

BRESSI GARAGE

LANDMARKS BOARD BRIEFING PACKAGE | 08.26.2022



PROJECT INFORMATION

PROPERTY ADDRESS:
226-232 1st Avenue North
OWNER:
City of Seattle
LESSEE:
365 Group LLC
ARCHITECT:
Generator Studio LLC T 816.333.6527
HISTORIC PRESERVATION CONSULTANT:
Ron Wright & Associates/Architects T 206.728.4248

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PROJECT TEAM

1. INTRODUCTION

TEAM:

Our project team is comprised of experts who can ensure the long term success and longevity of the proposed project, while maintaining the historic character of the Bressi building and providing an expansion that feels rooted in its place, time and context.

GENERATOR STUDIO:

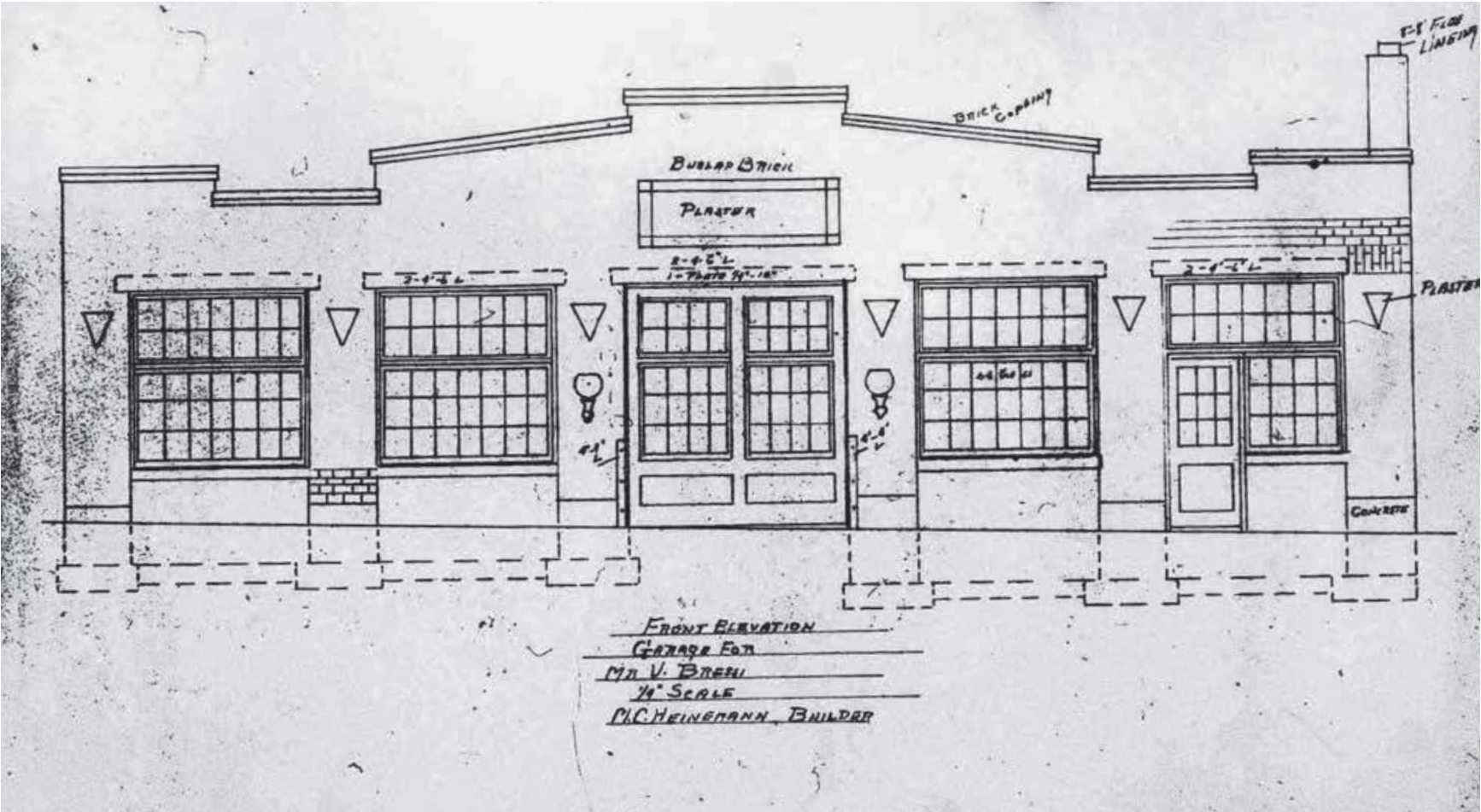
Generator Studio is a Kansas City architecture and interior design firm specializing in lifestyle, venue and live entertainment destinations. We are a group of architects and designers all-consumed with creating experiences and emotion through design. Our diverse portfolio of work is tied together by one thing: hospitality. At Generator, there is no formula for success. We approach each project and client with fresh eyes focused on creating developments that are inherently economically and environmentally sustainable.

Generator Studio is the architect of record for the proposed Bressi Garage renovation project, and recently completed the Kraken Community Iceplex in Northgate.

RON WRIGHT & ASSOCIATES/ARCHITECTS:

Ron Wright is the Historic Preservation Consultant for the proposed Bressi Garage renovations.

Founded in 1994, Ron Wright & Associates/Architects, P.S. (RWAA) is an established leader in the development of specialized projects for both public and private clients, with a specific focus on historic preservation projects. Our experience includes numerous projects within the City of Seattle, including the restoration of Union Station and Washington Hall, both awarded National Preservation Awards. We are completely versed in the interpretation of the Secretary of the Interior’s Standards for Rehabilitation, and the applicable Guidelines for Rehabilitating Historic Buildings, and have completed numerous projects reviewed by the Landmarks Preservation Board, the Pioneer Square Preservation Board, the International Special Review District, and the Pike Place Market Historical Commission. Other notable projects include the restoration of the Pioneer Square Pergola and the completion of the Lodge at St Edward State Park, both awarded Washington State Preservation Officer Valerie Sivinski Awards for Outstanding Historic Rehabilitation.



Front elevation, Bressi Garage. March 1923. M.C. Heinemann, Builder.
Courtesy Seattle Department of Planning and Development Microfilm Library.
From the Bressi Garage - Seattle Landmark Nomination file no. 198920-1515

SITE AERIAL

2. SITE CONTEXT



PROJECT SUMMARY

3. PROJECT SUMMARY

226-232 1st. Avenue North, the Bressi Garage, is a brick commercial building located at the NW corner of 1st Avenue North and Thomas Street in Lower Queen Anne. Bressi Garage was constructed in 1923 by M.C. Heinemann and operated by Dominick Bressi. Following the 1962 World's Fair and subsequent acquisitions and redevelopment of the surrounding area, Bressi Garage remains as the only building on its block which reflects the historic, pre-World's Fair Lower Queen Anne neighborhood character.

Designed to accommodate automobiles, Bressi Garage is a single story brick building with a square footprint of roughly 120' x 120.' Technically comprised of two buildings with a shared party wall, the building was first used as a parking facility by the city of Seattle. In 1973, the south bay was converted to an artist studio when leased by Pottery Northwest. Later in 1986, Seattle Center remodeled the north bay for use as its gardener's facility. In recent decades, the historic building has been re-roofed, with added internal columns to support the roof trusses and additional shoring measures to reinforce the load bearing brick walls. As part of the recent renovation of Key Arena into the now Climate Pledge Arena, Bressi Garage and the surrounding parcel was acquired to build the entry to Climate Pledge Arena's loading dock tunnel, which passes below the SE portion of the Bressi building, then moves north under Thomas St to the Arena.

This building is going to be renovated to better suit its new context in a vibrant, growing urban center and designated arts and cultural district. Adjacent to a celebrated sports and entertainment venue and a gateway point from Uptown into the Seattle Center campus. This update will make the building lively and inviting, so many people can enjoy it. Although the zoning would allow for expansion up to 85' above the Average Grade Level, the project proposes the addition of a single story above the historic wood trusses and structural deck. The primary level 2 enclosure is proposed to maintain an approximate 38.5' setback along the west and a 43.75' setback along the north, with an occupiable roof deck extending along the north end of the new level 2. The reasons for this massing strategy are many: 1) to maintain the historic character of the pedestrian experience along 1st Ave. N and Thomas St, 2) to prioritize the celebration and maintenance of the historic structure, 3) to align with the vertical datum and average heights of the surrounding structures, and 4) to preserve the existing view corridor west along Thomas St. Proposed massing and zoning are presented on pages 9-26.

The project proposes limited intervention at the existing historic facades, maintaining historic entry locations along 1st Ave. N and replacing existing non-historic infills along Thomas St. and the east alley to enhance pedestrian access. The proposed second level enclosure will rest above the historic roof structure, with new structure used to support the buckling existing historic wood trusses.



Photograph of the west and north elevations of Bressi Garage, ca. 1936.
Courtesy King County Property Record Card, Puget Sound Regional Archives.
From the Bressi Garage - Seattle Landmark Nomination file no. 198920-1515

EXISTING SITE PLAN

4. EXISTING SITE PLAN

Existing Site Conditions

The Bressi Garage site is located on the corner of 1st Avenue N and Lenny Wilkens Way. South of the Bressi Garage is a parking lot for satellite trucks and tunnel access completed as part of the Climate Pledge Arena (CPA) development. East of the Bressi Garage is a parking garage with capacity for 620 vehicles and 33 long-term bicycles.

1st. Avenue N is a Class III Pedestrian route. Lenny Wilkens Way is a designated Green Street. Right of way, landscaping, street and sidewalk improvements were made during the Climate Pledge Arena Development.



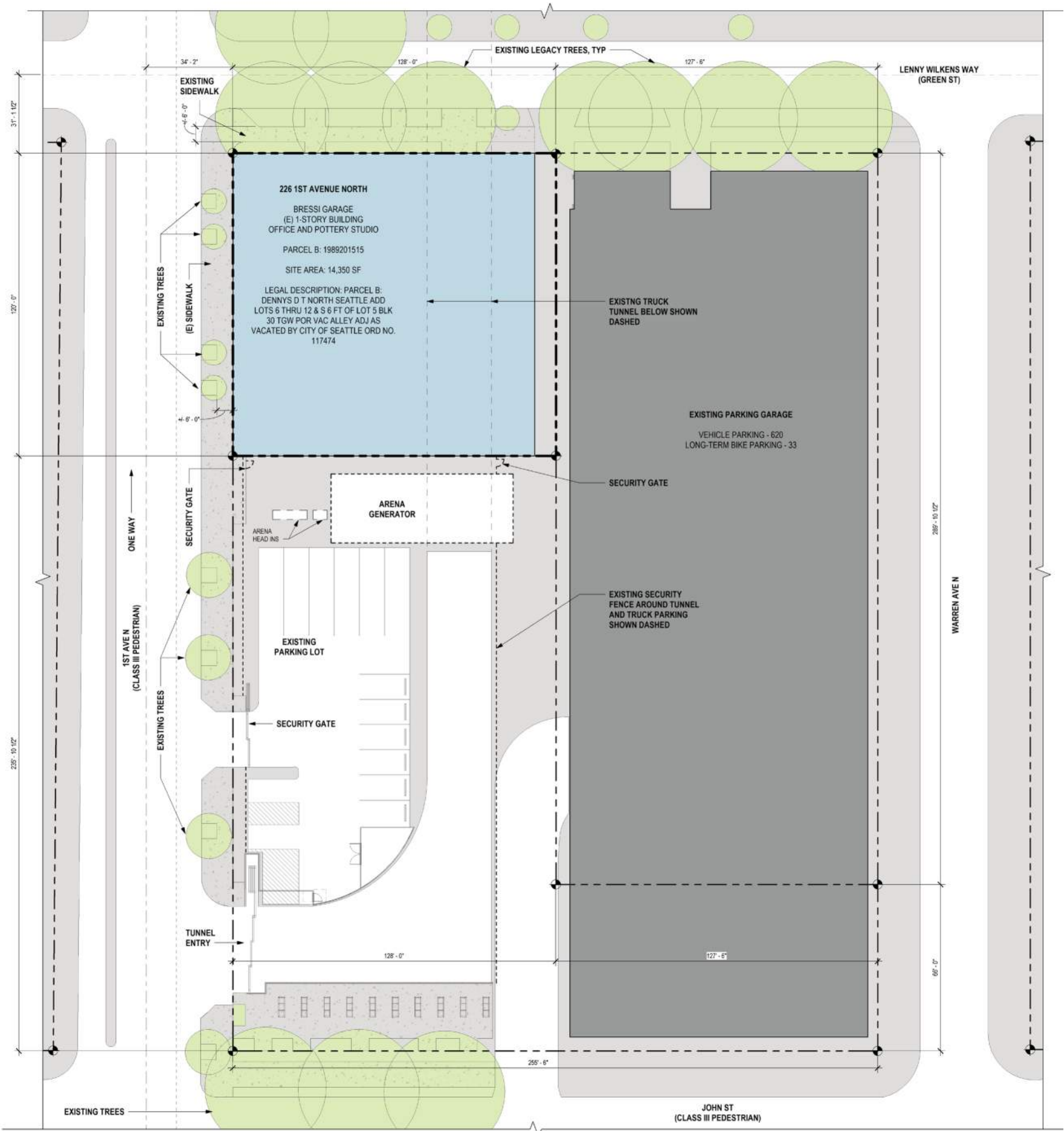
Existing - Northwest corner along Lenny Wilkens Way and 1st Ave N



Existing - Southwest corner along 1st Ave N



Existing - Northeast corner along 1st Ave N



EXISTING CONDITIONS

5. PHOTOGRAPHS



Southwest corner along 1st Ave N



Northwest corner along 1st Ave N



Existing alley and facade infill at west facade



Existing courtyard in north building



Existing north facade along Thomas St

EXISTING CONDITIONS

5. PHOTOGRAPHS



Southeast corner along existing alley



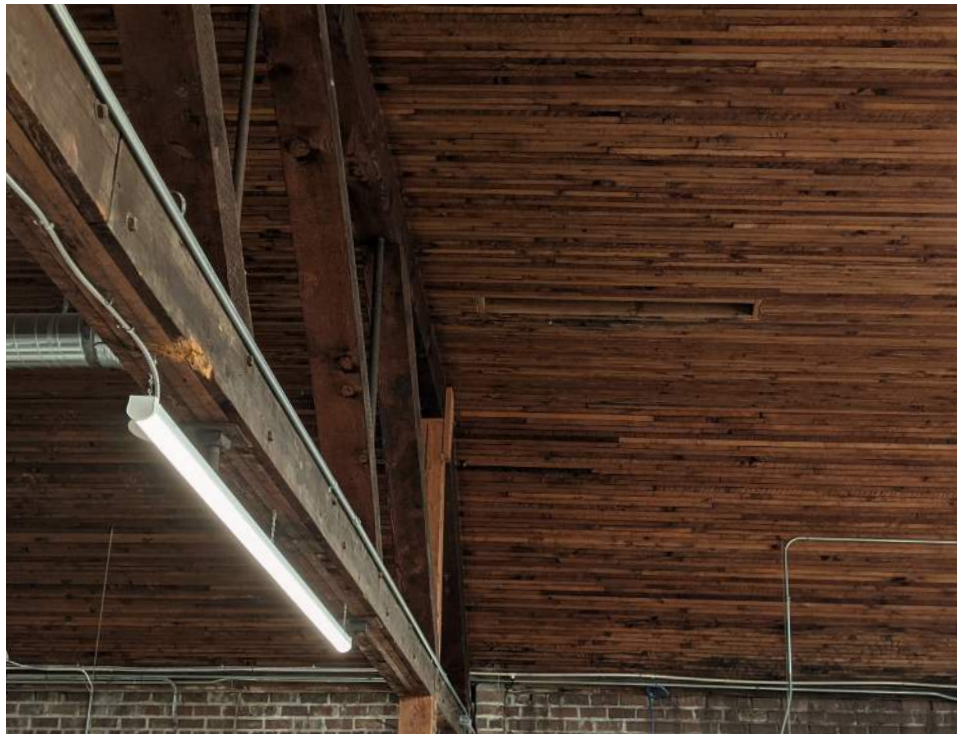
Southwest corner looking east



Existing truss condition at courtyard



Historic truss and roof deck



Historic truss and roof deck

6. ZONING ENVELOPE



PROJECT INFORMATION

6. ZONING ENVELOPE

Site Location
226-232 1st Avenue N.

Parcel No.
1989201515

Site Zoning
SM-UP 85 (M)

Overlays
Not Applicable

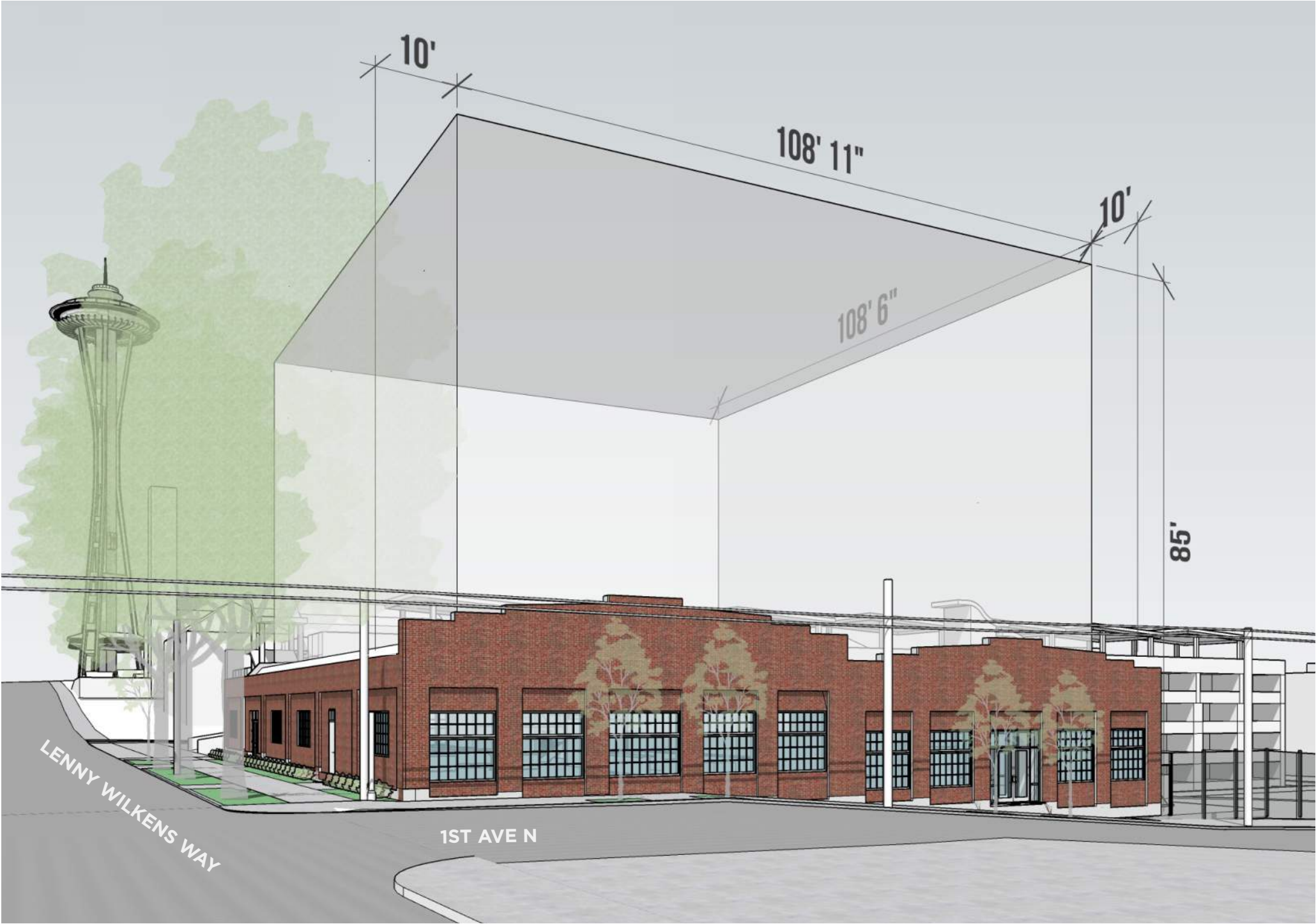
Lot Area
14,330 Gross Square Feet

Floor Area Ratio
5.25 per 23.48.720 Table A

Height
85 feet (23.48.025)

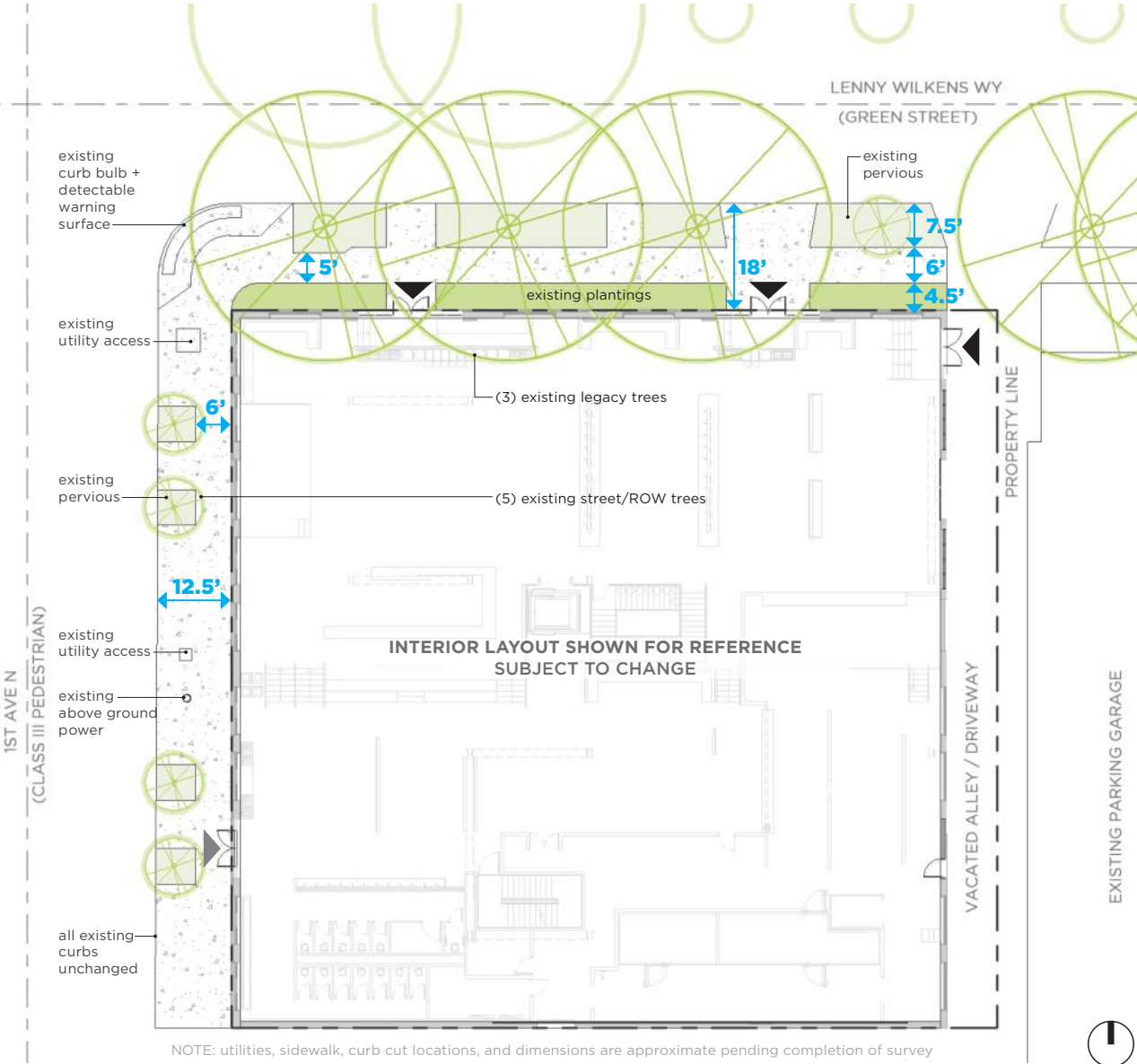
Setback Requirements (23.48.735)
In all SM-UP zones, any portion of a structure greater than 45 feet in height or 65 feet in height must be set back from a lot line that abuts a designated street shown on Map A for 23.48.735. A setback of an average of 10 feet from the lot line is required for any portion of a structure exceeding the maximum height that is permitted without a setback.

Permitted Uses (23.48.005)
All uses permitted except all high impact uses, all heavy manufacturing uses, general manufacturing uses (greater than 25,000 square feet of gross floor area for an individual business establishment, except pharmaceutical production), drive in businesses (except gas stations, jails, adult motion picture theaters and adult panorams, outdoor storage and kennels, animal husbandry, park and pool lots, work release centers, recycling solid waste management and mobile home parks. Mini - warehouses and warehouses, helistops and heliports are permitted as conditional uses.



LANDSCAPE + GREEN STREET PLAN

7. LANDSCAPE PLAN



EXISTING STREET IMPROVEMENTS AT NW CORNER

Curb bulb shortens pedestrian crossing distance.
Existing sidewalks are the minimum depth in all directions.



EXISTING 1ST AVE N IMPROVEMENTS

1st Ave N is divided with 2-way dedicated bike lanes, 1 traffic lane, 1 bus lane, and 1 parking lane.



EXISTING STREET IMPROVEMENTS ALONG LENNY WILKENS WAY

Existing pervious groundcover in ROW protects legacy and new trees.
New landscaping along historic facade is installed around existing pedestrian access points.



LENNY WILKENS WY IMPROVEMENTS

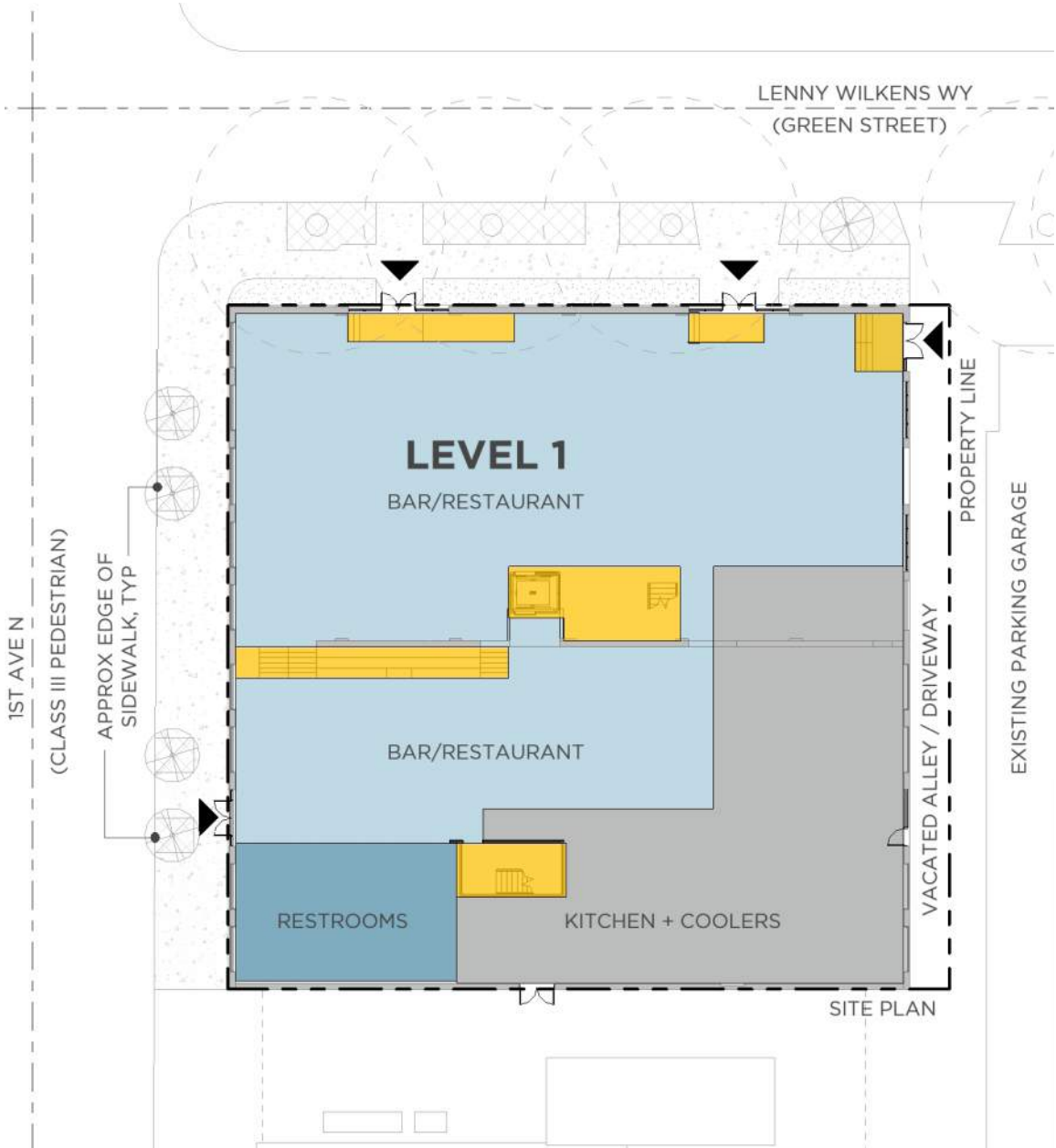
Lenny Wilkens Way along the north is improved with additional landscaping and new sidewalks.

KEY

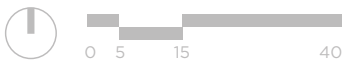
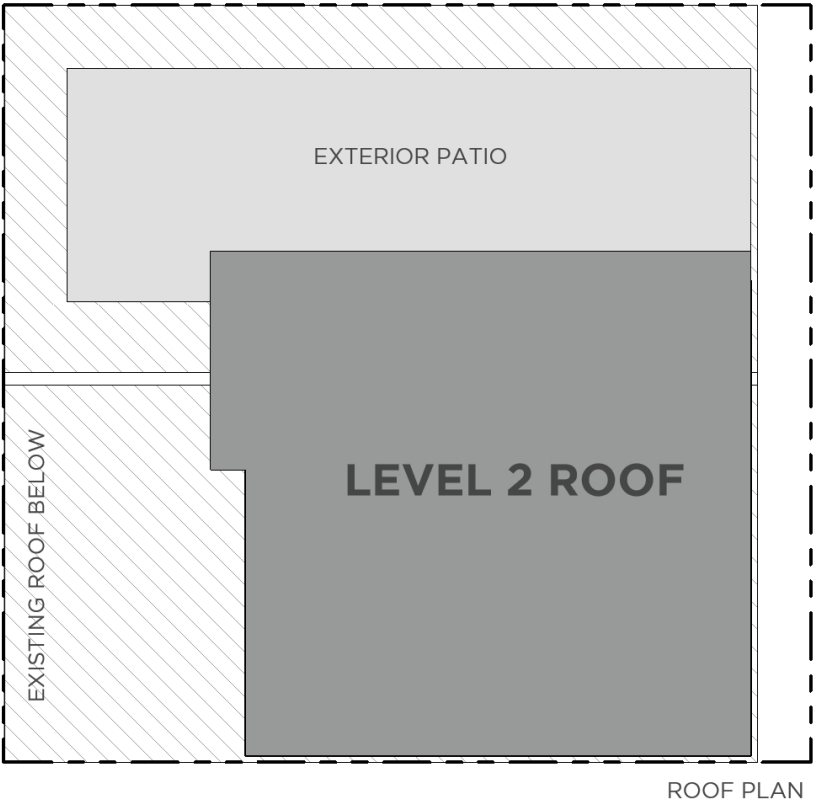
- ▶ NEW PUBLIC INGRESS / EGRESS
- ▶ EXISTING PUBLIC INGRESS / EGRESS

SITE PLAN

8. MASSING CONCEPT PLANS



- KEY**
- ▶ EGRESS / INGRESS
 - VERTICAL CIRCULATION
 - INTERIOR PROGRAMMING
 - SECOND FLOOR PATIO



MASSING

8. MASSING CONCEPT

OVERVIEW

The massing of the second floor is proposed in the southeast corner to meet development set back requirements, and to allow the Landmarked Bressi Garage to maintain the historic street and pedestrian experience. By placing the enclosed portion of the building in this nature, it also allows the second floor exterior patio to take advantage of the views to Climate Pledge Arena and the Space Needle. It will allow Bressi Garage to maintain its singular identity while the proposed glazed entrances blur the line between indoor and outdoor. The second floor enclosure is also pulled away from Lenny Wilkens Way to not encroach on the legacy trees.



NOTE: ALL SIGNS AND GRAPHICS ARE PLACEHOLDERS

NORTHWEST VIEW

8. MASSING CONCEPT



NOTE: ALL SIGNS AND GRAPHICS ARE PLACEHOLDERS

SOUTHWEST VIEW

8. MASSING CONCEPT



NOTE: ALL SIGNS AND GRAPHICS ARE PLACEHOLDERS

NORTH VIEW

8. MASSING CONCEPT



WEST ELEVATION CHANGES

9. ALTERATIONS TO EXISTING

Proposed Alterations - West Elevation

Originally there were two vehicle entries into the building on the west facade. At some point the northern entry was removed and infilled with a window and brick to match the adjacent windows. The other (#1) was demolished to create a portal in which an inset storefront was constructed for entry into the building. The proposed modifications are to demolish the existing inset storefront at location #1 and install a new storefront at the building face. This will create one entry point into the building on the west facade.



South building - existing conditions



South building - existing conditions



South building - existing conditions



Proposed West Elevation - 1st Avenue Facade



WEST ELEVATION CHANGES

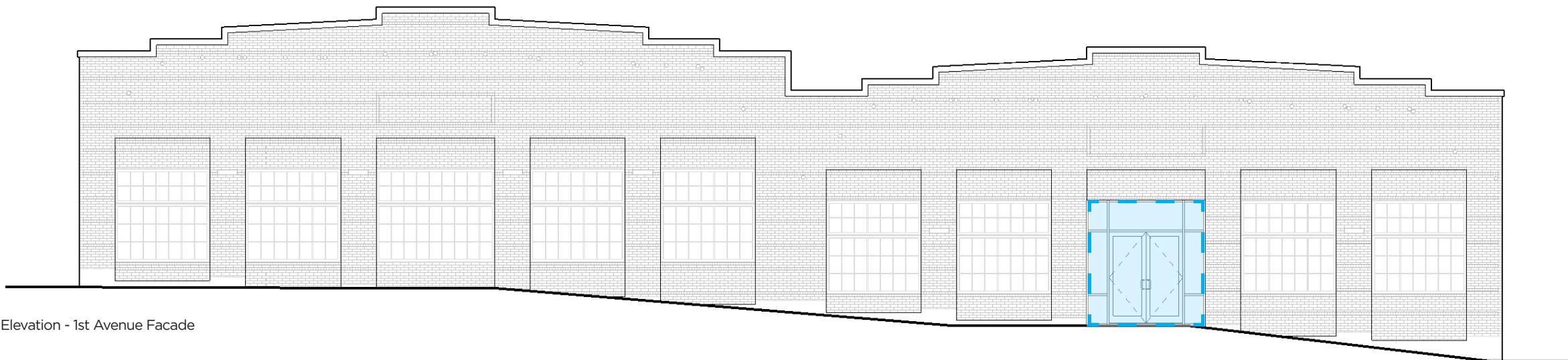
9. ALTERATIONS TO EXISTING



Existing West Elevation - 1st Avenue Facade



ARC #1 - Proposed West Elevation - 1st Avenue



ARC #2 - Proposed West Elevation - 1st Avenue Facade



WEST ELEVATION CHANGES

9. ALTERATIONS TO EXISTING



Proposed alterations



Existing elevation

NORTH ELEVATION CHANGES

9. ALTERATIONS TO EXISTING

Proposed Alterations - North Elevation

Currently on the north facade there are two openings into the building. The easternmost (#3) is an overhead door and the westernmost (#4) is a door with infill brick where a window used to be. At #3 the proposed modification is to remove the overhead door and surrounding brick within this bay and replace it with a storefront entrance. At #4 the proposed modification is to remove the existing door and surrounding brick within this bay and also replace it with a storefront entrance. This will bring much more transparency to the north facade and create a connection with Climate Pledge Arena.



North facade - existing conditions



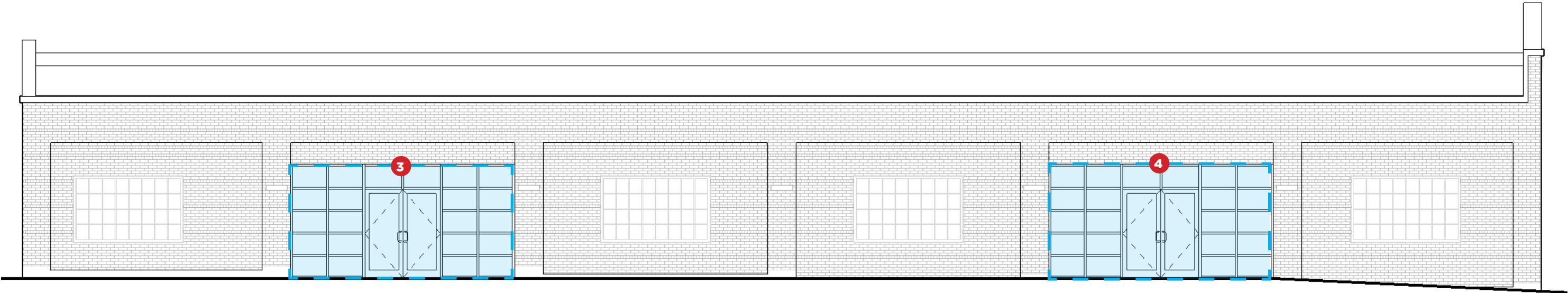
North facade - existing conditions



North facade - existing conditions



North facade - existing conditions

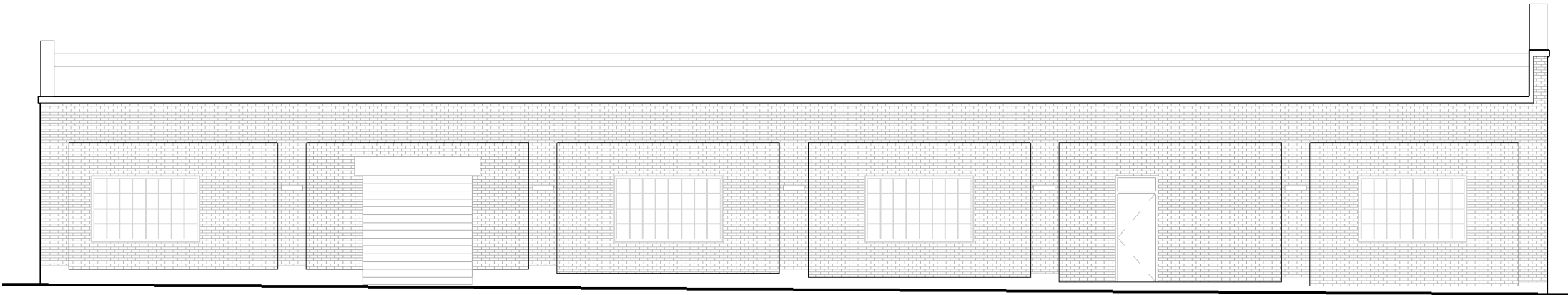


Proposed North Elevation - Thomas Street Facade

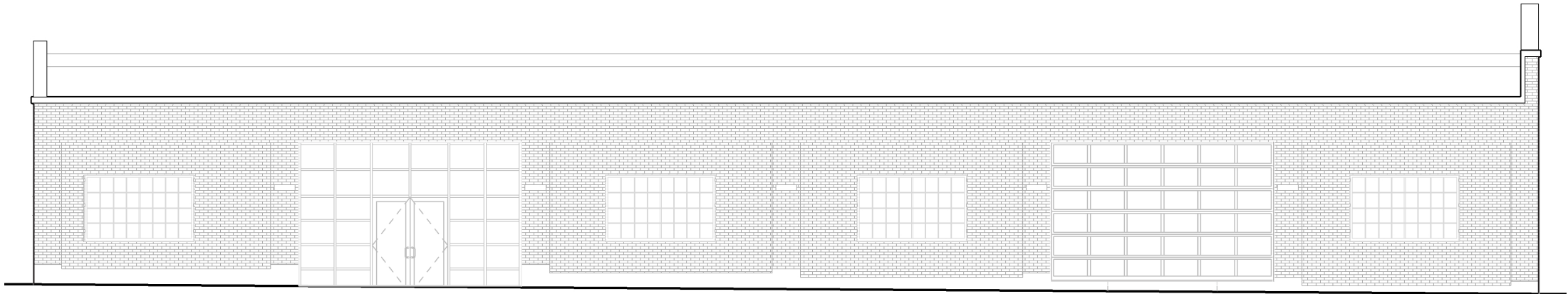


NORTH ELEVATION CHANGES

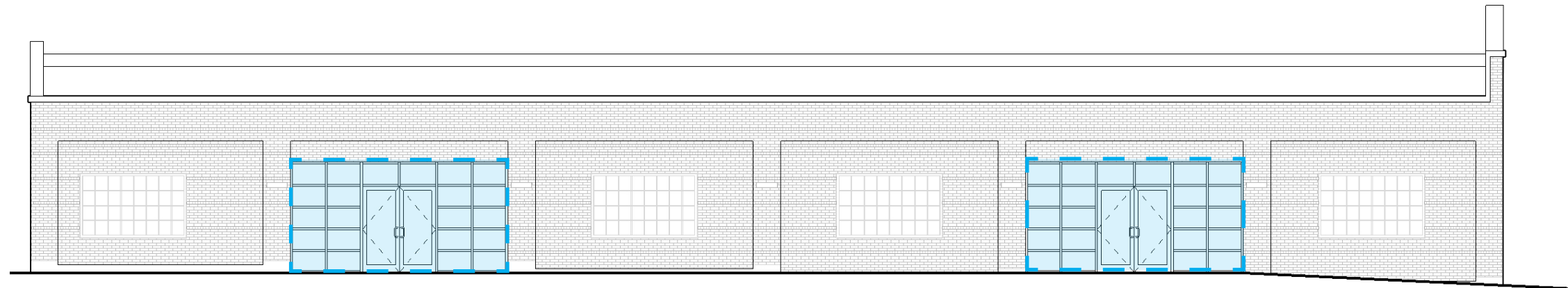
9. ALTERATIONS TO EXISTING



Existing North Elevation - Thomas Street Facade



ARC #1 - Proposed North Elevation - Thomas Street Facade



ARC #2 - Proposed North Elevation - Thomas Street Facade



NORTH ELEVATION CHANGES

9. ALTERATIONS TO EXISTING



Proposed alterations



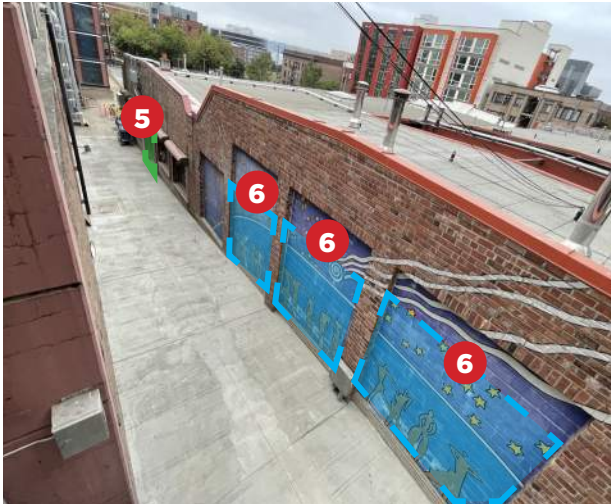
Existing elevation

EAST ELEVATION CHANGES

9. ALTERATIONS TO EXISTING

Proposed Alterations - East Elevation

On the East side of the building, large facade openings (#6 & #7) have been infilled with painted CMU. There is building access through a door at location #7. There is also a door and sliding door at location #5 that are not original to the building. The proposed modifications are to infill #5 with CMU. We are re-purposing the sliding door as an interior feature on the second floor. At locations marked #6 we would remove existing infill CMU and install a glazed overhead door at the central location and fixed glazed storefronts at the outer two locations. Due to a braced frame to support the new second floor patio it was not feasible for the outer two locations to be overhead doors. Above these new openings we are proposing metal panel infill. Keeping the existing CMU was not feasible structurally. At location #7 we would replace the CMU with a glazed storefront entry. This would allow for a building entry at #7 and transparency between indoor and outdoor at #6's. The overhead door could also be opened on nice days to create a connection to the outdoors.



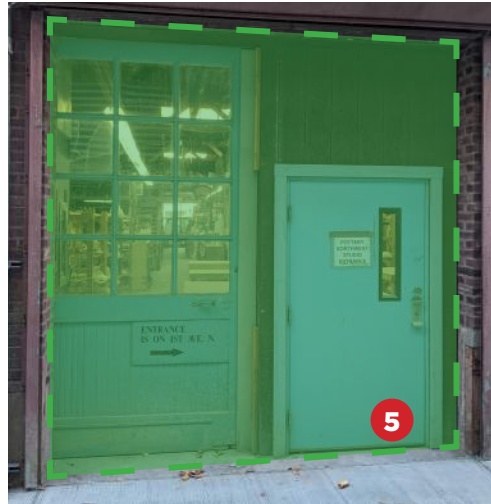
East Facade - existing alley conditions



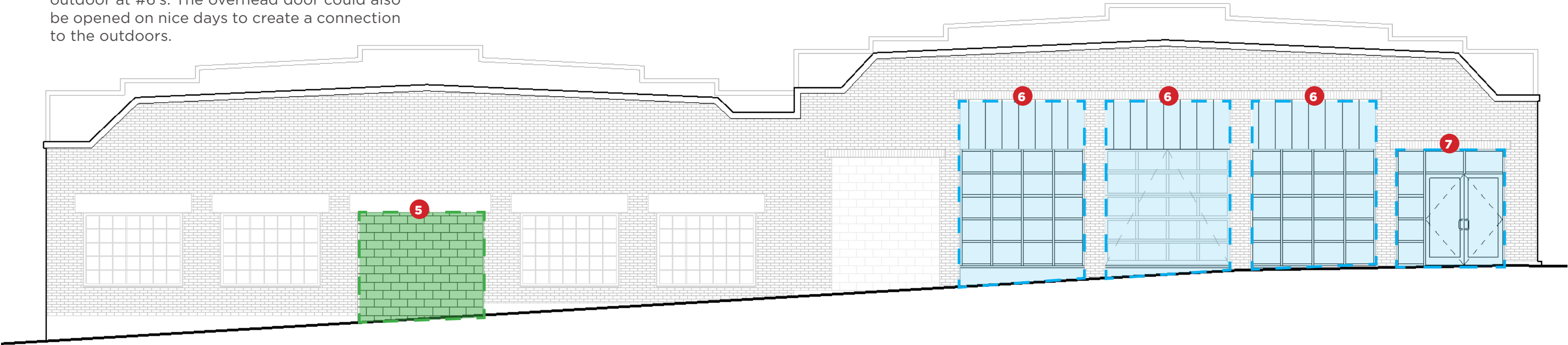
East Facade - existing alley conditions



East Facade - existing alley conditions



East Facade - existing alley conditions

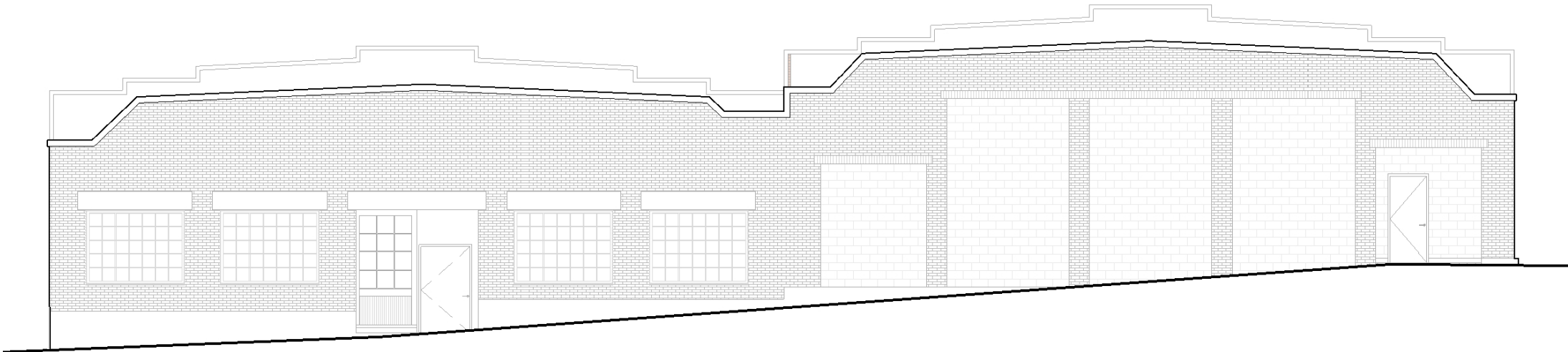


Proposed East Elevation - Alley Facade



EAST ELEVATION CHANGES

9. ALTERATIONS TO EXISTING



Existing East Elevation - Alley Facade



ARC #1 - Proposed East Elevation - Alley Facade



ARC #2 - Proposed East Elevation - Alley Facade



EAST ELEVATION CHANGES

9. ALTERATIONS TO EXISTING



SOUTH ELEVATION CHANGES

9. ALTERATIONS TO EXISTING

Proposed Alterations - South Elevation

At one time, a metal roof extended from the south facade. It has since been removed, but some metal flashing still remains. The proposed modifications on the south facade are to remove the metal flashing (#11) and also cut in a double door (#10) and add a ramp for building operations. The dashed area at the southwest corner indicates the approximate location of the existing gas equipment to remain.



South Facade - existing gas equipment
Shown dashed below



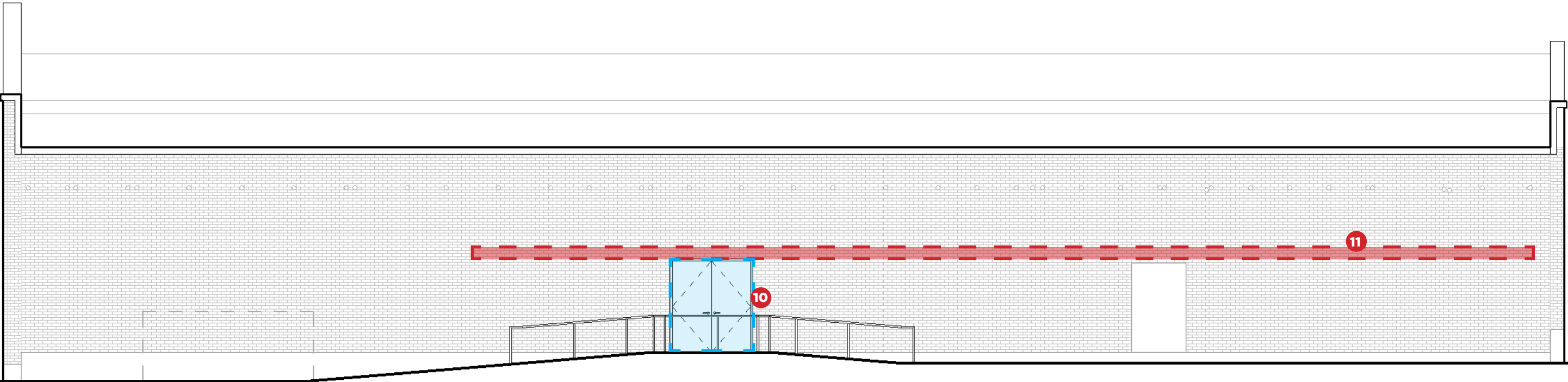
South Facade - existing conditions



South Facade - existing conditions



South Facade - existing conditions



Proposed South Elevation



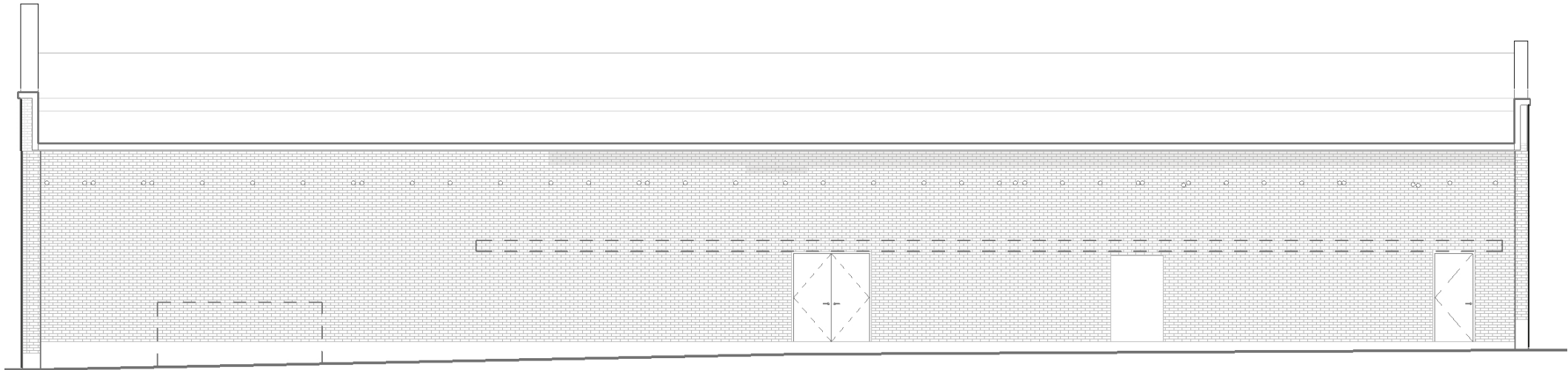
SOUTH ELEVATION CHANGES

9. ALTERATIONS TO EXISTING

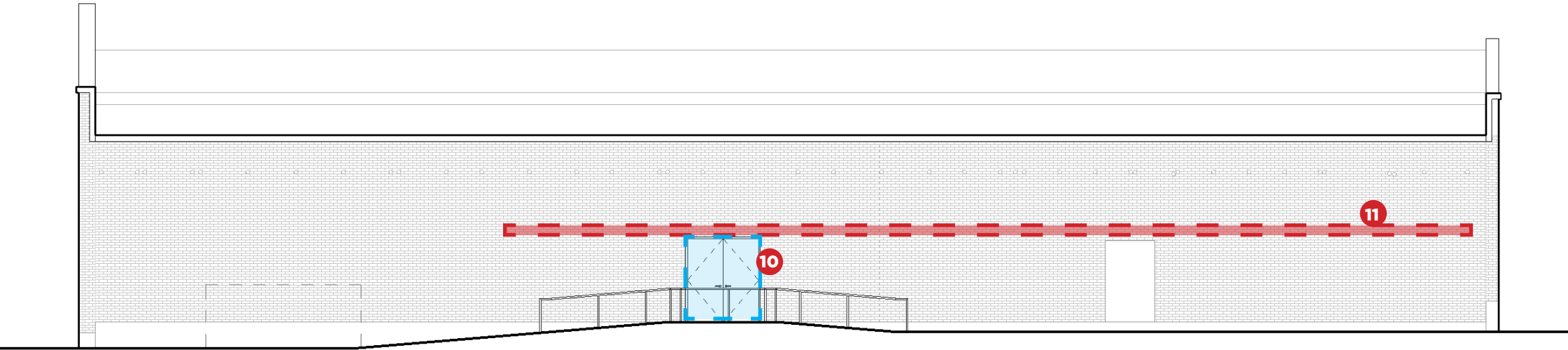
Existing South Elevation



ARC #1 - Proposed South Elevation



ARC #2 - Proposed South Elevation

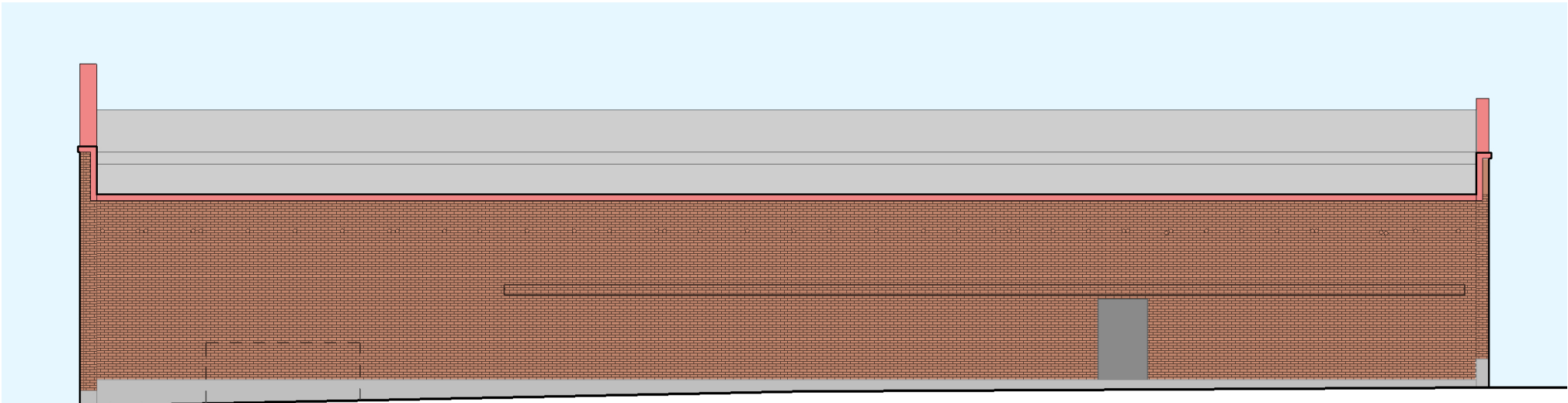


SOUTH ELEVATION CHANGES

9. ALTERATIONS TO EXISTING



Proposed alterations



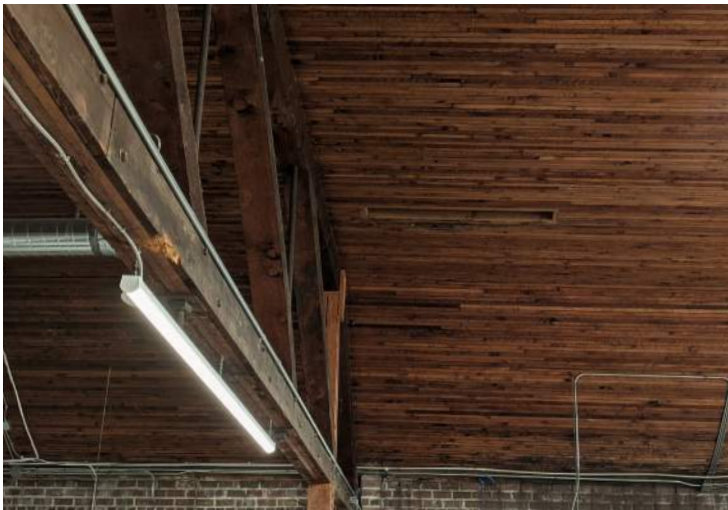
Existing elevation

EXISTING ROOF CHANGES

9. ALTERATIONS TO EXISTING

Proposed Alterations - Roof

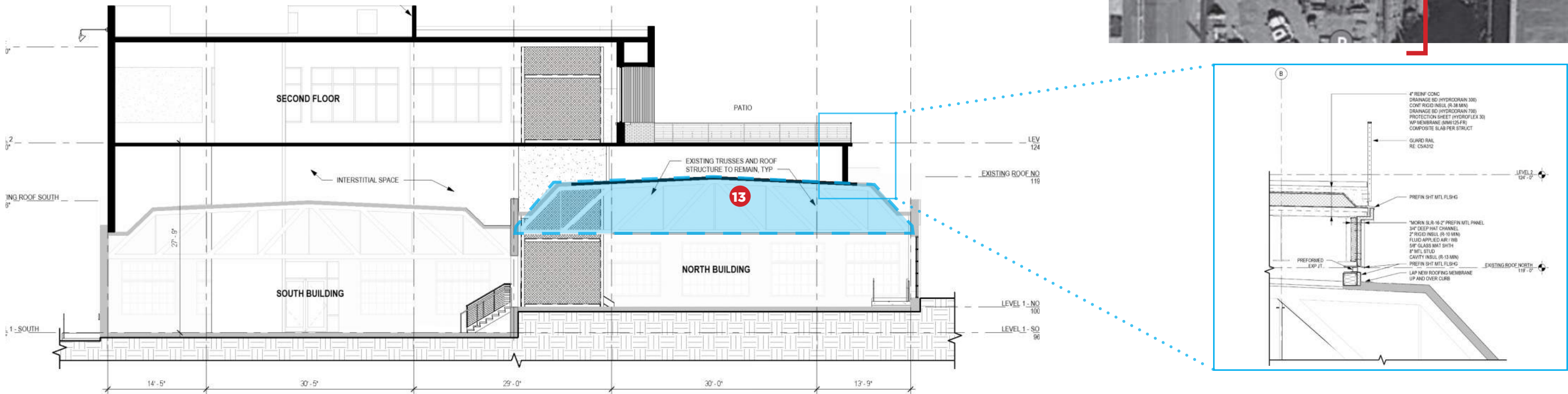
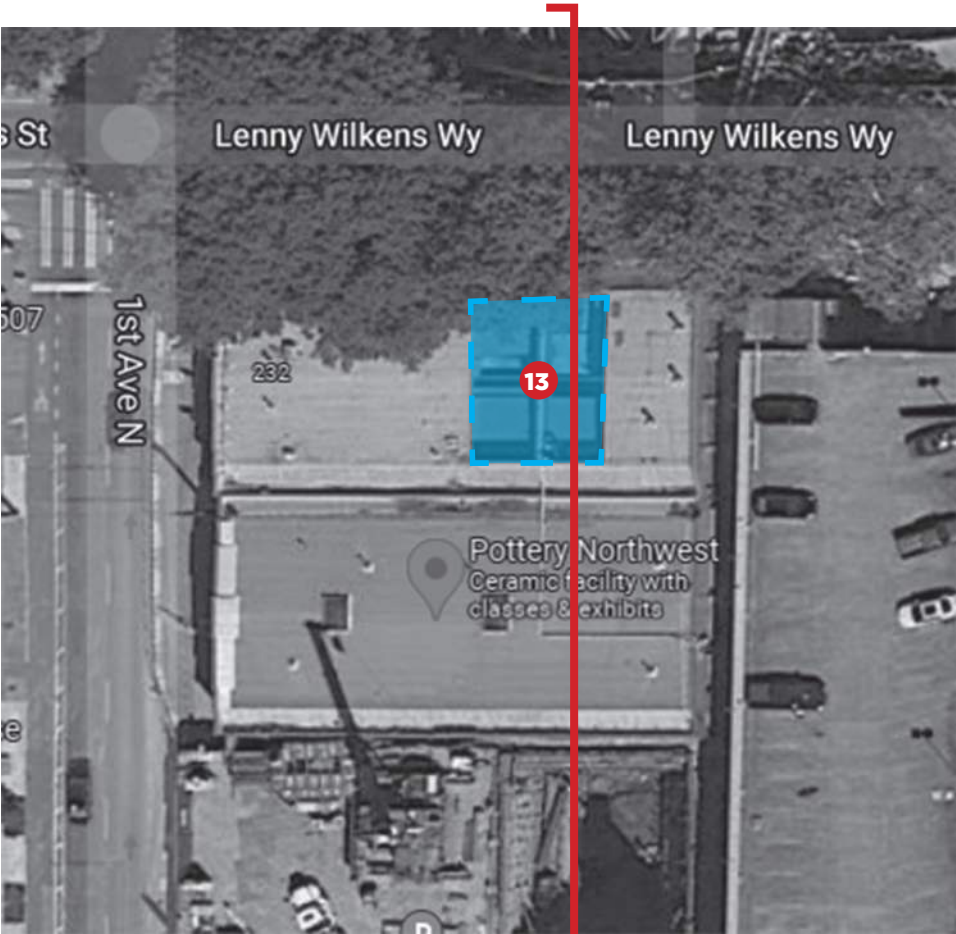
Currently in the northeast portion of the roof there are openings (#13) that were created during a previous alteration to the building. The exposed trusses were clad with corrugated metal panel. The proposed alteration is to remove the metal panel from the trusses and infill the roof to match the existing roof. Returning it to its original form.



Historic truss and roof



Roof - existing conditions



RECAP

10. MASSING OPTIONS SUMMARY

ARC #1 LANDMARK MASSING



ARC #2 LANDMARK MASSING



NOTE: ALL SIGNS AND GRAPHICS ARE PLACEHOLDERS