

February 2023

Request for Certificate of Approval 1051 Building Demolition Project

Attachment 1 - Project Description and Supplemental Information Washington State Department of Transportation

Introduction

The Washington State Department of Transportation (WSDOT) owns a former warehouse and factory, which has been converted to an office building, at 1051 1st Avenue South in Seattle, Washington (the 1051 Building). The 1051 Building has been used by WSDOT as offices for the Alaskan Way Viaduct Replacement Program and State Route 520 Program. During a recent survey, it was determined that the building is structurally unsafe and is no longer occupied for use. The building will be demolished for safety reasons, and the site will be graveled and fenced.

Above-ground demolition activities would begin from the parking lot on the west side of the building and would proceed east towards 1st Avenue South. Following the removal of the structure, ground disturbance would include removal of the concrete slab, vactoring around existing utilities, and replacement of the sidewalks that are currently located along 1st Avenue South and Royal Brougham Way in front of the building.

Existing conditions at the sidewalk on 1st Avenue South are shown in Figure 1. The sidewalks are not currently compliant with Pioneer Square Preservation Board standards. The sidewalk would be replaced with a wider sidewalk and Americans with Disabilities Act-compliant curb ramps, as follows:

- Existing asphalt portions of the sidewalk would be removed.
- Existing concrete sections of the sidewalk would remain.
- New concrete sidewalks would be installed within the footprint of the removed sidewalks.

The demolition activities would be limited to the footprint of the property parcel, and the adjacent sidewalk that will be replaced. Some temporary traffic control or work zone safety areas would be needed within the sidewalks south and east of the building, and possibly into the parking lane along southbound 1st Avenue South. The duration of temporary traffic control is expected to be less than three months.

Figure 1
Existing Sidewalk along 1st Avenue South



November 3, 2022, facing south

The sidewalks and parking lane along 1st Avenue South are in the Pioneer Square Preservation District. The 1051 Building and sidewalks along Royal Brougham Way are outside the Pioneer Square Preservation District and are not further discussed as part of this document.

Applicable Requirements

The Pioneer Square Preservation Board has published *Rules for the Pioneer Square Preservation District*, which includes "Section XVII. Sidewalk Treatment" (PSPB 2003). "Section XVII.A. Standards" states that "sidewalk paving and improvements shall be completed with one pound lamp-black per cubic yard of concrete, scored at two-foot intervals. This material shall be used for all projects of 1/4 block or greater size" (PSPB 2003, pg. 6).

The Project includes more than 1/4 block, so the material standards apply. Sidewalk prism lights are not present and special sidewalk treatments are not proposed as part of the Project, so those material standards do not apply. There are no areaways in the Project area.

Post-Project Condition

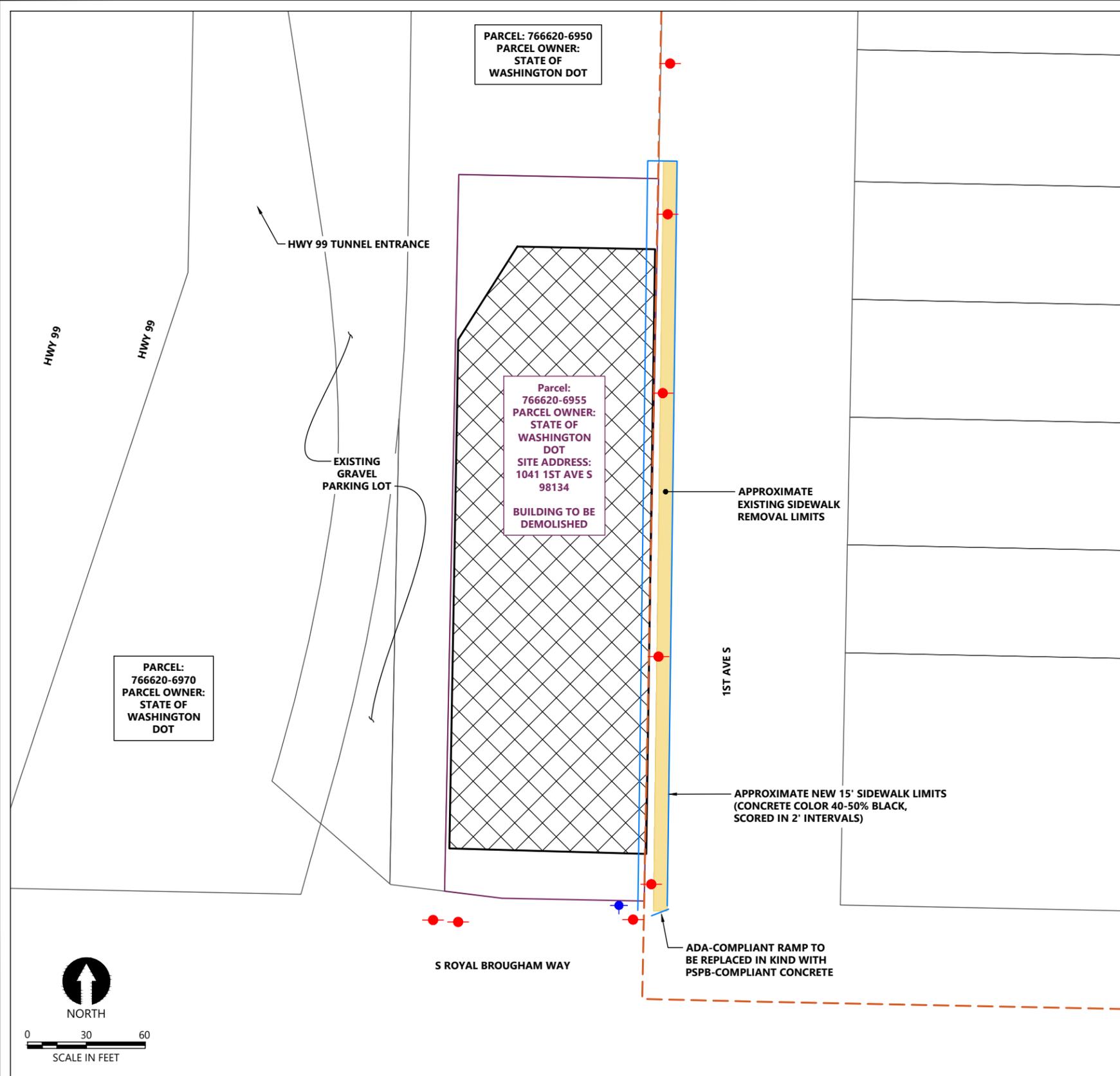
This application includes the post-Project condition proposed for the Project. The restored sidewalks will comply with Section XVII.A of *Rules for the Pioneer Square Preservation District*. There are no

street trees in the portion of the sidewalk within the District. No new permanent signage or poles/luminaries are included. Existing wood poles that support Seattle City Light utilities will remain, because moving the utilities underground is outside the scope and budget of the Project, and the Project area is not within Seattle City Light's target area for undergrounding utilities. Some temporary traffic control measures would be needed within the sidewalks south and east of the building, and possibly into the parking lane along southbound 1st Avenue South, but there would be no permanent changes to vehicle or pedestrian circulation.

Reference

PSPB (Pioneer Square Preservation Board), 2003. Rules for the Pioneer Square Preservation District. City of Seattle. July 2003. Accessed January 2023. Available at:
<https://www.seattle.gov/documents/Departments/Neighborhoods/HistoricPreservation/HistoricDistricts/PioneerSquare/PioneerSquare-Guidelines.pdf>

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Feb 08, 2023 12:03pm hmerick



- LEGEND:**
- BUILDING TO BE DEMOLISHED
 - PIONEER SQUARE PRESERVATION DISTRICT
 - PIONEER SQUARE NATIONAL HISTORIC DISTRICT
 - PARCEL BOUNDARY
 - APPROXIMATE SIDEWALK REMOVAL LIMITS
 - APPROXIMATE NEW SIDEWALK LIMITS
 - POWER POLE
 - FIRE HYDRANT (OUT OF SERVICE)

- NOTES:**
1. HORIZONTAL DATUM: WASHINGTON STATE PLANE NORTH ZONE, NAD83, U.S. SURVEY FEET
 2. AERIAL SOURCE: ©2023 MICROSOFT CORPORATION ©2022 MAXAR ©CNES (2022) DISTRIBUTION AIRBUS DS
 3. UTILITIES FROM CITY OF SEATTLE.

DRAFT-NOT FOR CONSTRUCTION



LEGAL DESCRIPTION:
SEATTLE TIDE LDS E
101.90 FT LESS POR PER DEED REC
#20220519000511 PLAT BLOCK: 329
PLAT LOT: 14 THRU 19

REVISIONS				
REV	DATE	BY	APP'D	DESCRIPTION

DESIGNED BY: J. CEBULA
DRAWN BY: H. MERRICK
CHECKED BY: B. BUNDY
APPROVED BY: J. JENSEN
SCALE: AS NOTED
DATE: FEBRUARY 2023

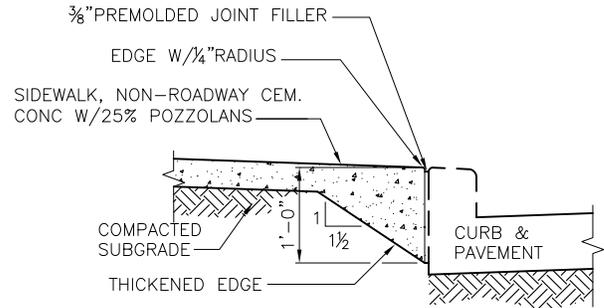
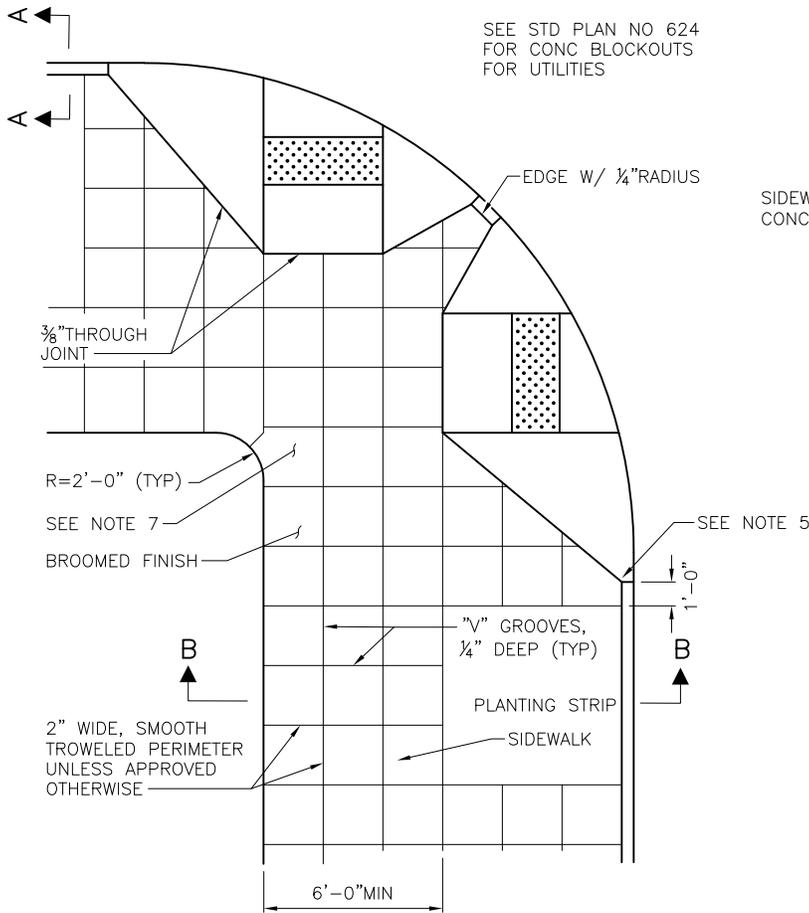
1051 BUILDING DEMOLITION PROJECT

SIDEWALK REPLACEMENT PLAN VIEW

D01

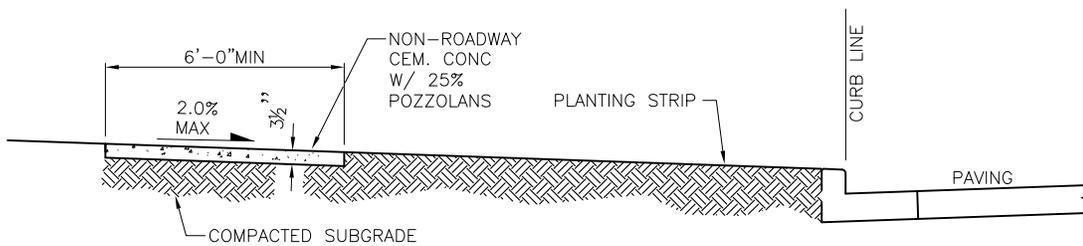
SHEET# 1 OF 1

PLAN INTENDED TO BE VIEWED IN COLOR. ADJACENT BLOCK IS "BLUE" INCH SCALE ACCORDINGLY



THROUGH JOINT @ SECTION A-A
UNLESS CURB IS MONOLITHIC WITH SIDEWALK

TYPICAL SIDEWALK & CURB RAMP DETAIL



SECTION B-B

NOTES:

1. 3/8" THROUGH AND CONTRACTION JOINTS MUST BE LOCATED AS REQUIRED BY SECTION 8-14.3(6).
2. SAWCUT SCORING MUST MATCH PATTERN IN ADJACENT EXISTING SIDEWALK OR MUST BE A 2' SQUARE SCORING PATTERN UNLESS OTHERWISE OTHERWISE APPROVED BY THE ENGINEER.
3. FOR CURB RAMPS, SEE STANDARD PLAN NO 422.
4. FOR TREE PITS, SEE STANDARD PLAN NO 424.
5. 12" MINIMUM BETWEEN EDGE OF RAMP WING AND PLANTING STRIP IS DESIRABLE.
6. ALL SIDEWALK MUST BE NON-ROADWAY CEM CONC W/ 25% POZZOLANS.
7. 6'-0" MINIMUM CONTINUOUS SIDEWALK MUST BE MAINTAINED AROUND CORNERS.

REF STD SPEC SEC 8-14



City of Seattle

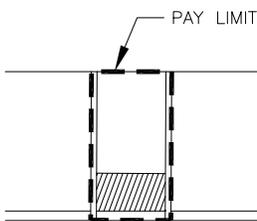
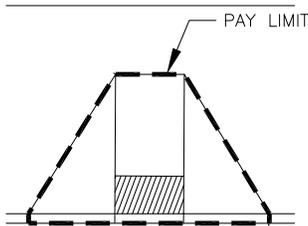
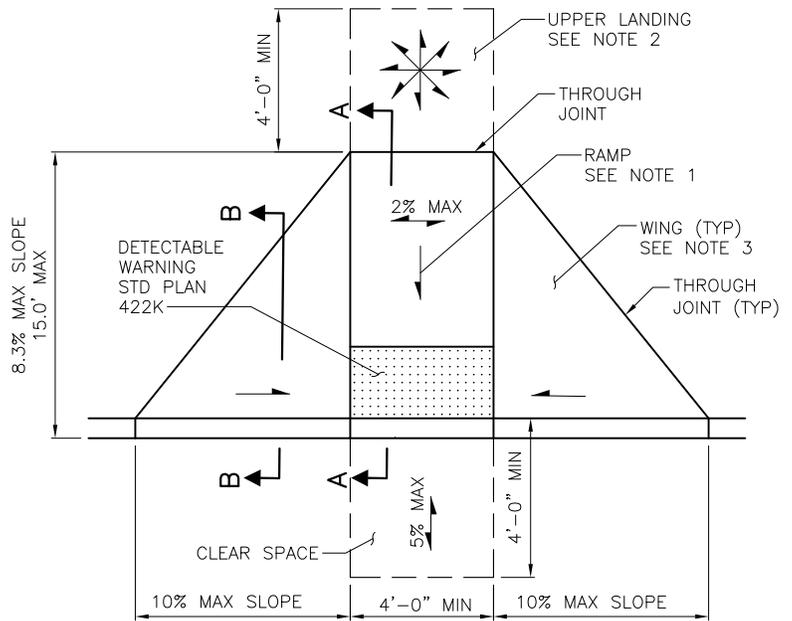
NOT TO SCALE

CONCRETE SIDEWALK DETAILS

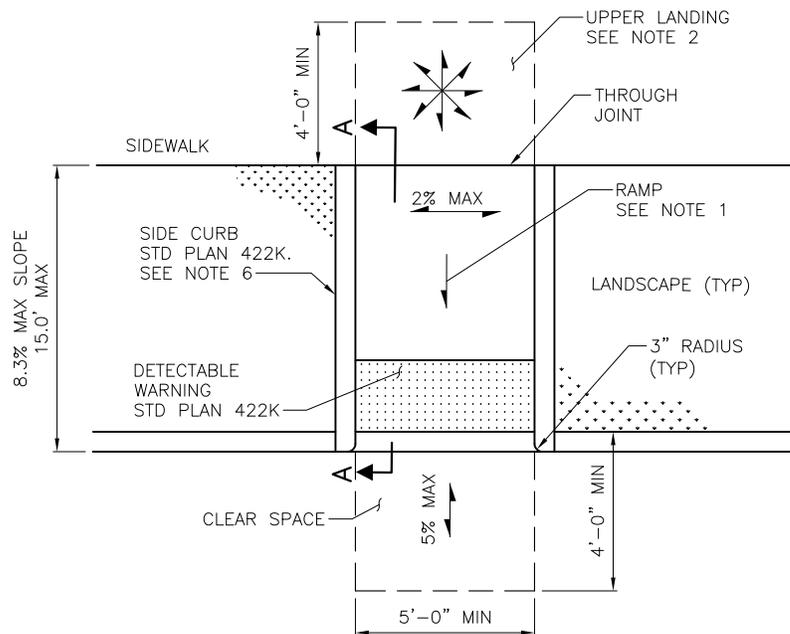
NOTES:

1. RAMP CENTERLINE MUST BE RADIAL/PERPENDICULAR TO THE ALIGNMENT OF THE FACE OF CURB.
2. THE SLOPE ON THE LANDING MUST BE A MINIMUM OF 0.5% IN ANY ONE DIRECTION AND MUST NOT EXCEED 2% IN ANY DIRECTION. UPPER LANDING AT THE TOP OF THE CURB RAMP MUST MATCH THE FULL WIDTH OF THE RAMP AND MUST HAVE A MINIMUM DEPTH OF 4'-0". IF THE LANDING IS LIMITED AT THE BACK-OF-SIDEWALK BY A PERMANENT VERTICAL BARRIER, THE DEPTH OF THE TURNING SPACE MUST BE 5'-0" MINIMUM, MEASURED PARALLEL TO THE RUN OF THE CURB RAMP.
3. WINGS MUST HAVE A MAXIMUM SLOPE OF 10%. WINGS MUST HAVE A BRUSHED FINISH PARALLEL TO THE CURB. THE CONCRETE WALK THICKENED EDGE ALONG THE CURB MUST CONTINUE THROUGH EACH WING.
4. RAMP SURFACE MUST HAVE A HEAVY BROOM BRUSHED SURFACE PARALLEL TO THE CURB.
5. REFER TO DETAILS 422K AND 422L FOR GENERAL NOTES AND TYPICAL SECTIONS.
6. RAMP WIDTH MUST BE 5'-0" MINIMUM WHEN SIDE CURB IS USED ON BOTH SIDES INSTEAD OF WINGS.

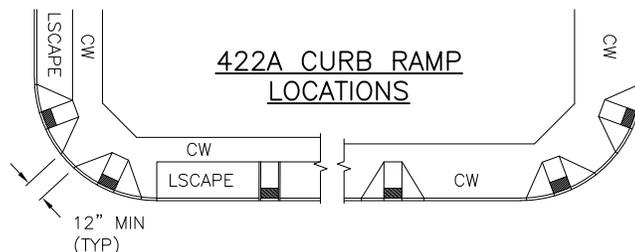
2% MAX
 = MAX SLOPE IN EITHER DIRECTION



PAY LIMITS



PERPENDICULAR CURB RAMPS
 (TYPE 422A)



422A CURB RAMP LOCATIONS

REF STD SPEC SEC 8-14



City of Seattle

NOT TO SCALE

CURB RAMP DETAILS

Figure 1

Excerpt from Proposed Materials Submission, January 23, 2023

Proposed Material

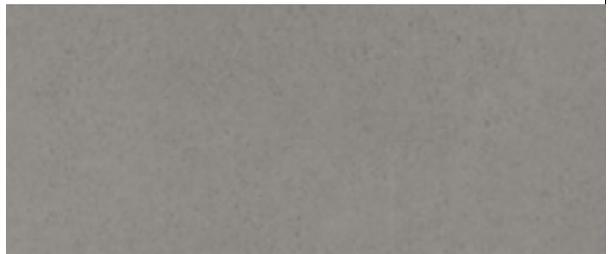
Concrete for the proposed sidewalk will mirror the examples above by creating a muted grey concrete mix at an approximate 40-55% Black, to be similar to the appearance of one pound of lamp-black per cubic yard of concrete. Figure 3 shows a comparison between undyed concrete and the proposed color.

Figure 3

Standard Concrete and Proposed Concrete



Standard Concrete (Approximately 20% Black)



Concrete at Approximately 47% Black