

**RECOMMENDATION MEETING
OF
SOUTH EAST DESIGN REVIEW BOARD**

Meeting Date: July 24, 2007
Report Date: July 31, 2007

BACKGROUND INFORMATION:

Project Number: 3001242
Address: 1400 S. Dearborn Street
Applicant: Bill Fuller, Fuller Sears Architects for Darrell Vange, Ravenhurst
Development
Board Members present: Ann Beeman (chair)
Mona Campbell, substitute
Michelle Wang
John Woodworth
Board Member absent Robert Mohn
DPD Staff: Vincent T. Lyons, Manager, Design Review Program

BACKGROUND:

The proposal was the subject of Early Design Guidance (EDG) public meetings on August 8, 2005 and September 27, 2005 at which time early design guidance was given to be considered in developing the proposed design. A record can be found in the MUP file for this application. The applicant has applied for a Master Use Permit. On July 20, 2006, March 27, 2007 and June 5, 2007 Design Review Board meetings were convened for the purpose of making Design Review Recommendations to DPD. At these meetings, site, floor plans, elevations, landscape plans and computer based modeling graphics of the proposed mixed-use building were presented.

PROJECT DESCRIPTION

The proposal is for a six-story building containing 543,000 sf of retail, 45,000 sf of administrative offices, 51,000 sf of warehouse and 596 residential units in 7 structures above the commercial base. Parking for about 2,200 vehicles would be provided within and under the structures. The project includes demolition of all existing structures on site and 260,000 cu.yds. of grading. Seattle Goodwill Industries would be relocated in the new project.

JULY 24, 2007 RECOMMENDATION MEETING

The purpose of this meeting, planned for 2 hours, was to consider the design response to the following elements of the design and to have the board make recommendations on these after hearing all input and considering reviews and recommendations made to date:

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- Residential/housing components
- Open Space/Public Amenity components and Green Factor (GF) designs
- Revisit the Weller curb cut departure request

SUMMARY OF BOARD RECOMMENDATIONS

The Board recommended approval with conditions of the Residential/housing components and Open Space/Public Amenity and GF components, also with conditions. The Board did not recommend granting the truck loading curb cut width departure on Weller. The Board agreed to meet on August 7th to entertain any new proposals on the truck loading on Weller and to primarily go over all the recommendations they are making to DPD.

APPLICANTS PRESENTATION

Residential/Housing Design Components

The applicant introduced representatives from the Seattle Housing Authority and Legacy Partners Residential Inc as two of the future developers of the housing components and Chris Snyder, Landscape Architect from Hewitt, Bill Fuller, Chris Snyder and Darrell Vange presented the 11 x 17 packets to the Board and also used a Power Point presentation.

The design for the residential component and landscaping, open space/public amenity spaces and GF elements is at the schematic stage. Details such as deck location and parapet design and detailed fenestration and materials selection will be developed by the individual developers of the residential component. The schematic design will form the bases for proposals in the future. Each residential design will be required to go through the Design Review/MUP process, including review by the DR Board. These provisos will also include a commitment to affordable and workforce housing, all of which most likely will be included in a Property Use and Development Agreement as a part of the overall permit process and rezone decision by City Council.

The master plan proposes approximately 596 units of housing in 7 residential structures which are now identified with alpha numeric designations: See p. 2 in the July 24th packet.

Building Identifier	Approximate Number of units	Location	Type
A1	84	NW corner at Weller	Senior Affordable
A2	84	SW corner Weller and Corwin Place	To be determined
B1	40	West side of Corwin, N. of Dearborn	
B2	62	NE corner of Dearborn and 13th	
C	170	SW corner of Corwin and Weller	
D1	87	NE corner of Corwin and Dearborn	
D2	69	NW corner of Rainier and Dearborn	
	~596		

The development goal is to achieve a mixed income community. The design shows 7 separate street level entrances/lobbies to the residential structures, with 3 off interior streets and 4 around the perimeter. (pp 4-5). The design includes three on-site residential loading areas and three vehicle entries to underground residential parking areas, one off 13th, one off Weller, and one off of Dearborn.

The applicant presented an area topographic drawing to use in discussing how the project design for the residential components makes a good transition to the surrounding area as it exists and may change with the proposed South Downtown Plan. (VTL- add more-see pp 24-29)

The architect has worked to integrate the residential structure into the architecture of the commercial base. For example, the light yellow color on the commercial base is also applied in the first three levels of the Building C on Weller, Corwin facades. (pp. 8-9). Orange colors are proposed as highlight colors. The design has attempted to clearly identify the residential entrances by proposing a dark weathered copper material that will run from the street level continuously to the roof parapet line.

The architect presented detailed materials and color boards that included cement panels (such as Ceraclad #7) and metal siding (such as AEP-Span Blue) (pp. 10-16)

Public Amenity/open space and Green Factor (.30 of site) schematic design

Chris Snyder, Landscape Architect described the approach to the outdoor areas adjacent to the 7 residential structures emphasizing the desire to create a hierarchy in outdoor spaces as well as in the concept for the landscaping components: (pp 17-24)

- Privacy decks at the base of the residential structures/units
- Common courtyards of about 650 sf that open out from the planned interior community rooms
- Green back-drop areas including green roofs and green walls
- Storm gardens with piped in grey water (pp 20-22)
- HVAC located as far away as possible from residential unit open space
- Dry areas in center surrounded with strips of green planting
- Tree mounds of 4 feet with low 2' high shrubs to allow physical separation, and visual openness and light into the open areas for the whole roof area.

The “green” elements and open usable deck areas are purposely located away from the street edges of the complex. A large area above 13th on the west is designed for HVAC and plain roof ballast. The open space above Dearborn for structures D1 and D2 are also designed away from the Dearborn commercial edge with good solar exposure.

Weller Street Truck Loading and Curb Cut/Driveway width Departure Request

The applicant presented additional analytic diagrams for alternative solutions. The Land Use Code allows DPD to increase curbcuts and driveway widths to 30 ft when large truck loading is involved. DPD staff reported that additional traffic pattern analysis is currently being conducted

by the traffic consultant and DPD's traffic expert, John Shaw. This information should be available prior to issuing the MUP decision.

- The first alternative showed that removed the truck loading access from Weller St. and located it off of the proposed truck access point on 13th. This would cause a loss of about 12,400 sf from the major retail tenant space, which is unacceptable to this tenant since they lose space and individual control of their loading space, which would not be on the same level as the store. (p. 31)
- The next diagram (p.32) responded to board's request to show alternative for trucks traveling westbound from Rainier to the Weller access point. This can be technically accomplished and may address the concerns for truck travel patterns traveling eastbound on Weller, and the turning issues at Jackson and 12th Avenue S.
- Access alternative from Weller using Corwin does not work according to the applicant (p.33).
- Reducing curb width on Weller as another possibility does not work since it compromises safety on Weller per SDOT

The applicant showed the built conditions at the Safeway Tribeca project on lower Queen Anne which has a 38'-8" cut and opening as a successful example of blending truck access and pedestrian environment. They noted how the Lumina project on Mercer has much wider openings on Mercer and has hired a graphic consultant to do visually interesting graphics to help improve the visual appearance. They also cited a section in a new book by Mark Hinshaw that noted how Seattle is on the cutting edge national of integrating large uses like grocery stores into mixed use structures in dense urban neighborhoods, noting Mr. Hinshaw's observation that living close to a vibrant urban core requires really good design and some compromises, but in the end help the overall goal of creating dense urban and pedestrian accessible cores in big cities.

BOARD COMMENTS AND CLARIFYING QUESTIONS

Board comments and questions below are in normal text and the *applicant answers/comments* are in italics:

Board members are interested in details of the proposed Green Screens particularly those proposed for the 5 recessed bays at the west end of the Weller facades. Chris Snyder responded that *This façade is set back significantly from Weller (p. 3). The screens themselves are more like a 3-D grid or cage and abut the vacant warehouse.* One board member noted that the design should limit the use of evergreen vines. *Access to residential loading off Rainier will be done through the freight elevator and a curb setback on Weller will be provided too.* Considerable discussion also occurred over the propose open space areas. *The proposed large courtyards will be about 60' x 80'. There are opportunities for including some pea-patches away from the residential "ground" level.*

One member commented that effective barriers of some kind should be included in the design to prevent users of the open space deck areas from waling out into the Green Roof areas. The "rain garden" idea is a good one and should somehow be carried down to the lower levels. *The applicant clarified that there will be no proposal for landscaping or usable open space or green roofs on the top roofs of the 7 residential structures. In the "root canal opening off of Rainier, there will be some run-off piped into storm detention pipes.*

The board members like the proposed copper “column” at each of the entries, but note that this feature is not currently as successful for the Weller St. façade design. *The stair towers at the corner of t Dearborn and Rainier have an all glass tower that serves the commercial uses and is completely separate from the weathered copper tower that serves the D2 residential structure.*

PUBLIC COMMENTS:

This meeting was well attended and extensive public comments were made. Many of the comments made regarding the size of uses and traffic impacts were reiterated at the July 24th meeting. One comment stated that the purpose for green roofs was to help cool down the heat gain and should be considered for the residential roofs as well. He felt the residential base level roof should be all green and that real play areas for families should be include. Also, comment made that the proposed amount of ventilation equipment is very large and needs more design scrutiny.

The residential design must be integrated into the overall design. The community wants certainty regarding the inclusion of affordable housing and mixed income housing in the proposal. *(Staff and applicant noted that this affordable housing element will be a contractual obligation with some specific dates and will be in the DPD decision and be included in the recommended conditions to the Hearing Examiner and Council for the PUDA.* Comments concerning proposed development and service upgrades to electrical service were provided to the Board.

Another commenter felt that the entrance of Weller for the senior housing was too close to the proposed truck loading and that the proposed Green Wall south of Weller would be dead space. Ventilation of vehicle and truck loading areas should be very sensitively designed particularly if any are proposed near the sidewalk levels. Avoid what happened in Fremont parking vent on N 34th Street at Peet’s Coffee.

Another commenter said the community was exploring whether to limit the size of retail uses propose for the area north of this project Trucks coming to the site should be reduced by one third. Look at Northgate type solution and not Lynnwood solution. This is a management of trucks issue and feels there are other options. Serious consideration should be given to combining truck loading off 13th.

RECOMMENDATIONS OF THE BOARD

Residential and Open Space components

The Board discussed the overall height bulk and scale of the 7 residential structures, and with aid of the larger scale topo map noted that the project area is in a bowl, surrounded by higher ground to the north and east and by the landscape buffer and I-90 structure to the south, with commercial structures to the west. Property to the north is industrial now but will be most likely re-zoned to NC with 85 height limits, while the lower zoning across Rainier is uphill. The design has moved the mass of structure C back from the Rainier commercial base to create a good scaler

relationship, while also creating the Lane Street Plaza midway along Rainier. The design includes a significant set back for structure D2 at the corner of Rainier and Dearborn and the opportunities opened up to the uphill properties for view corridors along Corwin Place and past structure C and through D1 and D2.

One board member thought that structure C was too high above the Weller Street and too high above the pedestrian passageway (“Post Alley”) between the east façade of structure C and the lower commercial element on Rainier.

- This area needs to be open to future design refinements to make this passageway a better space.
- The Board recommends approval of the Height Bulk and Scale (A-5, B-1) design of the residential components as shown on the Design Proposal packet dated July 24th.

The Board feels that the design of the residential entries to date seems to be a 2nd priority and need to be more studied.

- The Board discussed the residential entry designs and recommended that the weathered copper material should be brought down to the street level for all residential entries as shown on the Dearborn elevations. (A-2, A-5) The residential entry design on Weller should be changed to reflect this recommendation.

The Board appreciated the two and three dimensional graphics showing the proposed public amenity, open space and green factor designs (pp. 17-24). The board members initial discussion on these elements noted general satisfaction with the open space areas and the design’s ability to meet the green factor on site. Some concern that the lower roof above Rainier will need some high quality design work. Another member felt that the residential element should be more like the commercial base landscaping vs. running bamboo. The board likes the general size of the 60 x 80 foot “J” shaped open space (p. 17) adjacent to B1 and B2, but thinks the location on the north side of B2 will end up in shadows. This “Post Alley” spaces needs further design work.

- The Board recommends filling in the “J” with usable open space area to allow more sun and larger area for more outdoor activities such as pea patches and play equipment.

The board discussed the proposed edges between walkable surfaces, the green roofs and the common amenity spaces.

- The Board recommended that the applicant explore design techniques to make these edges softer but clear where access is allowed (not onto green roofs). Explore techniques like massing plant material along the edges and providing seating ledges that would also serve as barriers to the green roof, and other measures to create good usable and diverse outdoor areas. (A-7).
- The Board recommended creating more common lobby area spaces so that the residential entries also read well from the outside at this second level residential deck level.
- The Board recommends putting more large scale plantings in the open space areas.

Weller Curb Cut Departure Request

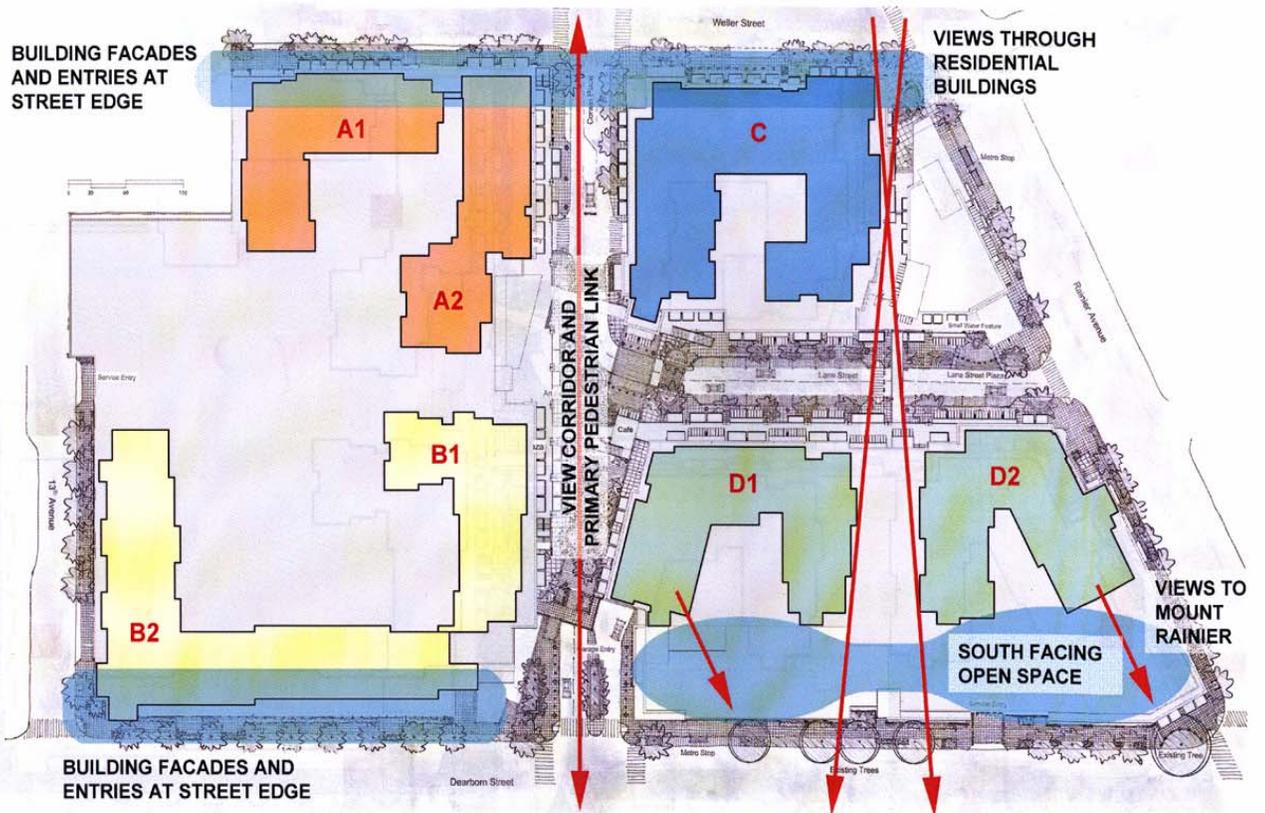
The board had recommended not approving the requested Weller truck curb cut/driveway width increase at the July 10th recommendation meeting, requesting the applicant to study additional solutions and present them at the July 24th meeting.

Initial Board discussion on the departure included the observation that the loading berth configuration remained as a constant in all the alternative studies and should also look at other internal configurations. They acknowledge the applicants point that the design for the whole site has strived to keep the number of curb cuts considerably below the number that could be allowed by the Land Use Code. The board inquired why there needed to be 4 large truck booths and why the big trucks couldn't use the loading area off of 13th if there were really only 2 or 3 large trucks coming to the Weller loading area?

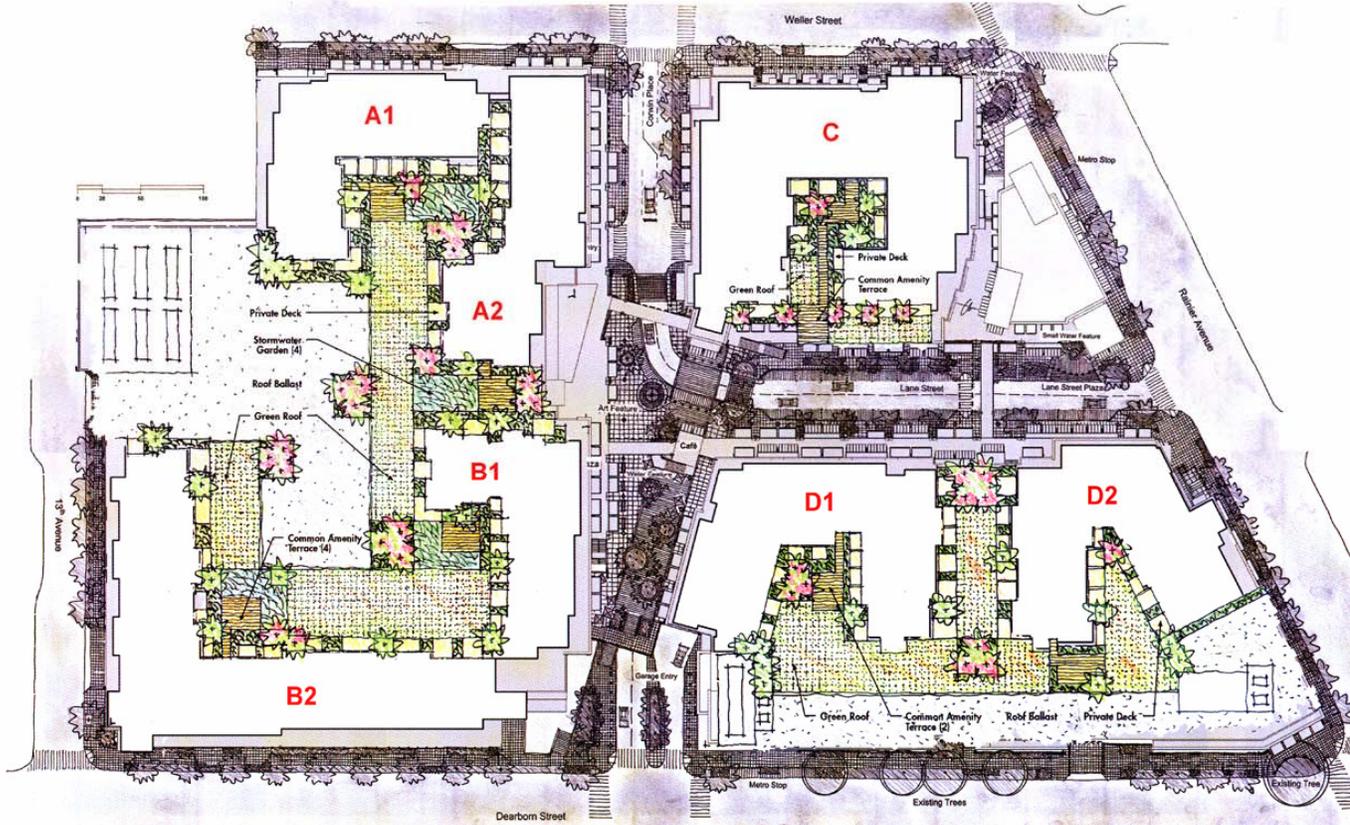
DPD staff noted how other truck openings in the City have been conditioned to make these openings more visually interesting by carefully designing the entry walls, light colors on the soffit, good baffled pedestrian scaled lighting on the inside walls and patterned paving in the sidewalk area to alert pedestrians of the opening. The nature of the doors was also discussed with suggestions like inseting the doors and keeping them closed when no trucks were leaving or coming. The viability of the shallow retailers was discussed, including that these smaller spaces may be more in keeping with smaller Little Saigon scale retailers. The Board still wants to see a more pedestrian design for Weller Streetscape experience.

- The Board recommended 4 to 1 to not approve the current departure request of 40 ft wide curb cut and driveway width. (A-4, A-8, A-9)
- The Board agreed to meet August 7th to go over the totality of their recommendations to DPD on this project one more time, limiting the applicant and public comment periods. The Board will be open to entertain a Code complying alternative for Weller at this August 7th meeting or another solution that adds more retail store frontage on Weller with a different truck loading access point which should include design features to the opening, lighting and door designs.

URBAN PLANNING DIAGRAM - RESIDENTIAL



RESIDENTIAL OPEN SPACE PLAN



FULLER SEARS
ARCHITECTS
DESIGN REVIEW BOARD #8
JULY 24, 2007

MAGNUSSON
KLEMENCIC
ASSOCIATES

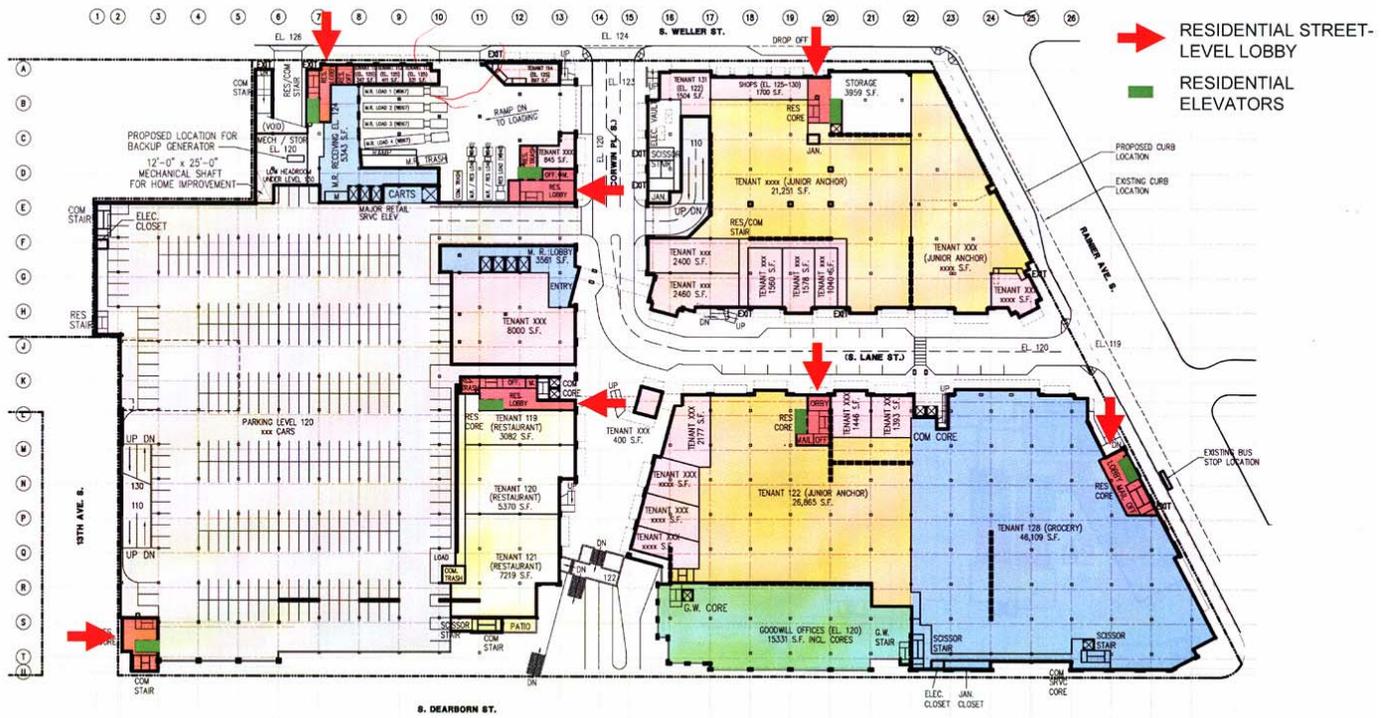
Dearborn Street

SEATTLE, WASHINGTON

HEWITT



RESIDENTIAL LOBBIES AND ELEVATORS



FULLER SEARS ARCHITECTS
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