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Cover Page

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SHEET #  
1.0



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1. Please describe the existing site, including location, existing uses and/or structures, topographical or other physical features, etc.

The site address is 13730 Lake City Way NE. This is a mid-block lot situated near the SE corner of NE 140th Street and east side of Lake City Way NE. Existing uses on site include a two-story retail/commercial space with at-grade parking lot. The topography of the site is sloped running east-west direction with west side (along Lake City Way NE) being the low side, with portions of the site along south and north sides being designated as steep slope area which was created by grading of the site under a previous building permit in an effort to allow for development of the site. The maximum elevation change, where steep slope occurs, is less than 20' from the low point at +236 up to a high point of +254. The steep slope is farther than 30' from any steep slope on adjacent properties. The west perimeter of the site is adjacent to Lake City Way NE. Existing site access is provided via a curb cuts off Lake City Way. The east perimeter of the site is adjacent to residential zone - L-1. The north & south perimeters of the site are adjacent to commercial zone C1-65.

2. Please indicate the site's zoning and any other overlay designations, including applicable Neighborhood-Specific Guidelines.

The site is zoned C1-65 in the Lake City neighborhood.

3. Please describe neighboring development and uses, including adjacent zoning, physical features, existing architectural and sitting patterns, views, community landmarks, etc.

Lake City Way is the local business district with various civic and commercial uses. All commercial activity in the neighborhood is concentrated along Lake City Way. The majority of the surrounding neighborhood to the east and southeast is comprised of multifamily, multistory apartment buildings.

Commercial uses along Lake City Way include car dealerships, car repair shops, retail establishments and restaurants.

Vehicle traffic is concentrated along Lake City Way and Erickson Place. Lake City Way turns into Bothell Way NE to the northeast, and links to I-5 to the southwest, thus providing a major thoroughfare between downtown Seattle and the outlying suburbs. Bus service is frequent along Lake City Way, making this neighborhood convenient for commuting to downtown Seattle and the University of Washington.

Sidewalks are generally provided throughout the area. Sidewalks are partially or completely improved as new buildings are added to enhance the pedestrian environment.

Small-scale, medium-scale and large-scale apartment buildings are existing in the vicinity of the site. Large-scale apartment complexes have been built in the neighborhood in recent years. The new buildings have commercial space on the ground floor and several stories of residential units above. They use similar materials, such as fiber-cement board and metal cladding, and they use color in an effort to provide and further enhance modulation and scale. They all provide marquees and awnings above commercial level to provide a sense of pedestrian oriented scale.

Existing apartment buildings along NE 137th Street and Erickson Place as well as NE 140th Street are mostly two to four stories with a few larger-scale apartment complexes. They do not appear to have any significant architectural features. Future mixed-use and commercial development is anticipated along Lake City Way in the vicinity of this site as it has been zoned C1-65.

The general zoning pattern along Lake City Way is commercial/retail and mixed-use driven and gradually become more residential in nature as lots move away from Lake City Way. The project site is on a city block zoned for C1-65 use. Along NE 137th Street and Erickson Place NE the lots are zoned for low-rise use. Across the street from the project site along Lake City Way, the lots are zoned for C1-65 use. Lots north of NE 140th Street are zoned for C1-40 along the east side of Lake City Way and C1-65 along the west side.

4. Please describe the proponent's development objectives, indicating types of desired uses, structure height (approx), number of residential units (approx), amount of commercial square footage (approx), and number of parking stalls (approx). Please also include potential requests for departure from development standards.

The proposal is to construct a 7-8-story mixed-use building, with 3,500 SF street level commercial, approximately 160 residential units (affordable senior housing), and a total of 69 parking stalls below grade. The proposed structure will have a total area of approximately 153,935 SF. Parking will be provided as mandated in SMC 23.47A.032 & 23.54.015 and will be located on P1. Level P1 will be completely below grade. Levels two through 8 will contain residential units (affordable senior housing) ranging from 1-bedrooms to 2-bedrooms, as well as community and activity rooms for the enjoyment of the residents. Please refer to the enclosed Project Data for more detailed information.

A list of departure requests is provided in the attached Departure Requests Sheet.

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Application Form

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# Project Data

1. Site Address: 13730 Lake City Way NE
2. Parcel Number: 3834000495
3. Lot Size: 28,950 S.F. (approximately) - ~ 0.66 Acres
4. Zoning: C1-65
5. Immediate Vicinity Zones:  
site abuts L-1 on the east side,  
C1-65 north and across Lake City Way (West side)
6. Applicable Building Code:  
2006 Seattle Building Code (SBC)
7. Proposed Use: Residential Mixed Use - affordable housing for seniors
8. Occupancy Classification:  
  
Commercial M  
Residential R-2  
Parking S-2
9. Gross Floor Area:

Floor Area:	TOTAL AREA	AREA USED FOR F.A.R.
Parking Area	21,015 sq.ft.	7,423 sq.ft.
Commercial Area	2,830 sq.ft.	2,830 sq.ft.
Residential Area	1,761 sq.ft.	1,761 sq.ft.
Mech/Elect/Elevator	1,100 sq.ft.	1,100 sq.ft.
Total - First Floor Area	26,706 sq.ft.	13,114 sq.ft.
Second Floor Residential	18,874 sq.ft.	18,874 sq.ft.
Third Floor Residential	18,874 sq.ft.	18,874 sq.ft.
Fourth Floor Residential	18,874 sq.ft.	18,874 sq.ft.
Fifth Floor Residential	18,766 sq.ft.	18,766 sq.ft.
Sixth Floor Residential	18,211 sq.ft.	18,211 sq.ft.
Seventh Floor Residential	17,178 sq.ft.	17,178 sq.ft.
Eight Floor Residential	9,511 sq.ft.	9,511 sq.ft.
<b>TOTAL -</b>	<b>146,994 sq.ft.</b>	<b>133,402 sq.ft.</b>

10. Allowable Building Height: Per SMC 23.47A.012 = 65'  
Height bonus for sloped sites: 1' for every 6% - 5' maximum  
Projections above height limit: clerestories,  
guardrails, elevator/stairs overruns
11. Allowable Density:  
Per SMC 23.47A.009 (A)  
no density limit for mixed-use
12. Setback Requirements: SMC 23.47A.014  
Below 13 feet: none Provided: none  
North property line: none Provided: 10'-6" minimum  
East property line: 15 feet (from 13' to 40')  
plus 2 feet for every 10 additional feet of height

Per exhibit 23.24A.014C Provided: 17' min.  
Street-level, street-facing facades:  
per SMC 23.47A.008 (A.3) must be located  
within 10 feet of the street lot line, unless wider  
sidewalks, plazas, or other approved landscaped  
or open spaces are provided  
Structure projections into required setbacks:  
exterior balconies and decks

13. F.A.R. Requirements: SMC 23.47A.013  
Max. F.A.R. = 4.75  
F.A.R. = Lot size X 4.75 = 28,950 X 4.75 = 137,513 S.F. allowed  
Proposed = 4.61 = 133,402 S.F.
14. Required Non-residential Uses @ Street-level, Street-facing Facade:  
SMC 23.47A.005 & 23.47A.008  
Commercial Space: 100' (3,304)  
Average depth for commercial space = 30 feet Provided: 33 feet  
Floor to floor min. height = 13 feet Provided: 13 feet
15. Required Residential Amenity Areas: SMC 23.47A.024  
Required: 5% of the total residential floor area = 5% X 116,639 S.F. =  
5,832 S.F.  
Provided: 5,960 S.F.
16. Required Landscaping: SMC 23.47A.016  
Required Green Factor = 0.30  
Provided: 0.325  
Green roof, planting strips along Lake City Way, east, north & south side
17. Required Parking: SMC 23.47A.032 & 23.54.015  
Senior Housing: 1 per 6 units  
Commercial: varies  
Provided parking: 69 stalls total  
Bicycle Parking:  
Multifamily: 1 per every 4 units  
Sales and Services: 1 per every 12,000 S.F.  
Provided: 56  
Driveway Sight Triangle: 10 feet triangle required

## Project Description

The project is a 8-story mixed-use building with 7 floors of residential units (affordable senior housing), approximately 160 units, above street-level, commercial uses & 1 level of below-grade parking with a total of approximately 69 stalls.

The building mass addresses Lake City Way NE, and surrounding lots.

The proposed building is arranged in a 'U' shape. Residential units are arranged along double-loaded corridors with internal landscaped courtyard.

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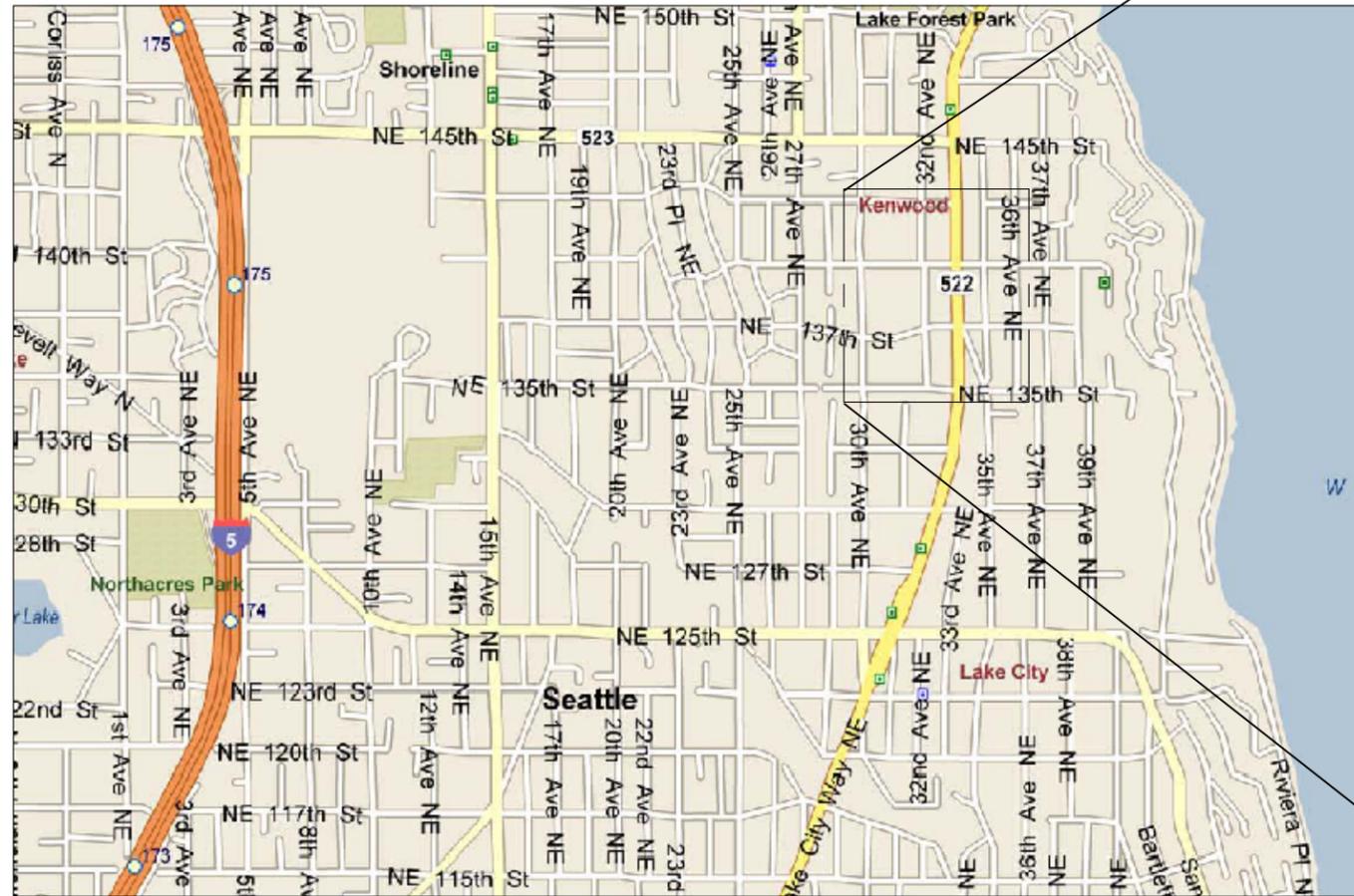
Development  
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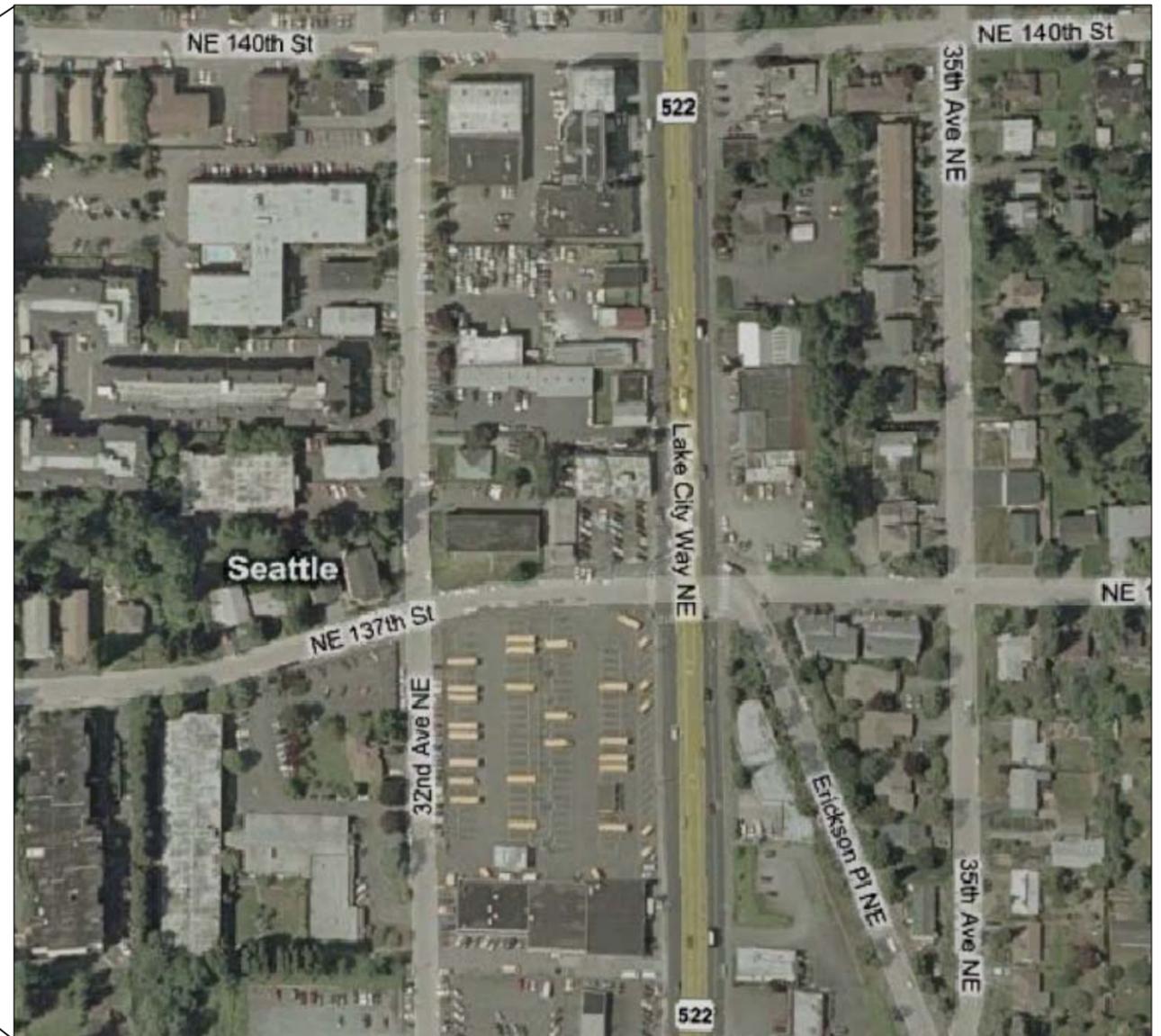
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Vicinity Map



Aerial Photograph

13730 LAKE CITY WAY NE  
EARLY DESIGN GUIDANCE  
DPD PROJECT # 30085115

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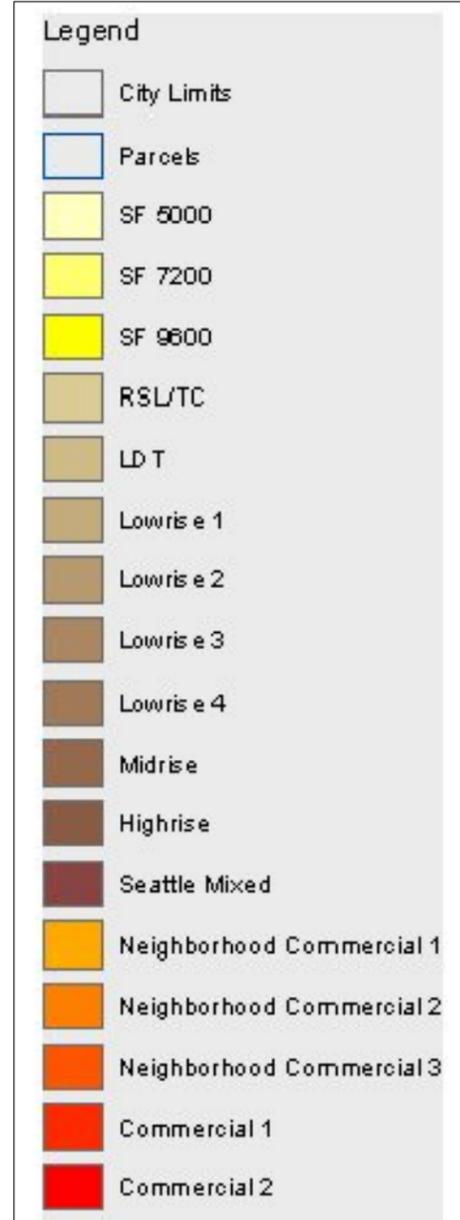
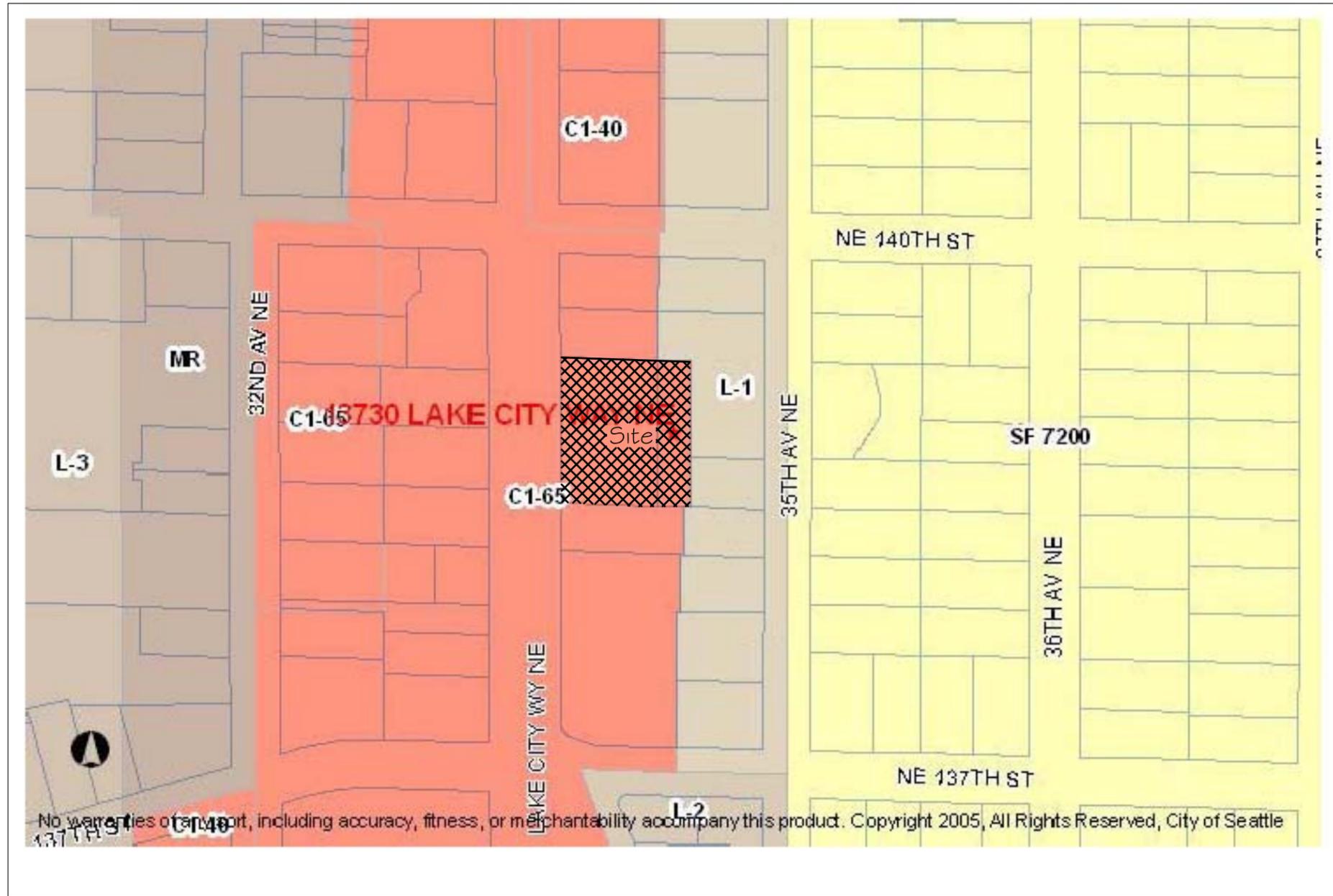
Urban Design  
Analysis

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Zoning Map  
Source: City of Seattle DPD ~ GIS Maps

13730 LAKE CITY WY NE  
EARLY DESIGN GUIDANCE  
DPD PROJECT # 30085115

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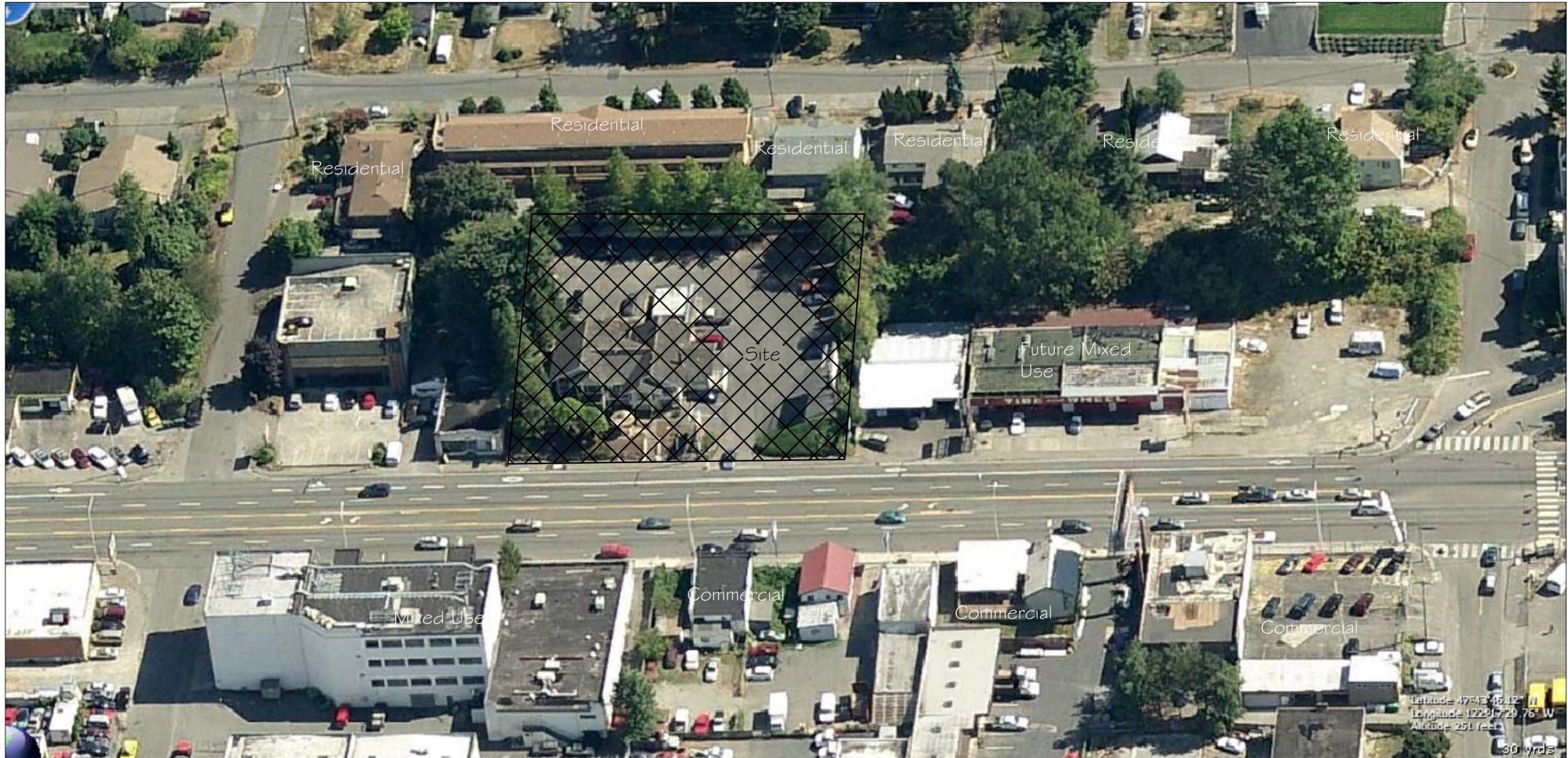
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Aerial View of Project Site  
*Looking east*



Looking East from across the Street from the Project Site



Looking West from the Project Site

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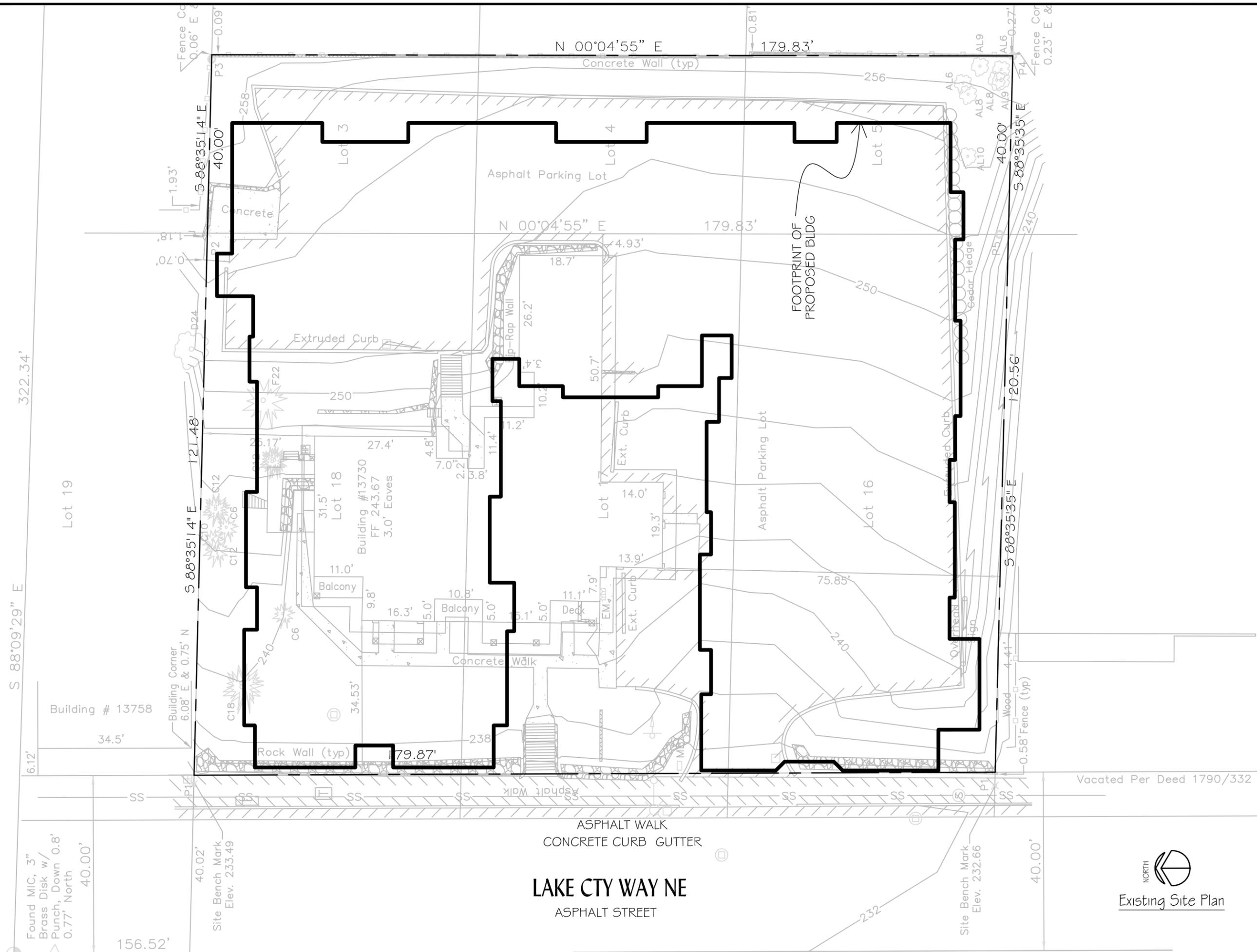
Site Analysis

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**LAKE CITY WAY NE**  
ASPHALT STREET

NORTH  
  
Existing Site Plan

View of Site & Existing Conditions



Partial Views of Site - Looking North / NW



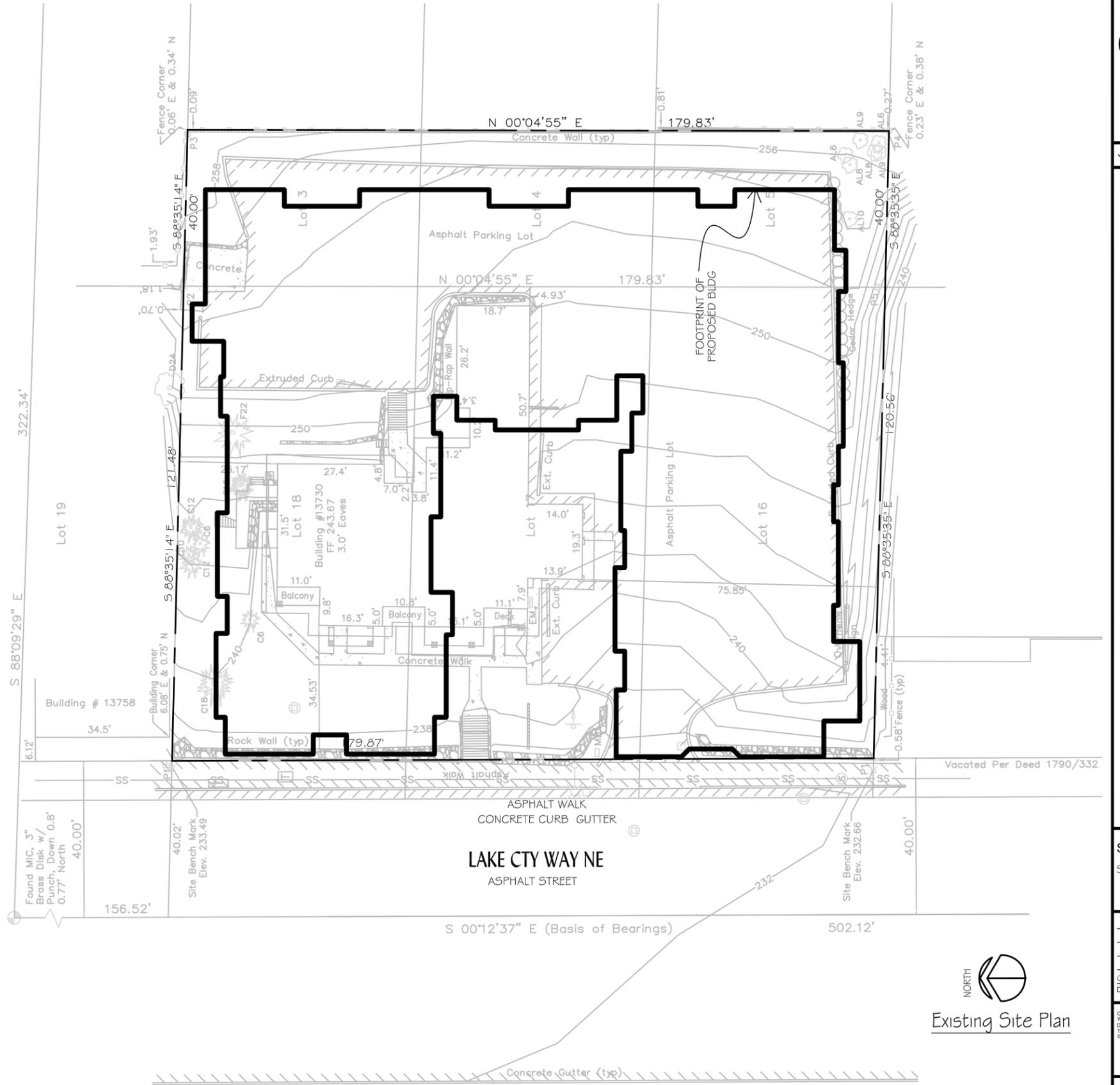
Partial View of Site Looking South



Partial Views of Site - Looking North



Partial View of Site Looking North / NE



NORTH   
Existing Site Plan

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# Applicable Design Review Guidelines

## A. Site Planning

### A-3 Entrances Visible from the Street

Entries should be clearly identifiable and visible from the street.

### A-4 Human Activity

New development should be sited and designed to encourage human activity on the street.

### A-5 Respect for Adjacent Sites

Building should respect adjacent properties by being located on their sites to minimize disruption of the privacy and outdoor activities of residents in adjacent buildings. Avoid locating exterior lights above the ground floor on the sides of structures facing residential uses.

### A-8 Parking and Vehicle Access

Siting should minimize the impact of automobile parking and driveways on the pedestrian environment, adjacent properties and pedestrian safety.

"Cut through" traffic - coming from outside the neighborhood and driving on residential streets to reach a destination outside the neighborhood - is a significant concern to residents. consider the following:

Vehicular traffic of the new development should ingress and egress toward the more intensive zoned area and not the lessor zoned area.

## B. Height, Bulk and Scale Compatibility

### B-1 Height, bulk and Scale Compatibility

Projects should be compatible with the scale of development anticipated by the applicable Land Use Policies for the surrounding area and should be sited and designed to provide sensitive transition to nearby, less intensive zones. Projects on zone edges should be developed in a manner that creates a step in perceived height, bulk, and scale between anticipated development potential of the adjacent zones.

## C. Architectural Elements and Materials

### C-2 Architectural Concept and Consistency

Building design elements, details and massing should create a well-proportioned and unified building form and exhibit an overall architectural concept. Buildings should exhibit form and features identifying the functions within the building. In general, the roofline or top of the structure should be clearly distinguished from its facade walls.

### C-4 Exterior Finish Materials

Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged.

Signs in the North District Neighborhoods. Design signs that are appropriate for the pedestrian scale and character that is envisioned for the area. Signs should be oriented and scaled for both pedestrians on sidewalks and persons in vehicles on streets within the immediate neighborhood.

## D. Pedestrian Environment

### D-4 Design of Parking Lots

In addition to the citywide guidance for the screening of highly visible parking lots the following supplemental guidance should be considered to carry the theme of landscaping used as screening around the sensitive perimeters of a parking lot throughout the lot.

As sites with large surface parking areas, such as auto dealerships, are redeveloped, consider locating parking under, beside or behind new structures. If surface parking lots are located between structures and the sidewalk, vegetated areas should be provided along the sidewalk to provide pedestrians a buffer from the parking lot. Vegetation should be of a height that pedestrians can still see above it and/or spaced so they have visibility around it for safety.

## Landscaping

### E-1 Landscaping to Reinforce Design Continuity with Adjacent Sites

Where possible, and where there is not another overriding concern, landscaping should reinforce the character of neighboring properties and abutting streetscape.

- For new development involving critical areas, consider using native plants and preserving or restoring habitat areas and incorporating them into the design for landscaping.
- Use native plant materials and evergreen trees in appropriate public areas to reestablish a natural northwest tree canopy. Consider extending creek corridors as part of a "green gateway" design feature.

### E-2 Landscaping to Enhance the Building and/or Site

Landscaping, including living plant material, special pavements, trellises, screen walls, planters, site furniture and similar features should be appropriately incorporated into the design to enhance the project.

- Use landscaping to further define and provide scale for open space. Lush plants, warm materials and pleasing details are encouraged. Retain existing mature trees wherever possible.
- Use lighting to emphasize landscaping where appropriate.

### E-3 Landscape Design to Address Special Site Conditions

The landscape design should take advantage of special on-site conditions such as highbank front yards, steep slopes, view corridors or existing significant trees and off-site conditions such as greenbelts, ravines, natural areas and boulevards.

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Prominent Entrance



Human Scale



Modulation



Color & Material



Color & Material

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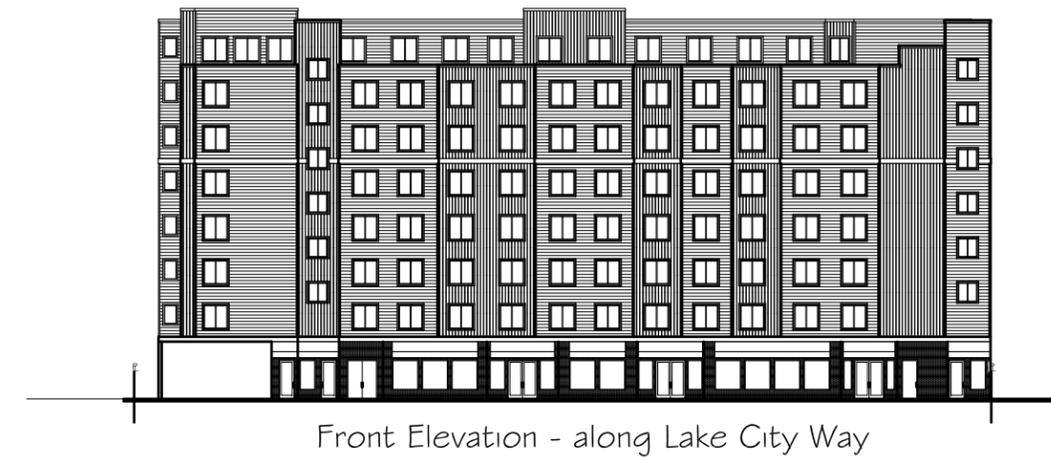
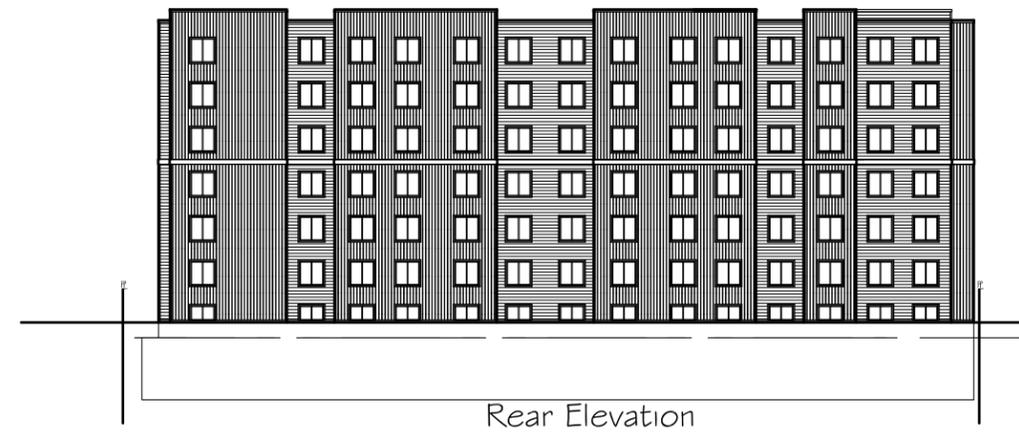
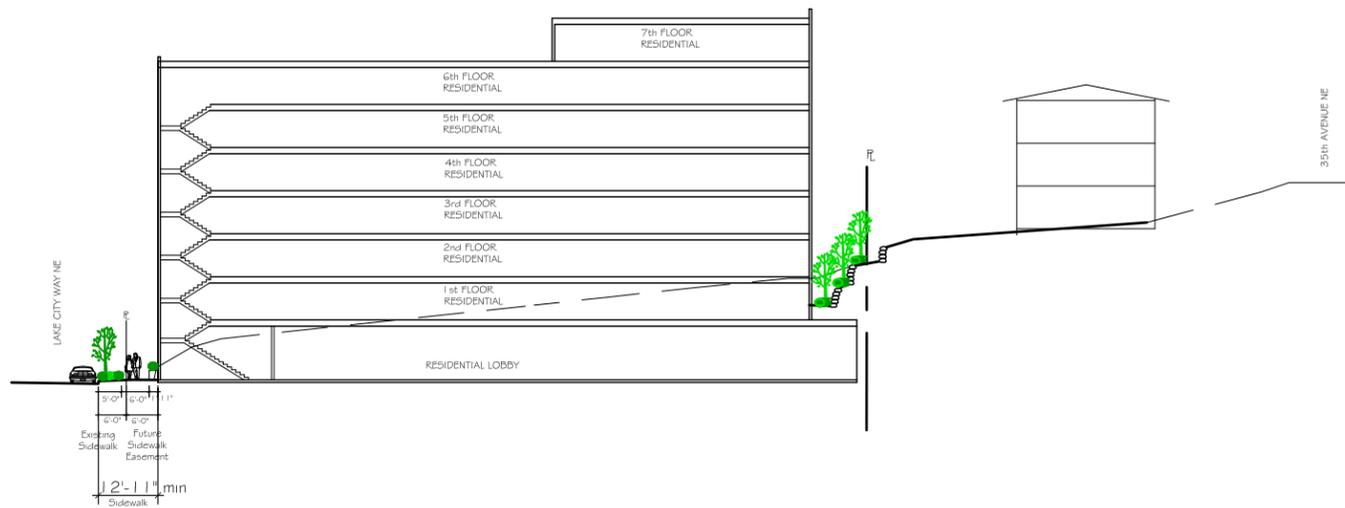
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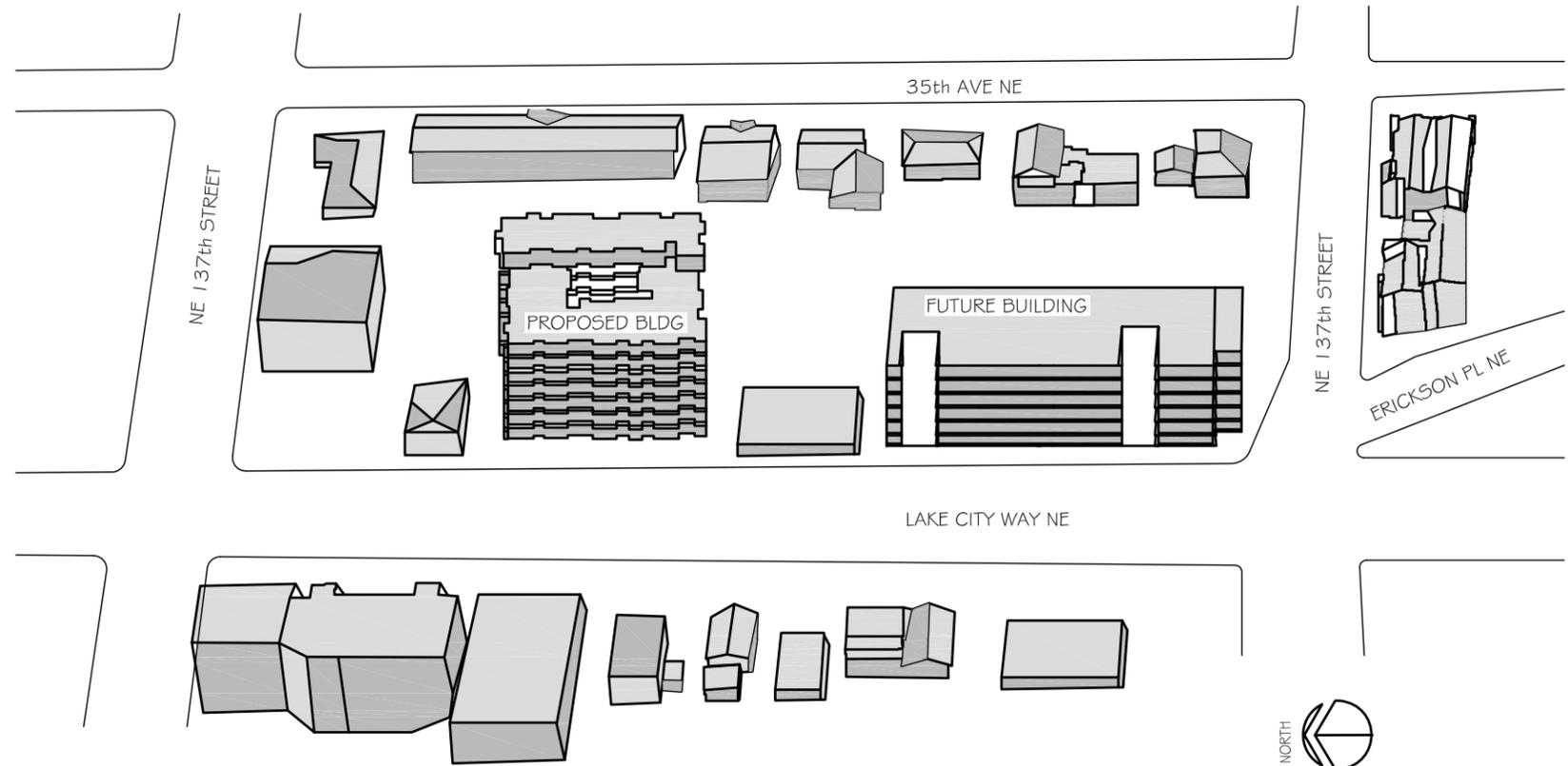
**Architectural Concept 1**

**Pros**

- Prominent residential entrance at southwest corner
- Expansive open space on roof
- Building modulation

**Cons**

- Lack of 'open' community courtyard
- Small courtyard on 2nd floor
- Lack of private balconies
- Increased internal circulation (inefficient floor plan)
- Neglects adjacent building relationship to the north & south



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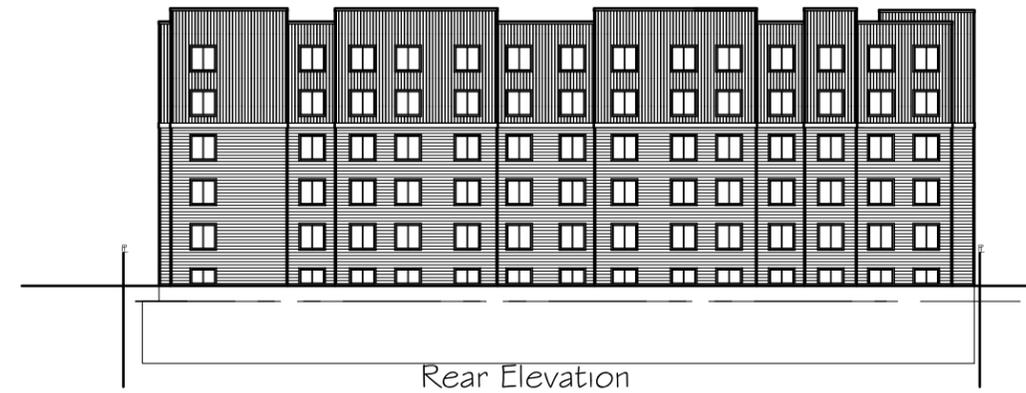
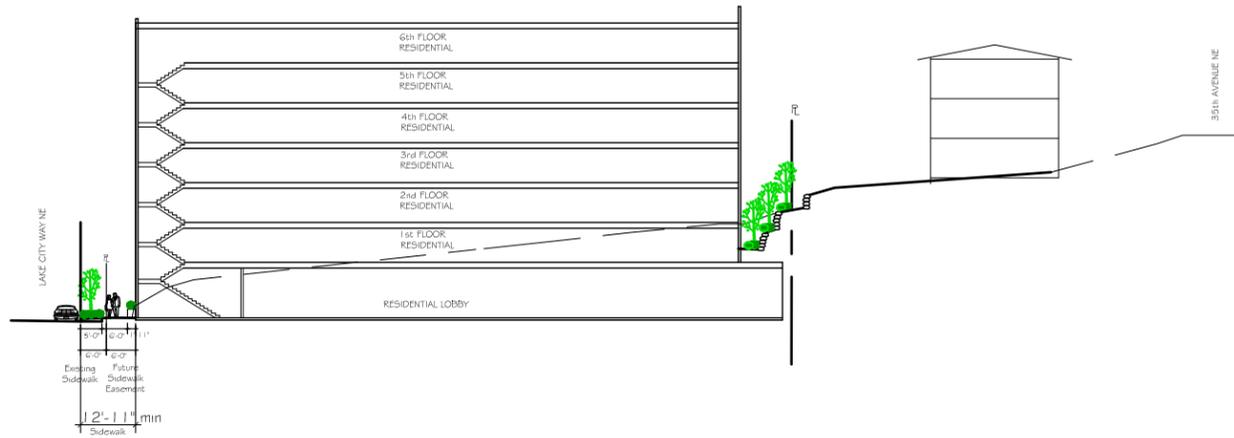
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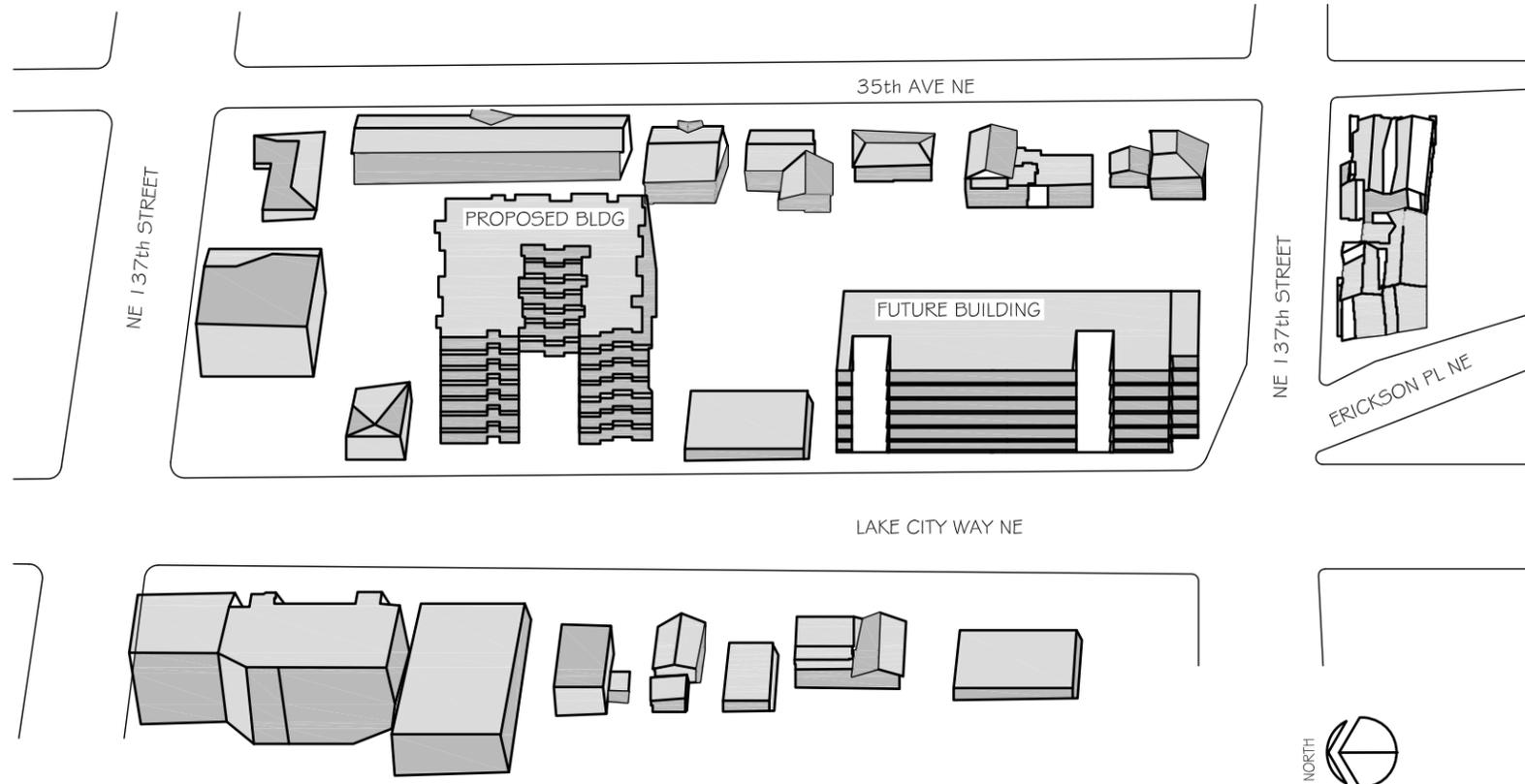
**Architectural Concept 2**

**Pros**

- Prominent residential entrance at southwest corner
- Expansive open space on roof
- Building modulation
- Large community courtyard on 2nd floor

**Cons**

- Lack of private balconies
- Increased internal circulation (inefficient floor plan)
- Neglects adjacent building relationship to the north & south



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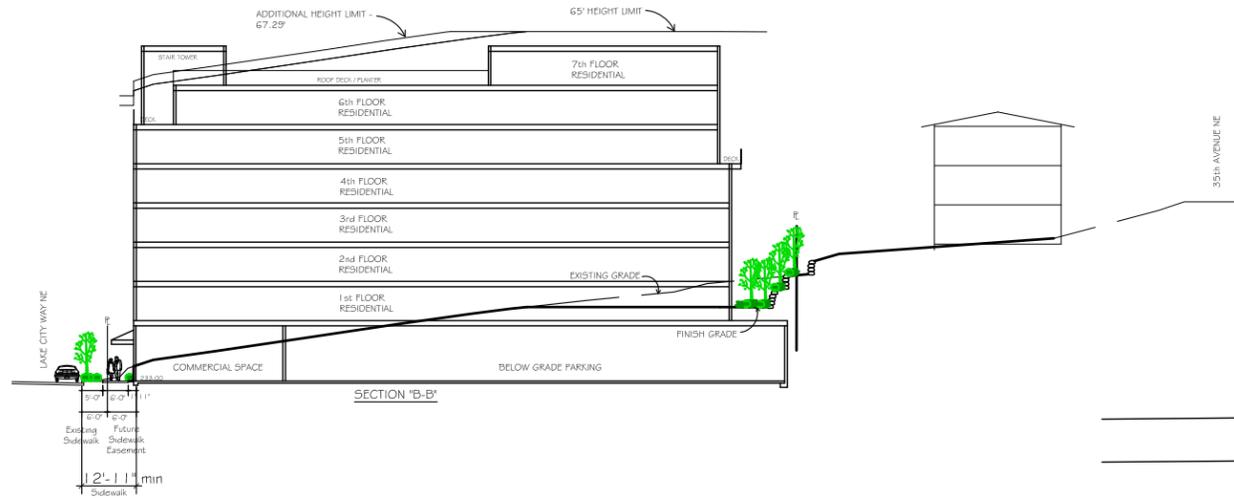
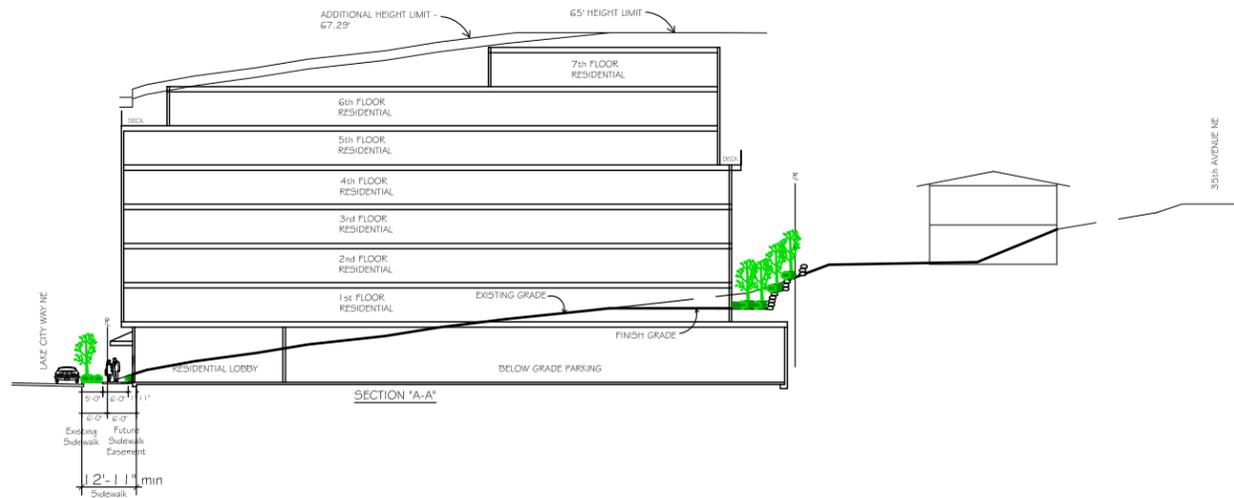
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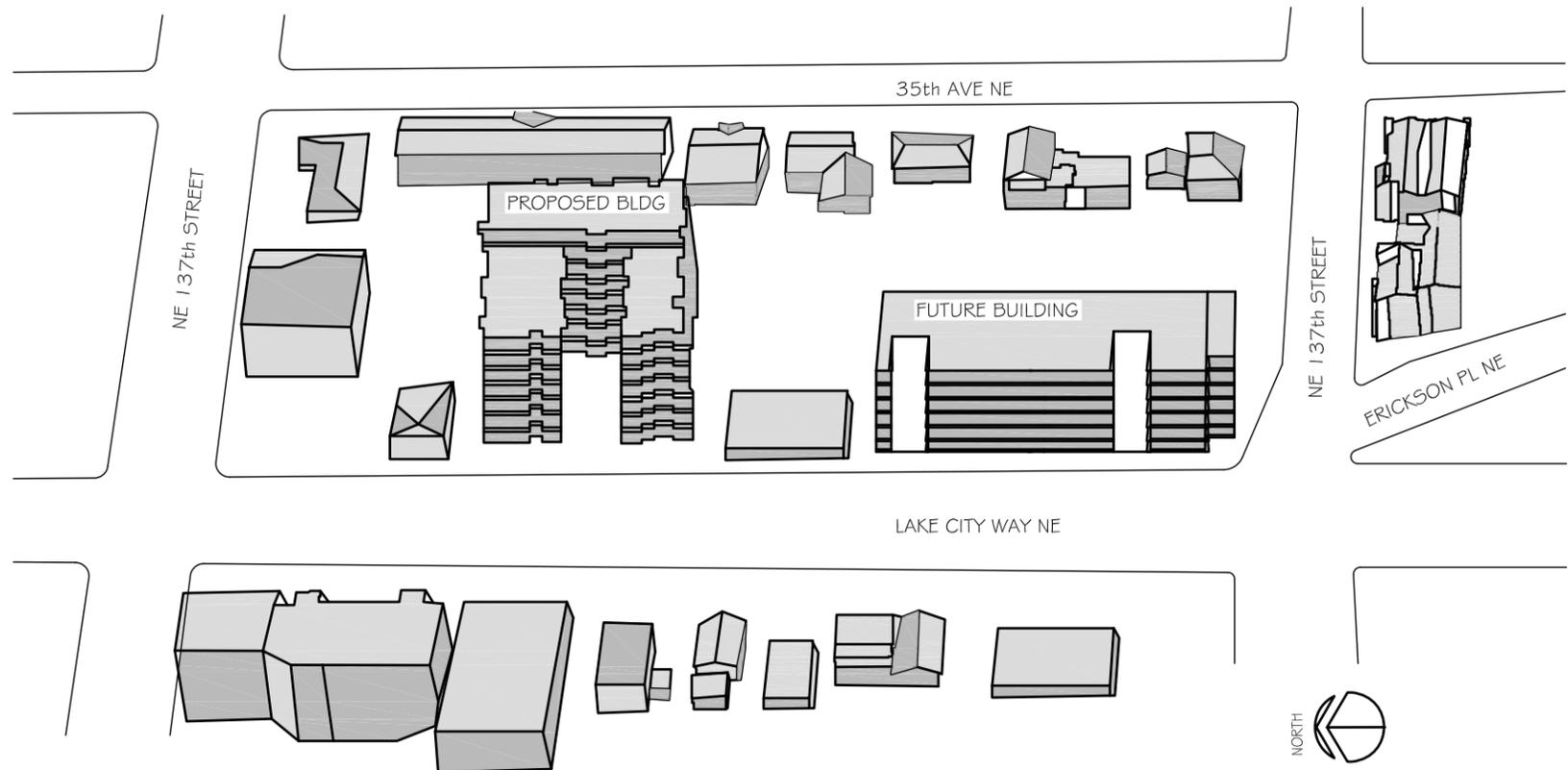
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Rear Elevation



Front Elevation - along Lake City Way



**Architectural Concept 3 (preferred concept)**

**Pros**

- Prominent residential entrance at southwest corner
- Large community courtyard on 2nd floor
- Expansive open space on roof
- Building modulation
- Accommodates adjacent building relationship to the north & south
- Select units with private balconies
- Terraced building modulation

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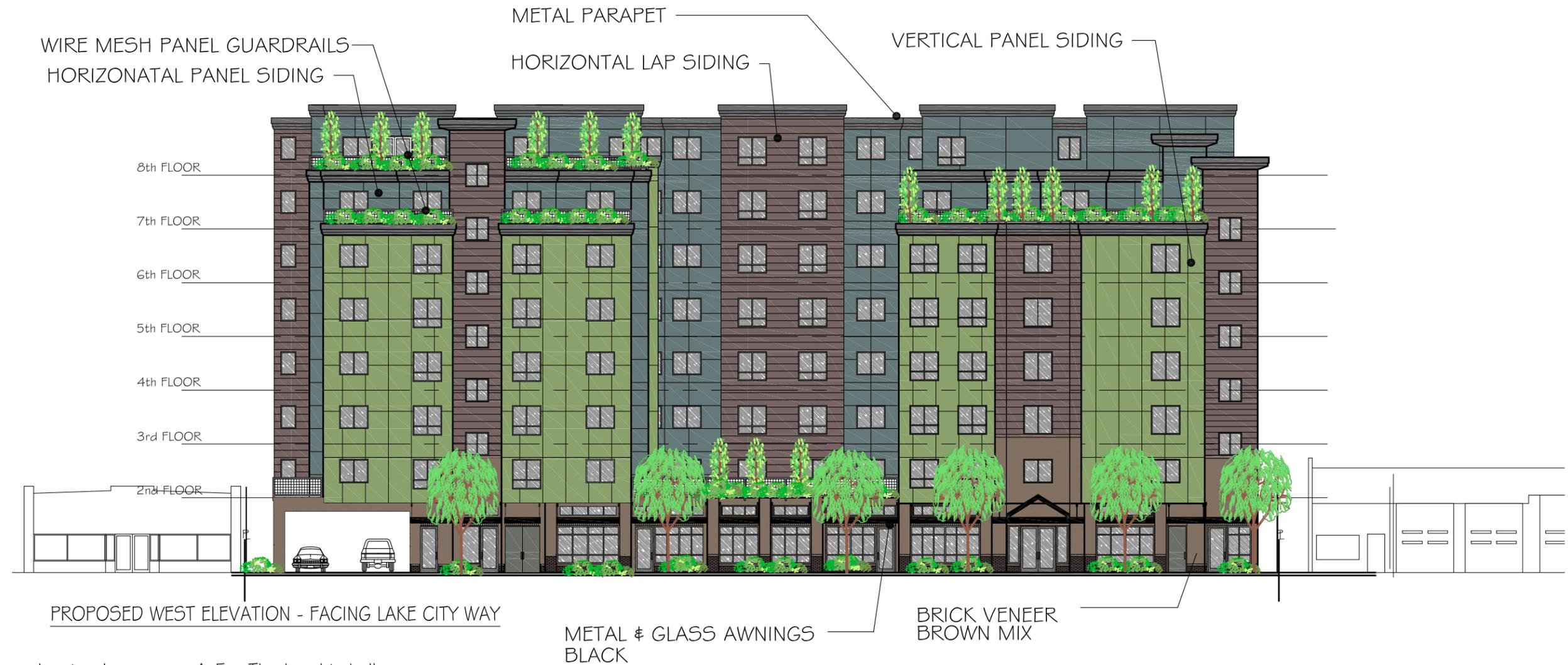
Architectural Concepts

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A-3 All commercial units along  
A-4 Lake City Way have  
C-3 entrances visible from the  
street to encourage human  
activity. Canopies and  
transparent storefront  
enhance the human scale.

C-2 Architectural concept and  
C-4 consistency has been  
achieved by using durable  
exterior finish materials  
applied to accentuate the  
primary masses of the  
building. Pattern and  
texture has been utilized in  
the material application to  
enhance the design and  
differentiate the building  
elements from one  
another. Terraced  
modulation along the west  
elevation with planting  
strips further accessorize  
the building.

A-5 The height, bulk  
B-1 and scale of this  
building has been  
concentrated to  
the west side of  
the site in order  
to take advantage  
of existing  
contours and  
slopes to build  
"into the slope",  
thereby  
respecting the  
adjacent  
properties by  
allowing better  
access to light  
and air to the  
lowrise zone to  
the east and  
southeast.



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Architectural  
Concepts

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APPROVED: JMJJP

SHEET #

A-3 An inviting entry plaza  
A-4 located at the southwest  
C-3 corner of the site provides  
a prominent entry to the  
residential portion of the  
building. Surrounding  
commercial spaces  
encourage human activity.  
Lush landscaping and  
overhead protection  
connects the building to  
the pedestrian at a human  
scale.

A-5 Massing along the east  
E-1 elevation has been scaled  
E-3 back and landscaping has  
been enhanced and aligned  
with the east property line  
to lighten the massing  
adjacent to the  
neighboring L-1 zone.

C-2 Architectural concept and  
C-4 consistency has been  
achieved by using durable  
exterior finish materials  
applied to accentuate the  
primary masses of the  
building. Pattern and  
texture has been utilized in  
the material application to  
enhance the design and  
differentiate the building  
elements from one  
another.



SHEET TITLE:  
Architectural  
Concepts

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**SHEET TITLE:**

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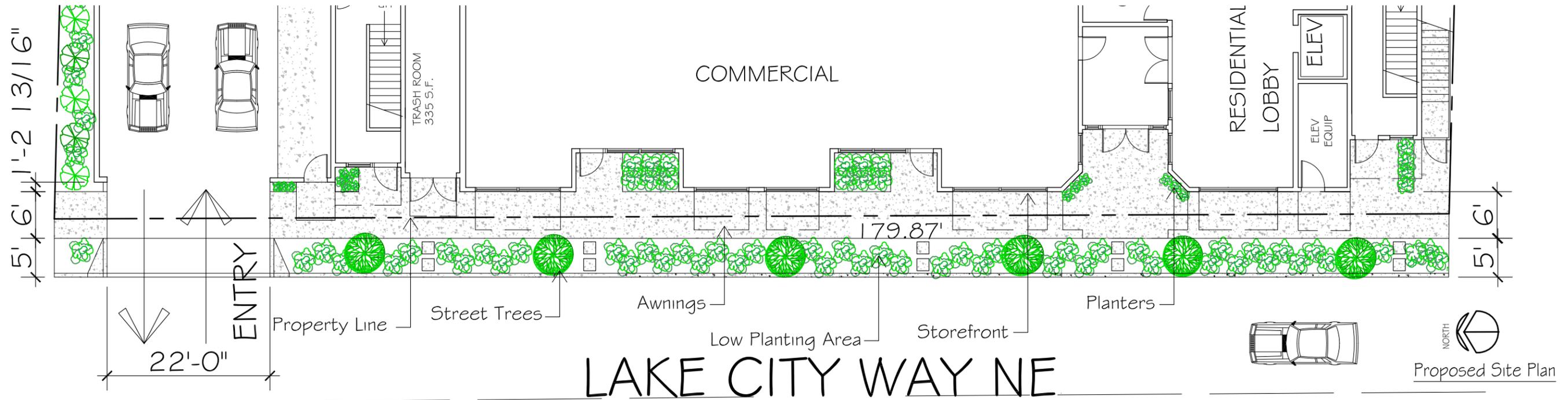
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SHEET #

17.0



PROPOSED BUILDING RENDERING



# LAKE CITY WAY NE

An inviting entry plaza located at the southwest corner of the site provides a prominent entry to the residential portion of the building. Surrounding commercial spaces encourage human activity. Lush landscaping and overhead protection connects the building to the pedestrian at a human scale. Rental/management office provides human presence along Lake City Way. Visual and physical connections provided via operable storefront and clear, non-reflective glazing. 5' wide landscaping strip buffers pedestrian zone (sidewalk) from the street along Lake City Way. Proposed sidewalk and landscaping buffer is consistent with the SDOT requirements.



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18.0

## Sustainable Design Commitment

We are committed to a design incorporating sustainable design principles to benefit residents, users and community.

## Proposed Sustainable Design Strategies

### Sustainable Design:

- reduces non-renewable resource consumption
- enhances the natural environment
- minimizes the use of toxins in our projects

### Site Features:

- urban site with access to public transportation and bicycle storage
- landscape design to capture the site's characteristics
- shielded exterior light fixtures to reduce light pollution

### Water Efficiency:

- low flow plumbing fixtures
- drip irrigation and low demand native plant landscaping

### Energy Performance:

- use of energy efficient appliances
- building systems testing and reporting for efficient operation

### Materials and Resources:

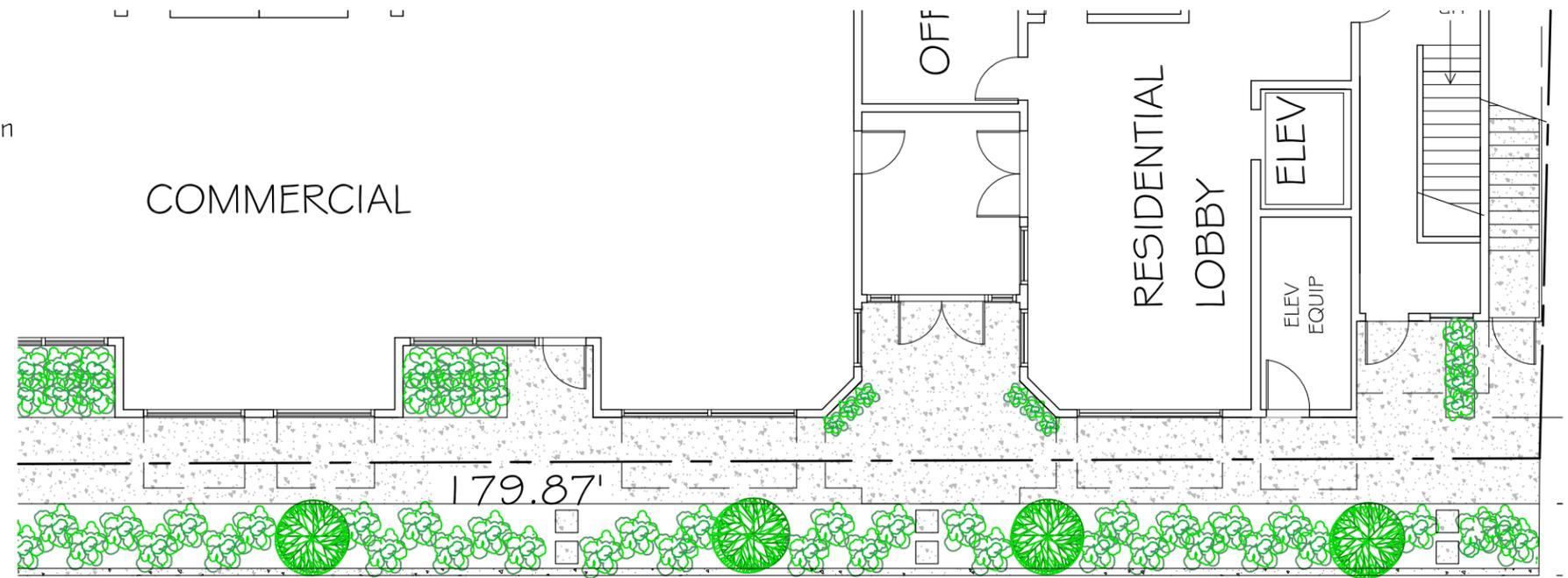
- locally manufactured materials (concrete, steel, masonry)
- recycled content materials (steel, concrete, wallboard)
- construction waste to be diverted from landfill (70% Goal)

### Indoor Environmental Quality:

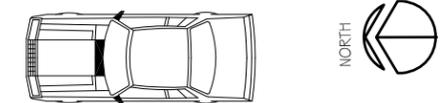
- fresh air from operable windows, sliders, and ventilation systems
- daylight and views throughout interior spaces
- healthy finishes: carpets, paints, and adhesives

### Innovative Features:

- rooftop garden
- education of residents and management in sustainable features and operation
- resident recycling program



low demand & water efficient landscape design



natural daylighting and ventilation

### SHEET TITLE:

Architectural  
Concept -  
Sustainable Design

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19.0

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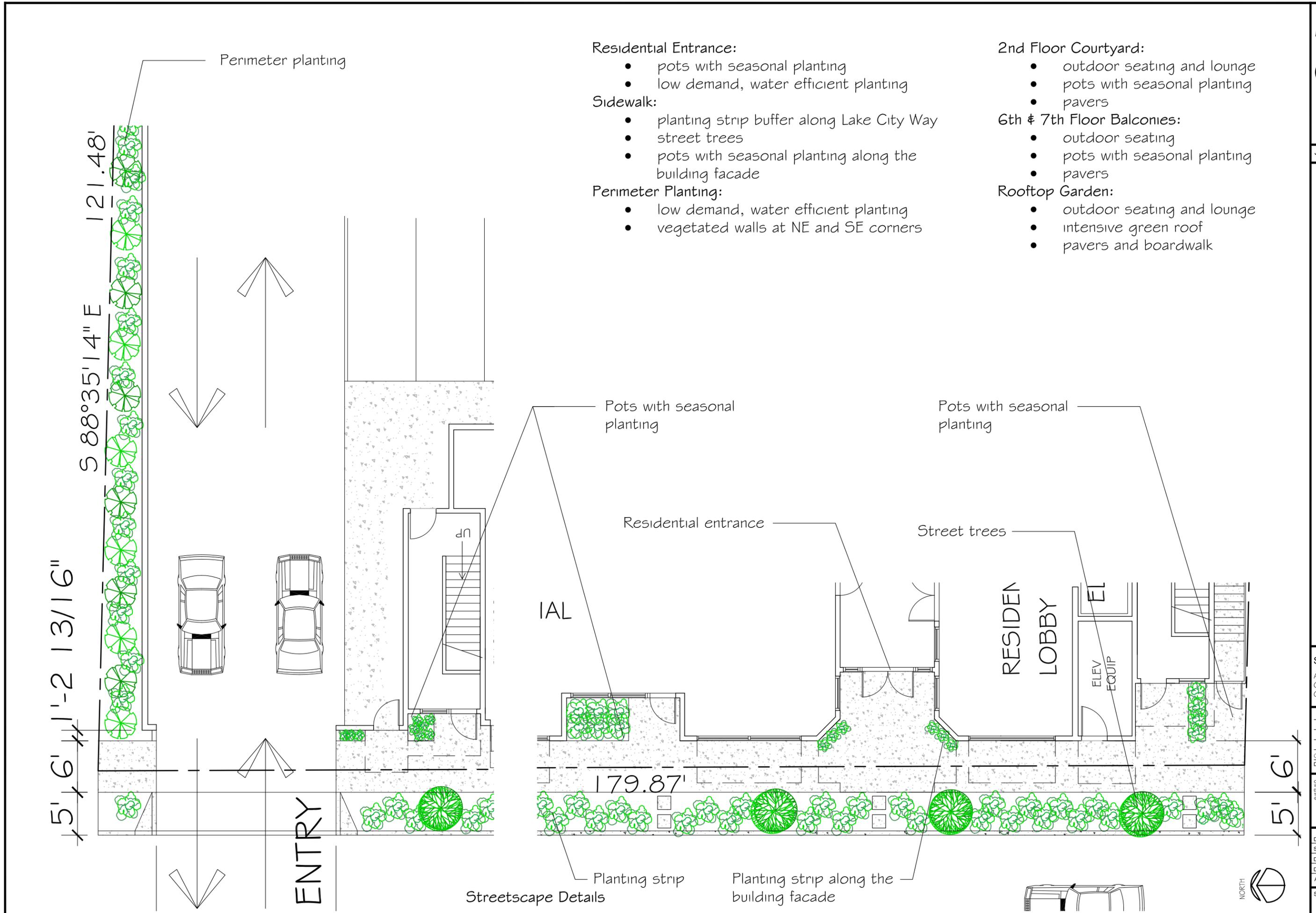
Architectural  
Concept -  
Green Factor

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SHEET #  
20.0



**Residential Entrance:**

- pots with seasonal planting
- low demand, water efficient planting

**Sidewalk:**

- planting strip buffer along Lake City Way
- street trees
- pots with seasonal planting along the building facade

**Perimeter Planting:**

- low demand, water efficient planting
- vegetated walls at NE and SE corners

**2nd Floor Courtyard:**

- outdoor seating and lounge
- pots with seasonal planting
- pavers

**6th & 7th Floor Balconies:**

- outdoor seating
- pots with seasonal planting
- pavers

**Rooftop Garden:**

- outdoor seating and lounge
- intensive green roof
- pavers and boardwalk

Pots with seasonal planting

Pots with seasonal planting

Residential entrance

Street trees

IAL

RESIDEN  
LOBBY

ELEV  
EQUIP

179.87'

Planting strip

Planting strip along the building facade

Streetscape Details



**SHEET TITLE:**

Architectural  
Concept -  
Green Factor

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SHEET #  
**210**

**Residential Entrance:**

- pots with seasonal planting
- low demand, water efficient planting

**Sidewalk:**

- planting strip buffer along Lake City Way
- street trees
- pots with seasonal planting along the building facade

**Perimeter Planting:**

- low demand, water efficient planting
- vegetated walls at NE and SE corners

**2nd Floor Courtyard:**

- outdoor seating and lounge
- pots with seasonal planting
- pavers

**6th & 7th Floor Balconies:**

- outdoor seating
- pots with seasonal planting
- pavers

**Rooftop Garden:**

- outdoor seating and lounge
- intensive green roof
- pavers and boardwalk



Outdoor seating & lounge  
intensive green roof  
pavers



Intensive green roof



Roof Garden Details



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Green Factor

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22.0



**Residential Entrance:**

- pots with seasonal planting
- low demand, water efficient planting

**Sidewalk:**

- planting strip buffer along Lake City Way
- street trees
- pots with seasonal planting along the building facade

**Perimeter Planting:**

- low demand, water efficient planting
- vegetated walls at NE and SE corners

**2nd Floor Courtyard:**

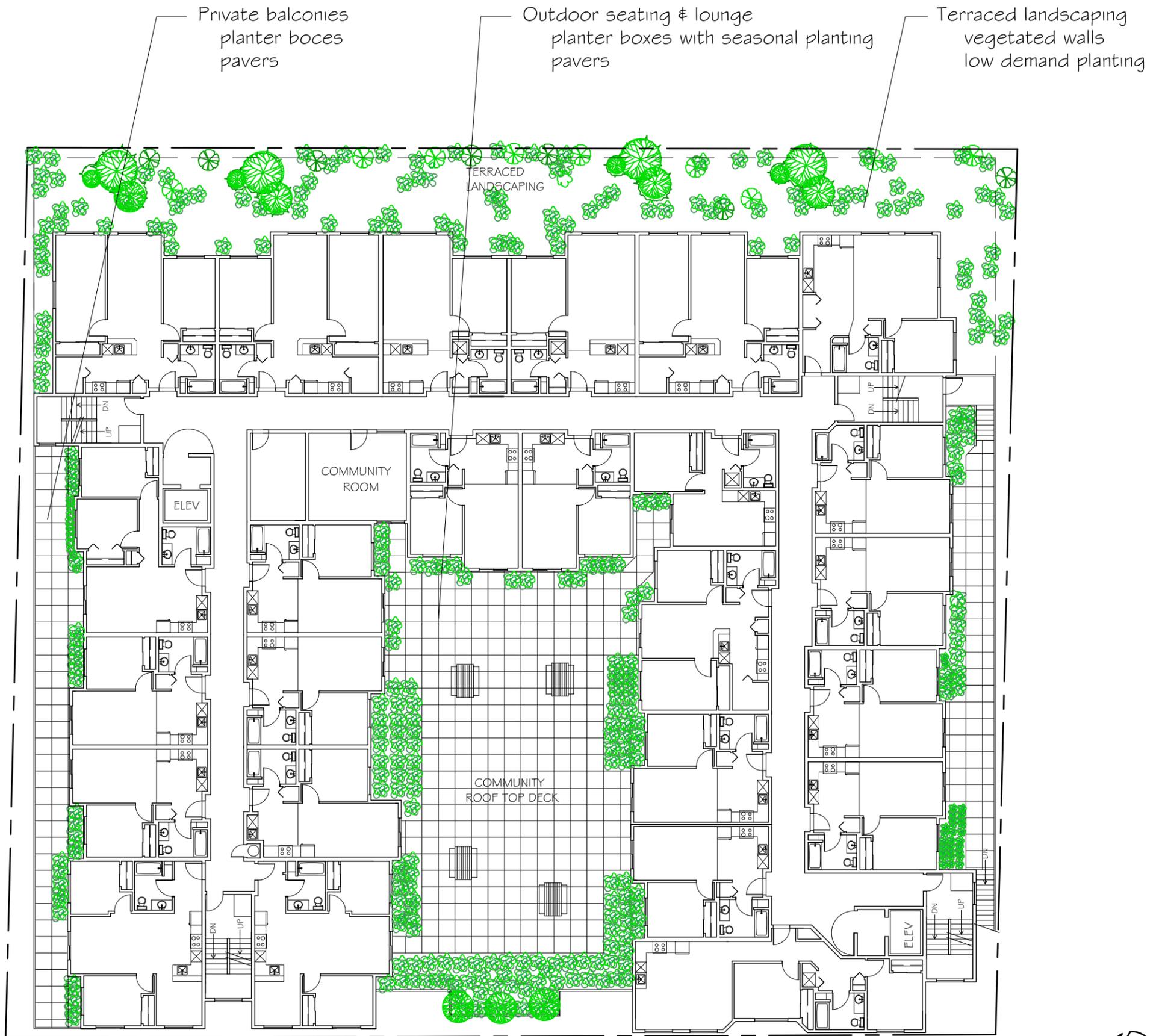
- outdoor seating and lounge
- pots with seasonal planting
- pavers

**6th & 7th Floor Balconies:**

- outdoor seating
- pots with seasonal planting
- pavers

**Rooftop Garden:**

- outdoor seating and lounge
- intensive green roof
- pavers and boardwalk



2nd Floor Courtyard Details





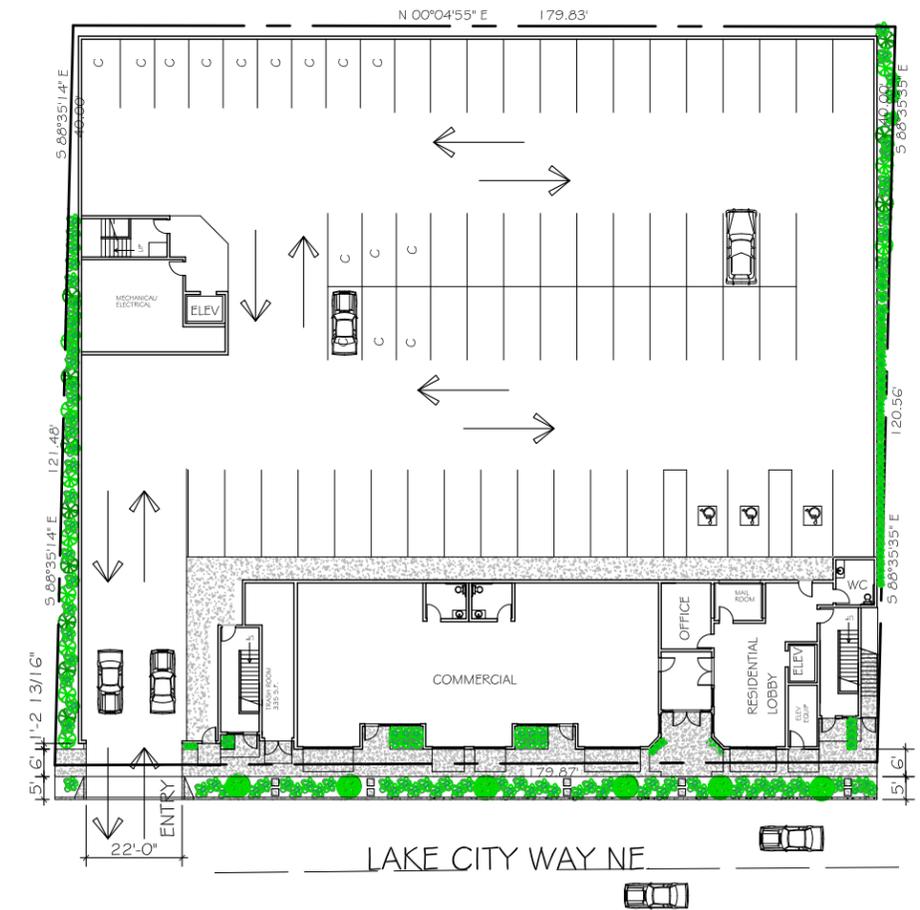
## Design Departure Requests

### SMC 23.47A.008 Street Level Development Standards

D.2. The residential uses on the street level are the main entrance, stair exits and the trash / refuse / recycling area. It is impractical for these spaces to be four feet above the street level as it causes accessibility issues and would require stairs or lengthy ramp systems. Due to the limited amount of street frontage that the residential areas are allowed to have - no more than 20% of the façade - to pull the entries a min of 10' back from the sidewalk would create deep, narrow spaces that are contrary to human scale and connectivity from the entry to the sidewalk, and would not feel very inviting or safe for the elderly - which is who this project is for. It appears that the intent of the code is for a separation and a sense of privacy for the occupants of residential "units", but in this case it is residential "uses" not "units".

### SMC 23.86.020 Modulation

The intent of the modulation requirements for building facades is to prevent long blank walls and buildings that are just big boxes. We feel that we have met the intent of the requirements for modulation both vertically and horizontally by not having any wall length greater than 34 feet - which only occurs on a few places, the majority of the wall lengths are under 30 feet in length. Each wall façade is stepped forward or back from adjacent façades by anywhere from 2 to 6 feet. There are changes in building materials and or colors at façade depth changes. The "U" shape of the building reduces the bulk of the building from along the street facing façade and the sides of the building step back up the to follow the slope on the lot. The rear façades step back vertically to break up the bulk of the façade that faces the existing up hill buildings. Were the building steps back planting was also used to soften the façade and provide more visual interest.



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Design Departures

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23.0



13730 LAKE CITY WAY NE  
EARLY DESIGN GUIDANCE  
DPD PROJECT # 30085115

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Project Samples

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