

Washington State Convention Center Addition Project: Co-Development

Design Review Board Recommendation Meeting 02
01-16-2018

	SITE B	SITE C
PROPERTY ADDRESS	920 Olive Way	1711 Boren Ave.
SDCI PROJECT #	3018096	3020177

OWNER	ARCHITECT	SDCI CONTACT
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WSCC EXISTING SITE

WSCC ADDITION PROJECT

SITE B

SITE C

SITE A

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PROJECT STATUS:

The Design Review Board granted approval to Site B and Site C at the Design Recommendation Meeting (DR) #1 on 05/17/2016. The approval included comments and recommendations from the Board, including both elements of the proposal that were supported and encouraged to not dramatically modify with recommendations to be further studied for review for the Master Use Permit Submittal.

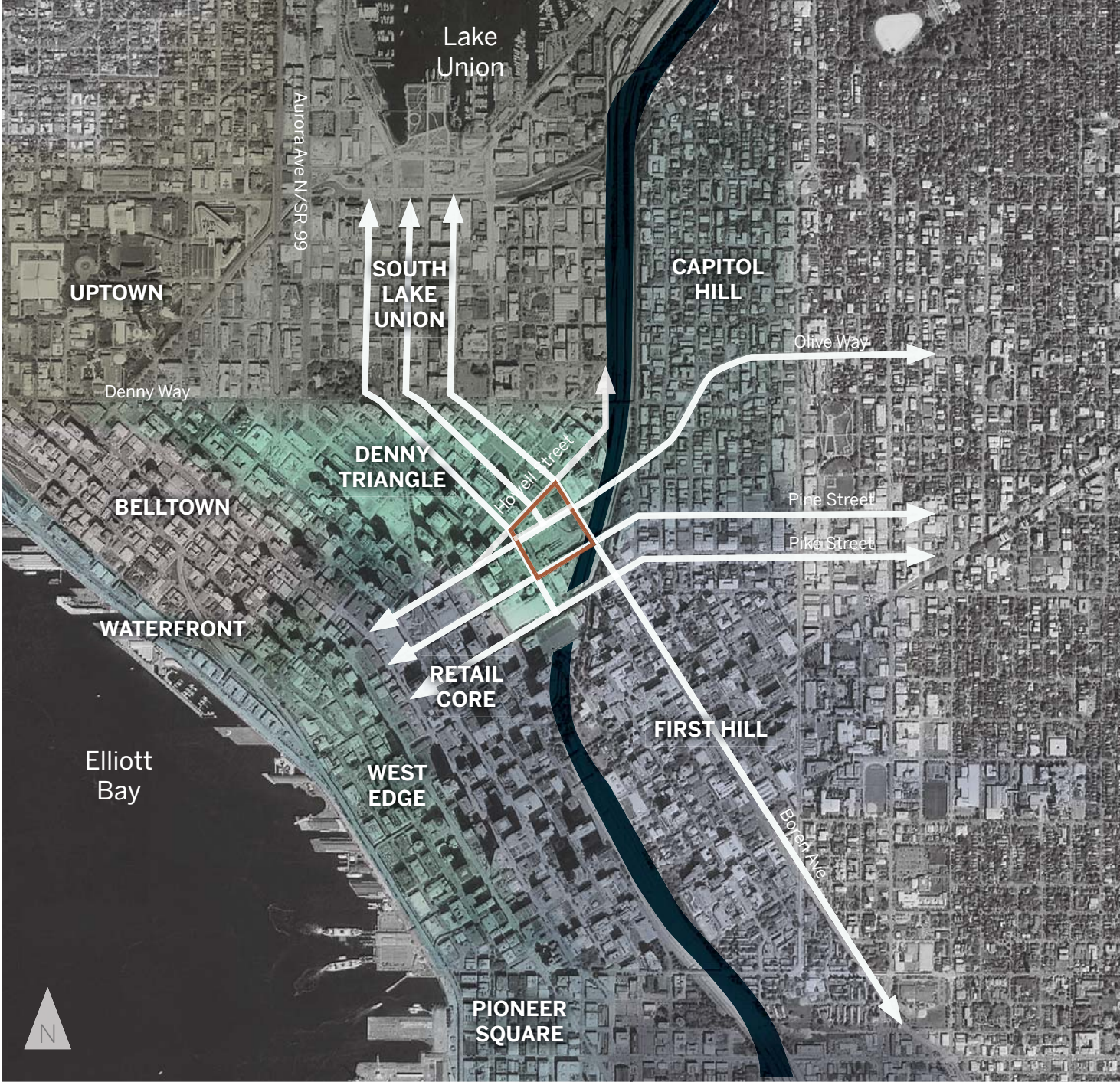
The Design Recommendation Meeting #2 is to cover response to recommendations and updates to departures, including six new departures on Site B and eight new departures on Site C based on further MUP review. Additional information has been provided for reference.

The project has also completed review with the Seattle Design Commission (SDC) for street and alley vacations including Public Benefits. This proposal includes further design evolution based on their feedback along with general design refinement overall.

1 DEVELOPMENT OBJECTIVES

PROJECT BACKGROUND

VICINITY



STREETS BORDERING PROJECT SITE

9-BLOCK STUDY AREA



- 9-BLOCK STUDY AREA
- WSCC ADDITION PROPERTIES
- FULL STREET / ALLEY VACATION
- AT GRADE SITE AREA INCLUDING VACATIONS

1 DEVELOPMENT OBJECTIVES PROJECT BACKGROUND

DEVELOPMENT SUMMARY

The proposal is to apply for Master Use Permits for development of a Convention Center Addition on a site consisting of 3 lots: Site A: 1600 9th Avenue, **SITE B**: 920 Olive Way, and **SITE C** 1711 Boren Avenue, that will collectively form the proposed Washington State Convention Center (WSCC) Addition Project. The 3 block site is bounded by Howell Street to the north, Pine Street to the south, 9th Avenue to the west, and Boren Avenue and I-5 to the east. Terry Avenue and Olive Way divide the site on the interior. Below grade street and full alley vacations will be required for this project.

The project will add approximately 1,230,000 SF of gross floor area to the existing Washington State Convention Center. This will include approximately 250,000 SF of new exhibition space, 120,000 SF of meeting rooms, a 60,000 SF. Ballroom space, approximately 20 convention center loading bays, and structured parking for up to 717 additional vehicles. The structure height is approximately 150-200 FT over 11 stories with 2 additional stories below grade. **The WSCC Addition project (Site A) received DRB approval at Design Review (DR) meeting #2 on July 19, 2016.**

SITE B is planned for a 290 ft., 404 unit residential tower with 65ft. podium.

SITE C is planned for a 240 ft. commercial building with approximately 500,000 sf of office use. Site C will also include the loading entrance and ramp for the WSCC Addition to access the convention center loading bays below Sites B and C.

SITE CONTEXT

The project site is located within the DMC 340/290-400 Downtown Mixed Commercial zone, within the Denny Triangle Urban Center Village. The Downtown Neighborhood Guidelines apply to this project.

The project site is bordered by the DMC 240/290-400 zone (Denny Triangle Urban Center Village Overlay) to the north, east, and south, and the DOC 2 500/300-500 zone (Commercial Core Urban Center Village Overlay) to the west.

The project occupies the intersection of several distinct and rapidly evolving neighborhoods, including Capitol Hill's Pike/Pine corridor, the Denny Triangle, South Lake Union, First Hill, and the Downtown commercial core.

Capitol Hill's traditional low-rise commercial development is being supplemented with new mid-rise mixed-use buildings. The neighborhood continues to promote a strong pedestrian community, interrupted only by the presence of I-5. The Denny Triangle and First Hill, connected via Boren Avenue, bookend the site to the north and south. Though a product of different eras, both neighborhoods contain higher density, taller residential and commercial development, along with notable institutional buildings. The Downtown neighborhood is the densest and tallest adjacent neighborhood, containing both high-rise commercial and residential development; it is the retail and cultural center of the city.

The site's proximity to Pike and Pine links itself to the waterfront via Pike Place Market and Westlake Center, and to the existing Washington State Convention Center along Ninth Avenue. Other notable landmarks include the historic Paramount Theatre and Worldmark-Camlin Seattle, adjacent to the site across Pine Street and Ninth Avenue. Due to the open space established by the presence of I-5, views to and from the project site to the south and east are both substantial and long-term. Views to the west, particularly from the higher elevations along Pine Street, provide a meaningful glimpse into the heart of the city.

PROCESS OVERVIEW

This DR #2 meeting at the Downtown Design Review Board builds on: EDG #1 held on (05/19/2015), which focused on the site context and urban design; EDG #2 held on (07/21/2015) which focused on building massing and street level designs; and EDG #3 (10/06/2015), during which the project received Early Design Guidance approval. DR #1 (05/17/2016) concluded with Design Review Board approval. The Design Proposal booklet and SDCI report from those meetings are available to view at the following link:

<http://www.web6.seattle.gov/dpd/edms/>

Search Past Reviews by typing in the SDCI project number: 3020176 (Site A), 3018096 (Site B), 3020177 (Site C).

NOTE: ALL DESIGN SHOWN FOR STREET ROW OUTSIDE OF PROPERTY LINES WERE REVIEWED AND APPROVED BY SEATTLE DESIGN COMMISSION AND ARE UNDERGOING CONTINUED REVIEW BY SDOT.

THIS BOOKLET IS FOR SITES B & C ONLY. SITE A WAS REVIEWED ON 7/19/16.

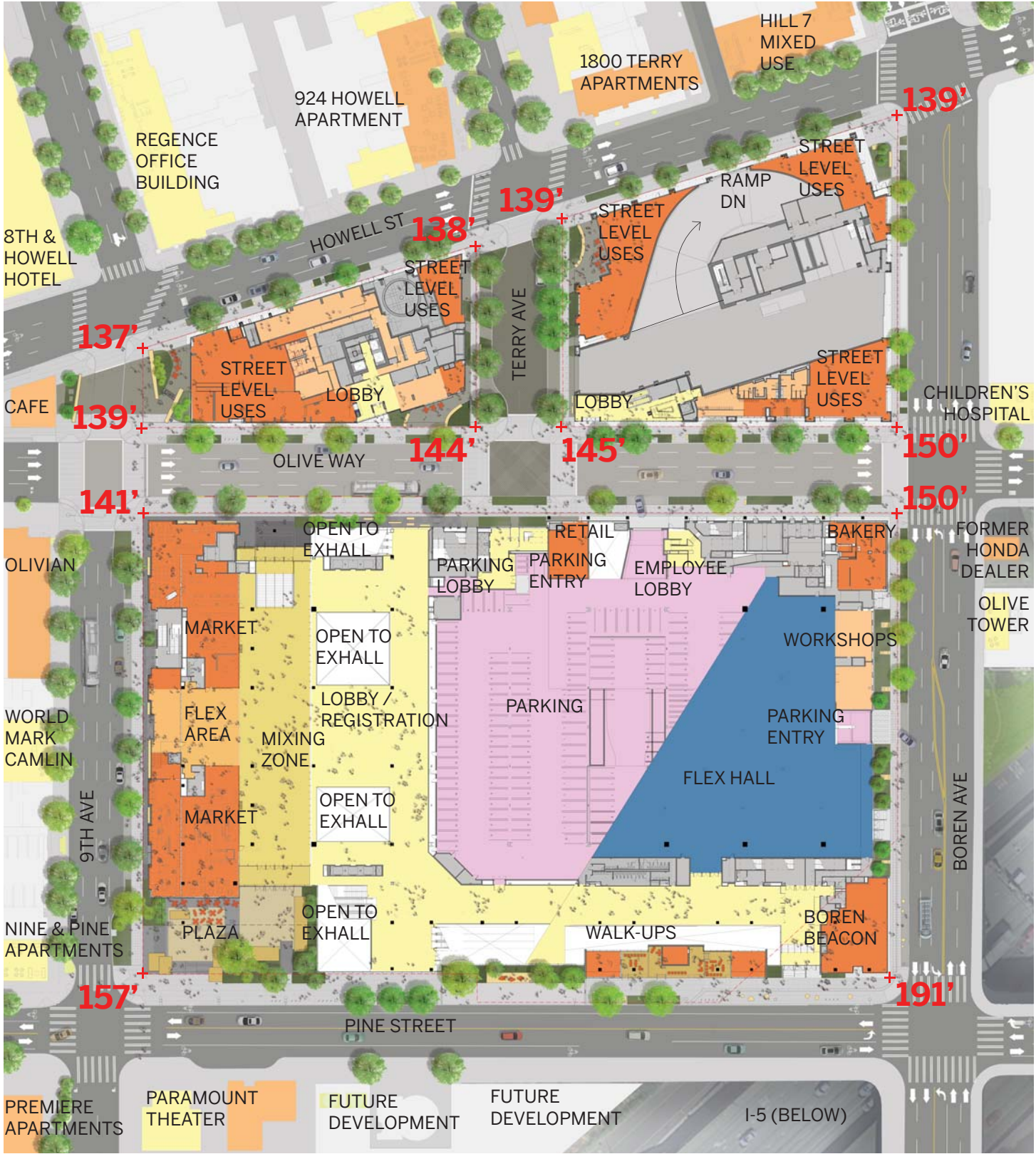
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DRB#1 CONDITION OF APPROVAL RESPONSE

DRB #1



DRB #2



DRB#1 CONDITION OF APPROVAL RESPONSE

CONDITION #1

SITE B - GROUND FLOOR USES; 1A:

Revise the entire ground floor frontage facing Olive Way, except for the yellow lobby zone shown on pg 56, to consist of true retail/commercial uses with doors direct to the sidewalk and southeast corner plaza. These uses and area should not be counted towards any Code- required amenity area. (C1, C3, C4)

RESPONSE:

Street level-use has been proposed along Olive Way per DRB guidance. See sheet A201 in MUP (Residential Tower).

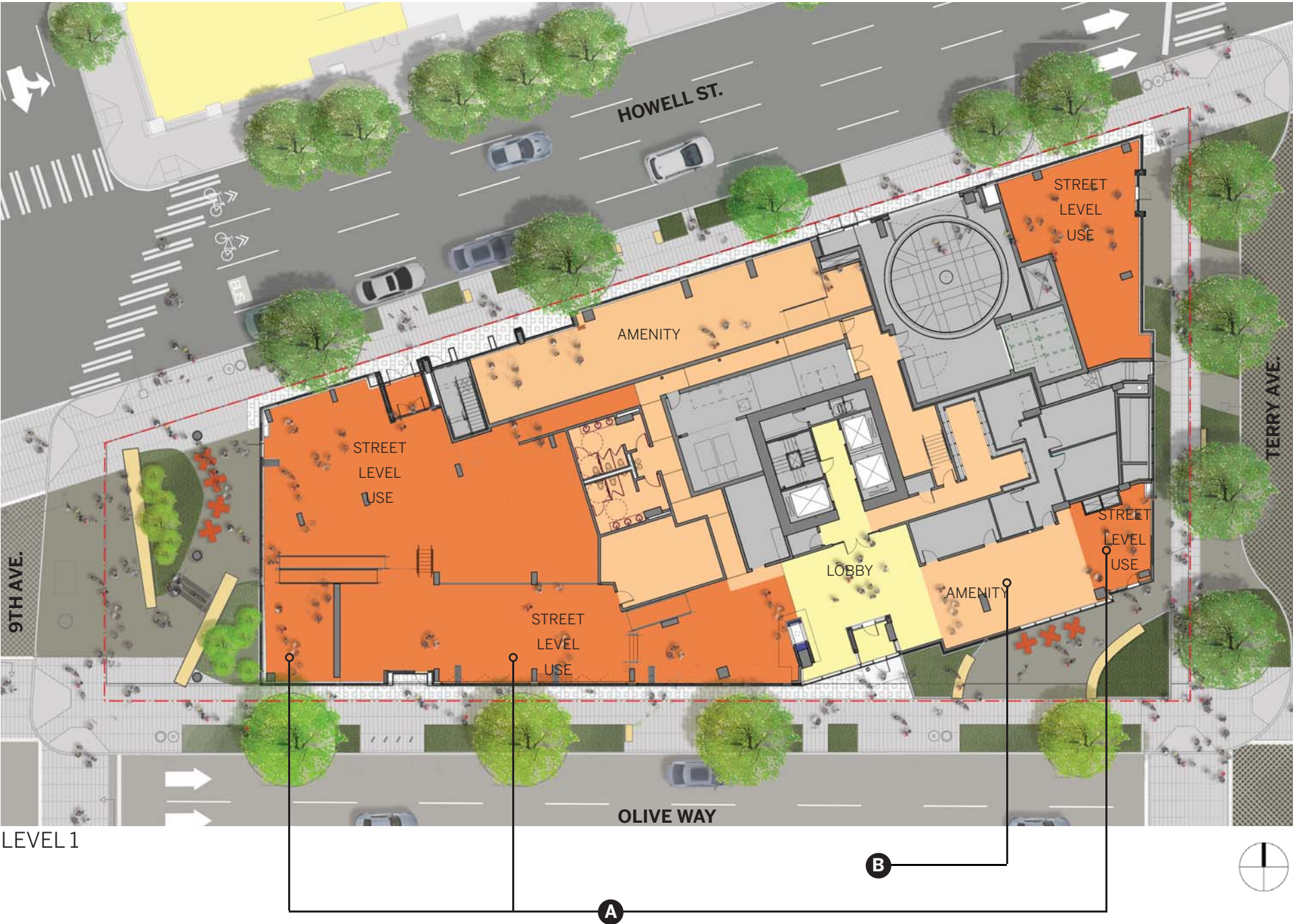
DRB #1



LEVEL 1

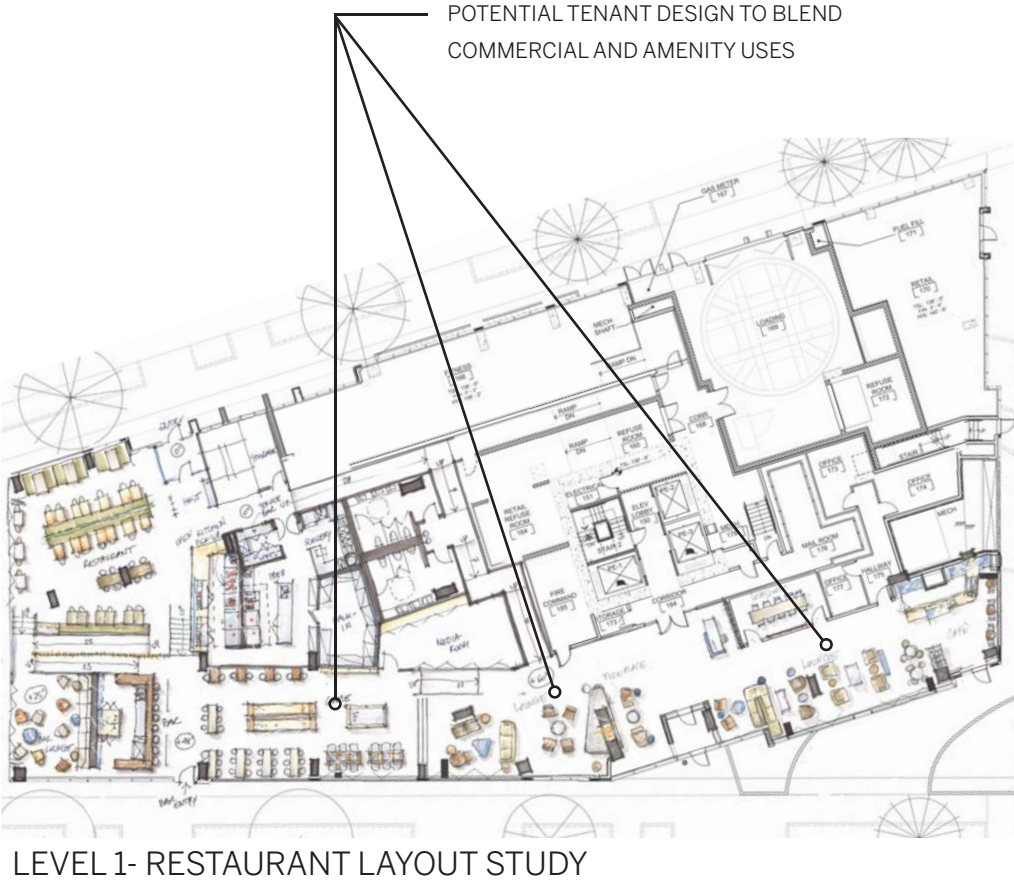
DRB#1 CONDITION OF APPROVAL RESPONSE

DRB #2



NOTES:

- A** - Street level-use provided along Olive Way.
- B** - Area adjacent to lobby remains amenity space needed to serve the main residential tower lobby, the space is intended to be shared with building tenants and retail space.



LEVEL 1

LEVEL 1- RESTAURANT LAYOUT STUDY

DRB#1 CONDITION OF APPROVAL RESPONSE

CONDITION #2

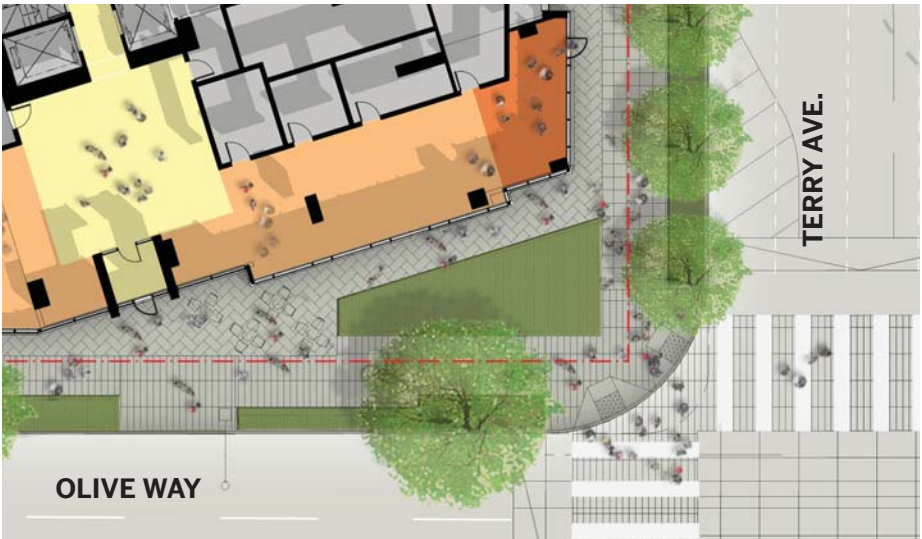
SITE B AND SITE C- CORNER PLAZA LANDSCAPES:

1c & 4a: Revise the design of the three plaza lawn panels to be more than simple turf, and to provide diverse pedestrian amenities such as seating, lighting and artwork. Add retail access doors directly onto the plazas to activate them. The building edge paving should be widened to facilitate the retail and doors adjacent, and the intersection corners should be eased to accommodate diagonal pedestrian desire lines. (C1, D1, D2)

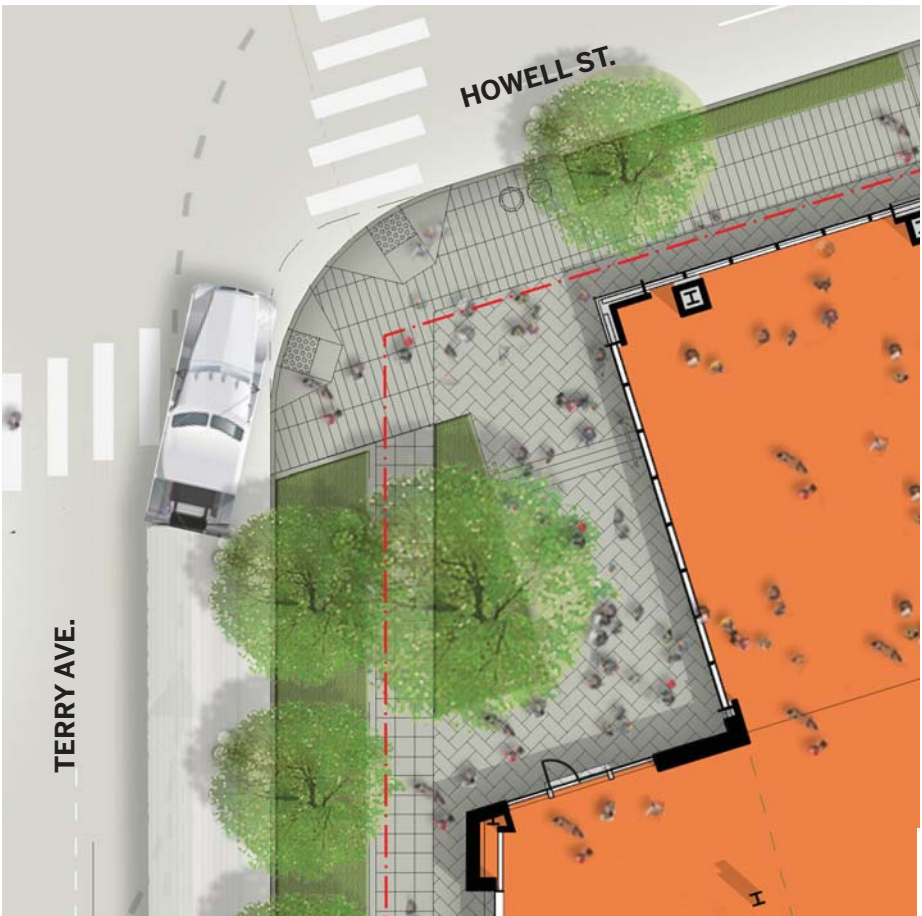
RESPONSE:

The plaza landscape designs have been revised to allow for easier pedestrian flow along the building, direct access from retail doors onto the plaza when sidewalk elevations allow, activated plazas with seating and connections to the retail. See sheet L111 (Residential Tower) and sheet L111 (Office Tower) in MUP.

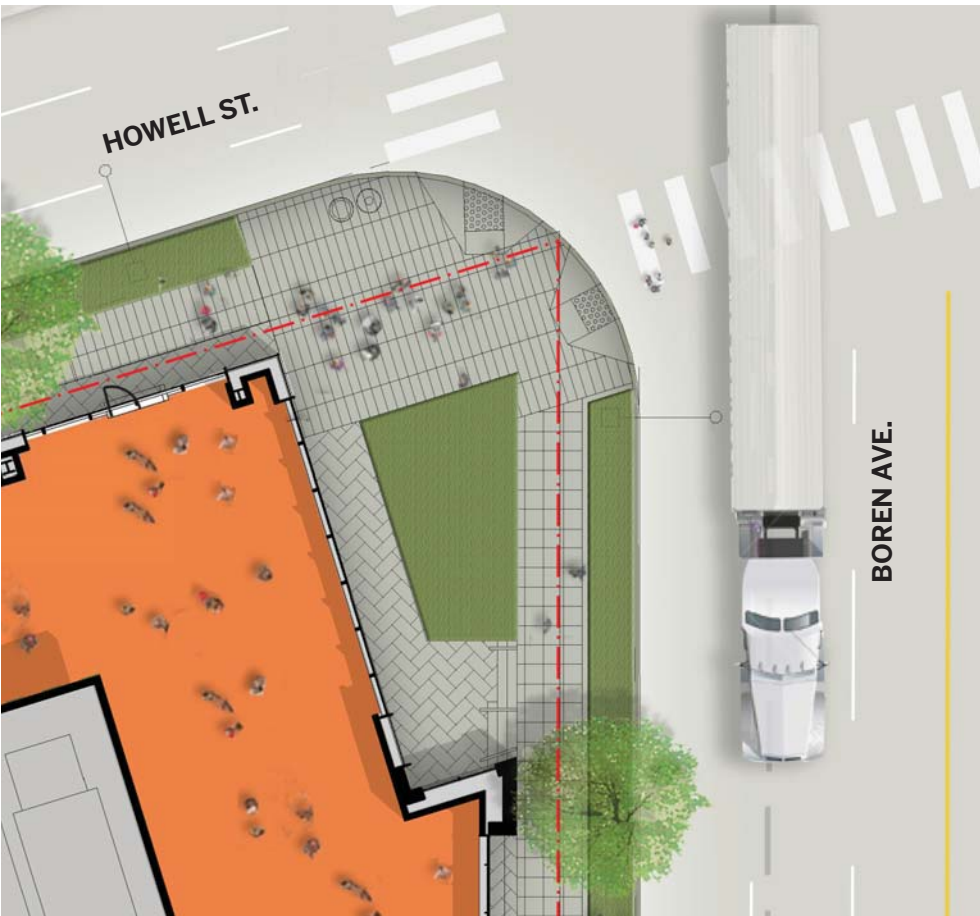
DRB #1



SITE B



SITE C



SITE C

DRB#1 CONDITION OF APPROVAL RESPONSE

DRB #2



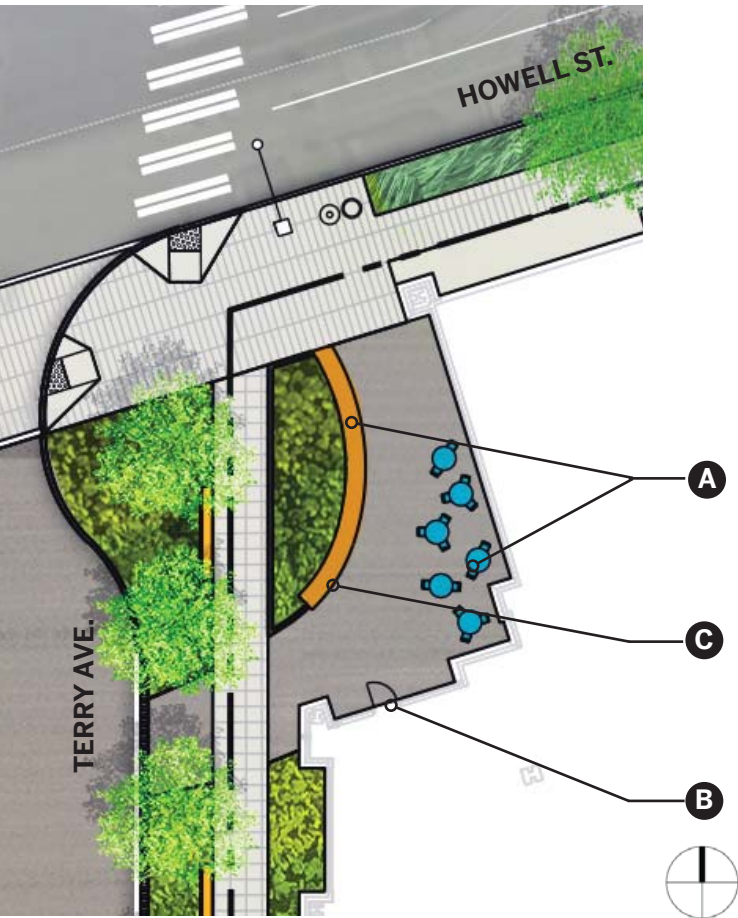
OLIVE WAY AND TERRY AVE. (SITE B)



OLIVE WAY AND TERRY AVE. (SITE B)

NOTES:

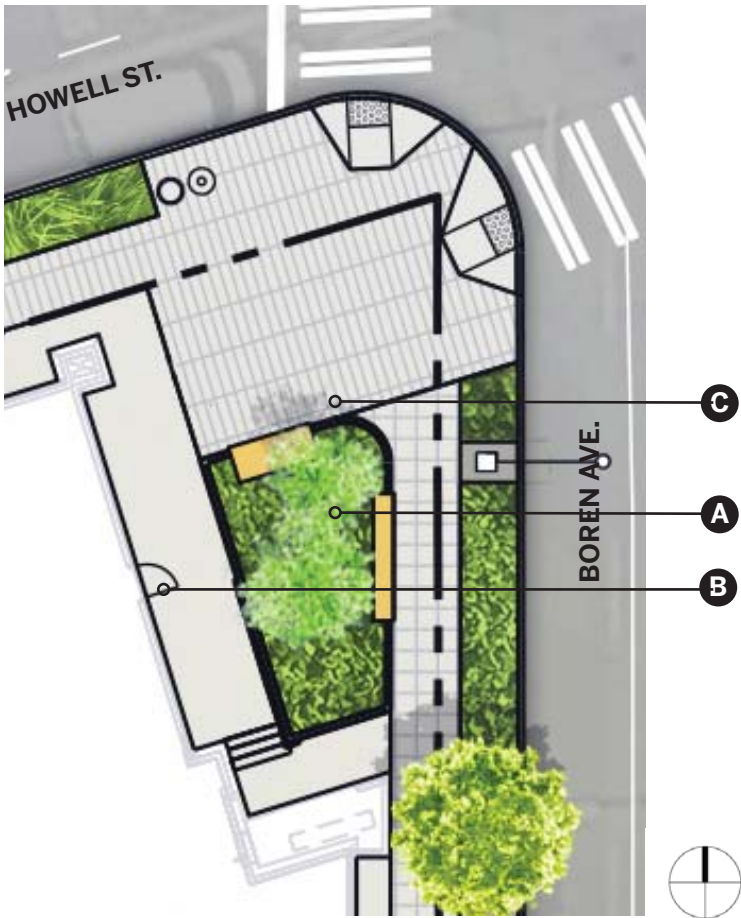
- A - Pedestrian amenities and seating added.
- B - Retail doors accessed directly to plaza.
- C - Corners eased to accommodate pedestrian desire lines.



TERRY AVE. AND HOWELL ST. (SITE C)



TERRY AVE. AND HOWELL ST. (SITE C)



HOWELL ST. AND BOREN AVE. (SITE C)



HOWELL ST. AND BOREN AVE. (SITE C)

DRB#1 CONDITION OF APPROVAL RESPONSE

CONDITION #3

SITE B - HOWELL FACADE; 2A;
The western part of the brick podium on Howell overlaps too much over the tower form above; revise the eastern two bays of brick podium [pg 67/top-middle] to the tower vocabulary. (B4, C2)

RESPONSE:
Consistent with DRB guidance, the western portion of the brick podium along Howell has been moved east to align with the tower edge above. See sheet G312, G322 and A302 in MUP (Residential Tower)

DRB #1



HOWELL STREET FACADE

DRB#1 CONDITION OF APPROVAL RESPONSE

DRB #2



HOWELL STREET FACADE

NOTES:
A - Brick podium pulled back to align with tower massing above.

DRB#1 CONDITION OF APPROVAL RESPONSE

CONDITION #4

SITE B - HOWELL FACADE; 2A;
Revise the level 2 spandrel cladding exposed on the Olive and Terry elevations to be a darker tone to complement the podium brick and not dilute the light color of the tower coming to grade. (B4, C2)

RESPONSE:
Level 2 spandrel color is now a darker tone, consistent with DRB guidance. See sheet G321 and G322 in MUP (Residential Tower).

DRB #1



OLIVE WAY FACADE



TERRY AVENUE FACADE

DRB#1 CONDITION OF APPROVAL RESPONSE

DRB #2



OLIVE WAY FACADE

- NOTES:**
- A - Metal panel area with darker tone.
 - B - Stone pier (dark)



TERRY AVENUE FACADE

DRB#1 CONDITION OF APPROVAL RESPONSE

CONDITION #5

SITE B - LOBBY ENTRANCE; 2C;
Scale up the main lobby entrance to at least 2 stories height, corresponding with the interior volume, and detailed with materials, colors and forms not similar to the retail entry boxes. (C2, C4)

RESPONSE:
The building entrance has been designed with a two story expression per DRB guidance. A new departure is requested on sheet G114 in the MUP for the entrance canopy which is now above the maximum 15' requirement for overhead weather protection. See sheet G321 in MUP (Residential Tower).

DRB #1



OLIVE WAY FACADE

DRB#1 CONDITION OF APPROVAL RESPONSE

DRB #2



OLIVE WAY FACADE



NOTES:

- A** - Two story main entrance to match interior volume.
- B** - Wood ceiling and integrated lighting extends from interior lobby space to exterior soffit of canopy.
- C** - Solid wood door with glass vision panel.
- D** - Wood mullions.

DRB#1 CONDITION OF APPROVAL RESPONSE

CONDITION #6

SITE B - WEST ELEVATION AND FLAT IRON CONDITION; 2D;

Add brick to wrap the two west corners to frame the large 'glass bay' [65, 68]. Break down the glass bay with articulation, possibly darker framing or mullions, but spandrels should be minimal and few to emphasize this bay as a response to the angled street views from the west. Revise the trellis material to not be wood, but retain the wood soffit shown over the public plaza [68]. (B1, C2)

RESPONSE:

Preferred Option
Consistent with DRB guidance, our preferred design of the West elevation has a brick facade wrapping the building corners to frame the large glass bays. Dark mullions provide articulation within the glass bay. The wood trellis at Level 7 has been eliminated. See sheet G311 and G322 in MUP (Residential Tower).

Option 2
Consistent with DRB guidance, the design of the West elevation has a brick facade wrapping the building corners to frame the large glass bay with dark metal panel and dark mullions for articulation. The wood trellis at Level 7 has been eliminated.

Option 3
Consistent with DRB guidance, the design of the West elevation has a brick facade wrapping the building corners to frame an extruded glass and metal bay. The glass balconies from the DRB #1 design remain. The bay is articulated with dark metal panel and dark mullions per the Board's guidance. The wood trellis at Level 7 has been eliminated.

DRB #1



9TH AVENUE ELEVATION



9TH AVENUE FACADE

DRB#1 CONDITION OF APPROVAL RESPONSE

DRB #2

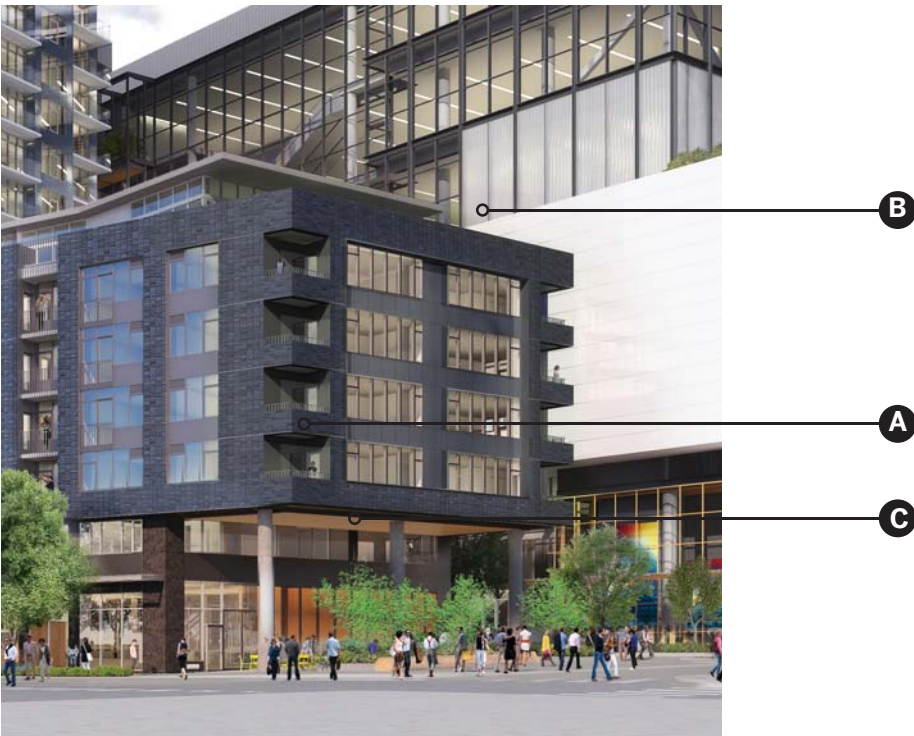


- NOTES:**
- A** - Brick wrapping the corner to frame the large glass bay.
 - B** - Wood trellis removed
 - C** - Wood soffit remains

9TH AVENUE ELEVATION (PREFERRED OPTION)



9TH AVENUE FACADE (PREFERRED OPTION)



9TH AVENUE FACADE (OPTION 2)



9TH AVENUE FACADE (OPTION 3)

DRB#1 CONDITION OF APPROVAL RESPONSE

CONDITION #7

SITE B & SITE C - THREE LOADING DOORS; 2E & 5C;
Design the three overhead or sectional doors to include an artful treatment, with high pedestrian visual interest, and/or be translucent glass (the gray door image shown on pg 76 was too generic; the two orange examples shown on pg 43 had more visual interest). (C3, E3)

RESPONSE:
Consistent with DRB guidance, the garage doors on Site B and C will have a custom design that provides pedestrian visual interest, Site C doors to be at least 51% open to outside air. See sheet G312 and G322 in MUP (Residential Tower) and sheet G311, G313, G321 and G322 in MUP (Office Tower). SDC requested artist-designed garage doors as a vacation condition.

DRB #1



SITE B: TERRY AVENUE



SITE C: BOREN AVENUE



SITE C: TERRY AVENUE

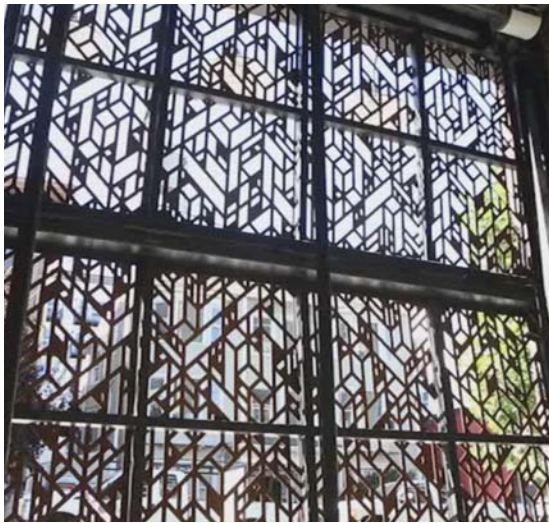


DRB#1 CONDITION OF APPROVAL RESPONSE

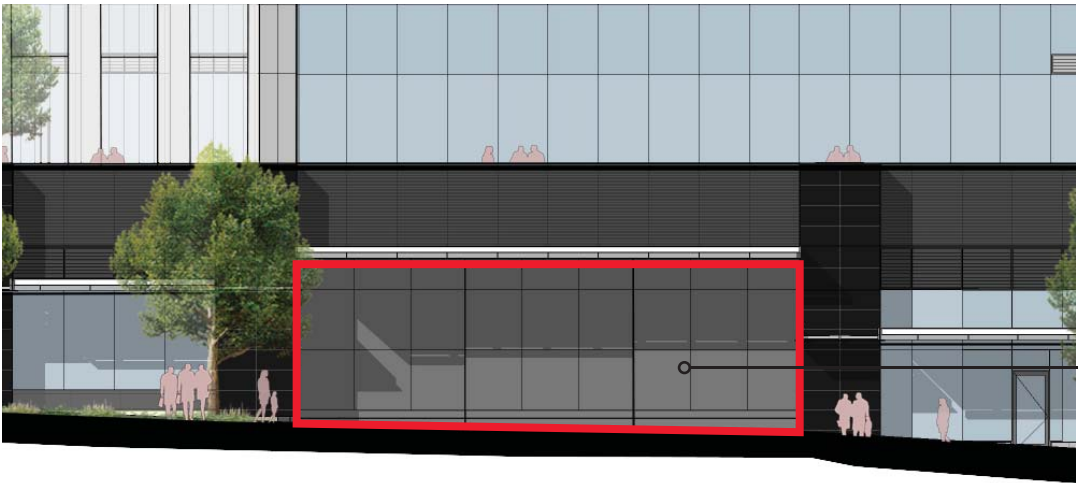
DRB #2



SITE B: HOWELL STREET



SAMPLE REFERENCE IMAGE: 45% OPEN



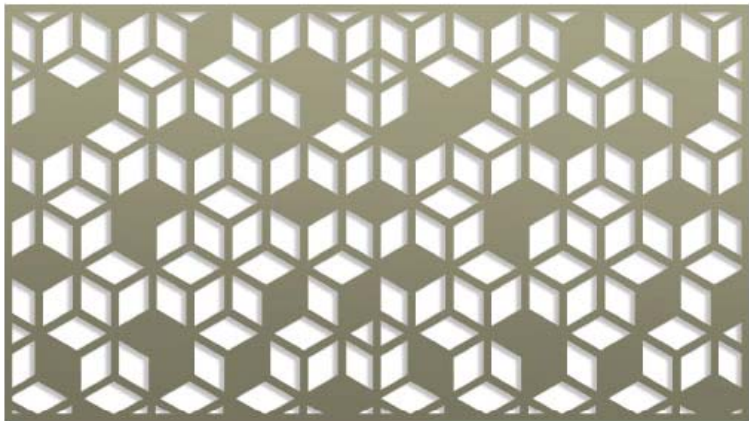
SITE C: BOREN AVENUE



SAMPLE REFERENCE IMAGE: 45% OPEN



SITE C: TERRY AVENUE



SAMPLE REFERENCE IMAGE: 50% OPEN

NOTES:
A - Area of custom design by artist.

DRB#1 CONDITION OF APPROVAL RESPONSE

CONDITION #8

SITE B & SITE C - THREE LOADING DOOR PORTAL RETURNS; 2E & 5C;

Design quality materials consistent with the exterior design return approximately 15 ft into the two large truck openings, regardless of how long or often the doors will be open. (E3)

RESPONSE:

An artist designed graphic is proposed for the south return wall of the Site C loading dock. See sheet A201 in MUP (Office Tower). No graphic or material return is planned on the north side of the loading entries on Site C, the north wall does not return and is interrupted and not consistent for a material return. Site B is not proposing a quality material return, there are no walls returning at the loading entrance. See sheet A201 in MUP (Residential Tower). Site B loading doors will fold inward, displaying the custom panel design at the portal returns.

DRB #1



SITE B: TERRY AVENUE



SITE C: BOREN AVENUE



SITE C: TERRY AVENUE

SITE B - 2E

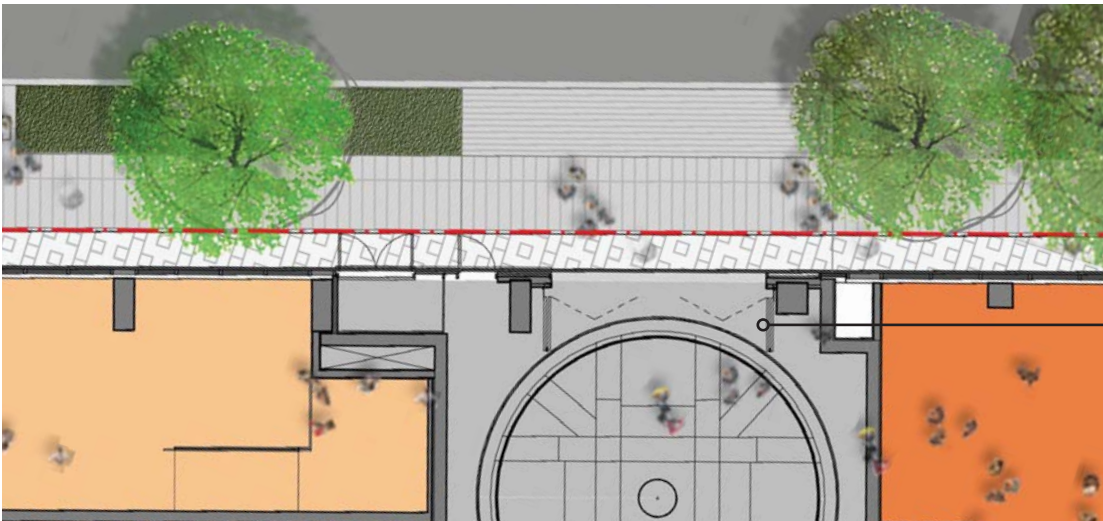
The Board supported the co-location of loading, trash and two exit doors at the mid-block on Terry, and the continued effort to minimize all those blank widths. The Board recommended the overhead doors be given an artful treatment, with pedestrian visual interest, and/or be translucent glass (the perforated gray image shown at the meeting was too generic; while the two orange examples were more visually interesting). **Because it is a large opening onto a Green Street, the Board recommended quality materials consistent with the exterior design return approximately 15 ft into the loading opening.** Also see discussion under B departure #3. (C3, E3)

RESPONSE:

Loading opening no longer located on Terry Ave. (Green Street). The loading door is now located on Howell St. for Site B.

DRB#1 CONDITION OF APPROVAL RESPONSE

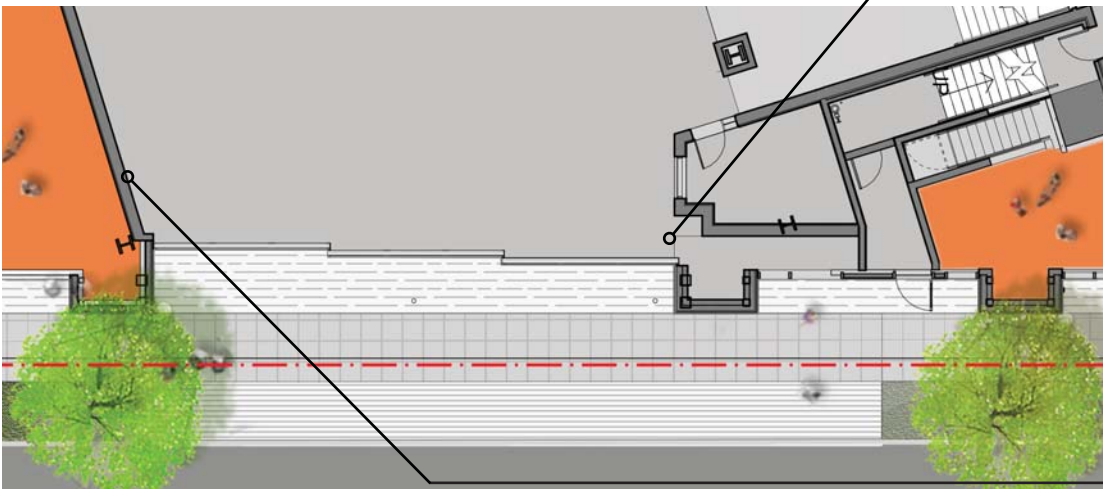
DRB #2



SITE B: HOWELL STREET

A

No return wall on North side, large pocket for the sliding gate breaks the return.

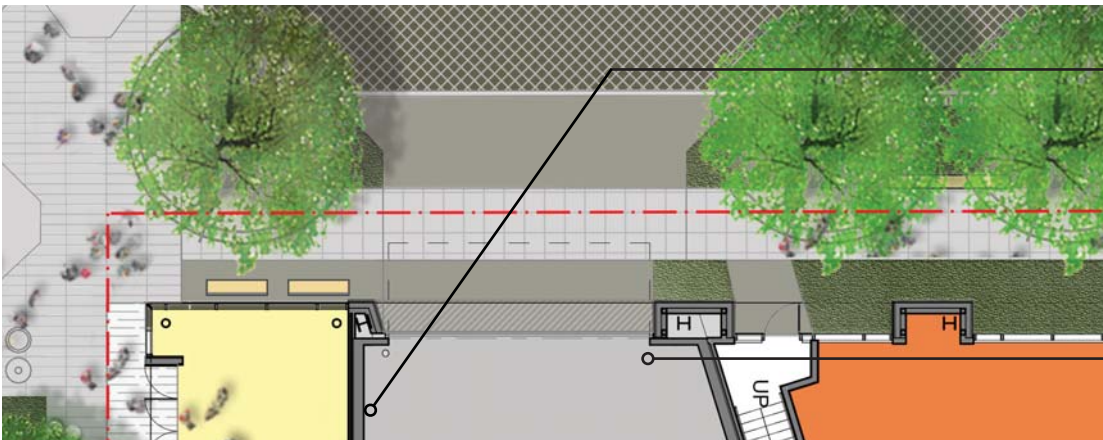


SITE C: BOREN AVENUE

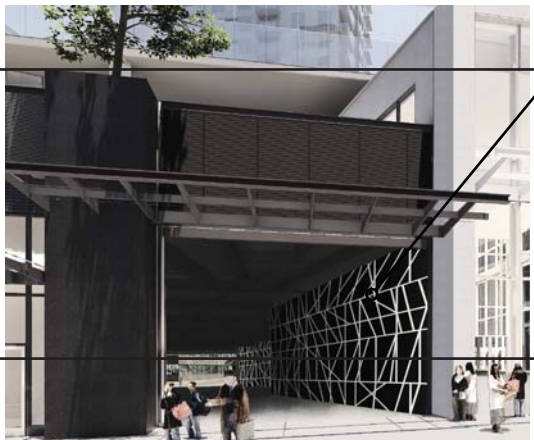


REFERENCE IMAGE (FINAL DESIGN BY ARTIST)

B



SITE C: TERRY AVENUE



REFERENCE IMAGE (FINAL DESIGN BY ARTIST)

B

No return wall on North side for material return.

NOTES:

A - Loading doors no longer located on a green street, portal returns will be hidden from view by inward folding custom designed door panels.

B - Custom graphic design by artist proposed for large return wall on south side of loading area.

DRB#1 CONDITION OF APPROVAL RESPONSE

CONDITION #9

SITE B & SITE C - CANOPIES; 2F;
Redesign to eliminate all canvas canopies and use only translucent or fritted glass for variation within a predominantly clear glass canopy wrap. (C5, C2)

RESPONSE:
All canvas canopies have been eliminated from the design. Site B and Site C will have clear glass canopies, consistent with DRB guidance.

DRB #1



SITE B: OLIVE WAY



SITE C: OLIVE WAY

DRB#1 CONDITION OF APPROVAL RESPONSE

DRB #2



SITE B: OLIVE WAY



SITE C: OLIVE WAY

NOTES:

A - Continuous clear glass canopies

DRB#1 CONDITION OF APPROVAL RESPONSE

CONDITION #10

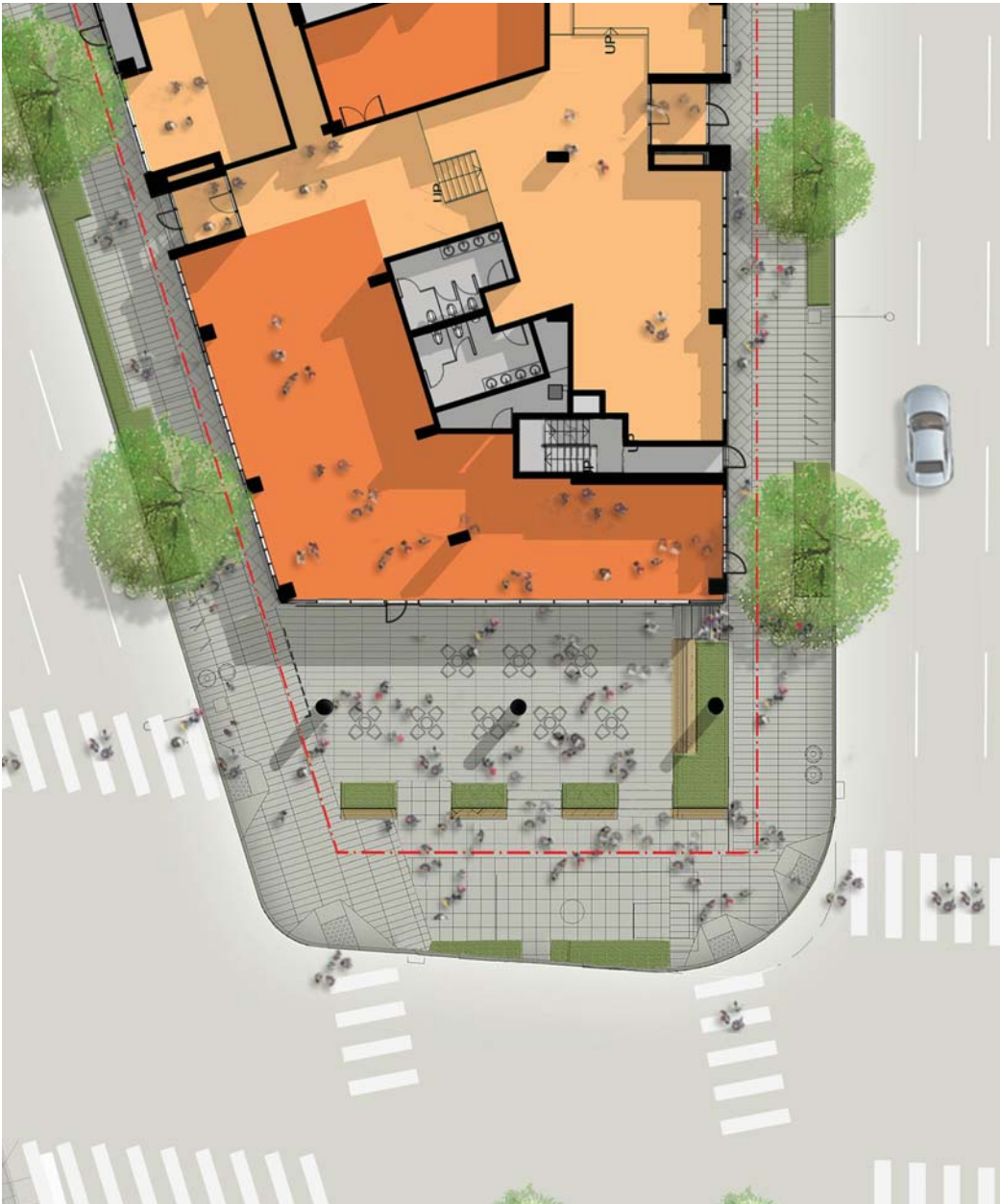
SITE B - PUBLIC PLAZA; 3B & 3C;

Revise the plaza design to match the basic design of the “Alternate 2” shown at the meeting, with a consistent 12ft wide sidewalk and one consolidated planter along 9th Avenue. The planter should include edge seating and 3 medium height trees that work with the fourth tree at the southwest corner to vertically define the Green Street. Any steps from 9th to the plaza should be wide and as few risers as possible, and the steps at the southwest building corner should be widened to 6ft minimum. Include a wide ground level planter at the curb edge shown in Alternate 2, with lush, vertical plantings to reinforce the Green Street, (D2, D3)

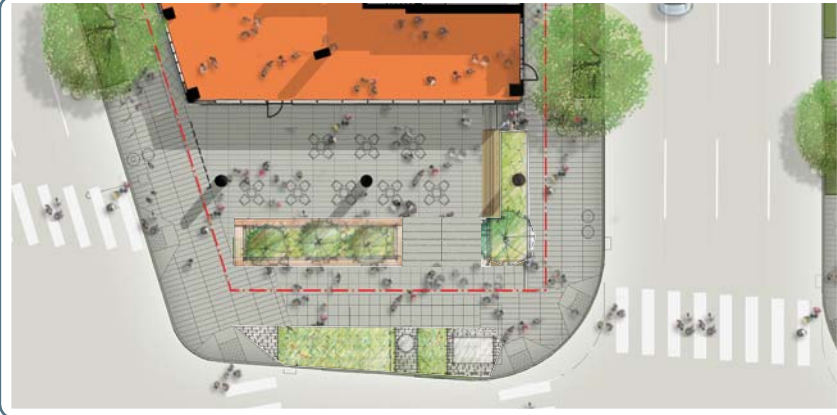
RESPONSE:

The 9th Avenue Plaza has been designated a Public Benefit Open Space. The proposed design has been approved by the Seattle Design Commission. The new design remains consistent with the Alternate 2 design with a wide sidewalk along 9th Ave, one consolidated planter, wide steps down to the plaza and edge seating. A planter at the curb edge has been eliminated as proposed utilities do not allow sufficient soil depth for healthy planting.

DRB #1



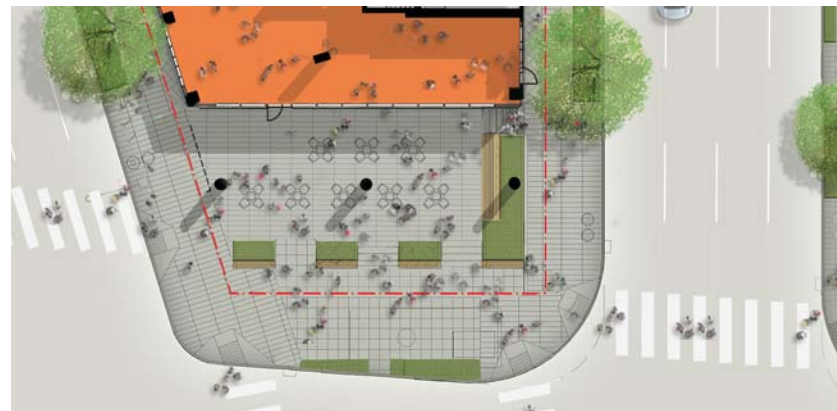
SITE B: 9TH AVENUE



9th Avenue Plaza - Alternate 2



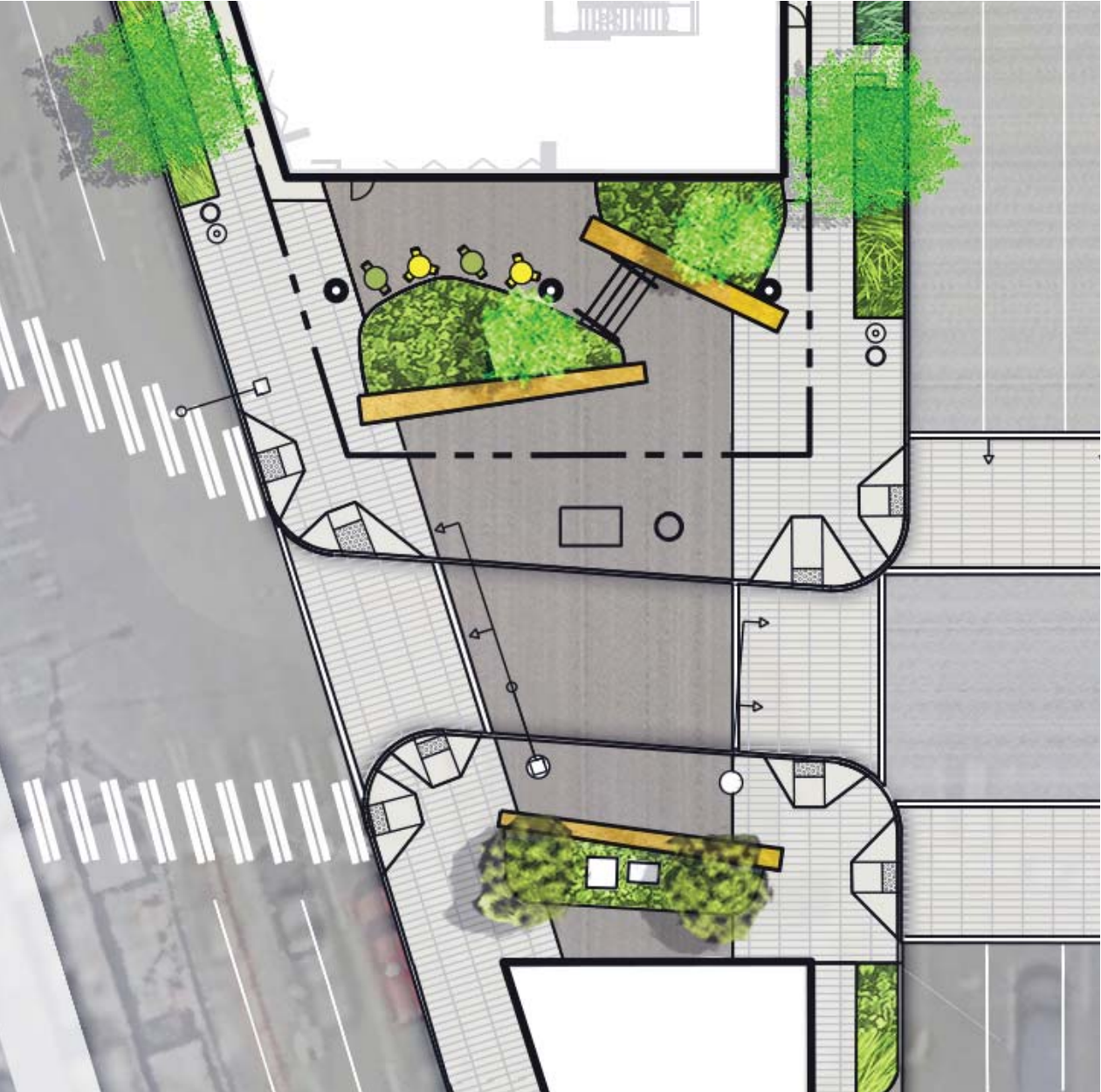
9th Avenue Plaza - Alternate 1



9th Avenue Plaza - Current Design

DRB#1 CONDITION OF APPROVAL RESPONSE

DRB #2



SITE B: PROPOSED 9TH AVENUE PUBLIC PLAZA



SITE B: PROPOSED 9TH AVENUE PUBLIC PLAZA

DRB#1 CONDITION OF APPROVAL RESPONSE

CONDITION #11

SITE C - GROUND LEVEL FACADE REVISIONS; 4B, 5A, 5B;

Revise the middle bay glass into the loading ramp on Howell Street [91/middle] be about 50% clear with frit or patterns at pedestrian eye-level; revise the upper metal panel/louvers between the black piers [97] to a darker tone, but not matching the stone; revise and strengthen the edges of the southwest entry volume. (C2) 12)

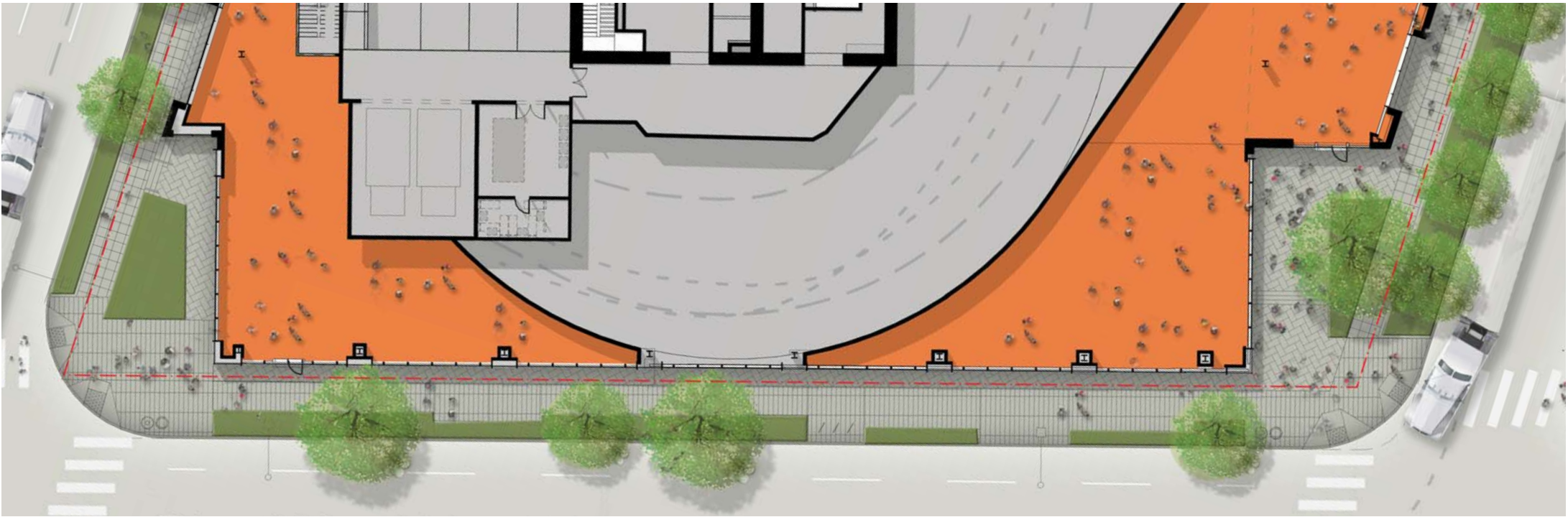
RESPONSE:

The middle glass bay looking into the loading ramp along Howell St. will have a custom 50% glass frit, designed by an artist. See sheet G312 and G322 in MUP (Office Tower). SDC requested artist-designed frit as a vacation condition. The artist has the option to extend the frit design east another bay if desired by artist and future retail tenant.

DRB #1



SITE C: HOWELL STREET - MIDDLE GLASS BAY



SITE C: HOWELL STREET - MIDDLE GLASS BAY

DRB#1 CONDITION OF APPROVAL RESPONSE

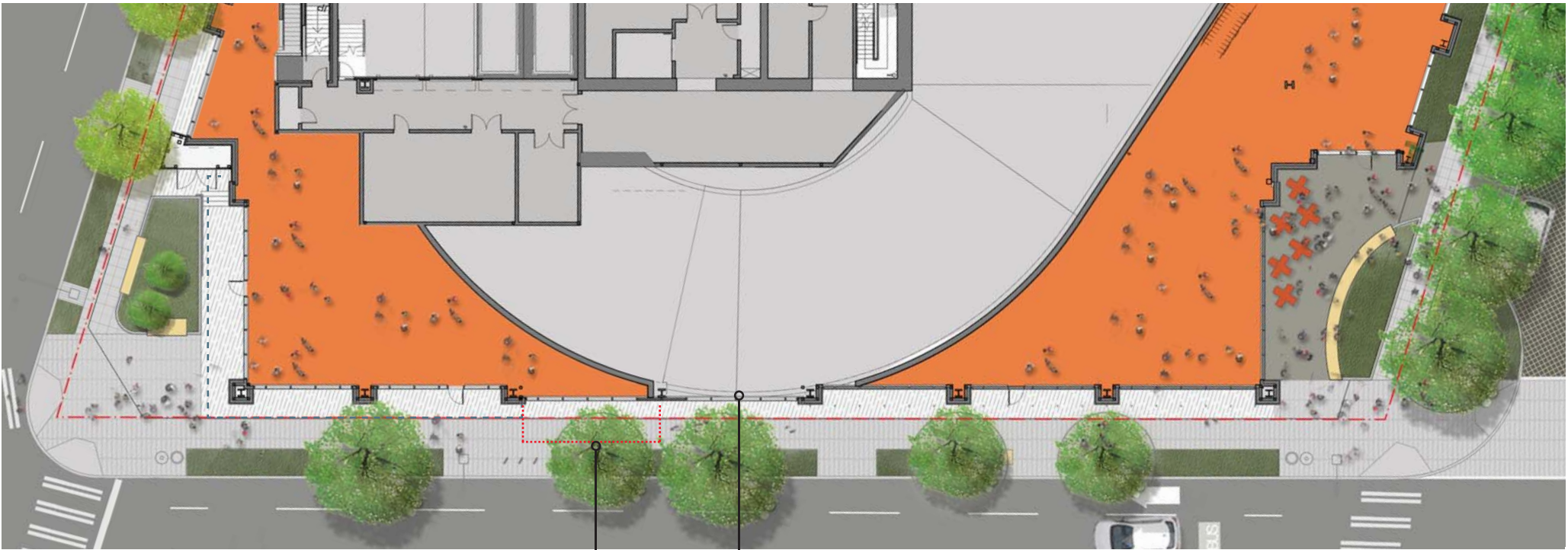
DRB #2



SITE C: HOWELL STREET - MIDDLE GLASS BAY

NOTES:

A - Custom 50% frit pattern by artist.



SITE C: HOWELL STREET - MIDDLE GLASS BAY

Extent of potential expansion of artist design.



DRB#1 CONDITION OF APPROVAL RESPONSE

CONDITION #11

SITE C - GROUND LEVEL FACADE REVISIONS; 4B, 5A, 5B;

Revise the middle bay glass into the loading ramp on Howell Street [91/middle] to be about 50% clear with frit or patterns at pedestrian eye-level; **revise the upper metal panel/louvers between the black piers [97] to a darker tone, but not matching the stone; revise and strengthen the edges of the southwest entry volume. (C2) 12)**

RESPONSE:

The metal panel/louver areas between the black stone piers along Terry Avenue, Olive Way and Boren Avenue are a darker tone. See sheet G321 and G322 in MUP (Office Tower). The lobby entry box edges have been widened to give it a stronger definition. See sheet G310 and G321 in MUP (Office Tower).

DRB #1



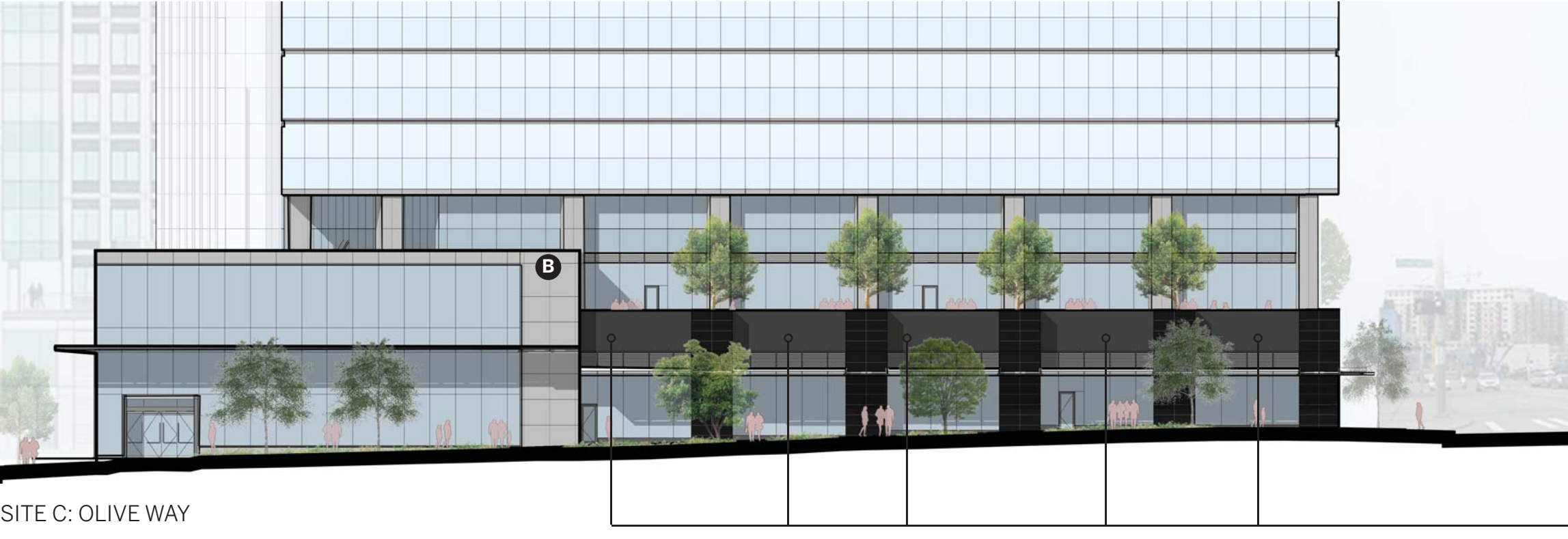
SITE C: OLIVE WAY



SITE C: OLIVE WAY

DRB#1 CONDITION OF APPROVAL RESPONSE

DRB #2



NOTES:

- A - Metal panel area with darker tone.
- B - Metal panel at southwest entry volume increased to strengthen edges.



SITE C: OLIVE WAY

DRB#1 CONDITION OF APPROVAL RESPONSE

CONDITION #12

SITE C - TOWER MODULATION AND REVEALS; 6B;
All three vertical reveals should be made deeper if possible (in particular the south side of the Terry Green Street reveal, and they should be a consistent dark gray spandrel and glass, as shown on updated renderings at the meeting. In particular, the Howell Street reveal should not include expressed horizontals, but one dark, vertical pilaster is acceptable. (B2, B4)

RESPONSE:
All tower reveals will have a transparent glazing that will appear deeper and read darker than the more reflective glazing of the typical curtain wall and spandrels. The vertical pilaster at the Howell Street recess has been eliminated from the design at the ground level and above in the tower. Removing the pilaster gives the recess a stronger presence on the facade and creates a clear separation between the tower massing.

DRB #1



SITE C: OLIVE WAY



SITE C: BOREN AVENUE



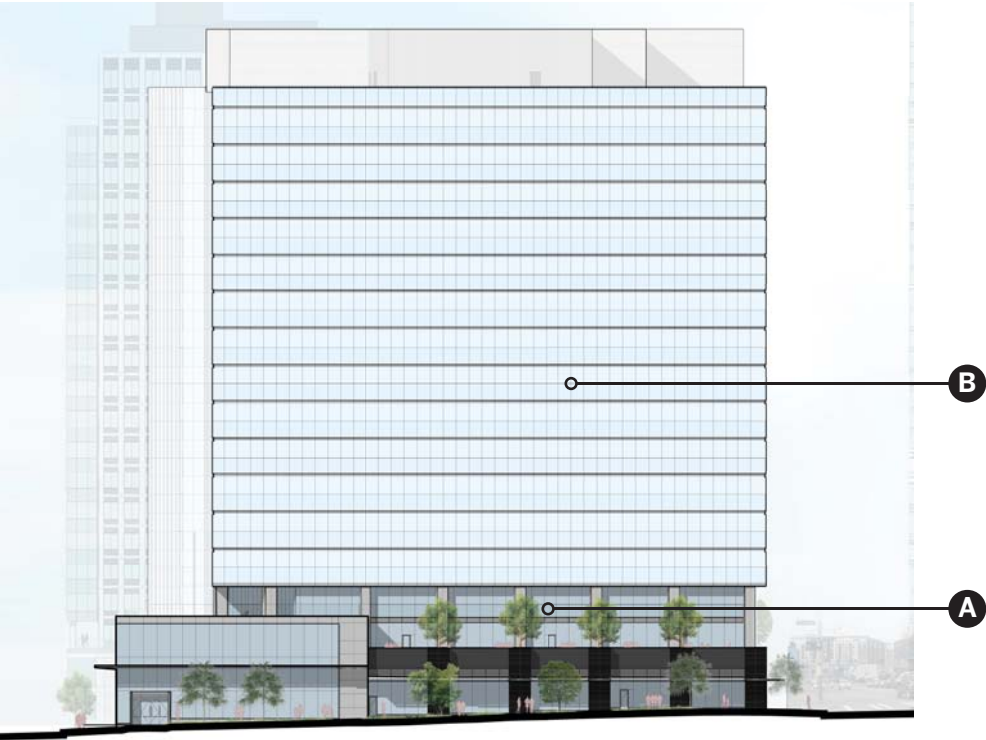
SITE C: HOWELL STREET



SITE C: TERRY AVENUE

DRB#1 CONDITION OF APPROVAL RESPONSE

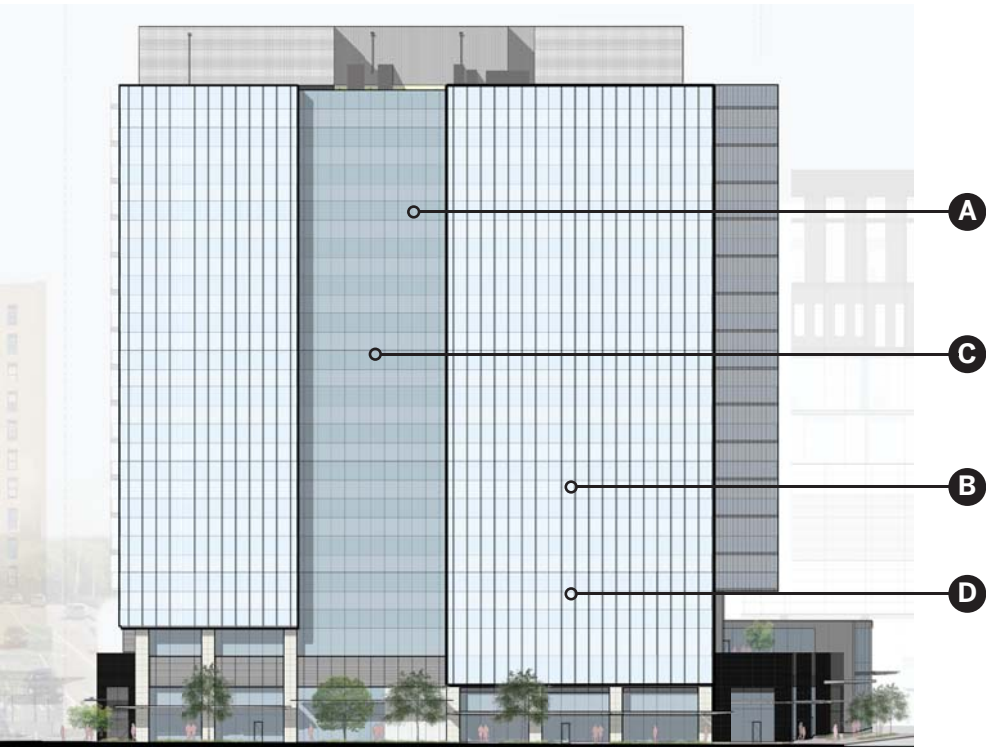
DRB #2



SITE C: OLIVE WAY



SITE C: BOREN AVENUE



SITE C: HOWELL STREET



SITE C: TERRY AVENUE

NOTES:

- A** - Transparent glazing and darker spandrel in the reveals will read darker than surrounding reflective glazing.
- B** - Reflective glazing
- C** - Vertical pilaster and horizontal reveals removed.
- D** - 4" deep vertical fins, typical on north volume.



3 DESIGN UPDATES **SITE B** - RESIDENTIAL

OLIVE WAY ELEVATION
DRB #1



3 DESIGN UPDATES

SITE B - RESIDENTIAL

OLIVE WAY ELEVATION

DRB #2

BOARD CONDITION #9

SITE B & SITE C - CANOPIES; 2F;

Redesign to eliminate all canvas canopies and use only translucent or fritted glass for variation within a predominantly clear glass canopy wrap. (C5, C2)

RESPONSE:

All canvas canopies have been eliminated from the design. Site B and Site C will have clear glass canopies consistent with DRB guidance.

UPDATE:

Dark stone piers have been added at the ground level to break up the length of the storefront facade and provide more detail at the pedestrian scale. Operable windows have been added at the street level facade to connect the interior retail spaces to the street.

UPDATE:

Louvers added at ground level for servicing retail. The louvers allow the retail space to function successfully which is consistent with Board's feedback.

UPDATE:

Door relocated west and wood portal and vestibule removed. The new exit door location is recessed between the stone piers and allows for a continuous retail space along Olive Way. The previous door location was centered in the space and did not promote the free flowing retail concept the Board desired. The wood clad vestibule was eliminated to provide more prominence to the main retail entrance along Howell Street and create a continuous open retail space to better activate the Olive Way street frontage.

BOARD CONDITION #4

SITE B - HOWELL FACADE; 2A;

Revise the level 2 spandrel cladding exposed on the Olive and Terry elevations to be a darker tone to complement the podium brick and not dilute the light color of the tower coming to grade. (B4, C2)

RESPONSE:

Level 2 spandrel color is now a darker tone, consistent with DRB guidance.



UPDATE:

Enclosed amenity area added at roof to support the exterior amenity space.

UPDATE:

Vertical reveal to provide separation between the tower and podium massing.

UPDATE:

Solid brick return provided to strengthen the edge of the podium as it returns into the tower.

BOARD CONDITION #5

SITE B - LOBBY ENTRANCE; 2C;

Scale up the main lobby entrance to at least 2 stories height, corresponding with the interior volume, and detailed with materials, colors and forms not similar to the retail entry boxes. (C2, C4)

RESPONSE:

The building entrance has been designed with a two story expression per DRB guidance. A new departure is requested on sheet G114 in MUP for the entrance canopy which is now above the maximum 15' requirement for overhead weather protection. See sheet G321 in MUP (Residential Tower).

3 DESIGN UPDATES **SITE B** - RESIDENTIAL

TERRY AVENUE ELEVATION
DRB #1



3 DESIGN UPDATES

SITE B - RESIDENTIAL

TERRY AVENUE ELEVATION

DRB #2

BOARD CONDITION #4

SITE B - HOWELL FACADE; 2A;

Revise the level 2 spandrel cladding exposed on the Olive and Terry elevations to be a darker tone to complement the podium brick and not dilute the light color of the tower coming to grade. (B4, C2)

RESPONSE:

Level 2 metal panel color is now a darker tone, consistent with DRB guidance.



UPDATE:

Generator intake louver area has been modified. Revised design is more integrated into the facade and will have a custom artist designed screen over the louvers.

UPDATE:

Dark stone piers added to ground level facade to provide more detail at the pedestrian scale.

UPDATE:

Wood clad entrance portal added to signify main retail storefront.

UPDATE:

Louvers added at ground level for servicing retail. The louvers allow the retail space to function successfully which is consistent with Board's feedback.

UPDATE:

Loading door has been relocated to Howell Street. Additional glass storefront added along Terry Avenue.

3 DESIGN UPDATES **SITE B** - RESIDENTIAL

HOWELL STREET ELEVATION
DRB #1



3 DESIGN UPDATES

SITE B - RESIDENTIAL

HOWELL STREET ELEVATION

DRB #2

UPDATE:

Vertical reveal to provide separation between the tower and podium massing.

BOARD CONDITION #7

SITE B & SITE C - THREE LOADING DOORS; 2E & 5C;

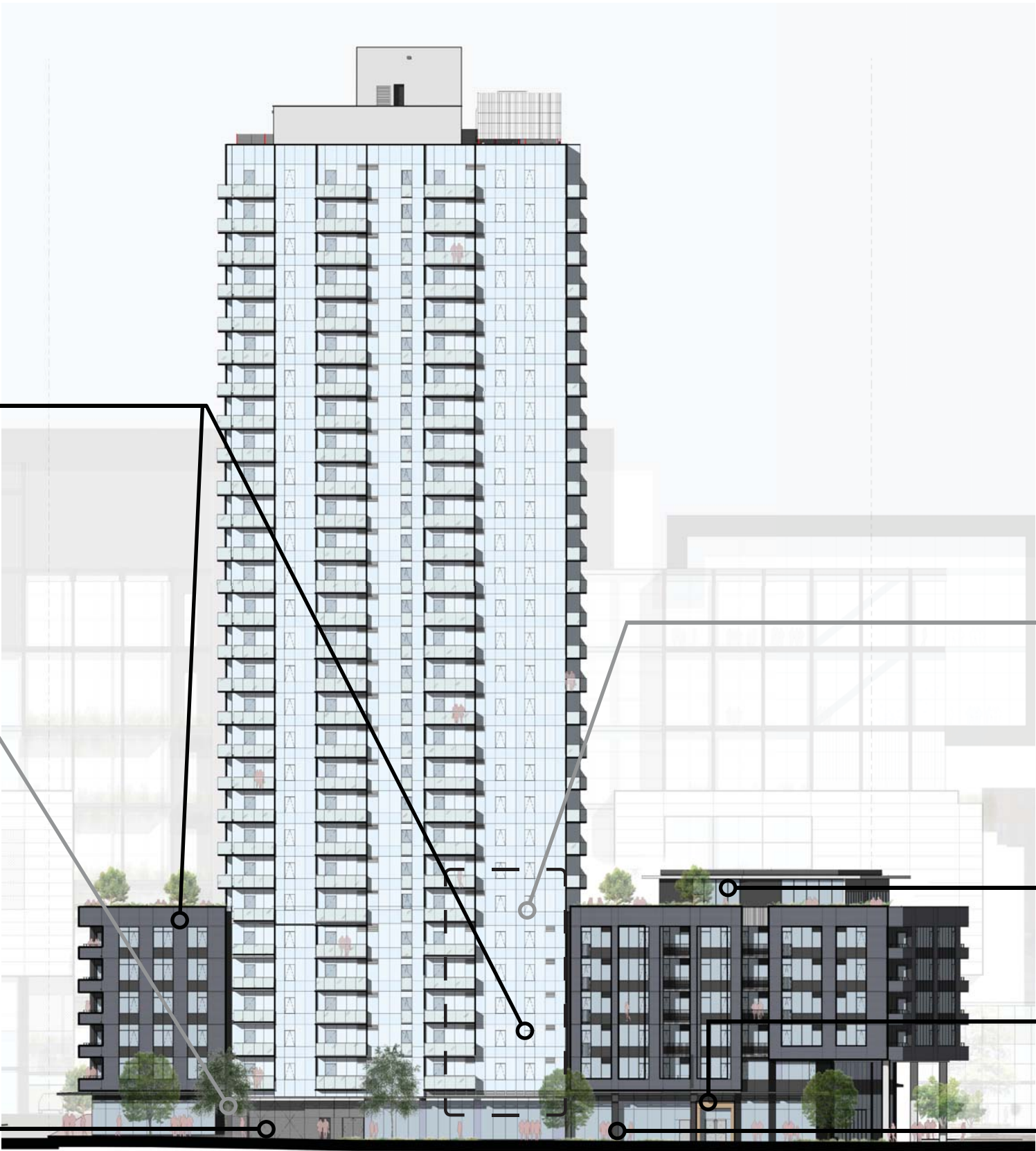
Design the three overhead or sectional doors be include an artful treatment, with high pedestrian visual interest, and/or be translucent glass (the gray door image shown on pg 76 was too generic; the two orange examples shown on pg 43 had more visual interest). (C3, E3)

RESPONSE:

Consistent with DRB guidance, the garage doors on Site B and C will have a custom design that provides pedestrian visual interest.

UPDATE:

Ground level facade changes due to relocation of loading door from Terry Avenue to Howell Street. Gas meter relocated along Howell St. to combine the service areas in one place along the storefront. The loading door will be custom design by an artist while the surrounding service areas will have a perforated metal finish.



BOARD CONDITION #3

SITE B - HOWELL FACADE; 2A;

The western part of the brick podium on Howell overlaps too much over the tower form above; revise the eastern two bays of brick podium [pg 67/top-middle] to the tower vocabulary. (B4, C2)

RESPONSE:

Consistent with DRB guidance, the western portion of the brick podium along Howell has been moved east to align with the tower edge above.

UPDATE:

Level 7 pavillion has been modified, dark metal panel added to match the podium below.

UPDATE:

Louvers added at ground level for servicing retail. The louvers allow the retail space to function successfully which is consistent with Board's feedback.

UPDATE:

Ground level facade setback 3' to create an 18' sidewalk width for a potential future bus stop location.

3 DESIGN UPDATES **SITE B** - RESIDENTIAL

9TH AVENUE ELEVATION
DRB #1



3 DESIGN UPDATES

SITE B - RESIDENTIAL

9TH AVENUE ELEVATION

DRB #2

UPDATE:
Rooftop screen lowered for better overall proportion with the 9th Avenue facade.

UPDATE:
Elevator penthouse raised to provide access to the rooftop amenity.

BOARD CONDITION #4

SITE B - HOWELL FACADE; 2A;
Revise the level 2 spandrel cladding exposed on the Olive and Terry elevations to be a darker tone to complement the podium brick and not dilute the light color of the tower coming to grade. (B4, C2)

RESPONSE:
Level 2 metal panel color is now a darker tone, consistent with DRB guidance.

UPDATE:
Dark stone piers have been added at the ground level to provide more detail at the pedestrian scale.

UPDATE:
Operable windows have been added at the street level facade to connect the interior retail spaces to the street.



BOARD CONDITION #6

SITE B - WEST ELEVATION AND FLAT IRON CONDITION; 2D;
Add brick to wrap the two west corners to frame the large 'glass bay' [65, 68]. Break down the glass bay with articulation, possibly darker framing or mullions, but spandrels should be minimal and few to emphasize this bay as a response to the angled street views from the west. Revise the trellis material to not be wood, but retain the wood soffit shown over the public plaza [68]. (B1, C2)

RESPONSE:
Consistent with DRB guidance, the west elevation has a brick facade wrapping the building corners to frame the large glass bay. Dark mullions provide articulation within the glass bay. The wood trellis at Level 7 has been eliminated. See sheet G311 and G322 in MUP (Residential Tower).

UPDATE:
Public benefit space at street level has been updated and approved by the Seattle Design Commission.



VIEW LOOKING SOUTHEAST AT 9TH AVE. AND HOWELL STREET



VIEW LOOKING SOUTHEAST AT 9TH AVE. AND HOWELL STREET



VIEW LOOKING SOUTHWEST AT TERRY AVE. AND HOWELL ST.



VIEW LOOKING SOUTHWEST AT TERRY AVE. AND HOWELL ST.



DRB #1

VIEW LOOKING NORTHWEST AT TERRY AVE. AND OLIVE WAY



DRB #2

VIEW LOOKING NORTHWEST AT TERRY AVE. AND OLIVE WAY

3 DESIGN UPDATES RENDERINGS **SITE B** - RESIDENTIAL

DRB #1



VIEW LOOKING NORTH AT OLIVE WAY



VIEW LOOKING NORTH AT OLIVE WAY

3 DESIGN UPDATES RENDERINGS **SITE B** - RESIDENTIAL

DRB #1



VIEW LOOKING WEST ON OLIVE WAY



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VIEW LOOKING WEST ON OLIVE WAY



VIEW OF OLIVE WAY PLAZA LOOKING TOWARD TERRY AVE.

3 DESIGN UPDATES RENDERINGS **SITE B** - RESIDENTIAL

DRB #2



VIEW LOOKING TOWARD OLIVE WAY ENTRANCE

3 DESIGN UPDATES SITE C - OFFICE

OLIVE WAY ELEVATION
DRB #1



3 DESIGN UPDATES

SITE C - OFFICE

OLIVE WAY ELEVATION

DRB #2

BOARD CONDITION #11

SITE C - GROUND LEVEL FACADE REVISIONS; 4B, 5A, 5B;

Revise the middle bay glass into the loading ramp on Howell Street [91/middle] be about 50% clear with frit or patterns at pedestrian eye-level; revise the upper metal panel/louvers between the black piers [97] to a darker tone, but not matching the stone; revise and strengthen the edges of the southwest entry volume. (C2) 12)

RESPONSE:

The metal panel/louver areas between the black stone piers along Terry Avenue, Olive Way and Boren Avenue are a darker tone. The lobby entry box edges have been widened to give it a stronger definition.

UPDATE:

Canopy raised to better align with adjacent architecture.



UPDATE:

Louvers added beyond at terrace to service the lobby directly adjacent to outdoor terrace amenity space.

UPDATE:

Clerestory windows changed to louvers to service retail spaces at ground level. The louvers allow the retail space to function successfully which is consistent with the Board's feedback.

UPDATE:

Stone piers lowered to align with the building facade and strengthen the podium edge.

BOARD CONDITION #9

SITE B & SITE C - CANOPIES; 2F;

Redesign to eliminate all canvas canopies and use only translucent or fritted glass for variation within a predominantly clear glass canopy wrap. (C5, C2)

RESPONSE:

All canvas canopies have been eliminated from the design. Site B and Site C will have clear glass canopies consistent with DRB guidance.

3 DESIGN UPDATES SITE C - OFFICE

TERRY AVENUE ELEVATION
DRB #1

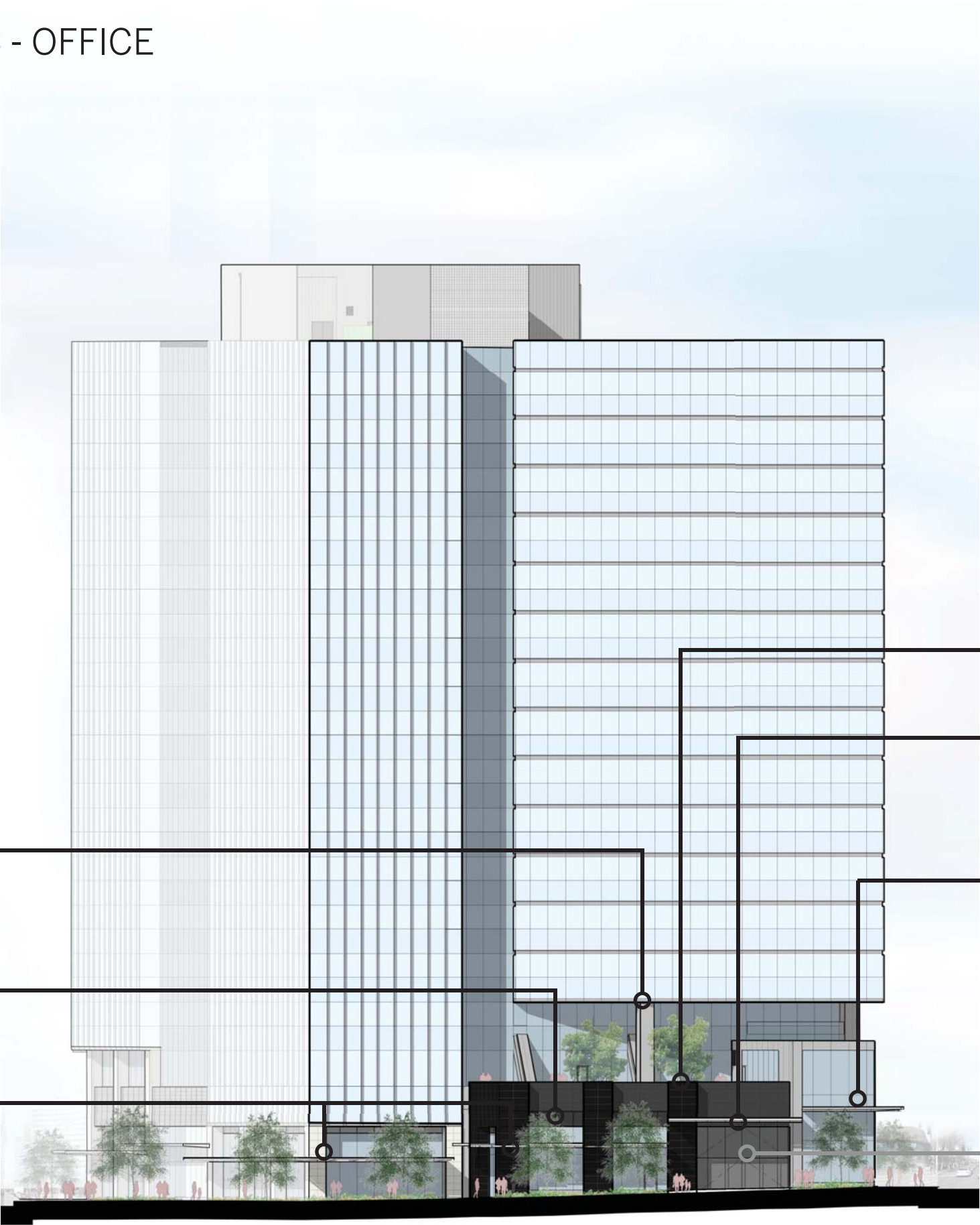


3 DESIGN UPDATES

SITE C - OFFICE

TERRY AVENUE ELEVATION

DRB #2



UPDATE:
Column added for structural integrity of tower design above.

UPDATE:
Clerestory windows changed to louvers to service retail spaces at ground level. The louvers allow the retail space to function successfully which is consistent with the Board's feedback.

UPDATE:
Canopies added at corners for continuous overhead weather protection.

UPDATE:
Stone piers lowered to align with the building facade and strengthen podium edge.

UPDATE:
Canopy lowered to no longer align with lobby canopy. Separating the canopy from the lobby helps signal to pedestrians that this is a different area with an active driveway.

UPDATE:
Canopy raised to align with architectural systems along Olive Way.

BOARD CONDITION #7
SITE B & SITE C - THREE LOADING DOORS; 2E & 5C;
Design the three overhead or sectional doors to include an artful treatment, with high pedestrian visual interest, and/or be translucent glass (the gray door image shown on pg 76 was too generic; the two orange examples shown on pg 43 had more visual interest). (C3, E3)

RESPONSE:
Consistent with DRB guidance, the garage doors on Site B and C will have a custom design that provides pedestrian visual interest. The Site C door will be a minimum of 51% open to outside air.

3 DESIGN UPDATES SITE C - OFFICE

HOWELL STREET ELEVATION
DRB #1



3 DESIGN UPDATES

SITE C - OFFICE

HOWELL STREET ELEVATION

DRB #2

BOARD CONDITION #12

SITE C - TOWER MODULATION AND REVEALS; 6B;

All three vertical reveals should be made deeper if possible (in particular the south side of the Terry Green Street reveal, and they should be a consistent dark gray spandrel and glass, as shown on updated renderings at the meeting. In particular, the Howell Street reveal should not include expressed horizontals, but one dark, vertical pilaster is acceptable. (B2, B4)

RESPONSE:

All tower reveals will have a transparent glazing that will read darker than the more reflective glazing of the typical curtain wall and spandrels.

UPDATE:

Vertical pilaster removed at tower and ground level below. Removing pilaster gives the recess a stronger presence on the facade and creates a clear separation between the tower massing.

UPDATE:

Canopy raised for better proportions at the building and pedestrian level.

BOARD CONDITION #11

SITE C - GROUND LEVEL FACADE REVISIONS; 4B, 5A, 5B;

Revise the middle bay glass into the loading ramp on Howell Street [91/middle] be about 50% clear with frit or patterns at pedestrian eye-level; revise the upper metal panel/ louvers between the black piers [97] to a darker tone, but not matching the stone; revise and strengthen the edges of the southwest entry volume. (C2) 12)

RESPONSE:

The middle glass bay looking into the loading ramp along Howell St. has a custom designed glass frit, consistent with DRB guidance.



UPDATE:

Elevator penthouse and mechanical screening raised to provide access to rooftop amenity.

UPDATE:

Canopy shifted to align with the recess above and connect the tower massing to the pedestrian experience.

UPDATE:

Louvers added to service retail space. The louvers allows the retail space to function successfully which is consistent with the Board's feedback.

UPDATE:

Canopy raised for better proportion at the building and pedestrian level.

3 DESIGN UPDATES **SITE C** - OFFICE

BOREN AVENUE ELEVATION
DRB #1



3 DESIGN UPDATES

SITE C - OFFICE

BOREN AVENUE ELEVATION

DRB #2



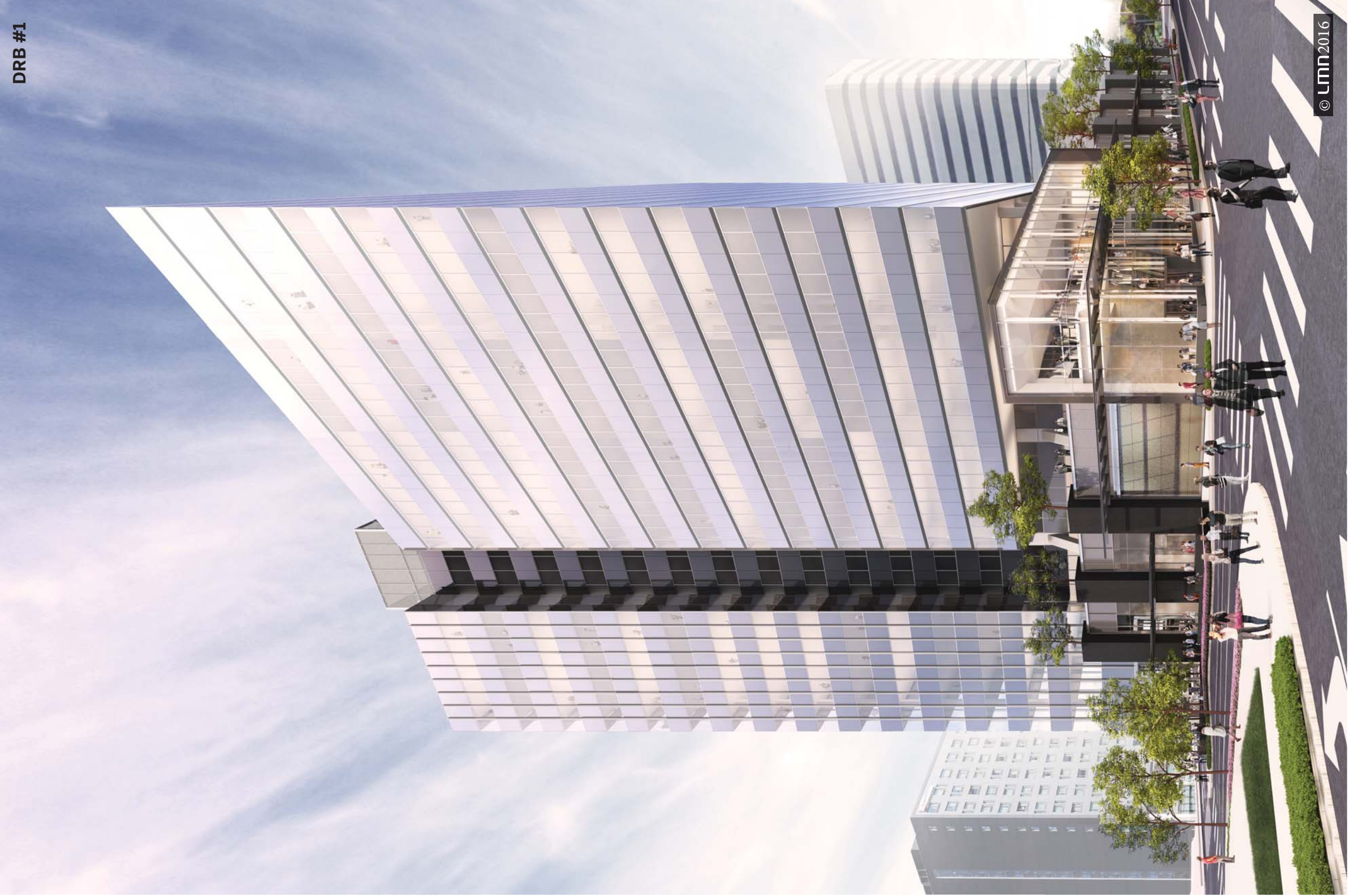
UPDATE:
Clerestory windows changed to louvers to service retail spaces at ground level. The louvers allow the retail space to function successfully which is consistent with the Board's feedback.

UPDATE:
Stone piers lowered to align with the building facade and strengthen podium edge.

UPDATE:
Louvers added to service office space and align with louvers above.

BOARD CONDITION #7
SITE B & SITE C - THREE LOADING DOORS; 2E & 5C;
Design the three overhead or sectional doors be include an artful treatment, with high pedestrian visual interest, and/or be translucent glass (the gray door image shown on pg 76 was too generic; the two orange examples shown on pg 43 had more visual interest). (C3, E3)

RESPONSE:
Consistent with DRB guidance, the garage doors on Site B and C will have a custom design that provides pedestrian visual interest. The Site C door will be a minimum of 51% open to outside air.



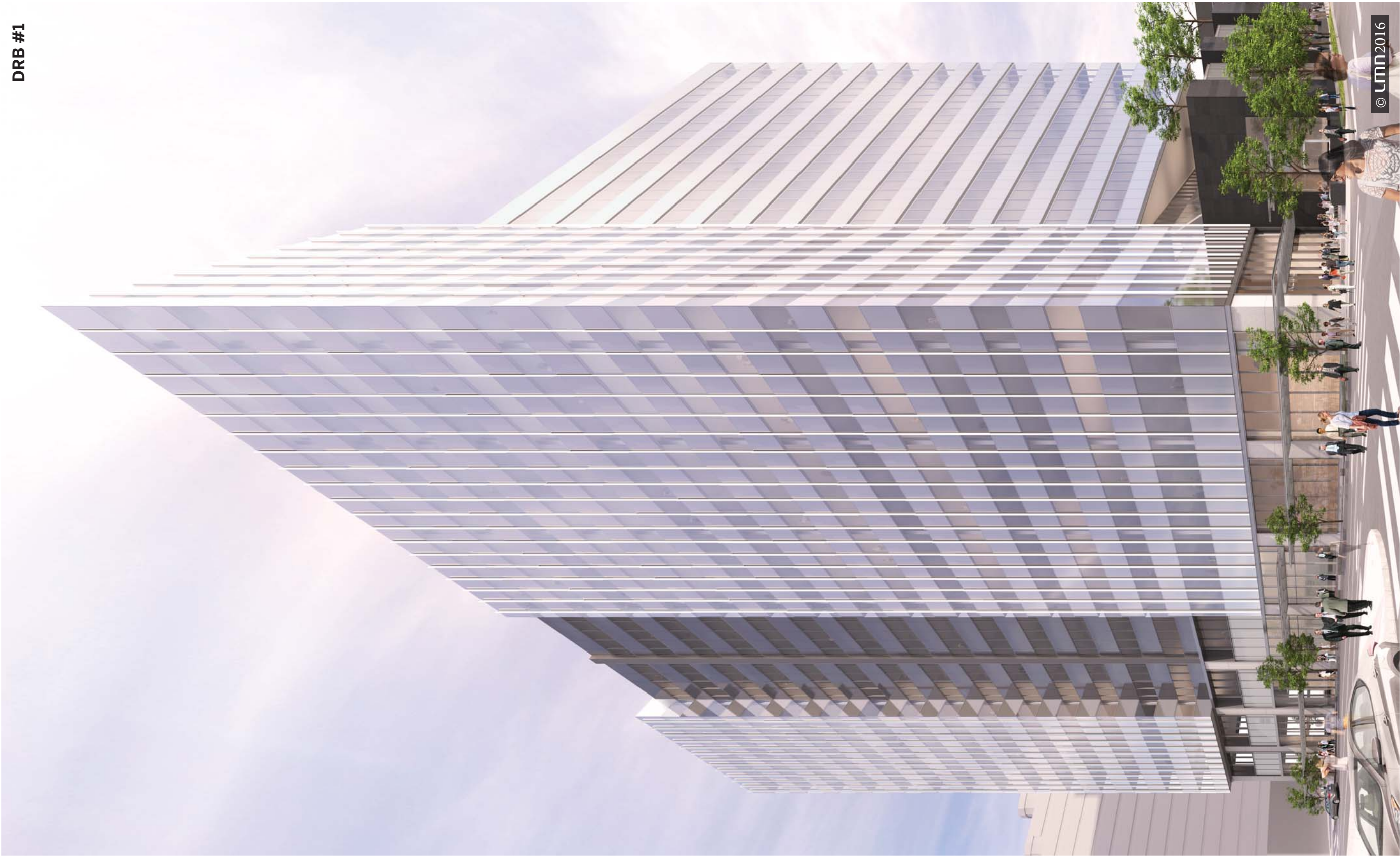
VIEW LOOKING NORTHEAST AT TERRY AVE. AND OLIVE WAY



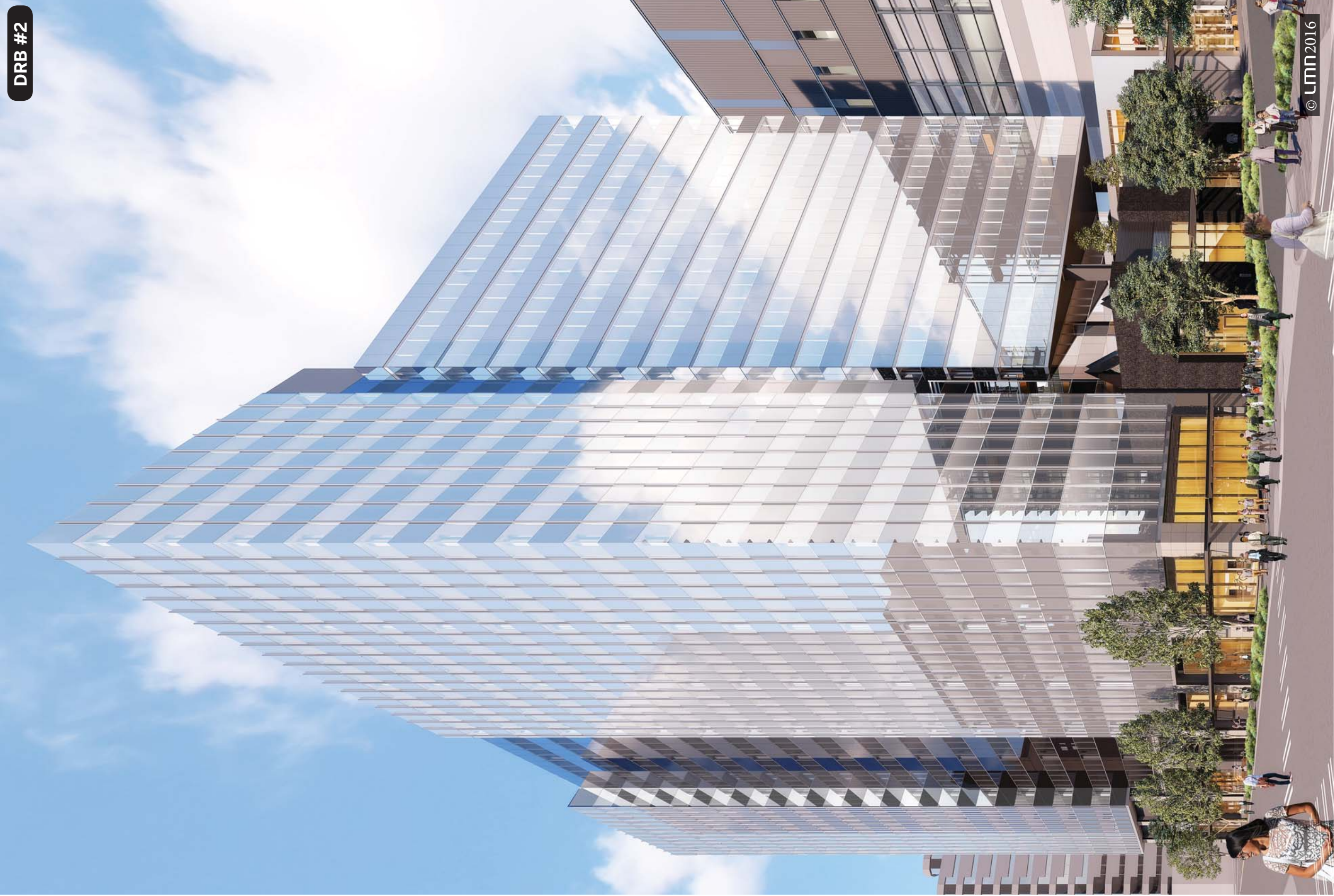
DRB #2

VIEW LOOKING NORTHEAST AT TERRY AVE. AND OLIVE WAY

DRB #1



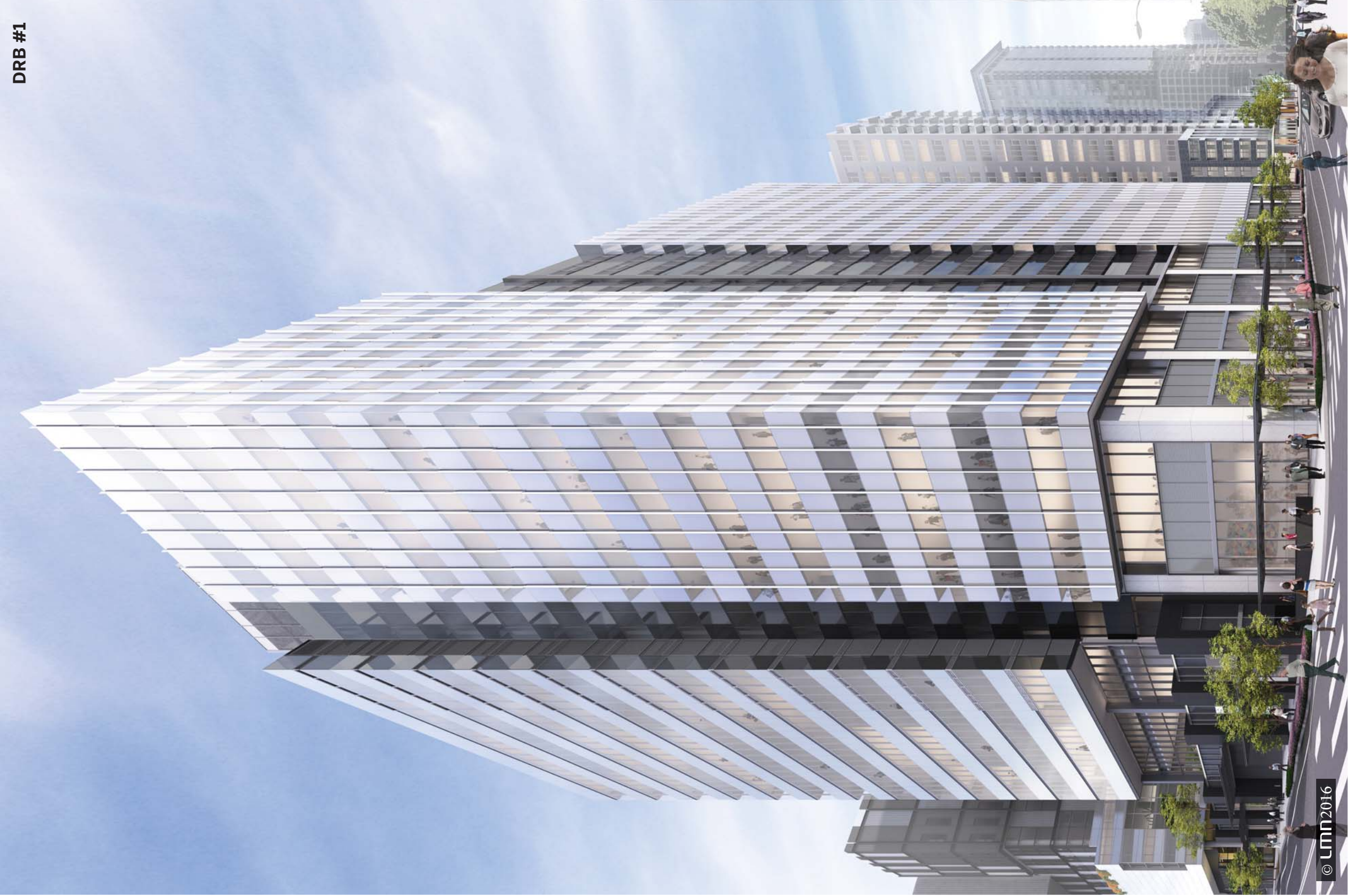
VIEW LOOKING SOUTHEAST AT TERRY AVE. AND HOWELL ST.



DRB #2

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VIEW LOOKING SOUTHEAST AT TERRY AVE. AND HOWELL ST.



VIEW LOOKING SOUTHWEST AT BOREN AVE. AND HOWELL ST.



VIEW LOOKING SOUTHWEST AT BOREN AVE. AND HOWELL ST.



VIEW LOOKING WEST ON OLIVE WAY



VIEW LOOKING WEST ON OLIVE WAY

3 DESIGN UPDATES LANDSCAPE

SITE B PLAZA AT 9TH AVE, HOWELL ST, AND OLIVE WAY



DRB # 1 - 05-17-2016



DRB #2



3 DESIGN UPDATES LANDSCAPE



UPDATE:

Refined plaza design to increase visual interest for pedestrians, provide additional seating and planting opportunities, minimize walls, and increase circulation through the space, per SDC guidance.

RESPONSE CONDITION #10:

Updated design maintains basic circulation and programming strategy as recommended "Alternate 2" design. Stair height has been minimized and width increased to ease pedestrian circulation through the space. Additional planting has been added to maximize lush, vertical planting and reinforce Green Street concept.

UPDATE:

Upgraded concrete roadway finish to correspond to plaza paving color and texture, for greater visual continuity within this "urban room" Green Street context. Added concrete crosswalks with side striping, per SDC guidance.

DRB #2

3 DESIGN UPDATES LANDSCAPE

TERRY AVE GREEN STREET - CORNER PLAZAS AT SITES B AND C



DRB # 1 - 05-17-2016



DRB #2



3 DESIGN UPDATES LANDSCAPE



RESPONSE CONDITION #2:
Direct retail access has been added to both Site B and Site C plazas.

UPDATE:
Terry Ave corner plaza geometry incorporates gentle curves that relate to larger Green Street concept of arcing green bands visually tying the space together to create an urban room with continuity with Terry Ave north of Howell St.

RESPONSE CONDITION #2:
Lush elevated planters tie into larger Terry Ave Green Street planting palette.

RESPONSE CONDITION #2:
Additional seating opportunities provided, both fixed and moveable furnishings.

RESPONSE CONDITION #2:
Paving zone against the building facade has been widened and corner planter reconfigured to better accommodate diagonal pedestrian desire lines that continue toward the 9th Ave Green Street.

DRB #2

3 DESIGN UPDATES LANDSCAPE

SITE C BOREN & HOWELL CORNER PLAZA



DRB # 1 - 05-17-2016



DRB #2



3 DESIGN UPDATES LANDSCAPE



RESPONSE CONDITION #2:
Lush elevated planters include a variety of low shrubs as well as small vine maple trees for added visual interest.

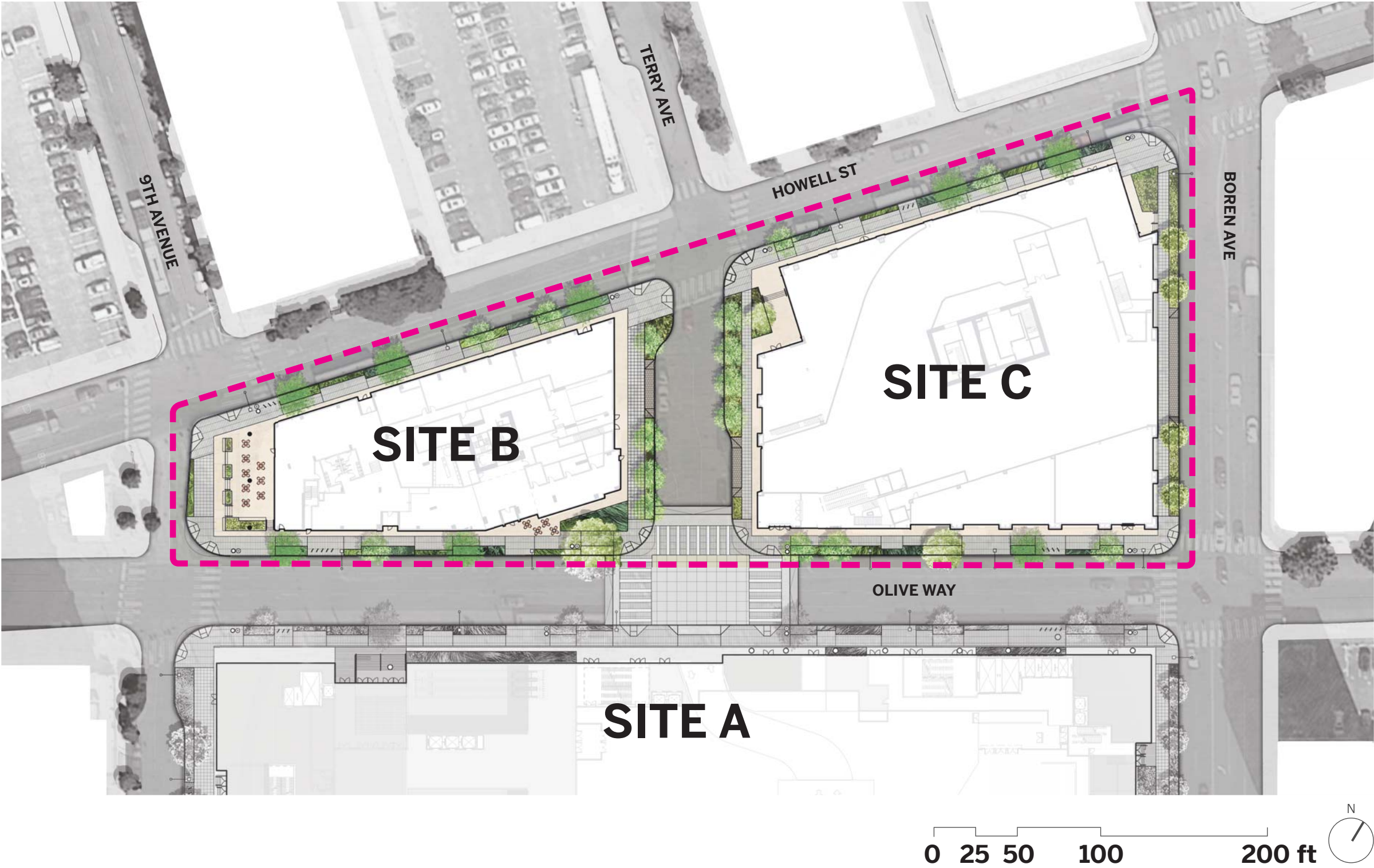
RESPONSE CONDITION #2:
Direct retail access has been added to both Site B and Site C plazas.

RESPONSE CONDITION #2:
Additional seating opportunities provided with two custom wood benches integrated into concrete planter walls.

DRB #2

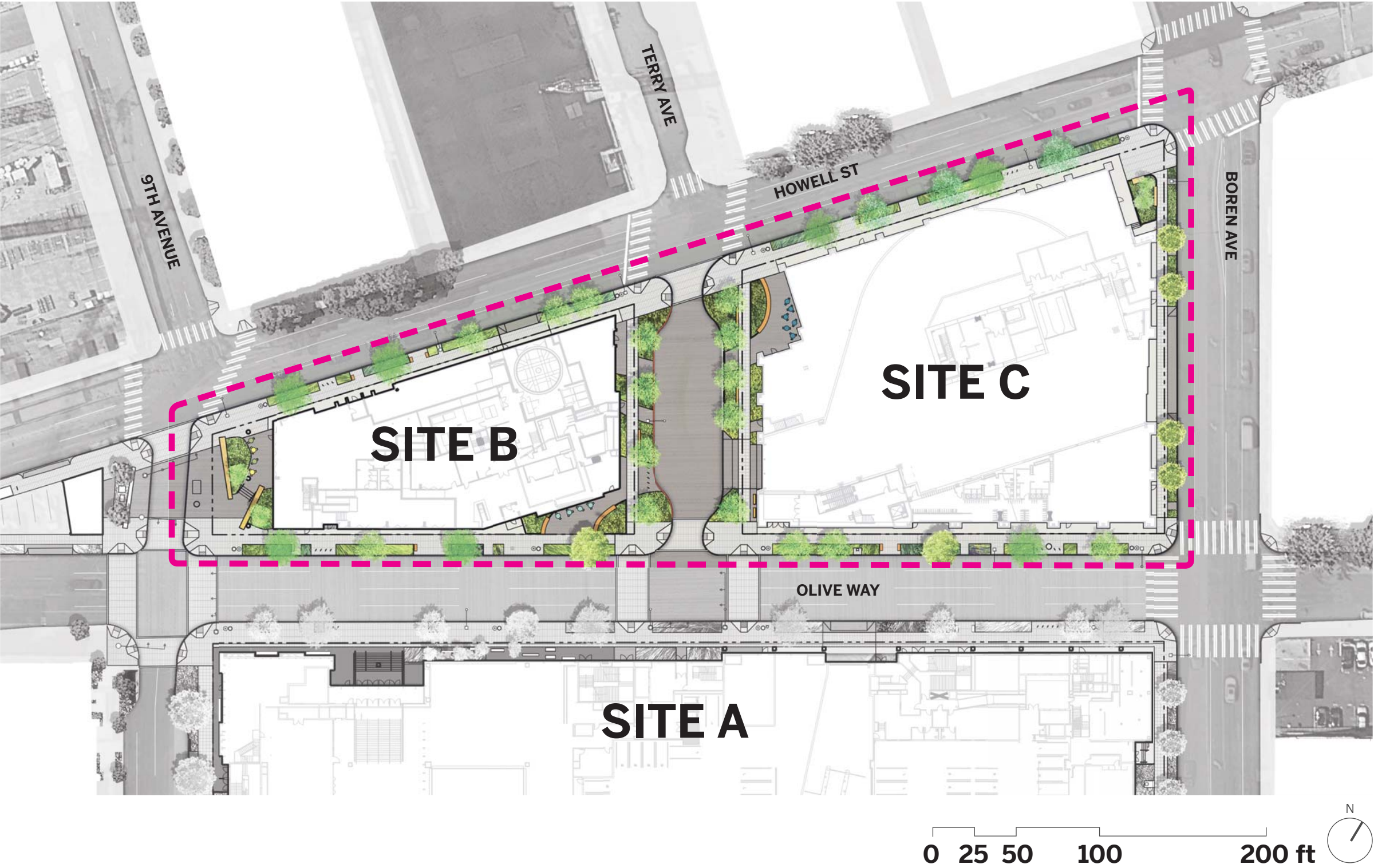
3 DESIGN UPDATES LANDSCAPE

DRB # 1 - 05-17-2016 - PRESENTATION BOOKLET PAGE 101



3 DESIGN UPDATES LANDSCAPE

DRB #2



4 SITE C ALTERNATE FLOOR PLANS SITE C - OFFICE - ALTERNATE

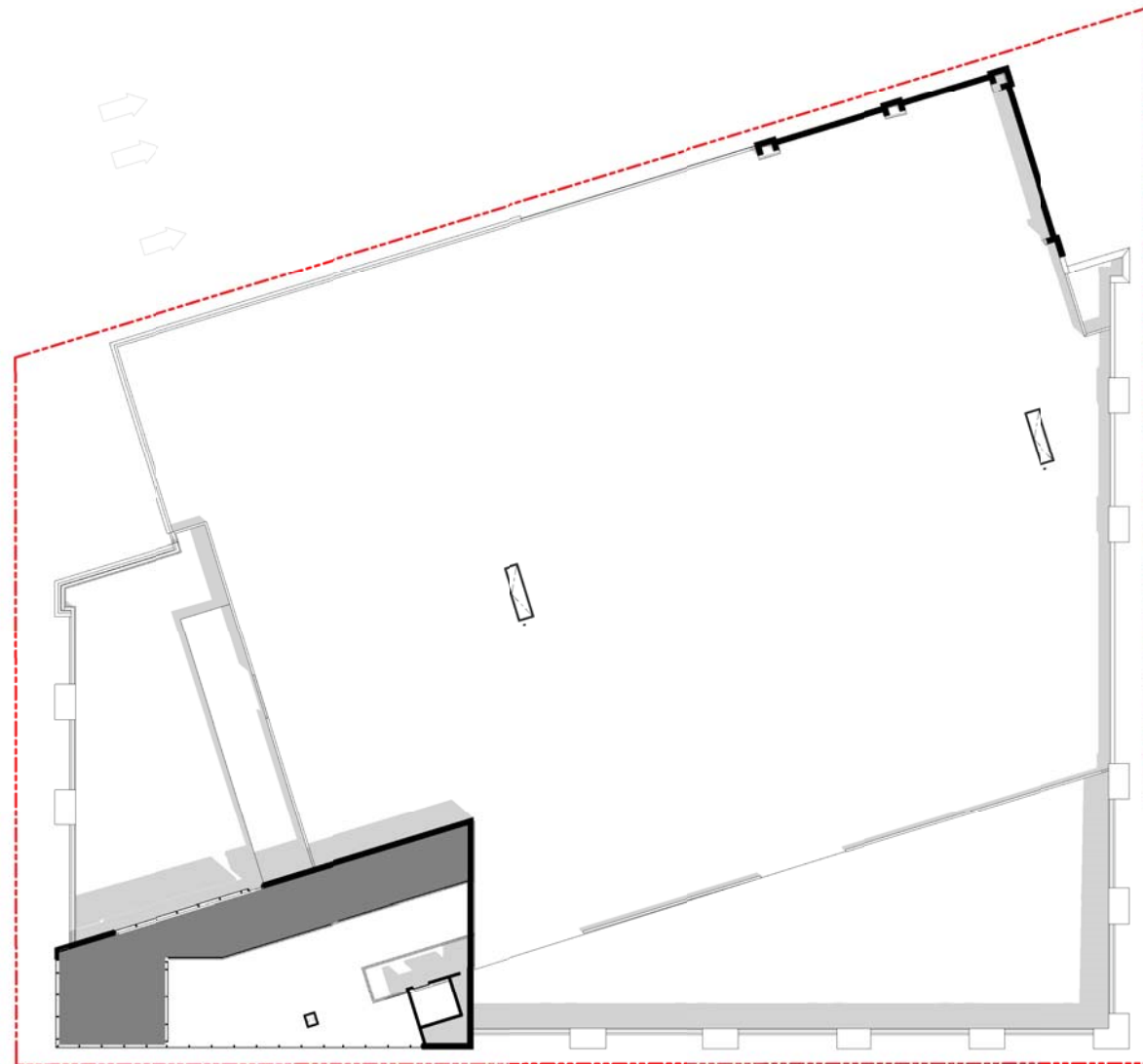
SITE C - ALTERNATE DESIGN

The alternate design for Site C looks at the possibility of an interim condition where the proposed office tower is not built concurrently with the Washington State Convention Center Addition (WSCCA). The WSCCA loading dock entrance and ramp are located in the podium of Site C, these elements are required for the WSCCA to function and will be built as part of the WSCCA. If an interim condition occurs the following alternate design is being proposed until the office tower begins construction. If the office tower is constructed concurrently with the WSCCA then no interim condition is necessary and the full tower will be built.



LEVEL 1

4 SITE C ALTERNATE FLOOR PLANS SITE C - OFFICE - ALTERNATE



LEVEL 2

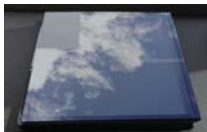


ROOF

4 SITE C ALTERNATE ELEVATIONS SITE C - OFFICE - ALTERNATE



A: Clear Vision (GL - 1)



B: Clear Spandrel (GL - 2)



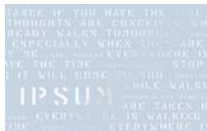
C: Reflective Vision (GL - 3)



D: Reflective Spandrel (GL - 4)



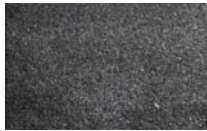
E: Custom Metal Design (PERF-MTL SCREEN)



F: Fritted Glass (GL - 5)



G: Light Stone (STN - 1)



H: Dark Stone (STN - 2)



I: Metal Panel (CMPST MTLP-2 LIGHT)



J: Metal Panel (CMPST MTLP-1 DARK)



K: Louver Dark (LVR-1)



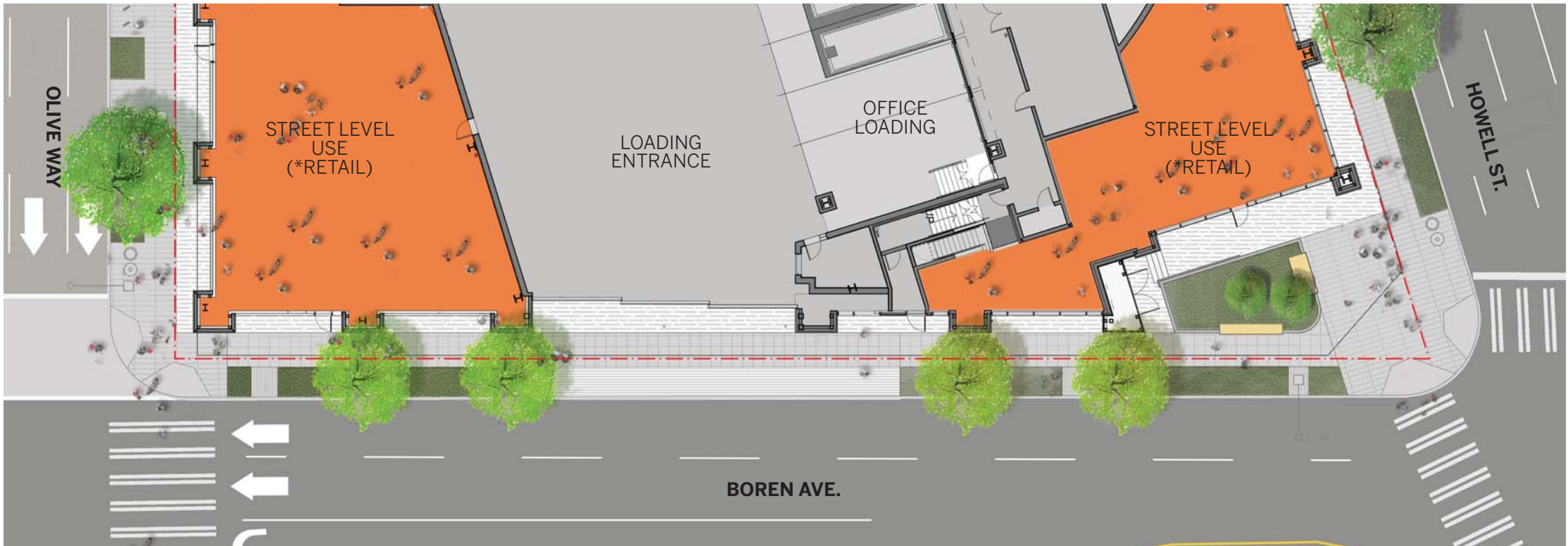
L: Louver Light (LVR-2)



M: Box Ribbed Metal Panel (MTLP-1)



N: Fiber Cement Panel (FCP-1)

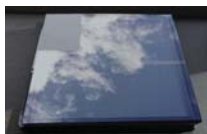


*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2021.

4 SITE C ALTERNATE ELEVATIONS SITE C - OFFICE - ALTERNATE



A: Clear Vision (GL - 1)



B: Clear Spandrel (GL - 2)



C: Reflective Vision (GL - 3)



D: Reflective Spandrel (GL - 4)



E: Custom Metal Design (PERF-MTL SCREEN)



F: Fritted Glass (GL - 5)



G: Light Stone (STN - 1)



H: Dark Stone (STN - 2)



I: Metal Panel (CMPST MTLP-2 LIGHT)



J: Metal Panel (CMPST MTLP-1 DARK)



K: Louver Dark (LVR-1)



L: Louver Light (LVR-2)



M: Box Ribbed Metal Panel (MTLP-1)



N: Fiber Cement Panel (FCP-1)

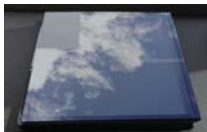


*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2021.

4 SITE C ALTERNATE ELEVATIONS SITE C - OFFICE - ALTERNATE



A: Clear Vision (GL - 1)



B: Clear Spandrel (GL - 2)



C: Reflective Vision (GL - 3)



D: Reflective Spandrel (GL - 4)



E: Custom Metal Design (PERF-MTL SCREEN)



F: Fritted Glass (GL - 5)



G: Light Stone (STN - 1)



H: Dark Stone (STN - 2)



I: Metal Panel (CMPST MTLP-2 LIGHT)



J: Metal Panel (CMPST MTLP-1 DARK)



K: Louver Dark (LVR-1)



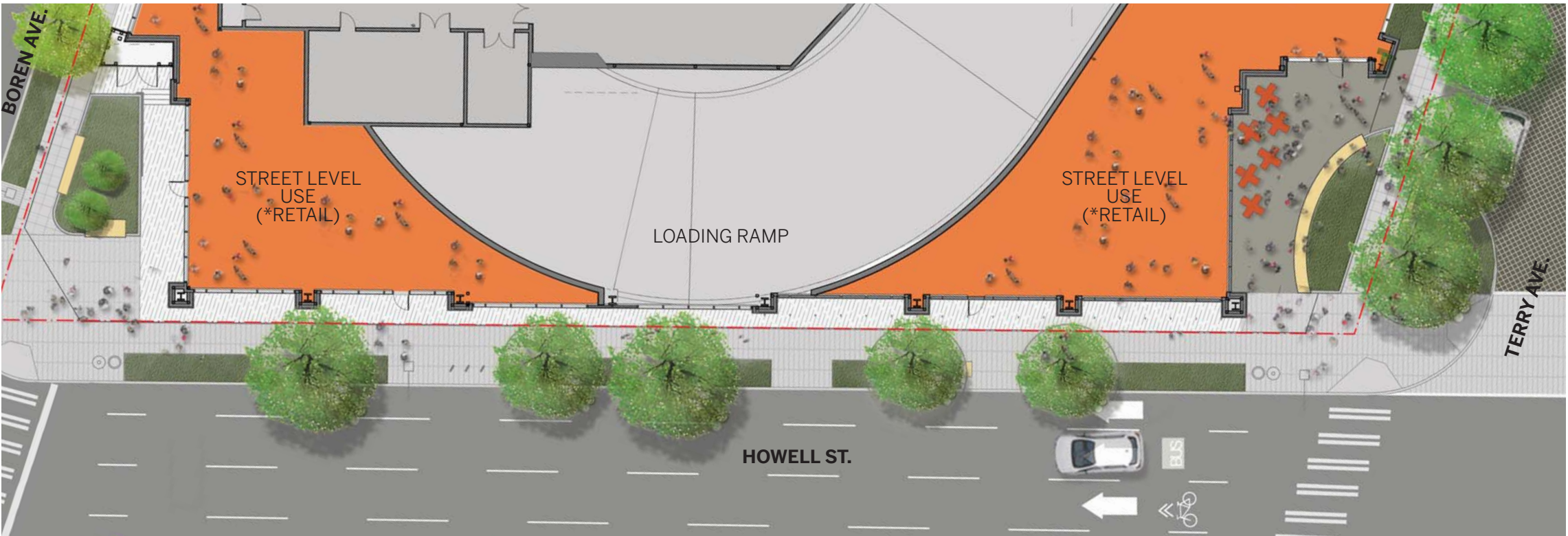
L: Louver Light (LVR-2)



M: Box Ribbed Metal Panel (MTLP-1)



N: Fiber Cement Panel (FCP-1)



*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2021.

4 SITE C ALTERNATE ELEVATIONS SITE C - OFFICE - ALTERNATE



A: Clear Vision (GL - 1)



B: Clear Spandrel (GL - 2)



C: Reflective Vision (GL - 3)



D: Reflective Spandrel (GL - 4)



E: Custom Metal Design (PERF-MTL SCREEN)



F: Fritted Glass (GL - 5)



G: Light Stone (STN - 1)



H: Dark Stone (STN - 2)



I: Metal Panel (CMPST MTLP-2 LIGHT)



J: Metal Panel (CMPST MTLP-1 DARK)



K: Louver Dark (LVR-1)



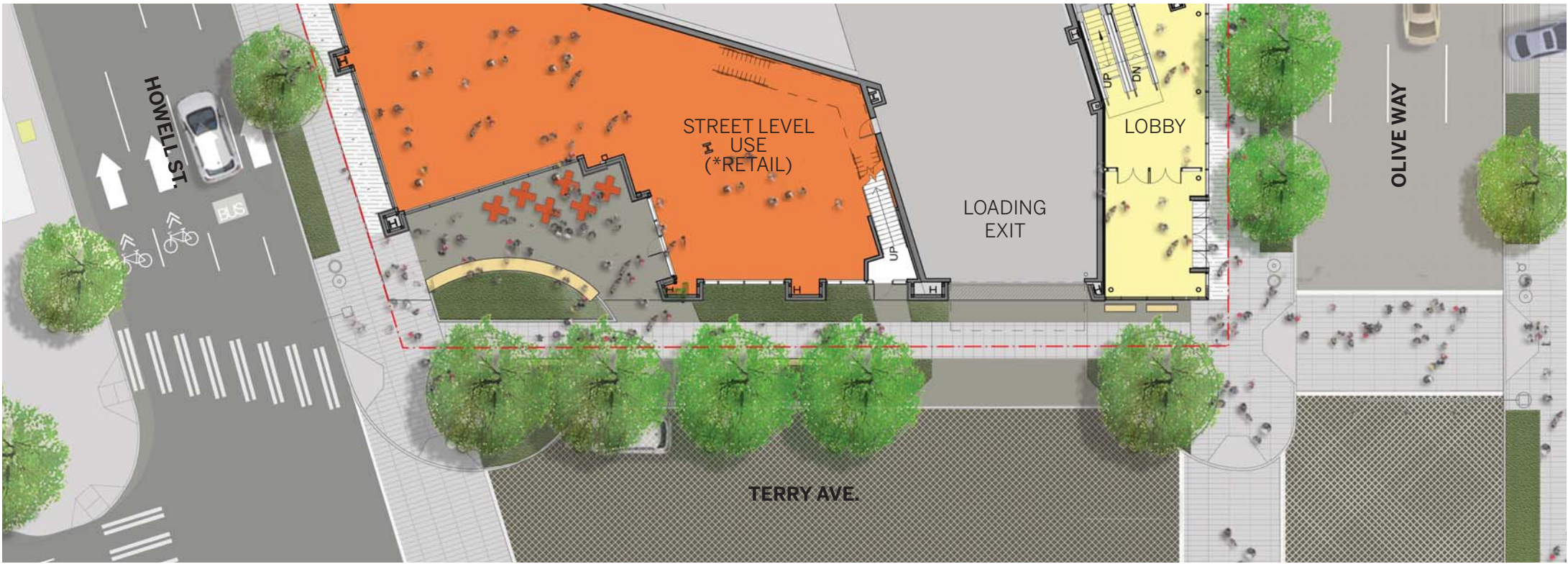
L: Louver Light (LVR-2)



M: Box Ribbed Metal Panel (MTLP-1)



N: Fiber Cement Panel (FCP-1)



*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2021.

4 SITE C ALTERNATE ELEVATIONS SITE C - OFFICE - ALTERNATE



VIEW LOOKING NORTHEAST AT TERRY AVE. AND OLIVE WAY

4 SITE C ALTERNATE ELEVATIONS SITE C - OFFICE - ALTERNATE



VIEW LOOKING SOUTHEAST AT TERRY AVE. AND HOWELL ST.

4 SITE C ALTERNATE ELEVATIONS SITE C - OFFICE - ALTERNATE



VIEW LOOKING SOUTHWEST AT BOREN AVE. AND HOWELL ST.

4 SITE C ALTERNATE ELEVATIONS **SITE C** - OFFICE - ALTERNATE



VIEW LOOKING WEST AT OLIVE WAY

5 PROPOSED DEPARTURES

DEPARTURES

SITE B - RESIDENTIAL

DEPARTURE #1 (FACADE SETBACK LIMIT) - THIS DEPARTURE WAS PREVIOUSLY APPROVED BY THE DESIGN REVIEW BOARD ON MAY 15, 2016

CHANGE IN SCOPE

Item #	Development Standard	Requirement	Modification Requested	Rationale
1	23.49.056.B.2.A.1 FACADE SETBACK LIMIT 23.49.056.B.2.D FACADE SETBACK LIMIT	For structures greater than 15 feet in height, the setback limits apply to the facade between an elevation of 15 feet and the minimum facade height (25 feet). The maximum setback of the facade at an intersection is 10 feet. The minimum distance the facade must conform is 20 feet from the corner along each street.	Relief from the setback limits applying between 15 feet and the minimum facade height (25 feet). Relief from the 10 foot maximum setback limit at the intersection of 9th Avenue and Olive Way. Further design refinement has created small changes in a dimension.	The building setback along 9th Avenue creates a larger outdoor amenity for seating or dining adjacent to active street level uses. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guideline section.

Relevant Design Guidelines

B3.1 building orientation - orient open space toward street intersections and towards street fronts with the highest pedestrian activity (open space oriented towards intersection and active streets)

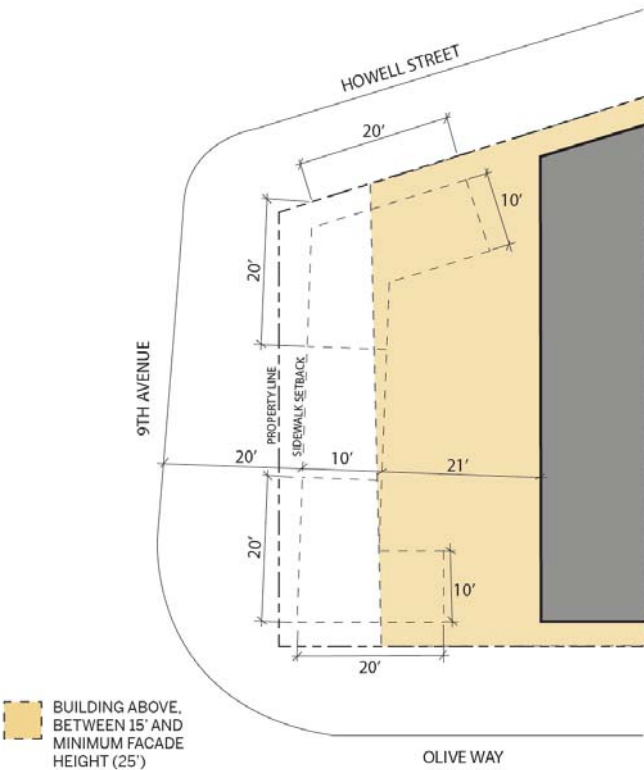
B3.3 pedestrian amenities at the ground level - setting the building back to create space adjacent to the sidewalk conducive to pedestrian-oriented activities (building setback to create space adjacent to the sidewalk)

D1.1 pedestrian enhancements

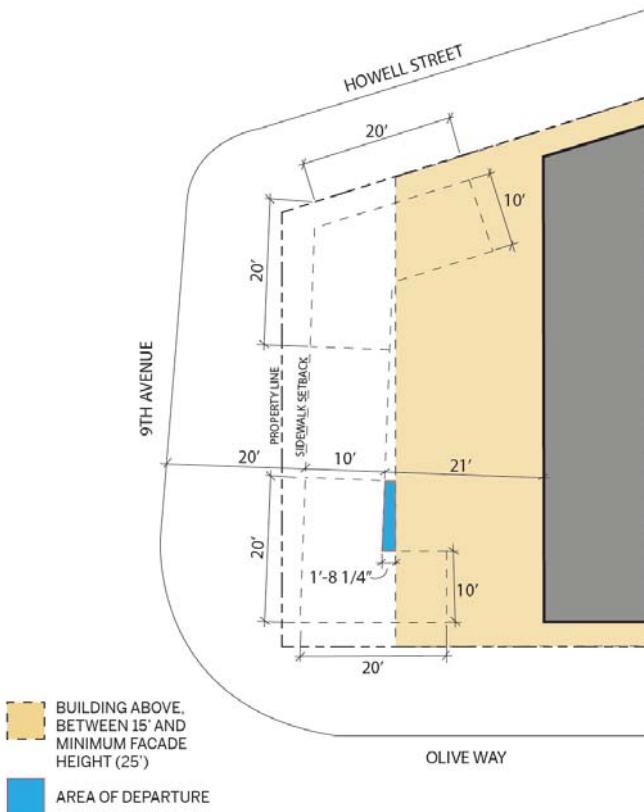
B. Preferable open space locations are to the south and west of tower development, or where the siting of the open spaces would improved solar access to the sidewalk (located on the west facade at the sidewalk)

C. Orient to receive the maximum direct sunlight possible, while providing shade in the warmer months. (setback and overhang balance the solar needs)

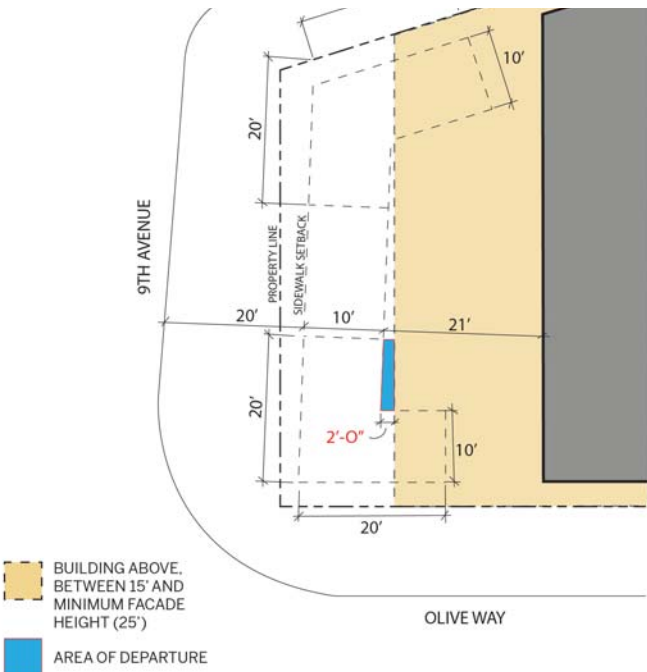
CODE BASELINE



DRB 1



DRB 2



Change in scope shown in RED

5 PROPOSED DEPARTURES

DEPARTURES

SITE B - RESIDENTIAL

DEPARTURE #2 (LANDSCAPING IN STREET RIGHT OF WAY) - NEW DEPARTURE

Item #	Development Standard	Requirement	Modification Requested	Rationale
2	23.49.056.F.1 23.49.056.F1a LANDSCAPE IN STREET RIGHT OF WAY	<p>The square footage of landscaped area provided shall be at least 1.5 times the length of the street lot line in linear feet. Total area of landscape required along lot line: 1.5 x 60.66 LF = 90.99 SF.</p> <p>Landscape area shall be at least 18 inches wide and shall be located in the public right-of-way along the entire length of the street lot line, except for building entrances, vehicular access or other connections between sidewalk and the lot, provided that the exceptions may not exceed 50% of the total length of the street lot line. Total length of landscape required along lot line: 50% of 60.66 LF = 30.33 LF.</p>	<p>Total landscape provided at R.O.W. edge is 0 SF.</p> <p>Total landscape provided within the lot line, adjacent to 9th Avenue R.O.W. is 356.2 SF.</p> <p>34 linear feet of landscape is provided within the lot line.</p>	<p>Due to utility congestion and clearance requirements from SCL, area within the public right of way will have insufficient soil volume for healthy planting. The proposed design works with DRB and SDC feedback to provide a wide sidewalk at the edge with seating at planter edges and concentrates the landscape in two areas to create a welcoming public plaza.</p>

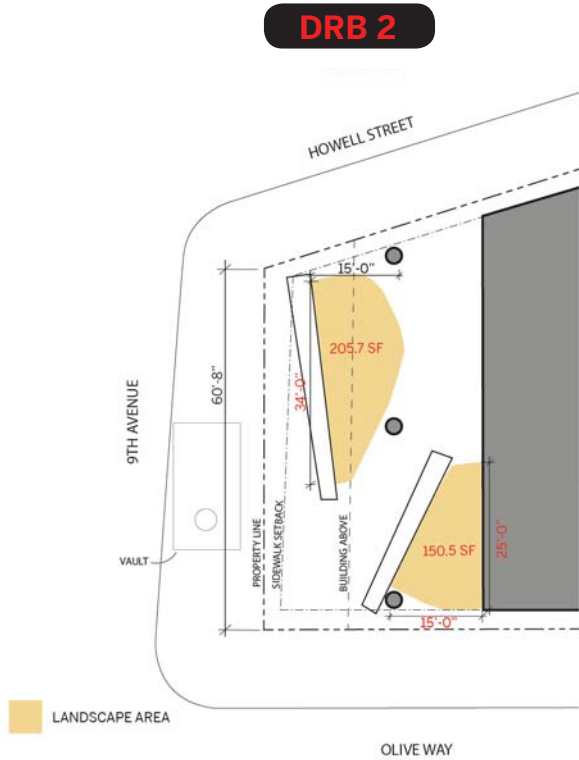
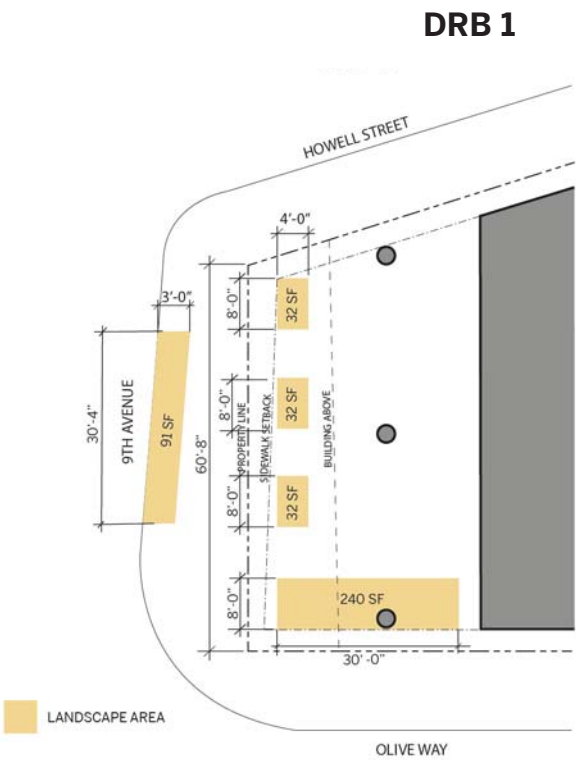
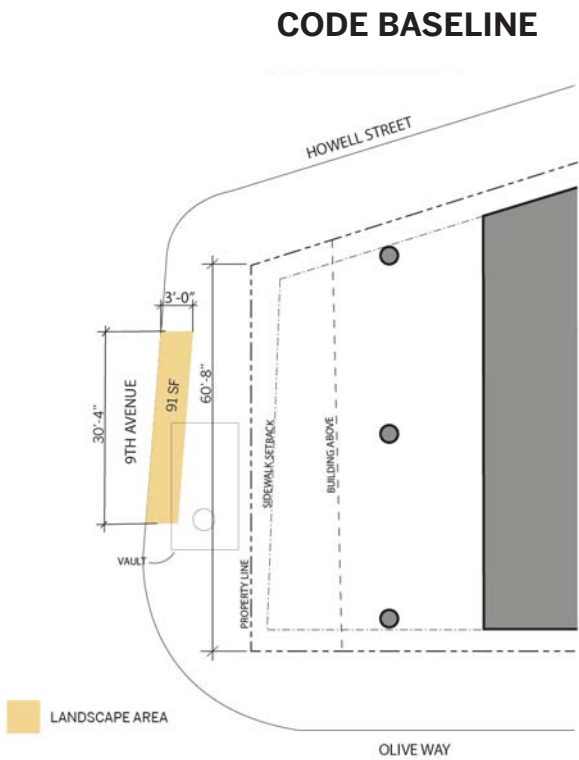
Relevant Design Guidelines

C1.1 Articulate the street level facade to provide an entertaining experience with street front open space.

D.1.F Provide inviting and usable open space with landscaping that enhances the open space and architecture.

D.2 Enhance the building with landscape.

D.3 Provide elements that define the place.



This departure was included in the MUP Submittal #2 but was not included in the DRB Presentation on 05/17/2016. The design and scope of the departure has changed. This is a new departure to the DRB.

5 PROPOSED DEPARTURES

DEPARTURES

SITE B - RESIDENTIAL

DEPARTURE #3 (FACADE SETBACK LIMITS) - THIS DEPARTURE WAS PREVIOUSLY APPROVED BY THE DESIGN REVIEW BOARD ON MAY 15, 2016

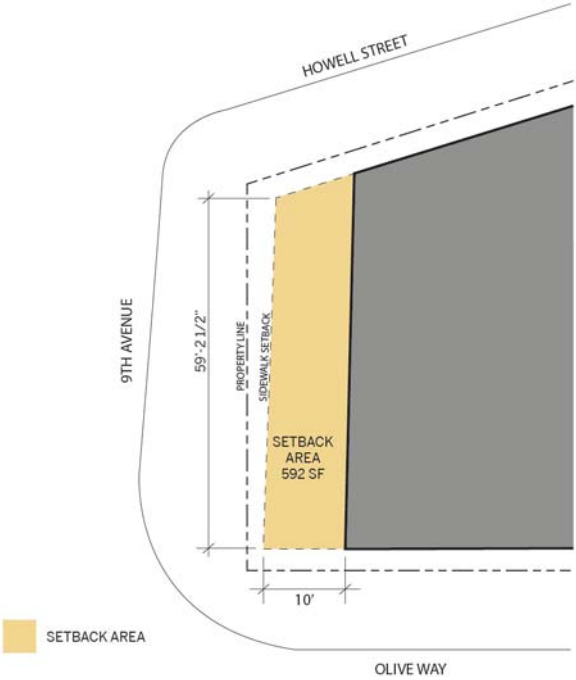
CHANGE IN SCOPE

Item #	Development Standard	Requirement	Modification Requested	Rationale
3	23.49.056.C.2.b Facade Setback Limits 23.49.056.C.2.c Facade Setback Limits	<p>The maximum area of all setbacks between the street lot line and facade along each street frontage of a lot shall not exceed the area derived by multiplying the averaging factor by the width of street frontage of the structure along that street.</p> <p>The maximum width, measured along the street lot line, of any area exceeding a depth of 15 feet from the street lot line shall not exceed 80 feet or 30% of the lot frontage on that street, whichever is less.</p>	<p>Proposed area of setback to be greater than limit allowed per factor calculations.</p> <p>Proposed width of area exceeding 15 feet from the street lot line is greater than 30% of the street frontage.</p> <p>Further design refinement has created small changes in the overall dimensions and total square footage.</p>	The building setback along 9th Ave. creates a larger public outdoor amenity adjacent to active street level uses.This departure allows the project to better meet the intent of the design guidelines described in the relevant design guideline section.

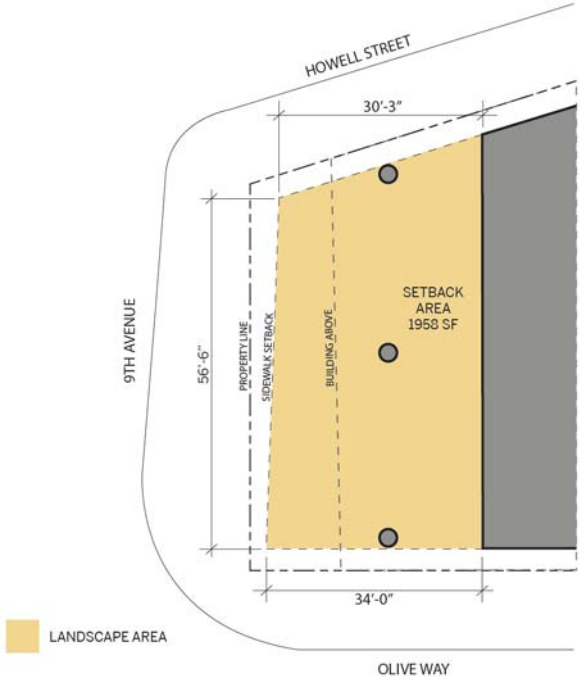
Relevant Design Guidelines

- B3.3 pedestrian amenities at the ground level - setting the building back to create space adjacent to the sidewalk conducive to pedestrian-oriented activities (building setback to create space adjacent to the sidewalk)
- B4.1 massing
- A. Setbacks, projections, and open space (set backs and projections define building volumes, and exterior open spaces)
- D1.1 pedestrian enhancements
- B. Preferable open space locations are to the south and west of tower development, or where the siting of the open spaces would improved solar access to the sidewalk (located on the west facade at the sidewalk)
- C. Orient to receive the maximum direct sunlight possible, while providing shade in the warmer months. (setback and overhang balance the solar needs)

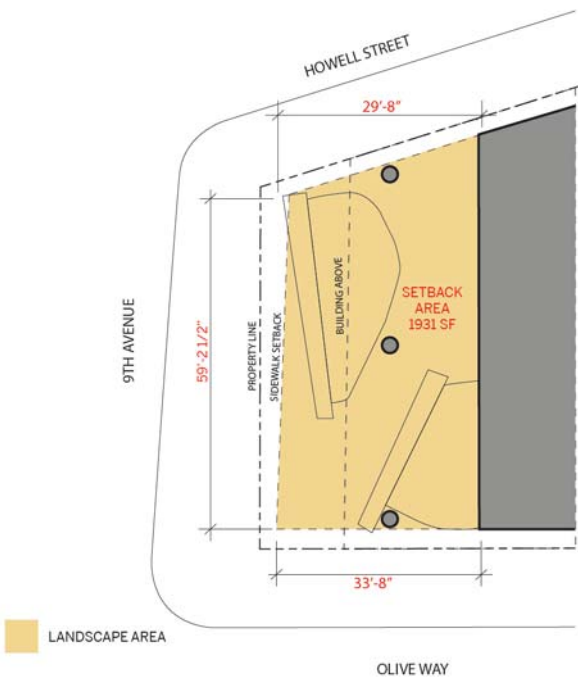
CODE BASELINE



DRB 1



DRB 2



Change in scope shown in RED

5 PROPOSED DEPARTURES

DEPARTURES

SITE B - RESIDENTIAL

DEPARTURE #4 (STREET LEVEL USE) - THIS DEPARTURE WAS PREVIOUSLY APPROVED BY THE DESIGN REVIEW BOARD ON MAY 15, 2016

CHANGE IN SCOPE

Item #	Development Standard	Requirement	Modification Requested	Rationale
4	23.49.009 Street Level Use	A minimum of 75% of each street frontage at the street level where street level uses are required, must be occupied by uses listed in subsection 23.49.009.a.	Relief from the 75% street level use required along Terry Avenue. 58% street level use is requested. Relocating the garage door to Howell Street allowed the project to increase its street level frontage to 58%, a higher percentage than what was approved at DRB #1.	The building loses some street level frontage by creating a setback for a public benefit open space at the corner of Terry Ave. and Olive Way. The generator is a necessary building use and is reduced to a minimum dimension. Locating the intake in this location allows the project to provide more street level use along the Olive Way and Howell Street frontages. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guideline section.

Relevant Design Guidelines

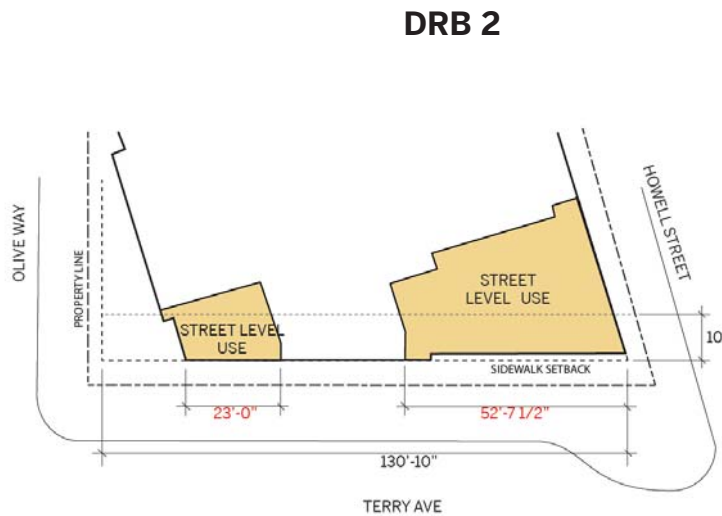
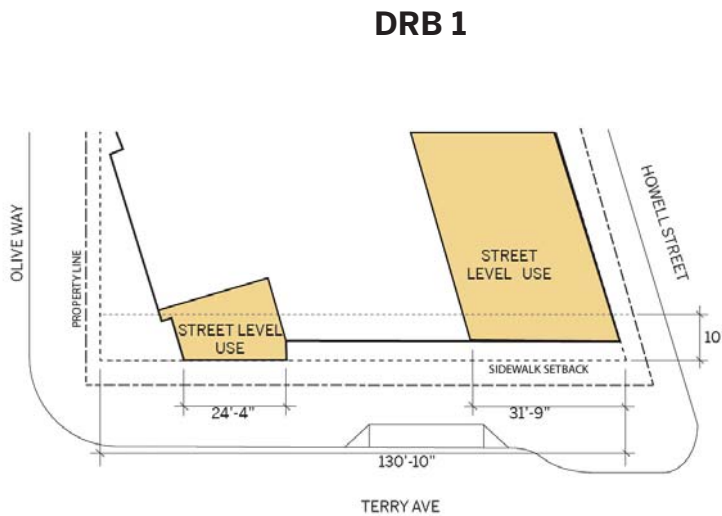
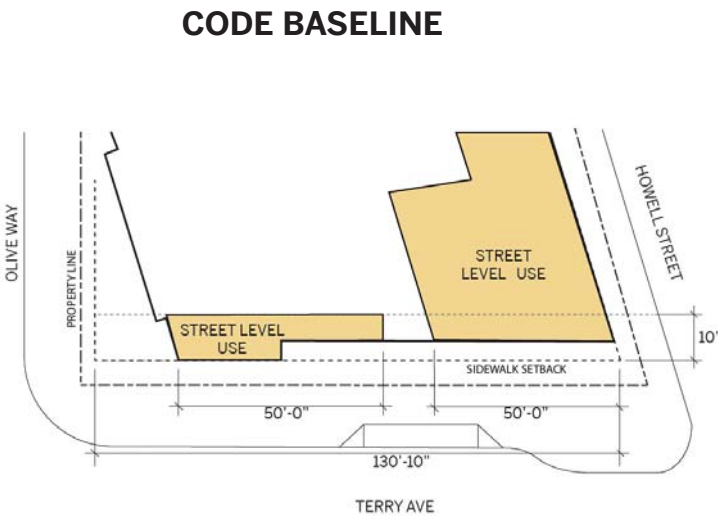
- C1.1 street level uses
- A. Reinforce existing retail concentrations;
 - B. Vary in size, width, and depth
 - C. Enhance main pedestrian links between areas; and
 - D. Establish new pedestrian activity

The street level uses for the building have been distributed across all four frontages, with a focus on corners to provide variety, enhance links to adjacent areas, and provide new activity at primary circulation areas.

- C1.3 street level articulation for pedestrian activity - setting the building back to create space adjacent to the sidewalk conducive to pedestrian-oriented activities
- G. Windows that encourage pedestrians to look into building interior
 - I. Street front open space

Articulation and activity are promoted with additional open space at grade along with the views into the building interior of street level uses as well as other uses.

- E3.1 integrating service areas
- A. Plan service areas for less visible locations
 - B. Screen service areas to be less visible



Change in scope shown in RED

5 PROPOSED DEPARTURES

DEPARTURES

SITE B - RESIDENTIAL

DEPARTURE #5 (BLANK FACADE LIMIT) - NEW DEPARTURE

Item #	Development Standard	Requirement	Modification Requested	Rationale
5	23.49.056.D.2.a Blank Facade Requirements	Blank facades shall be no more than 15 feet wide except segments with garage doors may exceed a width of 15 feet and may be as wide as the driveway plus 5 feet.	Relief from the 15 feet wide blank facade. The project is proposing a 32' wide blank facade.	The generator intake is a necessary building service and has been reduced to a minimum dimension. Concentrating the intake in this area allows for a more activated street frontage along Olive Way and Howell Street. To enhance the pedestrian experience the louvers will be fronted with a custom metal screen design by a local artist. This departure allows the project to meet the intent of the design guidelines described in the relevant design guideline section.

Relevant Design Guidelines

- C1.1 street level uses
- A. Reinforce existing retail concentrations;
 - B. Vary in size, width, and depth
 - C. Enhance main pedestrian links between areas; and
 - D. Establish new pedestrian activity

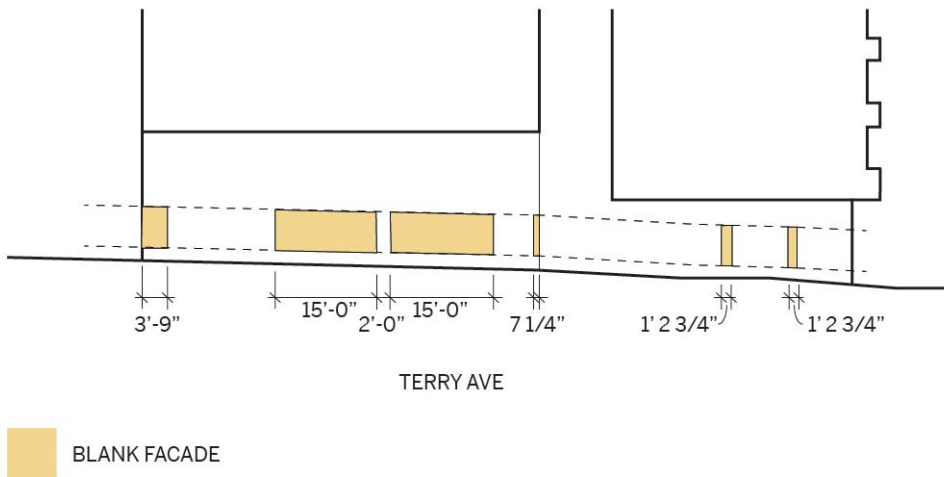
Transparency and correlating street level uses for the building have been distributed accross all four frontages, with a focus on corners to provide variety, enhance links to adjacent areas and provided new activity at primary circulation areas.

- C2.1 Modulation of facades
- A. Fenestration pattern
 - B. Exterior finish materials
 - C. Other architectural elements

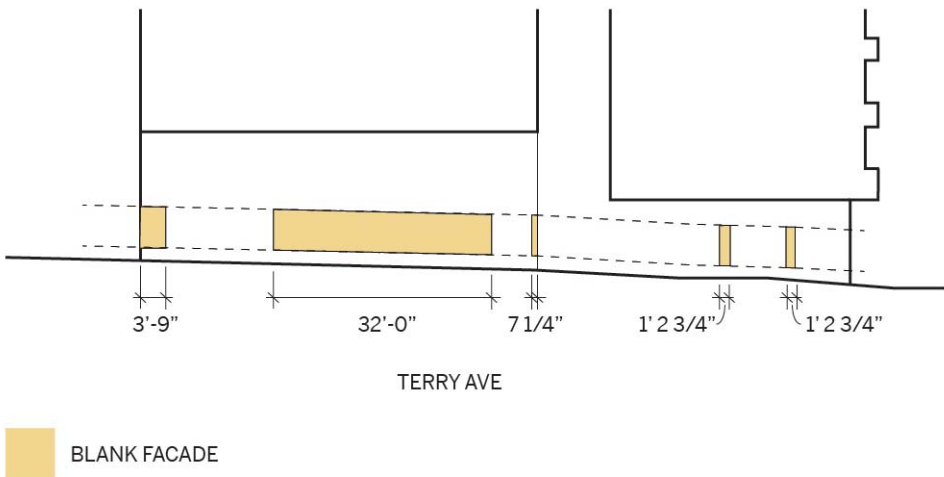
- E3.1 integrating service areas
- A. Plan service areas for less visible locations
 - B. Screen service areas to be less visible

The modulation of teh facade creates a hierarchy at the corners with more active and transparent pedestrian oriented spaces in high visibility locations. Service area is screened consistent with the facade articulation to be less dominate and compliment the architecture.

CODE BASELINE



DRB 2



5 PROPOSED DEPARTURES

DEPARTURES

SITE B - RESIDENTIAL

DEPARTURE #6 (FACADE SETBACK LIMIT) - THIS DEPARTURE WAS PREVIOUSLY APPROVED BY THE DESIGN REVIEW BOARD ON MAY 15, 2016

CHANGE IN SCOPE

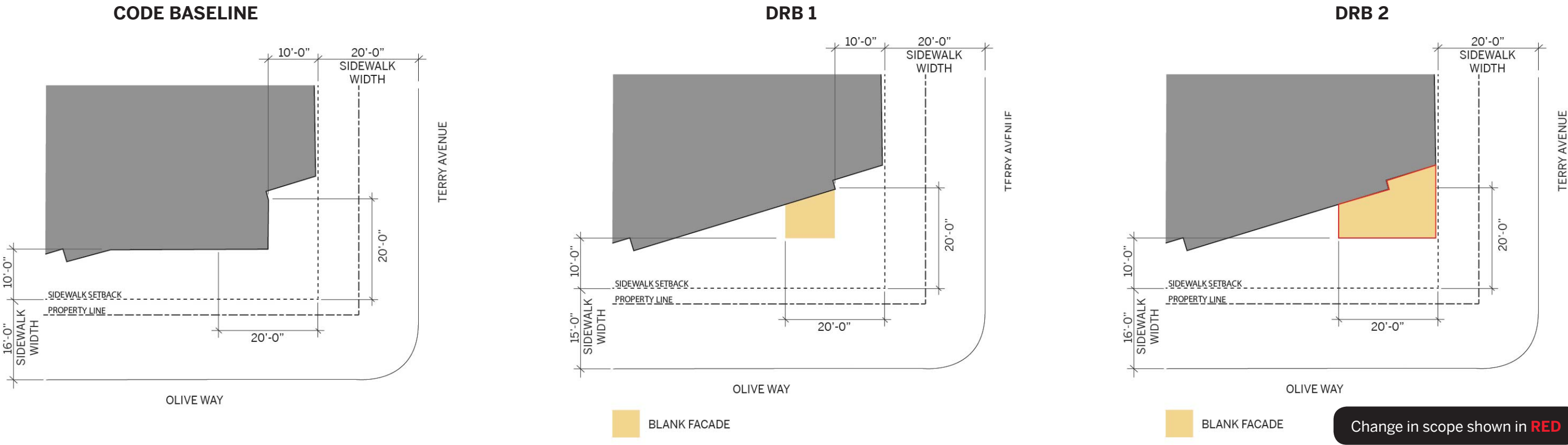
Item #	Development Standard	Requirement	Modification Requested	Rationale
6	23.49.056.B.2 Facade Setback Limit	The maximum setback of the facade at an intersection is 10 feet. The minimum distance the facade must conform is 20 feet from the corner along each street.	Relief from the 10 foot maximum setback limit at the intersection of Olive Way and Terry Ave. The building edge remains unchanged from DRB #1 but the departure extent has been modified based on further MUP review.	The building is set back at the corner of Olive Way and Terry Ave. to provide a wider sidewalk and planted area along the green street with better solar access. The corner creates a transition to the north entry of the proposed convention center beyond and provides a generous terminus to the green street. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guideline section.

Relevant Design Guidelines

A1.1 response to context
A. A change in street grid alignment that yields a site having a nonstandard shape (facade orientation responds to shift in street grid)
D. Access to direct sunlight-seasonally or at particular times of day (facade is set back to allow for solar access)

A1.2 response to planning efforts
(Denny triangle neighborhood, transitional environment, design for future context - informality, use of green streets and pocket planting, strengthen pedestrian connections)

D1.1 pedestrian enhancements
A. Open space elements should enhancement a pedestrian oriented, urban environment that has the appearance of stability, quality, and safety (clearly defined edges at active intersection)



5 PROPOSED DEPARTURES

DEPARTURES

SITE B - RESIDENTIAL

DEPARTURE #7 (UPPER LEVEL GREEN STREET SETBACK) - THIS DEPARTURE WAS PREVIOUSLY APPROVED BY THE DESIGN REVIEW BOARD ON MAY 15, 2016

CHANGE IN SCOPE

Item #	Development Standard	Requirement	Modification Requested	Rationale
7	23.49.056.F.4 Upper Level Green Street Setback	On a designated green street, a continuous upper level setback of 15 feet is required for portions of the structure above a height of 45 feet.	Relief from the setback of 15 feet for portions of the structure above a height of 45 feet. Further design refinement has setback all the podium facades further than what was approved at DRB #1 .	The entire tower is setback beyond what is required by code to open up the green street and provide greater solar access. A portion of the podium is within the required setback in order to clarify the massing and intersection of building forms along the shift in the street grid at Howell street. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guideline section.

Relevant Design Guidelines

A1.1 response to context

A. A change in street grid alignment that yields a site having a nonstandard shape

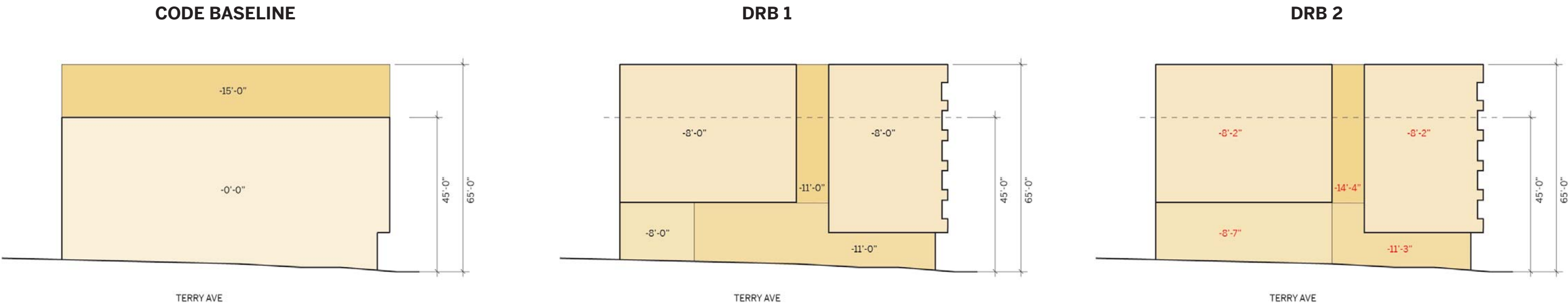
C. Patterns in urban form, such as nearby buildings that employ distinctive and effective massing compositions

D. Access to direct sunlight-seasonally or at particular times of day

B4.1 massing

A. Setbacks, projections, and open space

B. Relative sizes and shapes of distinct building volumes



Change in scope shown in RED

5 PROPOSED DEPARTURES

DEPARTURES

SITE B - RESIDENTIAL

DEPARTURE #8 (OVERHEAD WEATHER PROTECTION) - NEW DEPARTURE

Item #	Development Standard	Requirement	Modification Requested	Rationale
8	23.49.018.D Overhead Weather Protection	The lower edge of the overhead weather protection must be a minmum of ten (10) feet and a maximum of fifteen (15) feet above the sidewalk	The lower edge of the overhead weather protection at the building main entrance on Olive Way is seventeen (17) feet above the sidewalk	The design review board recommended the building entrance be raised to have a two story expression (recommended condition #5), in response the entrance canopy has been raised above the maxium allowed 15 feet to accentuate the two story lobby expression and provide a distinct architectural feature at the building entrance. This departure allows the project to better meet the intent of the design guidelines deccribed in the relevant design guideline section.

Relevant Design Guidelines

B3.3 Pedestrian amenities at the ground level - setting the building back to create space adjacent to the sidewalk conducive to pedestrian - oriented activities (building setback to create a space adjacent to the sidewalk)

B4.1 Massing

A. Setbacks, projections and open space (setbacks and projections define building volumes and exterior open spaces)

C1.1 Street level uses

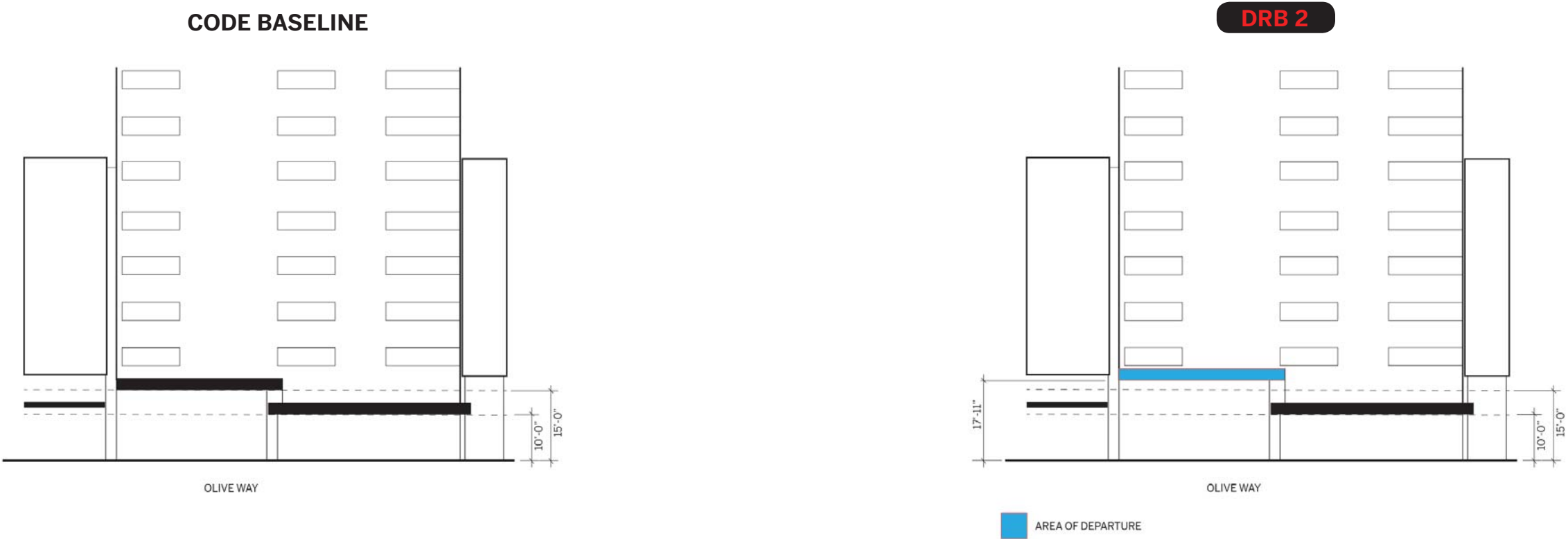
A. Reinforce existing retail concentrations

B. Vary in size, width and depth

C. Enhance main pesdestian links between areas; and

D. Establish new pedestrian activity

C1.3 Street level articulation for pedestrian activity - setting the building back to create space adjacent to the sidewalk conducive to pedestrian-oriented activities



5 PROPOSED DEPARTURES

DEPARTURES

SITE B - RESIDENTIAL

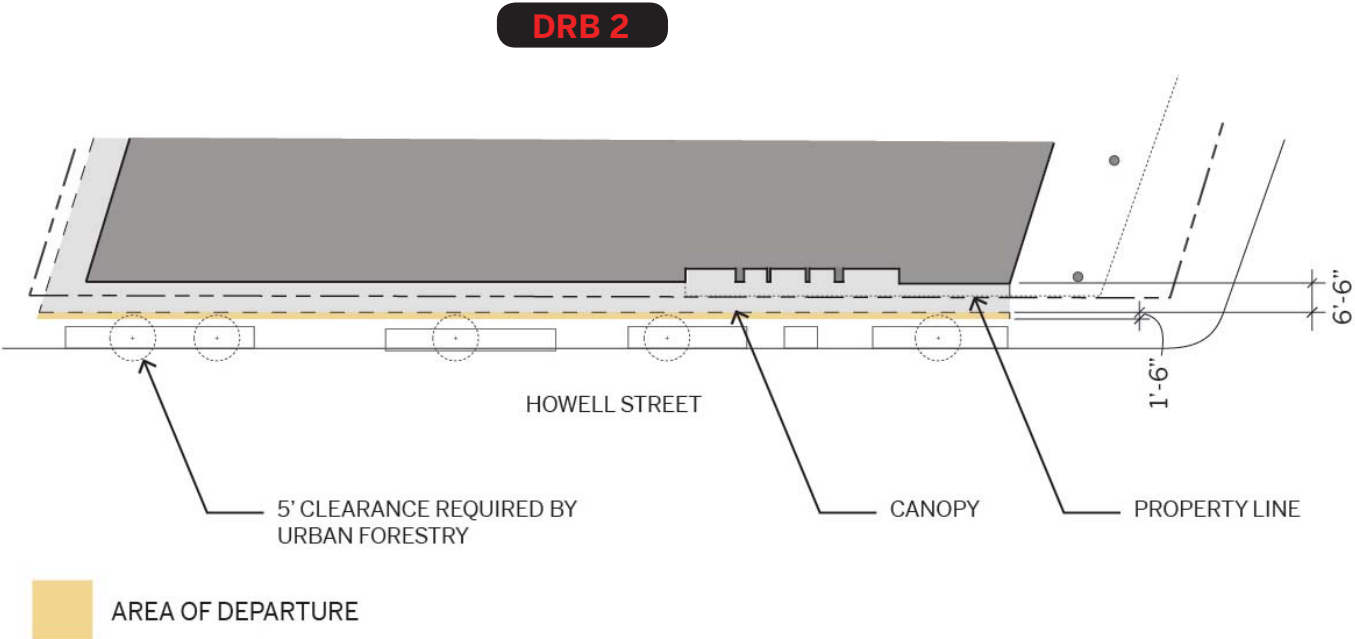
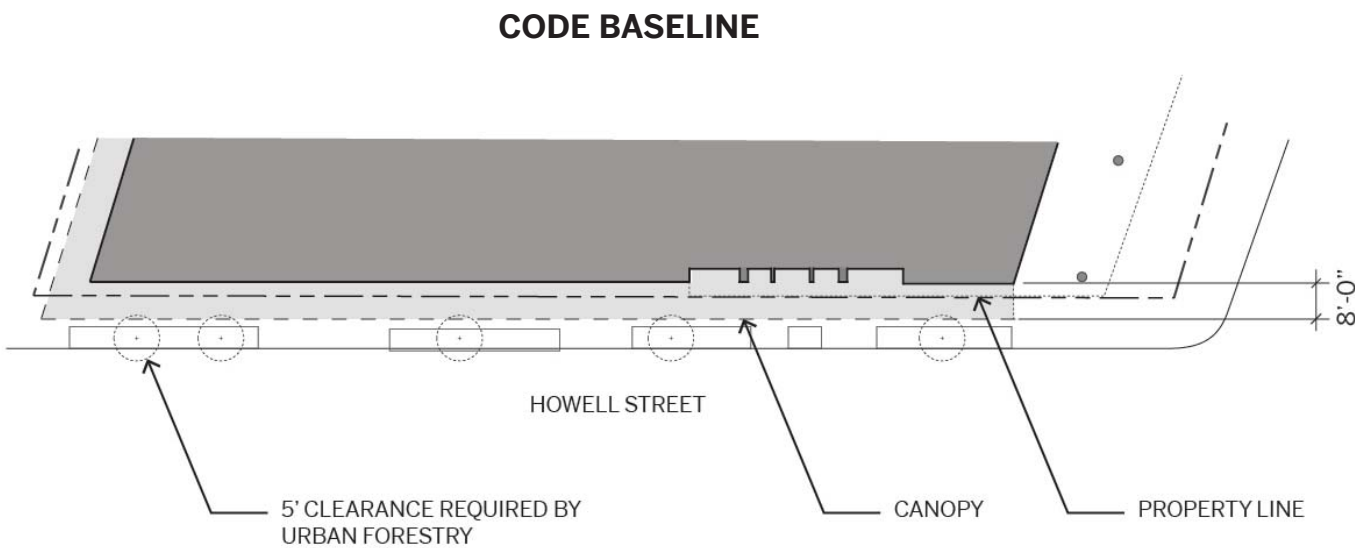
DEPARTURE #9 (OVERHEAD WEATHER PROTECTION) - NEW DEPARTURE

Item #	Development Standard	Requirement	Modification Requested	Rationale
9	23.49.018.D Overhead Weather Protection	Overhead weather protection shall have a minimum dimensions of eight feet measured horizontally from the building wall.	The project is requesting the overhead weather protection to be six feet, six inches from the building wall along Howell St.	This departure is a response to urban forestry's requirement that all overhead weather protection be a minimum of five feet from the center of the tree, reducing the canopy width and allowing the appropriate space for the trees to grow will help ensure the health and growth of the trees and allow the project to better meet the intent of the design guidelines described in the relevant design guideline section.

Relevant Design Guidelines

D2 Enhance the building with landscaping:

F. Increase privacy and security through screening and/or shading.
I. Provide identity and reinforce a desired feeling of intimacy and quiet.
K. Consider how the building will be viewed from the upper floors of nearby buildings as well as from the sidewalk.



5 PROPOSED DEPARTURES

DEPARTURES

SITE B - RESIDENTIAL

DEPARTURE #10 (OVERHEAD WEATHER PROTECTION) - NEW DEPARTURE

Item #	Development Standard	Requirement	Modification Requested	Rationale
10	23.49.018.D Overhead Weather Protection	Overhead weather protection shall have a minimum dimension of eight feet measured horizontally from the building wall.	Theprojectisrequestingtheoverheadweather protection to be seven feet, six inches from the building wall along Olive Way.	This departure is a response to urban forestry's requirement that all overhead weather protection be a minimum of five feet from the center of the tree. Reducing the canopy width and allowing the appropriate space for the trees to grow will help ensure the health and growth of the trees and allow the project to better meet the intent of the design guidelines described in the relevant design guideline section.

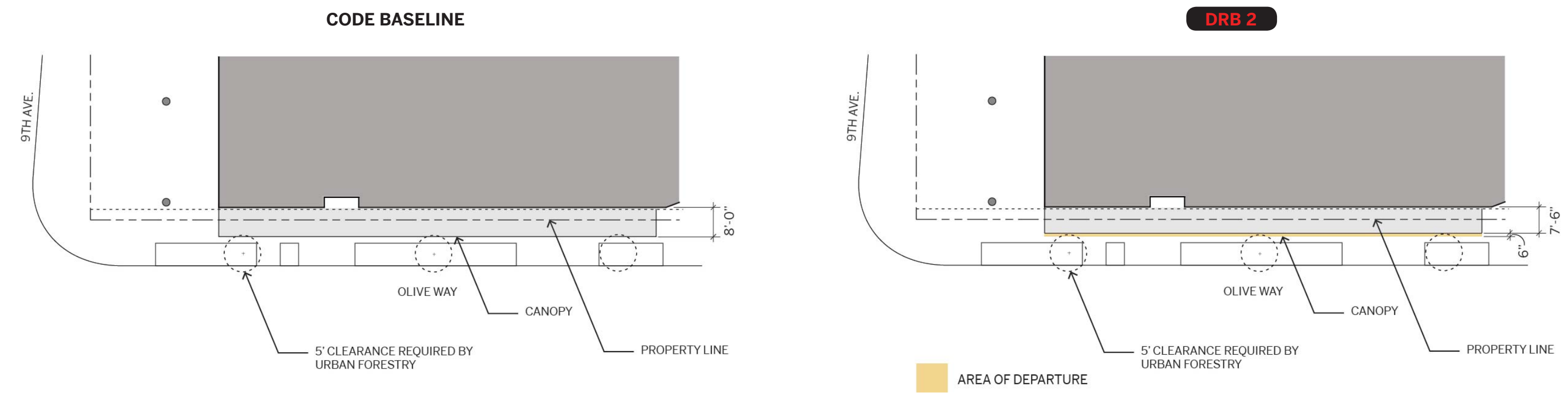
Relevant Design Guidelines

D2 Enhance the building with landscaping:

F. Increase privacy and security through screening and/or shading.

I. Provide identity and reinforce a desired feeling of intimacy and quiet.

K. Consider how the building will be viewed from the upper floors of nearby buildings as well as from the sidewalk.



5 PROPOSED DEPARTURES

DEPARTURES

SITE B - RESIDENTIAL

DEPARTURE #11 (MINIMUM SIDEWALK WIDTH) - NEW DEPARTURE

Item #	Development Standard	Requirement	Modification Requested	Rationale
11	23.49.022 Minimum Sidewalk Width	Map 1C sidewalk widths. Howell Street sidewalk width is 18' (when on a one-way street, only the side with transit stops shall be 18'; the other shall be 15')	Relief from the 18' sidewalk width requirement along Howell Street. The project is requesting a 15' sidewalk width with a designated area (50' - 0") where an 18' sidewalk width is provided and a potential transit stop can be located.	The project is providing an 18ft sidewalk width in a designated area for a potential bus stop location. Many sidewalks with a width of 15 feet or less support bus stops. The combination of the proposed sidewalk width, landscape and canopy height promotes pedestrian comfort and scale described in the C2 design guideline. The designated widened area allows people to congregate and wait at the building edge, out of the walkway path and protected from the elements by the building canopy above. Recessing the waiting area into the building also eliminates the need for a stand alone bus shelter (6' - 0" X 9' - 0") that can impede pedestrian traffic and create visual clutter along the street frontage. There is no bus stop planned for this site, but an area is provided in the event that a future bus stop is needed. This departure allows the project to better meet the intent of the design guideline listed in the relevant design guideline section

Relevant Design Guidelines

C1.3 Street-level articulation for pedestrain activity - consider setting portions of the building back slightly to create spaces conducive to pedestrian - oriented activities such as vending, resting, sitting or dining. Further articulate the street level facade to provide engaging pedestrian experience via;

f. Windows that encourage pedestrians to look into the building interior

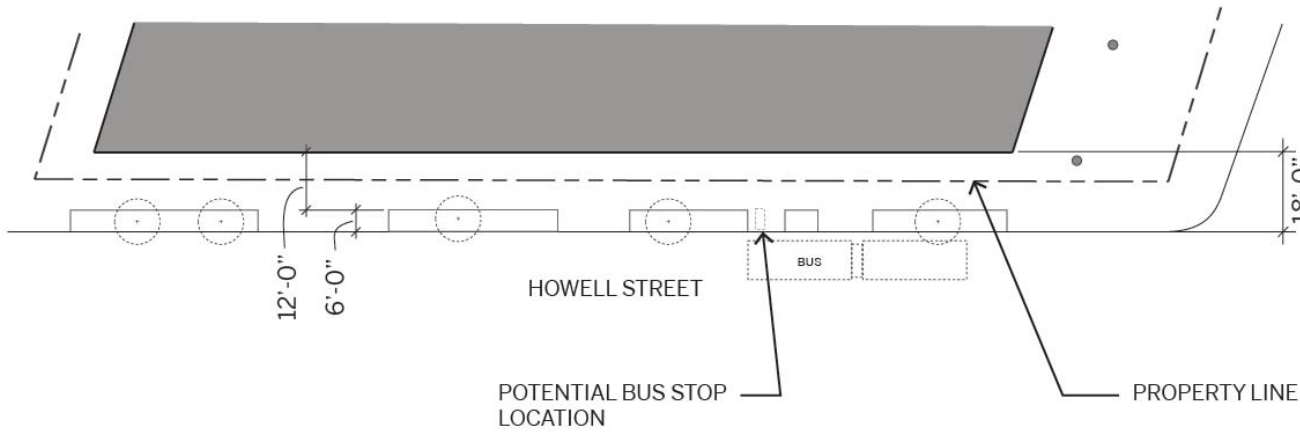
i. Street front open space that features art work, street furniture, and landscaping

C2 Design facade of many scales - design architectural features fenestration patterns and material compositions that refer to the scale of human activities contained within. Building facades should be composed of elements scaled to promote pedestrian comfort, safety and orientation.

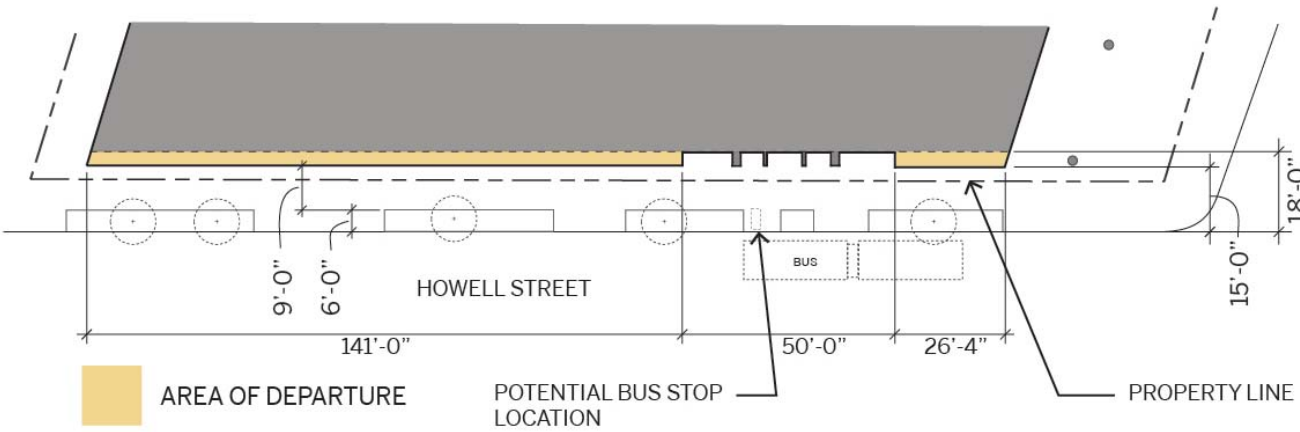
D1.1 Pedestrain enhancements - where a commercial or mixed use building is set back from the sidewalk pedestrian enhancements should be considered in the resulting street frontage. Downtown the primary function of any open space between commercial buildings and the sidewalk is to provide access into the building and opportunities for outdoor activities such a vending, resting, sitting or dining.

The recessed portion of the facade provides the opportunity to install a rail for predestians to lean against and rest as well as provide opportunity for pedestrians to look into the building amenity and retail spaces.

CODE BASELINE



DRB 2



The sidewalk width is unchanged from the previously approved DRB #1 design. It was clarified that this was a deviation and needed a departure. The 3' recess has been added to the design since DRB #1 in response to a potential future bus stop.

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5 PROPOSED DEPARTURES

DEPARTURES

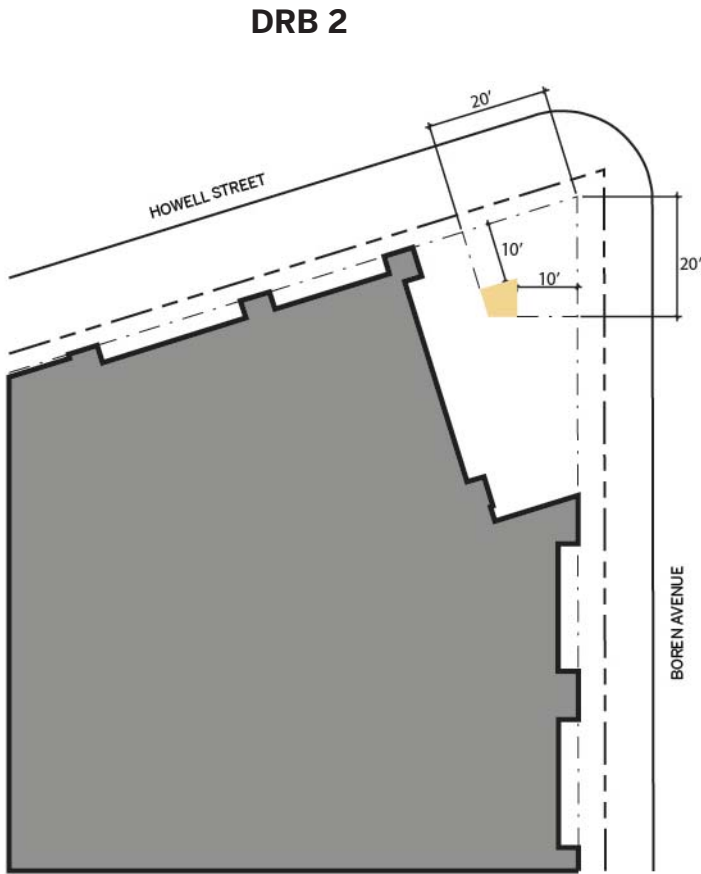
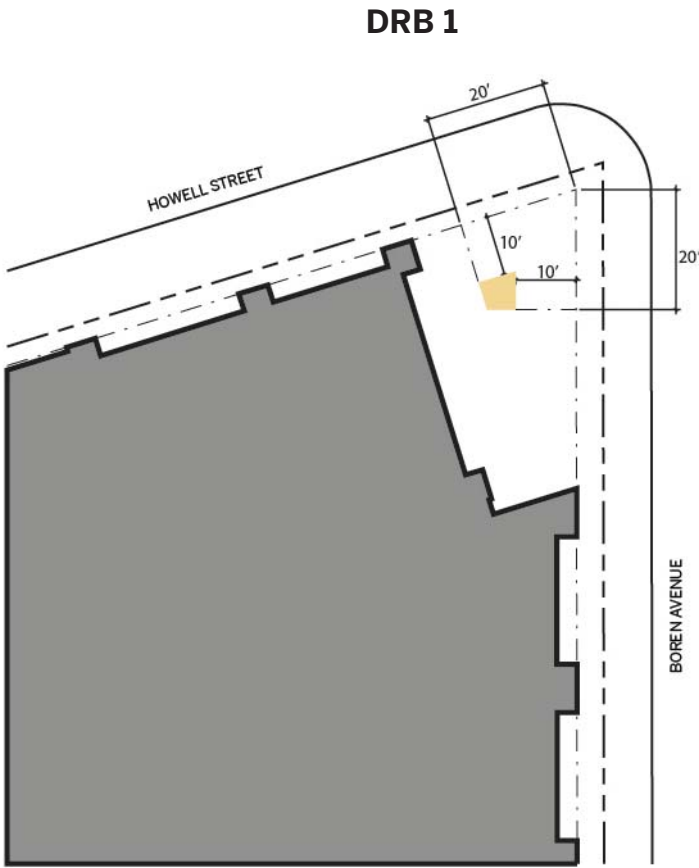
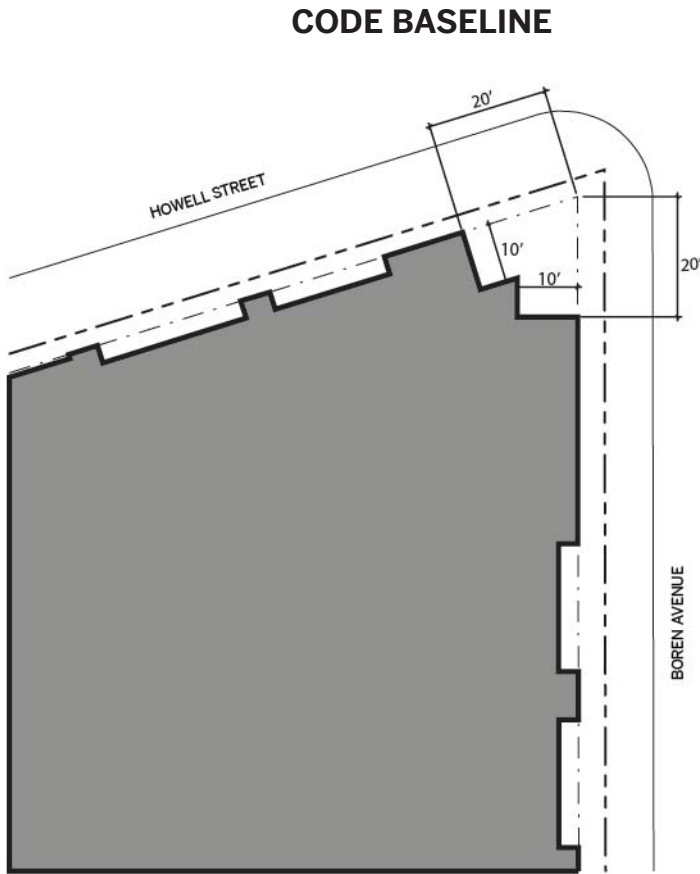
SITE C - OFFICE

DEPARTURE #1 (FACADE SETBACK LIMIT) - THIS DEPARTURE WAS PREVIOUSLY APPROVED BY THE DESIGN REVIEW BOARD ON MAY 15, 2016

Item #	Development Standard	Requirement	Modification Requested	Rationale
1	23.49.056.B.2D Facade Setback Limit	The maximum setback of the facade at an intersection is 10 feet. The minimum distance the facade must conform is 20 feet from the corner along each street.	Relief from the maximum setback of the facade from the street lot line at intersections.	The proposed design responds to the shift in the street grid at Howell Street, reflected in the massing of the tower, the form of the tower is carried through to the ground, adding clarity to the overall design and providing for a more generous sidewalk at an otherwise sharply angled intersection. The additional pedestrian space at grade eases the transition through the intersection and allows for better visibility across the changing street grid. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guideline section.

Relevant Design Guidelines

- A1.1 response to context
- A. A change in street grid alignment that yields a site having a nonstandard shape
- D. Access to direct sunlight-seasonally or at particular times of day
- B4.1 massing
- A. Setbacks, projections, and open space
- B. Relative sizes and shapes of distinct building volumes



5 PROPOSED DEPARTURES

DEPARTURES

SITE C - OFFICE

DEPARTURE #2 (FACADE SETBACK LIMIT) - THIS DEPARTURE WAS PREVIOUSLY APPROVED BY THE DESIGN REVIEW BOARD ON MAY 15, 2016

Item #	Development Standard	Requirement	Modification Requested	Rationale
2	23.49.056.B.2D Facade Setback Limit	The maximum setback of the facade at an intersection is 10 feet. The minimum distance the facade must conform is 20 feet from the corner along each street.	Relief from the maximum setback of the facade from the street lot line at intersections.	The proposed design responds to the shift in the street grid at Howell Street, reflected in the massing of the tower, the form of the tower is carried through to the ground, adding clarity to the overall design and providing for a more generous sidewalk. The additional pedestrian space at grade eases the transition through the intersection and allows for better visibility across the changing street grid. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guideline section.

Relevant Design Guidelines

A1.1 response to context

A. A change in street grid alignment that yields a site having a nonstandard shape

D. Access to direct sunlight-seasonally or at particular times of day

B4.1 massing

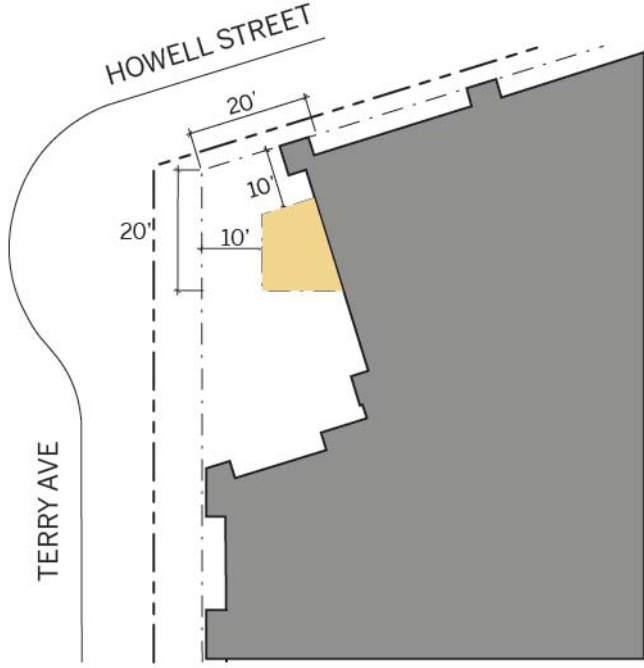
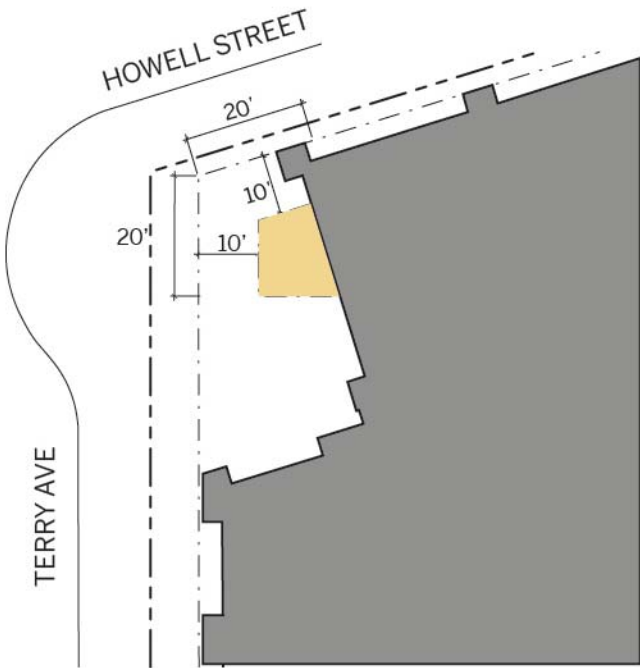
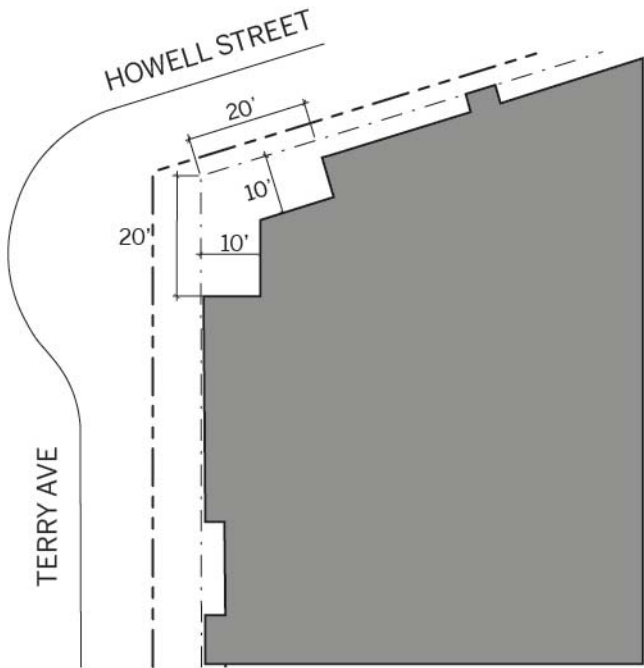
A. Setbacks, projections, and open space

B. Relative sizes and shapes of distinct building volumes

CODE BASELINE

DRB 1

DRB 2



5 PROPOSED DEPARTURES

DEPARTURES

SITE C - OFFICE

DEPARTURE #3 (STREET LEVEL USE) - THIS DEPARTURE WAS PREVIOUSLY APPROVED BY THE DESIGN REVIEW BOARD ON MAY 15, 2016

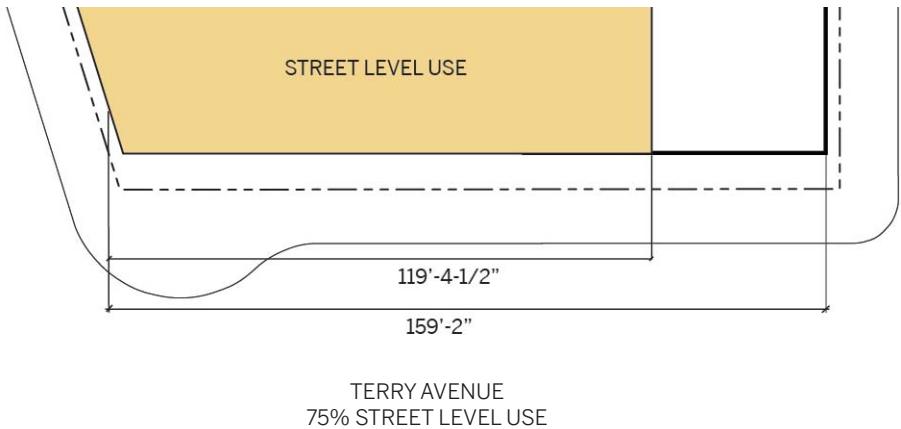
CHANGE IN SCOPE

Item #	Development Standard	Requirement	Modification Requested	Rationale
3	23.49.009 Street Level Use	A minimum 75% of each street frontage at street level where street level uses are required must be occupied by uses listed in subsection 23.49.009.a required street level use shall be located within 10 feet of street line.	Relief from the 75% street level use requirement along Terry Ave., and from street level use within 10 feet of street lot line. Further design refinement and clarification from MUP review has reduced the amount of street level use approved at DRB #1.	The building is providing loading egress along Terry Ave. (being reviewed under type 1 application process) with the smallest building opening possible. The main pedestrian entrance is located at the corner of Terry Ave. and Olive Way with the desire to bring part of the lobby frontage along Terry to activate the facade at street level. The remaining frontages along Howell Street, Olive Way and Boren Ave. have been maximized for street level uses, where none are required by code. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guidelines section.

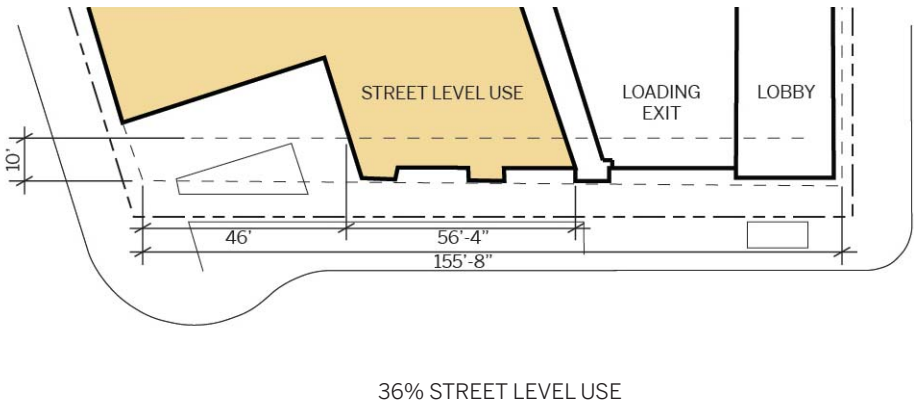
Relevant Design Guidelines

- C1.1 street level uses
- A. Reinforce existing retail concentrations;
 - B. Vary in size, width, and depth
 - C. Enhance main pedestrian links between areas; and
 - D. Establish new pedestrian activity
- C1.2 Retail orientation
- Concentrating the street level uses along Terry at the corner of Howell, allows a potential retail tenant to “spill-out” onto the sidewalk where there is greater depth and more visibility.
- C1.3 Street level articulation for pedestrian activity - setting the building back to create space adjacent to the sidewalk conducive to pedestrian-oriented activities
- G. Windows that encourage pedestrians to look into building interior
 - I. Street front open space

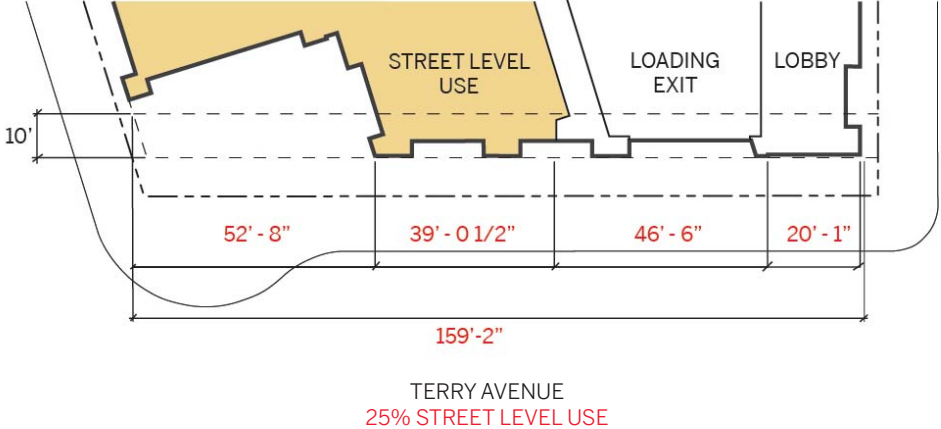
CODE BASELINE



DRB 1



DRB 2



Change in scope shown in RED

5 PROPOSED DEPARTURES

DEPARTURES

SITE C - OFFICE

DEPARTURE #4 (BLANK FACADE LIMIT) - NEW DEPARTURE

Item #	Development Standard	Requirement	Modification Requested	Rationale
4	SMC 23.49.056.D.2.c Blank Facade Limits	The total width of all blank facade segments, including garage doors, shall not exceed 40 percent of the street facing facade of the structure on each street frontage	Relief from the 40 percent blank facade width on Terry Avenue. 43 percent blank frontage is being proposed.	A wider than typical garage door is required to allow for the exiting of large semi-trucks from the Washington State Convention Center loading dock below the site. The door width has been reduced to the minimum required for the trucks to safely exit the building. The garage door will have a custom designed perforation that will add visual interest to the facade and some transparency to the door, providing a larger garage door in this location allows this project and the Washington State Convention Center to consolidate garage doors for loading, reducing the overall number and length of garage doors for both sites. This allows the project to provide more active frontages. This departure allows the project to better meet the intent of the design guidelines listed in the relevant design guidelines section.

Relevant Design Guidelines

C1.1 Street Level Uses

A. Reinforce existing retail concentrations;

B. Vary in size, width and depth

C. Enhance main pedestrian links between areas; and

D. Establish new pedestrian activity

Consolidate service areas, allow for more street level use and pedestrain activity over both site C and A (Washinton State Convention Center)

E3.1 Integrating service areas

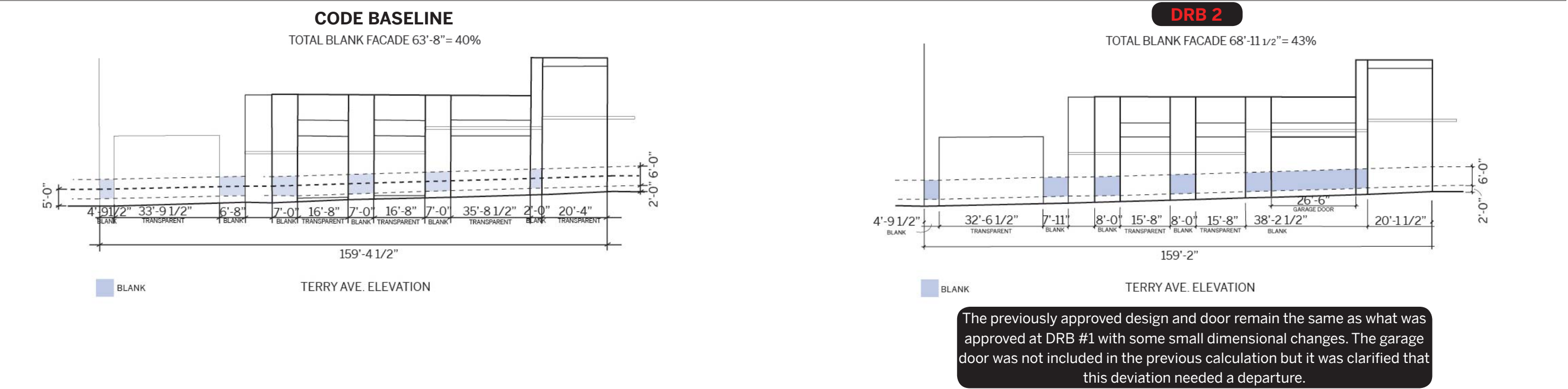
A. Plan service areas for less visible locations

B. Screen service areas to be less visible

Consolidate service areas reduce the overall visibility of service doors over both site C and site A (Washinton State Convention Center)

C3.1 Desirable facade elements - facades which for unavoidable programmatic reasons may have few entries or windows should receive special design treatment to increase pedestrian safety, comfort and interest. Enliven these facades by providing:

b. Visibility into building interiors.



5 PROPOSED DEPARTURES

DEPARTURES

SITE C - OFFICE

DEPARTURE #5 (FACADE TRANSPARENCY) - NEW DEPARTURE

Item #	Development Standard	Requirement	Modification Requested	Rationale
5	23.49.056.C Facade Transparency	A minimum of 60 percent of the street level facing facade shall be transparent	Relief from the 60 percent transparency requirement. 58% transparency is proposed.	A wider than typical garage door is required to allow for the exiting of large semi-trucks from the Washinton State Convention Center loading dock below the site. The door width has been reduced to the minimum required for the trucks to safely exit the building. The garage door will have a custom designed perforation that will add visual interest to the facade and some transparency to the door. Providing a larger garage door in this location allows this project and the Washington State Convention Center to consolidate gargage doors for loading, reducing the overall number and length of garage doors for both sites. This allows the project to provide more active frontages. This deparature allows the project to better meet the intent of the design guidelines listed in the relevant design guidelines section.

Relevant Design Guidelines

C1.1 Street Level Uses

A. Reinforced existing retail concentrations;

B. Vary in size, width and depth

C. Enhance main pedestrain links between areas; and

D. Establish new pedestrain activity

Consolidate service areas allow for more street level use and pedestrain activity over both site C and site A (Washington State Convention Center)

E3.1 Integrating service areas

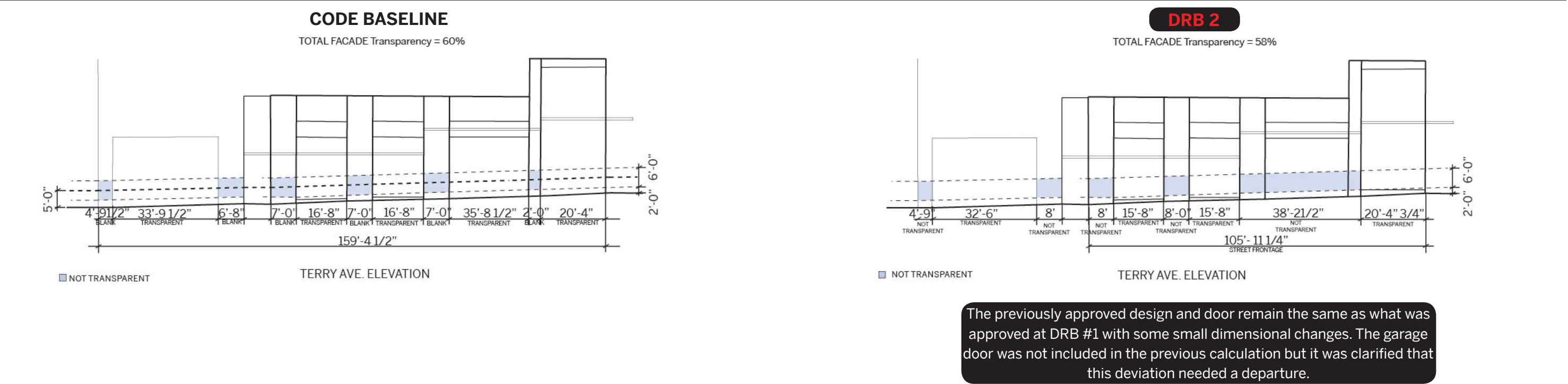
A. Plan sercive areas for less visible locations.

B. Screen service areas to be less visible

Consolidate service areas, reduce the overall visibility of service doors over both site C and Site A (Washington State Convention Center)

C3.1 Desirable facade elements - facades which for unavoidable programmatic reasons may have few entries or windows should receive special design treatment to increase pedestrian safety, comfort, and interest. Enliven these facades by providing:

b. Visibility into building interiors



5 PROPOSED DEPARTURES

DEPARTURES

SITE C - OFFICE

DEPARTURE #6 (OVERHEAD WEATHER PROTECTION) - THIS DEPARTURE WAS PREVIOUSLY APPROVED BY THE DESIGN REVIEW BOARD ON MAY 15, 2016

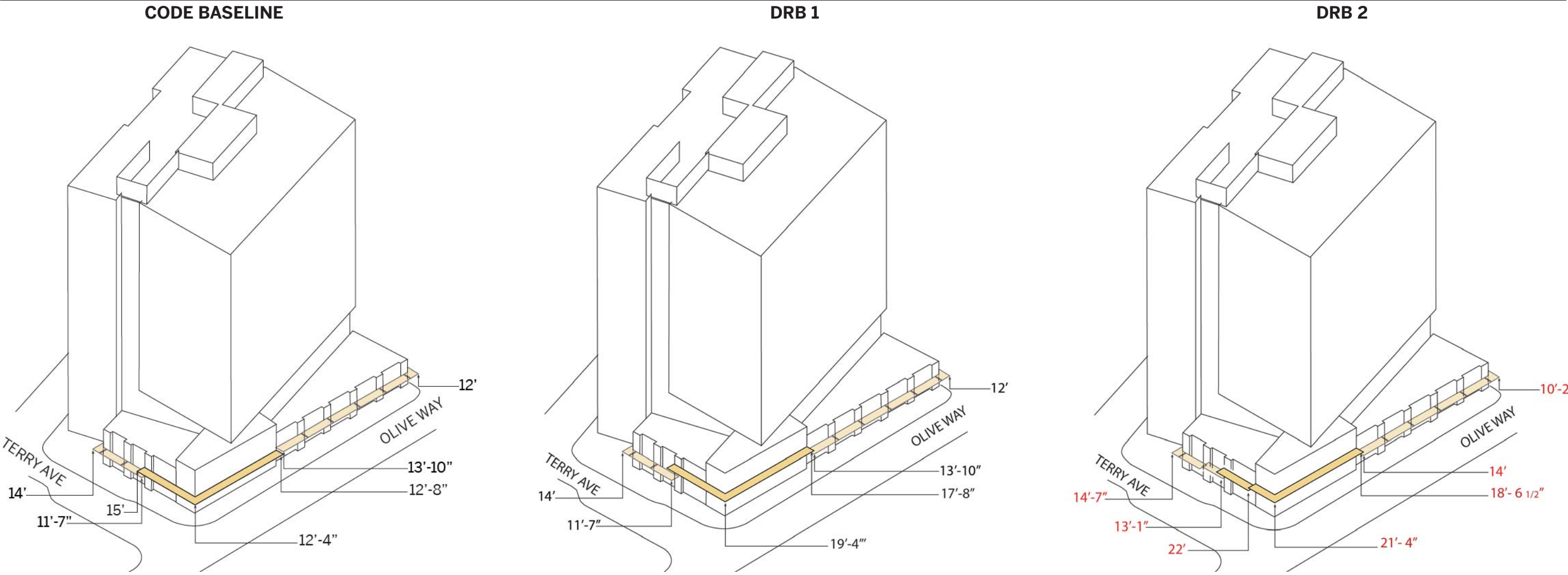
CHANGE IN SCOPE

Item #	Development Standard	Requirement	Modification Requested	Rationale
6	23.49.018 Overhead Weather Protection	The lower edge of the overhead weather protection must be a minimum of 10 feet and a maximum of 15 feet above the sidewalk.	Relief from the 15 feet maximum height limit above the sidewalk. Further design refinement and change in sidewalk elevation has changed the height of the entrance canopy from what was approved at DRB #1. The canopy has been adjusted to better align with adjacent architectural elements.	A taller canopy has been provided to signal the pedestrian entrance to the building and over the loading door for truck exiting. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guidelines section.

Relevant Design Guidelines

A1.1 response to context
B. A site having a dramatic topography or contrasting edge conditions

The canopies are sited to reinforce the massing and modulation of the building facade, stepping along the grade in larger increments



Change in scope shown in RED

5 PROPOSED DEPARTURES

DEPARTURES

SITE C - OFFICE

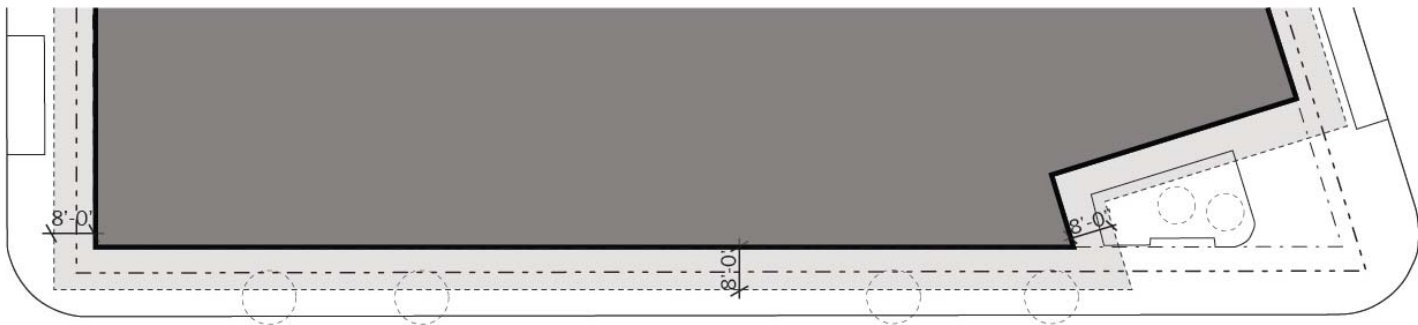
DEPARTURE #7 (OVERHEAD WEATHER PROTECTION) - NEW DEPARTURE

Item #	Development Standard	Requirement	Modification Requested	Rationale
7	23.49.018.B Overhead Weather Protection	Overhead weather protection shall have a minimum dimension of eight (8) feet measured horizontally from the building wall or must extend to a line two (2) feet from the curb line, whichever is less.	Relief from the 8ft minimum dimension from the building wall. A canopy dimension of 4' 0" is being requested along Boren Ave. at the stone piers.	This departure is a response to urban forestry's requirement that all overhead weather protection be a minimum of five feet from the center of the tree, reducing the canopy width and allowing the appropriate space for the trees to grow will help ensure the health and growth of the trees. Changing the width of the canopies helps create a strong architectural rhythm at a pedestrian scale. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guideline section.

Relevant Design Guidelines

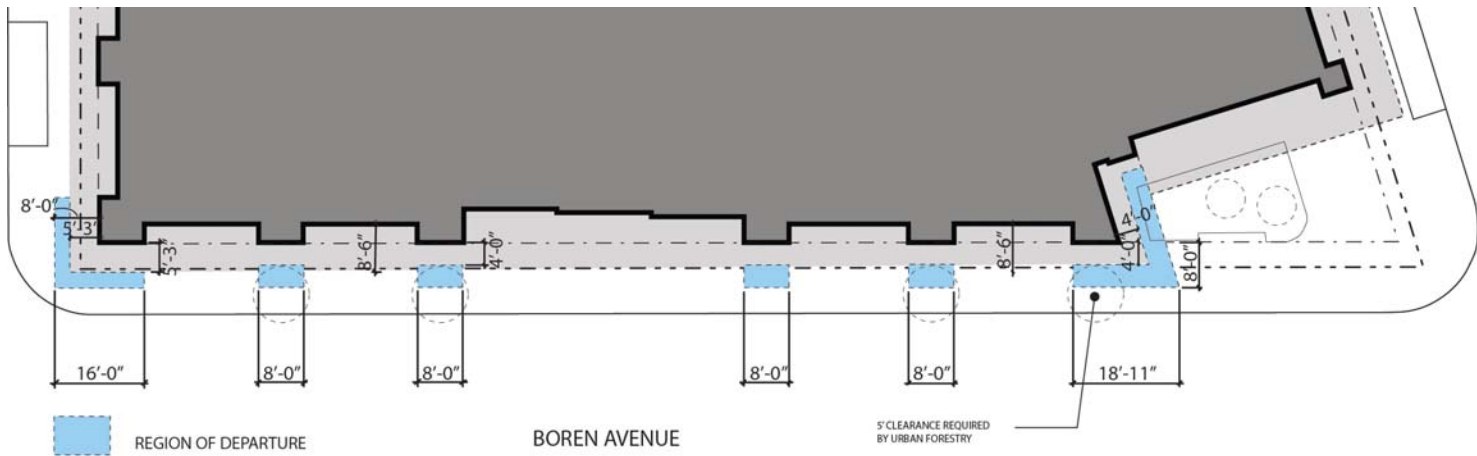
- C.5 Encourage overhead weather protection. Overhead weather protection should be designed with consideration given to:
- b. Uses occurring within the building or in the adjacent streetscapes environment.
 - g. The scape of the space defined by the height and depth of the weather protection.
- D.2 Enhance the building with landscaping
- i. Provide identity and reinforce a desired feeling of intimacy and quiet

CODE BASELINE



BOREN AVENUE

DRB 2



BOREN AVENUE

5 PROPOSED DEPARTURES

DEPARTURES

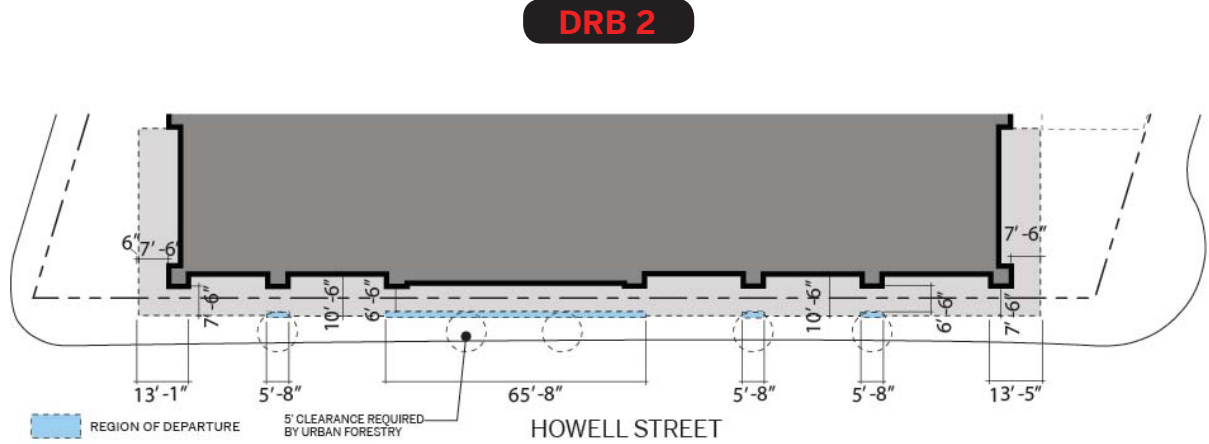
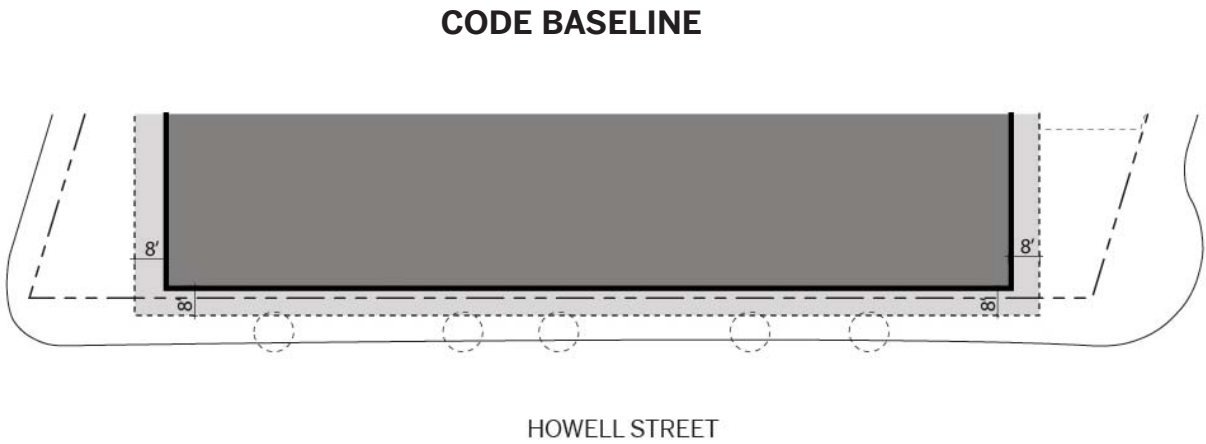
SITE C - OFFICE

DEPARTURE #8 (OVERHEAD WEATHER PROTECTION) - NEW DEPARTURE

Item #	Development Standard	Requirement	Modification Requested	Rationale
8	23.49.018.B Overhead Weather Protection	Overhead weather protection shall have a minimum dimension of eight (8) feet measured horizontally from the building wall or must extend to a line two (2) feet from the curb line, whichever is less.	Relief from the 8ft minimum dimension from the building wall. A canopy dimension of 6'-6" is being requested along Howell St. at the stone piers and middle bays.	This departure is a response to urban forestry's requirement that all overhead weather protection be a minimum of five feet from the center of the tree, reducing the canopy width and allowing the appropriate space for the trees to grow will help ensure the health and growth of the trees. Changing the width of the canopies helps create a strong architectural rhythm at a pedestrian scale. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guideline section.

Relevant Design Guidelines

C.5 Encourage overhead weather protection. Overhead weather protection should be designed with consideration given to:
b. Uses occurring within the building or in the adjacent streetscapes environment.
g. The scape of the space defined by the height and depth of the weather protection.
D.2 Enhance the building with landscaping
i. Provide identity and reinforce a desired feeling of intimacy and quiet



5 PROPOSED DEPARTURES

DEPARTURES

SITE C - OFFICE

DEPARTURE #9 (OVERHEAD WEATHER PROTECTION) - NEW DEPARTURE

Item #	Development Standard	Requirement	Modification Requested	Rationale
9	23.49.018.B Overhead Weather Protection	Overhead weather protection shall have a minimum dimension of eight (8) feet measured horizontally from the building wall or must extend to a line two (2) feet from the curb line, whichever is less.	Relief from the 8ft minimum dimension from the building wall. A canopy dimension of 6'-6" is being requested along Olive Way at the stone piers.	This departure is a response to urban forestry's requirement that all overhead weather protection be a minimum of five feet from the center of the tree, reducing the canopy width and allowing the appropriate space for the trees to grow will help ensure the health and growth of the trees. Changing the width of the canopies helps create a strong architectural rhythm at a pedestrian scale. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guideline section.

Relevant Design Guidelines

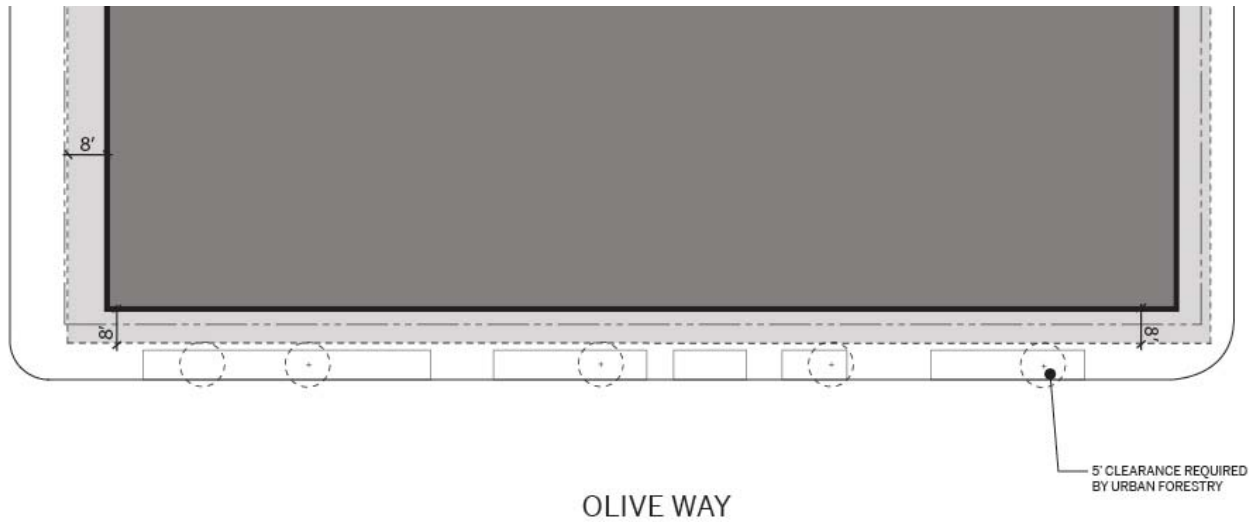
C.5 Encourage overhead weather protection. Overhead weather protection should be designed with consideration given to:

- b. Uses occurring within the building or in the adjacent streetscapes environment.
- g. The scape of the space defined by the height and depth of the weather protection.

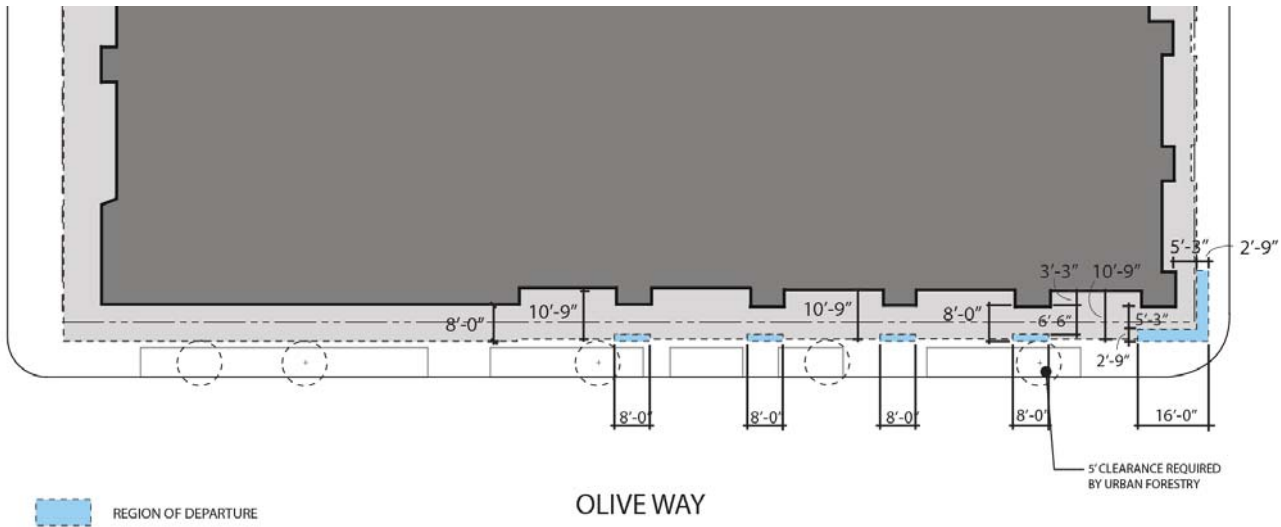
D.2 Enhance the building with landscaping

- i. Provide identity and reinforce a desired feeling of intimacy and quiet

CODE BASELINE



DRB 2



5 PROPOSED DEPARTURES

DEPARTURES

SITE C - OFFICE

DEPARTURE #10 (MINIMUM FACADE HEIGHT) - THIS DEPARTURE WAS PREVIOUSLY APPROVED BY THE DESIGN REVIEW BOARD ON MAY 15, 2016

Item #	Development Standard	Requirement	Modification Requested	Rationale
10	23.49.056.A Minimum Facade Height	A minimum facade height of 25 feet is required on class 1 pedestrian streets (Olive Way). When different minimum facade heights are established at the corner of the lot, the higher minimum height shall continue to be provided around the corner for the distance equal to the higher minimum required. (23.86.024)	Relief from the minimum facade height of 25 feet along Olive Way and the higher required minimum height for a length of 25 feet as the facade turns the corner along Boren Avenue.	A one story retail base is proposed along Olive Way to allow direct access to an open space above from the second floor main building lobby. The ground floor facade is held at 23 feet to provide a solid 42" parapet wall at the second level terrace. If the facade is raised to the code required 25' it would raise the parapet wall to 5'-6", blocking the view for people using the terrace. Large trees are proposed at the second level terrace along Olive Way to create a taller presence at the facade. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guidelines section.

Relevant Design Guidelines

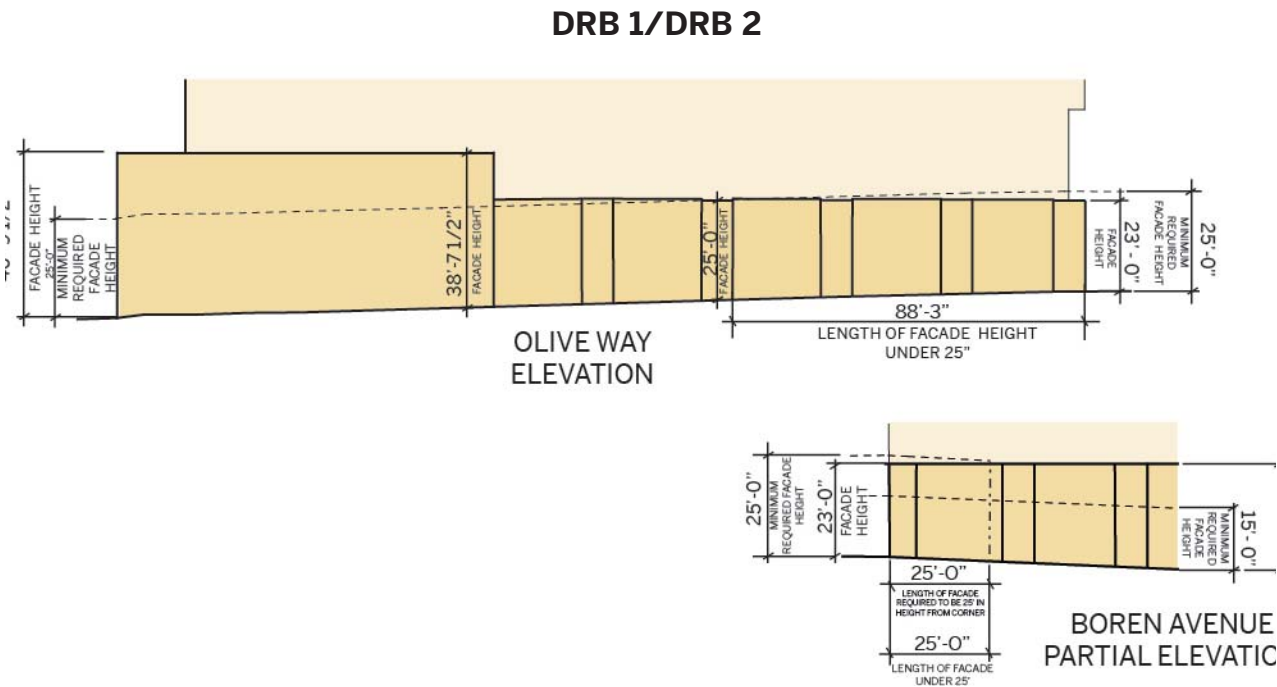
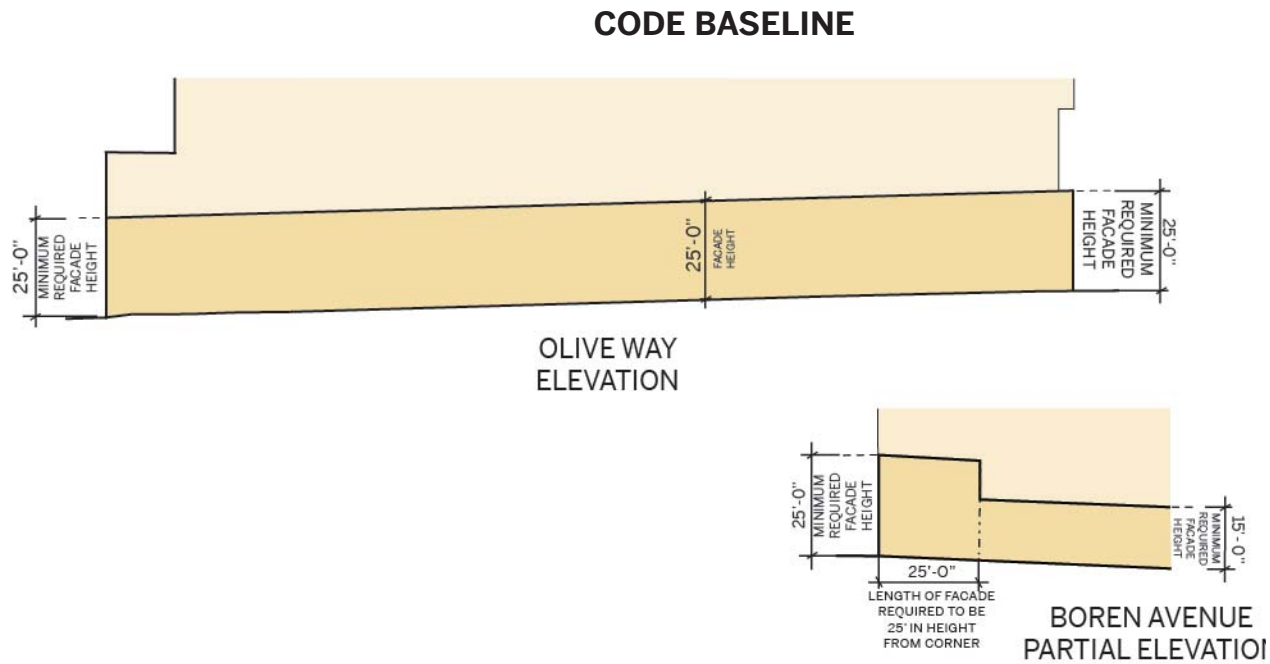
A1.1 response to context

B. A site having dramatic topography or contrasting edge conditions

B4.1 Massing

A. Setbacks, projections and open space (Setbacks and projections define building voumes and exterior open spaces)

The nonstandard shaped that is a result of the shifting street grid at Howell Street is best expressed through distinct setbacks that clearly define building volumes and exterior spaces



5 PROPOSED DEPARTURES

DEPARTURES

SITE C - OFFICE

DEPARTURE #11 (SIDEWALK WIDTH) - NEW DEPARTURE

Item #	Development Standard	Requirement	Modification Requested	Rationale
11	SMC 23.49.022.A.1 Sidewalk Width	Map 1C sidewalk widths, Howell street sidewalk width is 18' (when on a one-way street, only the side with transit stops shall be 18'; the other side shall be 15')	Relief from the 18 feet sidewalk width requirement along Howell Street. The project is requesting a 15ft width at the building columns and for a 55ft length at the glass facade looking into the truck ramp.	<p>The project is meeting the 18' sidewalk width requirement for the majority of the Howell St. facade. At the columns the sidewalk width is reduced to 15' to help break down the scale and length of the facade at the pedestrian level and meet the C1.3 design guidelines for street level articulation.</p> <p>At the mid-block of Howell St. the facade steps out 3' to align and strengthen the building mass and facade modulation of the tower above. The storefront glass in this area will be enhanced with an artist designed custom frit pattern. The project is providing the required 18 feet sidewalk width in a designated area for a potential bus stop location. This area allows people to congregate and wait at the building edge, out of the walkway path and protected from the elements by the building canopy above. Recessing the waiting area into the building also eliminates the need for a stand alone bus shelter (6'-0' X 9'-0") that can impede pedestrain traffic and create visual clutter along the street frontage. There is no bus stop planned for this site but an area is provided in the event that a future bus stop is needed. This departure allows the project to better meet the intent of the design guidelines listed in the relevant design guide section.</p>

Relevant Design Guidelines

C1.3 Street-level articulation for pedestrian activity - consider setting proportions of the building back slightly to create spaces conducive to pedestrian - oriented activities such as vending, resting, sitting, or dining. Further articulate the street level facade to provide engaging pedestrian experience via:

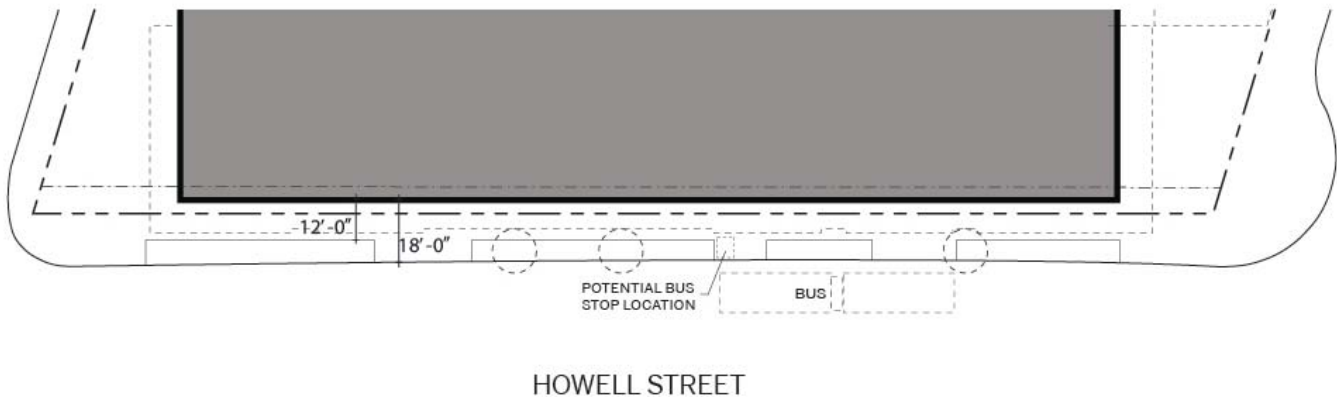
- f. Windows that encourage pedestrians to look into the building interior.
- i. Street front open space that features art work, street furniture, and landscaping.

C2 Design facade of many scales - design architectural features, fenestration patterns, and material compositions that refer to the scale of human activities contained within. Building facades should be composed of elements scaled to promote pedestrian comfort, safety and orientation.

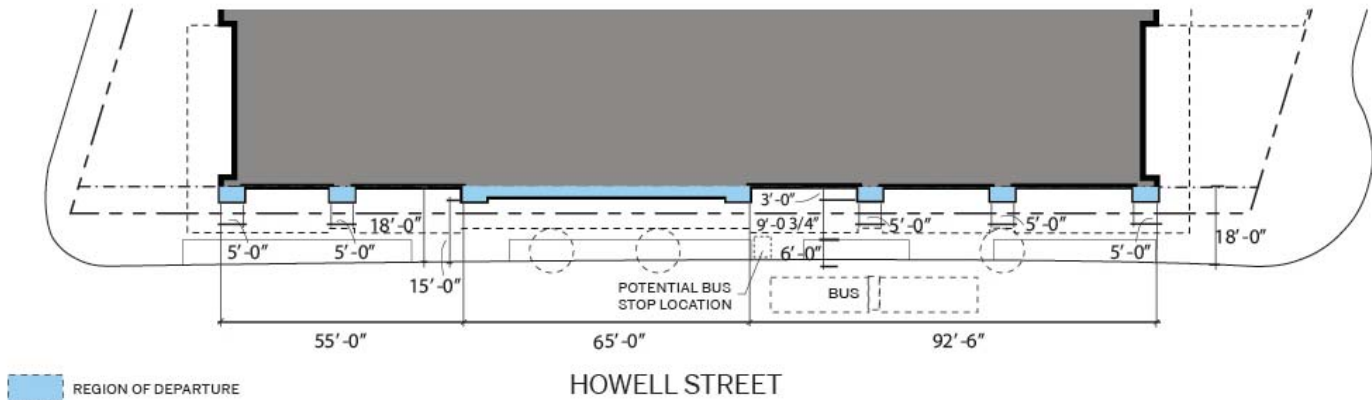
C3.1 Desirable facade elements - facades which for unavoidable programmatic reasons may have few entries or windows should receive special design treatment tp increase pedestrain safety, comfort and interest. Enliven these facade by providing

- b. Visibility into building interiors
- f. Small setback, indentions or other architectural means of breaking up the wall surface

CODE BASELINE



DRB 2



The sidewalk width is unchanged from the previously approved DRB #1 design. It was clarified that this was a deviation and needed a departure.

5 PROPOSED DEPARTURES

DEPARTURES

SITE C - OFFICE

DEPARTURE #12 (UNMODULATED FACADE) - THIS DEPARTURE WAS PREVIOUSLY APPROVED BY THE DESIGN REVIEW BOARD ON MAY 15, 2016

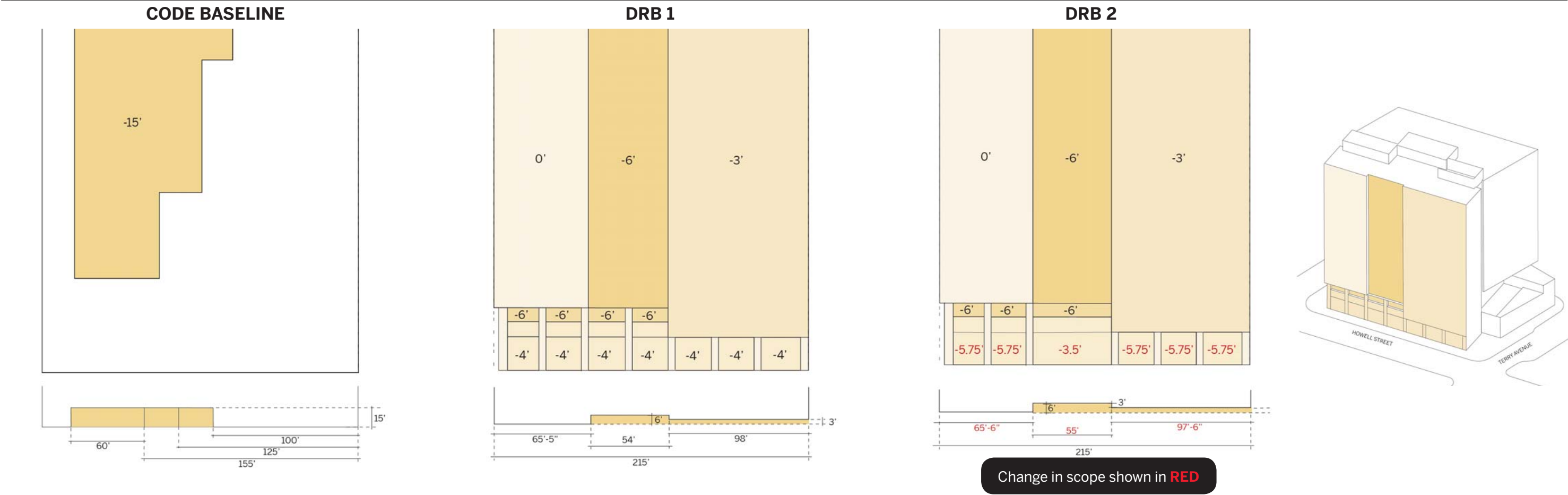
CHANGE IN SCOPE

Item #	Development Standard	Requirement	Modification Requested	Rationale
12	23.49.058.C Unmodulated Facade	Facade modulation is required above a height of 85 feet above the sidewalk of any portion of a structure located within 15 feet of a street lot line. The maximum length of unmodulated facade within 15 feet of a street lot line is 155 feet at a height between 86-160 feet, 125 feet at a height between 161-240 feet, and 100 feet at a height between 241-500 feet. Any portion of a facade exceeding the maximum length of a facade prescribed above (listed in 23.49.058 table a) shall be set back a minimum of 15 feet from the street lot line for a minimum distance of 60 feet before any other portion may be within 15 feet of the street lot line.	Relief from the upper level facade modulation requirements between 85' and 240'. The design is proposing a 55' -0" wide modulation that is setback 6' from the property line starting at 42'-6 1/8" (from Howell St. datum) and extending up to 240'. This provides an additional 1,700 sf of modulation along the full tower width at Howell Street. The design is proposing a 6 foot setback at the modulation rather than the required 15 foot setback. Further design refinement has changed the depth of setback at the ground level from what was approved at DRB #1.	The proposed facade modulation supports the overall building massing response to the shift in the street grid at Howell Street, by providing a clear frame of reference to both grids. The proposed modulation occurs over a larger area than required by code, and creates the simplified singular move encouraged by the design review board. The size and scale of the setback, a direct response to the building across Howell Street to the north, defines itself as a distinct element from the rest of the building facade. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guidelines section.

Relevant Design Guidelines

A1.1 response to context
A. A change in street grid alignment that yields a site having a nonstandard shape

B4.1 massing
A. Setbacks, projections, and open space (set backs and projections define building volumes, and exterior open spaces)
B. Relative sizes and shapes of distinct building volumes (distinct volumes relate to scale of surrounding context)



5 PROPOSED DEPARTURES

DEPARTURES

SITE C - OFFICE

DEPARTURE #13 (CURB CUT WIDTH) - NEW DEPARTURE

Item #	Development Standard	Requirement	Modification Requested	Rationale
13	SMC 23.54.030.F.2.B.1 Curb Cut Width	The minimum width of curb cuts for one way traffic is 12 feet and the maximum width is 15 feet	Relief from the curb cut width requirement on Terry Avenue and Boren Avenue. A curb cut of 30 feet is being requested.	The curb cut is required for the exiting of both office and Washington State Convention Center loading trucks. The curb cut has been reduced to the minimum dimension required for the trucks to safely exit the building without interrupting the flow of traffic in adjacent vehicle lanes. Pedestrian safety features will be incorporated into the design, including landscape and diversion elements against the building facade to direct pedestrians away from the exiting trucks, mirrors and audible system. Providing a larger curb cut in this location allows this project and the Washington State Convention center to consolidate curb cuts for loading, reducing the overall number and length of curb cuts for both sites. This allows the project to provide more active frontages. The larger opening in the facade has been treated as a design feature with a feature wall spanning from Terry Avenue to Boren Avenue. This departure allows the project to better meet the intent of the design guidelines described in their relevant design guideline section.

Relevant Design Guidelines

C1.3 Street-level uses

A. Reinforce existing retail concentrations;

B. Vary in size, width, and depth

C. Enhance main pedestrian links between areas; and

D. Establish new pedestrian activity

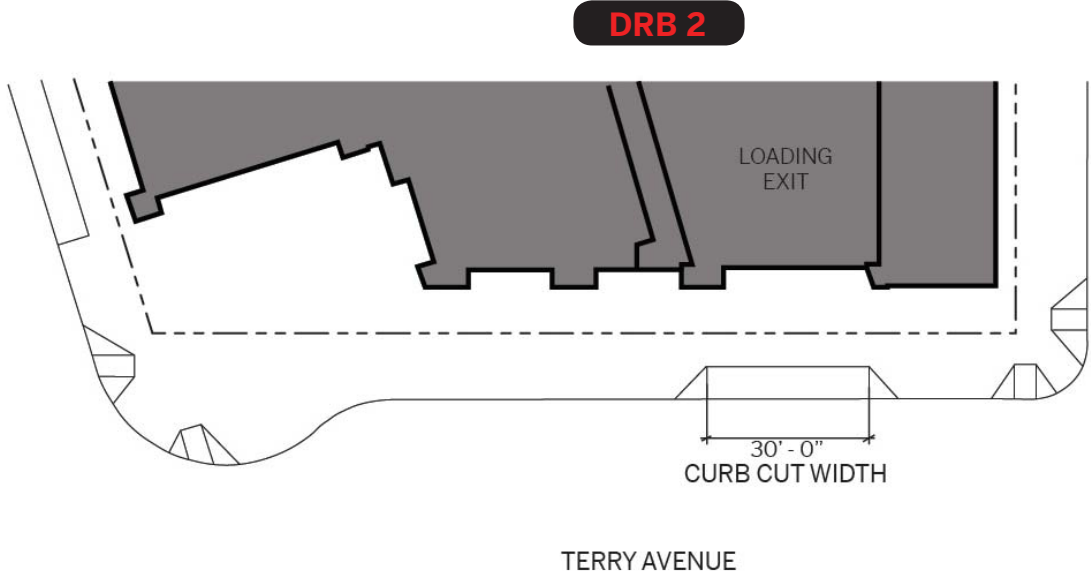
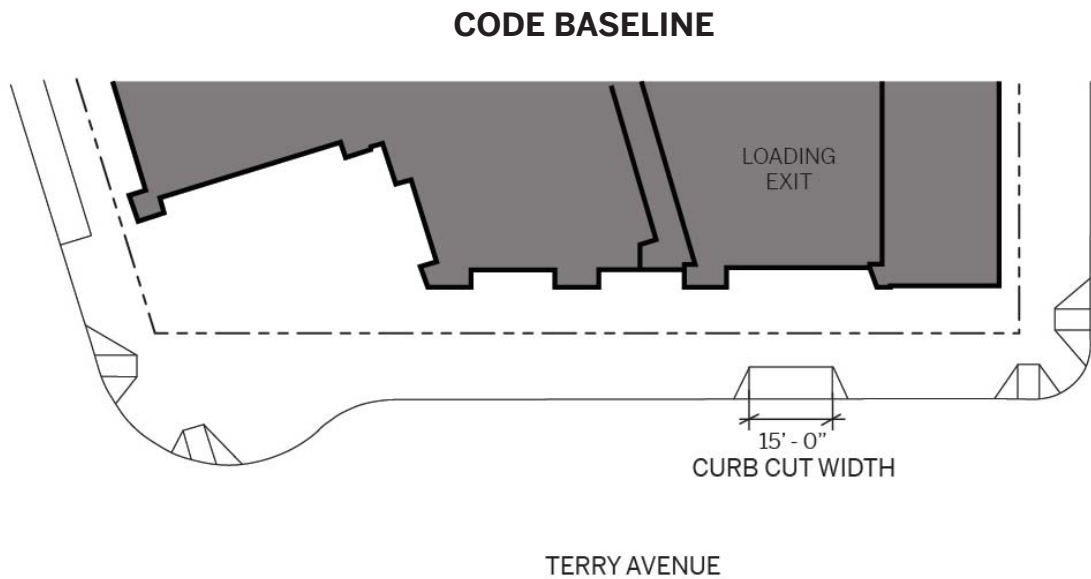
Consolidate service areas allow for more street level use and pedestrian activity or both site C and A (Washington State Convention Center)

E3.1 Integrating service areas

A. Plan service areas for less visible locations

B. Screen service areas to be less visible

Consolidate service areas, reduce the overall visibility of service doors over both site C and site A (Washington State Convention Center)



The curb cut width is unchanged from the previously approved DRB #1 design. It was clarified that the overall width was a deviation and needed a departure.

5 PROPOSED DEPARTURES

DEPARTURES

SITE C - OFFICE

DEPARTURE #14 (CURB CUT WIDTH) - NEW DEPARTURE

Item #	Development Standard	Requirement	Modification Requested	Rationale
14	SMC 23.54.030.F.2.B.1 Curb Cut Width	The minimum width of curb cuts for one way traffic is 12 feet and the maximum width is 15 feet	Relief from the curb cut width requirement on Terry Avenue and Boren Avenue. A curb cut of 58 feet and 10 inches is being requested.	This curb cut is required for loading truck access. The curb cut has been reduced to the minimum dimension required for the trucks to safely enter the building without interrupting the flow of traffic in adjacent vehicle lanes. Providing a larger curb cut in this location allows this project and the Washington State Convention Center to consolidate curb cuts for loading, reducing the overall number and length of curb cuts for both sites. This allows the project to provide more active frontage. The larger opening in the facade has been treated as a design feature with an interior graphic wall spanning from Terry Avenue to Boren Avenue. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guideline section.

Relevant Design Guidelines

C1.3 Street-level uses

A. Reinforce existing retail concentrations;

B. Vary in size, width, and depth

C. Enhance main pedestrian links between areas; and

D. Establish new pedestrian activity

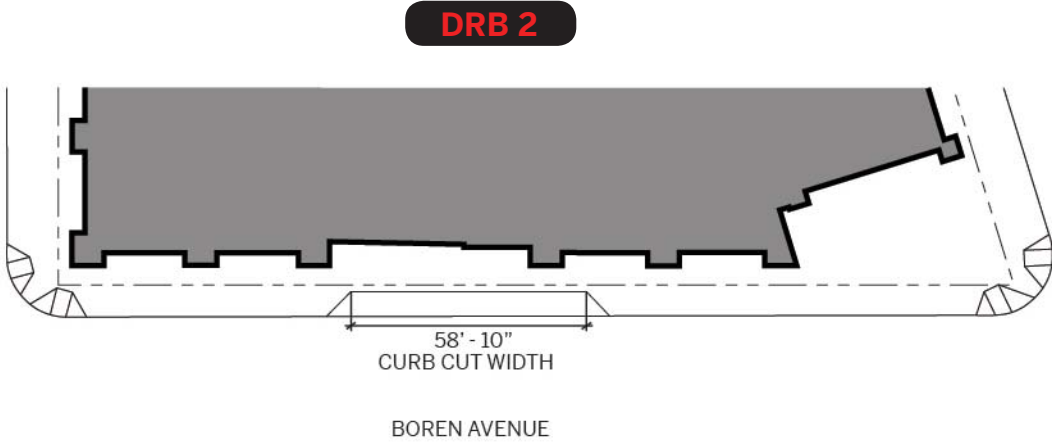
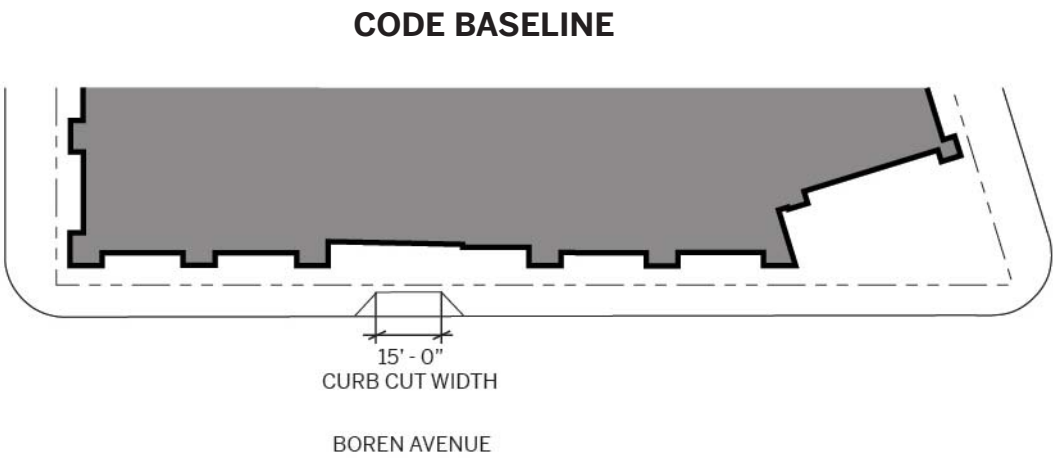
Consolidate service areas allow for more street level use and pedestrian activity or both site C and A (Washington State Convention Center)

E3.1 Integrating service areas

A. Plan service areas for less visible locations

B. Screen service areas to be less visible

Consolidate service areas, reduce the overall visibility of service doors over both site C and site A (Washington State Convention Center)

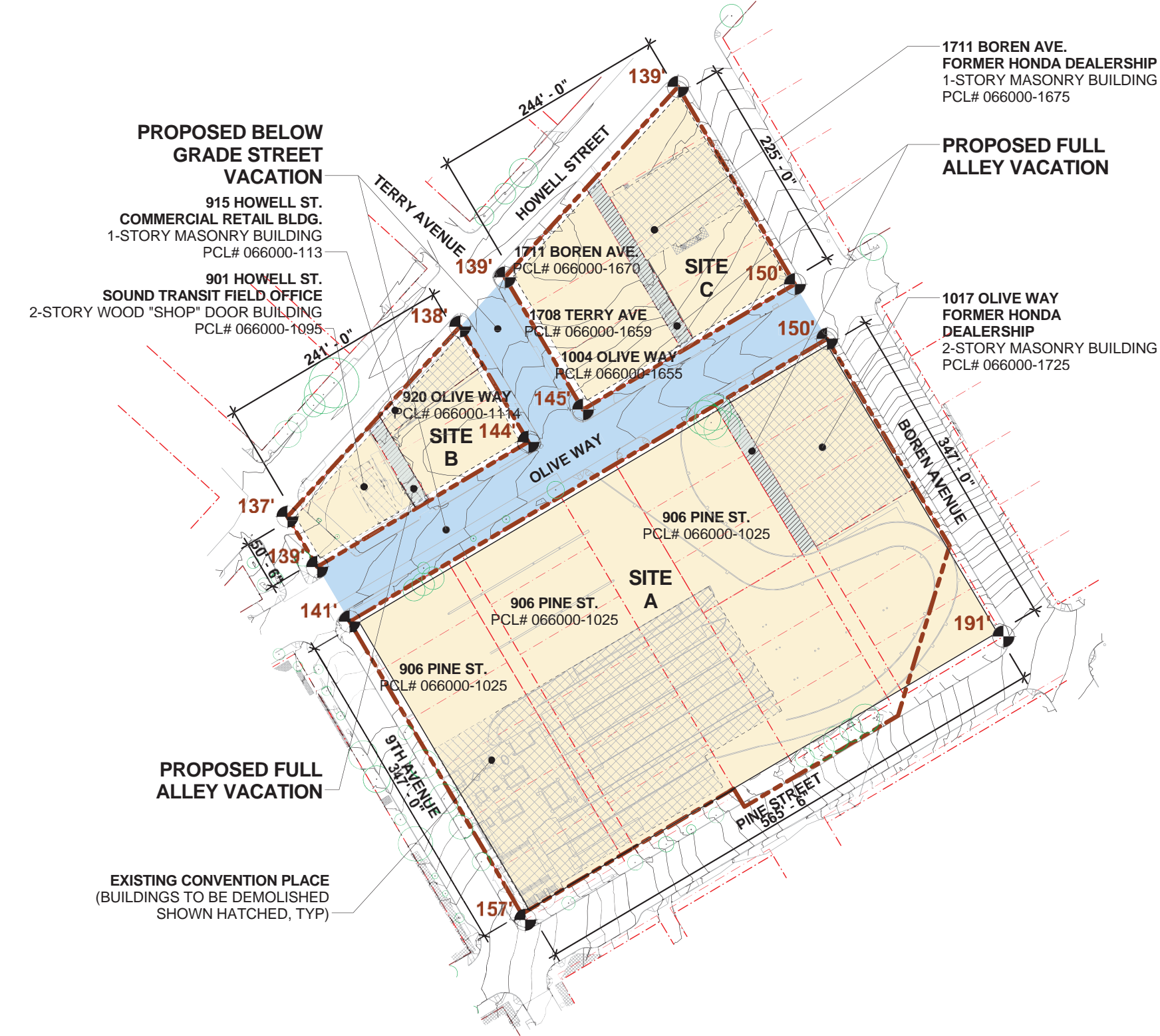


The curb cut width has been reduced by 6 inches from the previously approved DRB #1 design. It was clarified that the overall width was a deviation and needed a departure.

6 APPENDIX

PROJECT BACKGROUND

EXISTING SITE PLAN



EXISTING SITE AREA

The project site consist of 3 blocks bounded by Howell Street to the north, Pine Street to the south, 9th Ave. to the west, and Boren Ave. and I-5 to the east. Terry Ave. and Olive Way divide the site in the interior. The site slopes significantly from the highest point at the intersection of Boren Ave. and Pine Street to the lowest point at the intersection of 9th Ave. and Howell Street.

Proposed vacations include remaining alleys on Site A, B, & C as well as subterranean vacations of Olive Way and Terry Avenue.

EXISTING BUILDINGS

The current use on the site is Convention Place Station. There is also a vacant former Honda dealership on Site A. Sites B and C also contain vacant former Honda dealership facilities and a small commercial retail building.

EXISTING LANDSCAPE

The Arborist collected data (06/25/2015) on all trees of significant size, 6" and greater. The research concluded that no exceptional trees are located on the site.

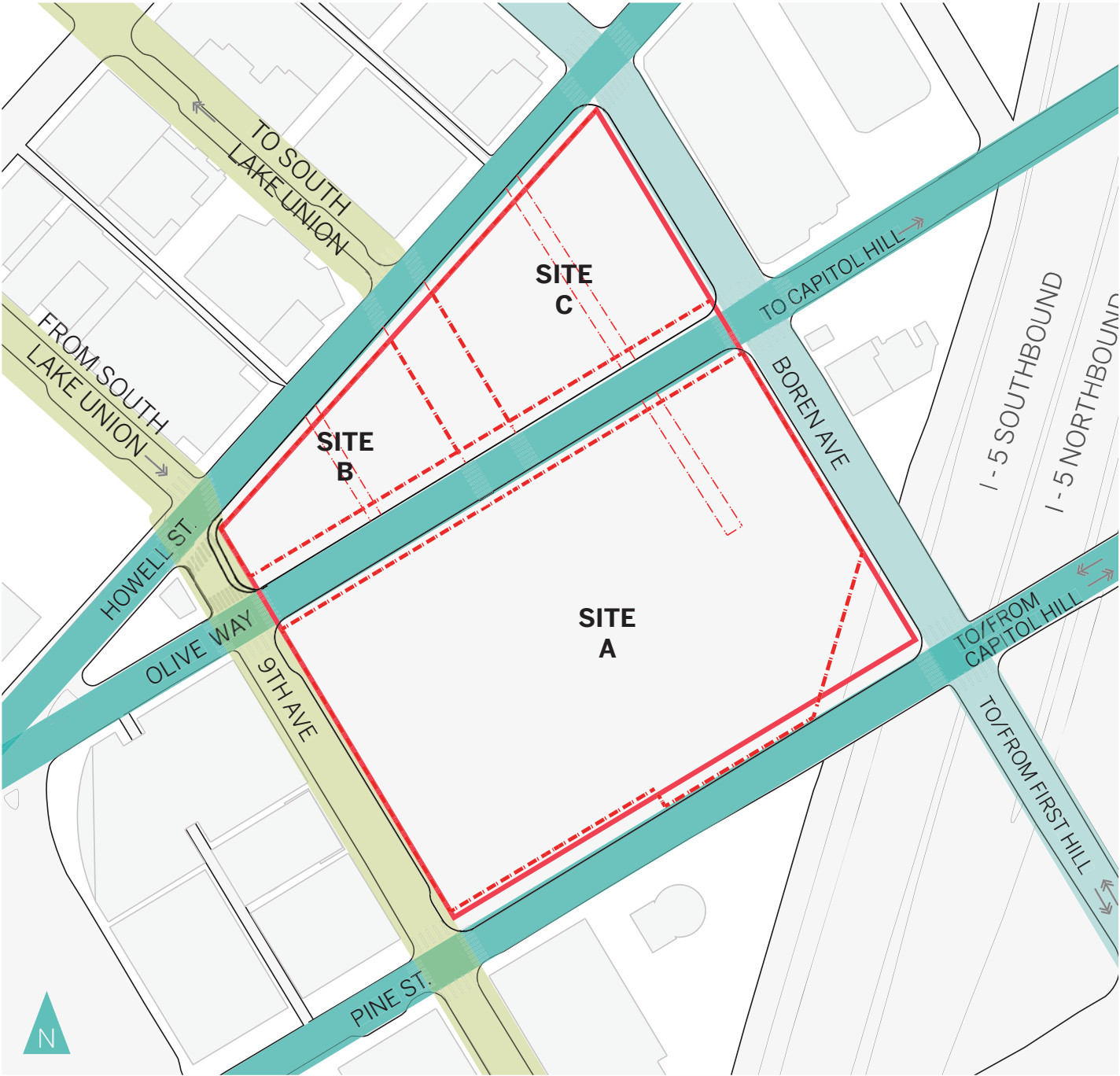
DOCUMENT KEY



6 APPENDIX

PROJECT BACKGROUND

STREET CLASSIFICATIONS SUMMARY



- PRINCIPAL TRANSIT STREET

PRINCIPAL ARTERIAL

GREEN STREET
- PROJECT AREA

PROPERTY LINE

PROPOSED FULL ALLEY VACATION

FRONTAGE CLASSIFICATION SUMMARY



- CLASS I STREET FRONTAGE

CLASS II STREET FRONTAGE

GREEN STREET FRONTAGE
- PROJECT AREA

PROPERTY LINE

PROPOSED FULL ALLEY VACATION

NOTE Landscape Requirement per Denny Triangle Urban Center Village Code applies to all frontages

6 APPENDIX

PROJECT BACKGROUND

SITE B - RESIDENTIAL

PROPERTY ADDRESSES

SITE A	SITE B	SITE C
1600 9th Ave.	920 Olive Way	1711 Boren Ave.

KING COUNTY PARCEL NUMBERS

SITE A	SITE B	SITE C
#0660001025	#0660001095	#0660001655
#0660001700	#0660001113	#0660001659
#0660001725	#0660001114	#0660001670
		#0660001675

ZONING SMC 23.49.056 MAP A

SITE A/B/C
Denny Triangle Urban Center Village
Downtown Mixed Commercial – DMC 340/290-400

SITE AREA

SITE A	SITE B	SITE C
202,509 sf*	25,551 sf*	50,979 sf*

* Includes vacated alleys

DESIGN REVIEW SMC 23.41
Required

STRUCTURE HEIGHT 23.49.008

Max height = 400 feet
The proposed height is approximately 290 feet.

STREET-LEVEL USE 23.49.009 MAP 1G

9TH AVE. – Not Required
HOWELL STREET – Not Required
OLIVE WAY – Not Required
TERRY AVE. - 75% Required
Street level use is provided to exceed development standards along Howell Street and Olive Way. See proposed departure at Terry Ave.

FLOOR AREA RATIO 23.49.011

Base = 5, Max = 10 - Site ~ 25,551 SF
Exemptions: Residential use, street level use, below-grade use.
Proposed FAR is approximately 1.1

RESIDENTIAL UNITS: 404

COMMON RECREATION AREA 23.49.010

5 % of the total residential gross floor area
Required = 18,242 SF
Proposed total = 19,402 SF
Proposed Indoor = 7,345 SF
Proposed Outdoor = 9,957 SF
Proposed Green St. Improvement = 2,100 SF (23.49.010.B9)
Amenity area is proposed for certain interior areas of the building, along with exterior amenity space on the podium and tower rooftop.

OVERHEAD WEATHER PROTECTION & LIGHTING 23.49.018

Required along Terry Ave., 9th Ave., Olive Way, Boren Ave., and Howell St.
Canopies are proposed along the entire building per development standards, except along 9th Ave. where a large overhang and open space is provided.

CAR PARKING 23.49.019.E

None required, none provided.

BICYCLE PARKING 23.49.019.E

Residential - 1 space per every two dwelling units
Retail - 1 space per 5,000 SF of retail space
Bicycle parking has been proposed to meet development standards.

MINIMUM SIDEWALK WIDTH 23.49.022

9TH Ave. = Varies per Green Street requirements
HOWELL STREET = 18 feet (with transit stops)
OLIVE WAY = 15 feet
TERRY Ave. = Varies per Green Street requirements
The building has been setback from the property line to meet or exceed the sidewalk width requirements. See departure request on Howell St.

MINIMUM FACADE HEIGHT 23.49.056.A

25 feet on Class 1 Pedestrian:
Olive Way, 9th Ave., Terry Ave., Howell Street
15 feet on Class 2 Pedestrian: N/A
The proposed facade height meets development standards.

FACADE SETBACK LIMITS 23.49.056.B.2

Required from 15’-0” to 25’-0” along Class 1 and Green Streets: Olive Way, 9th Ave., Terry Ave., Howell Street
There are three departure requests to create open public benefit spaces.

FACADE TRANSPARENCY 23.49.056.C

Class 1 = 60% transparency required between 2’-0” and 8’-0” above sidewalk:
The proposed design meets the facade transparency development standards for the site.

BLANK FACADE LIMITS 23.49.056.D

Class 1 and Green Street:
Maximum width of blank facade = 15’-0”:
See departure request at Terry Ave.

STREET TREES 23.49.056.E

Required on all streets
Street trees are proposed along all sides per development standards

LANDSCAPING 23.49.056.F

Area provided shall be 1.5 times the length of the street lot line: 23.49.056.F.1
Green Street planting: 23.49.056.F.2
Landscaping in setbacks of area more than 300 SF require 20% landscape: 23.49.056.F.3
See departure request at 9th Ave.

GREEN STREET SETBACK 23.49.056.F4

2’-0” setback required
50 % of setback shall be landscaped
Green Street setbacks are provided to meet development standards.

UPPER LEVEL GREEN STREET SETBACK 23.49.058.G

15 foot setback required above 45 feet (along 9th Ave. and Terry Ave.)
See departure request at Terry Ave.

MAXIMUM TOWER WIDTH

The maximum tower width for portions of a structure above a height of 85 feet shall be 120 feet or 80% of the width of the lot measured in the Avenues (9th and Terry)
The proposed maximum tower width meets development standards.

SOLID WASTE STORAGE 23.54.040

Residential
575 SF plus 4 SF for each additional unit above 100
Retail
50% of the requirement for nonresidential development in mixed use development that contains both residential and nonresidential use
A space smaller than the required area for solid waste storage is being proposed. A solid waste management plan is being proposed.

6 APPENDIX

PROJECT BACKGROUND

SITE C - OFFICE

STRUCTURE HEIGHT 23.49.008
Maximum Height= 340ft
The proposed height is approximately 240 feet.

STREET-LEVEL USE 23.49.009 MAP 1G
BOREN AVE. – Not Required
HOWELL STREET – Not Required
OLIVE WAY – Not Required
TERRY AVE. – 75% Required
Street level use is provided to exceed development standards along Boren Ave., Howell Street, and Olive Way.
See proposed departure at Terry Ave.

FLOOR AREA RATIO 23.49.011 SMC
Base = 5, Max = 10 - Site ~ 50,980 SF
Exemptions: Residential use, street level use, below grade
Proposed FAR is approximately 9.9.

OPEN SPACE 23.49.016
20 SF required per 1,000 SF of office area.
Open space is proposed to meet development standards.
Two large terraces will be accessible from the second floor lobby space and at the tower roof.

OVERHEAD WEATHER PROTECTION & LIGHTING 23.49.018
Canopies are proposed along the entire building per development standards. See three departure requests.

CAR PARKING 23.49.019.E
None required, none provided.

BICYCLE PARKING 23.49.019.E
1 space per 5000 SF of office area use up to 50 spaces.
1 space per 10,000 SF office area above 50 spaces.
Bicycle parking has been proposed to meet development standards.

SHOWER FACILITY 23.49.019.F
1 shower per gender per 250,000 SF of office
The shower facility has been proposed to meet the development standards.

OFF STREET LOADING 23.49.019.G
Off street loading required = Low Use (Table A 23.54.035)
Off street loading bays are proposed to meet development standards.

MINIMUM SIDEWALK WIDTH 23.49.022
BOREN Ave. = 12 feet
HOWELL STREET = 15 feet (18 feet with transit stops)
OLIVE WAY = 15 feet
TERRY Ave. = Varies per Green Street requirements
The building design has been setback from the property line to meet or exceed sidewalk width requirements.
See departure request.

MINIMUM FACADE HEIGHT 23.49.056.A
25 feet on Class 1 Pedestrian:
Olive Way, Terry Ave., Howell Street
15 feet on Class 2 Pedestrian: Boren Ave.
Minimum facade heights are proposed to meet development standards. See departure request at Olive Way.

FACADE SETBACK LIMITS 23.49.056.B.2
Required from 15'-0" to 25'-0" along Class 1 Green Streets:
Olive Way, Terry Ave., Howell Street
Required from 0'-0" to 15'-0" along Class 2: Boren Ave.
There are two departure requests.

FACADE TRANSPARENCY 23.49.056.C
Class 1 = 60% transparency required between 2'-0" and 8'-0" above sidewalk: Howell Street, Olive Way, Terry Ave.
Class 2 = 30% Transparency required between 2'-0" and 8'-0" above sidewalk: Boren Ave.
The proposed design meets and exceeds the facade transparency development standards for the site. See departure request at Terry Ave.

BLANK FACADE LIMITS 23.49.056.D
Class 1 and Green Street:
Maximum width of blank facade = 15'-0":
Howell Street, Olive Way, Terry Ave.
Class 2: Maximum width of blank facade = 30'-0":
Boren Ave.
The proposed design meets development standards for the site. See departure request at Terry Ave.

STREET TREES 23.49.056.E
Required on all streets
Street trees are proposed along all sides per development standards.

LANDSCAPING 23.49.056.F
Area provided shall be 1.5 times the length of the street lot line: 23.49.056.F
Green Street planning: 23.49.056.F.2
Landscaping in setbacks of area more than 300 SF require 20% landscape: 23.49.056.F.3
The landscaping proposed has been designed to meet development standards.

GREEN STREET SETBACK 23.49.056.F4
2'-0" setback required
50 % of setback shall be landscaped
Green Street setbacks are provided to meet development standards.

MAXIMUM LENGTH OF UNMODULATED FACADE 23.49.058.B
Within 15'-0" of property line
155 feet between 86'-0" and 160'-0" (Max length of unmodulated facade)
125 feet between 161'-0" and 240'-0" (Max length of unmodulated facade)
See departure request at Howell St.

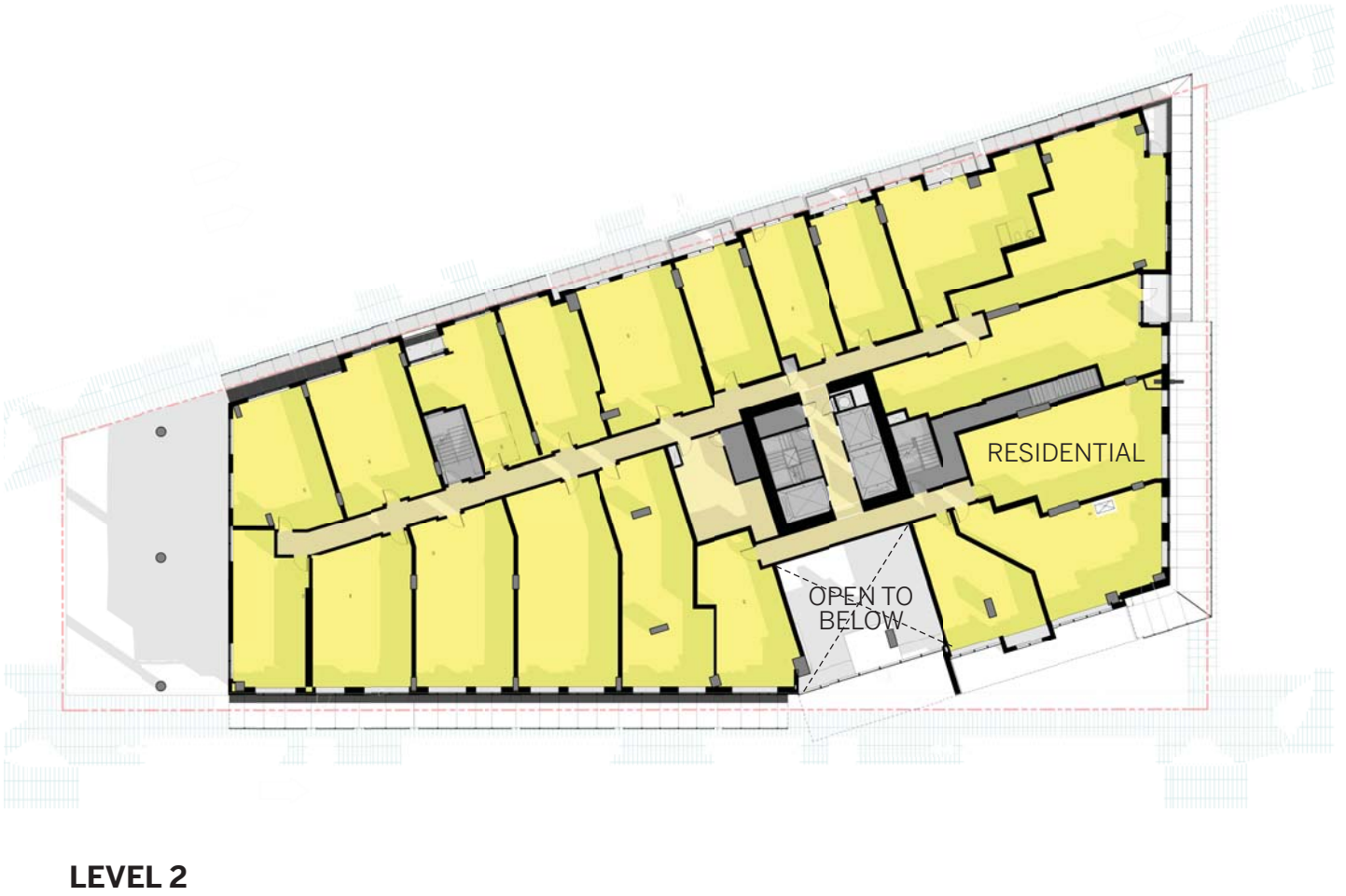
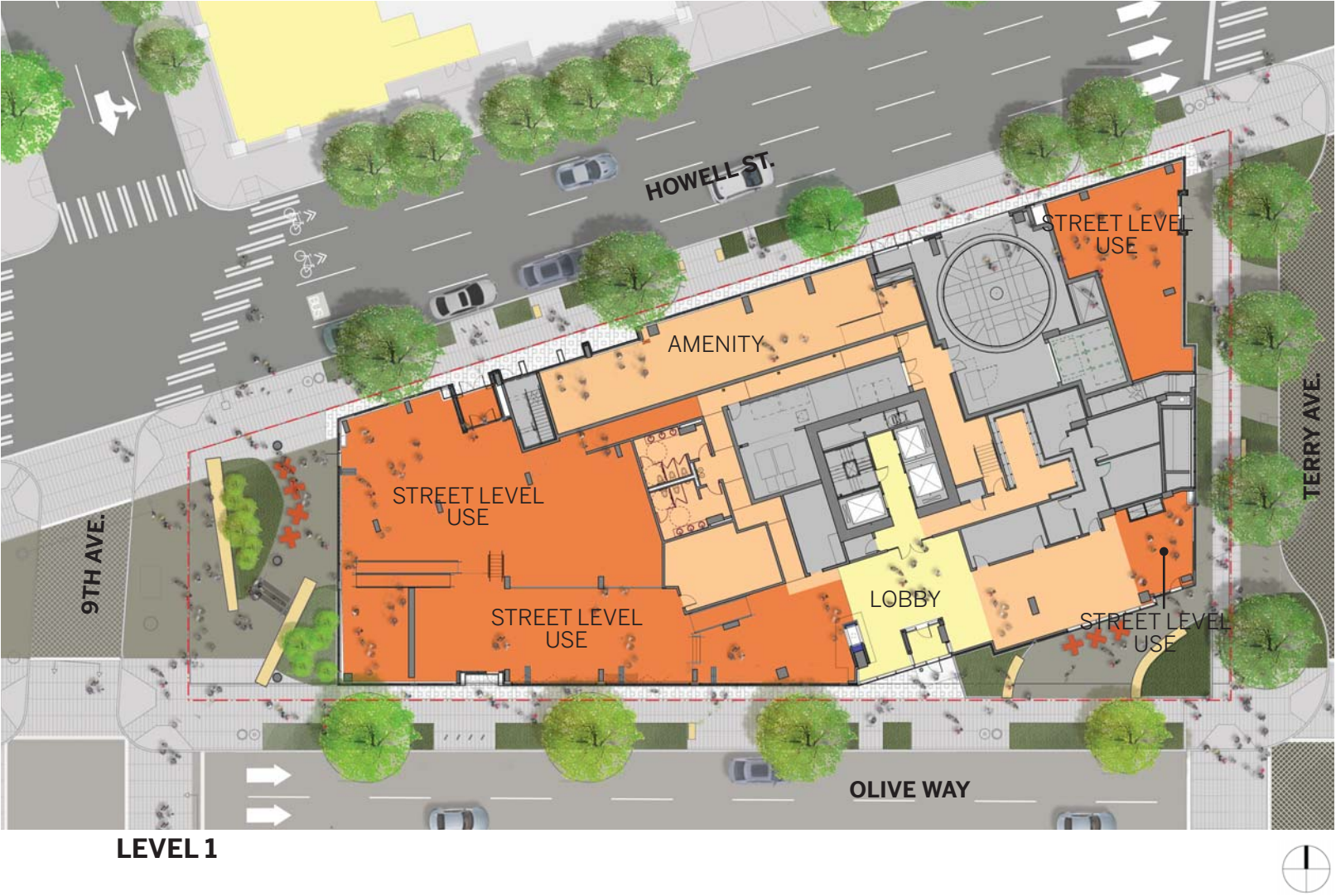
UPPER LEVEL GREEN STREET SETBACK 23.49.058.G
15 foot setback required above 45 feet (along Terry Ave.)
The tower is setback beyond what is required to offer more space and light for the green street.

SOLID WASTE STORAGE 23.54.040
Office required: 500 SF of shared storage
Retail required: 125 SF of shared storage
The proposed solid waste storage exceeds development standards.

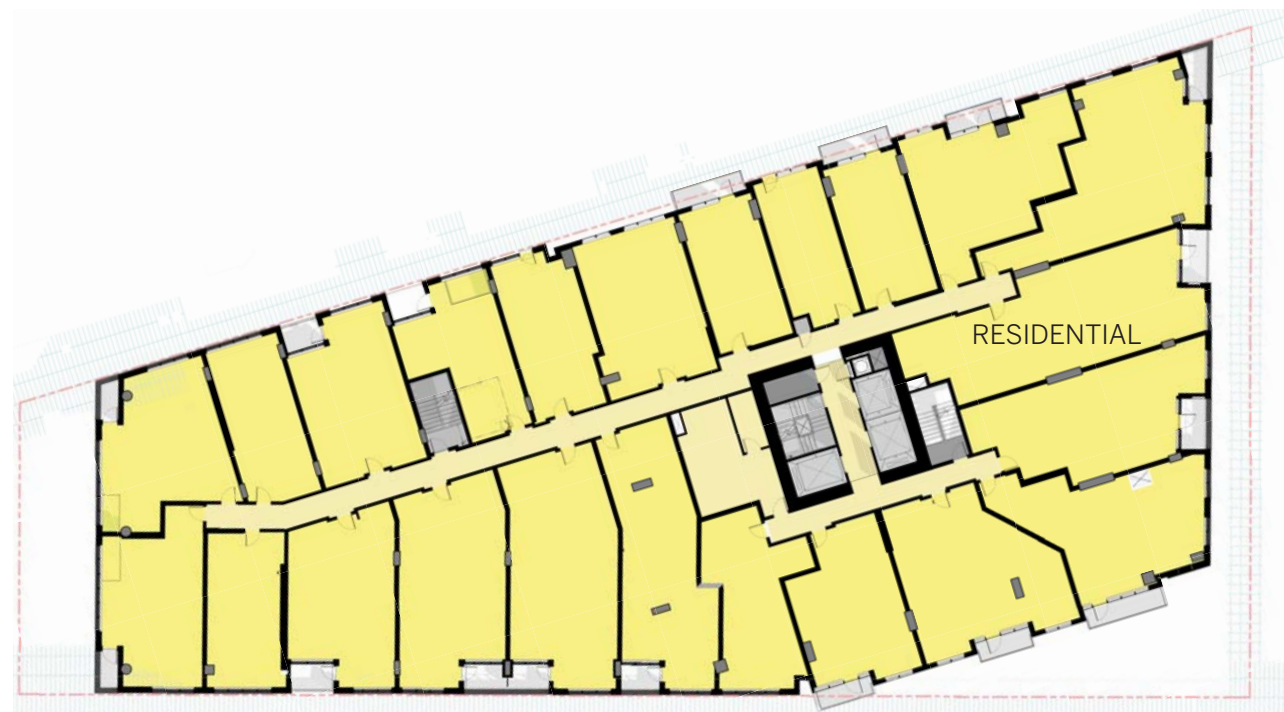
6 APPENDIX

FLOOR PLANS

SITE B - RESIDENTIAL



*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2021.



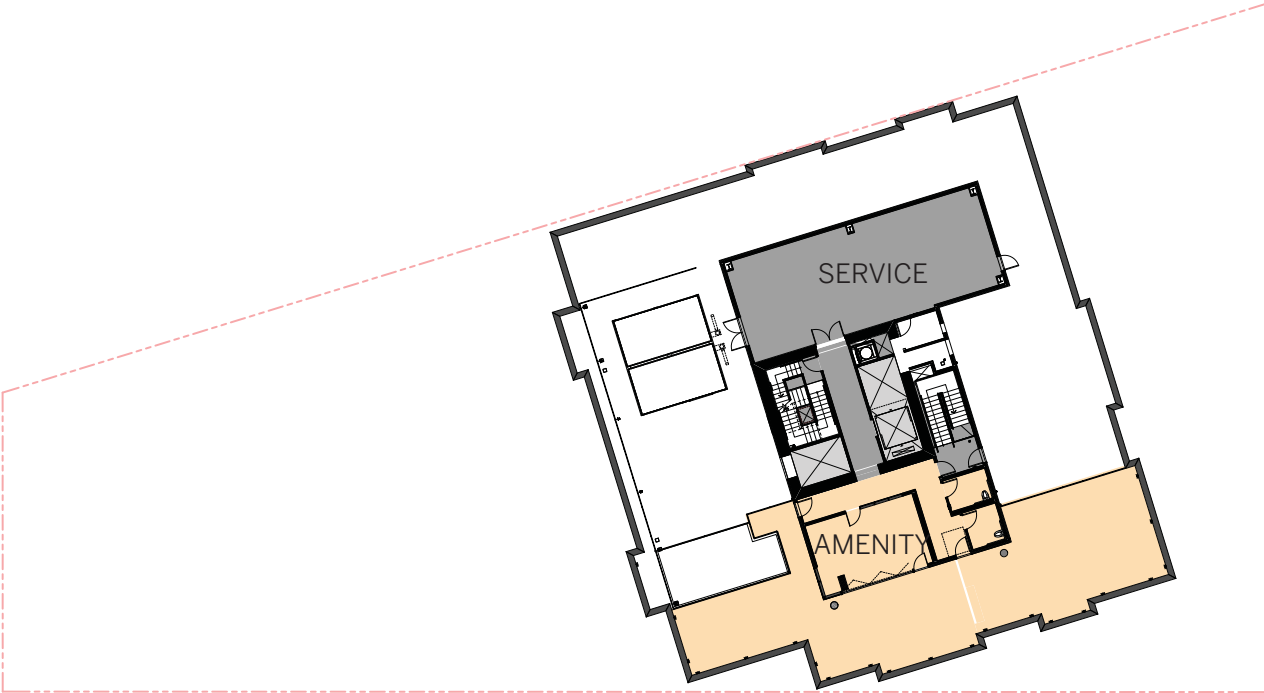
LEVEL 3-6



LEVEL 7



LEVEL 8-29



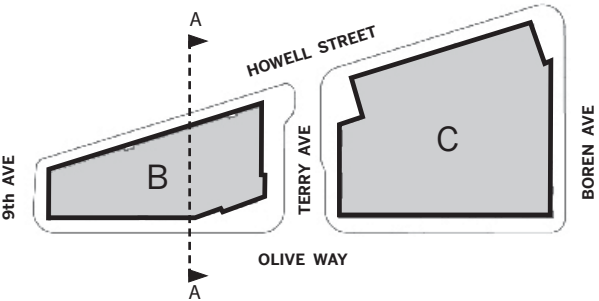
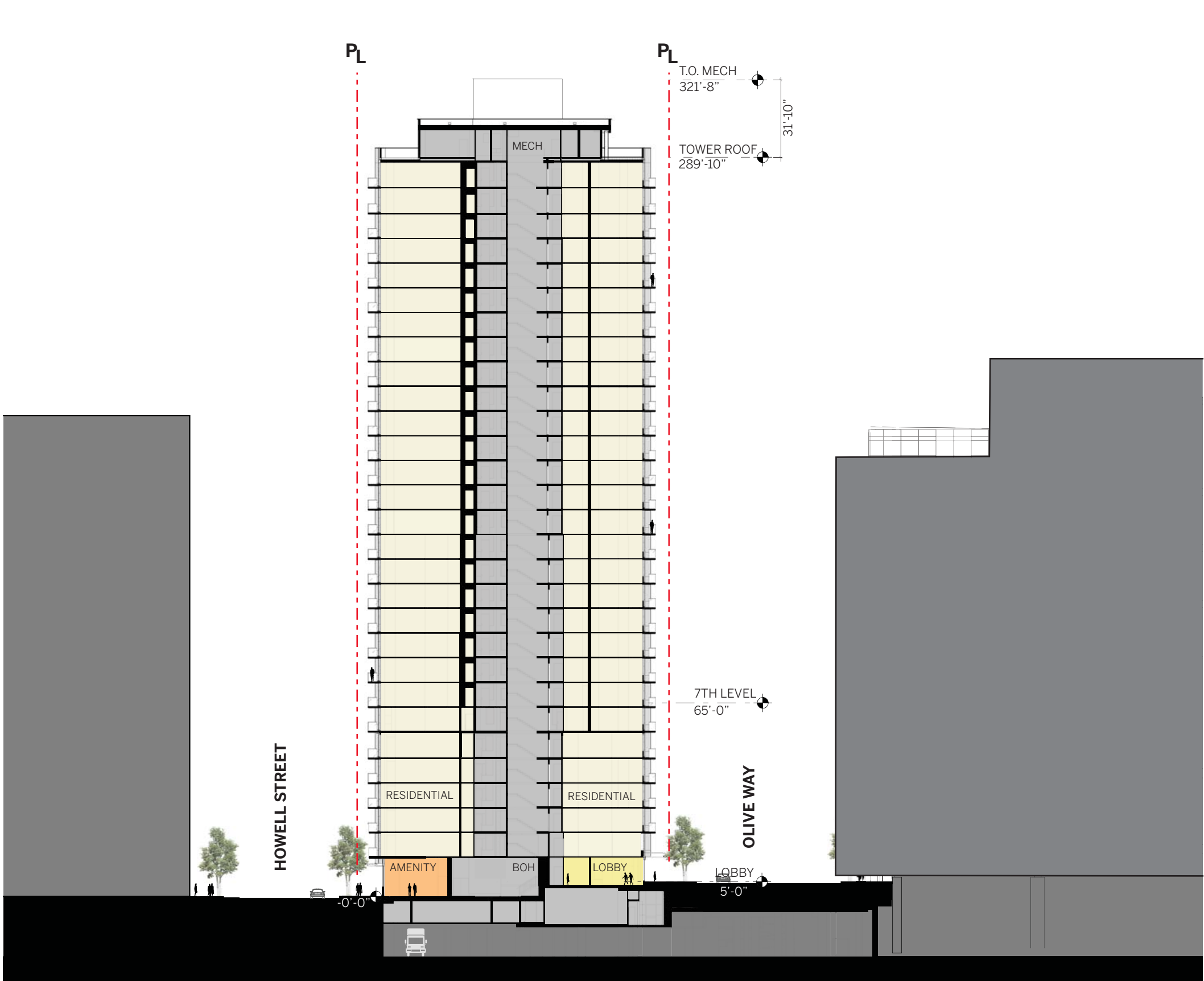
LEVEL 30

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6 APPENDIX

SECTIONS

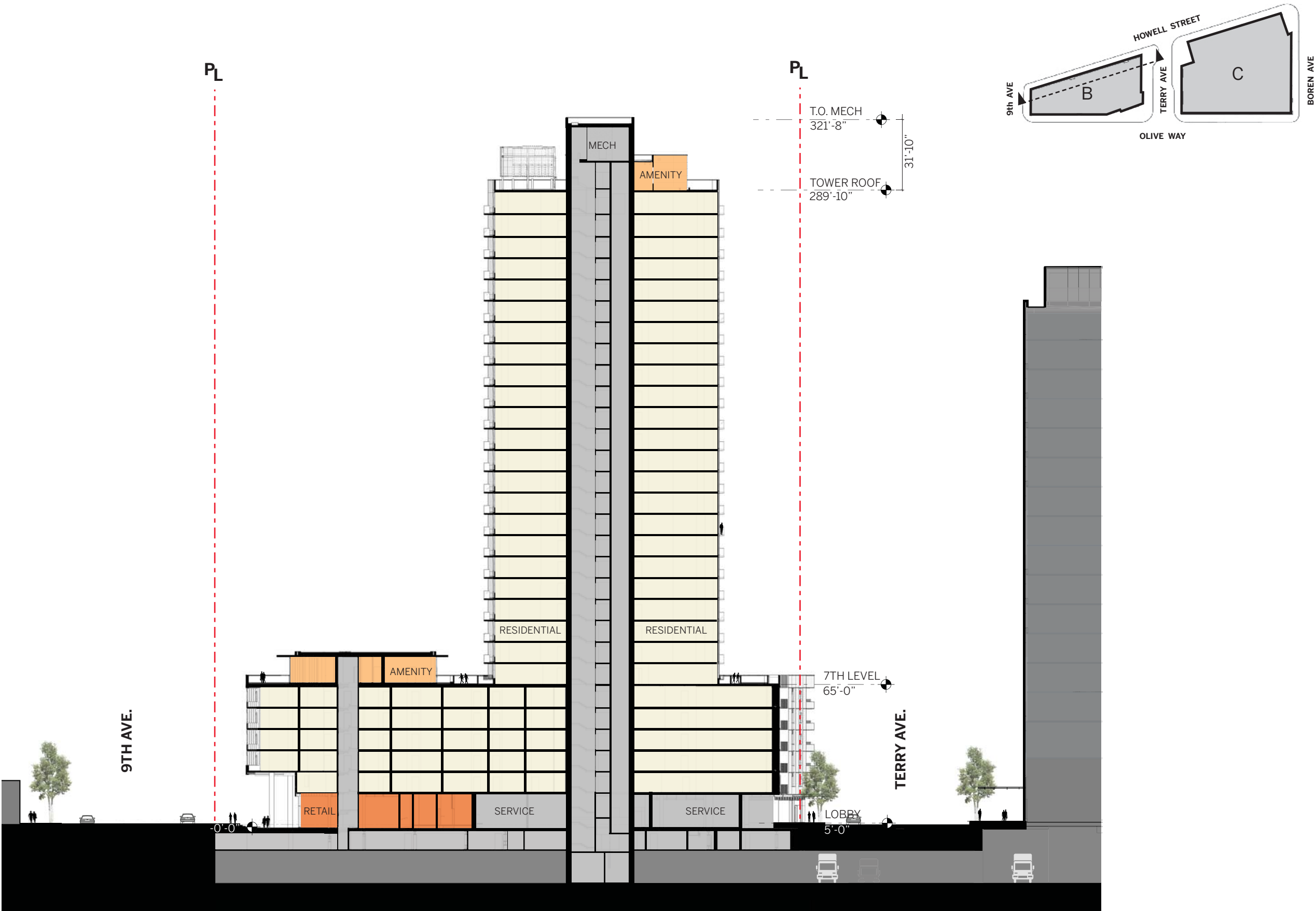
SITE B - RESIDENTIAL



6 APPENDIX

SECTIONS

SITE B - RESIDENTIAL



6 APPENDIX

ELEVATIONS

SITE B - RESIDENTIAL



A: Clear Vision (GL - 1)



B: Clear Spandrel (GL - 2)



C: Reflective Vision (GL - 3)



D: Reflective Spandrel (GL - 4)



E: Custom Design Metal Screen (MTLP-2 LIGHT)



F: Custom Design Metal Screen (MTLP-1 DARK)



G: Norman Brick (BRK-1)



H: Wood (WD - 1)



I: Metal Panel (CMPST MTLP-2 LIGHT)



J: Metal Panel (CMPST MTLP-1 DARK)



K: Louver - Podium (LVR-1)



L: Louver - Tower (LVR-2)



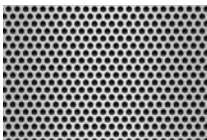
M: Louver - Podium Reveal (LVR-3)



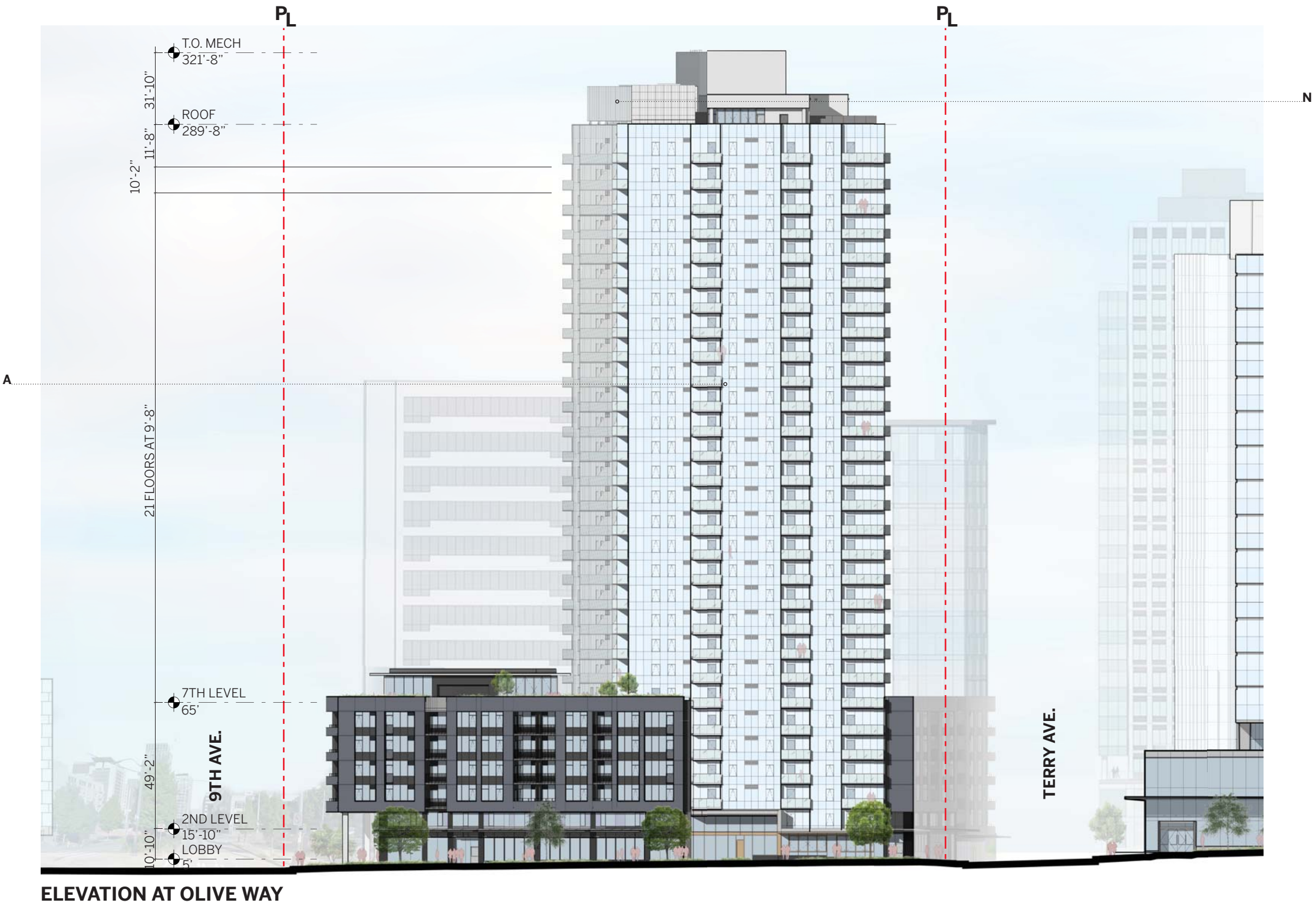
N: Metal Screen (GRTG-1)



O: Stone (STN - 1)



P: Perforated Metal (MTPL-3)



6 APPENDIX

ENLARGED ELEVATIONS

SITE B - RESIDENTIAL



*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2021.



6 APPENDIX

ELEVATIONS

SITE B - RESIDENTIAL



A: Clear Vision (GL - 1)



B: Clear Spandrel (GL - 2)



C: Reflective Vision (GL - 3)



D: Reflective Spandrel (GL - 4)



E: Custom Design Metal Screen (MTLP-2 LIGHT)



F: Custom Design Metal Screen (MTLP-1 DARK)



G: Norman Brick (BRK-1)



H: Wood (WD - 1)



I: Metal Panel (CMPST MTLP-2 LIGHT)



J: Metal Panel (CMPST MTLP-1 DARK)



K: Louver - Podium (LVR-1)



L: Louver - Tower (LVR-2)



M: Louver - Podium Reveal (LVR-3)



N: Metal Screen (GRTG-1)



O: Stone (STN - 1)



P: Perforated Metal (MTPL-3)



ELEVATION AT TERRY AVE.

6 APPENDIX

ENLARGED ELEVATIONS

SITE B - RESIDENTIAL



*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2021.



6 APPENDIX

ELEVATIONS

SITE B - RESIDENTIAL



A: Clear Vision (GL - 1)



C: Reflective Vision (GL - 3)



E: Custom Design Metal Screen (MTLP-2 LIGHT)



G: Norman Brick (BRK-1)



I: Metal Panel (CMPST MTLP-2 LIGHT)



K: Louver - Podium (LVR-1)



M: Louver - Podium Reveal (LVR-3)



O: Stone (STN - 1)



B: Clear Spandrel (GL - 2)



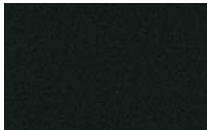
D: Reflective Spandrel (GL - 4)



F: Custom Design Metal Screen (MTLP-1 DARK)



H: Wood (WD - 1)



J: Metal Panel (CMPST MTLP-1 DARK)



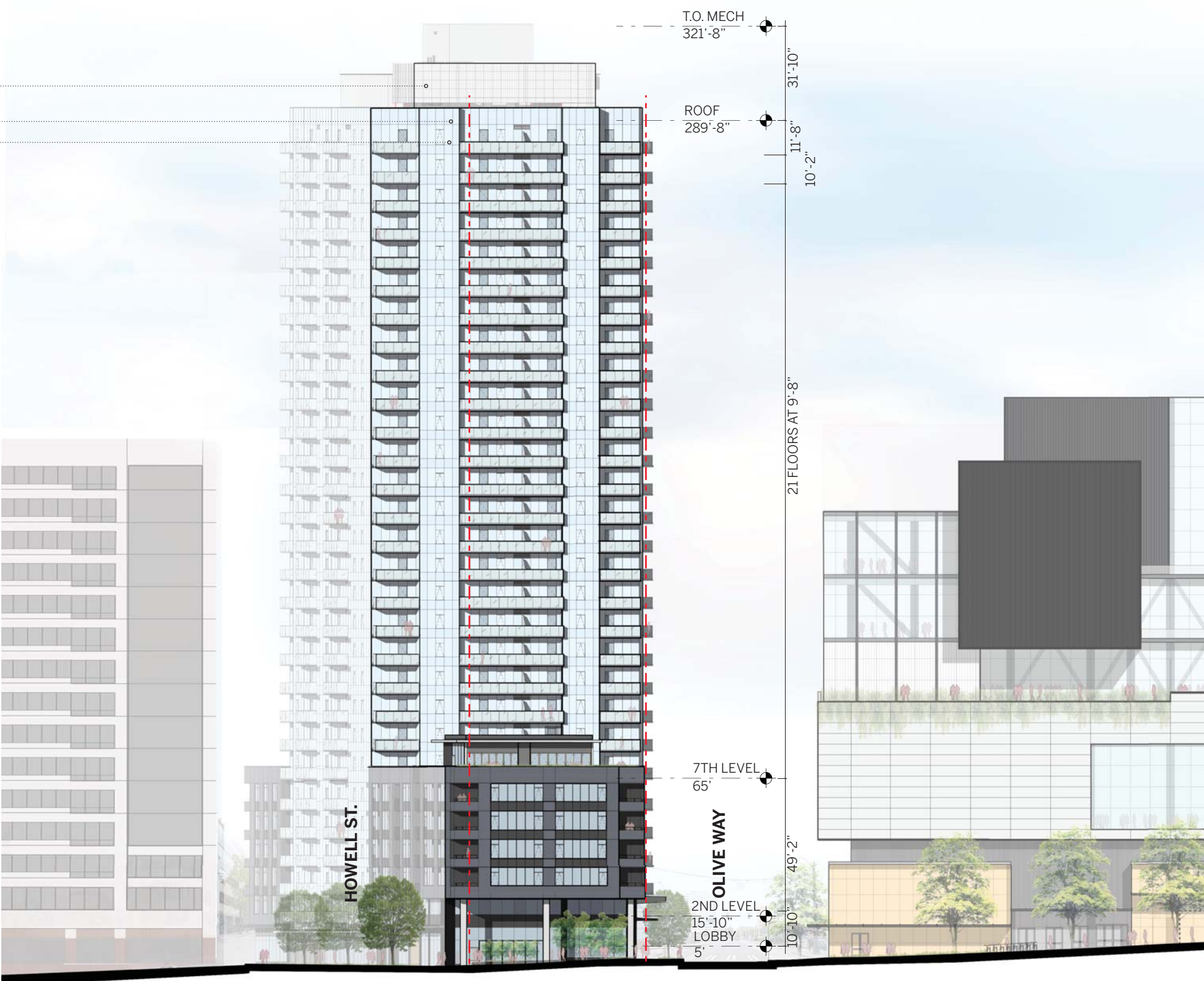
L: Louver - Tower (LVR-2)



N: Metal Screen (GRTG-1)



P: Perforated Metal (MTPL-3)



ELEVATION AT 9TH AVE.

6 APPENDIX

ENLARGED ELEVATIONS

SITE B - RESIDENTIAL



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6 APPENDIX

ELEVATIONS

SITE B - RESIDENTIAL



A: Clear Vision (GL - 1)



B: Clear Spandrel (GL - 2)



C: Reflective Vision (GL - 3)



D: Reflective Spandrel (GL - 4)



E: Custom Design Metal Screen (MTLP-2 LIGHT)



F: Custom Design Metal Screen (MTLP-1 DARK)



G: Norman Brick (BRK-1)



H: Wood (WD - 1)



I: Metal Panel (CMPST MTLP-2 LIGHT)



J: Metal Panel (CMPST MTLP-1 DARK)



K: Louver - Podium (LVR-1)



L: Louver - Tower (LVR-2)



M: Louver - Podium Reveal (LVR-3)



N: Metal Screen (GRTG-1)



O: Stone (STN - 1)



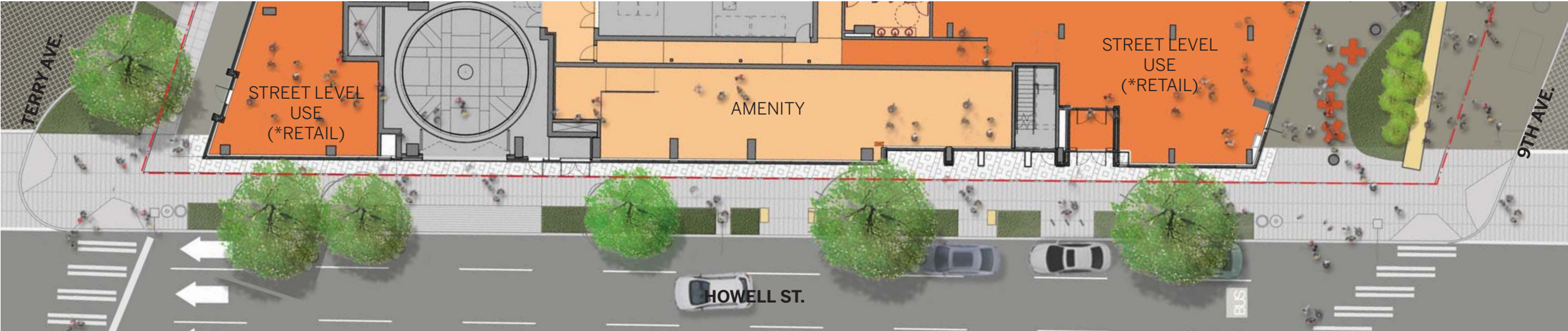
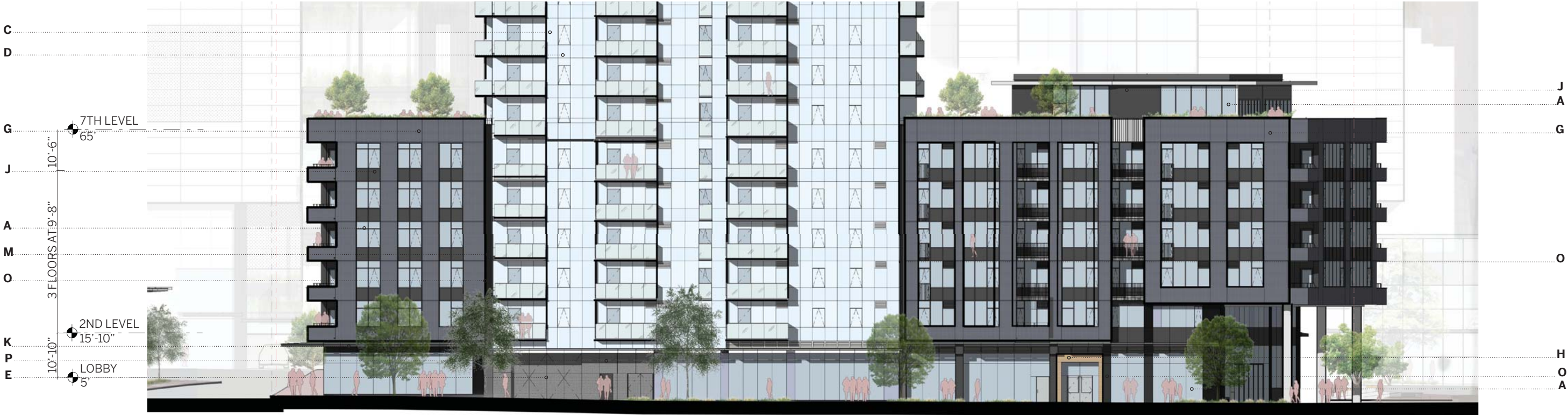
P: Perforated Metal (MTPL-3)



6 APPENDIX

ENLARGED ELEVATIONS

SITE B - RESIDENTIAL



*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2021.

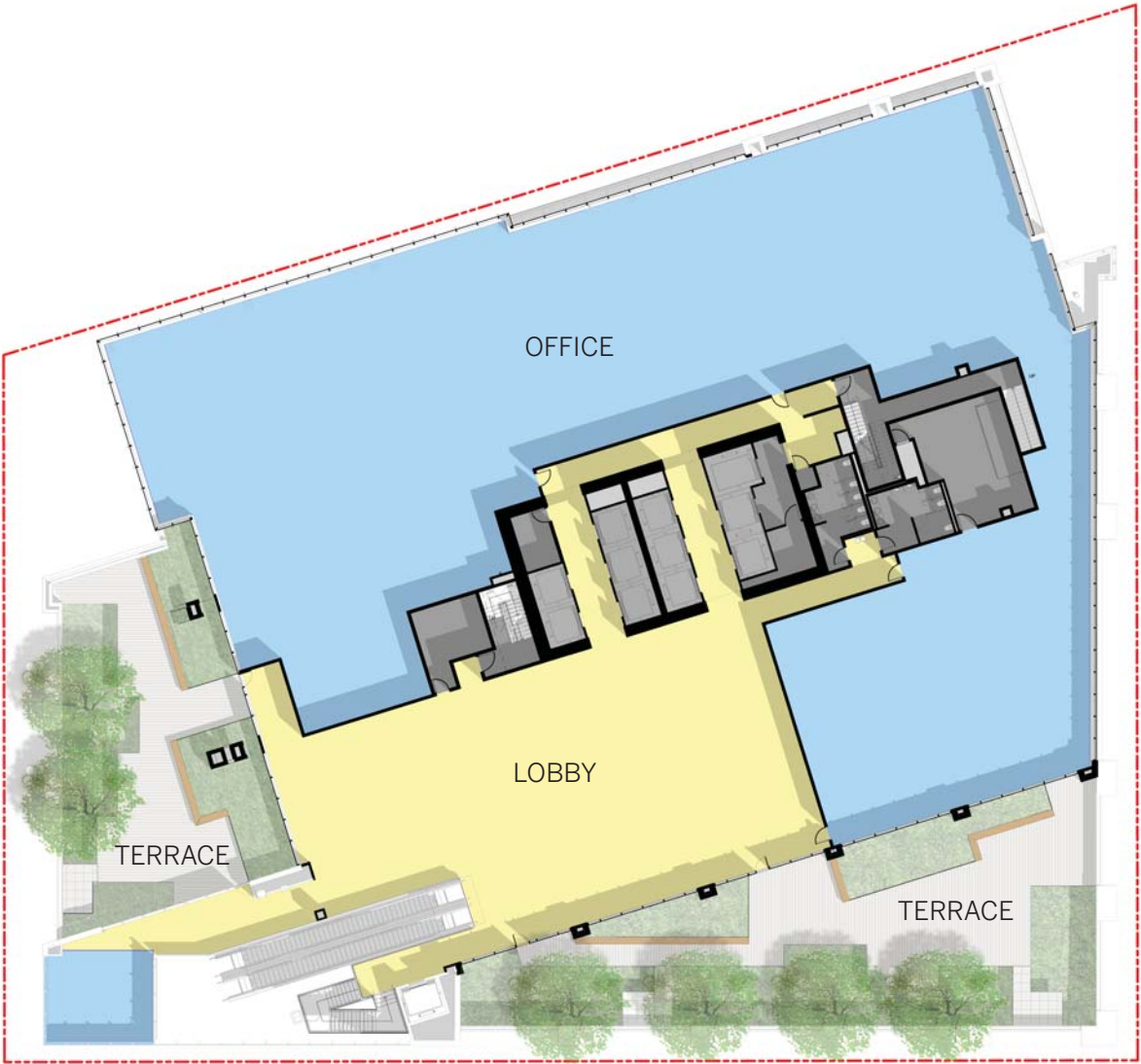
6 APPENDIX

FLOOR PLANS

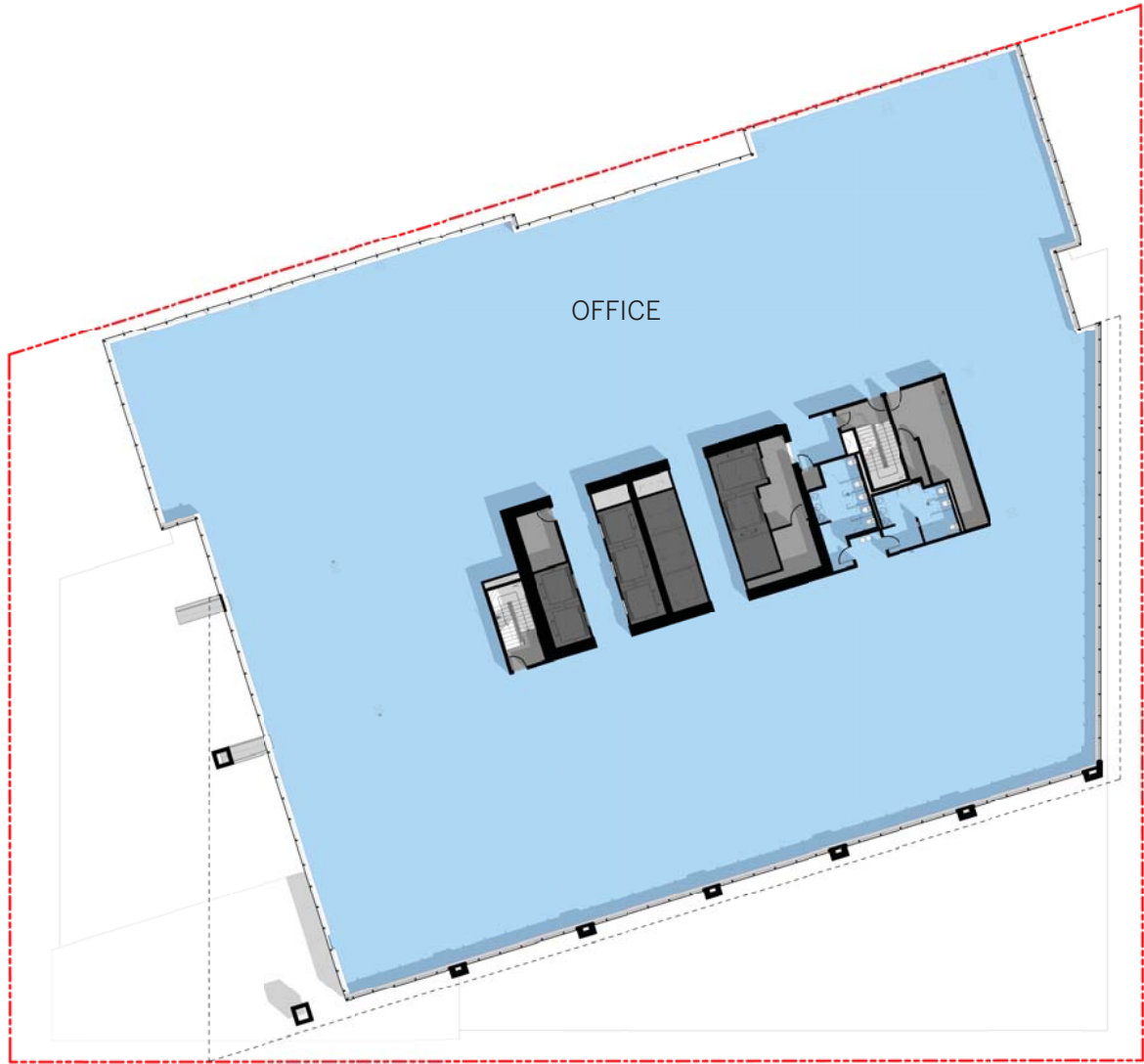
SITE C - OFFICE



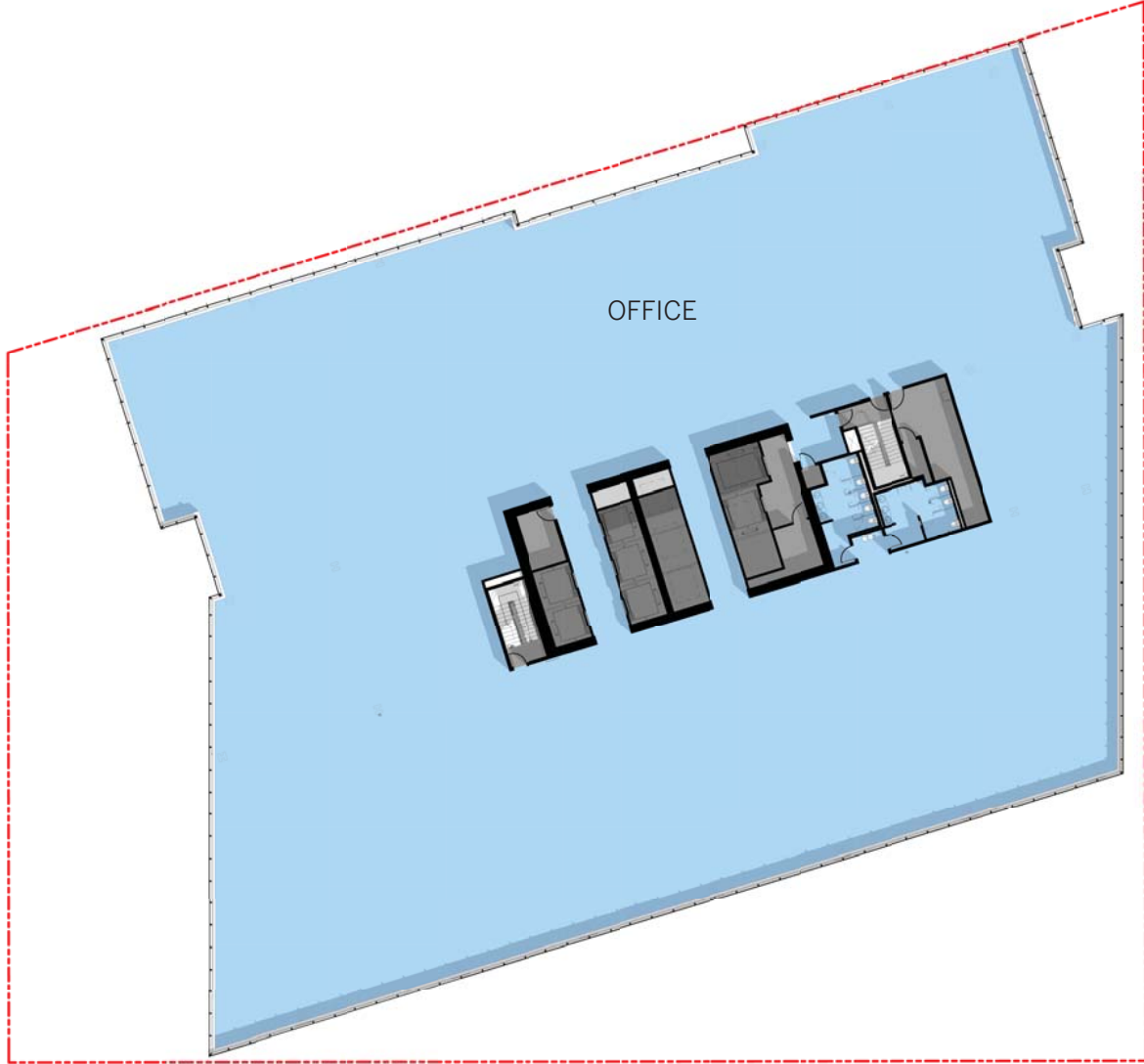
LEVEL 1



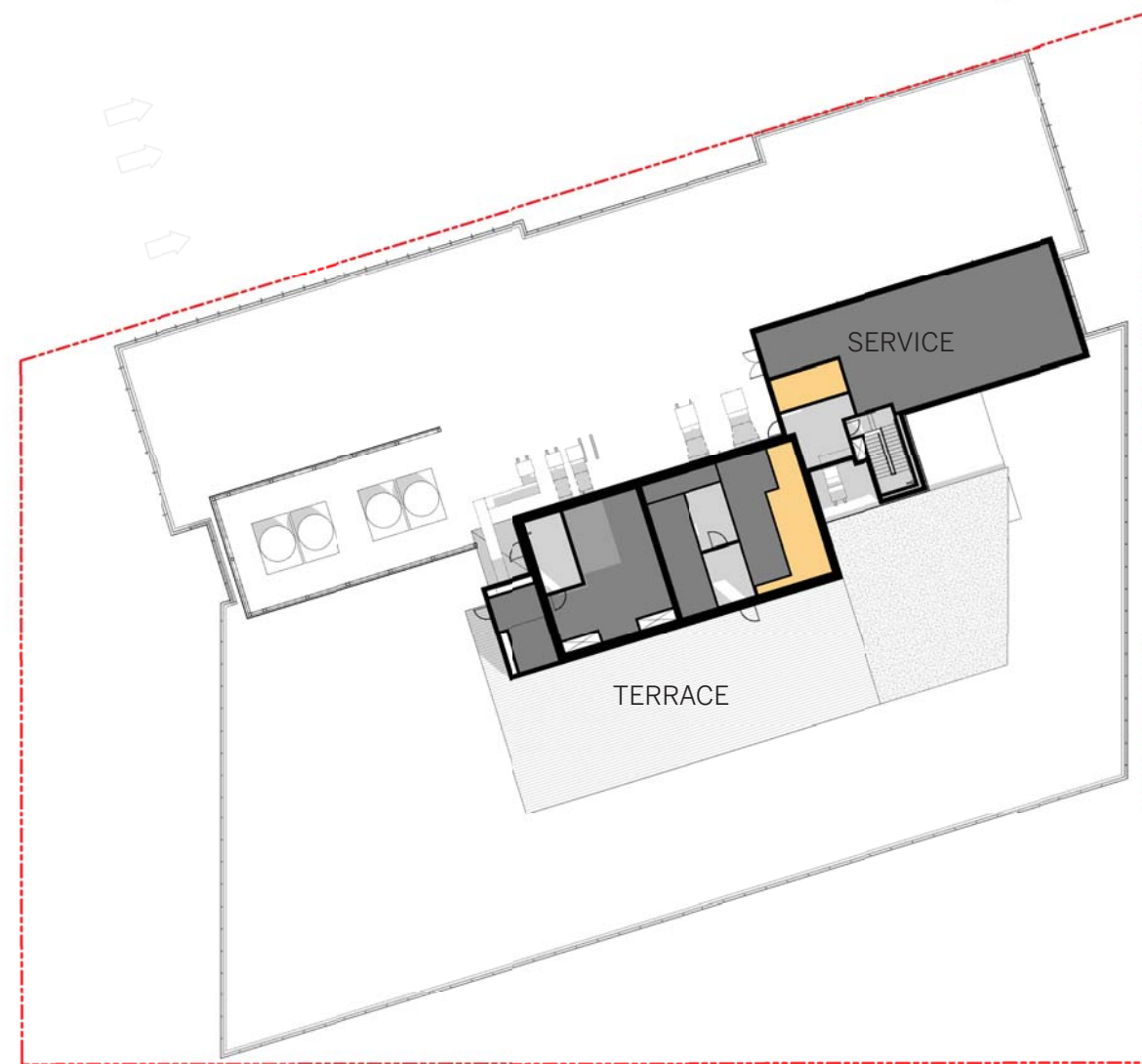
LEVEL 2



LEVEL 3



LEVEL 4-16



LEVEL 17 ROOF PLAN

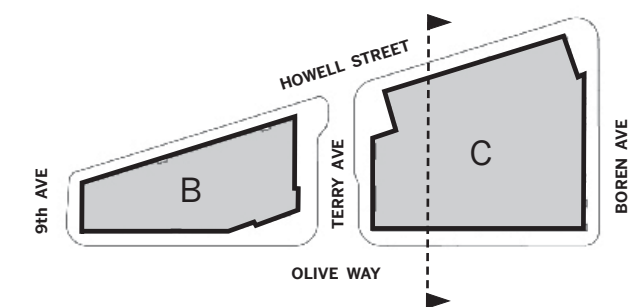
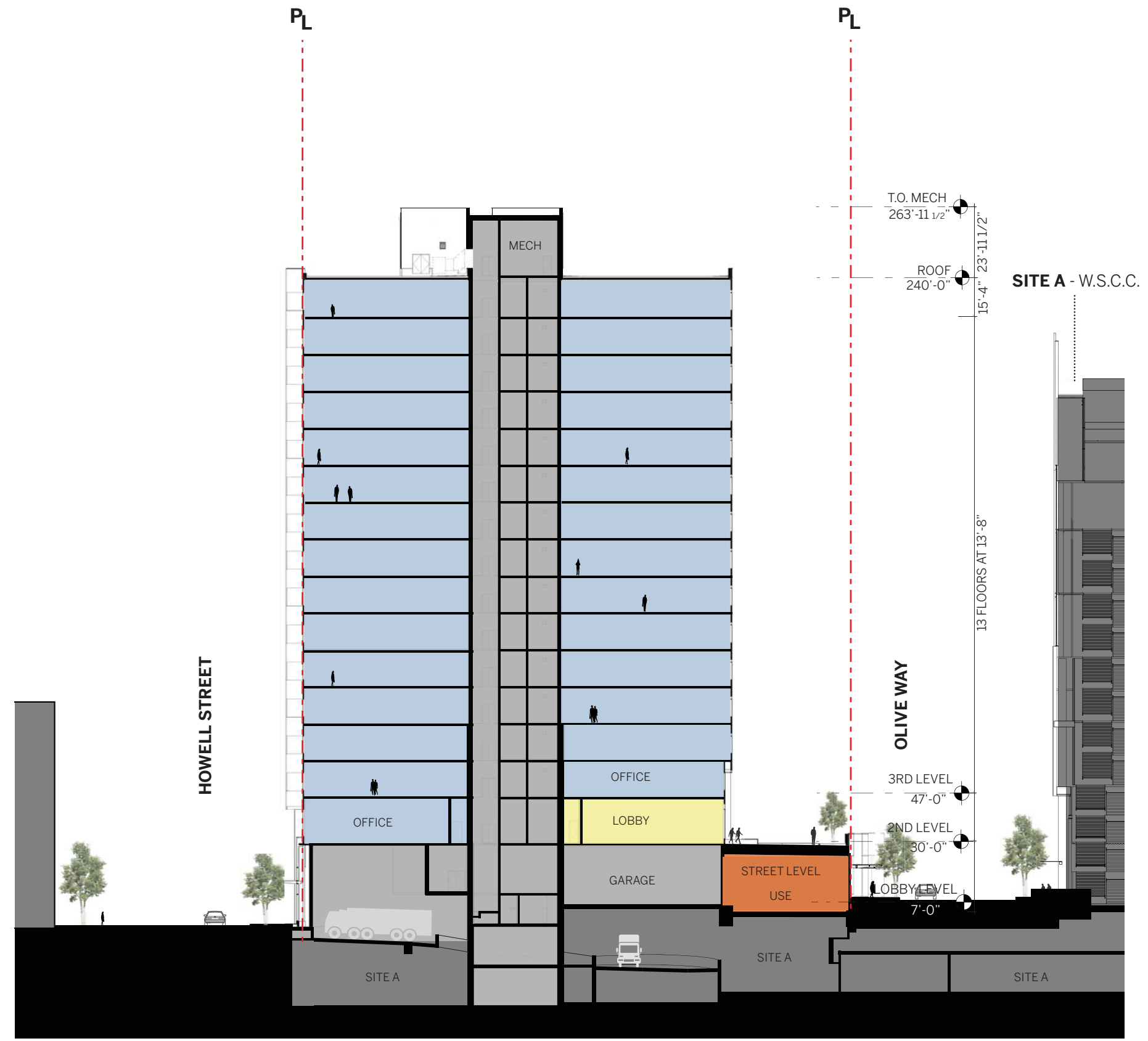


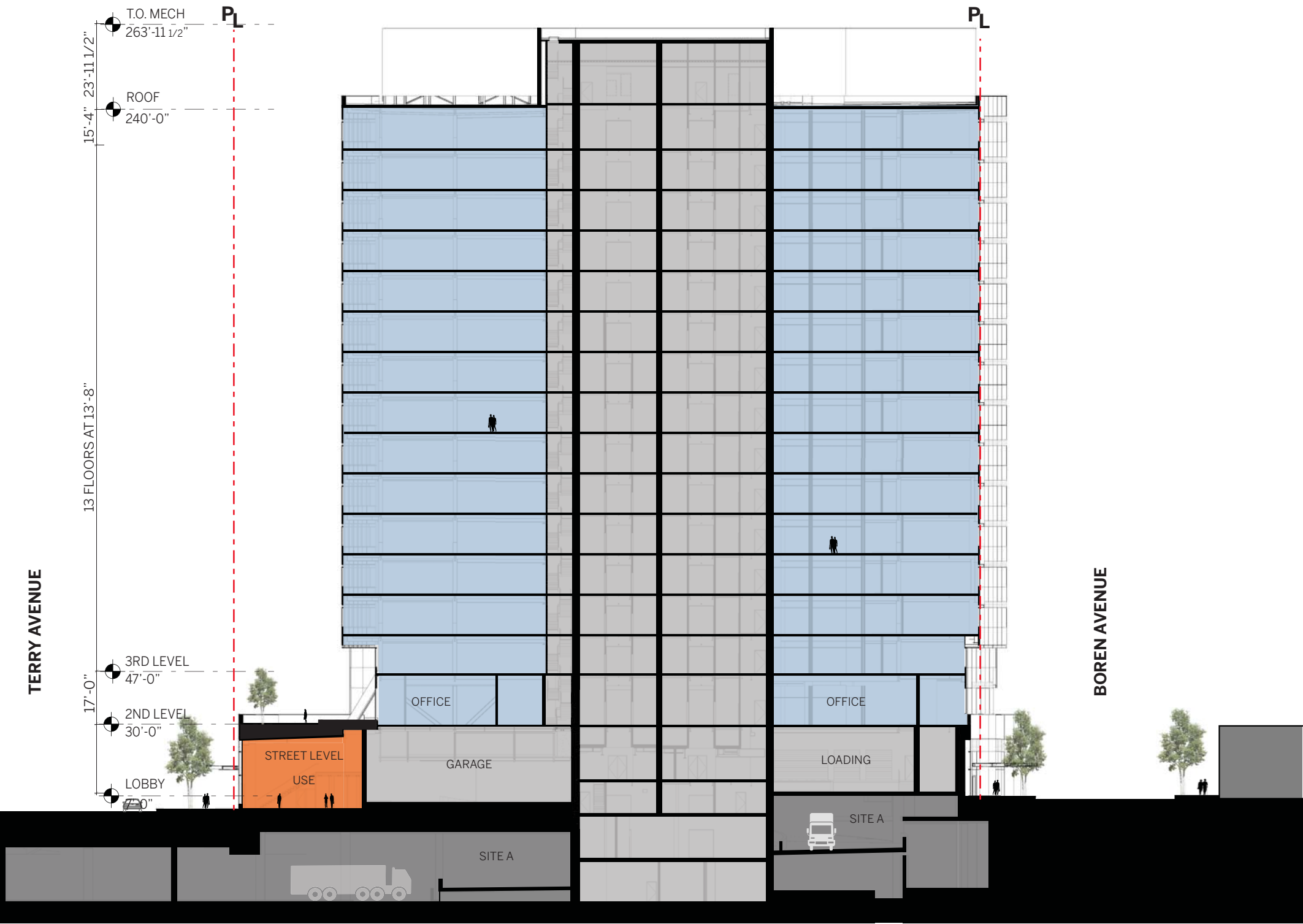
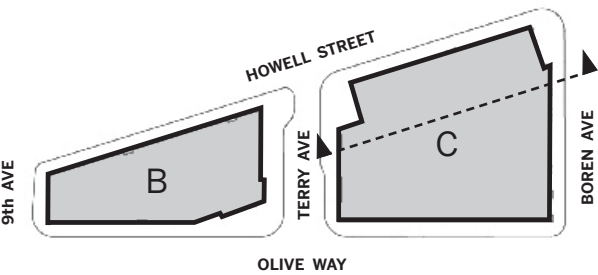
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6 APPENDIX

SECTIONS

SITE C - OFFICE





6 APPENDIX

ELEVATIONS

SITE C - OFFICE



A: Clear Vision
(GL - 1)



B: Clear Spandrel
(GL - 2)



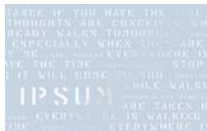
C: Reflective Vision
(GL - 3)



D: Reflective Spandrel
(GL - 4)



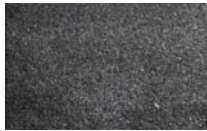
E: Custom Metal Design
(PERF-MTL SCREEN)



F: Fritted Glass
(GL - 5)



G: Light Stone
(STN - 1)



H: Dark Stone
(STN - 2)



I: Metal Panel
(CMPST MTL P-2 LIGHT)



J: Metal Panel
(CMPST MTL P-1 DARK)



K: Louver Dark
(LVR-1)



L: Louver Light
(LVR-2)



M: Box Ribbed Metal Panel
(MTLP-1)



N: Metal Screen
(GRTG-1)

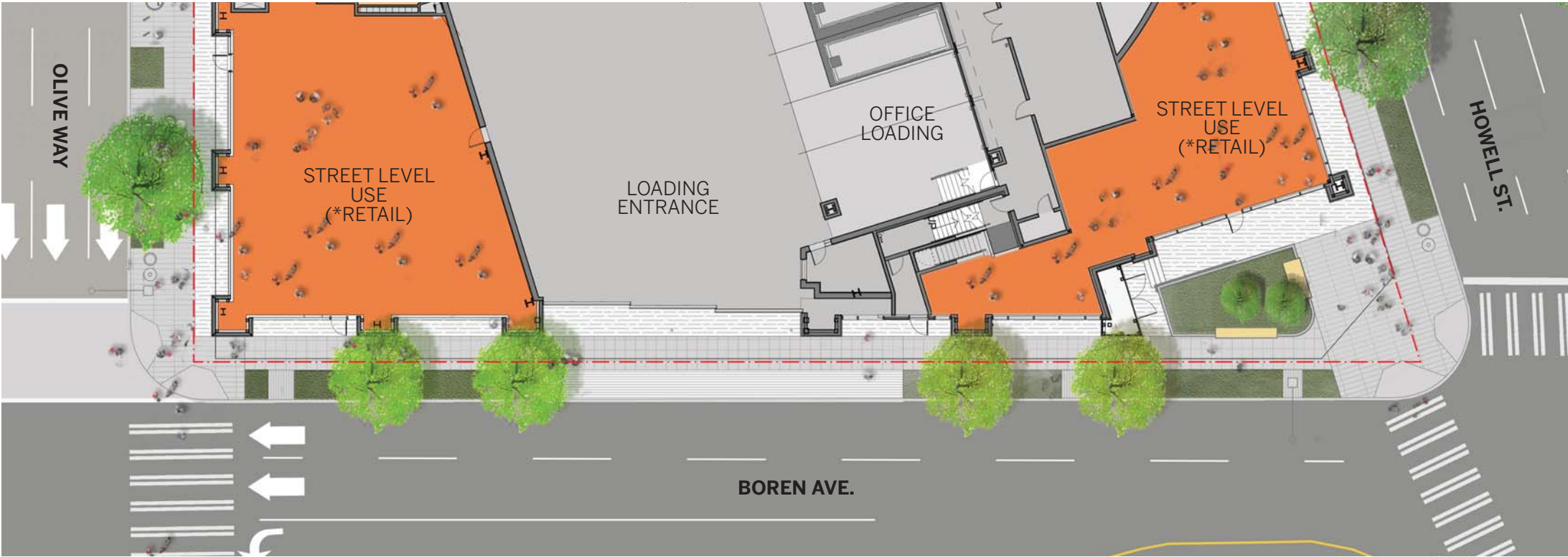


ELEVATION AT BOREN AVE.

6 APPENDIX

ELEVATIONS

SITE C - OFFICE



*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2021.

6 APPENDIX

ELEVATIONS

SITE C - OFFICE



A: Clear Vision
(GL - 1)



B: Clear Spandrel
(GL - 2)



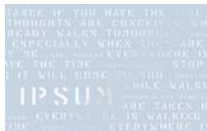
C: Reflective Vision
(GL - 3)



D: Reflective Spandrel
(GL - 4)



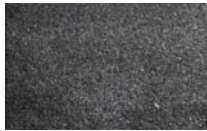
E: Custom Metal Design
(PERF-MTL SCREEN)



F: Fritted Glass
(GL - 5)



G: Light Stone
(STN - 1)



H: Dark Stone
(STN - 2)



I: Metal Panel
(CMPST MTLP-2 LIGHT)



J: Metal Panel
(CMPST MTLP-1 DARK)



K: Louver Dark
(LVR-1)



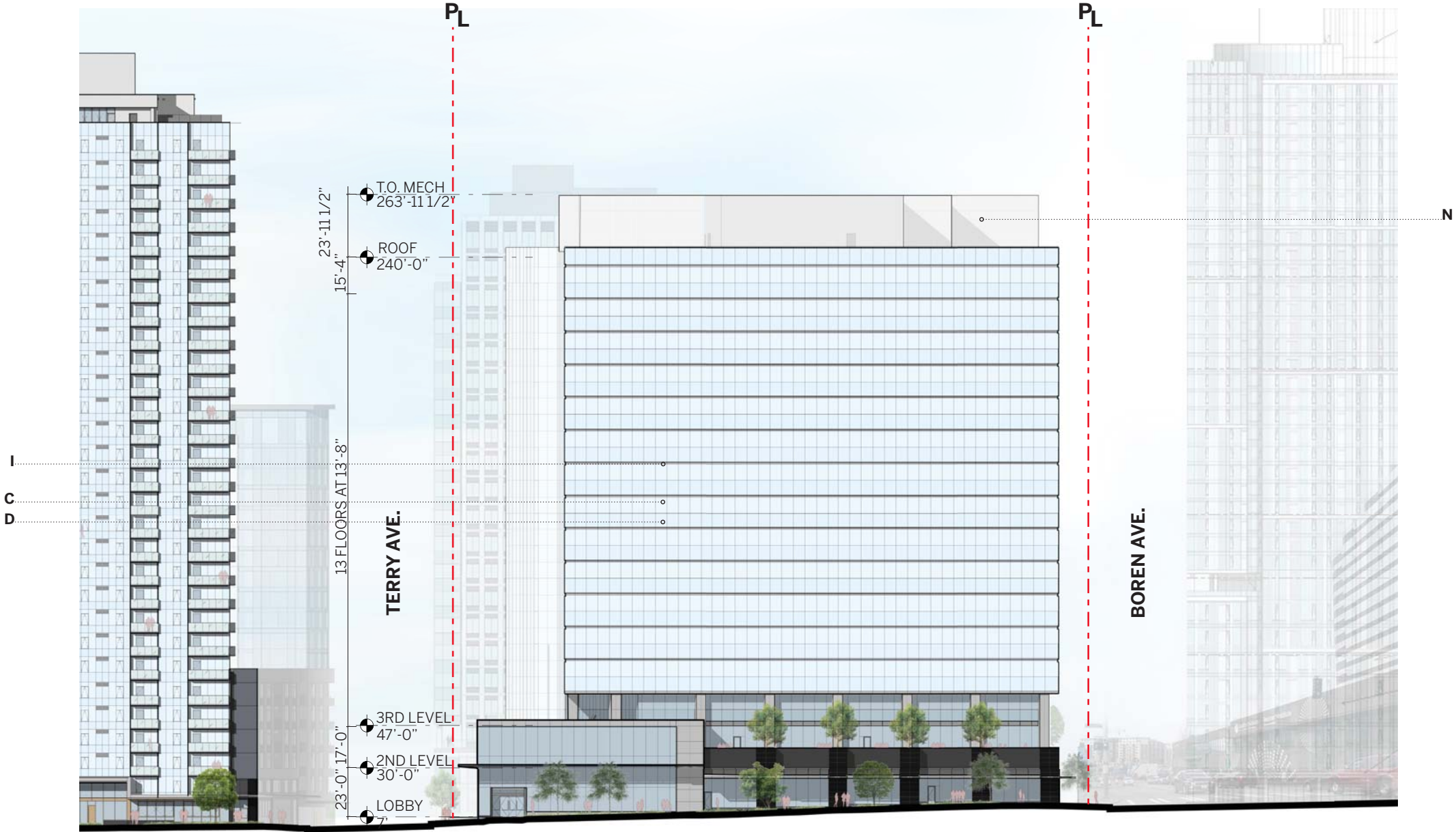
L: Louver Light
(LVR-2)



M: Box Ribbed Metal Panel
(MTLP-1)



N: Metal Screen
(GRTG-1)



ELEVATION AT OLIVE WAY

6 APPENDIX

ELEVATIONS

SITE C - OFFICE



*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2021.

6 APPENDIX

ELEVATIONS

SITE C - OFFICE



A: Clear Vision
(GL - 1)



B: Clear Spandrel
(GL - 2)



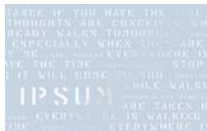
C: Reflective Vision
(GL - 3)



D: Reflective Spandrel
(GL - 4)



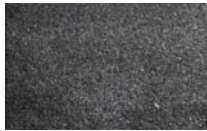
E: Custom Metal Design
(PERF-MTL SCREEN)



F: Fritted Glass
(GL - 5)



G: Light Stone
(STN - 1)



H: Dark Stone
(STN - 2)



I: Metal Panel
(CMPST MTL-2 LIGHT)



J: Metal Panel
(CMPST MTL-1 DARK)



K: Louver Dark
(LVR-1)



L: Louver Light
(LVR-2)



M: Box Ribbed Metal Panel
(MTLP-1)

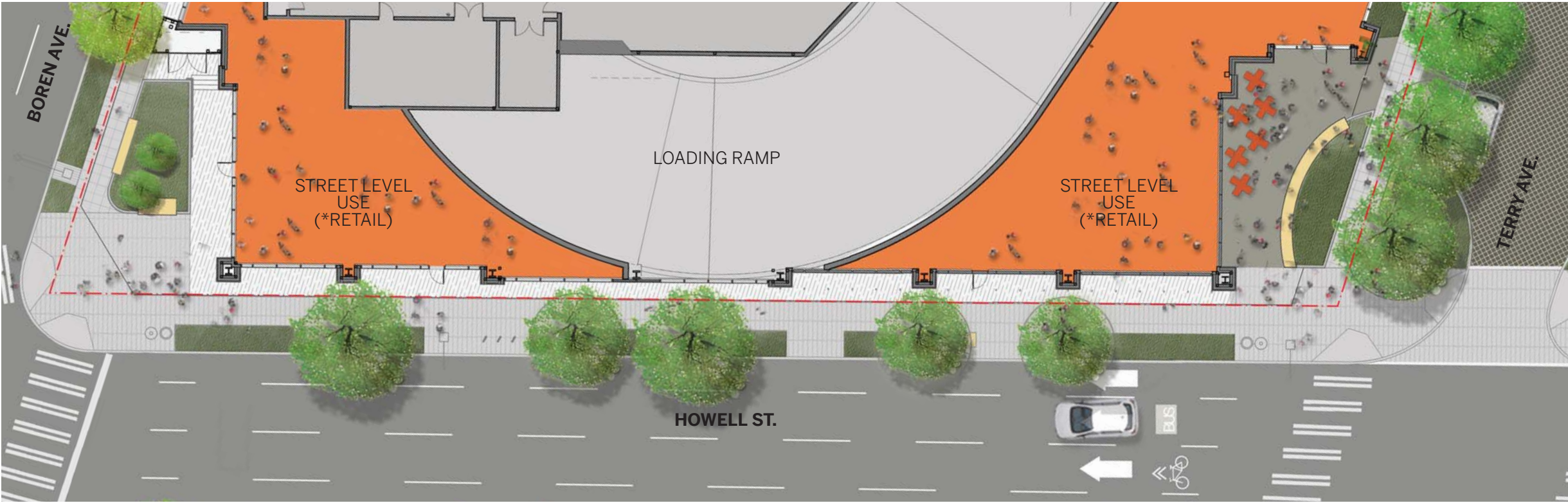


N: Metal Screen
(GRTG-1)



ELEVATION AT HOWELL ST.

6 APPENDIX ELEVATIONS SITE C - OFFICE



*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2021.

6 APPENDIX

ELEVATIONS

SITE C - OFFICE



A: Clear Vision
(GL - 1)



B: Clear Spandrel
(GL - 2)



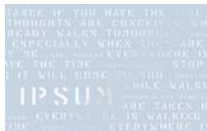
C: Reflective Vision
(GL - 3)



D: Reflective Spandrel
(GL - 4)



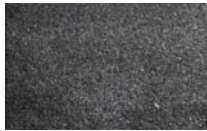
E: Custom Metal Design
(PERF-MTL SCREEN)



F: Fritted Glass
(GL - 5)



G: Light Stone
(STN - 1)



H: Dark Stone
(STN - 2)



I: Metal Panel
(CMPST MTL-2 LIGHT)



J: Metal Panel
(CMPST MTL-1 DARK)



K: Louver Dark
(LVR-1)



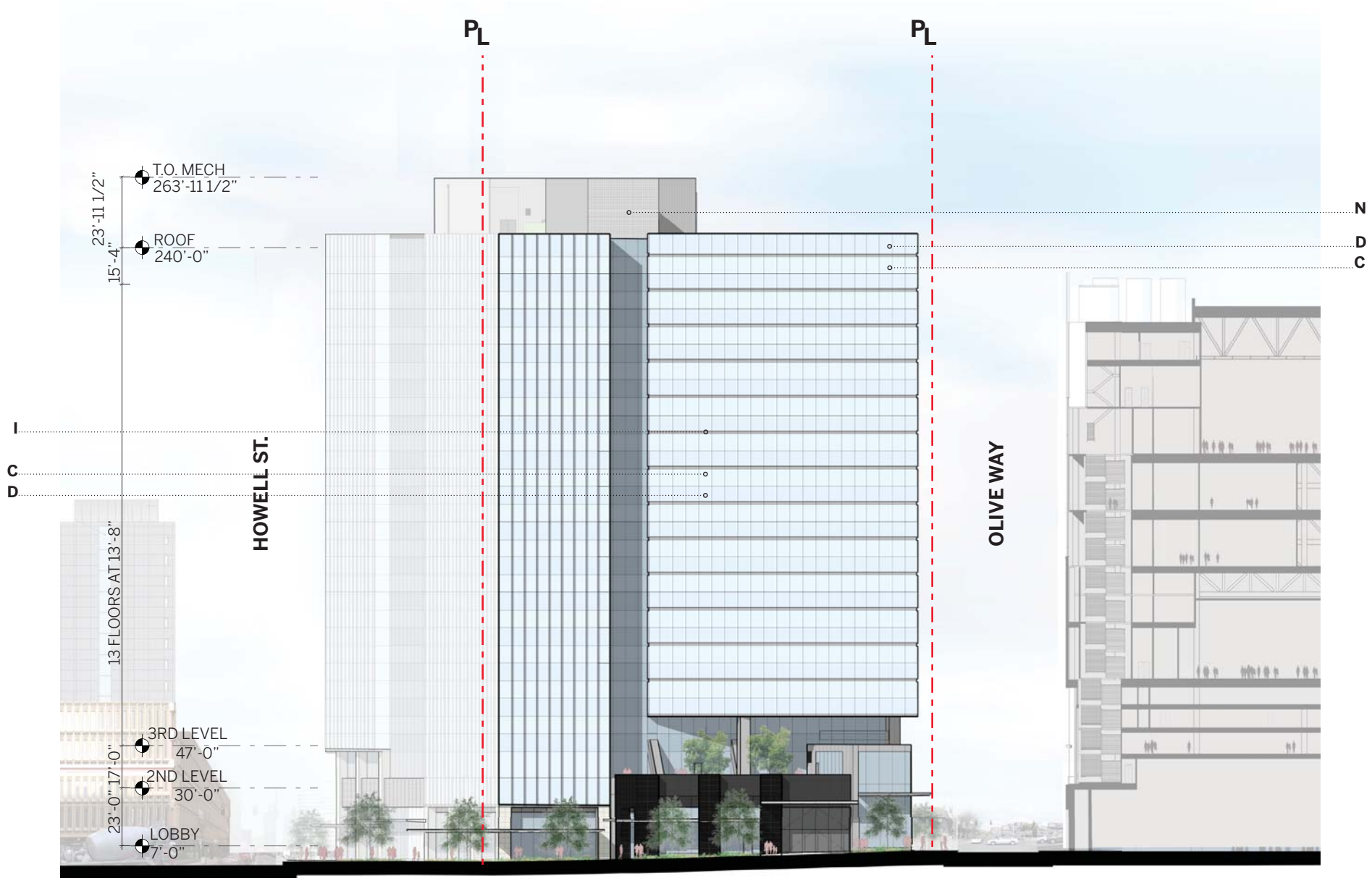
L: Louver Light
(LVR-2)



M: Box Ribbed Metal Panel
(MTLP-1)



N: Metal Screen
(GRTG-1)

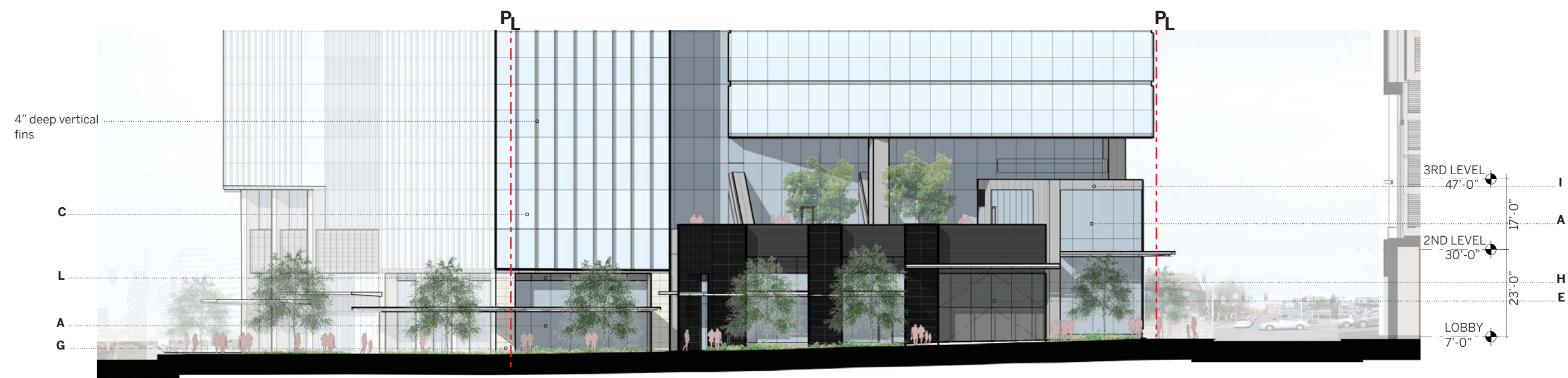


ELEVATION AT TERRY AVE.

6 APPENDIX

ELEVATIONS

SITE C - OFFICE



*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2021.

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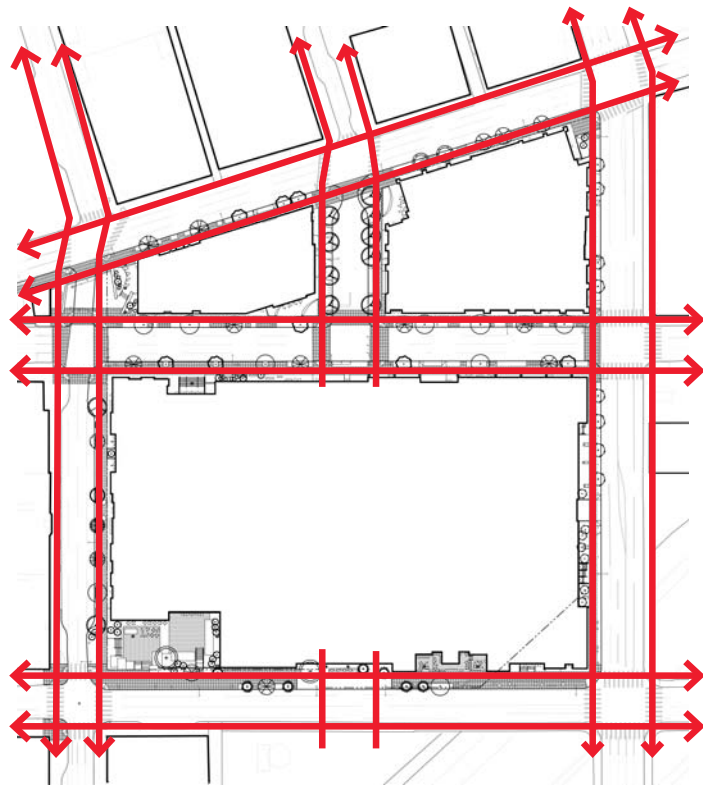
6 APPENDIX LANDSCAPE

INTRODUCTION

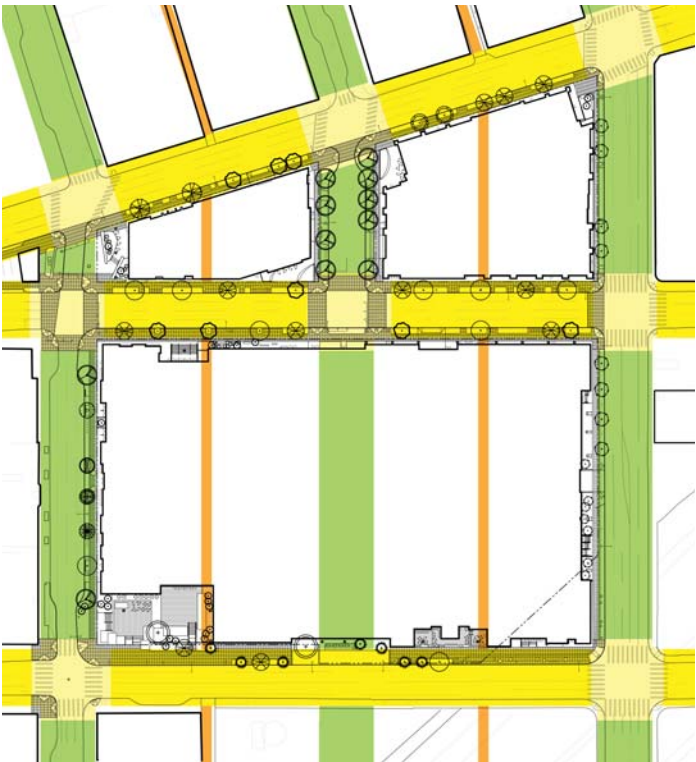
Critical to the success of this urban convention center is the ability to balance the scale and extents of the building required by its program with the grain and texture of the surrounding city. Rather than appearing as a super-block project, the streets and building will relate to the grain and texture of their surrounding neighborhoods. The building edges will create a continuously active experience on all sides for visitors and residents alike.

The site design strengthens the connectivity and continuity of the city streets. A distinct approach to avenues and streets offers a shift in character that builds on city standards to offer an additional layer of orientation rooted in the city’s basic relationship between hills and Elliot Bay.

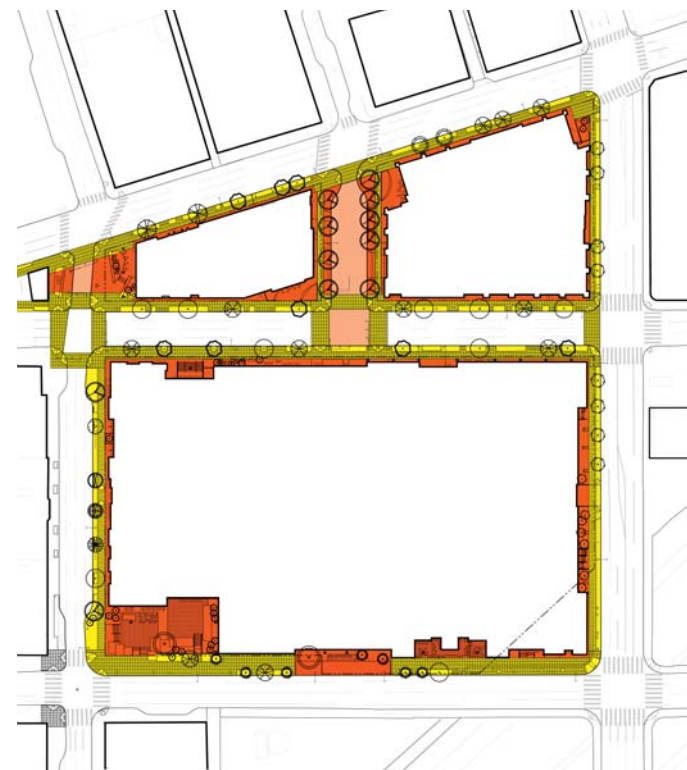
The project reinforces the continuity of the city streets by clearly articulating the edge between the street and WSCC Addition. This allows for a wider variety of conditions within the WSCC Addition footprint and supports multiple identities around the multi-block project, while extending the larger continuity of city streets. An additional layer of site elements offers pedestrian amenities that bridge the public programs of the WSCC Addition and the experience of convention visitors.



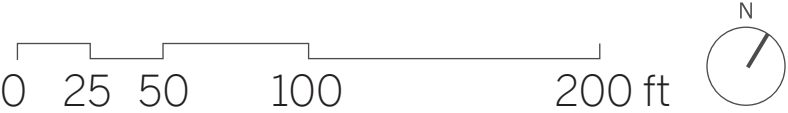
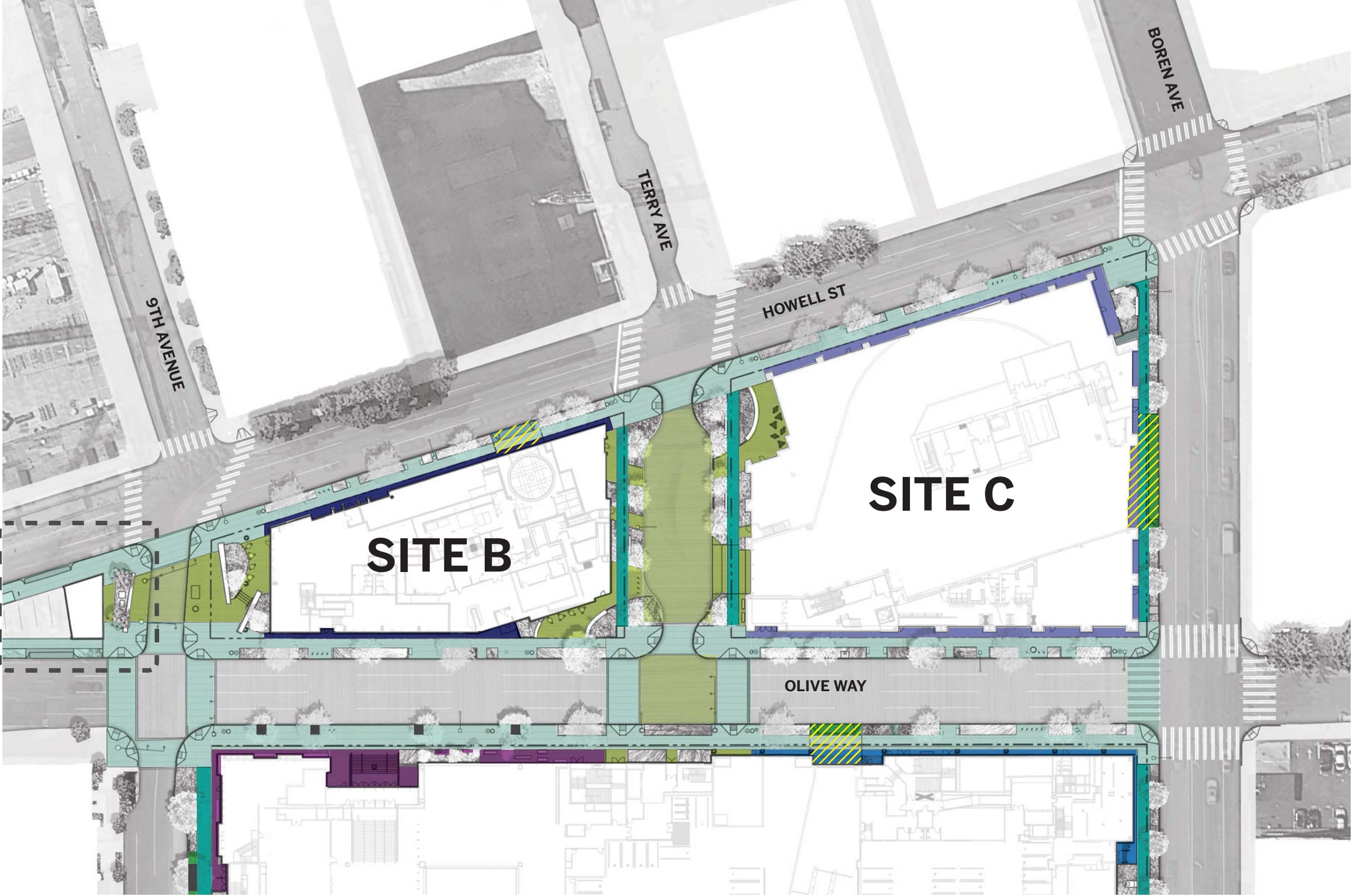
URBAN CONNECTIVITY AND CONTINUITY



ORIENTING STREETS AND AVENUES


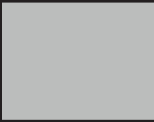









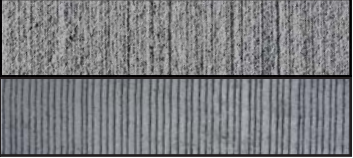

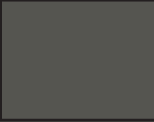



6 APPENDIX LANDSCAPE

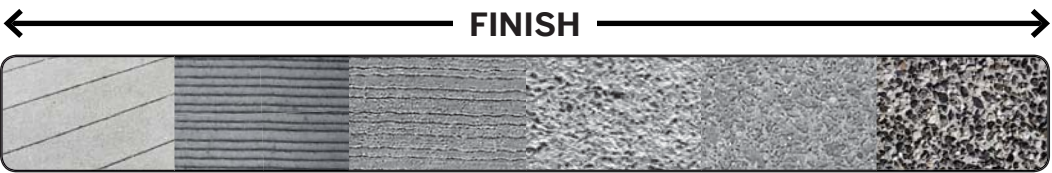


6 APPENDIX LANDSCAPE


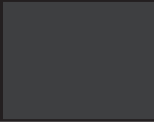




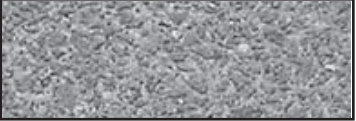
Right of Way Paving Materials

ZONE	MATERIAL	COLOR	FINISH
	HILL STREETS Color: City Standard “light gray” Finish: Light broom		
	AVENUES Color: City Standard “light gray” Finish: Medium broom		
	LANDSCAPE ZONE Color: Integral Color - Charcoal Finish: Heavy trowel at 6” o.c.		
	GREEN STREETS Color: Integral Color - Charcoal Sidewalk Finish: Heavy broom Roadway Finish: Raked		
	TREE PITS Material: Stabilized gravel Color: Gray and Black mixed		

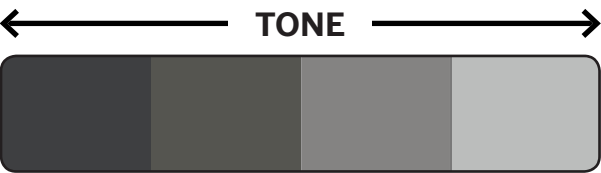
Concrete Paving Gradients



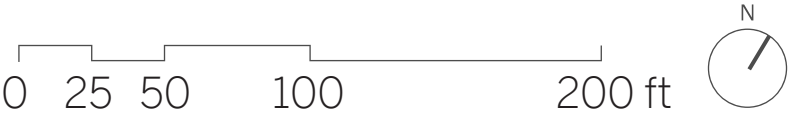
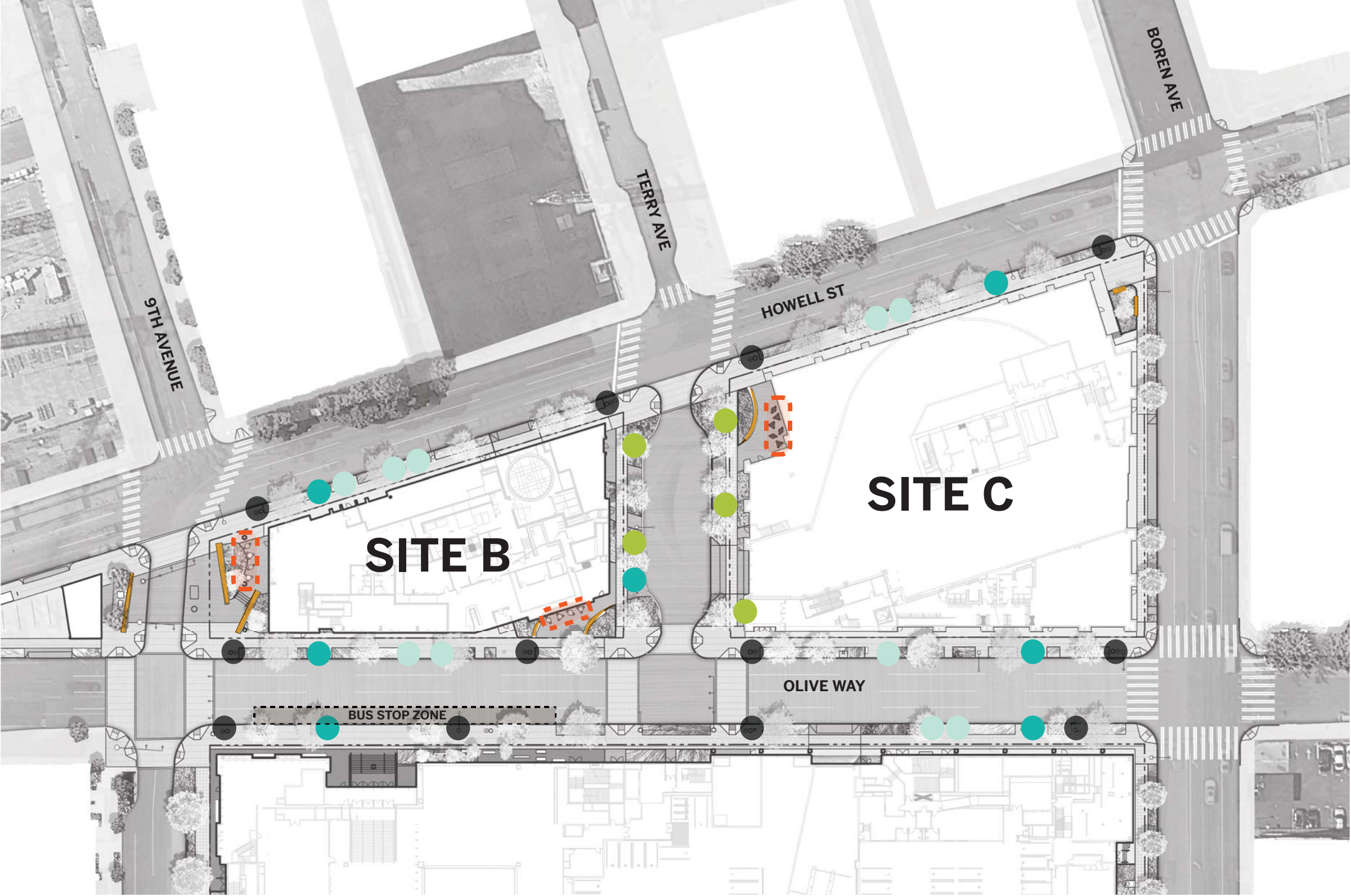
Property Line Paving Materials

ZONE	MATERIAL	COLOR	FINISH
	WSCC WEST Material: Basalt Finish: Flamed, Dark Gray		
	WSCC EAST Color: City Standard “light gray” Finish: Water Wash		
	OFFICE TOWER Color: City Standard “light gray” Finish: Light sand blast		
	RESIDENTIAL TOWER Color: City Standard “light gray” Finish: Exposed aggregate		

Additional Distinctions



6 APPENDIX LANDSCAPE



6 APPENDIX

LANDSCAPE

Primary Site B&C Street Furnishings



Planting Protection Fence
(Adjoining all planting areas)



Wood Green Street bench
Woody Bench
Modern Street and Site
Furnishings Company



Wood ROW bench
Spring Bench
Duluth Timber Company
(various salvaged wood species)



Bicycle Rack
Westport, No-Scratch
Sportworks



Trash/Recycling Receptacle
RR-55 series
Urban Accessories



Moveable Tables and Chairs
Various models and finish colors
Fermob

Custom Wood Street Furnishings



Western juniper lumber



Douglas fir



Custom metal bench with wood seat,
located at the Flatiron Plaza
(9th Ave / Olive Way / Howell St)

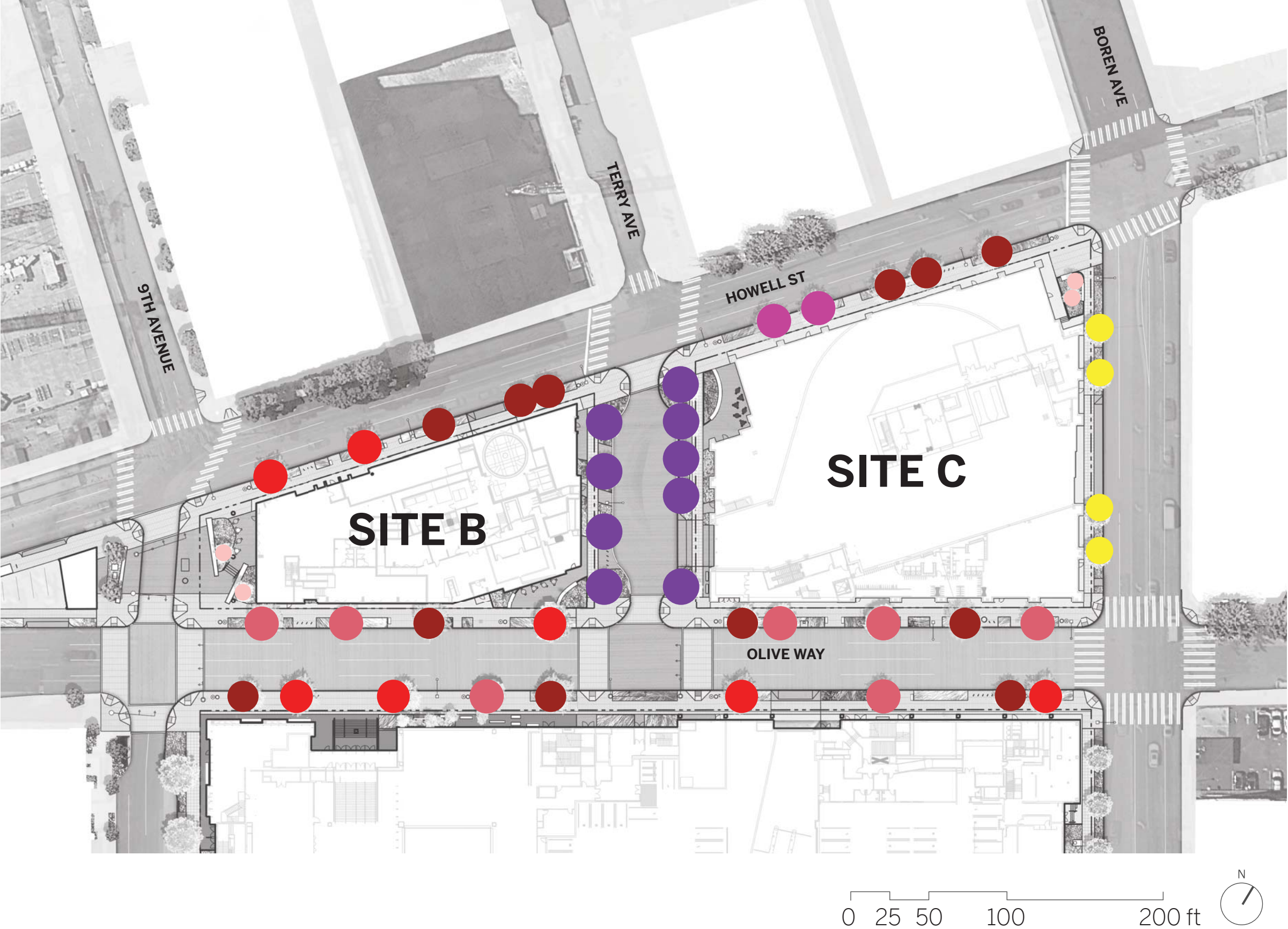


Custom curved wood bench integrated into
concrete planters at the Terry Ave Green
Street plazas



Custom wood benches, integrated into
a concrete planter at the Denny Triangle
Corner Plaza (Boren Ave & Howell St)

6 APPENDIX LANDSCAPE



6 APPENDIX

LANDSCAPE

Site A Street Trees



Acer circinatum
Vine maple



Acer saccharum 'Bonfire'
Sugar maple 'Bonfire'



Liquidambar styraciflua
American sweetgum



Liriodendron tulipifera
'Emerald City'
Emerald City tulip tree



Platanus x acerifolia
'Columbia'
Columbia plane tree

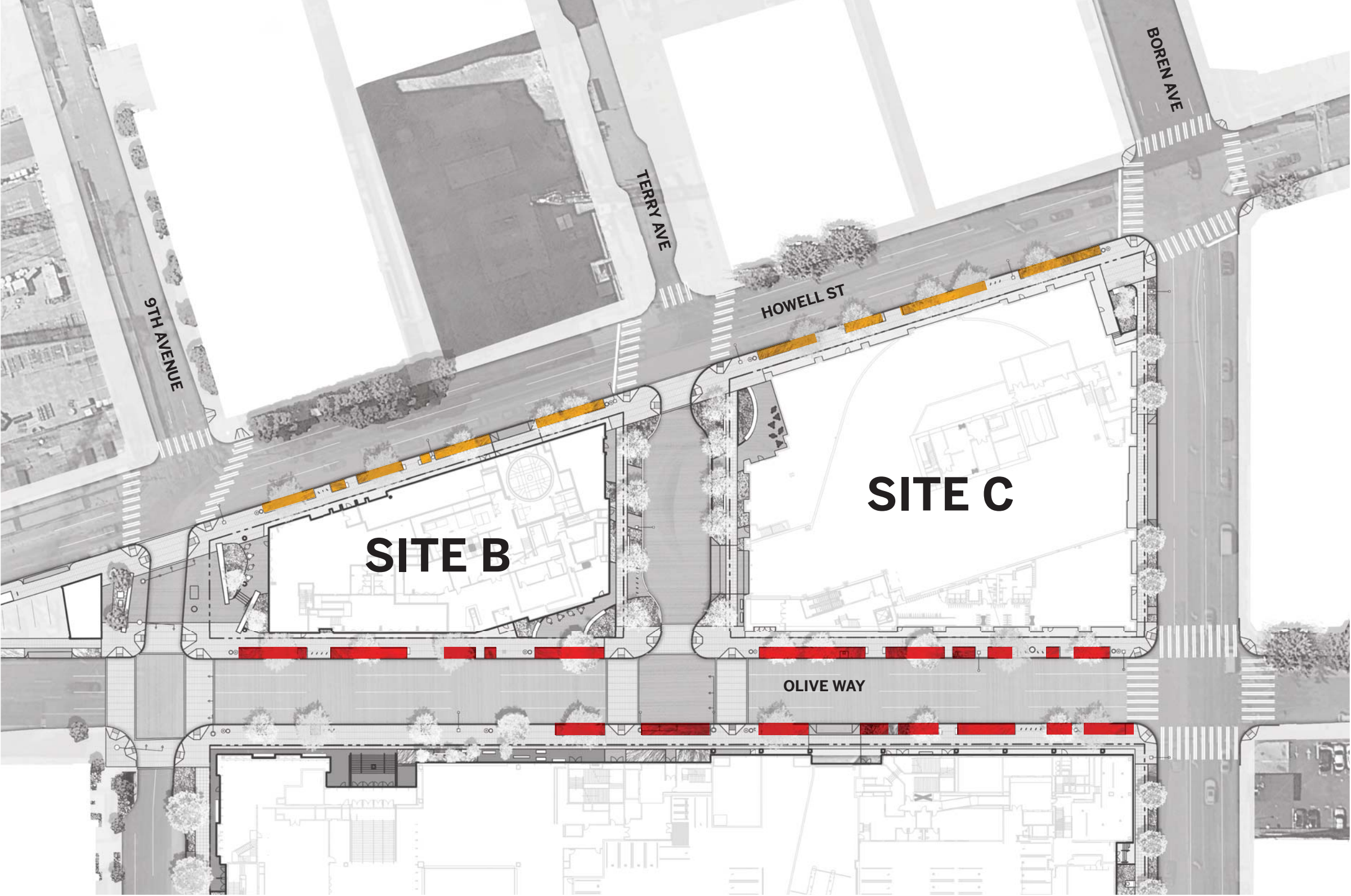


Ulmus parvifolia 'Emer II'
Allee elm



Zelkova serrata 'Green Vase'
Japanese zelkova

6 APPENDIX LANDSCAPE



6 APPENDIX

LANDSCAPE

Olive Way Streetscape Planting



Arctostaphylos uva-ursi
'Massachusetts'
Massachusetts kinnikinnick



Camassia leictlinii
Californian white camass



Cornus sericea 'Arctic Fire'
Arctic Fire red twig dogwood



Deschampsia cespitosa
'Goldtau'
Goldtau Tufted Hair Grass



Gaultheria shallon
Salal



Sedum album
White stonecrop



Vaccinium ovatum
'Thunderbird'
Thunderbird Evergreen Huckleberry

Howell St Streetscape Planting



Arctostaphylos uva-ursi
'Massachusetts'
Massachusetts kinnikinnick



Ceanothus 'Wheeler Canyon'
Wheeler Canyon California lilac



Ceanothus griseus 'Kurt Zadnik'
Kurt Zadnik California lilac



Cornus sericea 'Kelsey'
Kelsey's dwarf redosier dogwood



Epimedium grandiflorum
'Queen Esta'
Barrenwort



Lonicera nitida 'Maigrun'
Box honeysuckle

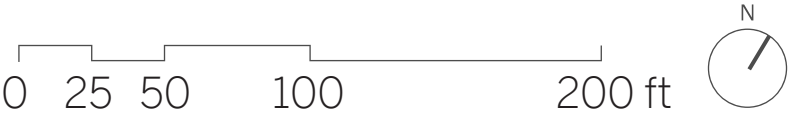
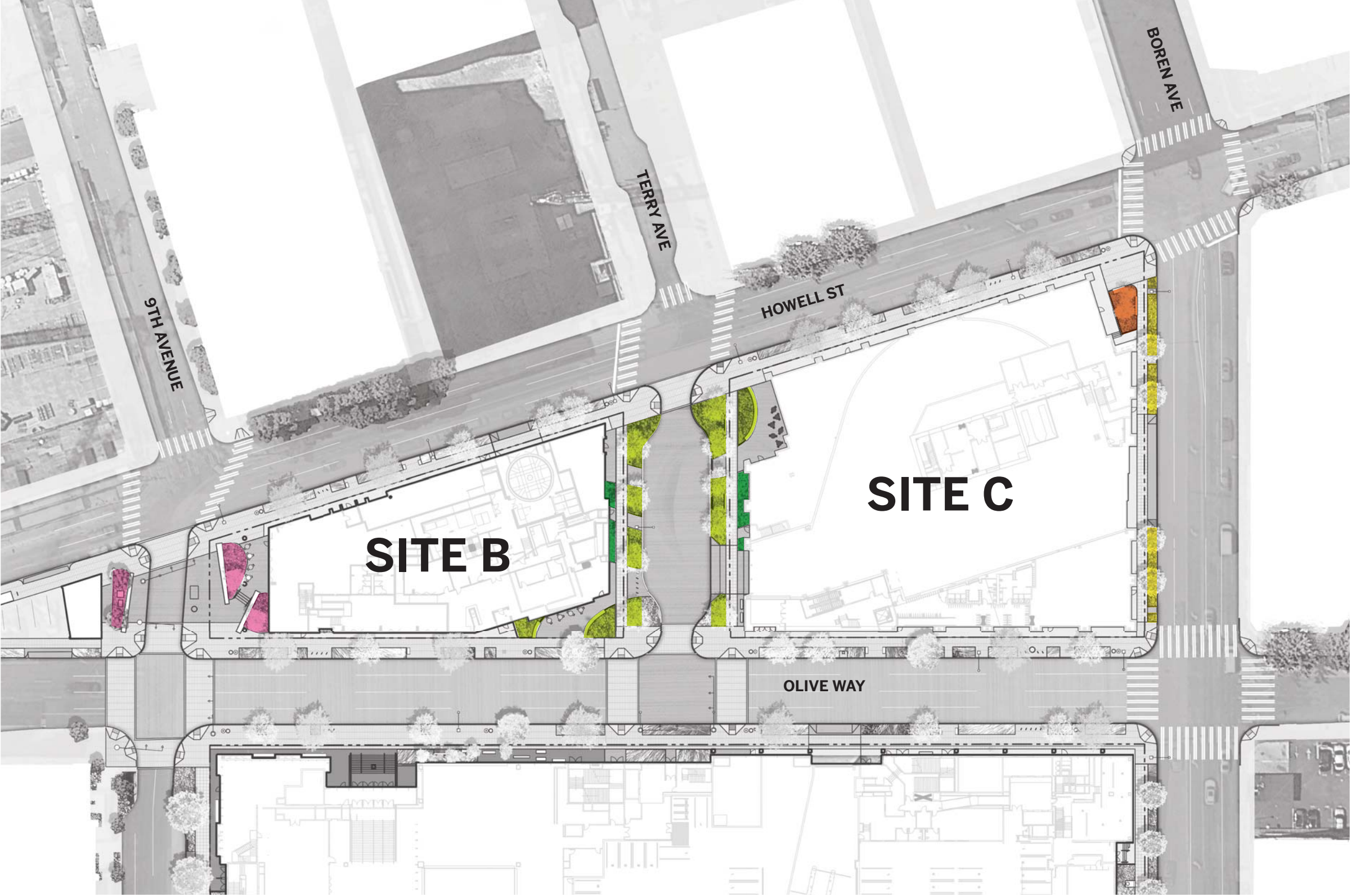


Mahonia repens
Creeping mahonia



Sesleria autumnalis
Autumn moor grass

6 APPENDIX LANDSCAPE



6 APPENDIX

LANDSCAPE

Terry Ave Green Street - Streetscape



Cornus sericea 'Arctic Fire'
Arctic Fire red twig dogwood



Fragaria Vesca
Wild strawberry



Gaultheria shallon
Salal



Philadelphus lewisii 'Blizzard'
Mock orange



Vaccinium angustifolium 'Burgundy'
Burgundy wild lowbush blueberry



Vaccinium ovatum 'Thunderbird'
Thunderbird evergreen huckleberry

Terry Ave Green Street - Facade



Adiantum pedatum
Northern maidenhair fern



Asarum caudatum
Wild ginger



Cornus unalaschensis
Western bunchberry



Mahonia nervosa
Dwarf Oregon grape



Polystichum munitum
Western sword fern



Ceanothus griseus var. horizontalis 'Yankee Point'
Yankee Point California lilac



Sedum album
White stonecrop

Boren Ave Streetscape

Boren Ave Corner Plaza



Arctostaphylos uva-ursi 'Massachusetts'
Massachusetts kinnikinnick



Mahonia aquifolium 'Compacta'
Compact Oregon grape

Flatiron Plaza (9th Ave)



Ilex crenata 'Helleri'
Japanese holly



Arctostaphylos uva-ursi 'Massachusetts'
Massachusetts kinnikinnick



Mahonia nervosa
Dwarf Oregon grape



Polystichum munitum
Western sword fern



Vaccinium ovatum 'Thunderbird'
Thunderbird Evergreen Huckleberry

6 APPENDIX LANDSCAPE

Site B - Residential Terraces



6 APPENDIX

LANDSCAPE

Residential Terrace Planting



Arctostaphylos uva-ursi
'Massachusetts'
Massachusetts kinnikinnick



Camassia quamash
Camas



Fragaria Vesca
Wild Strawberry



Iris tenax
Oregon iris



Gaultheria shallon
Salal



Gymnocarpium dryopteris
Oak fern



Lewisia columbiana
Columbian lewisia



Mahonia nervosa
Dwarf Oregon grape



Maianthemum dilatatum
False lily-of-the-valley



Philadelphus lewisii
'Blizzard'
Mock orange



Polystichum munitum
Western sword fern



Rhododendron 'Pink Walloper'
Pink Walloper rhododendron



Rosa x 'David Austin'
David Austin rose



Vaccinium ovatum
'Thunderbird'
**Thunderbird Evergreen
Huckleberry**



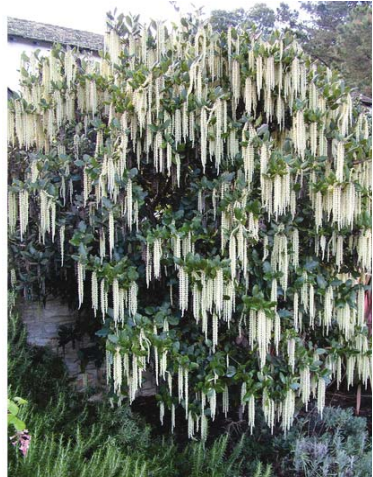
Acer circinatum
Vine maple



Amelanchier alnifolia
Serviceberry

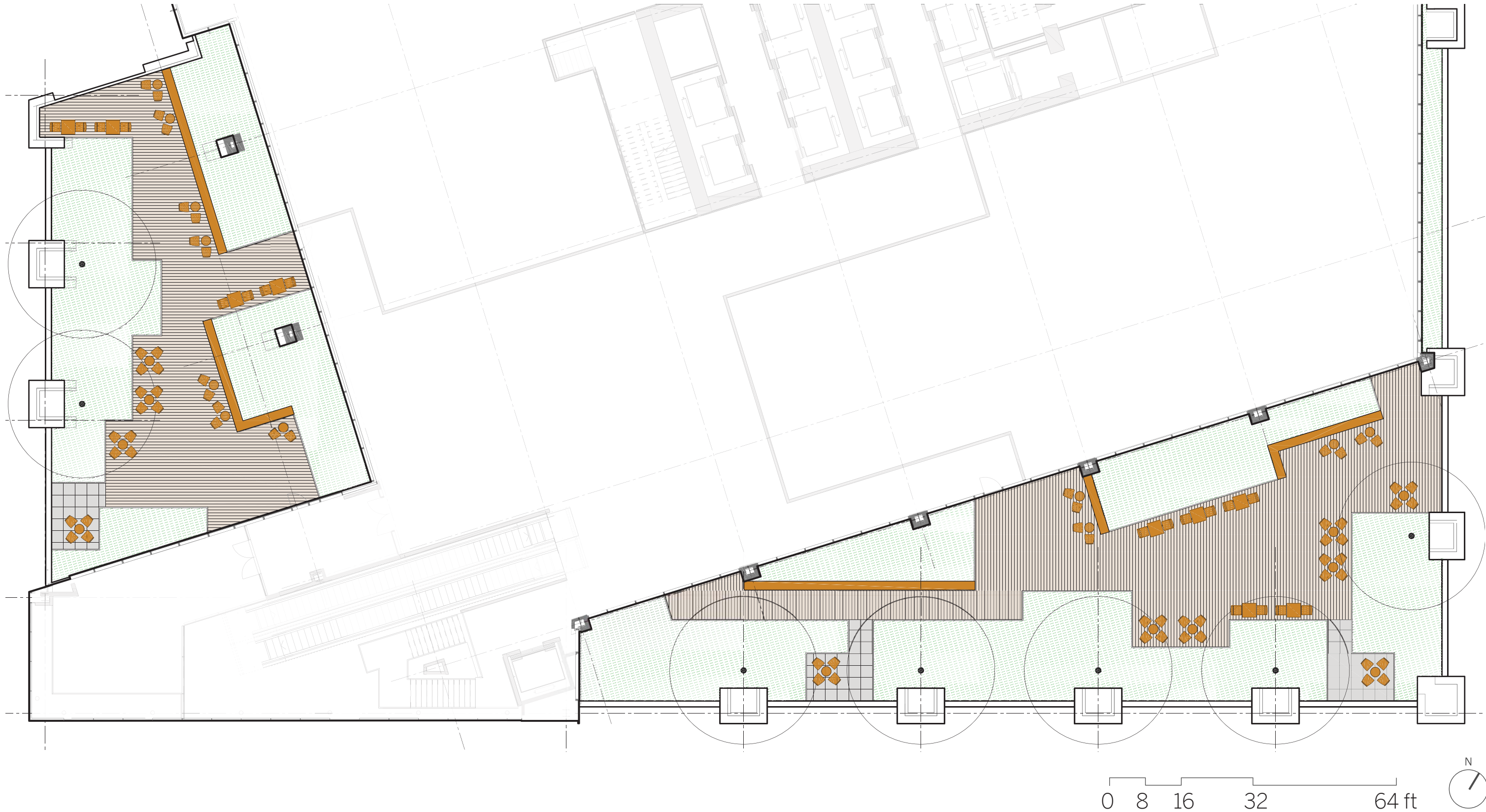


Cornus x 'Venus'
Venus hybrid dogwood



Garrya elliptica
Coast silktassel

Site C - Office Terraces



6 APPENDIX

LANDSCAPE

Office Terrace Planting



Gymnocarpium dryopteris
Oak fern



Mahonia nervosa
Dwarf Oregon grape



Maianthemum dilatatum
False lily-of-the-valley



Oxalis oregana
Redwood sorrel



Polystichum munitum
Western sword fern



Smilacina racemosa
Solomon's plume



Vaccinium ovatum
'Thunderbird'
**Thunderbird Evergreen
Huckleberry**



Zelkova serrata 'Green Vase'
Japanese zelkova

6 APPENDIX

LIGHTING DESIGN

SITE B - RESIDENTIAL



6 APPENDIX

LIGHTING DESIGN

SITE C - OFFICE



1

LED PEDESTRIAN POLE LIGHTING

High-output and low glare luminaire to provide uniform illumination along walkways.



2

LED PLANTER UPLIGHT

LED uplight to highlight landscape elements.



3

LED CYLINDER SCONCE - LARGE

High-output wall sconce at tall canopies to highlight the office entry.



4

LED CYLINDER SCONCE - SMALL

LED wall sconce to illuminate walking surfaces along the building perimeter.



5

LED HANDRAIL LIGHT

LEDs are integrated into handrails to illuminate exterior walking surfaces and stairs.



6

LED BENCH LIGHT

Indirect LED lighting provides a soft glow of illumination below landscape benches.



7

LED RECESSED STEP LIGHT

LED step lights are recessed into planter walls to provide low-level illumination along walking surfaces.



8

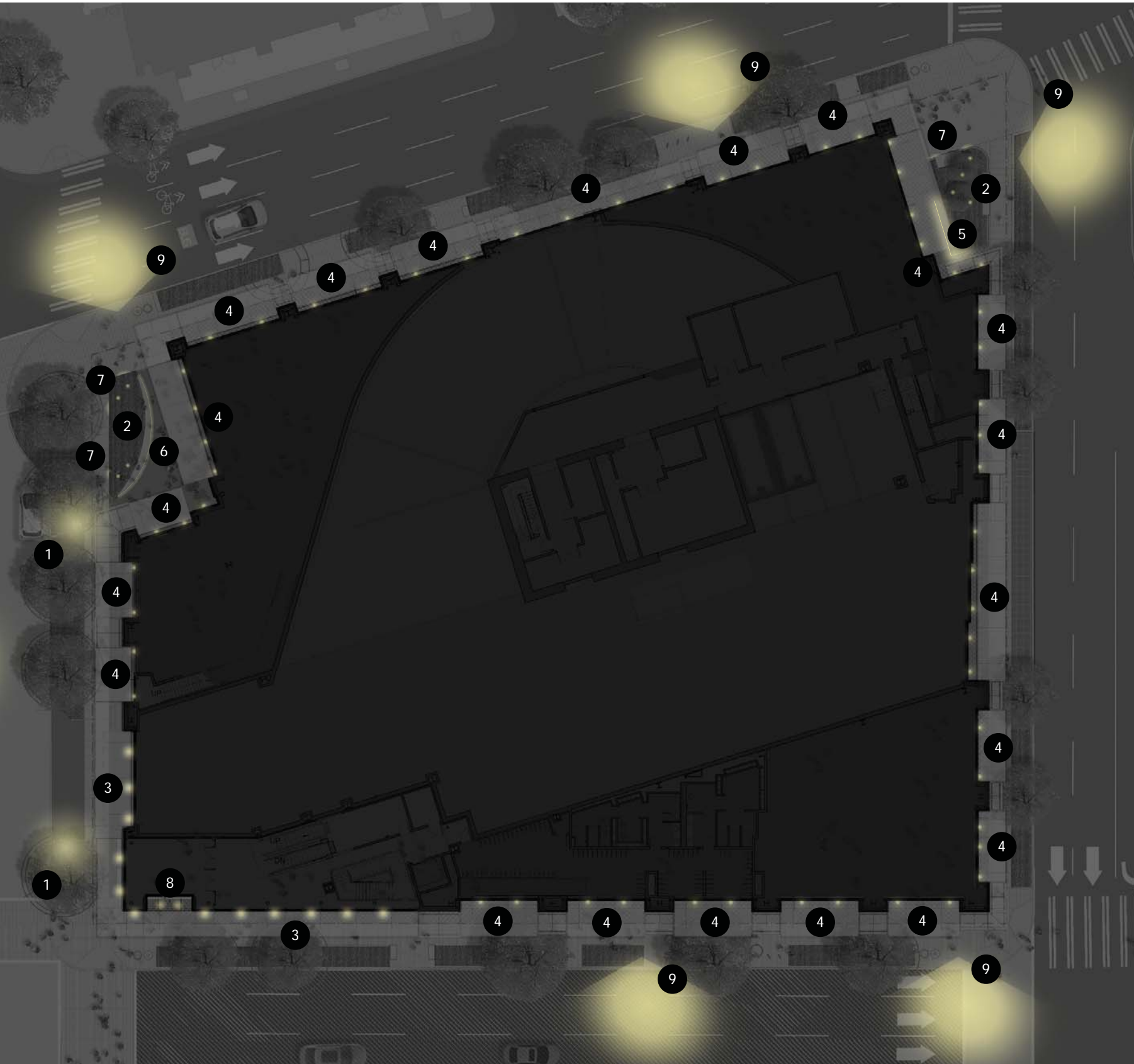
LED RECESSED DOWN LIGHT

6.0" aperture downlights are recessed into canopy, illuminating the office entry.

9

ROADWAY LUMINAIRE

High-output street luminaires; preexisting and future locations by others.





6 APPENDIX SIGNAGE SITE C - OFFICE



