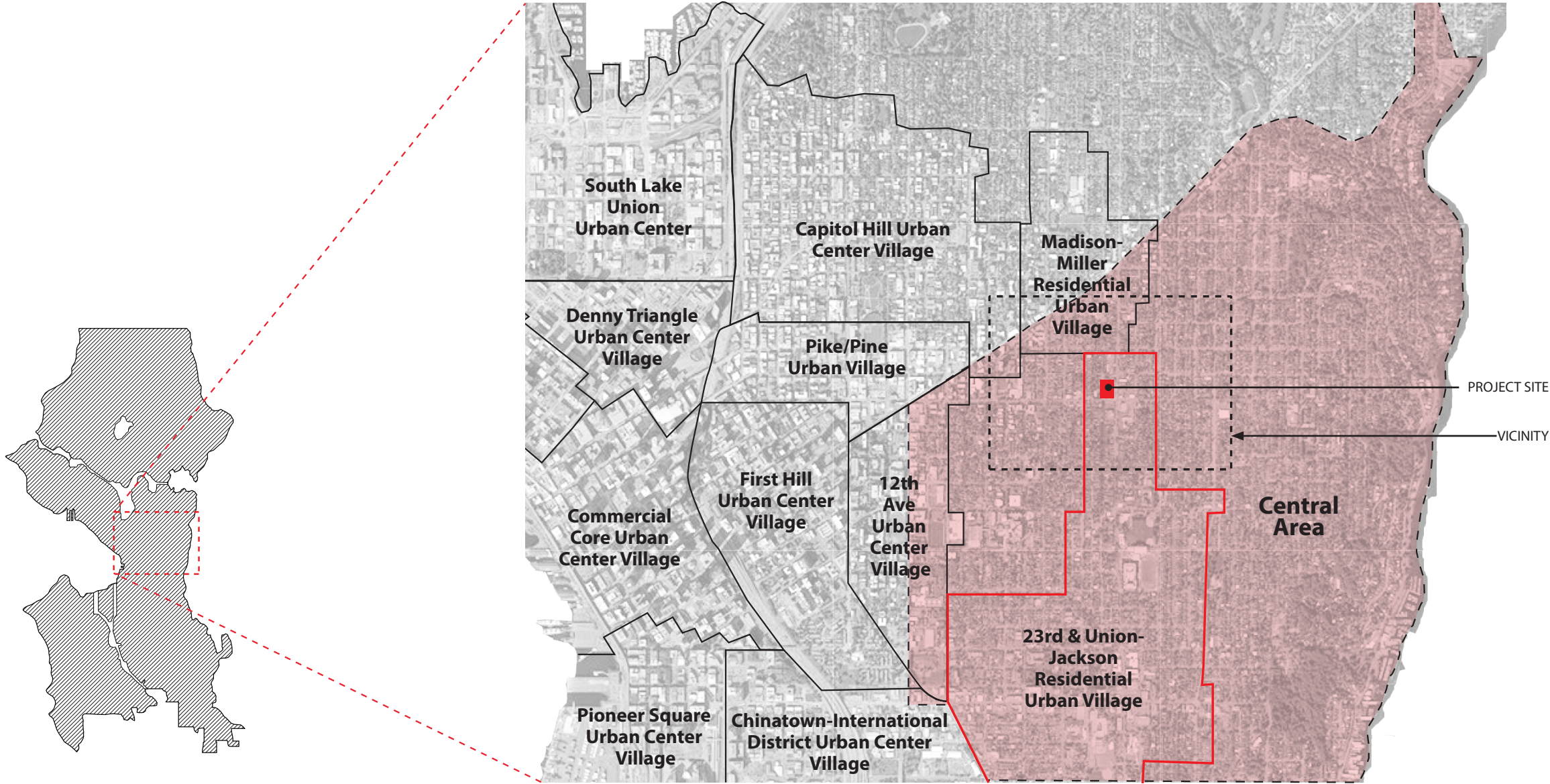
Weinstein A+U LLC
T (206) 443-8606

Description:

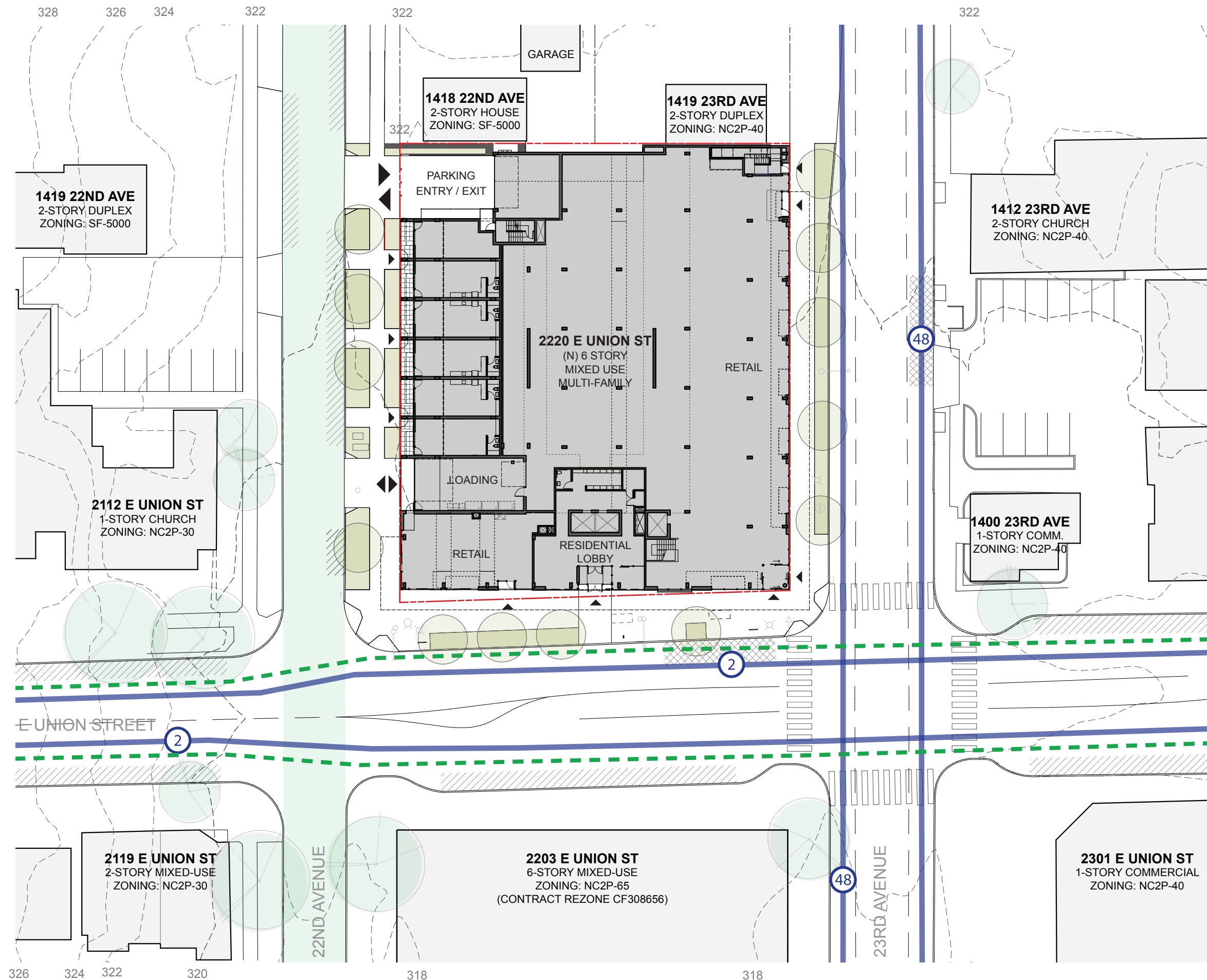
The project site consists of three parcels located between 22nd and 23rd Avenues on the north side of E Union Street.

The proposed project will replace the existing gas station with a six-story, market-rate apartment building 65-feet in height with an area of 185,280-sf including the below grade parking level. The building will contain 144 residential apartment units, resident lobby and amenity spaces, an outdoor, landscaped courtyard and roof terrace, on-site building management and leasing offices, street level commercial space, and two below-grade parking levels with 50 nonresidential spaces and 98 residential spaces.

A legislative rezoning that includes the project site is currently being considered based on the recommendation of the 23rd Avenue Action Plan Urban Design Framework (23rd Avenue UDF). Due to the indeterminate timeline of the legislative process, the proposed project is seeking a Contract Rezone that is consistent with the recommendation of the 23rd Avenue UDF. The project site is proposed to be rezoned from the existing NC2P-40 and NC2-40 zoning to NC2P-65 and NC2-65 zoning respectively.







CONTEXT ANALYSIS

Neighborhood Context

- Prominent site located at the main intersection of the Union Core, one of three community nodes identified in the 23rd Avenue Urban Design Framework
- Northwest portion of the site borders single-family zoning (single-family house)
- Comparably scaled six-story mixed-use apartment development located south of the site across E Union Street (The Central, 2203 E Union St)
- Auto-oriented one to two-story buildings currently located on commercially zoned lots to the north and south of the site along either side of 23rd Avenue

Street & Bus System

- 23rd Avenue is a principal arterial and the primary north-south connection on the east side of Seattle
- MT #48 bus route runs along 23rd Avenue and connects the Central Area to Montlake, the University District and Ballard to the north, and Mount Baker to the south
- E Union Street is a minor arterial and principal bicycle route; primary east-west connection for the immediate neighborhood
- MT #2 serves the E Union Street corridor and connects the Central Area to Downtown Seattle and Madrona
- 22nd Avenue is a narrower residential street with limited speed and through traffic capacity

Streetscape

- Bicycle improvements and traffic calming measures were recently installed by the City on 22nd Avenue as part of the Central Area Neighborhood Greenway
- Pedestrian improvements such as new sidewalks, planting beds and street trees are planned as part of the City's 23rd Avenue Corridor Complete Streets Project

Note: Additional context analysis originally presented at Early Design Guidance is located in the Appendix

(N) Building Footprint	
Street Parking Area	
Bus Stop Area	
Metro Bus Route	
Greenway Street	
Bike Way / Sharrow	
Building Entry / Exit	

APPROVED EDG CONCEPT

Key Design Features

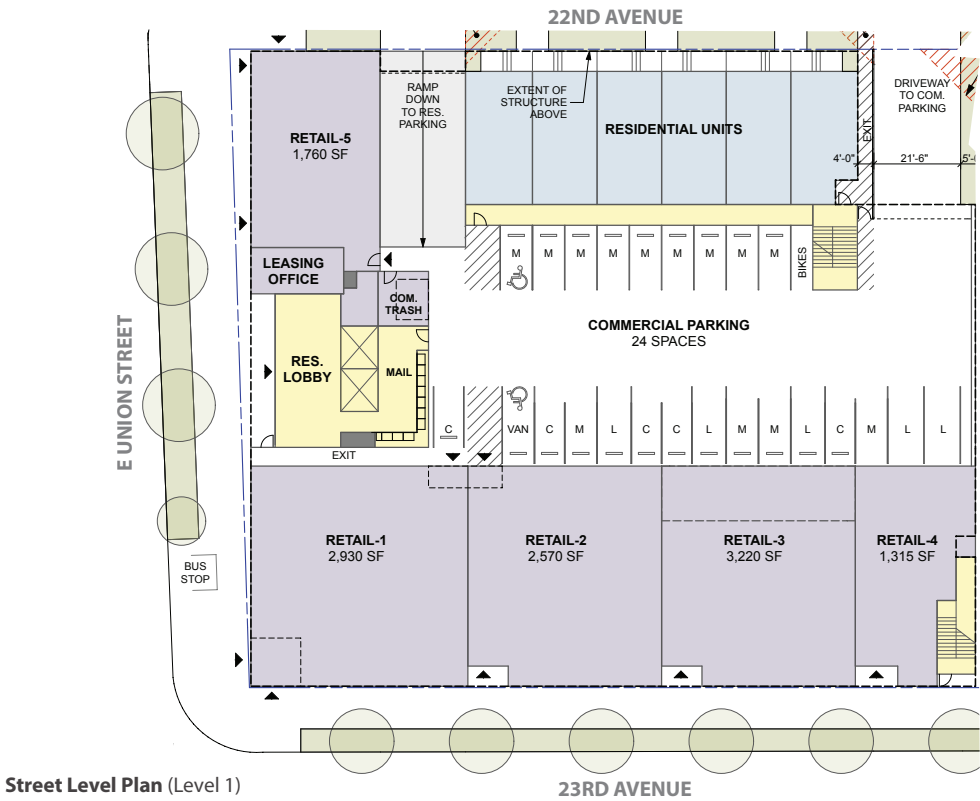
- U-shaped residential floorplans surround elevated courtyard
- Residential lobby centrally located on E Union Street
- Two-story ground-related residential units located on 22nd Avenue
- Inset balconies on 23rd Avenue and E Union Street provide massing modulation
- Locate commercial & residential parking access on 22nd Avenue.



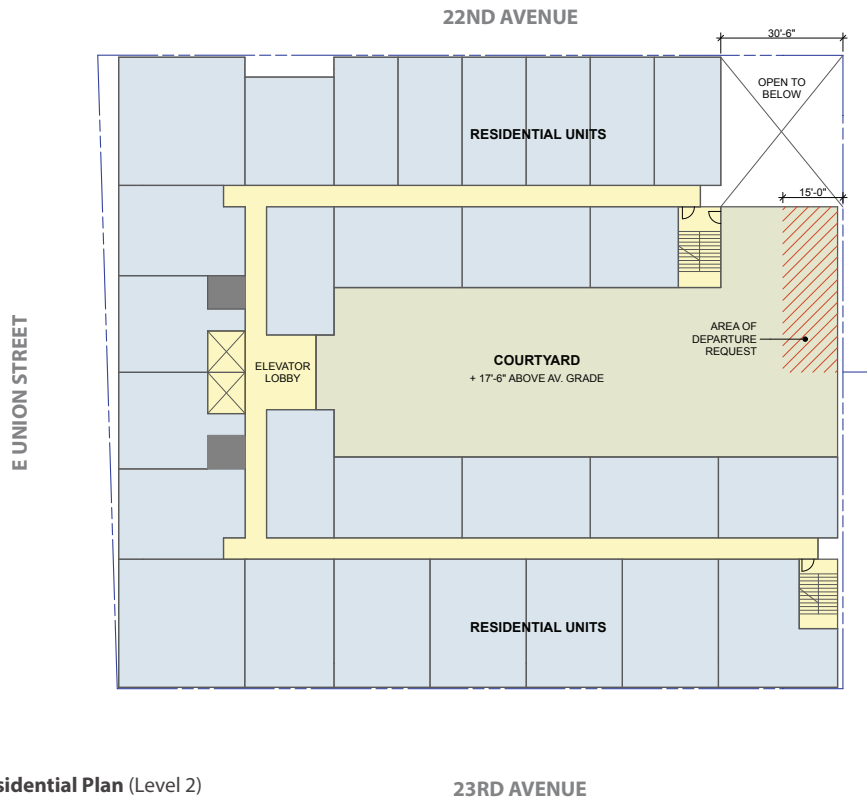
Northwest Corner- looking south along 22nd Ave



Southeast Corner - looking across 23rd Ave and E Union St intersection



Street Level Plan (Level 1)



Typical Residential Plan (Level 2)



The bullets points to the right summarize the guidance provided to the design team by the Board at the Early Design Guidance meeting on March 25, 2015. The guidance has been grouped into categories that appear on the right side of the following two-page spreads with a brief description of how the design has been developed in response to the Board's guidance.

1 Height, Bulk & Scale

- General support for Preferred Option, a U-shaped building massing with a second level courtyard opening to the north, and massing modulation provided by inset balconies along 23rd Avenue and E Union Street
- Board felt massing at northwest corner of the building did not fully address transition to single-family zone and requested the building's relationship to its neighbor on north property line be more clearly depicted
- Board supported the residential character along 22nd Avenue, but expressed concerns about building massing seeming "top heavy" above the townhouse units and noted the corner at E Union Street seemed "overly bulky"
- Board requested a four-season shadow study be provided to help assess the project's impact on the adjacent residential building

2 Streetscape on 23rd Avenue & E Union Street

- General support for the location of commercial spaces along 23rd Avenue and E Union Street
- Board requested studying and providing more overhead weather protection along 23rd Avenue and E Union Street
- Board expressed concern about the width of the residential lobby on E Union Street and emphasized the importance of an active streetscape. Noted that the activation of the street was a design issue

3 Streetscape on 22nd Avenue

- General support for the location of ground-related residential along 22nd Avenue
- Board emphasized the importance of the pedestrian environment on 22nd Avenue and the need to address the two parking entries and building massing (as noted under #1)

4 Materials

- Board stated high quality materials and detailing were important to establish a precedent for future development in the neighborhood

5 Parking, Loading & Trash Staging

- Board was supportive of commercial loading in concept, but requested more information be provided to understand how loading space would actually work and to better understand potential impacts on neighbors
- Board felt both commercial and residential garbage and recycling staging on 22nd Avenue needed study

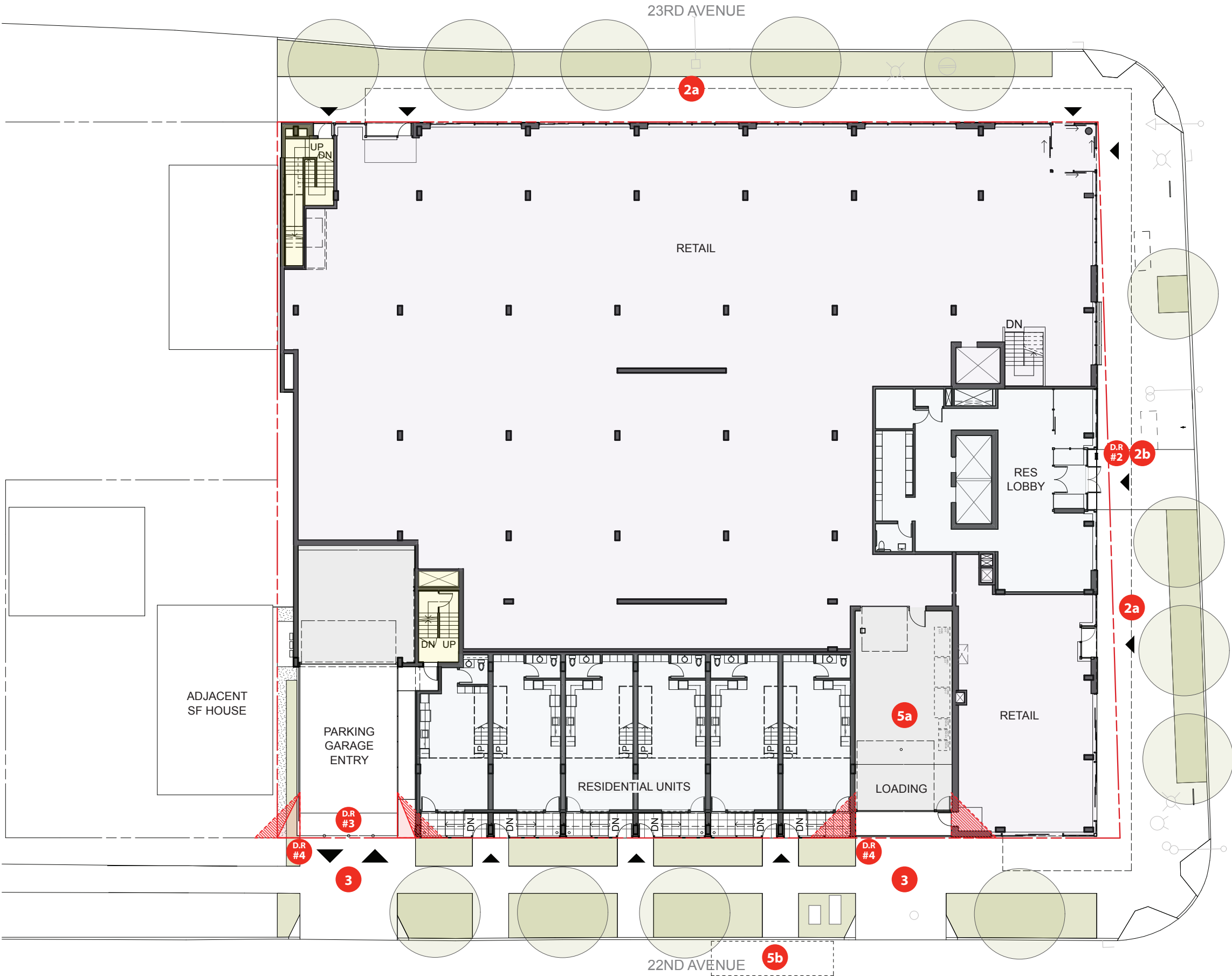
6 Landscaping

- Board endorsed second level courtyard, but wanted to better understand its design and spatial characteristics
- Board requested more information for rooftop terrace



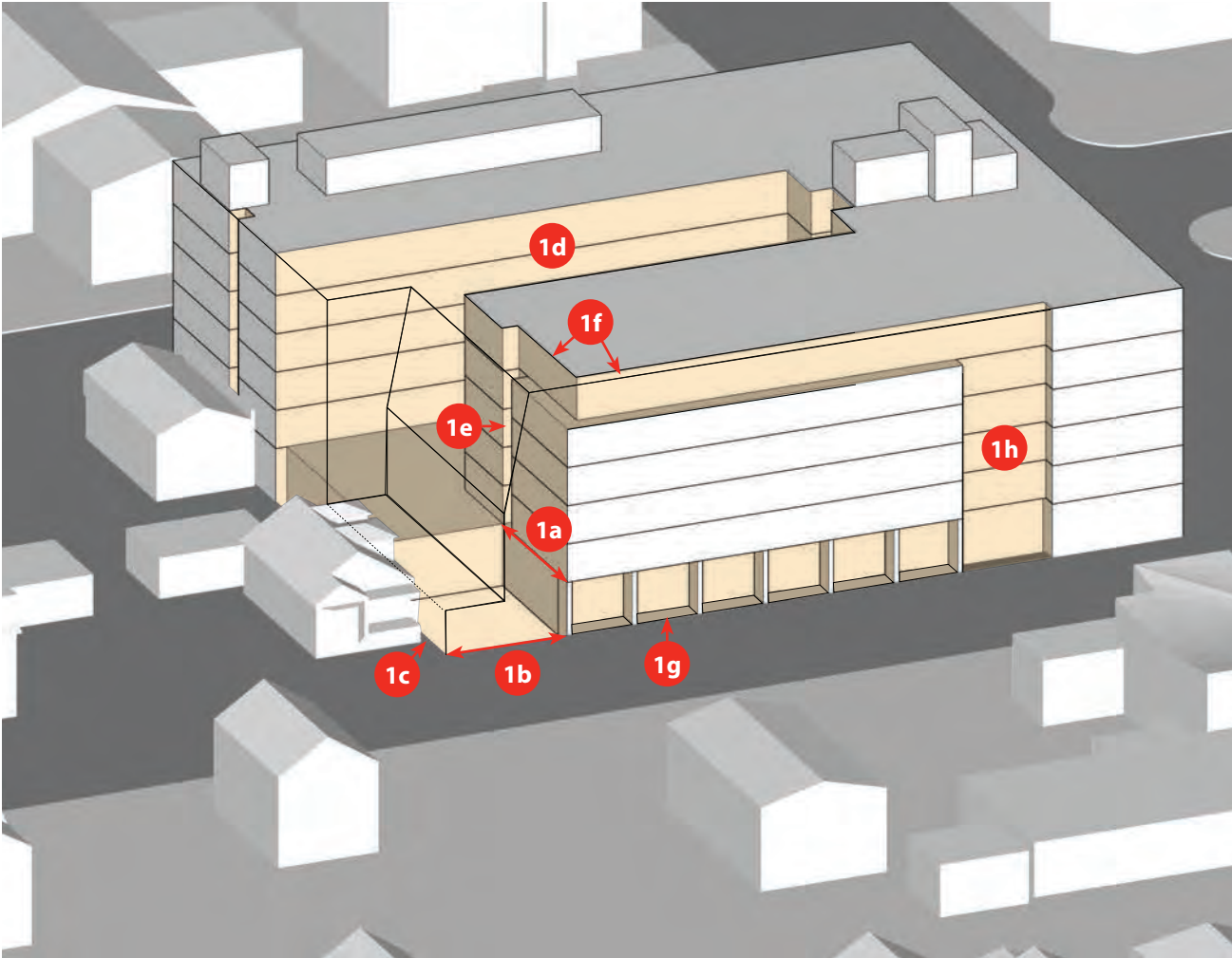
- CS2 A1 Sense of Place
- CS2 B1 Site Characteristics
- CS2 B2 Connection to Street
- PL1 A2 Adding to Public Life
- PL3 C3 Retail Edges, Ancillary Activities

STREET LEVEL FLOOR PLAN

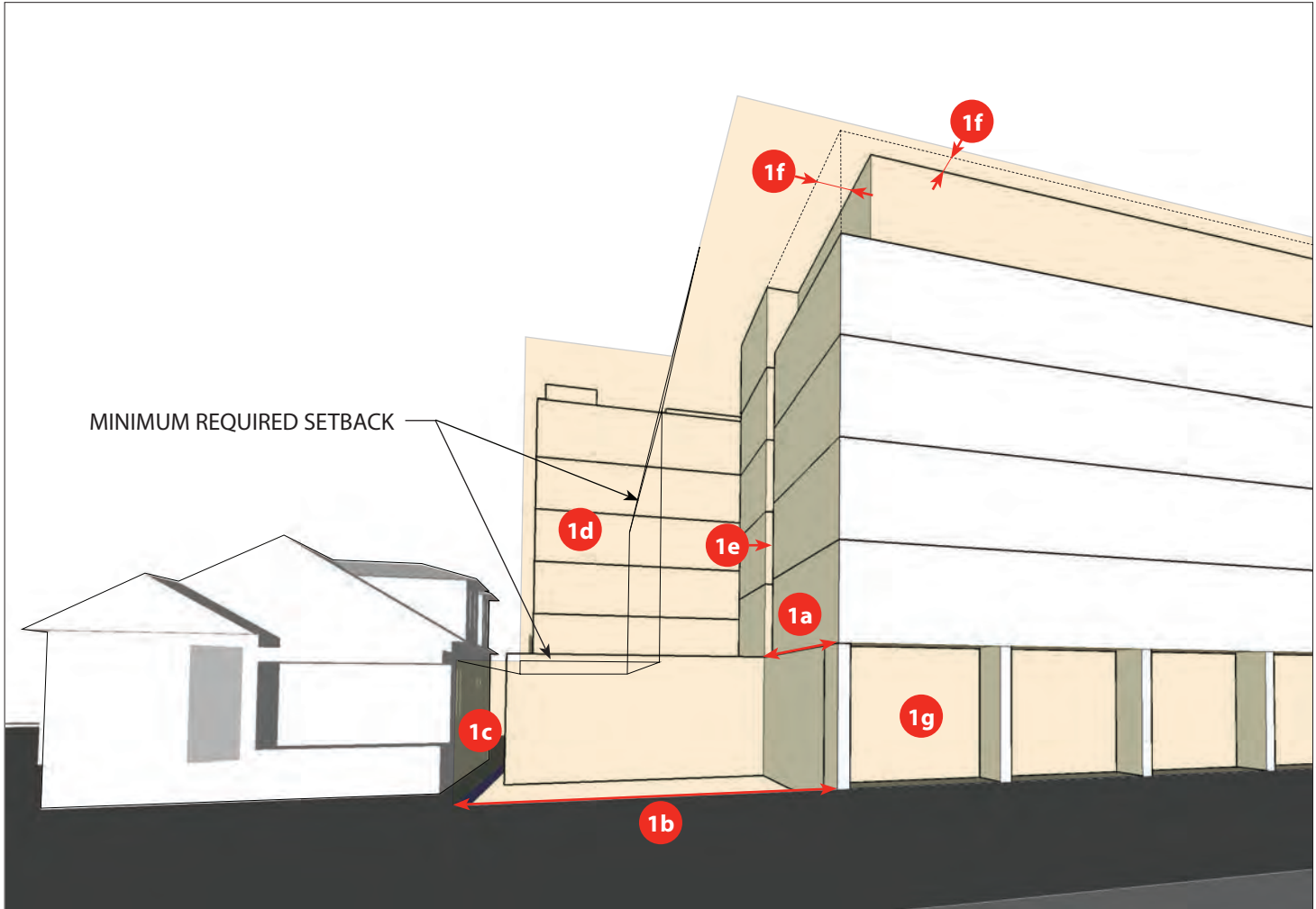


- CS2 A1 Sense of Place
 - CS2 A2 Architectural Presence
 - CS2 B1 Site Characteristics
 - CS2 B2 Connection to Street
 - CS2 B3 Character of Open Space
 - CS2 C1 Corner Sites
 - CS2 D3 Zone Transitions
 - CS2 D4 Massing Choices
 - CS2 D5 Respect for Adjacent Sites
 - PL1 A2 Adding to Public Life
 - PL3 C3 Retail Edges, Ancillary Activities
 - PL4 A1 Serving All Modes of Travel
 - DC1 B1 Access Location & Design
 - DC1 C1 Below-grade Parking
 - DC1 C4 Service Uses
- Height, Bulk & Scale**
- 2a Continuous overhead weather protection along 23rd Avenue and E Union Street
 - 2b Recessed vestibule & overhead signage at main entry is flanked by active uses
- Streetscape on 23rd Ave & E Union St**
- 3 Previously proposed two parking entries consolidated to one parking entry
- Parking, Loading & Trash Storage**
- 5a Commercial loading and trash storage separated from parking
 - 5b Trash staging on 22nd Avenue adjacent to commercial loading curb cut
- D.R. #2 Departure Request #2, see p.46
- D.R. #3 Departure Request #3, see p.48
- D.R. #4 Departure Request #4, see p.48

HEIGHT BULK & SCALE: SINGLE FAMILY ZONE EDGE



PERSPECTIVE VIEW OF ZONE EDGE TREATMENT (ALLOWABLE ZONING ENVELOPE SHOWN SHADED)



DETAIL PERSPECTIVE VIEW OF ZONE EDGE TREATMENT (ALLOWABLE ZONING ENVELOPE SHOWN SHADED)

The subject property borders NC2 zoning on all sides, except for the western 79 feet of the north property line, which abuts SF 5000 zoning. The SF-zoned property to the north is developed with a 1½ story single family residence that sits just one foot from the north property line of the rezone site (the house being nonconforming to single family side yard setbacks.)

In the case of development in an NC zone adjacent to a residential zone, the Land Use Code requires tiered setbacks. The first 13-feet of structure requires no setback; between 13 and 40-feet, a 15-foot setback is required; above 40-feet, an additional 2-feet of setback is required for every 10-feet above 40-feet (resulting in a 20-foot setback at 65-feet).

Given that the existing single family home sits so close to the north property line, the Code-

required setbacks would provide a greater sense of encroachment as no setbacks are required for the first 13-feet of building height. This Code-compliant setback would put building structure on the north property line very close to the windows in the single family home. The Code-required setbacks provide bulk relief above the-13 feet, but the majority of the rezone building mass would only be-15 feet from the single family dwelling and zone.

To address the unique circumstances of the rezone site and single family dwelling to the north, a more sensitive building design is proposed as zone edge transition:

- At the ground level, a 5-foot landscaped area and screening would be provided as a buffer for the single family dwelling to the north.

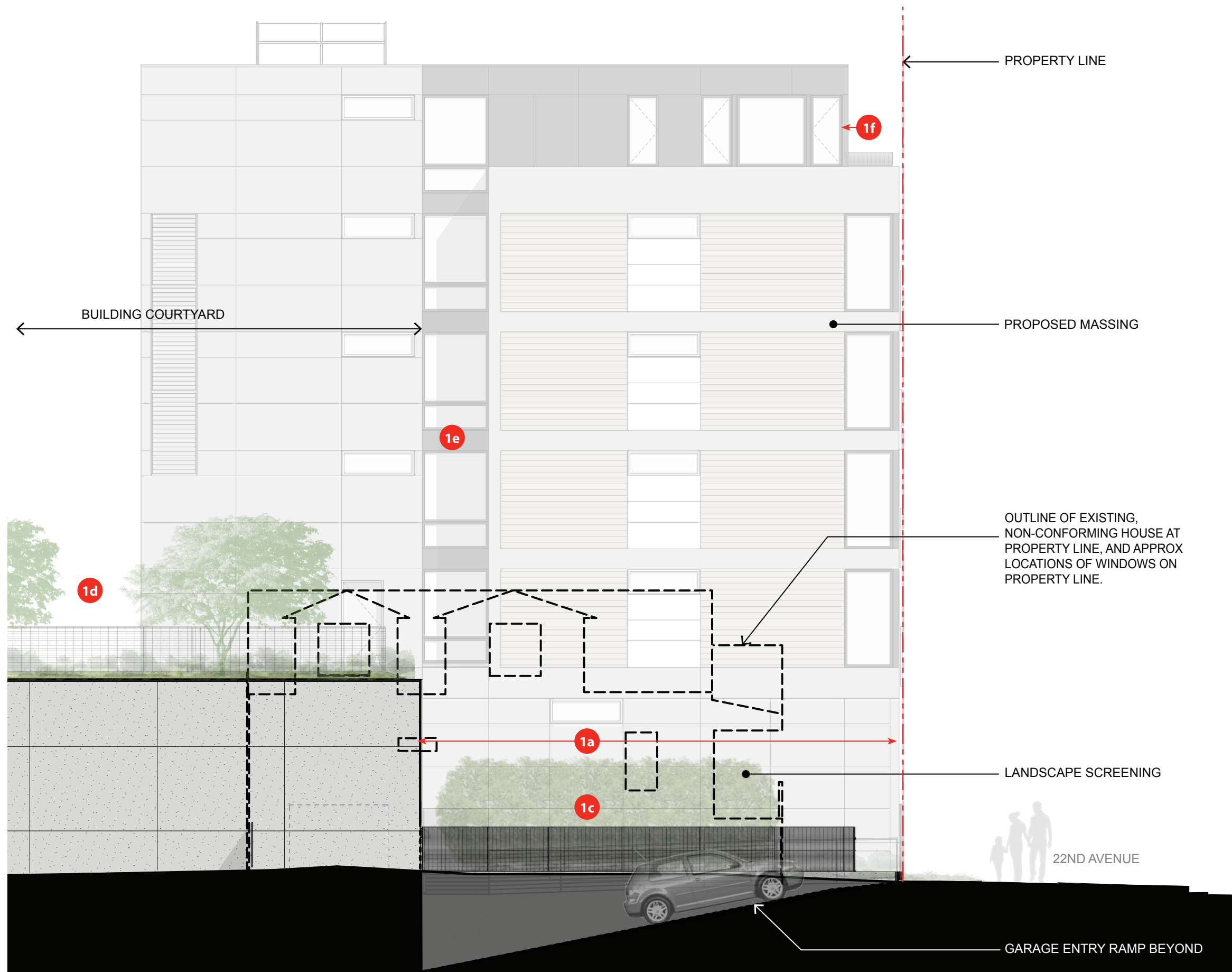
- The entire building—from the ground level through the fifth floor—is set back 30 feet-6 inches from the north property line, for the westerly 38 feet of the building. This is a 100% increase over the Code-required (15-foot) setback for the 13 to 40-foot portion of the building, and is a 55% increase over the Code-required (20-foot) setback for the 40 to 50-foot portion of the building. (An even greater setback is provided in this location for the sixth floor; see below.)
- For the westerly 38-feet of the building, the sixth floor is set back another four feet from the primary façade, for a total of 34 feet-6 inches from the north property line. This provides additional bulk and massing relief to the single family dwelling
- To the east of the single family dwelling is a carport structure with a substantial setback from

the property line. In this area (the easterly 41 feet of the building abutting the SF zone), the proposed building is built to the property line and is about 16 feet-8 inches in height. Above that height, the proposed building sets back 30 feet-6 inches.

- The building’s courtyard is oriented toward the north. This substantially reduces building bulk and massing to the north as the building form is divided into two wings.
- The northern building façade adjacent to the single-family zone includes a careful selection of exterior materials to soften the sense of massing.

All of these features together create a transition in zoning heights that is appropriate to the particular characteristics of this rezone site and its neighbors to the north.





- CS2 D3** Zone Transitions
- CS2 D4** Massing Choices
- CS2 D5** Respect fo Adjacent Sites

Height, Bulk & Scale

- 1a** Massing set back from front lot line in response to neighbor's windows and proximity to side lot line
- 1b** Large setback provided from side lot line to provide buffer to neighboring house
- 1c** Planted 5-foot buffer between proposed project and neighboring house
- 1d** Courtyard aligns with house and rear yards of neighbors to the north to preserve daylight access and visual relief
- 1e** Notch provides massing relief and organizes building into smaller scale elements
- 1f** Top floor set back additional 4' to improve daylight access and reduce perceived mass
- 1g** Porches at street-level residential provide help to diminish massing
- 1h** 22nd Avenue facade organized into smaller scale elements to transition to residential zone to the north of the project site

PARTIAL NORTH ELEVATION WITH WINDOW MAPPING OF THE ADJACENT HOUSE



COURTYARD LEVEL FLOOR PLAN



- CS2 B2 Daylight & Shading
- CS2 B3 Character of Open Space
- CS2 D3 Zone Transitions
- CS2 D4 Massing Choices

Height, Bulk & Scale

- 1a Massing set back from front lot line in response to neighbor's windows and proximity to side lot line
- 1b Large setback provided from side lot line to provide buffer from neighboring house
- 1c Planted 5-foot buffer between proposed project and neighboring house
- 1f Top floor set back additional 4' to improve daylight access and reduce perceived mass
- 1g Porches at street-level residential provide help to diminish massing
- 1i Lighter color palette serves to diminish mass

Note: 22nd Avenue right-of-way is 70-feet wide, separating northwest portion of site from the single family zone located across the street to the northwest of the site

Streetscape on 22nd Ave

- 3 Fin walls added at street-level residential units to ground massing

Materials

- 4 22nd Ave palette is wood siding, metal trim, painted fiber cement board & windows

Landscaping

- 6 Second level courtyard provides mix of individual and shared spaces

D.R. #1 Departure Request #1, see p.42



BUILDING SECTION FACING NORTH

- CS2 B2 Daylight & Shading
- CS2 B3 Character of Open Space
- CS2 D3 Zone Transitions
- CS2 D4 Massing Choices

Height, Bulk & Scale

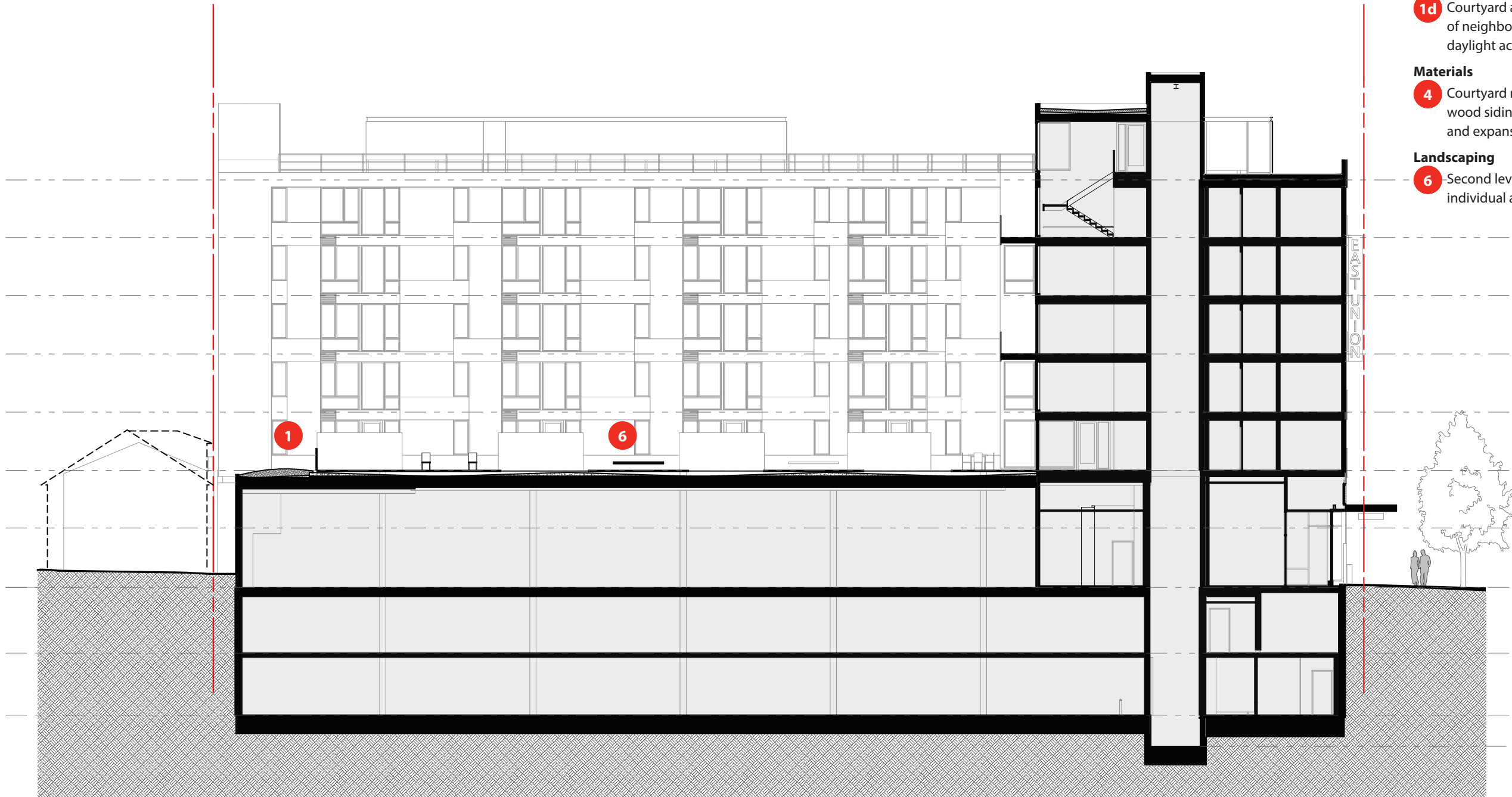
- 1d Courtyard aligns with house and rear yards of neighbors to the north to preserve daylight access and visual relief

Materials

- 4 Courtyard material palette consists of wood siding, painted fiber cement board, and expansive windows

Landscaping

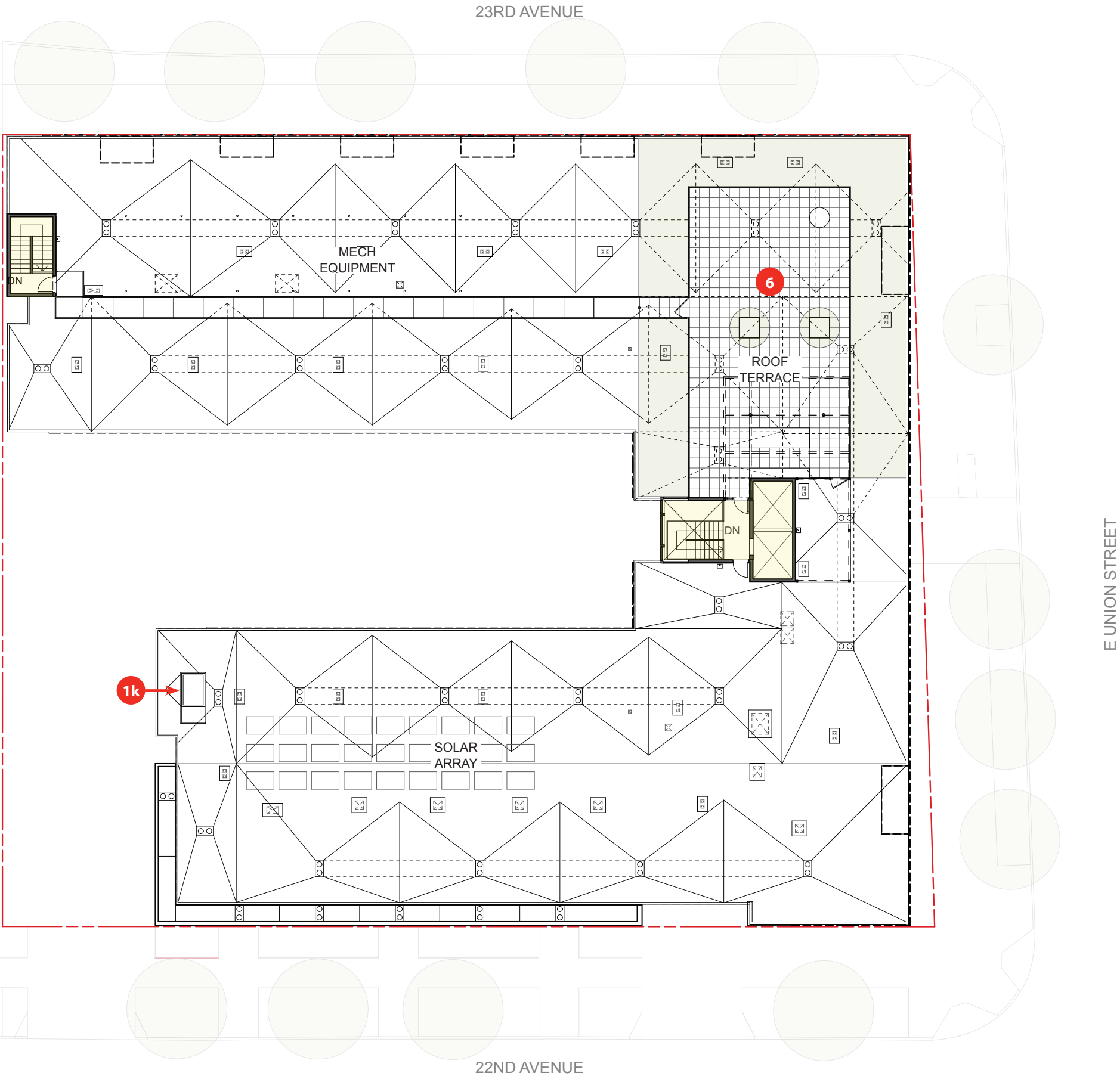
- 6 Second level courtyard provides mix of individual and shared spaces



TYPICAL RESIDENTIAL LEVEL FLOOR PLAN



ROOF TERRACE PLAN



- CS2 B2 Daylight & Shading
 - CS2 B3 Character of Open Space
 - CS2 D3 Zone Transitions
 - CS2 D4 Massing Choices
- Height, Bulk & Scale**
- 1f Top floor set back additional 4' to improve daylight access and reduce perceived mass
 - 1j "Notch" at and above loading area separates material palettes
 - 1k A roof hatch in lieu of stair penthouse reduces bulk adjacent to single family zone
- Landscaping**
- 6 Roof terrace amenity space located away from single family zone and toward views to east and southeast



- CS2 A1** Sense of Place
- CS2 A2** Architectural Presence
- CS2 B2** Connection to Street
- CS2 C1** Corner Sites
- PL1 A2** Adding to Public Life
- PL3 C3** Retail Edges, Ancillary Activities

BUILDING SECTION FACING WEST

CS2
B1

Site Characteristics

CS2
B2

Connection to Street

CS2
B3

Character of Open Space

CS2
D4

Massing Choices

PL1
A2

Adding to Public Life

PL3
B2

Ground-level Residential

PL3
C3

Retail Edges, Ancillary Activities

Streetscape on 23rd Ave & E Union St

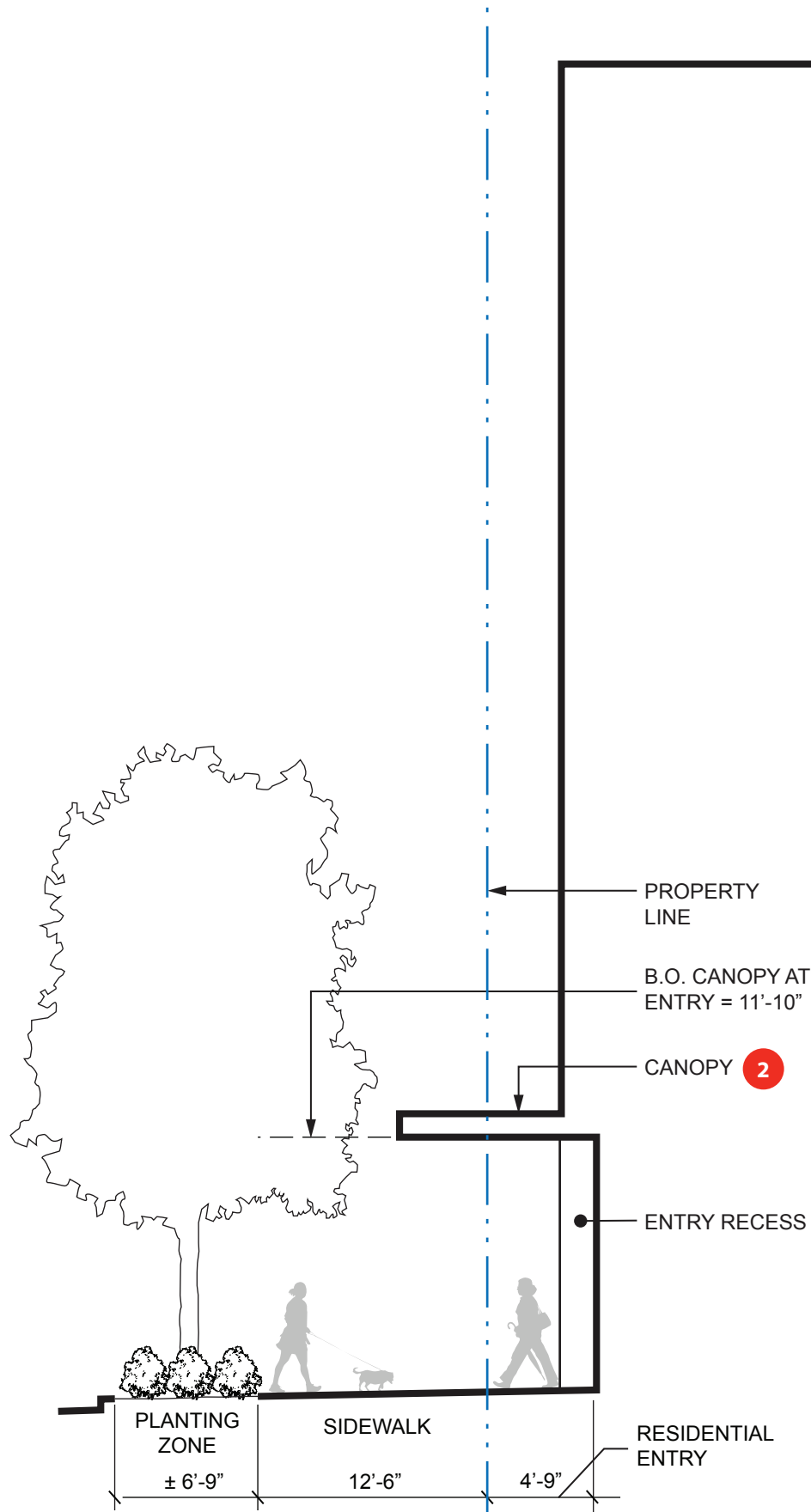
2

Continuous overhead weather protection along 23rd Avenue and E Union Street

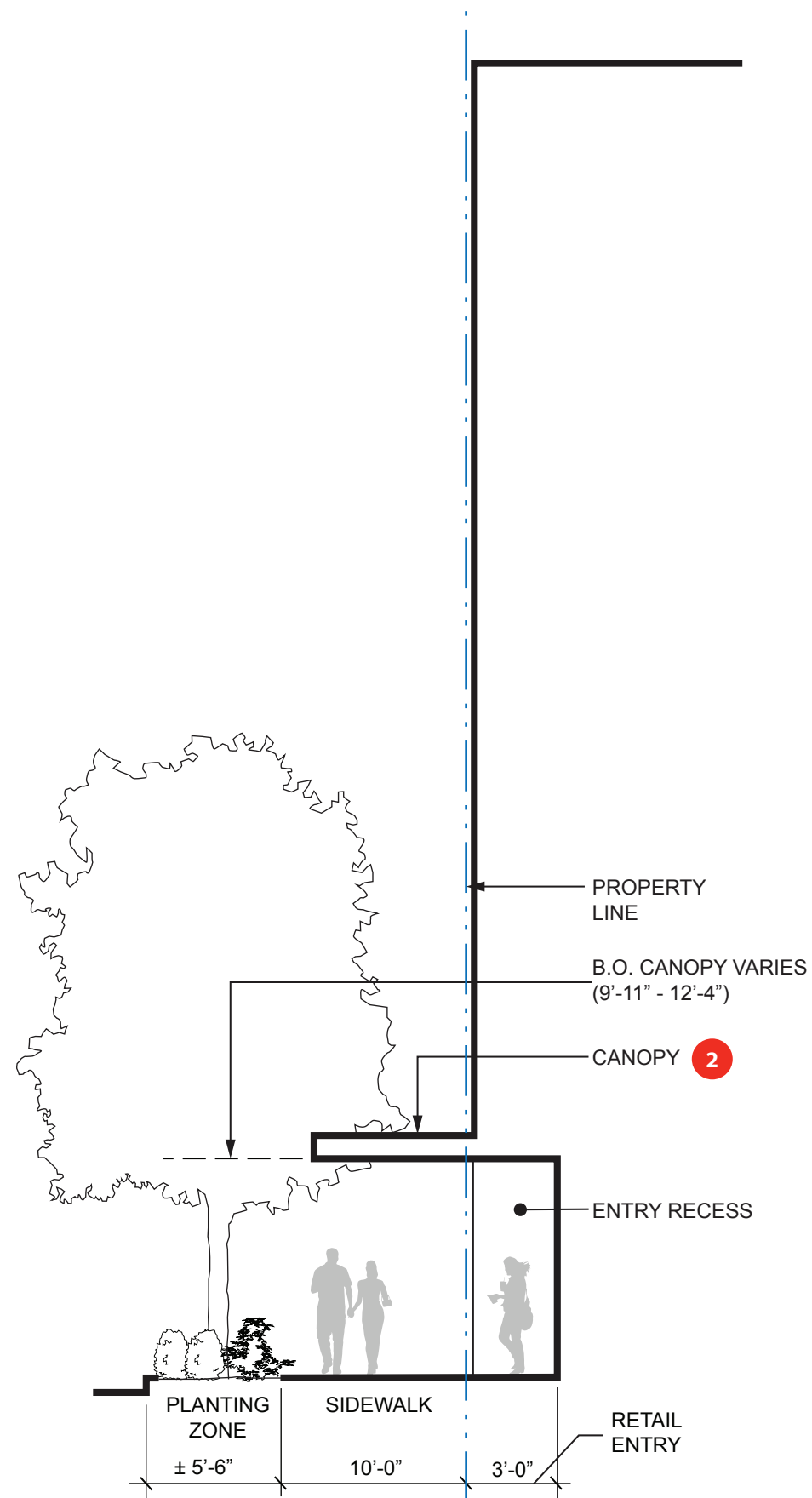
Materials

4

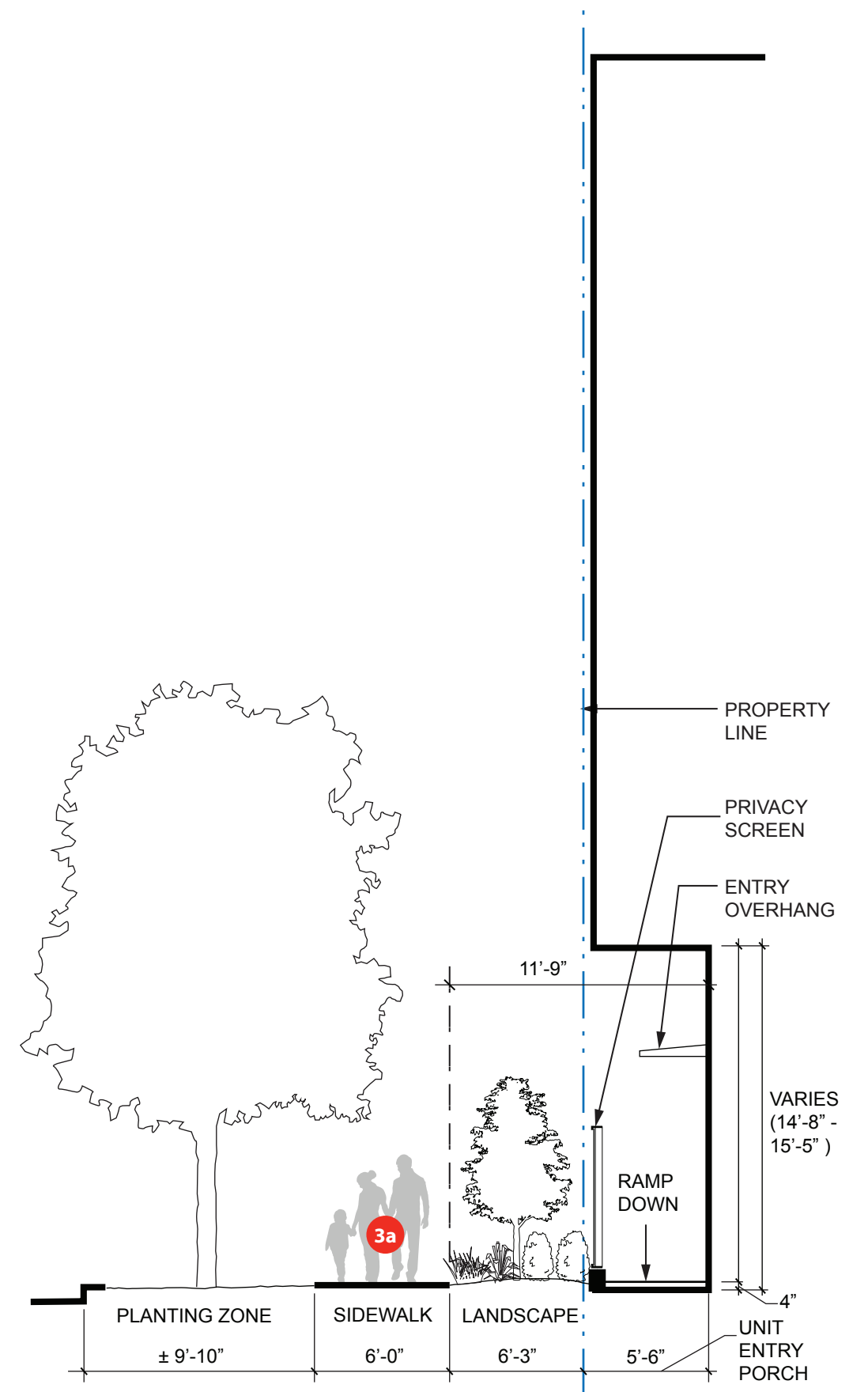
23rd Ave & E Union St palette uses wood siding, metal trims, painted fiber cement board & large windows



STREET SECTION AT E UNION STREET

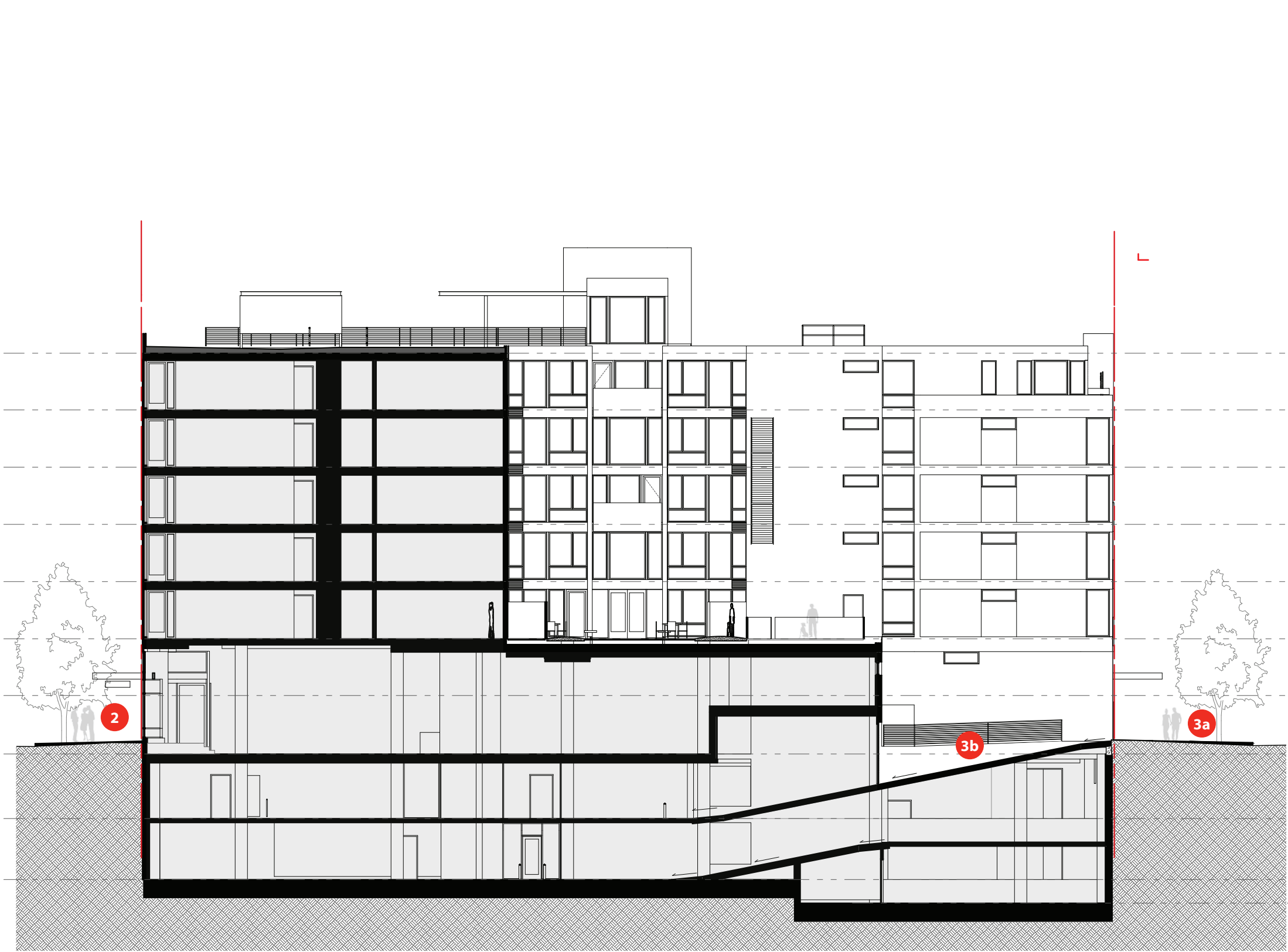


STREET SECTION AT 23RD AVENUE



STREET SECTION AT 22ND AVENUE

BUILDING SECTION THRU GARAGE ENTRY



- CS2 B1 Site Characteristics
 - CS2 B2 Connection to Street
 - CS2 D4 Massing Choices
 - PL3 C3 Retail Edges, Ancillary Activities
 - DC1 B1 Access Location & Design
- Streetscape on 23rd Ave & E Union St**
- 2 Continuous overhead weather protection along 23rd Avenue and E Union Street
- Streetscape on 22nd Ave**
- 3a Lushly landscaped R.O.W on 22nd Ave with plantings at both sides of sidewalk
 - 3b Setback at parking entry location improves visibility to sidewalk



- CS2 A1** Sense of Place
- CS2 A2** Architectural Presence
- CS2 B1** Site Characteristics
- CS2 B2** Connection to Street
- CS2 C1** Corner Sites
- PL1 A2** Adding to Public Life
- PL3 C3** Retail Edges, Ancillary Activities

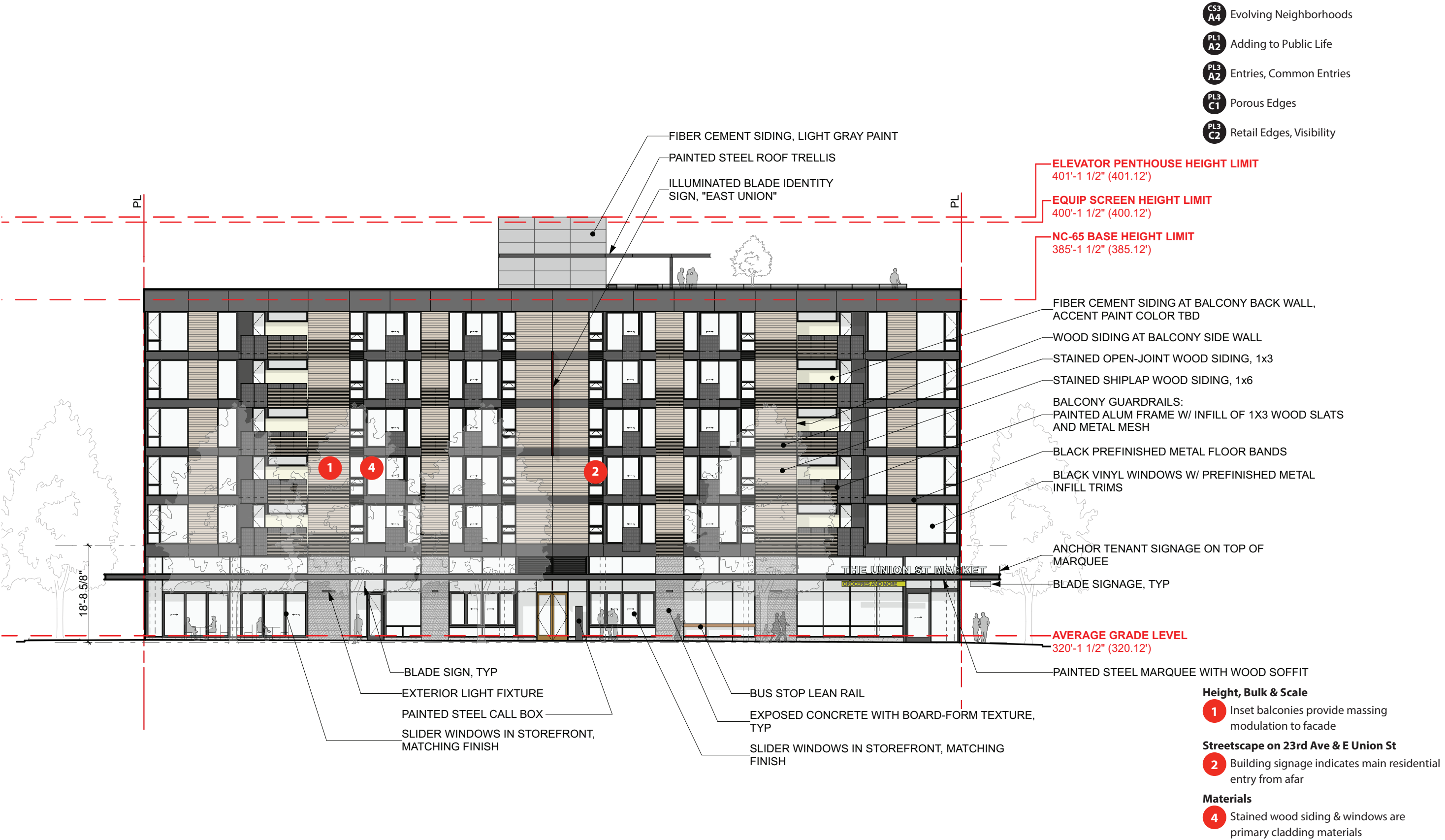
EAST ELEVATION (23RD AVENUE)





- CS2 A1 **Corner of Place**
- CS2 A2 **Architectural Presence**
- CS2 B2 **Connection to Street**
- CS2 C1 **Corner Sites**
- PL1 A2 **Adding to Suburb Life**
- PL3 C3 **Retail Edges, Ancillary Activities**

SOUTH ELEVATION (E UNION STREET)

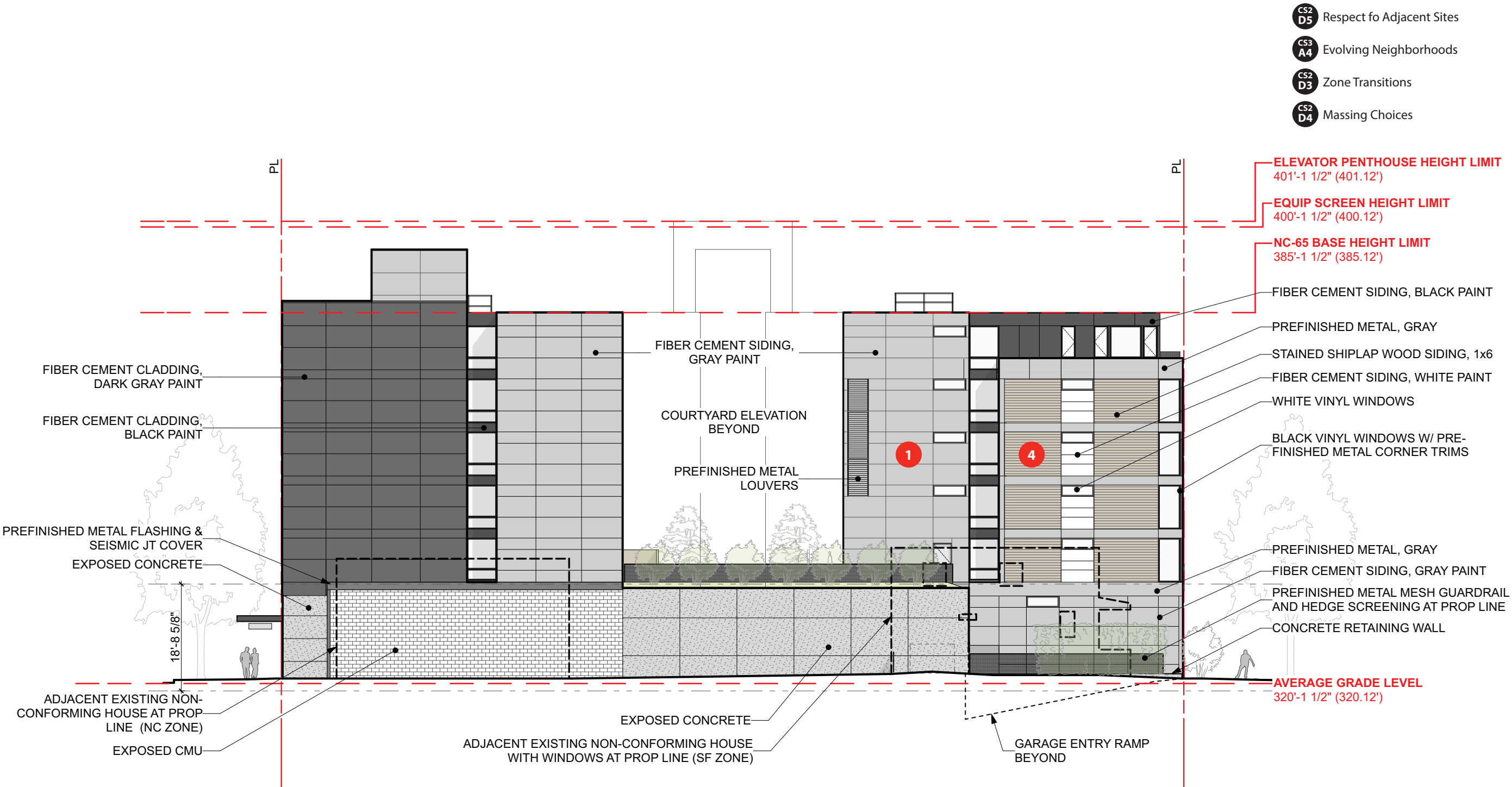


WEST ELEVATION (22ND AVENUE)



- CS2 B1 Site Characteristics
- CS2 B2 Connection to Street
- PL1 A2 Adding to Public Life
- PL3 B2 Ground-level Residential

NORTH ELEVATION (PROPERTY LINE)

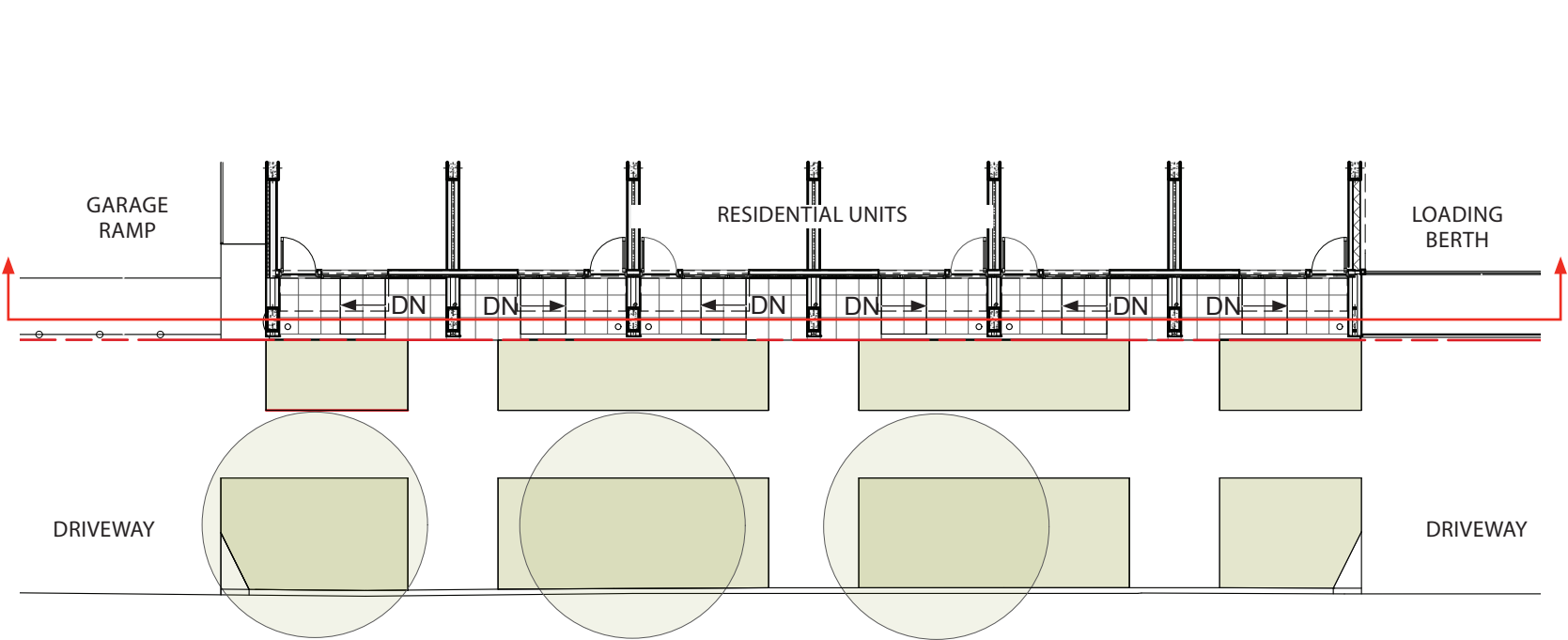


- Height, Bulk & Scale**
- 1** Lighter color palette serves to diminish mass and reflect light
- Materials**
- 4** Stained wood siding & windows are primary cladding materials



- CS2 B1 Site Characteristics
- CS2 B2 Connection to Street
- PL3 B2 Ground-level Residential

DETAIL WEST ELEVATION (22ND AVENUE)



PARTIAL STREET LEVEL PLAN: 22ND AVENUE MULTISTORY UNITS

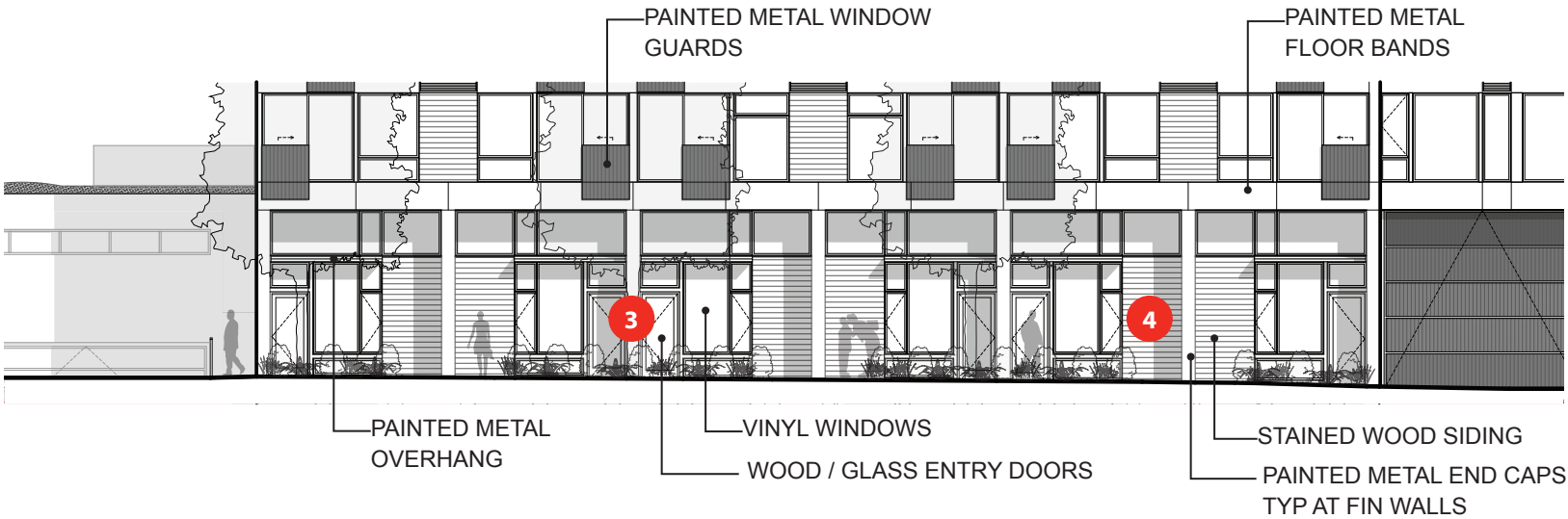
- CS2 B2 Connection to Street
- PL1 A2 Adding to Public Life
- PL3 A3 Individual Entries
- PL3 B2 Ground-level Residential
- DC2 D1 Human Scale

Streetscape on 22nd Ave

- 3 Fin walls added at street-level residential units to ground massing

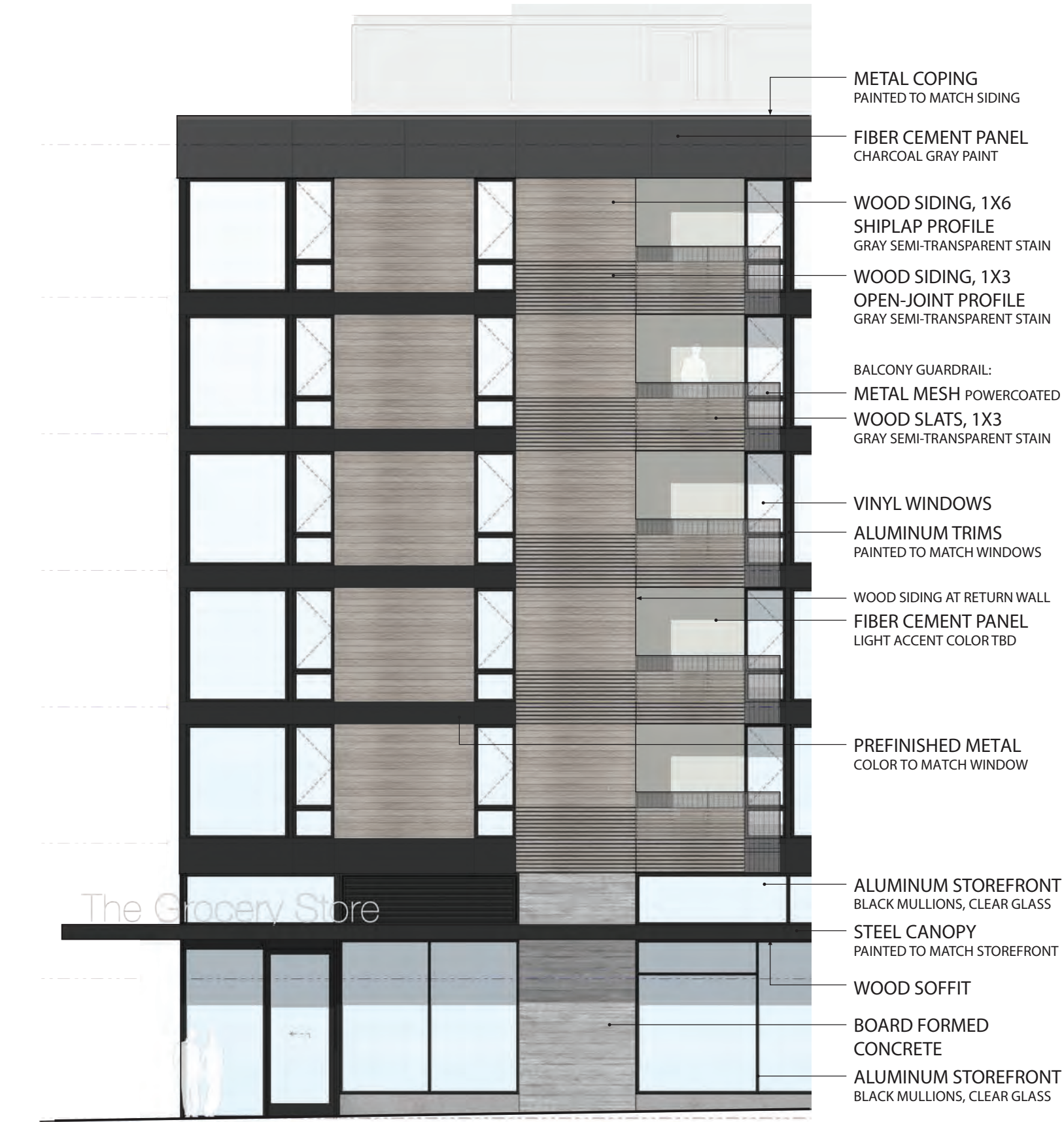
Materials

- 4 Stained wood siding & windows are primary cladding materials

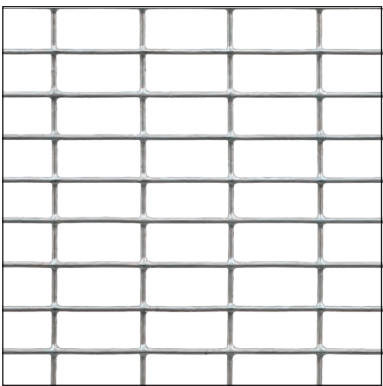


22ND AVENUE UNIT ENTRIES BEHIND PRIVACY SCREENS

MATERIAL PALETTE: 23RD AVENUE & E UNION STREET



PARTIAL ELEVATION DETAIL: 23RD AVENUE



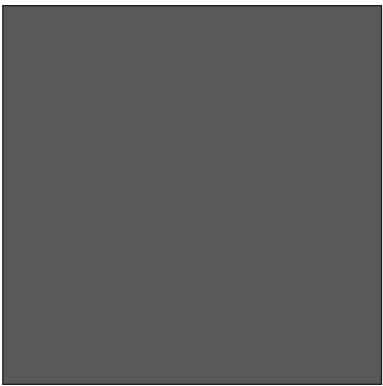
METAL MESH, POWDER COATED



WOOD SIDING, 1X3 & 1X6



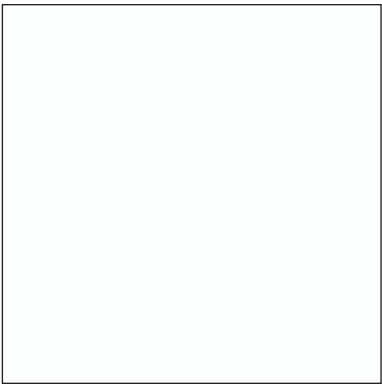
BOARD FORMED CONCRETE



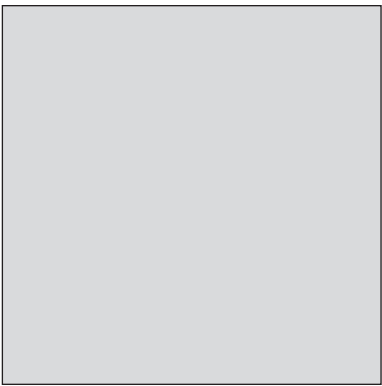
CHARCOAL GRAY PAINT



WOOD ENTRY DOOR

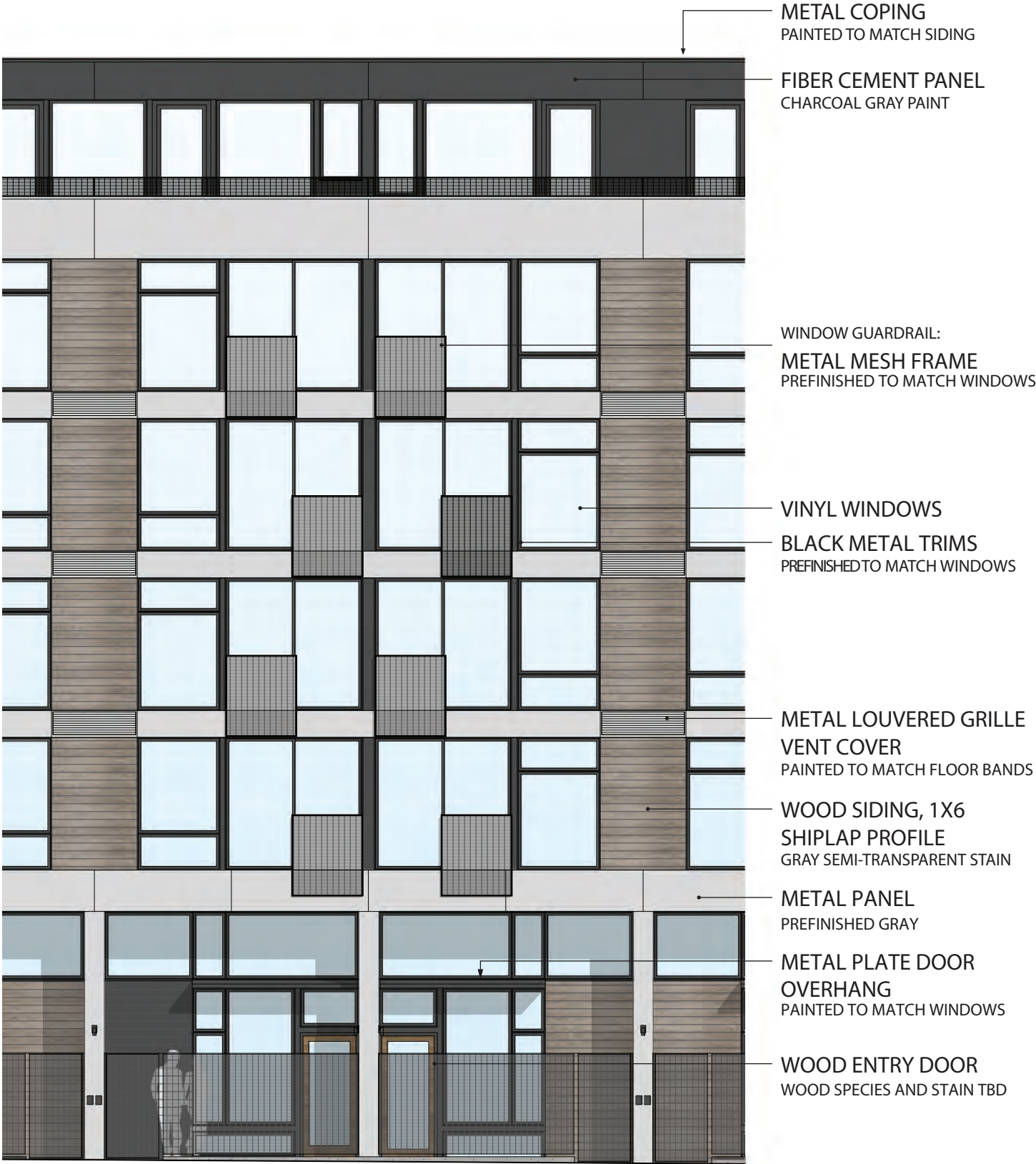


WHITE PAINT



GRAY PAINT

MATERIAL PALETTE: 22ND AVENUE & COURTYARD

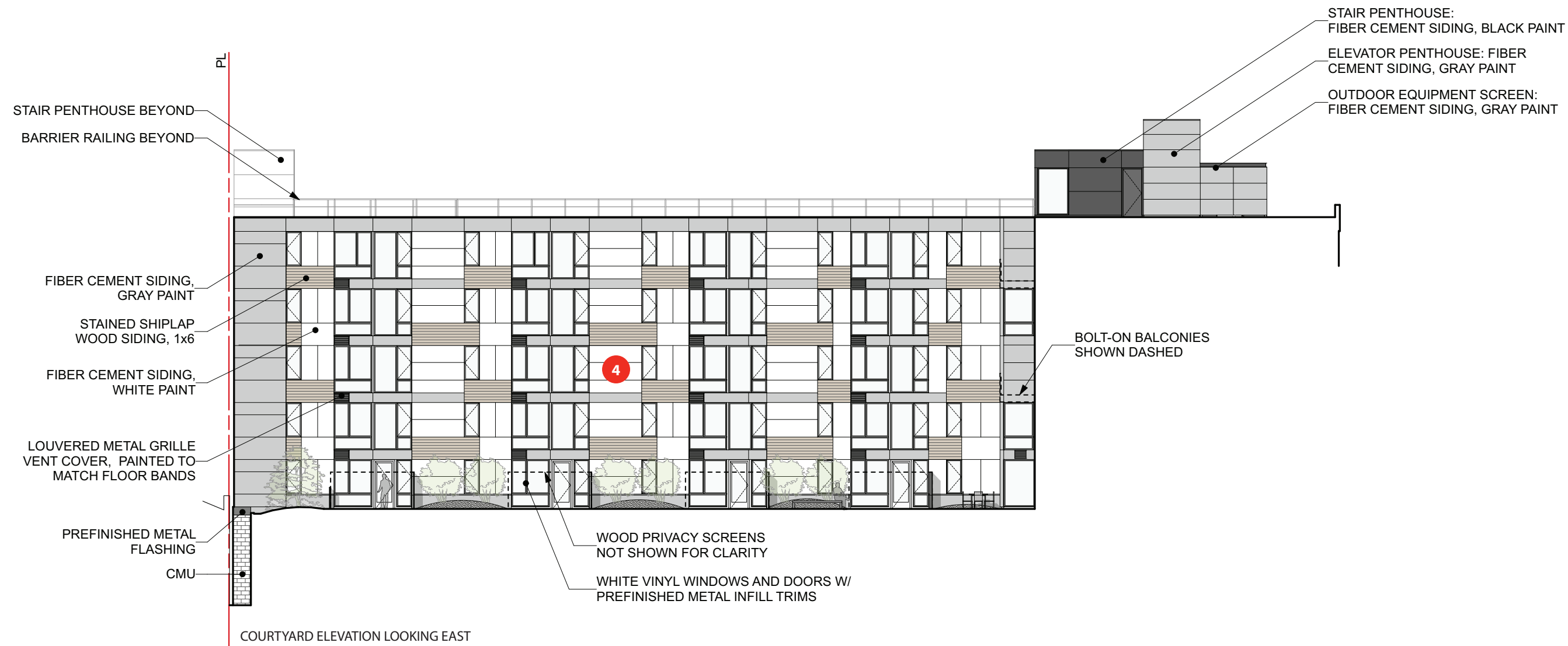


PARTIAL ELEVATION DETAIL: 22ND AVENUE



PARTIAL ELEVATION DETAIL: COURTYARD

COURTYARD ELEVATION SOUTH & EAST

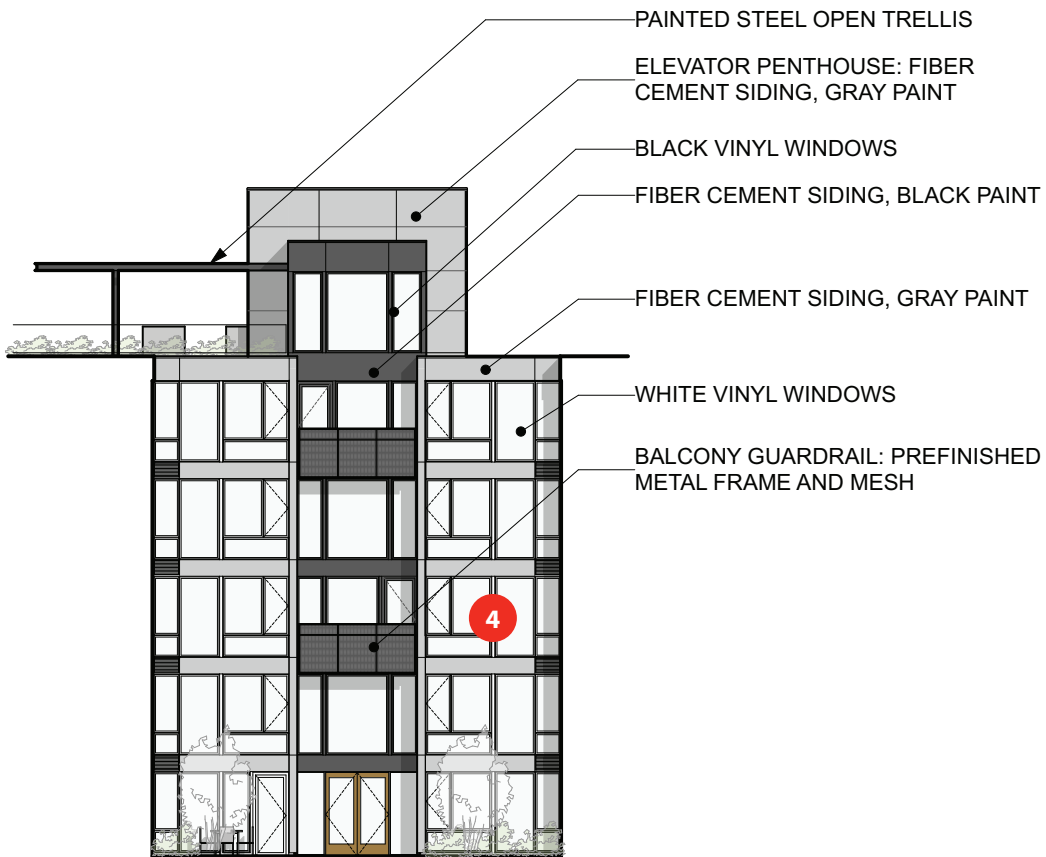


CS2 B3 Character of Open Space

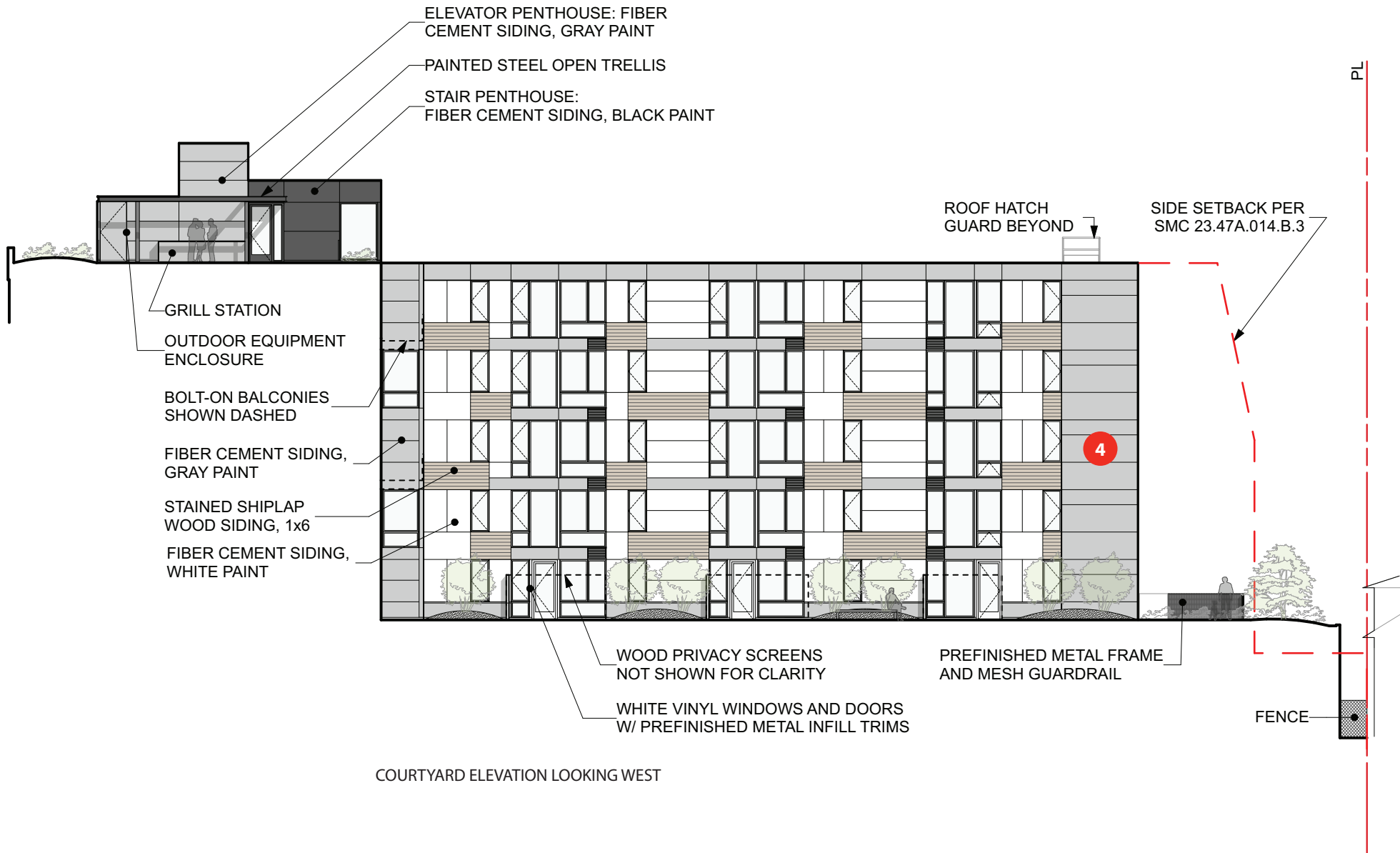
DC2 D1 Human Scale

COURTYARD ELEVATION WEST

- CS2 B3 Character of Open Space
- DC2 D1 Human Scale



COURTYARD ELEVATION LOOKING SOUTH



COURTYARD ELEVATION LOOKING WEST

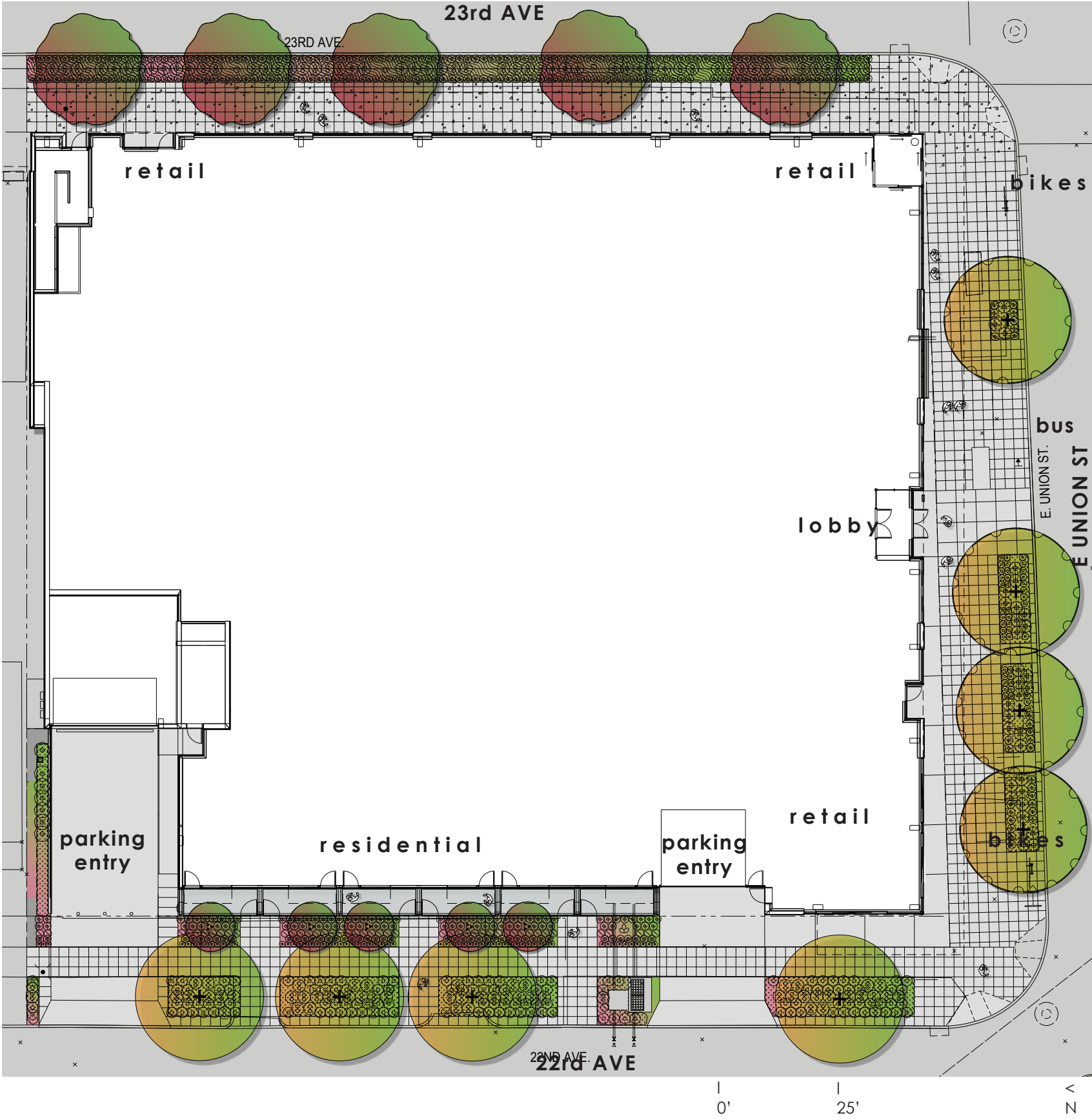
Materials
4 Stained wood siding & windows are primary cladding materials

STREET LEVEL LANDSCAPE PLAN

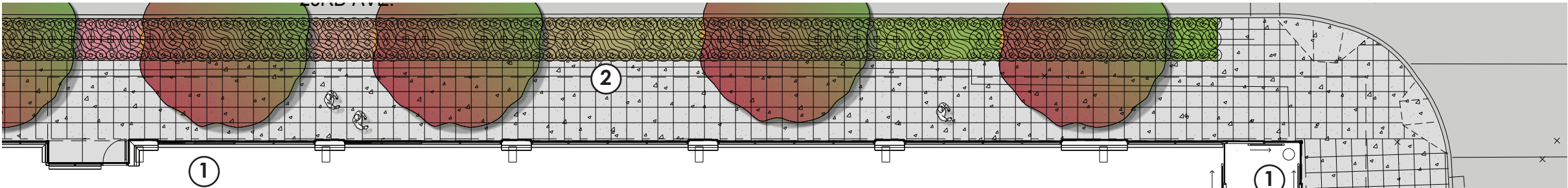
ROW CONCRETE PAVING:
1. PER COS STD. PLAN 420 W/ THE
FOLLOWING EXCEPTION: SAND COATED
THROUGH JTS.

2' X 2' SCORING
W/ SAWCUT JTS.

4' X 4' SCORING
W/ SAWCUT JTS.



STREET LEVEL DETAIL



23rd Avenue East

- 23rd Avenue East**
1 Retail entry
2 Lush Streetscape

- East Union Street**
3 Bus Stop location
4 Residential entry
5 Outdoor dining

- 22nd Avenue East**
6 Residential entries
7 Generous planting strip
8 Landscape screening



1 Retail entry

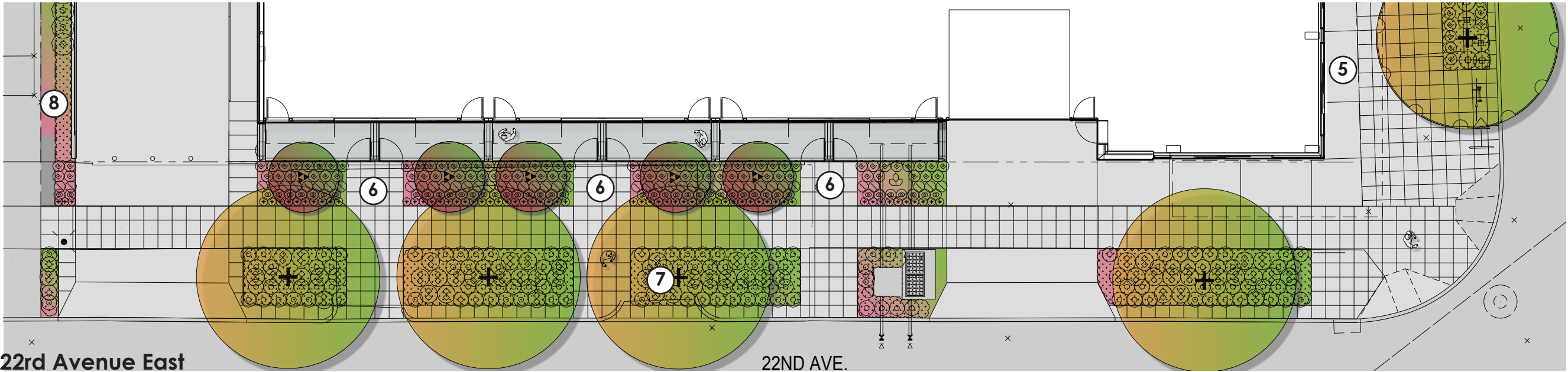


5 Outdoor Dining



7 Generous planting strip

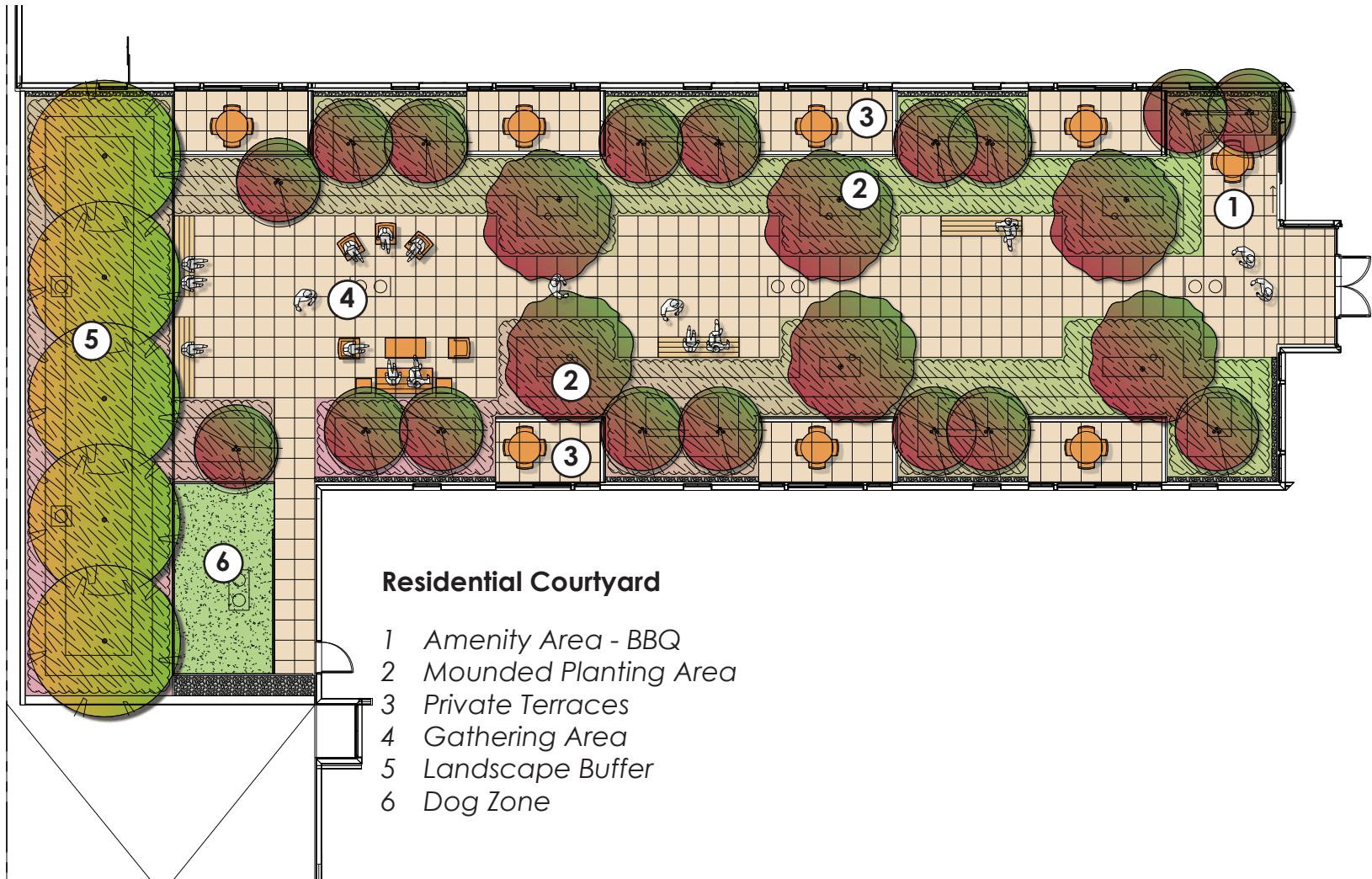
East Union Street



22nd Avenue East

22ND AVE.

PODIUM LEVEL DETAIL



Residential Courtyard

- 1 Amenity Area - BBQ
- 2 Mounded Planting Area
- 3 Private Terraces
- 4 Gathering Area
- 5 Landscape Buffer
- 6 Dog Zone

MATERIALS & FINISHES 2ND FLOOR & ROOF

SYMBOL	DESCRIPTION
	2' X 2' PRECAST PAVERS ON PEDESTALS ABBOTSFORD CONCRETE PRODUCTS 800.663.4091, PAVER COLOR/FINISH: TEXADA-NATURAL, PEDESTAL SYSTEM: APPIAN WAY BY ABBOTSFORD
	GREEN ROOF PLANTING: AVRS TRAYS FROM COLUMBIA GREEN TECHNOLOGIES W/ 5.25" MIN. PLANTING SOIL 503-683-9123 SEDUM TILE PRE-VEGETATED MATS AVAILABLE FROM ETERA, CONTACT DAVID GILMORE 360.661.2767
	FIBERGLASS PLANTER: 48" X 48" X 36" HT., WILSHIRE BY TOURNESOL SITEWORKS OR APPROVED EQUAL. COLOR-BLACK OR TBD SET ON SHIMS OR PEDESTALS

	1-1/2" WASHED DRAIN ROCK, FLUSH WITH TOP OF ADJACENT PAVING/GREEN ROOF TRAY
	"ULTIMATE K9 TURF" (FOR DOG AREA)
	SITE FURNITURE BY OWNER
	GAS FIREPLACE TBD
	GAS BBQ TBD
	TREE UPLIGHT



1 Amenity Area - BBQ



2 Mounded Planting Area

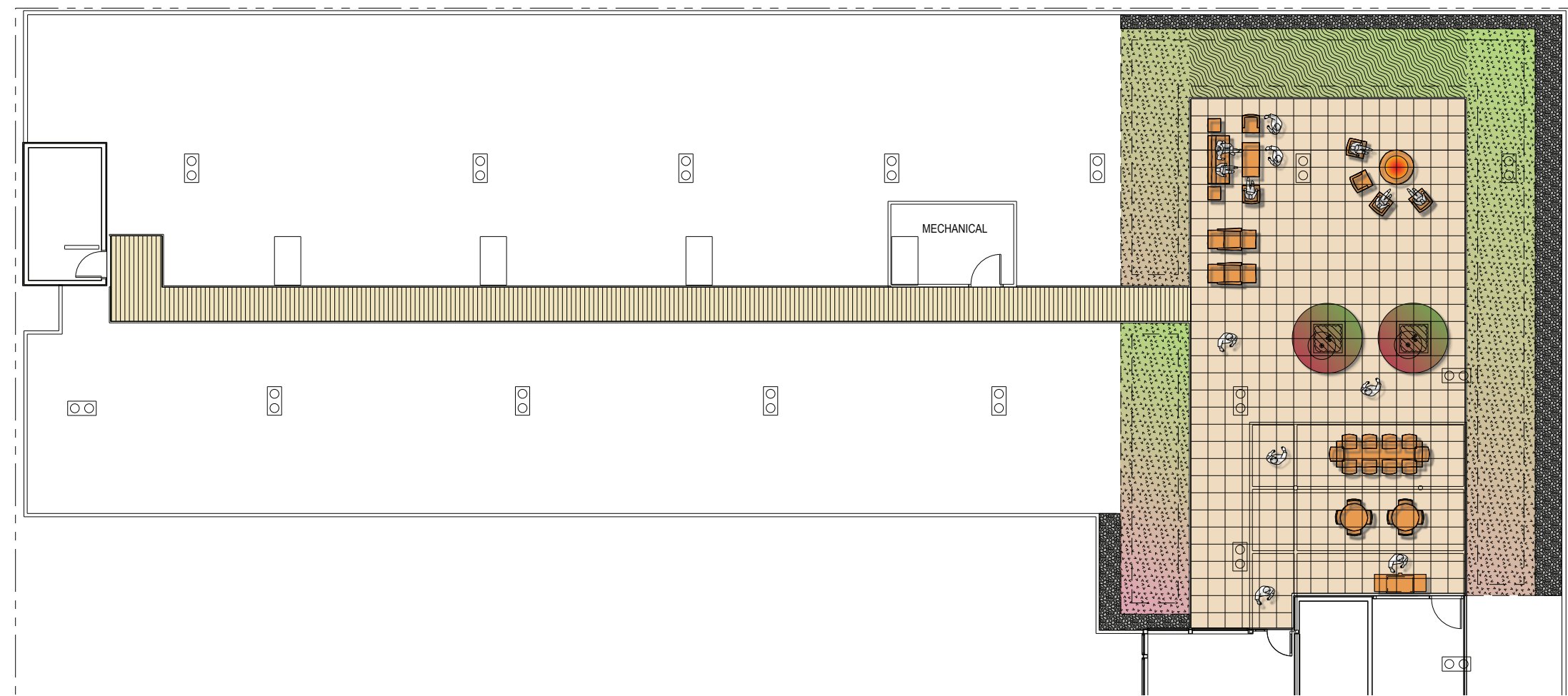


4 Gathering Area



6 Dog Zone

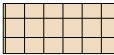
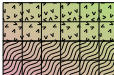

ROOF LEVEL DETAIL



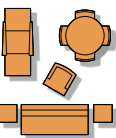





Mounded Bed with Sedums, Grasses



Fire and View

MATERIALS & FINISHES 2ND FLOOR & ROOF	
SYMBOL	DESCRIPTION
	2' X 2' PRECAST PAVERS ON PEDESTALS ABBOTSFORD CONCRETE PRODUCTS 800.663.4091, PAVER COLOR/FINISH: TEXADA-NATURAL, PEDESTAL SYSTEM: APPIAN WAY BY ABBOTSFORD
	GREEN ROOF PLANTING: AVRS TRAYS FROM COLUMBIA GREEN TECHNOLOGIES W/ 5.25" MIN. PLANTING SOIL 503-683-9123 SEDUM TILE PRE-VEGETATED MATS AVAILABLE FROM ETERA, CONTACT DAVID GILMORE 360.661.2767
	FIBERGLASS PLANTER: 48" X 48" X 36" HT., WILSHIRE BY TOURNESOL SITEWORKS OR APPROVED EQUAL. COLOR-BLACK OR TBD SET ON SHIMS OR PEDESTALS

	1-1/2" WASHED DRAIN ROCK, FLUSH WITH TOP OF ADJACENT PAVING/GREEN ROOF TRAY
	"ULTIMATE K9 TURF" (FOR DOG AREA)
	SITE FURNITURE BY OWNER
	GAS FIREPLACE TBD
	GAS BBQ TBD
	TREE UPLIGHT

ROOF TERRACE & COURTYARD LIGHTING PLAN



A Trellis mounted bullet light

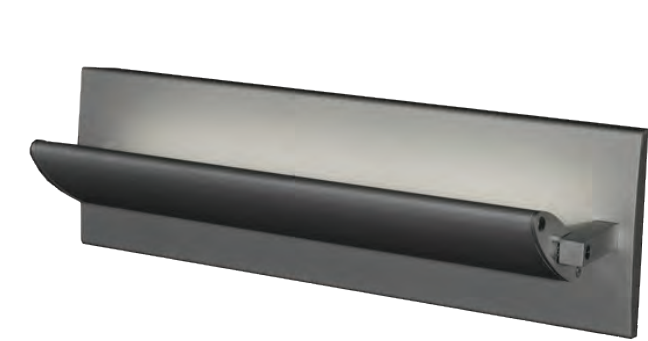


B Landscape accent light

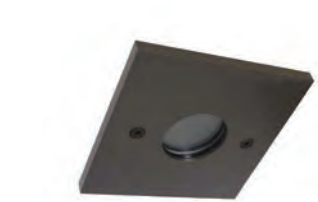


C Stake mounted pathway light

STREET LEVEL LIGHTING & SIGNAGE PLAN



A Pier-mounted uplight



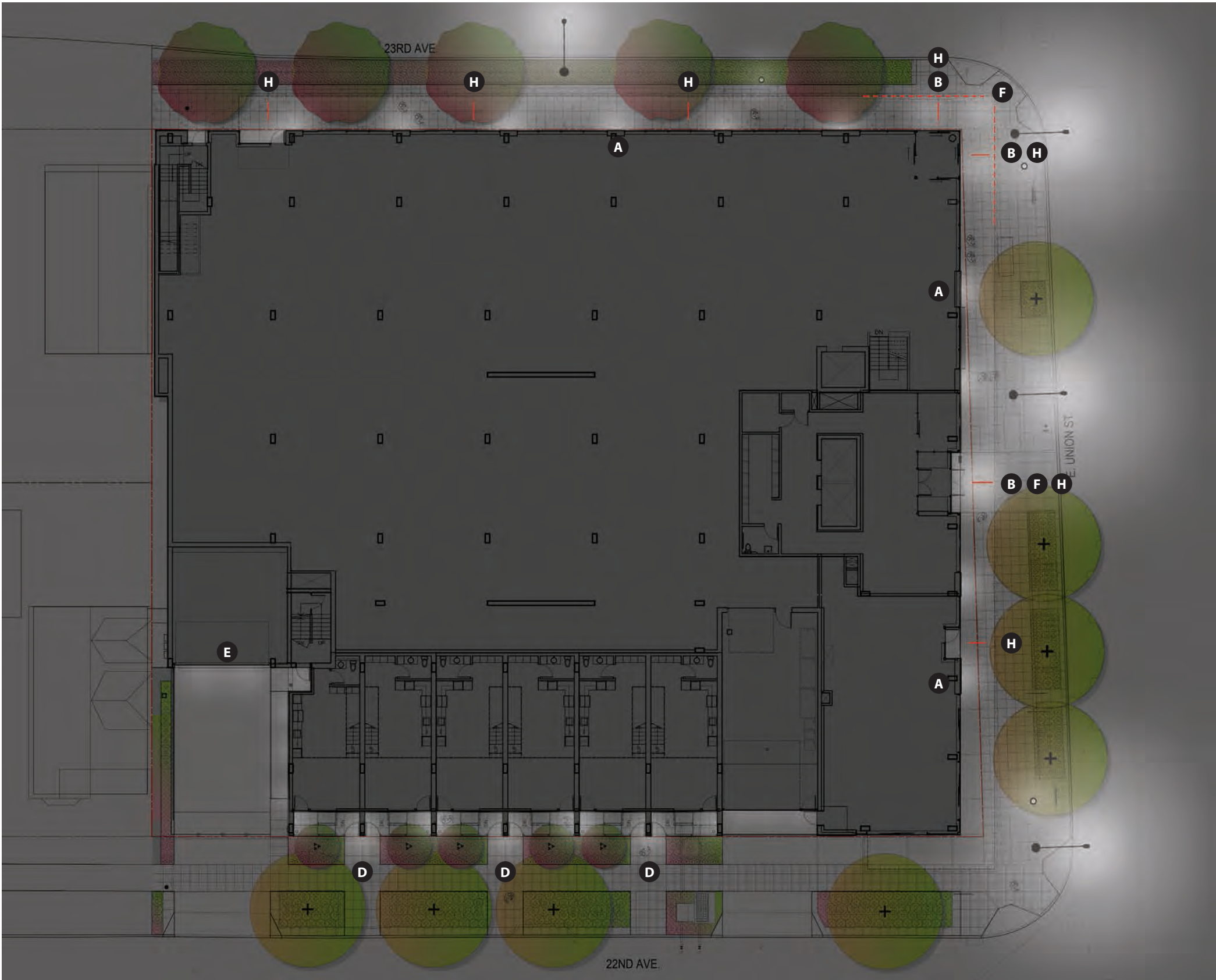
B Canopy downlight



D Residential entry light



E Garage entry light





F Wall mounted internally illuminated building identity sign, approx. 20 ft tall



G Marquee mounted identity sign at anchor tenant entry, externally or internally illuminated



H Soffit mounted blade signs below marquee, externally illuminated with aimable spot light



H Directional signage light

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REQUESTED DEVELOPMENT STANDARD DEPARTURES

DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
<div>1</div> <div><u>SMC 23.47A.014 B3 Side Setback Above 13-feet for Residential Zone to the North</u> Where lot is adjacent to a residentially zoned lot and has residential uses, a side yard setback of 15-feet is required for portions of the structure above 13-feet up to 40-feet above grade with an additional sloping setback of 2-feet for every 10-feet above 40-feet</div>	Extend the zero setback height from 13’ above average grade level to approximately 16’-9” above average grade level, for a distance of 41-feet, of which approximately 14-feet is adjacent to the neighboring house	<p>The departure seeks to allow the portion of the building adjacent to the residentail zone to extend 3’-9” above the 13-feet specified in the code. The proposed encroachment occurs about 38-feet off the front lot line on 22nd Avenue and would maintain the elevation of the proposed building’s courtyard immediately to the east and enable access to the required exit stair with a structurally efficient slab. To lessen the proposed encroachment into the required setback, the design team has stepped the structural slab and has held the occupied portions of the courtyard back 15-feet from the property line. The space between the property line and the occupiable area of the courtyard is landscaped to provide a privacy and visual buffer with the neighbor.</p> <p>Please note that this modest encroachment at this location is in the context of providing setbacks from the residential zone far in excess of the code requirements in all other locations adjacent to that zone (please see pp. 8–9). (Design Guidelines CS2-D.3 Zone Transitions, DC1-C.2 Visual Impacts, DC2-A.1 Reducing Perceived Mass, DC2-C.3 Fit With Neighboring Buildings)</p>
<div>2</div> <div><u>SMC 23.47A.005 C.1.a, 23.47A.005 D.1, and 23.47A.008 C.1 Street Level Uses in Pedestrian-Designated Zones</u> Design Standard: Maximum of 20% of façade length for residential uses, and minimum 80% of façade length for certain types of non-residential uses.</div>	Allow for residential lobby that is approximately 29% of the total façade length on E Union Street and allow for nonresidential use that is approximately 71% of the total façade length on E. Union Street	<p>Please note that the street frontage along 23rd Avenue exceeds the minimum required façade length with approximately 95% of the street frontage being occupied by non-residential uses.</p> <p>This departure request is for a lobby that is approximately 45’-4” wide, in order to accommodate the intended use of the space and provide the type of activity at the street that the code section intends to promote. The lobby interior will be designed and managed as an active resident amenity space with opportunities for both programmed and unprogrammed activities. Planned uses include periodic resident socials or happy hours and open houses. Several lounge seating clusters, a large communal table, and laptop bar will provide a variety of spaces for individuals and groups alike, to do work, hold meetings, or simply socialize. Portions of the storefront will be operable to allow the lobby to open and engage the sidewalk when weather and activities permit. (Design Guidelines CS2B2 Connection to the Street, PL3-A Street Level Interaction-Entries)</p>
<div>3</div> <div><u>SMC 23.54.030 D2 Driveway Widths</u> Development Standard: Minimum width for non-residential driveway is 22-feet for two-way traffic</div>	Allow non-residential driveway width of 21’-6”	The proposed driveway width is 6-inches narrower than the minimum required in order to accommodate a landscape buffer between the driveway and the neighbor to the north. The narrower dimension will also server to minimize the presence of the driveway so as to not detract from the pedestrian environment along 22nd Avenue. (Design Guidelines CS2-B.2 Connection to Street, DC1-C.2 Visual Impacts)
<div>4</div> <div><u>SMC 23.54.030 G Sight Triangles for Driveways</u> 10-foot sight triangle required on both sides of driveways less than 22-feet wide</div>	Allow for use of mirrors and textured pavement in lieu of providing sight triangle at parking garage access and loading berth	Eliminating the sight triangle requirement helps diminish the size of the parking garage entry and the loading berth entry, lessens their appearance from the sidewalk, and keeps the entries in scale with the surrounding neighborhood. Mirrors and textured pavement will be employed and are passive ways to mitigate potential pedestrian-automobile conflicts and are used frequently in downtown Seattle and in other mixed-use projects located in Neighborhood Commercial zones. Shielded lighting at the sides of the parkgin garage entry provides a subtle, unobtrusive way to supplement the use of mirrors and address low-light situations. (Design Guidelines CS2-B.2 Connection to Street, DC1-C.2 Visual Impacts)
<div>5</div> <div><u>SMC 23.54.030 B2 Required Distribution of Size of Parking Spaces</u> Where 20 or more non-residential parking spaces are provided, provide a minimum of 35% and maximum of 65% small spaces, and a minimum of 35% large spaces</div>	Allow 24 (48%) of the proposed commercial parking spaces to be small, 1 space (2%) to be large, and the remaining 25 spaces to be medium.	The proposed distribution of parking space sizes permits a straightforward, efficient parking layout that maximizes the number of parking spaces dedicated for commercial use. Providing the requisite 35% large spaces would reduce the overall number of parking spaces provided from 50 to 36, would require that all the proposed medium-sized spaces become small sized spaces, and would require a configuration with a dead-end aisle rather than a loop that allows users to easily navigate through the garage without forcing vehicles to back-up in order to turn around and exit the garage. (Design Guideline DC1-C.1 Below-grade Parking)

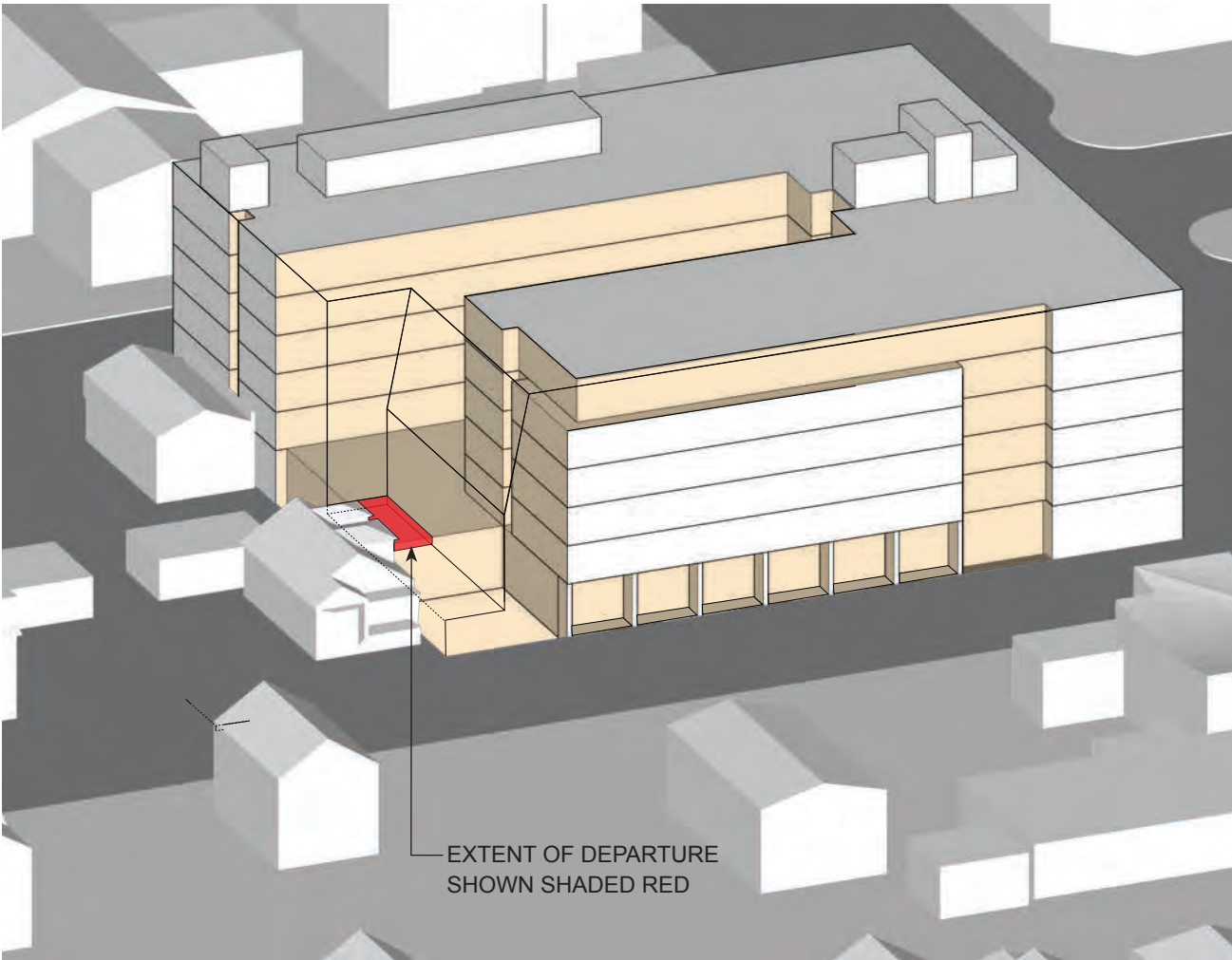
The following Development Standard Departures have been identified as necessary to achieve the proposed building design.

Departures #1, #3, #4 and #5 were presented to the Board during at Early Design Guidance and the Board indicated a willingness to entertain the requests, but requested further information how the departures made for a better design and clear illustrations regarding the functionality and impacts of the departures as the design progressed.

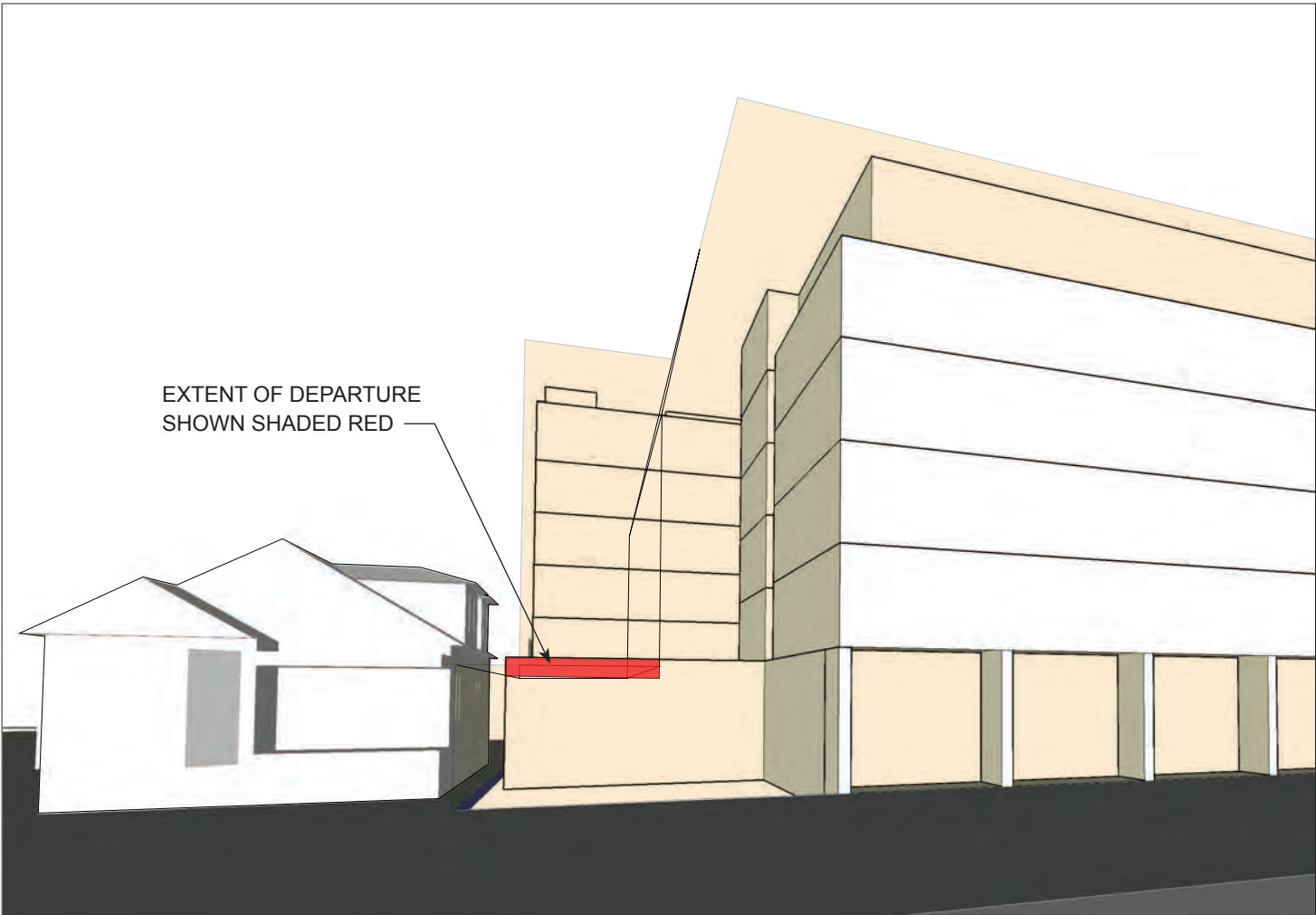
Departure #2 has been identified and added subsequent to the Early Design Guidance meeting, but was a subject of discussion during the meeting.

DEPARTURE 1: SIDE SETBACK ABOVE 13-FEET

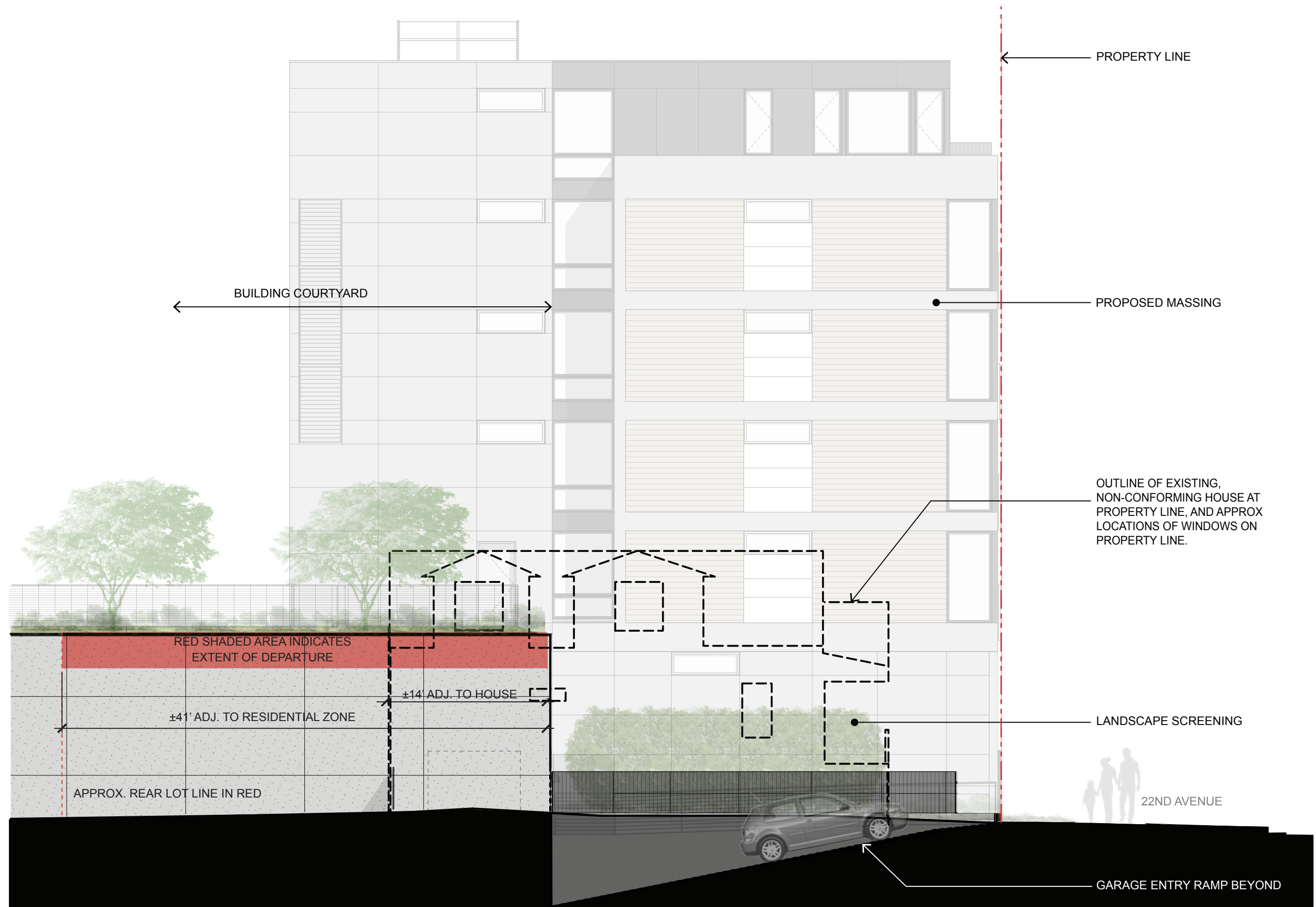
DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
<p>1 <u>SMC 23.47A.014 B3 Side Setback Above 13-feet for Residential Zone to the North</u> Where lot is adjacent to a residentially zoned lot and has residential uses, a side yard setback of 15-feet is required for portions of the structure above 13-feet up to 40-feet above grade with an additional sloping setback of 2-feet for every 10-feet above 40-feet</p>	<p>Extend the zero setback height from 13' above average grade level to approximately 16'-9" above average grade level, for a distance of 41-feet, of which approximately 14-feet is adjacent to the neighboring house</p>	<p>The departure seeks to allow the portion of the building adjacent to the residential zone to extend 3'-9" above the 13-feet specified in the code. The proposed encroachment occurs about 38-feet off the front lot line on 22nd Avenue and would maintain the elevation of the proposed building's courtyard immediately to the east and enable access to the required exit stair with a structurally efficient slab. To lessen the proposed encroachment into the required setback, the design team has stepped the structural slab and has held the occupied portions of the courtyard back 15-feet from the property line. The space between the property line and the occupiable area of the courtyard is landscaped to provide a privacy and visual buffer with the neighbor.</p> <p>Please note that this modest encroachment at this location is in the context of providing setbacks from the residential zone far in excess of the code requirements in all other locations adjacent to that zone (please see pp. 8–9). (Design Guidelines CS2-D.3 Zone Transitions, DC1-C.2 Visual Impacts, DC2-A.1 Reducing Perceived Mass, DC2-C.3 Fit With Neighboring Buildings)</p>



PERSPECTIVE VIEW OF DEPARTURE REQUEST (ALLOWABLE ZONING ENVELOPE SHOWN SHADED ORANGE)

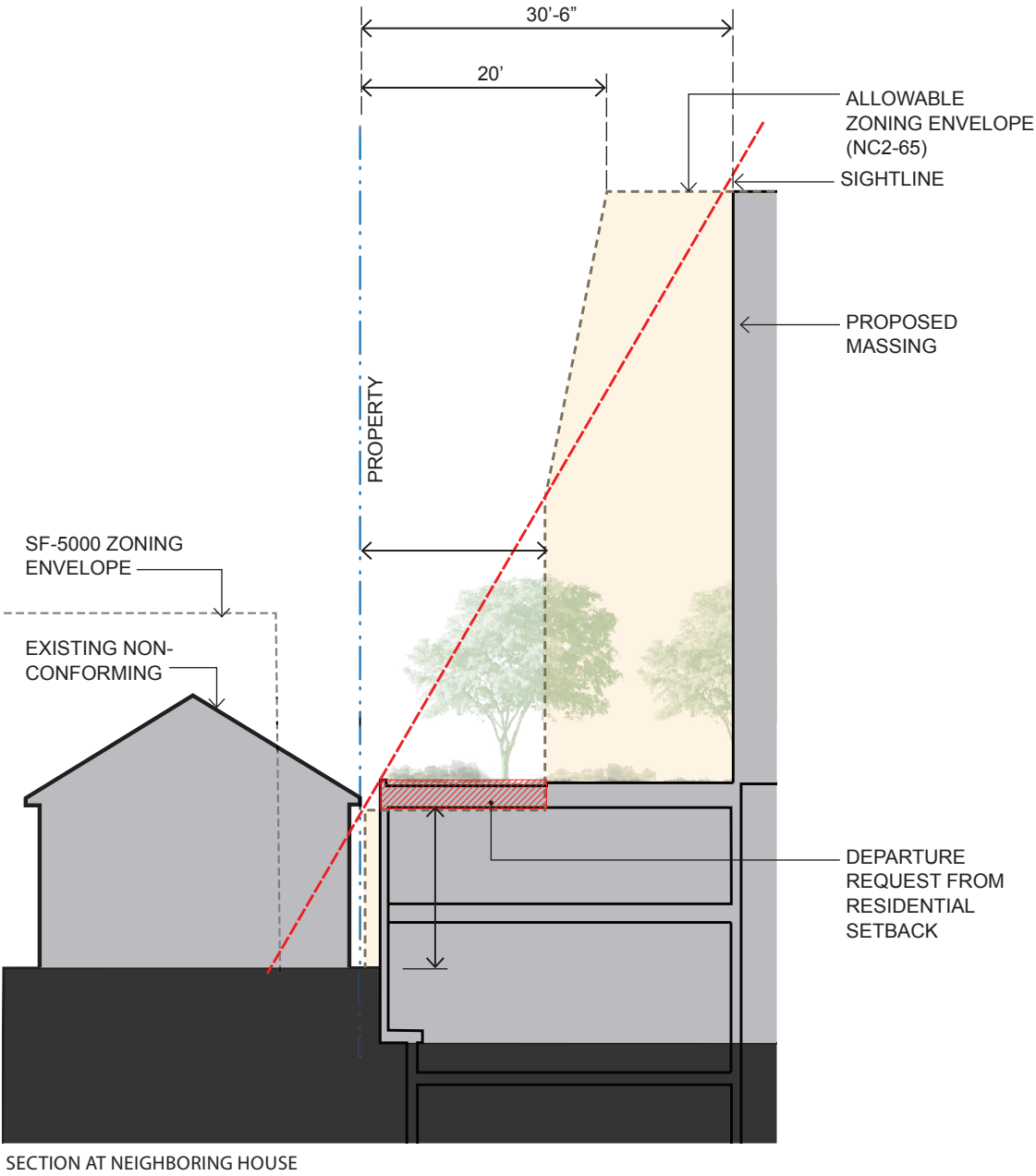
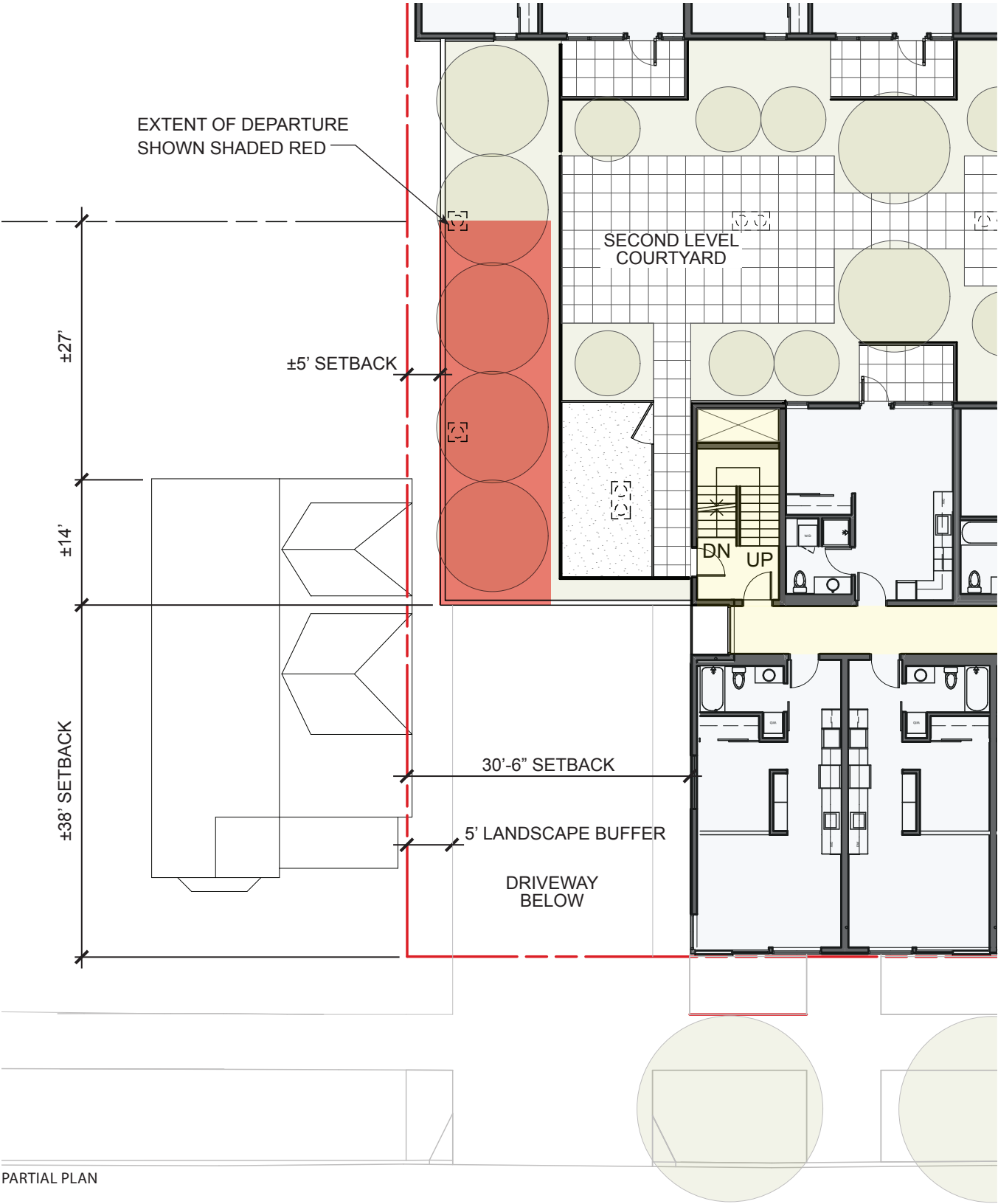


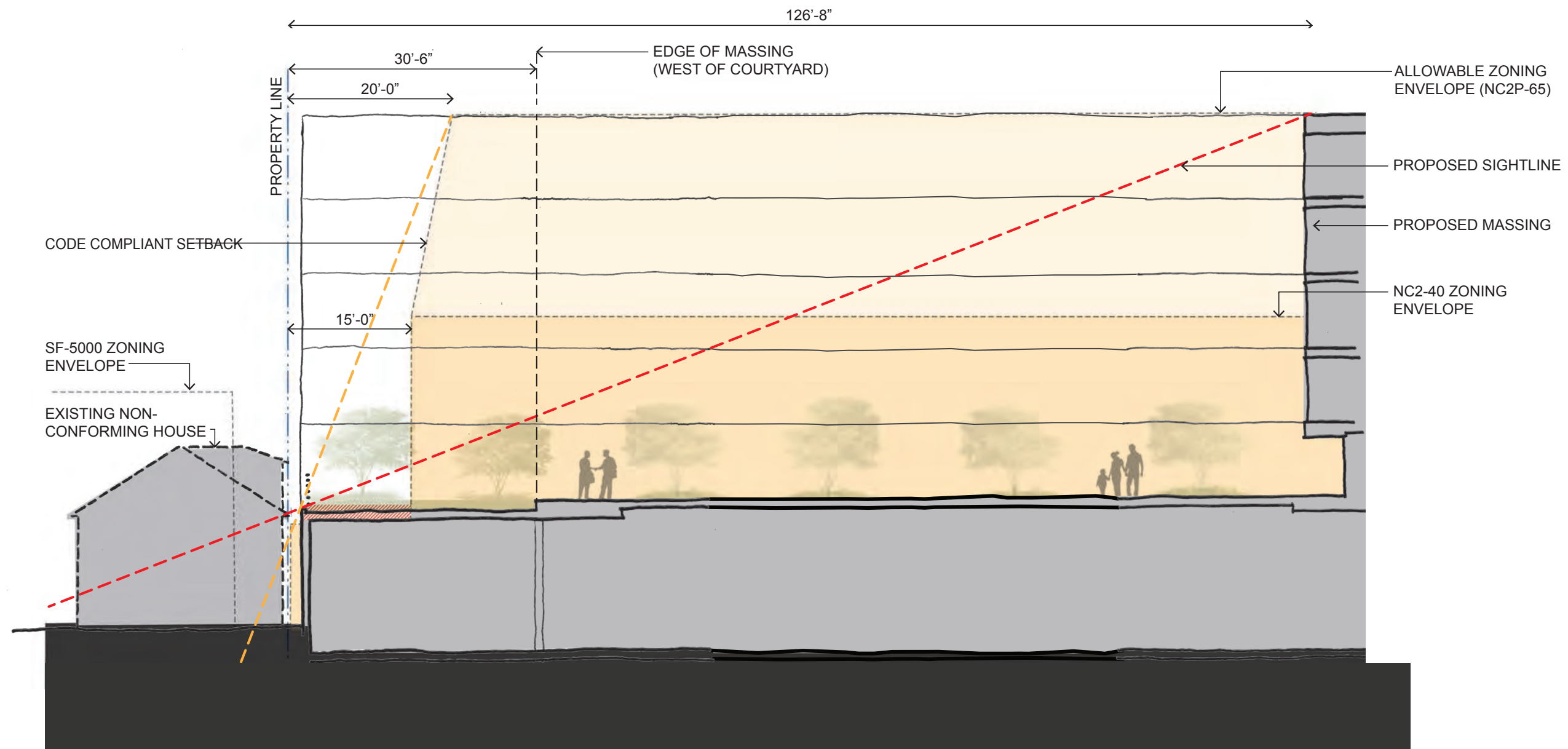
DETAIL PERSPECTIVE VIEW OF DEPARTURE REQUEST (ALLOWABLE ZONING ENVELOPE SHOWN SHADED ORANGE)



PARTIAL NORTH ELEVATION WITH WINDOW MAPPING OF THE ADJACENT HOUSE RELATIVE TO DEPARTURE REQUEST

DEPARTURE 1: SIDE SETBACK ABOVE 13-FEET (CONTINUED)





SECTION AT COURTYARD OF PROPOSED BUILDING

DEPARTURE 2: STREET-LEVEL USES IN PEDESTRIAN-DESIGNATED ZONES

DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
<p>2 <u>SMC 23.47A.005 C.1.a, 23.47A.005 D.1, and 23.47A.008 C.1 Street Level Uses in Pedestrian-Designated Zones</u></p> <p>Design Standard: Maximum of 20% of façade length for residential uses, and minimum 80% of façade length for certain types of non-residential uses.</p>	<p>Allow for residential lobby that is approximately 29% of the total façade length on E Union Street and allow for nonresidential use that is approximately 71% of the total façade length on E. Union Street</p>	<p>Please note that the street frontage along 23rd Avenue exceeds the minimum required façade length with approximately 95% of the street frontage being occupied by non-residential uses.</p> <p>This departure request is for a lobby that is approximately 45'-4" wide, in order to accommodate the intended use of the space and provide the type of activity at the street that the code section intends to promote. The lobby interior will be designed and managed as an active resident amenity space with opportunities for both programmed and unprogrammed activities. Planned uses include periodic resident socials or happy hours and open houses. Several lounge seating clusters, a large communal table, and laptop bar will provide a variety of spaces for individuals and groups alike, to do work, hold meetings, or simply socialize. Portions of the storefront will be operable to allow the lobby to open and engage the sidewalk when weather and activities permit. (Design Guidelines CS2B2 Connection to the Street, PL3-A Street Level Interaction-Entries)</p>



ACTIVE LOBBY PRECIDENTS

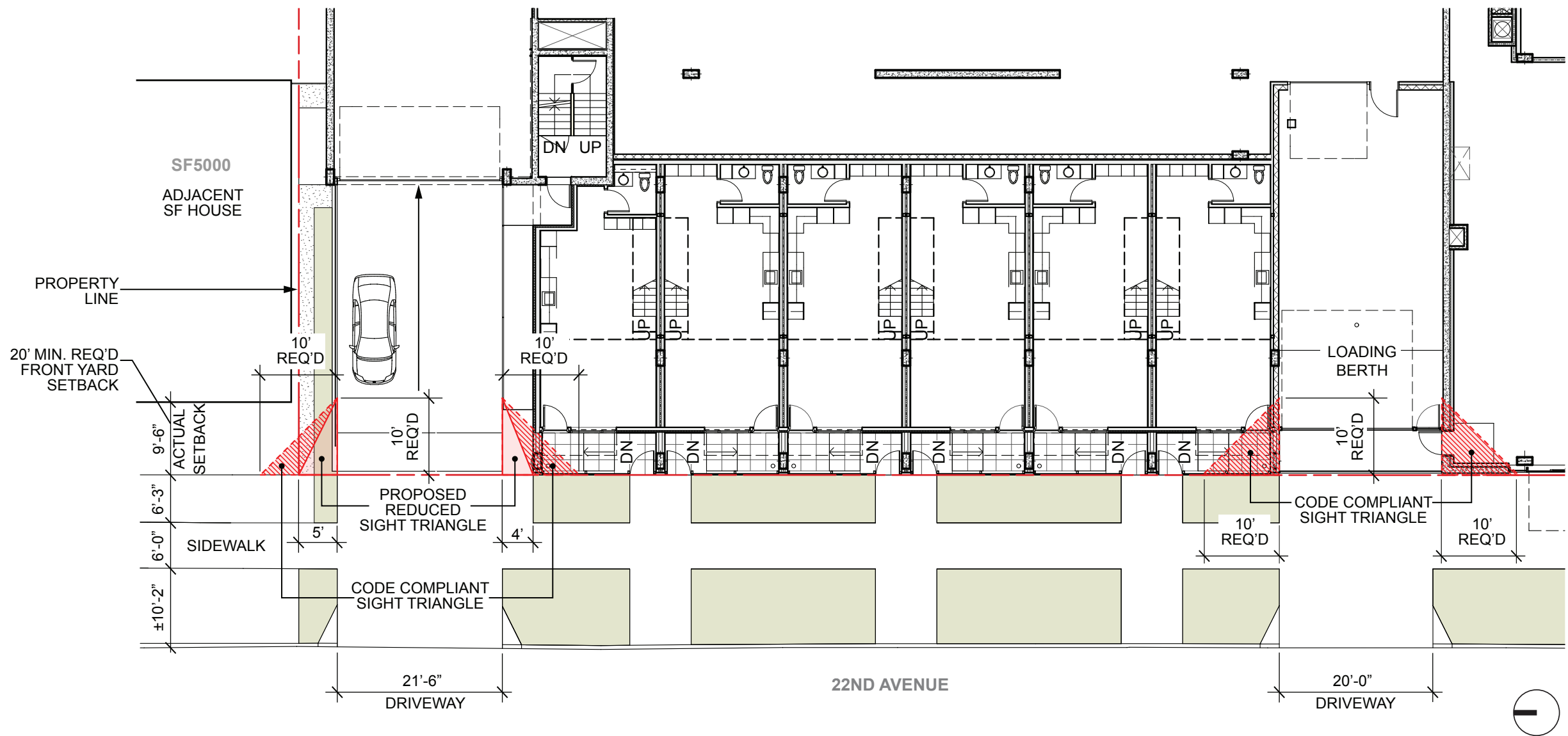
RESIDENTIAL LOBBY FLOOR PLAN

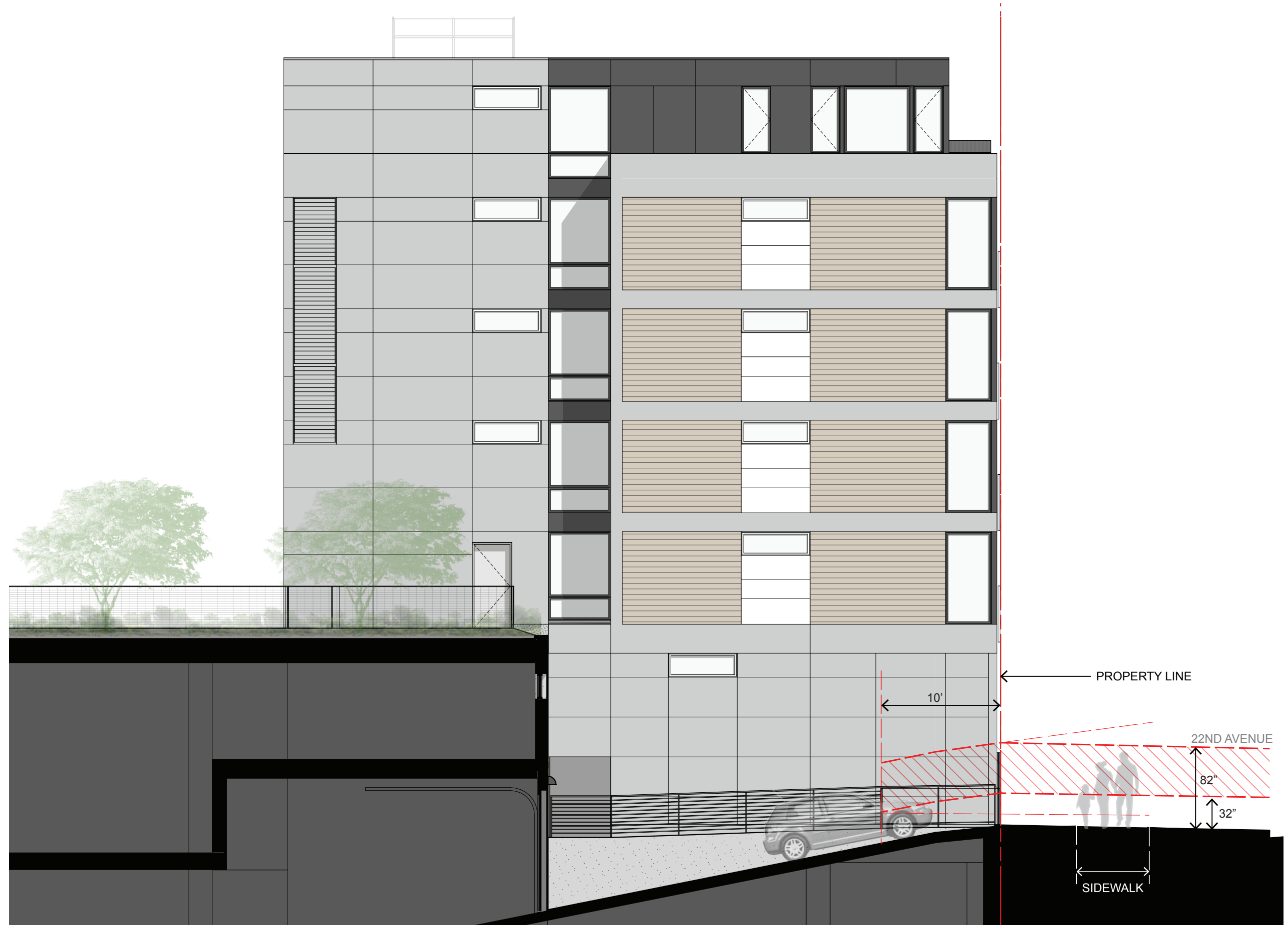


RESIDENTIAL LOBBY SKETCH

DEPARTURES 3 & 4: DRIVEWAY WIDTH & SIGHT TRIANGLES

DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
<div>3</div> <div>SMC 23.54.030 D2 Driveway Widths</div> <div>Minimum width for non-residential driveway is 22-feet for two-way traffic</div>	Allow non-residential driveway width of 21'-6"	The proposed driveway width is proposed to be 6-inches narrower than the minimum required in order to accommodate a landscape buffer between the driveway and the neighbor to the north. The narrower dimension will also server to minimize the presence of the driveway so as to not detract from the pedestrian environment along 22nd Avenue. (Design Guidelines CS2-B.2 Connection to Street, DC1-C.2 Visual Impacts)
<div>4</div> <div>SMC 23.54.030 G Sight Triangles for Driveways</div> <div>10-foot sight triangle required on both sides of driveways less than 22-feet wide</div>	Allow for use of mirrors and textured pavement in lieu of providing sight triangle at parking garage access and loading berth	Eliminating the sight triangle requirement helps diminish the size of the parking garage entry and the loading berth entry, lessens their appearance from the sidewalk, and keeps the entries in scale with the surrounding neighborhood. Mirrors and textured pavement will be employed and are passive ways to mitigate potential pedestrian-automobile conflicts and are used frequently in downtown Seattle and in other mixed-use projects located in Neighborhood Commercial zones. Shielded lighting at the sides of the parkgin garage entry provides a subtle, unobtrusive way to supplement the use of mirrors and address low-light situations. (Design Guidelines CS2-B.2 Connection to Street, DC1-C.2 Visual Impacts)





RESPONSE TO GUIDELINES

Absent neighborhood specific Design Guidelines, the citywide Seattle Design Guidelines serve to guide the design and development of the project. However, the 23rd Avenue UDF does identify specific design considerations and ambitions for the site that should be taken into consideration. The Design Review Board identified Priority Design Guidelines for the project at the Early Design Guidance meeting and are shown to the right. Alignment with the ambitions expressed in the 23rd Avenue UDF are indicated with a check mark.

SEATTLE DESIGN GUIDELINES		DESIGN RESPONSE
CS1 Natural Systems and Site Features Use natural systems and features of the site and its surroundings as a starting point for project design	B2 Sunlight & Natural Ventilation, Daylight and Shading Maximize daylight for interior and exterior spaces and minimize shading on adjacent sites through the placement and/or design of structures on the site	<p>The proposed design prioritizes natural light at interior spaces by providing ample windows within individual living spaces and at strategic moments in residential corridors to provide relief and views. At street level, the design of the commercial frontage emphasizes transparency and its greater than required floor-to-floor height admits light above the continuous overhead weather protection and deep into to the commercial spaces.</p> <p>The exterior open space balances the desire for natural light within the courtyard with the maintenance of solar access to the rear yards of the neighbors located north of the project. Additional considerations for light access are given at the northwest corner of the project site where a single-family house abuts the property line. The proposed building design increases the applicable setback building from the required 15’ (20’ at its upper extent) to a setback of 30’-6” increasing up to 34’-6” at the uppermost level.</p>
CS2 Urban Pattern and Form Strengthen the most desirable forms, characteristics, & patterns of the streets, block faces, & open spaces in the surrounding area	✓ A1 Location in the City and Neighborhood, Sense of Place Emphasize attributes that give Seattle, the neighborhood, and/or the site its distinctive sense of place. Design the building and open spaces to enhance areas where a strong identity already exists, and create a sense of place where the physical context is less established	<p>The proposed development promotes activity along its 23rd Avenue and E Union Street frontages with greater than required transparency, operable windows, continuous overhead weather protection and an integrated bus stop.</p> <p>The ground-related dwelling units along 22nd Avenue respond to the residential character further north of the street with extensive landscaping and small porches at each unit.</p>
	✓ A2 Location in the City and Neighborhood, Architectural Presence Evaluate the degree of visibility or architectural presence that is appropriate or desired given the context, and design accordingly.	<p>The proposed design give prominence to its primary street frontages by locating the most active features along 23rd Avenue and E Union Street. The street level corner spaces are reserved for commercial where visibility is key. The main residential entry is located at the center of the building’s E Union Street frontage opposite the main entry to the mixed-use building, The Central, located on the south side of E Union Street.</p>
	✓ B1 Urban Pattern and Form, Adjacent Sites, Streets, and Open Spaces, Site Characteristics Allow characteristics of sites to inform the design, especially where the street grid and topography create unusually shaped lots that can add distinction to the building massing.	<p>23rd Avenue and E Union Street are both busy arterials with public transportation service making these logical locations for commercial activity. The building’s corners have been identified as key locations for commercial space due to their prominent exposure, height, and high transparency. The building’s location at the intersection of these arterials also supports the location of its main residential entry where it is visible and readily identifiable from both streets. The proximity of the building to the existing bus stop on E Union Street offers another opportunity for the building to actively engage its surrounding environs by integrating the stop into the building design.</p> <p>Conversely, 22nd Avenue is a narrower and quieter residential street, more conducive to ground-related housing and a small commercial space that is hoped to become a café or restaurant. The reduced traffic volumes and speed encountered on 22nd Avenue are a more conducive location for the garage entry, which is supported by the site’s zoning code regulations (SMC 23.47A.005.D.2 & 23.47A.032.A.2a)</p>

SEATTLE DESIGN GUIDELINES		DESIGN RESPONSE
	✓ B2 Location in the City and Neighborhood, Connection to the Street Identify opportunities for the project to make a strong connection to the street and public realm.	Please refer to the Design Response to CS2-B.1 above
	✓ B3 Location in the City and Neighborhood, Character of Open Space Contribute to the character and proportion of surrounding open spaces.	Please refer to the Design Response to CS2-B.1 above
	✓ C1 Urban Pattern and Form, Relationship to the Block, Corner Sites Corner sites can serve as gateways or focal points; both require careful detailing at the first three floors due to their high visibility from two or more streets and long distances.	Please refer to the Design Response to CS2-B.1 above
	✓ D1 Urban Pattern and Form, Height Bulk and Scale, Existing Development & Zoning Review the height, bulk, and scale of neighboring buildings as well as the scale of development anticipated by zoning for the area to determine an appropriate complement and/or transition.	<p>The project site as currently zoned is a mix of Neighborhood Commercial 2 with a Pedestrian Overlay (NC2P-40 at 2200 & 2220 E Union St) and Neighborhood Commercial 2 (NC2-40 at 1408 22nd Avenue) both with a 40’ height limit. The proposed project seeks to reclassify the zones to have a 65’ height limit thus changing the zoning to be NC2P-65 and NC2-65 respectively.</p> <p>SDCI has released recommendations for zoning changes in and around the subject property to implement the 23rd Avenue Action Plan and Urban Design Framework, including a recommendation for a 65-foot height limit on the project site. The contract rezone is consistent with the height and density being recommended in those zoning changes.</p> <p>See page 8 for a description of the minimum required setbacks and the setbacks provided by the proposed project.</p>
	✓ D3 Urban Pattern and Form , Height Bulk and Scale, Zone Transitions For projects located at the edge of different zones, provide an appropriate transition or complement to the adjacent zone(s). Projects should create a step in perceived height, bulk and scale between the anticipated development potential of the adjacent zone and the proposed development.	<p>The proposed project provides a 30’-6” setback at the north property line and the northerly upper floor is set back an additional 4’, on the west side of the project site as a means to transition to the single-family zone and minimize the proposed project’s impact on the neighboring single-family building to the north.</p> <p>These setbacks provide a substantially greater degree of separation between the proposed building and the existing single-family building than required by the Land Use Code. The project’s courtyard is also oriented toward the north, significantly reducing the building mass along the north property line and providing visual relief. In addition, strategies to modulate the proposed building’s massing are proposed along each of its primary street frontages and the driveway area next to the single-family zone will be screened with landscaping.</p>
	✓ D4 Urban Pattern and Form, Height, Bulk, and Scale, Massing Choices Strive for a successful transition between zones where a project abuts a less intense zone.	Please refer to the Design Response to CS2-D.1 and D.3 above

RESPONSE TO GUIDELINES (CONTINUED)

SEATTLE DESIGN GUIDELINES		DESIGN RESPONSE
	<div>✓</div> <div>D5 Urban Pattern and Form, Height Bulk and Scale, Respect for Adjacent Sites Respect adjacent properties with design and site planning to minimize disrupting the privacy and outdoor activities of residents in adjacent buildings</div>	A five-foot wide landscape buffer and a green wall structure are proposed between the driveway and adjacent property to mitigate the presence of the driveway and mitigate potential light and glare.
<div>CS3 Architectural Context and Character Contribute to the architectural character of the neighborhood</div>	<div>✓</div> <div>A2 Architectural Context & Character, Emphasizing Positive Neighborhood Attributes, Contemporary Design Explore how contemporary designs can contribute to the development of attractive new forms and architectural styles as expressed through use of new materials or other means.</div>	The building design will use wood siding, brake metal trims and accents, fiber cement board, and pre-finished metal fabrications at the exterior cladding. Large, expansive windows will admit plentiful daylight and provide territorial views. Inset unit decks will allow access to exterior space and activate the primary street frontages of the building while serving to modulate the building’s massing. A richly patterned façade registers the building’s organization while delivering a humanizing texture.
	<div>✓</div> <div>A4 Architectural Context & Character, Emphasizing Positive Neighborhood Attributes, Evolving Neighborhoods In neighborhoods where architectural character is evolving or otherwise in transition, explore ways for new development to establish a positive and desirable context for others to build upon in the future.</div>	<p>In response to the need to sensitively transition to the adjacent single-family zone to the north, the design has paid particular attention to the relationship of the ground-related residential units to the street; incorporating subtle changes to the character of the building to differentiate it along 22nd Avenue; setback the upper level along 22nd Avenue to decrease the building scale; and setback along the side lot line at the north of the property well in excess of the minimum required setback.</p> <p>The design also incorporates a modulation strategy consisting of inset exterior decks along 23rd Avenue and E Union Street that simultaneously expresses the building’s interior organization while also working to instill the project with humanizing elements that further add to the activity of the streets.</p>
<div>PL1 Connectivity Complement and contribute to the network of open spaces around the site and the connections among them</div>	<div>✓</div> <div>A2 Connectivity, Network of Open Spaces, Adding to Public Life Seek opportunities to foster human interaction through an increase in the size and quality of project-related open space available for public life.</div>	Consistent with the goals of the 23rd Avenue Action Plan and the Urban Design Framework, the proposed design seeks to foster the types of activity and vitality desired at the intersection of 23rd Avenue and E Union Street. A continuous, tall, and highly transparent street level is patterned to provide logical locations for entries to accommodate a variety of business types and sizes along 23rd Avenue. The activation of the street level continues along E Union Street with an integrated bus stop, a clearly marked (and active) residential entry and leasing office, and a small commercial space with operable storefront that is sited to allow for sidewalk seating by a future tenant. These design considerations also contribute to Design Guidelines PL3-C.1, PL3-C.2, PL3-C.3
<div>PL3 Street-Level Interaction Encourage human interaction and activity at the street-level with clear connections to building entries and edges</div>	<div>✓</div> <div>A1 Street-Level Interaction , Entries, Design Objectives Design primary entries to be obvious, identifiable, and distinctive with clear lines of sight and lobbies visually connected to the street.</div>	The building’s location at the intersection of 23rd Avenue and E Union Street supports the location of its main residential entry where it is visible and readily identifiable from both streets. The entry is proposed to be located at the center of the building’s E Union Street façade, opposite the main entry of The Central located on the south side of E Union Street. The lobby will be highly transparent with a small lounge, community table, laptop bar and operable windows to help activate the adjacent streetscape.

SEATTLE DESIGN GUIDELINES		DESIGN RESPONSE
✓	A2 Street-Level Interaction , Entries, Common Entries Multi-story residential buildings need to provide privacy and security for residents, but also be welcoming and identifiable to visitors.	Please refer to the Design Response to PL3-A.1
✓	A3 Street-Level Interaction, Entries, Individual Entries Ground-related housing should be scaled and detailed appropriately to provide for a more intimate type of entry.	The project proposes ground-related housing along 22nd Avenue where this use is in keeping with existing uses further to the north. A landscaped setback provides separation and privacy between the dwelling units and the public realm by clearly articulating the boundary between public and private. The landscaped buffer maintains the privacy of the unit, while a security grille provides an additional screening element that also secures the porch and entry to the unit.
✓	A4 Street-Level Interaction, Entries, Ensemble of Elements Design the entry as a collection of coordinated elements including the door(s), overhead features, ground surface, landscaping, lighting, and other features.	The main residential entry occurs at the center of the E Union Street frontage at the inflection point in the building patterning allowing the entry location to be understood at a distance. Closer in to the building, the building massing is inset at the entry creating a highly transparent glass vestibule with views into the lobby and flanked to each side by board formed concrete walls. The continuous overhead weather protection along E Union Street deepens above the entry, but changes material to admit light from overhead and further highlight the point of entry.
✓	B2 Street-Level Interaction, Residential Edges, Ground-level Residential Privacy and security issues are particularly important in buildings with ground-level housing, both at entries and where windows are located overlooking the street.	Please refer to the Design Response to PL3-A.3
✓	C1 Street-Level Interaction, Retail Edges, Porous Edges Engage passersby with opportunities to interact visually with the building interior using glazing and transparency. Create multiple entries where possible and make a physical and visual connection between people on the sidewalk and retail activities in the building	Please refer to the Design Response to PL1-A.2
✓	C2 Street-Level Interaction, Retail Edges, Visibility Maximize visibility into the building interior and merchandise displays. Consider fully operational glazed wall-sized doors that can be completely opened to the street, increased height in lobbies, and/or special lighting for displays	Please refer to the Design Response to PL1-A.2
✓	C3 Street-Level Interaction, Retail Edges, Ancillary Activities Allow space for activities such as sidewalk vending, seating, and restaurant dining to occur. Consider setting structures back from the street or incorporating space in the project design into which retail uses can extend.	Please refer to the Design Response to PL1-A.2

RESPONSE TO GUIDELINES (CONTINUED)

SEATTLE DESIGN GUIDELINES		DESIGN RESPONSE
PL4 Active Transportation Incorporate design features that facilitate active forms of transportation such as walking, bicycling and use of transit	✓ A1 Active Transportation, Entry Locations and Relationships, Serving all Modes of Travel Provide safe and convenient access points for all modes of travel.	<p>The building design carefully considers the pedestrian environment at each of the building's frontages. Transparency and multiple openings along 23rd Avenue and E Union Street promote safety and imbue the streets with activity. New continuous planting strips along both frontages provide a buffer between the sidewalk and street traffic. The residential character of 22nd Avenue is reinforced by the presence of ground-related units with their porches and landscaping. Vehicular access on 22nd Avenue will be clearly indicated with a change in paving pattern.</p> <p>Bicyclists can access the building's bicycle facilities (a secured storage room, a repair room, and a washing room) via the residential garage entry. The bicycle facilities are located adjacent to the building's core.</p>
	✓ B3 Active Transportation, Planning Ahead for Bicyclists, Bike Facilities Facilities such as bike racks and storage, bike share stations, shower facilities and lockers for bicyclists should be located to maximize convenience, security, and safety	<p>The project site's 22nd Avenue frontage has recently been designated a bikeway and bicycle facilities are located within the building accordingly. Secure long-term bicycle storage for residents is provided are the below-grade parking level accessed from 22nd Avenue. Adjacent to the bicycle storage room are a repair room and bike & pet wash room. All these facilities are located proximate to the building's elevator core and main lobby. Short-term bicycle storage will be steel bicycle racks occurring in the E Union Street right-of-way.</p>
	✓ B3 Active Transportation, Planning Ahead for Bicyclists, Bike Connections Facilitate connections to bicycle trails and infrastructure around and beyond the project.	<p>The project site fronts the 22nd Avenue bikeway and provides access to its bicycle facilities from 22nd Avenue.</p>
DC1 Project Uses and Activities Optimize the arrangement of uses and activities on site	✓ B1 Project Uses and Activities, Vehicular Access and Circulation, Access location and Design Choose locations for vehicular access, service uses, and delivery areas that minimize conflict between vehicles and non-motorists wherever possible. Emphasize use of the sidewalk for pedestrians, and create safe and attractive conditions for pedestrians, bicyclists, and drivers.	<p>Pedestrian activity is encouraged along all three building frontages, but 23rd Avenue and E Union Street are both identified as principal pedestrian streets thus requiring to be parking accessed from 22nd Avenue (refer to SMC 23.47A.005.D.2 & 23.47A.032.A.2a). To accommodate vehicle circulation while safeguarding pedestrian, the design provides parking users with a landscape strip of approximately 6' between the edge of ramp and the sidewalk as a buffer before crossing the sidewalk. A separate loading berth has also been sized and configured to accommodate delivery vehicles internally.</p> <p>To minimize the outward appearance of the garage entry and loading dock the full required sight lines have not been accommodated. Instead, the project proposes the use of mirrors and textured pavement to mitigate potential pedestrian-automobile conflicts.</p> <p>Trash and recycling collection and storage for commercial and residential uses occur internal to the building. Commercial trash and recycling storage is located in loading dock. Residential trash and recycling storage occurs in the below-grade parking level. Building management will move trash and recycling containers for curbside pick-up on collections days.</p>

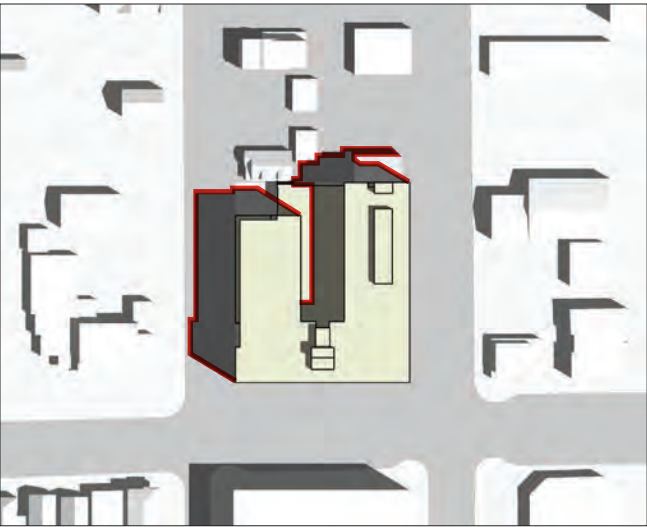
SEATTLE DESIGN GUIDELINES		DESIGN RESPONSE
	✓ C1 Project Uses and Activities, Parking and Service Uses, Below-Grade Parking Locate parking below grade wherever possible. Where a surface parking lot is the only alternative, locate the parking in rear or side yards, or on lower or less visible portions of the site.	Two below-grade parking levels are proposed to accommodate both commercial and residential users.
	✓ C2 Parking and Service Uses, Visual Impacts Reduce the visual impacts of parking lots, parking structures, entrances, and related signs and equipment as much as possible.	Please refer to the Design Response to DC1-C.1, and to the discussion of Light and Glare in the SEPA Checklist.
	✓ C3 Project Uses and Activities, Parking and Service Uses, Multiple Uses Design parking areas to serve multiple uses such as children's play space, outdoor gathering areas, sports courts, woonerf, or common space in multifamily projects.	The proposed parking is internal to the building and is nearly exclusively parking (mechanical, utility, and trash storage uses are co-located with the parking). The appearance of the parking garage entry is minimized at street level. Please refer to the Design Response to DC1-B.1.
	✓ C4 Parking and Service Uses, Service Uses Locate and design service entries, loading docks, and trash receptacles away from pedestrian areas or to a less visible portion of the site to reduce possible impacts of these facilities on building aesthetics and pedestrian circulation.	Please refer to the Design Response to DC1-B.
DC2 Architectural Concept Develop an architectural concept that will result in a unified & functional design that fits well on the site & within its surroundings	✓ A1 Massing, Site Characteristics and Uses Arrange the mass of the building taking into consideration the characteristics of the site and the proposed uses of the building and its open space.	Please refer to the Design Responses to CS1-B.2 and CS2-D.3

RESPONSE TO GUIDELINES (CONTINUED)

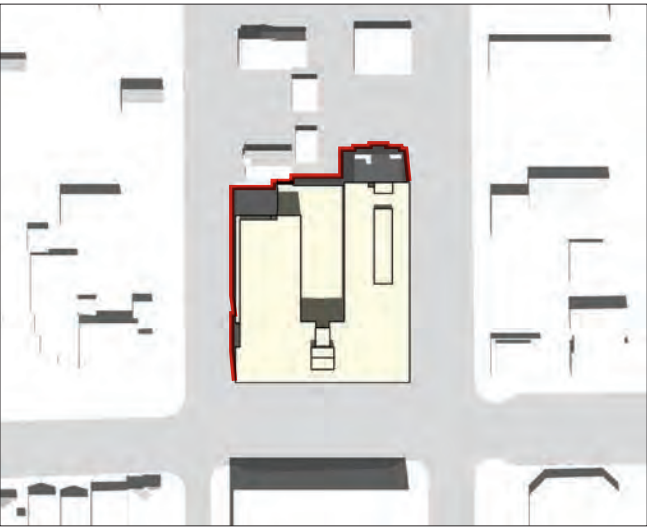
SEATTLE DESIGN GUIDELINES		DESIGN RESPONSE
	<div>✓</div> <div>A2 Architectural Concept, massing, Reducing Perceived Mass</div> <div>Use secondary architectural elements to reduce the perceived mass of larger projects.</div>	<p>The proposed design employs a number of strategies to imbue the building's massing with a relatable, human scale. The strategies employed are a result of the building's underlying organization. A tall, glassy street level along 23rd Avenue and E Union Street establishes a distinctly different order at the base of the building that distinguishes the commercial uses from the residential levels above. A continuous marquee serves to further articulate the base, while providing overhead weather protection and an opportunity for signage. The tall base transforms along 22nd Avenue into two-story ground-related dwelling units evoking the townhouse form.</p> <p>At the residential levels, inset decks are used along the 23rd Avenue and E Union Street frontages to provide intrinsic modulation that creates a multi-layered façade and embeds directionality to the units and the façade's ordering. The ordering inflects along E Union Street to subtly indicate the residential entry. Where inset decks would result in impractically sized residential units, railings are employed to maintain a relationship to the exterior as well as continue the layered façade organization.</p> <p>Material choice and location are used to express the building's order while also establishing scale. Subtle differences in the wood siding profiles, spacing, and color, work together to produce a textured façade that is further enriched by the transparency of the windows and railings at each of the decks. Painted fiber cement board is used to reflect light where light access is diminished, which also contrasts with the wood siding's texture and color.</p>
	<div>✓</div> <div>D1 Scale and Texture, Human Scale</div> <div>Incorporate architectural features, elements, and details that are of human scale into the building facades, entries, retaining walls, courtyards, and exterior spaces in a manner that is consistent with the overall architectural concept.</div>	<p>Please refer to the Design Response to DC2-A.2</p>

SHADOW STUDIES: ENTIRE YEAR

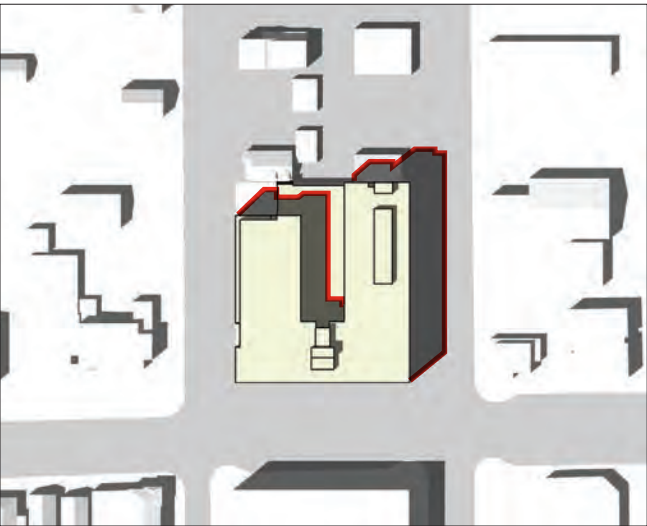
June 21



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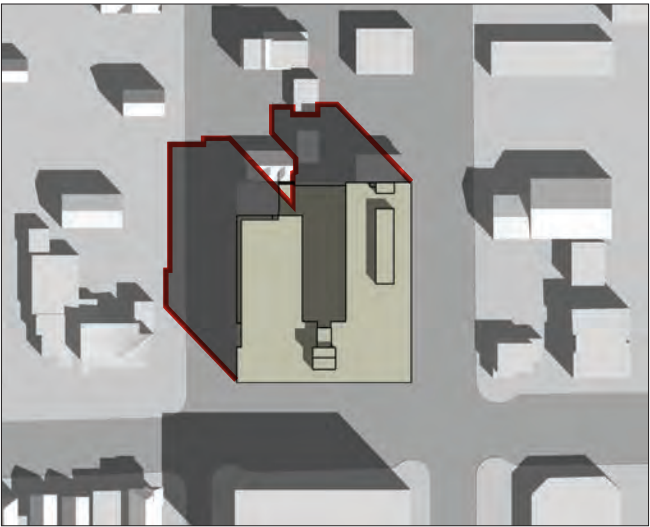


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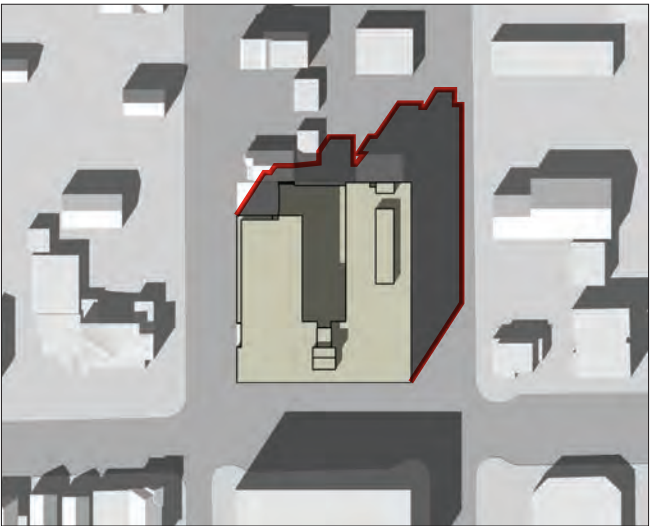
March/September 21



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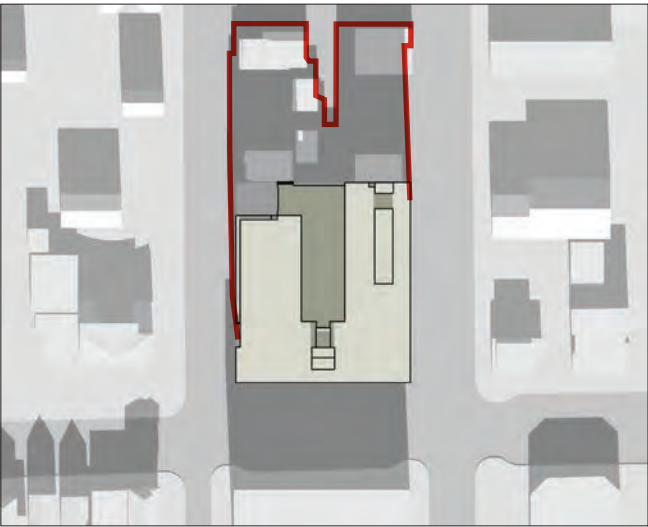


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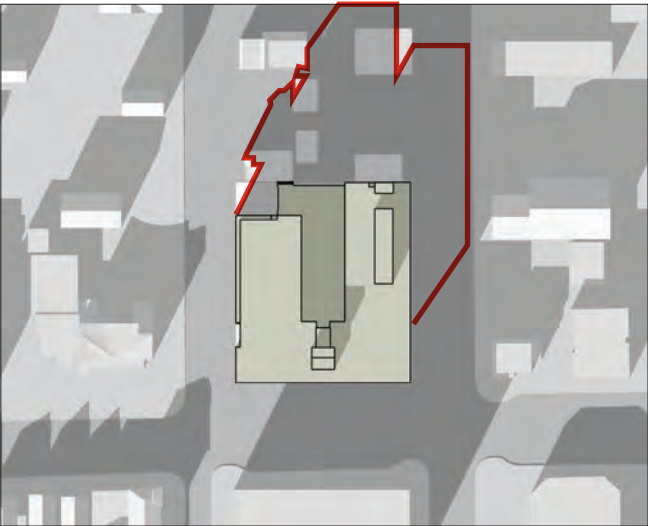
December 21



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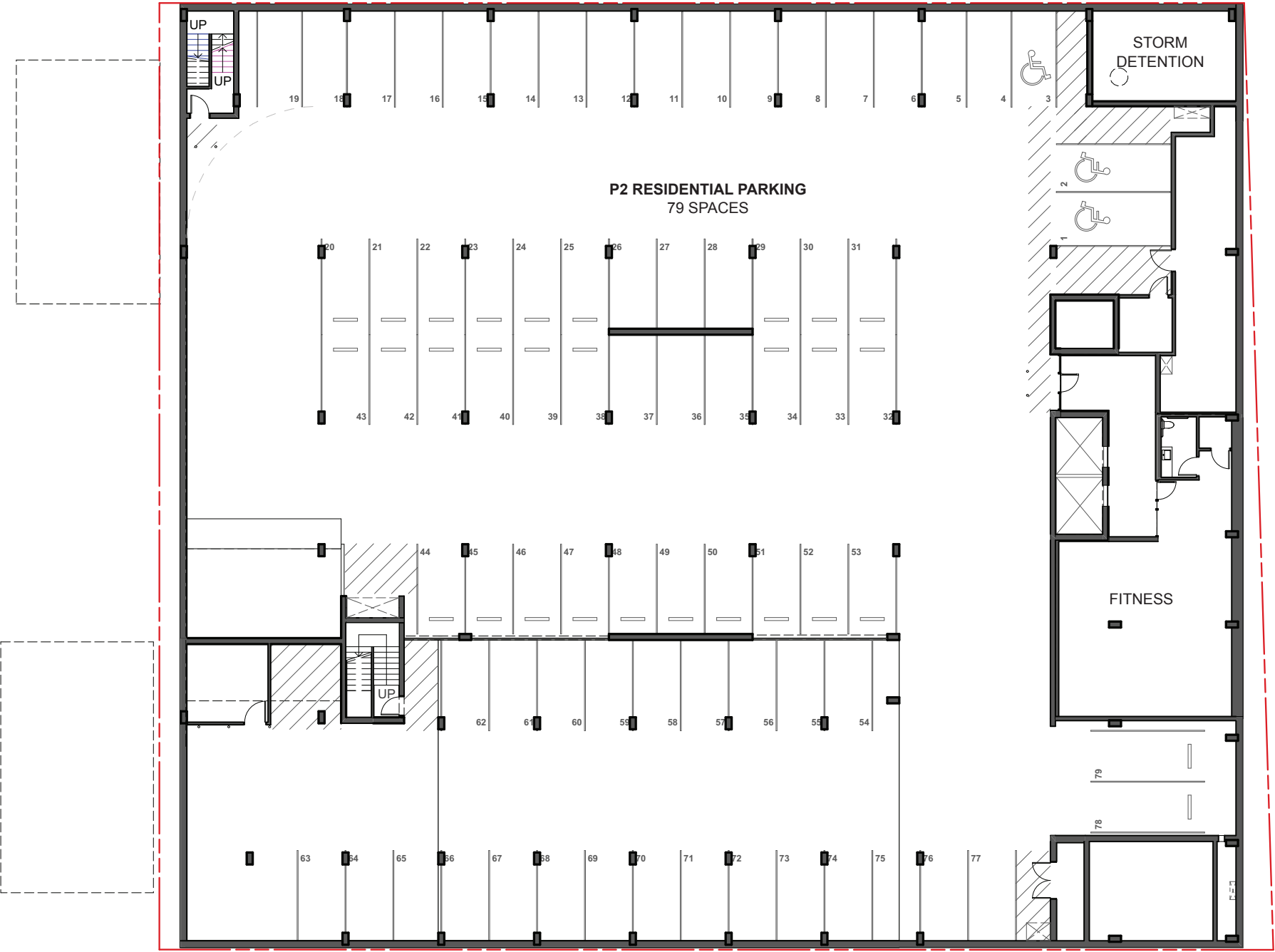
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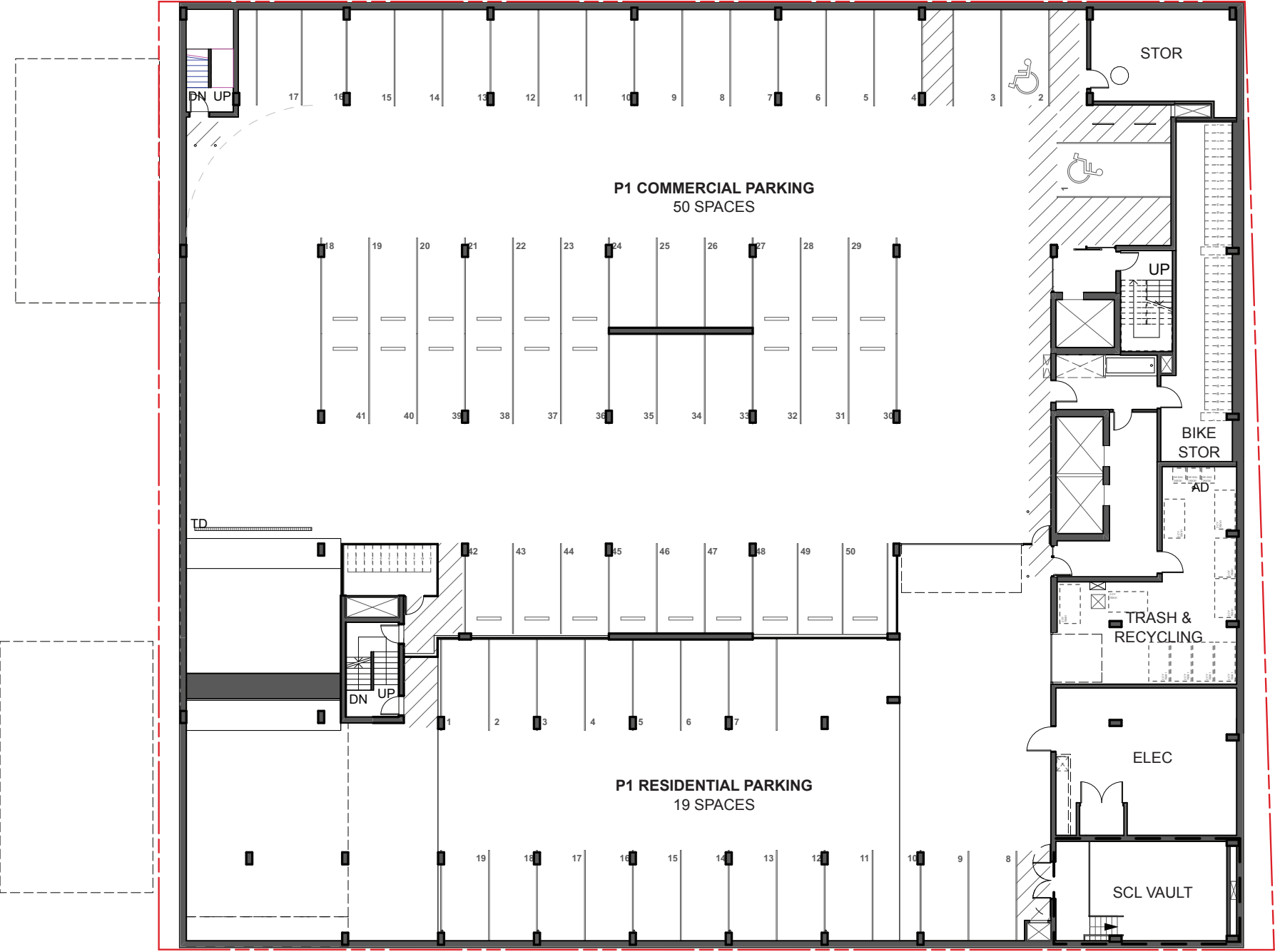
For the purpose of assessing the potential shadows created by the project, a series of shadow mappings were generated using a model of the project's vicinity. The model is based on GIS data available from the City of Seattle to portray the topography and building massing (building footprint and height). The resulting model provides a reasonably accurate representation of the proposed project site and its surroundings.

The model was used to simulate the sun angles based on the latitude and longitude of the project site (latitude 47°36'48"N, longitude 122°18'11"W). The analysis evaluates the shadows generated by the proposed project according to the City of Seattle's Department of Planning and Development's "Design Review: Early Design Guidance Proposal Packet Requirements" for a graphic shadow analysis, which requests graphical analysis of four representative days of the year: March 21st and September 21st, the equinoxes; June 21st, the summer solstice and the longest day of the year; December 21st, the winter solstice and the shortest day of the year. Consistent with the requirements, shadows were generated for the above dates at three times through the course of a day (10am, 12 noon, and 2pm).

The model generates shadows for the proposed building and the surrounding neighborhood context. The areas newly in shadow as a result of the proposed project are outlined in red to distinguish those areas from the existing shadow condition.

LEVEL P2 FLOOR PLAN





REPRESENTATIVE PROJECTS

Weinstein A+U is recognized as one of the Northwest’s leading design firms and has continually demonstrated design excellence on a broad array of projects for State, City, Federal, private, and not-for-profit clients. We are passionate about our city and the shaping of its urban neighborhoods through the integration of architecture and urban design is central to our practice.

Well-designed and thoughtful urban housing is a special concern of ours, and we have worked aggressively to advance the expectations of mixed-use projects in Seattle, both technically and aesthetically. While each project presents very specific challenges, a number of recurring themes inform much of our work and form the basis of our approach to housing design:

- All of our buildings are situational and are inseparable from their sites. They sit comfortably amongst their established neighbors, drawing from established precedents while looking to the future
- Well-designed unit plans are essential to the success of a housing project. While the functionality of each unit type is important, the organization of units across a floor plate and their influence on building elevations is equally important
- Appropriately located and proportioned open space is a significant design determinant for most mixed-use and urban housing projects
- We avoid arbitrary façade embellishment. Instead we utilize the organization of individual units and their aggregation to establish the pattern and rhythm of multi-family facades that is furthered informed by site organization and orientation. Plans correlate to elevations and variation occurs within an established system
- The constrained budgets for typical mixed-use projects demand careful consideration of a project’s primary orientation and configuration to provide cost effective sustainable design strategies
- The scale and proportion of new mixed-use buildings must address, but need not directly reflect, those of adjacent structures. Plan, section, and elevation strategies should be integrated to provide a comprehensible “read” of the building’s composition and organization



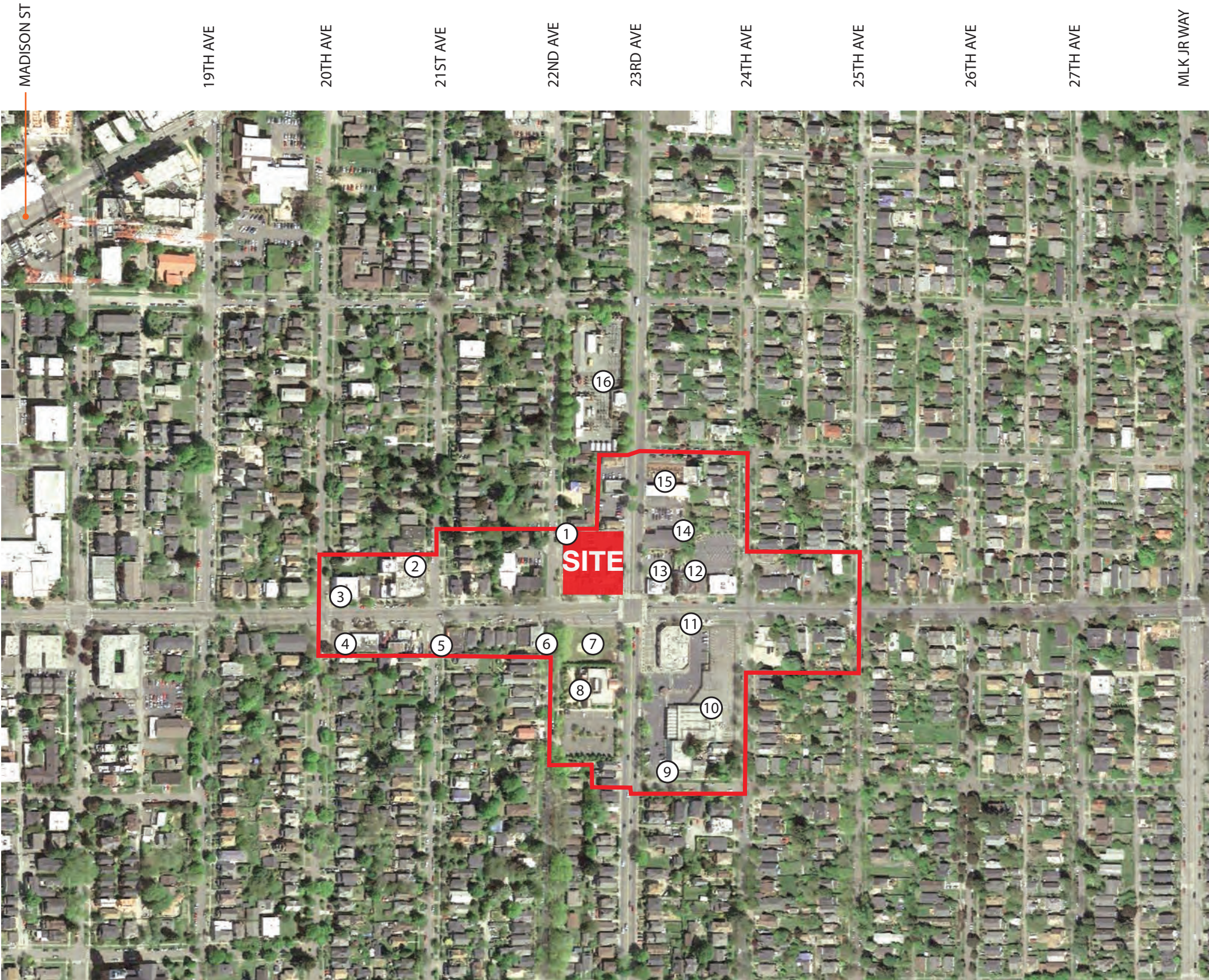
- 1 **Agnes Lofts**, 1433 12th Avenue
- 2 **19th and Mercer Mixed-Use Building**, 526 19th Avenue E
- 3 **2026 E Madison Mixed-Use Building**, (unbuilt)
- 4 **The Rooster Mixed-Use Building**, 900 NE 65th Street (under construction)
- 5 **Ventana at the Market**, 2100 Western Ave
- 6 **SCCA Patient House**, 207 Pontius Ave N
- 7 **Compass Center Housing**, 1753 NW 56th Street
- 8 **Belroy Apartments**, 703 Bellevue Ave E
- 9 **Banner Building**, 2600 Western Avenue



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ZONING DATA

PRELIMINARY SEATTLE ZONING CODE ANALYSIS		
PARCEL NO	7228500570, 7228500571, 7228500585	
LOT AREA	29,044-SF (0.67 ACRES)	
ZONING	Proposal is a contract rezone to NC2P-65/NC2-65	
PERMITTED USES	<ul style="list-style-type: none">• Retail sales and services, Multi-purpose (Grocery Store) ≤50,000-SF• Restaurant, entertainment, offices ≤25,000-SF (as conditional use)• Residential uses permitted outright	23.47A.005.C
STREET LEVEL STANDARDS	<ul style="list-style-type: none">• Non-residential street level frontage to comply with blank façade provisions.• Transparency requirements modified to apply to façade between 2 and 8 feet above the sidewalk.• Street level nonresidential required to have 13-ft floor to ceiling height• Min of 80% of the width of street-level street-facing facade on principal pedestrian street shall be occupied by uses listed in 23.47A.005D.1• Width of driveway (not to exceed 22'()) may be subtracted from the width of the street-facing facade.• At least one street-level street-facing facade containing residential use shall have a visually prominent pedestrian entry.• Dwelling units along the street-level street facing facade shall be min. 4' above or 4' below sidewalk grade, or set back min. 10' from sidewalk.	
STRUCTURE HEIGHT	<ul style="list-style-type: none">• 65' as zoned.• Open railings, planters, skylights, clerestories, greenhouses, solariums, parapets and firewalls permitted up to 4' above height limit• Mech. equipment, Stair and elevator penthouses allowed to extend up to 15' above height limit, total coverage of features 25% of roof area.	
FAR	<ul style="list-style-type: none">• Total FAR for any single use within a mixed-use building 4.25• Total FAR for mixed use building 4.75• Gross floor area below grade is not chargeable against allowable FAR• Portions of a story no more than 4' above existing or finished grade, whichever is lower, excluding access	123,437-SF Max. Area for Single-Use 137,959-SF Max. Area for Mixed-Use Below-Grade parking exempted
SETBACKS	<ul style="list-style-type: none">• 15' triangular setback where a lot abuts the intersection of a side lot line and front lot line of a lot in a residential zone.• Side or rear setback when abutting residentially-zoned lot: 15' for structures above 13' in height up to 40' with an additional setback of 2' for every 10' above 40'• No entrance, window, or opening permitted within 5' of an abutting residentially-zoned lot• Fences, freestanding walls and other similar structures ≤6' in height above grade permitted in required setbacks along segment of the fence, but in no case may any of the fence exceed 8 feet.	
PARKING	<ul style="list-style-type: none">• No minimum parking required for Urban Center Village• Bike parking<ul style="list-style-type: none">- Eating and drinking: long term: 1 per 12,000 sf; short term: 1 per 4,000 sf- Sales and Service: long term: 1 per 12,000 sf; short term: 1 per 4,000 sf- Residential: long term: 1 per 4 dwelling units; short term: none	
AMENITY AREA	<ul style="list-style-type: none">• Amenity spaces equivalent to 5% of residential gross floor area required for residential uses<ul style="list-style-type: none">- Common amenity area: min area 250-sf; 10' min horizontal dimension- Private balconies: min area 60-sf; 6' min horizontal dimension- All residents must access at a common or private amenity area	
PARKING ACCESS	<ul style="list-style-type: none">• If no alley and lot abuts two or more streets, access permitted across one of the side street lot lines per 23.47A.032.C; curb cuts permitted per 23.54.030.F.2.A.1• Access to parking shall be from a street that is not a principal pedestrian street	22nd Avenue is only street not designated a principal pedestrian street per 23.47A.005.D.2
LOADING	<ul style="list-style-type: none">• For uses with <16,000 sf that provides a loading space on a street or alley, the loading berth reqs. may be waived by director if SDOT finds that the street or alley berth is adequate.• Medium demand, 10,000 sf – 60,000 sf: 1 loading berth required<ul style="list-style-type: none">- Width and clearance: 10' wide and 4' vertical clearance- Minimum lenght: 35' long	



Location

The proposed project is in Seattle’s Central Area neighborhood on the northwest corner of the intersection between 23rd Avenue and E Union Street. The site is located in the 23rd&Union-Jackson Residential Urban Village and falls within the Union Core, the northernmost of the three community nodes identified the 23rd Avenue Action Plan Urban Design Framework (shown outlined in red).

Existing Uses

The project site is currently occupied by three separate uses corresponding to the three existing parcels. The largest parcel of the three parcels is home to a Union 76 gas station (KCA # 7228500570). The gas station includes a single-story pre-fabricated steel building oriented parallel to 23rd Avenue. The structure is roughly centered north-to-south and located about 20-feet off the parcel’s western (mid-block) property line. A covered fuel island is located between the station building and 23rd Avenue with access to the station provided by large curb cuts along both 23rd Avenue and E Union Street. Twelve surface parking spaces are located adjacent to the station building and along the north property line.

The larger of the two parcels along 22nd Avenue is occupied by a single-story wood-framed building that is home to Cappy’s Boxing Gym and Seattle Kajukenbo & Kung Fu Kids (KCA # 7228500571). The building nearly covers the entire parcel save for the northern 7-feet, which is enclosed by a fence and appears to be used for trash storage. No parking is located on-site.

- 1. Richland Minimart & Gas Station, Cappy’s Boxing Gym, Pea Patch
- 2. Central Cinema, Hollow Earth Radio, Rare Medium (camera store), 20/20 Bicycle Shop
- 3. Katy’s Corner Cafe
- 4. Chucks Hop Shop
- 5. Union Market
- 6. Adey Abeba Eithiopian Restaurant
- 7. (under construction)
- 8. Casey Family Program (child services)
- 9. Midtown Coin Laundry
- 10. USPS (not in service)
- 11. Liquor Store, Earl’s Cuts & Style, Chesterfield Pharmacy
- 12. Neighbor Lady (bar)
- 13. Uncle Ike’s (retail marijuana shop)
- 14. Mt. Calvary Church
- 15. Seasuds Carwash
- 16. Power Substation

EXISTING SITE ANALYSIS

Topography

- Gently sloping down north-to-south with a change of approximately 2-feet
- Relatively flat east-to-west with a half of a foot change in elevation

Neighboring Buildings

- Two-story single-family house (1418 22nd Avenue) to the north
- Two-story duplex house (1419 23rd Avenue) located to the north
- Two-story Lutheran Church of the Good Shepard (2116 E Union Street) located across 22nd Avenue to the west
- Six-story mixed-use building (The Central) under construction across E Union Street to the south
- Single-story commercial buildings to the east across 23rd Avenue (Mid Town Center, Uncle Ike's)
- Single-story Mount Calvary Christian Center across 23rd Avenue (1412 23rd Avenue)

Solar Access

- Excellent light access throughout the year

View Access

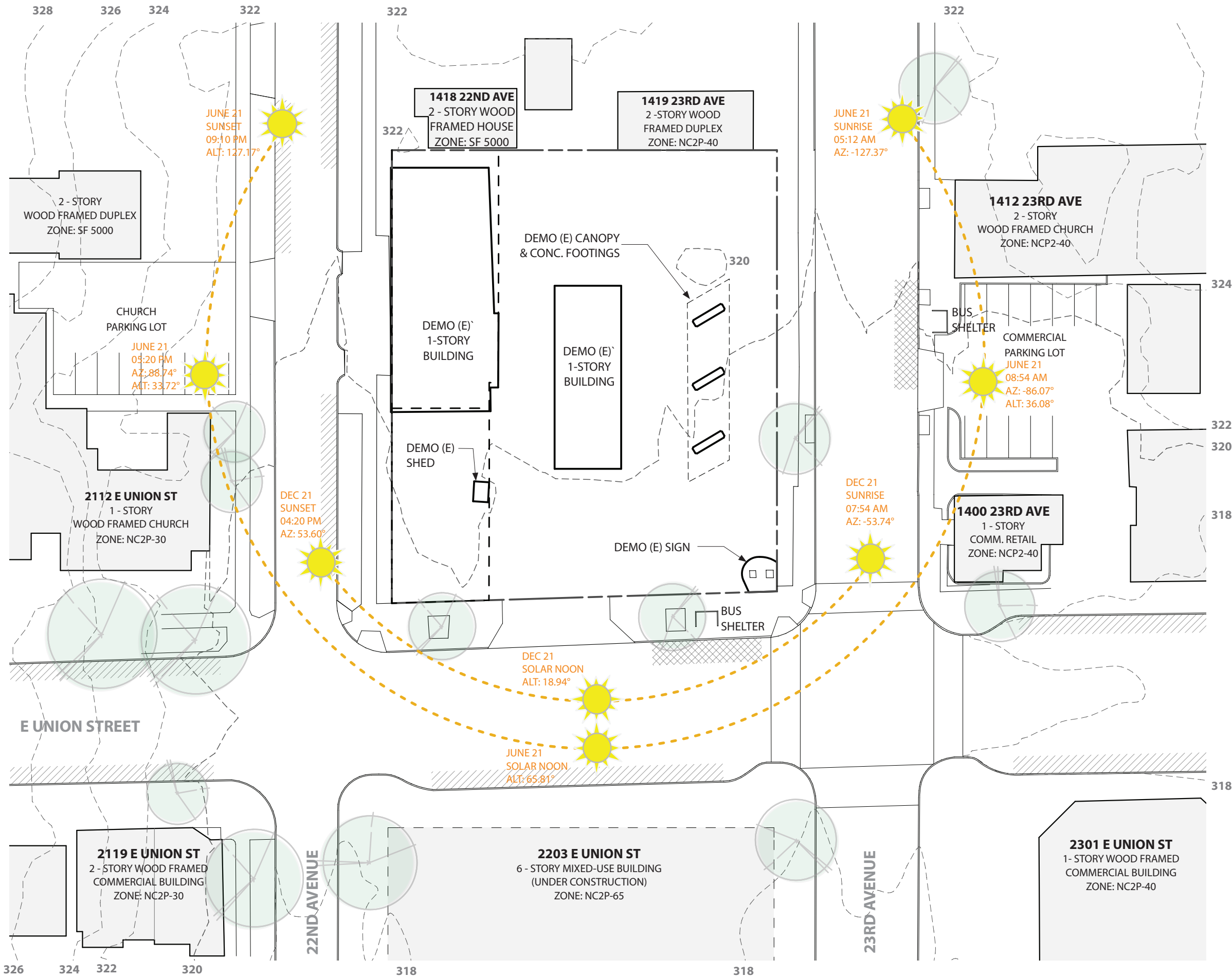
- Regional view potential to east and southeast
- Territorial views to west and south

Structure Height

- As currently zoned, the NC2-40 zone has a base height limit of 40-feet. The proposed Contract Rezone seeks to change the zoning to NC2-65 which would increase the height limit to 65-feet
- Average grade plane elevation is +320.17' resulting in a maximum elevation of +385.17'

Allowable Building Area

- As currently zoned, the maximum allowable FAR for a qualifying mixed-use building in an NC2-40 zone is 3.25. The proposed Contract Rezone seeks to change the zoning to NC2-65, which would increase the allowable FAR for a mixed-use building to 4.75
- The aggregate lot area of the proposed site is 29,044 sf
- The allowable gross area of a mixed-use building under the site's current zoning is 94,393 sf. The proposed Contract Rezone would change the allowable gross area to 137,959 sf.
- All gross floor area located underground and portions of a story that extend no more than 4-feet above grade are exempt from FAR limitations



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EDG CONCEPTS: COMPARISON



Alternate 1

Pros

- No Departure Required
- Building creates strong commercial street-level uses along 23rd Ave and E Union St.
- Residential units entry from courtyard

Cons

- No dedicated commercial parking garage at the street level
- Tall building mass close to the single-family-zoned lot to the North.
- Proximity to the non-conforming house on the lot situated at the property line, more significant impact than typically the case

Summary

Stories: 6 (5-over-1 + 1 below grade)
Unit Count: 146
Parking:
 Residential: 80 spaces
 Commercial: None
Ground Floor Uses:
 23rd Avenue: Retail, E Union St: Retail
 22nd Avenue: Residential + Parking Access
Potential Departures:
 None



Alternate 2

Pros

- Separate non residential parking garage
- Additional space between the proposed building and the adjacent residential lot
- Round-the clock use along E Union St.
- Two-story street-level residential units on the 22nd Avenue side of the building provide a transition in scale and use that is appropriate for the more residential character of that street.

Cons

- Parking access is located to the north end of the site along 22nd Avenue with separate access to a street-level commercial garage and a single below-grade residential parking level.

Summary

Stories: 6 (5-over-1 + 1 below grade)
Unit Count: 146
Parking:
 Residential: 88 spaces
 Commercial: 24 spaces
Ground Floor Uses:
 23rd Avenue: Retail, E Union St: Retail + Residential Entry
 22nd Avenue: Residential + Parking Access
Potential Departures:

- Residential uses at street level
- Triangular setback abutting a residential lot
- Side setback above 13 ft.
- Openings adjacent to a residential lot
- Non-residential driveway width
- Parking space size distribution
- Sight triangles



Alternate 3 (Preferred)

Pros

- Round-the clock use along E Union St.
- Additional space between the proposed building and the adjacent residential lot
- Two-story street-level residential units on the 22nd Avenue side of the building provide a transition in scale and use that is appropriate for the more residential character of that street.
- Parking access to the commercial and residential garages is separated

Cons

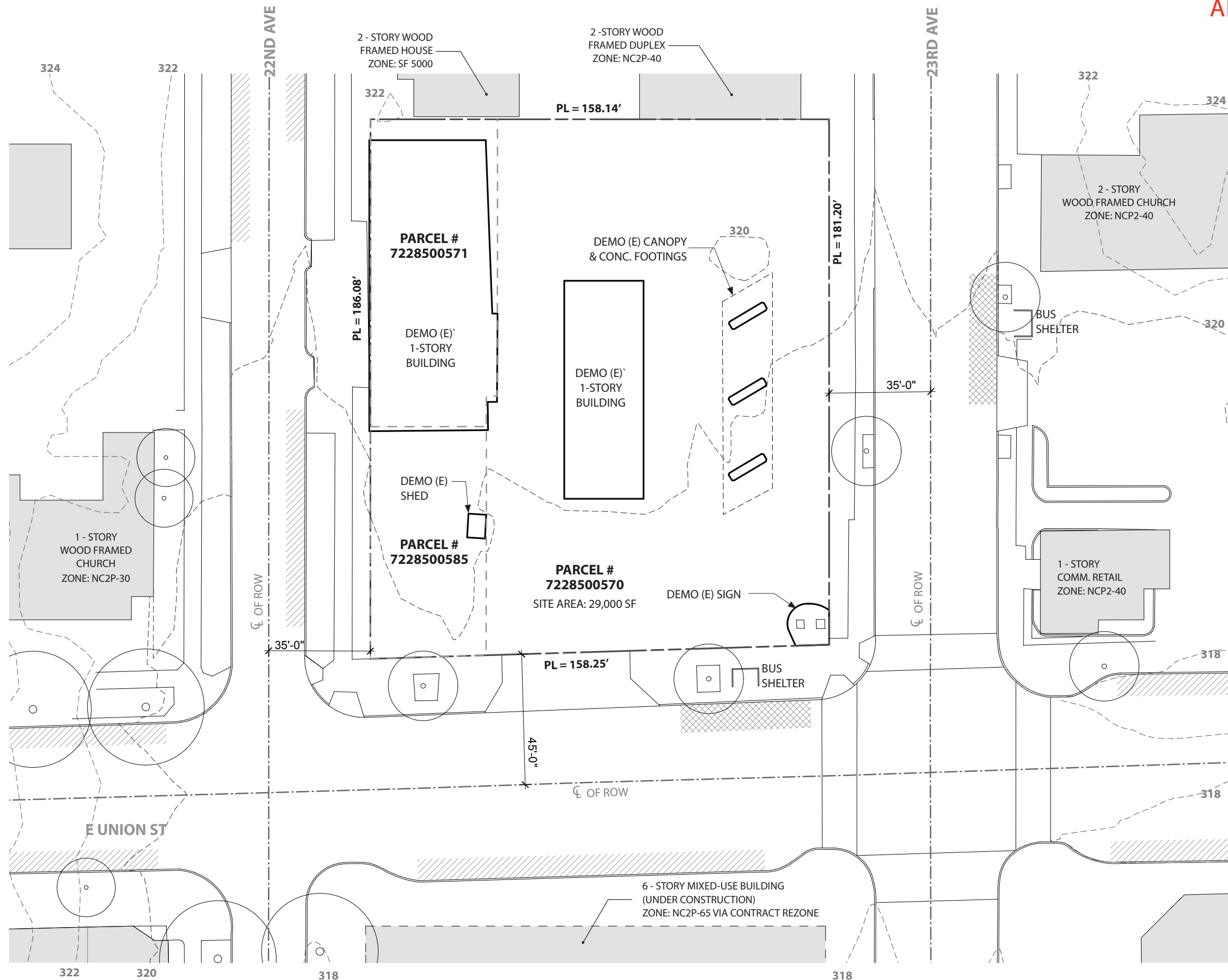
- Parking access is located to the north end of the site along 22nd Avenue with separate access to a street-level commercial garage and a single below-grade residential parking level

Summary

Stories: 6 (5-over-1 + 1 below grade)
Unit Count: 146
Parking:
 Residential: 88 spaces
 Commercial: 24 spaces
Ground Floor Uses:
 23rd Avenue: Retail, E Union St: Retail
 22nd Avenue: Residential + Parking Access
Potential Departures:

- Residential uses at street level
- Triangular setback abutting a residential lot
- Side setback above 13 ft.
- Openings adjacent to a residential lot
- Non-residential driveway width
- Parking space size distribution
- Sight triangles

APPENDIX - EXISTING SITE PLAN



The smallest of the parcels is located at the intersection of 22nd Avenue and E Union Street and is currently occupied by Alleycat Acres, a neighborhood run urban farm (KCA #7228500585). The farm is bounded by modest rockeries along both of its street frontages and low fence along its east property line. One small wood framed shed is the only structure located on site.

The proposed project will demolish the existing structures and paved surfaces located on the project site. The proposed building footprint would occupy nearly the entirety of the site.

The site's frontage along 23rd Avenue is 181' in length. It currently has a sixteen-foot wide sidewalk along the entire length interrupted by two 28' wide curb cuts at the north side of the site providing access to the gas station. A lone street tree (12" caliper maple) is located at about 70 feet north of the south property line. No overhead power lines or bus trolley lines are present along this portion of 23rd Avenue.

The E Union Street frontage is 158' in length with a large, 45' wide driveway roughly centered on the site and providing access to the gas station. A 19'-20' wide sidewalk runs the length of the block with a Metro bus shelter located just beyond the intersection with 23rd Avenue. Two street trees (14" and 10" deciduous) are located toward the east and west ends of the property frontage.

The 22nd Avenue street frontage is 186' in length with two small abandoned curb cuts located in front of the northern parcel. A six-foot sidewalk is present for the entire length with generous planting strips located either side. No street trees are located along the property frontage.

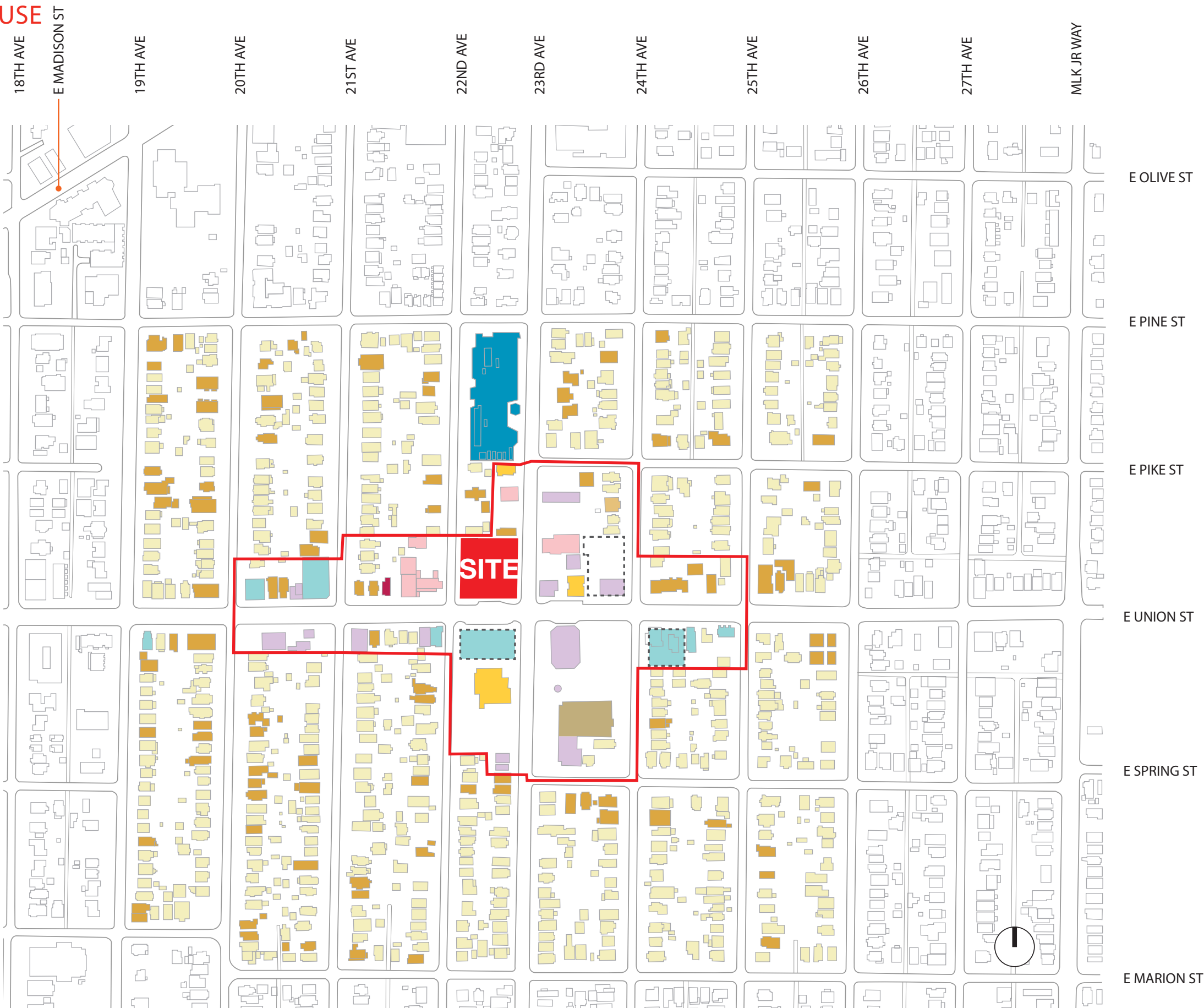
The topography of the site is very benign with elevation changes of just over two-feet along the 23rd Avenue and 22nd Avenue frontages (dropping from north [el. $\pm 321'$] to south [el. $\pm 319'$], a slope of 1.2%), a change of less than one foot along the E Union Street frontage (climbing east [el. $\pm 318.8'$] to the west [el. $\pm 319.31'$], a slope of 0.3%), and a half-foot difference along the north property line (a slight rise from east [el. $\pm 321'$] to west [el. $\pm 321.5'$], a slope of 0.3%).

APPENDIX - SITE CONTEXT: LAND USE

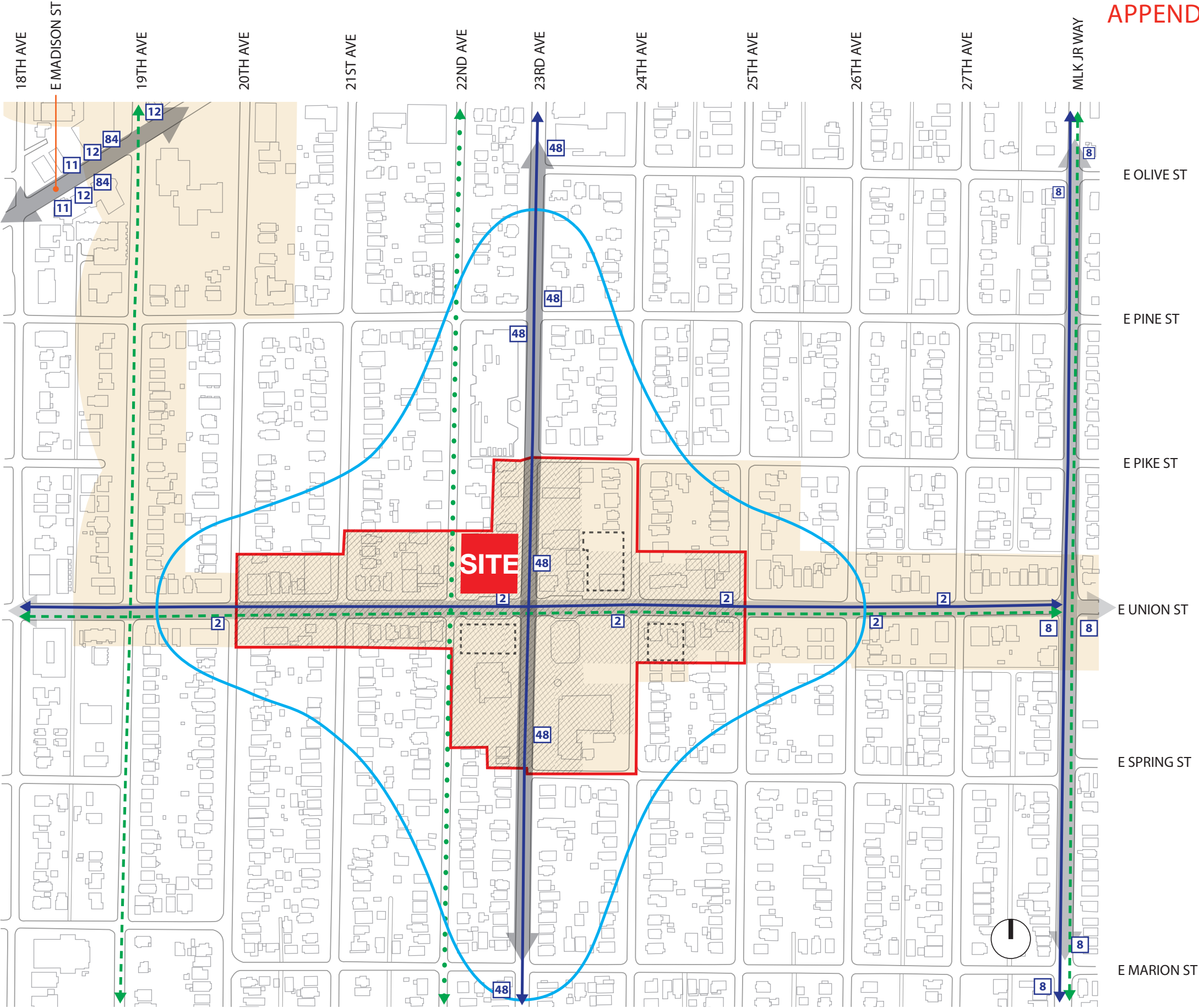
The site is bounded by public rights-of-way to the east (23rd Avenue), south (E Union Street), and west (22nd Avenue). The north property line abuts two properties—a two-story single-family house on 22nd Avenue (zoned SF5000) and a two-story duplex on 23rd Avenue (zoned NC2P-40). One and two-story buildings primarily occupy the properties on the blocks immediately surrounding the project site. A Lutheran church and associated surface parking lot are located opposite the site along 22nd Avenue (zoned NC2P-30). A small two-story mixed-use building located to the southwest of the site on the opposite side of the intersection of 22nd Avenue and E Union Street contains an Ethiopian restaurant at street-level and apartments above (zoned NC2P-30). A new six-story mixed-use building, The Central, is currently under construction to the south of the site across E Union Street (zoned NC2P-65 via a Contract Rezone granted in July 2008). The Midtown Center retail center (zoned NC2P-40) is located to the southeast of the site on the opposite side of the intersection of 23rd Avenue and E Union Street. And the retailer, Uncle Ike’s, and church, Mount Calvary, are located to the east of the site across 23rd Avenue (both zoned NC2P-40).

Legend

- 23rd Avenue UDF Union Core
- Single Family Residential
- Multi Family Residential
- Mixed Use
- Retail
- Office
- Religious
- School / Institutional
- Utility



APPENDIX - SITE CONTEXT: TRANSIT MODES



The project site is served by two King County Metro bus lines—the #2 and the #48—providing connections west to Downtown, south to Mt. Baker and north to the University District and Ballard. The entirety of the project site falls within the Frequent Transit Corridor designation shown shaded to the left.

Existing bike lanes along E Union Street and MLK Jr Way provide modal choices to the neighborhood and will be supplemented by the Central Area Greenway soon to be implemented along 22nd Avenue.

Due to the intended commercial focus of the Union Core, most of the properties in the immediate vicinity of the project site fall within a Pedestrian Overlay. The relative flatness of the neighborhood promotes walkability with several largely residential blocks located within a five minute walking distance of the project site.

Legend

Main Car Arterials:

- Principal Arterial / Regional Connector
- Minor Arterial / Commercial Connector

23rd Avenue UDF Union Core

Frequent Transit Corridors

Pedestrian Overlay

5 Minute Pedestrian Walkshed

Bus Routes

Bus Stops

Current Bicycle Routes

Planned Greenway & Bicycle Route

APPENDIX - CONTEXT: EXISTING ZONING

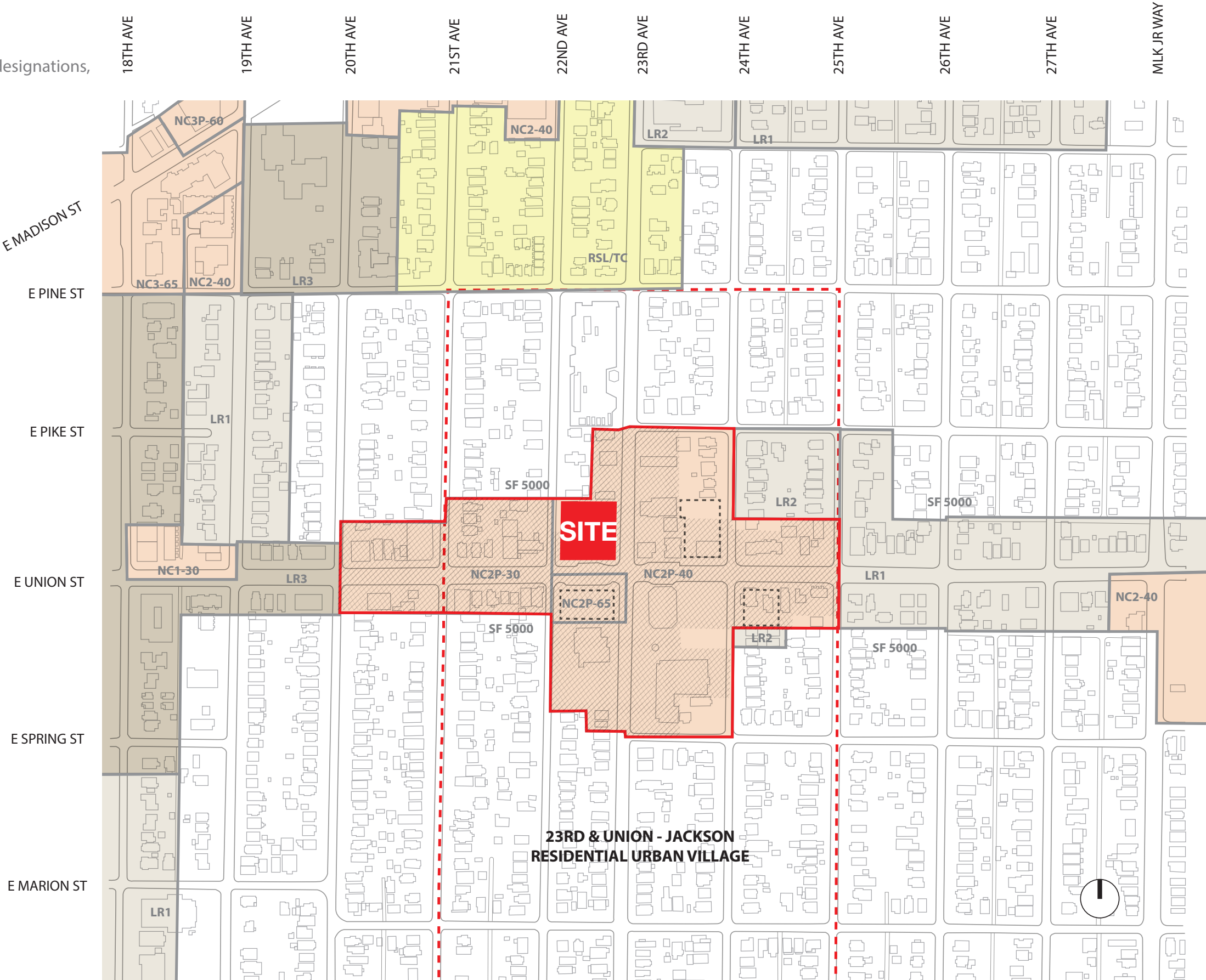
Please indicate the sites zoning and any other overlay designations, including applicable Neighborhood Design Guidelines.

As previously indicated, the proposed site is located within the 23rd&Union-Jackson Residential Urban Village. All three parcels are zoned NC2-40, with the two parcels abutting 23rd Avenue and E Union Street located within the pedestrian overlay and all parcels fall within a Frequent Transit Corridor.

The zoning of the parcels abutting the project site to the north vary with the site to the northwest being zoned SF 5000 and the site to the northeast zoned NC2P-40. To the west of the project site and across 22nd Avenue, the zoning is NC2P-30. The parcels located to the east of the site across 23rd Avenue are zoned NC2P-40. Lastly, the parcel to the south of the site across E Union Street is zoned NC2P-65.

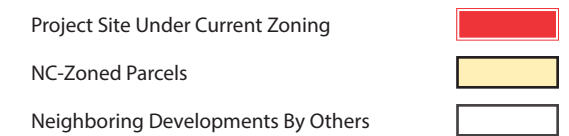
Existing Zoning Legend

NC	<div></div>
LR1 & LR2	<div></div>
LR3	<div></div>
RSL/TC	<div></div>
Pedestrian Overlay	<div></div>
23rd Avenue UDF Union Core	<div></div>
23rd & Union-Jackson RUV	<div></div>
Zone Boundaries	<div></div>
Neighboring Developments by Others	<div></div>





* Zoning Heights Shown As Approximation



APPENDIX - CONTEXT: UDF RECOMMENDED ZONING

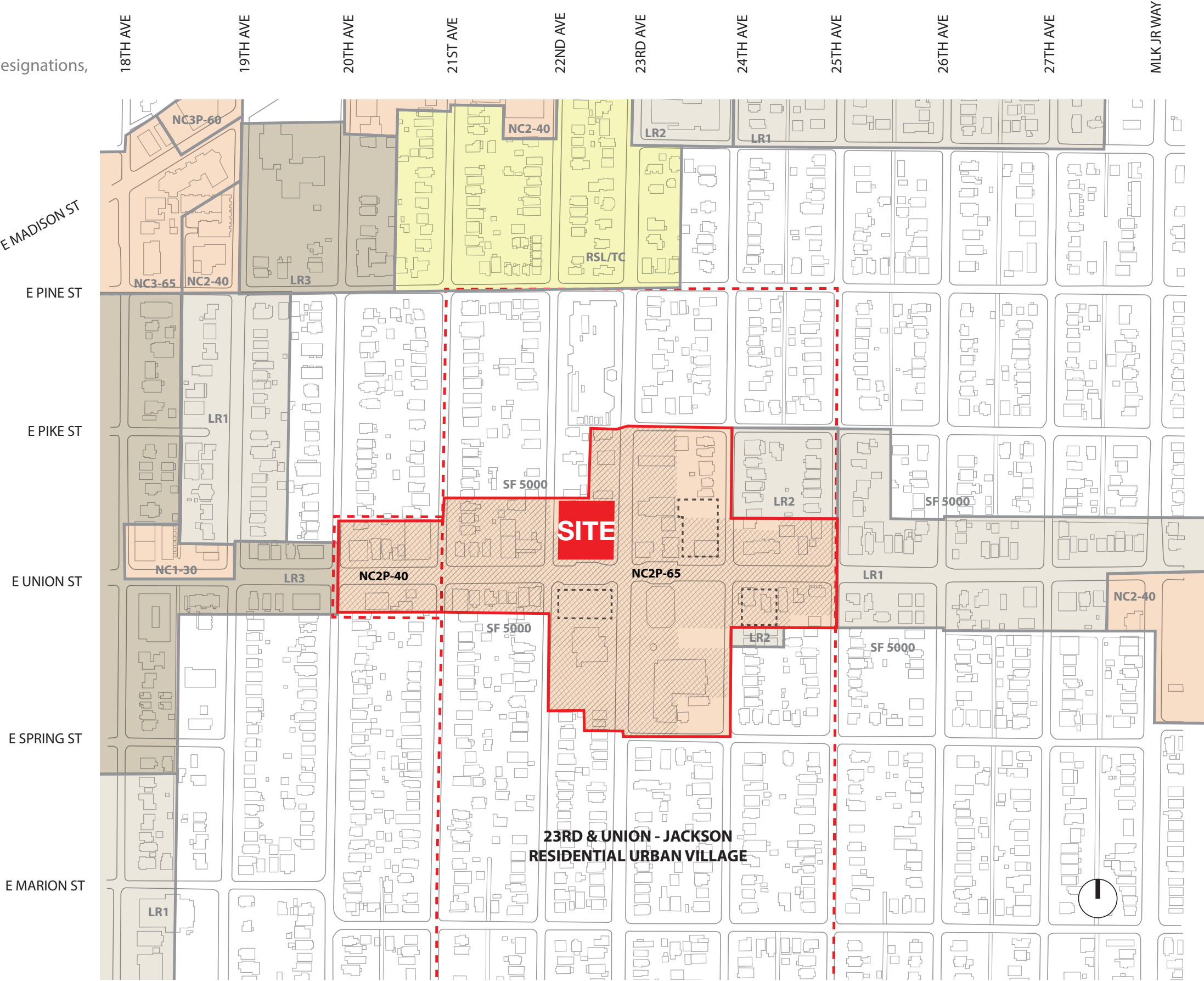
Please indicate the sites zoning and any other overlay designations, including applicable Neighborhood Design Guidelines.

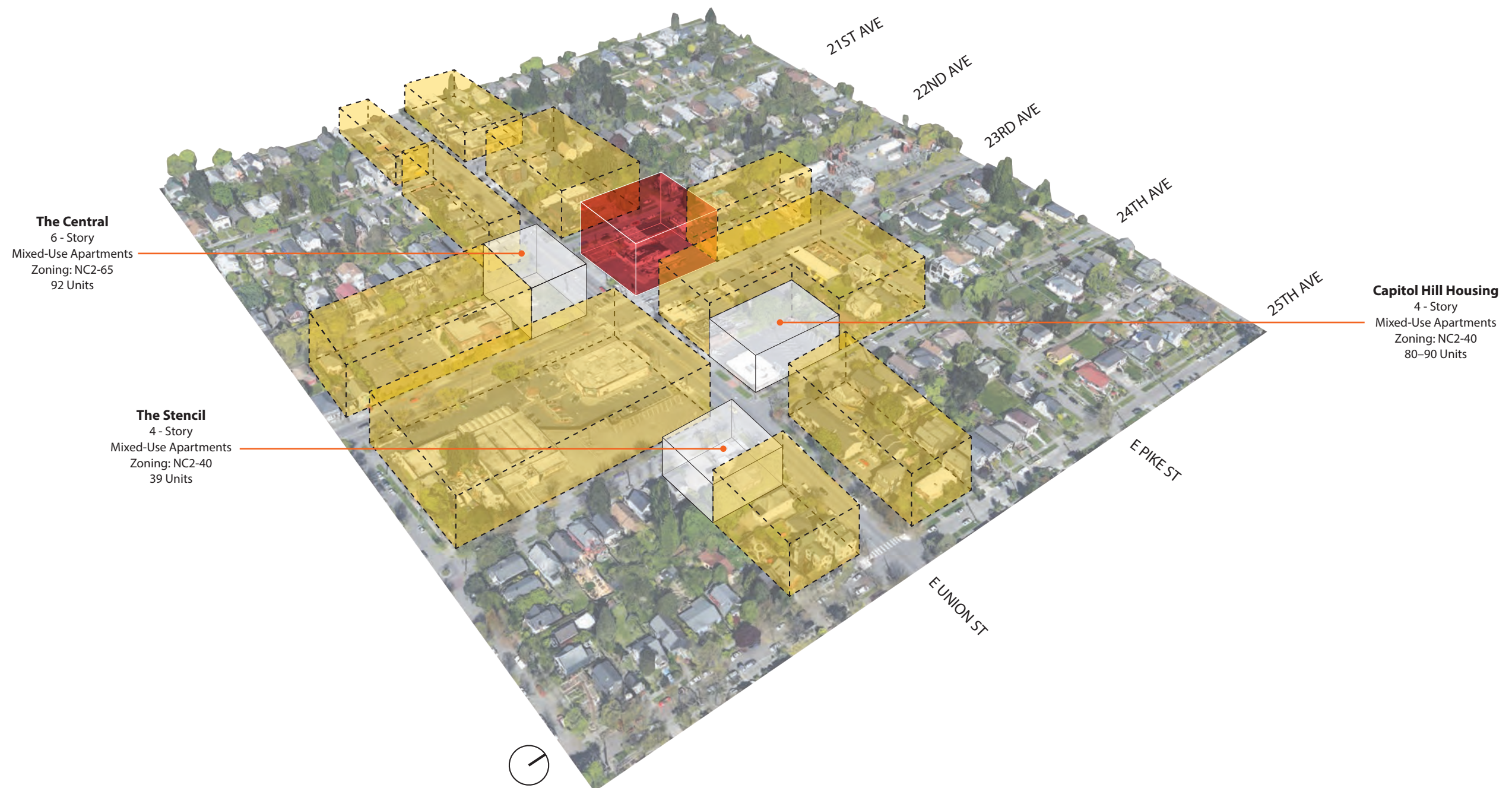
Consistent with the 23rd Avenue UDF, but occurring on a more predictable timeline, the design proposal includes a Contract Rezone of the site to change the allowable height from forty-feet (NC2-40) to sixty-five feet (NC2-65). The increase in allowable height also increases the maximum development density for a mixed-use building from an FAR of 3.25 to an FAR of 4.75, but does not change the underlying limitations on commercial uses in NC2 zones. The existing Pedestrian Overlay would remain in place.

The 23rd Avenue UDF recommends rezoning the parcels within the 23rd Avenue UDF Union Core. The SF 5000 zoned parcel abutting the northwest corner of the site is not within the 23rd Avenue UDF Union Core and is not proposed to change. The parcels to the east across 23rd Avenue and to the west across 22nd Avenue are recommended to be rezoned to NC2P-65. The parcel across E Union Street and to the south of the site is already zoned NC2P-65.

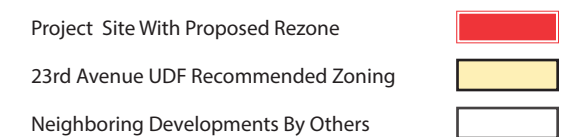
23rd Avenue UDF Rezone Recommendations

NC	<div></div>
LR1 & LR2	<div></div>
LR3	<div></div>
RSL/TC	<div></div>
Pedestrian Overlay	<div></div>
23rd Avenue UDF Union Core	<div></div>
23rd & Union-Jackson RUV	<div></div>
Zone Boundaries	<div></div>
Neighboring Developments by Others	<div></div>

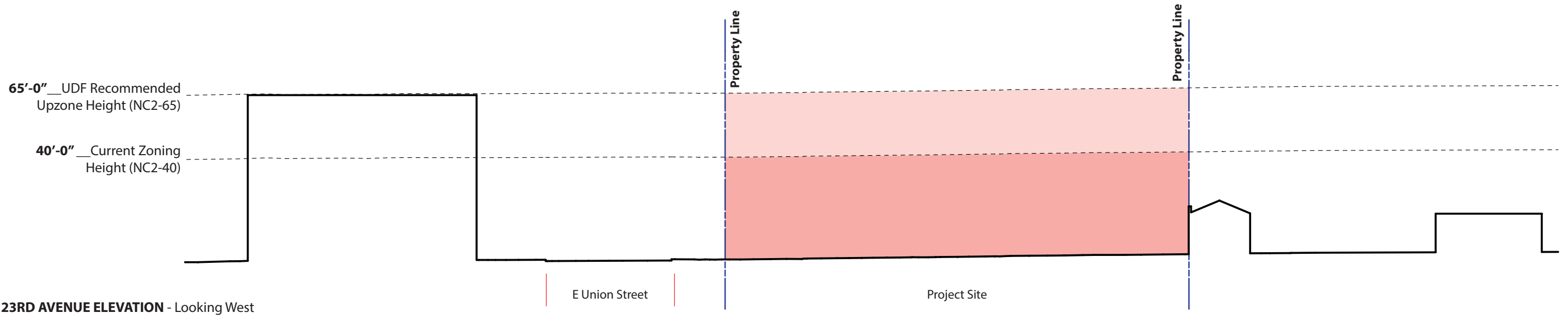
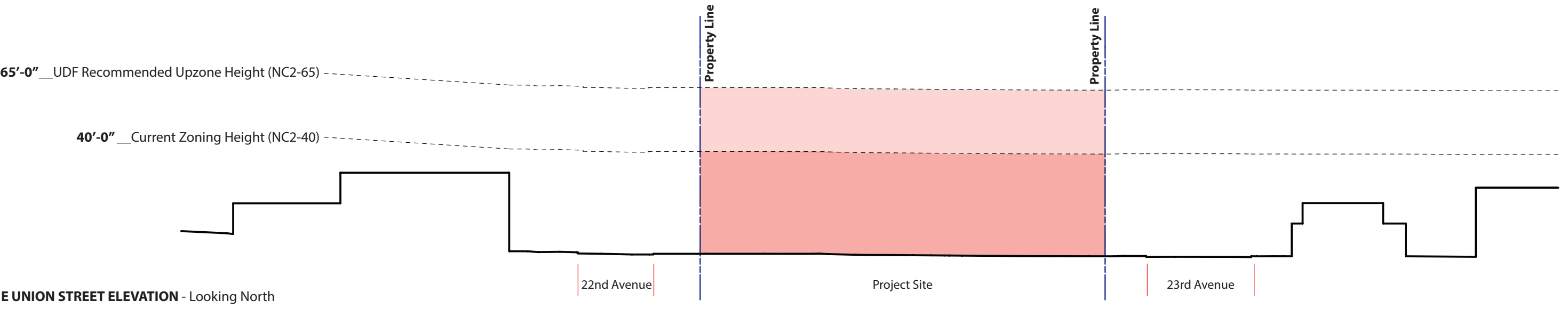
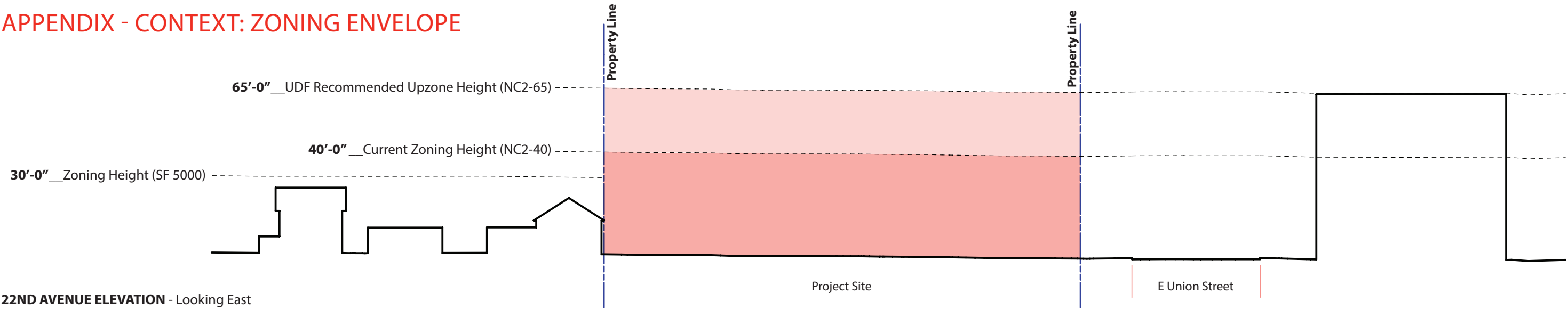




* Zoning Heights Shown As Approximation



APPENDIX - CONTEXT: ZONING ENVELOPE





E UNION STREET

PROJECT SITE

23rd Avenue forms the eastern edge of the site and is one of the neighborhood's primary north-south connections. The street's topography is in the vicinity of the site nearly flat with a slight rise from south to north of just over one percent. Zoning of the properties along 23rd Avenue match the site's Neighborhood Commercial zoning and are also part of the Pedestrian Overlay (NC2-40P). Due to the street's width, traffic volumes, and it's easy connections with neighborhoods to the north and south, 23rd Avenue is a largely auto-oriented commercial thoroughfare.

The neighborhood's automobile orientation has influenced its existing built form in stark contrast to the adjacent residential streets. Small one and two-story buildings are located on the commercially zone lots to the north and south of the site along either side of 23rd Avenue. Surface parking lots occur with regularity either between neighboring buildings or between the building and it's street lot line. The currently planned (and soon to start) 23rd Avenue Corridor Complete Streets Project intends to reconfigure of the right-of-way along 23rd Avenue to changed the character of the street (reducing it from four lanes to three) and provide a more conducive environment for public transit, bicyclists and pedestrians.

The buildings located on the west side of 23rd Avenue and immediately proximate to the site house social service organizations focused on disadvantaged youths and foster children. These organizations include a teen center run by Mount Calvary Christian Center, located in a single-story commercial building and unclear if still active; the Friends of the Children King County, located in a small two-story single-family residence; and the Casey Family Programs Office, located in two-story office building (circa 1999). All these organizations and the intervening parking lots are zoned NC2-40.

The east side of 23rd Avenue is characterized by single-story commercial buildings, such as the Mid-Town Center, Uncle Ike's, and Sea Suds Car Wash with Mount Calvary Christian Center located in their midst. Similar to other development along 23rd Avenue, surface parking lots separate the commercial structures from one another or from the street. All these lots are zoned NC2-40. Despite the auto-oriented nature of 23rd Avenue, the properties along the street located within a Pedestrian Overlay.



5

OPPOSITE OF PROJECT SITE

E UNION ST



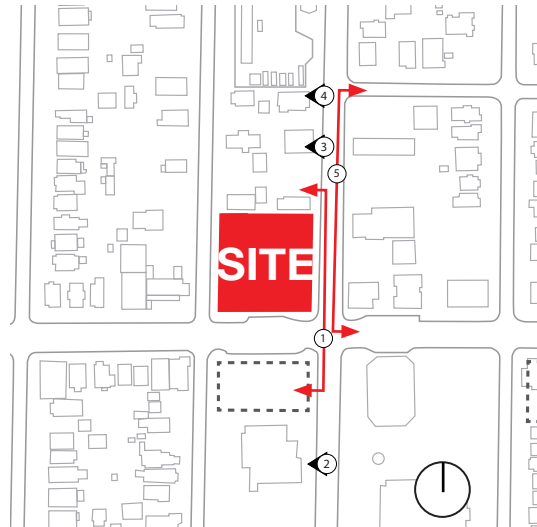
2



3



4



APPENDIX - CONTEXT: 22ND AVENUE

22nd Avenue establishes the western edge of the site and its highly residential character. Like 23rd Avenue, the street's topography is nearly flat with a slight rise from south to north of just over one percent. Zoning of the properties along 22nd Avenue vary with Neighborhood Commercial zoning occurring due west of the site transitioning to single family (SF 5000) to the north of the site on either side of the street.

South of E Union Street, the zoning is more varied, with NC2-40 (west) and NC2-65 (east) occurring at the intersection with E Union Street. Beyond the intersection the zoning changes to SF 5000 along the west side of the street and NC2-40 along the street's east side.

The street's residential character is reinforced by it's narrow dimensions (on-street parking narrows the street to a single travel lane for stretches) and similarly sized housing located tight to the sidewalk. Large sideyards and street facing porches provide continuity to the character of a street that has single-family, duplex, triplex houses, and a small apartment building.



PROJECT SITE



E UNION STREET



E UNION ST

OPPOSITE OF PROJECT SITE



APPENDIX - CONTEXT: E UNION STREET



22ND AVENUE

PROJECT SITE

23RD AVENUE

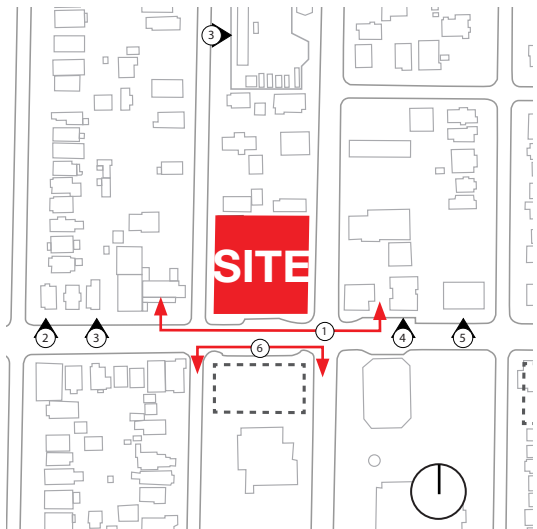
E Union Street defines the southern edge of the site and serves as an opportunity to extend the small commercial node located slightly uphill and to the west of the site. The street's topography adjacent to the site is comparatively flat, rising slightly to the west with a slope less than one-half of a percent before crossing 22nd Avenue and gaining approximately 19-feet over the next block. To the east, the street maintains its flatness until crossing 24th Avenue and steeply sloping downward.

Despite E Union Street's wide right-of-way, the street's lower traffic volumes and more convenient access to the surrounding residential neighborhoods have nurtured a small, block long neighborhood center between 20th and 21st Avenues characterized by small, two-story mixed-use buildings and featuring local businesses such as Central Cinema, 20/20 Cycle, Hollow Earth Radio, Katy's Café, Chuck's Hop Shop, and Union Market. The properties within this existing commercial center and extending east to 24th Avenue are all zoned Neighborhood Commercial (varying from NC2-30 to NC2-65) and are within the Pedestrian Overlay.



23RD AVENUE

OPPOSITE OF PROJECT SITE

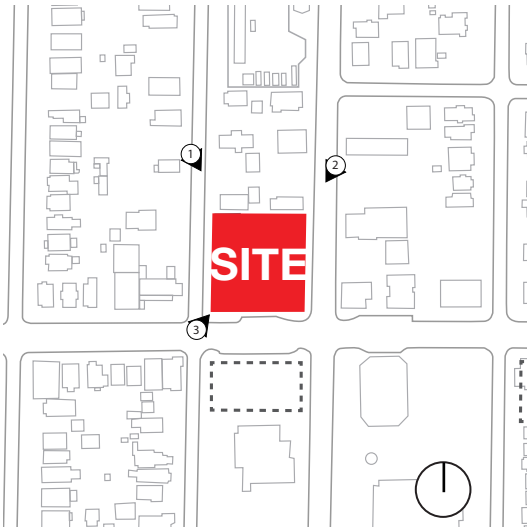


APPENDIX - CONTEXT: EXISTING SITE

As previously indicated, the project site combines three parcels measuring roughly 158-feet by 183-feet. A Union 76 gas station and mini-mart, a single-story commercial building, and an urban farm currently occupy the site with all existing structures to be demolished.

Existing curb cuts along E Union Street and 23rd Avenue provide access to the gas station, which occupies the most prominent corner of the site and is therefore the dominant feature of the site. The urban farm located at the corner of 22nd Avenue and E Union Street is dormant and unused throughout the winter months, but is transformed during the summer.

The project site affords wide streetscapes on all three frontages. As noted above, the Union 76 influences the form and use of streetscapes along E Union Street and 23rd Avenue resulting in large, relatively vacant hardscapes. The streetscape along 22nd Avenue on the other hand has generous planting strips to either side of the sidewalk.





A house and a duplex are located along the project site's north property line. Both structures are located tight to the property line with little to no setback. The two-story duplex along 23rd Avenue is located on an NC2P-40 site and has non-conforming openings facing south toward the project site at its ground level. Similarly, the location of the single-family house on 22nd Avenue is a non-conforming condition with less than a 5-foot setback from the property line. The structure also has non-conforming openings facing south toward the project site at both its ground floor and upper floor. The intervening side yard between the house and the project site is currently unused on the house property (for lack of space) and used to storage garbage containers on the project site.

