



credit: chas redmond



credit: joe mabel



3078 SW AVALON WAY

**caron**

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PROJECT HISTORY

The Early Design Guidance meeting was held on September 13, 2012

The Master Use Permit was submitted on February 12, 2013

The Recommendation Meeting #1 was held on November 20, 2013

Soon after the September 13 EDG meeting, Caron Architecture was approached to design a second building on the three parcels to the north of this project. That project under DPD # 3014100, was slated to be designed concurrently with this proposal, 3013303. The schedule for this proposed project was slightly delayed so the two project schedules could be synchronized. Both MUP sets were submitted within weeks of each other but the buildings were placed on hold by the owners during the MUP review period due to a myriad of factors, and the application for the other project was eventually cancelled. The decision was made late summer to continue moving forward with this project only. Comments and concerns raised through the design review process for both buildings have been incorporated into this design. This packet specifically addresses comments stemming from the Recommendation #1 meeting.

EXISTING SITE

The proposed project is zoned MR and abuts a SF 5000 zone across the alley to the west. A height limit of 60’ is allowed in this zone.

The site has approximately a 25’ slope from east to west and will have views of downtown Seattle to the north, North Delridge to the east, and Mt. Rainier to the southeast. It is currently terraced with rockery and concrete retaining walls and built out with single story structures.

The significant grade change from the alley to SW Avalon Way creates challenges for the site, as well as the adjacent properties. The drop in grade creates wonderful view opportunities and provides a diminished building height along the alley, adjacent to the SF 5000 zone. It also provides opportunities to access a multi-tiered underground parking garage from both the alley and SW Avalon Way, although no parking is required for the site. There are numerous newer multi-family buildings in the vicinity with garage access from SW Avalon Way.

PROJECT DESCRIPTION:

The proposed development will create an urban apartment building with street front residential units along SW Avalon Way and an enhanced pedestrian experience in the West Seattle Junction Hub Urban Village.

The project provides an exciting opportunity to create a high density infill project in one of the most scenic and lively neighborhoods in Seattle. Given the proximity to the Junction Hub Urban Village, high frequency transit, and breathtaking views, the site is situated perfectly to allow future residents the opportunity to enjoy all West Seattle has to offer. The proposed project will embrace and enhance the urban fabric of this burgeoning neighborhood.

Parking is not required for this site, although approximately 60 stalls will be provided in the structure. Vehicular access will be provided both from Avalon Way SW and from the alley to accommodate site grades.

The ground level of the preferred scheme consists of a residential lobby, underground parking garage, utilities, and five residential units facing the street with direct, individual street access.

The second level consists of nine residential units, a partially buried parking garage accessed off of the alley, as well as bicycle storage and utilities.

The third level contains 17 residential units, a common elevated courtyard, refuse and service rooms.

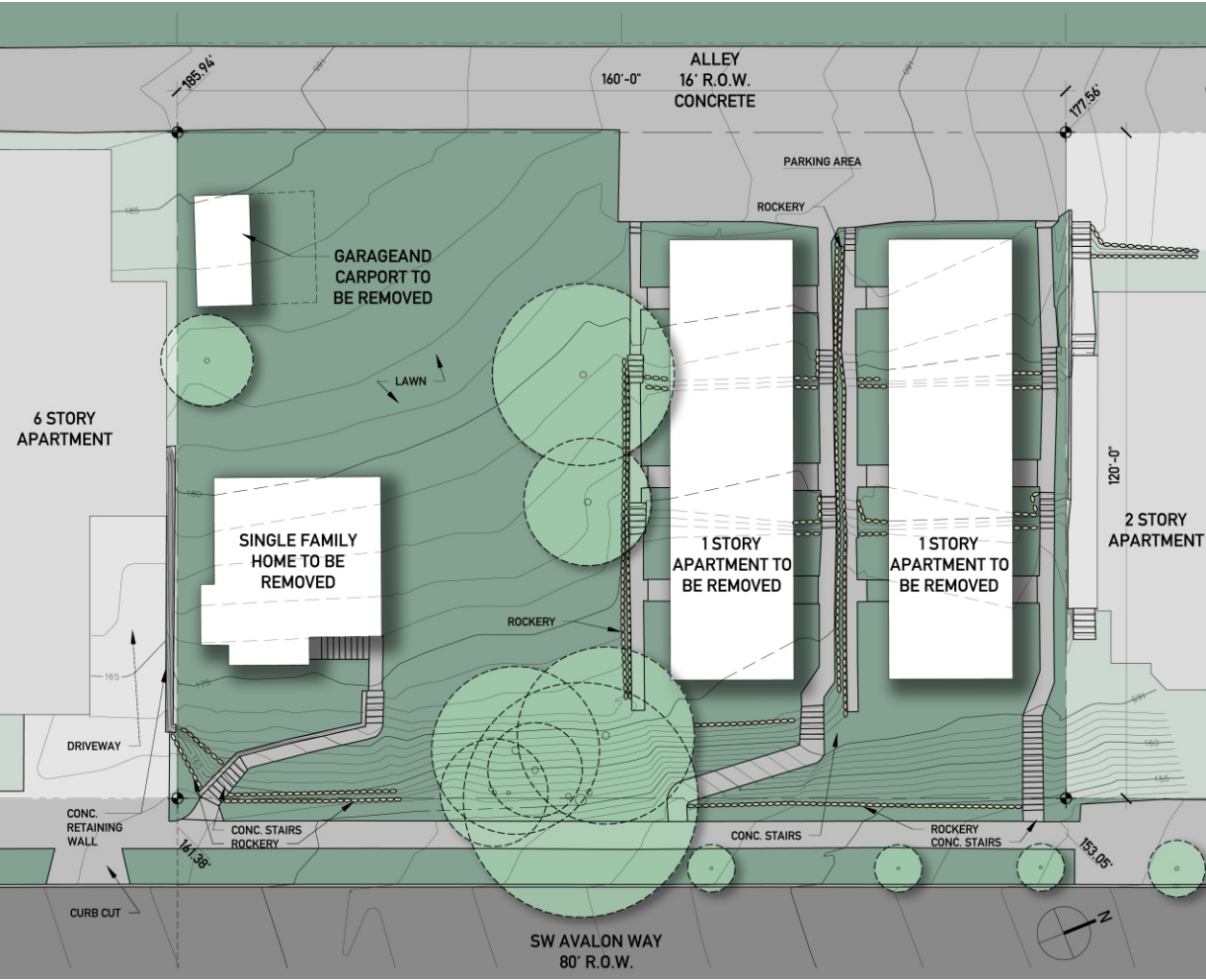
The fourth thru sixth floors each contain 18 units per floor, and the seventh floor contains 17 units. All floors contain a mix of unit sizes and bedroom counts.

The elevated courtyard, which sits atop the second level parking level’s roof, contains spaces for gathering, grilling and relaxation, as well as a grassy area to soften the space. There are also private amenity areas for the three units which have direct access to the courtyard. Parking will be located within the structure and vehicular access will be provided both from SW Avalon Way and from the alley to accommodate site grades.

DEVELOPMENT OBJECTIVES

1.16.2014      RECOMMENDATION 2 PACKAGE  
3078 SW AVALON WAY    DPD PROJECT #:3013303

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EXISTING SITE PLAN

DEVELOPMENT STATISTICS SUMMARY:

- Lot Size	19,196 SF	Floor Area Summary	
- FAR	3.2		
- Allowable FAR	61,427.20 SF	FAR SF	Units
- Proposed FAR	61,368.75 SF	Roof	645.36
- Parking Stalls	60 Stalls	7th Floor	9,494.70 17
		6th Floor	10,360.48 18
		5th Floor	10,866.29 18
		4th Floor	10,866.29 18
		3rd Floor	9,848.26 17
		2nd Floor	8,159.85 9
		1st Floor	1,127.52 5
		Total	61,368.75 sf 102 Units





- ZONING LEGEND**
- NC3-65 Neighborhood Commercial
  - LR 3 Low rise 3
  - LR 1 Low Rise 1
  - SF 5000 Single Family
  - MR Multi-Family

### ZONING MAP

This area of West Seattle is a mixture of residential uses, with single and multi-family structures and no commercial uses along SW Avalon Way. The alley abutting the west property line is the barrier between the single-family and multi-family zones. The newer projects appear to be built up to the rear setback against the alley. The urban tree canopy in the area is limited mainly to the older vintage multi-family sites and single-family zones, although SW Avalon Way is lined with small, young street trees.

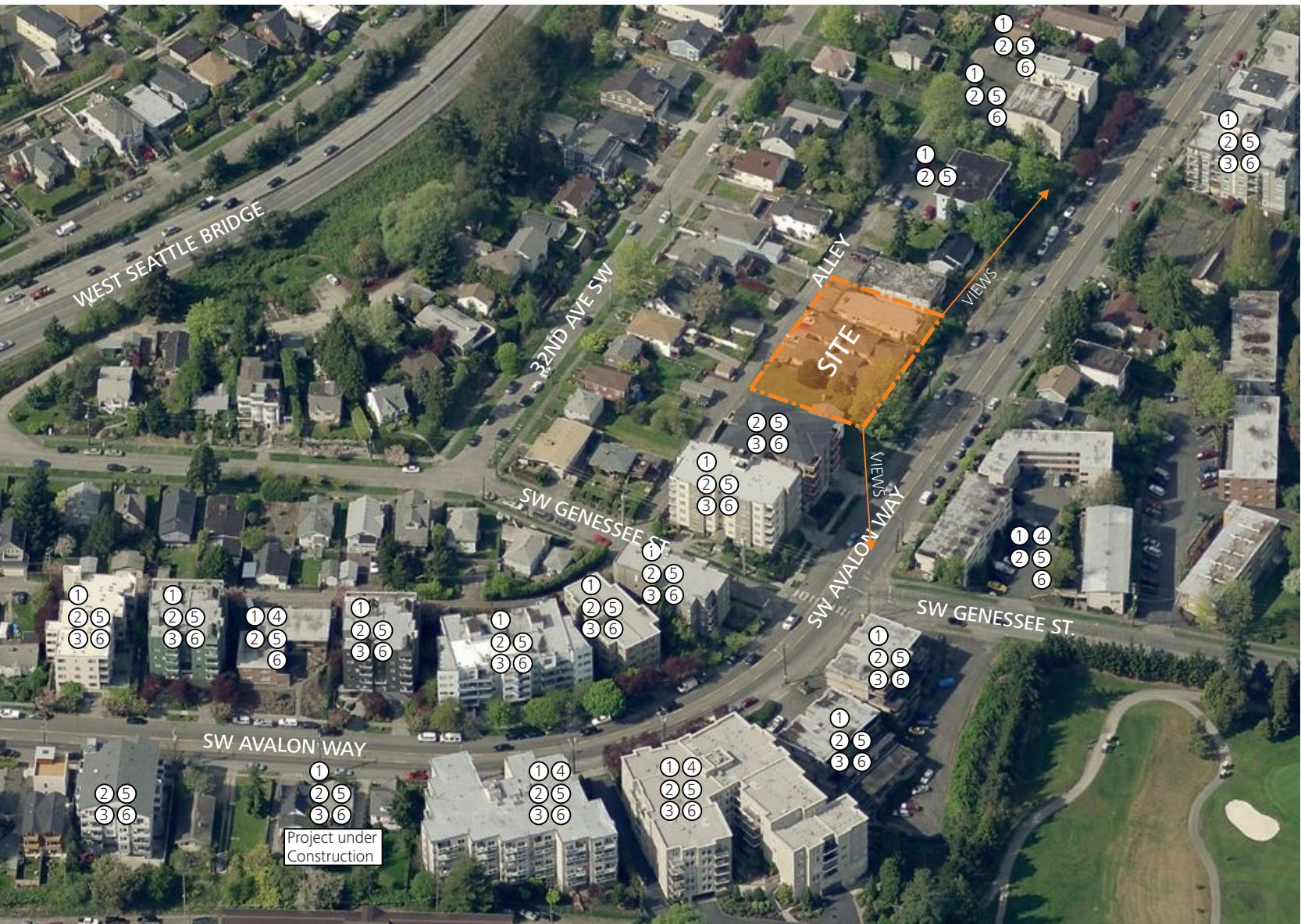
SW Avalon Way is an arterial and frequent transit corridor, as is the West Seattle Bridge and Fauntleroy Way SW to the west. Vehicular traffic is heavy on SW Avalon Way and pedestrian traffic is low to moderate. The area is relatively dense but the side streets are mostly quiet and service local traffic. The neighborhood is walkable, with the West Seattle Junction Hub Urban Village Center only a few blocks away.

The proposed project is zoned MR and abuts a SF 5000 zone across the alley to the west. A height limit of 60' is allowed in this zone. As the site is across an alley from the SF 5000 zone, Ordinance 124307 forbids the inclusion of the 15' bonus height with the addition of affordable housing, as allowed per SMC 23.45.510.

The site has a 32 foot slope from east to west and will have views of downtown Seattle to the north, North Delridge to the east, and Mt. Rainier to the southeast. It is currently terraced with rockery and concrete retaining walls and built out a single-family home and two apartment buildings.

The significant grade change from the alley to SW Avalon Way creates challenges for the site, as well as the adjacent properties. The drop in grade creates wonderful view opportunities and provides a diminished building height along the alley, adjacent to the SF 5000 zone. It also provides opportunities to access a multi-tiered underground parking garage from both the alley and SW Avalon Way, although no parking is required for the site.

### CONTEXT ANALYSIS



### SITE AERIAL

#### NEIGHBORHOOD DESIGN CUES (X)

1. Flat Roof
2. Large Balconies
3. Driveway from SW Avalon Way
4. Masonry Veneer
5. Modulated Facade
6. Neutral Colors

The multi-family buildings in the vicinity are a mixture dating from the middle of the 20th century to present date, and range from two to seven stories tall. Most have similar design features, such as flat roofs with little or no overhangs, large decks, and little decorative adornment. The mid-century structures accentuate horizontal massing. Exterior materials include masonry, concrete, fiber-cement, vinyl, wood, and stucco.

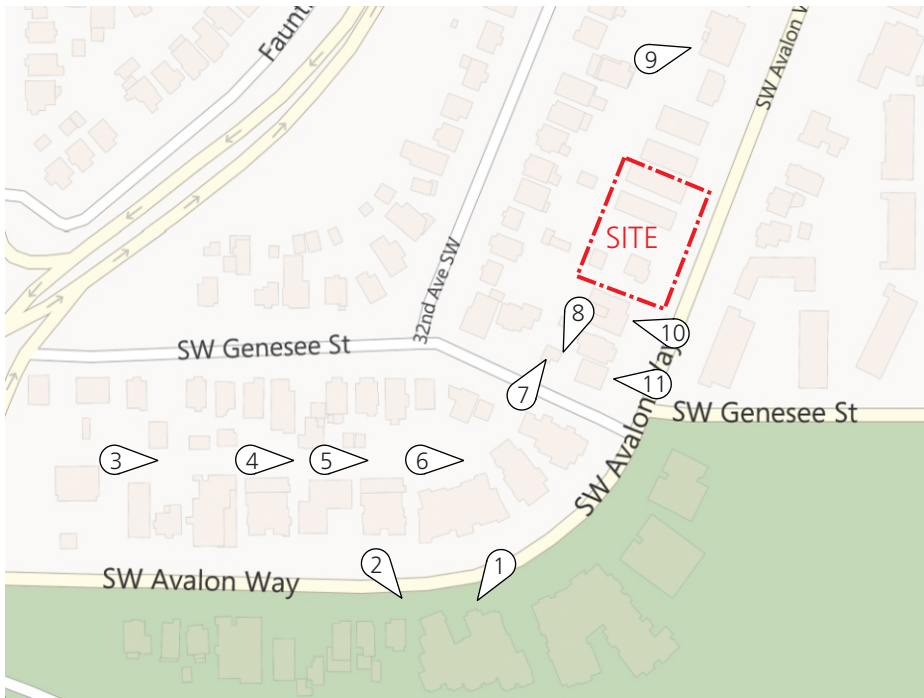
Nearly every building takes garage access from SW Avalon Way even though all abut an alley. This is most likely due to the challenging grades located throughout the area.





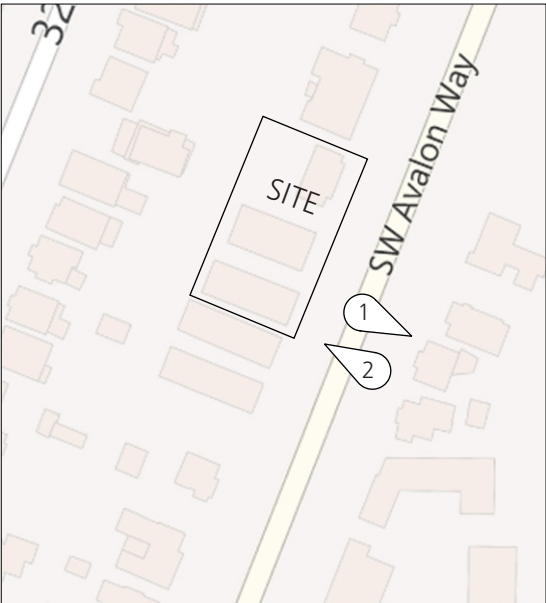
NEIGHBORHOOD PHOTOS





NEIGHBORHOOD PHOTOS





1 - SW AVALON WAY - LOOKING EAST



2 - SW AVALON WAY - LOOKING WEST

STREET CONTEXT

SMC SECTION	REQUIREMENT	PROVIDED
<b>USE</b> <b>23.45</b>	The proposed use of multi-family apartments are allowed outright by the seattle land use & zoning code with updated ordinance 123495.	575 sf. + 4 sf. Per unit over 100 units required    575 sf + 8 sf = 583 sf - 15% per 23.54.040 C = 495.55 sf required
<b>23.41.004</b>	A Master Use Permit (MUP) is required, including full design review	
<b>HEIGHT</b> <b>23.45.514</b>	Base height limit is 60-ft. Height is measured from the average grade level per director's rule 4-2012 and smc 23.86.006.a.	Proposed height is 55'-8 3/4" to top of roof at the rooftop deck and within the height limit allowed under SMC 23.45.514.
<b>23.45.514 J.6</b>	Elevator/stair penthouses may extend up to 16-ft. Above height limit.	Elevator/stair penthouse is under the allowed height limit
<b>FAR</b> <b>23.45.510</b>	Total permitted FAR = 3.2 x 19,196 sf. = 61,427,20 sf allowed.	Proposed FAR : 61,368.75 sf
This site is within the physical area defined by new ordinance #124307, which went into effect on November 7, 2013; modifying the MR zone adjacent to single family zones to not be eligible for the extra residential area or additional height, as allowed per SMC 23.45.510.D. The design herein in com plies with this new ordinance.		
<b>SETBACK</b> <b>23.45.518</b>	Front setback: 5 ft. Minimum, 7 ft. Average Rear setback: 10 ft. Minimum Side setback: 7 ft. Minimum, 10 ft. Average	Front setback: 7.0 ft. Rear setback: 23.02 ft. South side setback: 31.17 ft. Avg. North side setback: 10.0 ft.
<b>LANDSCAPE</b> <b>23.45.524</b>	Green factor score of 0.5 or greater is required for any lot with development containing more than one dwelling unit. Street trees are required on SW Avalon Way.	A green factor of 0.5 or greater is provided. Street trees have been provided.
<b>PARKING</b> <b>23.45.020</b>	No parking required for this site due to urban village overlay and proximity to frequent transit.  Bicycle parking is required 1 space per 4 units = 25.75 spaces required	60 on-site parking spaces have been provided  35 bicycle spaces are provided.
<b>SOLID WASTE</b> <b>23.45.040</b>	575 sf. + 4 sf. Per unit over 100 units required    575 sf + 8 sf = 583 sf - 15% per 23.54.040 C = 495.55 sf required	536.98 sf trash and recycling provided.



A SITE PLANNING

A-1 Respond to Site characteristics

The refined scheme pushes the building toward SW Avalon Way to create a continuous urban edge along the street and provide a relief and transition to the SF zone across the alley. Given the topographical nature of the site, one entry to the garage is proposed from SW Avalon Way, similar to existing precedents in newer apartments buildings on adjacent sites. A second vehicular entry is provided from the alley. This arrangement also provides decreases vehicular traffic in the alley. Services will be provided from the alley.

A-2 Streetscape compatibility

The street facing façade at the pedestrian level is modulated and adorned with finer textured materials to provide human scale details such as brick masonry, canopies, low fences, and appropriate modulation. The main pedestrian entry and entries to the residential units are accessed directly off the street, further enlivening SW Avalon Way.

A-3 Entrances visible from street

Both the main lobby entrance (defined by a prominent 1 1/2 story tall space and significant entry canopy), as well as individual entries to the walk-up units are directly off SW Avalon Way.

A-4 Human Activity

Not applicable, zoning and street character do not support commercial use, however pedestrian entries are enlivening SW Avalon Way.

A-5 Respect for other sites

The pedestrian courtyard is located on the alley side to provide a greater setback from adjacent the SF zone. Minimal windows have been placed along the southern property line, as well as the portion of the building closest to the alley, to provide a privacy for the adjacent apartments and single family neighbors.

A-7 Residential open space

The pedestrian courtyard provides maximum sun exposure to the common outdoor amenity area.

B HEIGHT, BULK, AND SCALE

B-1 Height, Bulk and Scale compatibility.

The majority of the mass of the building is pulled away from the SF zone toward the street. The façade is broken into base, body and top, further decreasing the perception of the height of the building. The upper most stories are further stepped back from the façade, providing additional relief.

C ARCHITECTURAL ELEMENTS AND MATERIALS

C-1 Architectural Context

We have taken cues from adjacent multifamily projects, providing corner decks and balconies, lightly painted materials on the upper levels and light colored brick along the street and courtyard.

C-2 Architectural Concept

The base has been differentiated to decrease the bulk and height appearance on SW Avalon Way. The body and base have been tied together with a rhythm of openings and modulation. The base is also interlocking with the body of the building at the intersection of the residential lobby. High quality materials are used both along SW Avalon Way, as well as the alley.

C-3 Human Scale

The unit entries and main lobby entry feature canopies, bringing the scale down. An exterior bench and differentiated paving adds pedestrian detail. Walk-up units have small yards defined by low fences separating semi private spaces from the public street.

D PEDESTRIAN ENVIRONMENT

D-5 Visual Impact of Parking Structures

One entry to the parking garage is proposed from Avalon Way. The entry is minimized to de-emphasize the auto by limiting parking on this level to only 30 cars. The garage opening is recessed and kept to a minimum height to further decrease it’s importance compared to the pedestrian entry. Furthermore the vehicle entrance is similarly modulated to the street level walk-up units, following the rhythm of the base.

D-6 Screening of Dumpsters, Utilities and Service Areas

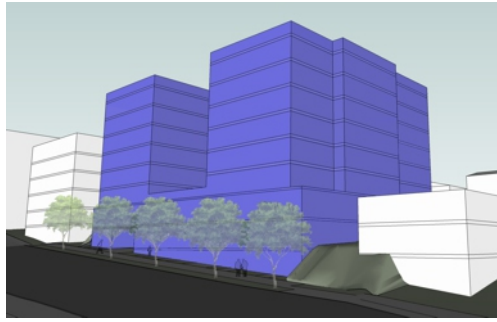
All utilities and trash areas are contained within structure and will be rolled to the alley only on pickup day.



RELEVANT DESIGN GUIDELINES

Recommendation #1 Direction		Architect’s Response
<p><b>A-2 Streetscape Compatibility</b>            At the Initial Recommendation Meeting, the Board supported the generous openings and proportions of the ground floor, and the proposed patios, low walls and cedar slat fence design as shown on pg 22, but requested the following refinements to that ground level:</p> <ul style="list-style-type: none"> <li>• Raise the floor levels of the two north units 18”-24” to afford some vertical privacy layering for occupants.</li> <li>• Add integrated louvers or screens in all ground floor units to provide occupant privacy options, and add scale and pedestrian inteest to the street.</li> <li>• Integrate fritted or translucent glass in the glass panes to provide some privacy.</li> <li>• Change the wall-mount fixtures or add a material 'plaque' behind them to create a better scale for these objects against the wall surface.</li> <li>• Consider a deeper offset of the top brick course to create a stronger shadow and larger reveal with the white wall surface above.</li> </ul>	Elevations, Level 1 Plan, Perspectives	<p>The two northernmost street level units have been raised one foot to the same elevation as the lobby. Frosted film and wood screens have been provided up to five feet to afford privacy but still allow ample light to penetrate the units. “Plaques” have been included behind the sconces at the brick pilasters, and the soldier course has been increased from one inch to two inches.</p>
<p><b>A-5 Respect for Adjacent Sites</b>            At the Initial Recommendation Meeting, the Board applauded the reflected window analysis (pg 10/12) and the careful placement of proposed windows to respect adjacent windows and balconies.</p> <p>The Board requested a more solid, continuous row of tree canopies along the alley, to create a privacy screen to the neighbors from upper levels of the proposed building, but maintaining eye level visibility to the alley. The tree species selected should maintain leaves as long as possible, be mature at planting and have planter troughs large enough to not stunt the root balls.</p>	Landscape Plan	<p>The tree species has been changed to one that will maintain its leaves longer and will have a mature height which will provide greater obscuring of the building.</p>
<p><b>A-8 Parking and Vehicle Access</b>            At the Initial Recommendation Meeting, the Board agreed the two portals reduce alley impacts, but do not to force all vehicles onto Avalon. The Board supported all measures to ensure the sight triangles and safety of both vehicle access points, including the right-in/right-out limit at Avalon Way.</p>	Site Plan, Level 1 Plan, Level 2 Plan, Level 3 Plan	<p>The sight triangle at the Avalon driveway has been retained and the alley portal is wider than required to increase sight lines.</p>
<p><b>B-1 Height, Bulk, and Scale Compatibility</b>            At the Initial Recommendation Meeting, the Board applauded the reduction of the north wall length, and the elimination of the departure for Structure Depth. The Board appreciated the revised design is appreciably less bulky than shown at EDG, but requested the following two options (or a combination) be studied to further reduce the building height impacts, but did not request elimination of an entire floor of units:</p> <p>Reduce the current floor-to-floor heights to lower the top parapets 3-5 ft.</p> <p>Study pushing the parking deeper into the site, lowering the entire building, even if this means a steeper ramp off Avalon, and/or other implications which should be clearly shown as pros and cons at the next meeting. The Board realized a terraced courtyard might be one on these implications, to not create a moat along the alley.</p>	Sections, Elevations	<p>The floor-to-floor height has been reduced eight inches per floor, for a total reduction of 3’-2”. A study was conducted regarding internal ramping and is included in this packet. Given the constraints of the site and the necessity for access from both the alley and the street, it is not feasible. The building under the allowed height limit by more than 4’-3”.</p>
<p><b>C-4 Exterior Finish Materials</b>            At the Initial Recommendation Meeting, the Board agreed the proposed design exhibits generally good proportions, but requested the following refinements to better respond to context:</p> <ul style="list-style-type: none"> <li>• Maintain the dark, corrugated material attic and 'gasket' facing Avalon, but reduce the amount (and/or color) of that same dark material on the alley façade, where it is overwhelming, especially the three of five levels closest to the alley.</li> <li>• Add more brick, or another masonry material, as a base on all alley façades.</li> <li>• Reduce the large window sizes/proportions on the white bays facing the alley.</li> <li>• Add more fine-scale and warmer materials on the alley facades, possibly a new material not found elsewhere on the project. The Board agreed the project would be cohesive even if certain materials are used only on the alley façade.</li> </ul>	Elevations, Perspectives	<p>The metal siding facing the alley has been reduced and a horizontal lap siding with a new color has been introduced to help soften the building toward the single family zone, but has been maintained on the street facade. Brick masonry has been added to the walls closest to the alley. Translucent glazing has been included at the popped out areas facing the alley to increase privacy, while maximizing the amount of natural light for the units. The pure white fields have been replaced with a warmer shade of white.</p>
<p><b>D-1 Pedestrian Open Spaces and Entrances</b>            At the Initial Recommendation Meeting, the Board recognized the side yard stair/path was no longer viable, and supported the open, fence-less design shown along the alley (pg 20).</p> <p>The proposed courtyard design appears cold and minimal (pg 25), with little amenity to attract users or sociability (also see EDG guidance under E-2). The Board requested a more diverse and interesting design for the courtyard, including different paving patterns and more warm and contrasting materials such as wood and metal planters, not all concrete of identical height.</p>	Site plan, Level 1 Plan, Level 3 Plan, Landscape Plan, Perspectives	<p>The courtyard has been redesigned to increase interest. The pavers have been changed to a linear shape with a random color pattern. A narrower paver is used to help to distinguish the private terraces from the public amenity space. This pattern ties in to the redesigned lobby entry paving pattern. The planters have been modified to give a mix of textures and heights. A variety of seating and gathering spaces have been included and the landscaping modified.</p>
<p><b>D-8 Treatment of Alleys</b>            At the Initial Recommendation Meeting, the Board requested larger scale elevations and more details on all lighting along the alley and side yards. The Board requested further studies to reduce the height and size of the parking vent box, and exploration of whether it can be located somewhere less visible such as the north wall onto the alley ramp.</p>	Lighting Plan, Perspectives, Elevations, Level 2 Plan, Level 3 Plan	<p>The site lighting plan has been modified to provide downlighting along the side yards, while not blinding neighbors and residents. The garage ventilation exhaust has been relocated to the northwest corner to be less visible and obtrusive.</p>
<p><b>D-12 Residential Entries and Transitions</b>            At the Initial Recommendation Meeting, the Board supported the large canopy, address numbers and bench which identify the primary lobby entrance on Avalon Way, but requested a more interesting scoring pattern at this important transition– at least similar to the residential patios nearby.</p>	Site Plan, Level 1 Plan	<p>The paving outside of the lobby entry has been redesigned to match the revised courtyard. The unit patios have also been modified.</p>
<p><b>E-2 Landscaping to Enhance the Building and/or Site.</b>            At the Initial Recommendation Meeting, the Board agreed certain blank walls needed green screens or other treatments: the 10+ ft tall south retaining wall on Avalon; all the concrete surfaces of the parking vent at the southwest corner (or a different design for venting that is less pronounced on the alley; see comments under D-8). The Board requested a more clear and specific landscape plan at the next meeting, not showing confusing materials or designs (pg 37/38) not relevant to the proposal. Each specific tree should be clearly identified, and its height, canopy and other features at planting clearly described.</p>	Elevations,Perspectives, Level 1Plan,Level 2 Plan, Level 3 Plan	<p>A green screen has been added to the south retaining wall, and the garage exhaust has been relocated. The landscaping plan has been modified and clarified.</p>





## CONCEPT A

### SUMMARY

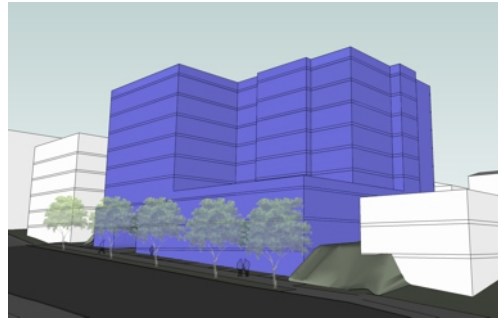
Massing Option A is an “H” shaped structure atop a two and a half story concrete podium. A diminished auto entry and prominent pedestrian entry are incorporated. This option creates a large courtyard facing SW Avalon Way.

### ADVANTAGES

- Maximizes FAR
- Breaks massing along SW Avalon Way
- Code compliant

### CHALLENGES

- Less view units
- Large mass against residential zone, unsympathetic to neighbors
- Courtyard has only morning light



## CONCEPT B

### SUMMARY

Massing Option B is an “L” shaped structure atop a two and a half story concrete podium. A diminished auto entry and prominent pedestrian entry are incorporated. This option pushes the bulk of the massing away from SW Avalon Way toward the alley.

### ADVANTAGES

- Maximizes FAR
- Less mass on Avalon Way
- Plaza has views
- More southern facing units
- Code compliant

### CHALLENGES

- Less view units
- Large mass against residential zone, unsympathetic to neighbors
- Courtyard has only morning light
- Large quantity of units face adjacent apartment building to the south



## CONCEPT C

presented at the 09.12.2012 EDG meeting

### SUMMARY

The preferred Option C is also an “L” shaped building, however, the bulk of the mass has been pushed toward SW Avalon Way creating a large courtyard adjacent to the alley and single family neighbors. This option diminishes the impact of the building on the single family neighbors the most of the three options. It provides a direct correlation to the adjacent project, and is most suited to the site.

### ADVANTAGES

- Massing visually breaks up building scale
- Maximizes Unit Count
- Maximizes number of units with view
- Softens transition to single family zone
- Integrates with existing topography
- Sensitive to adjacent apartment building to the south
- Courtyard has almost full sun
- Maximizes FAR

### CHALLENGES

- Courtyard not facing view
- More north-facing units



CONCEPT C REFINED AND REDUCED  
presented at the 11.21.2013 REC meeting

### SUMMARY

The “Refined and Reduced” Concept C further reduces the massing. In conformance with the new Ordinance 124307, a story has been eliminated and the FAR has been reduced to comply with the base MR zoning. The podium has been sunken into the ground further and the wing closest to the SF 5000 zone has been pulled back. The building steps along all facades at the upper levels to reduce bulk. Both the rear yard and facade length departures have been removed. The Northeast corner has been lowered one floor to diminish the perceived height from the street.

### ADVANTAGES

- Massing visually breaks up building scale
- Maximizes number of units with view
- Softens transition to single family zone
- Street front is more enlivened with 2nd floor units
- Integrates with existing topography
- Street facade more pedestrian in scale
- Sensitive to adjacent apartment building to the south
- Courtyard has almost full sun
- Maximizes FAR
- Departures minimized

### CHALLENGES

- Courtyard not facing view
- More north-facing units



## CONCEPT C FURTHER REFINED

### SUMMARY

The further refined Concept C further reduces the perceived height by removing 8 inches from each floor to floor height, for a total height reduction of 3’-2”. The courtyard has been redesigned and increased privacy and interest have been added at the street level, as well as facing the single-family neighbors. A new material has been introduced on the west facade to provide greater transition to the residential neighborhood across the alley.

### ADVANTAGES

- Massing visually breaks up building scale
- Decreased height is more sympathetic to surrounding uses
- Maximizes number of units with view
- Softens transition to single family zone
- Street front is more enlivened with 2nd floor units
- Integrates with existing topography
- Street facade more pedestrian in scale
- Sensitive to adjacent apartment building to the south
- Courtyard has almost full sun
- Maximizes FAR
- Departures minimized

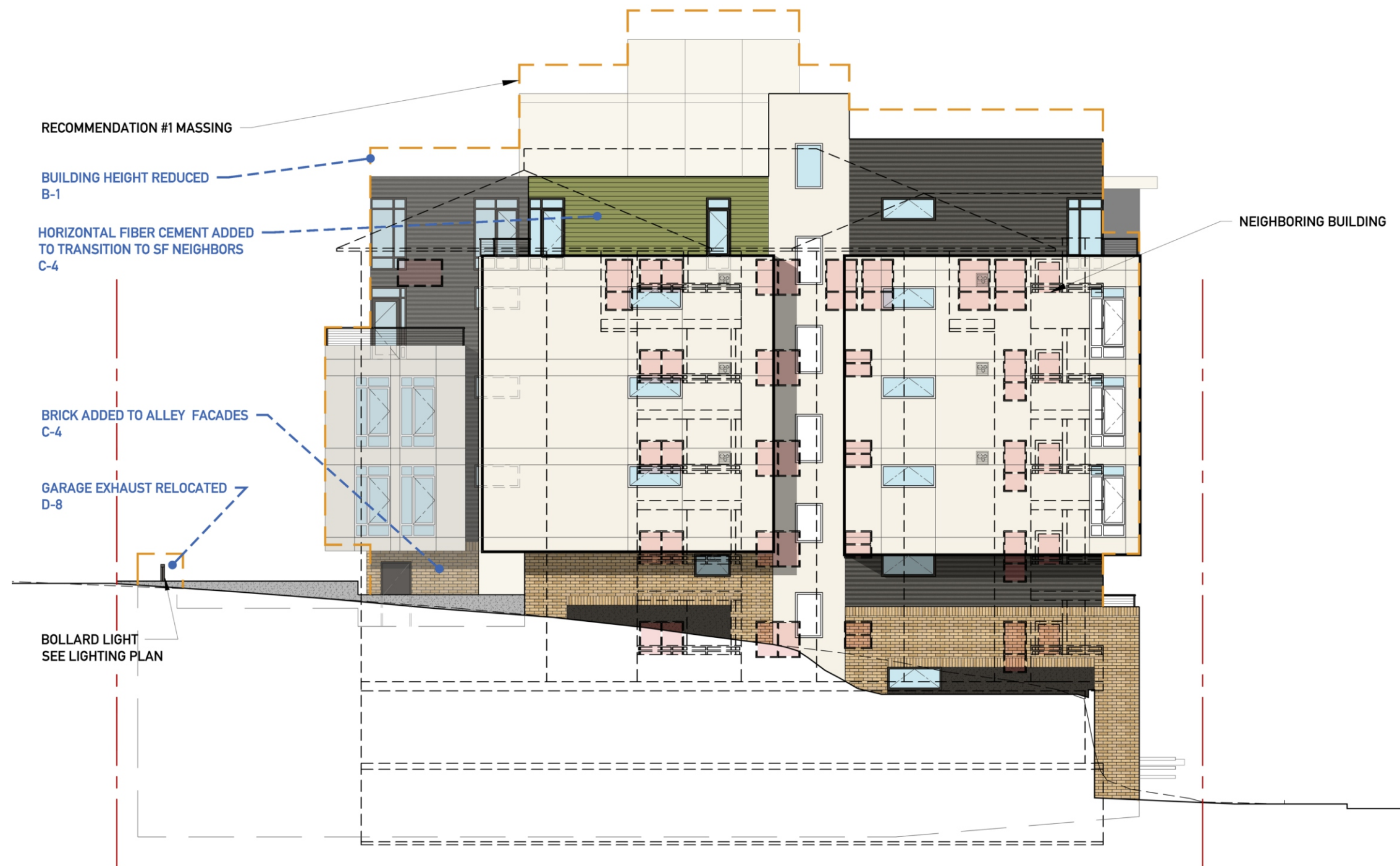
### CHALLENGES

- Courtyard not facing view
- More north-facing units

## CONCEPT SUMMARY



NORTH ELEVATION



SOUTH ELEVATION





WEST ELEVATION FACING ALLEY



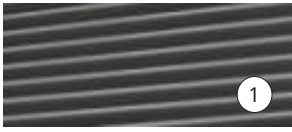





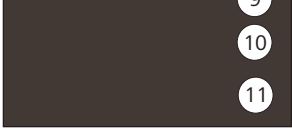
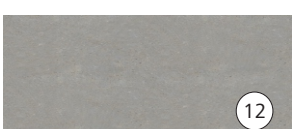
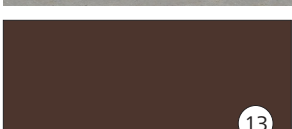

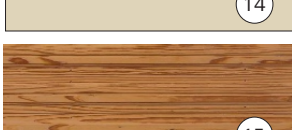
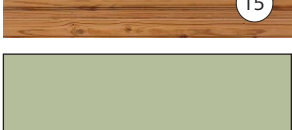





EAST ELEVATION







	1	CORRUGATED METAL SIDING : CHARCOAL COLOR
	2	FIBER-CEMENT PANEL : SHERWIN WILLIAMS SW 7566 WESTHIGHLAND WHITE
	3	ABET LAMANITI WOOD PANEL : 754 MEG-Sei
	4	VINYL WINDOW
	5	METAL PANEL : GLOSS WHITE COLOR
	6	MUTUAL MATERIALS BRICK : MOJAVE BLEND
	7	STOREFRONT DOOR/WINDOW
	8	VINYL WINDOW
	9	COLORED CONCRETE
	10	LOUVER/FLASHING
	11	METAL BALCONY : CHARCOAL COLOR
	12	CONCRETE : NATURAL COLOR
	13	PREFABRICATED PLANTER : BROWN COLOR
	14	1'x4' PAVERS OR 6"x4' : VARYING COLORS
	15	HORIZONTAL CEDAR FENCE: NATURAL COLOR
	16	FIBER CEMENT LAP SIDING, 7" REVEAL : SHERWIN WILLIAMS SW 6424 TANSY GREEN

## SW AVALON WAY EXTERIOR MATERIALS

1.16.2014 RECOMMENDATION 2 PACKAGE  
3078 SW AVALON WAY DPD PROJECT #:3013303

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ALLEY EXTERIOR MATERIALS





LOOKING NORTH ON SW AVALON WAY

1.16.2014 RECOMMENDATION 2 PACKAGE  
 3078 SW AVALON WAY DPD PROJECT #:3013303

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VIEW FROM SW AVALON WAY





ALLEY LOOKING SOUTH





WEST BUILDING FACADE





ALLEY LOOKING NORTH

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LOOKING SOUTH ON SW AVALON WAY





LOOKING SOUTH ON SW AVALON WAY

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LOOKING NORTH ON SW AVALON WAY





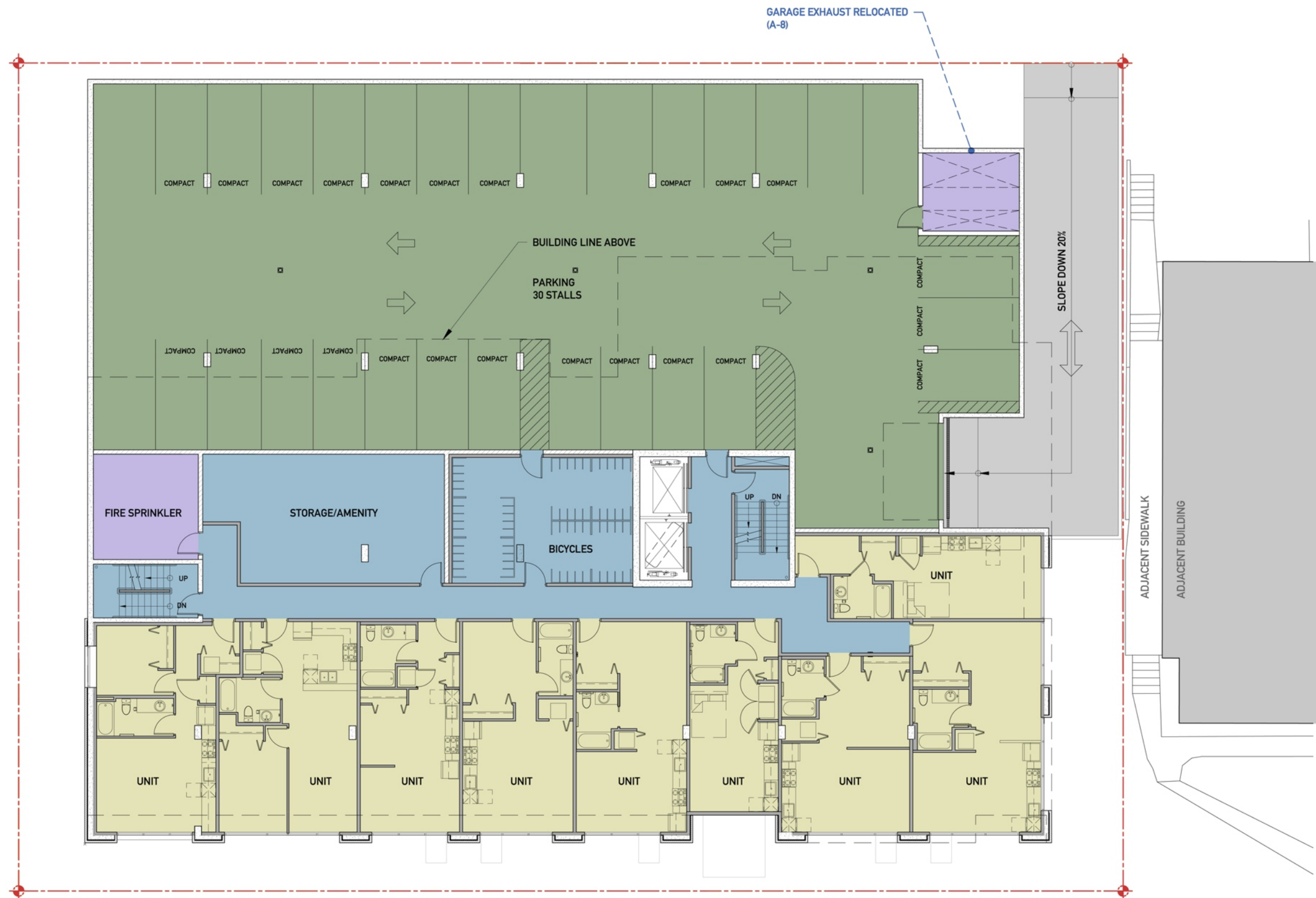
SITE PLAN





LEVEL 1 FLOOR PLAN - STREET LEVEL



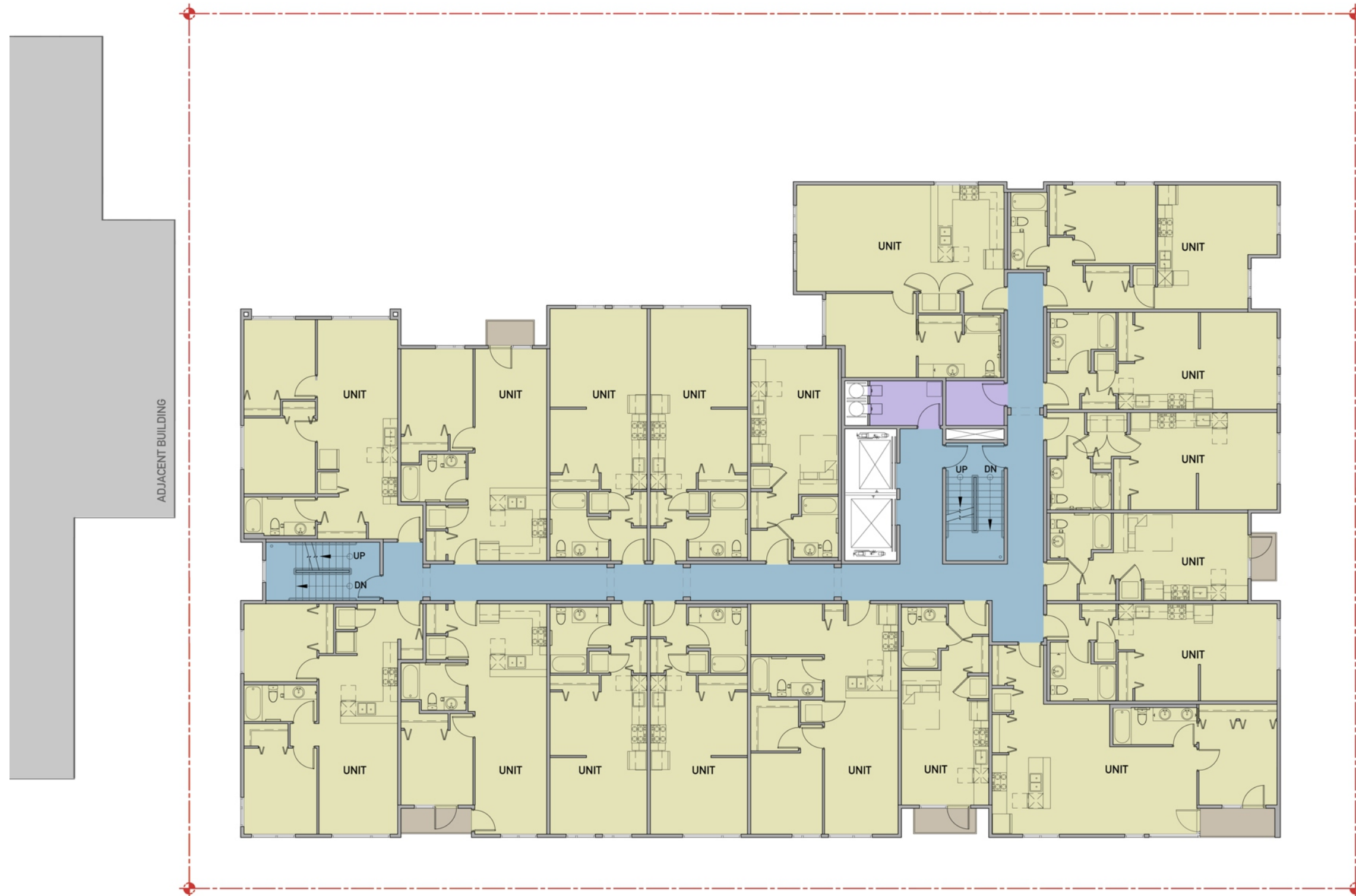


LEVEL 2 FLOOR PLAN



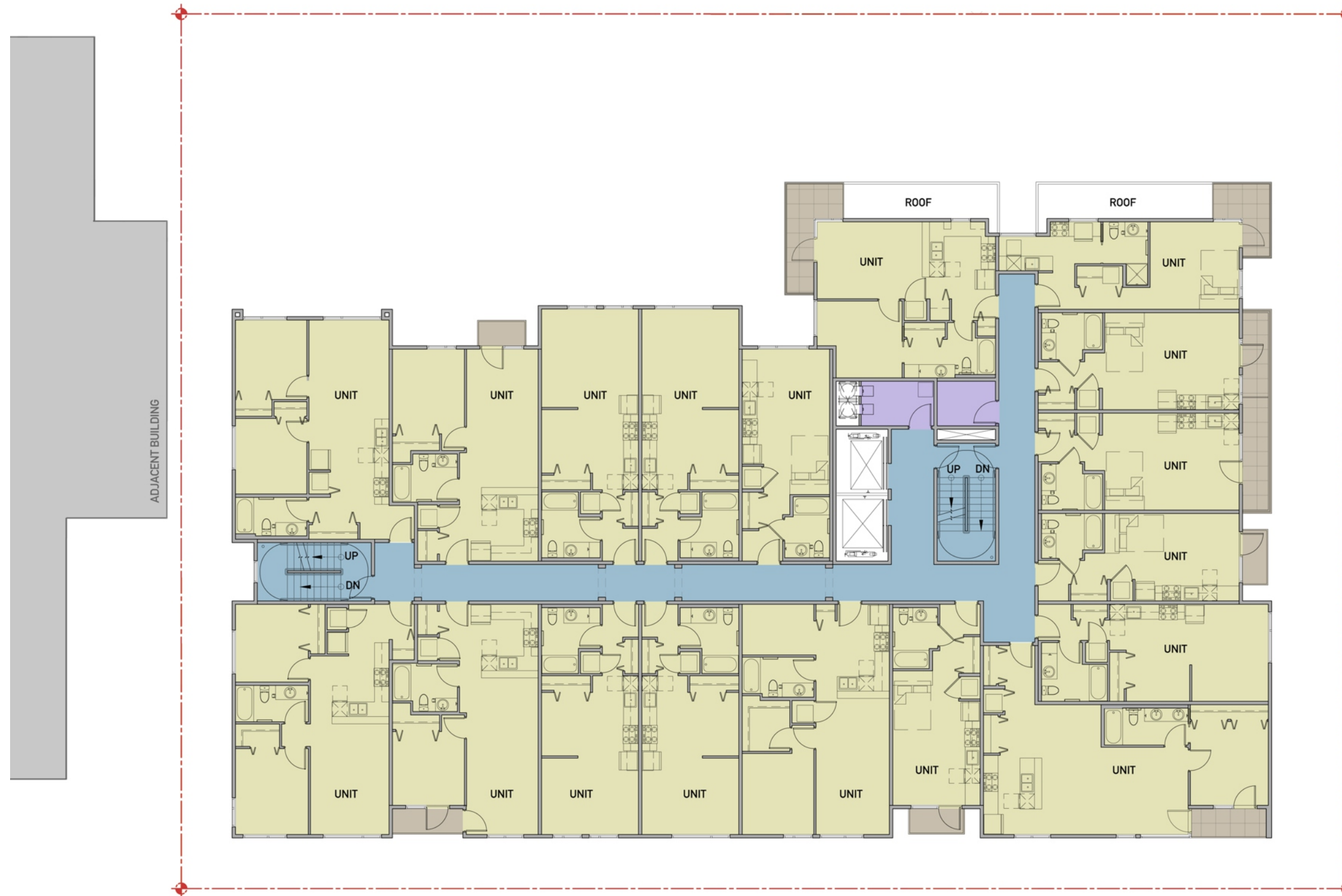






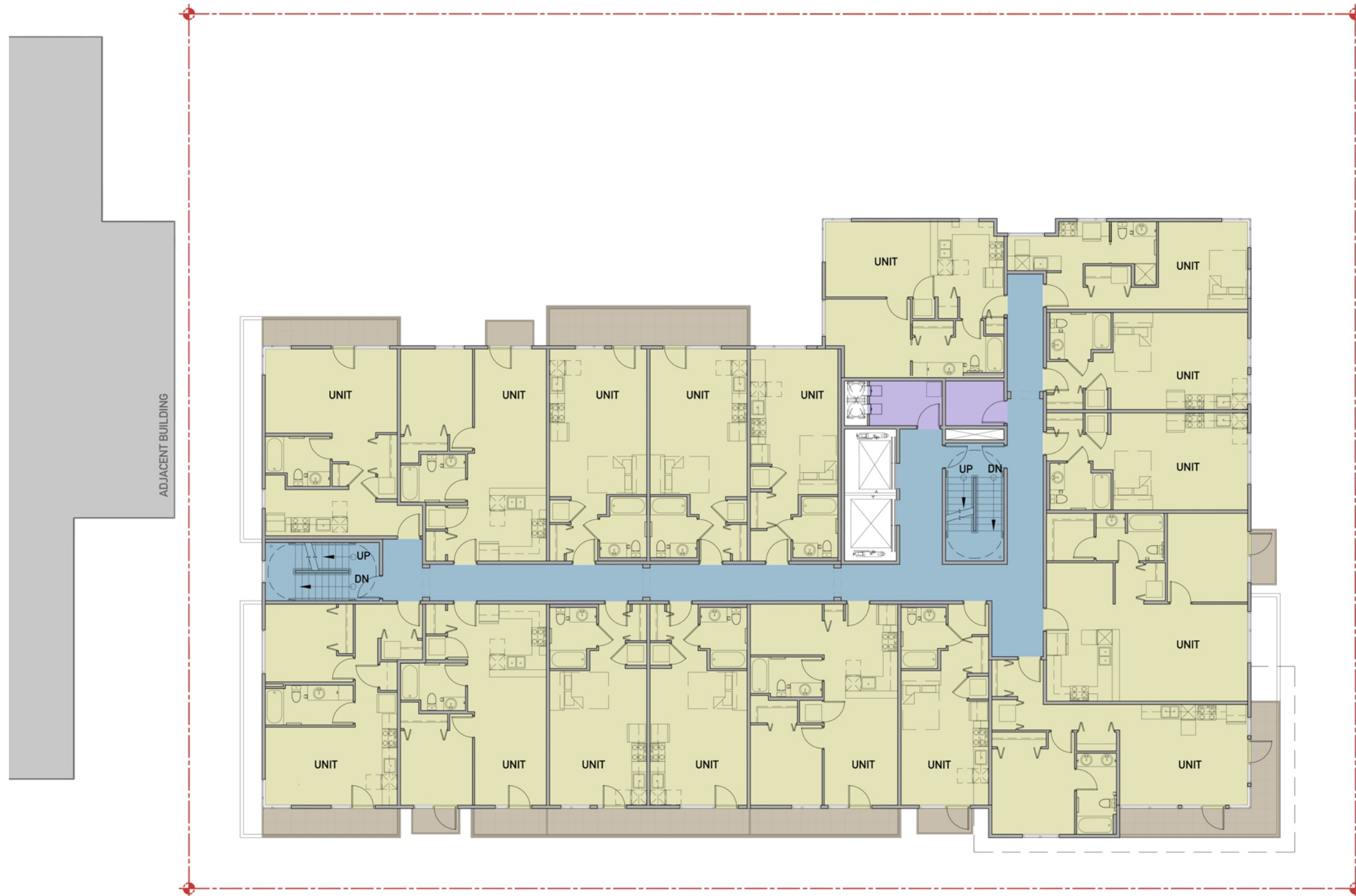
LEVELS 4 AND 5 FLOOR PLAN





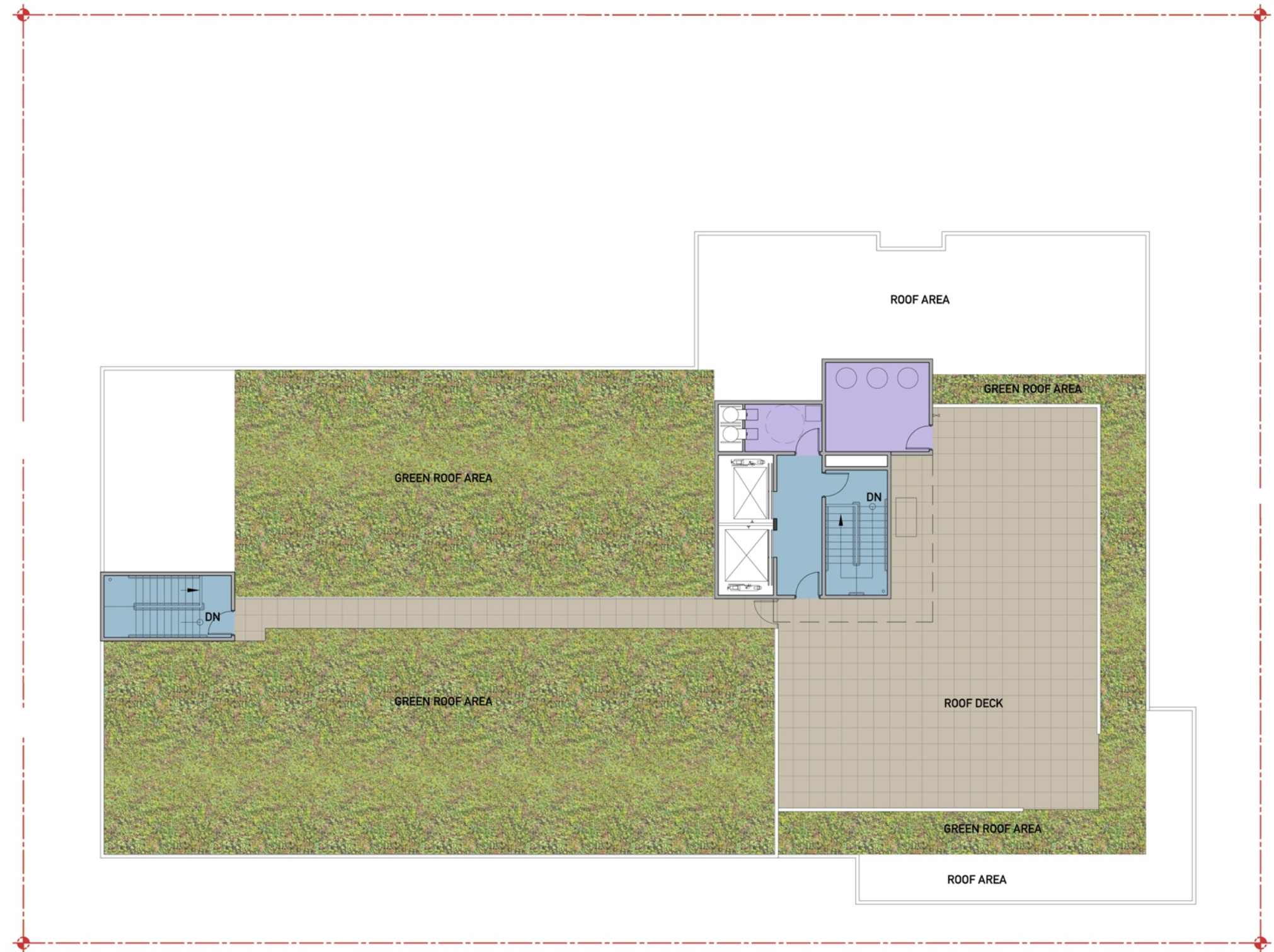
LEVEL 6 FLOOR PLAN





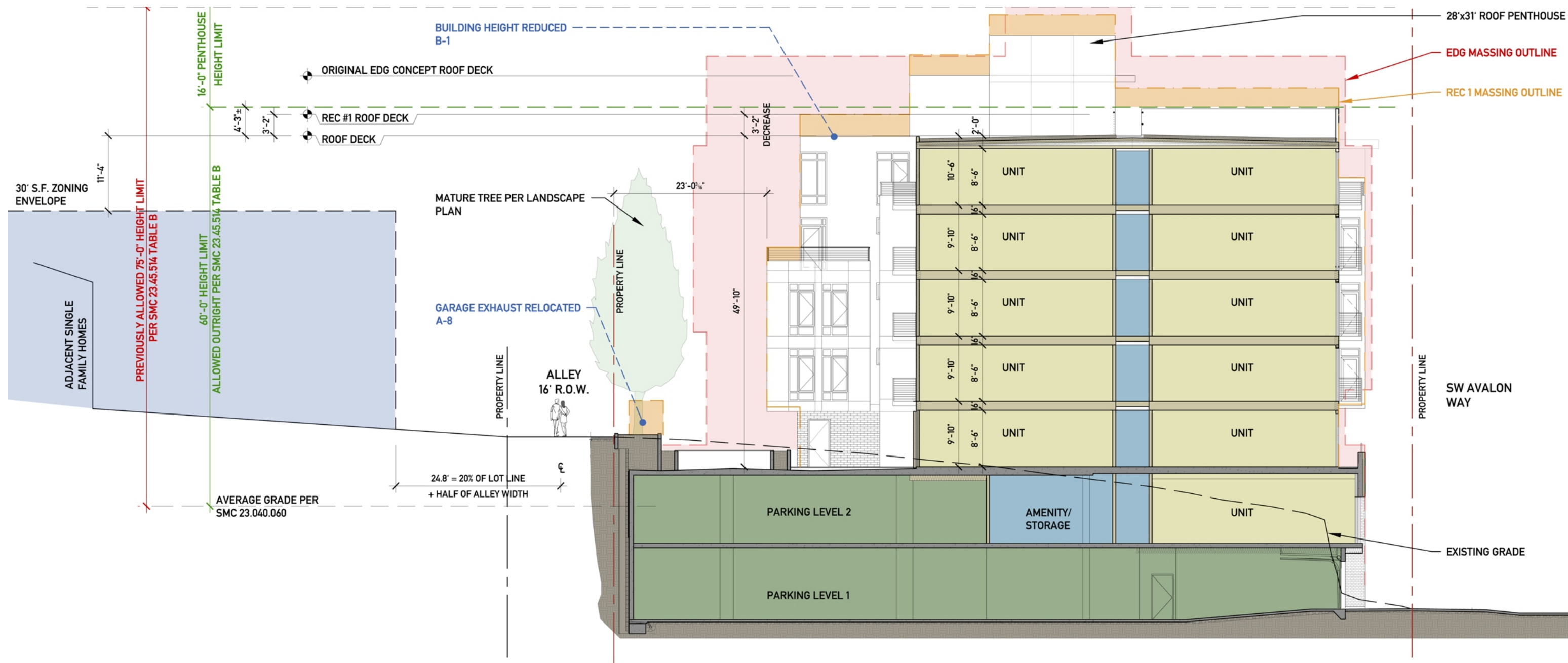
LEVEL 7 FLOOR PLAN





ROOF PLAN





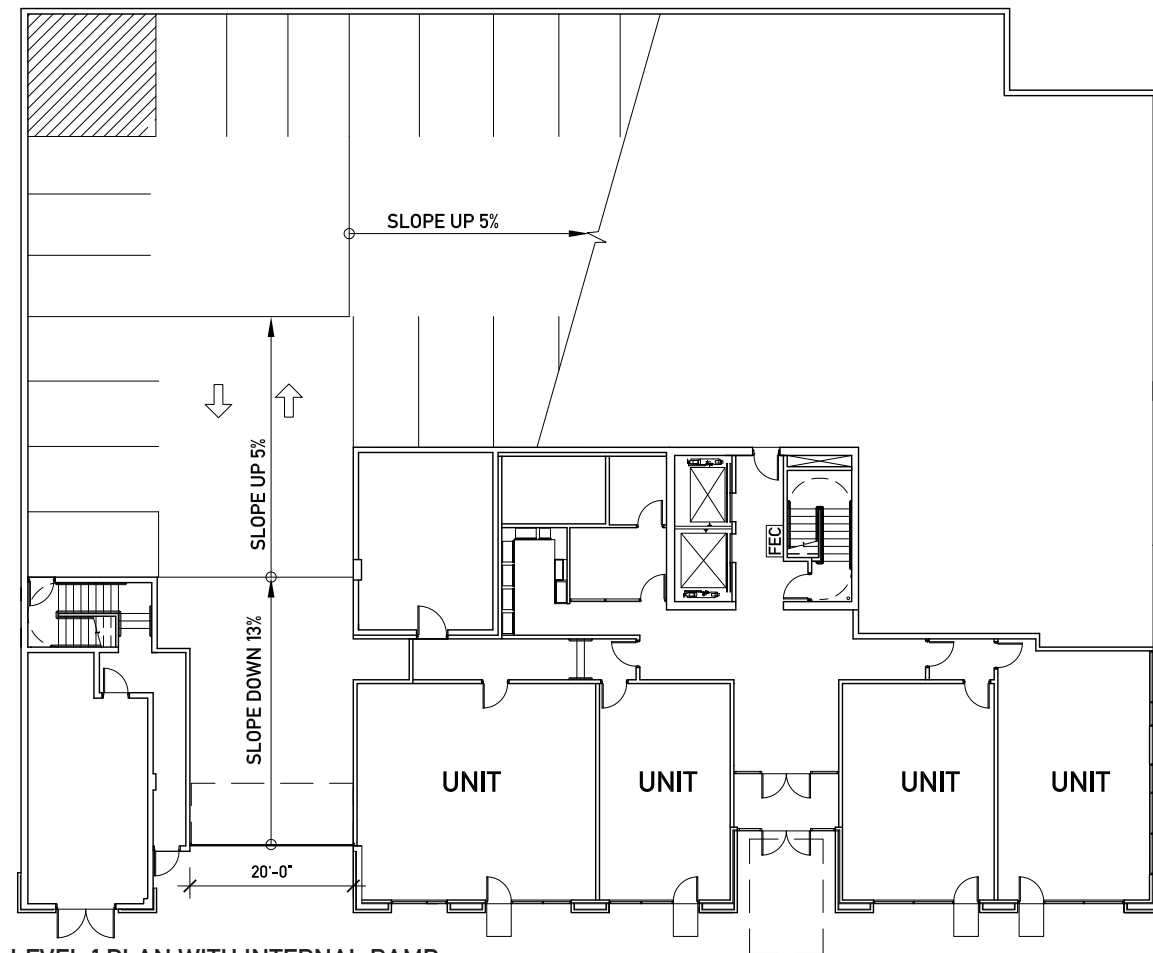
SITE SECTION



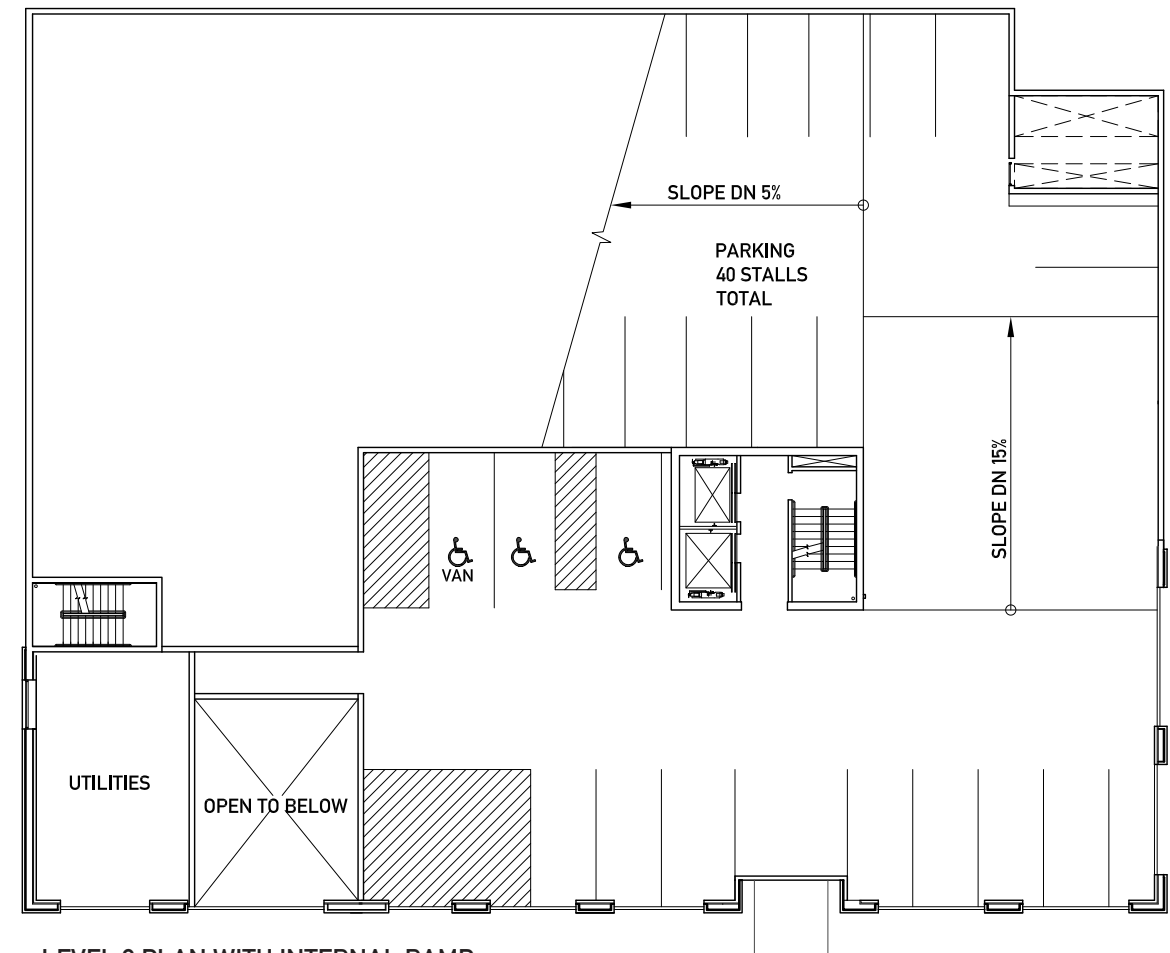


BUILDING SECTION AT RAMP

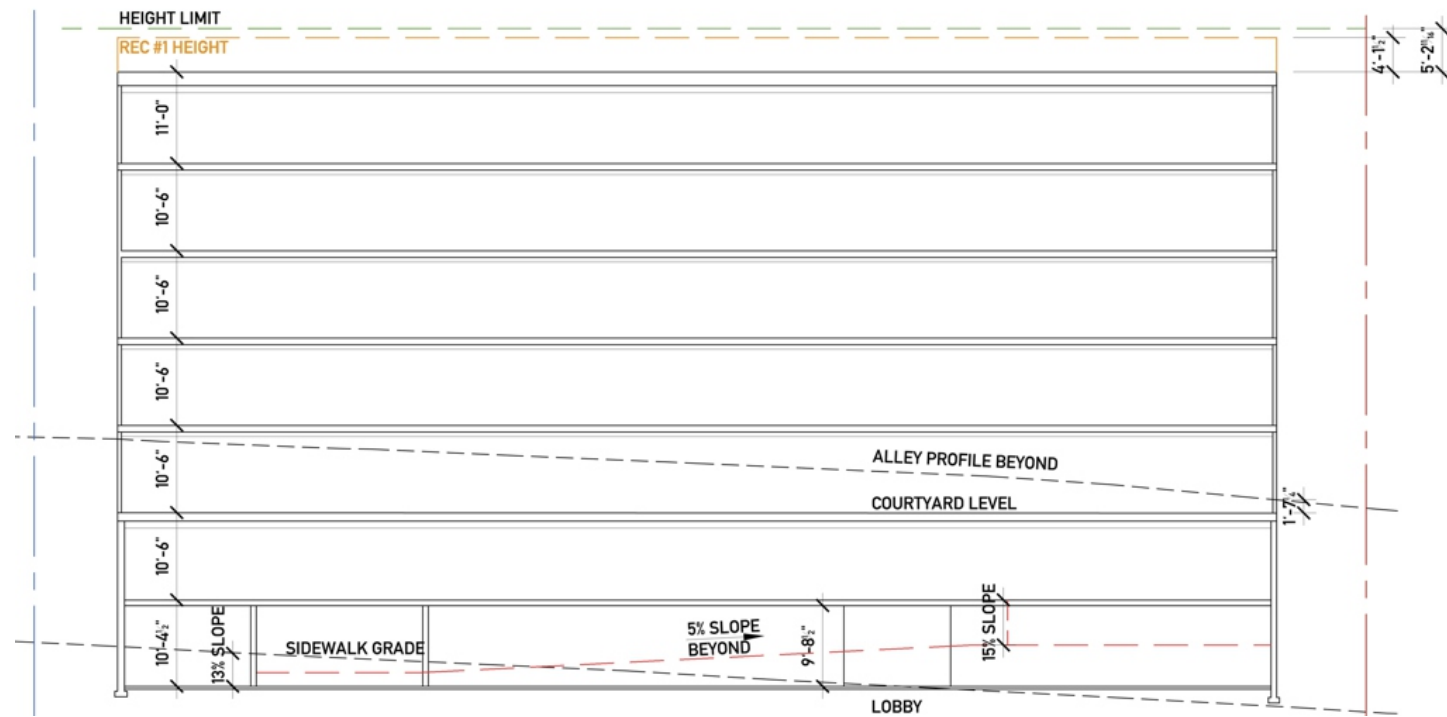




LEVEL 1 PLAN WITH INTERNAL RAMP



LEVEL 2 PLAN WITH INTERNAL RAMP



INTERNAL RAMP SECTION

#### HEIGHT REDUCTION STUDY

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#### Internal Ramp Pros:

- 4'-1" height reduction
- One parking area, not split
- No departure required for ramp slope
- No alley access

#### Internal Ramp Cons:

- Full redesign of lower two levels and courtyard
- Loss of street facing units
- 20' wide access required off Avalon Way
- All traffic directed onto Avalon Way
- Loss of 23 parking Stalls
- Loss of inviting lobby
- Courtyard up to 10' below alley grade
- Departure required for access from Avalon Way
- Increased excavation
- Nearly identical height reduction as proposed design



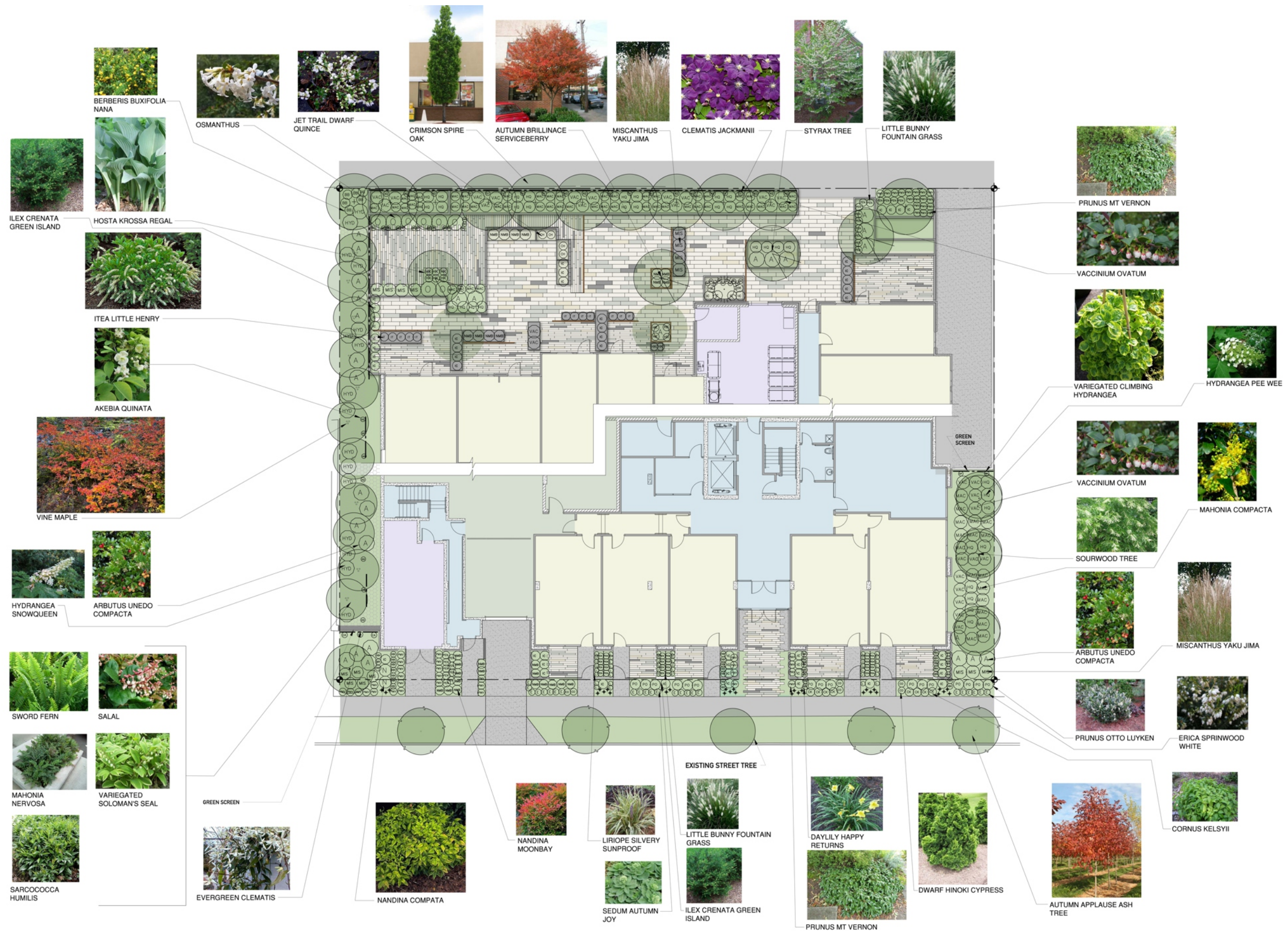




CODE REFERENCE / REQUIREMENT	DEPARTURE REQUEST:	RATIONAL FOR DEPARTURE:
<p>SMC 23.45.536</p> <p>C. Access to parking.</p> <p>1. Alley access required. Except as otherwise expressly required or permitted in subsections C or D of this Section 23.45.536, access to parking shall be from the alley if the lot abuts an alley and one of the conditions in this subsection 23.45.536.C.1 is met.</p> <p>a. The alley is improved to the standards of subsection 23.53.030.C;</p> <p>b. The development gains additional FAR pursuant to Section 23.45.510.C; or</p> <p>c. The Director determines that alley access is feasible and desirable to mitigate parking access impacts, improve public safety, and/or maintain on-street parking capacity.</p> <p>4. On steeply sloping lots, the Director may permit the use of both an alley and a street for access, provided that the following conditions are met:</p> <p>a. access from the street is to common parking garage in or under the structure, that is underground or extends no more than 4 feet above grade.</p> <p>b. the siting of development results in an increased Green Factor score, larger ground-level amenity areas, and/or reduced surface parking area than if alley access alone is used.</p> <p>c. In LR zones, if the project uses both the alley and street for access to parking other than required barrier-free parking spaces, the project does not qualify the higher FAR limit in Section 23.45.510.B.</p>	<p>Allow a second access from SW Avalon Way</p>	<p>This departure would allow a second access from SW Avalon Way in addition to access from the alley. SMC 23.45.536 C.4 states; <i>On steeply sloping lots, the director may permit the use of both an alley and a street for access, provided that the following conditions are met: a. Access from the street is to a common parking garage in or under the structure, that is underground or extends no more than 4 feet above grade. B. The siting of development results in increased green factor score, larger ground floor amenity areas, and/or reduced surface parking area than if alley access alone is used.</i></p> <p>The garage level accessed from SW Avalon Way meets the provisions of SMC 23.45.536 C.4 as it is a common parking garage under the structure and the level which would be accessed by the second access from SW Avalon Way extends no more than four feet above grade. The departure also increases the amenity area of the project and reduces surface parking, as the proposed pedestrian courtyard would become surface parking in order to maintain the same parking count. Although no parking is required for this site, the proposed project provides 61 under-building parking spaces hidden from view. Eliminating surface parking and replacing it with pedestrian amenity area benefits the project's residents as well as the neighbors to the south and west who abut and look out at the courtyard the departure allowance also increases safety in the alley by providing more “eyes on the street” than a parking area would.</p>
<p>SMC 23.54.030</p> <p>D. Driveway. Driveway requirements for residential and nonresidential uses are described below. When a driveway is used for both residential and nonresidential parking, it shall meet the standards for nonresidential uses described in subsection 23.54.030. D.2.</p> <p>3. Driveway slope for all uses. No portion of a driveway, whether located on a lot or on a right-of-way, shall exceed a slope of 15 percent, except as provided in this subsection 23.54.030.D.3.</p>	<p>Allow 20% driveway slope</p>	<p>The steeper ramp slope allows the building to be sunken further into the ground and reduce the overall height of the buildings as seen from the single-family zone across the alley to the west.</p>

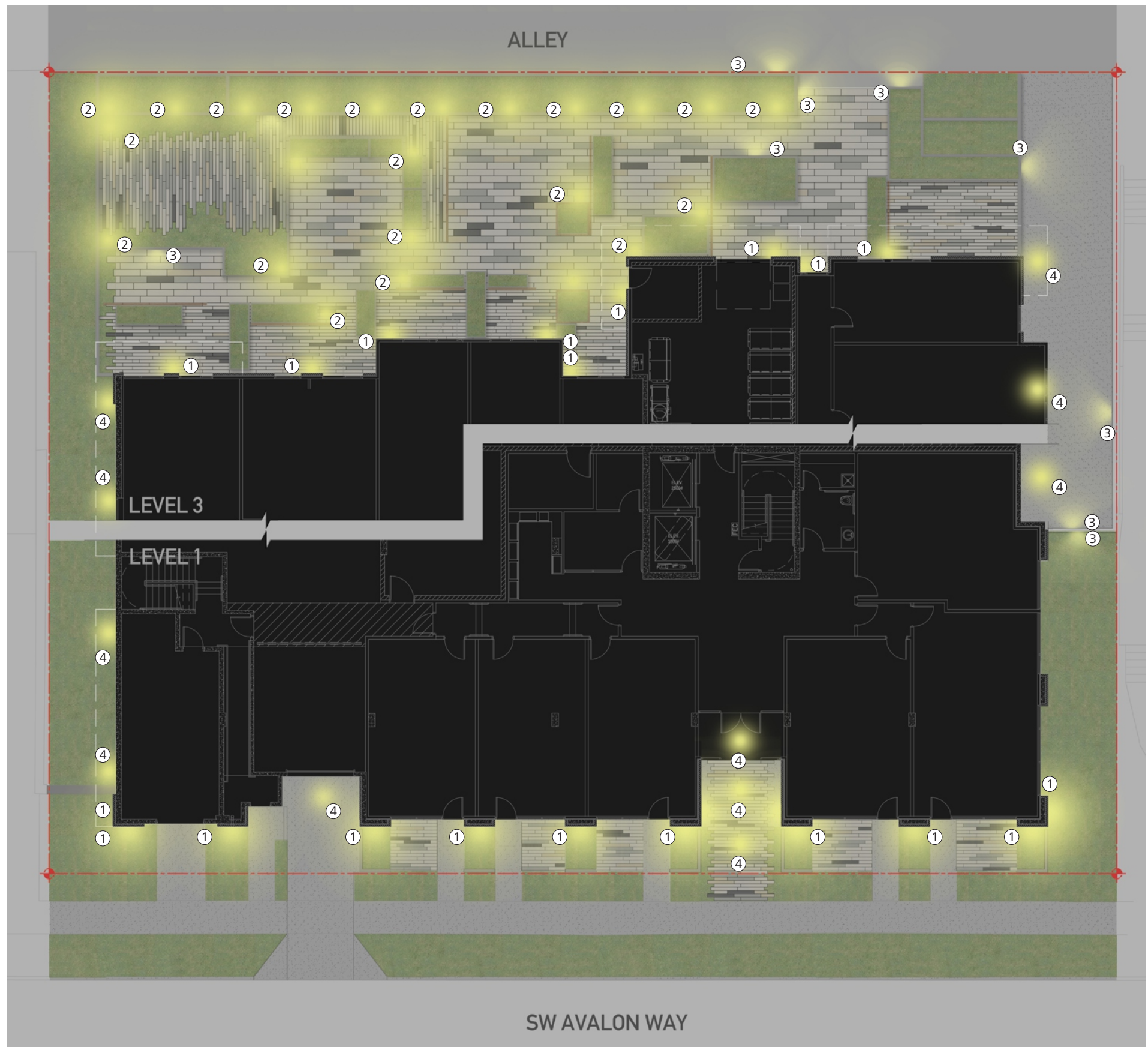
DEPARTURE REQUEST





LANDSCAPE PLAN





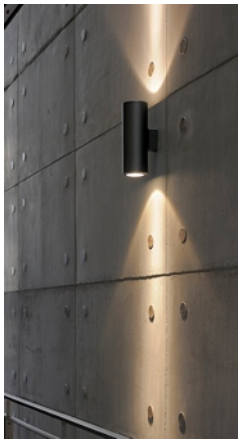
EXTERIOR LIGHTING PLAN

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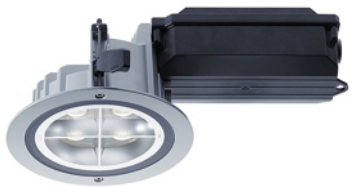
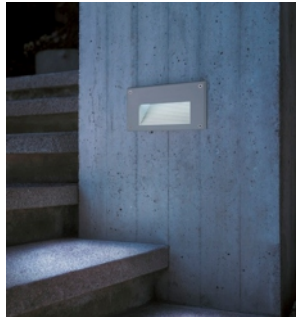
① SLV Lighting Rox Pro G8.5



② ERCO Lighting Midipoll



③ SLV Lighting Downunder LED



④ 4" Recessed LED Can Light







THANK YOU FOR YOUR CONSIDERATION!





VOLTA



BREMERTON WATERFRONT CONDOMINIUMS



23RD AND MADISON



306 QUEEN ANNE AVE N (Currently under construction)



LEGACY AT PRATT PARK



MURIEL'S LANDING

RELATED PROJECTS

