

West Seattle and Ballard Link Extensions

*Seattle Design Commission
Briefing:
Interbay-Ballard Stations and
Guideway Structures*

March 17, 2022



Agenda

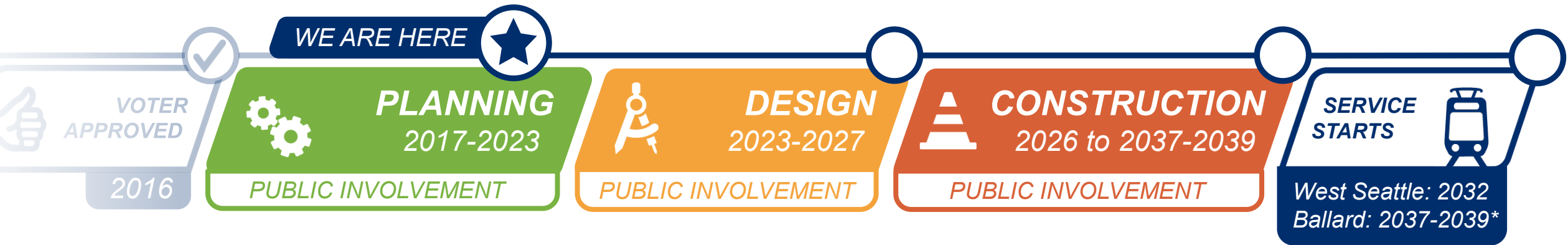
- Welcome and introductions
- Project orientation
- Guideway structures and bridges
- Smith Cove Station
- Ballard Station

Agenda

- Welcome and introductions
- ***Project orientation***
- Guideway structures and bridges
- Smith Cove Station
- Ballard Station

West Seattle and Ballard Link Extensions

Project timeline





PLANNING



DES

2017–2019

Alternatives development

- ✓ Feb–March 2018: Early scoping
- ✓ Feb–April 2019: Scoping
- ✓ May–Oct 2019: Board identified preferred alternatives and other DEIS alternatives



2019–2023

Environmental review

Early 2022: Publish Draft EIS

Public comment period

Board confirms or modifies preferred alternatives

2023: Publish Final EIS

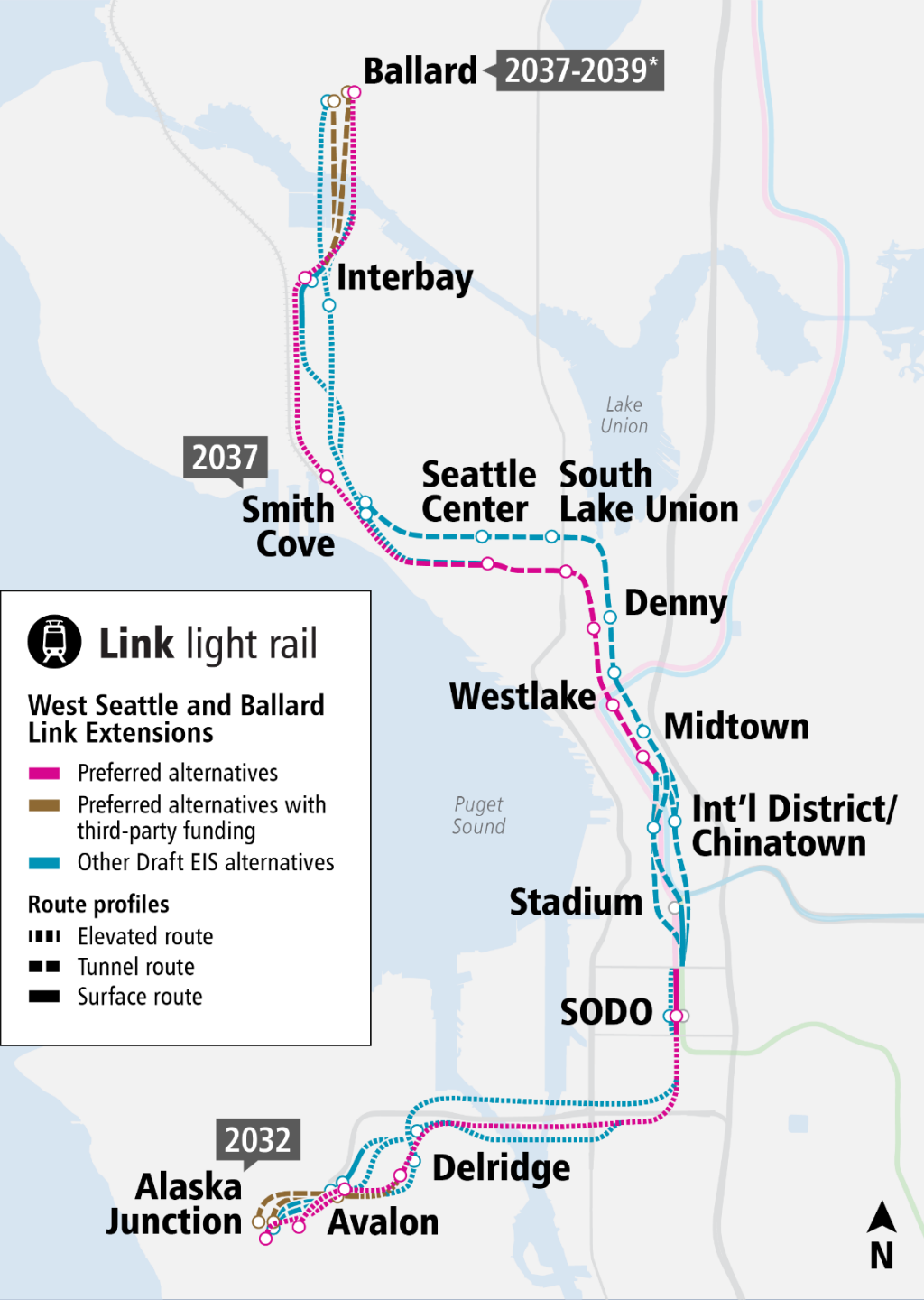
Board selects projects to be built

Federal Record of Decision

PUBLIC INVOLVEMENT

Draft EIS alternatives

What we're studying in this phase







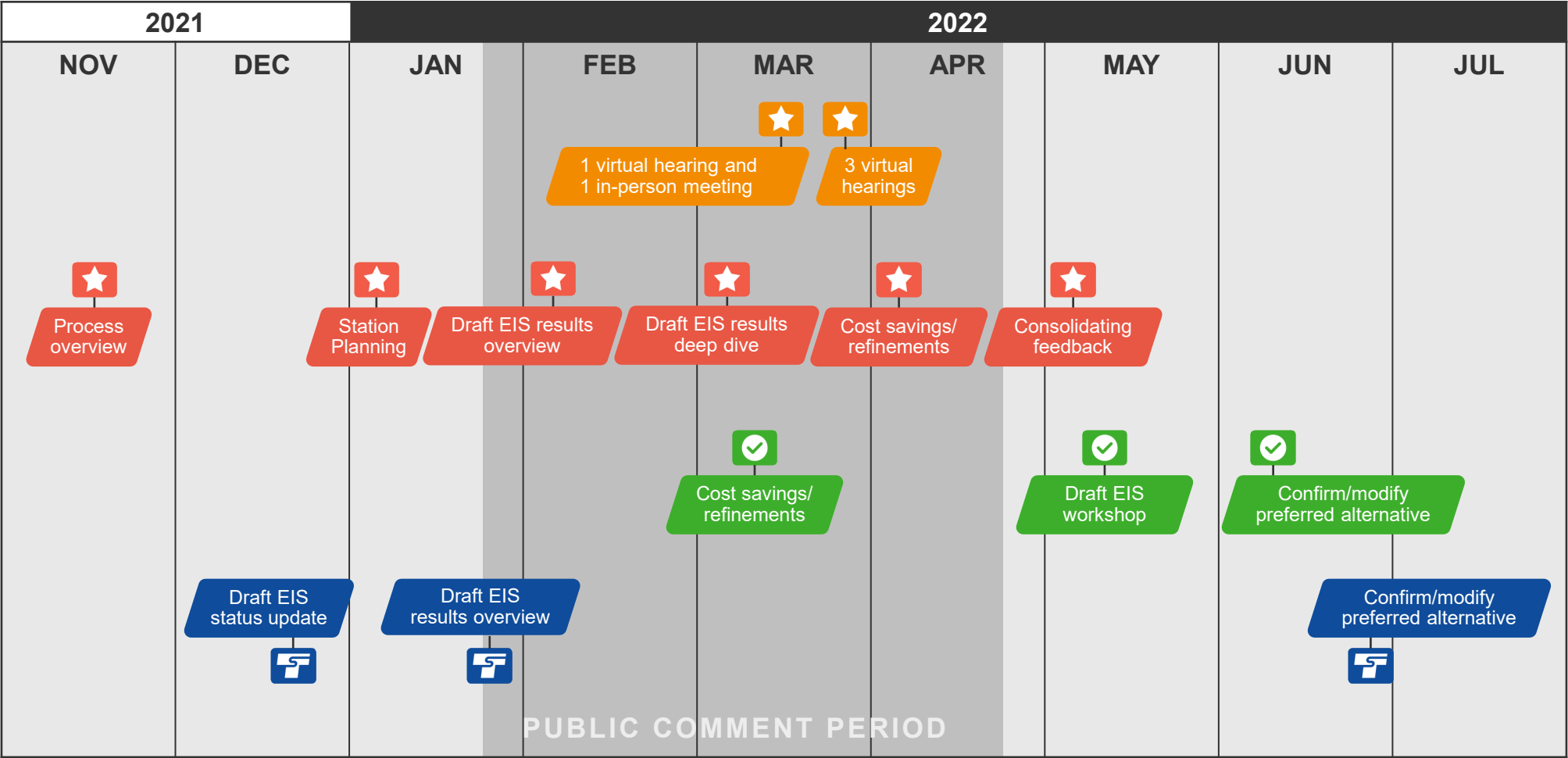
- Preferred Alternatives
- Preferred Alternatives with Third-Party Funding
- Other Draft EIS alternatives

*Dates reflect an affordable schedule based on current financial projections and cost estimates, and a target schedule.

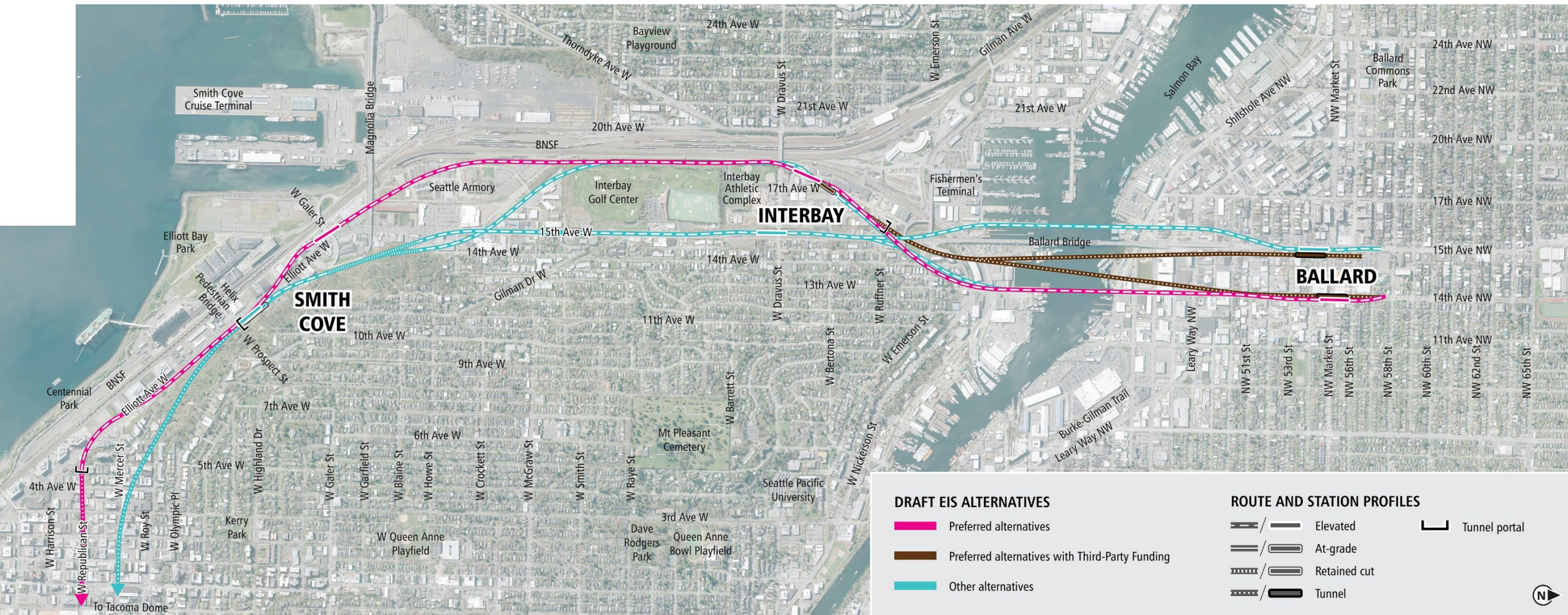
Community engagement and collaboration

Draft Environmental Impact Statement (EIS)

-  Draft EIS Public Meetings
-  Community Advisory Groups
-  Sound Transit System Expansion Committee
-  Sound Transit Board



***Overview of WSBLE
Alternatives in Interbay-
Ballard***





PM Peak Travel Times (in 2042)

NW Market St/15th Ave NW to Westlake

Without Link: 38 mins

With Link: 11mins



PM Peak Transit Reliability (in 2042)

Ballard Link Project Corridor

Without Link: E/F rating

With Link: A rating

DRAFT EIS ALTERNATIVES

- Preferred alternatives
- Preferred alternatives with Third-Party Funding
- Other alternatives

ROUTE AND STATION PROFILES

- Elevated
- At-grade
- Retained cut
- Tunnel
- Tunnel portal



All Smith Cove Alternatives

Ridership (in 2042) -
Average Daily Boardings: 2,600
Principal mode of access: Walk - 65%



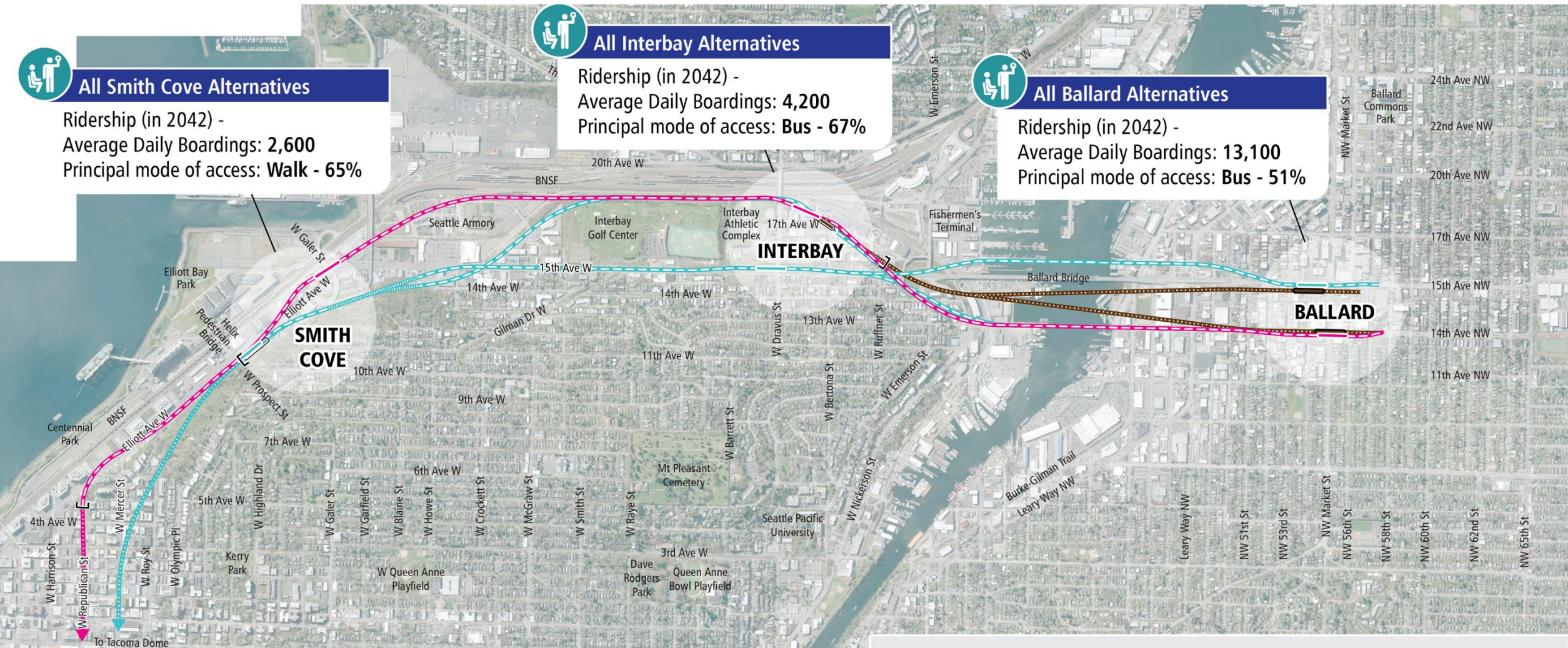
All Interbay Alternatives

Ridership (in 2042) -
Average Daily Boardings: 4,200
Principal mode of access: Bus - 67%



All Ballard Alternatives

Ridership (in 2042) -
Average Daily Boardings: 13,100
Principal mode of access: Bus - 51%



DRAFT EIS ALTERNATIVES

- Preferred alternatives
- Preferred alternatives with Third-Party Funding
- Other alternatives

ROUTE AND STATION PROFILES

- / Elevated
- / At-grade
- / Retained cut
- / Tunnel
- Tunnel portal



Elevated Galer St

Bus routes serving station: 10
 Bus stop zones at station: 6
 Bus service with direct access to entrances*: 70%



17th Ave Alternatives

Bus routes serving station: 3
 Bus stop zones at station: 3
 Bus service with direct access to entrances*: 66%



15th Ave Alternatives

Bus routes serving station: 5
 Bus stop zones at station: 6
 Bus service with direct access to entrances*: 63%



Elevated 15th Ave

Bus routes serving station: 3
 Bus stop zones at station: 3
 Bus service with direct access to entrances*: 50%



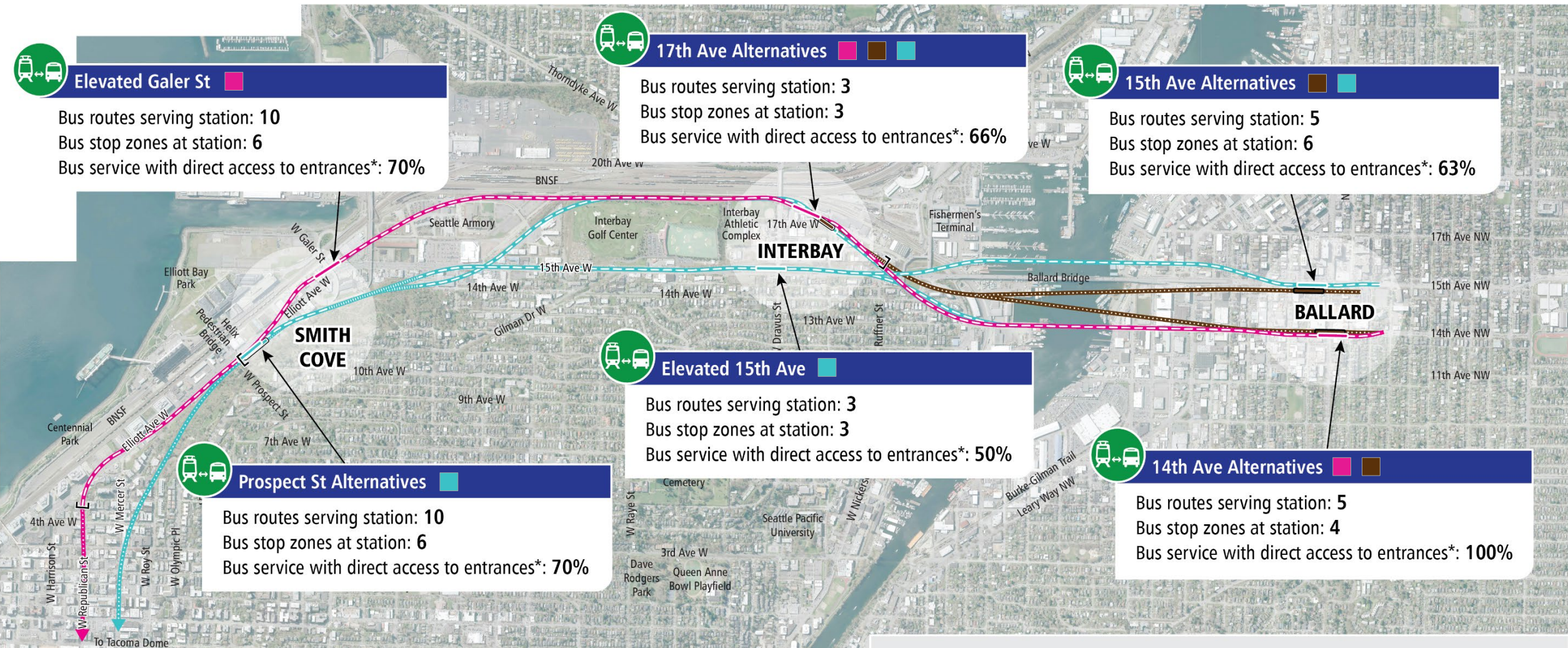
14th Ave Alternatives

Bus routes serving station: 5
 Bus stop zones at station: 4
 Bus service with direct access to entrances*: 100%



Prospect St Alternatives

Bus routes serving station: 10
 Bus stop zones at station: 6
 Bus service with direct access to entrances*: 70%



DRAFT EIS ALTERNATIVES

- Preferred alternatives
- Preferred alternatives with Third-Party Funding
- Other alternatives

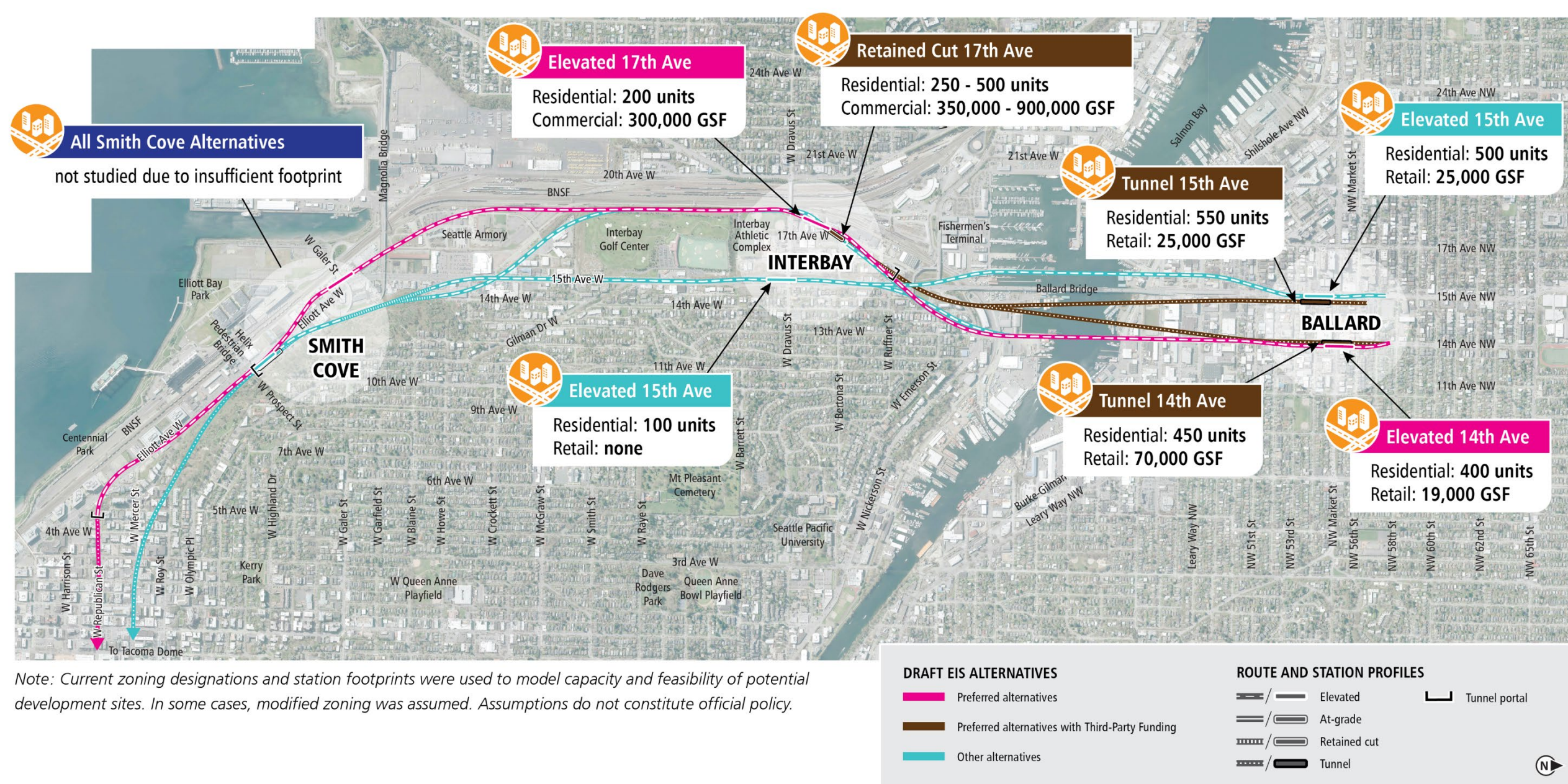
ROUTE AND STATION PROFILES

- Elevated
- At-grade
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Note: Assumes MetroConnects 2040 vision network and service designations.

*Measured by number of peak hour trips serving stops on the same block not requiring street crossings and within 100 feet of a primary station entrance.



South Interbay

Galer Street Station/ Central Interbay

Project cost
(2019\$ in billions) **\$1.3B**

Residential
displacements **174 units**

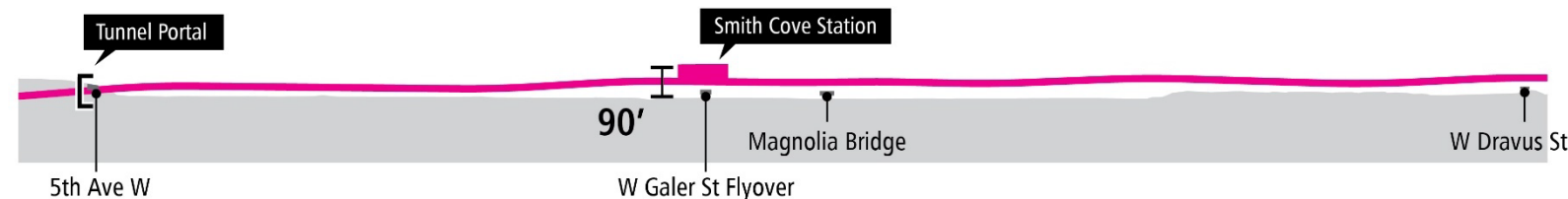
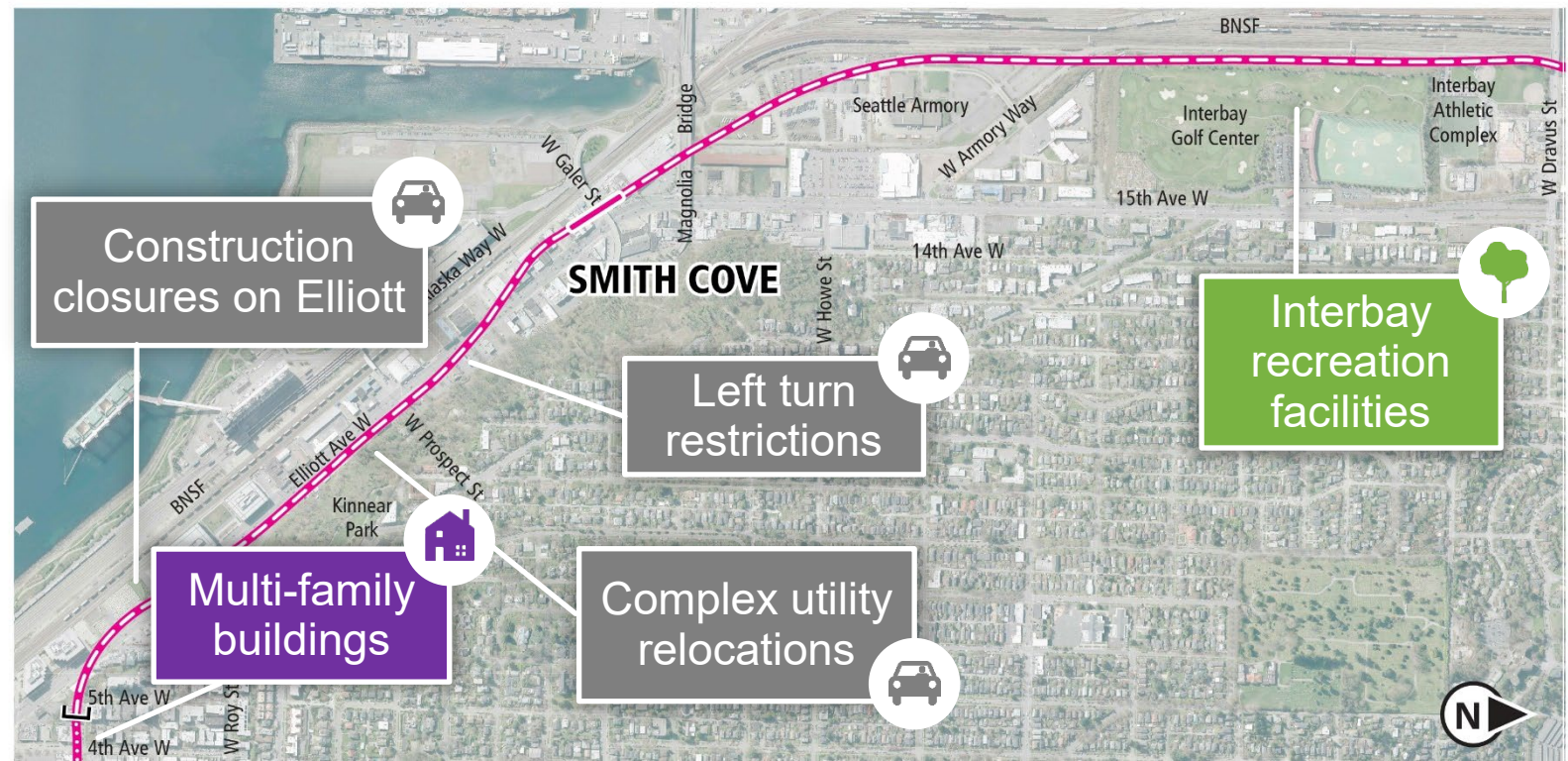
Historic property
effects **7 properties**

Park effects
(permanent) **3.1 acres**

Biodiversity effects
(permanent) **<0.1 acre**

Roadway effects
(Guideway) **0.4 mile**

Other considerations



Diagrams are not to scale and all measurements are approximate.
The above information is for illustration only. Please refer to DEIS for further detail.

Prospect Street Station/ 15th Avenue

Project cost
(2019\$ in billions) **\$1.4-1.5B**

**Residential
displacements** **123 units**

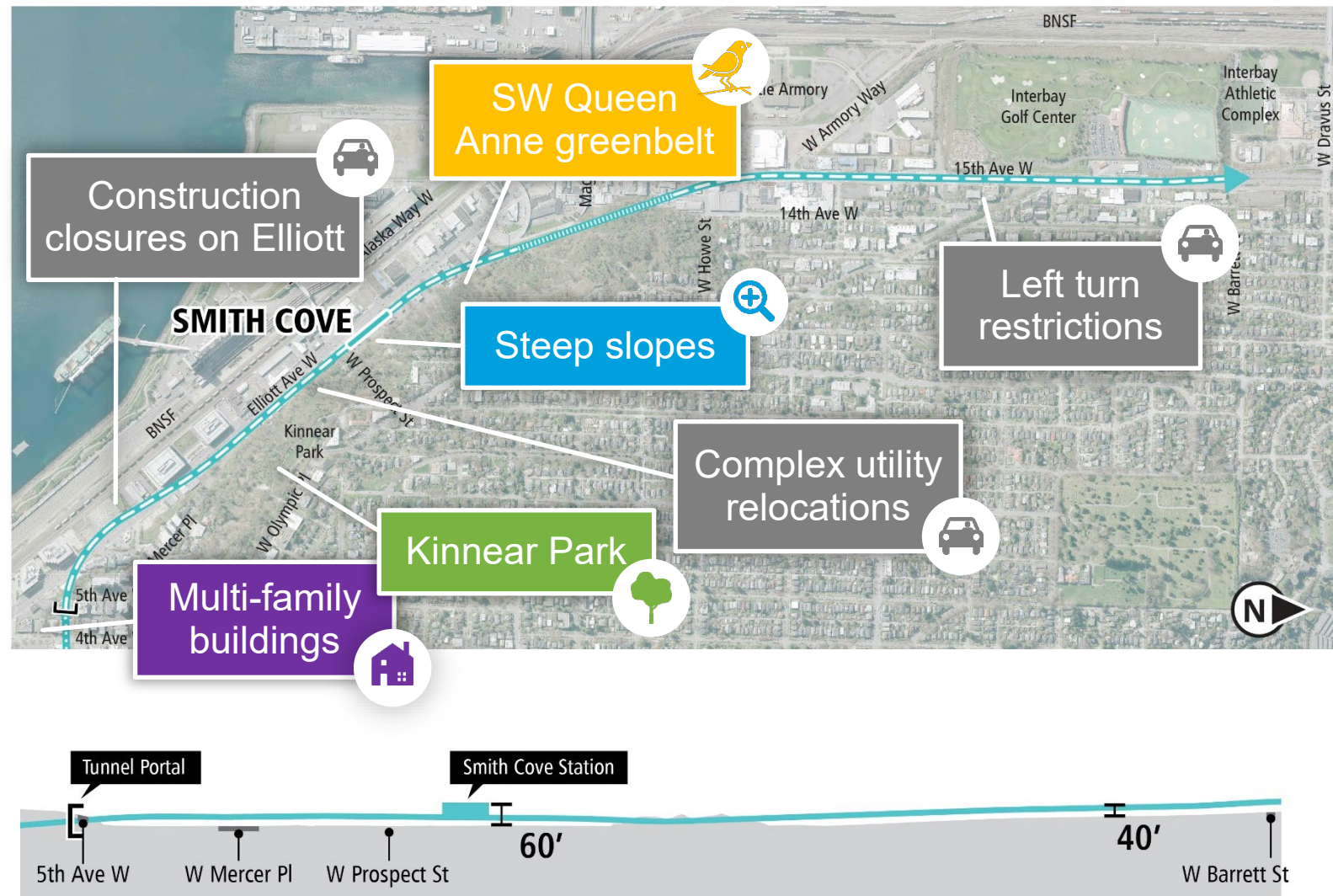
**Historic property
effects** **8 properties**

**Park effects
(permanent)** **0.7 acres**

**Biodiversity effects
(permanent)** **3.8 acres**

**Roadway effects
(Guideway)** **1.0 mile**

Other considerations



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Prospect Street Station/ Central Interbay

Project cost
(2019\$ in billions) **\$1.5-1.6B**

Residential
displacements **5 units**

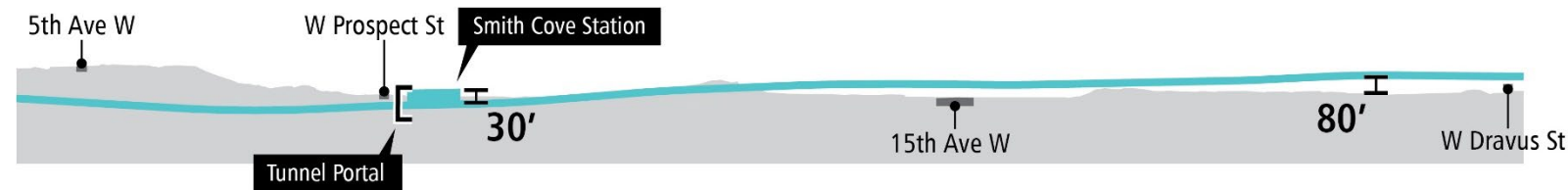
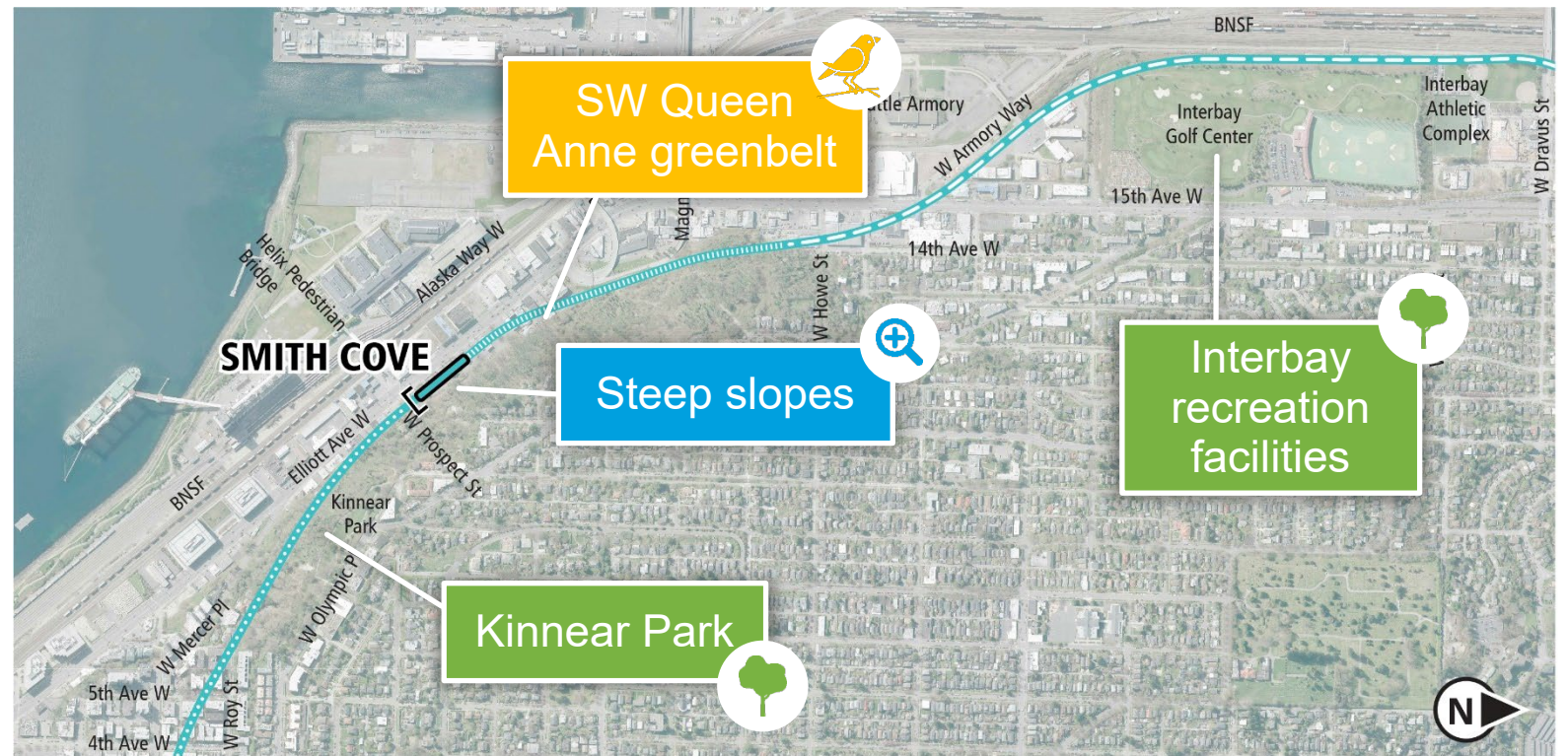
Historic property
effects **2 properties**

Park effects
(permanent) **4.0 acres**

Biodiversity effects
(permanent) **5.5 acres**

Roadway effects
(Guideway) **0.1 mile**

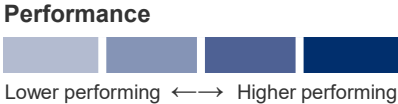
Other considerations



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	Galer Street Station/ Central Interbay	Prospect Street Station/ 15th Avenue	Prospect Street Station/ Central Interbay
Project cost  (2019\$ in billions)	\$1.3B	\$1.4-1.5B	\$1.5-1.6B
Residential displacements 	174 units	123 units	5 units
Historic properties effects 	7	8	2
Park effects  (Permanent)	3.1 acres	0.7 acres	4.0 acres
Biodiversity effects  (Permanent)	<0.1 acre	3.8 acres	5.5 acres
Roadway effects  (Guideway)	0.4 mile	1.0 mile	0.1 mile
Other considerations 	Queen Anne hillside steep slopes		Queen Anne hillside steep slopes

The above information is for illustration only. Please refer to DEIS for further detail.



South Interbay

Interbay/Ballard

Elevated 14th Avenue

Project cost
(2019\$ in billions) **\$1.5-1.6B**

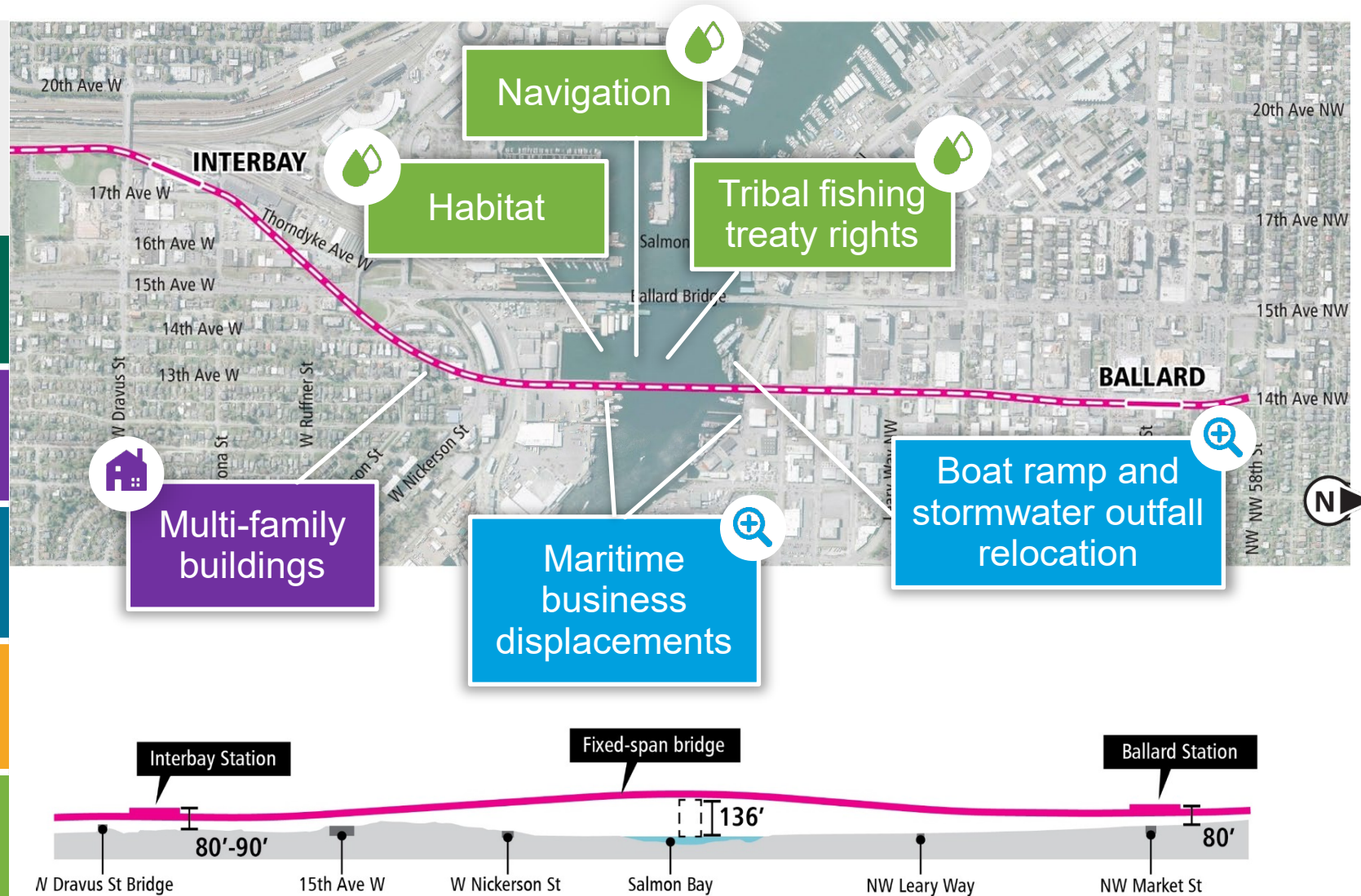
Residential displacements **105 units**

Historic property effects **7 properties**

Employee displacements **610**

In-water effects (Permanent) **1.2 acre**

Other considerations



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The above information is for illustration only. Please refer to DEIS for further detail.*

Tunnel 14th Avenue

Project cost
(2019\$ in billions) **\$1.5B**

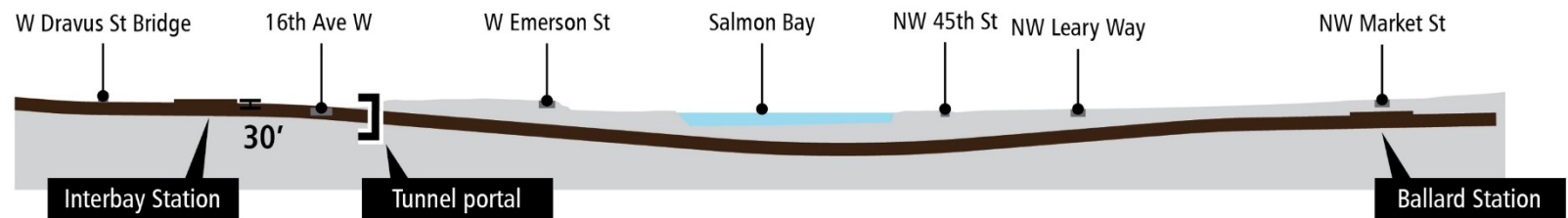
Residential
displacements **14 units**

Historic property
effects **4 properties**

Employee
displacements **380**

In-water effects
(Permanent) **none**

Other considerations 



Diagrams are not to scale and all measurements are approximate.
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Tunnel 15th Avenue Station Option

Project cost
(2019\$ in billions) **\$1.7B**

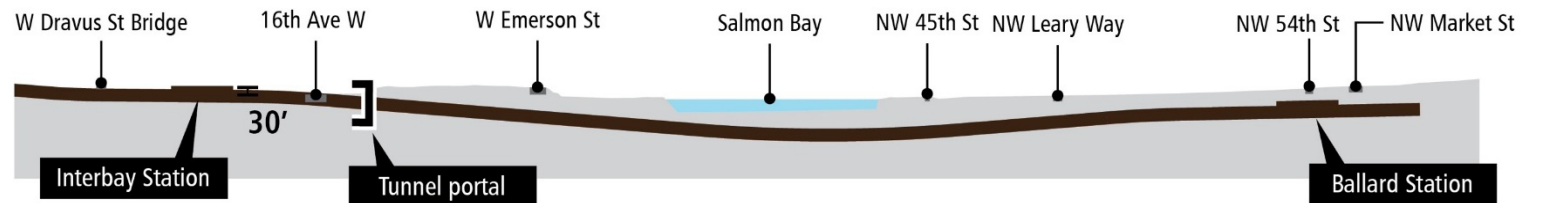
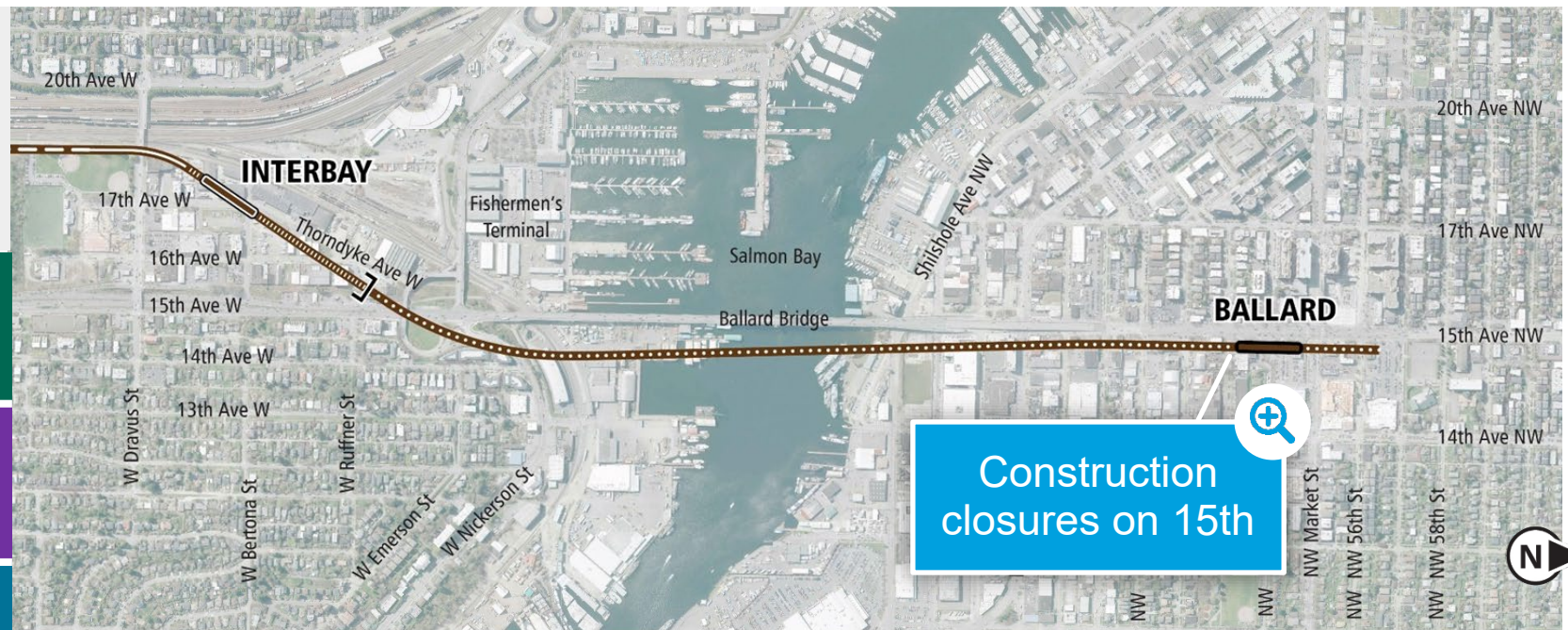
Residential
displacements **21 units**

Historic property
effects **3 properties**

Employee
displacements **370**

In-water effects
(Permanent) **none**

Other considerations 



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Elevated 14th Avenue Option (from Prospect/15th)

Project cost
(2019\$ in billions) **\$1.6B**

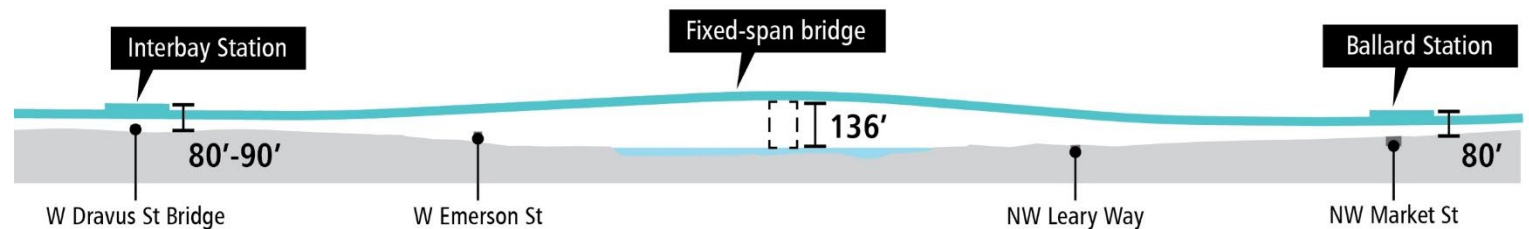
Residential displacements **151 units**

Historic property effects **7 properties**

Employee displacements **400**

In-water effects (Permanent) **1.2 acre**

Other considerations



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Elevated 15th Avenue

Project cost
(2019\$ in billions) **\$1.5B**

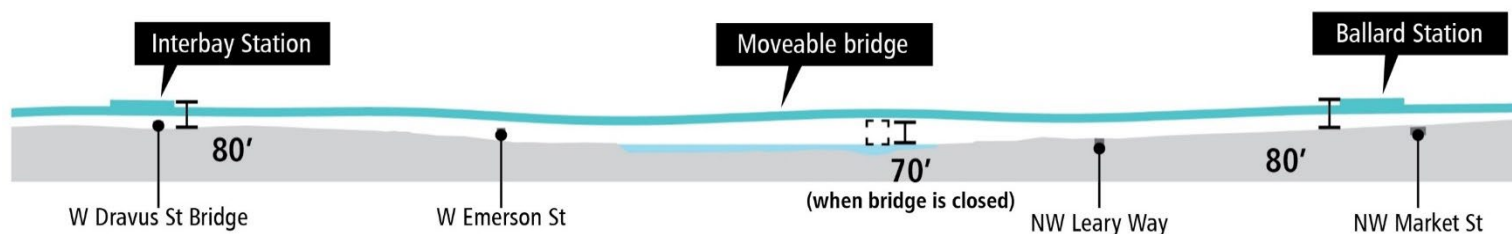
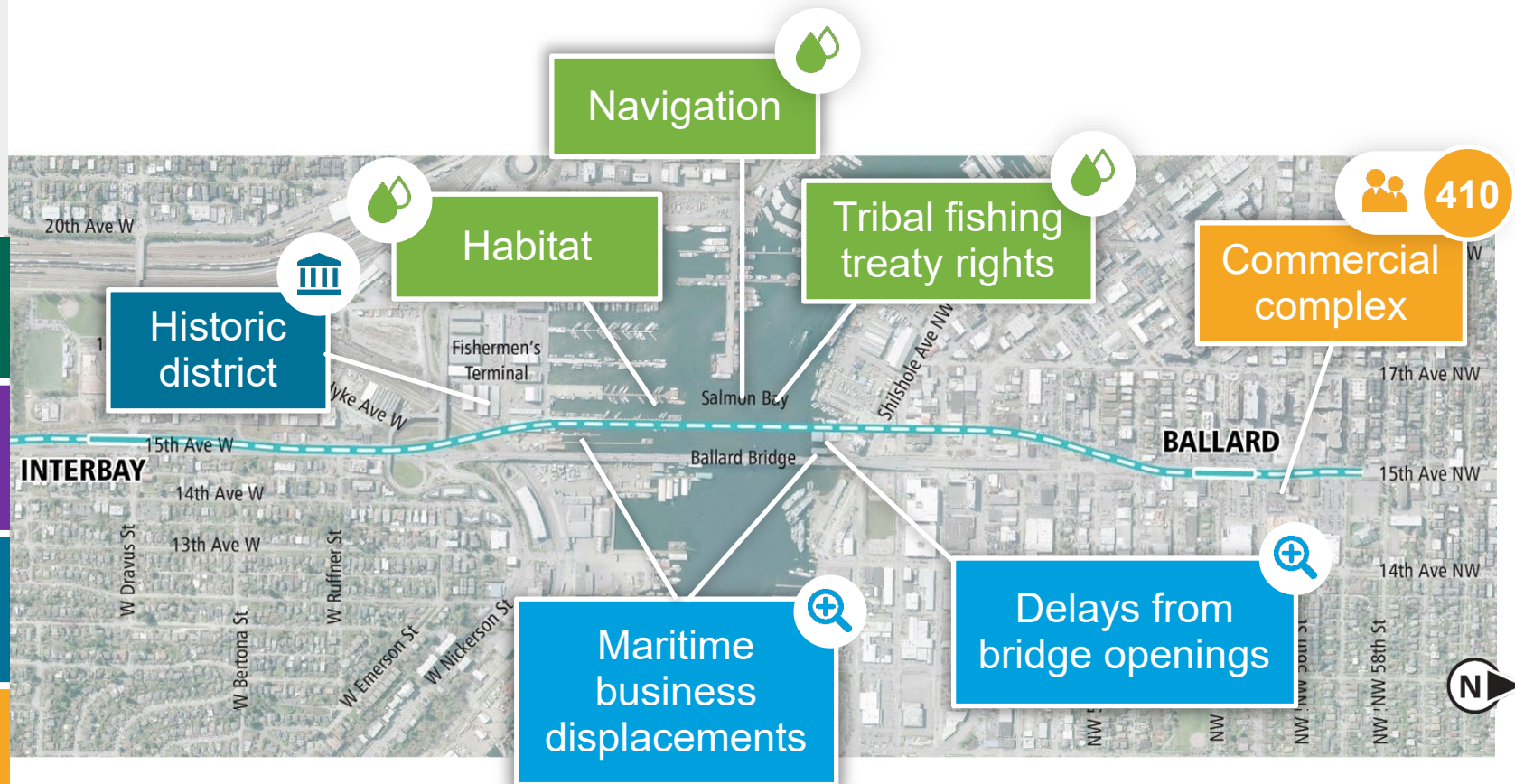
Residential
displacements **25 units**

Historic property
effects **10 properties**







Employee
displacements **620**

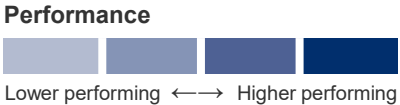
In-water effects
(Permanent) **0.8 acre**

Other considerations



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	Elevated 14th Avenue	Tunnel 14th Avenue	Tunnel 15th Avenue Option	Elevated 14th Avenue Option (from Prospect/15th)	Elevated 15th Avenue
Project cost (2019\$ in billions) 	\$1.5-1.6B	\$1.5B	\$1.7B	\$1.6B	\$1.5B
Residential displacements 	105 units	14 units	21 units	151 units	25 units
Historic property effects 	7	4	3	7	10
Employee displacements 	610	380	370	400	620
In-water effects (Permanent) 	1.2 acre	none	none	1.2 acre	0.8 acre
Other considerations 	Maritime business displacements		Construction closures on 15th	Maritime business displacements	Maritime business displacements
	Boat ramp and stormwater outfall relocation			Boat ramp and stormwater outfall relocation	Delays from bridge opening
<div>The above information is for illustration only. Please refer to DEIS for further detail.</div> <div><div>Performance</div><div><div></div><div></div><div></div><div></div></div></div>					



Agenda

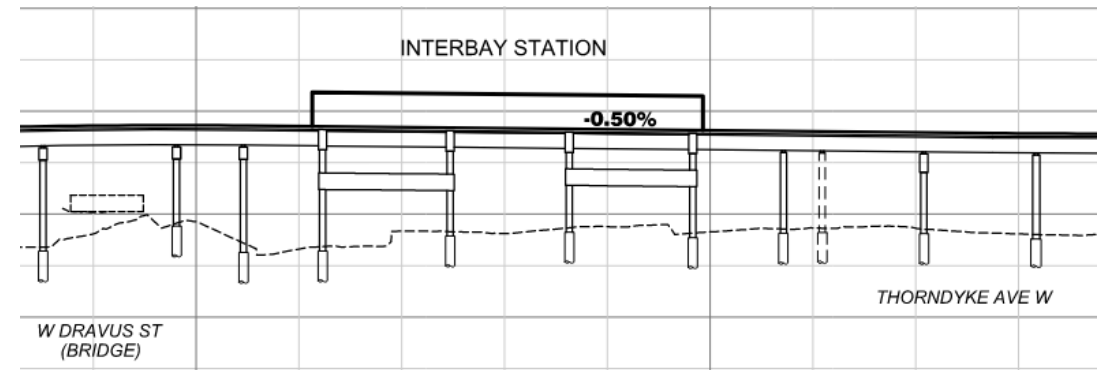
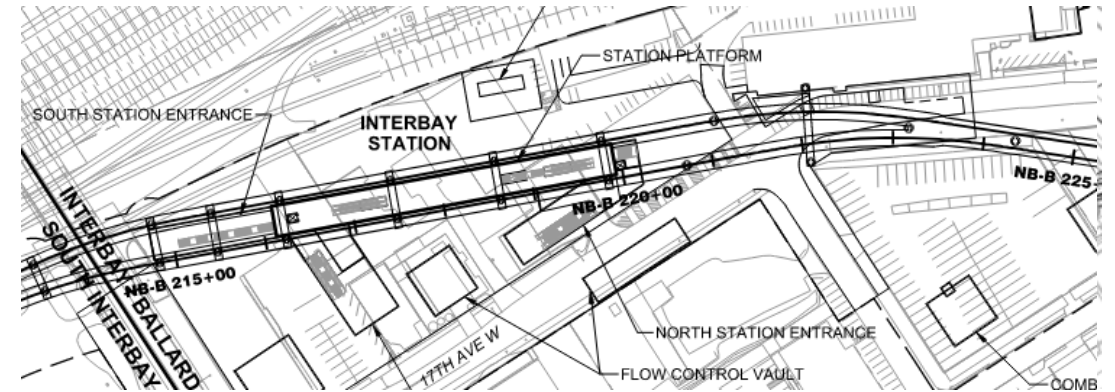
- Welcome and introductions
- Project orientation
- ***Guideway structures and bridges***
- Smith Cove Station
- Ballard Station

***Interbay Station to Ballard Station
Elevated Guideway Concept Design (DEIS)***

Environment for Guideway Type Development



W Dravus St / 17th Ave W



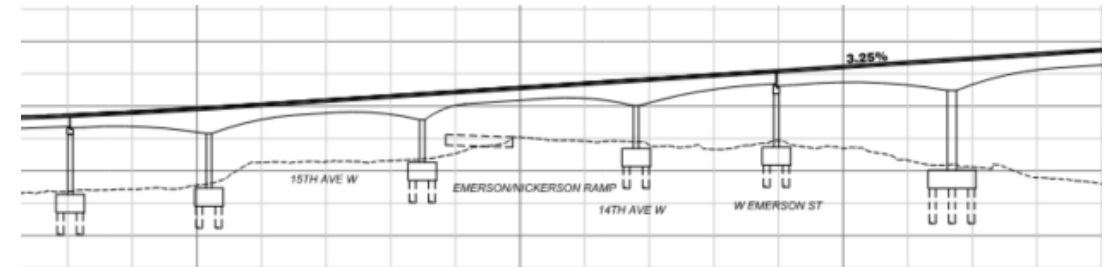
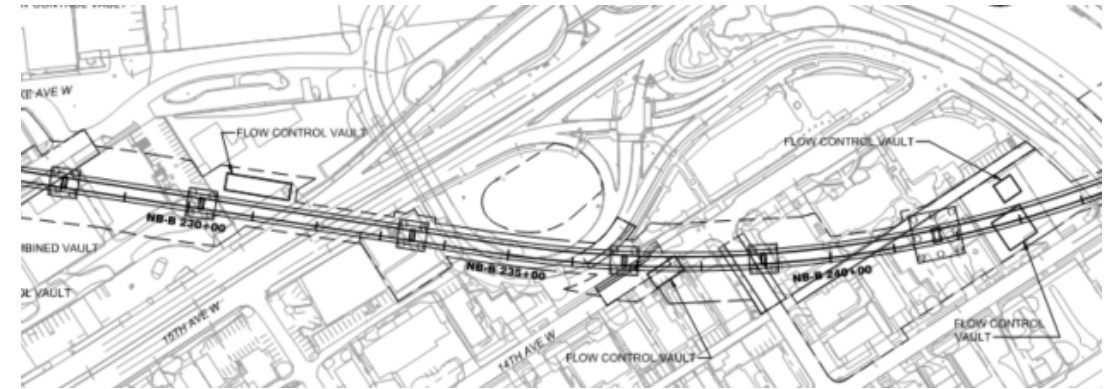
Interbay Station on IBB-1a Alignment

Transitioning to Neighborhood

Environment for Guideway Type Development



Emmerson/Nickerson Ramp



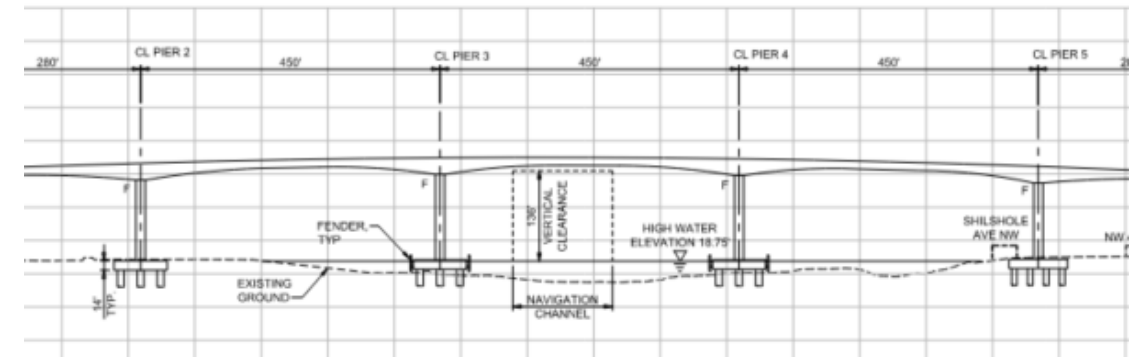
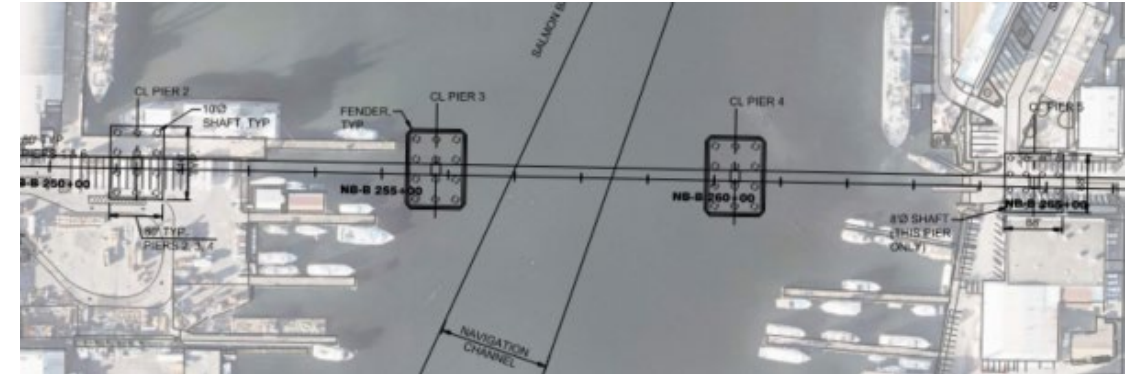
Guideway on IBB-1a Alignment

Major Urban Infrastructure

Environment for Guideway Type Development



Ballard Bascule Bridge



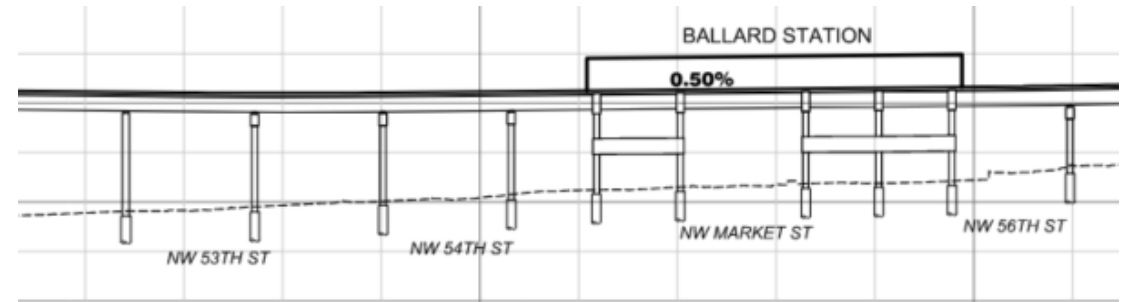
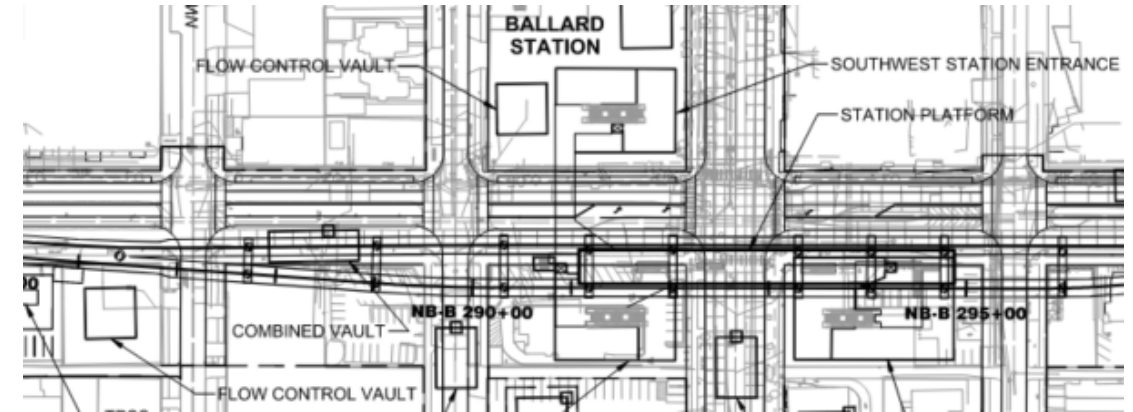
Segmental Box Bridge Option on IBB-1a Alignment

Maritime and Navigation

Environment for Guideway Type Development



14th Avenue NW near NW Market St, Ballard



Ballard Station on IBB-1a Alignment

Neighborhood

Guideway Design Fundamentals

- ***Environment:*** Minimize the effects due to column placement including infrastructure, major utilities, and ECAs
- ***Structural:*** Keep bridge span lengths within optimal range for structural performance, and bridge types within current ST bridge type inventory when possible.
- ***Geo-technical:*** Use appropriate foundation type and ground improvement methods to minimize environmental effects
- ***Rail-Structure:*** Minimize special track work requirements due to longer and lighter bridge spans

Guideway Structure Summary

Interbay Station to Ballard Station Segment (14th Ave Alignments)

Structural Type	Length (ft)	% of Total Length
Typical Dual Tracks	2,410	22%
Typical Twin Single Track	1,435	13%
Transition (Dual to Twin Single)	510	5%
Stations (Center platform configuration)	760	7%
Long and Very Long Spans	5,712	53%
Total Length	10,827	100%

- Majority of the guideways require long-spans or very long-span bridge structures.
- Long spans and very long spans are typically segmental box girders with balanced cantilever construction.
- Water crossing bridge types vary.

Guideway – Length, Depth, and Width

	Span Length (ft)	Depth at Mid-Span (ft)	Depth at Pier (ft)	Width (ft)
Typical Span	90 ft – 200 ft	8 ft – 10 ft	8 ft – 10 ft	30 ft or more
Long Span	200 ft - 400 ft	5 ft – 10 ft	11 ft – 22 ft	30 ft or more
Very Long Span	400 ft – 700 ft	10 ft – 18 ft	22 ft – 38 ft	30 ft or more
Water Crossing	> 700 ft	varies	varies	30 ft or more

- Typical spans can be prestressed I girder, prestressed tub girder, or segmental box girder.
- Long spans and very long spans are segmental box girders with balanced cantilever construction.
- Water crossing bridge types vary.

Guideway Structural Height

Neighborhood	Approximate Structural Height at Rail
Interbay Station to Nickerson	40 ft - 140 ft
Salmon Bay Water Crossing	140 ft - 155 ft
Leary to Ballard Station	90 ft - 50 ft

Guideway Dual Track (Typical Span Length)



Prestressed I Girders



**Prestressed Tub
Girders**



**Post-Tensioned Box
Girders**

Guideway Twin Single Track (Typical Span Length)



Prestressed I Girders



Prestressed Tub Girders

Guideway Dual Track (Long Span Length)



Balanced Cantilever Segmental Box Girder



Balanced Cantilever Segmental Box Girder

Salmon Bay Crossing
Bridge type preliminary concepts

Very Long Span Bridge Type



Segmental Box Girder bridge concept

Very Long Span Bridge Type



Steel Network Arch bridge preliminary concept

Very Long Span Bridge Type



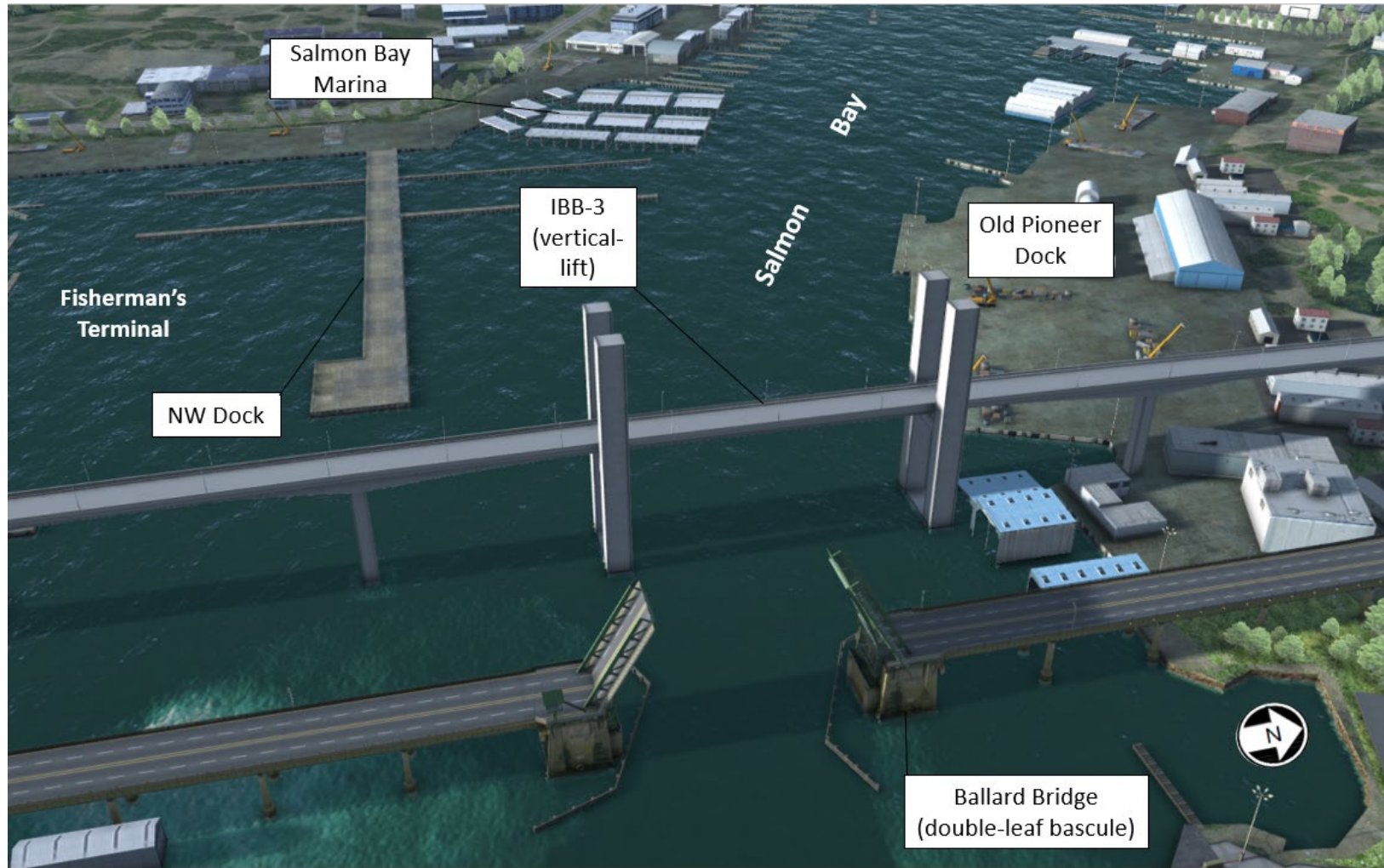
Extradosed bridge preliminary concept

Very Long Span Bridge Type



Cable Stayed bridge preliminary concept

Movable Long Span Bridge



Vertical Lift Bridge preliminary concept

Structural design feature concepts

Possible Structural Design Features

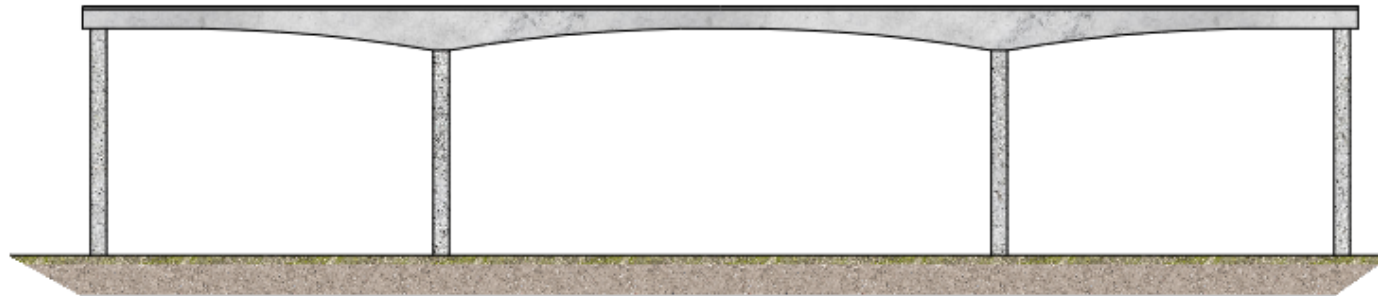
- ***Span length, structural types, and use of materials***
- ***Column proportion***
 - single mast vs. double-blade
 - plumb vs. tapering
 - single column vs. straddle bents
 - diamond-shape, H-shape, or A-shape towers
- ***Girder proportion***
 - constant depth vs. haunched depth
 - cords vs. smooth curve for horizontal curvature
 - above-deck vs. under deck for truss and arch bridges
 - single cable plane vs. double cable planes for extradosed or cable stayed bridges

Span Length, Structural Types, and Use of Materials



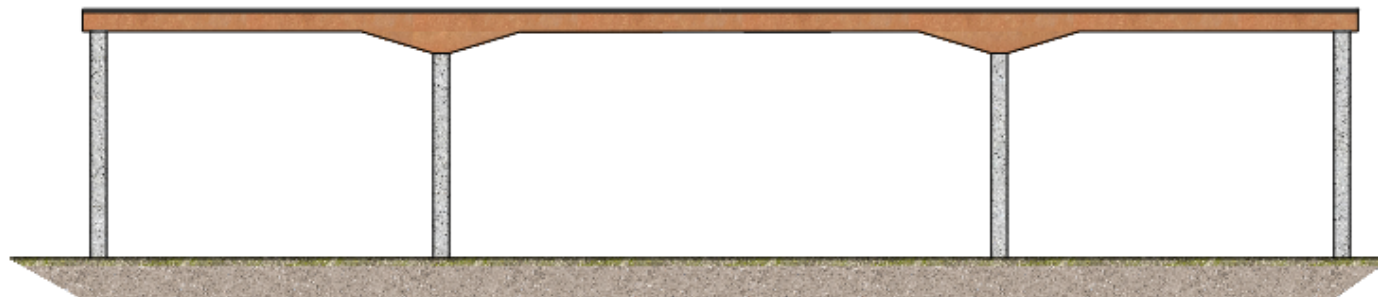
Short-Span Concrete Girder Guideways

Constant Girder Depth



Long-Span Concrete Segmental Box Girder Guideways

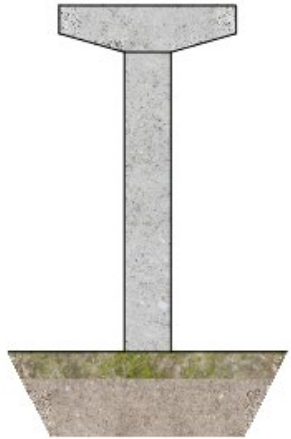
Haunched Girder



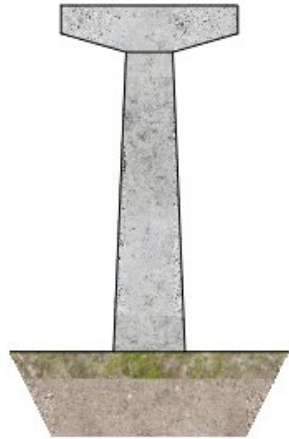
Long-Span Steel Girder Guideways

*Haunched Girder
Depth*

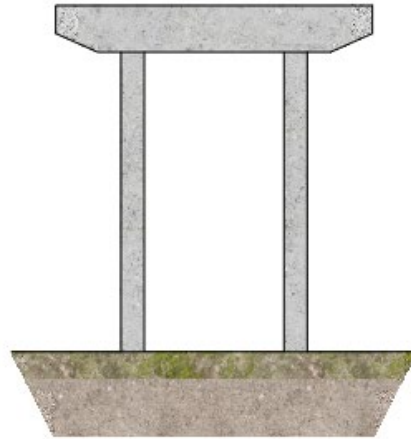
Column Proportions



Plumb

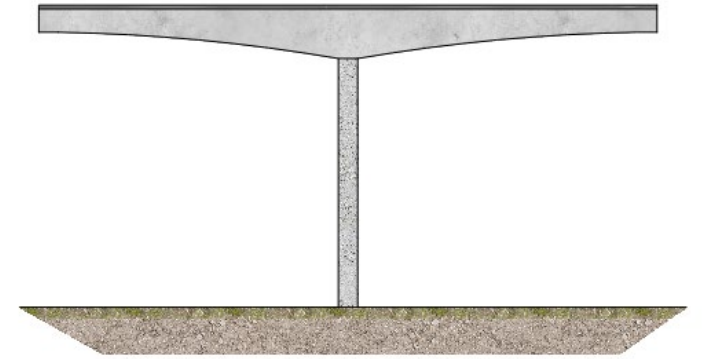


Tapered

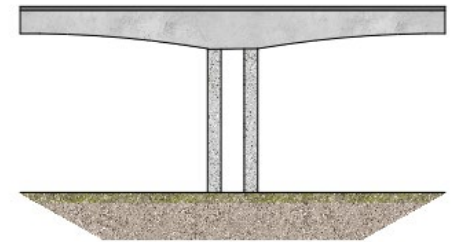


Straddled

PIER ELEVATIONS



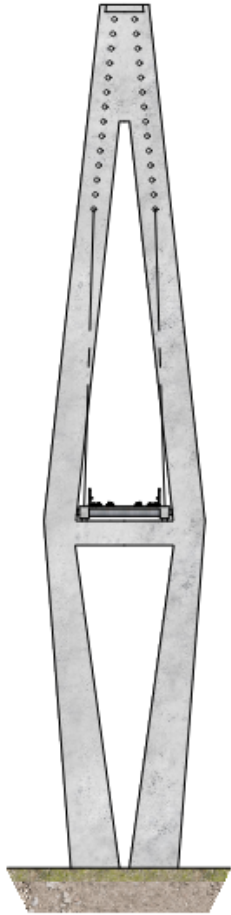
Single Mast



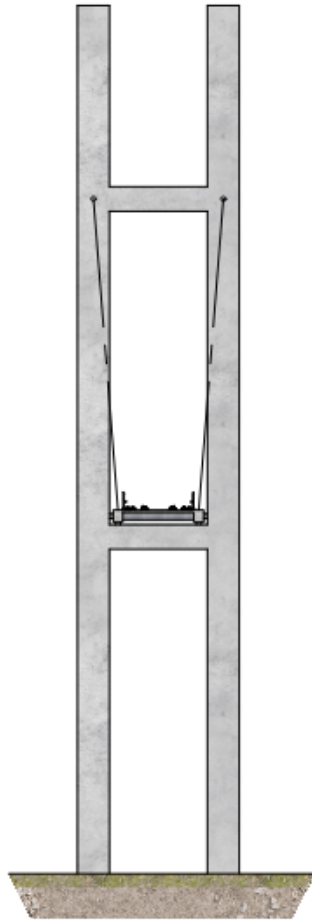
Double Blades

PIER SIDE-VIEW

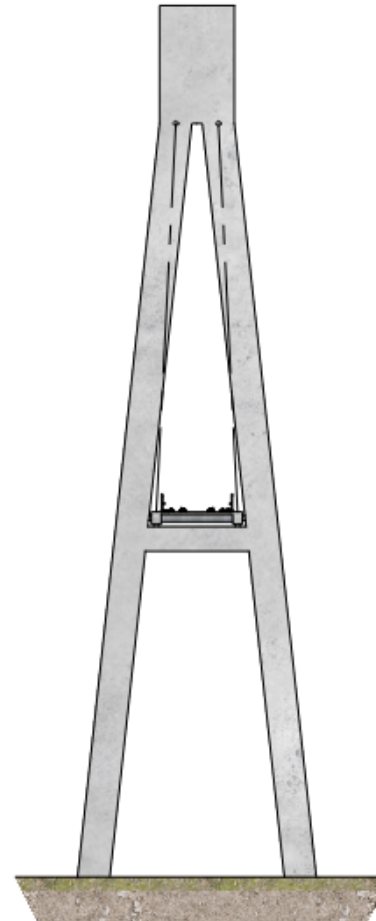
Some Pylon Options for Cable-Stayed and Extradosed Bridges



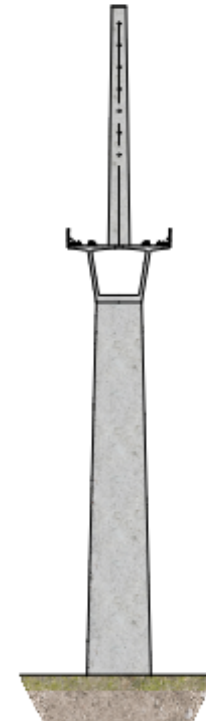
Diamond Tower
Two Cable Planes



H Tower
Two Cable Planes



A Tower
Two Cable Planes



Single Mast Tower
One Cable Plane

Corded and Smooth Girders for Horizontal Curve



Corded Girders



Smooth Girders

Materials – Steel Girders



Portland TriMet



Chicago Transit

Sound Transit currently has no steel plate or steel box girder bridge in its system

Architectural Details

Aesthetic Enhancement – Color Shape Rustication



Color and Rustication



Shape and Rustication

Aesthetic Enhancement – Decorative Railing



Decorative Sound Walls/Screening



Decorative Railing

Possible Aesthetic Enhancement - Artwork



Decorative Painting



Decorative Attachment

Constructability



Drilled Shaft Installation



Ground Improvement (Jet Grouting)

Poor Soil Conditions

Agenda

- Welcome and introductions
- Project orientation
- Guideway structures and bridges
- ***Smith Cove Station***
- Ballard Station



DRAFT EIS ALTERNATIVES

- Preferred alternative
- Preferred alternatives with Third-Party Funding
- Other alternatives and design options

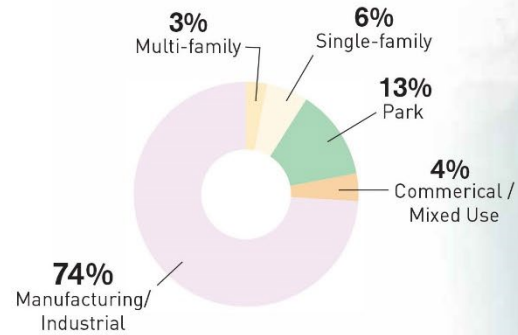
ROUTE AND STATION PROFILES

- / — Elevated
- / --- At-grade
- ... / ... Tunnel
- ┌ Tunnel portal



- ***Enhance mobility and access for communities of color and low-income populations***
- ***Create opportunities for equitable development that includes expanding housing and community assets for communities of color***
- ***Create a sense of belonging for communities of color at all stations***

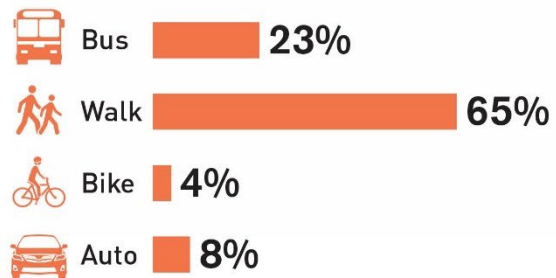
Existing land use in the station area ⁽¹⁾



Ridership/daily boardings ⁽²⁾



How people will travel to the station ⁽²⁾



Bike facilities within ⁽³⁾ 10-minute bikeshed



Living and working in ^(1,4) the station area 2040



What we heard so far

NEIGHBORHOOD FEEDBACK

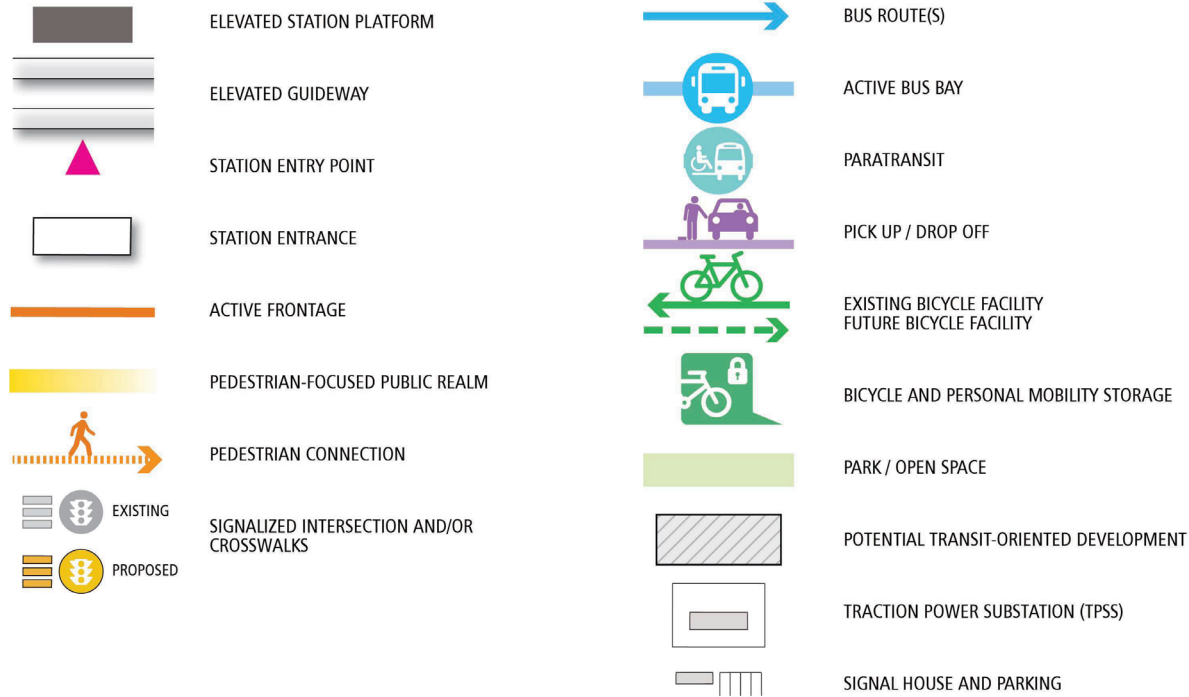
- 1 Integrate station with the surrounding neighborhood and with multiple modes of transportation
- 2 Interest in additional development/attractions in the neighborhood
- 3 Improve connections to and between public spaces and to the waterfront
- 4 Improve and enhance Elliott Bay Trail connection
- 5 Enhance connection to/from Queen Anne neighborhood
- 6 Design new Magnolia Bridge to support connectivity and development
- 7 Provide safe and convenient connection across Elliott Ave and BNSF tracks
- 8 Capitalize on publicly-owned land to draw more jobs and people to the area



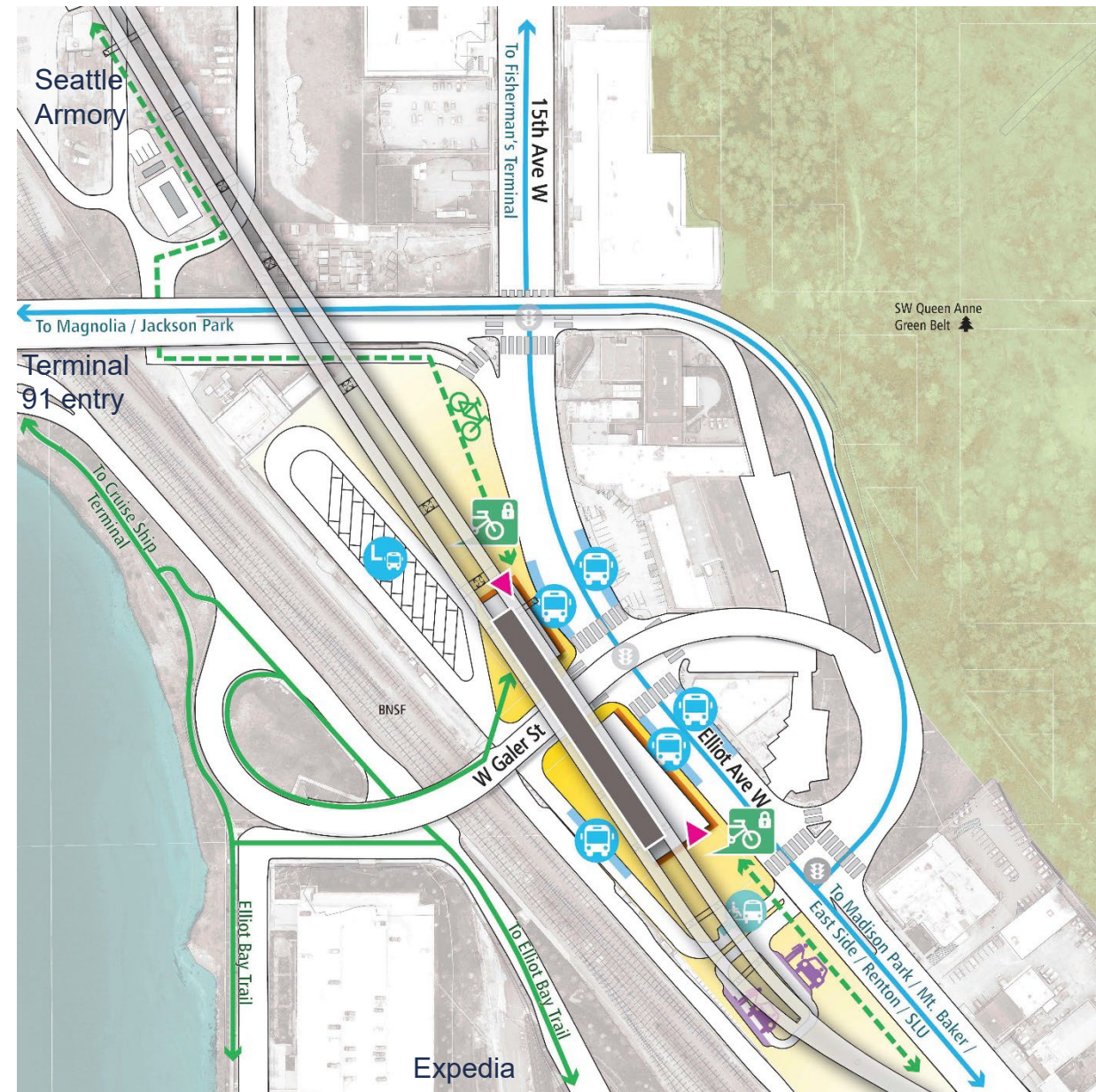
Note: Neighborhood feedback gathered from in-person and on-line events during alternatives development 2018-2019.

Preferred alternative

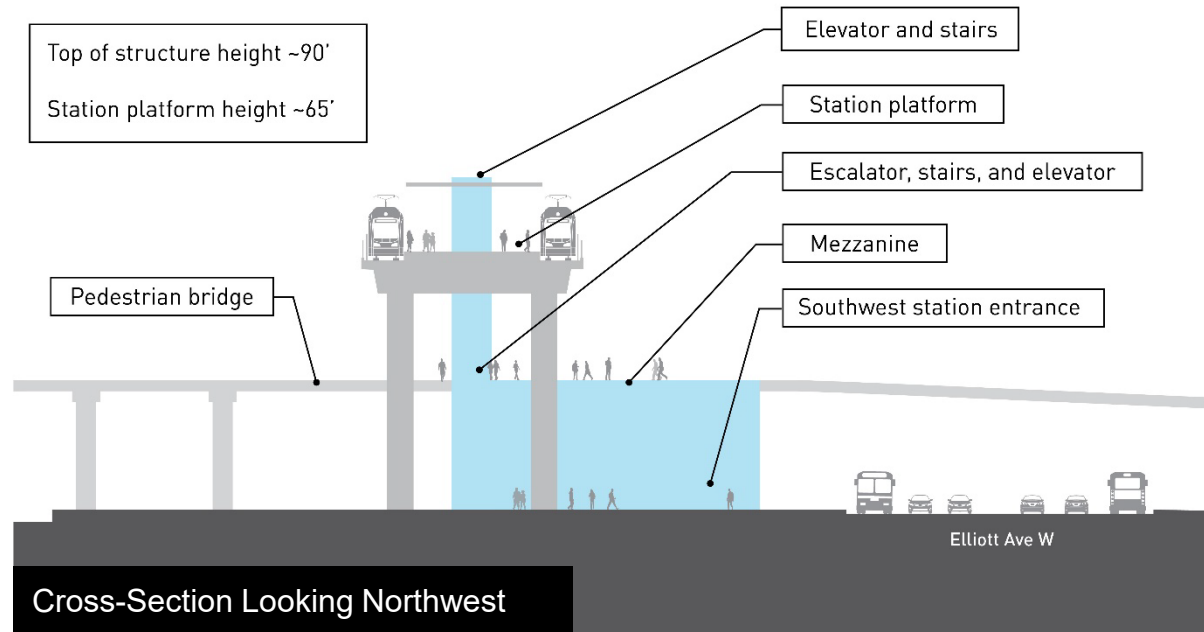
Elevated Galer St Station



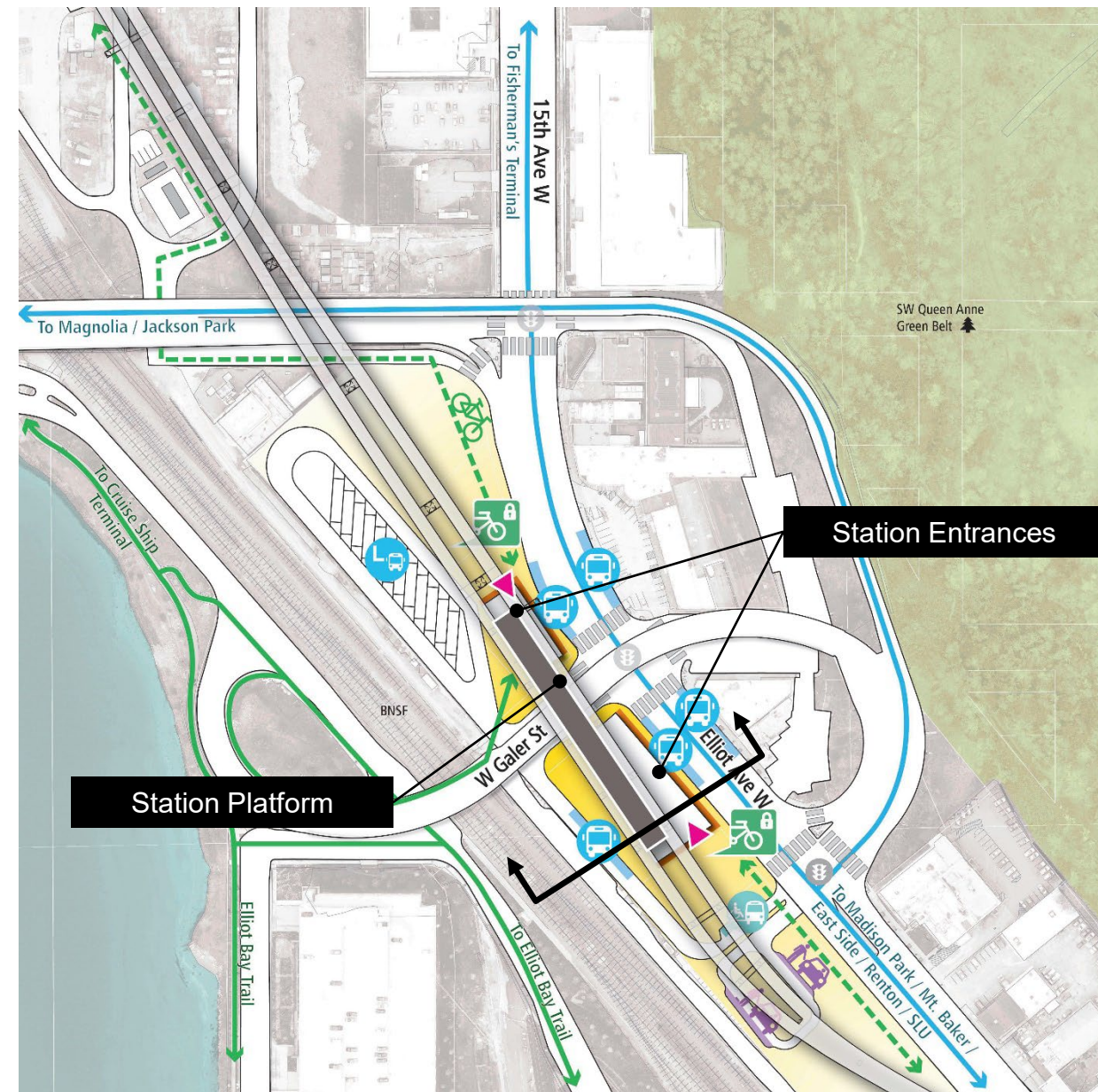
Conceptual design subject to change. All measurements are approximate. Diagrams for illustration only.



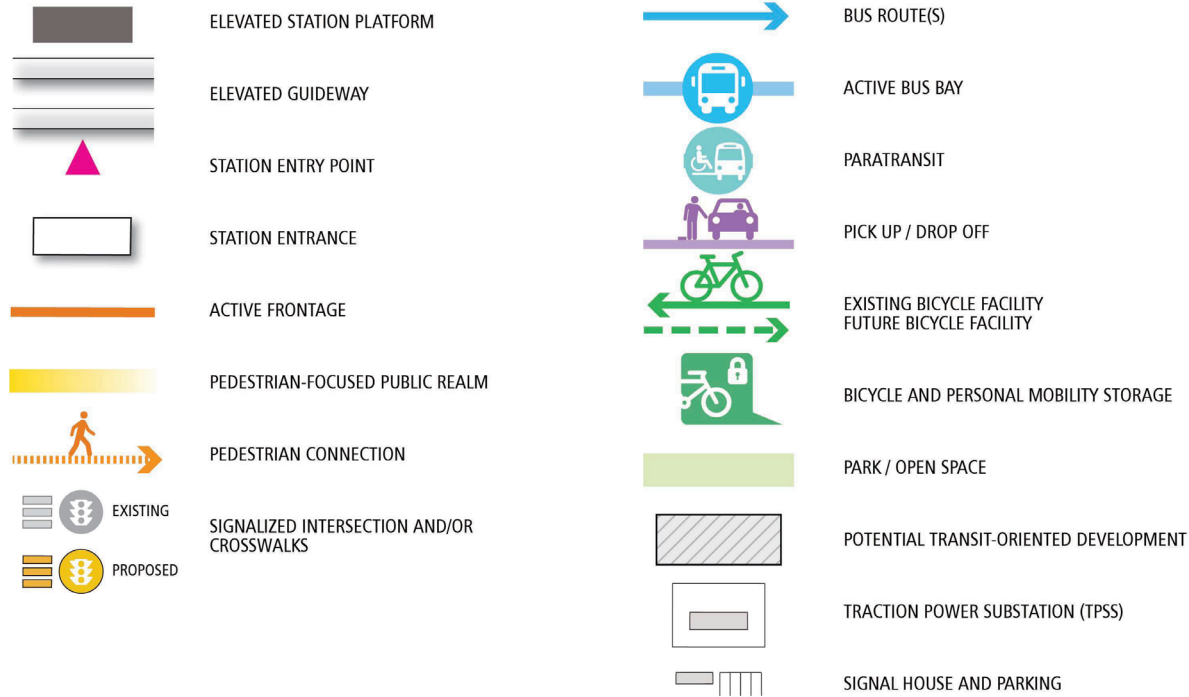
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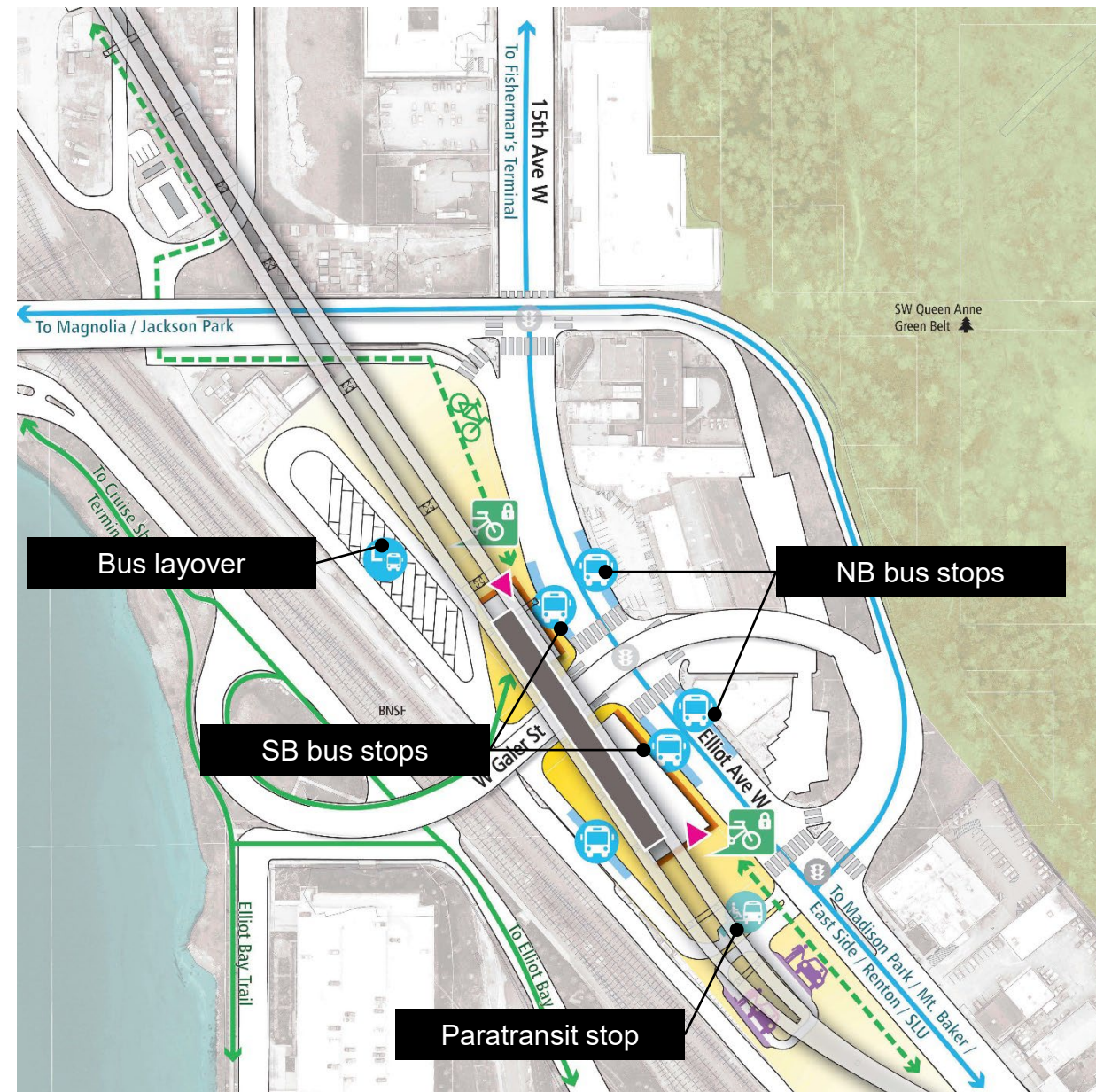
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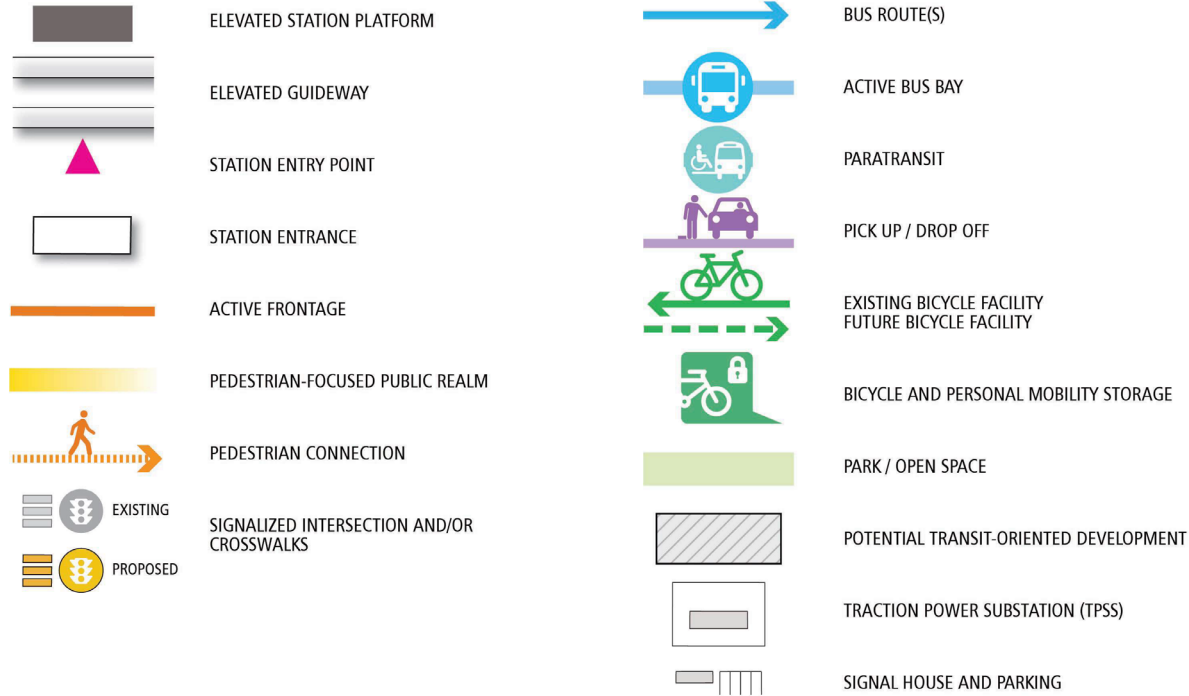
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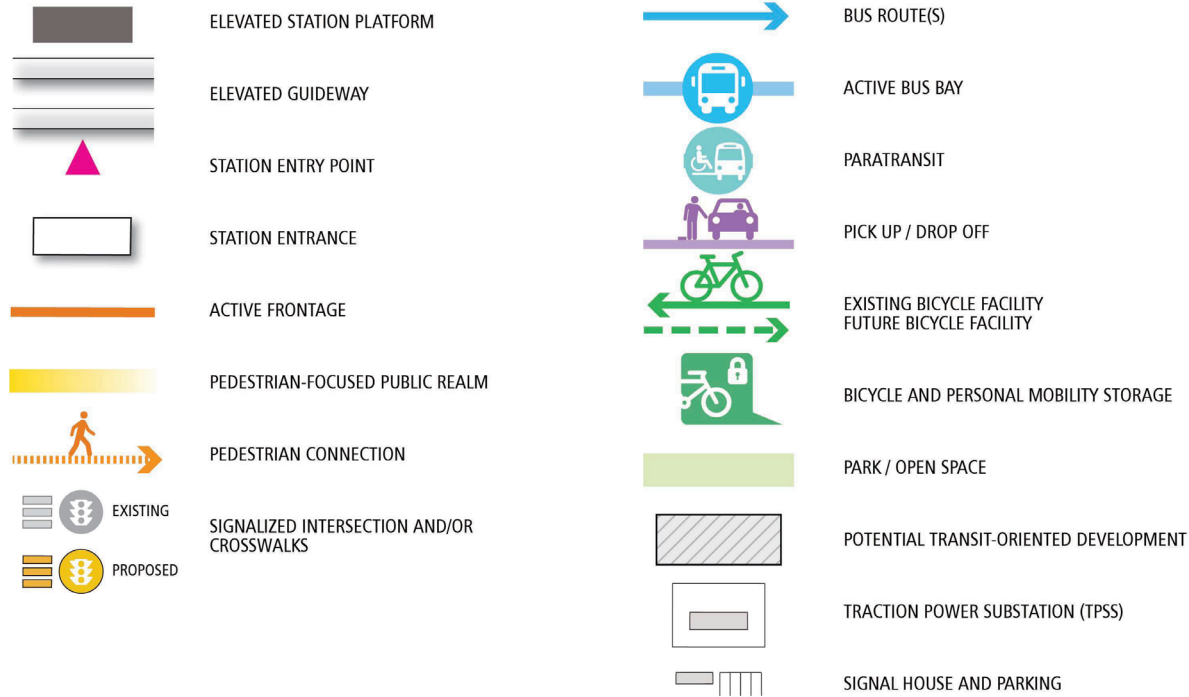
Elevated Galer St Station



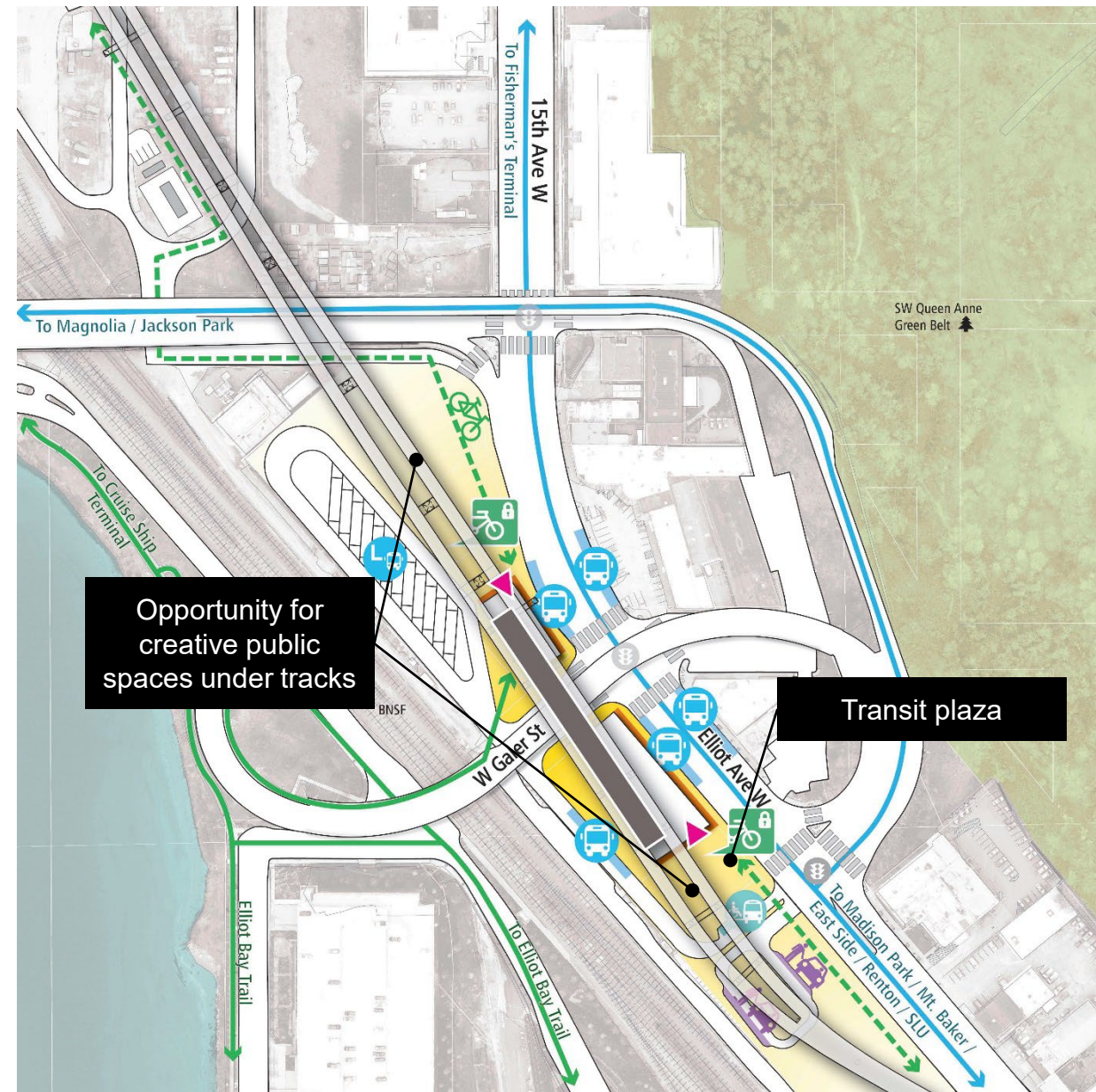
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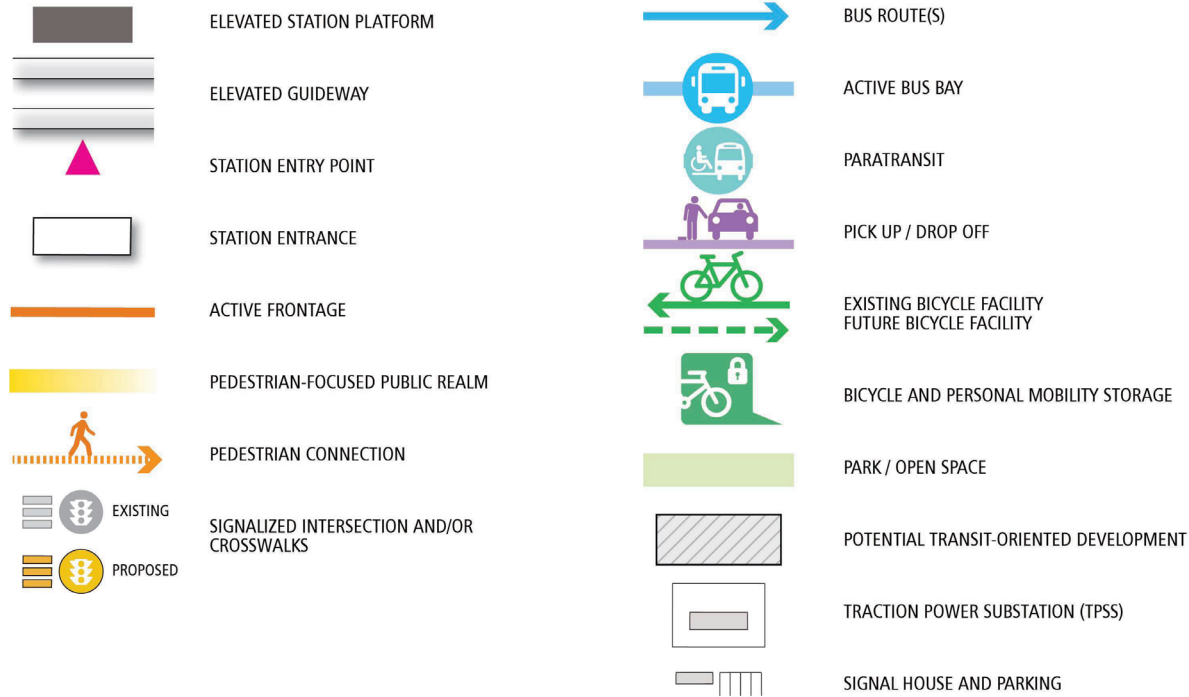


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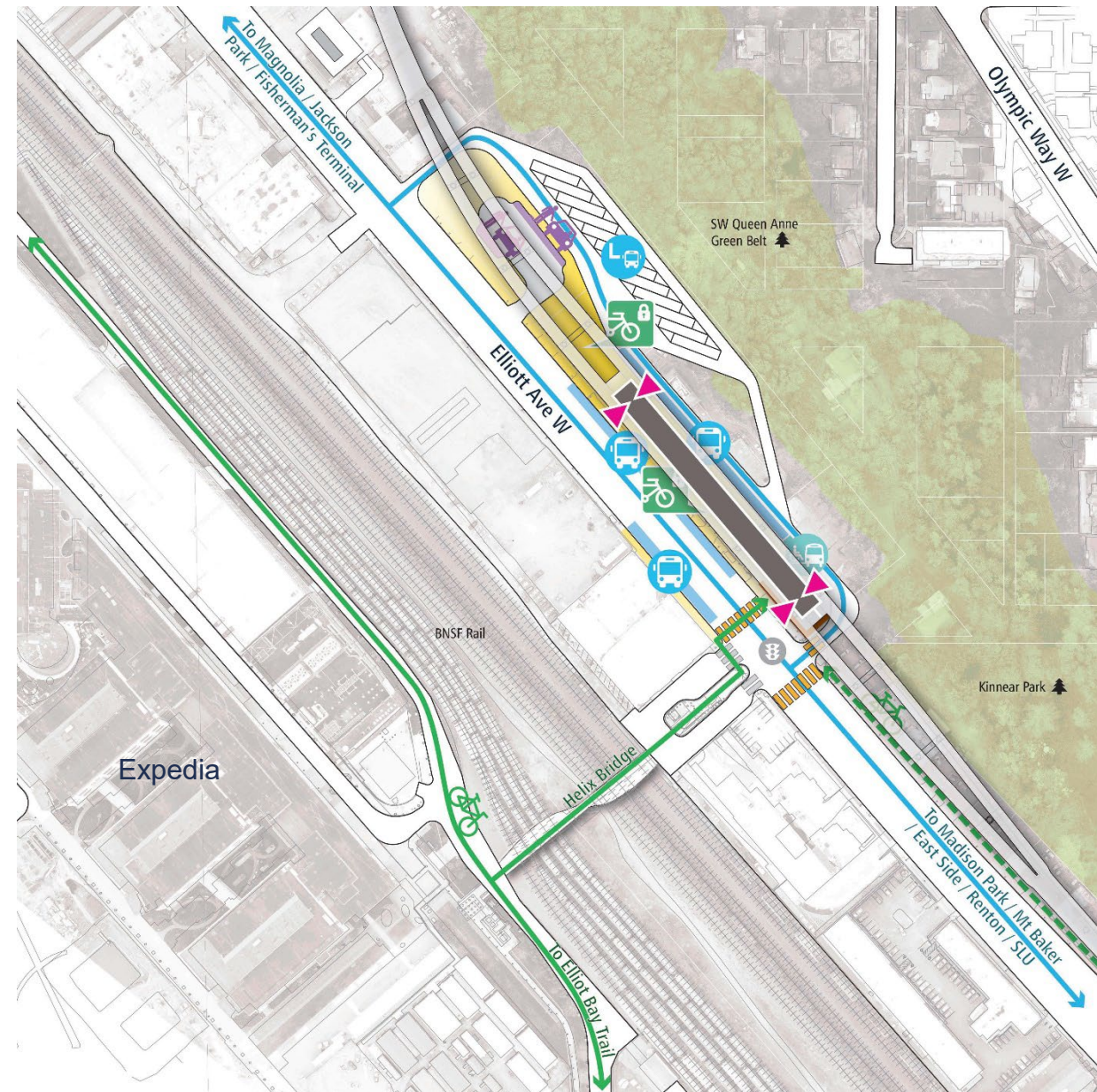


Other Draft EIS alternatives

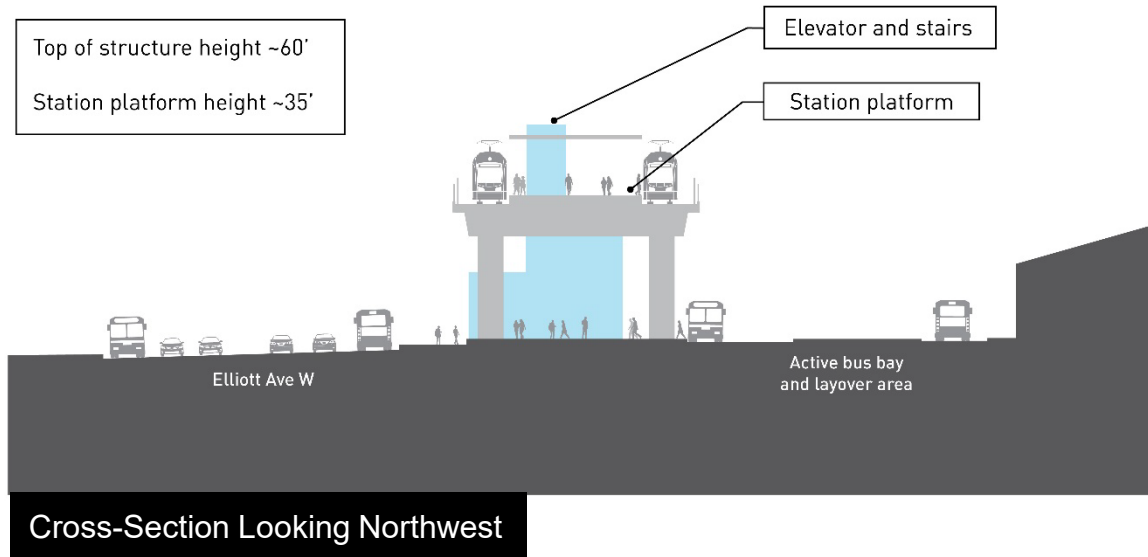
Elevated Prospect St Station



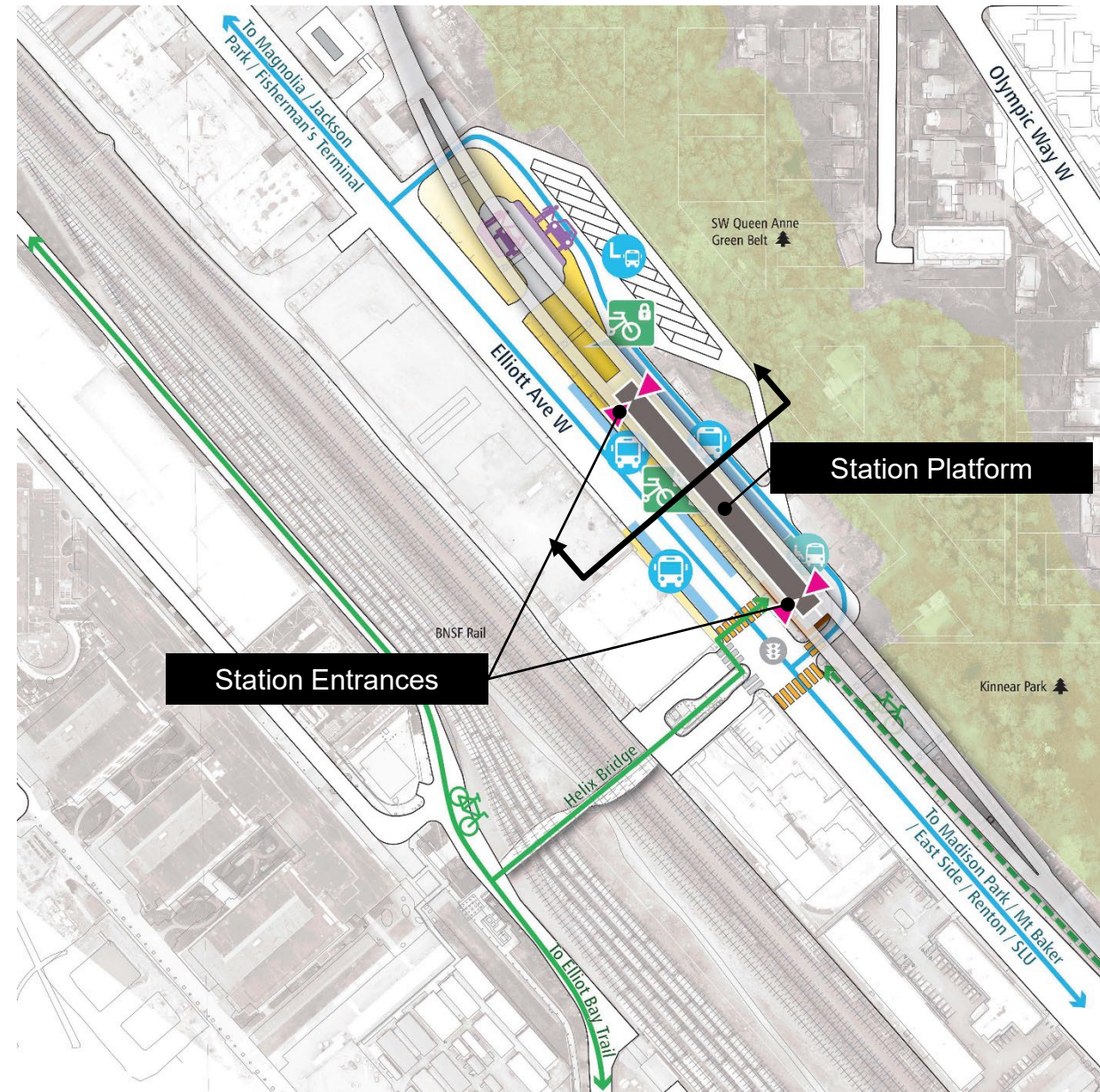
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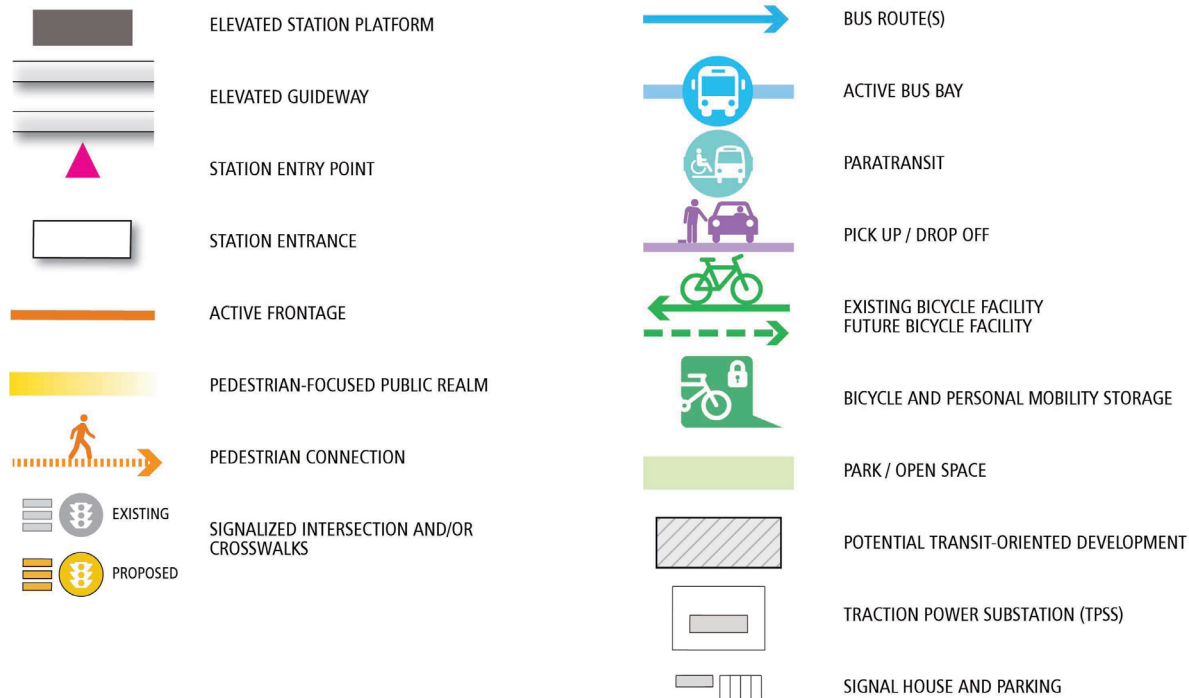
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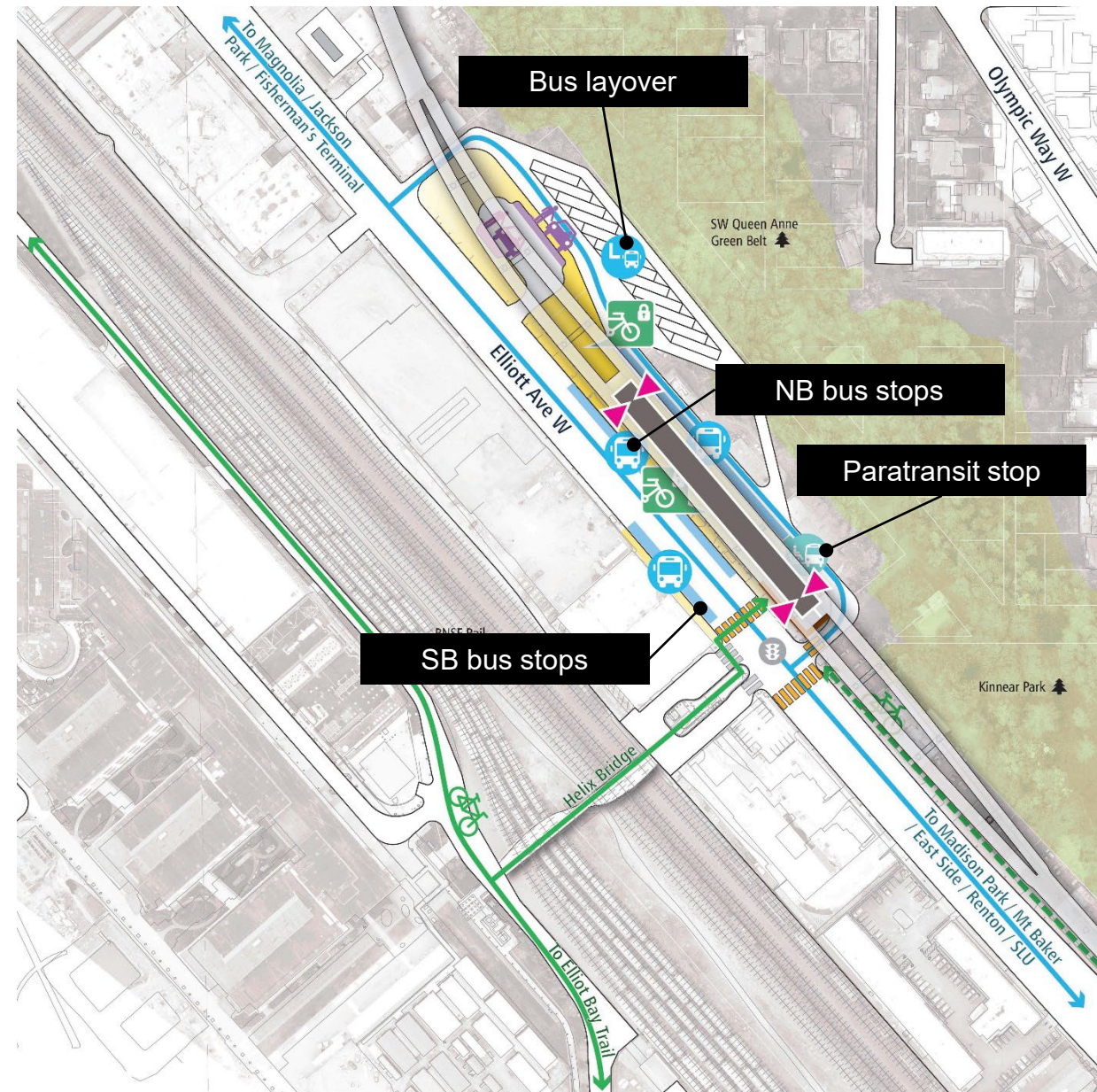
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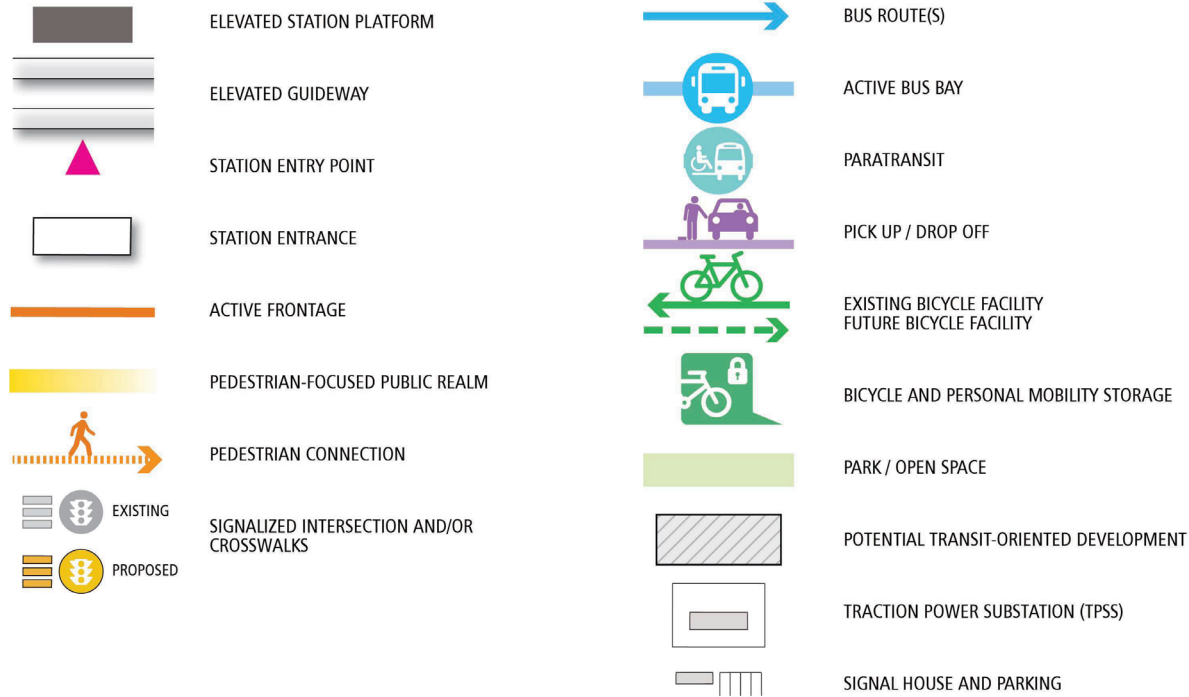
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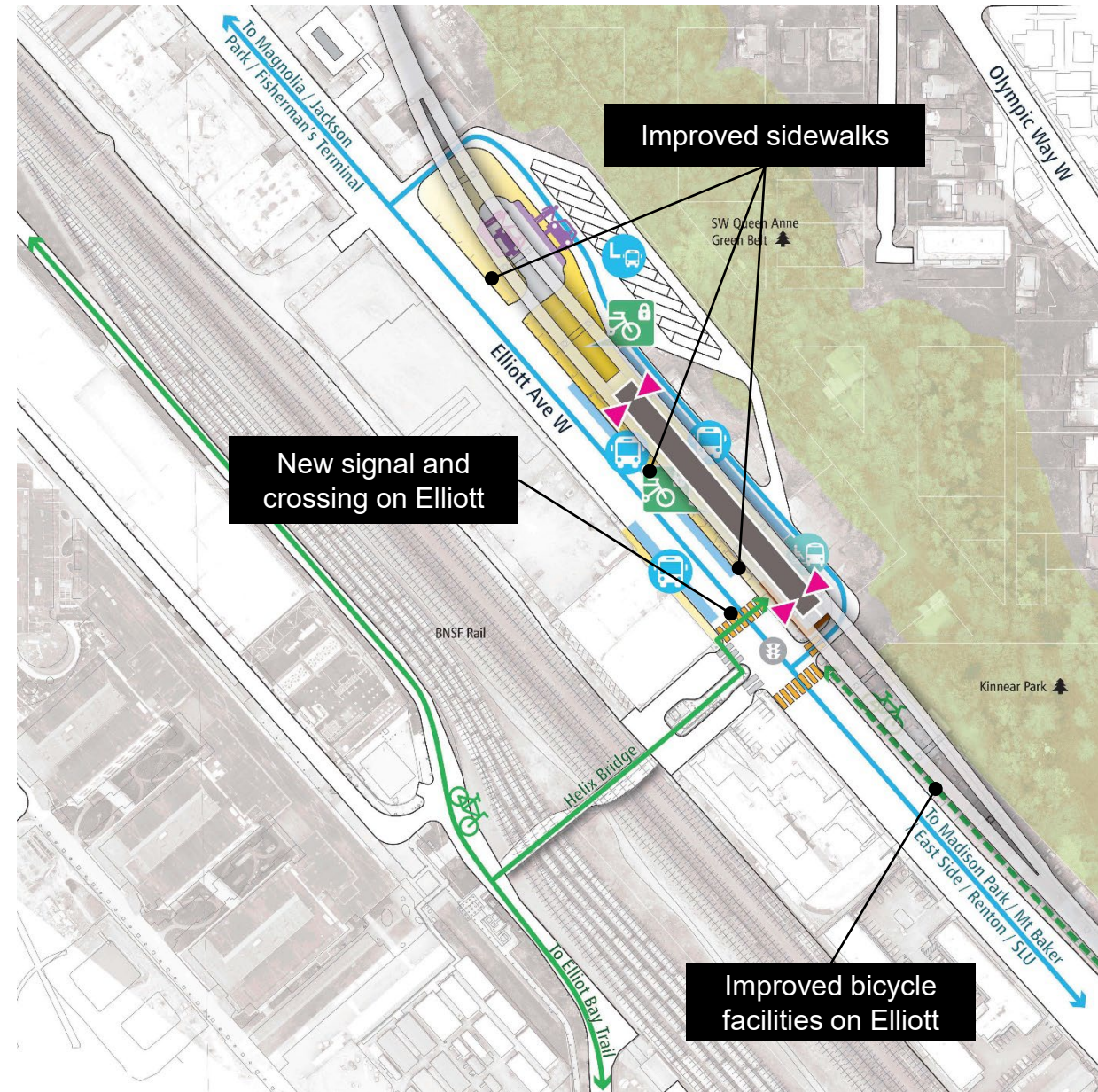
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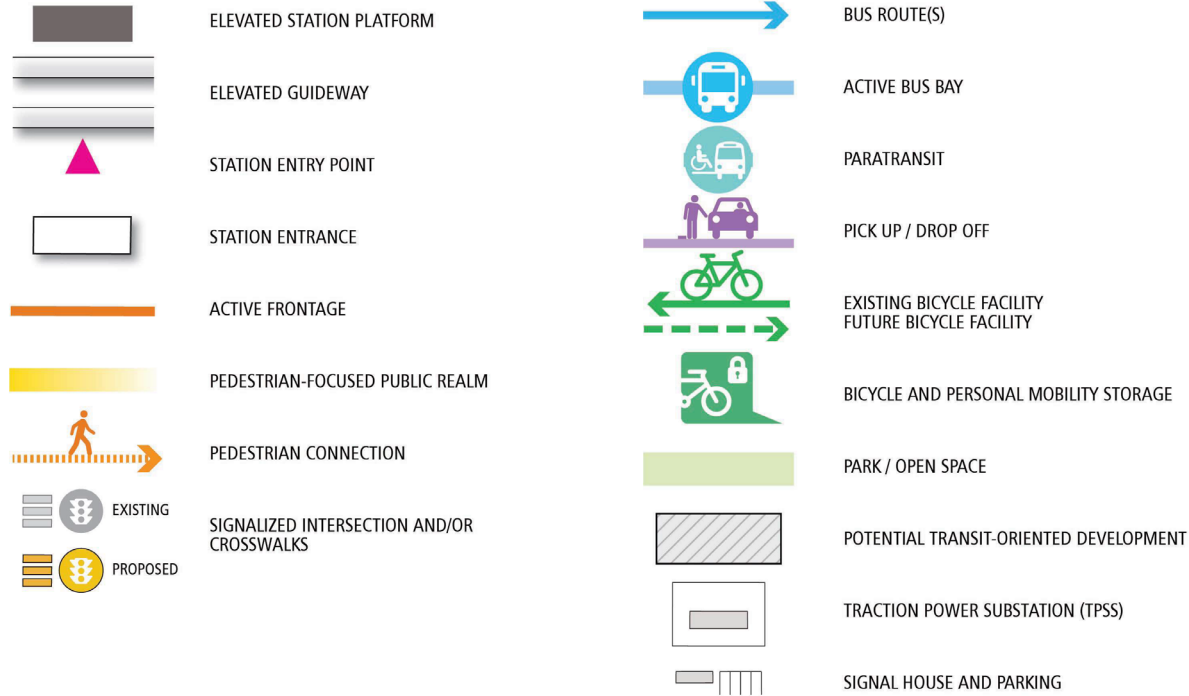
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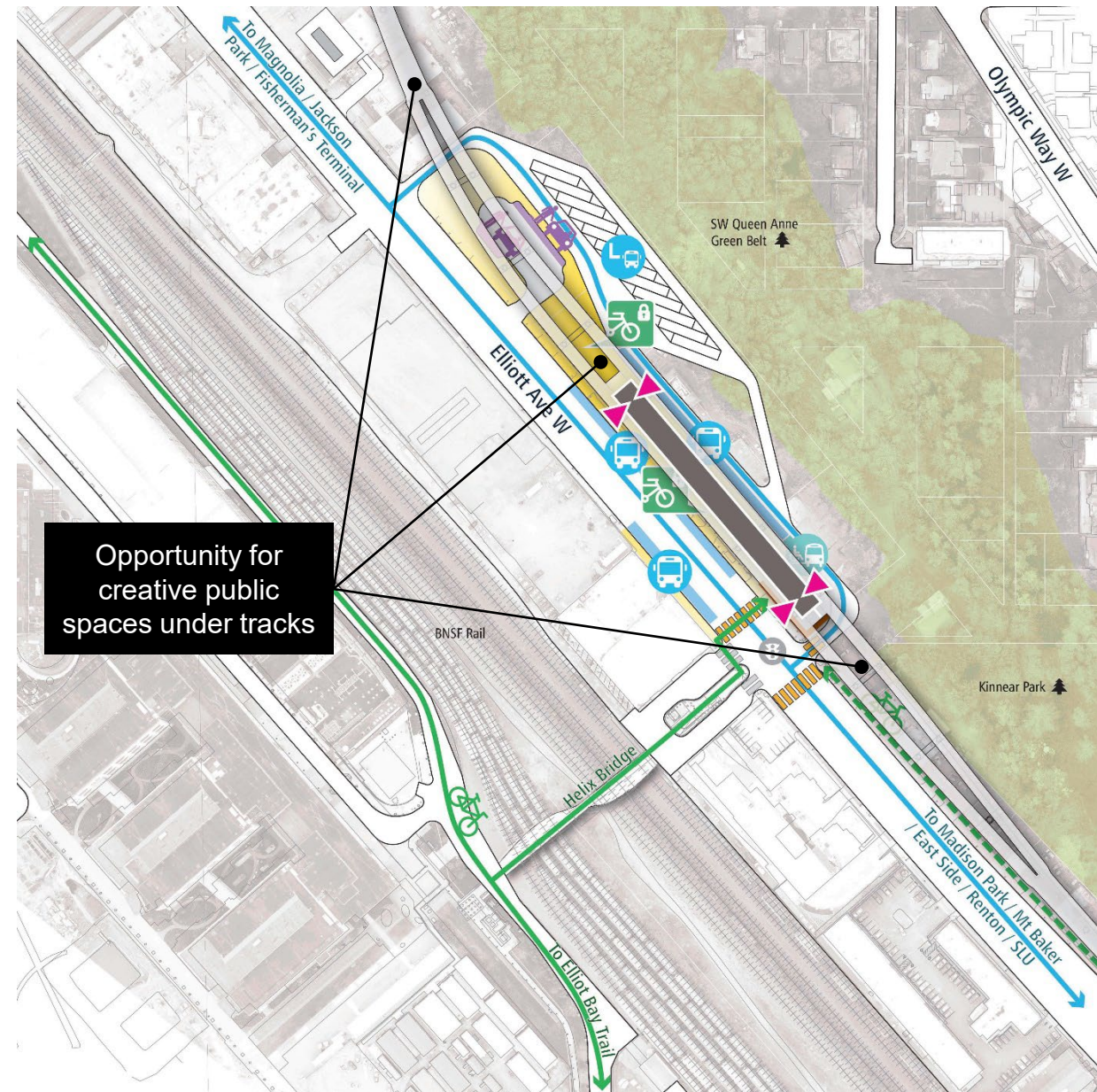
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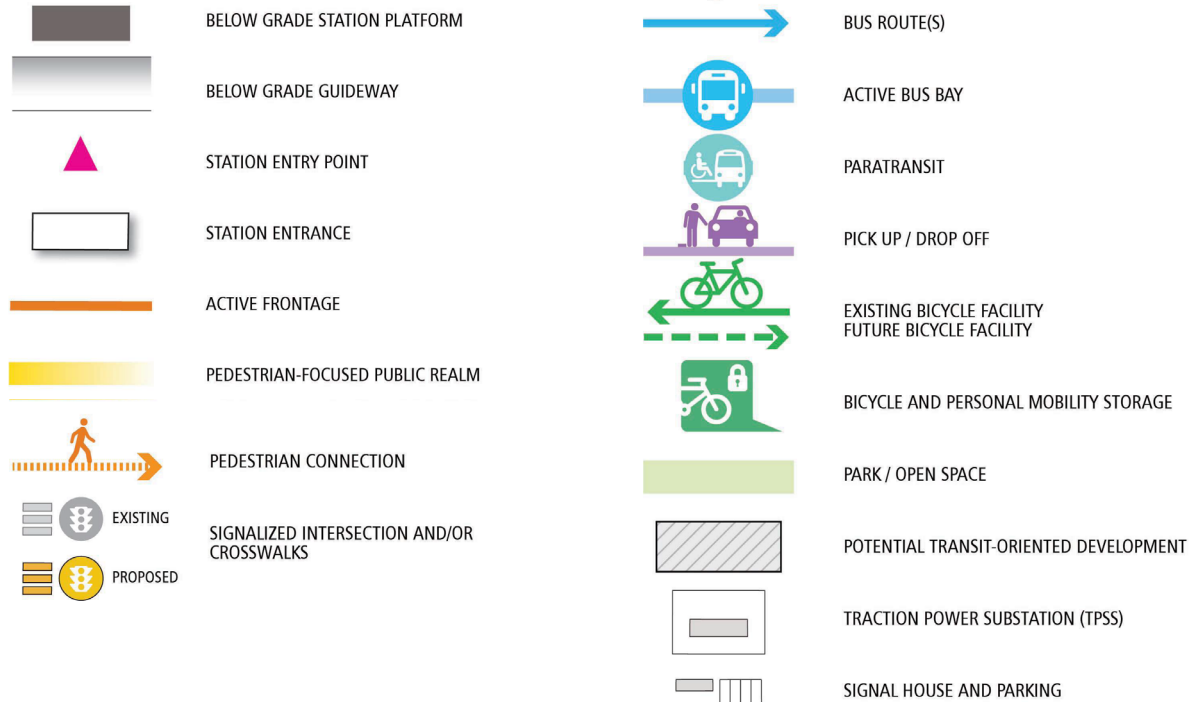
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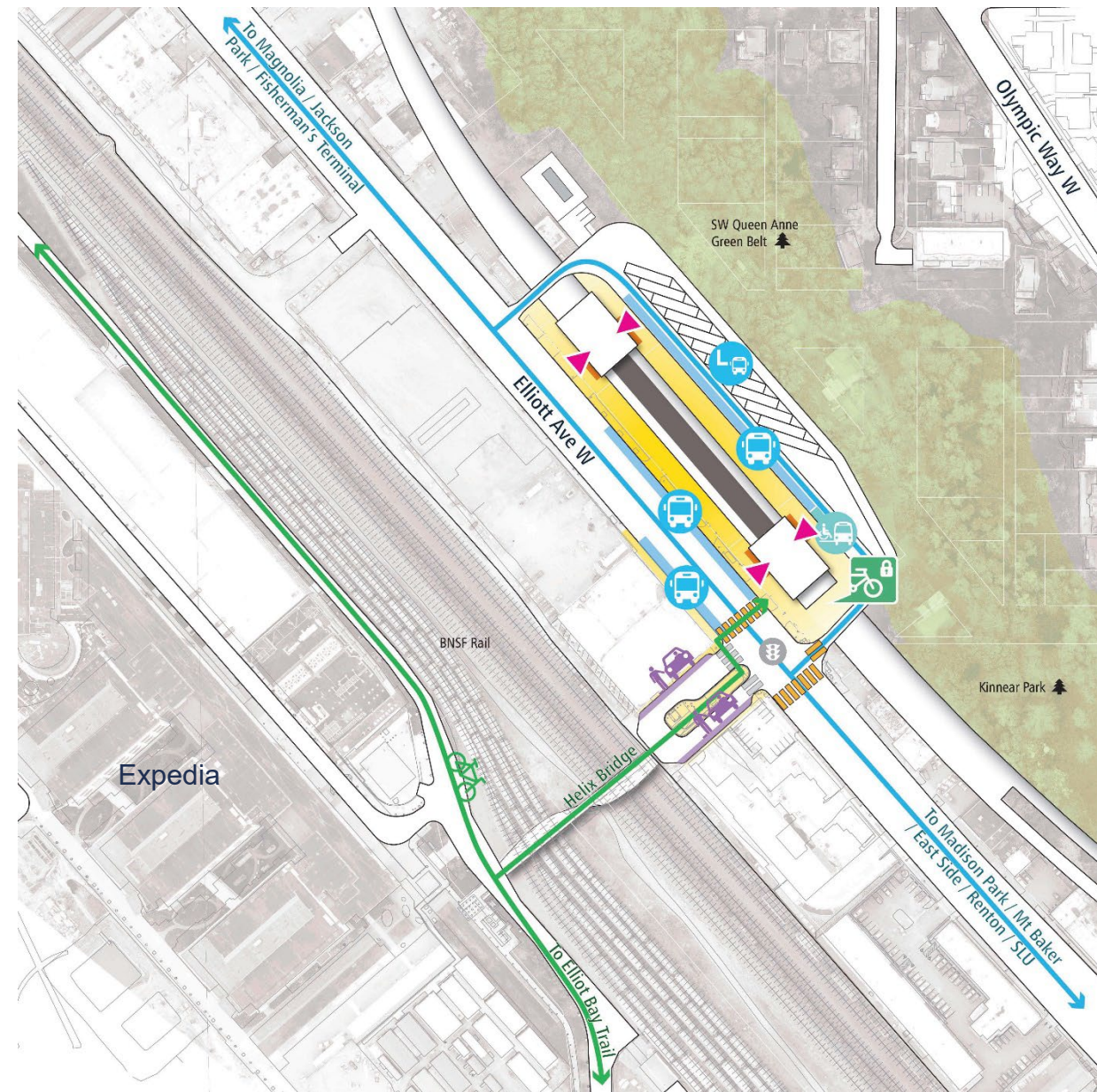
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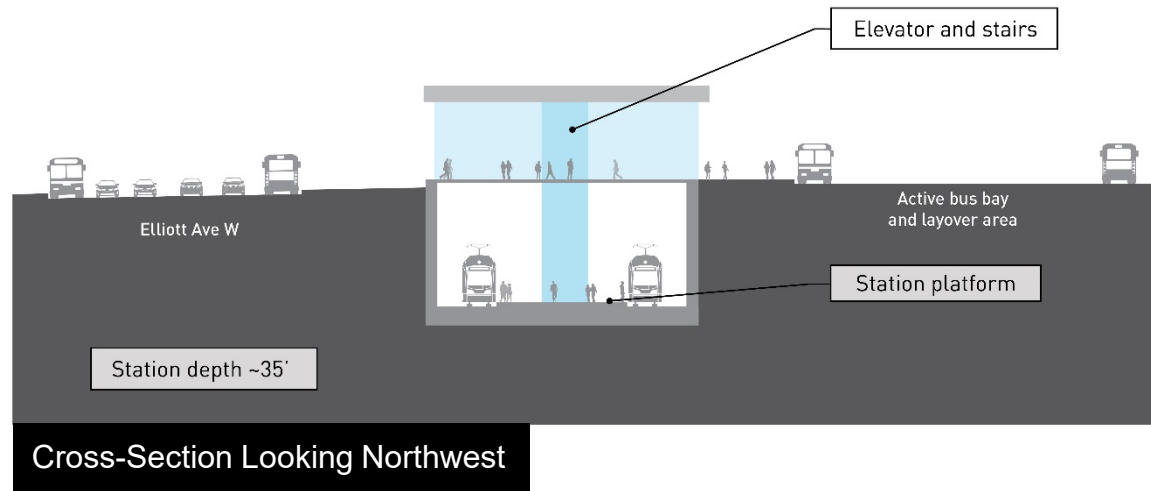
Retained Cut Prospect St Station



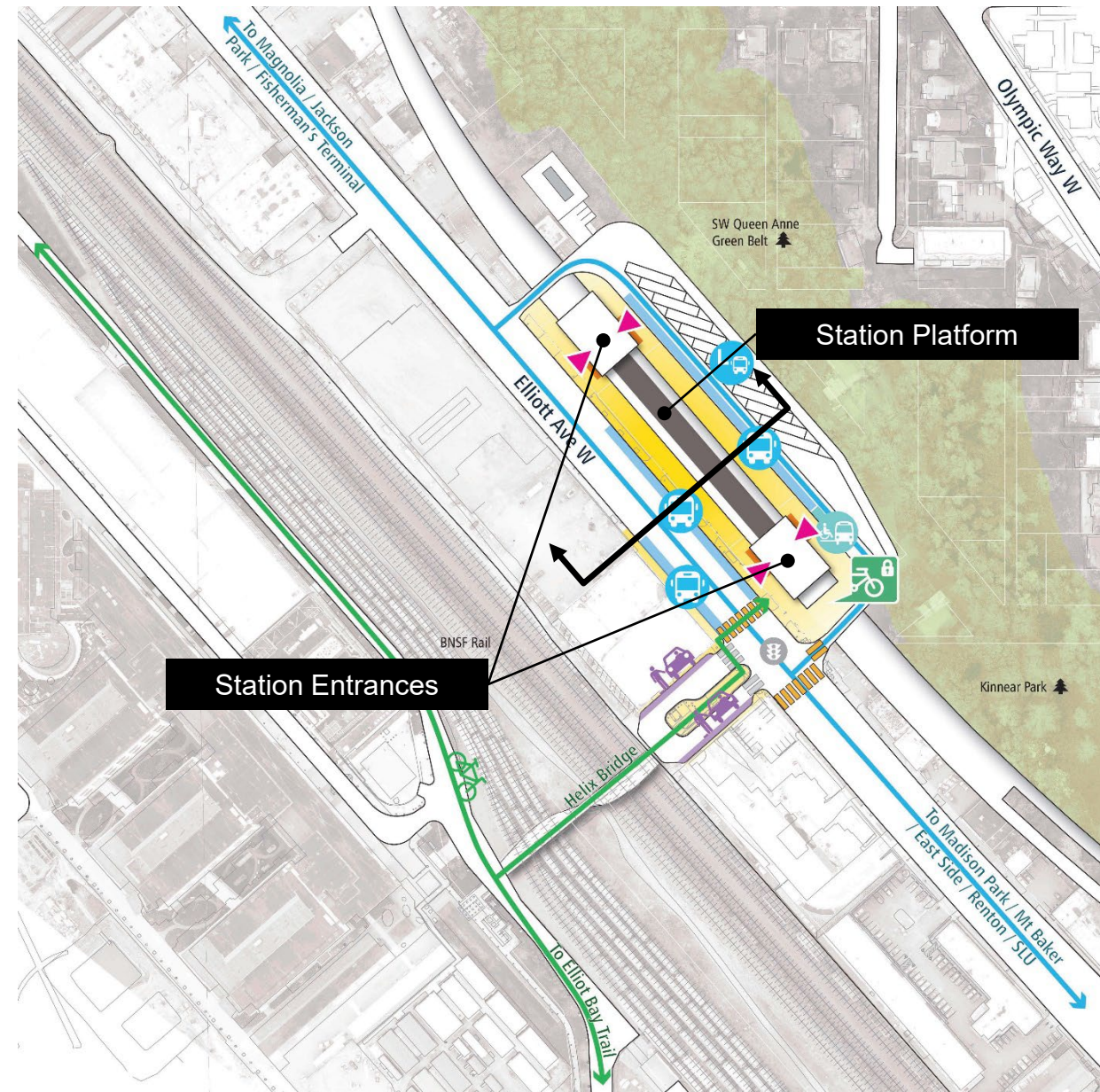
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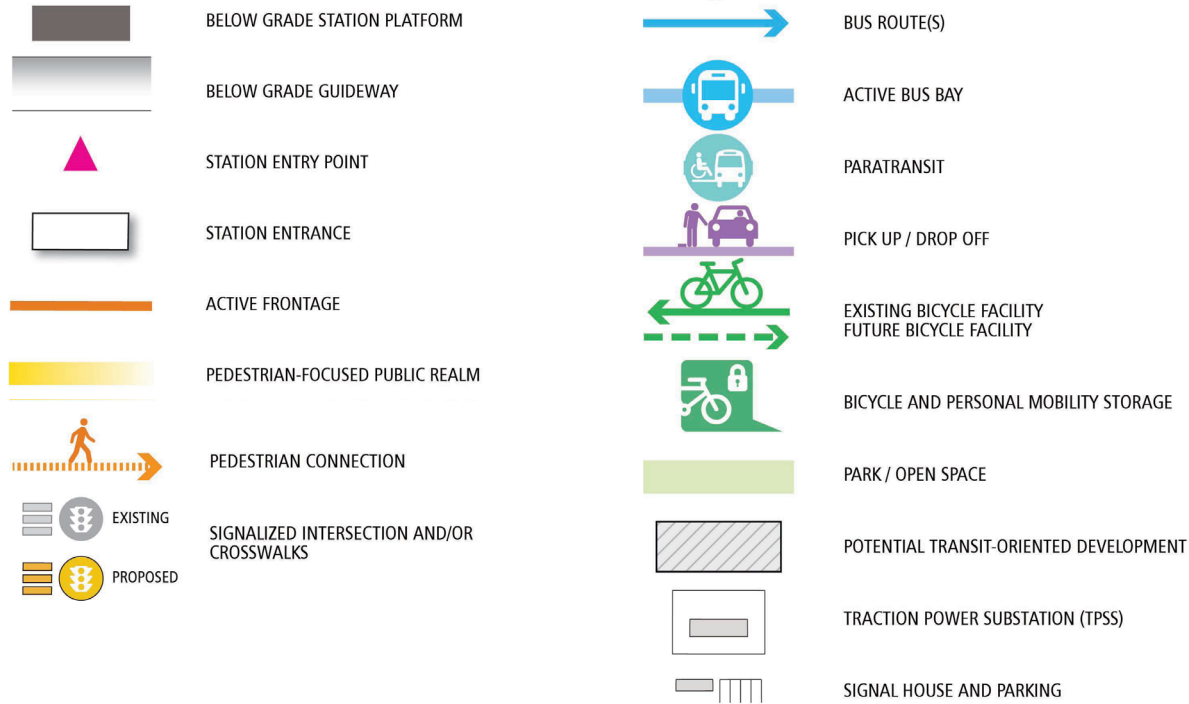
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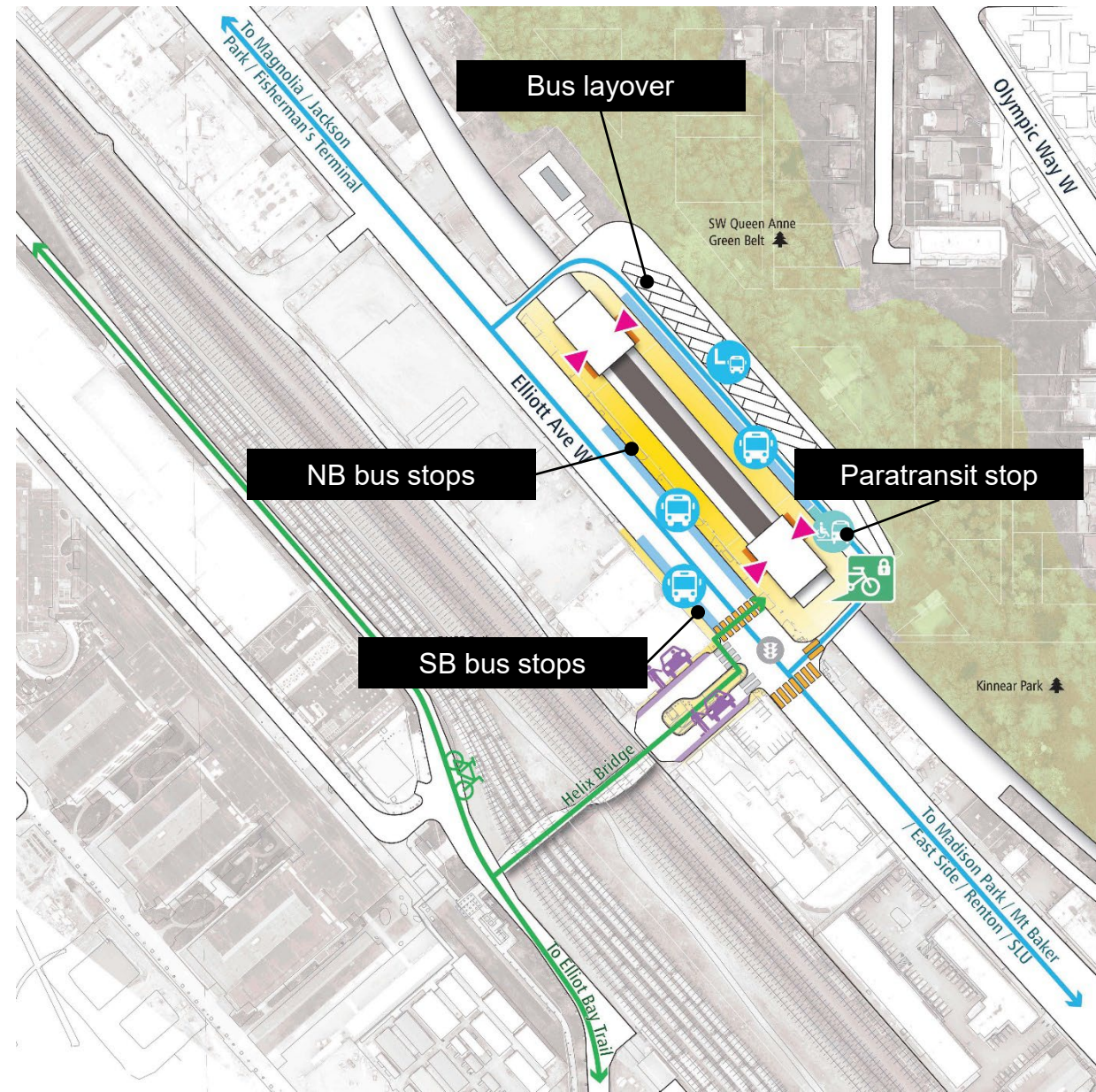
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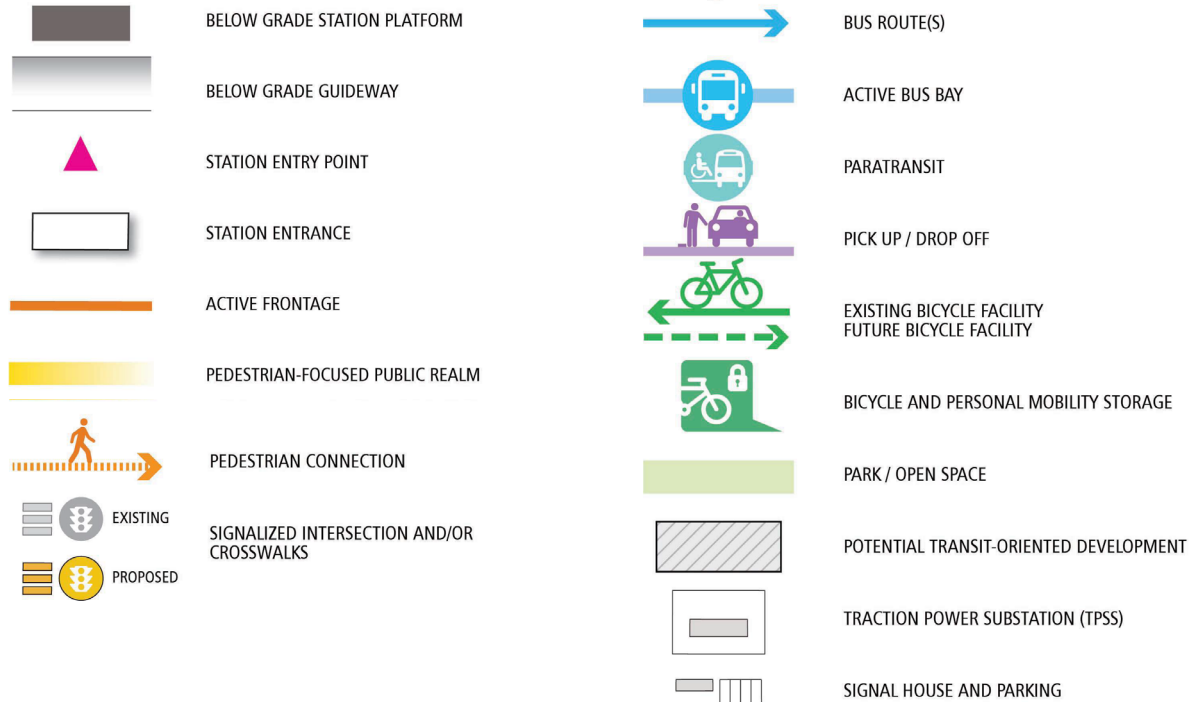
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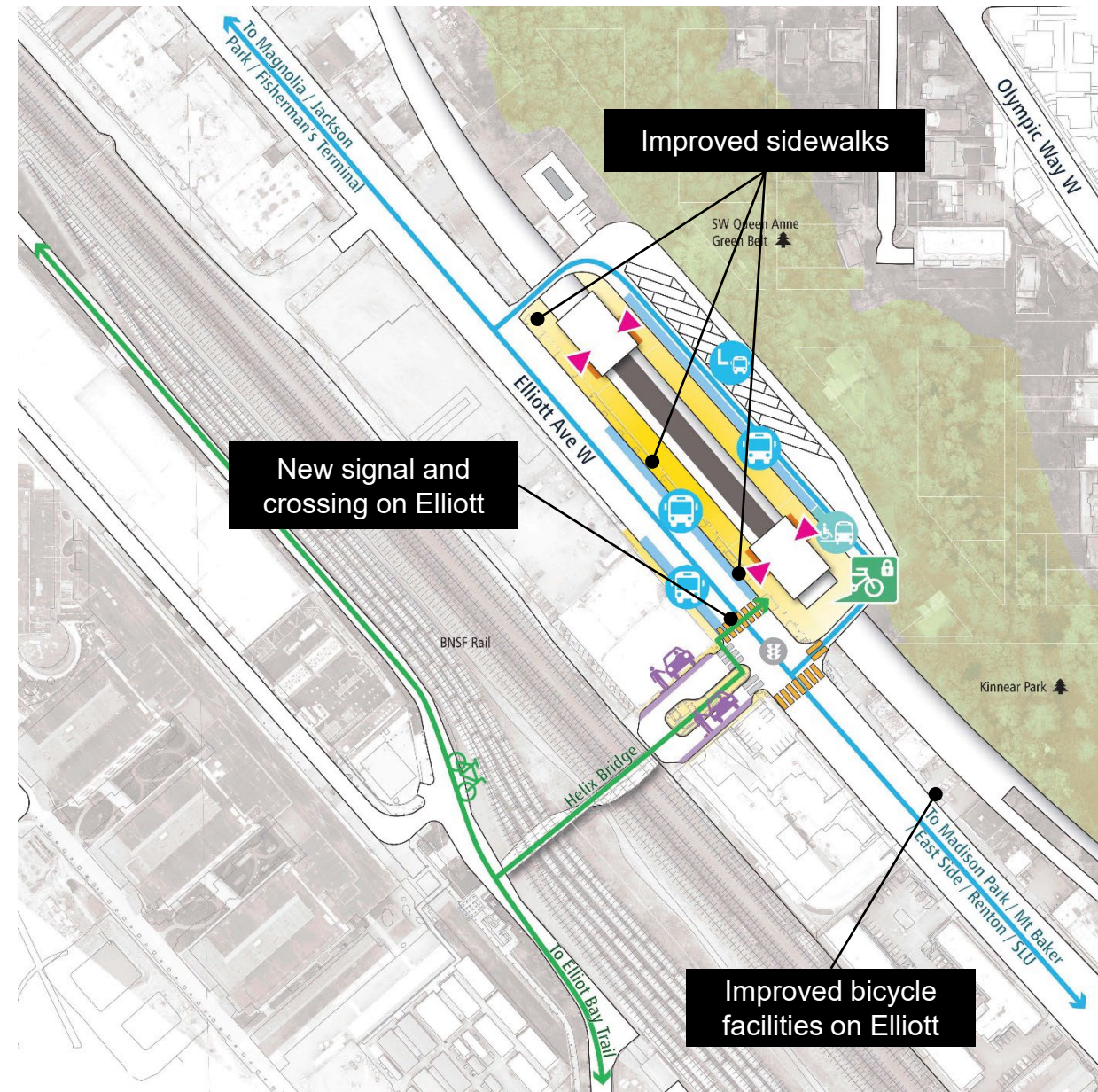
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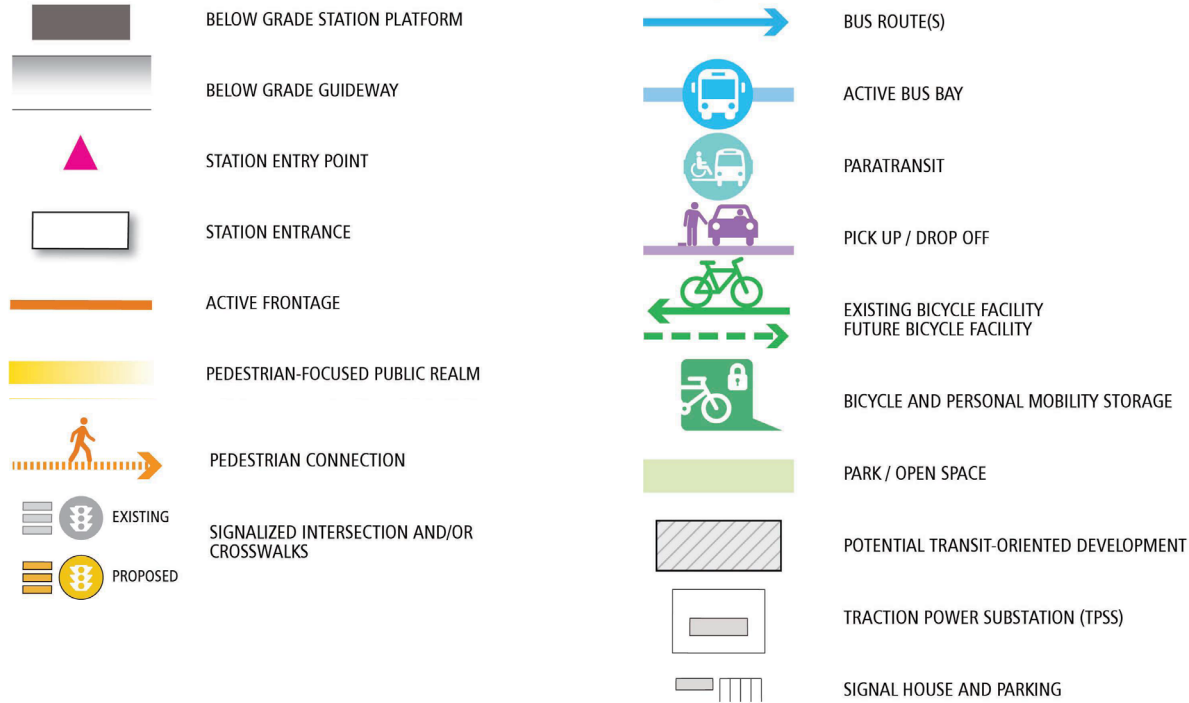
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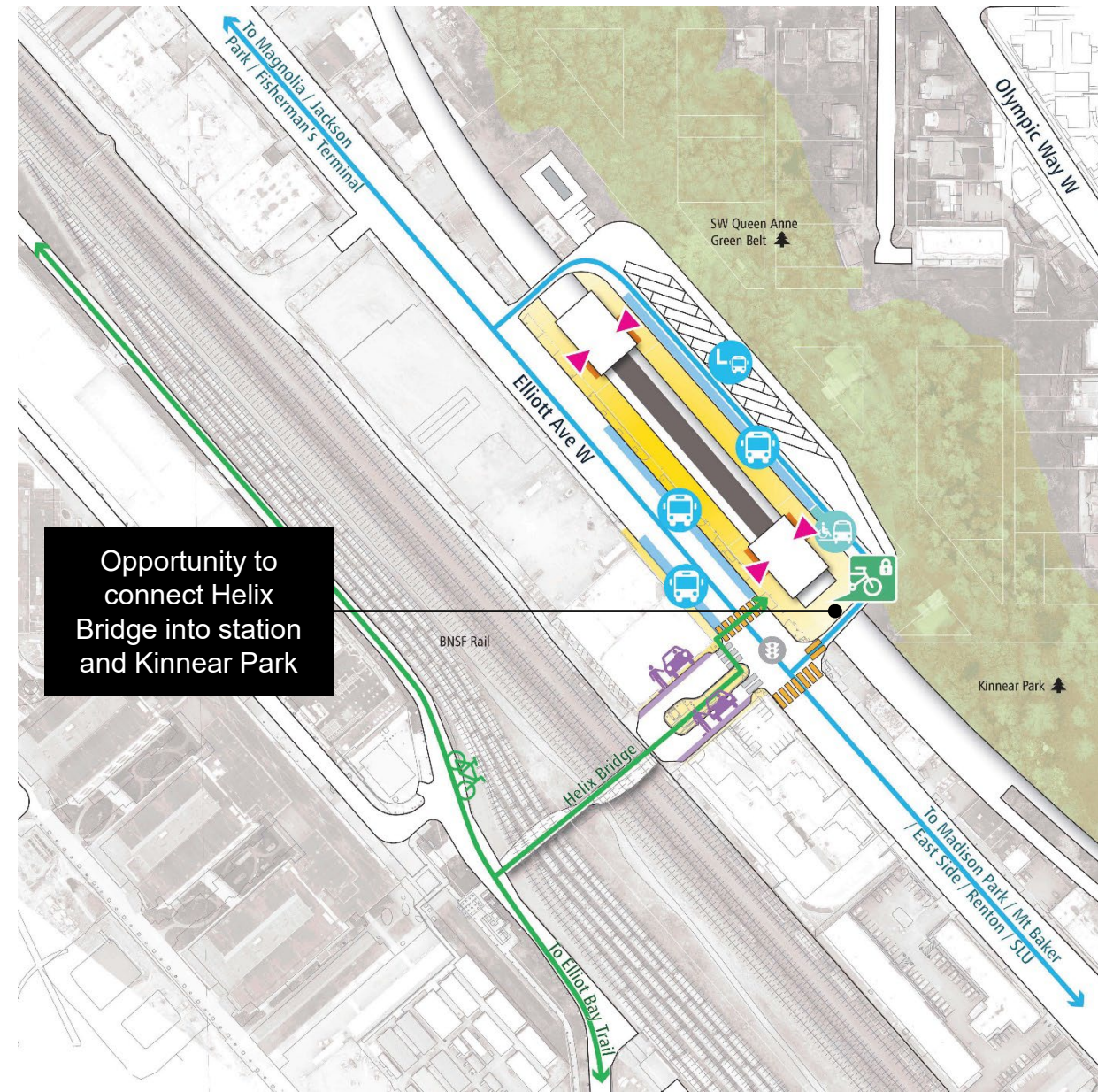
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Retained Cut Prospect St Station



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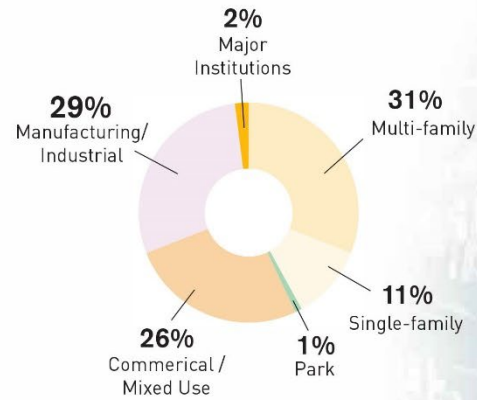
Agenda

- Welcome and introductions
- Project orientation
- Guideway structures and bridges
- Smith Cove Station
- ***Ballard Station***



- ***Enhance mobility and access for communities of color and low-income populations***
- ***Create opportunities for equitable development that includes expanding housing and community assets for communities of color***
- ***Create a sense of belonging for communities of color at all stations***

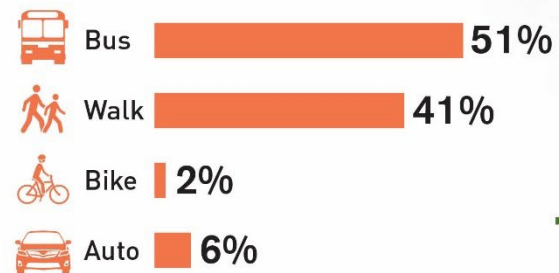
Existing land use in the station area ⁽¹⁾



Ridership/daily boardings ⁽²⁾



How people will travel to the station ⁽²⁾



Bike facilities within 10-minute bikeshed ⁽³⁾



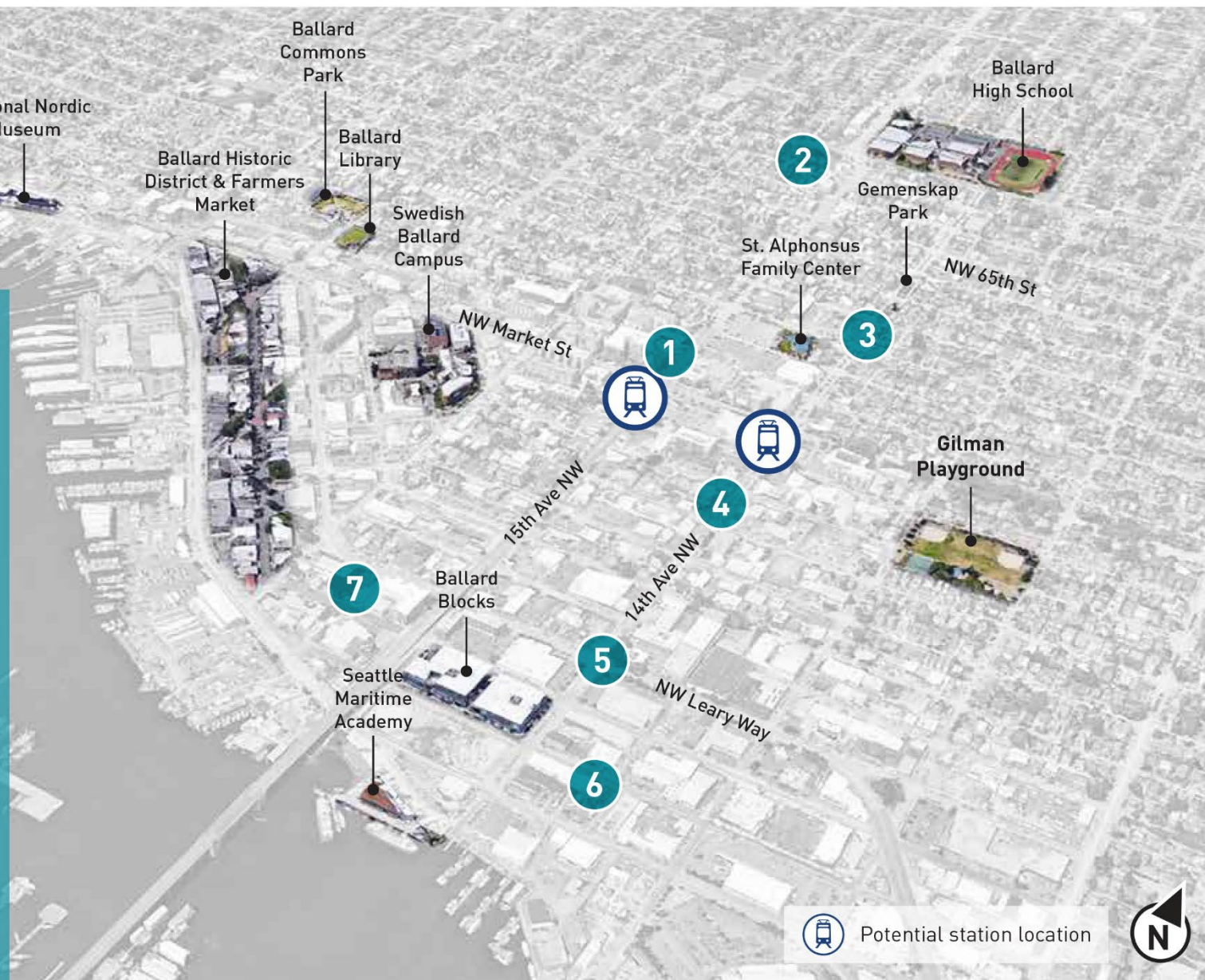
Living and working in the station area 2040 ^(1,4)



What we heard so far

NEIGHBORHOOD FEEDBACK

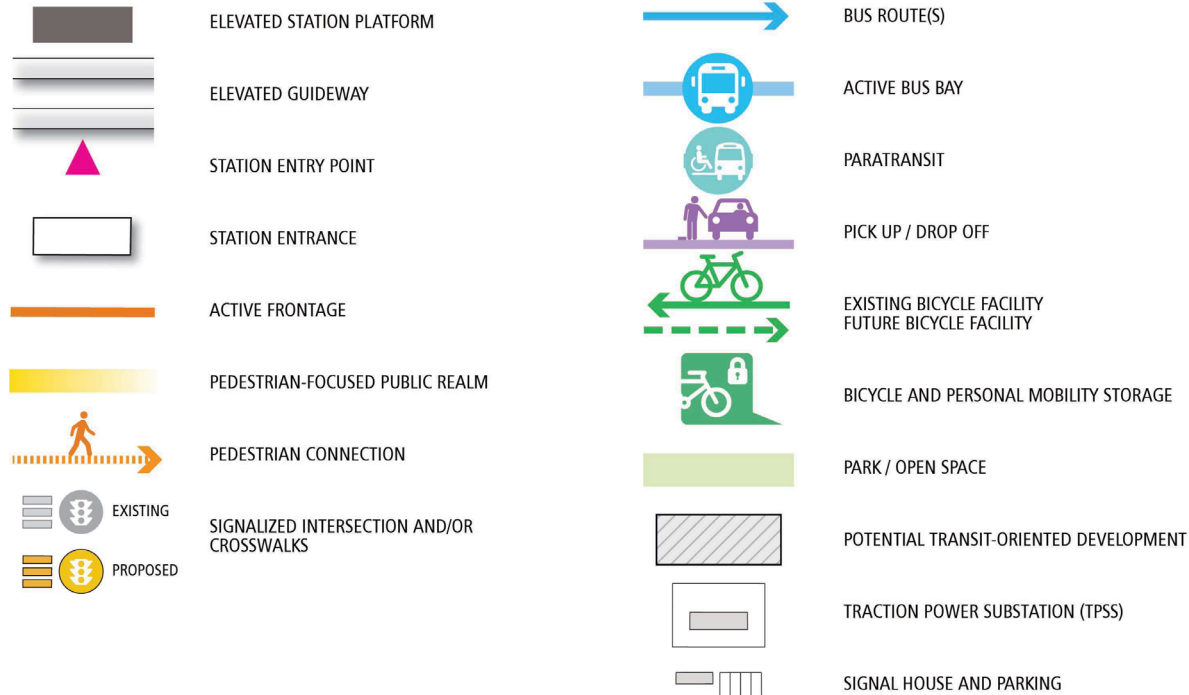
- 1 Intersection of 15th Ave NW and NW Market St is a difficult environment for pedestrians
- 2 Concerned with how light rail will be extended north in the future
- 3 Extending Gemenskap Park south along on 14th Ave NW is important
- 4 14th Ave NW is a planned pedestrian route
- 5 New development is bringing more foot traffic to industrial area, but area is not safe for pedestrians
- 6 Desire to maintain and support current and future industrial uses
- 7 Opportunity to mix light industry with housing



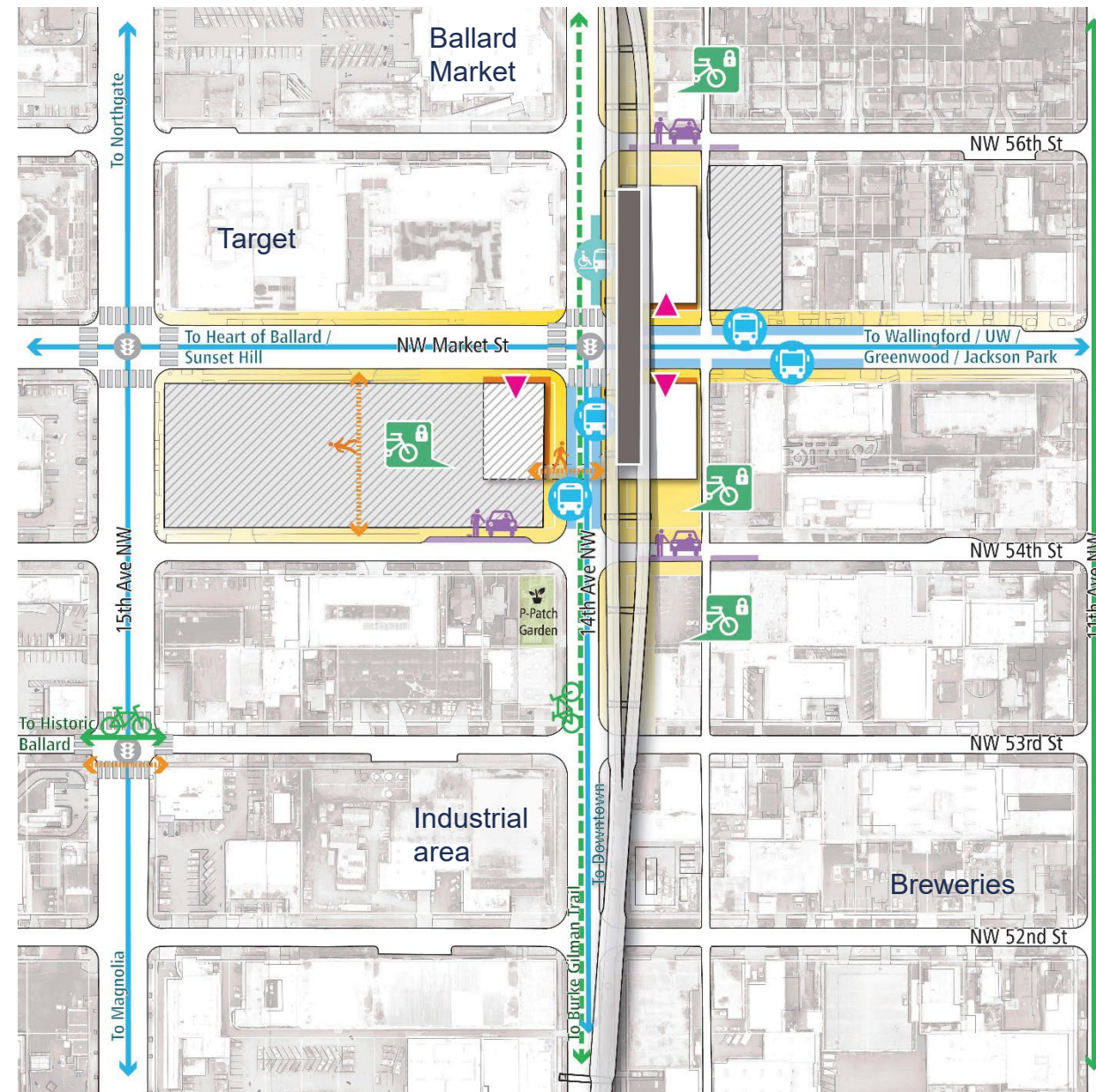
Note: Neighborhood feedback gathered from in-person and on-line events during alternatives development 2018-2019.

Preferred alternative

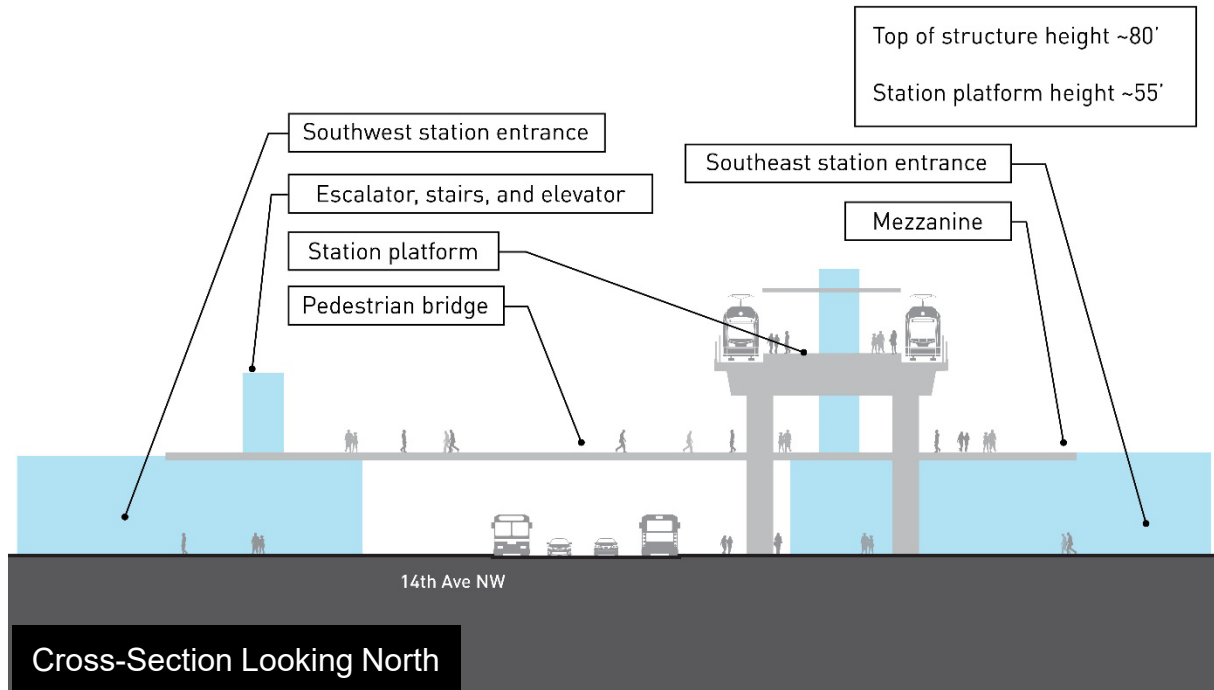
Elevated 14th Ave Station



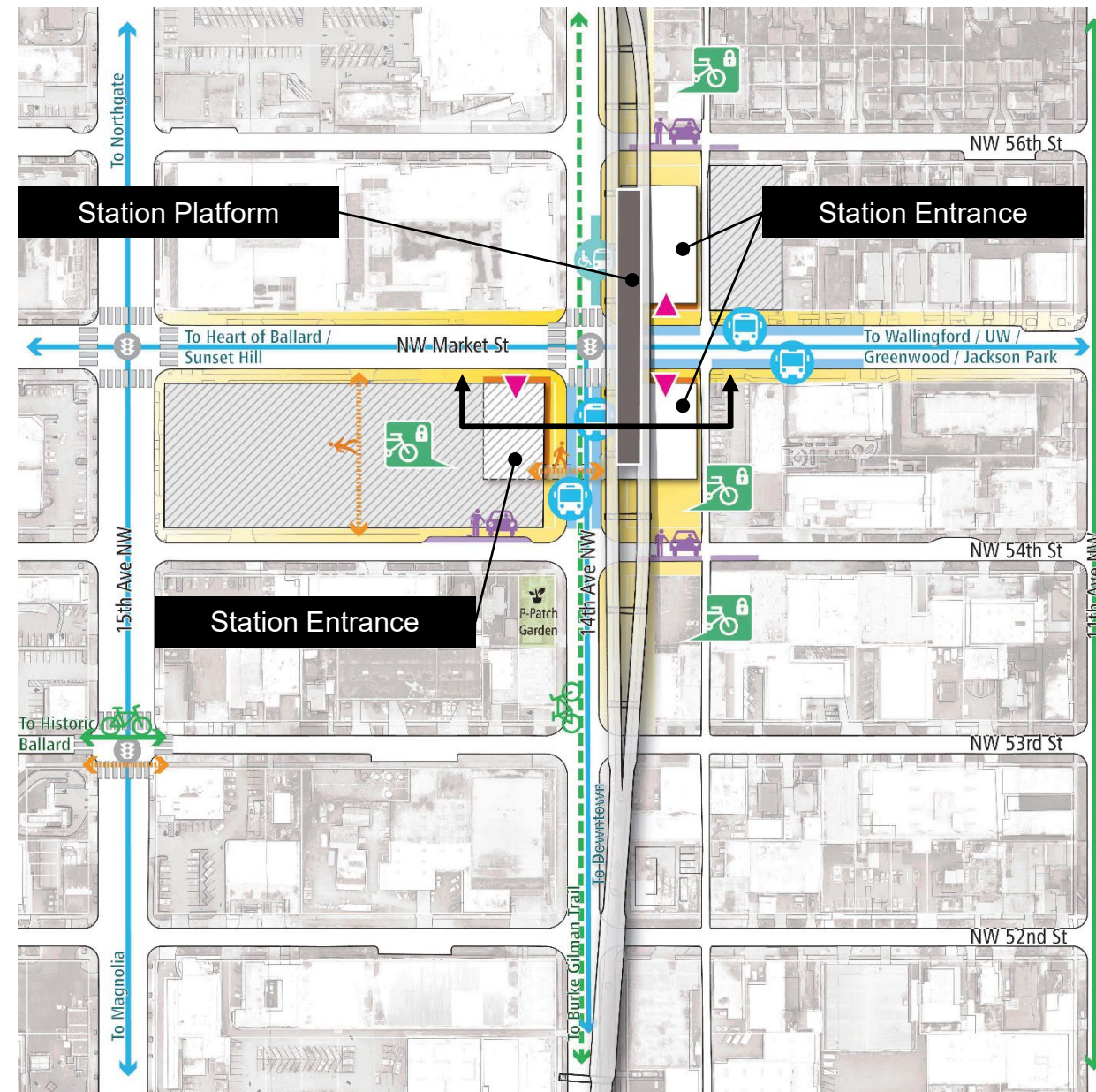
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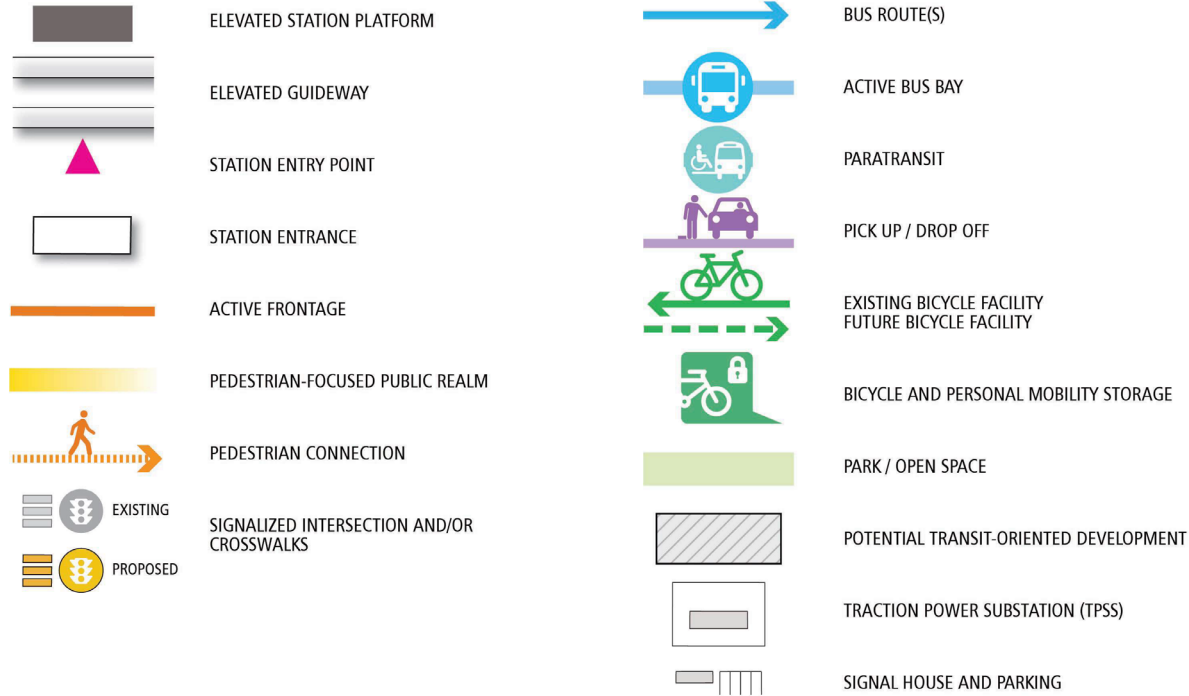
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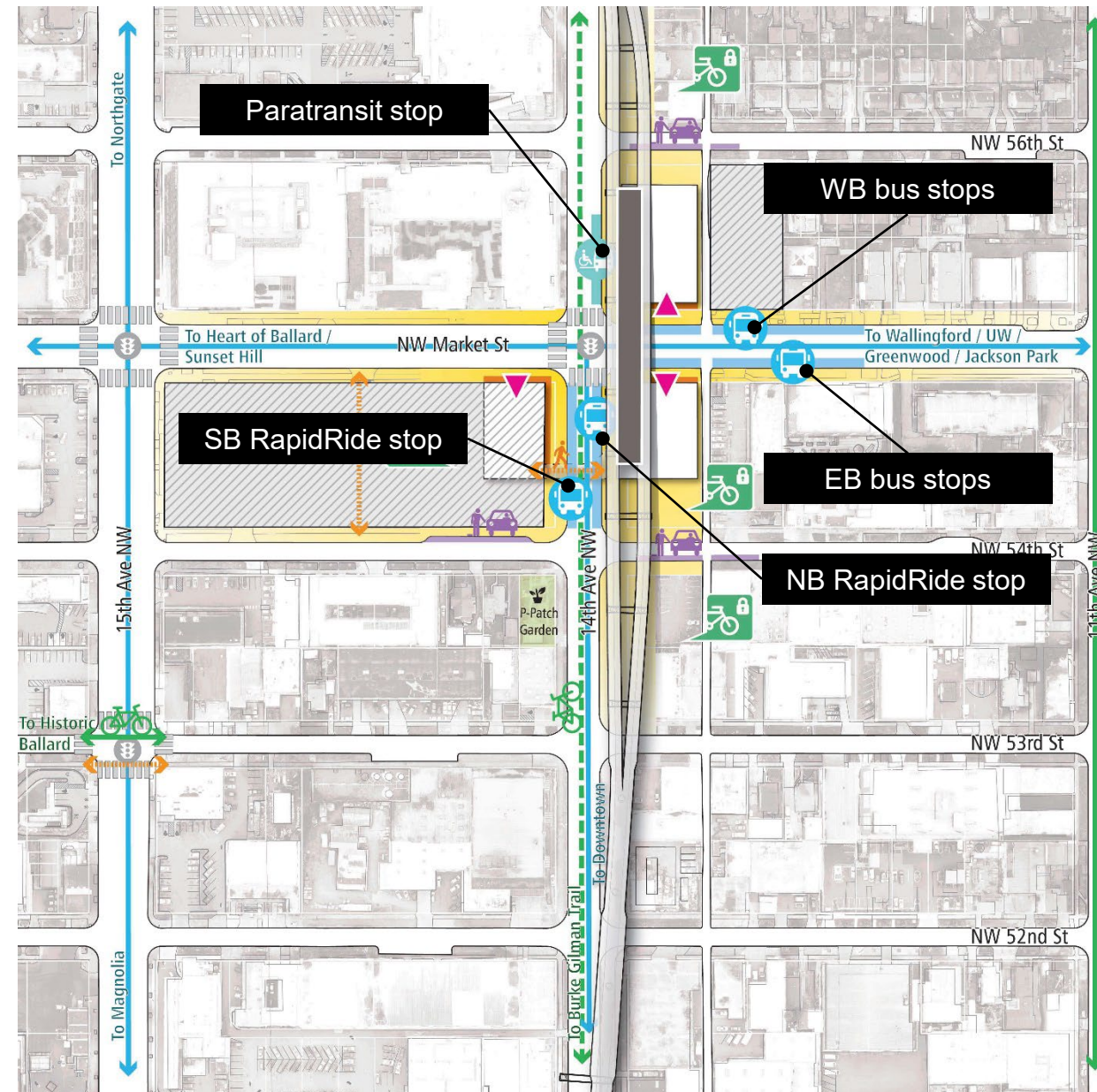
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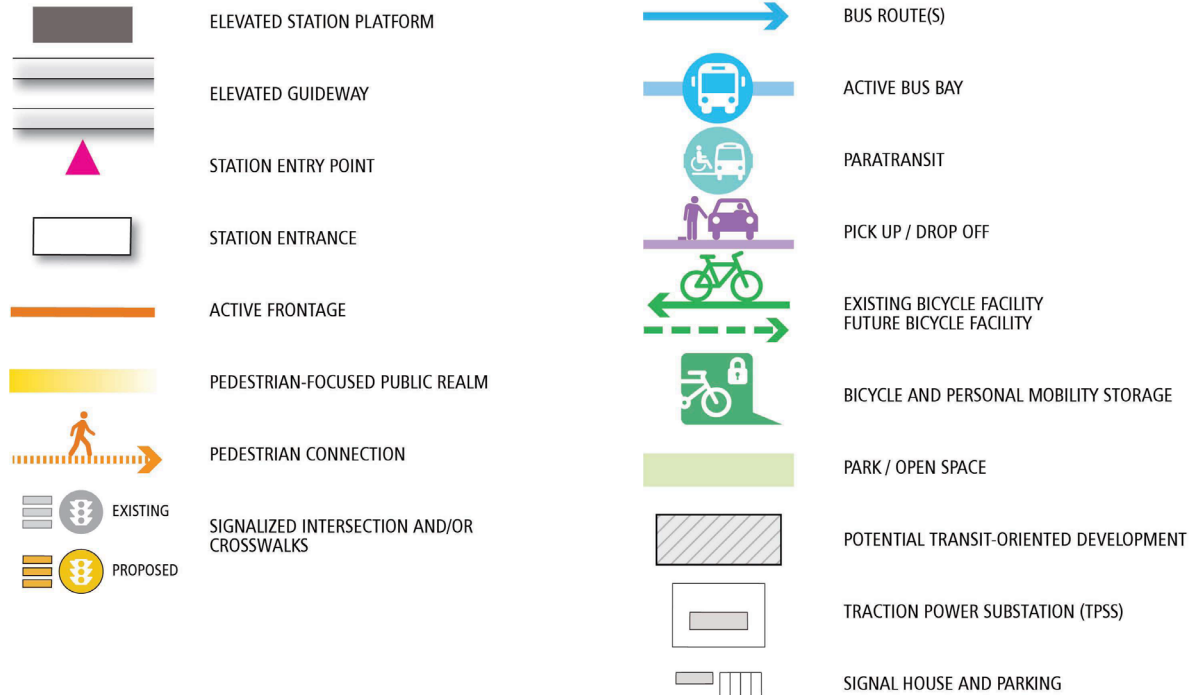
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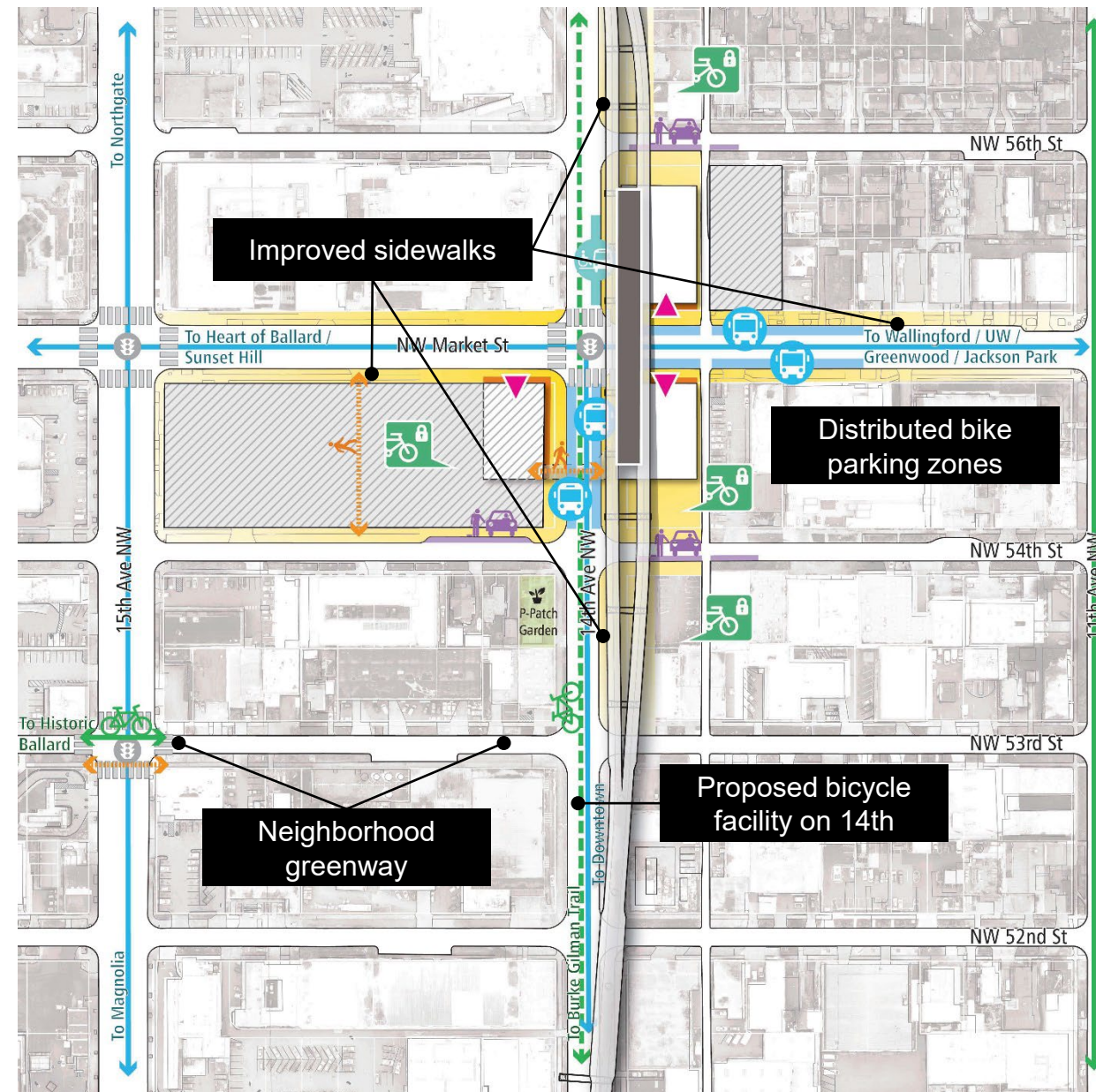
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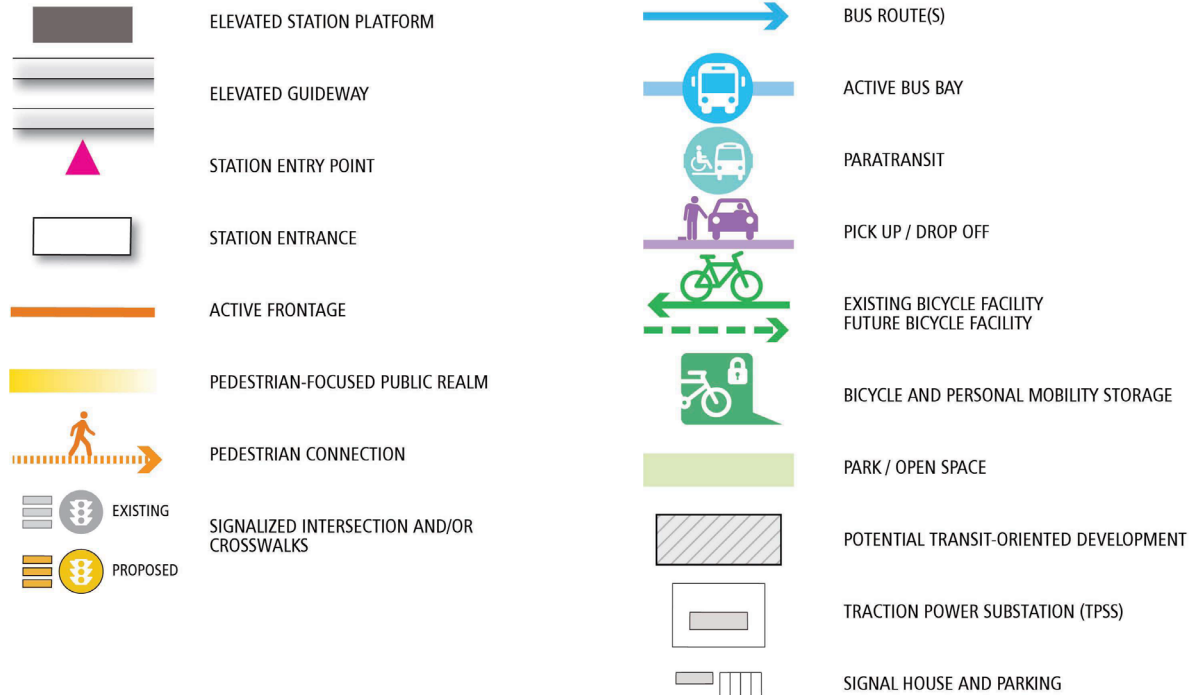
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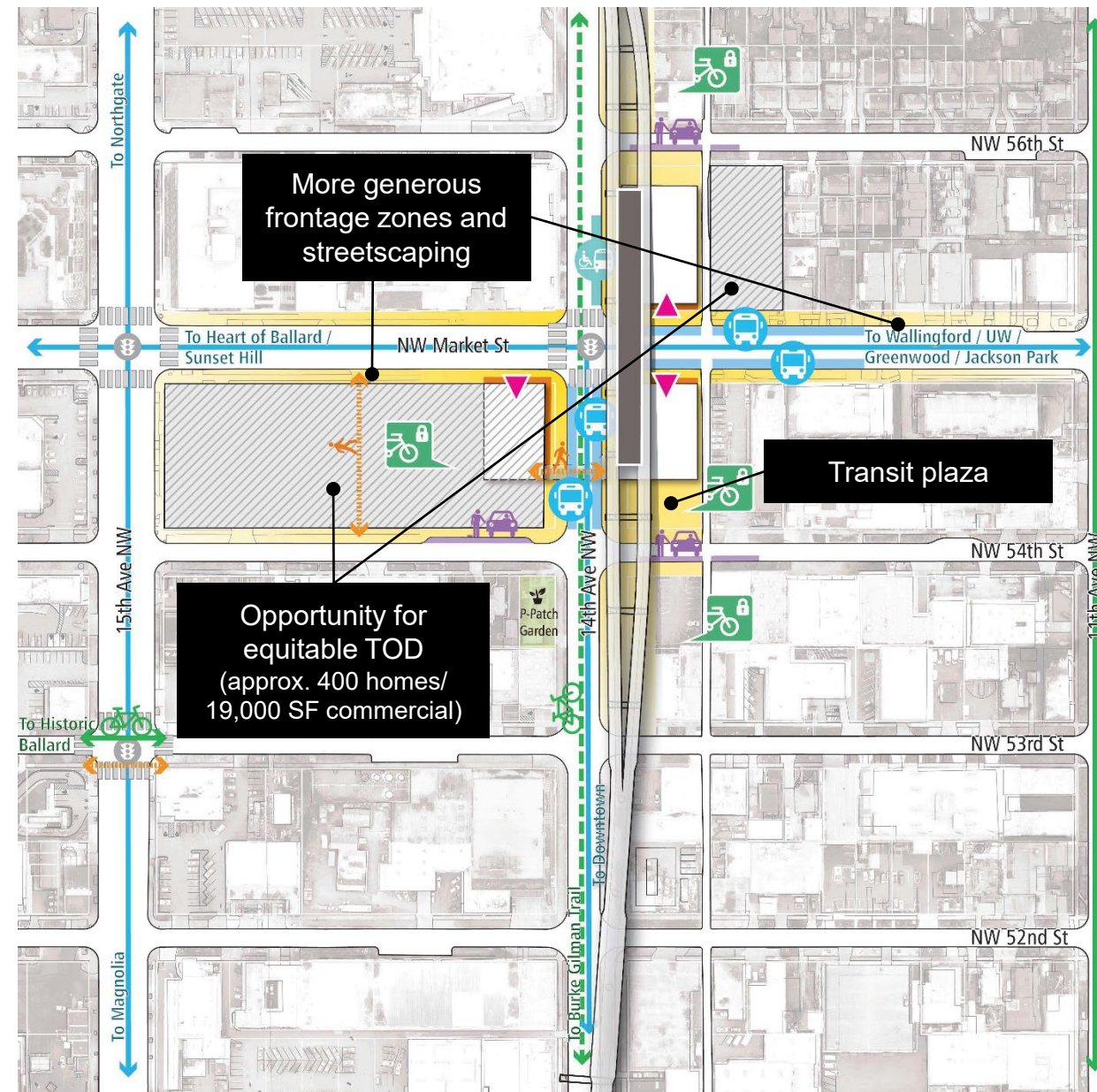
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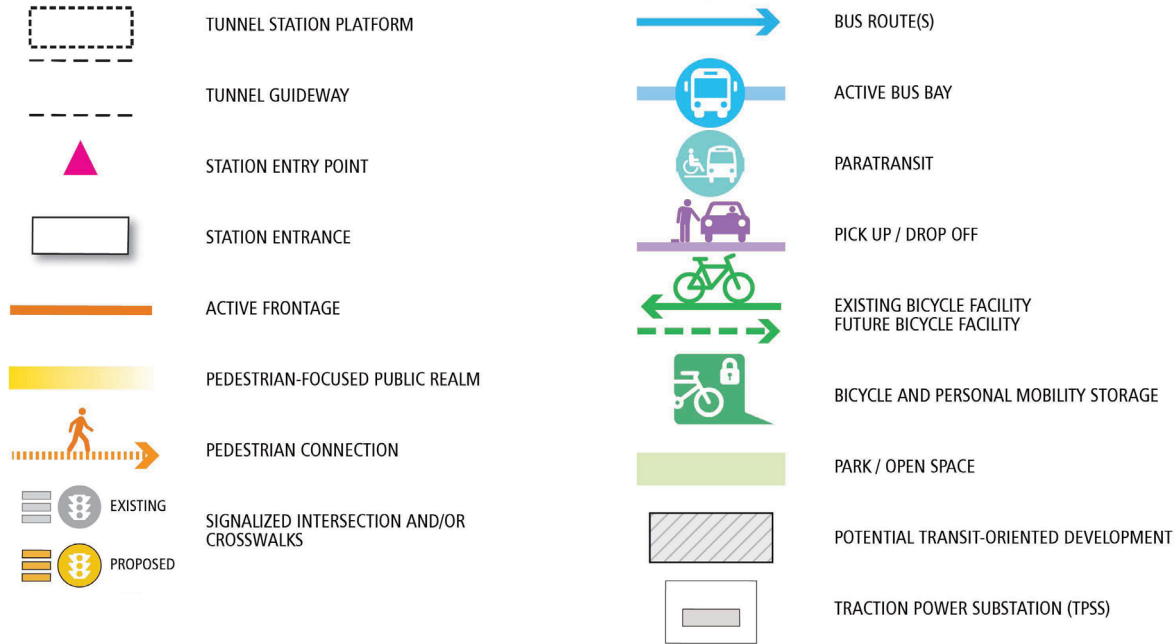


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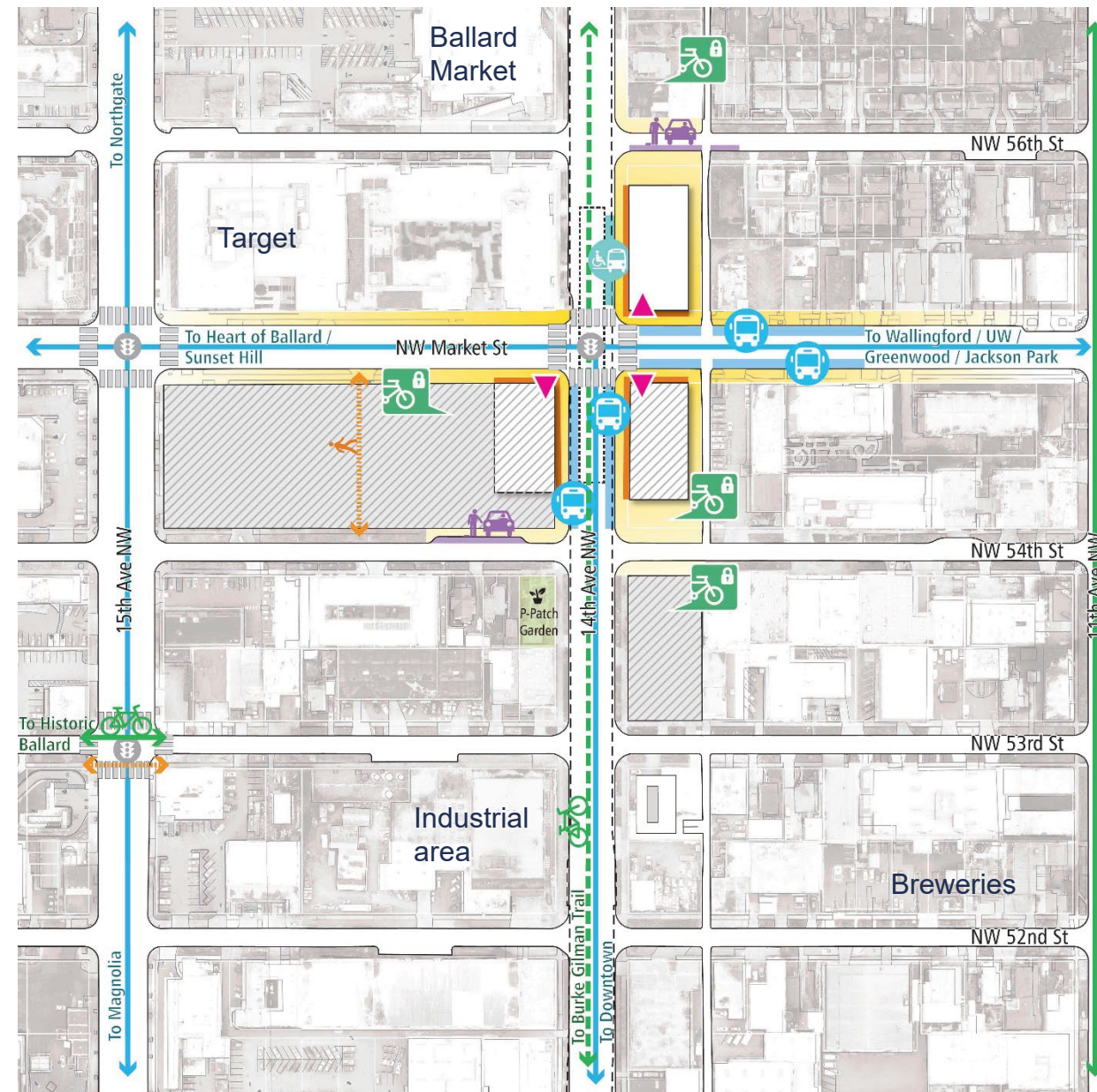


***Preferred alternatives with
third party funding***

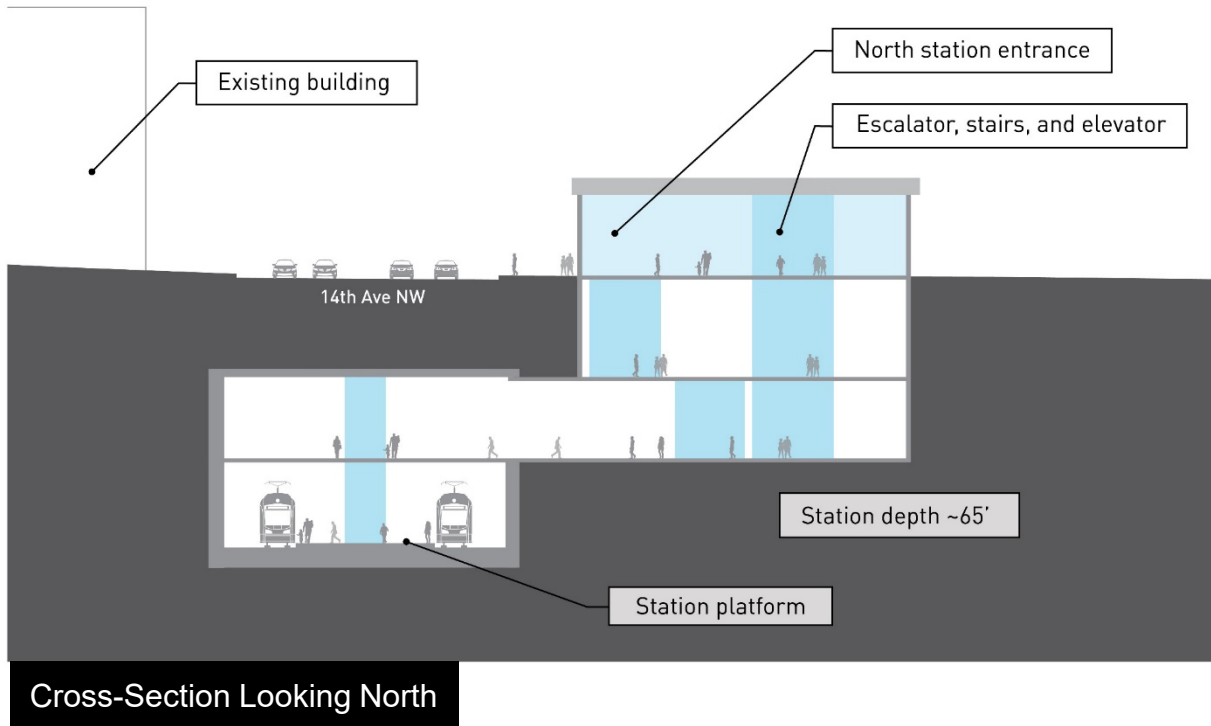
Tunnel 14th Ave Station



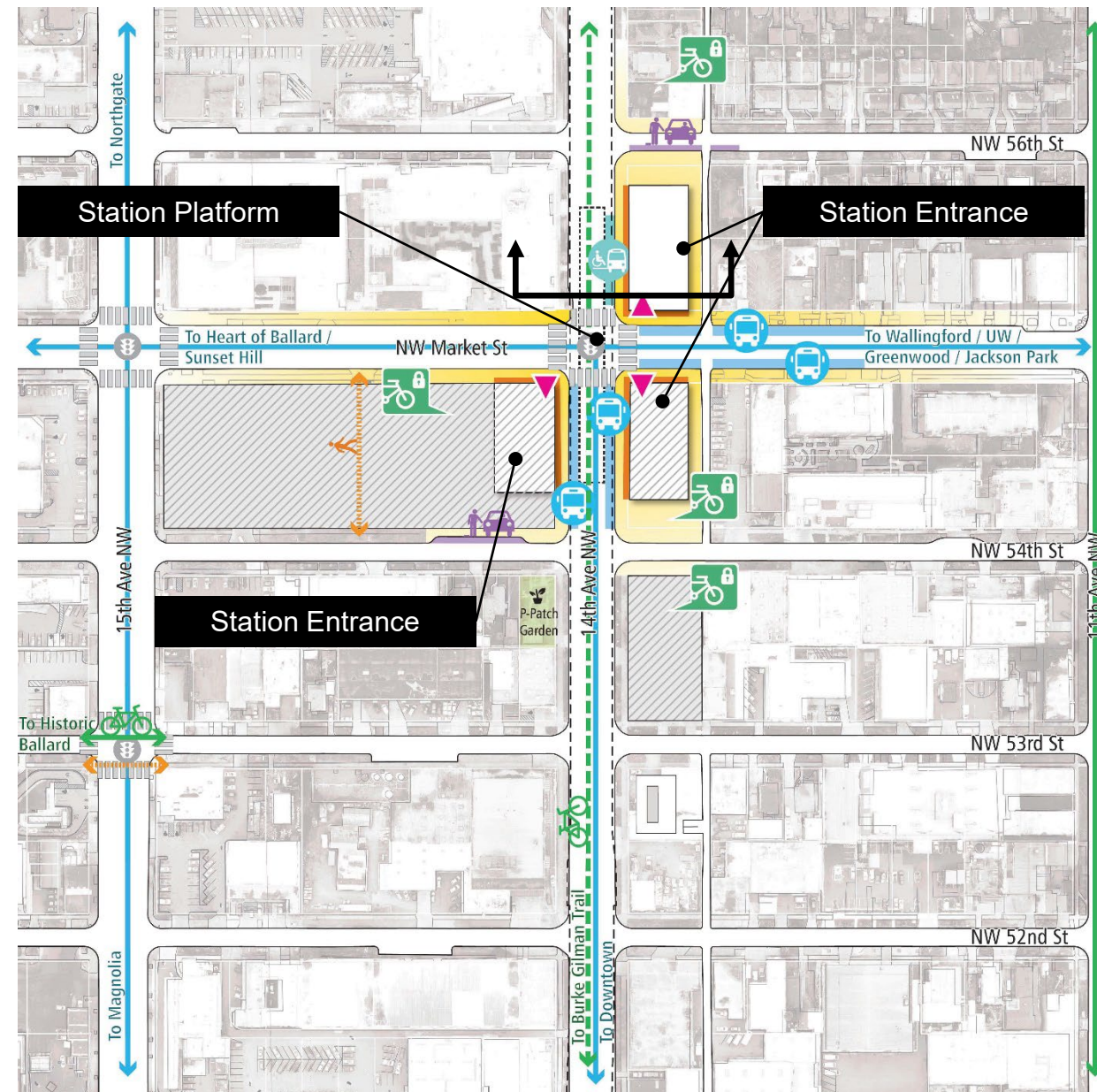
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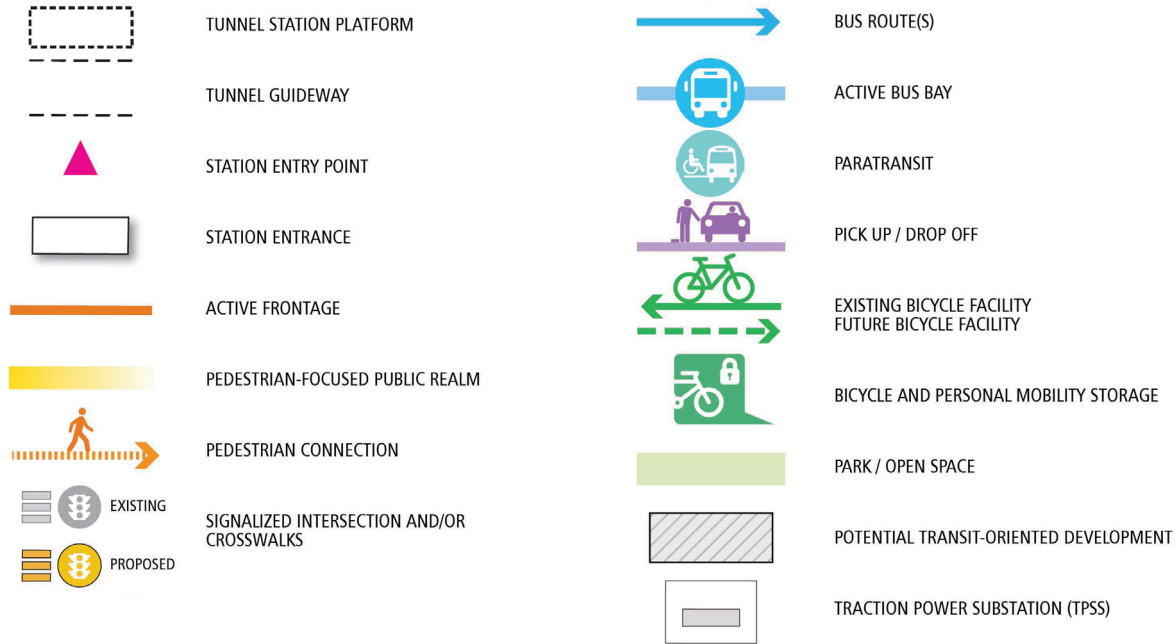
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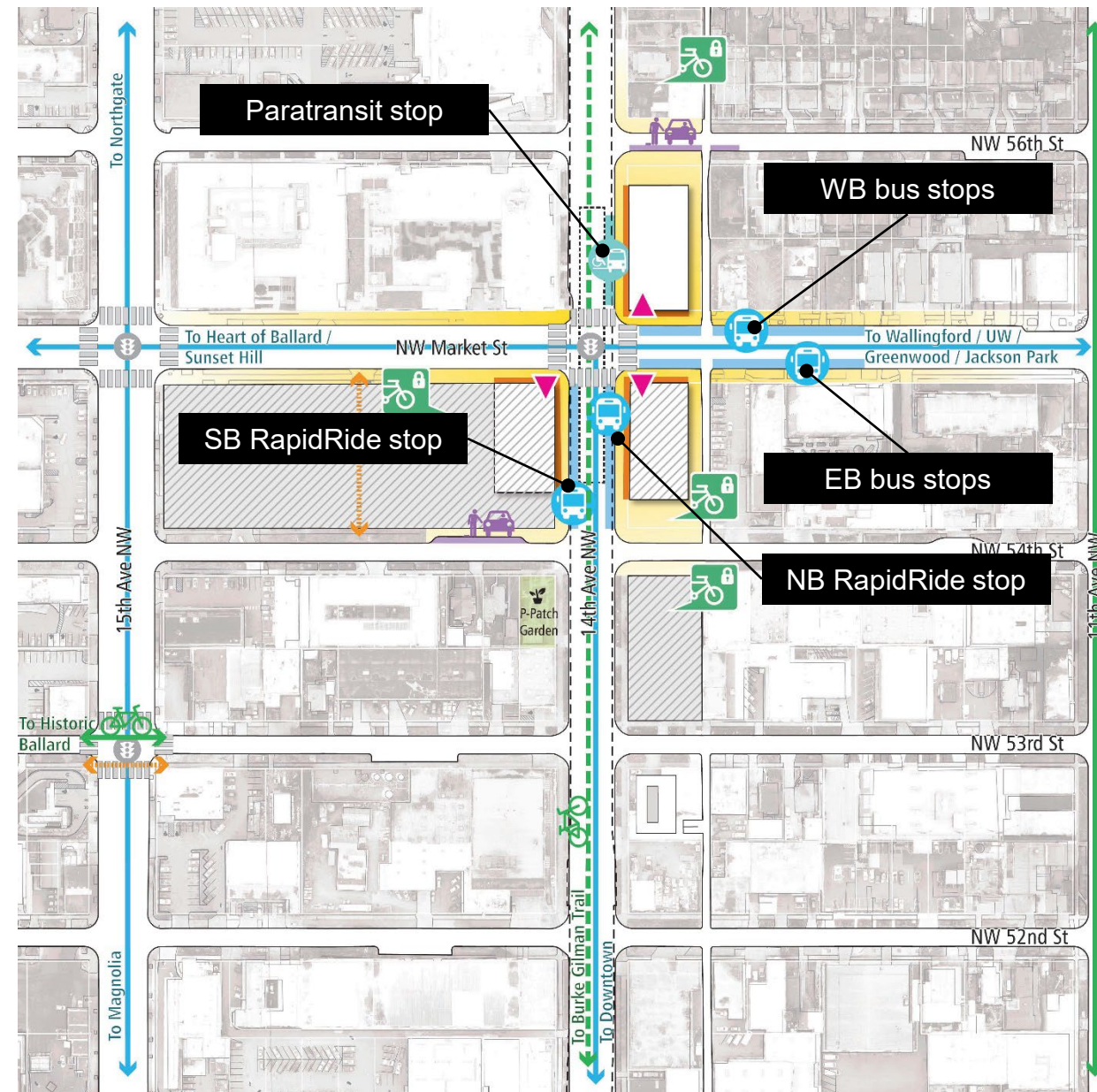
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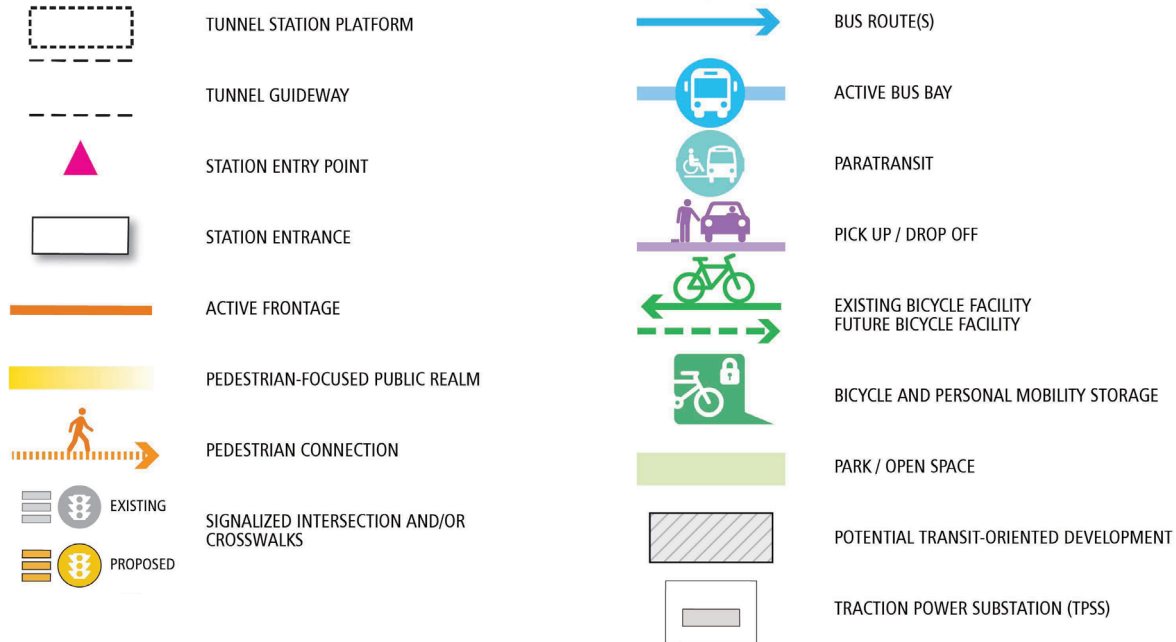
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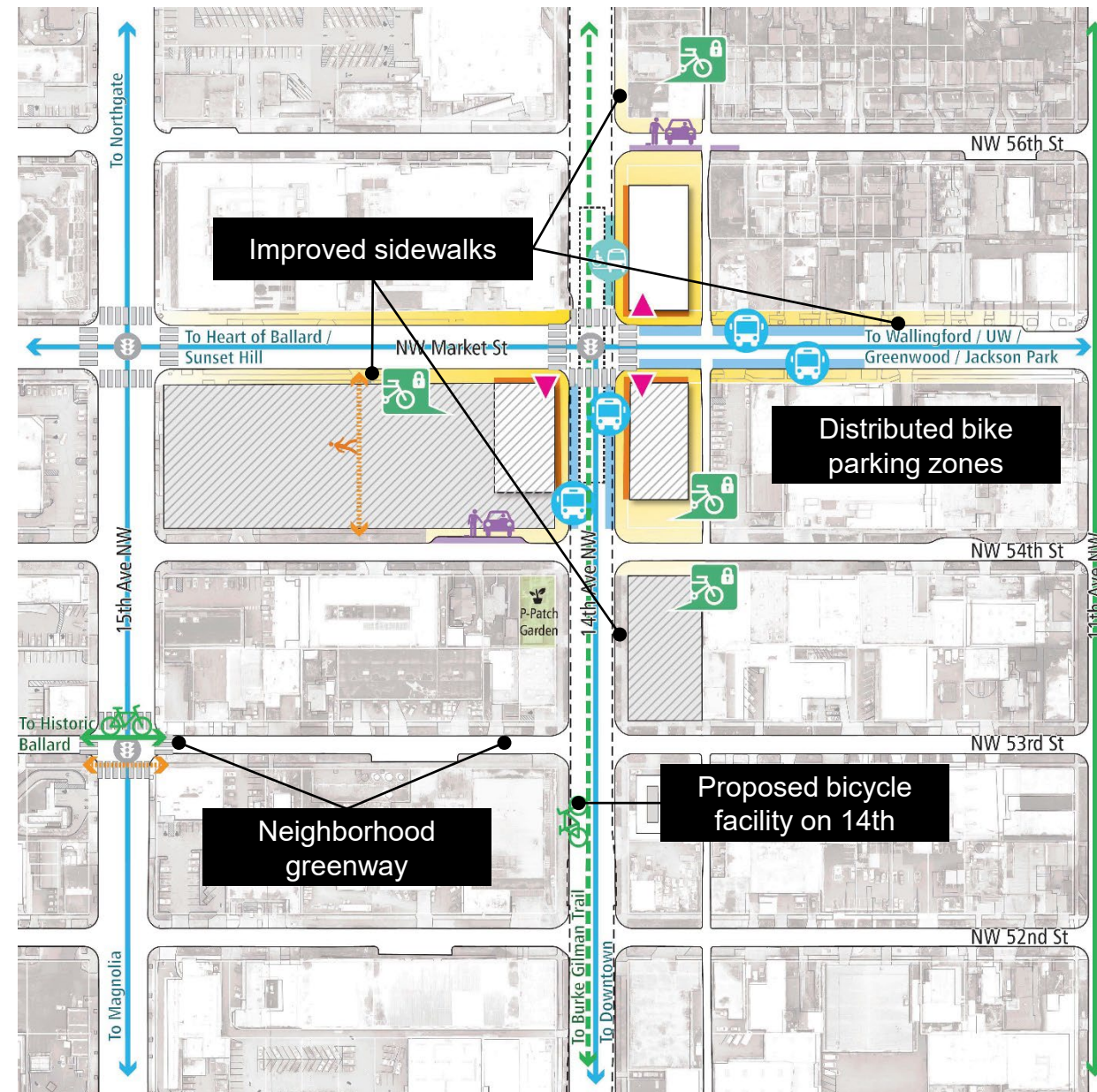
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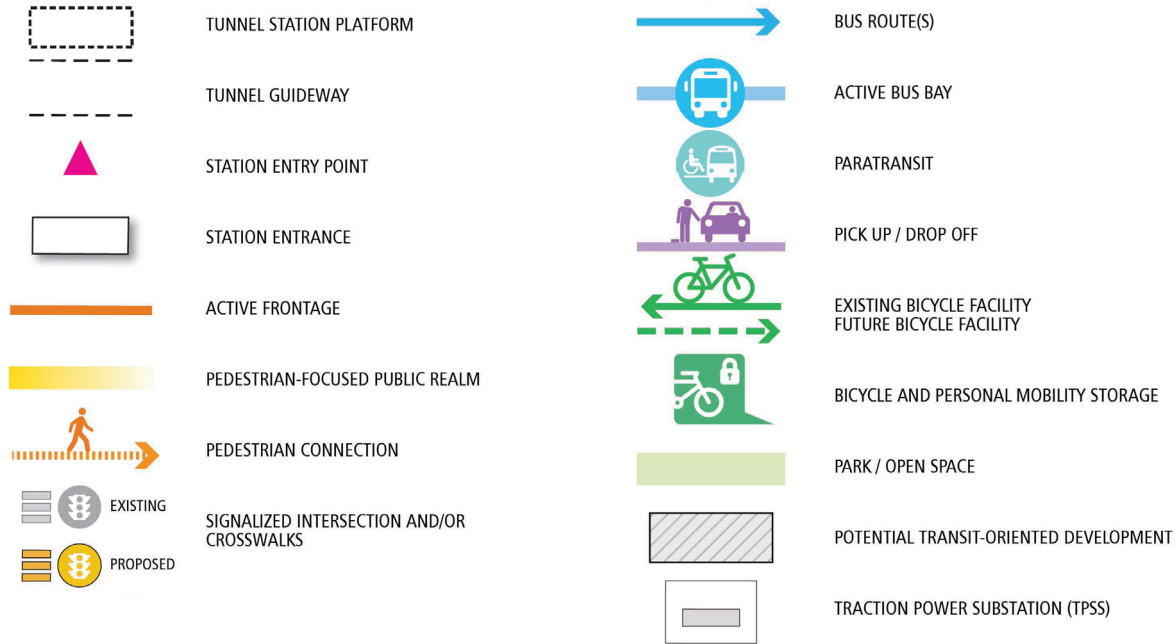
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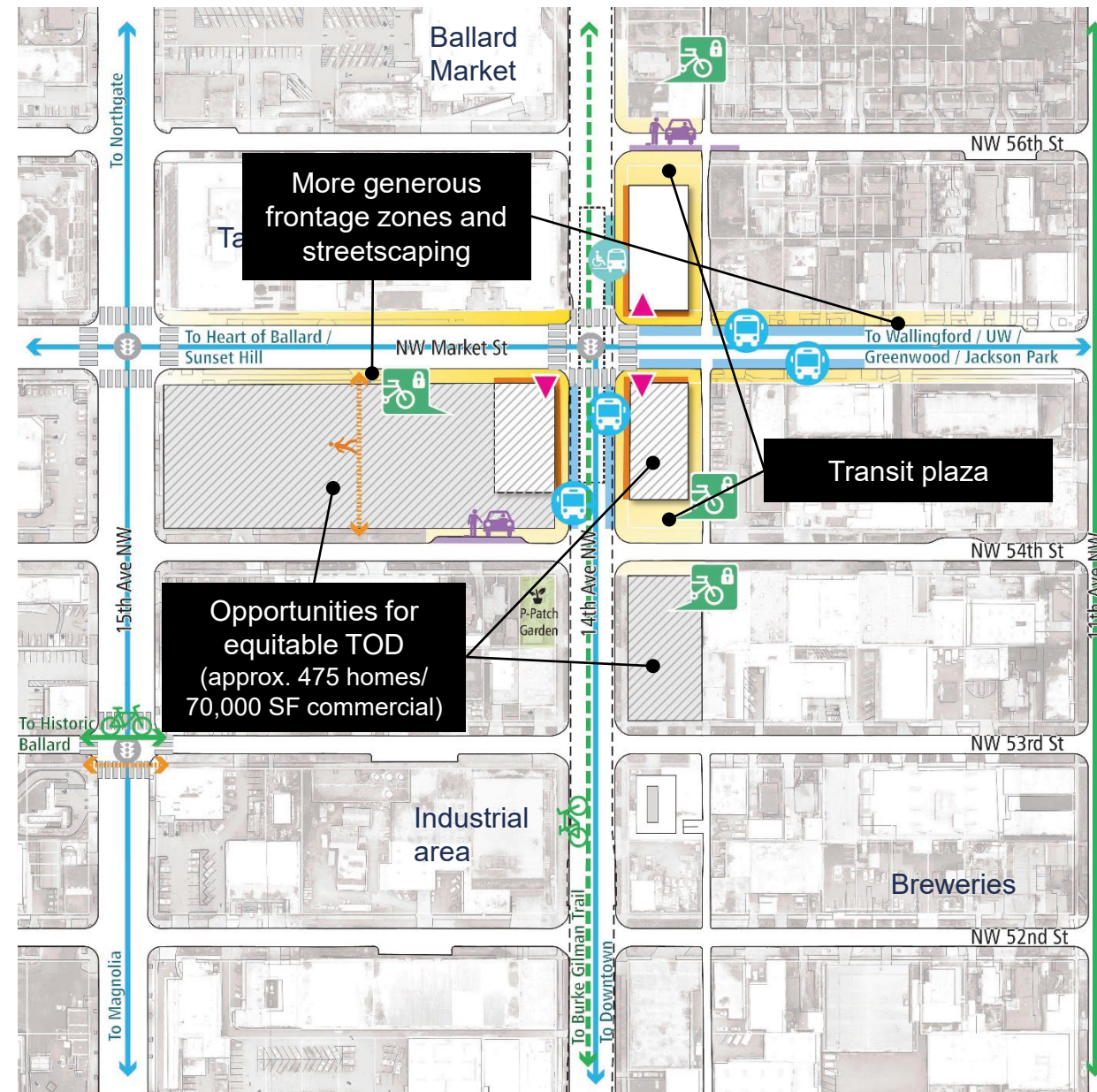
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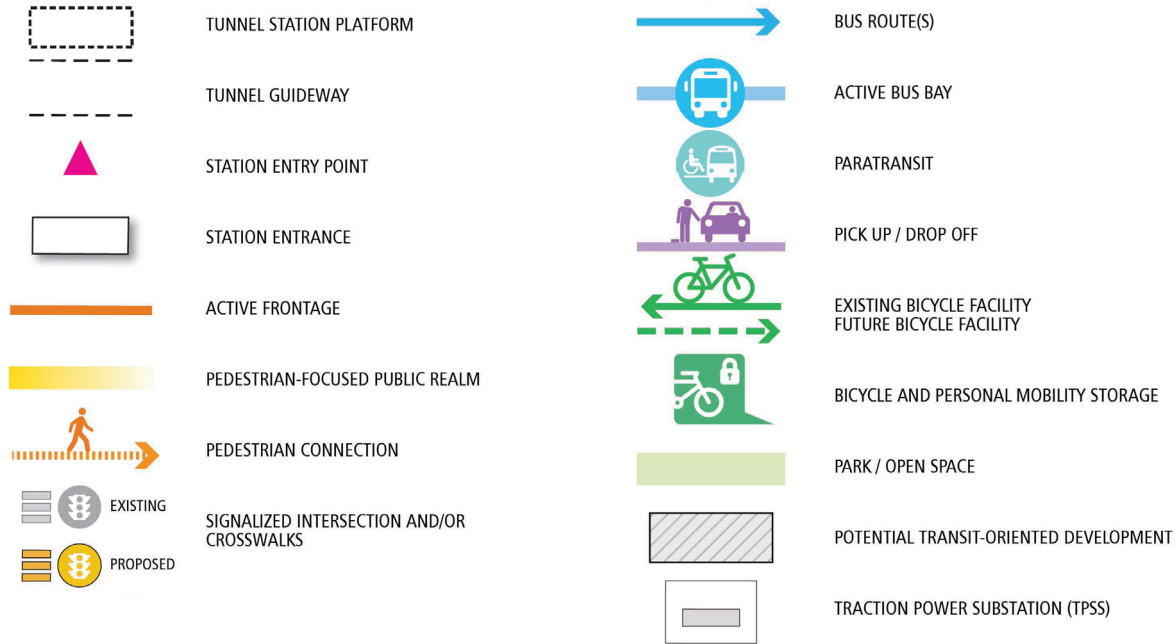
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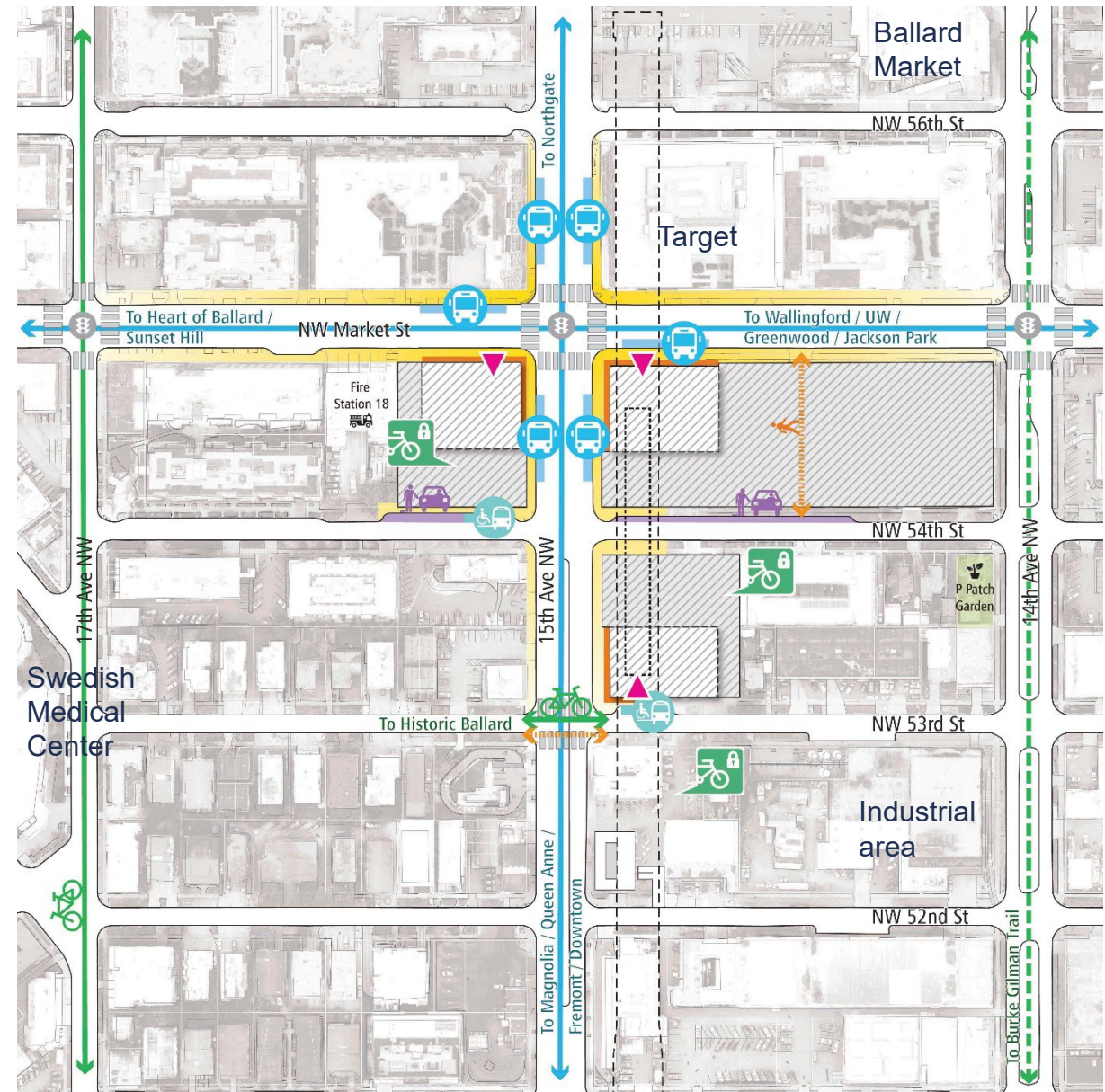
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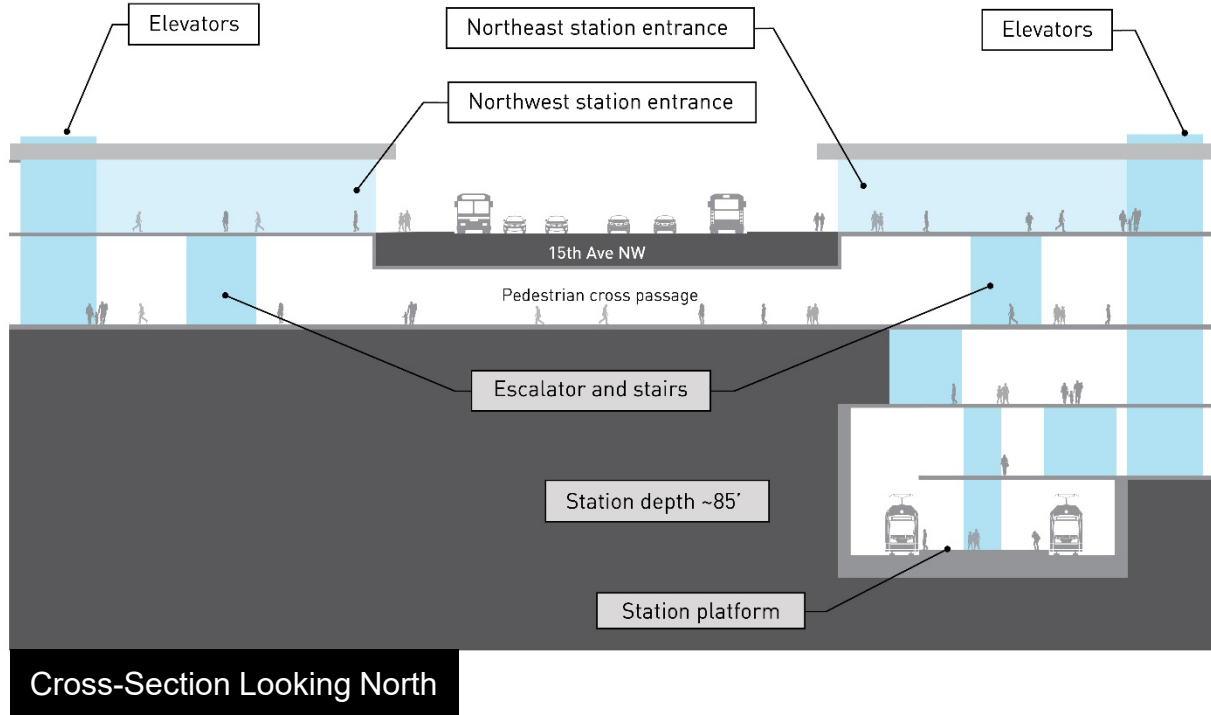
Tunnel 15th Ave Station



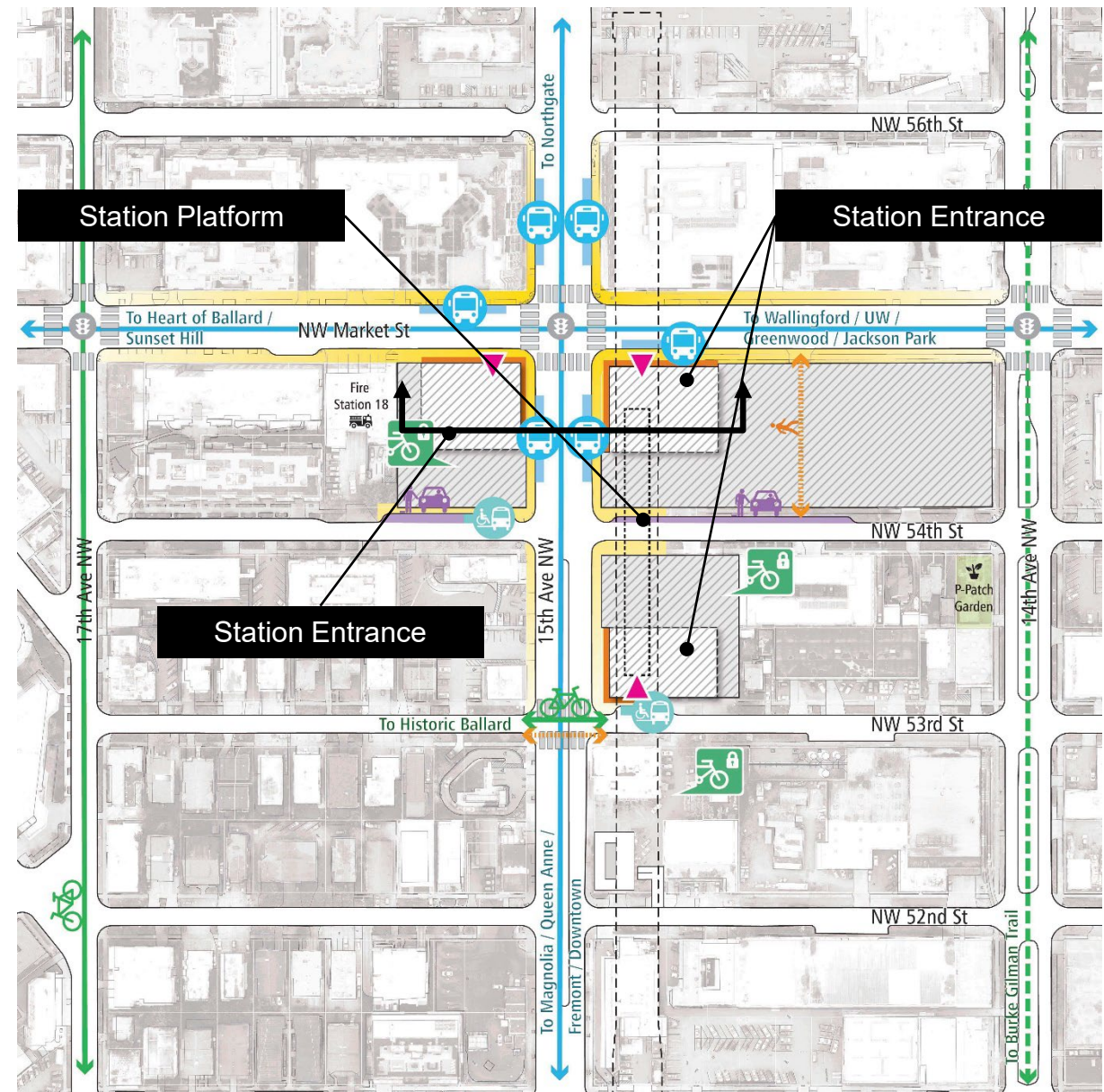
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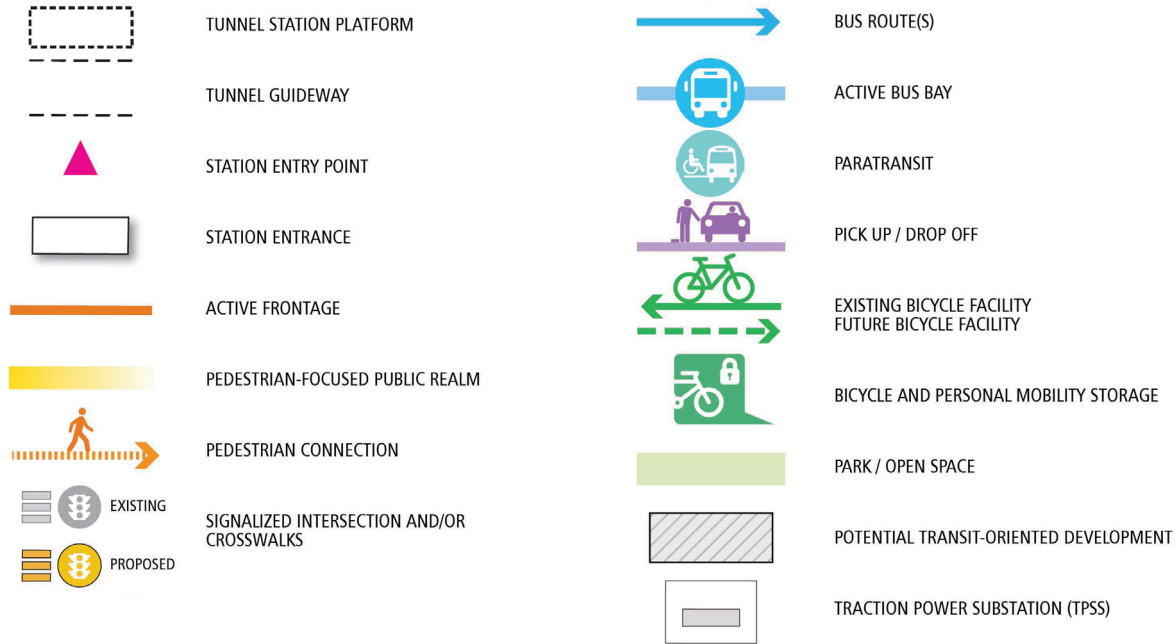
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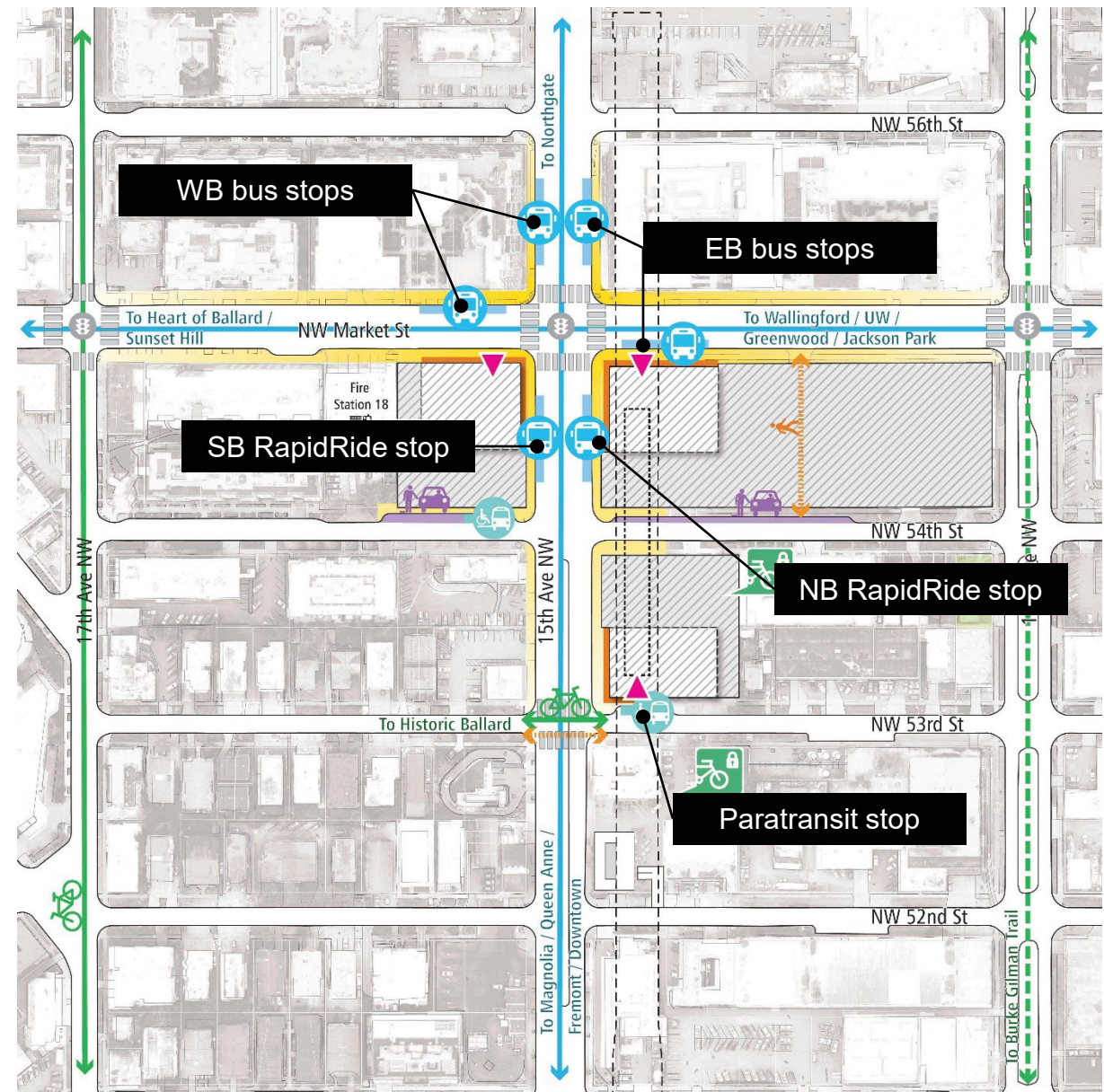
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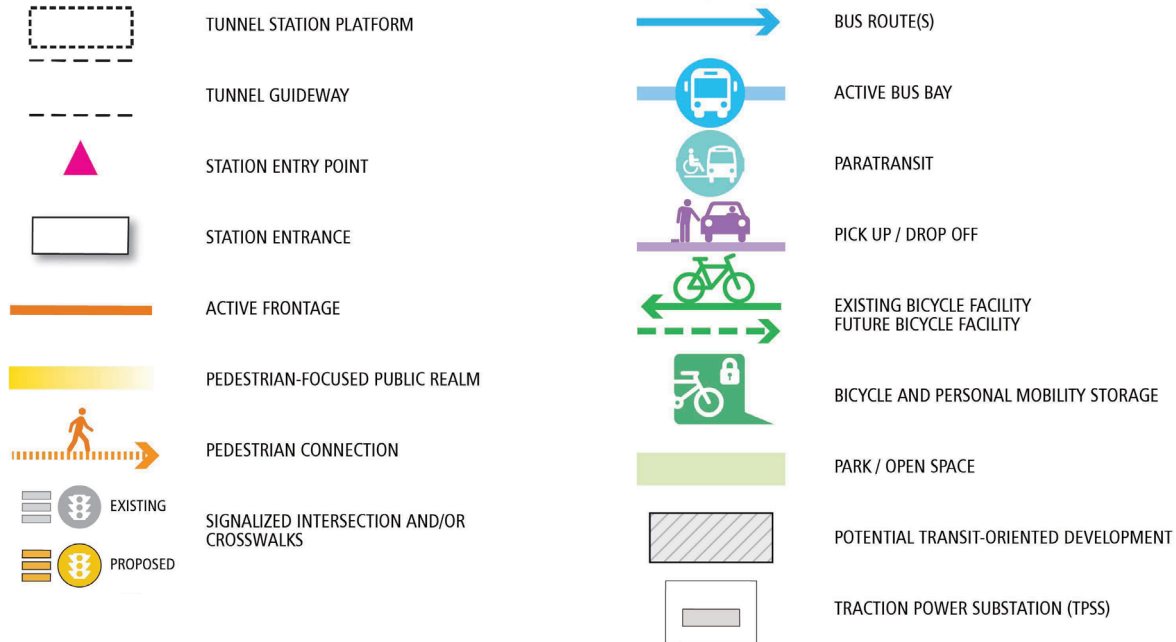
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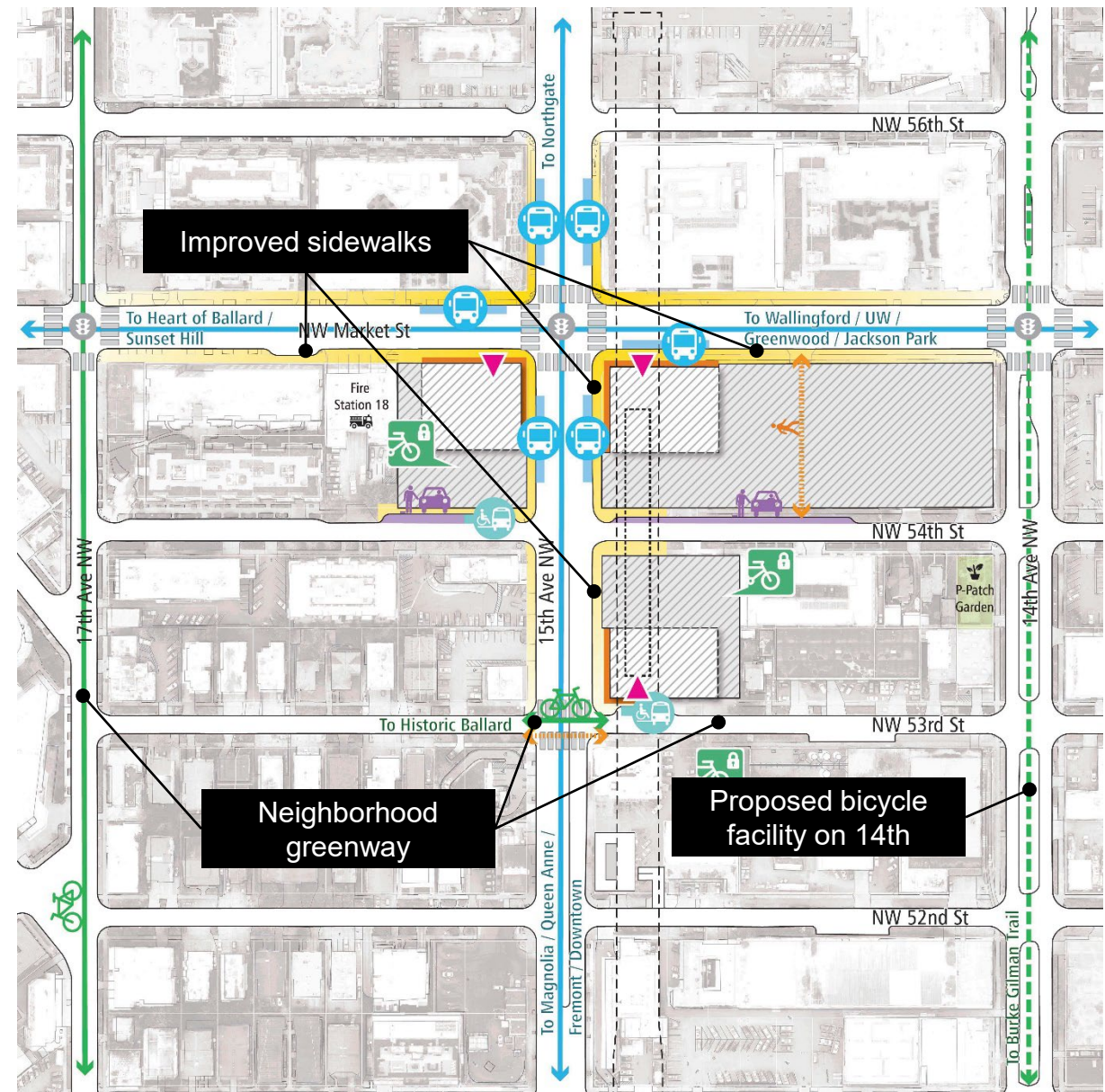
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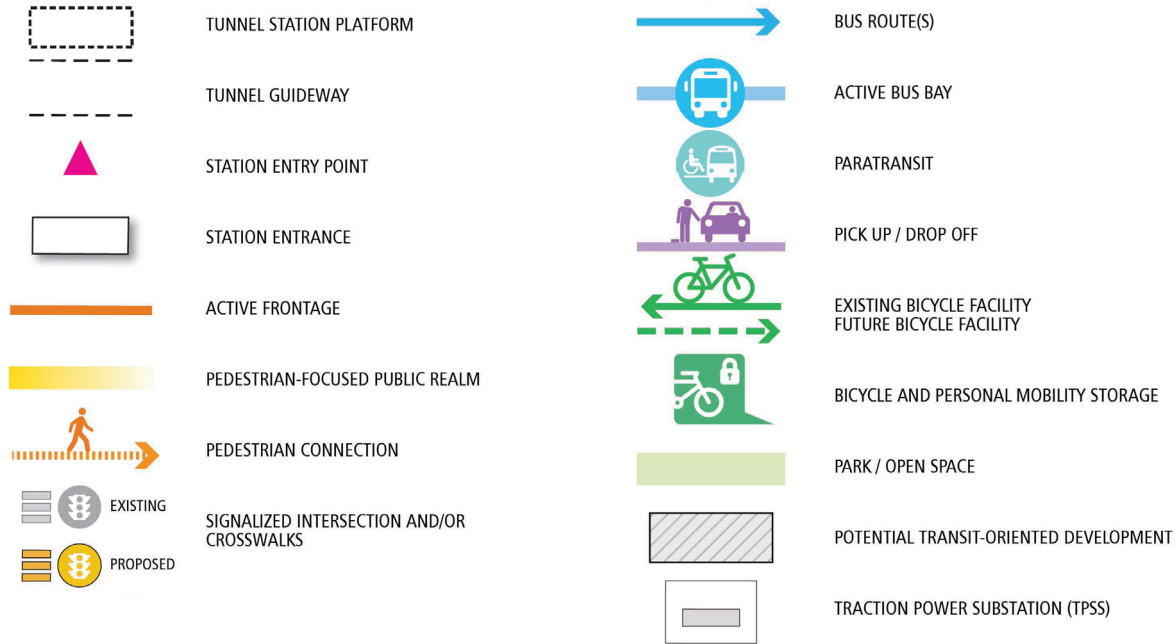
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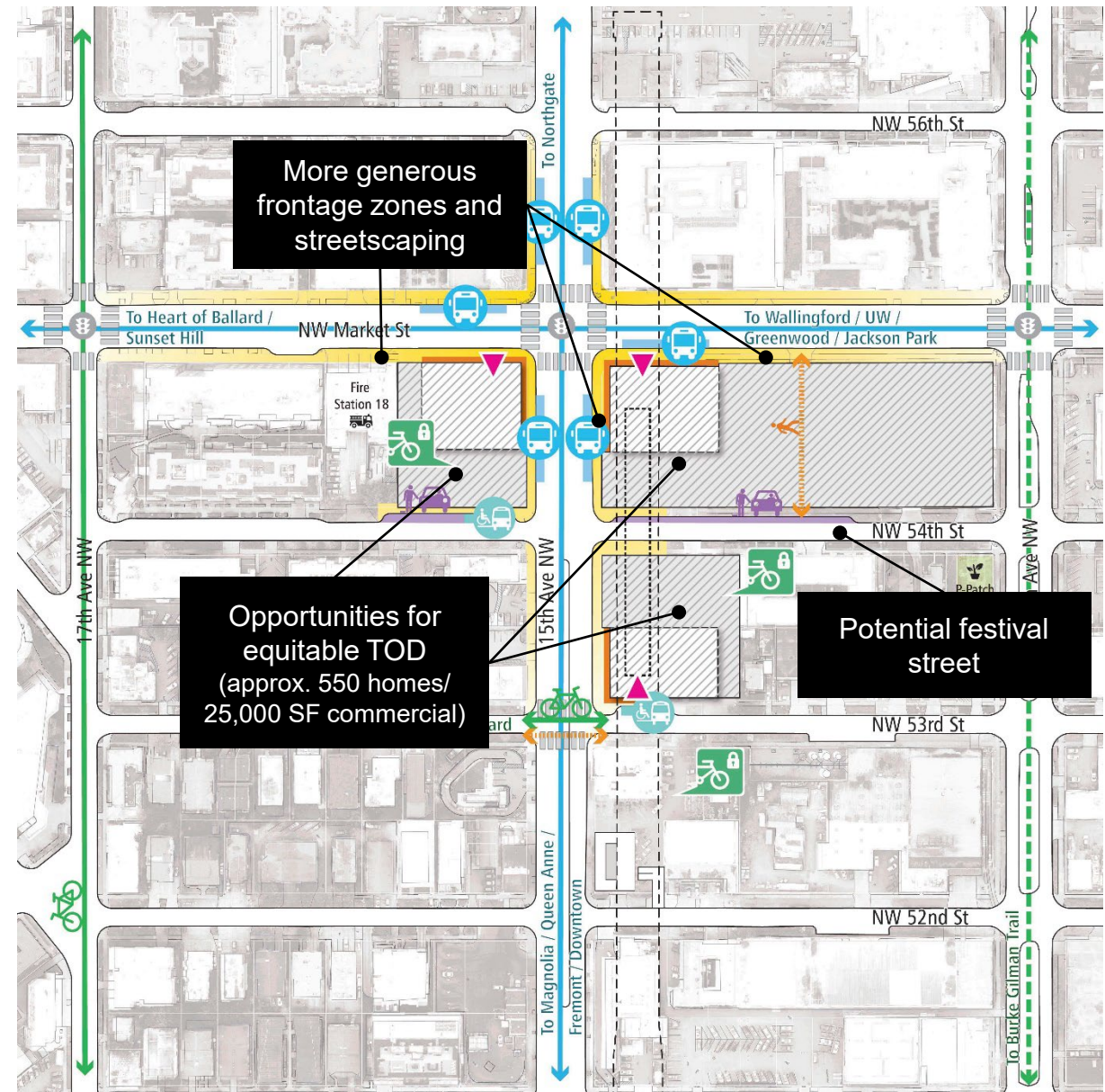
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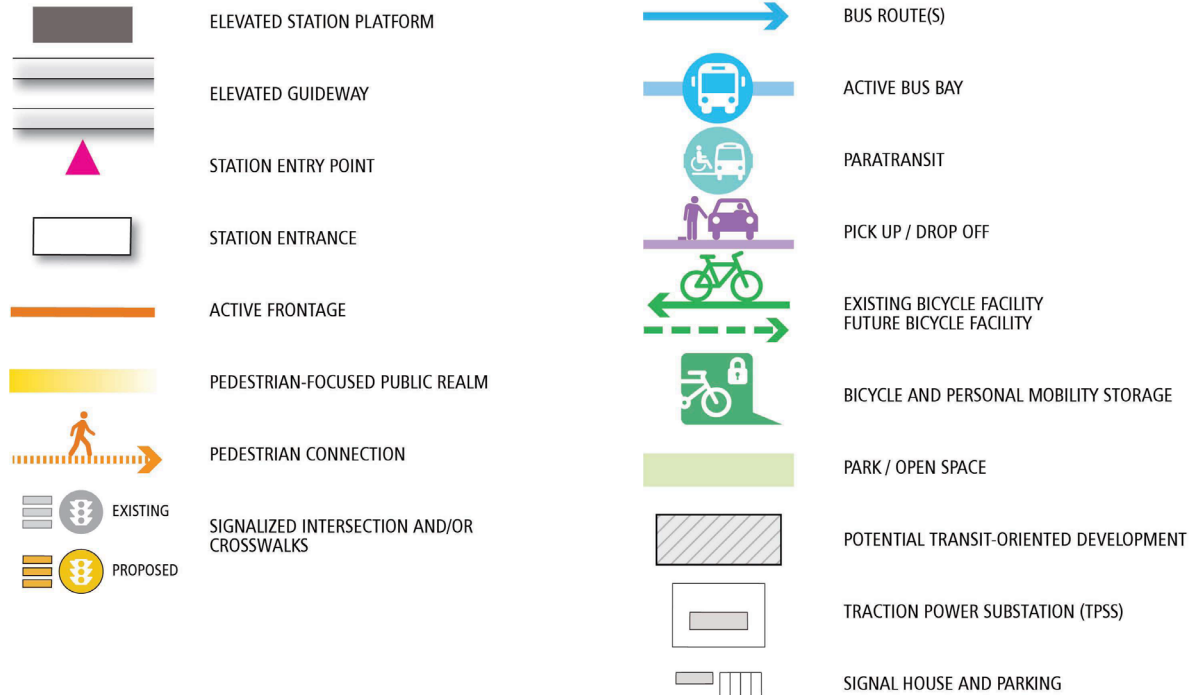


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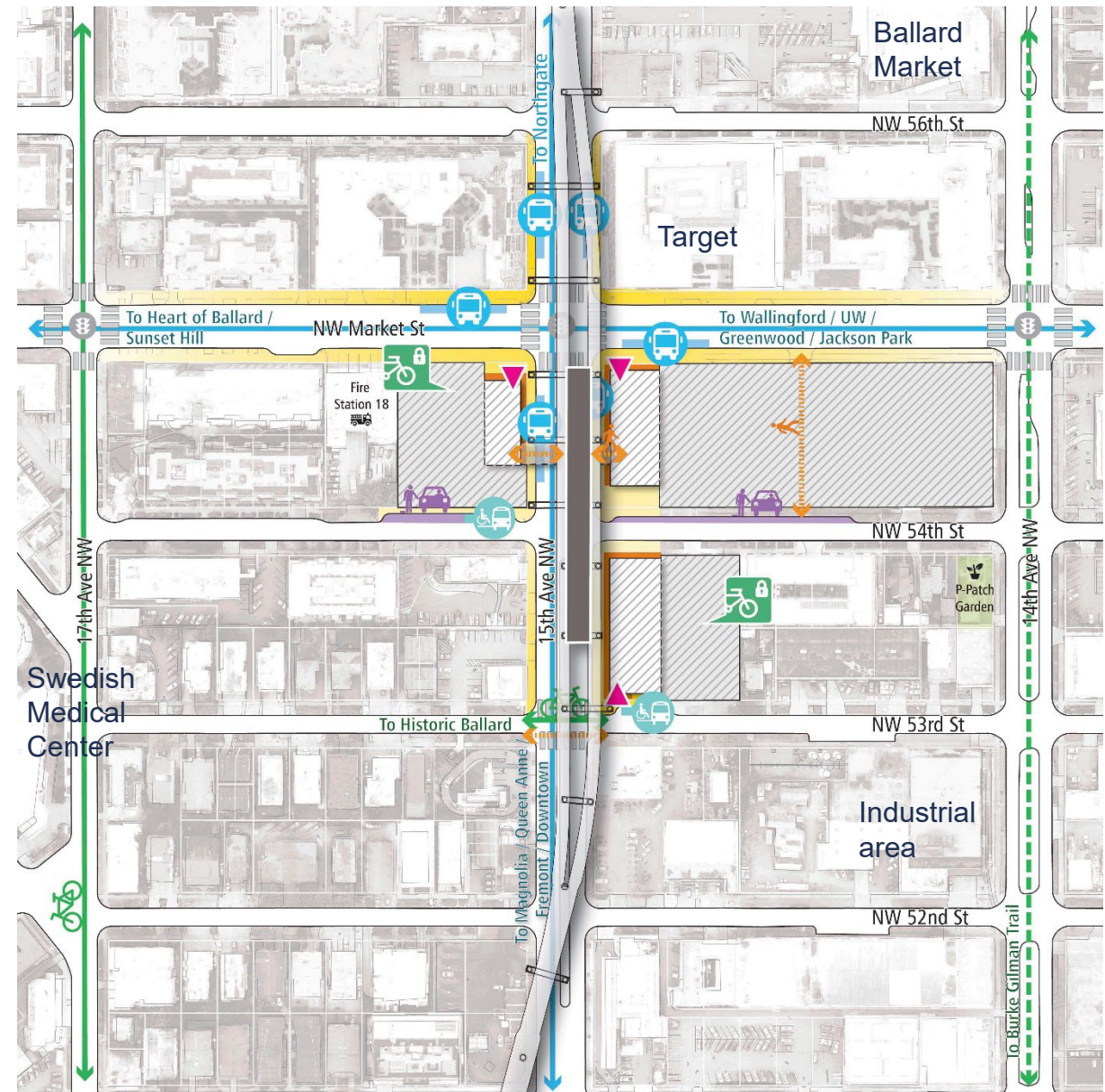


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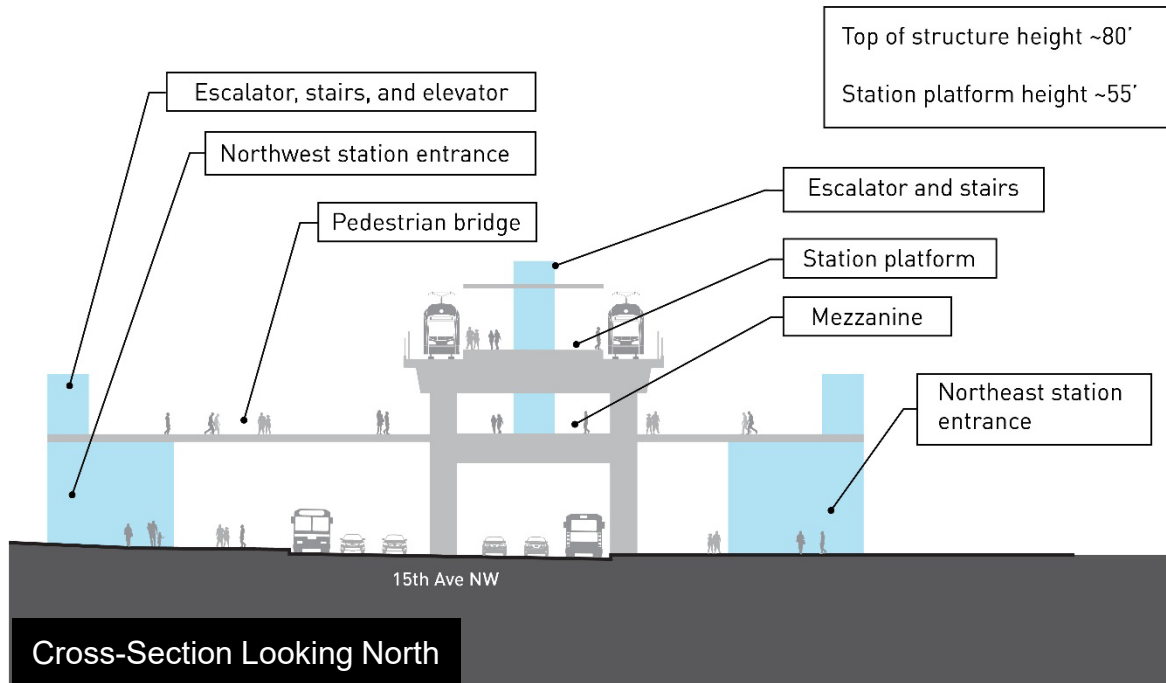
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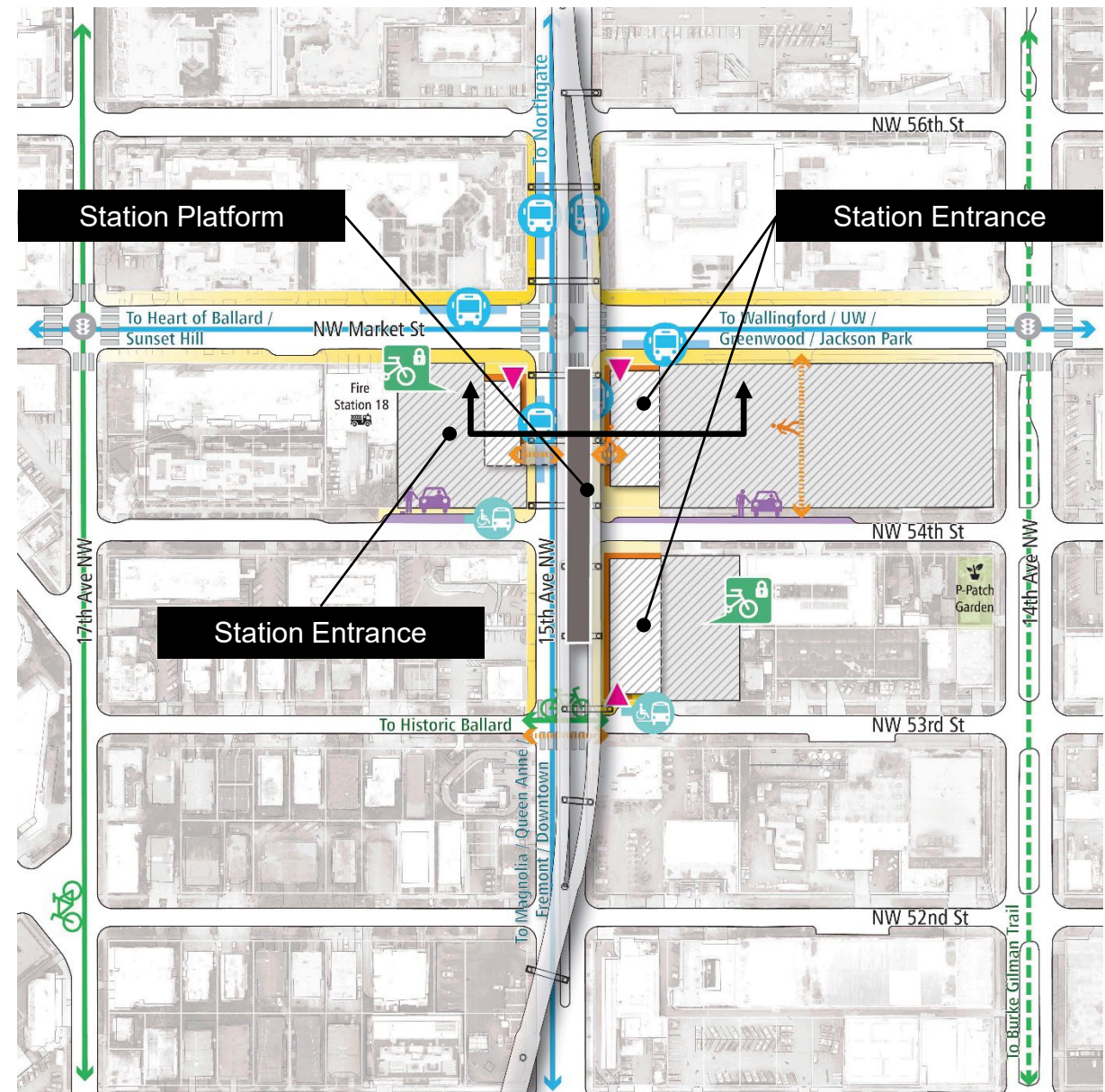
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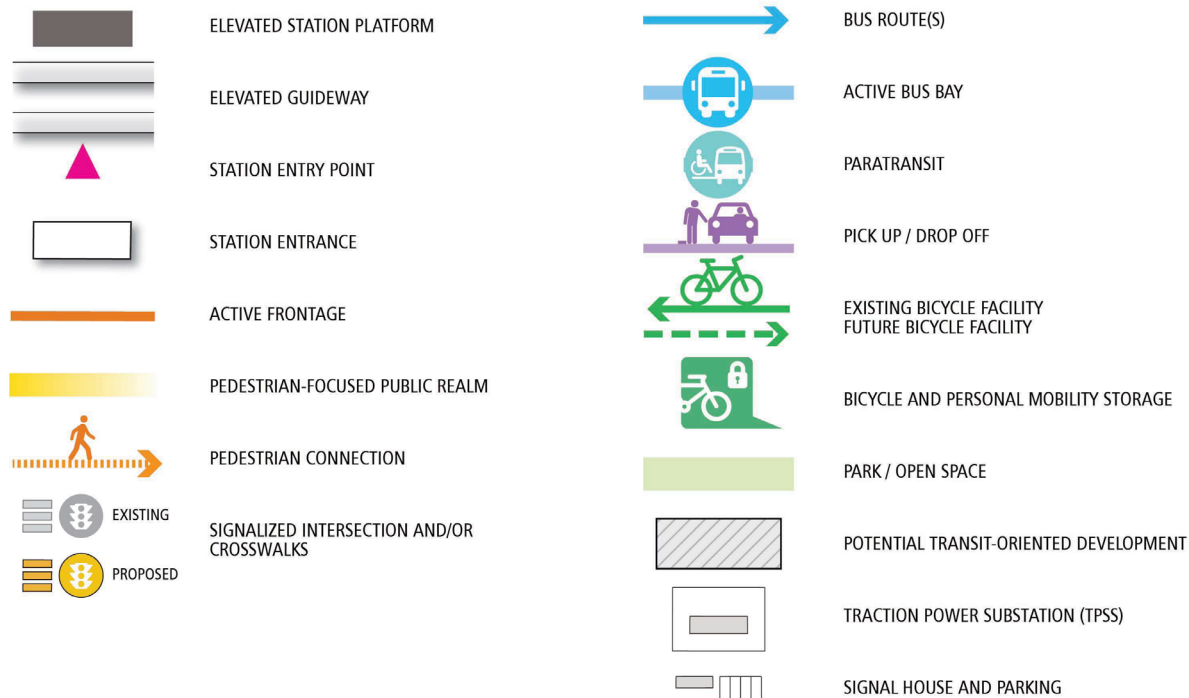
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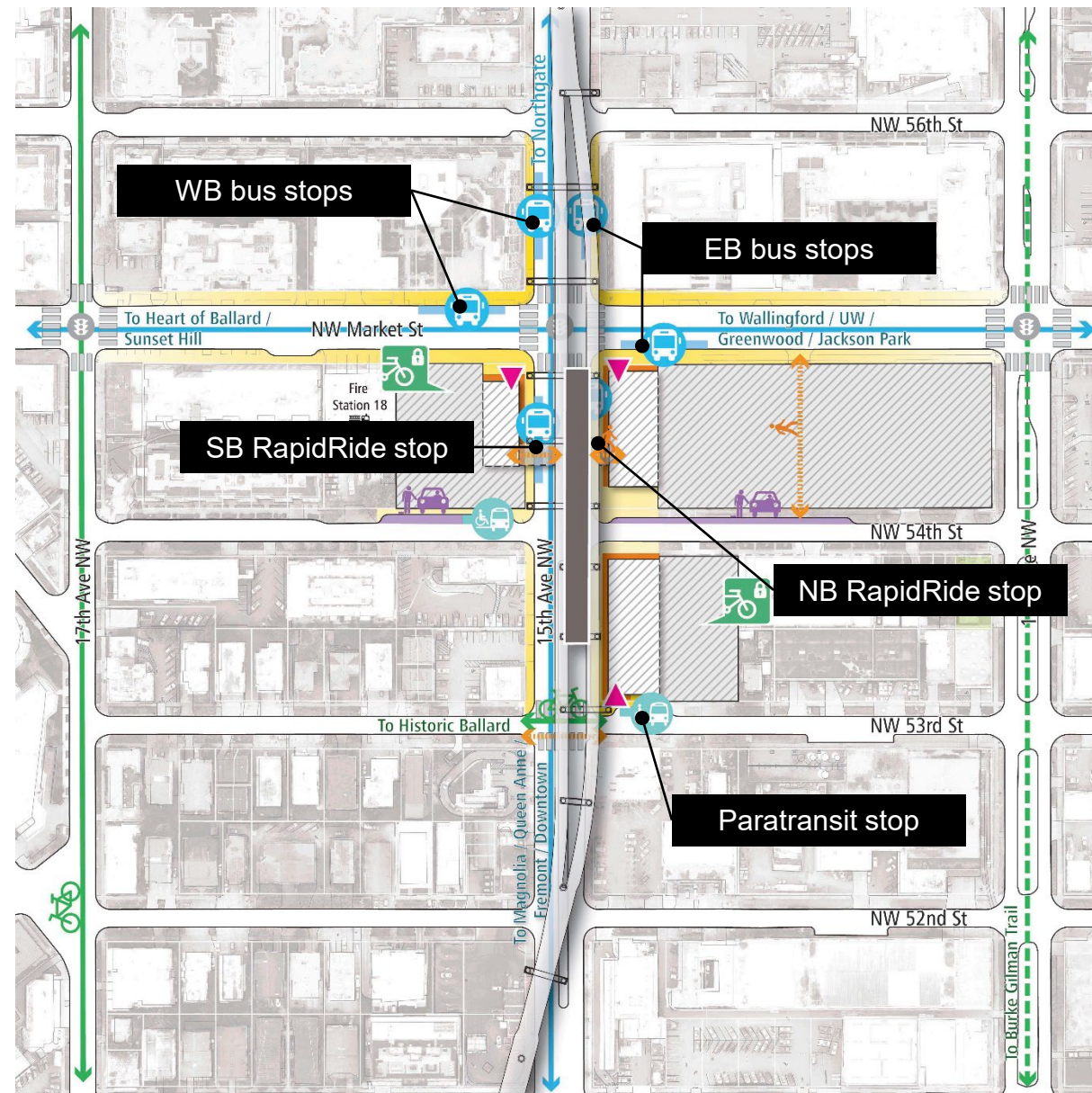
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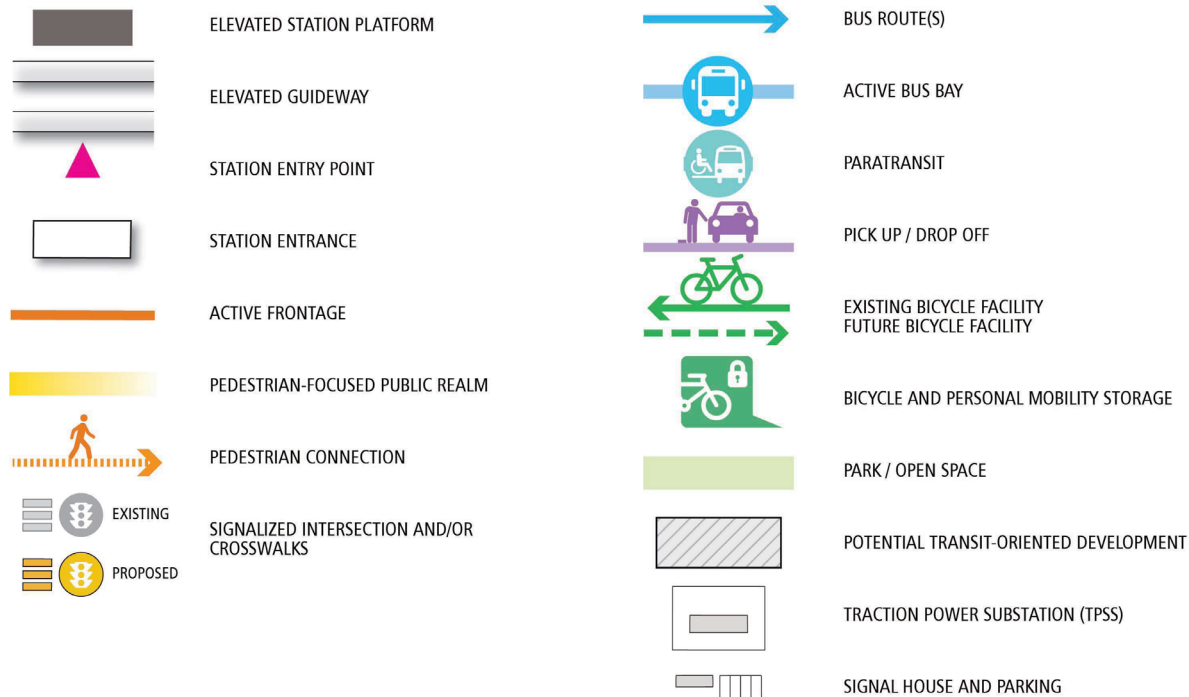
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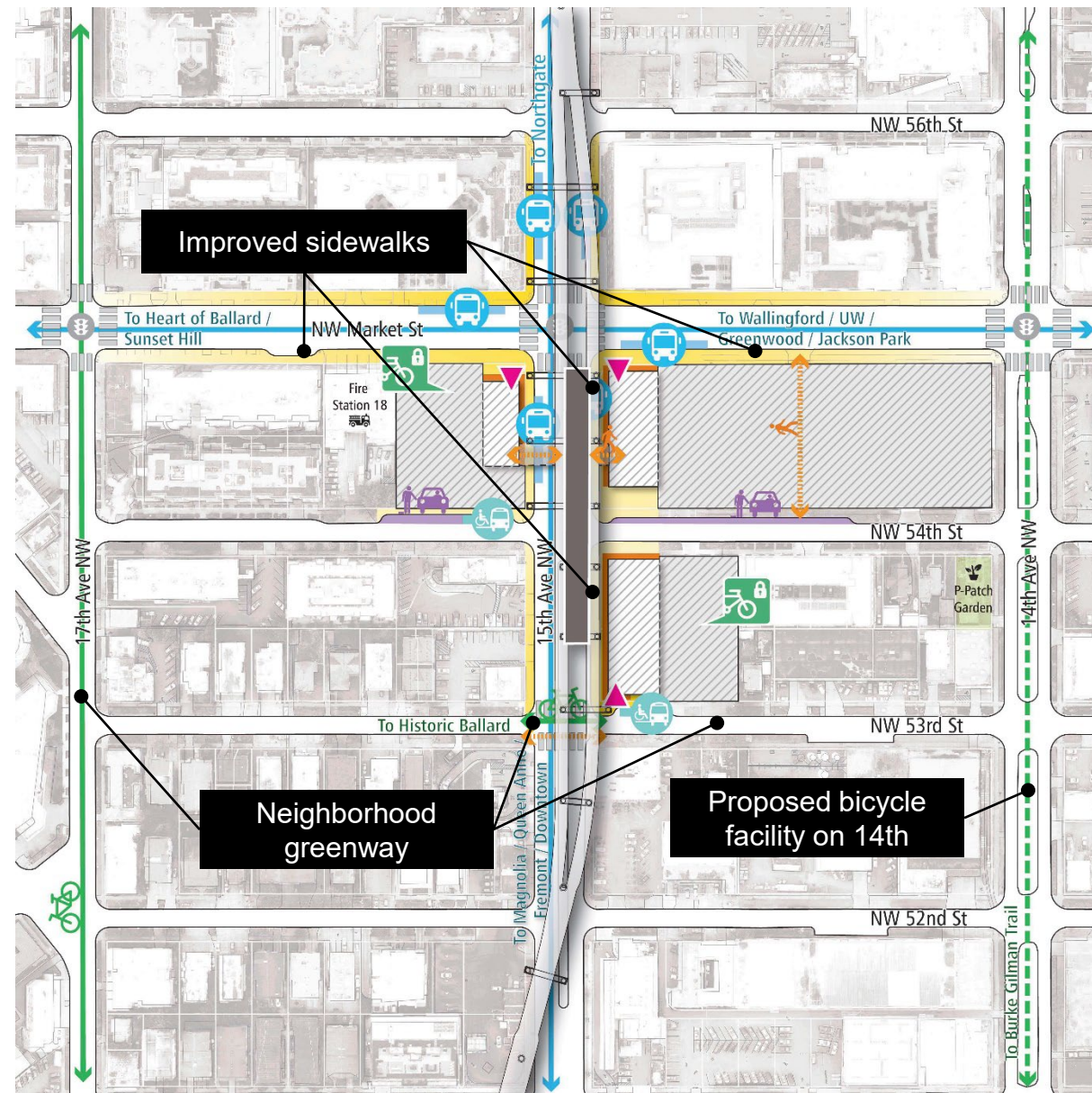
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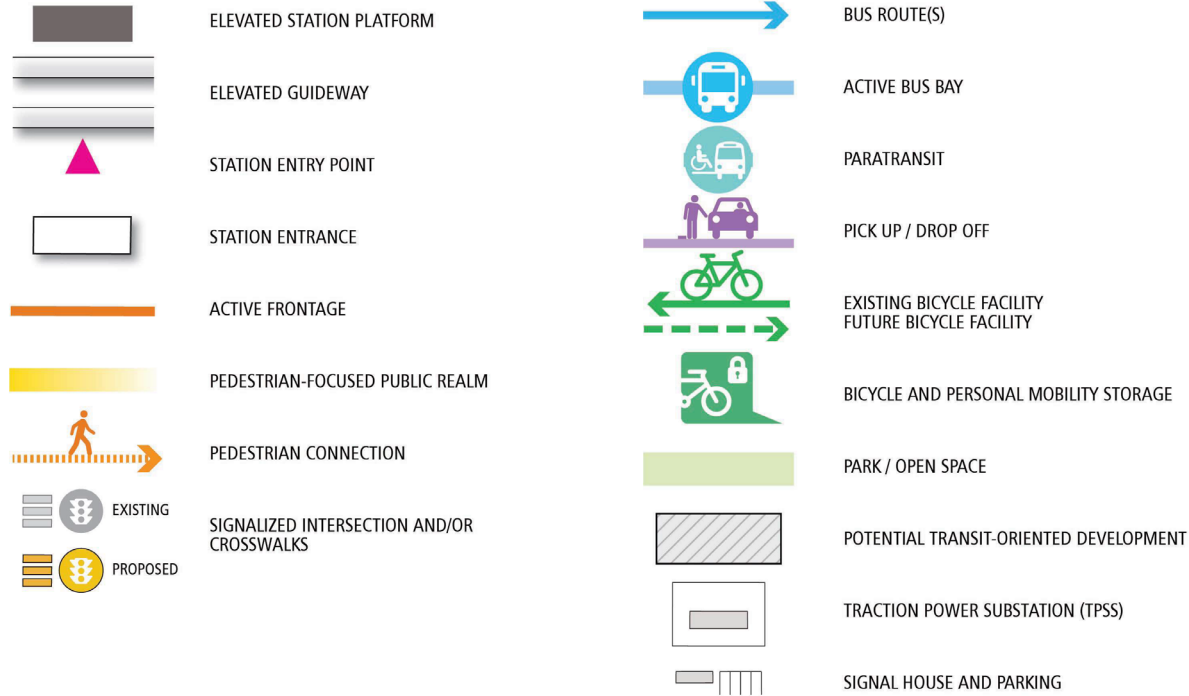
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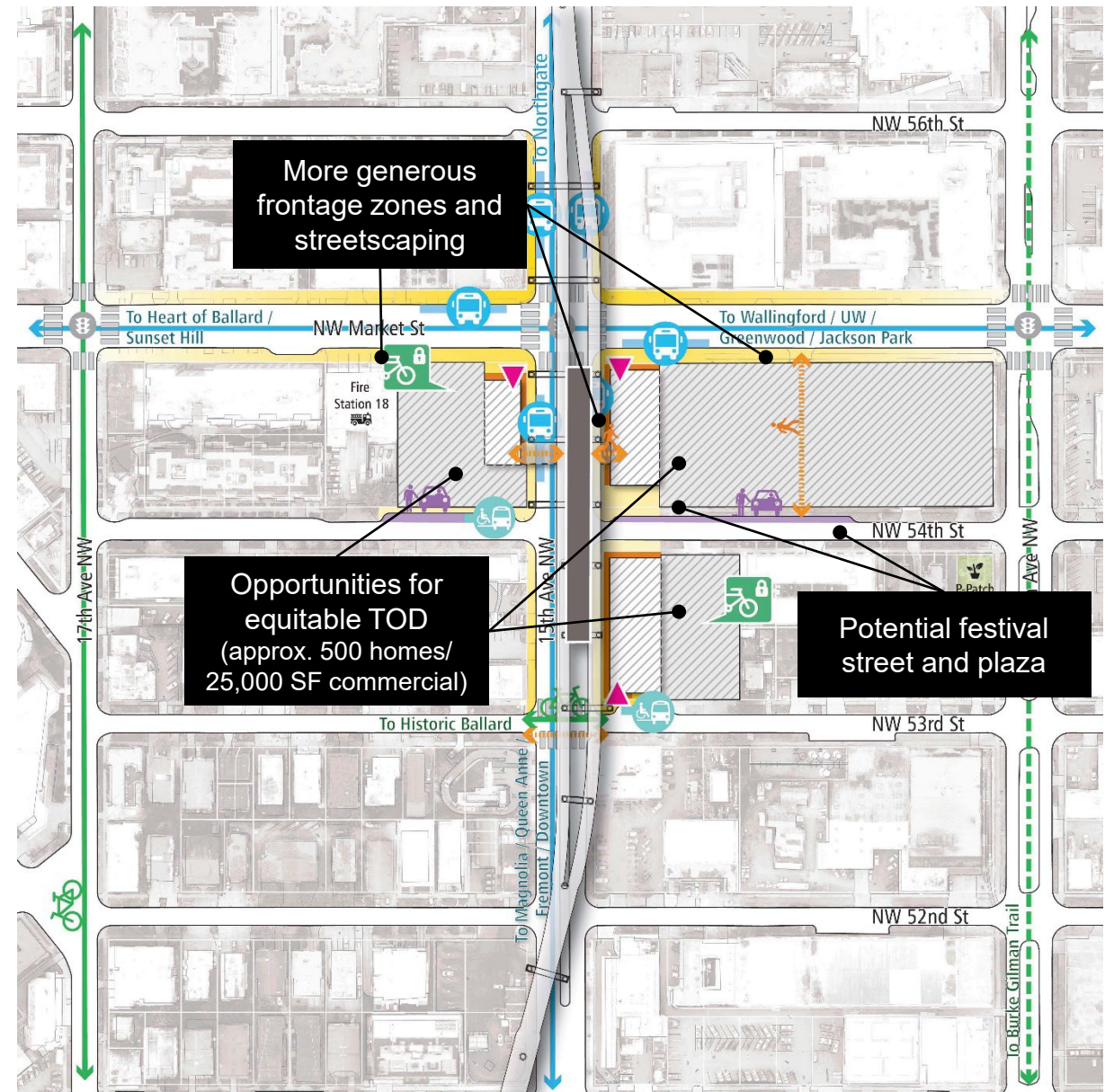
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