



BELLWETHER UCC HOUSING

RECOMMENDATION MEETING

06.29.2015 | DPD #3016764 | I4-034



WEBER THOMPSON

CONTENTS

RESIDENTIAL OBJECTIVES

RESIDENTIAL USES	Approx. 133 affordable residential units; A mix of Studio, Open one, 1 bedroom, 2 bedroom, and 3 bedroom units
FAR	SITE AREA 30,240 SF x 5.75 (FAR) = 173,880 SF ALLOWABLE PROPOSED FAR 138,604 SF REMAINING FAR 35,276 SF
USE DISTRIBUTION BY FLOOR	LEVEL 1 Commercial NonProfit/Residential Lobby/Grade-related Units/Commercial Parking LEVEL 2 Residential Units/Mechanical/Bicycle Parking/Residential Parking LEVEL 3 Residential Units/Amenity/Outdoor Terrace Play Area/Laundry LEVEL 4-6 Residential Units/Laundry LEVEL 7 Residential Units/Outdoor Terrace
DEVELOPMENT GOALS	<ul style="list-style-type: none">• 65' Height• 133 Apartments• 60 Commercial parking stalls• 53 Residential parking stalls
CONSTRUCTION TYPES	Two levels of (Type I) Concrete frame made of noncombustible materials. Five levels of Type 5 wood frame construction.



INTRODUCTION

EDG 2 Proposal Review	4-5
Future Context.....	6-7

DESIGN PROPOSAL

Overall NW View	9
Commercial Corner + Entries	10-12
Overall NE View + Alley.....	13-15
Site Sections.....	16-17
Overall SE View	18
Overall SW View	19
Residential Entry	20-21
Grade-Related Residential	22
Commercial Parking Garage Entry	23

MATERIALS

Building Materials.....	26-27
Signage Concepts	28-29
Lighting + Light Fixtures	30

ELEVATIONS + PLANS

Color Elevations	32-34
Site Plan	35
Floor Plans	36-40

LANDSCAPE

Landscape Plans.....	42-43
Plantings.....	44-45

PROPOSED DEPARTURES	47-59
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DESIGN GUIDELINES IDENTIFIED IN EDG	61-67
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APPENDIX.....	69-91
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FROM EDG 2 PROPOSAL



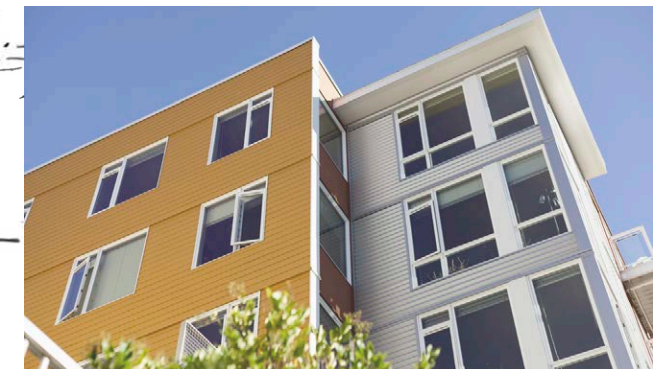
STAGGERED PLAYFUL WINDOWS ALONG NE 50TH ST



PATTERNED WINDOWS ALIGNED AT THE CORNER
CONVEYING A STILLNESS OR PAUSE AT THE MAIN
INTERSECTION OF THE SITE



HORIZONTAL DATUM REFLECTIVE OF UPPER
CORNICE DATUM LINE OF THE UCC EAST
FACADE. CREATING A PROPORTION OF 1/3
– 2/3 ON THE UPPER MODULATION OF THE
PROPOSED SOUTH STRUCTURE



ENHANCED CORNER TREATMENT SIGNIFYING
THE CORNER INTERSECTION AND MARKING THE
SET-BACK RESIDENTIAL DATUM NORTH OF NE
50TH ST ALONG 15TH AVE NE

FROM EDG 2 PROPOSAL



LANDSCAPED RESIDENTIAL STOOPS ALONG 15TH AVE NE



CONNECTOR CORRIDOR BETWEEN NORTH AND SOUTH STRUCTURES, BREAKING MASS OF BUILDING INTO TWO PART WITH A 1/3 – 2/3 PROPORTION



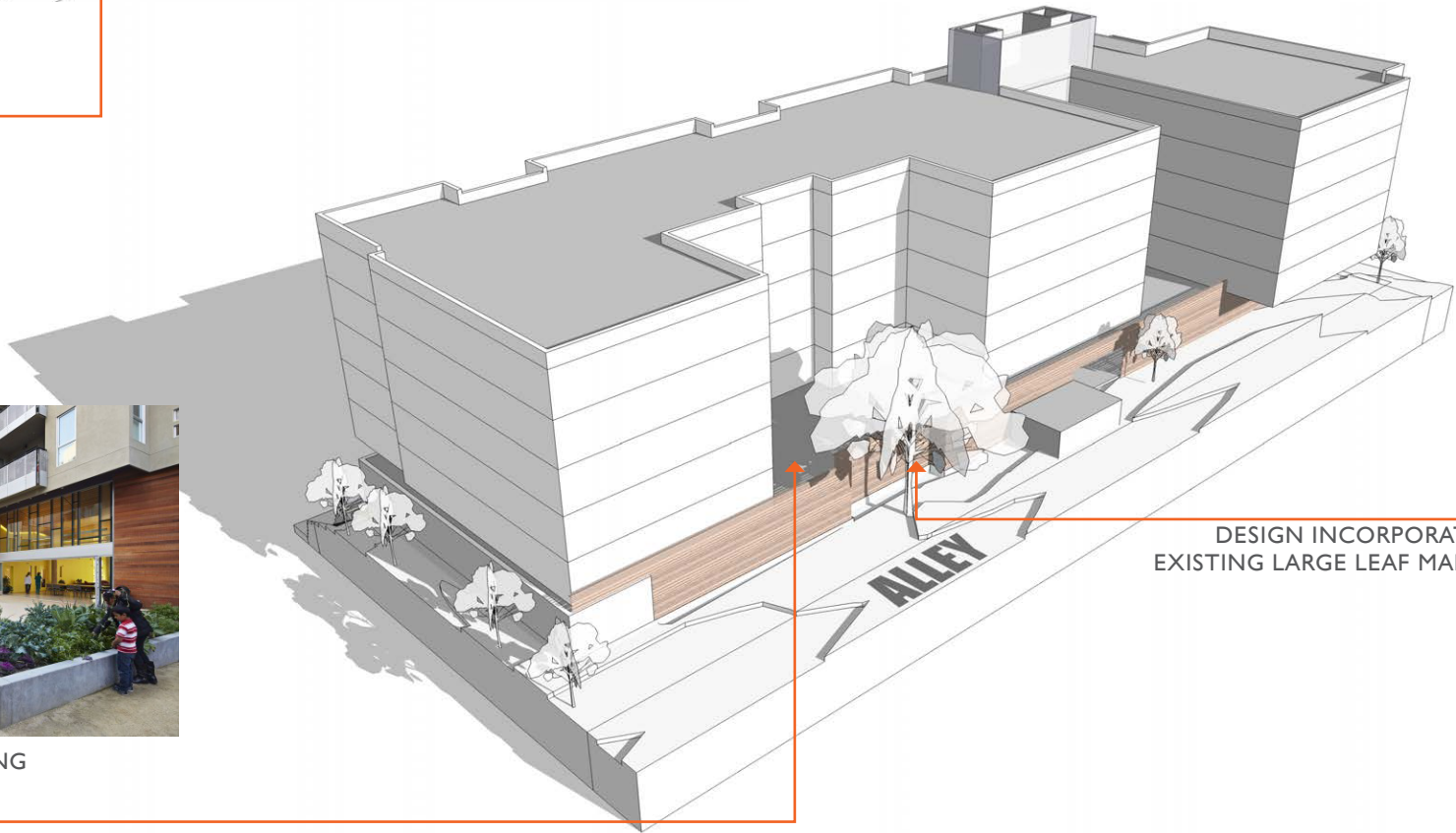
RESIDENTIAL ENTRIES WITH LANDSCAPED STOOPS – RANGING 6'-8' FROM SIDEWALK EASEMENT



FIELD AND CORNER FENESTRATION – SPECIAL CORNER TREATMENT

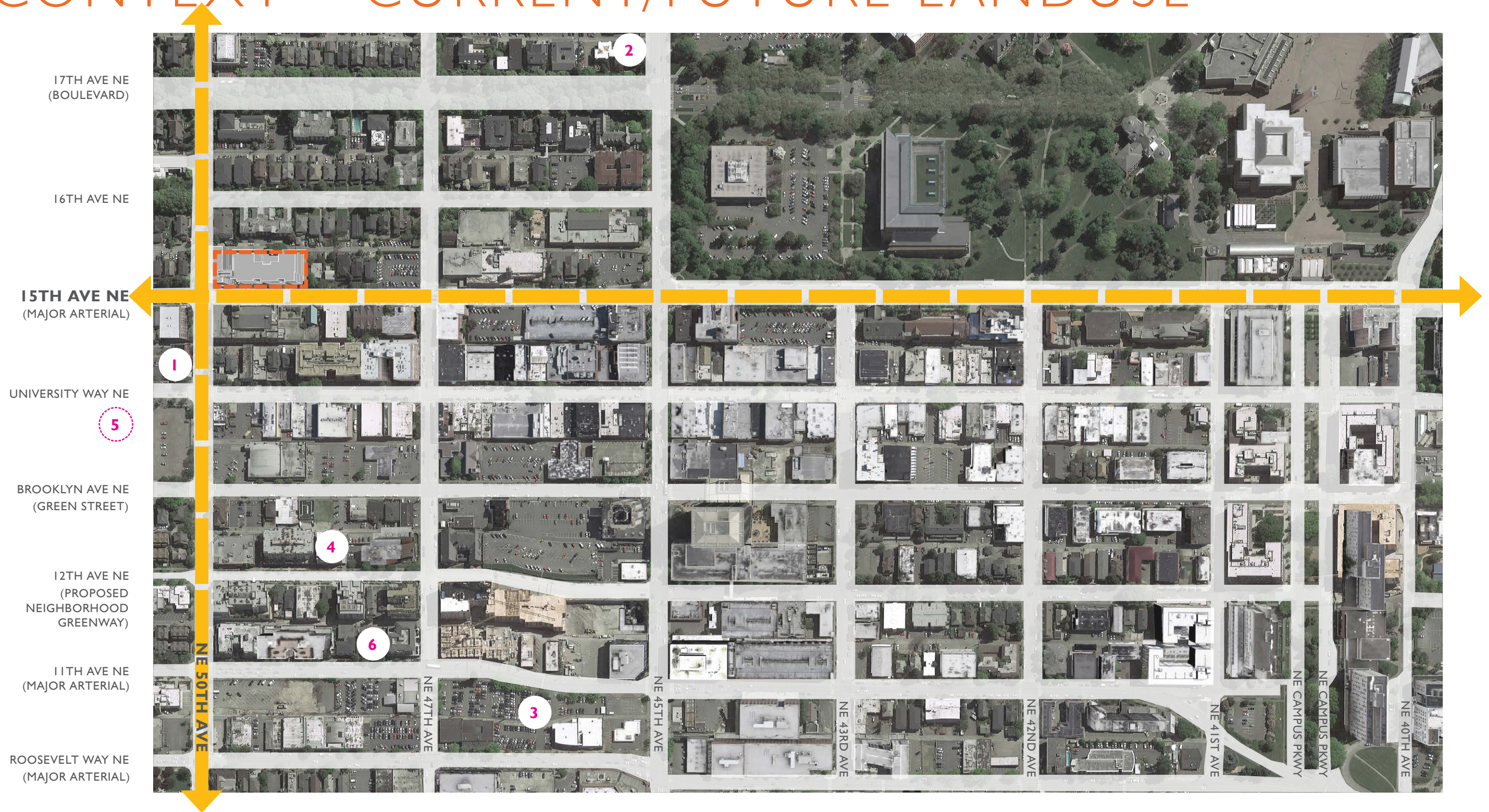


FAMILY UNITS SURROUNDING ACTIVE COURT



DESIGN INCORPORATES EXISTING LARGE LEAF MAPLE

CONTEXT – CURRENT/FUTURE LANDUSE



CONTEXT – MULTIFAMILY FUTURE LANDUSE



DESIGN PROPOSAL _____

OVERALL NW VIEW



BIRD'S EYE VIEW FROM NORTHWEST

COMMERCIAL CORNER



VIEW TO COMMERCIAL SPACE



BELLWETHER UCC HOUSING

06.29.2015 | 14-034 | 10



COMMERCIAL CORNER



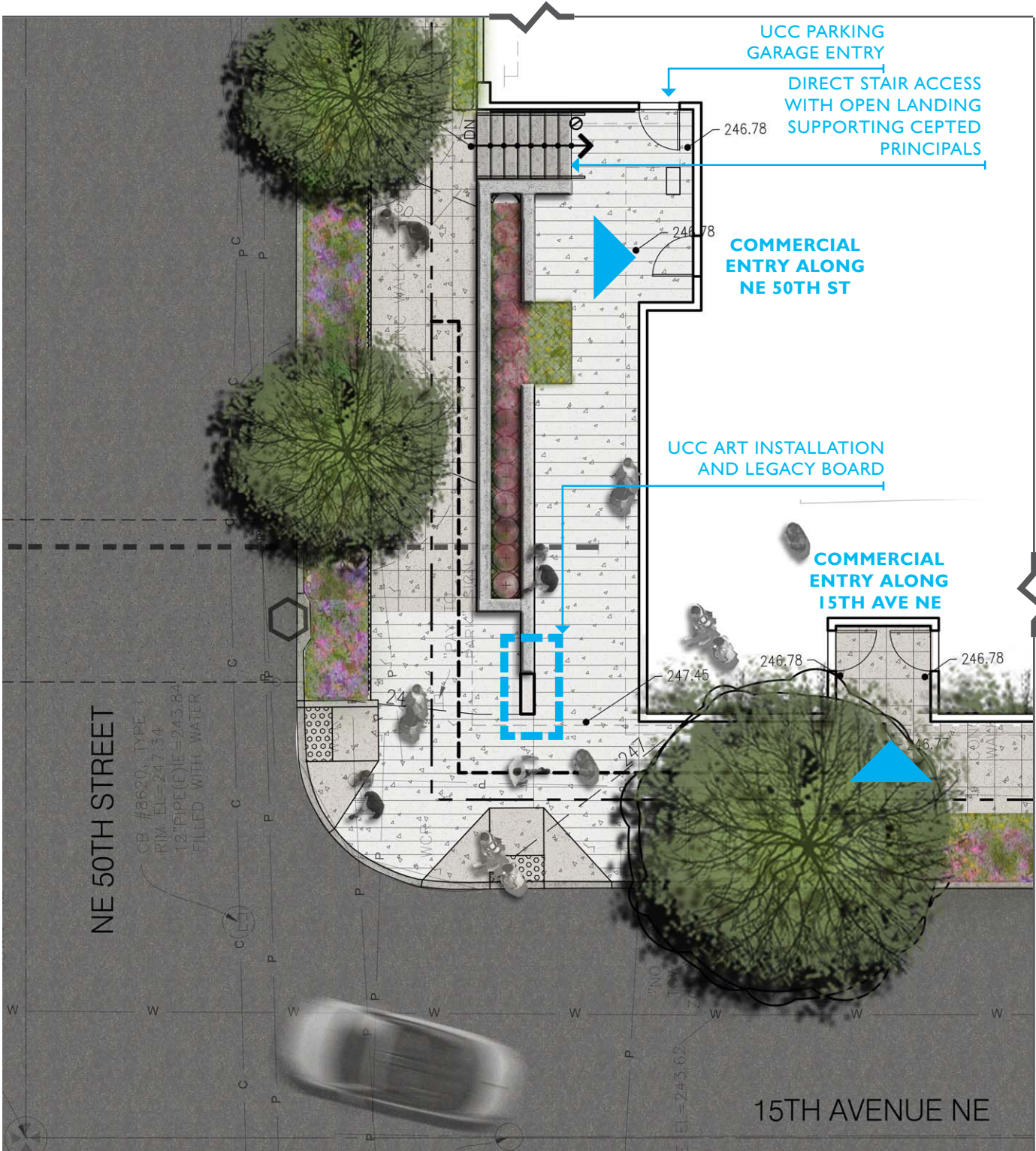
COMMERCIAL CORNER, PATHWAY TO UCC PARKING GARAGE AND UCC LEGACY INSTALLATION

UCC LEGACY INSTALLATION

University Christian Church as a development sponsor, has been given a space on the prominent corner of the site to incorporate a legacy board, displaying an abstraction art element that symbolizes the UCC mission. A collaboration including members of UCC and Bellwether will develop a visual theme and symbol for this project, tied in with the name of the new home for the future residents. Materials can include cut metal or aluminum, mosaic ceramics and glass, stamped concrete as well as integrated lighting.



SCREEN PRINTS – UCC ARTIST CONCEPT AS ABSTRACTIONS OF TREES



COMMERCIAL CORNER AND UCC LEGACY INSTALLATION LANDSCAPE PLAN



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06.29.2015 | 14-034 | 11



COMMERCIAL ENTRIES



VIEW TO COMMERCIAL ENTRY ALONG NE 50TH STREET



VIEW TO COMMERCIAL ENTRY ALONG 15TH AVENUE WEST

OVERALL NE VIEW + ALLEY



AERIAL VIEW FROM NORTHEAST

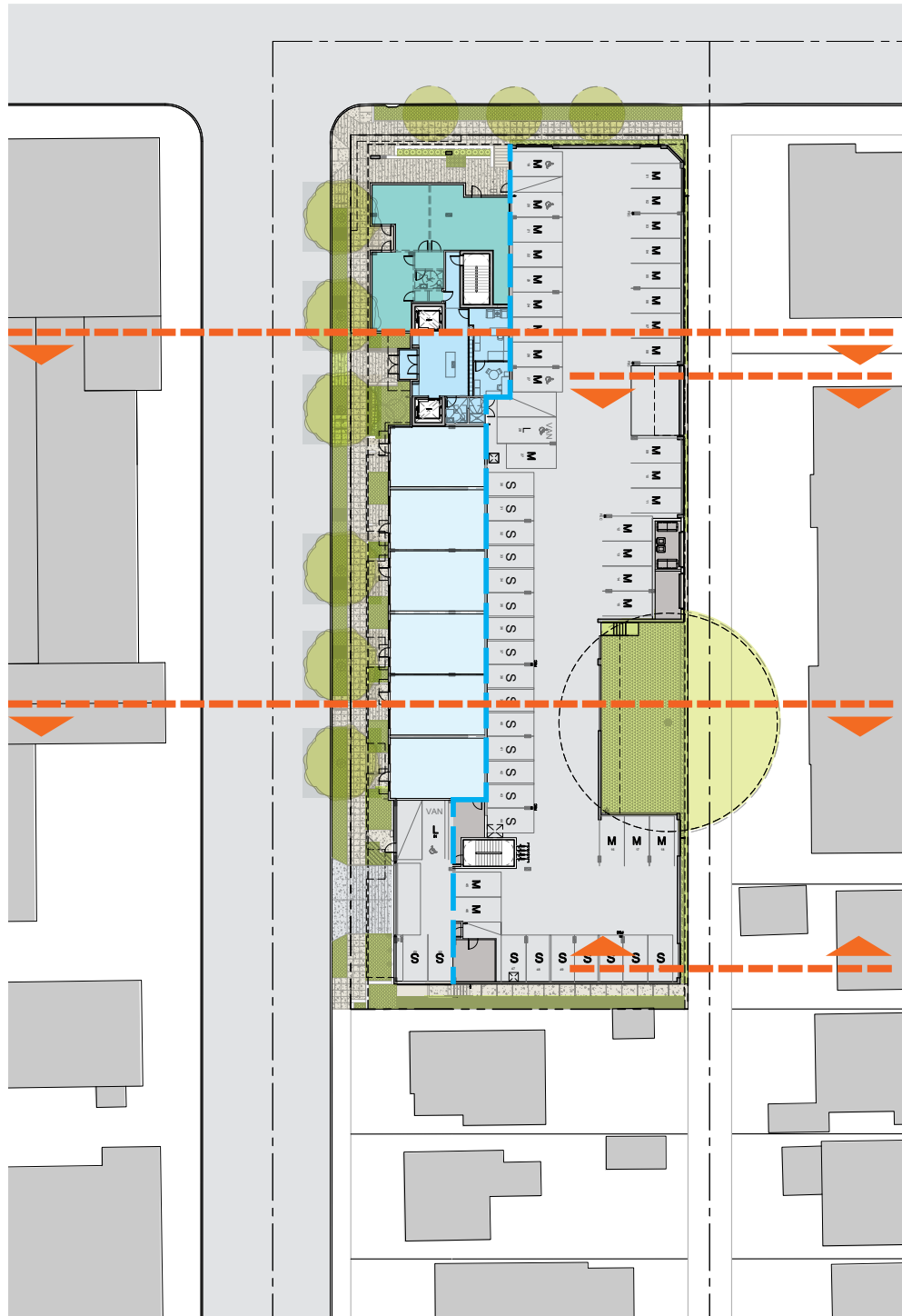


ALLEY VIEW LOOKING SOUTH AT EXIT



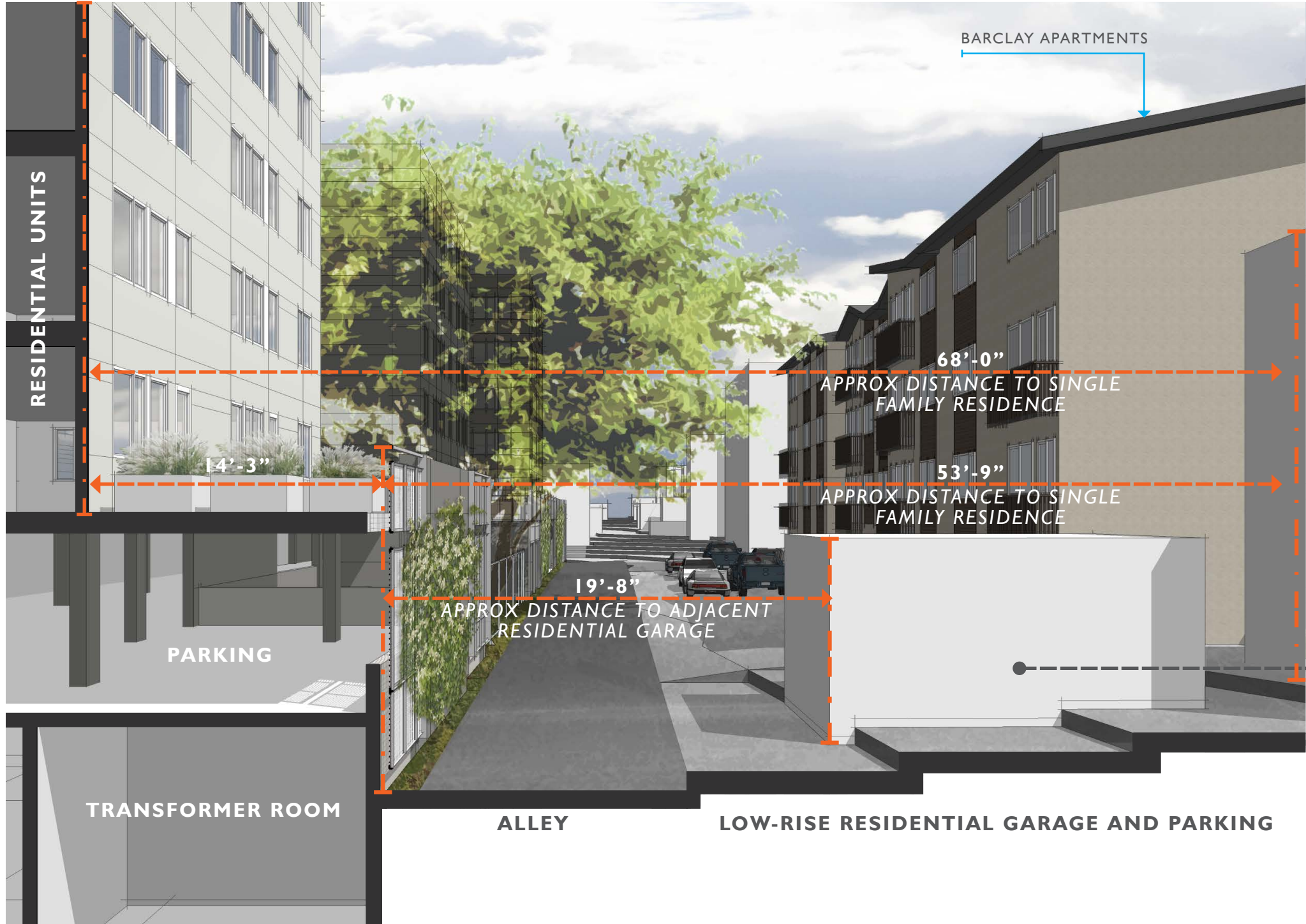
ALLEY VIEW LOOKING NORTH ALONG EXISTING TREE

ALLEY SECTION



SECTION VIEW FROM NORTH THRU RESIDENTIAL PARKING EXIT

ALLEY SECTION



ROSS-ALLEY LOW-RISE PARKING

SECTION VIEW FROM SOUTH THRU UPPER LEVEL RESIDENTIAL, PARKING AND CROSS-ALLEY LOW-RISE PARKING

SITE SECTION



SITE SECTION



OVERALL SE VIEW



AERIAL VIEW FROM SOUTHEAST



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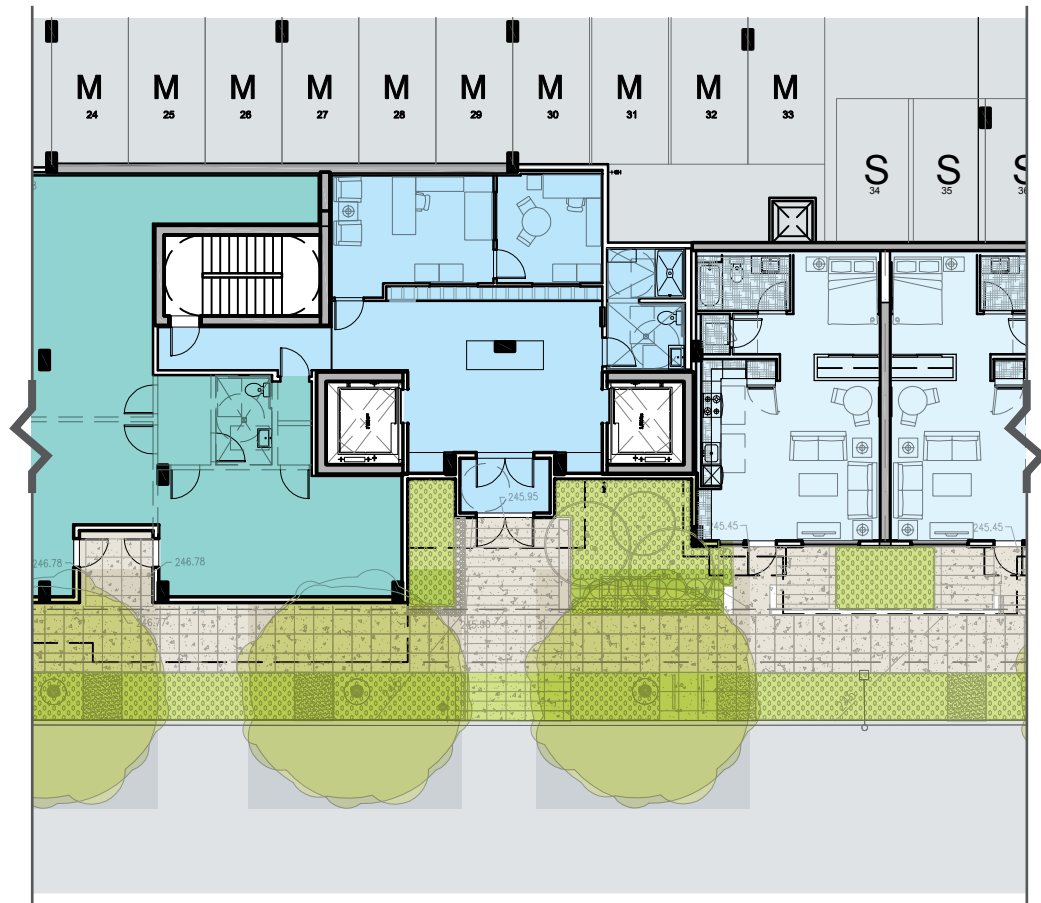
06.29.2015 | 14-034 | 18



OVERALL SW VIEW



RESIDENTIAL ENTRY



MAIN RESIDENTIAL LOBBY ENTRY PLAN



ELEVATION OF CONNECTOR ELEMENT AT RESIDENTIAL ENTRY



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06.29.2015 | 14-034 | 20



RESIDENTIAL ENTRY



RESIDENTIAL ENTRY VIEW



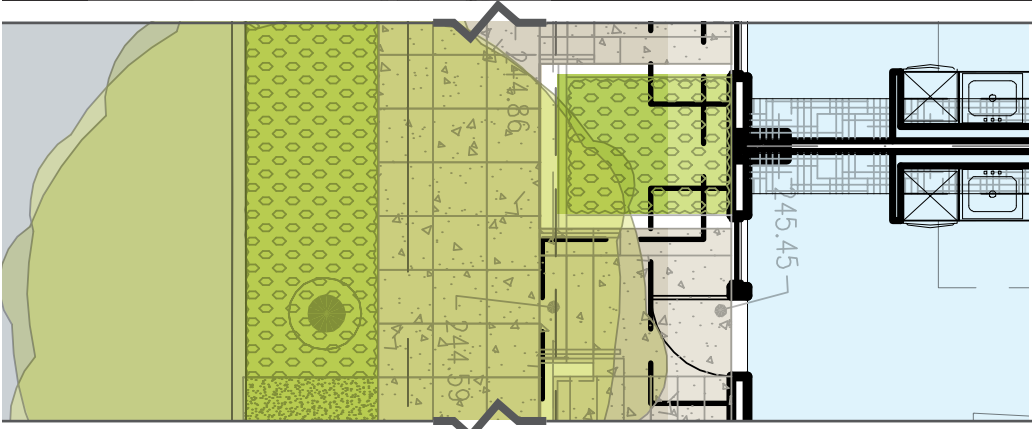
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06.29.2015 | 14-034 | 21

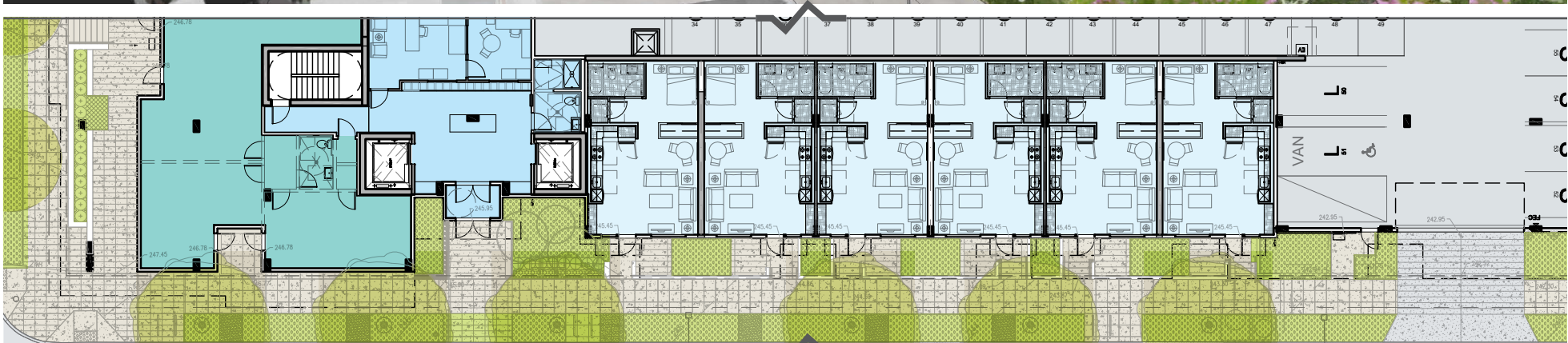
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GRADE-RELATED FLATS



SECTION/SITE PLAN AT TYPICAL GRADE-RELATED FLAT ENTRY



GRADE-RELATED FLAT ENTRIES

PUBLIC PARKING GARAGE ENTRY



PEDESTRIAN VIEW AT 5TH AVE NE GARAGE ENTRY



PARTIAL ELEVATION ALONG 15TH AVENUE



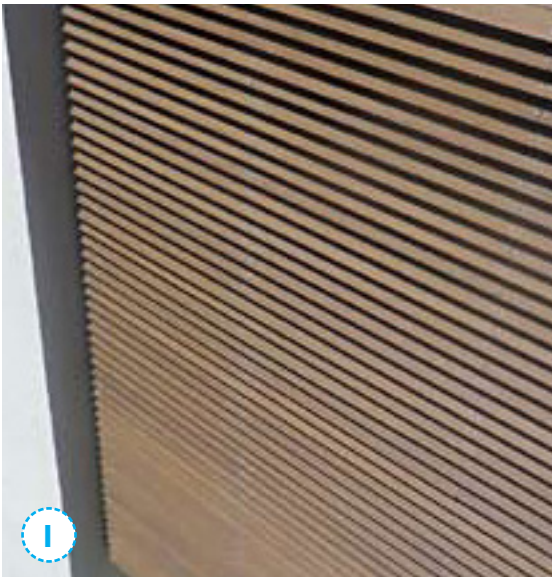
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06.29.2015 | 14-034 | 23



MATERIALS

MATERIALS



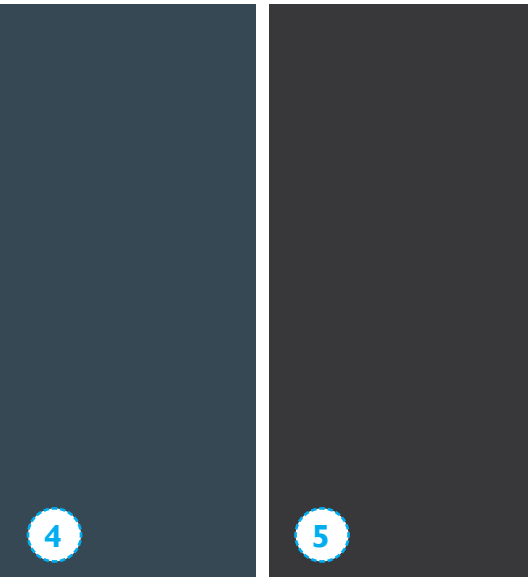
PLANK SCREENING OF VARIED WIDTHS AT GARAGE, SOFFITS, AND CANOPY SOFFITS



SIMILAR VINYL WINDOW COMPOSITION AT SPANDREL PANEL



CERACLAD 8" REVEAL HORIZONTAL – ASH



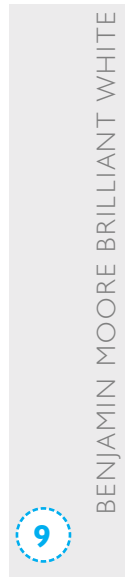
NORTH BUILDING COLOR SCHEME



MATERIALS



VARIED WIDTH
HARDIPLANK



SOUTH FAMILY BUILDING COLOR SCHEME



NEUTRAL BRICK



BOARD-FORMED
CONCRETE



WOOD TEXTURE
STEEL DOOR



RESYSTA/STEEL
FRAMED CANOPY



METAL MESH SCREENING
L2 GARAGE &
L3 GUARDRAILS



STAINLESS STEEL/GLASS
RAILING AT AMENITY
L3 ROOF DECK



HORIZONTAL TERRACOTTA-COLOR THICK GAUGE ALUMINUM
PANELS TIE INTO HORIZONTAL COLOR ACCENTS ABOVE



BUILDING SIGNAGE



1 RESIDENTIAL ENTRY SIGNAGE – CANOPY MOUNTED PROJECT NAME AND/OR ADDRESS SIGNAGE

2 PARKING SIGNAGE – WALL MOUNTED SIMPLE PUBLIC PARKING SIGNAGE ALONG 15TH AVE NE, OR WALL MOUNTED BLADE SIGN



BUILDING SIGNAGE

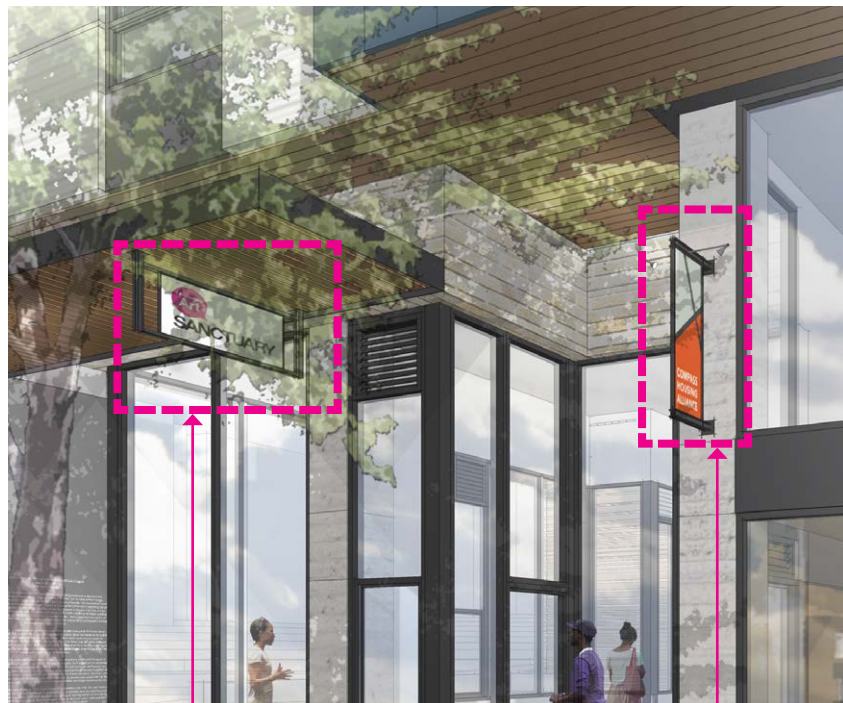


3 GRADE RELATED RESIDENTIAL – STAMPED CONCRETE MONUMENT SIGNAGE OR STEEL CUT NUMBERS ATTACHED TO CONCRETE

4 COMMERCIAL SIGNAGE – INTEGRATED BLADE SIGNAGE AT NORTHWEST CORNER COMMERCIAL SPACES, WALL AND SOFFIT-MOUNTED



3



4



4

4

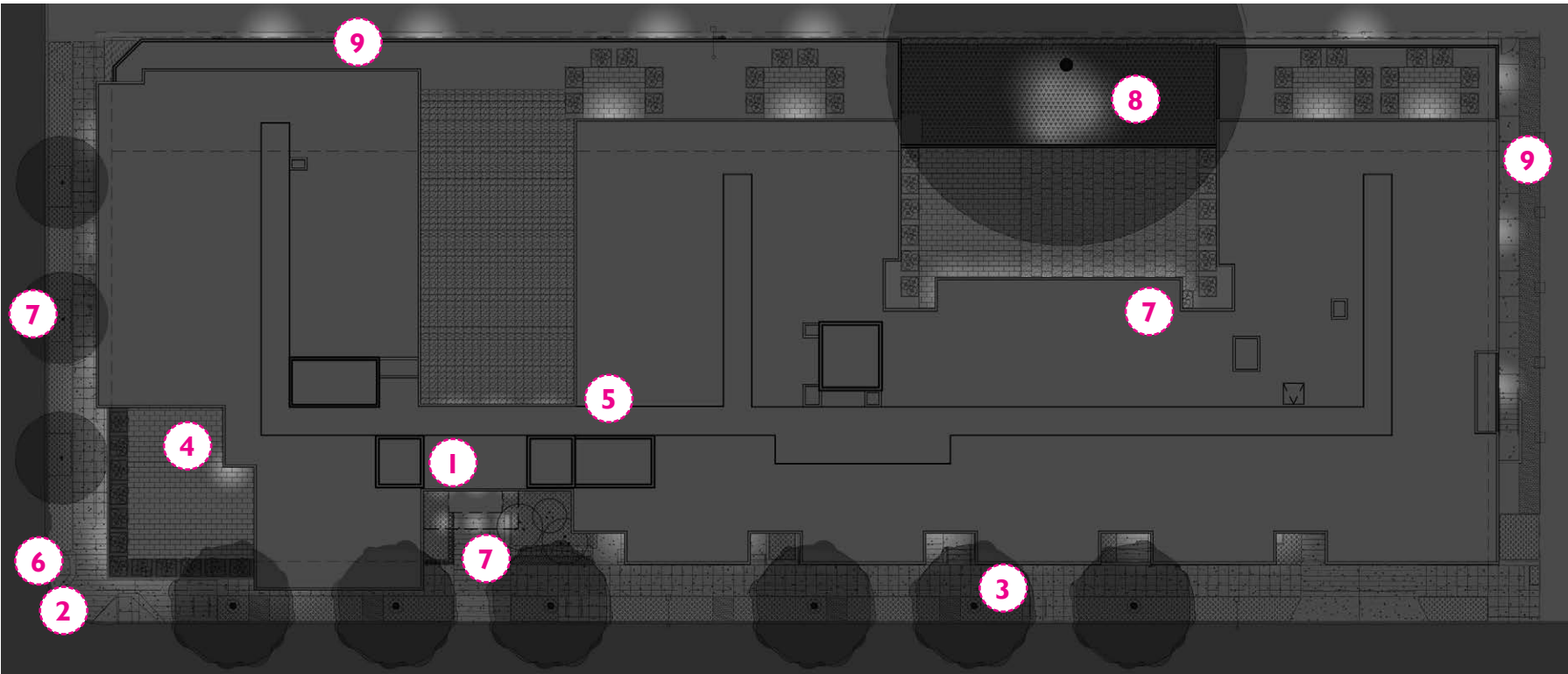
LIGHTING



1 MULLION MOUNT FIXTURE – EVERY LEVEL ALONG GLAZED LINKAGE ELEMENT



2 FEATURE UPLIGHT – LEGACY ART AT COMMERCIAL CORNER



3 WALL SCONCE – RESIDENTIAL STOOPS AND COMMERCIAL ENTRIES



4 WALL FIXTURE – LEVEL 7 AMENITY DECK



5 FLUSH MOUNT CEILING FIXTURE – INTERIOR CORRIDOR



6 SPOT LIGHT – LEGACY ART AT COMMERCIAL CORNER



7 RECESSED LED DOWNLIGHT – RESIDENTIAL ENTRY AND COMMERCIAL SOFFITS



8 LANDSCAPE SPOT LIGHT – PRESERVED SIGNIFICANT TREE



9 WALL SURFACE – SAFETY LIGHTING AT PERIMETER



ELEVATIONS + PLANS

ELEVATIONS



ELEVATIONS



ELEVATIONS



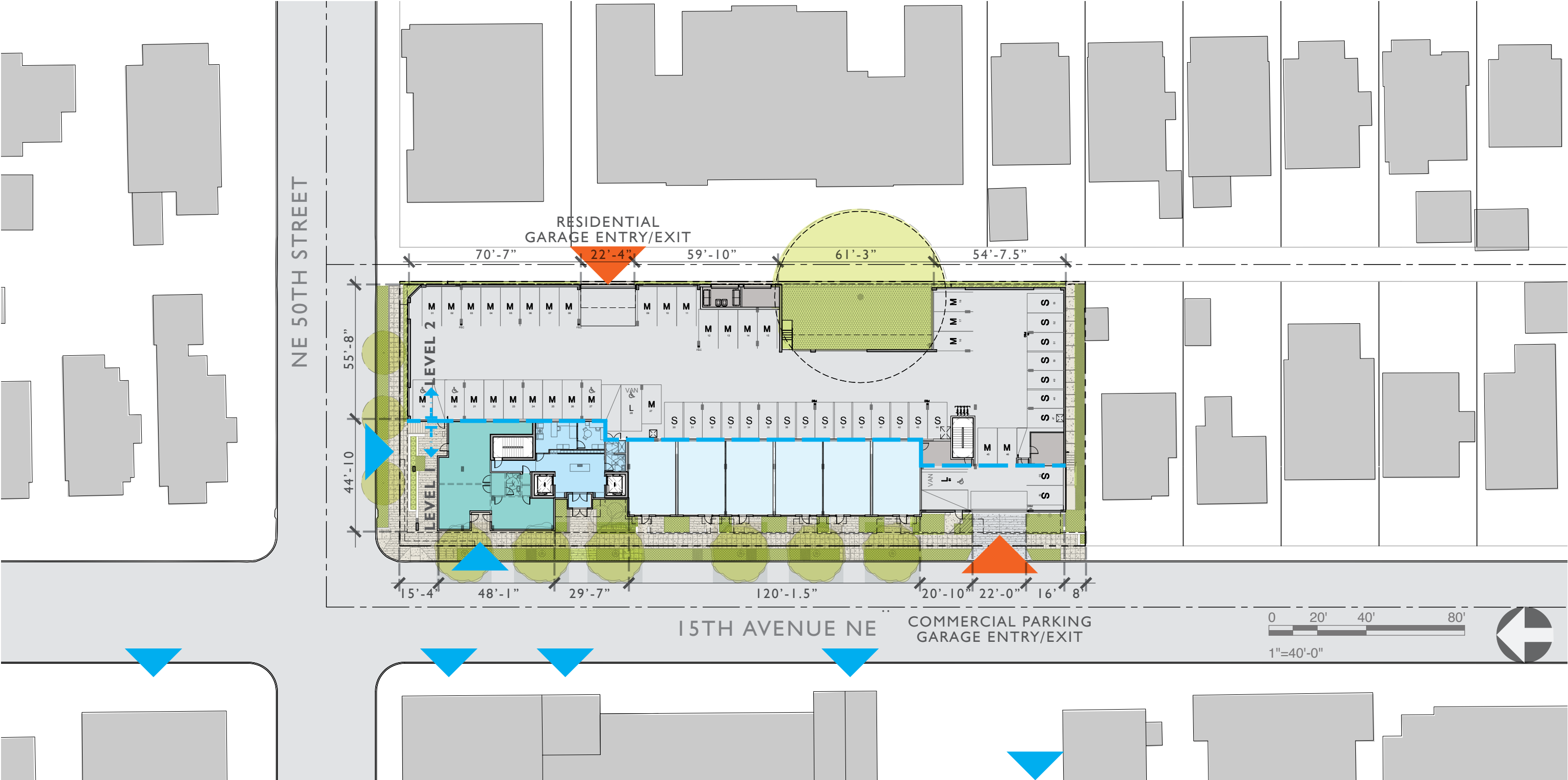
BELLWETHER UCC HOUSING

06.29.2015 | 14-034 | 34

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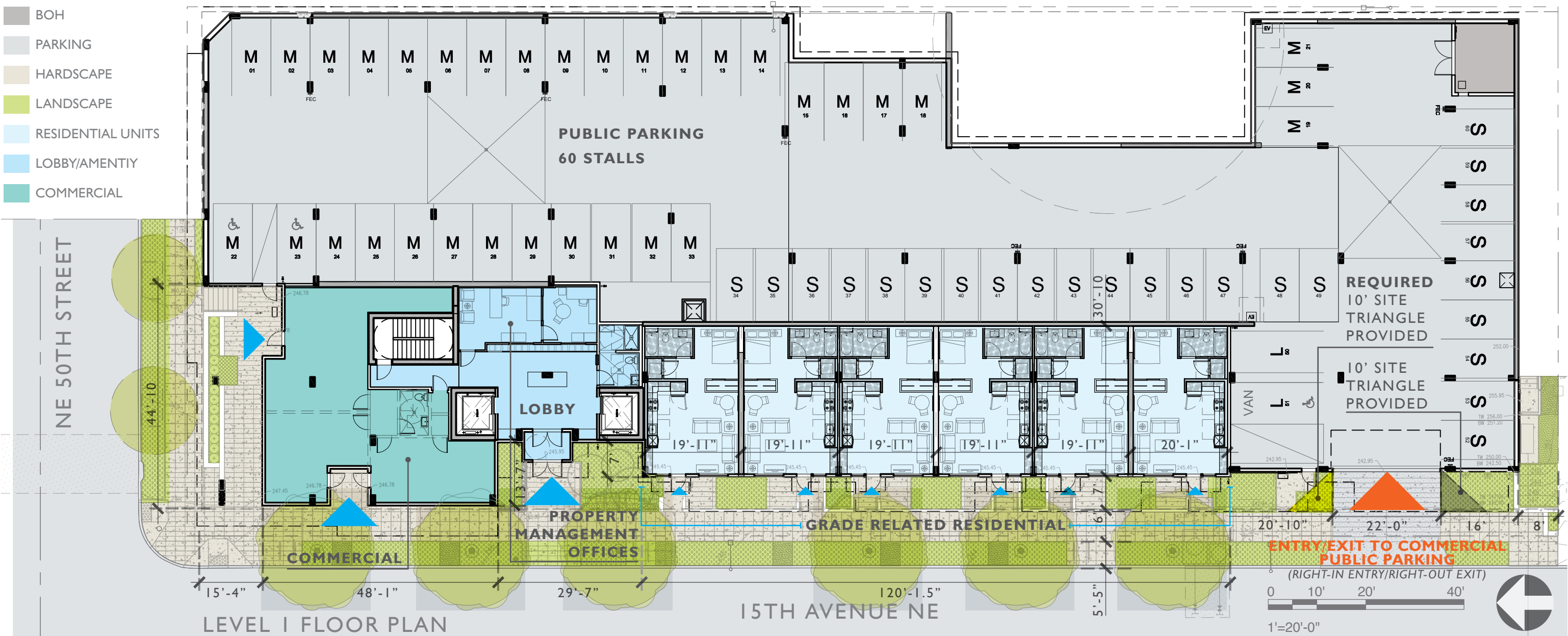


SITE PLAN

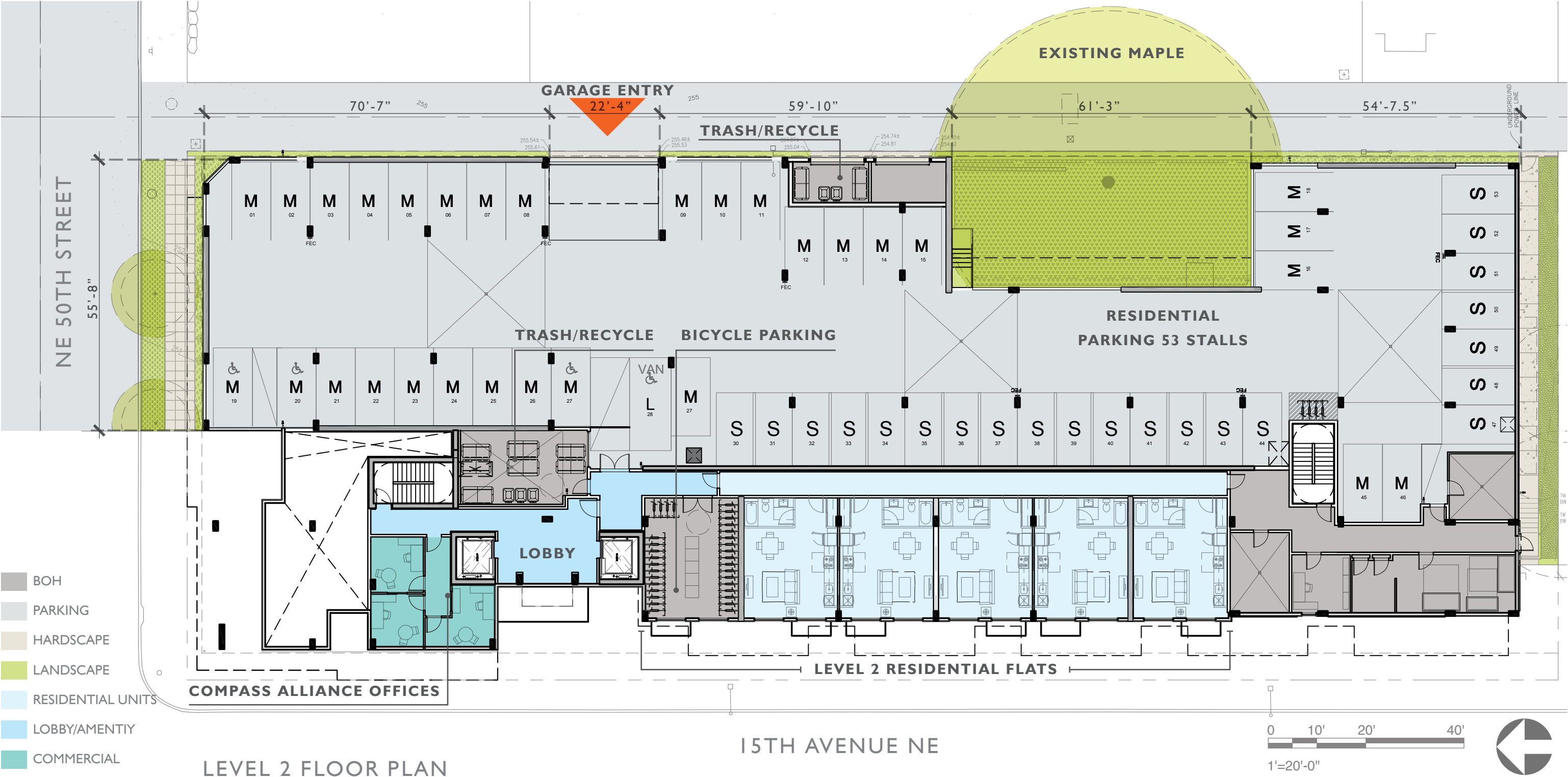


SITE PLAN

FLOOR PLAN



FLOOR PLAN



FLOOR PLAN



FLOOR PLAN



FLOOR PLAN



LEVEL 7 FLOOR PLAN



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06.29.2015 | 14-034 | 40

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LANDSCAPE

GROUND LEVEL LANDSCAPE PLAN



L3 AND L7 LEVELS LANDSCAPE PLAN



PLANTINGS

TREES



ULMUS PARVIFLOLIA 'ELMER II' – ALLEE ELM



ACER CIRCINATUM – VINE MAPLE



SUN PLANT MIX



MAHONIA X MEDIA 'WINTER SUN' – WINTER SUN MAHONIA

SUN PLANT MIX



MAHONIA NERVOSA – LONGLEAF OREGON GRAPE



PACHISTIMA MYRSINITES – OREGON BOXWOOD



RIBES SANGUINEUM – FLOWERING CURRENT



SPIRAEA DOUGLASII – DOUGLAS SPIRAEA



SUN PLANT MIX



PLECTRITIS CONGESTA – SEA BUSH



AQUILEGIA FORMOSA – WESTERN COLUMBINE



FRAGARIA CHILOENSIS – COASTAL STRAWBERRY



MISCANTHUS SINENSIS 'ADAGIO' – DWARF MAIDEN GRASS



DESCHAMPSIA CESPITOSA – TUFTED HAIRGRASS



PLANTINGS

SUN PLANT MIX



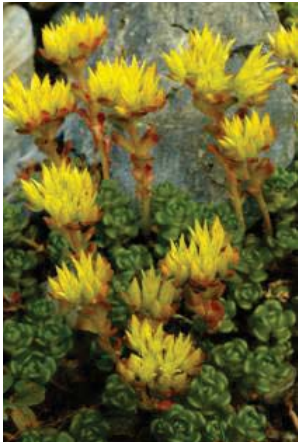
ACHILLEA MILLEFOLIUM – YARROW



LUPINUS POLYPHYLLUS – LARGE-LEAVED LUPINE



SEDUM OREGANUM – OREGON STONECROP



JUNCUS EFFUSUS – PACIFIC RUSH

SUN PLANT MIX



HYDRANGEA ANOMALA SUBSP. PETIOLARIS – CLIMBING HYDRANGEA



SHADE



DICENTRA FORMOSA – BLEEDING HEART



ARUNCUS DIOICUS – GOAT'S BEARD

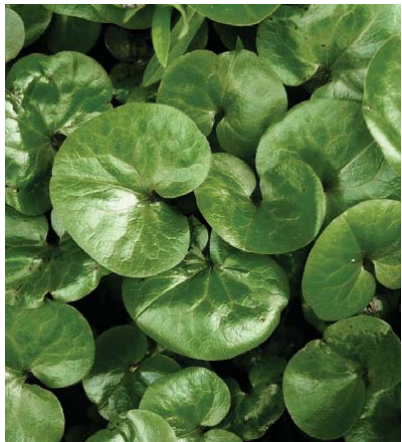


POLYSTICHUM MANITUM – SWORD FERN

SHADE



ASARUM CAUDATUM – WILD GINGER



OXALIS OREGANA – REDWOOD SORREL



GAULTHERIA SHALLON – SALAL



DEPARTURES



PROPOSED DEPARTURE I

ALLEY SETBACK

REQUIREMENT: PER PROPERTY USE AND DEVELOPMENT AGREEMENT (PUDA) FOR COMMERCIAL, MIXED USE OR RESIDENTIAL PROJECTS ON THE WEST HALF OF SUBJECT BLOCK; ALL BUILDING ELEMENTS ABOVE 13 FEET SHALL BE SETBACK 30 FEET FROM THE EAST PROPERTY LINE, PROVIDED THAT ONE-HALF THE WIDTH OF THE ABUTTING ALLEY MAY BE COUNTED AS PART OF THE REQUIRED SETBACK. A DEPARTURE MAY BE GRANTED THROUGH DESIGN REVIEW AS PART OF MUP WHERE IT IS FOUND THAT ANY ALLOWED REDUCTIONS OF THIS REQUIRED SETBACK ADEQUATELY ACCOMPLISH A SENSITIVE AND APPROPRIATE TRANSITION OF HEIGHT BULK AND SCALE ACROSS THE ALLEY TO THE EAST.

REQUEST: IN ORDER TO PROVIDE GREATER ARTICULATION OF THE FACADE ALLOW 3 AREAS OF ENCROACHMENT INTO THE 30 SETBACK IN EXCHANGE FOR GREATER SETBACK DEPTHS IN OTHER AREAS.

RATIONALE: THE DESIGN PROPOSES THREE INTRUSIONS INTO THE PUDA SETBACK FOR A TOTAL OF **1,723 SF² PER LEVEL OR 3,408 CU. YD.** WHICH ARE OFFSET BY THE PRESERVATION OF THE BIG LEAF MAPLE AND TWO LARGE RECESSED COURTYARDS WHICH RESULT IN A TOTAL SETBACK AREA OF **3,092 SF² PER LEVEL OR 6,116 CU. YD.**

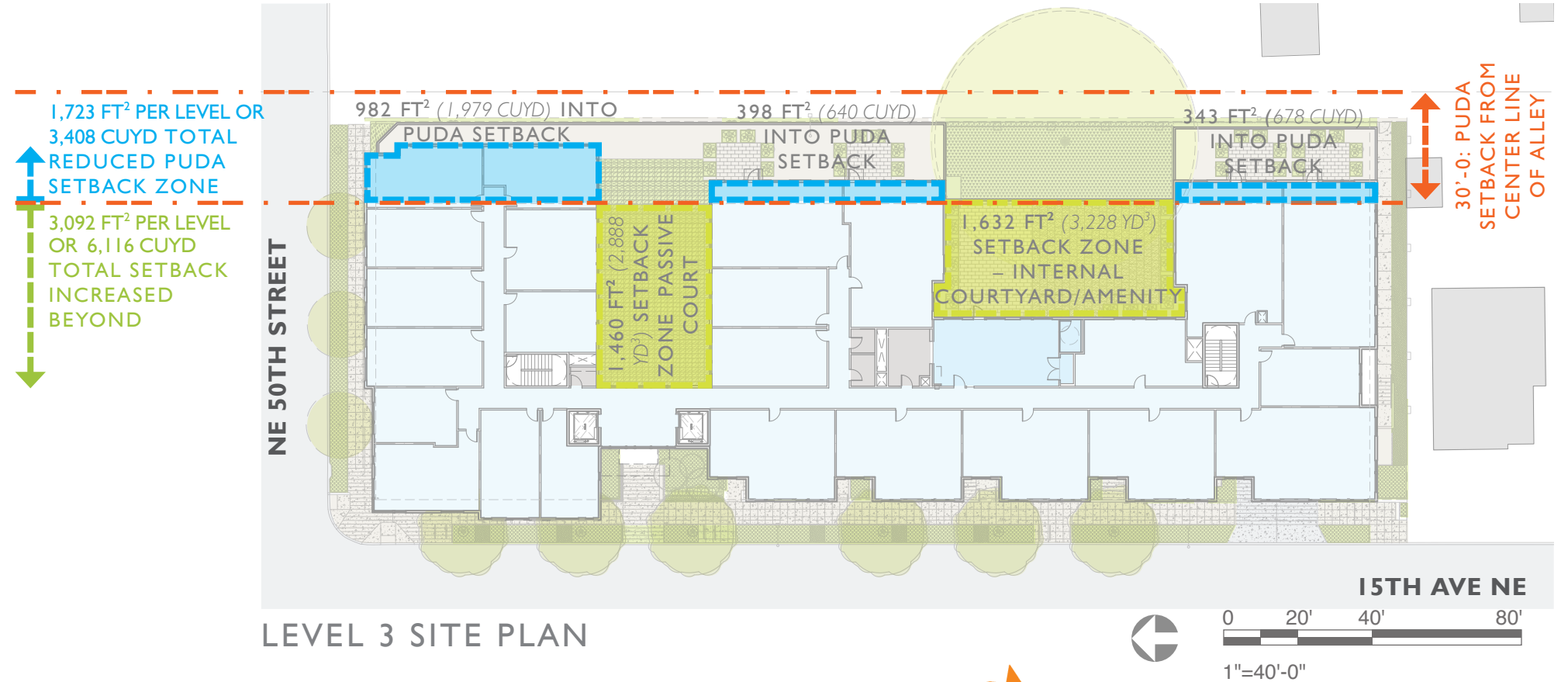
THIS DEPARTURE BETTER MEETS GUIDELINES CS2-D-1 EXISTING DEVELOPMENT AND ZONING AND PL2-B-1 EYES ON THE STREET PROPOSING A DEEPER ARTICULATION OF THE BUILDING ALLEY FACADE WITH TWO LARGE COURTYARDS, BROKEN DOWN STRUCTURE WIDTH WITH SMALLER BAYS AND THE PRESERVATION OF THE BIG LEAF MAPLE WHICH ADDRESS THE PUDA CONCERN FOR CREATING A SENSITIVE TRANSITION TO THE LESS INTENSIVE ZONE. THE COURTYARDS ALSO OFFER OPPORTUNITY FOR 'EYES ON THE STREET' AT THE ALLEY.

THE BOARD EXPRESSED PRELIMINARY SUPPORT FOR THIS DEPARTURE AT EDG.

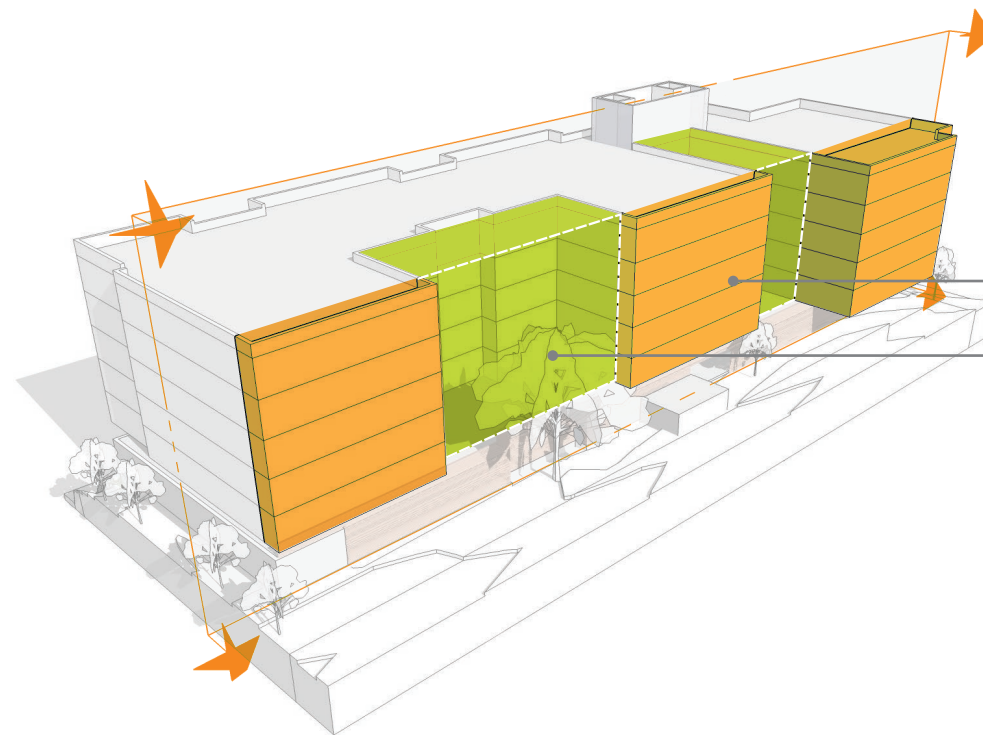
REFERENCED GUIDELINES:

CS2-D-1. EXISTING DEVELOPMENT AND ZONING: REVIEW THE HEIGHT, BULK, AND SCALE OF NEIGHBORING BUILDINGS AS WELL AS THE SCALE OF DEVELOPMENT ANTICIPATED BY ZONING FOR THE AREA TO DETERMINE AN APPROPRIATE COMPLEMENT AND/OR TRANSITION.

PL2-B-1. EYES ON THE STREET: CREATE A SAFE ENVIRONMENT BY PROVIDING LINES OF SIGHT AND ENCOURAGING NATURAL SURVEILLANCE.



LEVEL 3 SITE PLAN



PROPOSED FLOOR AREA WITHIN PUDA ZONE =
1,723 OR 3,408 CUYD

PROPOSED SETBACK AREA INCREASED BEYOND PUDA =
3,092 FT² PER LEVEL OR 6,116 CUYD

PROPOSED DEPARTURE 2

PARKING SPACE REQUIREMENTS FOR NON-RESIDENTIAL USES

REQUIREMENT: NO PARKING IS REQUIRED HOWEVER IF NON-RESIDENTIAL PARKING IS PROVIDED IT MUST MEET THE PARKING REQUIREMENTS OF 23.54.030.B.2; WHEN 20 OR MORE PARKING SPACES ARE PROVIDED A MINIMUM 35% OF THE PARKING SPACES SHALL BE STRIPED FOR LARGE VEHICLES AND A MINIMUM OF 35% AND MAX OF 65% STRIPED FOR SMALL STALL VEHICLES.

REQUEST: REDUCE THE MINIMUM LARGE STALL REQUIREMENT.

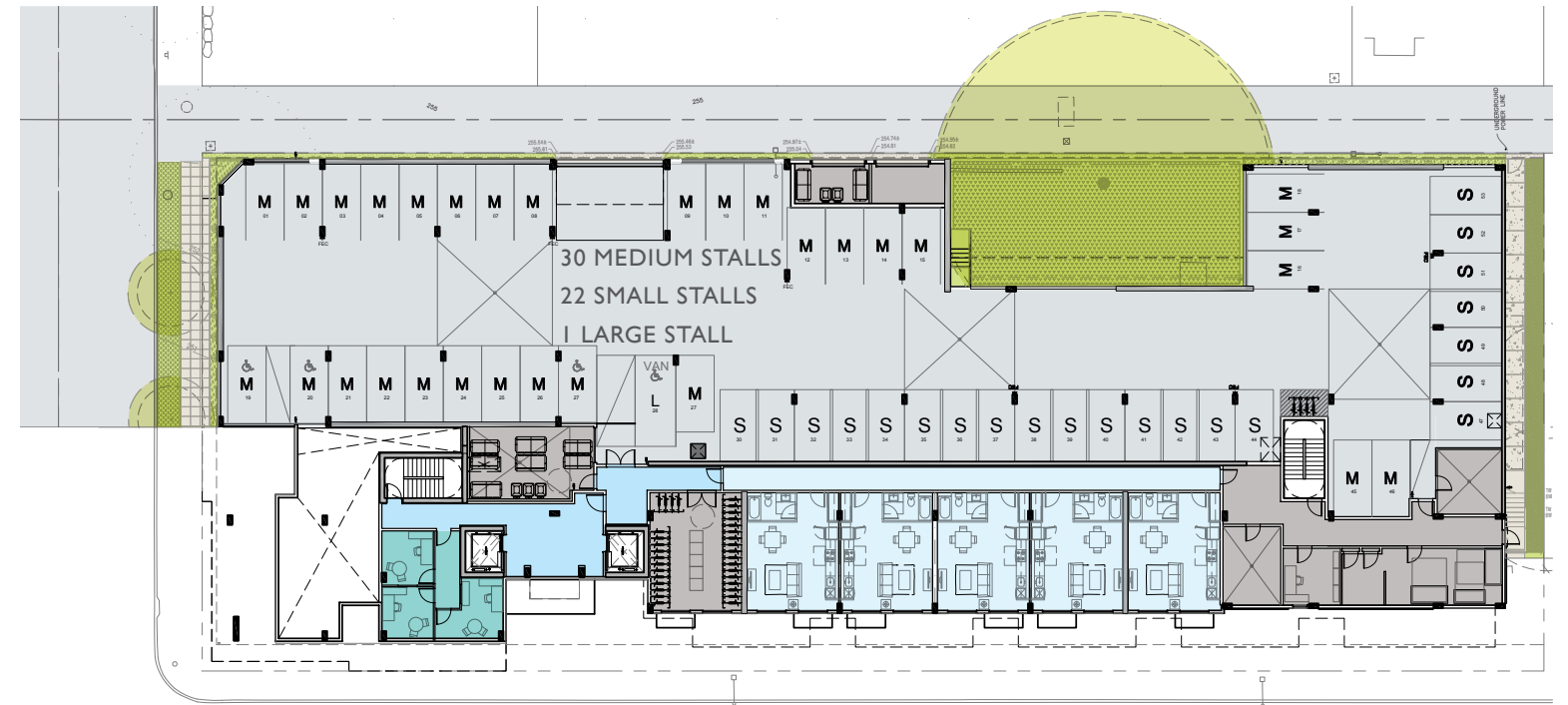
RATIONALE: THE PROPOSED DESIGN WITH 3 (2.7%) LARGE STALLS, 60 (53.0%) MEDIUM STALLS AND 50 (44.3%) SMALL STALLS BETTER SATISFIES SEVERAL OF THE DESIGN GUIDELINES ALONG 15TH AVENUE NE; ALLOWING FOR INCREASED SETBACK ALONG THE ROW TO BETTER SERVE THE PEDESTRIAN EXPERIENCE BY OFFERING MORE SPACE FOR LANDSCAPING, MORE PRIVACY FOR GROUND LEVEL UNITS, BETTER SEMIPUBLIC TRANSITION TO GROUND LEVEL UNITS AND A BETTER OPPORTUNITY FOR WIDER OUTDOOR SPACE DIRECTLY ADJACENT TO THE SIDEWALK AS WELL AS PROVIDING A DEEPER COMMERCIAL SPACE.

- **3 LARGE STALLS PROPOSED**
- **60 MEDIUM STALLS PROPOSED**
- **50 SMALL STALLS PROPOSED**

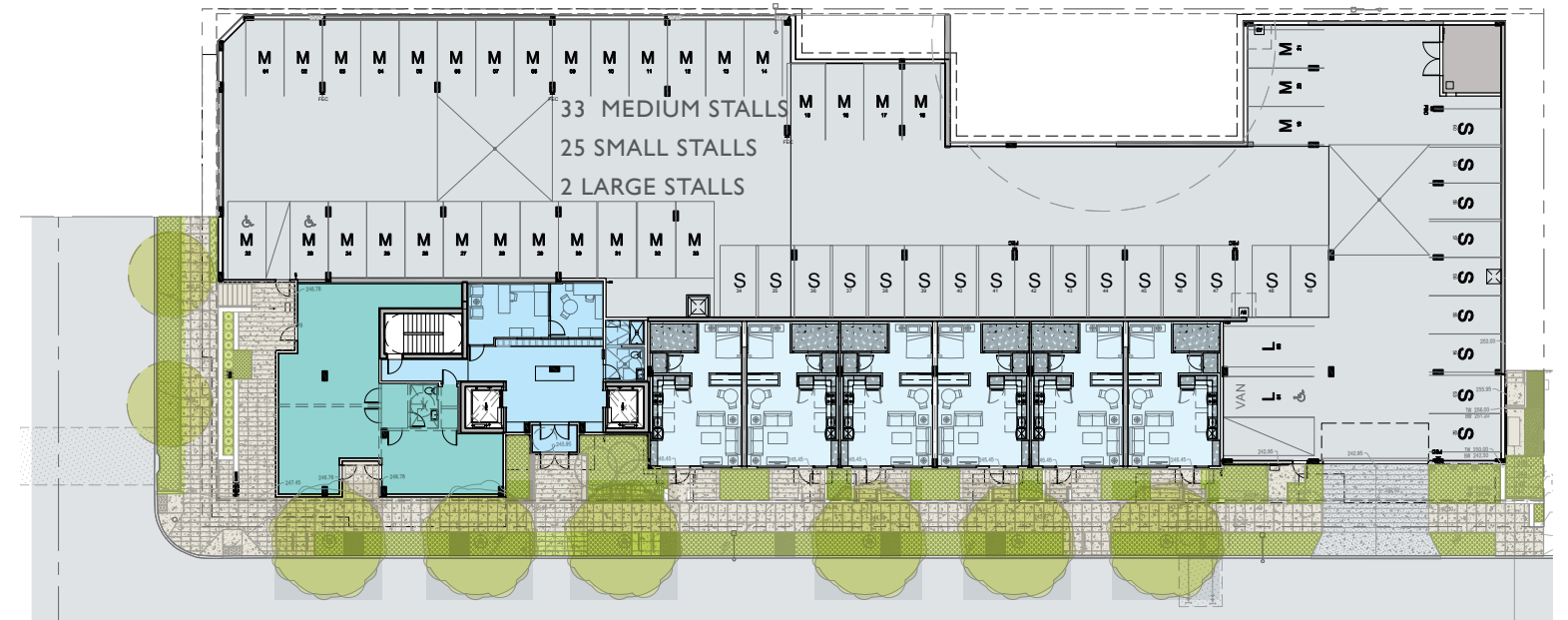
THE BOARD EXPRESSED PRELIMINARY SUPPORT FOR THIS DEPARTURE AT EDG.

REFERENCED GUIDELINES:

PL3 STREET-LEVEL INTERACTION: ENCOURAGE HUMAN INTERACTION AND ACTIVITY AT THE STREET-LEVEL WITH CLEAR CONNECTIONS TO BUILDING ENTRIES AND EDGES.



LEVEL 2 SITE PLAN



LEVEL 1 SITE PLAN

PROPOSED DEPARTURE 3

PARKING LOCATION AND ACCESS

REQUIREMENT: PARKING WITHIN A STRUCTURE SHALL BE SEPARATED FROM STREET-LEVEL, STREET-FACING FACADES BY ANOTHER PERMITTED USE.

REQUEST: GIVEN SITE CONSTRAINTS, ALLOW PARKING STRUCTURE TO ABUT STREET-FACING FACADE ALONG A PORTION OF NE 50TH STREET.

RATIONALE: EMPLOYING CEPTED PRINCIPALS FOR EYES ON THE STREET THE PROPOSED PARKING IS ENCLOSED WITH A SCREENED SYSTEM OF VARYING PLANK WIDTHS AND OPENINGS, COMPLEMENTING SHEARWALLS PLANTED WITH CLIMBING HYDRANGAEA, PROVIDING A SAFE AND SECURE GRADE-LEVEL ACCESS TO PUBLIC PARKING WITH CONNECTION TO THE COMMERCIAL CORNER FOR DAYCARE AND SENIOR CITIZENS FREQUENTING THE COMMERCIAL PARKING; BETTER MEETING DESIGN GUIDELINE PL2-A-2, ACCESS CHALLENGES.

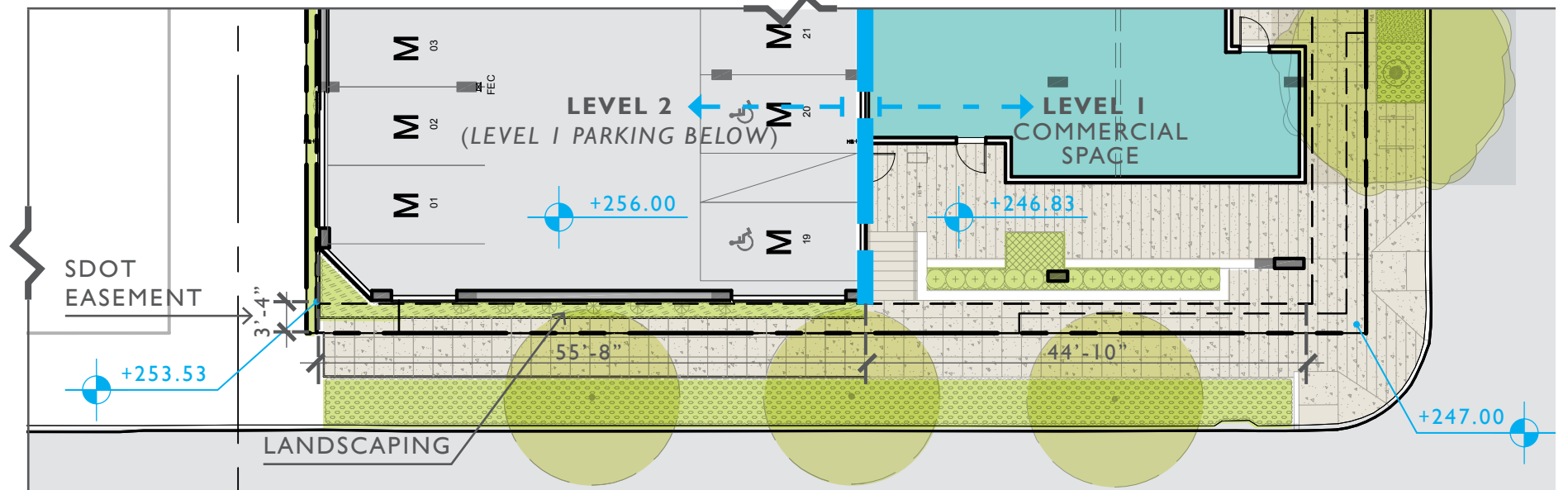
BY INTEGRATING PARKING INTO EXISTING 6-FOOT SITE-SLOPE ALONG NE 50TH ST, AS WELL AS THE EXISTING BIG LEAF MAPLE INTO THE PROJECT, THE PROPOSAL MAXIMIZES ACTIVE USE ALONG STREET FRONTAGES WITHIN GIVEN SITE CONDITIONS, ADDRESSING CS2-D-2 EXISTING SITE FEATURES .THE TWO LEVELS OF PARKING ADJACENT TO NE 50TH STREET STRADDLE THE GRADE BUT DO NOT MEET THE GRADE. TO THE WEST A COMMERCIAL CORNER IS ESTABLISHED AS THE BUILDING MEETS GRADE FOR 45% OF THE FACADE ON NE 50TH AVENUE. TO PRESERVE THE EXISTING SIGNIFICANT TREE ONSITE, A MAXIMUM OF 20% OF THE ROOT ZONE MAY BE DISTURBED PUSHING PARKING NORTH.

TO THE WEST, GRADE RELATED FLATS ARE ESTABLISHED PROVIDING LANDSCAPED STOOPS ALONG THE MAIN PEDESTRIAN THOROUGHFAIR ON 15TH AVENUE NE, SERVING THE INTENT OF *PL3 STREET-LEVEL INTERACTION*.

EFFORTS TO SAVE THE EXISTING TREE IN CONJUNCTION WITH PROVIDING ACTIVE USE OPEN SPACE ALONG THE STREET FRONTAGES SEVERELY IMPACT THE PARKING LAYOUT IN THE SOUTH PORTION OF THE SITE AND REDUCE THE PROGRAM REQUIRED PARKING YIELD.



NORTH ELEVATION STREET-LEVEL PERSPECTIVE



PARTIAL SITE PLAN

PROPOSED DEPARTURE 4A

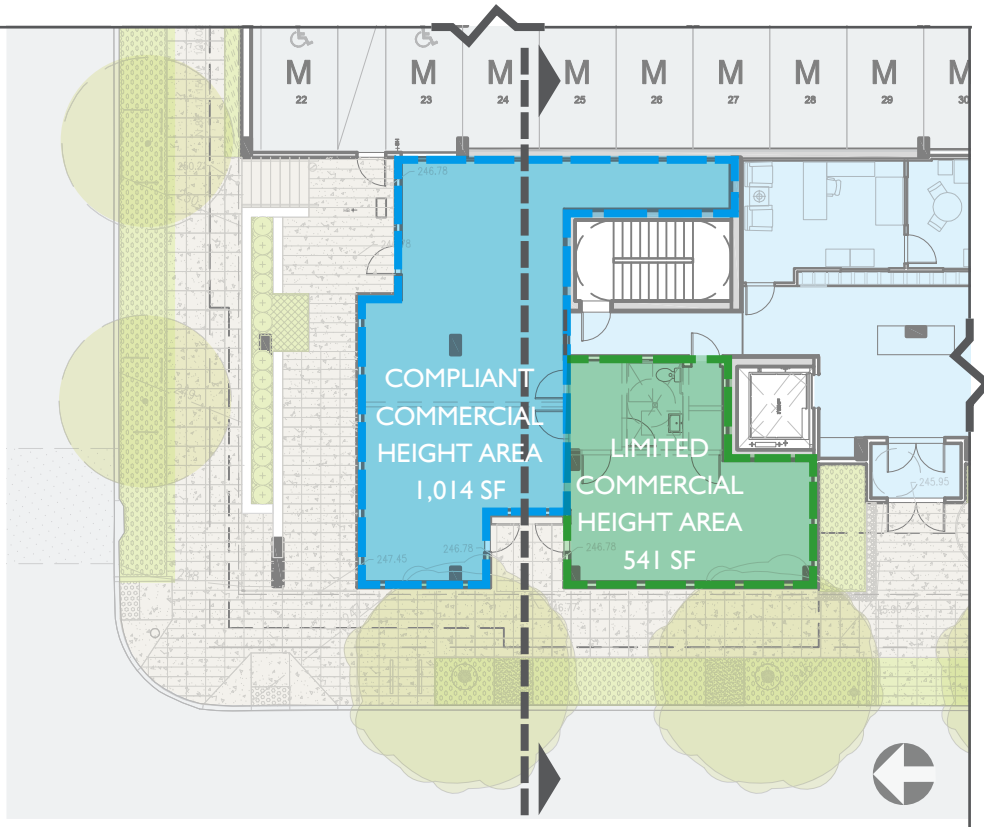
MINIMUM NON-RESIDENTIAL HEIGHT

REQUIREMENT: HEIGHT AND DEPTH PROVISIONS FOR NEW STRUCTURES OR NEW ADDITIONS TO EXISTING STRUCTURES. NON-RESIDENTIAL USES AT STREET LEVEL SHALL HAVE A FLOOR-TO-FLOOR HEIGHT OF AT LEAST 13 FEET.

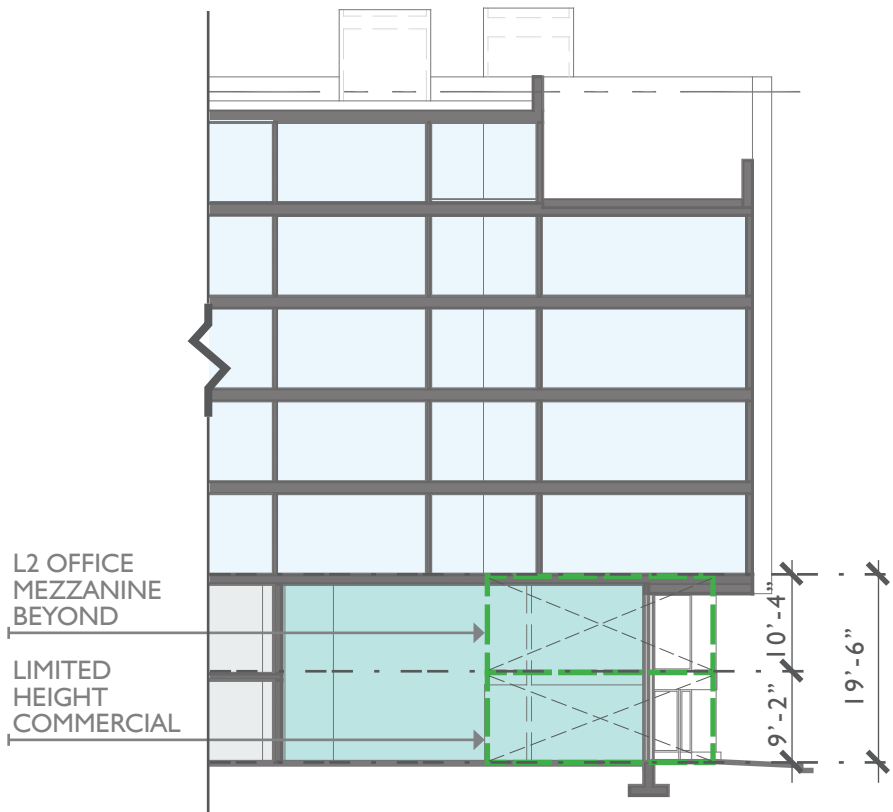
REQUEST: CONSIDER A MINIMUM COMMERCIAL HEIGHT OF 9'-2" FLOOR TO FLOOR WITHIN THE LIMITED AREA OF 35% (541 SF OUT OF 1,555 SF) OF TOTAL COMMERCIAL SPACE; 65% OF THE COMMERCIAL SPACE (1,014 SF) IS IN COMPLIANCE WITH 19'-6" FLOOR TO FLOOR HEIGHT.

RATIONALE: THE STREET-LEVEL COMMERCIAL USE IS LIMITED VIA THE PUDA TO OFFICE SPACE AND SUPPORT SERVICES FOR A RELIGIOUS FACILITY-AFFILIATED ENTITY, OR NON-PROFIT SOCIAL OR HUMAN SERVICE ORGANIZATION CONSISTENT WITH THE MISSION OF A RELIGIOUS FACILITY-AFFILIATED ENTITY. THE SPACE IS PLANNED TO ABSORB AS MANY AS THREE SEPARATE COMMERCIAL ENTITIES WITH OPPORTUNITY TO COMBINE THE COMMERCIAL SPACE FOR ONE FUNCTION TO ADDRESS POTENTIAL LIMITED MARKET OF QUALIFIED TENANTS. THE VARIED HEIGHTS WITHIN THE SPACE ALLOW FOR A HIGH FLOOR TO FLOOR OPPORTUNITY AT THE MAIN CORNER WITH A LIMITED AREA OF REDUCED HEIGHT APPROACHING THE RESIDENTIAL ENTRY. *DCI-A-3. FLEXIBILITY: BUILD IN FLEXIBILITY SO THE BUILDING CAN ADAPT OVER TIME TO EVOLVING NEEDS, SUCH AS THE ABILITY TO CHANGE RESIDENTIAL SPACE TO COMMERCIAL SPACE AS NEEDED.*

THE AVAILABLE NON-RESIDENTIAL USE SPACE HAS BEEN MAXIMIZED BY THE INCLUSION OF AN OFFICE MEZZANINE FOR COMPASS-ALLIANCE HOUSING, PROVIDER OF SOCIAL SERVICES, LOCATED ON THE SECOND FLOOR WHICH REDUCES HALF OF THE 19'-6" FLOOR-TO-FLOOR HEIGHT TO 9'-2" FLOOR-TO-FLOOR. THE 19'-6" FLOOR TO FLOOR IS MAINTAINED AT THE CORNER INTERSECTION AND ALONG NE 50TH STREET.



LEVEL I SITE PLAN



PARTIAL SECTION

PROPOSED DEPARTURE 4B

MINIMUM NON-RESIDENTIAL DEPTH

REQUIREMENT: HEIGHT AND DEPTH PROVISIONS FOR NEW STRUCTURES OR NEW ADDITIONS TO EXISTING STRUCTURES. NON-RESIDENTIAL USES SHALL EXTEND AN AVERAGE DEPTH OF AT LEAST 30 FEET AND A MINIMUM DEPTH OF 15 FEET FROM THE STREET-LEVEL STREET-FACING FACADE.

REQUEST: CONSIDER A 13'-7" MINIMUM NON-RESIDENTIAL DEPTH AT THE COMMERCIAL SPACE ADJACENT TO THE ELEVATOR CORE.

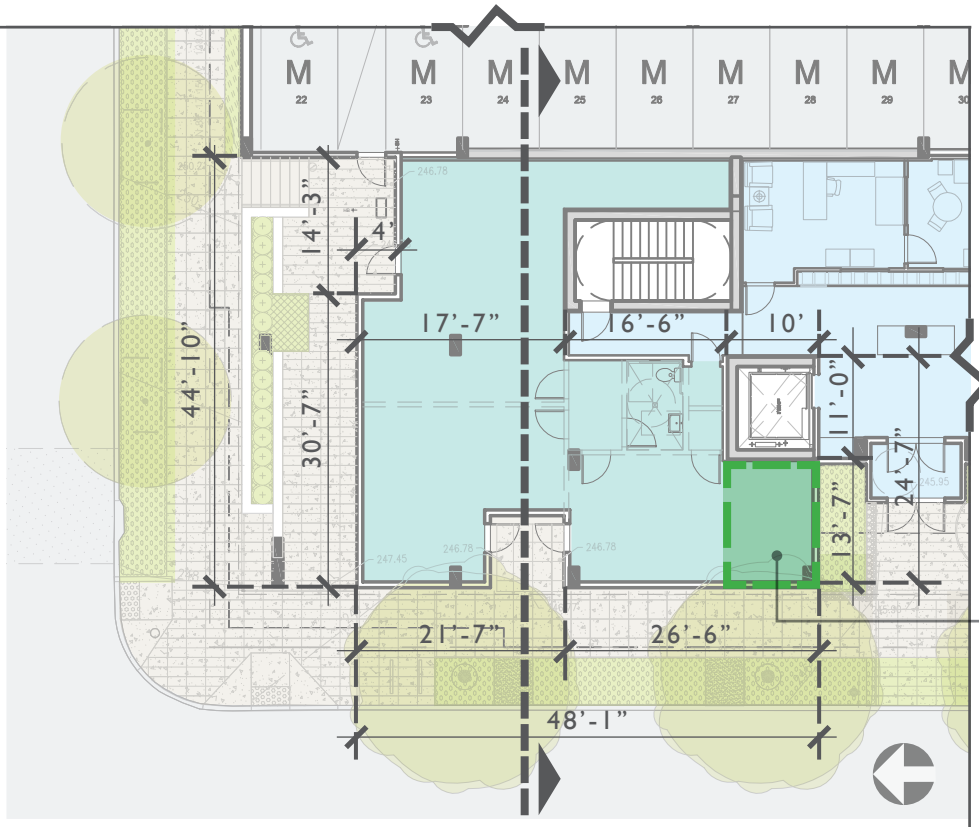
RATIONALE: CONSOLIDATING THE NON-RESIDENTIAL COMMERCIAL USE AT THE CORNER INTERSECTION ALLOWS FOR A MORE SENSITIVE TRANSITION ALONG THE LENGTH OF THE 15TH AVENUE NE FRONTAGE. CREATING A PAUSE AND LINK FROM THE COMMERCIAL CORNER TO THE GRADE RELATED RESIDENTIAL FLATS WITH THE CONTEXT MASSING RESPONSE OF ELEVATOR TOWER PLACEMENT REFLECTING THE LOCATION OF THE UNIVERSITY CHRISTIAN CHURCH TOWER ACCROSS THE AVENUE.

THE COMMERCIAL SPACE MEETS THE AVERAGE DEPTH REQUIREMENT HOWEVER IT HAS A SECTION THAT IS 13'-7" DEEP DUE TO THE BUILD UP OF ELEVATOR AND STAIR CORES AGAINST THE DOUBLE LOADED PARKING AISLE GARAGE. THE DEPTH OF THIS SPACE IS STILL VERY USEFUL TO THE COMMERCIAL OFFICE FUNCTIONS REQUIRED.

REFERENCED GUIDELINES:

DC1-A-1. VISIBILITY: LOCATE USES AND SERVICES FREQUENTLY USED BY THE PUBLIC IN VISIBLE OR PROMINENT AREAS, SUCH AS AT ENTRIES OR ALONG THE STREET FRONT.

(PL3-II HUMAN ACTIVITY)PL3-II-1. RECESSED ENTRIES: ON MIXED USE CORRIDORS, WHERE NARROW SIDEWALKS EXIST (LESS THAN 15' WIDE), CONSIDER RECESSING ENTRIES TO PROVIDE SMALL OPEN SPACES FOR SITTING, STREET MUSICIANS, BUS WAITING, OR OTHER PEDESTRIAN ACTIVITIES. RECESSED ENTRIES SHOULD PROMOTE PEDESTRIAN MOVEMENT AND AVOID BLIND CORNERS.



LEVEL I SITE PLAN

CALCULATION – AVERAGE COMMERCIAL DEPTH AT 15TH AVE:

$$44'-10'' \text{ DEPTH @ } 17'-7'' \text{ (36\% FACADE WIDTH)} = 16.14'$$

$$30'-7'' \text{ DEPTH @ } 4'-0'' \text{ (08\% FACADE WIDTH)} = 2.45'$$

$$24'-7'' \text{ DEPTH @ } 16'-6'' \text{ (34\% FACADE WIDTH)} = 8.36'$$

$$13'-7'' \text{ DEPTH @ } 10'-0'' \text{ (22\% FACADE WIDTH)} = 2.99'$$

$$\text{AVERAGE DEPTH @ } 48'-1'' \text{ (100\% FACADE WIDTH)} = 29.94' \approx 30'$$

AREA NOT MEETING 15'-0" DEPTH REQUIREMENT



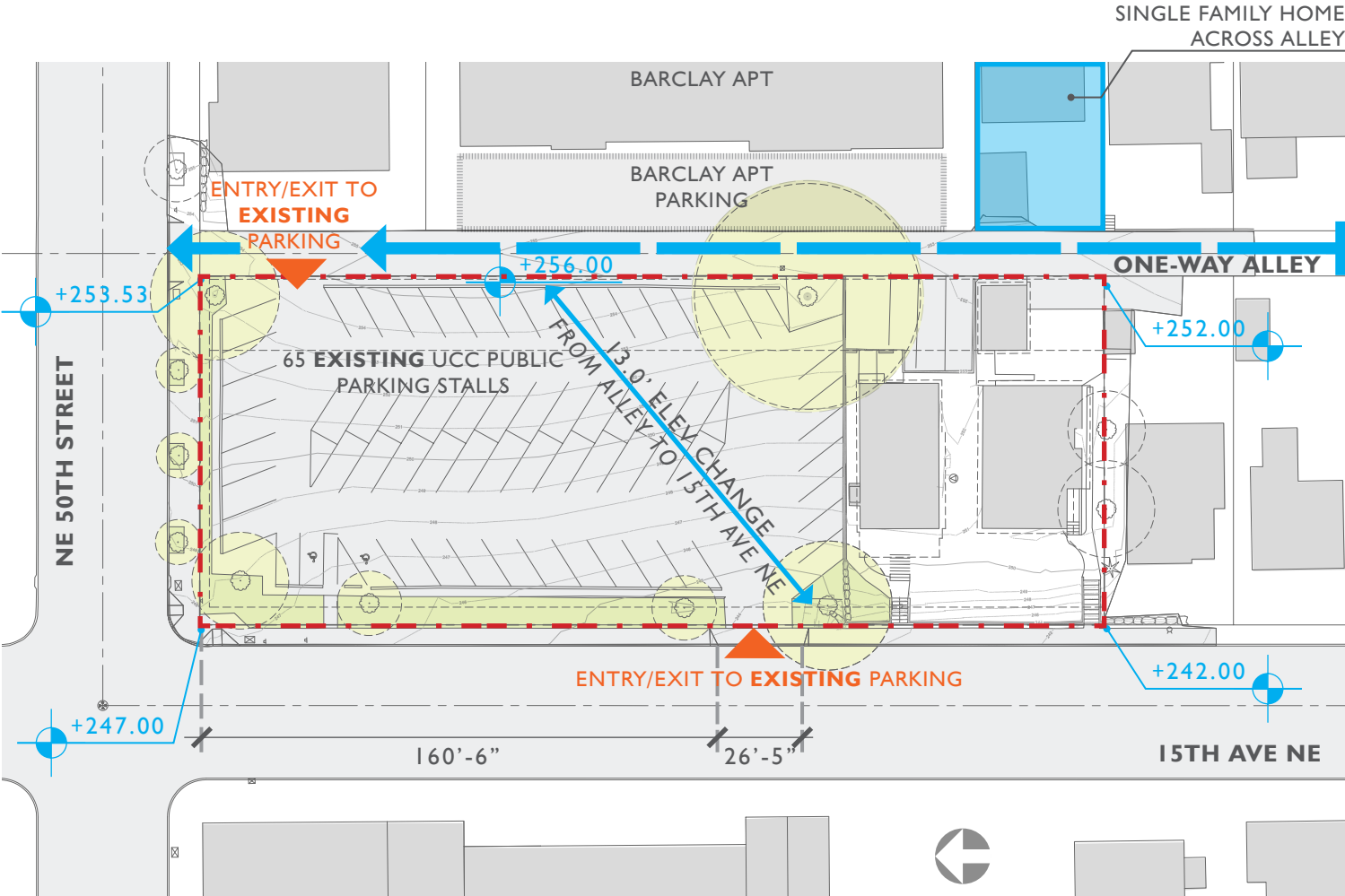
PROPOSED DEPARTURE 5

PARKING ACCESS VIA A PRINCIPAL ARTERIAL

REQUIREMENT: IN NC ZONES, THE CODE REQUIRES ALLEY ACCESS IF FEASIBLE AND THE ALLEY IS IMPROVED. BOTH APPLY HERE.

REQUEST: ALLOW A SINGLE 22' WIDE DRIVEWAY ACCESS AT THE SOUTH END OF THE 15TH AVENUE FRONTAGE WHICH IS APPROXIMATELY 73.5 FEET SOUTH OF THE 26.5' EXISTING PARKING CURBCUT AND CLOSER TO MID BLOCK.

RATIONALE: IN ADDITION TO ALLEY ACCESS SERVING THE SECOND LEVEL OF PARKING, WHICH IS RESERVED FOR RESIDENTS, THE APPLICANT PROPOSES TO ACCESS THE PUBLIC PARKING LOT ON THE FIRST LEVEL OF PARKING FROM OFF 15TH AVENUE NE. ALLEY ACCESSED PUBLIC COMMERCIAL PARKING IS NOT DESIRABLE FROM THE STANDPOINT OF PUBLIC ACCESS IN A ALLEY SERVING SINGLE FAMILY AND MULTI-FAMILY RESIDENTIAL HOMES.

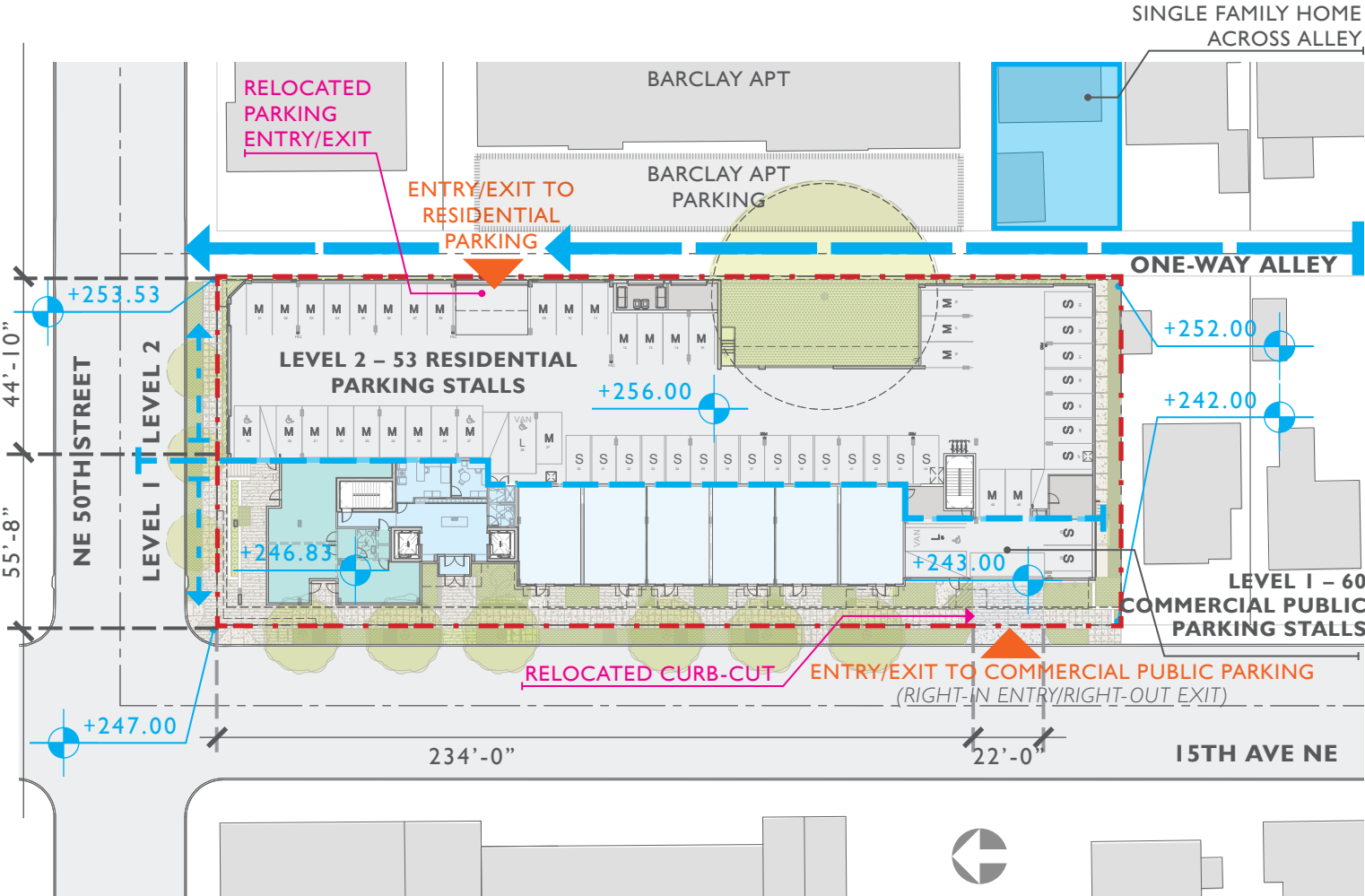


EXISTING SITE PLAN

FURTHERMORE, ALLEY ACCESSED COMMERCIAL PARKING IS NOT FEASIBLE FOR THE PROJECT DUE TO CONSTRAINTS EXPLAINED IN MORE DETAIL WITHIN THE FOLLOWING ANALYSES: LISTING OF SITE CONSTRAINTS, ALIGNMENT OF DESIGN MEETING THE INTENT OF DESIGN GUIDELINES, 15TH AVENUE NE ANALYSIS, ALLEY ACCESS CONDITIONS, AND PRECEDENT PROJECT CURB-CUTS.

THE PROJECT IS REQUESTING A SINGLE, 22'-WIDE DRIVEWAY AT THE SOUTH END OF THE 15TH AVENUE NE FRONTAGE, 73.5' SOUTH OF THE EXISTING 26.5' CURB CUT, WHICH WILL BE ABANDONED.

THE BOARD ENDORSED THE TWO POINTS OF ACCESS FROM THE ALLEY AND 15TH AT EDG AS THE DESIGN DISTRIBUTES VEHILES ON THE TWO RIGHTS-OF-WAY.



PROPOSED LEVEL 1/LEVEL 2 SITE PLAN

PROPOSED DEPARTURE 5 CONTINUED

GIVEN THE SITE AND OPERATIONAL CONSTRAINTS DISCUSSED BELOW, INTERNAL RAMPING FOR A TWO-LEVEL PARKING GARAGE IS NOT FEASIBLE BECAUSE IT (1) ELIMINATES PARKING STALLS, (2) LOWERS RESIDENTIAL YIELD, (3) DESTROYS THE GROUND-RELATED UNITS, (4) SPLITS THE COMMERCIAL LOT BETWEEN THE TWO LEVELS, (5) MIXES SOME COMMERCIAL STALLS WITH THE RESIDENTIAL STALLS

SITE CONSTRAINTS: LOT SIZE, EXCEPTIONAL TREE, ALLEY WIDTH, ONE-WAY ALLEY, LOT SLOPE; INFEASIBILITY OF 50TH ACCESS.

- WITH A COMPRESSED SITE DEPTH OF 108' THE FOLLOWING DIMENSIONAL REGULATORY REQUIREMENTS CREATE SITE CONSTRAINTS LEADING TO THE PROPOSED COMMERCIAL PARKING ACCESS: 1-FOOT ALLEY DEDICATION; SDOT 5'-6" RIGHT OF WAY EASEMENT FOR 15TH AVENUE NE; 30' RETAIL DEPTH; GRADE CHANGE ALLEY TO 15TH AVE NE; AND PRESERVATION OF THE EXCEPTION TREE ALONG THE ALLEY.
- THE PRESERVATION OF THE EXCEPTIONAL TREE SIGNIFICANTLY IMPACTS THE ALLEY FRONTAGE AVAILABLE TO PROVIDE MULTIPLE PARKING LEVEL ACCESS POINTS, TRANSFORMER VAULT, WASTE COLLECTION, AND EFFICIENT DOUBLE LOADED PARKING AISLE.
- THE ONE WAY ALLEY CREATES A HARDSHIP FOR CREATING A SUCCESSFUL AND ACCESSIBLE PUBLIC PAY PARKING GARAGE BY ELIMINATING ITS STREET PRESENCE AS WELL AS CREATING THE TEMPTATION FOR UNSAFE ILLEGAL TURNS IN AND OUT OF THE NORTH END OF THE ONE WAY ALLEY, AS CURRENTLY OCCURS.
- THE SITE SLOPES APPROXIMATELY 13 FEET WITH THE COMMERCIAL PARKING ACCESS LOCATED ON THE LOW-SIDE OF THE SITE ALONG 15TH AVE NE, WHEREAS RESIDENTIAL PARKING ACCESS IS LOCATED ALONG THE HIGH-SIDE OF THE SITE ACCESSED OFF THE ALLEY.
- GIVEN THE OPERATIONS ALONG EASTBOUND NE 50TH STREET, WITH QUEUING FROM THE 15TH AVENUE NE SIGNAL, FURTHER CONCENTRATION OF TRAFFIC ALONG THIS SECTION IS LESS IDEAL.

BUILDING/OPERATIONAL CONSTRAINTS: COMMERCIAL LOT SHOULD BE SEPARATE FROM THE RESIDENTIAL LOT; ALLEY/ROW DEDICATION; ENTRANCE TO COMMERCIAL LOT SHOULD BE OBVIOUS FROM ARTERIAL; SPLIT ACCESS REDUCES CIRCLING; INTERNAL RAMPING WOULD REDUCE PARKING YIELD; ALLOWING SPLIT ALLOWS BETTER GROUND-RELATED UNITS ON 15TH.

- THE PROPOSED PUBLIC PARKING TRAFFIC IS NOT COMPATIBLE WITH RESIDENTIAL USES ALONG THE ALLEY, AS TRAFFIC ALONG THE ALLEY HAS BEEN IDENTIFIED AS A CONCERN FROM THE NEIGHBORS, THE SPLIT ACCESS ADDRESSES THIS CONCERN.
- THE PROPOSED ENTRY LOCATION FOR COMMERCIAL PARKING INTUITIVELY SITES PUBLIC PARKING IN THE PUBLIC REALM. PUBLIC ACCESS FROM 15TH IS MORE DIRECT AND CONVENIENT FOR USERS, REDUCING THE LIKELIHOOD OF CIRCULATION THROUGH THE NEIGHBORHOOD TO ACCESS IT FROM THE ONE WAY ALLEY (NORTHBOUND ONLY).

MITIGATION: IMPROVES EXISTING CONDITION; RIGHT-IN/RIGHT-OUT; PAVING; SETBACKS

- MITIGATION AT THE 15TH AVENUE NE DRIVEWAY PROVIDES BETTER SIGHT DISTANCE THAN IS PROVIDED AT THE ALLEY CONNECTION TO 50TH STREET, RIGHT-IN AND RIGHT-OUT ONLY ACCESS, 10-FOOT SITE TRIANGLES PROVIDED ON EITHER SIDE OF THE GARAGE OPENING, ELIMINATION OF 1ST STALL SOUTH OF THE CURBCUT, AND THE SPECIAL PAVING AT DRIVE-ASILE TO INDICATE USE TO PEDESTRIANS.
- FUTURE DEVELOPMENT SOUTH OF THE PROJECT ALONG 15TH AVENUE NE IS LIKELY TO INCREASE TRAFFIC ALONG THE ALLEY. THIS PROPOSAL REDUCES FUTURE VOLUMES AT THE ALLEY CONNECTION.
- THE PROPOSAL IMPROVES THE EXISTING CONDITIONS, CREATES NO SAFETY CONCERNS, AND DOES NOT AFFECT OPERATIONS ALONG 15TH AVE NE.
- THE REMOVAL OF THE EXISTING 26.5' CURB CUT AND 65 STALL COMMERCIAL SURFACE PARKING LOT REPLACED BY THE PROPOSED 22' CURB CUT TO ACCESS A SCREENED 60-PARKING STALL COMMERCIAL PARKING STRUCTURE.



ENTRY/EXIT TO UCC PUBLIC PARKING
(RIGHT-IN ENTRY/RIGHT-OUT EXIT)

PAVING CHANGE TO INDICATE
DRIVE AISLE TO PEDESTRIANS

REQUIRED 10' SITE TRIANGLE PROVIDED

LOW PLANTINGS

10' SITE TRIANGLE PROVIDED

PROPOSED DEPARTURE 5 CONTINUED

RATIONALE FOR THE REQUEST BASED ON SERVING DESIGN GUIDELINES:

CS2-D-5	<i>RESPECT FOR ADJACENT SITES:</i>	RESPECT ADJACENT PROPERTIES WITH DESIGN AND SITE PLANNING TO MINIMIZE DISRUPTING THE PRIVACY OF RESIDENTS IN ADJACENT BUILDINGS.
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COMMERCIAL PARKING ACCESS IS PROPOSED ALONG THE COMMERCIAL FRONTAGE OF 15TH AVE NE. THIS AVOIDS INTRODUCING NON-RESIDENTIAL TRAFFIC IN THE ALLEY, LOCATING A COMMERCIAL GARAGE ENTRY OPPOSITE A SINGLE FAMILY HOME AS WELL AS PREVENTING UNNECESSARY CIRCULATION THROUGH THE RESIDENTIAL SECTIONS OF 16TH AVENUE NE AND NE 47TH STREET.

CS2-D-2	<i>EXISTING SITE FEATURES:</i>	USE CHANGES IN TOPOGRAPHY, SITE SHAPE, AND VEGETATION OR STRUCTURES TO HELP MAKE A SUCCESSFUL FIT WITH ADJACENT PROPERTIES.
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THE PROPOSED PROJECT PRESERVES THE EXISTING BIG LEAF MAPLE TREE, CREATING AN OPEN LANDSCAPED NICHE ALONG THE ALLEY. TO PRESERVE THE EXISTING SIGNIFICANT TREE ONSITE, A MAXIMUM OF 20% OF THE ROOT ZONE MAY BE DISTURBED.

THERE IS APPROXIMATELY 13 FEET OF GRADE CHANGE WITH THE HIGH-SIDE ALONG THE ALLEY AND THE LOW SIDE TO THE WEST ALONG ON 15TH AVENUE NE. THE COMMERCIAL PARKING ACCESS IS LOCATED ALONG 15TH AVE NE, ON THE LOW-SIDE OF THE SITE, WHEREAS RESIDENTIAL PARKING ACCESS IS LOCATED ALONG THE HIGH-SIDE OF THE SITE OFF THE ALLEY. WITHOUT INTERNAL RAMPING, GRADE RELATED FLATS ARE ACCOMODATED PROVIDING LANDSCAPED STOOPS ALONG THE MAIN PEDESTRIAN THOROUGHFAIR ON 15TH AVE NE, CONCEALING THE SURFACE PARKING STRUCTURE.

PL2-A-2	<i>ACCESS CHALLENGES:</i>	ADD FEATURES TO ASSIST PEDESTRIANS IN NAVIGATING SLOPED SITES, LONG BLOCKS, OR OTHER CHALLENGES.
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A 6' TOPOGRAPHY DROP ON NE 50TH STREET FROM THE ALLEY TO 15TH AVE NE IS MODULATED WITH STAIRS, RAMPS, BENCHES AND PLANTERS, ESTABLISHING A COMMERCIAL OPEN SPACE FOR PEDESTRIAN INTERACTION AND SECURE GRADE-LEVEL ACCESS TO PUBLIC PARKING, PARTICULARLY IMPORTANT FOR DAY-CARE DROP-OFFS AND SENIOR CITIZENS.

PL2-B-1	<i>SAFETY AND SECURITY, EYES ON THE STREET:</i>	CREATE A SAFE ENVIRONMENT BY PROVIDING LINES OF SIGHT AND ENCOURAGING NATURAL SURVEILLANCE.
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THE COMMERCIAL PARKING STRUCTURE IS LOCATED FOR OPEN PUBLIC USE AND ACCESS ALONG 15TH AVE NE, WITH PEDESTRIAN ACCESS ALONG THE PUBLIC CORNERS AND AVENUE OF THE PROJECT. USING SCREENING AND LOW-PLANTINGS, VISIBILITY INTO AND FROM THE PUBLIC PARKING STRUCTURE PROVIDES A SAFE AND SECURE ENVIRONMENT. LOCATING THE COMMERCIAL ACCESS OFF THE ALLEY WOULD REDUCE VISIBILTY DUE TO MINIMAL ACTIVITY OF THE RESIDENTIAL-USE 14 FOOT WIDE ALLEY.

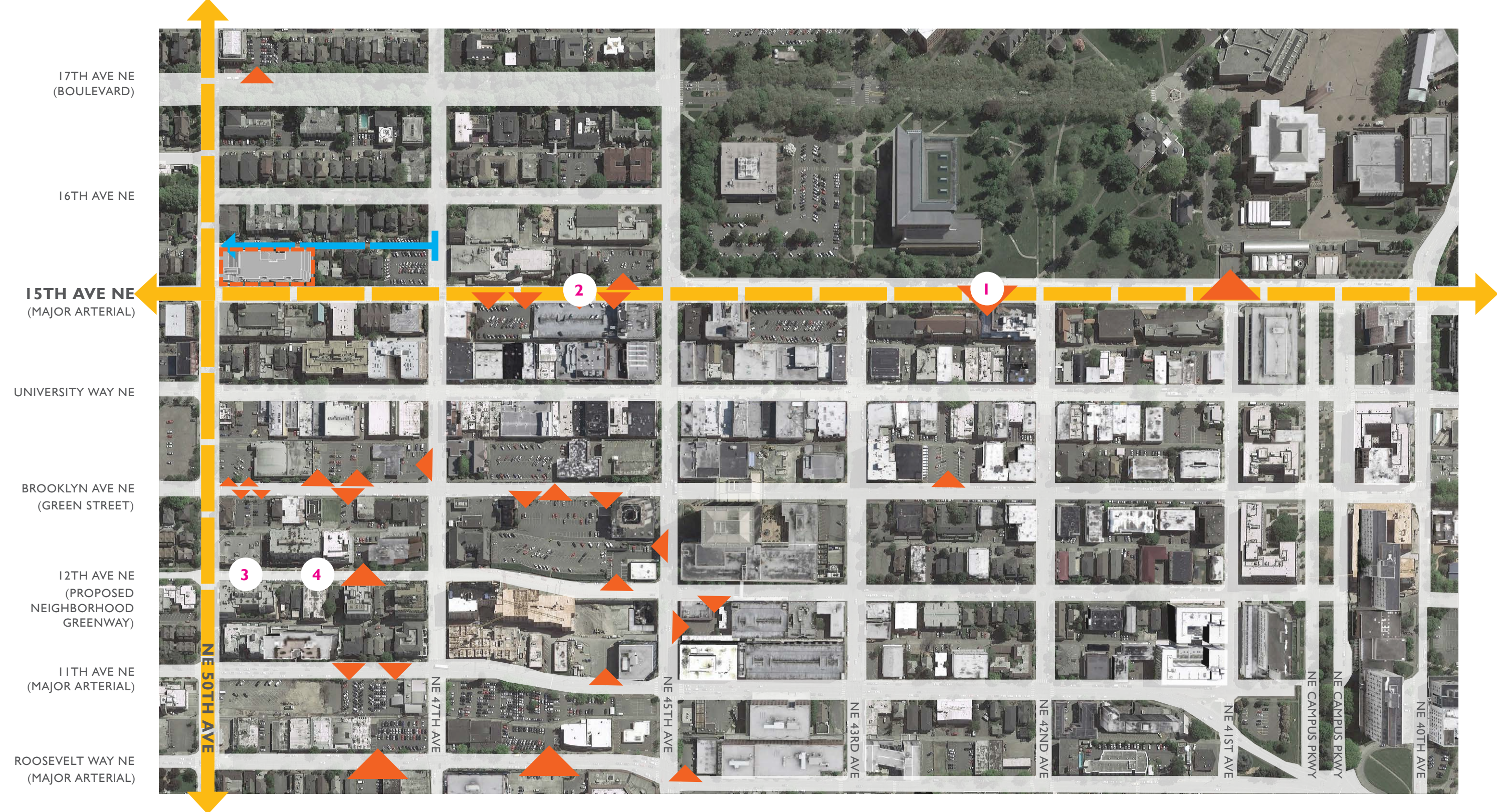
DCI-B-1	<i>VEHICULAR ACCESS AND CIRCULATION. ACCESS LOCATION AND DESIGN:</i>	CHOOSE LOCATIONS FOR VEHICULAR ACCESS, SERVICE USES, AND DELIVERY AREAS THAT MINIMIZE CONFLICT BETWEEN VEHICLES AND NON-MOTORISTS WHEREVER POSSIBLE. EMPHASIZE USE OF THE SIDEWALK FOR PEDESTRIANS, AND CREATE SAFE AND ATTRACTIVE CONDITIONS FOR PEDESTRIANS, BICYCLISTS, AND DRIVERS.
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THE PROPOSED ENTRY LOCATION FOR COMMERCIAL PARKING INTUTIVELY SITES PUBLIC PARKING IN THE PUBLIC REALM. THE MITIGATION CREATES SAFE AND ATTRACTIVE CONDITIONS PROVIDING: MID-BLOCK LOCATION, PAVEMENT CHANGES AT DRIVE AISLE, SETBACK/SIGHT TRIANGLES, RIGHT IN/RIGHT OUT TURNING ONLY.



PROPOSED DEPARTURE 5 CONTINUED

CONTEXT ANALYSIS – VEHICLE ACCESS TO PARKING



BELLWETHER UCC HOUSING

06.29.2015 | 14-034 | 55



PROPOSED DEPARTURE 5 CONTINUED

CONTEXT ANALYSIS – VEHICLE ACCESS TO PARKING *CONTINUED*

DUE TO THE INFLUENCE OF THE UNIVERSITY, THE NEIGHBORHOOD HAS VERY HIGH VOLUMES OF PEDESTRIAN, CYCLISTS, AND TRANSIT RIDERS AND IS IN SUPPORT OF THE CITY OF SEATTLE'S LAND USE POLICIES AND TRANSPORTATION INVESTMENTS FOCUSED AT DECREASING VEHICLE USE WHILE INCREASING LIVABILITY AND EQUITABILITY WHILE REDUCING ENVIRONMENTAL IMPACTS. THE UDF PARTICIPANTS HAVE STRESSED HAVING SAFE AND PLEASANT STREETS THAT SERVE ALL MODES OF TRAVEL.

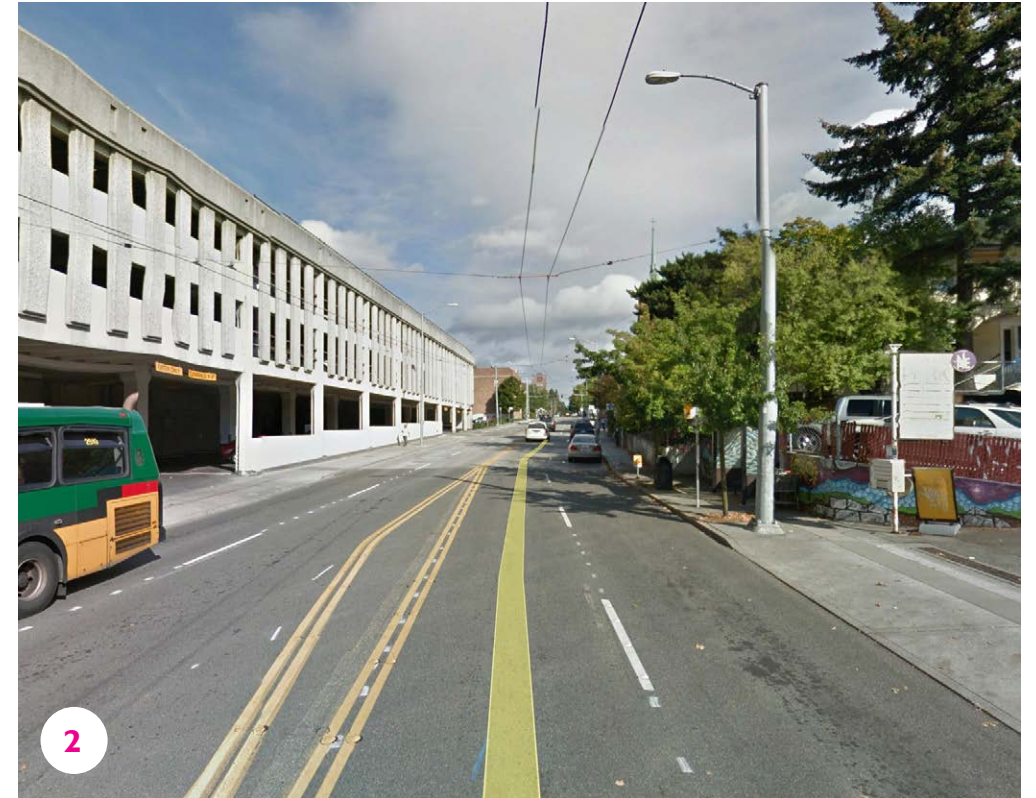
VEHICLE ACCESS IN THE NEIGHBORHOOD OCCURS VIA AVENUES, STREETS AND ALLEYS. BOTH COMMERCIAL AND RESIDENTIAL USES ARE SERVED BY SURFACE AS WELL AS STRUCTURED PARKING. ACCESS TO COMMERCIAL PARKING IS TYPICALLY VIA STREETS AND ALLEYS WHERE ACCESS TO RESIDENTIAL PARKING IS TYPICALLY VIA ALLEYS. ACCESS POINTS OCCUR ALONG NORTH-SOUTH AVENUES IRREGARDLESS OF STREET TYPE DESIGNATIONS INCLUDING; STRUCTURES ALONG 15TH AVENUE NE AT THE UNIVERSITY FRONTAGE, ALONG ROOSEVELT WAY NE AND 11TH AVENUE NE, BOTH MAJOR ARTERIALS, AS WELL AS NEW BUILDINGS ON 12TH AVENUE NE WHICH IS IDENTIFIED IN THE UDF AS A PROPOSED NEIGHBORHOOD GREEN STREET.

THE EXISTING ALLEY IS 14' WIDE AND SERVES A VARIETY OF STUDENT AND SINGLE FAMILY HOUSING AS WELL AS A HANDFUL OF FAITH COMMUNITY AND OFFICE USES WITHIN CONVERTED SINGLE FAMILY STRUCTURES. PARKING FOR THESE USES IS ACCOMMODATED BOTH WITHIN STRUCTURES, ON SURFACE LOTS AS WELL AS DENSELY PARKED ALONG THE ALLEY IN DIAGONAL, PERPENDICULAR AND PARALLEL CONFIGURATIONS. ADDITIONALLY, CIRCULATION ALONG THE ALLEY IS ONE WAY IN A NORTHBOUND DIRECTION. THIS TRAFFIC CIRCULATION PATTERN IS COORDINATED WITH TRAFFIC CIRCULATION ALONG THE STREETS TO FACILITATE MOBILITY WITHIN THE HIGH TRAFFIC VOLUME AREAS AROUND THE UNIVERSITY. ACCESS TO THE LARGE SURFACE PARKING LOT AT THE SOUTH END OF THE BLOCK IS VIA NE 47TH AVENUE. A ONE-FOOT DEDICATION IS REQUIRED OF THE SITE IN AN EFFORT TO INCREASE THE WIDTH CLOSER TO THE REQUIRED 16' WIDTH. THESE CONDITIONS ARE COMMON AT ALLEYS IN THE SURROUNDING VICINITY.

THE PROPOSAL INCLUDES ACCESS FOR COMMERCIAL USES VIA 15TH AVENUE NE AND FOR RESIDENTIAL USES VIA THE ALLEY.



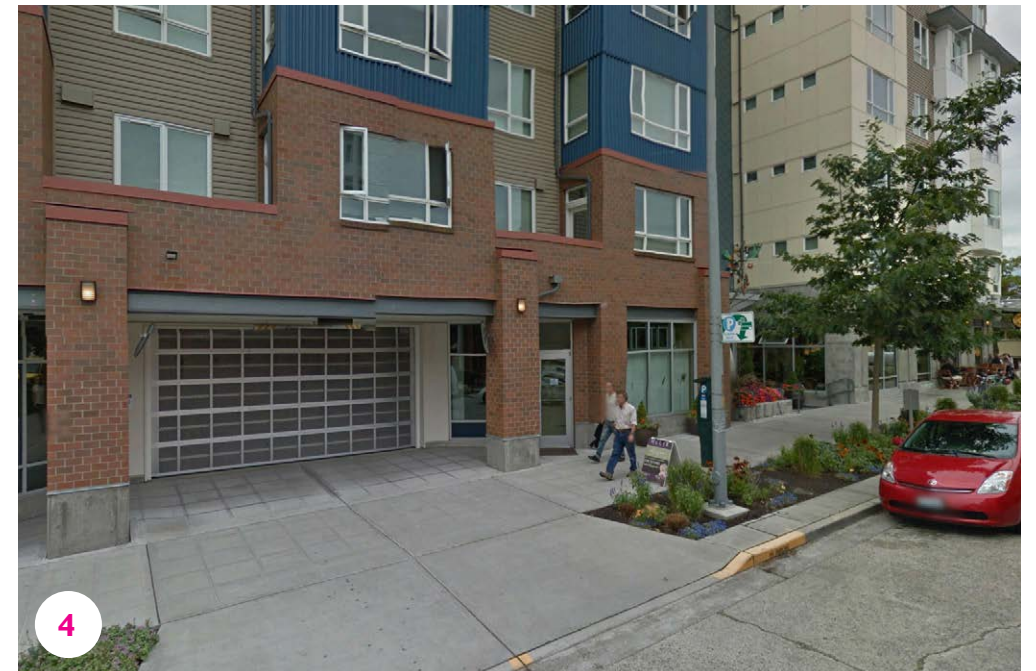
GEORGE F. RUSSEL JR HALL – CURB CUT PARKING ENTRY OFF NE 15TH AVE



PARKING GARAGE ON 15TH AVE NE & 42ND ST



WALGREENS OFF NE 12TH AVE AND NE 50TH ST – PARKING GARAGE CURB CUT ENTRY



HELIX APARTMENST OFF NE 12TH AVE AND NE 50TH ST – PARKING GARAGE CURB CUT ENTRY

PROPOSED DEPARTURE 5 CONTINUED

CONTEXT ANALYSIS – ALLEY VEHICLE ACCESS TO PARKING

ALLEY ACCESS CONS:

- MOVES ALL SITE TRAFFIC TO THE ALLEY, 16TH AVENUE, AND 47TH STREET AWAY FROM A PRINCIPAL ARTERIAL AND MAJOR TRANSPORTATION STREET.
- CREATES POTENTIAL CONFLICT WITH MIXED RESIDENTIAL AND COMMERCIAL TRAFFIC IN ALLEY.
- INCREASES VOLUME AND FREQUENCY IN THE ALLEY DUE TO COMMERCIAL USE.
- ADDS COMMERCIAL VEHICLE TRAFFIC TO A HIGH PEDESTRIAN/RESIDENTIAL USE ALLEY.
- LOCATING TWO GARAGE ACCESS POINTS AT HIGH AND LOW ELEVATIONS OF SITE ALLEY FRONTAGE ELIMINATES THE NEED FOR INTERNAL RAMPING WHICH IS OTHERWISE NOT FEASIBLE ON THIS SITE DUE TO THE SHALLOW PARCEL DEPTH AND TOPOGRAPHY.
- THE DRIVEWAY LOCATION AT THE LOW POINT OF THE ALLEY IS DIRECTLY ACROSS FROM A SINGLE FAMILY RESIDENCE.
- THE EXISTING NARROW ALLEY REDUCES PEDESTRIAN CIRCULATION SPACE WITHIN THE ALLEY.
- THE EXISTING ONE WAY ALLEY CIRCULATION, INTENDED TO ADDRESS VEHICLE TURNING RADIUS AT THE ENTRY AND EXIT POINT CREATE INCREASED PEDESTRIAN VEHICLE CONFLICT.
- UCC BOUND VEHICLES MOVING FROM AND TO THE WEST THROUGH THE ALLEY WILL RESULT IN TEMPTATION FOR ILLEGAL LEFT TURNS ONTO NE 50TH ST AT THE NORTH END OF THE ALLEY CREATING VEHICLE CONFLICTS.
- THE NARROW ALLEY WIDTH RESTRICTS THE REQUIRED COMMERCIAL TURNING RADIUS AND PUSHES THE RADIUS ONTO THE SITE.
- THE UCC CONGREGATION ELDERS HAVE DECREASED VISION AND DEXTERITY WHICH WILL BE CHALLENGED BY THE ALLEY WIDTH AND PEDESTRIAN ACTIVITY.
- AN ACCESS POINT TO A PUBLIC PAY PARKING LOT WILL BE DIFFICULT TO SIGN WHEN CIRCULATION TO THE ACCESS POINT IS LOCATED OFF SITE VIA THE SOUTH ALLEY ENTRY.
- ALLEY FRONTAGE IS CHALLENGED BY THE SLOPED TOPOGRAPHY, EXISTING UTILITY POLES AND EXCEPTIONAL TREE, ADDING MULTIPLE GARAGE ACCESS POINTS EXACERBATES THIS CONDITION.

ALLEY ACCESS PROS:

- MOVES ALL SITE TRAFFIC TO THE ALLEY AWAY FROM A PRINCIPAL ARTERIAL AND MAJOR TRANSPORTATION STREET.
- ELIMINATES POTENTIAL OF VEHICLE AND PEDESTRIAN CONFLICTS.



ALLEY FROM SITE



ALLEY SOUTH OF SITE



ALLEY SOUTH OF SITE

PROPOSED DEPARTURE 5 CONTINUED

PRECEDENT CURB-CUT PROJECT ANALYSIS

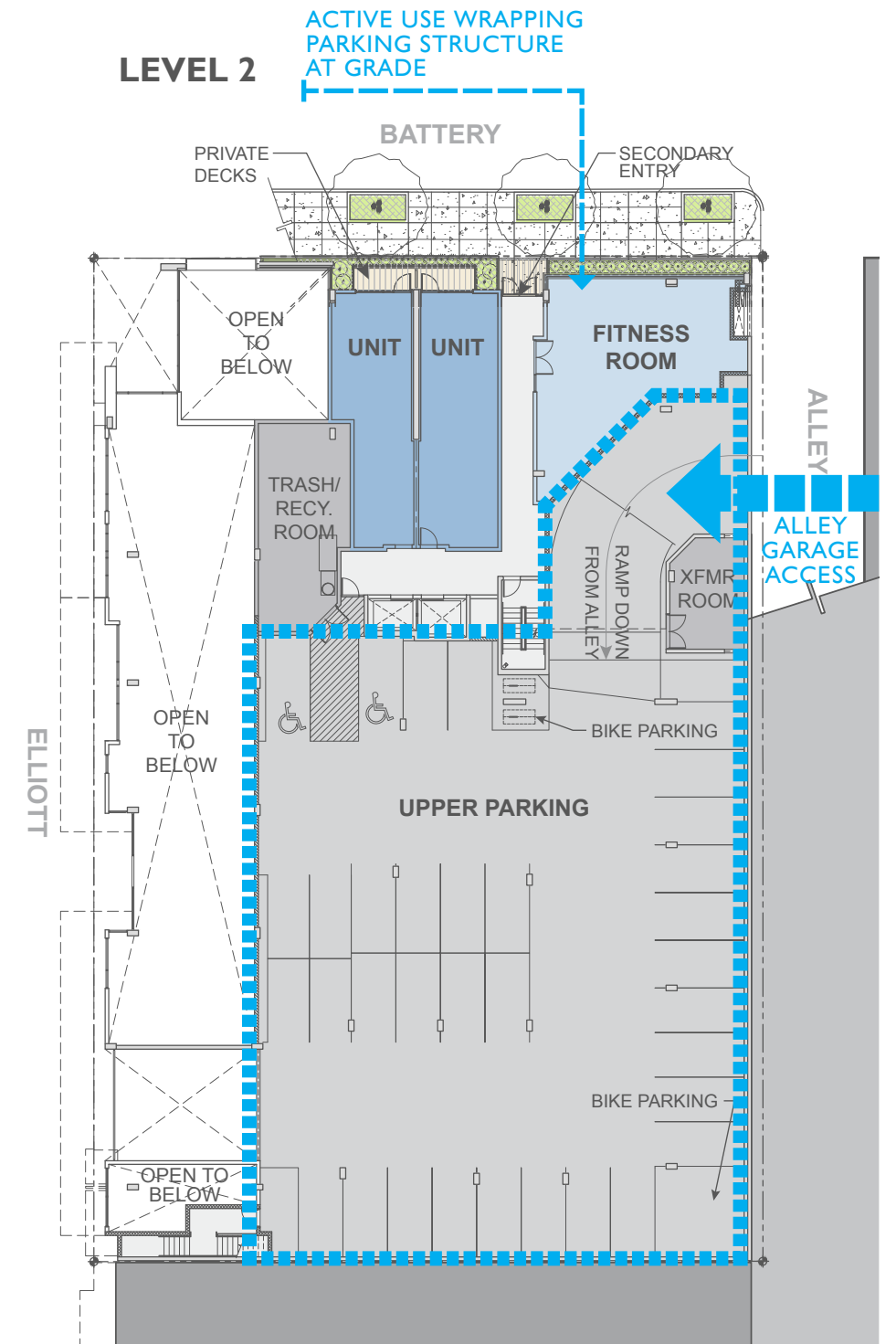
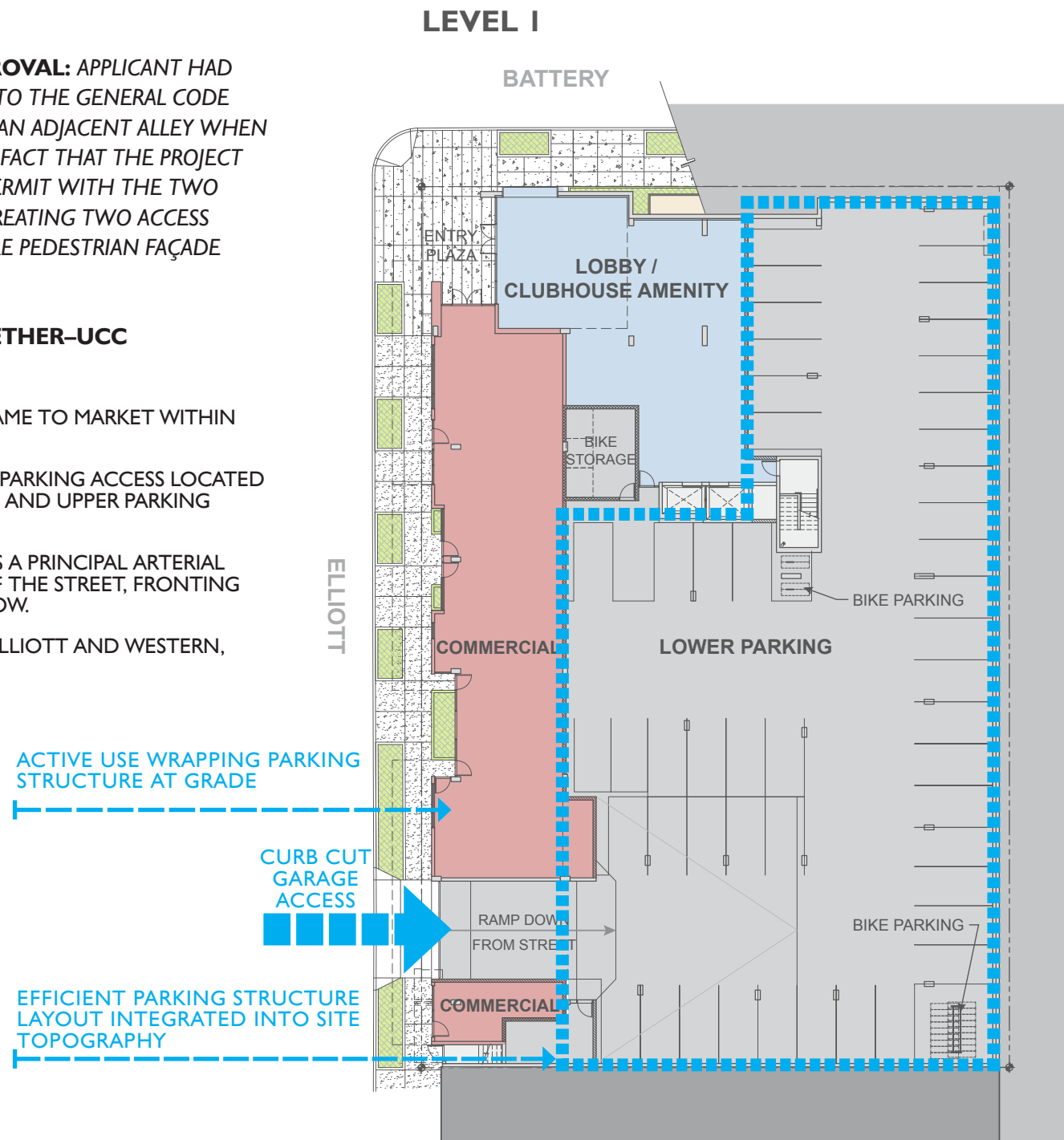
ARTHOUSE – 2334 ELLIOTT AVENUE

DPD#3012499

MUP CITATION REGARDING CURB-CUT APPROVAL: APPLICANT HAD RECEIVED APPROVAL FROM DPD FOR AN EXCEPTION TO THE GENERAL CODE PROVISION REQUIRING VEHICLE ACCESS ONLY FROM AN ADJACENT ALLEY WHEN ONE IS PRESENT. THE APPROVAL WAS BASED ON THE FACT THAT THE PROJECT HAD ALREADY OBTAINED A STREET IMPROVEMENT PERMIT WITH THE TWO ACCESS DRIVEWAY SCHEME APPROVED, AND THAT CREATING TWO ACCESS POINTS ALLOWED THE PROJECT TO WRAP THE ENTIRE PEDESTRIAN FAÇADE WITH ACTIVE USES, INSTEAD OF PARKING GARAGE.

SIMILAR CONDITONS TO PROPOSED BELLWETHER-UCC APARTMENTS:

- A RECENTLY DEVELOPED PROJECT, ARTHOUSE CAME TO MARKET WITHIN THE LAST YEAR
- TOPOGRAPHICAL CONDITIONS ON SITE, LOWER PARKING ACCESS LOCATED ALONG ELLIOTT AVENUE AT LOWER GRADE LEVEL, AND UPPER PARKING LOCATED AT ALLEY WITH HIGHER GRADE LEVEL.
- SIMLIAR TO 15TH AVENUE NE, ELLIOTT AVENUE IS A PRINCIPAL ARTERIAL WITH A STRIPED BIKE LANE ON THE EAST SIDE OF THE STREET, FRONTING THE PROJECT WHEREAS 15TH AVE NE IS A SHARROW.
- BATTERY IS ONLY AN ACCESS STREET BETWEEN ELLIOTT AND WESTERN, WHEREAS NE 50TH IS A COLLECTOR ARTERIAL.
- NO EXISTING TREES PRESERVED



PROPOSED DEPARTURE 5 CONTINUED

PRECEDENT CURB-CUT PROJECT ANALYSIS

1420 EAST MADISON STREET

DPD#3013776

RECOMMENDATION CITATION REGARDING CURB-CUT APPROVAL:

ACCESS FROM E. MADISON ST. RESULTS IN A PROJECT COMPATIBLE WITH EXISTING DEVELOPMENT AND MINIMIZES POTENTIAL IMPACTS BY PLACING ALL PARKING BELOW GRADE. THE ONLY

VIABLE VEHICULAR ENTRANCE TO THE SITE WAS AT THE SW CORNER, NECESSITATED BY THE SITE'S 14'+ SLOPE, MAKING ACCESS INFEASIBLE FROM THE NW CORNER (PER EDG). TRAFFIC ANALYSIS

INDICATES MINIMAL TRAFFIC IMPACT FOR A DRIVEWAY AT THE MADISON LOCATION. TRASH/RECYCLING PICK UP WILL BE LOCATED ON 15TH AVE. THE PROPOSED ACCESS MINIMIZES IMPACTS ON THE PARAMOUNT APARTMENTS, ALLOWING THE ADJACENT WALKWAY/UTILITY ACCESS TO BE

DESIGNED IN A PEDESTRIAN-FRIENDLY MANNER. PROPOSED ACCESS ELIMINATES 7 ABOVE GRADE PARKING STALLS, REDUCING THE WESTERN BLANK WALL FROM 24' TO 12' HIGH.

SIMILAR CONDITIONS TO PROPOSED BELLWETHER-UCC APARTMENTS:

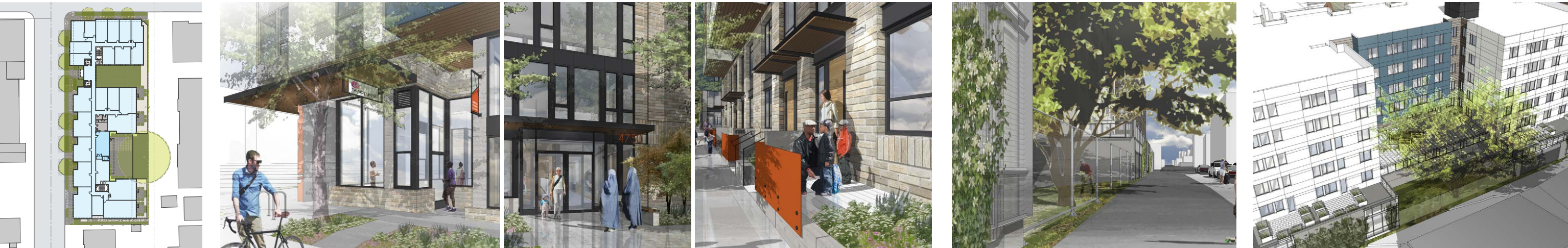
- TOPOGRAPHICAL CONDITIONS ON SITE WITH PARKING ACCESS AT LOWEST GRADE LEVEL ONSITE ALONG E. MADISON STREET
- MADISON STREET IS A PRINCIPAL ARTERIAL SIMILAR TO 15TH AVE NE. HOWEVER, UNLIKE THE ZONING AT THE BELLWETHER UCC SITE, THE ZONING AT EAST MADISON HAS A PEDESTRIAN DESIGNATION. THE DRB APPROVED THE CURB CUT ACROSS A MORE ACTIVE PEDESTRIAN STREETScape EVEN THOUGH SDOT OPPOSED THIS CONDITION FOR 1420 EAST MADISON STREET.
- NO EXISTING TREES PRESERVED



DESIGN GUIDELINES IDENTIFIED IN EDG_____

DESIGN GUIDELINES

TITLE	DESCRIPTION	DRB GUIDANCE AT EDG 2	RESPONSE
CS-1 NATURAL SYSTEMS AND SITE FEATURES	<p>Use natural systems and features of the site and its surroundings as a starting point for project design.</p> <p>RELATED UNIVERSITY SUPPLEMENTAL GUIDANCE:</p> <p>II. Landscape Design to Address Special Site Conditions</p> <p>i. Existing Trees</p>	<p>The Board preferred option 1A, which preserves the exceptional Big Leaf Maple. The proposal is not vastly improved by the non-preservation option. The applicant will need to make the open space work with the existing tree. Preservation of the tree will go a long way to making this development proposal sensitive to its surrounding context.</p>	<p>The Big Leaf Maple Tree has been incorporated into the project as a feature element and will be preserved. The south family focused building wraps around the existing tree, forming an elevated courtyard that holds play equipment for the residents’ children with visual access and proximity to two and three bedroom units. As discussed at the second EDG, the cherry tree does not meet the criteria for exceptional tree status and is near the end of its healthy maturity, see arborist report submitted with MUP, and will deteriorate over the next 5-10 years as has occurred with cherry trees planted throughout the University Christian Church property. The corner of NE 50th St. and 15th Ave NE has been developed as an active pedestrian open space with circulation to multiple uses within the proposed project, landscaping, seating areas as well as a legacy art piece to commemorate the UCC’s contribution to the development.</p>
CS-2 URBAN PATTERN AND FORM	<p>Strengthen the most desirable forms, characteristics, and patterns of the streets, block faces and open spaces in the surrounding area.</p> <p>RELATED UNIVERSITY SUPPLEMENTAL GUIDANCE:</p> <p>A. Location in the City and Neighborhood</p> <p>1. Sense of Place</p> <p>2. Architectural Presence</p> <p>RELATED UNIVERSITY SUPPLEMENTAL GUIDANCE:</p> <p>II. Respect for Adjacent Sites</p> <p>i. Zoned Edge Areas</p>	<p>The diagrams illustrate the proposal’s potential sensitivity toward the church’s massing, by integrating the datum lines and creating an axial relationship with the tower. The design’s evolution will need to create viable and high quality street edges along 15th Ave. and 50th St. At the Recommendation meeting, the street edges should be represented in high detail. There should be viable soft edges along these rights of way.</p> <p>Preservation of the Big Leaf Maple and the quality of the large open space surrounding the tree will help ensure a sensitive transition to the neighborhood across the alley. The Board’s departure decision rests on the preservation of the tree and the quality of the open space surrounding it.</p>	<p>High quality street edges have been developed in response to the surrounding building variety using pedestrian scaled design elements and materials at the street edge including brick veneer and detailing, window pairings and verticality, accent paving at the commercial corner, residential lobby entry and public parking entry, bike and bench street furnishings, varied landscaping, eddies created by grade-related residential stoops, and panel screening texture which also provides visual access to public parking.</p> <p>The Big Leaf Maple tree has been preserved, creating an open landscaped niche along the alley with open visual access into the parking structure, providing a safe environment within the parking structure as well as along the alley by means of visual access. The landscaped grade surrounding the base of the tree also provides supplemental secure landscaped open space for residential use. The south, family-focused building wraps around the existing tree, forming a large elevated courtyard that accommodates play equipment for the residents’ children with visual access and proximity to two and three bedroom units. This courtyard as well as the deep courtyard at the tower element provide greater variety of relief from the lower intensity zone across the alley than as required by the PUDA.</p>



DESIGN GUIDELINES

	TITLE	DESCRIPTION	DRB GUIDANCE AT EDG 2	RESPONSE
CS-2	URBAN PATTERN AND FORM	<p>RELATED UNIVERSITY SUPPLEMENTAL GUIDANCE:</p> <p>IV. Height, Bulk and Scale</p> <p>i. Reduce Visual Bulk</p>	<p>In general, the diagrams explaining the rationale behind the massing concept were well received. The bays facing 15th Ave appear over scaled. The architect must be cognizant of how materials, detailing and composition can reduce their scale to provide greater sense of intimacy and fine grain to match other buildings along the avenue.</p>	<p>Materials reinforce the overall composition and reduce scale through brick base, bay modules and panel patterning. Further development of the massing along 15th Ave has been made to reflect a more frequent bay module that reflects modules from the UCC structure across the Avenue; previously presented in the second EDG meeting as three bays now has five bays. Inset bays create relief from the longer façade in form and in a varied plank pattern texture to provide pedestrian scaled detail. Added scale identifying base, middle, and top facade zones is achieved through setbacks at the lower two floors defining grade-related units, a horizontal woven pattern of panels and two floor grouping of windows on the bays at middle floors, and a single floor expression of top floor windows</p>
CS-3	ARCHITECTURAL CONTEXT AND CHARACTER	<p>Contribute to the architectural character of the neighborhood.</p> <p>RELATED UNIVERSITY SUPPLEMENTAL GUIDANCE:</p> <p>I. Architectural Elements and Materials</p> <p>i. Incorporate Local Architectural Character</p>	<p>The Board praised the applicant's exercise into diagramming the contextual design cues. The next step for the architect requires development of the elevations (their materials, composition and detailing) to engage with the neighborhood context in a sympathetic manner. The Board emphasized the desire for high quality materials both for long term maintenance and to match the quality of materials imbued in the nearby structures. See Board guidance for DC4-I remarking on the potential risk of overusing cementitious panels.</p>	<p>Refinements of design elements and materiality throughout all facades of the structure have been made, with a focus to pull in neighborhood character. Key design elements have been identified to convey special attention to the project context; the NW feature corner has a reduced height creating an exterior terrace on Level 7, while utilizing high grade horizontal textured cement panels as infill between the "window wall effect" composition of openings, relating to the scale of the adjacent building at the NW corner and breaking down the overall scale of the building composition; The gasket or interior walkway between the North and South buildings is composed using a modular patterned "window wall effect" and flanked with a patterned brick tower reflecting the UCC tower across 15th Ave; The commercial corner provides two-story visual access into the commercial space with detail in brick, storefront glazing and with high soffits clad in slats evoking the warmth of wood; Grade-related stoops fold in the neighborhood and collegiate context using a woven pattern of rhythmic modules of brick, metal and patterned windows along the facade of the setback structure fronted with a 7' deep landscaped platform tucked under the undulating bays 19'-6" above with soffits clad in slats evoking the warmth of wood as steps and ledges unfold to meet the pedestrian edge and create an intimate series of eddies for pedestrian interaction.</p>



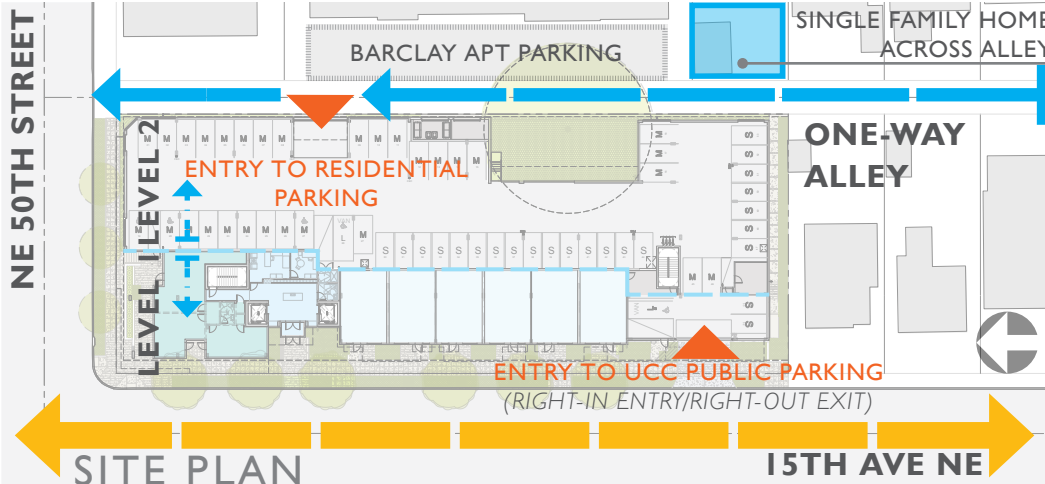
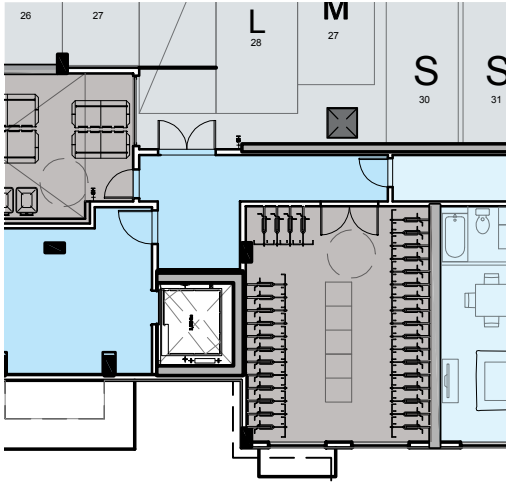
DESIGN GUIDELINES

	TITLE	DESCRIPTION	DRB GUIDANCE AT EDG 2	RESPONSE
PL-1	CONNECTIVITY	Complement and contribute to the network of open spaces around the site and the connections among them.	<p>Several times during the EDG proceedings the Board mentioned the importance of developing the 15th Ave edge to provide a continuity of landscaping that provides interesting incidence and visual eddies that act to reduce the 280 linear feet along the right of way.</p> <p>See Board guidance for PLI-A. The streetscape should provide the discrete garden rooms and linear plantings that extend along the University District rights of way.</p> <p>Use the guidance on active, ground-level open space to inform the landscape design.</p>	<p>A series of open spaces have been established surrounding the structure including a continuous seven-foot deep landscaped platform along NE 15th Ave. with integrated semi-public defensible stoops to each of the individual grade related residential units extending south to mid block. The seating walls of the stoops provide opportunities for pedestrian interaction and eddies of activity. Moving north, this platform of landscaping meets grade and connects to the main residential entry's fourteen foot deep landscaping that frames the modular glazed portion of the vertical gasket between the north and south structures. The commercial corner is sculpted away providing an open space for pedestrian interaction as well as an integrated art element design in collaboration with UCC as a legacy element speaking to the project's partnership. The structure is set back from the property line by 15' to 19'-4" along the West portion of the NE 50th St. facade, creating a landscaped edge between the steep topography change towards the alley from the corner NW, and allowing for a landscaped edge providing pedestrian access to the commercial spaces. Along the Alley, 80 percent of the root zone of the existing Large Leaf Maple is left undisturbed and provides landscaped relief within the predominantly paved alley. Smaller areas of setback along the alley accommodate pockets of vines and shrubs to soften the garage walls. The structure is set back along the South facade eight feet providing a landscape transition to the adjacent property and NE 15th Ave ROW.</p>
PL-2	WALKABILITY	Create a safe and comfortable walking environment that is easy to navigate and well-connected to existing pedestrian walkways and features.	<p>Safety and security issues in the alley were not discussed at the meeting. It is important that designer remain cognizant of these issues as the design evolves.</p> <p>The subject of overhead weather protection did not receive consideration. Plantings between the building and the sidewalk should not be covered by projecting bays or canopies.</p>	<p>Elevated open space at the active and passive courts as well as four private terraces are located overlooking the alley to provide "eyes on the alley" for security. Additionally, with the open landscaped niche along the alley and where grade/ structure allows, open visual access has been provided into and from the parking structure further ensuring a safe environment with additional "eyes on the alley."</p> <p>Overhead weather protection is provided at commercial entries, the main residential entry and individual residential stoops through a variety of canopies reflecting scale and use. Undulating bays along the west facade of NE 15th Ave. randomly overhang the grade related residential landscape stoops. The overhang is vertically separated by nineteen feet six inches and includes three projecting portions approximately seven feet deep by twenty-four feet wide. Landscape in this area will be irrigated and drought tolerant as required by code.</p>



DESIGN GUIDELINES

TITLE		DESCRIPTION	DRB GUIDANCE AT EDG 2	RESPONSE
PL-3	STREET LEVEL INTERACTION	Encourage human interaction and activity at the street level with clear connections to building entries and edges.	Privacy of the residents was not discussed. However, as the design evolves, the applicant should ensure the privacy of the residents across the alley as well as those tenants in the proposed building who will overlook the alley and the neighbors.	Development along the alley has changed greatly since the first EDG and no longer includes off-alley parking greatly reducing privacy issues. Significant horizontal separation, equaling that of many ROW's, is provided between the proposed structure and existing apartments further enhancing resident privacy. Approximate dimensions along the alley where the structure pushes towards the alley that are not within the sections of active and passive courtyards are as follows: 58'-4" from residential unit to across the alley Barclay Apartments, 68'-0" to the single family residence, 41'-0" adjacent to the Royal Manor Apartments. Fenestration of the proposed structure averages 24% whereas the fenestration at the Barclay Apartments across the alley measures approximately 26.6% glazing area.
		RELATED UNIVERSITY SUPPLEMENTAL GUIDANCE: II. Human Activity	The Board noted that the quality of both the landscaping and the building along the alley are critical to achieving a sensitive scale in relationship to the neighbors.	See response to PLI-A for description of open space and relief along the 15th Ave streetscape. At the alley the landscaped courtyard created by the existing Big Leaf Maple provides relief to the heavily hardscaped and structured alley. Additional landscape is incorporated along parking garage walls, which are treated as areas both solid and screened, allowing for light and air access into the parking structure without the intrusion of cars and mechanical spaces behind the screening.
PL-4	ACTIVE TRANSPORTATION	Incorporate design features that facilitate active forms of transportation such as walking, bicycling, and use of transit.	Discussion among the Board members did not focus on this issue.	Bike storage has been located at the Level 2 residential parking lobby for ease of access and operations for bicycle commuters and family bicycle/stroller storage.
DC-I	PROJECT USES AND ACTIVITIES	Optimize the arrangements of uses and activities on site.	The preferred commercial parking access via 15th has been set back and landscaped so not to dominate the pedestrian streetscape and provide space for pedestrians and vehicle awareness. This location reduces the impact of parking on adjacent residential uses and examples of similar commercial access via the arterial can be found in surrounding blocks.	On alley parking has been eliminated from the proposed development with all parking being accommodated within the structure eliminating conflicts with pedestrians and vehicles as well as providing better alley visibility in response to CEPTD principals. The two points of parking access have been maintained in this proposal to better distribute residential and public pay parking and respect use of the alley by pedestrians and adjacent residential properties. The alley facade composition incorporates metal screens to provide additional "eyes on the alley". Security fencing will enclose the landscape courtyard below the Big Leaf Maple to further enhance alley security.



DESIGN GUIDELINES

TITLE		DESCRIPTION	DRB GUIDANCE AT EDG 2	RESPONSE
DC-2	ARCHITECTURAL CONCEPT	Develop an architectural concept that will result in a unified and functional design that fits well on the site and within its surroundings.	<p>The Board reiterated its concerns from the first EDG meeting. The overall organization of the massing along 15th Ave includes a corner volume and a larger modulated volume separated by a gasket. This proposed arrangement has the potential to reduce the projects overall scale if the elaboration during design development continues to reduce the building' scale through the adept use of materials (and their texture), compositional arrangement of the fenestration, detailing and even ornament.</p> <p>Preservation of the exceptional tree helps reduce the building mass along the alley by providing a large void in the façade. The two open spaces facing the alley allow portions of the building to exceed the 30 feet setback stipulated by the PUDA.</p> <p>Extensive blank walls along any of the facades would not be welcome by the Board.</p>	<p>As mentioned in section CS-3 under Architectural Context and Character, the project has been massed using specific feature elements to break down the scale of the building and create modulations and interest. The commercial corner is sculpted back creating public open space, allowing light and access into the nonprofit commercial spaces with the textures of overhead slat soffits, brick special paving, and legacy panel. The NW feature corner above is a lowered corner element that relates to the surrounding massing at the NW corner of 15th and NE 50th. This provides a backdrop for an isolated section of high grade textured panel against the backdrop of a rich dusty blue slate field of the North building pulling in a typical single residential family home color palette within the context. The dynamic composition of dark-grey framed openings are further defined with dark-grey base panels below the opening, reflecting more frequent openings as the North building is comprised of compact residential units. The gasket connecting the north and south building provides a dynamic transition between the two structures accented by window patterns and the brick tower that ties into the grade-related residential composition below, reflecting the character of the UCC tower landmark. The South building form is defined by the family-unit composition surrounding the existing Big Leaf Maple, and takes on a more formal characterization with a woven pattern of material and color reflecting the datum lines of the UCC structure while also pulling in the terracotta cotta color of the brick of the church. Additionally the elevated passive and active courts provide significant relief along the alley maintaining composed patterning of windows and material with special attention to developing an active courtyard beneath the existing Big Leaf Maple.</p>
	WALKABILITY: SECONDARY ARCHITECTURAL FEATURE	Create a safe and comfortable walking environment that is easy to navigate and well-connected to existing pedestrian walkways and features.	<p>The architect's challenge is the creation of a building possessing visual restraint at the same time providing an intimacy of scale that acts to reduce the overall size and massing as the structure relates to its surrounding context.</p>	<p>No extensive blank walls are presented for consideration. Landscape edges, glazing and screening are provided across all facades, providing a positive pedestrian experience at the project edges.</p> <p>Using the feature elements defined as the NW corner, the commercial corner, gasket linking the north and south structures, south structure bay modulation, and grade-related residential stoops combined with multiple intimate and public open spaces along the streetscape create an overall contextual response as well as an comfortable environment at the pedestrian level. The larger moves are reinforced through nuanced conditions at the structure's sculpted base through a complimentary material palette: brick patterning, storefront glazing, warm residential entry textures, slat and metal screened parking, metal panels, building signage, well-lit features and entries, accent paving, scaled canopies above residential and commercial entries, integrated site furnishing, and landscape.</p>



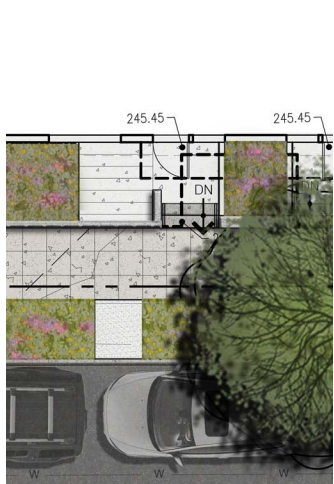
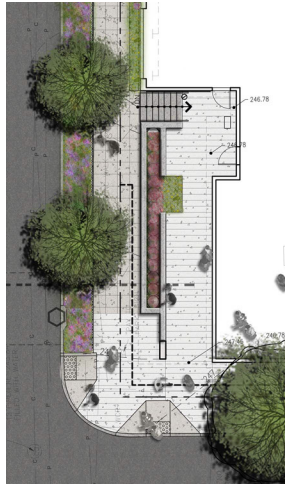
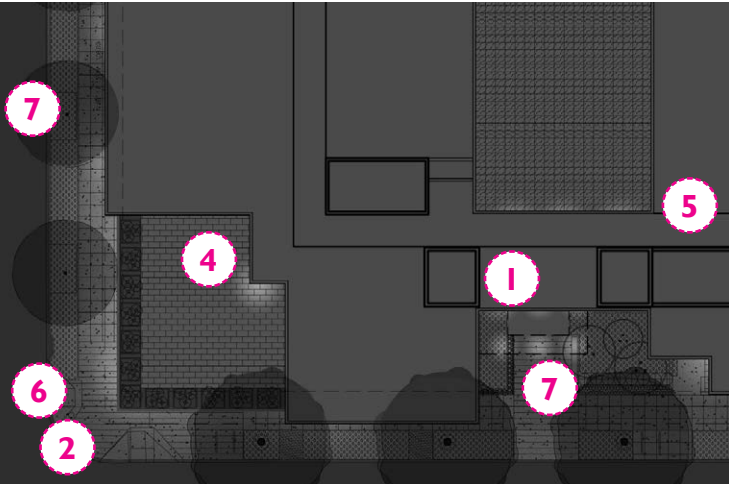
DESIGN GUIDELINES

	TITLE	DESCRIPTION	DRB GUIDANCE AT EDG 2	RESPONSE
DC-3	OPEN SPACE CONCEPT	Integrate open space design with the design of the building so that each complements the other.	The guidance provided at the first EDG meeting remains relevant as the design of the open spaces evolve. “Decks on the east side of the building to be used for children’s play and social areas need to be screened to ensure privacy and noise mitigation for the neighbors across the alley.”	The active courtyard, centered around the existing tree, is setback approximately 63’-0” from the Barclay Apartments across the alley and provide visual and audible mitigation to alley neighbors.
DC-4	EXTERIOR ELEMENTS AND FINISHES	Use appropriate and high quality elements and finishes for the building and its open spaces.	The presence of the large projecting bays on 15th appears imposing (see p. 55 of the booklet). Even with fenestration, the design of the bays must possess a better sense of scale. The Board noted that the entire length of the structure could not be entirely glazing and cementitious panel. The major facades will need a fine grain of detail to match the church and other well detailed buildings along 15th Ave. and within the immediate neighborhood.	The massing along 15th Ave has been modified to reflect a more frequent bay module that reflects modules and datums from the UCC structure; previously presented in the second EDG meeting as three bays now has five bays across 180’-0” of the South building. Inset bays create relief from the longer façade in form and add a varied plank pattern texture and color to provide fine grain design scale. The modular glazed gasket, running roughly 28’-0” along 15th Ave NE, flanked by the brick tower structure is set back 15’ from the ROW (20’ away from the property line including the SDOT setback). Cement panel patterning creates a weave across the bays. At areas adjacent to pedestrian paths, brick is the predominant material with aluminum storefront at retail and dark vinyl window colors at grade-related residential units. The large integrated canopy at the commercial and main residential entry, as well as the smaller canopies at grade-related residential entries are accented with the texture of plank soffits and framed with dark grey metal.
DC4-B	SIGNAGE	Add interest to the streetscape with exterior signs and attachments that are appropriate in scale and character to the project and its environs.	Second EDG Meeting: See guidance from the previous meeting: “First EDG Meeting: The applicant will need to provide a concept signage plan at the Recommendation meeting.”	The commercial corner features a building canopy integrated with a legacy panel to commemorate the UCC’s contribution to the development. Simple canopy mounted blade signage and discreet commercial, residential address, and parking signage will be used throughout as presented in the signage concept portion of this proposal and presentation.



DESIGN GUIDLINES

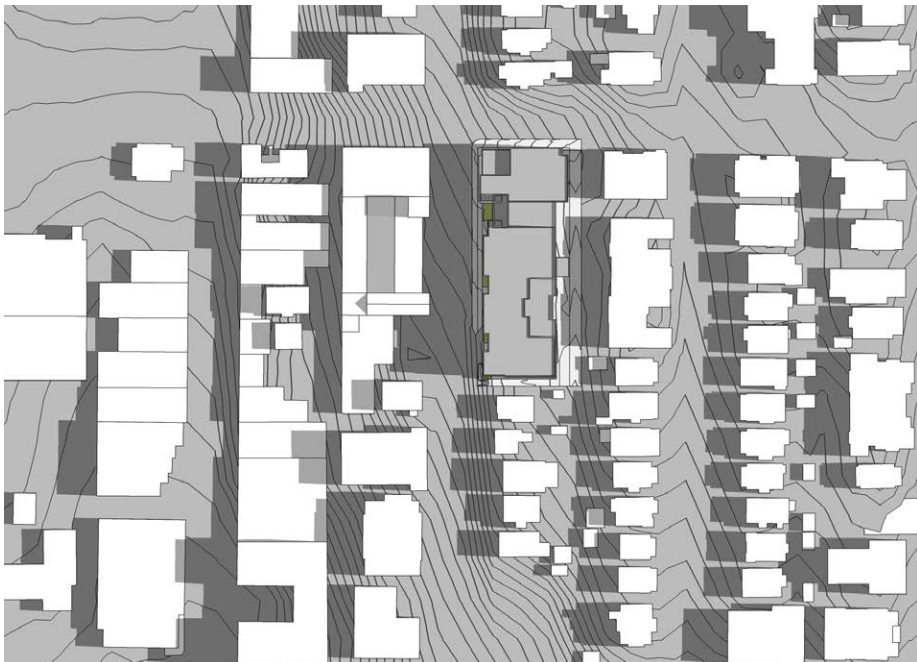
TITLE	DESCRIPTION	DRB GUIDANCE AT EDG 2	RESPONSE
DC4-C LIGHTING	Use lighting both to increase site safety in all locations used by pedestrians and to highlight architectural or landscape details and features such as entries, signs, canopies, plantings, and art.	See guidance from the previous meeting: <i>"First EDG Meeting: The applicant will need to provide a concept lighting plan at the Recommendation meeting."</i>	Lighting has been integrated into the overall design of this project proposal, lighting has been selected to provide site safety along the alley, side-yard, and parking structure, as well as to highlight the private residential grade-related stoops, main residential entry, commercial corner and legacy panel element as presented in the lighting concept portion of this proposal and presentation.
DC4-D TREES, LANDSCAPE, AND HARDSCAPE MATERIALS	Reinforce the overall architectural and open space design concepts through the selection of landscape materials.	See guidance from the previous meeting. The Board prefers the inclusion of colorful plantings to replace the prized cherry tree at the corner. <i>"First EDG Meeting: Use landscaping elements to provide a better pedestrian environment on 15th Ave. The length of the development site lends itself to providing a rich opportunity to create attractive places along this significant corridor that leads to the university."</i>	A variety of plant mixes and small trees arranged adjacent to open space along the building edge allow for visual seasonal interest throughout the year. In lieu of the aging Cherry tree at the intersection of 50th St and 15th Ave, a legacy panel with bench and landscaped edge has been designed to enhance the multiple building entries and associated open space.



APPENDIX

SHADOW STUDIES

SUMMER SHADOW STUDY



SUMMER SOLSTICE AT 8AM



SUMMER SOLSTICE AT 12PM

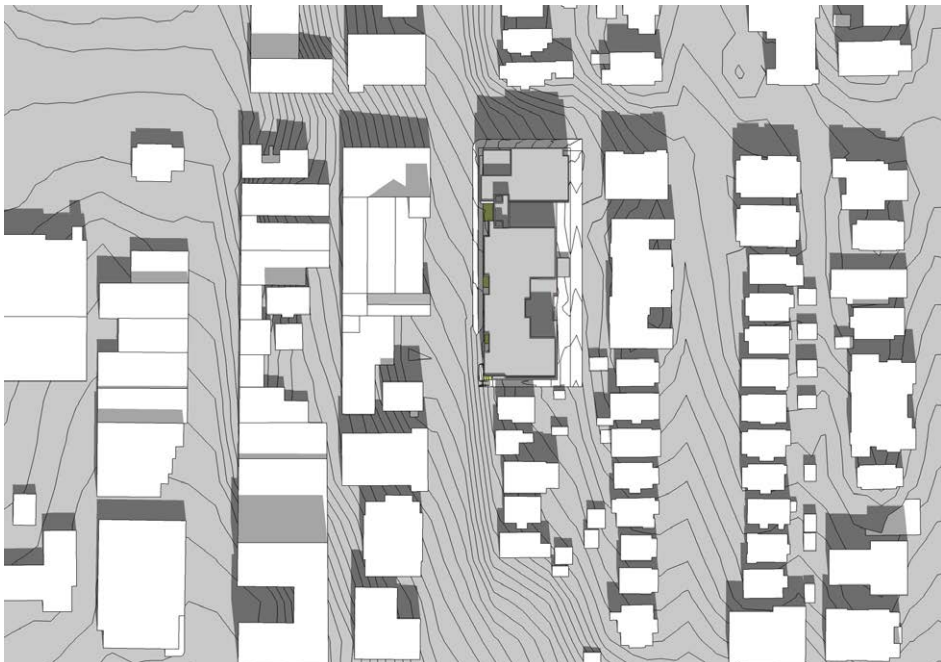


SUMMER SOLSTICE AT 6PM

SPRING SHADOW STUDY



SPRING SOLSTICE AT 8AM



SPRING SOLSTICE AT 12PM



SPRING SOLSTICE AT 5PM



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06.29.2015 | 14-034 | 69



SHADOW STUDIES

FALL SHADOW STUDY



FALL SOLSTICE AT 8AM



FALL SOLSTICE AT 12PM



FALL SOLSTICE AT 5PM

WINTER SHADOW STUDY



WINTER SOLSTICE AT 9AM



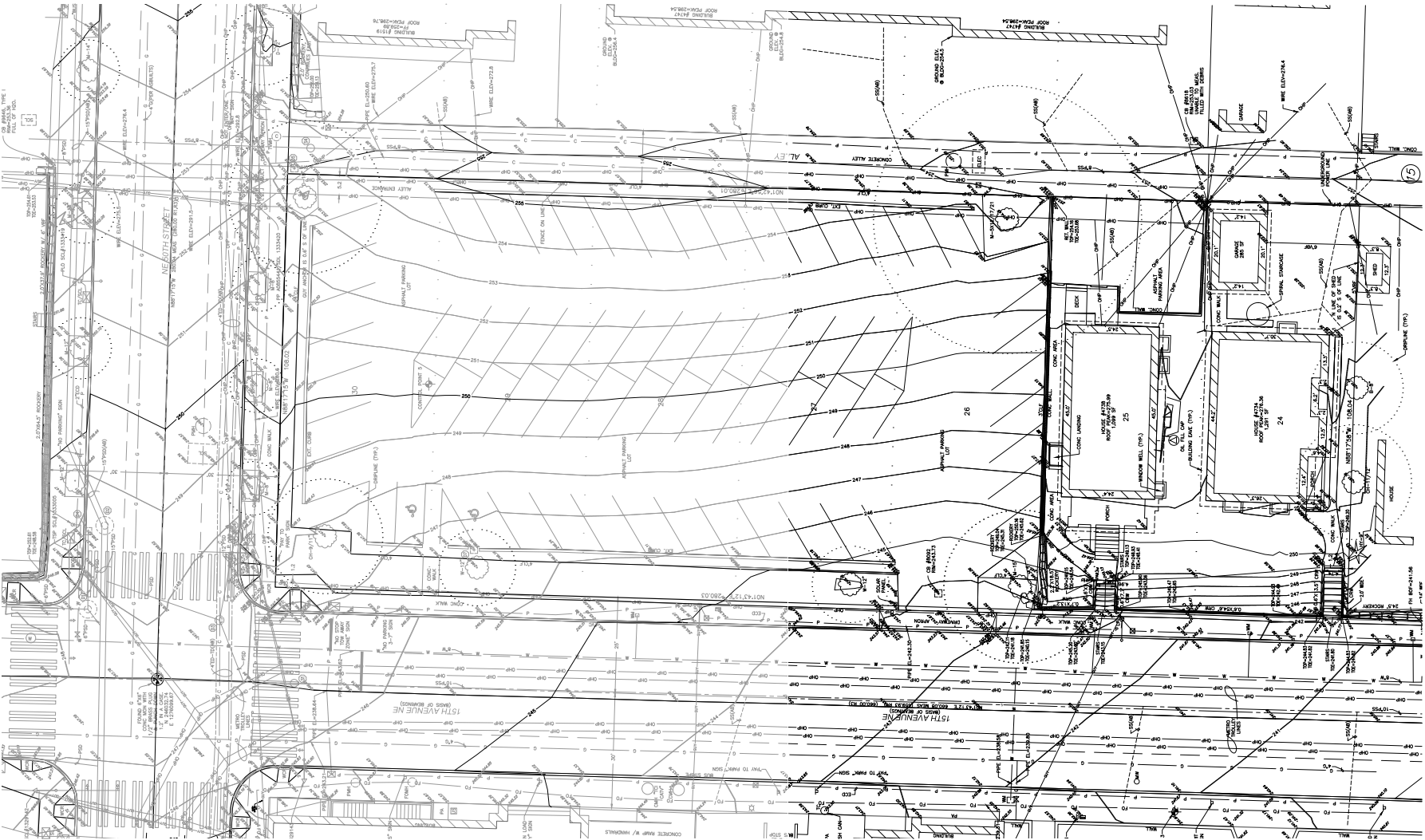
WINTER SOLSTICE AT 12PM



WINTER SOLSTICE AT 5PM

EXISTING SITE CONDITIONS

EXISTING SITE CONDITIONS AND PHYSICAL FEATURES



THE SITE IS LOCATED AT THE INTERSECTION OF 15TH AVENUE NE AND NE 50TH STREET AND ENCOMPASSES AN AREA OF 30,240 SQ.FT. THE SITE MEASURES 108' X 280' WITH THE LENGTH RUNNING IN THE NORTH-SOUTH DIRECTION ALONG 15TH AVENUE NE. A 14' PAVED ALLEY BORDERS THE EAST SITE FRONTAGE. A SURFACE PARKING LOT, ACCESSED BOTH FROM THE ALLEY AND 15TH AVENUE NE, OCCUPIES THE NORTHERN TWO-THIRDS OF THE SITE. TWO MULTIFAMILY WOOD FRAME RESIDENTIAL STRUCTURES, FORMERLY SINGLE-FAMILY HOUSES, SIT ON THE TWO SOUTHERN PARCELS. OVERHEAD POWER UTILITIES EXIST ALONG THE NORTH AND EAST FRONTAGE AS WELL AS OVERHEAD METRO TROLLEY LINES ALONG THE WEST FRONTAGE. EXISTING SIDEWALKS AND THE ALLEY DO NOT MEET SDOT WIDTH STANDARDS AND REQUIRE WIDENING. SITE AREA WILL BE PROVIDED TO ACCOMMODATE WIDENING ALONG THE STREET FRONTAGES IN THE FORM OF A 5'-6" EASEMENT AT THE 15TH AVENUE NE (WEST) FRONTAGE AND A 3'-0" EASEMENT AT THE NE 50TH STREET (NORTH) FRONTAGE. THE ALLEY REQUIRES A 1'-0" DEDICATION FOR THE SITE LENGTH. ADDITIONALLY, THE SITE REZONE INCLUDES A REQUIREMENT TO SETBACK 30' FROM THE CENTERLINE OF THE ALLEY FOR PORTIONS OF STRUCTURE ABOVE 13' TO ADDRESS THE CHANGE IN ZONING ACROSS THE ALLEY.



NE 50TH STREET FRONTAGE



15TH AVENUE NE FRONTAGE



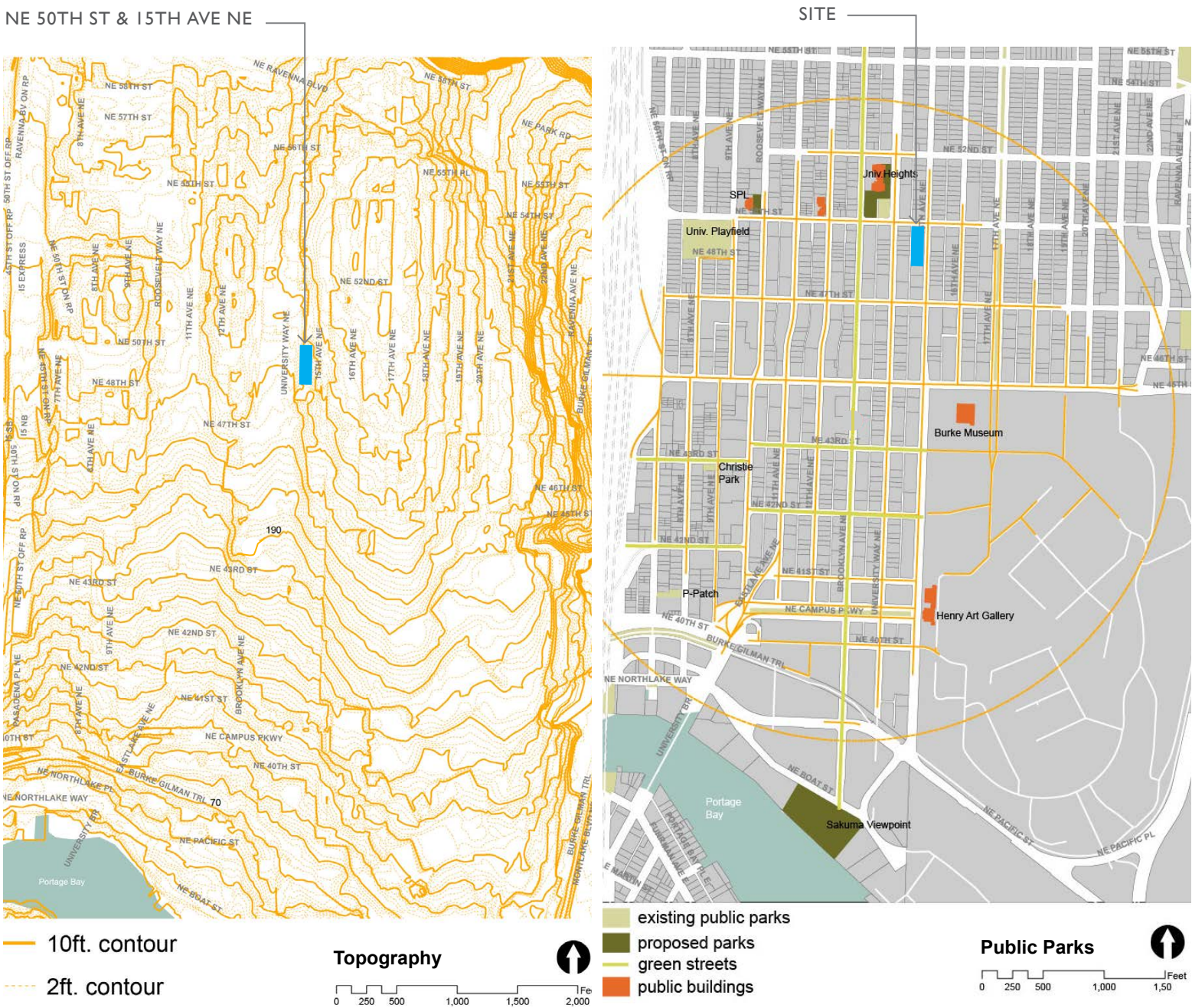
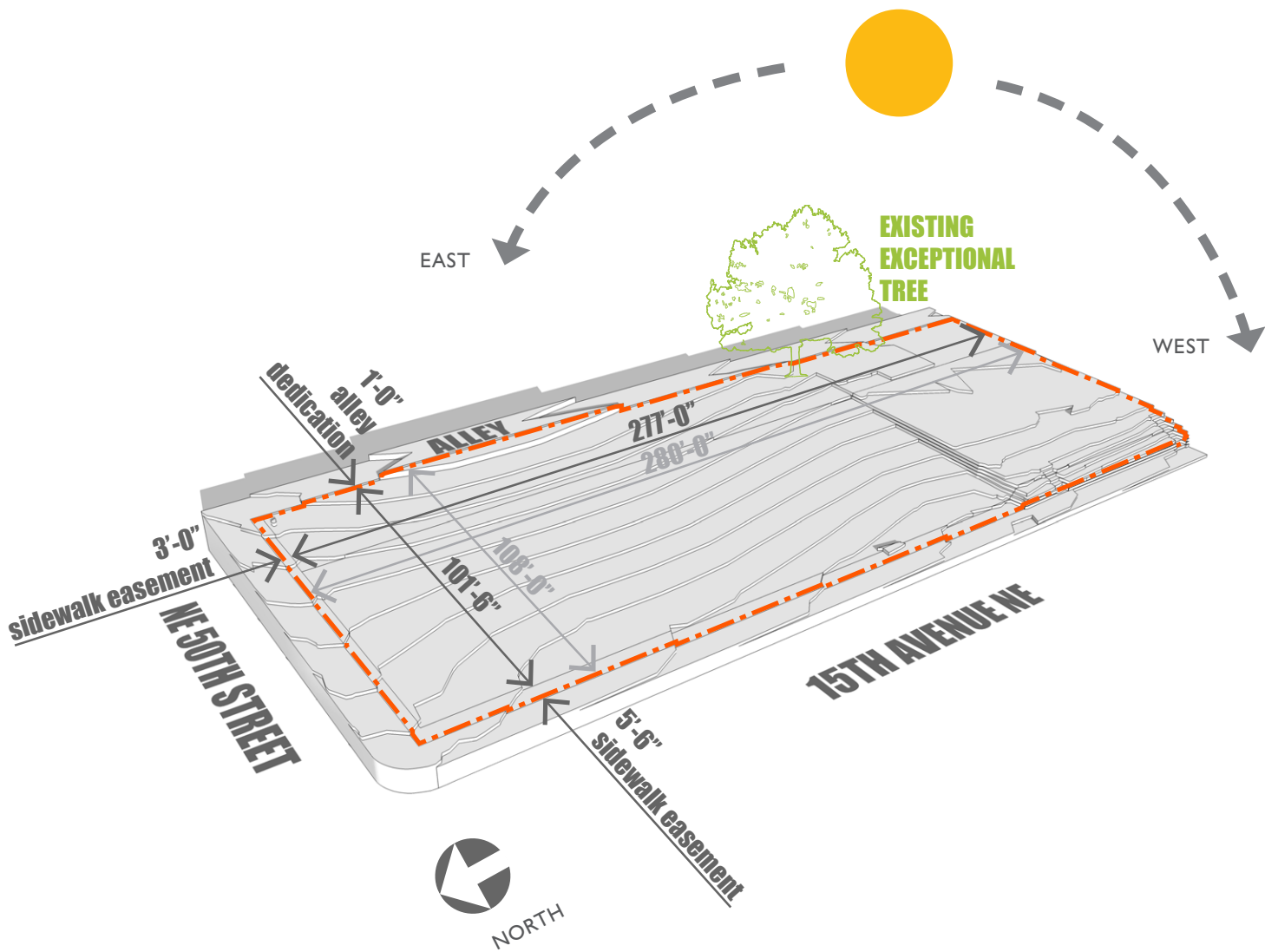
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06.29.2015 | 14-034 | 71



ENVIRONMENT

THE SITE ORIENTATION, LONGER FAÇADES FACING EAST AND WEST, SUPPORTS A MORE CONSISTENT SOLAR EXPOSURE, NATURAL DAY LIGHTING AND REDUCED SOLAR HEAT GAIN, PROVIDING COMFORTABLE SPACES FOR USERS AND POTENTIAL ENERGY SAVINGS. THE SITE SLOPES FROM A HIGH POINT OF 255', AT THE MID ALLEY FRONTAGE, DOWN TO A LOW POINT OF 242', AT THE SOUTHWEST PROPERTY CORNER, FOR A TOTAL OF 13' GRADE CHANGE. THIS TOPOGRAPHY CREATES AN OPPORTUNITY FOR GRADE RELATED ACCESS TO TWO SEPARATE FLOORS AT THE WEST AND EAST FRONTAGES. A NUMBER OF TREES AS WELL AS ONE EXCEPTIONAL TREE ARE LOCATED ON AND ADJACENT TO THE DEVELOPMENT. STREET TREES ARE PLANTED ALONG NE 50TH STREET AND REINFORCE A RELATIONSHIP TO THE UNIVERSITY HEIGHTS PLAYFIELDS TO THE WEST, THE UNIVERSITY AVENUE GREEN STREET ONE BLOCK AWAY AND THE RAVENNA GREEN TO THE NORTH. NO STREET TREES EXIST ALONG 15TH AVENUE NE FOR THE LENGTH OF THIS BLOCK AS WELL AS BLOCKS FURTHER SOUTH.



ARBORIST REPORT



1. Introduction

American Forest Management was contacted by Bellwether Housing and asked to compile an ‘Arborist Report’ for a development project involving four parcels in Seattle.

The proposed development encompasses four parcels, Parcel #8823902200, #8823902185, #8823902180 and #8823902175. Our assignment is to prepare a written report on present tree conditions, and to provide appropriate recommendations for the protection of retained trees during development of the property.

Date of Field Examination: March 28th, 2014

2. Description

Six significant trees were identified on the property. The subject properties are developed and currently have one parking lot and two houses.

Information for all significant property trees as well as significant neighboring trees can be found on the attached Tree Summary Tables. A copy of the site plan is also attached. Tree numbers shown on the copy of the site plan correspond with the Tree Summary Tables.

3. Methodology

Each tree in this report was visited. Tree diameters were measured by tape at DBH (diameter at 4 ½’ above ground). The tree heights were measured using a Spiegel Relaskop. Each tree was visually examined for defects and vigor. The tree assessment procedure involves the examination of many factors:

- The crown of the tree is examined for current vigor. This is comprised of inspecting the crown (foliage, buds and branches) for color, density, form, and annual shoot growth, limb dieback and disease. The percentage of live crown is estimated for coniferous species only and scored appropriately.
- The bole or main stem of the tree is inspected for decay, which includes cavities, wounds, fruiting bodies of decay (conks or mushrooms), seams, insects, bleeding, callus development, broken or dead tops, structural defects and unnatural leans. Structural defects include crooks, forks with V-shaped crotches, multiple attachments, and excessive sweep.
- The root collar and roots are inspected for the presence of decay, insects and/or damage, as well as if they have been injured, undermined or exposed, or original grade has been altered.

Based on these factors a determination of viability is made. Trees considered ‘non-viable’ are trees that are in poor condition due to disease, extensive decay and/or cumulative structural defects, which exacerbate failure potential. A ‘viable’ tree is a tree found to be in good health, in a sound condition

with minimal defects and is suitable for its location. Also, it will be wind firm if isolated or left as part of a grouping or grove of trees. A ‘borderline’ viable tree is a tree where its viability is in question. These are trees that are beginning to display symptoms of decline due to age, species related problems and/or man caused problems. Borderline trees are not expected to positively contribute to the landscape for the long-term and are not recommended for retention.

4. Observations/Discussion

Tree #101 is a Japanese cherry in the northwest corner of the property. The subject tree has two co dominant stems with good attachment and a full crown. The subject tree is viable.

Tree #102 is a red maple on the north east corner of the property. This tree is considered mature, has good form and a full crown. It is located under a power line, has large surface roots and has co dominant stems. The subject tree is considered viable.

Tree #103 is a big leaf maple located on the east edge of the subject property. This tree is considered mature. Big leaf maples are a native deciduous species. The subject tree forks approximately 3’ from the ground and has four co dominant stems. The branch attachment of the co dominant stems is generally good, with the exception of one poor branch attachment. The subject tree has large and widespread surface roots. The tree is an exceptional tree, per the City of Seattle, Director’s Rule 16-2008. The co dominant branches fork approximately 3’ from the ground. To determine if the tree was an exceptional tree the following equation (per the Directors Rule 16-2008) was used:
 $dbh = \sqrt{(stem1)^2 + (stem2)^2 + (stem3)^2 + (stem4)^2}$ Using this equation the dbh was determined to be 39”.

Tree #104 is a red maple on the west side of the property. This tree has a full crown, no visible defects and is considered viable.

Tree #105 and #106 are European weeping birches. European weeping birches are a popular landscape tree and are well suited for the Pacific Northwest. Tree #105 has some decay in a branch cavity. This decay is suspected to be localized. Both trees have full crowns and are viable.

Neighboring Trees

Trees #201, #202 and #203 are on Seattle Department of Transportation (SDOT) property. These trees are owned and maintained by SDOT. All three trees are in good health and are considered viable. Tree #201 is a red maple and trees #202 and #203 are sycamore maples.

Tree #204 is a Highclere holly on the south end of the property. This tree has good form and full foliage. Highclere hollies are native to Europe but do well as a landscape tree in the Pacific Northwest. This tree is viable.

Tree #205 is a European weeping birch on the neighbor’s property to the south. The trunk of this tree is covered in ivy. The tree has a full crown and given the current plan, can be feasibly protected during construction.

Tree #206 and #207 are bitter cherries. Both trees are covered in ivy but are in good condition. These trees can feasibly be protected during construction.

Tree #208 is an English holly on the neighbor’s property to the south. English hollies are native to Europe but are popular as a landscape tree. This tree is in good condition.

Tree #209 is a dead coastal redwood. This tree has a drip line extending on to the subject property. The subject tree is dead and could represent a risk. The neighbor should be informed of this.

5. Tree Retention and Removal

All of the trees on this property will be removed with the current design.

6. Tree Retention

The street trees north of the subject property can be feasibly retained given the current plan. The neighboring trees south of the property can also be protected. The extent of the drip lines for the neighboring trees is on the attached site plan map.

The limits of disturbance, as shown on the tree map and included on the tree table, are based on the size of the existing planting boxes. Roots are expected to still exist outside the limits of disturbance (planting boxes). Any work within the drip line should be monitored by a tree care professional.

7. Tree Replacement

The design will not allow for the retention of the exceptional tree #103, a big leaf maple on the east edge of the property. Seattle code dictates that the removal of an exceptional tree shall be replaced by one or more trees. The size and species of the replacement trees required shall be determined by the Director. I would recommend a replacement rate of 3:1.

8. Tree Protection Measures

If plans are changed and there are trees on the property that will be retained, the following guidelines are recommended to ensure that the designated space set aside for the preserved trees are protected and construction impacts are kept to a minimum.

- Tree protection barriers shall be initially erected at 5’ outside of the drip-line prior to moving any heavy equipment on site.
- Tree protection fencing shall only be moved where necessary to install improvements, but only as close as the Limits of Disturbance, as indicated on the attached plan.
- Excavation limits should be laid out in paint on the ground to avoid over excavating.
- Excavations within the drip-lines shall be monitored by a qualified tree professional so necessary precautions can be taken to decrease impacts to tree parts. A qualified tree professional shall monitor excavations when work is required and allowed up to the “Limits of Disturbance”.
- To establish sub grade for foundations, curbs and pavement sections near the trees, soil should be removed parallel to the roots and not at 90 degree angles to avoid breaking and tearing roots that lead back to the trunk within the drip-line. Any roots damaged during these excavations should be exposed to sound tissue and cut cleanly with a saw. Cutting tools should be sterilized with alcohol.
- Areas excavated within the drip-line of retained trees should be thoroughly irrigated weekly during dry periods.
- Preparations for final landscaping shall be accomplished by hand within the drip-lines of retained trees. Plantings within the drip lines shall be limited. Large equipment shall be kept outside of the tree protection zones.

There is no warranty suggested for any of the trees subject to this report. Weather, latent tree conditions, and future man-caused activities could cause physiologic changes and deteriorating tree condition. Over time, deteriorating tree conditions may appear and there may be conditions, which are not now visible which, could cause tree failure. This report or the verbal comments made at the site in no way warrant the structural stability or long term condition of any tree, but represent my opinion based on the observations made.

Nearly all trees in any condition standing within reach of improvements or human use areas represent hazards that could lead to damage or injury.

Please call if you have any questions or I can be of further assistance.

Sincerely,

Kelly Wilkinson

Kelly Wilkinson
ISA Certified Arborist #PN-7673A
ISA Tree Risk Assessment Qualified



Tree Canopy



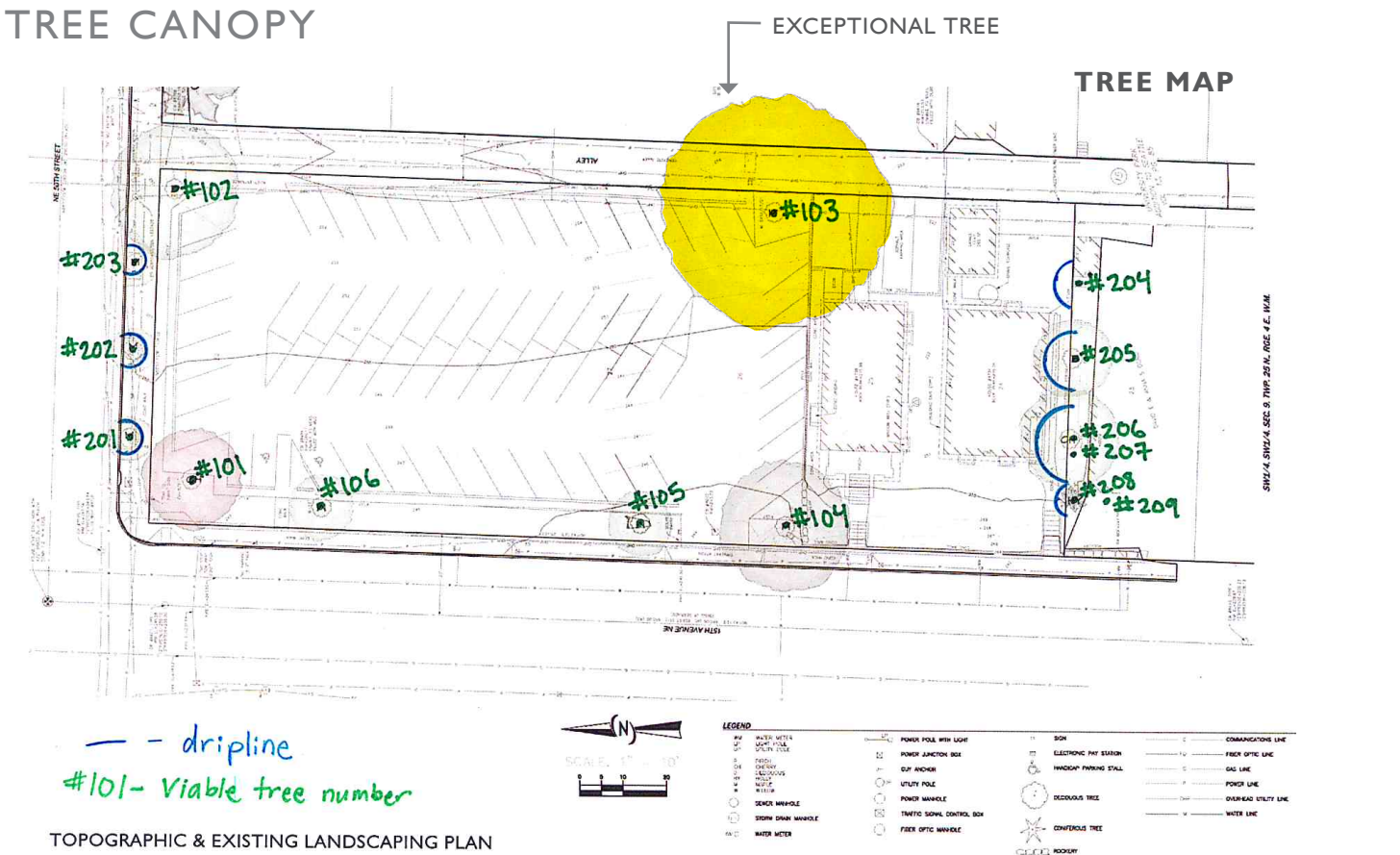
BELLWETHER UCC HOUSING

06.29.2015 | 14-034 | 73



ARBORIST REPORT

TREE CANOPY

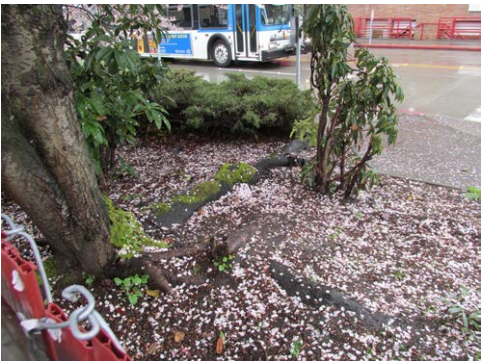


TREE SUMMARY TABLE

Tree/ Tag #	Species	Scientific Name	DBH (inches)	Height (feet)	Drip-Line/Limits of Disturbance (feet)				Condition	Proposal	Comments
					N	S	E	W			
101	Japanese cherry	<i>Prunus serrulata</i>	11, 9	16	12 / 12	22 / 7	15 / 2	12 / 10	good	viable	two co dominant stems, poor attachment, surface roots
102	red maple	<i>Acer rubrum</i>	18	45	15 / 3	16 / 2	15 / 4	21 / 3	good	viable	diameter taken at 4'
103	big leaf maple	<i>Acer macrophyllum</i>	26, 17, 12, 21	63	30 / 9	25 / 6	25 / 4	27 / 11	good	viable	co dominant stems, surface roots
104	red maple	<i>Acer rubrum</i>	18	40	13 / 6	10 / 4	15 / 5	15 / 4	good	viable	no concerns
105	European weeping birch	<i>Betula pendula</i>	12	52	5 / 5	12 / 4	8 / 2	5 / 3	fair	viable	past branch failure, decay at the base
106	European weeping birch	<i>Betula pendula</i>	13	54	12 / 6	15 / 6	10 / 4	8 / 2	good	viable	good form, full crown
Neighboring Trees											
201	red maple	<i>Acer rubrum</i>	8	30	4 / 2	2 / 1	2 / 3	2 / 3	good	viable	no concerns
202	sycamore maple	<i>Acer pseudoplatanus</i>	7	30	3 / 2	2 / 1	2 / 3	2 / 3	good	viable	full crown, no concerns
203	sycamore maple	<i>Acer pseudoplatanus</i>	9	32	4 / 2	2 / 1	2 / 3	2 / 3	good	viable	no concerns
204	Highbury holly	<i>Ilex x altaclarensis</i>	8, 12	70	5 / 3	x	x	x	good	viable	no concerns
205	European weeping birch	<i>Betula pendula</i>	17	75	7 / 4	x	7 / 4	10 / 5	good	viable	ivy covering the trunk, co dominant stems
206	Bitter cherry	<i>Prunus emarginata</i>	19	50	10 / 5	x	10 / 5	x	good	viable	ivy covering the trunk
207	Bitter cherry	<i>Prunus emarginata</i>	12	45	10 / 5	x	x	15 / 8	good	viable	ivy covering the trunk
208	English Holly	<i>Ilex aquifolium</i>	9	30	7 / 4	x	5 / 4	5 / 4	good	viable	full crown, good taper
209	coastal redwood	<i>Sequoia sempervirens</i>	25		5 / x	x	x	x	dead		possible risk

Drip-Line and Limits of Disturbance measurements from face of trunk

Tree #101 – Japanese cherry



Tree #201, #202 and #203 – Red maple, Sycamore maple, Sycamore



Tree #204 – Highbury holly



Tree #205 – European weeping birch



Tree #205, #206 and #207 – European weeping birch, Bitter cherry, I



Tree #105 – European weeping birch



Tree #106 – European weeping birch



Tree #104 – Red maple



Tree #208 and #209 – English holly and a dead coastal redwood



Tree #102 – Red maple



EXCEPTIONAL TREE

Memorandum

To: Richard Loo
Company: Bellwether Housing
From: Kelly Wilkinson
CC: Ann Williamson
Date: 5/19/2014
Re: **Review of Exceptional Tree Risks**

Mr. Loo,

American Forest Management was contacted by Bellwether Housing and asked to compile an ‘Arborist Report’ for a development project involving four parcels in Seattle. Upon completion of the report, American Forest Management was asked to provide a memo describing the risks associated with retaining tree #103, a big leaf maple on the subject property.

Tree #103 is a mature big leaf maple located on the east edge of the subject property. Big leaf maples are a native deciduous species. The tree is an exceptional tree, per the City of Seattle, Director’s Rule 16- 2008. To determine if the tree was an exceptional tree the following equation (per the Directors Rule 16-2008) was used:

$$dbh = \sqrt{(stem1)2+(stem2)2+(stem3)2+(stem4)2}$$

Using this equation the dbh was determined to be 39”.

The subject tree forks approximately 3’ from the ground and has four co dominant stems. Given the amount and size of the co dominant branches, branch failure in the future is likely. One of the branch attachments in particular has a poor v-shaped attachment and included bark, both of which are signs of concentrated stress. An ideal branch attachment has well defined curved angles (u-shape) rather than sharp transitions from one plane to the next (v-shaped). Over time the branch connections will weaken and branch failure will occur.

Additionally, the subject tree has large and widespread surface roots. The dripline of the subject tree is depicted on the attached map. There are still expected to be large roots beyond the dripline. If construction or grade changes were to occur within the dripline, the roots would be significantly compromised.

The overall risk rating of the subject tree is a 7, a moderate-high risk. A description of the risk categories is attached to this report.

Given the current design plan, retention of tree #103 is not feasible or recommended. Retaining tree #103 will limit development options on the property substantially. New tree plantings on the property would be a more reasonable option long term than retention.

Pictures of the subject tree, a map showing the dripline of the tree, and a tree risk assessment form are attached and made a part of this report.

There is no warranty suggested for any of the trees subject to this report. Weather, latent tree conditions, and future man-caused activities could cause physiologic changes and deteriorating tree condition. Over time, deteriorating tree conditions may appear and there may be conditions, which are not now visible which, could cause tree failure. This report or the verbal comments made at the site in no way warrant the structural stability or long term condition of any tree, but represent my opinion based on the observations made.

Nearly all trees in any condition standing within reach of improvements or human use areas represent hazards that could lead to damage or injury.

Please call if you have any questions or I can be of further assistance.

Sincerely,

Kelly Wilkinson

Kelly Wilkinson
ISA Certified Arborist #PN-7673A
ISA Tree Risk Assessment Qualified

Tree #103 – Big leaf maple



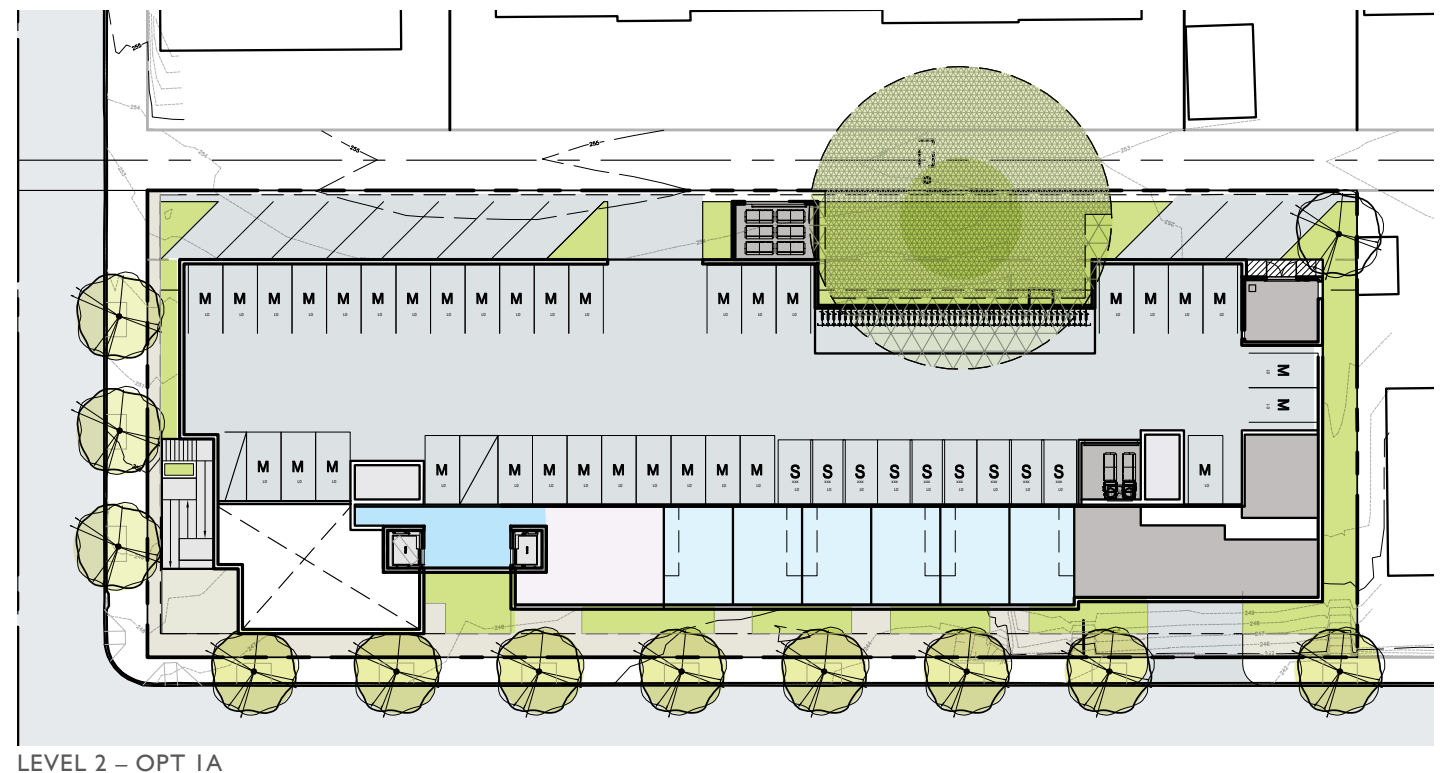
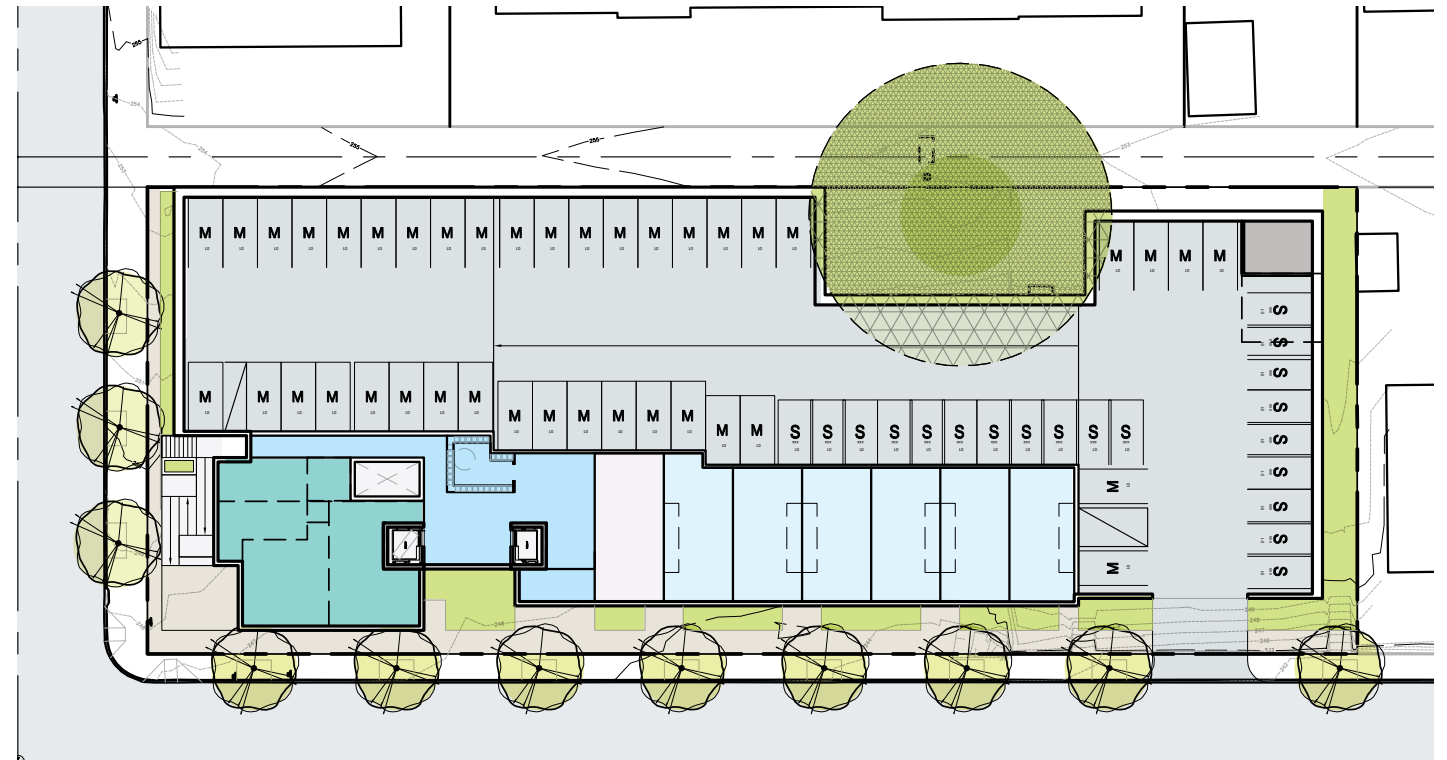
<div><div>PENDING</div><div>1</div></div> <div>RISK CATEGORIES WITHIN THE TWELVE POINT RATING SCHEME</div> <div></div>		
The Overall Risk Rating and Action Thresholds (3-12 points)		
Risk Rating	Risk Category	Interpretation and Implications
3	Low 1	Insignificant - no concern at all.
4	Low 2	Insignificant - very minor issues.
5	Low 3	Insignificant - minor issues not of concern for many years yet.
6	Moderate 1	Some issues but nothing that is likely to cause any problems for another 10 years or more.
7	Moderate 2	Well defined issues - retain and monitor. Not expected to be a problem for at least another 5 - 10 years.
8	Moderate 3	Well defined issues - retain and monitor. Not expected to be a problem for at least another 1 - 5 years.
9	High 1	The assessed issues have now become very clear. The tree can still reasonably be retained as it is not likely to fall apart right away, but it must now be monitored annually. At this stage it may be reasonable for the risk manager/owner to hold public education sessions to inform people of the issues and prepare them for the reality that part or the entire tree has to be removed.
10	High 2	The assessed issues have now become very clear. The probability of failure is now getting serious, or the target rating and/or site context have changed such that mitigation measures should now be on a schedule with a clearly defined timeline for action. There may still be time to inform the public of the work being planned, but there is not enough time to protracted discussion about whether or not there are alternative options available.
11	High 3	The tree, or a part of it has reached a stage where it could fail at any time. Action to mitigate the risk is required within weeks rather than months. By this stage there is not time to hold public meetings to discuss the issue. Risk reduction is a clearly defined issue and although the owner may wish to inform the public of the planned work, he/she should get on with it to avoid clearly foreseeable liabilities.
12	Extreme	This tree, or a part of it, is in the process of failing. Immediate action is required. All other, less significant tree work should be suspended, and roads or work areas should be closed off, until the risk issues have been mitigated. This might be as simple as removing the critical part, drastically reducing overall tree height, or taking the tree down and cordoning off the area until final clean up, or complete removal can be accomplished. The immediate action required is to ensure that the clearly identified risk of harm is eliminated. For areas hit by severe storms, where many extreme risk trees can occur, drastic pruning and/or partial tree removals, followed by barriers to contain traffic, would be an acceptable first stage of risk reduction. There is no time to inform people or worry about public concerns. Clearly defined safety issues preclude further discussion.
The Table shown above outlines the interpretation and implications of the risk ratings and associated risk categories. This table is provided to inform the reader about these risk categories so that they can better understand any risk abatement recommendations made in the risk assessment report.		

EXCEPTIONAL TREE

AN ARBORIST HAS CONFIRMED THE EXISTING BIG LEAF MAPLE ALONG THE SOUTH EAST ALLEY FRONTAGE IS AN EXCEPTIONAL TREE.

IN ORDER TO DEVELOP ADJACENT TO AND PRESERVE THE TREE THE FOLLOWING MEASURES WILL BE UNDERTAKEN:

- INCREASE AREA OF SHORING TO AN AREA 63' WIDE ALONG THE ALLEY AND 27' DEEP INTO THE SITE, REDUCING DEVELOPMENT BY 10,206 SF.
- CONDUCT CONSTRUCTION MONITORING OF THE TREE DURING EXCAVATION AND VERTICAL CONSTRUCTION.
- LIMIT CHANGING THE GRADE ONLY WITHIN 20% OF THE DRIP LINE,
- LIMIT CROWN PRUNING TO NO MORE THAN 20% MAXIMUM OF THE LIVE CROWN IN ANY GIVEN YEAR, WITH TREE PRUNING TO OCCUR BETWEEN OCTOBER AND FEBRUARY.
- TREE PROTECTION BARRIERS SHALL BE INITIALLY ERECTED AT 5' OUTSIDE OF THE DRIP-LINE PRIOR TO MOVING ANY HEAVY EQUIPMENT ON SITE.
- TREE PROTECTION FENCING SHALL ONLY BE MOVED WHERE NECESSARY TO INSTALL IMPROVEMENTS, BUT ONLY AS CLOSE AS THE LIMITS OF DISTURBANCE, AS INDICATED ON THE ATTACHED PLAN.
- EXCAVATION LIMITS SHOULD BE LAID OUT IN PAINT ON THE GROUND TO AVOID OVER EXCAVATING.
- EXCAVATIONS WITHIN THE DRIP-LINES SHALL BE MONITORED BY A QUALIFIED TREE PROFESSIONAL SO NECESSARY PRECAUTIONS CAN BE TAKEN TO DECREASE IMPACTS TO TREE PARTS. A QUALIFIED TREE PROFESSIONAL SHALL MONITOR EXCAVATIONS WHEN WORK IS REQUIRED AND ALLOWED UP TO THE "LIMITS OF DISTURBANCE."
- TO ESTABLISH SUB GRADE FOR FOUNDATIONS, CURBS AND PAVEMENT SECTIONS NEAR THE TREES, SOIL SHOULD BE REMOVED PARALLEL TO THE ROOTS AND NOT AT 90 DEGREE ANGLES TO AVOID BREAKING AND TEARING ROOTS THAT LEAD BACK TO THE TRUNK WITHIN THE DRIP-LINE. ANY ROOTS DAMAGED DURING THESE EXCAVATIONS SHOULD BE EXPOSED TO SOUND TISSUE AND CUT CLEANLY WITH A SAW. CUTTING TOOLS SHOULD BE STERILIZED WITH ALCOHOL.
- AREAS EXCAVATED WITHIN THE DRIP-LINE OF RETAINED TREES SHOULD BE THOROUGHLY IRRIGATED WEEKLY DURING DRY PERIODS.
- PREPARATIONS FOR FINAL LANDSCAPING SHALL BE ACCOMPLISHED BY HAND WITHIN THE DRIP-LINES OF RETAINED TREES. PLANTINGS WITHIN THE DRIP LINES SHALL BE LIMITED. LARGE EQUIPMENT SHALL BE KEPT OUTSIDE OF THE TREE PROTECTION ZONES.



EXCEPTIONAL TREE

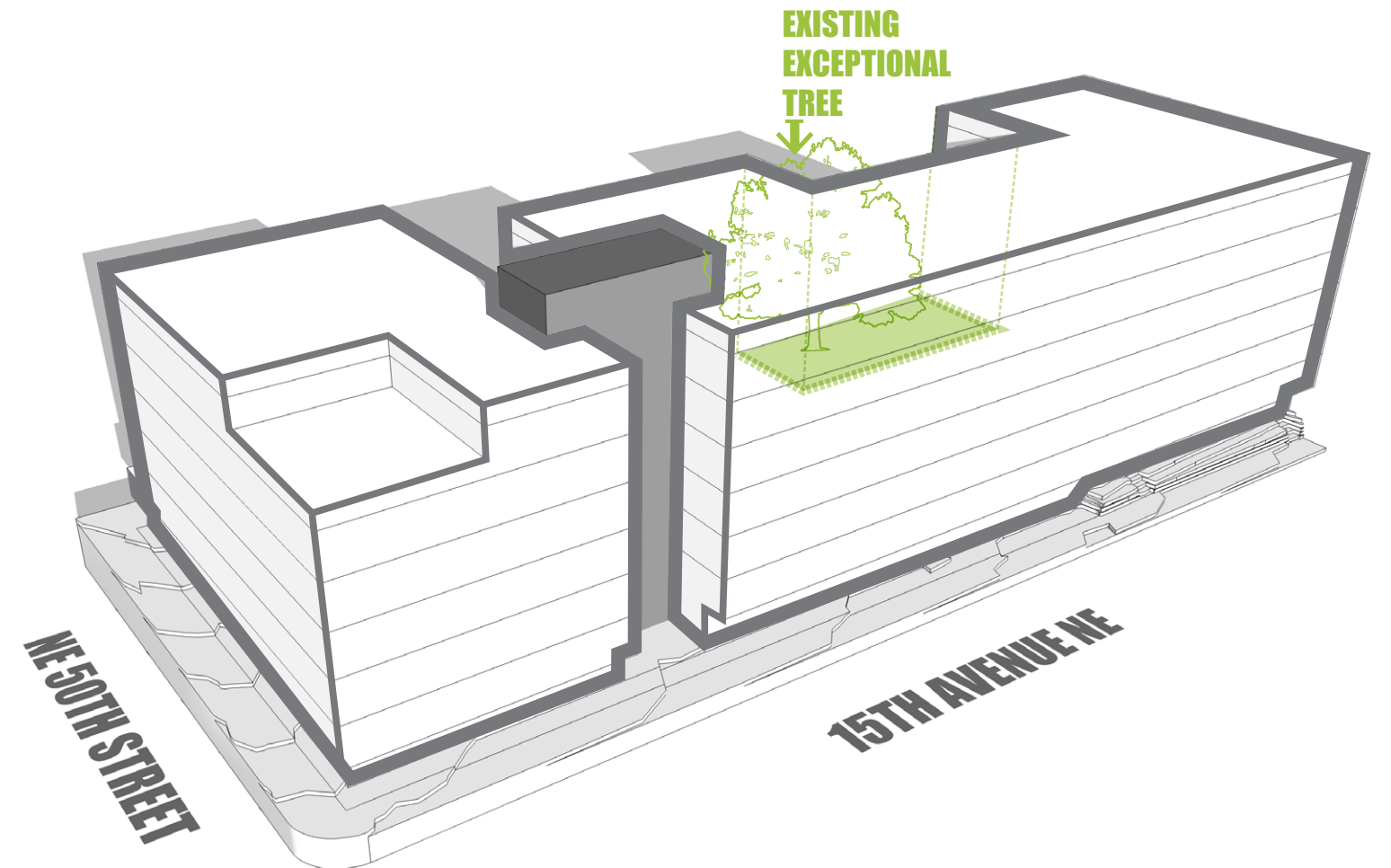
RETENTION OF THE EXISTING TREE PRESENTS THE FOLLOWING OPPORTUNITIES AND CHALLENGES:

TREE PRESERVATION PROS:

- PROVIDES NATURAL SCREENING TO THE ACTIVE COURTYARD.
- INFLUENCES MASSING TO CREATE DEEP COURTYARD ALCOVES AND MODULATION ALONG THE ALLEY.
- FOSTERS A RESIDENTIAL PEDESTRIAN SCALE ALONG THE ALLEY FRONTAGE.

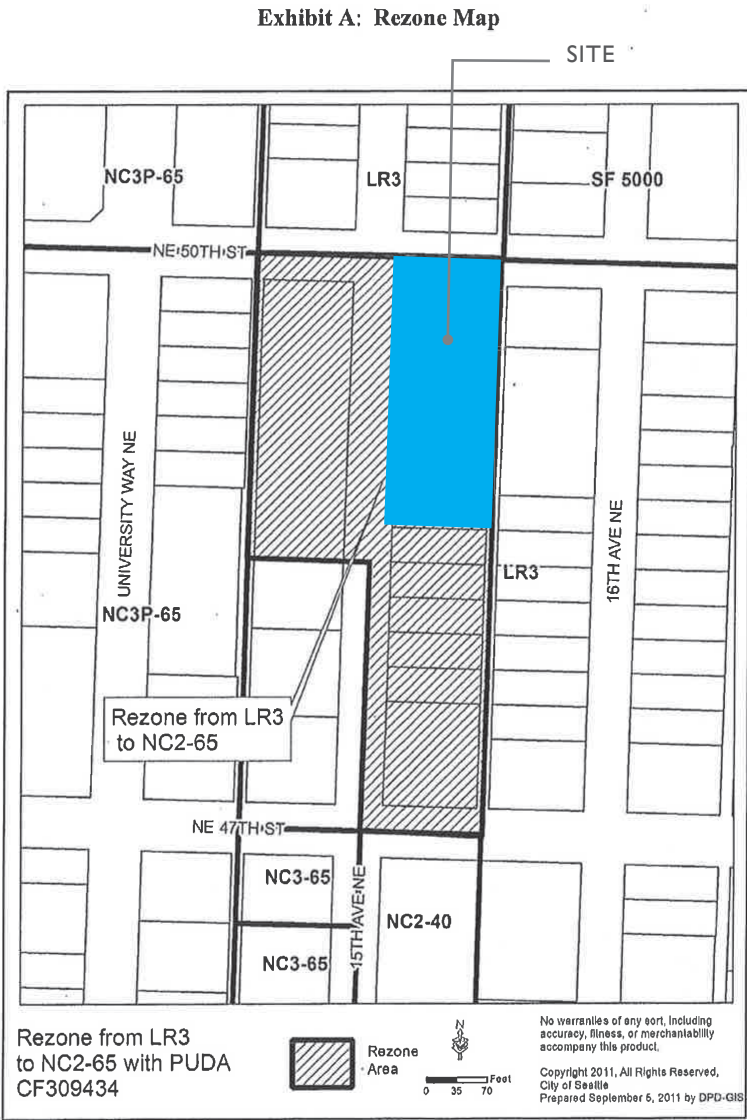
TREE PRESERVATION CONS:

- MOVING THE PROPOSED DEVELOPMENT OUTSIDE THE DRIP LINE IMPACTS PROGRAMMATIC REQUIREMENTS FOR PARKING GARAGES AND ZONING REQUIREMENTS FOR COMMERCIAL USE AND STREET LEVEL SEPARATION.
- HOLDING THE BUILDING OUTSIDE THE CRITICAL ROOT ZONE PUSHES THE PARKING GARAGE WEST ON THE SITE REDUCING THE DEPTH OF COMMERCIAL AND STREET LEVEL RESIDENTIAL SPACES.
- THE TREE AND ITS DRIP LINE OCCUPY 63' OF THE 280' ALLEY FRONTAGE, A 26.4% REDUCTION WHEN COMBINED WITH OTHER SITE SETBACKS, REDUCING OPPORTUNITIES FOR LOCATING VEHICLE ACCESS, BUILDING UTILITY SERVICES AND WASTE COLLECTION.
- THE INCLINATION OF BRANCHING CREATES A SECURITY RISK AS ACCESS TO THE PODIUM TERRACE LEVEL OF THE BUILDING.
- PRESERVATION INCREASES BOTH HARD AND SOFT CONSTRUCTION COSTS TO THE PROJECT TO PROTECT CORE ROOT ZONE.
- THE TREE IS IDENTIFIED BY SDOT VIA CITY ORDINANCE NO. 90047 AS A PROHIBITED TREE FOR USE ALONG STREETS DUE TO ITS SHALLOW ROOTS CAUSING SIDEWALK AND ROAD HEAVE, INAPPROPRIATE HEIGHT BELOW POWERLINES AND THEIR TENDENCY TO BREAK EASILY IN STORMS.



SITE ZONING

THE PROJECT SITE, ALONG WITH THE REST OF THE BLOCK TO THE WEST OF THE ALLEY AND THE UCC SITE, WAS REZONED IN 2011 FROM A LR3 TO NC2-65 ZONING. ADJACENT ZONING INCLUDES THE SINGLE FAMILY SF 5000 ZONE TO THE NORTHEAST, LOW-RISE LR3 AND LR2 ZONES EXTEND TO THE NORTHWEST AND SOUTHEAST, AND HIGHER DENSITY NC3-65 AND NC3-85 ZONES ARE LOCATED TO THE SOUTHWEST. THE SITE IS LOCATED WITHIN THE UNIVERSITY COMMUNITY URBAN CENTER-UNIVERSITY DISTRICT NW URBAN VILLAGE HOWEVER IT IS JUST OUTSIDE THE URBAN DESIGN FRAMEWORK STUDY AREA. AS AN EDGE SITE THE PROJECT SHOULD RESPOND TO THE UDF AND ITS PROPOSED REZONE.



B. In May 2008, the Owners submitted to the City an application, No. 3004384, to rezone the Property from LR3 to Neighborhood Commercial 3 with a 65 foot height limit. The purpose of the application is to allow the Property to be used for institutional, mixed-use, commercial and multi-family residential development of greater density.

C. Seattle Municipal Code (SMC) Section 23.34.004 allows the City to approve a contract rezone subject to “self-imposed restrictions upon the use and development of the property in order to ameliorate adverse impacts that could occur from unrestricted use and development permitted by development regulations otherwise applicable after the rezone.”

D. In February 2012 the Seattle City Council approved a rezone of the property to Neighborhood Commercial 2 with a 65 foot height limit (NC2 65), as shown in Exhibit B, subject to self-imposed restrictions set out in this Agreement.

NOW, THEREFORE, in consideration of the mutual agreements contained herein, the parties agree as follows:

AGREEMENT

Section 1. Agreement. Pursuant to SMC 23.34.004, the Owners hereby covenant, bargain and agree, on behalf of themselves and their successors and assigns, that, in the event that a master use permit application is made for any Property east of 15th Avenue NE, they will comply with the following limitations and conditions in consideration of the rezone of the Property from Lowrise 3 (LR 3) to Neighborhood Commercial 2 with a 65 foot height limit (NC2 65):

- For commercial, mixed use or residential projects, all building elements above 13 feet shall be set back 30 feet from the east property line of parcels on the east side of 15th Avenue NE (Lots 16-30, Block 15, University Park Addition), provided that one-half the width of the abutting alley may be counted as part of the required setback. A development standard departure from the setback may be granted by the Department of Planning and Development through design review, as part of a master use permit, where it is found that any allowed reductions of this required setback adequately accomplish a sensitive and appropriate transition of height bulk and scale across the alley to the east.
- Additional right-of-way setbacks and/or dedications shall be provided, pursuant to the Seattle Street Improvement Manual and the Seattle Municipal Code, for each element of redevelopment of the area rezoned without application of any exemption provisions thereof, including situations where the limited size of new construction would not otherwise require application of the provisions.
- The first new project that is situated in whole or in part on Lots 24 - 30, Block 15, University Park Addition shall be limited to proposals that

include primarily residential uses and shall include no fewer than 29 affordable units. “Affordable unit” means a residential unit affordable to those with an annual household income that does not exceed 50% of the annual median family income for the Seattle area, as published from time to time by the US Department of Housing and Urban Development with adjustments according to household size as determined by the Director of the Department of Planning and Development. In the event

that more than 145 residential units are proposed to be developed in the first new project that is situated in whole or in part on Lots 24 - 30, Block 15, University Park Addition, at least 20 percent of all residential units in that project shall be affordable units. The affordable-unit condition may be satisfied by the first new project on one or more of Lots 24-30 Block 15, University Park Addition, in which case no additional affordable units are required to be developed on the remainder of the lots. As an on-going condition of this Agreement, all affordable units required to be developed by this paragraph c must remain affordable to eligible households.

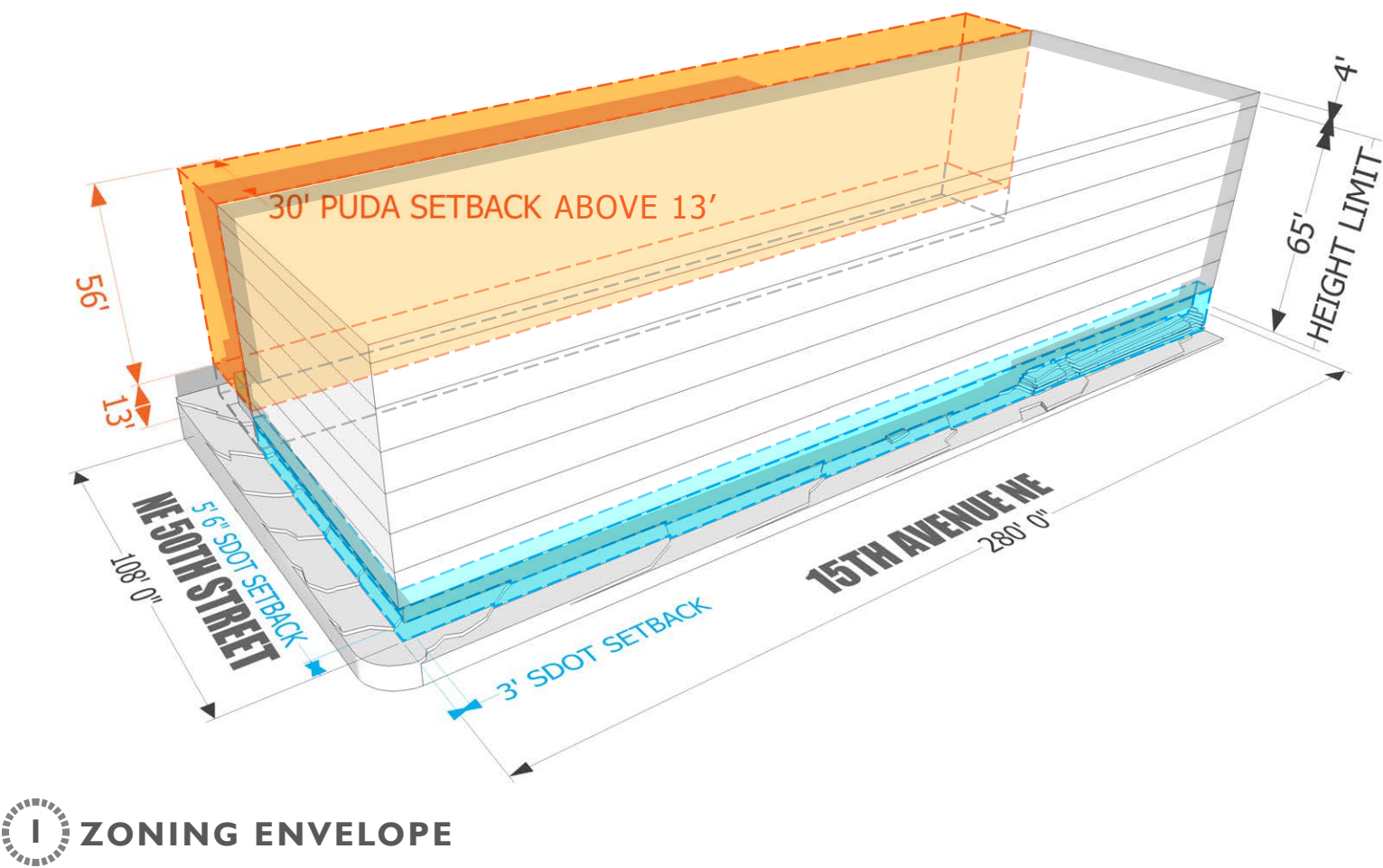
- Street-level commercial uses shall be limited to office space and support services for a religious facility-affiliated entity, or non-profit social or human service organization consistent with the mission of a religious facility-affiliated entity. Nothing shall preclude any such entity or non-profit organization from making limited retail sales of items or materials consistent with its goals and purposes including, without limitation, a religious facility-affiliated bookstore and religious facility-affiliated coffee shop.
- The Director of the Department of Planning and Development, as a Type I decision pursuant to Seattle Municipal Code Chapter 23.76, may grant relief from the restrictions of paragraph d on street-level commercial uses where it can be demonstrated that, despite best efforts, an owner has been unable to lease the ground floor commercial areas at reasonable rental rates for a period of nine months. However, with the exception of religious facility-related uses, uses that generally attract night-time crowds, or consistently generate a high demand for on-street parking, are prohibited.

Section 2. Agreement Runs With the Land. This Agreement shall be recorded in the records of King County by the City Clerk. The covenants hereof shall be deemed to attach to and run with the Property and shall be binding upon the Owners, their heirs, successors and assigns, and shall apply to after-acquired title of the Owners of the Property.

Section 3. Amendment. This Agreement may be amended or modified by written agreement between the Owners and the City; provided, such amendment shall be approved by the legislative authority of the City by ordinance.

LAND USE SUMMARY AND ENVELOPE ANALYSIS

LAND USE SUMMARY AND ENVELOPE ANALYSIS



SITE DESCRIPTION		
LOT SIZE	NORTH TO SOUTH	280'
	WEST TO EAST	108'
	TOTAL SITE AREA	30,240
	GRADE SLOPES DOWN TO THE SOUTH AND DOWN TO THE WEST	
PROPOSED USE		
APARTMENTS	FLATS AND GRADE RELATED TOWNHOUSES FOR LOW INCOME TENANTS	
COMMERCIAL	LIMITED TO OFFICE SPACE AND SUPPORT SERVICES FOR A RELIGIOUS FACILITY AFFILIATED ENTITY OR NON-PROFIT SOCIAL OR HUMAN SERVICE ORGANIZATION CONSISTENT WITH THE MISSION OF A RELIGIOUS FACILITY-AFFILIATED ENTITY.	
PARKING GARAGE	FOR CONTINUED USE BY UNIVERSITY CHRISTIAN CHURCH AND SEPARATE GARGE FOR APARTMENT TENANT USE.	
LAND USE CODE SUMMARY		
ZONING		
23.32	LAND USE MAP	60
23.47A	NC2-65	REZONED FROM LR-3 VIA PUDA 309434
	UNIVERSITY DISTRICT NW URBAN CENTER VILLAGE:	EDGE SITE
23.54.015 MAP A	UNIVERSITY DISTRICT PARKING IMPACT AREA	
23.61	NW 45TH STREET STATION OVERLAY DISTRICT	
	SEE UDF MAPS: SITE IS WITHIN 10 MINUTE STATION WALK BOUNDARY	
	MAXIMUM FAR IN A STATION OVERLY DISTRICT = 5.75, (MAX. FAR IN A S.O.D. - 5.75 @ 65' HT.)	
	30,240 SF (SITE AREA) X 5.75 (FAR FACTOR) = 173,880 SF MAXIMUM DEVELOPMENT AREA	
ADJACENT ZONES		
LR3	EXTENDING NORTHWEST AND SOUTHEAST FROM SITE	
NC3-65 (P)	ONE BLOCK WEST EXTENDING NORTH, SOUTH, AND SOUTHWEST FROM THE SITE	
NC2-65	ACROSS THE STREET AND SOUTH END OF PROJECT BLOCK	
NC2-40	EAST SIDE OF 15TH ONE BLOCK SOUTH	
SF5000	EXTENDING NORTHEAST FROM SITE	
MUP TYPE II		
SEPA		
TRANSPORTATION CONCURRENCY		
DESIGN REVIEW – NORTHEAST BOARD		
ADJACENT PUBLIC ROWS		
15TH AVE NE	PRINCIPAL ARTERIAL, MAJOR TRANSIT STREET, NOT PRINCIPAL PEDESTRIAN UDF-BLUE = REGIONAL CONNECTOR MAPED AT 80' WIDTH	
NE 50TH STREET	COLLECTOR ARTERIAL, MINOR TRANSIT STREET, NOT PRINCIPAL PEDESTRIAN UDF-GREY=LOCAL CONNECTOR MAPPED AS 60' WIDTH	
ALLEY	ONE-WAY HEADING NORTH CURRENTLY 14' WIDE, CONSIDERED IMPROVED PER TABLE B 23.53.030 AS 12' WIDE, 1' DEDICATION REQ. FOR ALLEY TO MEET MINIMUM RIGHT OF WAY WIDTH	

LAND USE SUMMARY

NEARBY PUBLIC ROWS

UNIVERSITY WAY NE
BROOKLYN AVE NE AND 17TH AVE NE

1 BLOCK WEST IS THE PRINCIAL PEDESTRIAN STREET
GREEN STREETS

23.47A.004	PERMITTED USES
APARTMENTS	NUMBER AND AFFORDABILITY LEVELS CONTROLLED BY PUDA AND PSA: MIN. 29 AFFORDABLE UNITS HOWEVER IF OVER 145 UNITS THEN 20% MUST BE AFFORDABLE
LIVE-WORK	MAXIMUM QUANTITY ONLY RESTRICTED WHERE PEDESTRIAN DESIGNATION
COMMERCIAL	CONTROLLED BY PUDA AS DESCRIBED IN THE SUMMARY ABOVE
PARKING	LIMITED TO 25,000 SF PER TABLE AS WELL AS REGULATED BY THE PSA
23.47A.005	STREET LEVEL USES
RESIDENTIAL USES	NO LIMIT PER PRE-APP MEETING CONFIRMATION, SITE DOESN'T MEET REQUIREMENTS OF C.1a. - c.l.g.
COMMERCIAL	NOT REQUIRED PER THE ZONING CODE PER LORI SWALLOW AT PRE-APP MEETING
LODGING	GENERAL LODGING UP TO 25,000 SF. WITH AUTO ACCESS VIA AN ARTERIAL OR B&B UNLIMITED SIZE
23.47A.008	STREET LEVEL DEVELOPMENT STANDARDS
BASIC REQUIREMENTS	BLANK FACADES BETWEEN 2' AND 8' ABOVE SIDEWALK MAY NOT EXCEED 20' IN WIDTH WITHOUT INCLUDING; WINDOWS, ENTRY, DOOR, STAIRS, STOOPS, PORTICOS, DECKS, BALCONIES, SCREENING AND LANDSCAPING ON THE FAÇADE. TOTAL OF ALL BLANK SEGMENTS MAY NOT EXCEED 40% OF THE STREET FAÇADE WIDTH STREET LEVEL FACADES SHALL BE LOCATED WITHIN 10' OF THE STREET LOT LINE UNLESS WIDER SIDEWALKS, PLAZAS OR OTHER APPROVED LANDSCAPED OR OPEN SPACES ARE PROVIDED
NON-RESIDENTIAL STREET LEVEL REQUIREMENTS	MIN 60% STREET FACING FAÇADE TRANSPARENCY BETWEEN 2'- AND 8' ABOVE SIDEWALK PROVIDE AVERAGE DEPTH OF 30' AND MIN DEPTH OF 15' FROM STREET LEVEL FAÇADE FLOOR TO FLOOR HEIGHT OF 13' MIN.
23.47A.008.	RESIDENTIAL STREET LEVEL REQUIREMENTS
	THE FLOOR OF STREET LEVEL RESIDENTIAL USE MUST BE 4' ABOVE OR BELOW SIDEWALK GRADE OR SETBACK AT LEAST 10' FROM PROPERTY LINE
23.47A.010	MAXIMUM SIZE OF NON-RESIDENTIAL USE
	NO LIMIT FOR USES ALLOWED UNDER THE PUDA
23.47A.012	STRUCTURE HEIGHT
	MAXIMUM OF 65' EXCEPT OPEN RAILINGS, PLANTERS, SKYLIGHTS, CLERESTORIES, GREENHOUSES, SOLARIUMS, PARAPETS AND FIREWALLS MAY EXTEND UP TO 4 FEET ABOVE THE HEIGHT LIMIT. SOLAR COLLECTORS WITH UNLIMITED COVERAGE MAY EXTEND UP TO 7' ABOVE THE HEIGHT LIMIT EXCEPT SPECIFIED ROOF FEATURES MAY EXTEND UP TO 15 FEET ABOVE THE HEIGHT LIMIT AS LONG AS THE COMBINED TOTAL COVERAGE OF ALL FEATURES GAINING ADDITIONAL HEIGHT DOES NOT EXCEED 20% OF THE ROOF AREA OR 25% OF THE ROOF AREA IF THE

TOTAL INCLUDES STAIR OR ELEVATOR PENTHOUSES OR SCREENED MECHANICAL EQUIPMENT.
STAIR AND ELEVATOR PENTHOUSES MAY EXTEND ABOVE THE APPLICABLE HEIGHT LIMIT UP TO 16 FEET

23.47A.013 TABLE B

FLOOR AREA RATIO (FAR)
MINIMUM 2 AT 65' HEIGHT WITHIN THE STATION OVERLAY DISTRICT
2 X 30,240 SF = 60,480 MAX. ALLOWABLE GROSS SF
MAXIMUM 5.75 AT 65' HEIGHT WITHIN THE STATION OVERLAY DISTRICT
5.75 X 30,240 SF = 173,880 MAX. ALLOWABLE GROSS SF
EXCLUDING: GROSS FLOOR AREA UNDERGROUND, PORTIONS OF A STORY THAT EXTEND MORE THAN 4 FEET ABOVE EXISTING OR FINISHED GRADE, WHICHEVER IS LOWER, EXCLUDING ACCESS, GROSS FLOOR AREA CONTAINING PARKING

23.47A.014

SETBACK REQUIREMENTS
PUDA REQUIRES 30' SETBACK FROM THE CENTERLINE OF THE ALLEY

ACROSS THE ALLEY FROM RESIDENTIAL ZONES PROVIDE 15 FOOT FOR PORTIONS OF STRUCTURES ABOVE 13 FEET IN HEIGHT TO A MAXIMUM OF 40, FOR EACH PORTION OF STRUCTURE ABOVE 40 FEET IN HEIGHT, ADDITIONAL SETBACK AT THE RATE OF 2 FEET OF SETBACK EVERY 10 FEET BY WHICH THE HEIGHT OF SUCH PORTION EXCEEDS 40 FEET. ONE-HALF OF THE WIDTH OF AN ABUTTING ALLEY MAY BE COUNTED AS PART OF THE REQUIRED SETBACK. THE ALLEY WIDTH AND LOCATION OF THE REAR LOT LINE SHALL BE DETERMINED PRIOR TO ANY DEDICATION THAT MAY BE REQUIRED FOR ALLEY IMPROVEMENTS.
SDOT REQUIRES 5.5 FOOT SIDEWALK EASEMENT ALONG 15TH AVE NW, A 3 FOOT SIDEWALK EASEMENT ALONG NE 50TH STREET, AND THE 1 FOOT ALLEY DEDICATION.
SCL REQUIRES 13 FOOT SETBACK FROM PRINCIPAL AND 7.5 FOOT SETBACK FROM SECONDARY LINES.
METRO REQUIRES 5' CLEARANCE FROM OPERABLE WINDOWS TO TROLLEY GUY WIRE ANCHORS.

23.47A.016

LANDSCAPE AND SCREENING
GREEN FACTOR OF .30 OR GREATER
STREET TREES SHALL BE PROVIDED ON 15TH AVENUE WITHIN A LANDSCAPE STRIP LOCATED AT THE BACK OF EXISTING CURB COMBINED WITH A 5'-6" DEDICATION TO ACCOMMODATE PEDESTRIAN SIDEWALK, MAINTAIN STREET TREES ALONG 50TH AVENUE AND PROVIDE A 3' DEDICATION TO ACCOMMODATE PEDESTRIAN SIDEWALK PER PRE-APPLICATION GUIDANCE FROM SDOT.
SCREENING AT SURFACE PARKING AREAS IS REQUIRED WHERE MORE THAN 20 PARKING SPACES
SCREENING FOR DUMPSTERS: 6 FOOT HIGH UNLESS WITHIN STRUCTURE

SCREENING FOR PARKING GARAGE OCCUPYING ANY PORTION OF THE STREET-LEVEL STREET-FACING FAÇADE BETWEEN 5 AND 8 FEET ABOVE SIDEWALK GRADE: 5' DEEP LANDSCAPE AREA ALONG STREET LOT LINE OR SCREENING BY THE EXTERIOR WALL OF THE STRUCTURE OR 6' HIGH FENCE BETWEEN THE STRUCTURE AND LANDSCAPED AREA

TABLE C

EXHIBIT B



LAND USE SUMMARY

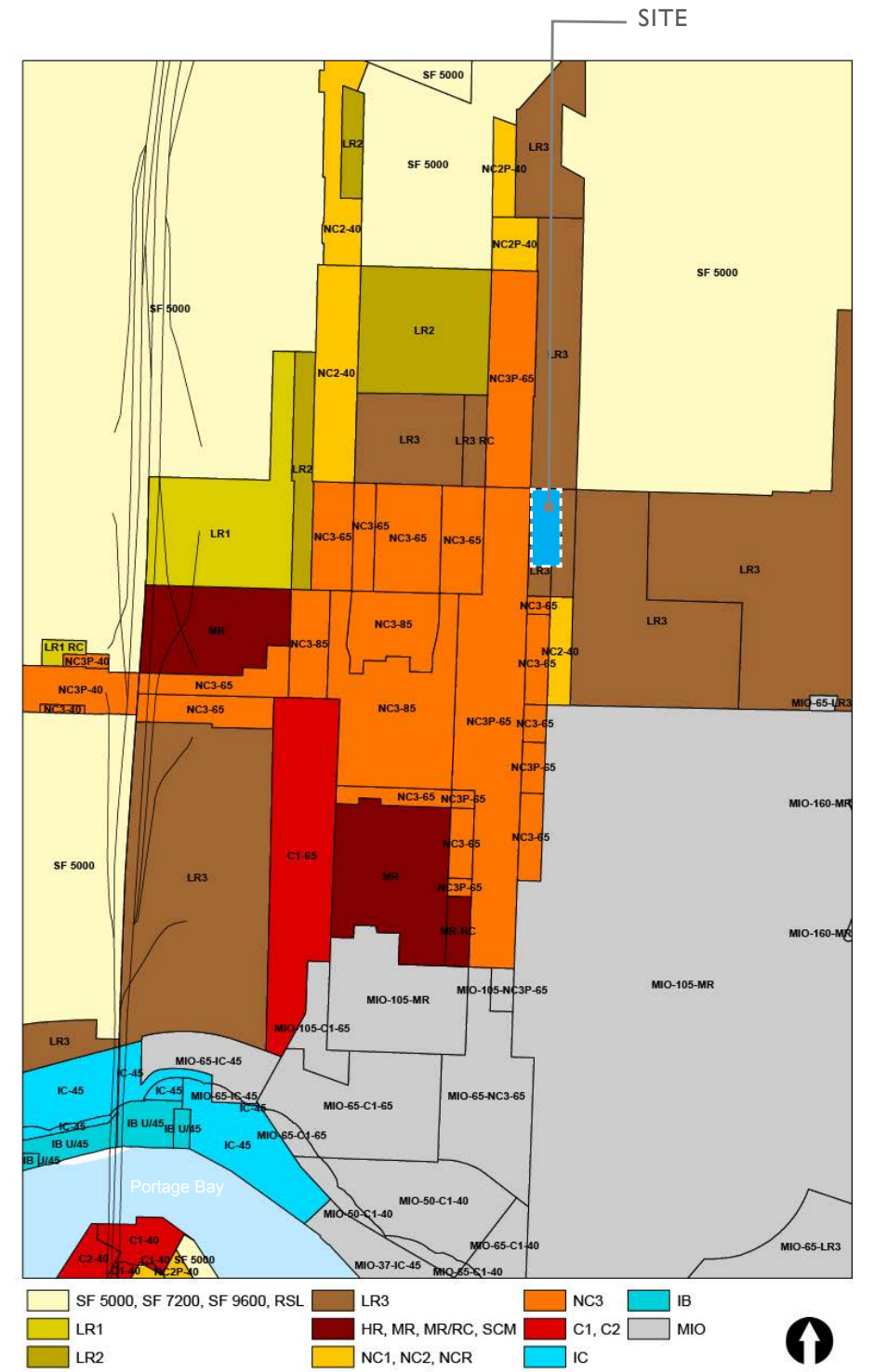
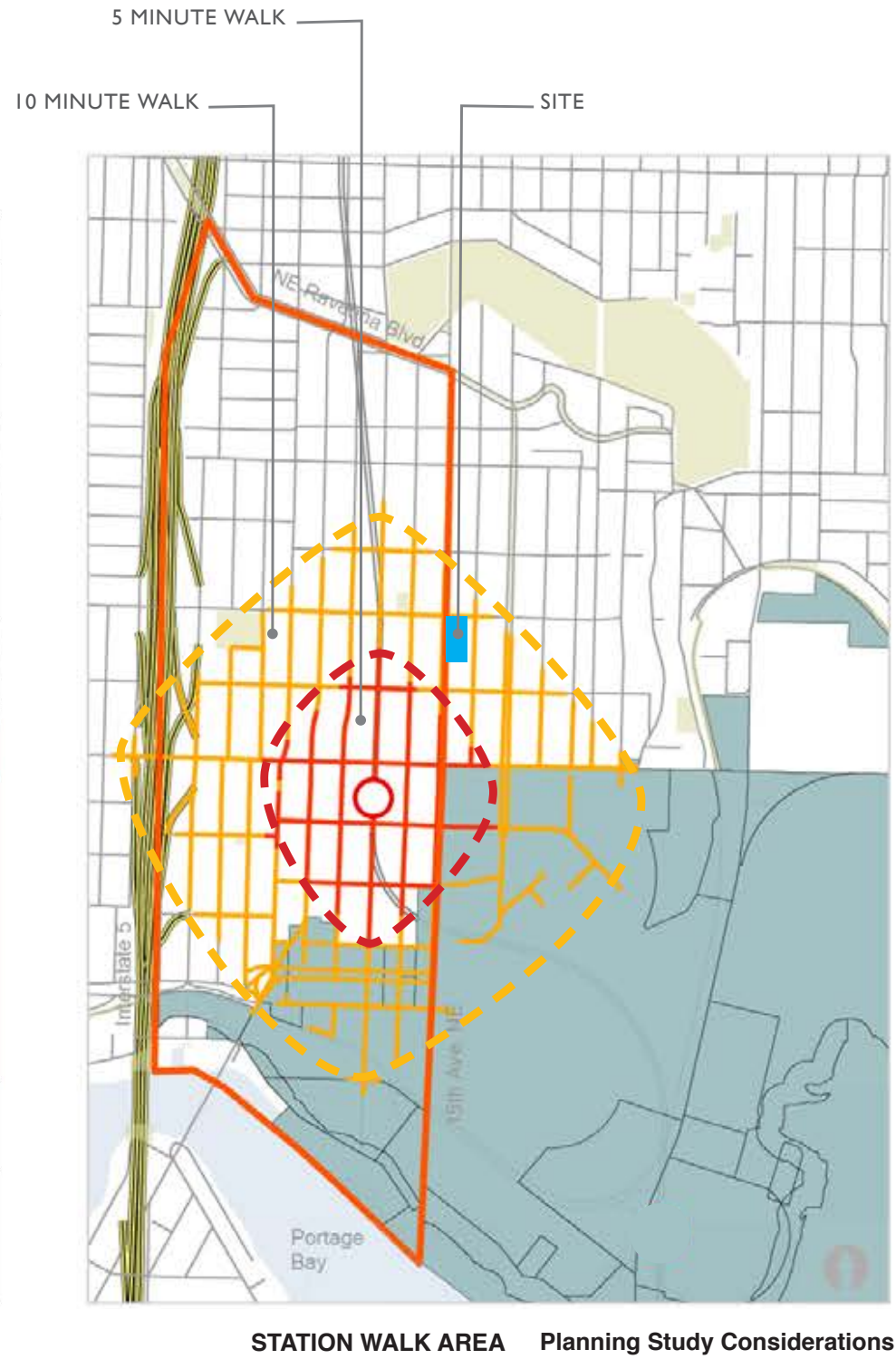
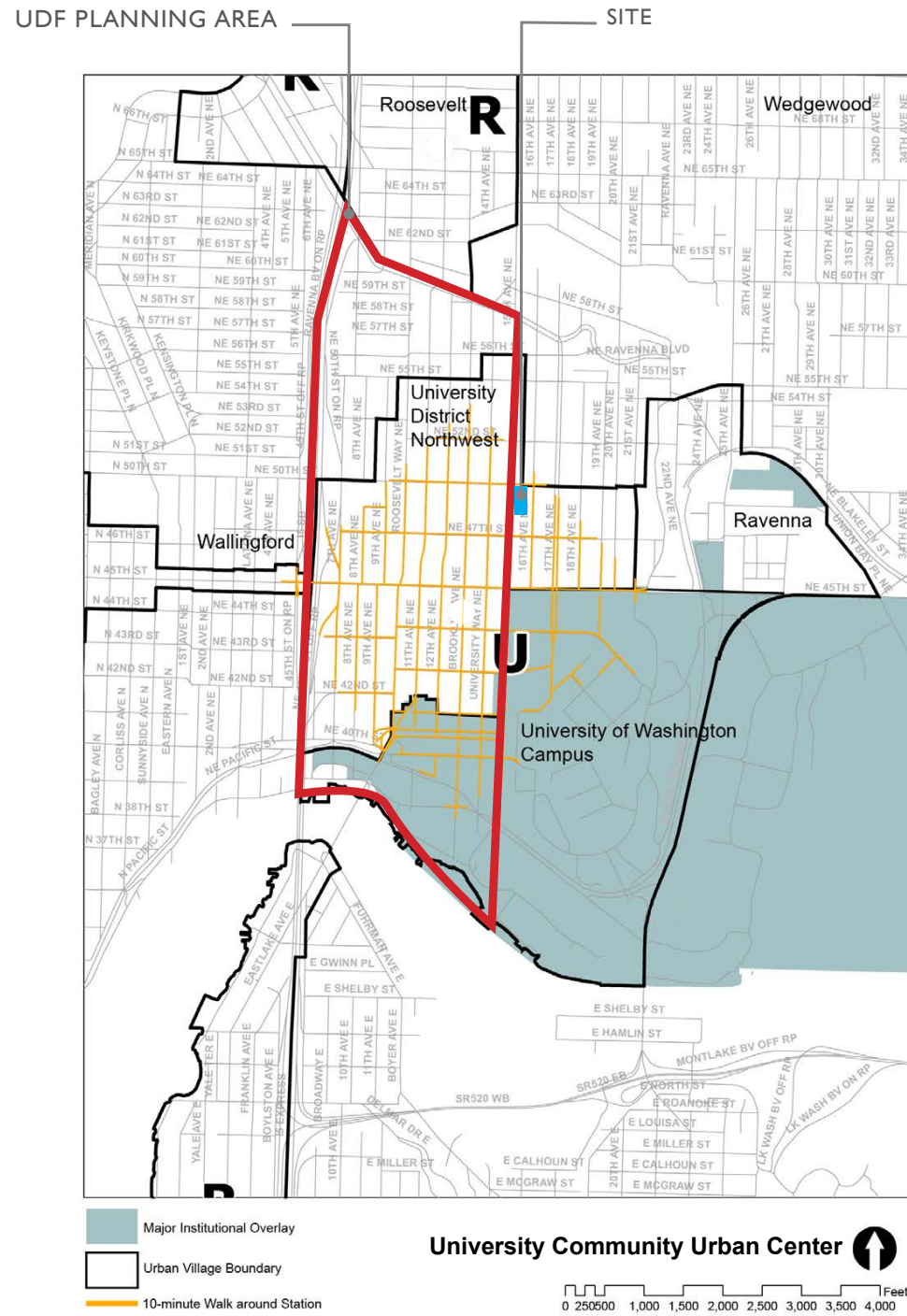
23.47A.024	AMENITY AREA REQUIRED AT 5% OF THE TOTAL GROSS FLOOR AREA IN RESIDENTIAL USE EXCLUDING AREA FOR ACCESSORY PARKING OR MECHANICAL EQUIPEMNT. COMMON AMENITY AREAS SHALL HAVE A MINMUM HORIZONTAL DIMENSION OF 10 FEET AND 250 SF PRIVATE AMENITY SHALL HAVE MINIMUM AREA OF 60 SF AND MINIMUM 6 FEET HORIZONTAL DIMENSION
23.47A.030	PARKING AND LOADING
23.54.015 TABLE A	NO VEHICLE PARKING REQUIRED FOR RESIDENTIAL AND NON-RESIDENTIAL USES IN URBAN VILLAGE OR STATION OVERLAY DISTRICT
23.54.015 TABLE E	BICYCLE PARKING REQUIRED AT 1 PER 4 DWELLING UNITS REQUIRED AT 1 PER 2,000 SF OFFICE SPACE
23.54.030	60 STALL MINIMUM VEHICLE PARKING FOR UCC IS REQUIRED BY P&S IF PARKING IS VOLUNTARILY PROVIDED THEN IT MUST MEET DIMENSIONAL REQUIREMENTS LARGE 8.5' X 19' 24' ASILE AT 90 DEG. PARKING MEDIUM 8' X 16' 22' ASILE AT 90 DEG. PARKING SMALL 7.5' X 15' 20' ASILE AT 90 DEG. PARKING BARRIER FREE 8' WIDE WITH 5' ACCESS ASILE BF VAN 8' WIDEX 19' LONG WITH 8' ACCESS ASILE
23.54.030.B.1	MINIMUM 60% MEDIUM STALLS FOR RESIDENTIAL USES
23.54.030.B.2	MIN 35% AND MAX. 65% STRIPED SMALL, MIN. 35% STRIPED LARGE FOR NON-RESIDENTIAL USE MIN. VEHICLE CLEARANCE SHALL BE AT LEAST 6'-9" ON AT LEAST ONE FLOOR. THERE SHALL BE AT LEAST ONE DIRECT ENTRANCE FROM THE STREET THAT IS AT LEAST 6'-9" IN HEIGHT FOR ALL PARKING GARGES ACCESSORY TO NON-RESIDENTIAL USES
23.54.030.D	DRIVEWAY SHALL BE 20' WIDE FOR TWO WAY RESIDENTIAL TRAFFIC DRIVEWAY SHALL BE 22' WIDE FOR TWO WAY COMMERCIAL TRAFFIC DRIVEWAY SHALL CONFORM TO THE MINIMUM 18' TURNING PATH RADIUS IN EXHIBIT B 23.54.030
23.54.030.E	TURNING AND MANUVERING AREAS SHALL BE LOCATED ON PRIVATE PROPERTY, EXCEPT THAT ALLEYS MAY BE CREDITED AS ASILE SPACE
23.54.030 Exhibit E	10' SIGHT TRIANGLE REQUIRED AT DRIVEWAY INTERSECTION
23.47A.032	PARKING LOCATION AND ACCESS IN NC ZONES ACCESS TO PARKING SHALL BE FROM THE ALLEY IF THE LOT ABUTS AN ALLEY IMPROVED TO THE STANDARDS OF SECTION 23.53.030.C OR THE DIRECTOR DETERMINES THAT ALLEY ACCESS IS FEASIBLE AND DESIREABLE TO MITIGATE PARKING ACCESS IMPACTS.
23.53.030.C TABLE B	THE MINIMUM ROW WIDTH FOR AN ALLEY TO BE CONSIDERED IMPROVED FOR LR3 IS 10' AND FOR NC2 IS 12' PAVING. THE CURRENT ALLEY WIDTH IS 14' SO IS CONSIDERED IMPROVED.

23.53.030.C TABLE C	MINIMUM REQUIRED WIDTH FOR AN EXISTING ALLEY SHALL BE 16' IN LR3 AND NC2 ZONES
23.53.030.F	A 1' DEDICATION IS REQUIRED OF EACH SIDE TO MEET 16' MINIMUM WIDTH
23.47A.032.B	PARKING SHALL NOT BE LOCATED BETWEEN A STRUCTURE AND A STREET LOT LINE. PARKING WITHIN A STRUCTURE SHALL BE SPEPARATED FROM STREET-LEVEL, STREET-FACING FACADES BY ANOTHER PERMITTED USE. PARKING TO THE SIDE OF A STRUCTURE SHALL NOT EXCEED 60 FEET OF STREET FRONTAGE. EXCEPTION: ACCESS TO OFF-STREET PARKING MAY BE FROM A STREET IF, DUE TO THE RELATIONSHIP OF AN ALLEY TO THE STREET SYSTEM, USE OF THE ALLEY FOR PARKING ACCESS WOULD CREATE A SIGNIFICANT SAFTEY HAZARD AS DETERMINED BY THE DIRECTOR.
23.47A.032.D	
23.54.040 RESIDENTIAL	SOLID WASTE AND RECYCLABLE MATERIALS STORAGE AND ACCESS 100 UNITS = 575 SF UNITS OVER 100 @ 4 SF/UNIT = <u>33x4=132</u> SF 707 SF -15% IF MIN. 20' STORAGE AREA WIDTH <u>103.65</u> SF 588 SF NON RESIDENTIAL 0 - 5,000 SF 82 / 2 <u>41</u> SF 547 SF



CONTEXT ZONING

*Maps used in this section taken from the *University District Urban Design Framework*, June 20, 2013 & the *University District Urban Design Framework – Existing Condition Report*, June 2012

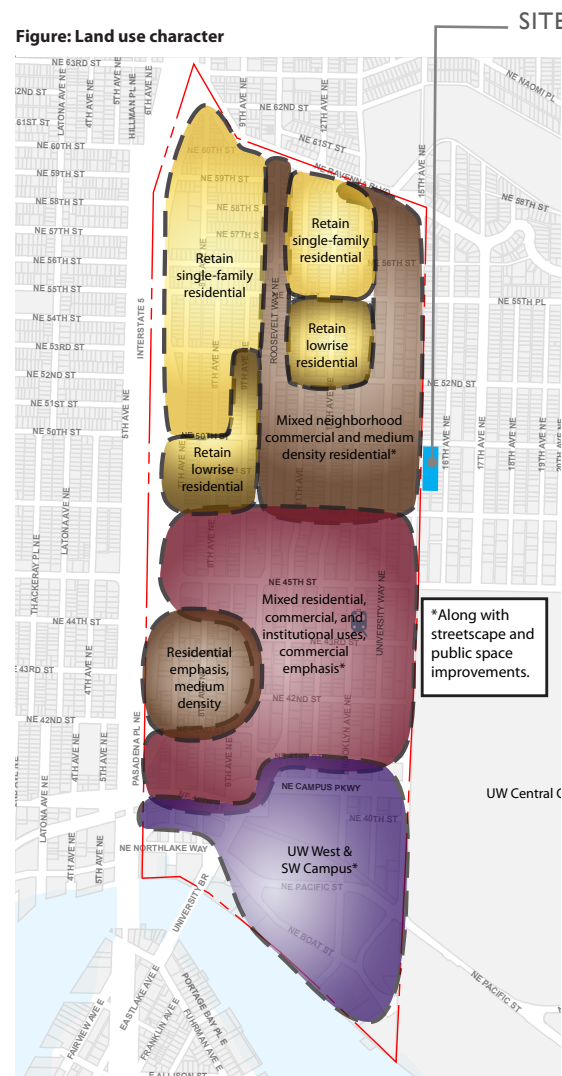


FUTURE LAND USE CHARACTER

THE U DISTRICT URBAN DESIGN FRAMEWORK CALLS FOR INCREASED BUILDING HEIGHTS WITHIN THE STUDY AREA TO STIMULATE REDEVELOPMENT AS A MEANS TO ACHIEVE DENSITY IN SUPPORT OF; HOUSING AND JOB GROWTH HEAR FUTURE LIGHT RAIL, INCREASED MARKET-RATE AND WORKFORCE HOUSING, NEW PUBLIC AMENITIES, AND A VARIETY OF BUILDING FORMS. THE JUNE 2013 REPORT IDENTIFIES THE AREA FOR INCREASED HEIGHT AS WELL AS A TIMELINE FOR THEIR IMPLEMENTATION BY 2015. THE SITE IS ACROSS THE STREET FROM THE INCREASED UDF AREA IDENTIFIED AS A MIXED NEIGHBORHOOD COMMERCIAL AND MEDIUM DENSITY RESIDENTIAL AND AS SUCH WILL ACT AS A TRANSITION BETWEEN THE LOW-RISE AND UP-ZONED UDF AREAS. PROPOSED HEIGHTS ALONG THE WEST SIDE OF THE BLOCK AT BETWEEN 65'-85' WITH ADDED HEIGHT FOR INCENTIVE ZONING WHERE PROJECTS INCORPORATE USES SUCH AS WORKFORCE HOUSING, LARGER-SIZED RESIDENTIAL UNITS TO ACCOMMODATE FAMILIES, SUPPORT SERVICES AND FACILITIES.

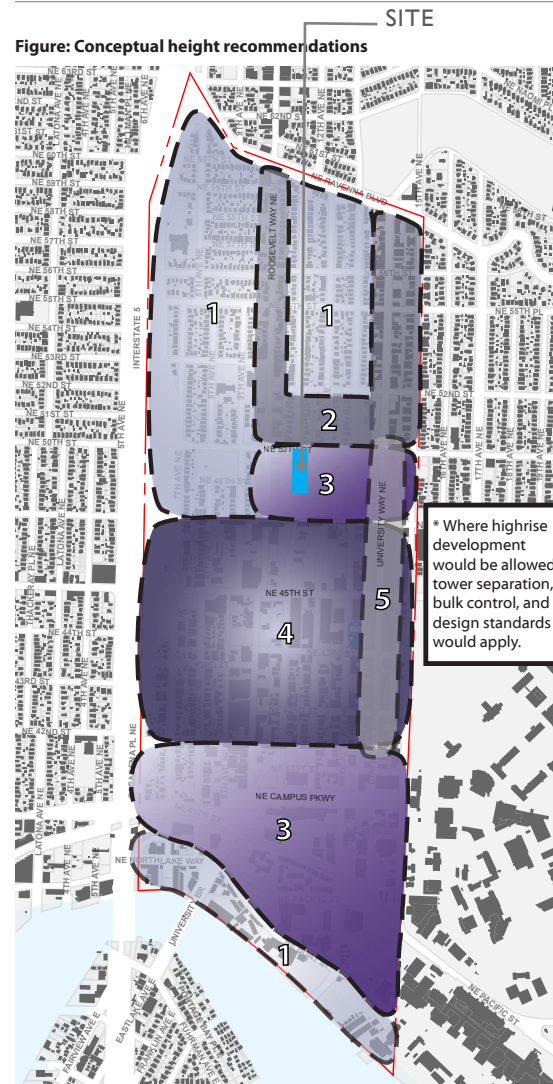
RECOMMENDATIONS

Figure: Land use character

U District Urban Design Framework
Seattle DPD

RECOMMENDATIONS

Figure: Conceptual height recommendations

U District Urban Design Framework
Seattle DPD

LEGEND

The height ranges described below are preliminary recommendations for new development, based on community input. They will inform the zoning alternatives that DPD will study in more detail.

- 1 These areas should continue to have predominantly lowrise and single family development, with maximum heights around 30-45'.
- 2 These areas may be appropriate for moderate increases in height, with maximum heights around 65-85' (continuing to favor wood frame, "5 over 1" or "5 over 2" construction).
- 3 These areas are appropriate for increased height and density, but not to the extent of area 4. Evaluate heights ranging from 85-240'.
- 4 This is the part of the neighborhood best suited for increased height and density. Evaluate heights ranging from 120-300'.
- 5 Evaluate a wide range of heights along the Ave, along with additional standards to help preserve the distinctive, fine-grained storefront character.

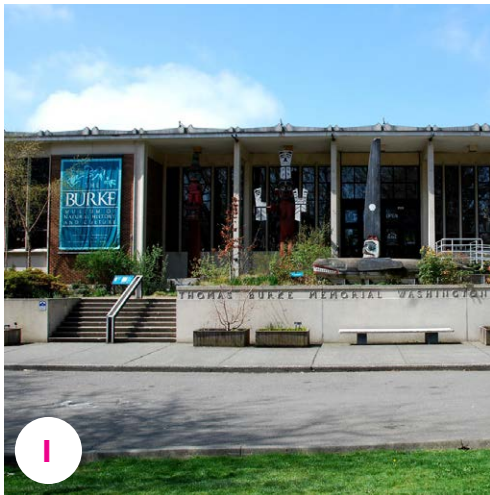
* Where highrise development would be allowed, tower separation, bulk control, and design standards would apply.



CONTEXT LAND USE – INSTITUTIONAL & COMMERCIAL

CONTEXT ZONING & LAND USE

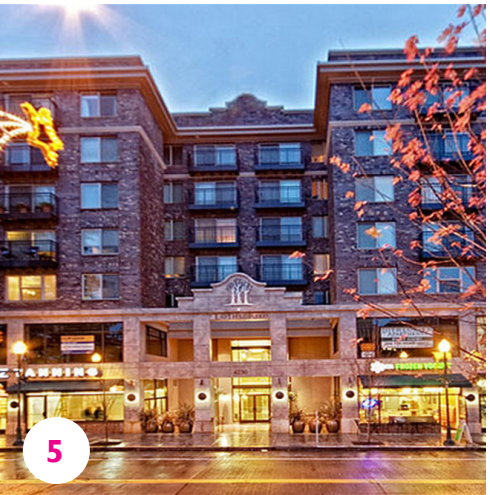
THE ZONE EDGE LOCATION IS INDICATED BY THE VARIETY OF SURROUNDING USES INCLUDING INSTITUTIONAL, COMMERCIAL AND HOUSING. A RETAIL AND MIXED USE CORE IS SOUTHWEST WHILE SINGLE FAMILY AND LOWRISE HOUSING IS LOCATED TO THE NORTH AND EAST OF THE SITE. BUILDINGS COVER A BROAD RANGE OF TYPES, STYLES, AND PERIODS. WOOD FRAME AND LATE VICTORIAN BUILDINGS AND STOREFRONTS SIT NEAR EARLY 20TH CENTURY BRICK BUILDINGS, CAMPUS GOTHIC BUILDINGS, AND CONTEMPORARY DEVELOPMENTS, AS WELL AS MANY FEATURELESS COMMERCIAL BUILDINGS FROM THE 1960'S-1990'S. NO ONE ARCHITECTURAL STYLE IS DOMINANT. IMAGES ON THIS AND FOLLOWING PAGES HIGHLIGHT A SELECTION OF THE BUILDING USES AND ARCHITECTURAL VARIETY.



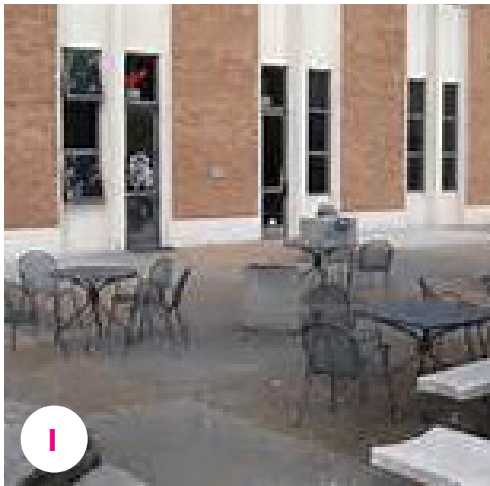
THE BURKE MUSEUM



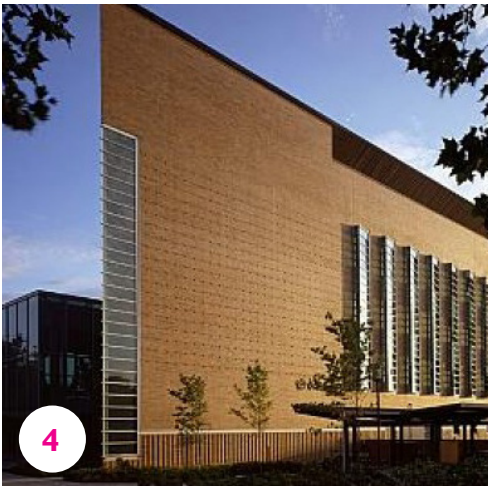
UW SCHOOL OF LAW



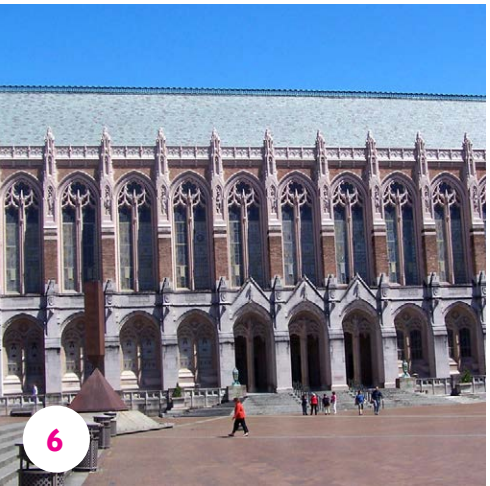
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THE BURKE MUSEUM



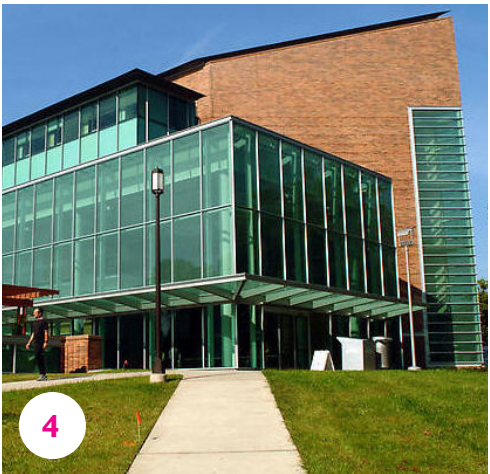
UW SCHOOL OF LAW



SUZZALLO LIBRARY



UW SCHOOL OF SOCIAL WORK



UW SCHOOL OF LAW

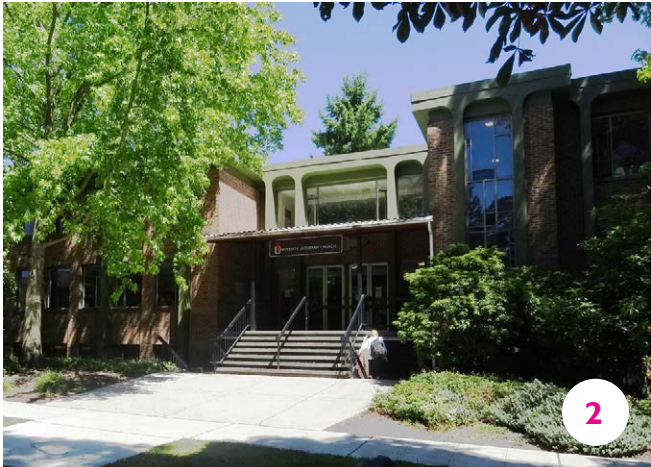


UW FOSTER SCHOOL OF BUSINESS

CONTEXT LAND USE- FAITH COMMUNITY



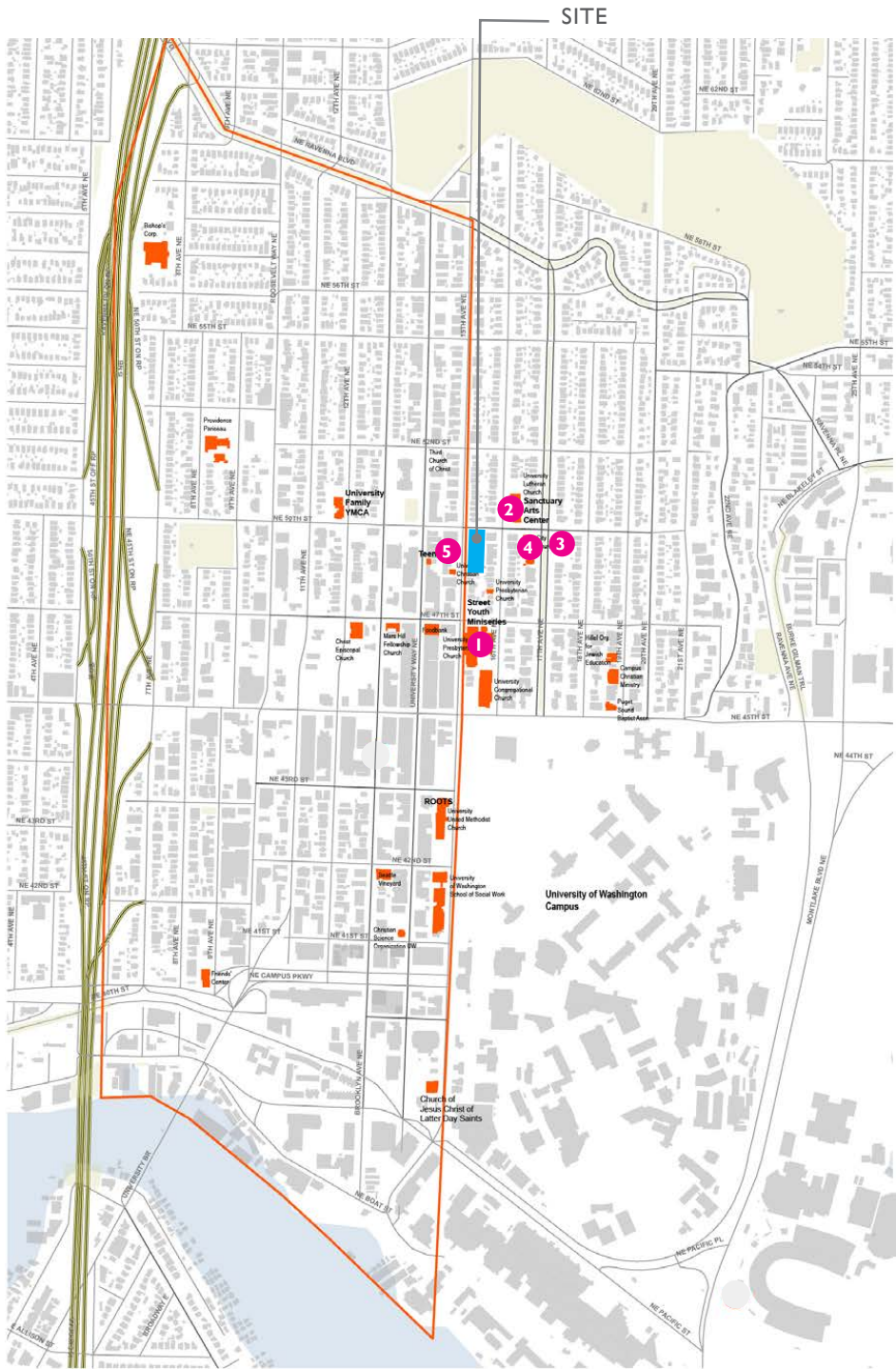
UNIVERSITY PRESBYTERIAN CHURCH



UNIVERSITY LUTHERAN CHURCH



THE CITY CHURCH



Location of Social Services and Faith Community



UNIVERSITY OF WASHINGTON HILLEL



UNIVERSITY CHRISTIAN CHURCH



UNIVERSITY PRESBYTERIAN CHURCH

CONTEXT LAND USE – HOUSING



NEIGHBORING SINGLE FAMILY RESIDENTIAL ACCROSS ALLEY



SINGLE FAMILY CONVERTED TO OFFICE



REPURPOSED RESIDENTIAL – ROOMING HOUSE



ADJACENT MID-RISE RESIDENTIAL – DATUMS & BRICK



ADJACENT SINGLE-FAMILY RESIDENTIAL



RESIDENTIAL – SINGLE FAMILY WITH ELEVATED STOOP



SINGLE FAMILY, LOWRISE AND MULTIFAMILY SURROUND THE SITE AND ARE FOCUSED TO THE NORTHERN UDF STUDY AREA. THESE USES INCLUDE SINGLE FAMILY HOMES CONVERTED INTO SMALLER ROOMS FOR GROUPS OF STUDENTS.



ACICIA COURT CORNER ELEMENT



POPLAR HALL– PATTERNED WINDOWS

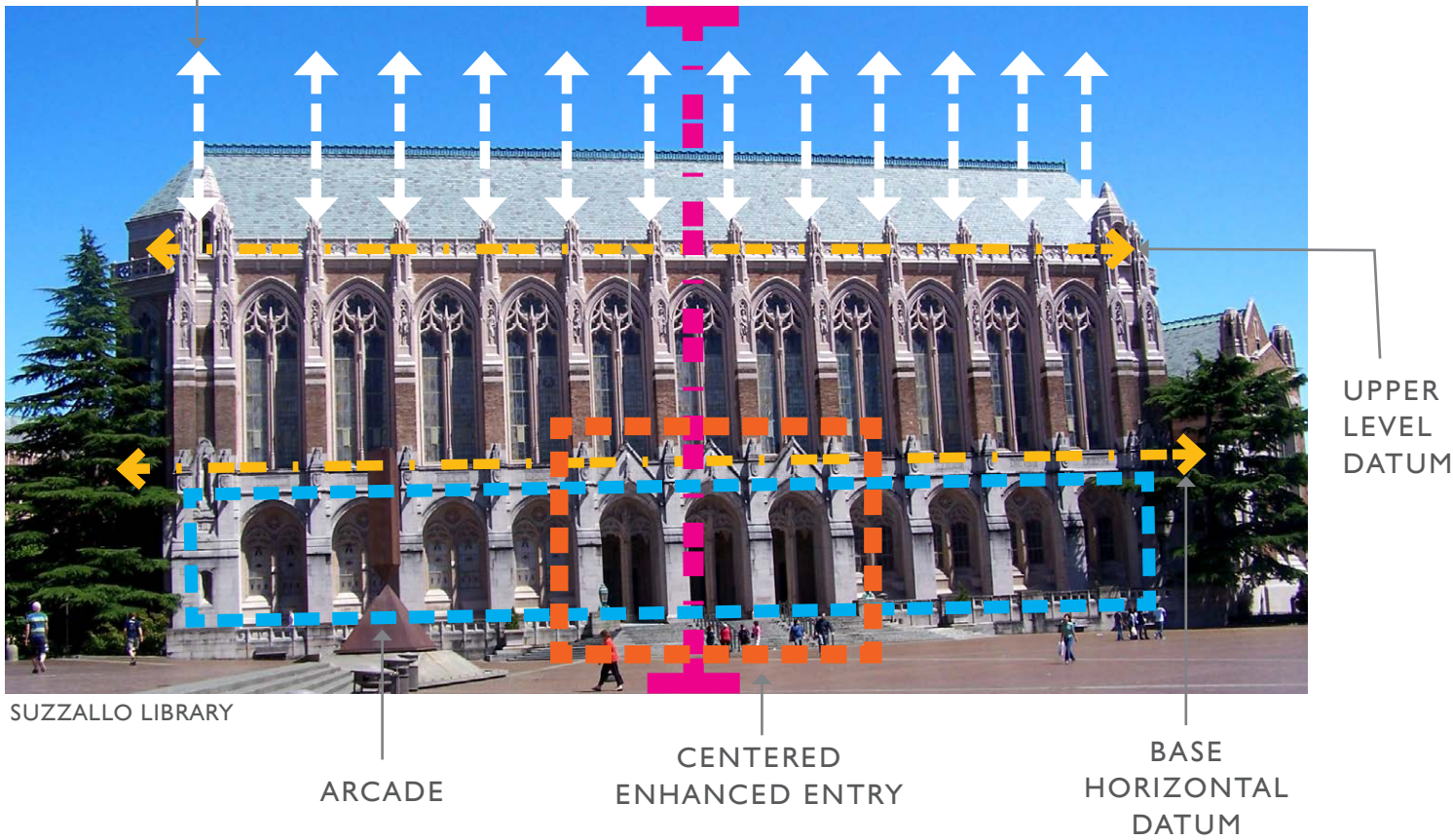
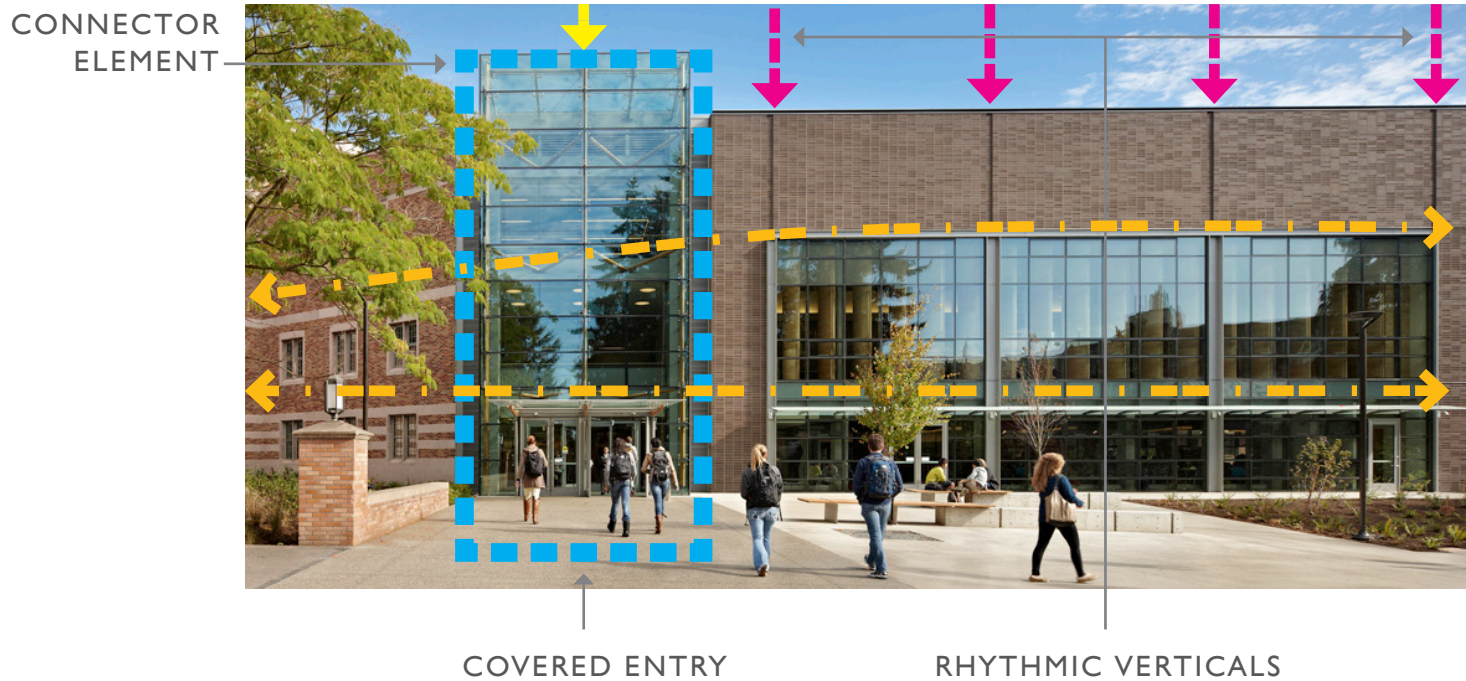
CONTEXT ANALYSIS – ARCHITECTURAL INFLUENCES

THE VARIETY OF USES & ARCHITECTURAL STYLES CREATE A RICH TAPESTRY AND SENSE OF PLACE. COMMERCIAL, MIXED USE, INSTITUTIONAL RELIGIOUS AND COLLEGIATE STRUCTURES INCORPORATE CAMPUS GOTHIC, REVIVAL AND MODERN ARCHITECTURAL STYLES. MULTIFAMILY HOUSING STRUCTURES INCORPORATE REVIVAL, LATE VICTORIAN, AND MODERN ARCHITECTURAL STYLES. RESIDENTIAL STRUCTURES INCORPORATE CRAFTSMAN, TUDOR, AND REVIVAL STYLES.

INSTITUTIONAL GOTHIC REVIVAL CHARACTERISTICS INCLUDE; VERTICAL ORIENTATION, LANCET-ARCHED OPENINGS, BUTTRESSES TOPPED WITH FINIAL, STEEP GABLE PITCHED ROOF, PARAPET, CRENELLATION AT RAILINGS OR LOW WALLS, RECESSED ENTRIES FRAMED BY ARCHES, SURROUNDED BY STONE, MOLDINGS, NICHES FOR STATUES, AND STONE CARVINGS, TALL NARROW TRACERY WINDOWS CAPPED WITH STONE DETAILING, QUATREFOIL (CLOVER), PLAQUE, RELIEF SCULPTURE, DETAIL ELEMENTS.

MULTIFAMILY AND REVIVAL ARCHITECTURAL CHARACTERISTICS INCLUDE; LARGE SIMPLE TWO OR THREE-STORY RECTANGLE, TYPICALLY BRICK, SYMMETRICAL, DENTIL MOLDINGS IN THE CORNICE, CENTERED AND PROMINENT ENTRANCE SHELTERED BY A PORTICO TOPPED WITH A FAN SHAPED TRANSOM LIGHT, GEOMETRIC DECORATIONS CONFINED TO ENTRY ELEMENT INCLUDING FAN SHAPE OR ELLIPTICAL GABLE WINDOWS, PALLADIAN WINDOWS, DECORATIVE BALUSTRADE, MULTI-PANEL DOUBLE-HUNG WINDOWS, SIDING MATERIALS INCLUDE BRICK, SHINGLE OR WOOD CLAPBOARD SIDING, AND CLASSICAL COLUMNS OR DENTILS UNDER EAVES.

MODERN ARCHITECTURAL CHARACTERISTICS INCLUDE; SIMPLE MASSING FORMS, SIMPLIFIED DETAILING LIMITED AND FOCUSED AT PEDESTRIAN LEVEL, CONTRASTING WALL MATERIALS AND TEXTURES, UNUSUAL WINDOW SHAPES AND PLACEMENTS, AND FACADE RESPONSES TO ENERGY CONSERVATION CONSIDERATIONS.

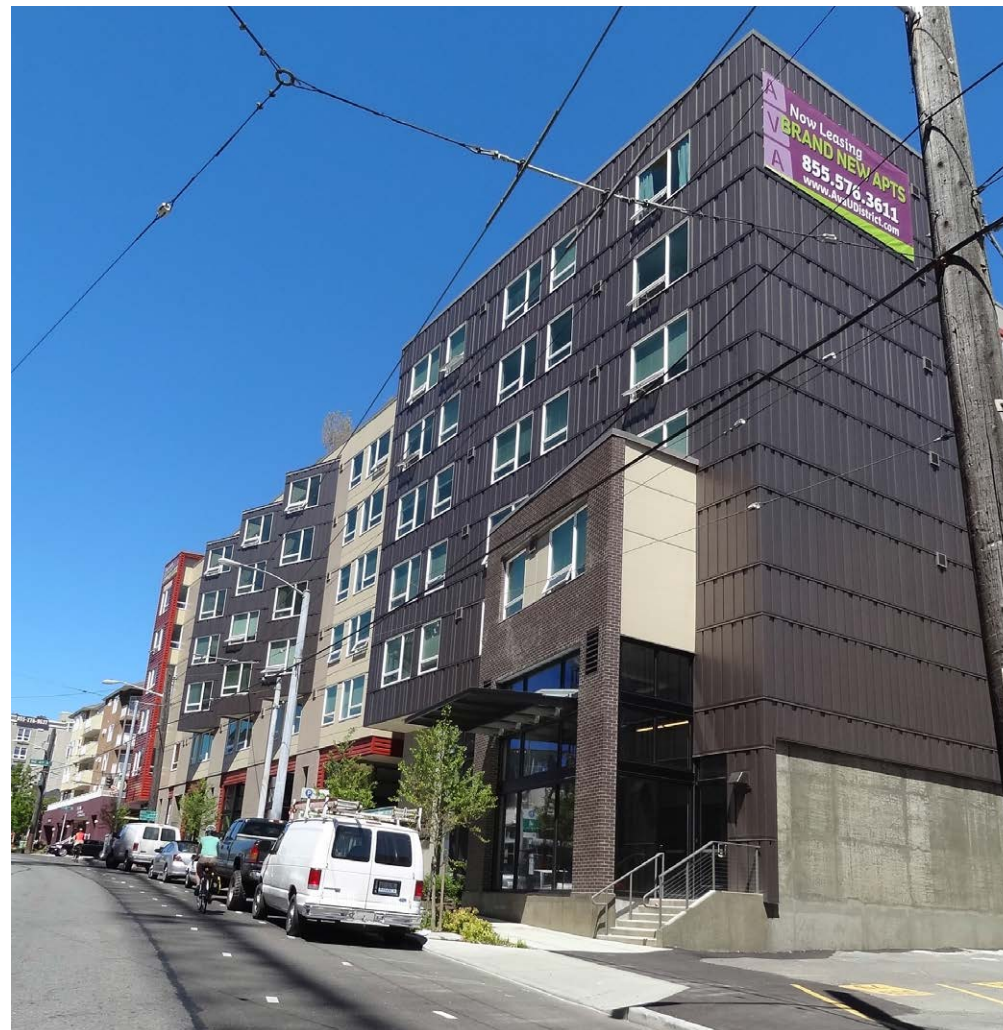


CONTEXT ANALYSIS — MIXED USE STREET EDGE

THE CONDITION OF STREET EDGES AND THE QUALITY OF THE PEDESTRIAN STREETScape IS PRIMARILY LINKED TO THE AGE OF REDEVELOPMENT. EXISTING NEIGHBORHOOD SIDEWALKS FAIL TO MEET SDOT REQUIREMENTS FOR MINIMUM WIDTH RESULTING IN A HARD STREETScape WITHOUT MUCH IF ANY LANDSCAPING TO SOFTEN THE EDGE AND BUFFER PEDESTRIANS FROM TRAFFIC AND ADJACENT DEVELOPMENT. ENHANCEMENT OF THE STREETScape IN NEWER DEVELOPMENTS HAVE INCLUDED; A LANDSCAPE STRIP AT BACK OF CURB, WIDENED SIDEWALKS, LANDSCAPE ALONG BUILDING EDGES, PARTICULARLY AT GRADE RELATED RESIDENTIAL AREAS, INCREASED OPEN SPACE AND OVERHEAD WEATHER PROTECTION AT BUILDING ENTRIES. MULTIFAMILY BUILDINGS FREQUENTLY HOLD THE STREET EDGE ABOVE GRADE AND SET BACK IN VARYING DEGREES AT THE SIDEWALK LEVEL TO PROVIDE INCREASED PEDESTRIAN CIRCULATION AND OPENScape.



AVA APARTMENTS U DISTRICT — SPECIAL CORNER TREATMENT



AVA APARTMENTS — STREET FRONT ENTRY



GOSSETT PLACE — OVERHANGING BAYS ALONG 12TH AVE NE

CONTEXT ANALYSIS — GRADE RELATED RESIDENTIAL STREET EDGE



AVA APARTMENTS – STREET LEVEL USES TO PROPERTY | STRUCTURE BEYOND



ELLIPSE APARTMENTS – TOWN HOME GRADE RELATED RESIDENTIAL



ROYAL MANOR APARTMENTS – ELEVATED STREET EDGE ALONG 50TH



WAYFARER PROPERTIES – ENTRY DETAIL @ 15TH

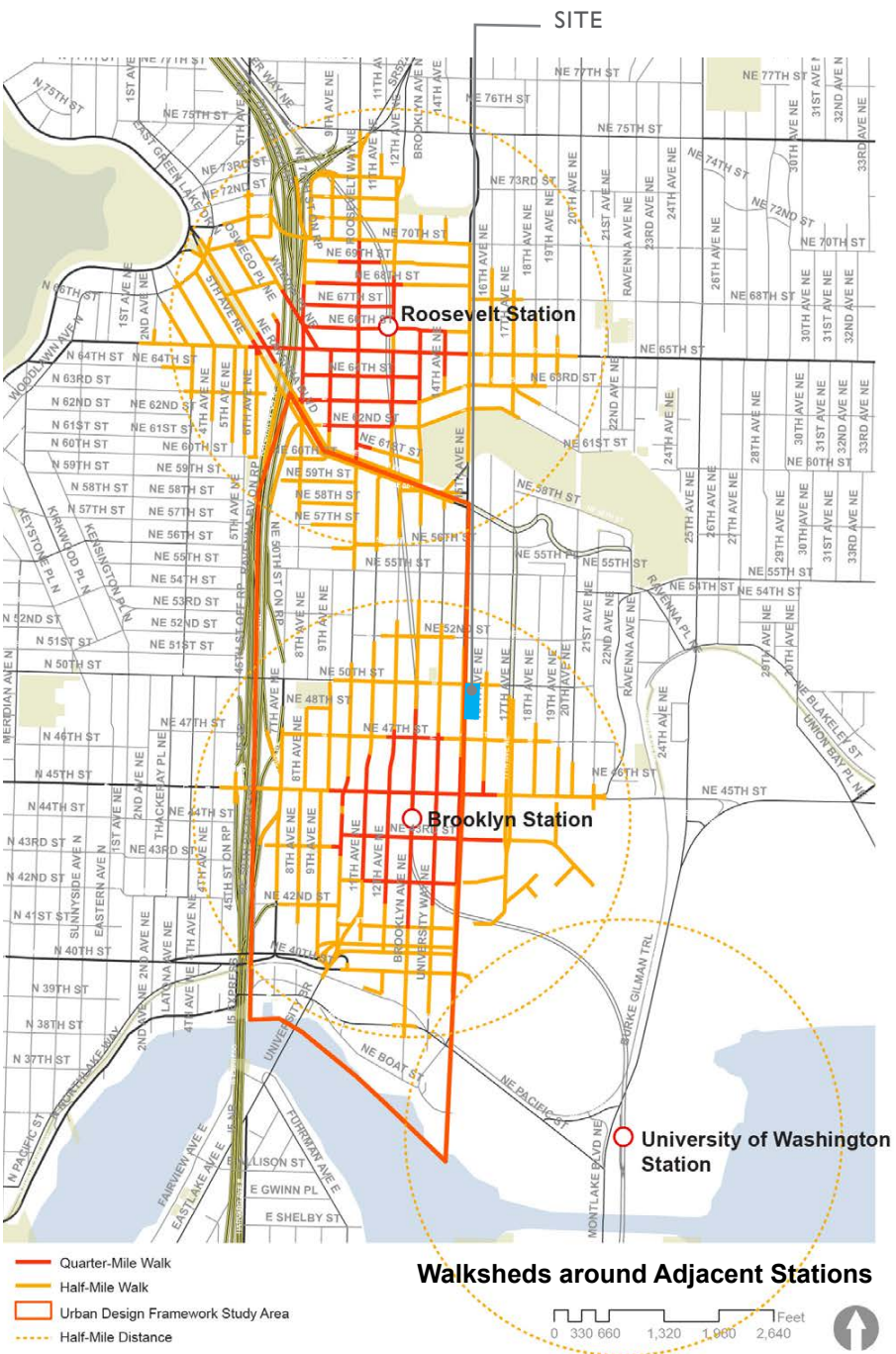
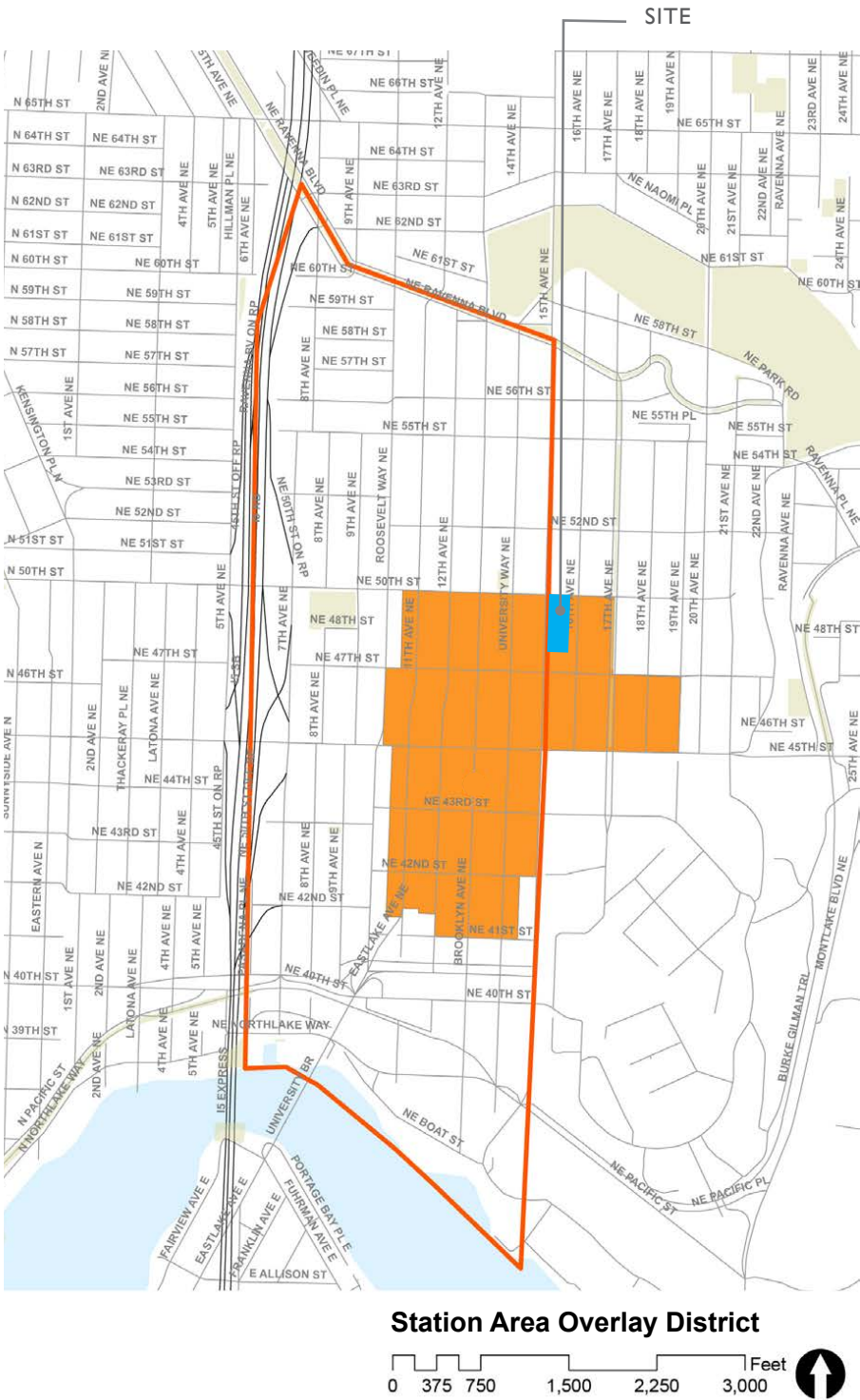


5043 BROOKLYN AVE NE APT – CONNECTOR ELEMENT/ STREET LEVEL USE

ACCESS AND CONNECTIONS

15TH AVE. NE IS A REGIONAL CONNECTOR AND A MAJOR TRANSIT CORRIDOR THAT FORMS THE WESTERN EDGE OF THE UNIVERSITY OF WASHINGTON CENTRAL CAMPUS AND THE EASTERN EDGE OF THE NEIGHBORHOOD BUSINESS DISTRICT. THE STREET CARRIES A NUMBER OF HEAVILY TRAVELED BUS ROUTES AS WELL AS SIGNIFICANT PEDESTRIAN ACTIVITY. THE WESTERN EDGE OF THE CAMPUS ALONG THIS STREET IS COMPRISED OF A LOW WALL THAT CREATES A HARD EDGE BETWEEN THE RIGHT-OF-WAY AND ABUTTING CAMPUS PROPERTY. PREVIOUS AREA PLANS RECOMMENDED PEDESTRIAN-ORIENTED IMPROVEMENTS ON LAND ABUTTING NE 50TH ST., THAT WOULD PROVIDE MORE VISUALLY WELCOMING ENTRANCES TO THE CAMPUS. THE TYPICAL RIGHT-OF-WAY WIDTH IS 80’.

IN SPITE OF A CONTINUOUS, REGULAR GRID, CONDITIONS THAT CHALLENGE PEDESTRIAN CIRCULATION INCLUDE LONG N/S BLOCKS, DISCONTINUOUS STREET EDGES, AND LARGE AREAS OF SURFACE PARKING. EAST-WEST CONNECTIONS, ESPECIALLY ALONG NE 43RD ST. AND NE 42ND ST. ACROSS THE AVE AND 15TH AVE. NE LACK ACTIVE STREET-FACING BUILDING FRONTAGES OR A SENSE OF ENCLOSURE, THEREBY LIMITING PEDESTRIAN ACTIVITY.



ACCESS AND CONNECTIONS

