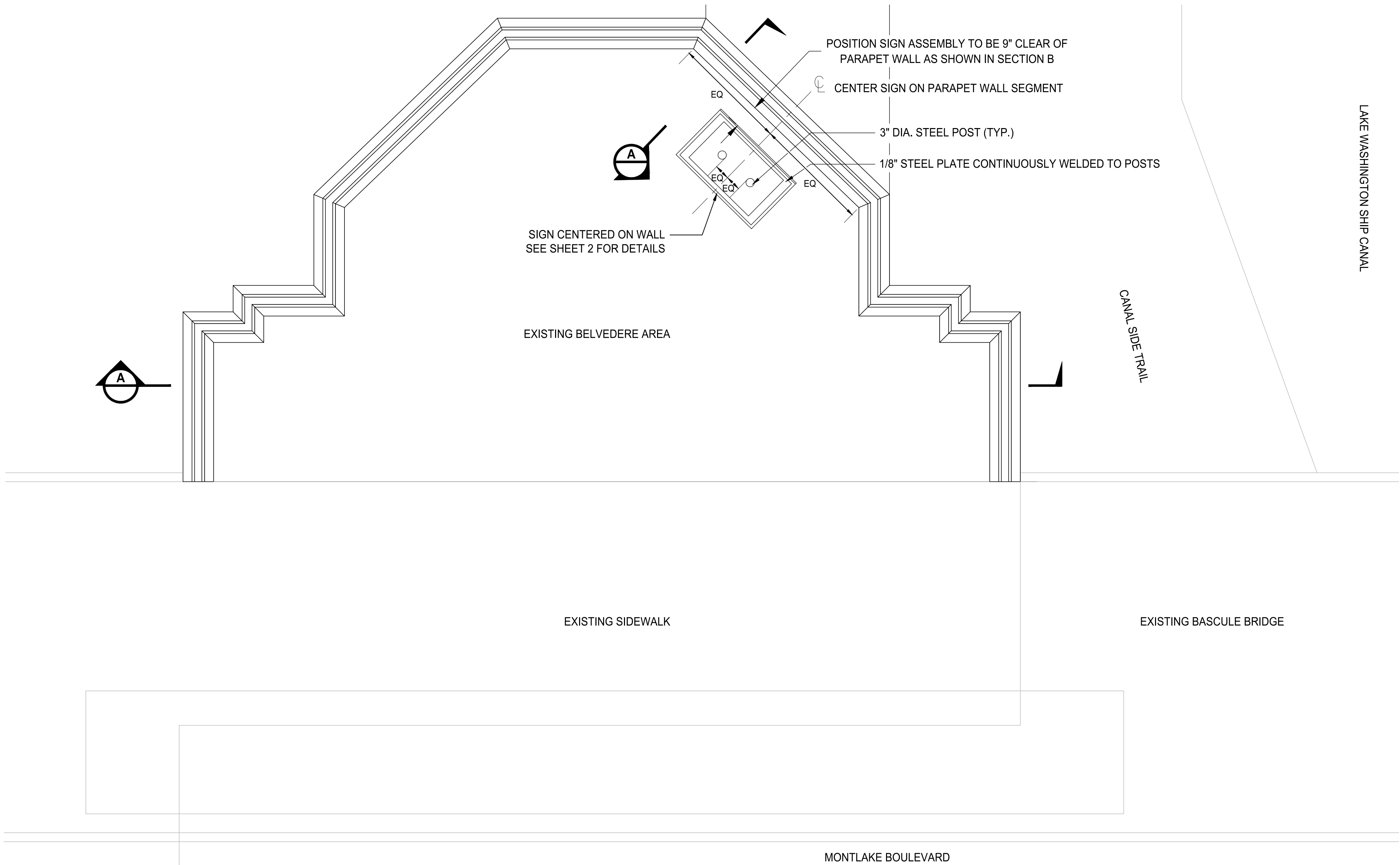


Figure 1. Photograph of Features to Be Altered. Montlake Bascule Bridge, view looking to the northwest.

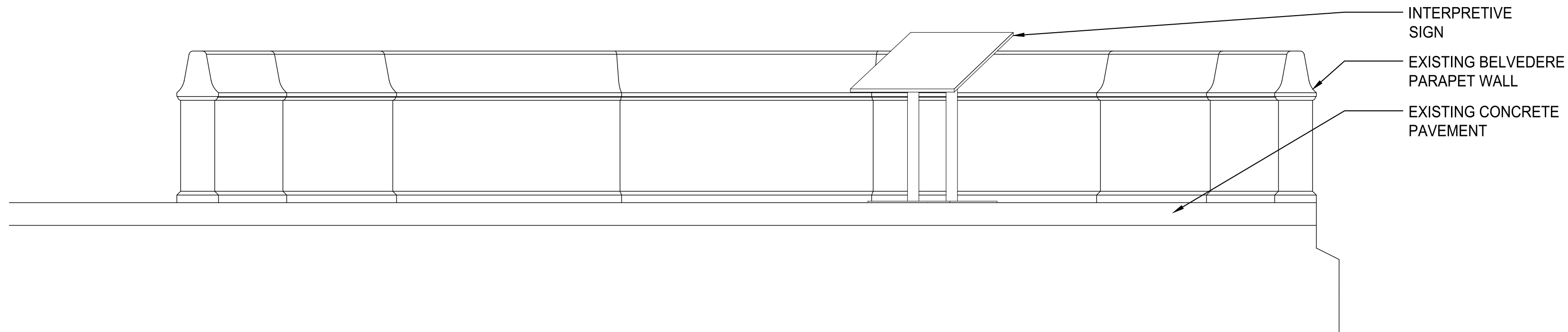


Figure 2. Sign Design Plan – Interpretive Sign Placement Plan & Section. Scaled drawing shows the sign installation layout in relation to the parapet wall, including heights and distance. *(Plans follow on next page)*

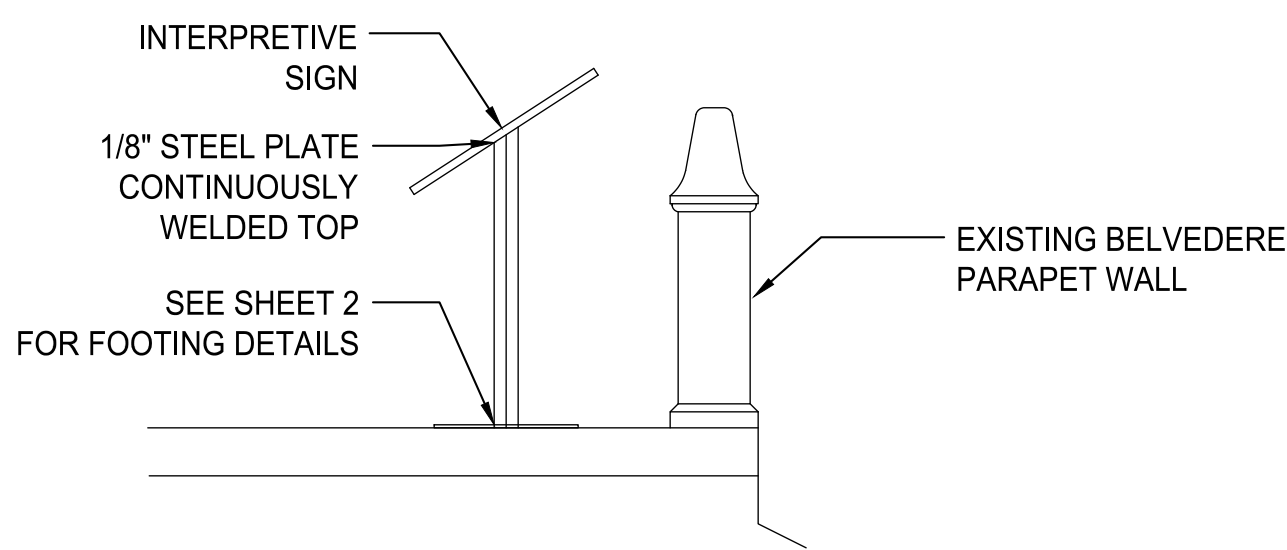
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SIGN PLACEMENT PLAN VIEW



SIGN PLACEMENT SECTION A



SIGN PLACEMENT SECTION B

NOTES:

- ARTWORK WITH FULL BLEED WILL BE PROVIDED BY THE OWNER.
- ANY PENETRATIONS TO CONCRETE SHALL BE PROTECTED. REFER TO WSDOT STANDARD SPECS, 6-02.3(18)A RESIN BONDED ANCHORS.
- SIGN SHALL BE LOCATED 9" CLEAR FROM THE CLOSEST FACE OF PARAPET WALL AS SHOWN IN SECTION B TO ALLOW FOR CLEANING AND MAINTENANCE OF MASONRY. PROVIDE AND INSTALL A MOCKUP OF THE SIGN ASSEMBLY TO VERIFY HORIZONTAL AND VERTICAL LOCATION FOR APPROVAL OF ENGINEER PRIOR TO INSTALLATION OF SIGN ASSEMBLY.
- VERIFY DIMENSIONS OF EXISTING FEATURES IN THE FIELD AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES PRIOR TO COMMENCING WORK.

NOT FOR CONSTRUCTION

REVISIONS	DATE	BY	DESIGNED
			J. SWENSON
			DRAWN
			J. SWENSON
			CHECKED
			APPROVED

ONE INCH AT FULL SCALE.
IF NOT, SCALE ACCORDINGLY

FILE NAME
SIGN INSTALL PLAN

JOB No.
554-1800-068

DATE
JANUARY 2025

PRELIMINARY

Parametrix
ENGINEERING · PLANNING · ENVIRONMENTAL SCIENCES

719 2ND AVENUE, SUITE 200 | SEATTLE, WA 98104
P 206.394.3700
WWW.PARAMETRIX.COM

PROJECT NAME

**SR520 BRIDGE REPLACEMENT
MONTLAKE BRIDGE
INTERPRETIVE SIGNAGE**

SEATTLE, WA

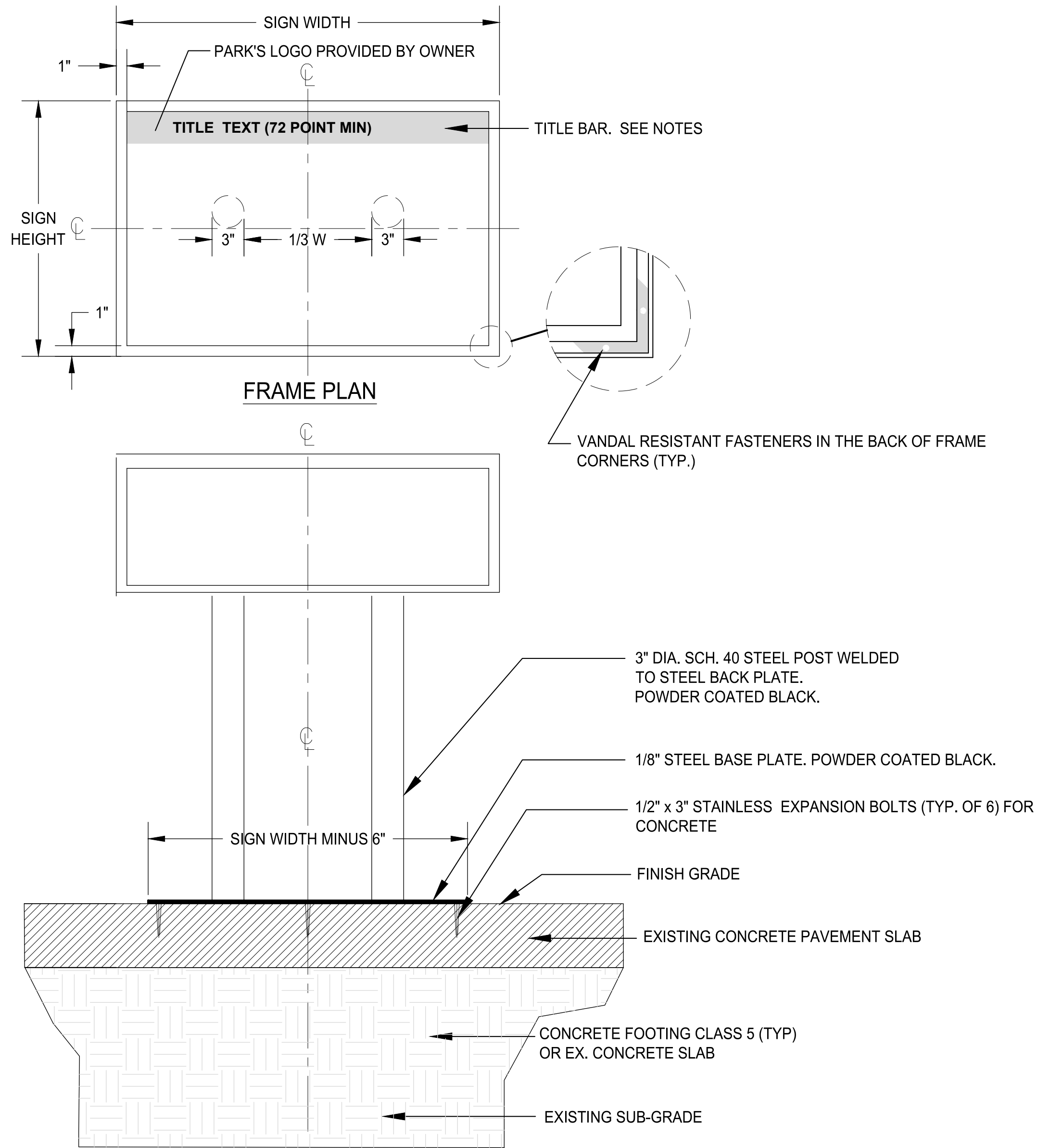
**INTERPRETIVE SIGN PLACEMENT
PLAN & SECTION**

DRAWING NO.
1 OF 2

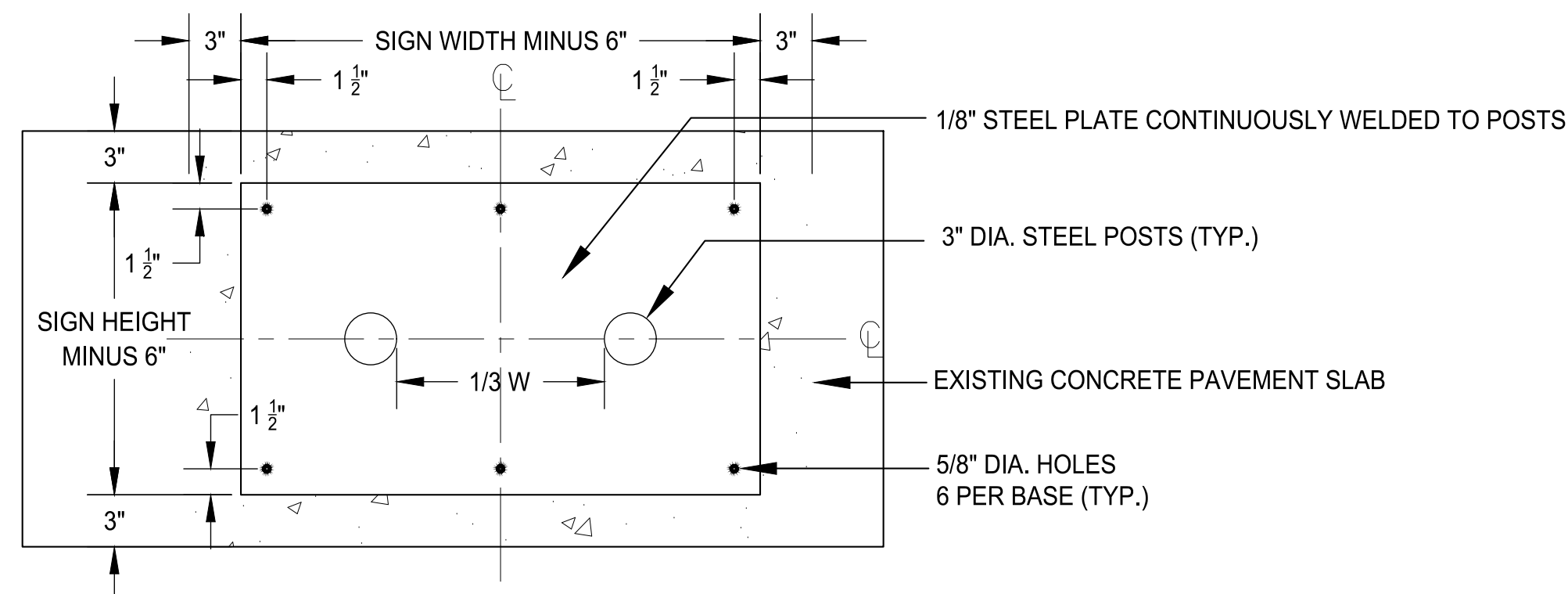
1

Figure 3. Sign Design Plan - Interpretive Sign Install Details. Scaled drawing shows installation, including bolt down footing details and existing concrete conditions. (*Plans follow on next page*)

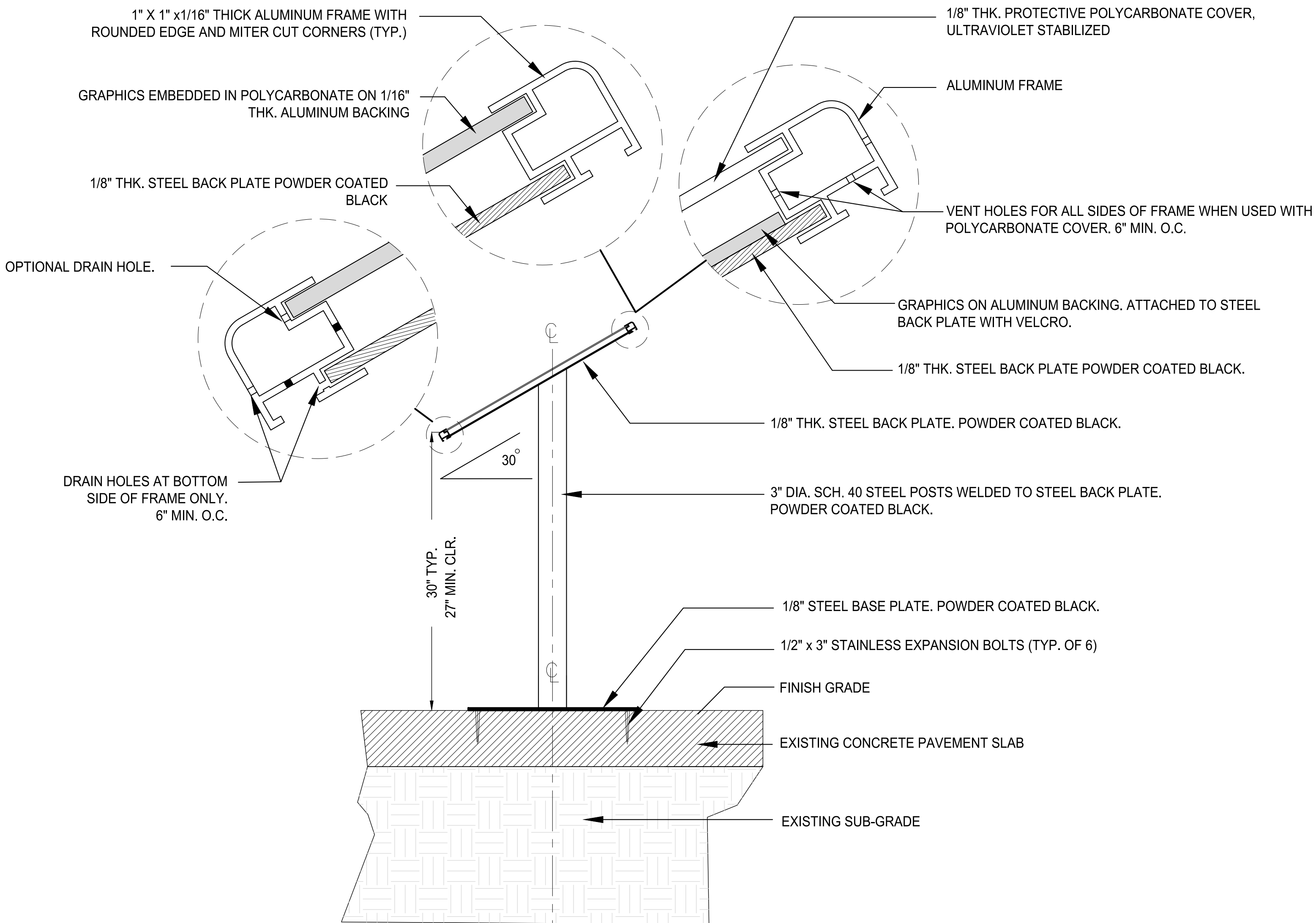
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PLOTED BY: swensjgn DATE: Tuesday, January 21, 2025 8:17:30 AM



FRONT ELEVATION
BOLT DOWN FOOTING METHOD



FOOTING PLAN
BOLT DOWN FOOTING METHOD



SIDE ELEVATION
BOLT DOWN FOOTING METHOD

NOTES:

- ANY PENETRATIONS TO CONCRETE SHALL BE PROTECTED. REFER TO WSDOT STANDARD SPECS, 6-02.3(18)A RESIN BONDED ANCHORS.
- PANEL SIZE (WxH): 36"x24".
- NO FIELD WELDING.
- CONTRACTOR SHALL ENSURE THAT DRAIN/VENT HOLES ARE DRILLED AS SPECIFIED.
- CONTRACTOR / OWNER SHALL ENSURE THAT THE GRAPHIC PANEL TYPE MEETS THE SPECIFICATIONS.
- DELIVER THE FOLLOWING ITEMS TO THE OWNER: FRAME TOUCH UP PAINT, EXTRA VANDAL-RESISTANT FASTENERS AND BIT/TOOL REQUIRED TO INSTALL / REMOVE THE FASTENERS.
- THE COMPLETE SYSTEM, EXCEPT CONCRETE FOOTING, IS AVAILABLE FROM SOLE SOURCE PROPRIETER: ARTCRAFT DISPLAY GRAPHICS INC. #112-1533 BROADWAY ST., PORT CONQUITLAM, B.C., CANADA V3C 6P3. PHONE 1-800-994-9451. WWW.ARTCRAFT.COM (OR APPROVED EQUAL.)
- EQUAL PRODUCT SHALL BE VANDAL RESISTANT AND GRAFFITI RESISTANT. GRAPHIC PANEL SHALL BE REPLACEABLE, WATERPROOF AND NON-FADING. MATERIALS SHALL BE RECYCLABLE. PROVIDE 1400 DPI PRINTING PROCESS.
- PROVIDE FINAL GRAPHIC FILES VIA ELECTRONIC FILE TRANSFER TO OWNER WITH RECORD DOCUMENT AT PROJECT CLOSE OUT.
- PROVIDE TEN YEAR MANUFACTURER'S MATERIAL AND WORKMANSHIP WARRANTY ON ALL PRODUCTS.

NOT FOR CONSTRUCTION

REVISIONS	DATE	BY	DESIGNED
			J. SWENSON
			J. SWENSON
			CHECKED
			APPROVED

ONE INCH AT FULL SCALE. IF NOT, SCALE ACCORDINGLY
FILE NAME SIGN_INSTALL_PLAN
JOB No. 554-1800-068
DATE JANUARY 2025

PRELIMINARY

Parametrix ENGINEERING · PLANNING · ENVIRONMENTAL SCIENCES
719 2ND AVENUE, SUITE 200 SEATTLE, WA 98104 P 206.394.3700 WWW.PARAMETRIX.COM

PROJECT NAME SR520 BRIDGE REPLACEMENT MONTLAKE BRIDGE INTERPRETIVE SIGNAGE SEATTLE, WA
--

INTERPRETIVE SIGN INSTALL DETAILS
--

DRAWING NO. 2 OF 2
2

Figure 4. Proposed Color Samples: Rendering of Interpretive Sign, version 1, view looking to the northwest.



Figure 5. Proposed Color Samples: Rendering of Interpretive Sign, version 2, view looking to the northwest.



Figure 6. Proposed Color Samples: Rendering of Interpretive Sign, version 3, view looking north.



Figure 7. Proposed Color Samples: Interpretive sign graphic design.

A PROSPEROUS PORTAGE

HISTORIC DISTRICT

THE MONTLAK BRIDGE

The first automobile passed over the Montlake Bridge in 1925. Designed by Seattle Engineering Department, with consultation from Seattle architects Edgar Blair, Harland Thomas, and A. H. Albertson. The bridge was constructed by the City of Seattle at a cost of \$670,000. Together, the Montlake Bridge and Montlake Cut are a City of Seattle Landmark.

EVOLUTION OF THE MONTLAK CUT

The land between Lake Washington and Lake Union has always been a transportation hub. Native Peoples used trails and local waterways as their highways and called this location sxWátSadweehL ("carry a canoe") as it was an important connection between the two bodies of water.

The Lake Washington Ship Canal, a navigable waterway for commercial vessels to transport goods and materials from inland to the Puget Sound, was proposed as early 1854. In 1883, the Lake Washington Improvement Company hired Chinese immigrants from the Wa Chong Company to hand dig a canal between Union Bay and Portage Bay, known as the Montlake Ditch. The ditch followed a diagonal route in roughly the same location as SR 520, south of where the Montlake Cut is located today.

As industry flourished along Lake Union, traffic demands soon overwhelmed the small portage canal. Construction of the Montlake Cut began in 1909 and was completed in 1916. Use of the canal has become diversified over time. Since 1920, the waterway has been the venue of the annual Seattle Yacht Club parade and crew races on Opening Day in May.

Want to Learn More? Additional Interpretive Sign Locations:
 1. A Prosperous Portage
 2. Alaskan Yukon Pacific Corporation
 3. Montlake's Structural Legacy
 4. Montlake's Early
 5. Montlake's Historic Architecture
 6. Working on Park Adventure & Good Design

The Montlake Ditch had a lock located at its eastern end. This 1911 picture shows the structure, which was used to control the flow of water in the canal.

Montlake Cut was built by the U.S. Army Corps of Engineers. While the Montlake Ditch was excavated by hand, this 1914 picture shows hydraulic machinery used for construction of the Montlake Cut.

This photo shows the bridge under construction in April 1925. It was completed in less than a year and is owned and operated by the Washington State Department of Transportation.

On June 23, 1946, the Seattle Yacht Club Opening Day parade through Montlake Cut included more than 300 vessels. The event remains an important celebration for the local boating community.