

EARLY DESIGN GUIDANCE

2301 8th Ave

2300 7th Ave



OWNER	ARCHITECT	STRUCTURE	CIVIL	LANDSCAPE ARCHITECT	MECHANICAL/ PLUMBING	ELECTRICAL	ELECTRICAL	ENERGY SERVICES/ SUSTAINABILITY	PROJECT MANAGEMENT	PROJECT ADDRESS
BRE-BMR 6th 8th LLC. 4570 Executive Dr, Suite 400 San Diego, CA 92121	Perkins + Will 1301 Fifth Avenue Suite 2300 Seattle, WA 98101	kpff 1601 Fifth Avenue Suite 1600 Seattle, WA 98101	kpff 1601 Fifth Avenue Suite 1600 Seattle, WA 98101	Site Workshop 3800 Woodland Park Ave Suite 200 Seattle, WA 98103	MacDonald-Miller 7717 Detroit Ave SW Seattle, WA 98106	Hargis 1201 Third Avenue Suite 600 Seattle, WA 98101	Cochran 12500 Aurora Avenue N Seattle, WA 98133	Rushing 1725 Westlake Avenue N Suite 300 Seattle, WA 98109	OAC 2200 First Avenue S Suite 200 Seattle, WA 98134	•2300 7th Ave •2301 8th Ave Seattle, WA 98121 PROJECT NUMBERS EDG #3040157



AGENDA

SECTION 1.0 / INTRODUCTION

SECTION 01 / PROJECT INFORMATION

- Development Summary
- Existing Site Plan
- Outreach Results
- Neighborhood Map
- Existing Site Conditions
- Urban Context And Character
- Urban Cues

SECTION 02 / ZONING

- Zoning Analysis
- EDG Design Guidelines

SECTION 03 / PROJECT INSPIRATION

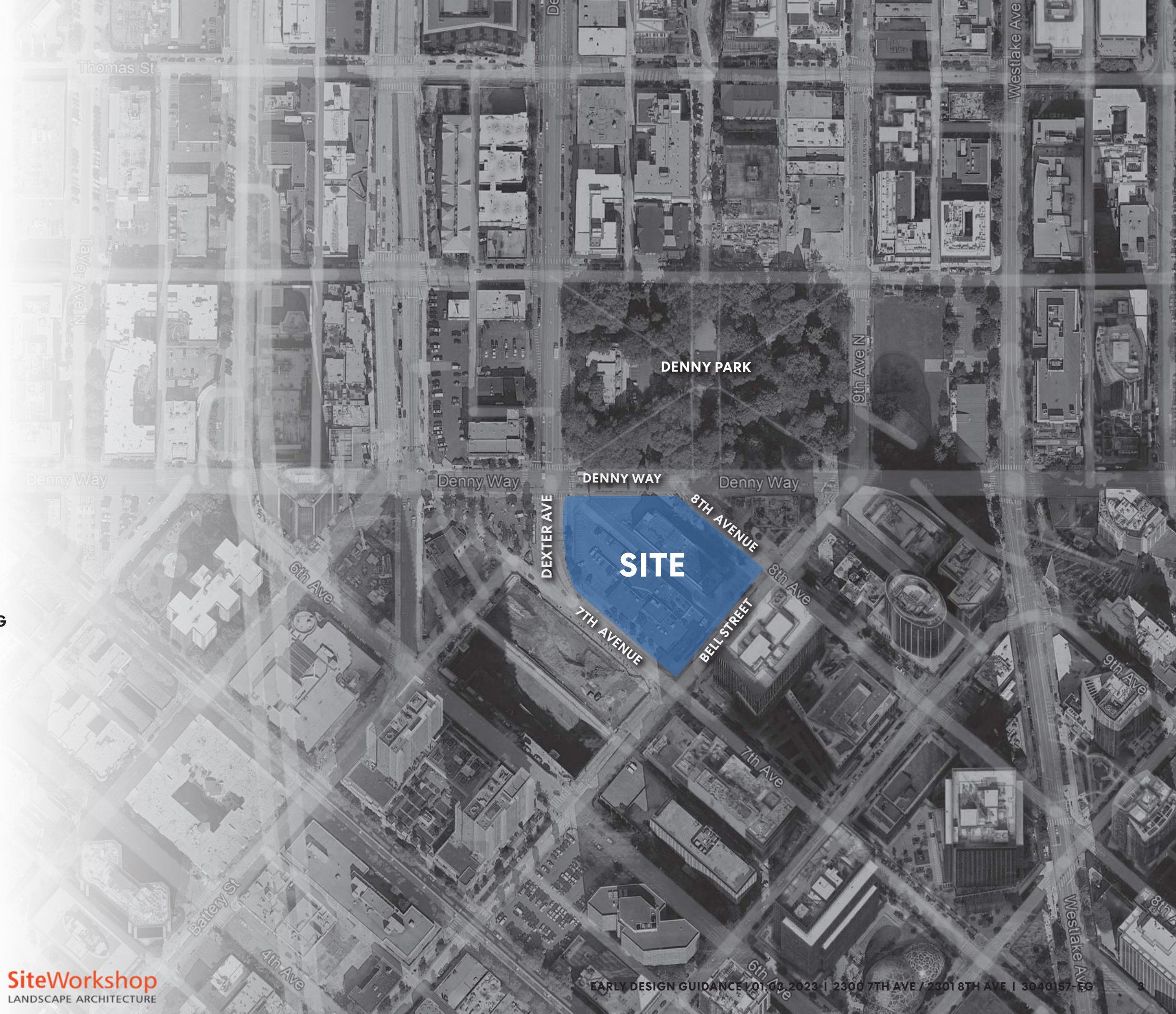
- Site History
- Site Vision
- Neighborhood Inspiration
- Site Approach

SECTION 04 / ARCHITECTURAL MASSING

- Urban Grid Diagrams
- Base - No Alley Vacation
- Scheme 01
- Scheme 02
- Scheme 03 (Preferred)

SECTION 05 / DEPARTURES

- Rooftop Requirements
- Facade Setback Limits
- Facade Modulation
- Green Street Blank Facade Limits
- Curb Cuts



01 PROJECT INFORMATION

1.0 | PROJECT INFORMATION DEVELOPMENT SUMMARY

Above-Grade Development:

- Area above grade: ~600,000 SF
- Two towers:
 - North tower: 18 Stories
 - South tower: 11 stories

Below-Grade Development:

- 4 full stories below grade
- ~600 below grade parking stalls

Site / Landscape Development:

- Through-block connection
- Open space



1.0 | PROJECT INFORMATION
EXISTING SITE PLAN

	PARCEL (A) EAST	PARCEL (B) WEST
Address:	2301 8TH AVE. SEATTLE, WA 98121	2300 7TH AVE. SEATTLE, WA 98121
Owner's name:	BMR OWNERSHIP ENTITY: BRE-BMR 8TH LLC	BMR OWNERSHIP ENTITY: BRE-BMR 8TH LLC
Legal Description:	LOTS 1-12, BLOCK Z, BELL'S 6TH ADD, LESS ADJ ST (CO #50890), PENDING ALLEY VAC	LOTS 1-12, BLOCK Z, BELL'S 6TH ADD, LESS ADJ ST (CO #50890), PENDING ALLEY VAC
Parcel Number:	069700-0435-03 LOTS 7-12	069700-0400-04 LOTS 1-6
MUP #:	3039757-LU	3039734-LU

AREA:
PARCEL A: 34,236 SF
PARCEL B: 35,029SF
ALLEY: 5,766SF

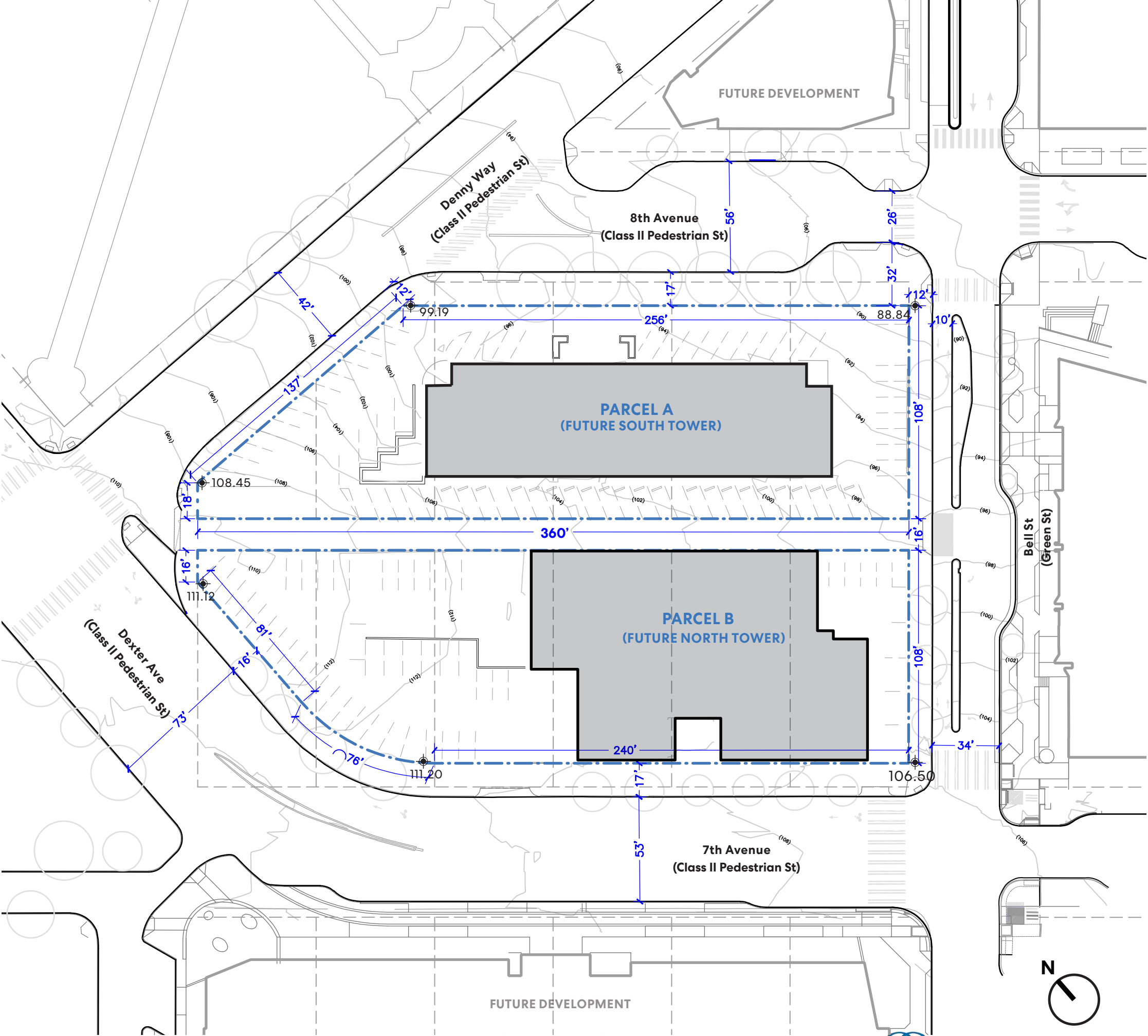
TOTAL LAND AREA: 75,031 SQUARE FEET

ZONE: DOWNTOWN MIXED COMMERCIAL
DMC 240/290-440

HEIGHT LIMIT: NON-RES 240FT

FAR: BASE FAR OF 5; MAX FAR OF 8

BASE FAR = 375,155SF (75,031 X 5.0)
MAX FAR = 600,248SF (75,031 X 8.0)



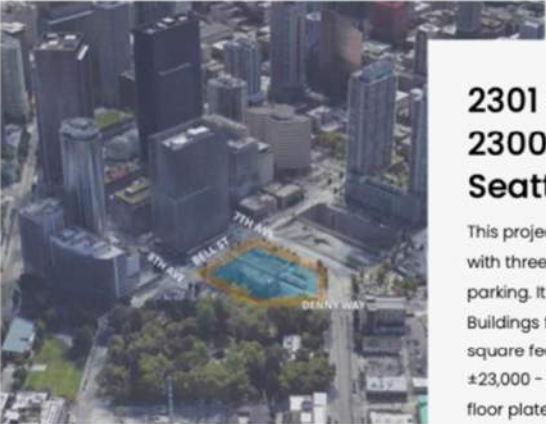
1.0 | PROJECT INFORMATION
OUTREACH

OUTREACH WEBSITE IMAGE

2301 8th Ave & 2300 7th Ave Project

Welcome to our Project Website, which is part of the City of Seattle’s Required Outreach in advance of Design Review. While the project is in its early stages, the information on this site will give you a sense of the project vision, timelines and how we’re approaching design.

Please feel free to take the Project Survey and/or leave Comments. Note that all information obtained will be part of the Documentation for this effort and is considered public comment.



2301 8th Ave & 2300 7th Ave, Seattle

This project proposes two towers with three levels of underground parking. It will be Class-A Buildings featuring ±600,000 square feet over expansive ±23,000 - ±26,000 square feet floor plates within each tower.

[Learn More](#)

OUTREACH SUMMARY

DESIGN COMMENTS

WHEN ASKED WHAT WAS MOST IMPORTANT IN THE NEW DESIGN 73% OF SURVEY RESPONDENTS FELT THAT RELATIONSHIP TO NEIGHBORHOOD CHARACTER WAS IMPORTANT. 65% FELT AN INTERESTING AND UNIQUE DESIGN WAS IMPORTANT. 56% NOTED THE USE OF ATTRACTIVE MATERIALS AND 9% FELT THAT ENVIRONMENTALLY FRIENDLY FEATURES WERE IMPORTANT. WHEN ASKED WHAT INSPIRES PEOPLE TO RETURN TO A BUILDING, OFFICE, RESTAURANT OR RETAILER 67% SAID GREAT PEOPLE AND SERVICE, 58% SAID THOUGHTFUL DESIGN THAT IS OPEN AND WELCOMING, 58% SAID A SENSE OF OPENNESS AND NATURAL LIGHT; 42% SAID LOCAL OR SMALL BUSINESSES, 42% SAID CALM, RESTFUL SPACES TO REFLECT AND RELAX, 33% SAID COLOR AND MATERIALS USED IN DESIGN AND 33% SAID BUSTLING, EXCITING ENERGY.

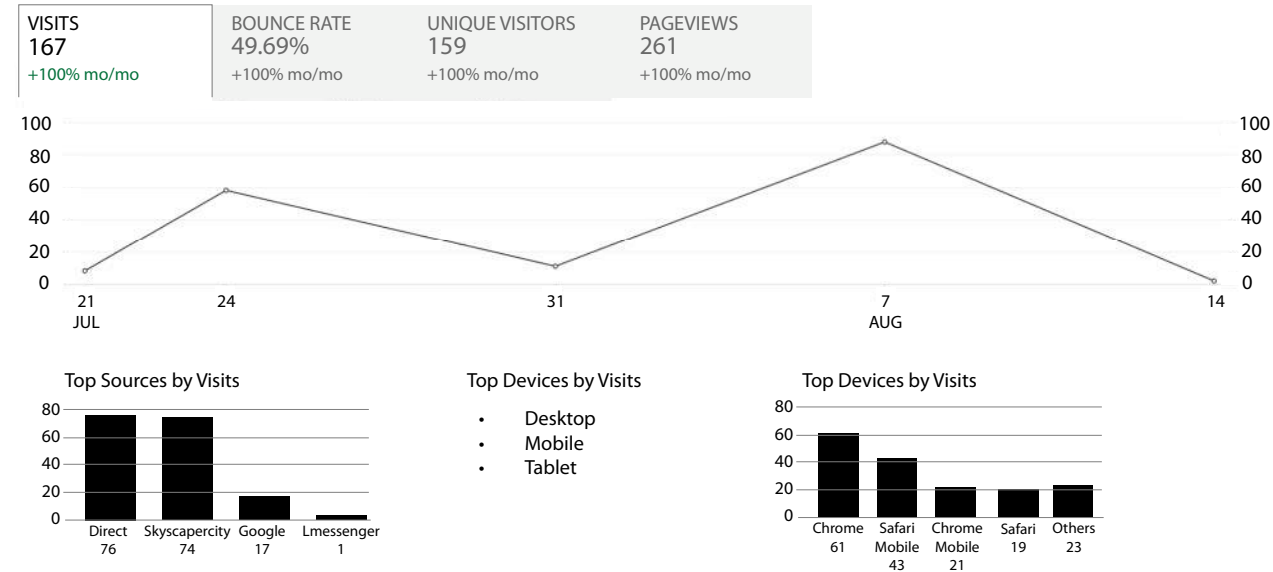
EXTERIOR

WHEN ASKED WHAT WAS MOST IMPORTANT IN THE EXTERIOR SPACE DESIGN 100% SAID LANDSCAPING. 58% SAID LIGHTING AND SAFETY FEATURES. 42% SAID SEATING OPTIONS AND PLACES TO CONGREGATE AND 8% SAID BIKE PARKING. ONE RESPONDENT ENCOURAGED A PEDESTRIAN FRIENDLY ENVIRONMENT, AND ANOTHER ENCOURAGED MAINTAINING CLEANLINESS.

HEIGHT AND SCALE

A COUPLE OF RESPONDENTS ASKED ABOUT BUILDING HEIGHT RELATED TO PARK ADJACENCY AND BLOCKING CURRENT VIEWS. OTHERS ENCOURAGED BLOCKING FEWER VIEWS FROM NEIGHBORS.

WEBSITE ANALYTICS



MISCELLANEOUS COMMENTS

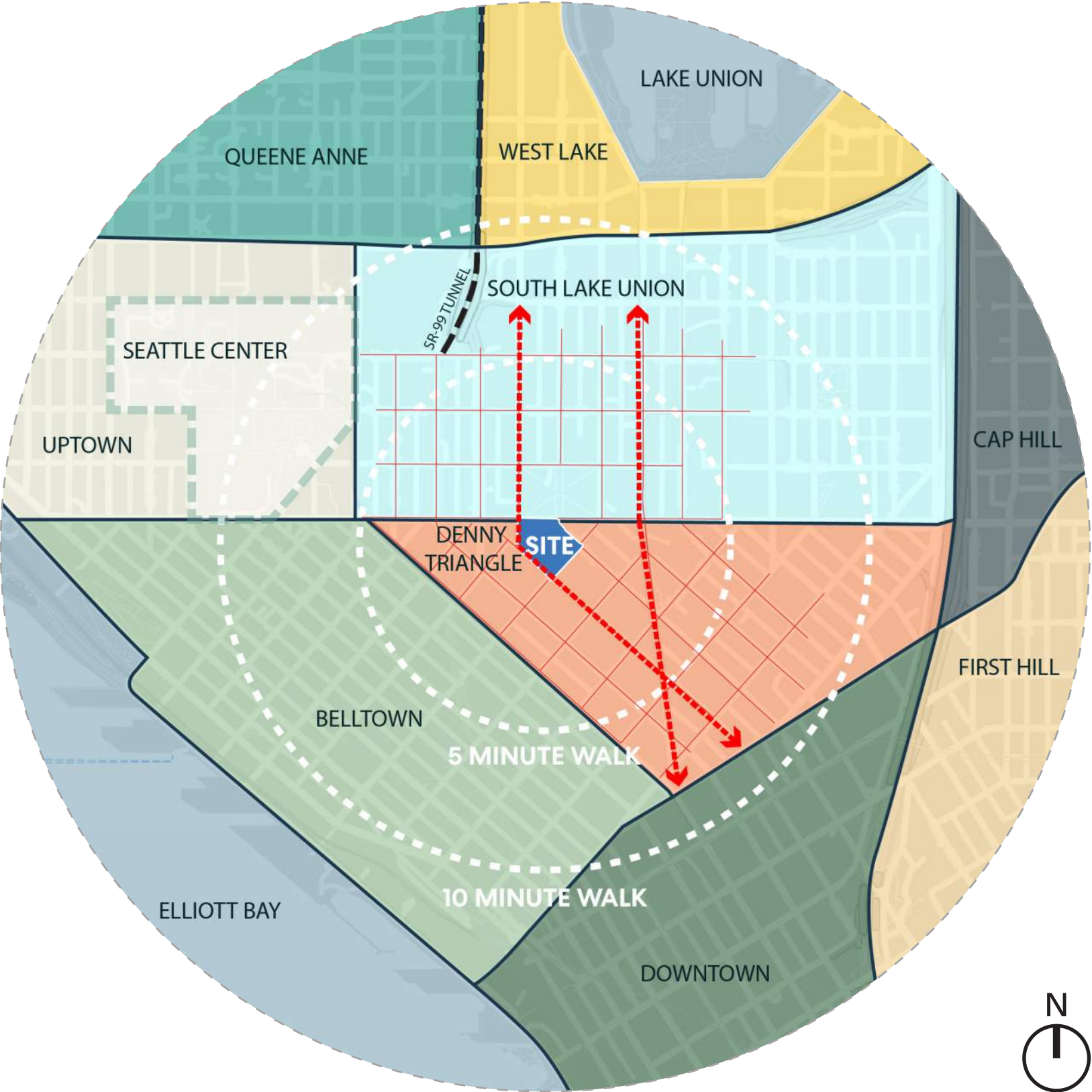
THERE WERE 2 COMMENTS VOICING SUPPORT OF NEW DEVELOPMENT IN THE AREA (COMMENCING AS SOON AS POSSIBLE!) WITH THE RECOGNITION THAT IT WILL BRING MORE SERVICES, TRANSIT AND PEDESTRIANS.



1.0 | PROJECT INFORMATION
NEIGHBORHOOD MAP

THE SITE IS THE NEXUS POINT WHICH CONNECTS THE ORTHAGONAL GRID OF SOUTH LAKE UNION WITH THE OFFSET GRID OF THE DOWNTOWN CORE.


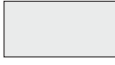





←--→ GRID SHIFT

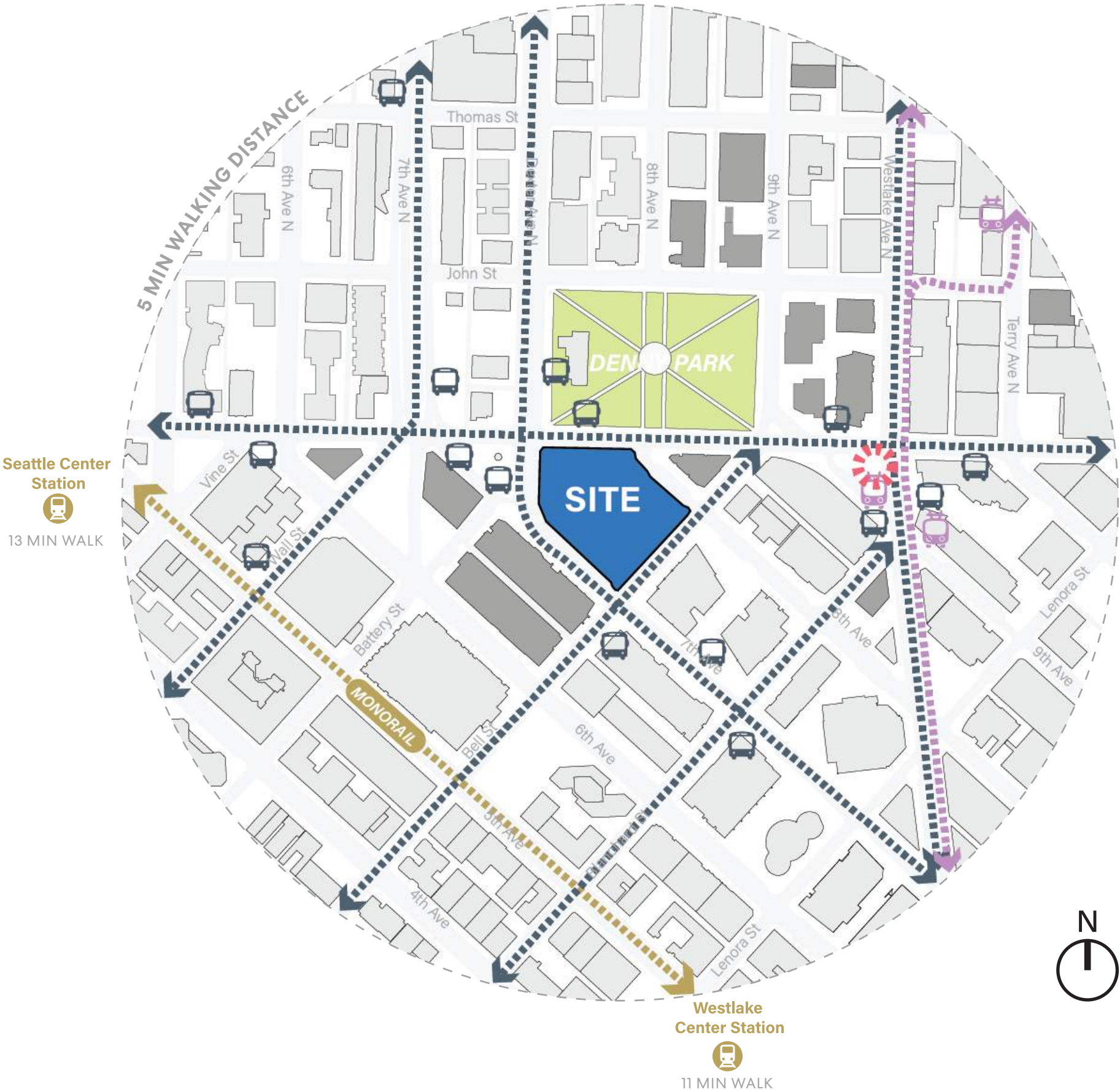


1.0 | PROJECT INFORMATION

PUBLIC TRANSPORTATION

THE SITE IS WELL CONNECTED TO THE PUBLIC TRANSPORTATION NETWORK WITH CLOSE PROXIMITY TO MAJOR BUS SERVICE, MONORAIL, STREETCAR AND A LIKELY FUTURE LIGHT RAIL TRANSIT STATION.

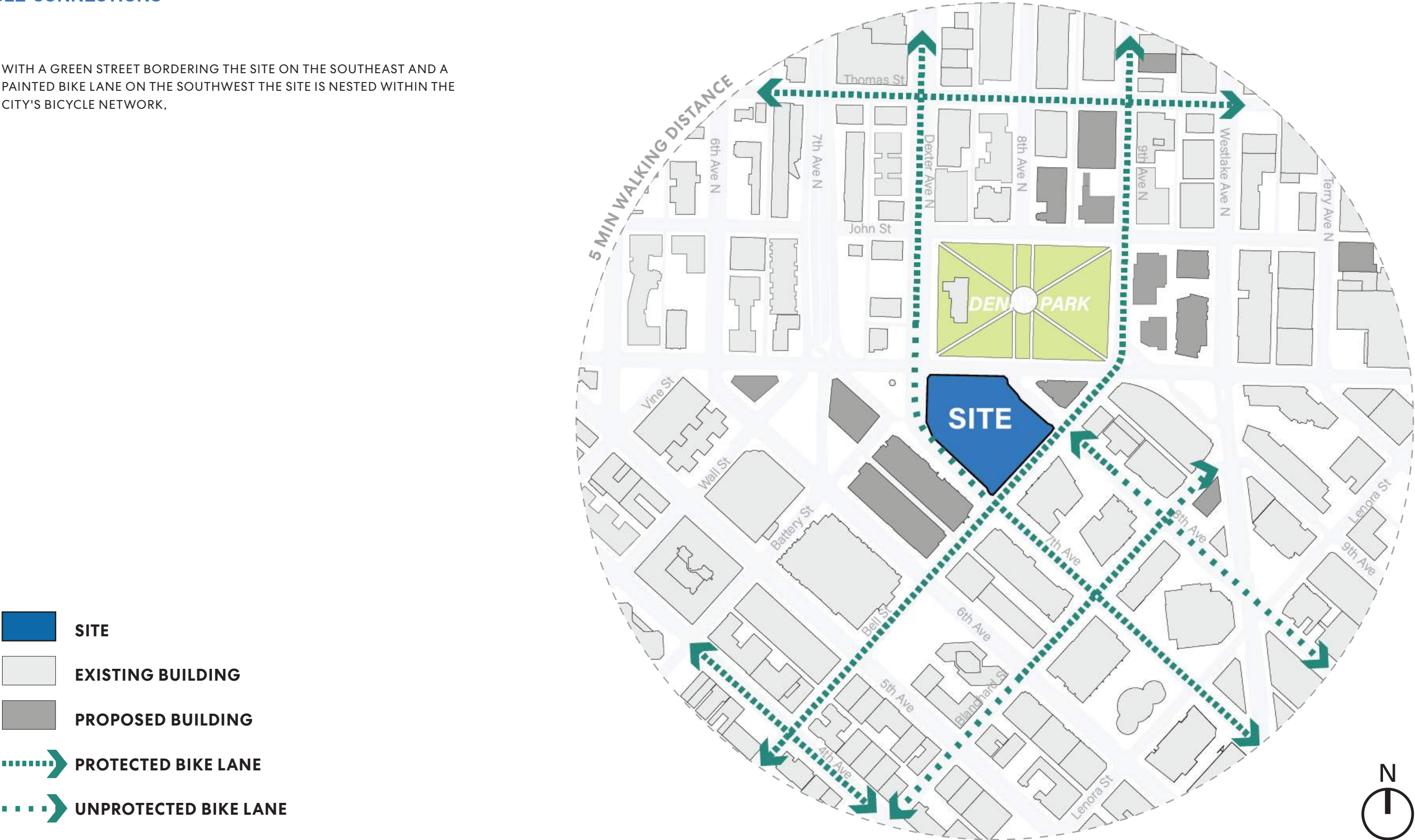
-  SITE
-  EXISTING BUILDING
-  PROPOSED BUILDING
-  BUS STOPS
-  STREET CAR
-  MONORAIL
-  POTENTIAL LIGHT RAIL STATION



1.0 | PROJECT INFORMATION

BICYCLE CONNECTIONS

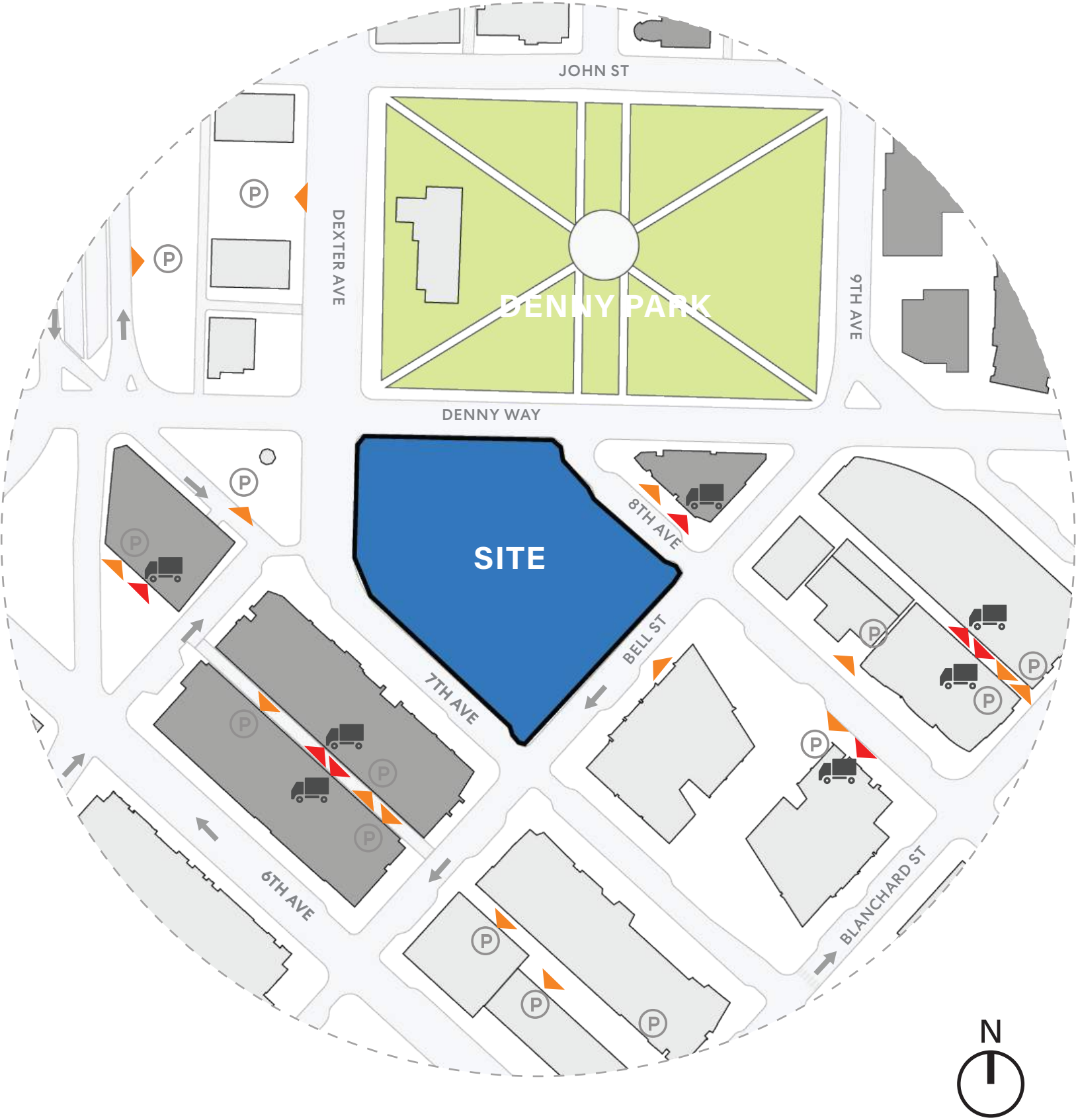
WITH A GREEN STREET BORDERING THE SITE ON THE SOUTHEAST AND A PAINTED BIKE LANE ON THE SOUTHWEST THE SITE IS NESTED WITHIN THE CITY'S BICYCLE NETWORK,



1.0 | PROJECT INFORMATION
LOADING ACCESS AND PARKING

EXISTING NEIGHBORHOOD LOADING PATTERNS FAVOR 8TH AVE FOR GARAGE AND LOADING DOCK ACCESS.

-  SITE
-  EXISTING BUILDING
-  PROPOSED BUILDING
-  LOADING
-  PARKING
-  ONE-WAY STREET



1.0 | PROJECT INFORMATION
PEDESTRIAN CONNECTIONS

NEW DEVELOPMENTS PATTERNS TO THE SOUTHEAST ARE CREATING A MORE POROUS URBAN GRID THAT CAN BE REPLICATED AND STRENGTHENED. WITH THE GREEN STREET TO THE SOUTHEAST THE SITE HAS STRONG PEDESTRIAN CONNECTIONS.








-  SITE
-  EXISTING BUILDING
-  PROPOSED BUILDING
-  DENNY PARK
-  LIGHT PEDESTRIAN FLOW
-  HEAVY PEDESTRIAN FLOW
-  GREEN STREET
-  CLASS II PEDESTRIAN STREET
-  CLASS III PEDESTRIAN STREET
-  ALLEY

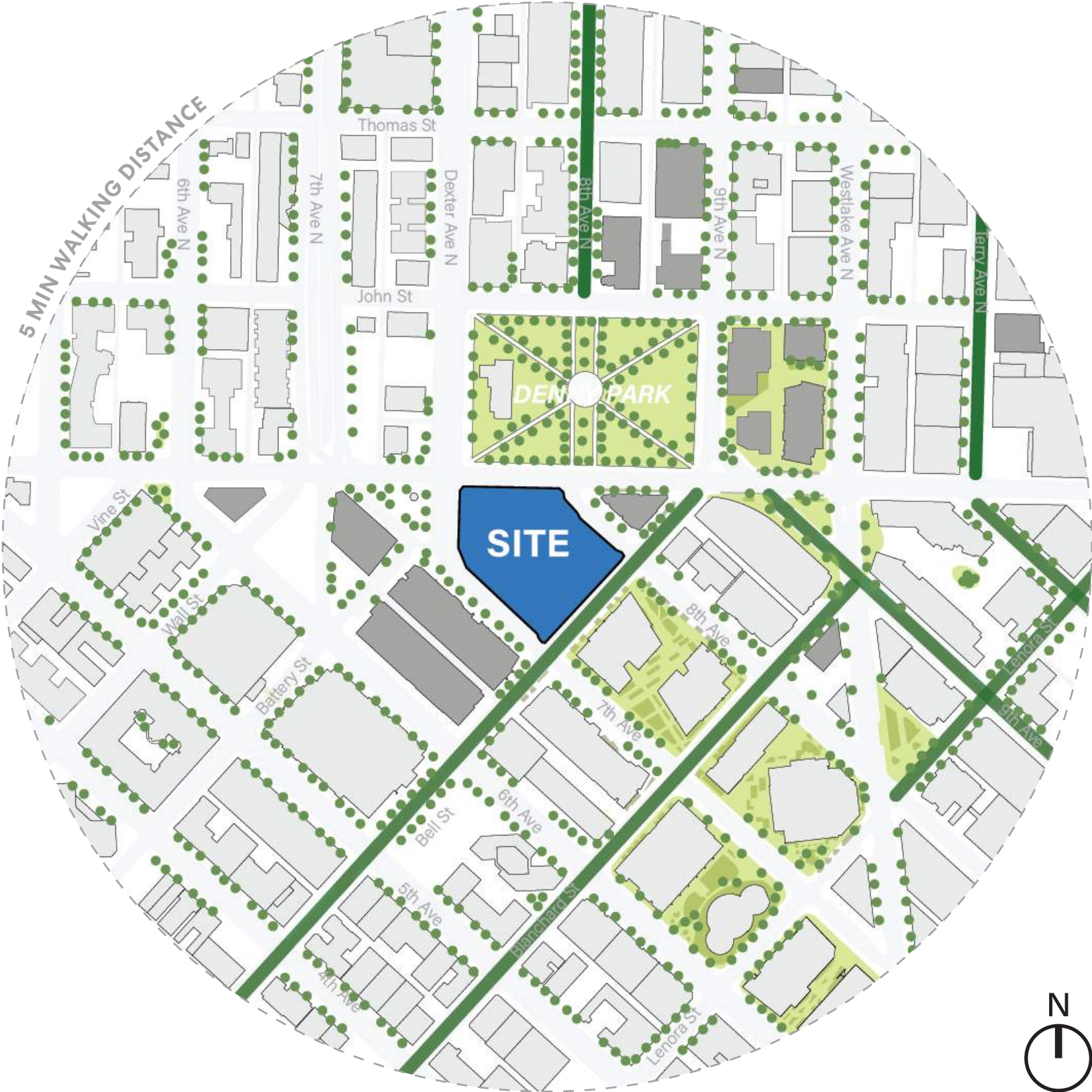


1.0 | PROJECT INFORMATION

OPEN SPACE AND GREEN STREETS

WITH THE NEW GREEN OPEN SPACE PLAZAS TO THE SOUTHEAST AND DENNY PARK TO THE NORTH THE OPPORTUNITY EXISTS TO MAKE A CONNECTION ACROSS DENNY WAY AND EXPAND THE CITY'S 'GREEN CANOPY'.

-  SITE
-  EXISTING BUILDING
-  PROPOSED BUILDING
-  URBAN ARBORETUM/PUBLIC
-  DENNY PARK
-  TREES
-  GREEN STREET

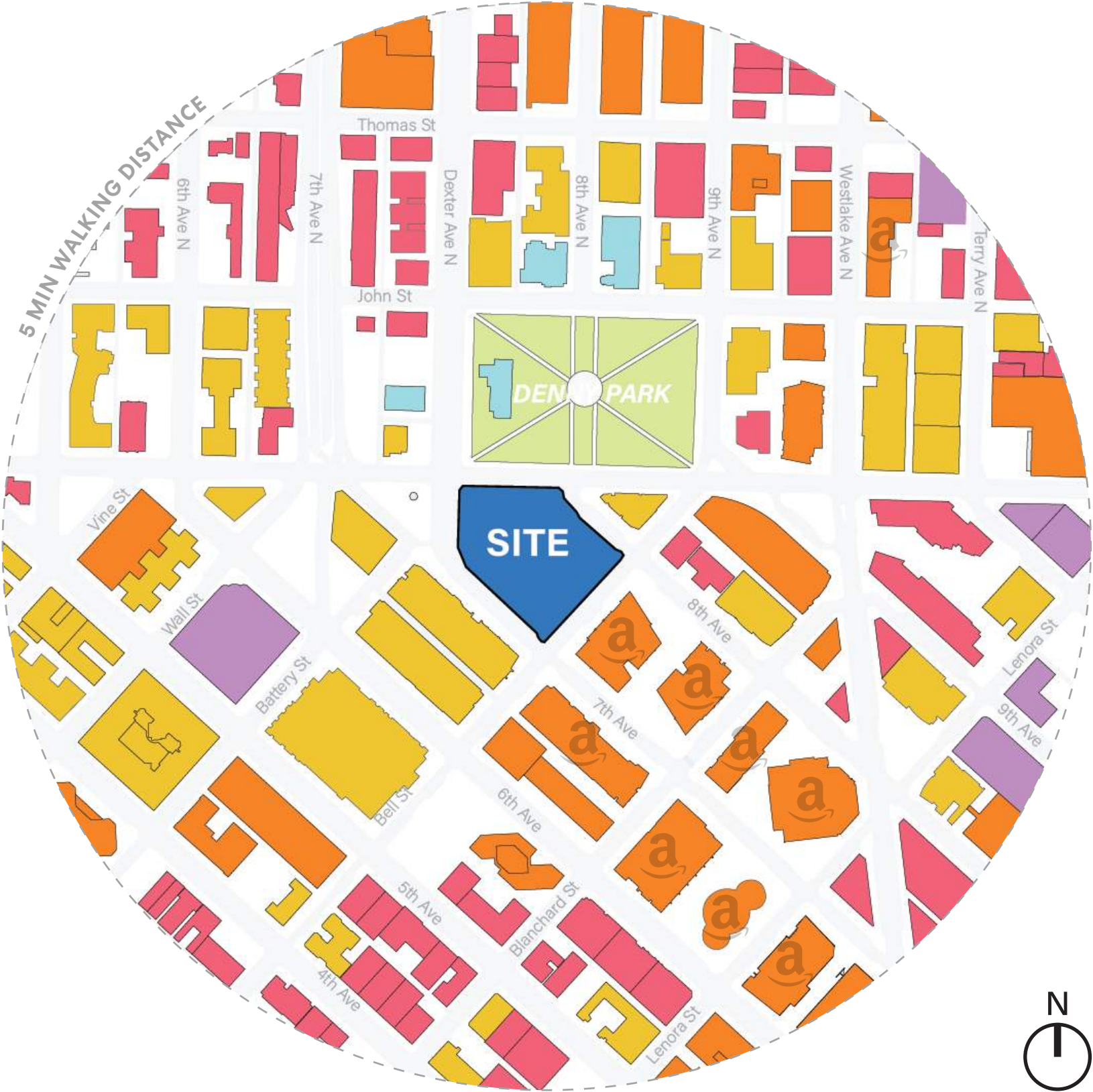


1.0 | PROJECT INFORMATION

SURROUNDING USES

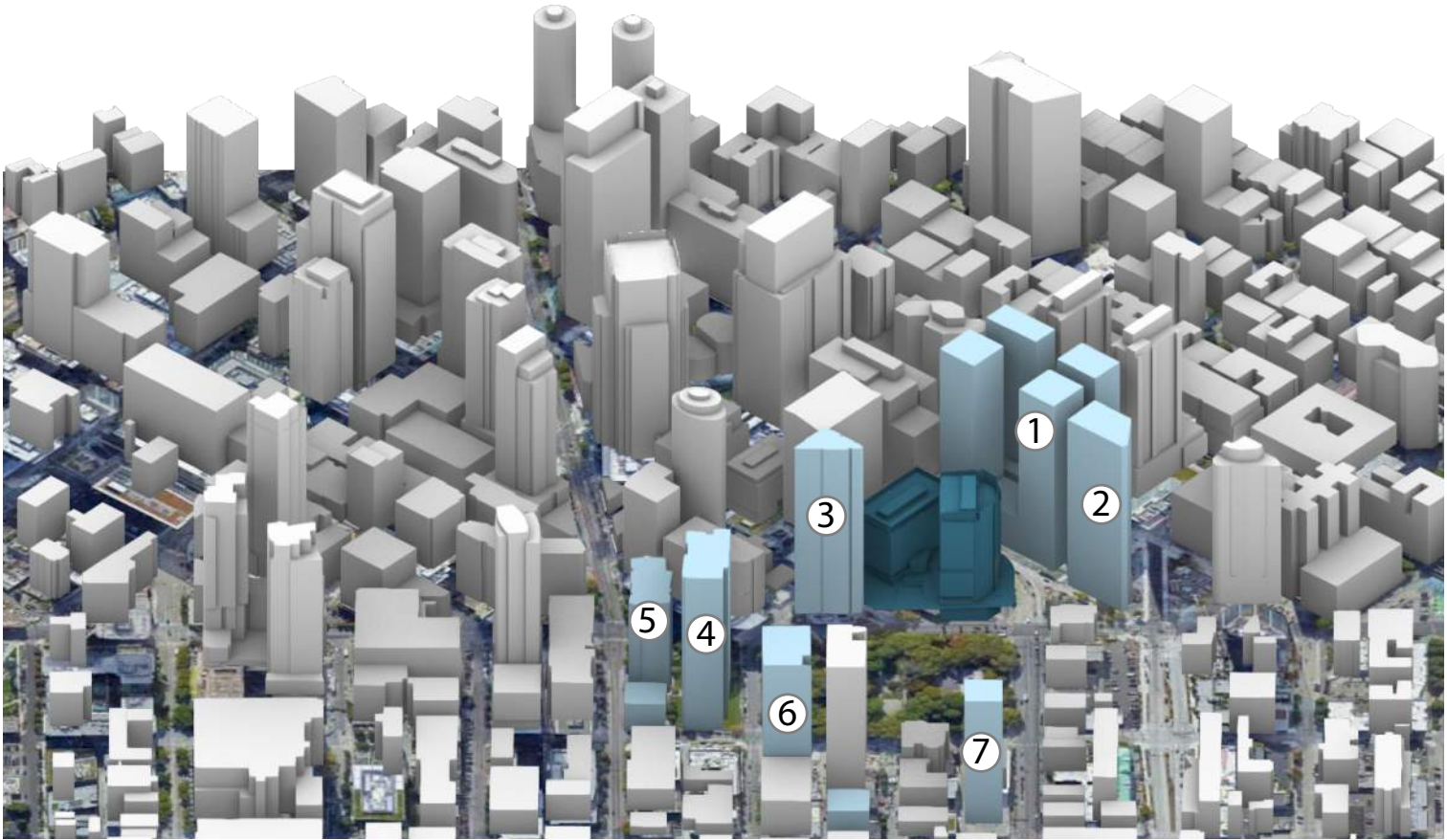
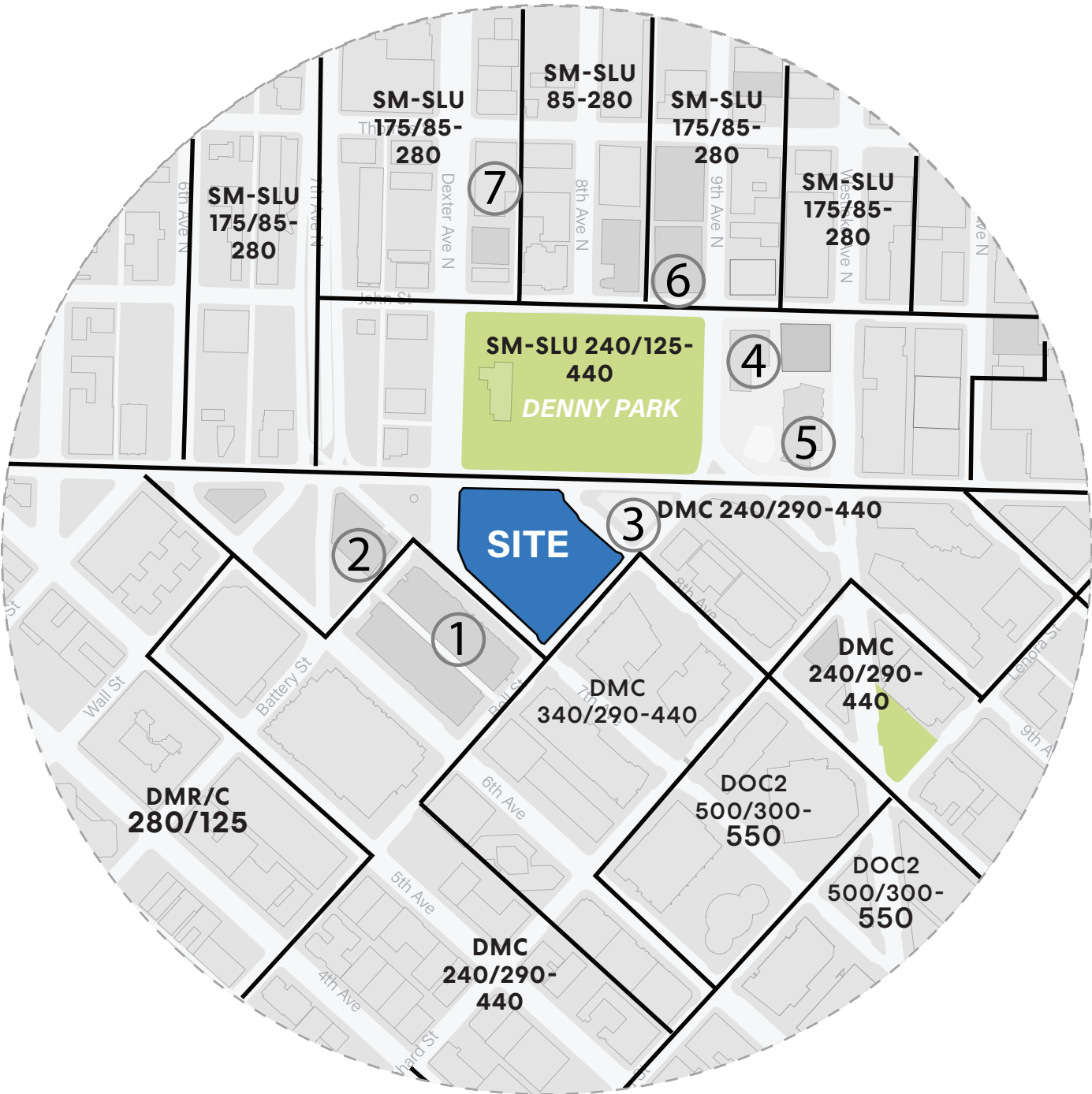
THE SITE IS WITHIN A DIVERSE COMMUNITY OF USES. IMMEDIATELY SURROUNDING THE SITE ARE A SERIES OF 440' RESIDENTIAL TOWERS AND DENNY PARK. THIS PROJECTS LAB/OFFICE PROGRAM AT 240' OFFERS RESPITE ALONG THE PARK.

-  SITE
-  RESIDENTIAL
-  OFFICE
-  COMMERCIAL
-  CHURCH/HEALTH
-  EDUCATION



1.0 | PROJECT INFORMATION PLANNED DEVELOPMENTS

SITTING DIRECTLY TO THE SOUTH OF DENNY PARK THE SITE IS ONE OF THE LAST IN THE AREA TO BE PLANNED FOR REDEVELOPMENT. THE SITE IS SURROUNDED BY FUTURE 440' RESIDENTIAL TOWERS. THIS PROJECT AIMS TO ANCHOR IT'S POSITION AT THE CORNER OF DEXTER AND DENNY WITH A STRIKING BUILDING THAT ALSO OFFERS RELIEF FROM SURROUNDING DEVELOPMENTS WITH AN OPEN, POROUS GROUND PLANE.



1.0 | PROJECT INFORMATION

DENNY WAY NEIGHBORHOOD OPEN SPACE CHARACTER



1. SEATTLE CENTER



2. WALGREENS DRIVE THRU



3. DENNY PARK



4. 111 WESTLAKE AVE N



5. 3RD AVE AND BOARD ST



6. 4TH AVE



7. 5TH AVE AND CEDAR ST



8. 5TH AVE AND TAYLOR ST



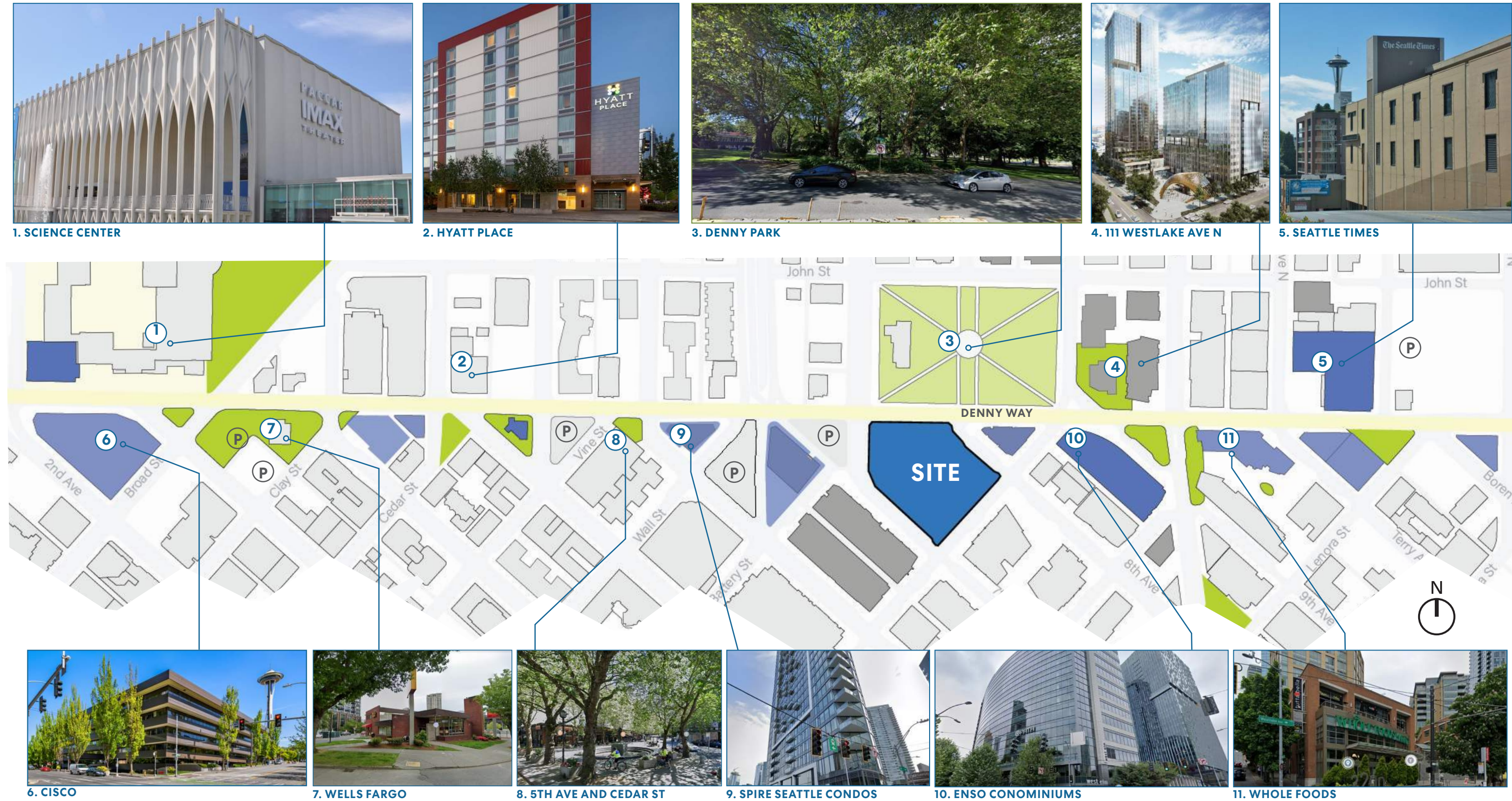
9. 6TH AVE AND DENNY WAY POCKET PARK



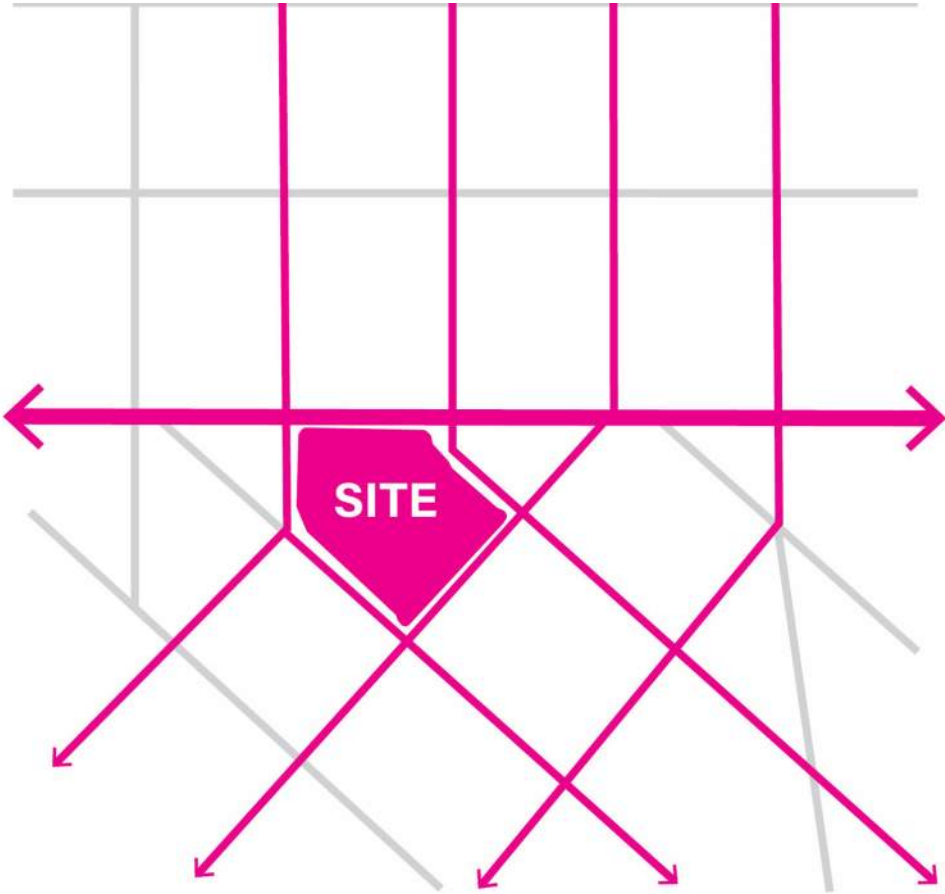
10. 9TH AVE AND WESTLAKE ST POCKET PARK

1.0 | PROJECT INFORMATION

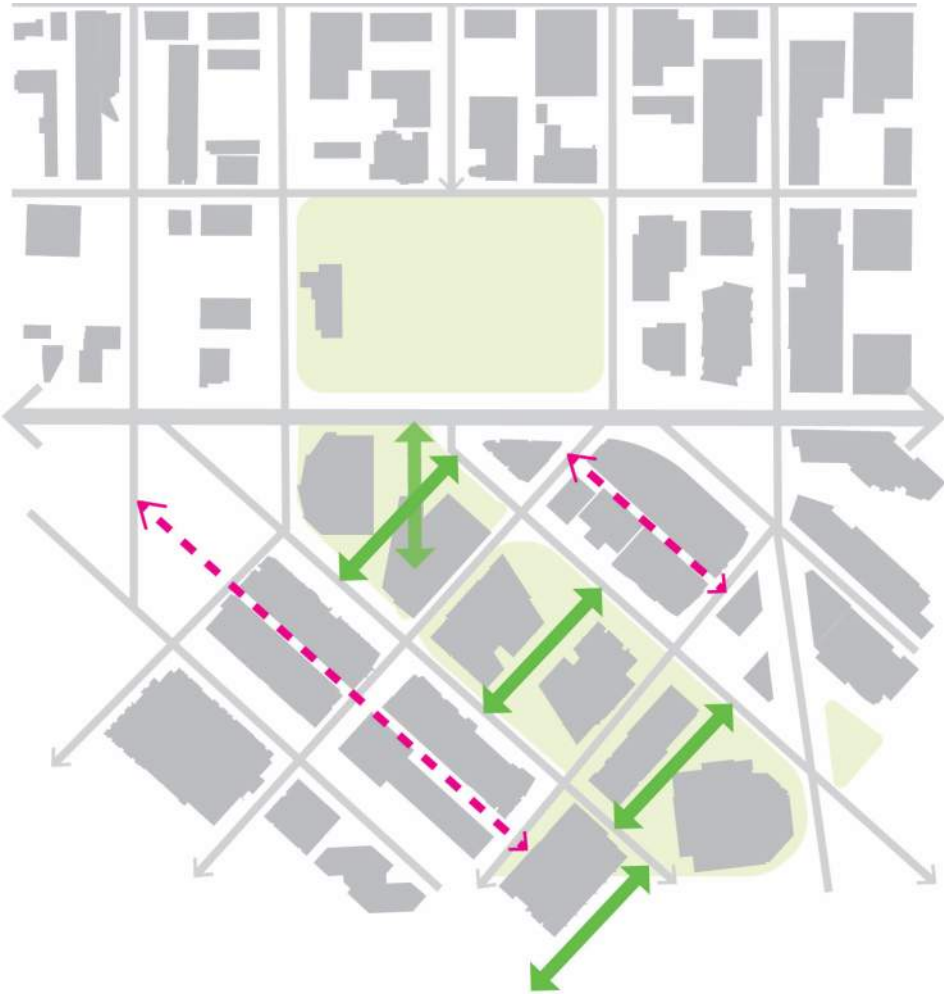
DENNY WAY NEIGHBORHOOD BUILDING CHARACTER



1. Stitch the Grid



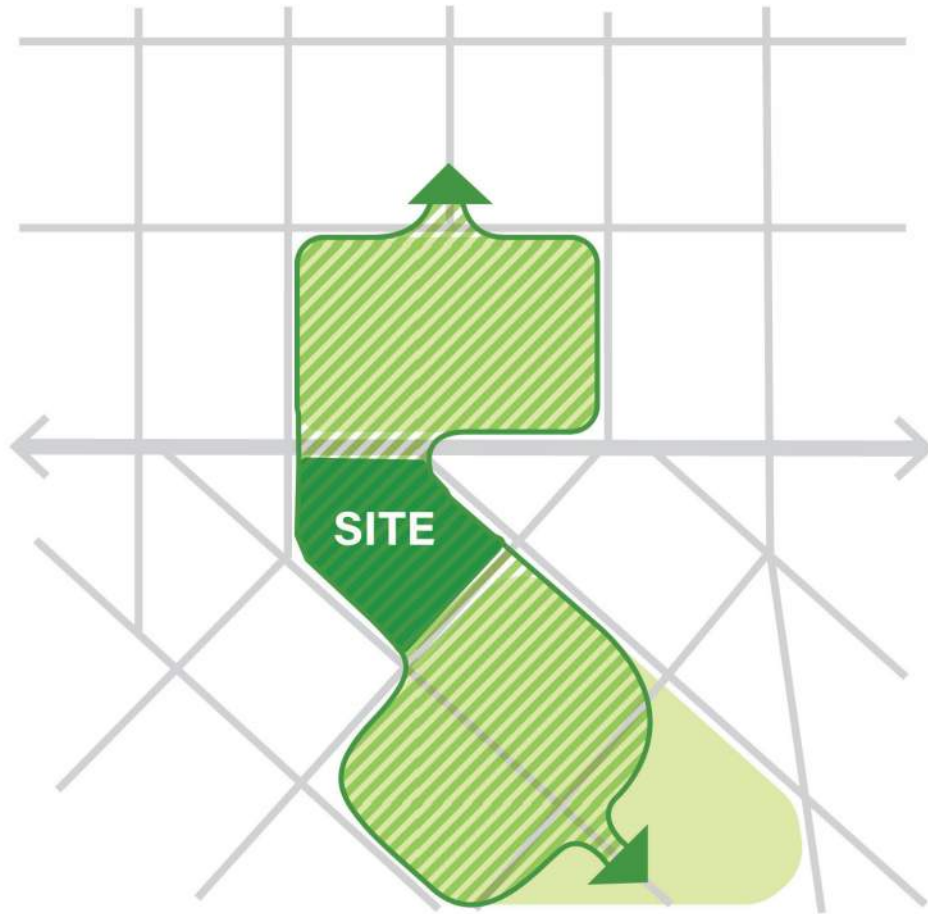
2. Complete the 'Urban Arboretum'



3. Follow block / massing pattern



1. Connect Green Space



2. Diversify Open Space Opportunities



3. Create Mid-block Porosity



GREEN CONNECTION



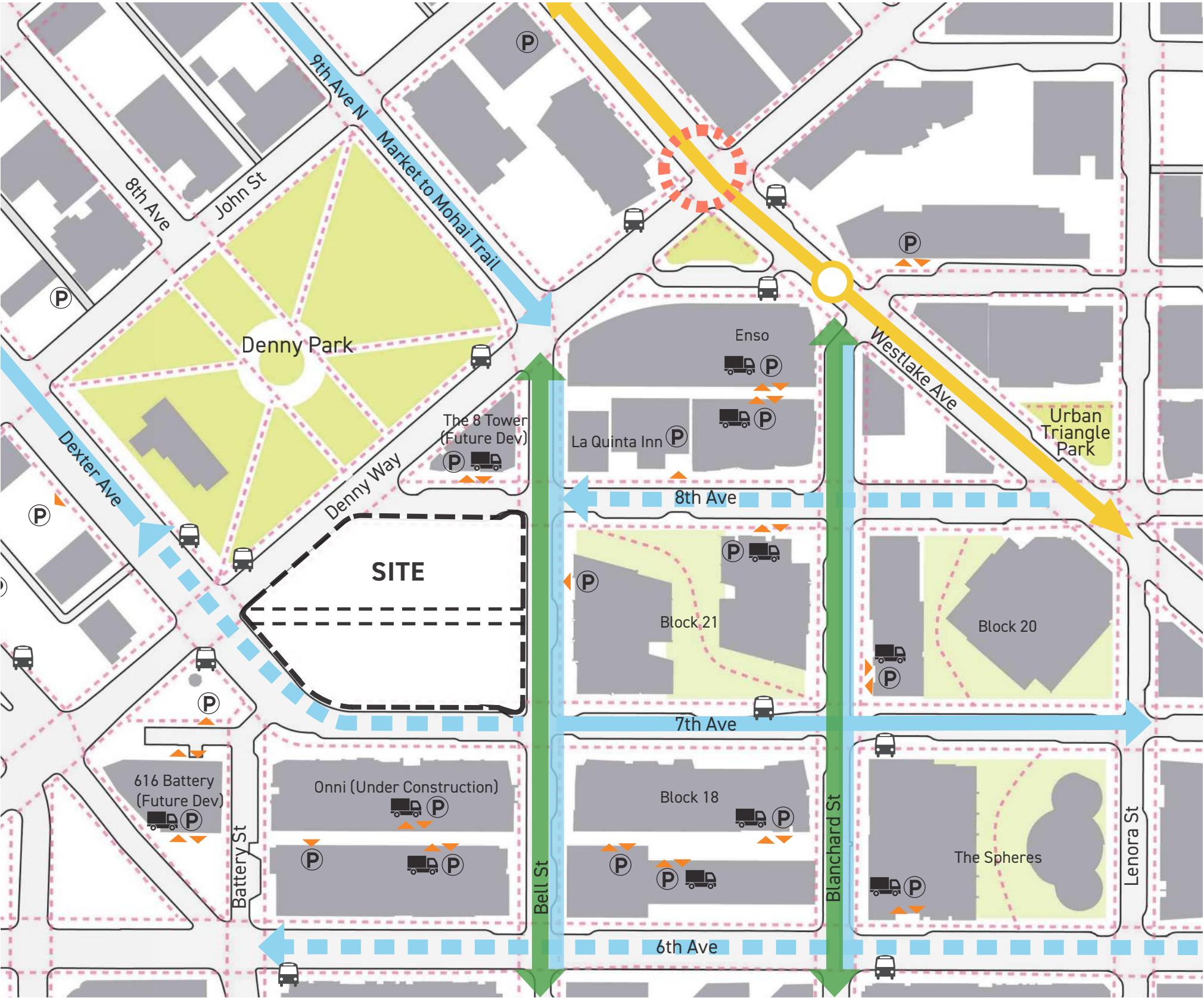
CONNECTION & GATEWAY

Located at the nexus point between downtown and South Lake Union on the corner of Dexter and Denny this new project signals a landscaped connection point and gateway to downtown. Influenced by the site history, the surrounding developments and a vision for a biophilic laboratory project this project seeks to connect the 'urban arboretum' to the south with Denny Park to the north.

GATEWAY TO DOWNTOWN



1.0 | PROJECT INFORMATION
NEIGHBORHOOD CONTEXT

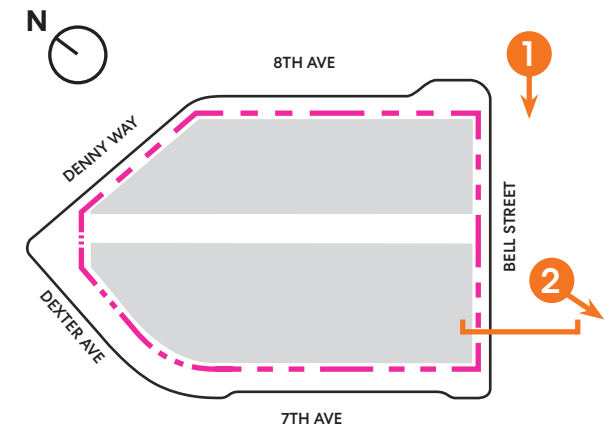
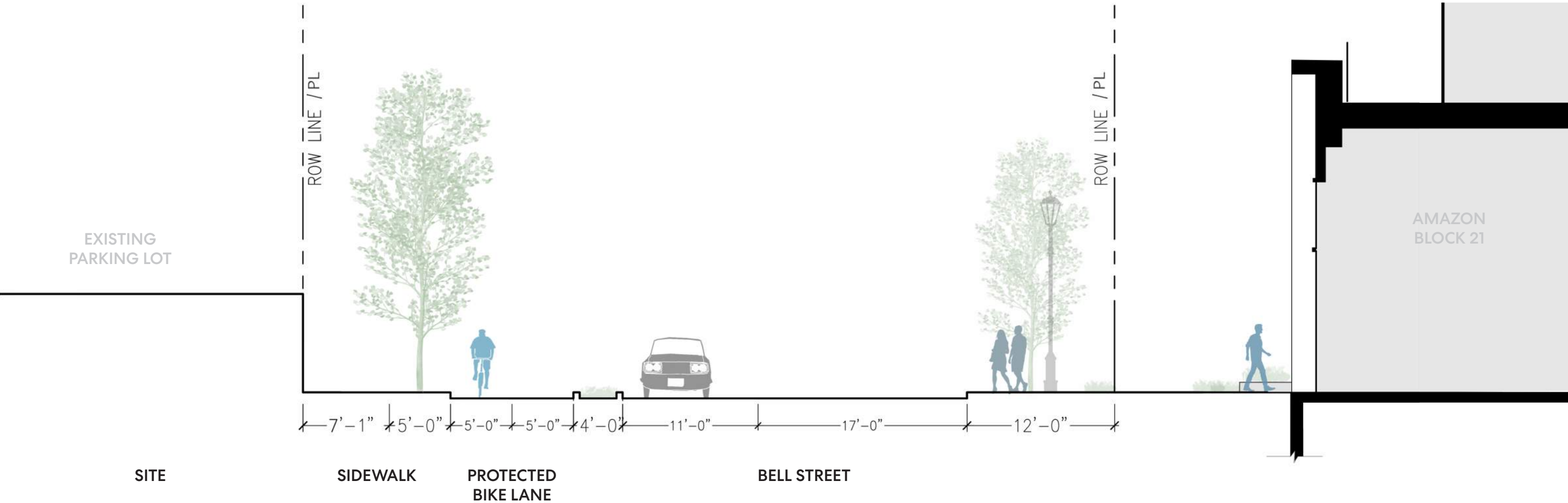


LEGEND

- Existing Alley
- Parks / Neighborhood Open Space
- Green Street
- Existing Protected Bike Lanes
- Existing Painted Bike Lanes
- Street Car Route And Stop
- Bus Stop
- Parking / Loading
- Vehicular Entry / Exit
- Potential Sound Transit Link Extension
- Pedestrian Circulation



1.0 | PROJECT INFORMATION
BELL STREET - EXISTING CONDITION



1 BELL ST. LOOKING WEST

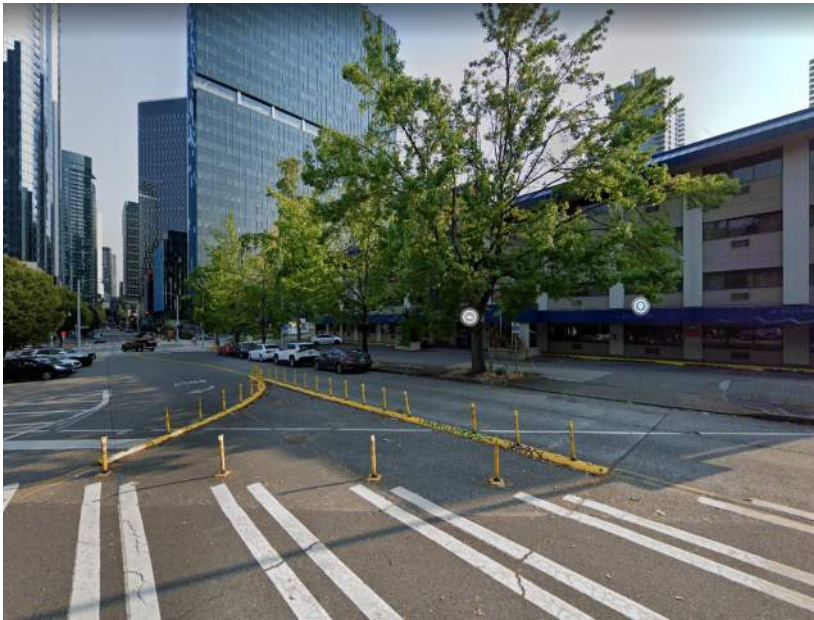
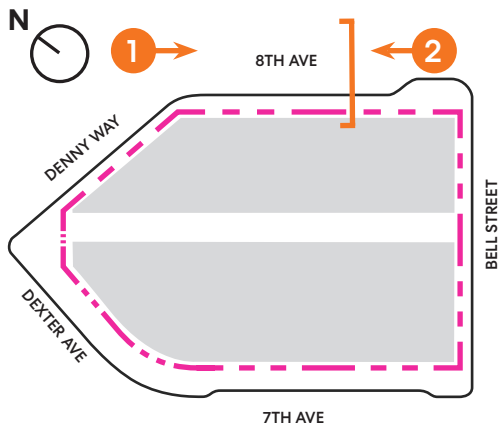
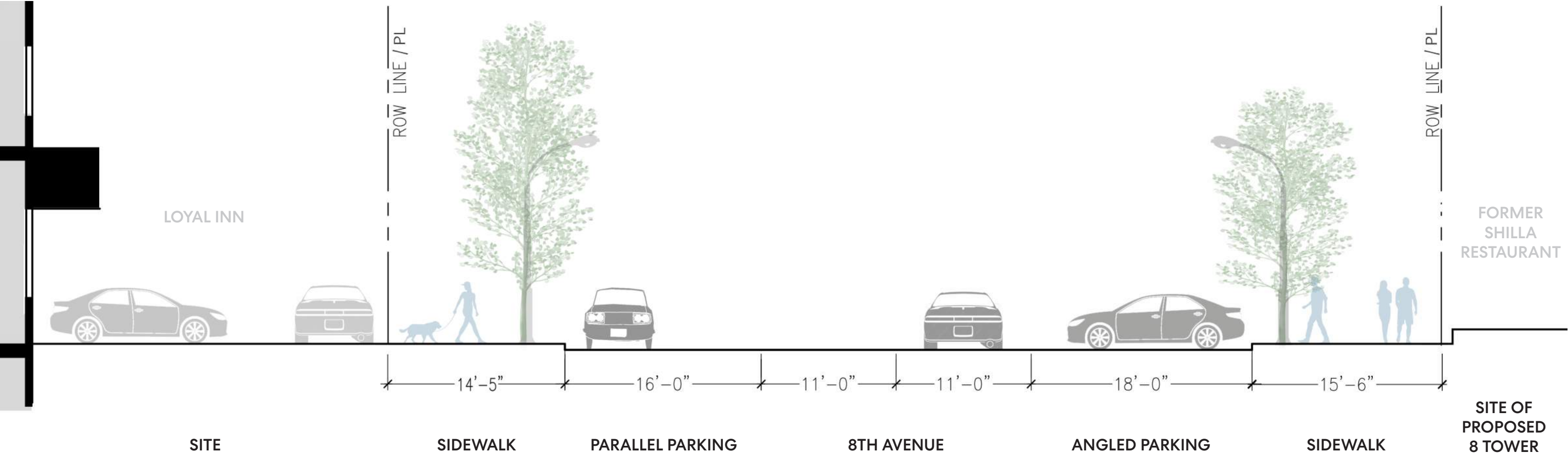


2 BLOCK 21 RETAIL FRONTING BELL



AMAZON BLOCK 21 AT BELL AND 8TH

1.0 | PROJECT INFORMATION
8TH AVENUE - EXISTING CONDITION



1 8TH AVENUE LOOKING SOUTHEAST

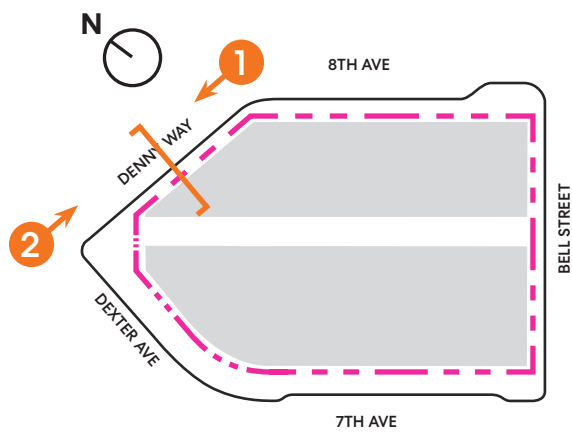
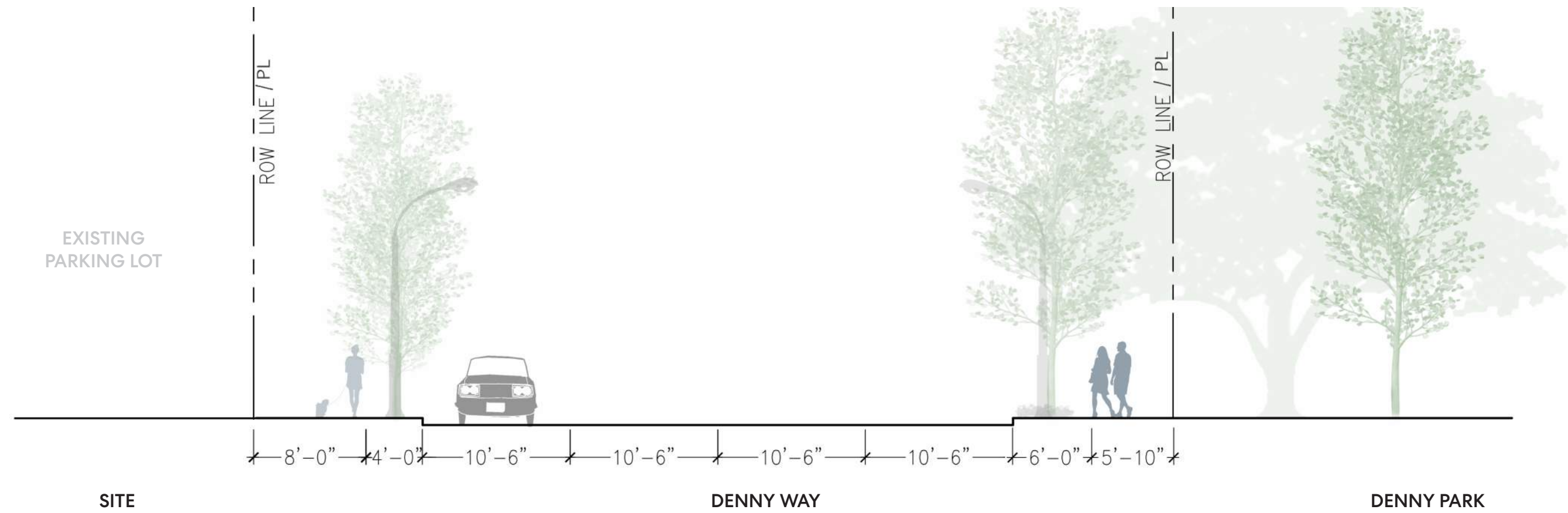


2 LOOKING NORTH ON 8TH AVENUE



PROPOSED 8 TOWER FRONTAGE ON 8TH AVE

1.0 | PROJECT INFORMATION
DENNY WAY - EXISTING CONDITION

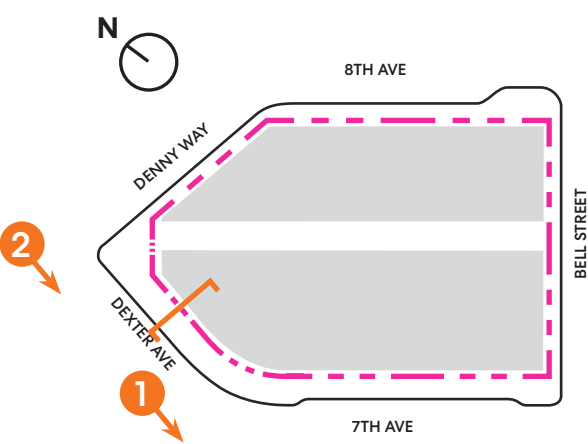
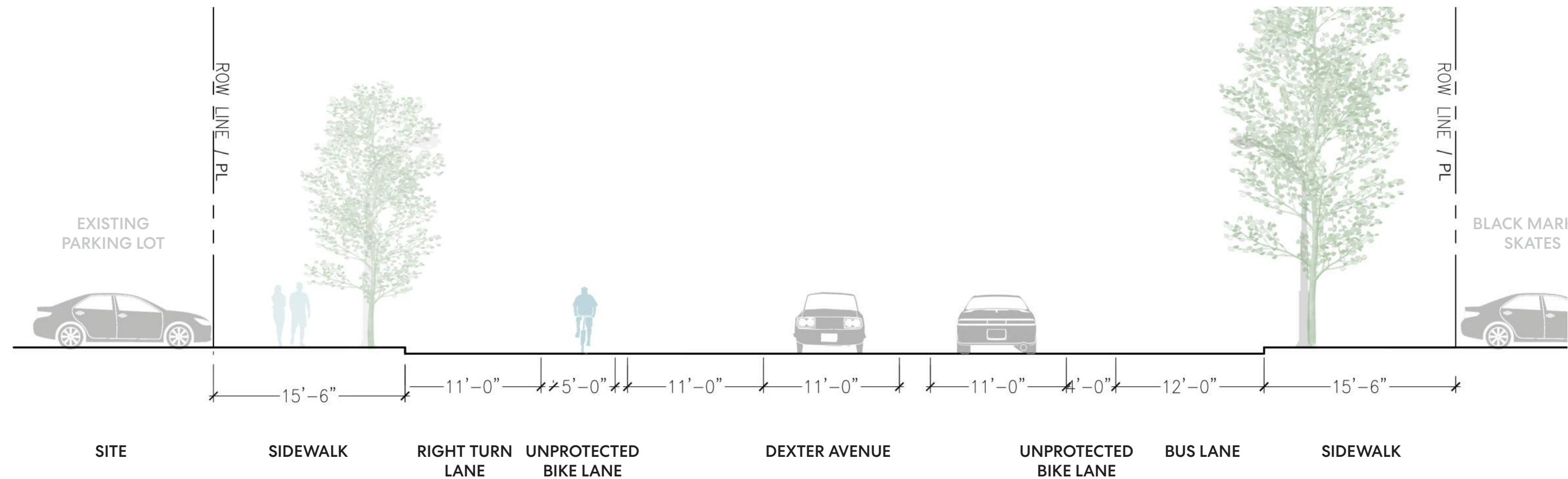


1 DENNY WAY AND 8TH AVENUE



2 DENNY WAY AND 7TH AVENUE

1.0 | PROJECT INFORMATION
DEXTER AVENUE - EXISTING CONDITION

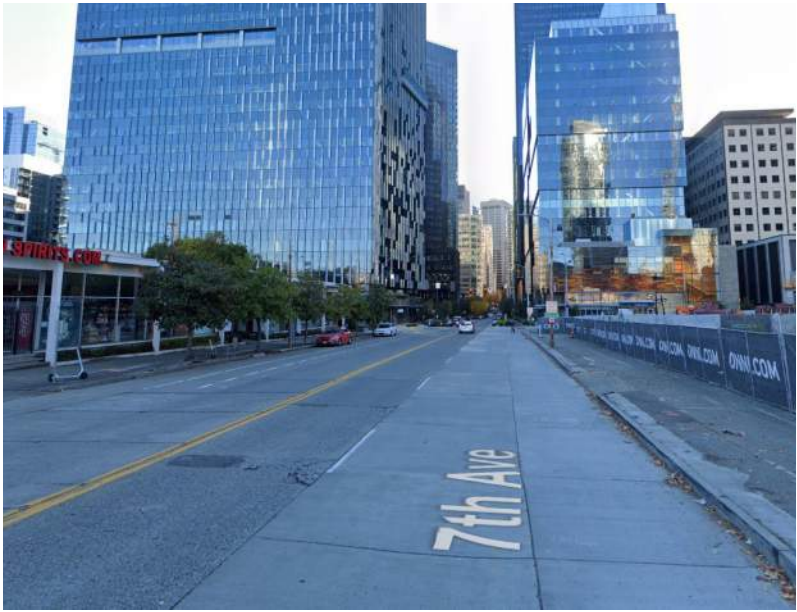
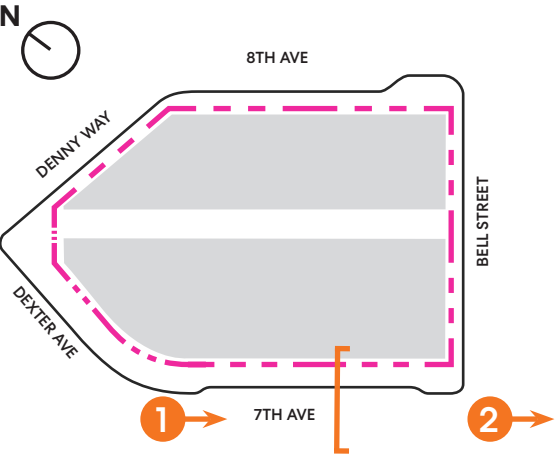
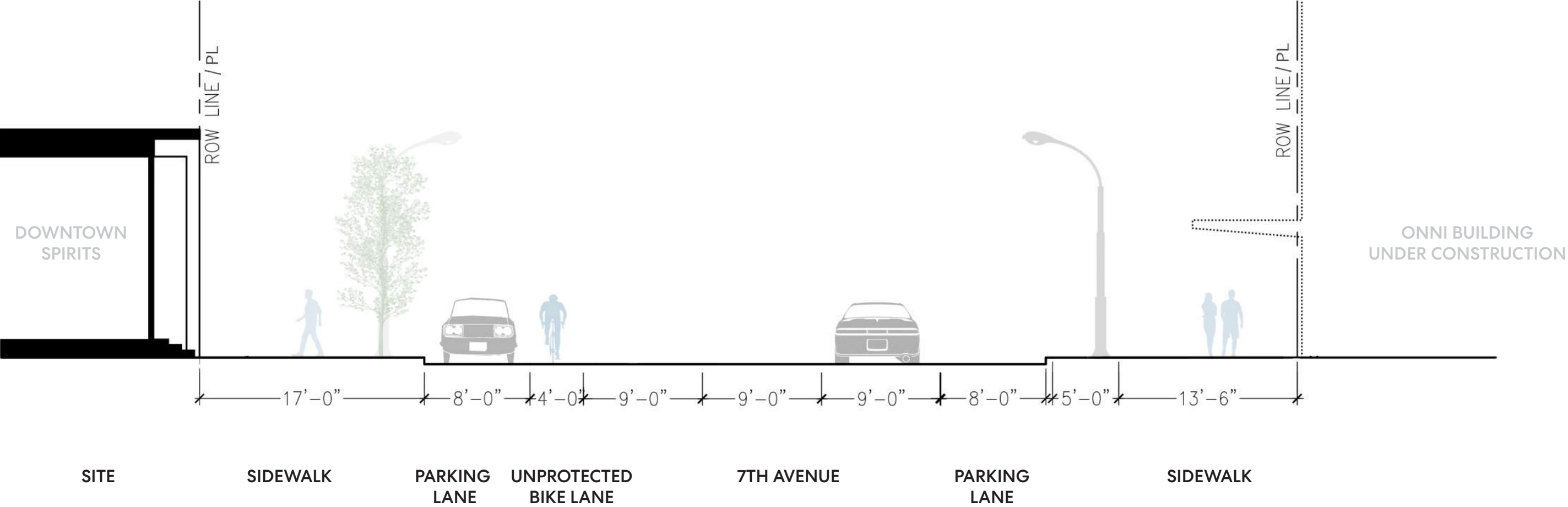


1 DEXTER AVE LOOKING SOUTH



2 DEXTER AVE AND DENNY WAY

1.0 | PROJECT INFORMATION
7TH AVENUE - EXISTING CONDITION



1 7TH AVE LOOKING SOUTH



2 7TH AVE STREETSCAPE SOUTH OF SITE

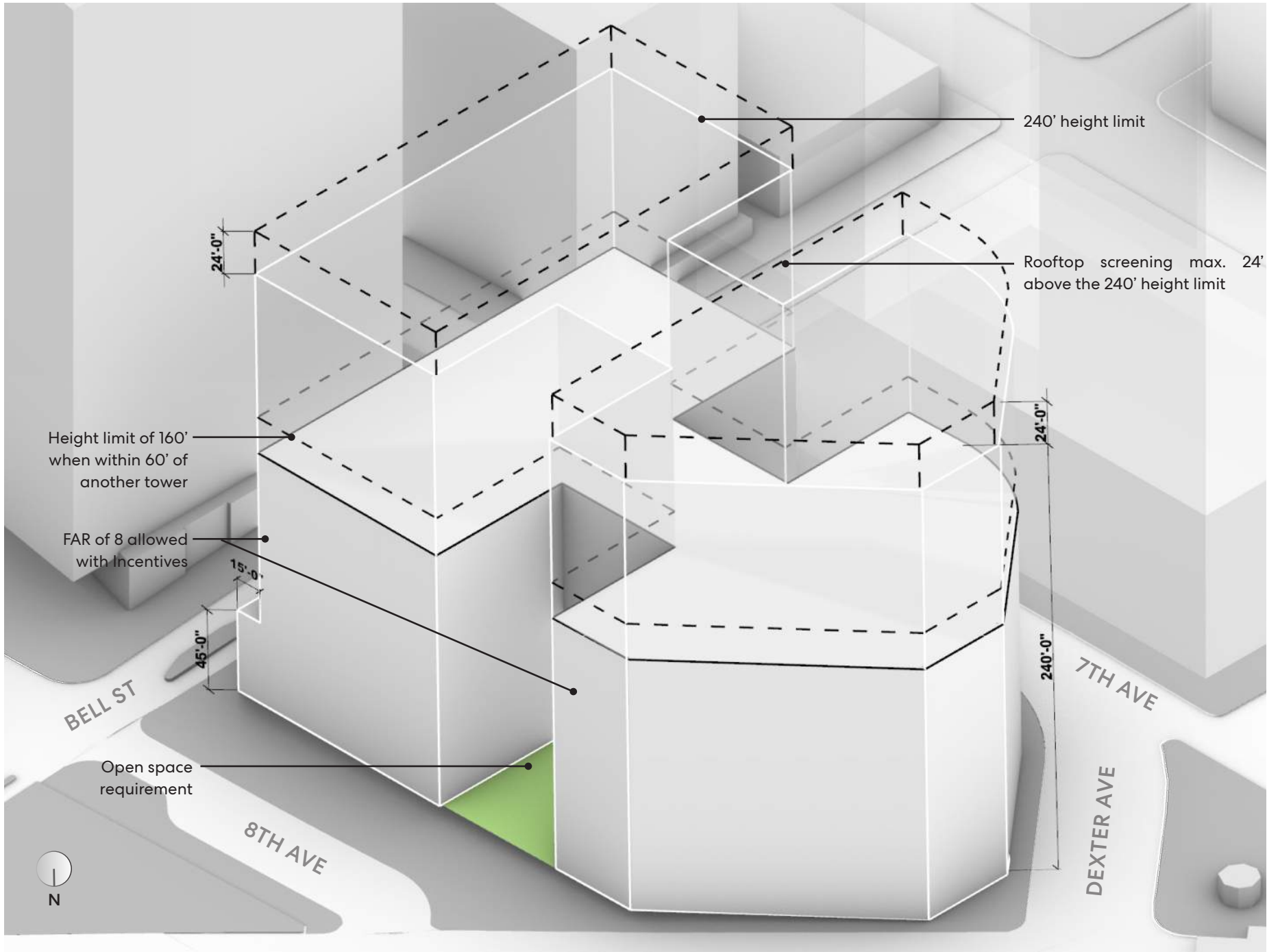


FUTURE ONNI BLOCK V STREETSCAPE

02 ZONING ANALYSIS

2.0 | ZONING ANALYSIS

ZONING DIAGRAM



Departures for this project include:

- Allow for reduction of required modulation depth along Dexter Ave for section of north tower above 160'
- Allow for setback departures
 - Exceed maximum area of setback limitations along Dexter Ave and Bell St.
 - Exceed 10' setback at intersection of Dexter Ave and Denny Way and Bell St.
- Allow for blank facade along Bell St. in excess of 15'
- Allow for rooftop mechanical equipment in excess of 50% of roof area
- Allow for (2) curb cuts along 8th Ave for access to the parking garage and the loading dock

SMC 23.49.008 Structure Height

In the DMC 240/290-440, zone structures that do not exceed 240 feet in height are permitted an FAR of 8 for non-residential uses.

Mechanical equipment can extend up to 15' above the applicable height limit as long as all rooftop features do not exceed 35% of the roof area

Rooftop screening above the 240' height limit is allowed up to 10% of the applicable height limit (24'). Approved screening may allow roof top coverage to exceed 35%.

SMC 23.49.011 - Floor area ratio (FAR) in DMC zones

Base FAR of 5; Max FAR of 8

SMC 23.49.016 Open Space Requirements

20sf per 1,000sf of office space when office space exceeds 85,000sf.

Private Open Space is not eligible for bonuses. Private Open Space shall be open to the sky and shall be consistent with the general conditions related to landscaping; seating and furnishings contained in the Downtown Amenity Standards.

On-site Public Open Space shall be eligible for amenity feature bonuses provided the open space is open to the public without charge and meet the Downtown Amenity Standards for Parcel Parks, Green Street Setbacks and Green Street Improvements.

SMC 23.49.056 Street Facade Requirements

Minimum facade height on Denny Way, Dexter Ave, 7th Ave and 8th Ave = 15' Minimum facade height on Bell St. = 25'

Maximum area of setback shall not exceed a factor of 10 multiplied by the width of the street frontage.

If setback is greater than 15' then shall not exceed 80' of length or 30% of the lot frontage whichever is less.

Maximum setback at intersections is 10'.

50% minimum facade transparency required at Bell St. 30% minimum at all other facades.

Blank facades on Bell St. shall be no more than 15' wide except segments with garage doors may exceed a width of 15' and may be as wide as the driveway plus 5'. Blank facade segments but be separate by a minimum of 2' of transparent facade.

Blank facades on all other streets shall be no more than 30' wide except segments with garage doors may exceed a width of 30' and may be as wide as the driveway plus 5'. Blank facade segments but be separate by a minimum of 2' of transparent facade.

SMC 23.49.058 - DMC Upper Level Development Standards

Facade modulation is required at a height 85' above the sidewalk for any portion of a structure within 15' of a street lot line. No modulation required for portions of a facade set back 15' from a lot line.

Maximum length of unmodulated facade within 15' of a property line between 85 and 160' above the sidewalk is 155'. Between 160' and 240' the maximum length of unmodulated facade is 125'.

Any portion exceeding the maximum length shall be setback a minimum of 15' for a minimum distance of 60' before any other portion may be within 15' of the property line.

If any tower exceeds 160' all portions of the tower above 125' must be separated from any other tower that is above 160' for a minimum of 60'.

On designated green streets a continuous upper-level setback of 15' measured from the property line is required for portions of the structure above a height of 45'.

2.0 | ZONING ANALYSIS
EDG DESIGN GUIDELINES

Design Review Guidelines

A-2 ENHANCE THE
SKYLINE

Design the upper portion of the building to promote visual interest and variety in the downtown skyline.

Responding to the larger context

A sculpted top can lend distinctive identity to the building while helping to orient people as they approach and go places downtown.

Design Review Guidelines

B-1 RESPOND TO
NEIGHBORHOOD
CONTEXT

Develop an architectural concept and compose the major building to reinforce desirable urban features existing in the surrounding neighborhood.

Relating to the neighborhood

Each building site lies within an urban neighborhood context having distinct features and characteristics to which the building design should respond.

Design Review Guidelines

B-4 DESIGN A WELL-
PROPORTIONED AND
UNIFIED BUILDING

Compose the massing and organize the publicly accessible interior and exterior spaces to create a well-proportioned building that exhibits a coherent architectural concept. Design the architectural elements and finish details to create a unified building, so that all components appear integral to the whole.

Exhibit a coherent architectural
concept

Buildings that exhibit form and features identifying the functions within the building help to orient people to their surroundings, enhancing their comfort and sense of security while downtown.

Design Review Guidelines

D-1 PROVIDE INVITING
AND USEABLE OPEN
SPACE

Design public open spaces to promote a visually pleasing, safe, and active environment for workers, residents, and visitors. Views and solar access from the principal area of the open space should be especially emphasized.

Public Amenities

New buildings downtown are encouraged to incorporate public spaces to enhance the pedestrian environment, reinforce the downtown open space network, and offset the additional demand for public open space from downtown employment.

Design Review Guidelines

D-2 ENHANCE THE
BUILDING WITH
LANDSCAPING

Enhance the building and site with substantial landscaping—which includes special pavements, trellises, screen walls, planters and site furniture, as well as living material.

Enhancing the streetscape and
open space

Landscape enhancements of the site may include some of the features listed below:

- b. include a special feature such as a courtyard, fountain or pool;
 - c. incorporate a planter guard or low planter wall as part of the architecture;
 - d. distinctively landscape the open areas created by building modulation;
 - e. soften the building by screening blank walls, terracing retaining walls, etc.
- Reinforce the desirable pattern of landscaping found on adjacent block faces

Design Review Guidelines

D-3 PROVIDE ELEMENTS
THAT DEFINE THE SPACE

Provide special elements on the facades, within public open spaces, or on the sidewalk to create a distinct, attractive, and memorable “sense of place” associated with the building.

Sense of place

Distinctive landscaping, street furniture, and special attractions can help establish a special identity for the building and providing orientation and comfort to those using it.

Incorporate one or more of the following as appropriate:

- a. public art;
- b. street furniture
- c. distinctive landscaping such as specimen trees and water features;
- f. public seating areas in the form of ledges, broad stairs, planters and the like, especially near public open spaces, bus stops, vending areas, on sunny facades, and other places where people are likely to want to pause and wait.

03 PROJECT INSPIRATION

OPEN SPACE CONNECTIVITY



PROVIDE A WELCOMING OASIS THAT SUPPORTS REJUVENATION AND CONNECTIONS. USE THIS PRIME LOCATION ADJACENT TO DENNY PARK TO CONNECTS PEOPLE, NEIGHBORHOODS AND CREATE LINKS WITH SURROUNDING GREEN SPACES.

ACTIVATE AND ENERGIZE



CREATE AN ENDURING PLACE THAT SUPPORTS PEDESTRIAN LIFE AND ACTIVITY. SOFTEN THE EDGES ALONG BUSY ARTERIALS TO PROVIDE SPACES FOR PEOPLE AND STREET LIFE. ACKNOWLEDGE THE PIVOTAL LOCATION WITH A THOUGHTFUL DESIGN.

OUTSTANDING LABS AND TENANT AMENITIES



PROVIDE A UNIQUE BIOPHILIC DESIGN APPROACH TO THE SITE, INTERIOR AMENITIES AND LAB SPACES THAT HAVE A STRONG RELATIONSHIP WITH DENNY PARK AND THE SURROUNDING NEIGHBORHOOD GREEN SPACES.

3.0 | PROJECT INSPIRATION
NEIGHBORHOOD INSPIRATION



URBAN ARBORETUM



STREET LEVEL INTERACTION



DENNY PARK



EXTERIOR ELEMENTS & FINISHES

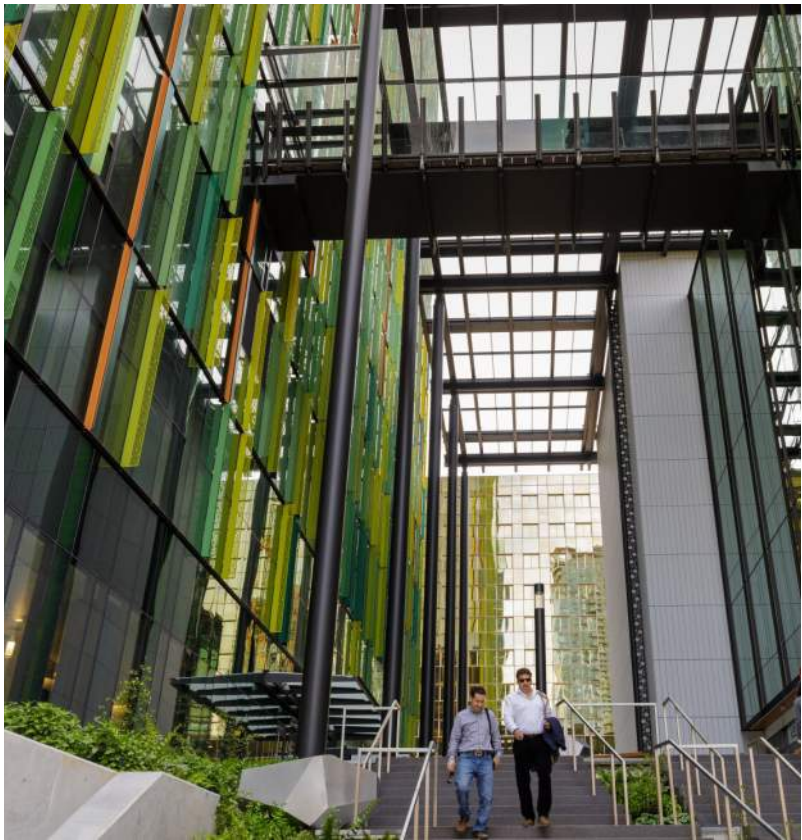
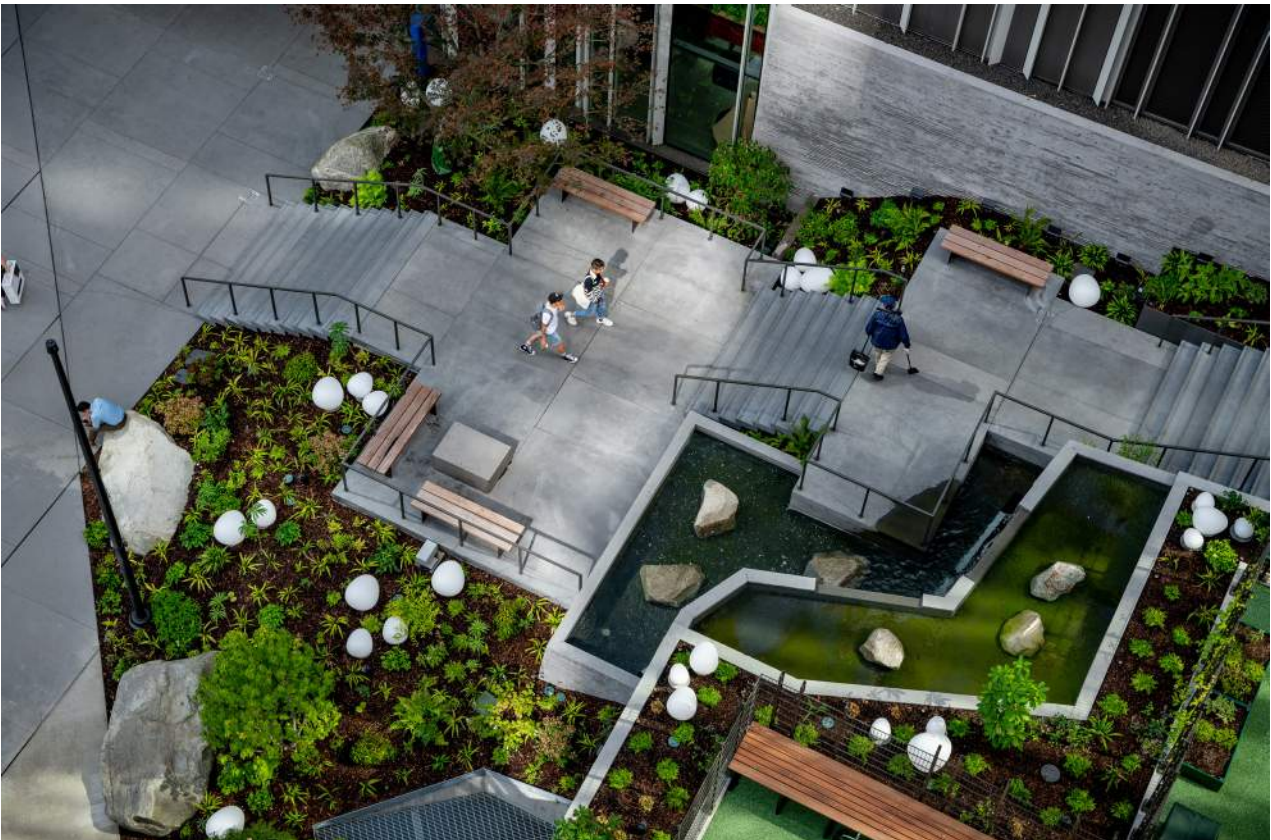


PUBLIC ART AND PLAZAS



URBAN PATTERN & FORM

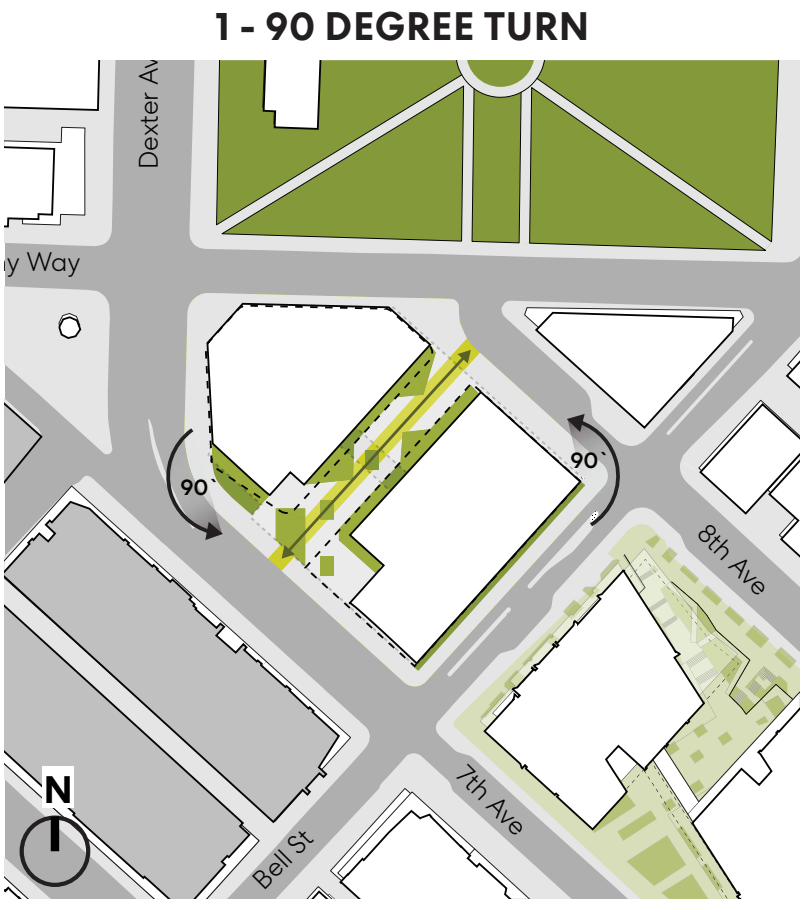
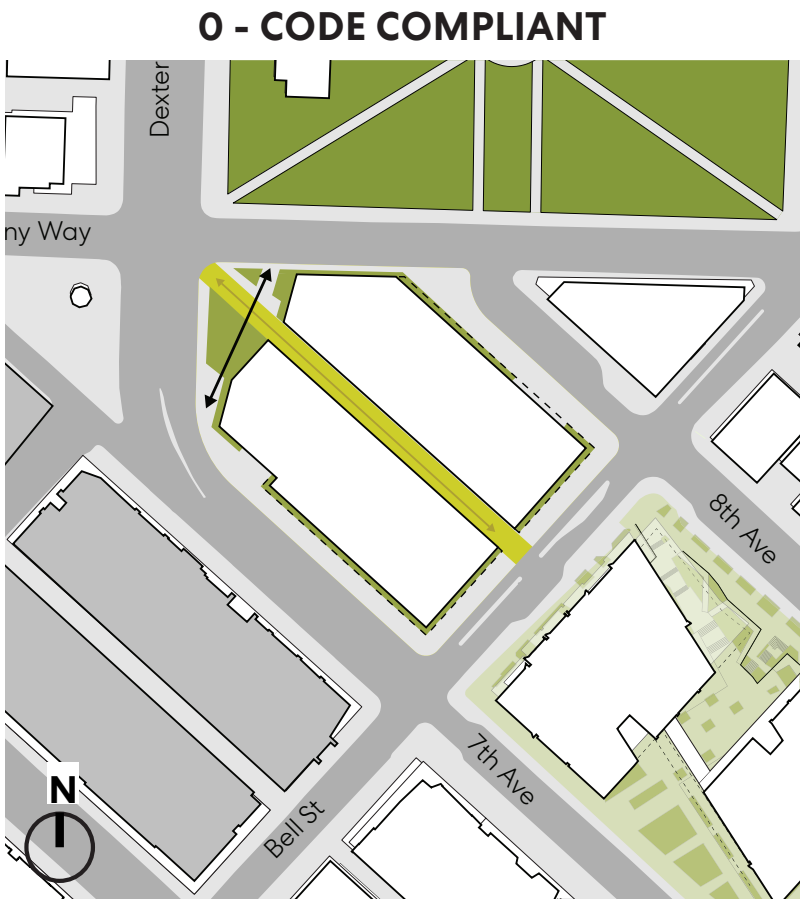
3.0 | PROJECT INSPIRATION
BIOPHILIA | DOWNTOWN GATEWAY



04 MASSING CONCEPTS

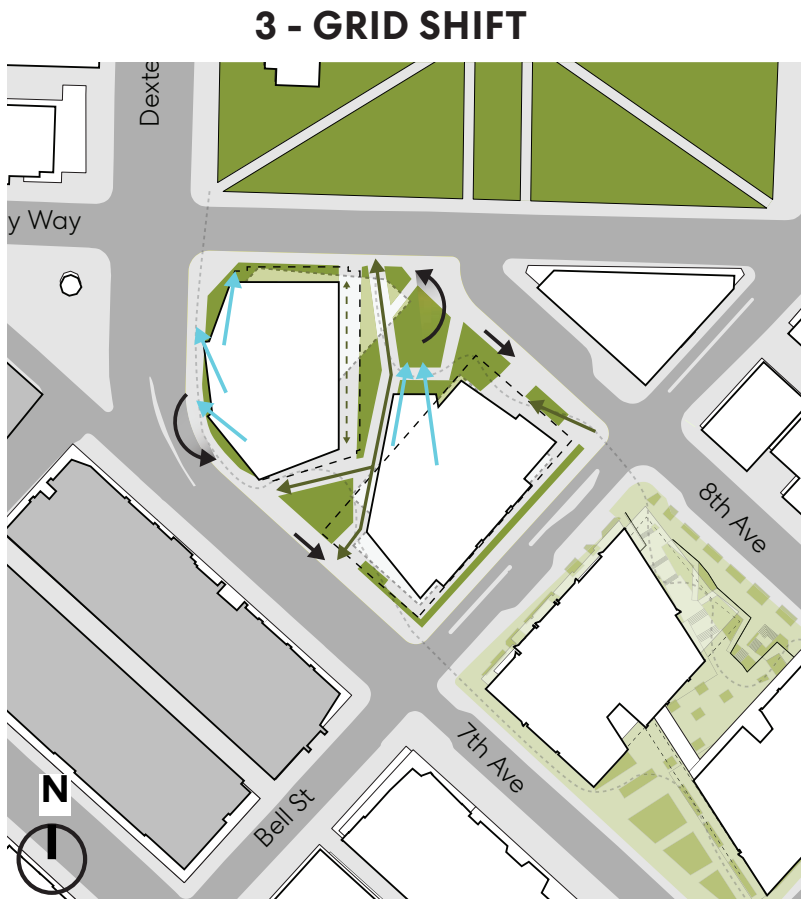
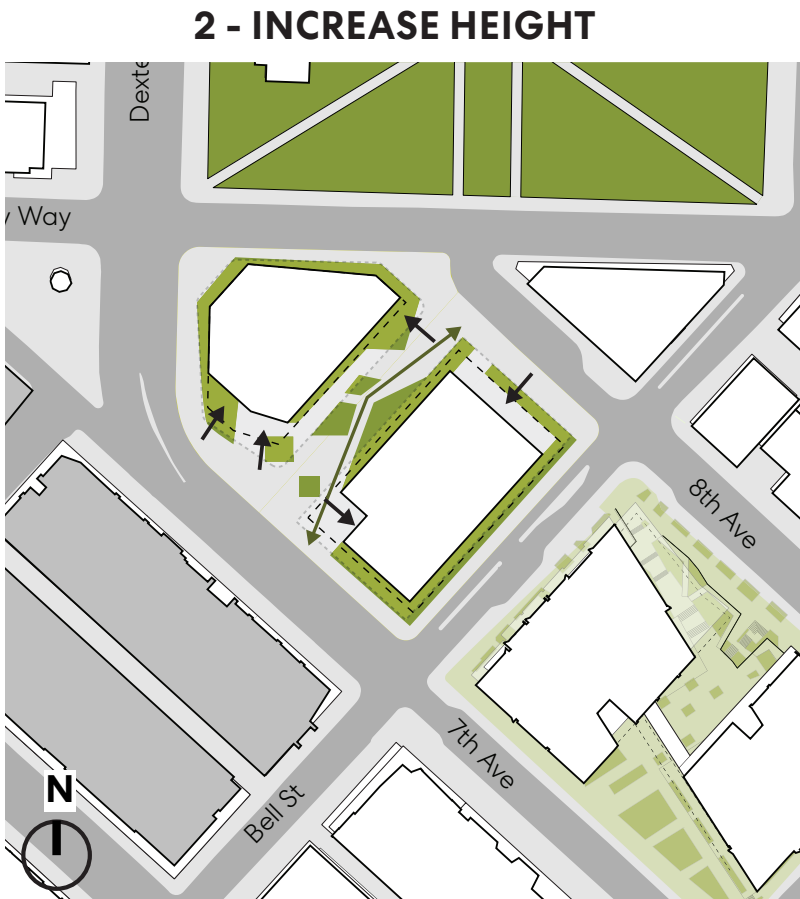
4.0 | MASSING CONCEPTS
URBAN GRID DIAGRAMS

- Existing alley ‘orphaned’
- Maintain loading off alley
- Building footprints parallel with property to the west



- Eliminate alley
- Rotate buildings parallel with developments to the southeast
- Provide pedestrian through-block connection
- Loading and garage access off 8th Ave

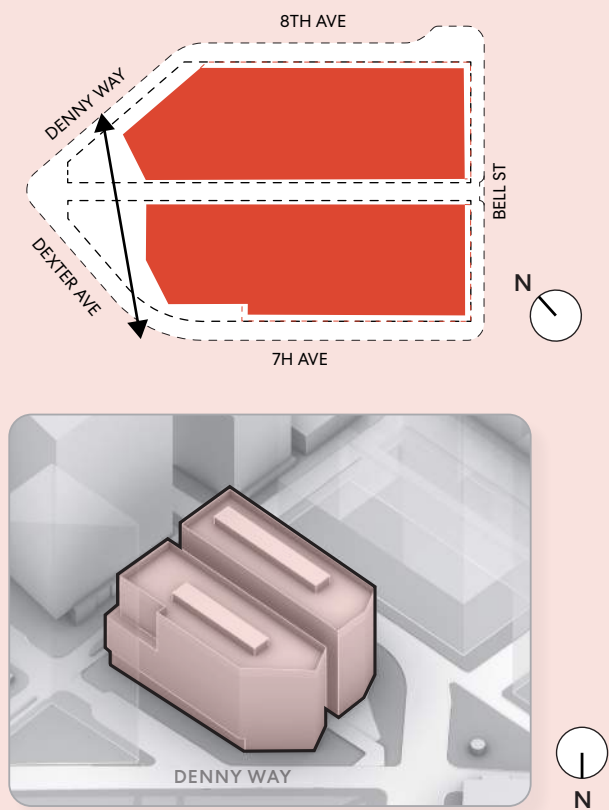
- Increase height of south tower to increase open space at grade
- Provide pedestrian through-block connection
- Loading and garage access off 8th Ave



- Make north building the taller tower to create iconic building at grid shift and establish gateway to downtown
- Rotate north building to open up towards Denny Park
- Link site plaza open spaces to language of open spaces to the south. Continue language of ‘urban arboretum’.
- Provide pedestrian through-block connection
- Loading and garage access off 8th Ave

4.0 | MASSING CONCEPTS
BASE CASE | CODE COMPLIANT

BASE (NO ALLEY VACATION)



Pros:

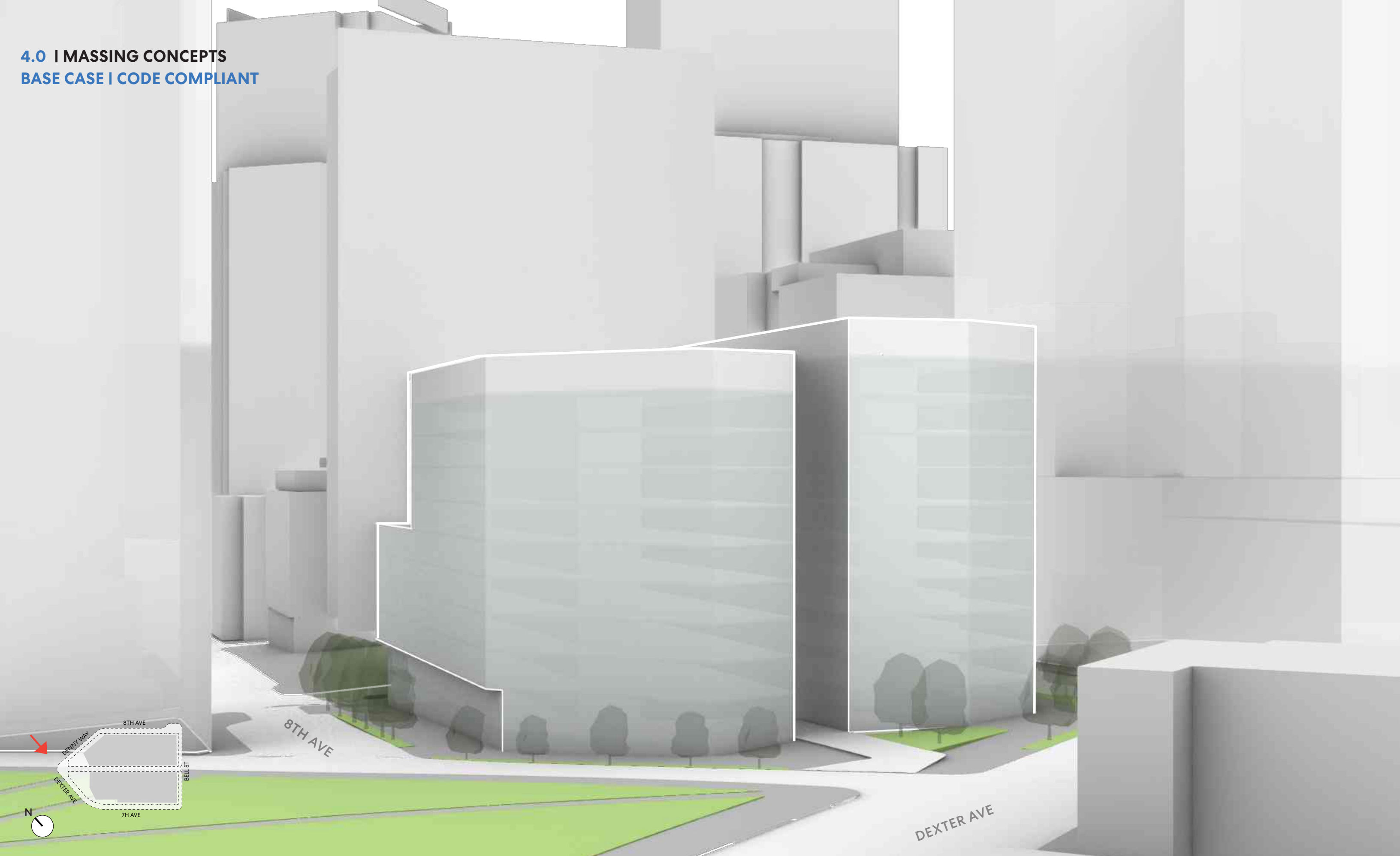
- No departures required

Cons:

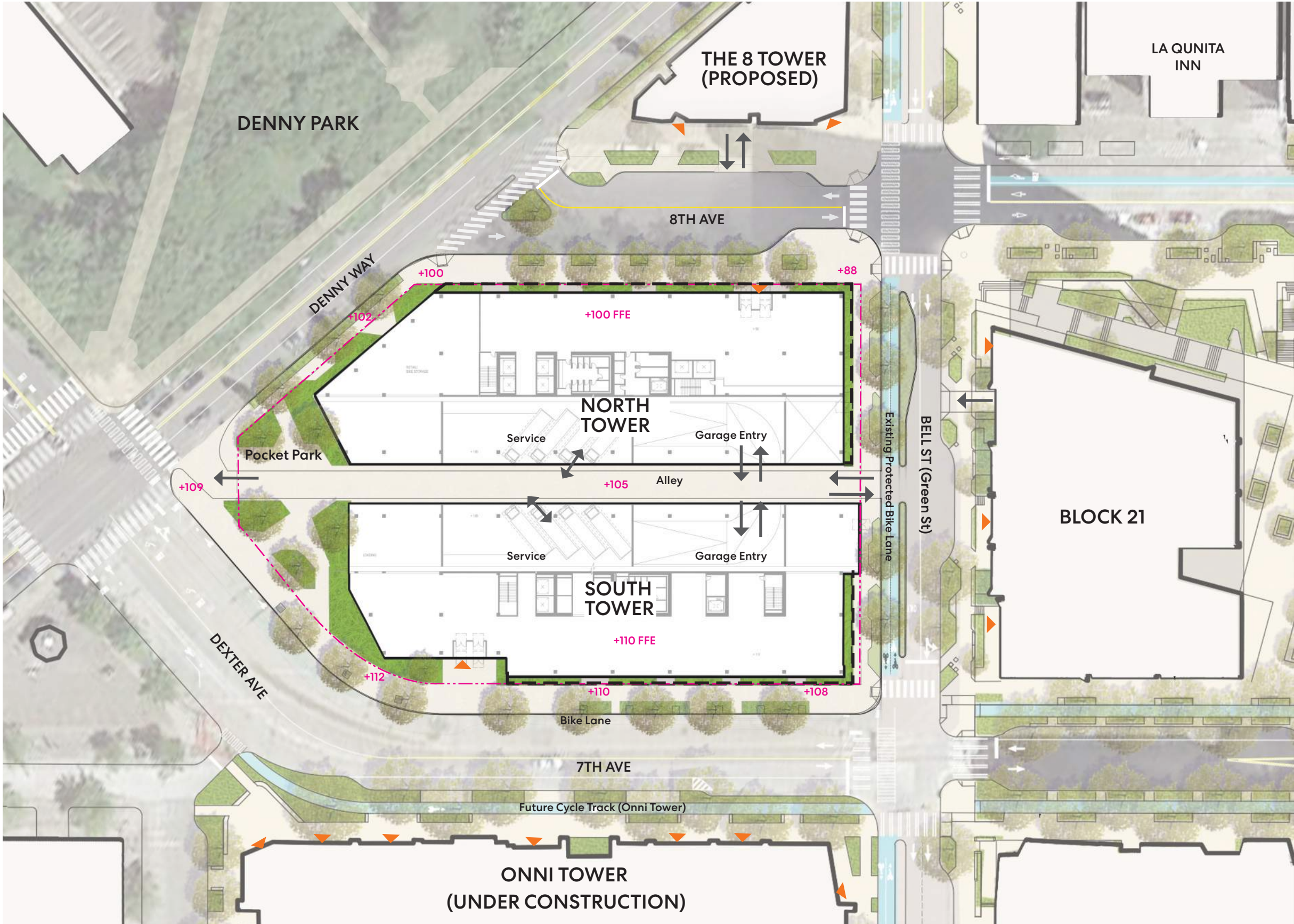
- Existing alley is accessed off of a green street
- Existing alley exits at the corner of Dexter Ave and Denny Way
- Massing does not reflect development patterns to southeast
- Massing does not address opportunity of downtown core 'Gateway' location
- Project open space located in unuseable corners of sites along Denny Way



4.0 | MASSING CONCEPTS
BASE CASE | CODE COMPLIANT



4.0 | MASSING CONCEPTS
BASE CASE | CODE COMPLIANT



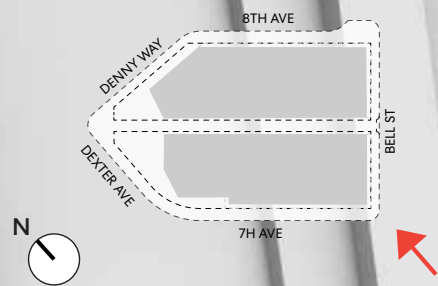
LEGEND

- Property Line
- Building Entry
- Vehicular Entry

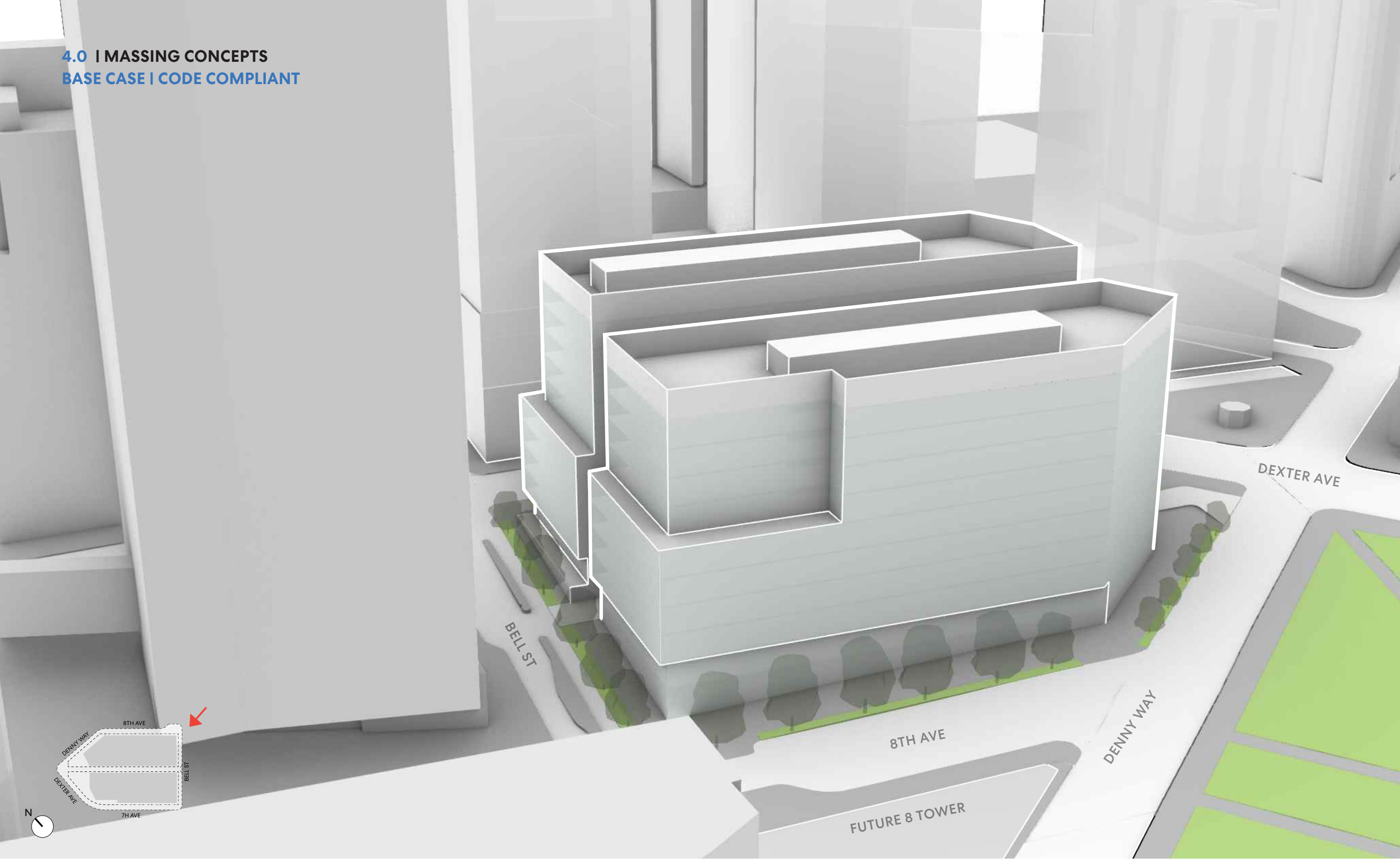
GROUND PLAN

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4.0 | MASSING CONCEPTS
BASE CASE | CODE COMPLIANT

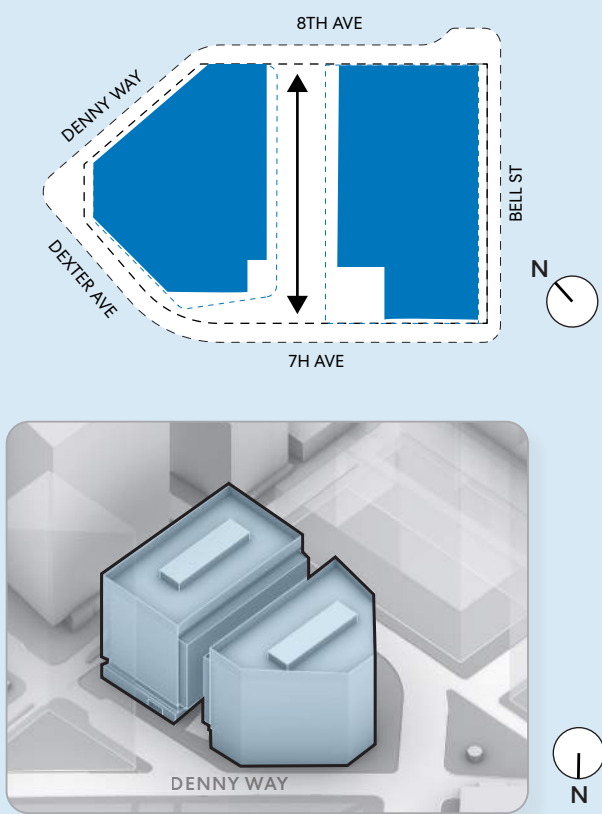


4.0 | MASSING CONCEPTS
BASE CASE | CODE COMPLIANT



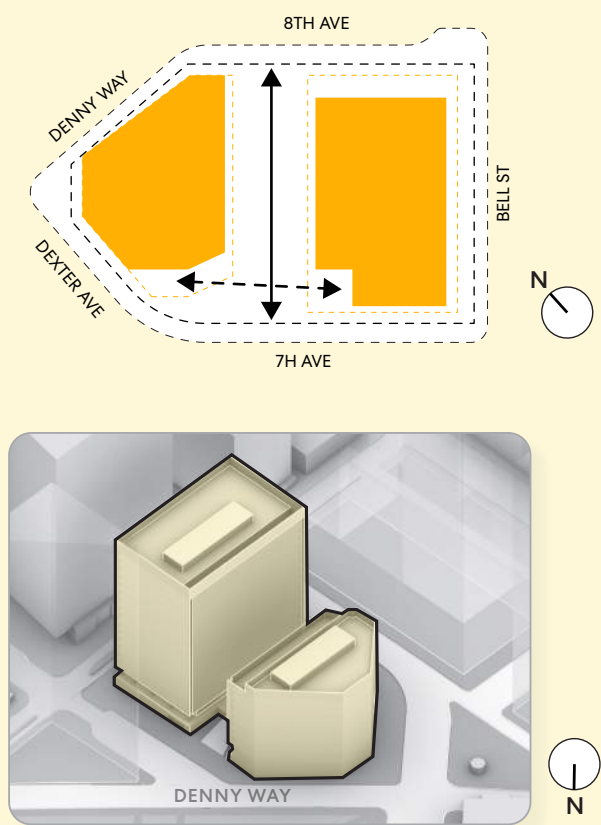
4.0 | MASSING CONCEPTS
PLAZA ORIENTATION STUDIES

SCHEME 01



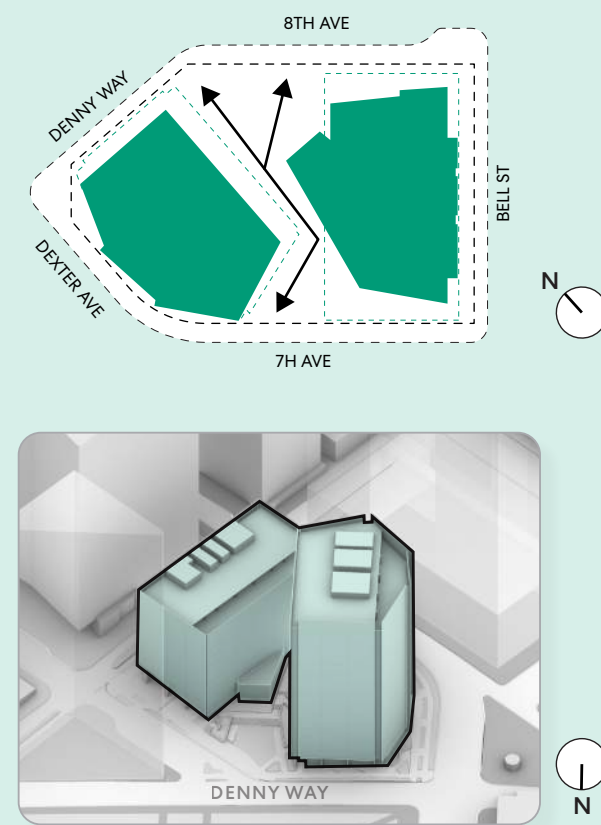
- Pros:**
- Allows for better utilization of site at corner of Denny and Dexter
 - Eliminates loading and parking access from a green street
 - Aligns with development pattern to south
 - No departures required
- Cons:**
- Through block connection is constricted
 - Minimal open space provided
 - Does not account for grid shift at Denny Way
 - Does not enhance the skyline
 - Does not respond to neighborhood context
 - Pedestrian experience is not significantly improved
 - Massing does not allow for optimal light and air around buildings
 - Project turns it's 'back' to Denny Park

SCHEME 02



- Pros:**
- Allows for more generous through block connection with greater than required open space
 - More light and air around buildings
 - No departures required
- Cons:**
- Does not account for grid shift at Denny Way
 - Lost opportunity for a significant building presence establishing a gateway to downtown at corner of Dexter and Denny
 - Does not allow for modulation of skyline
 - Project turns it's 'back' to Denny Park

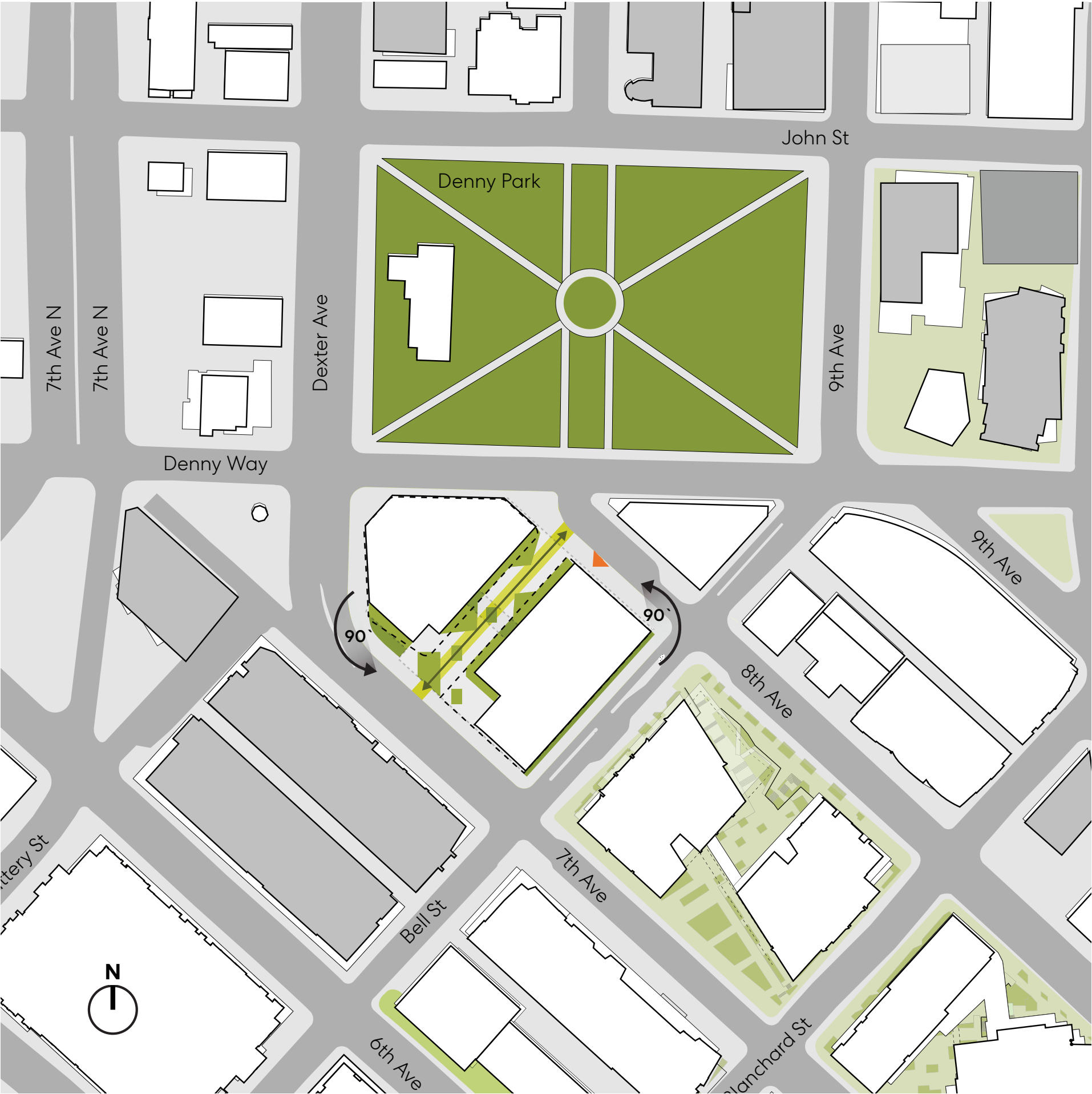
SCHEME 03 (PREFERRED)



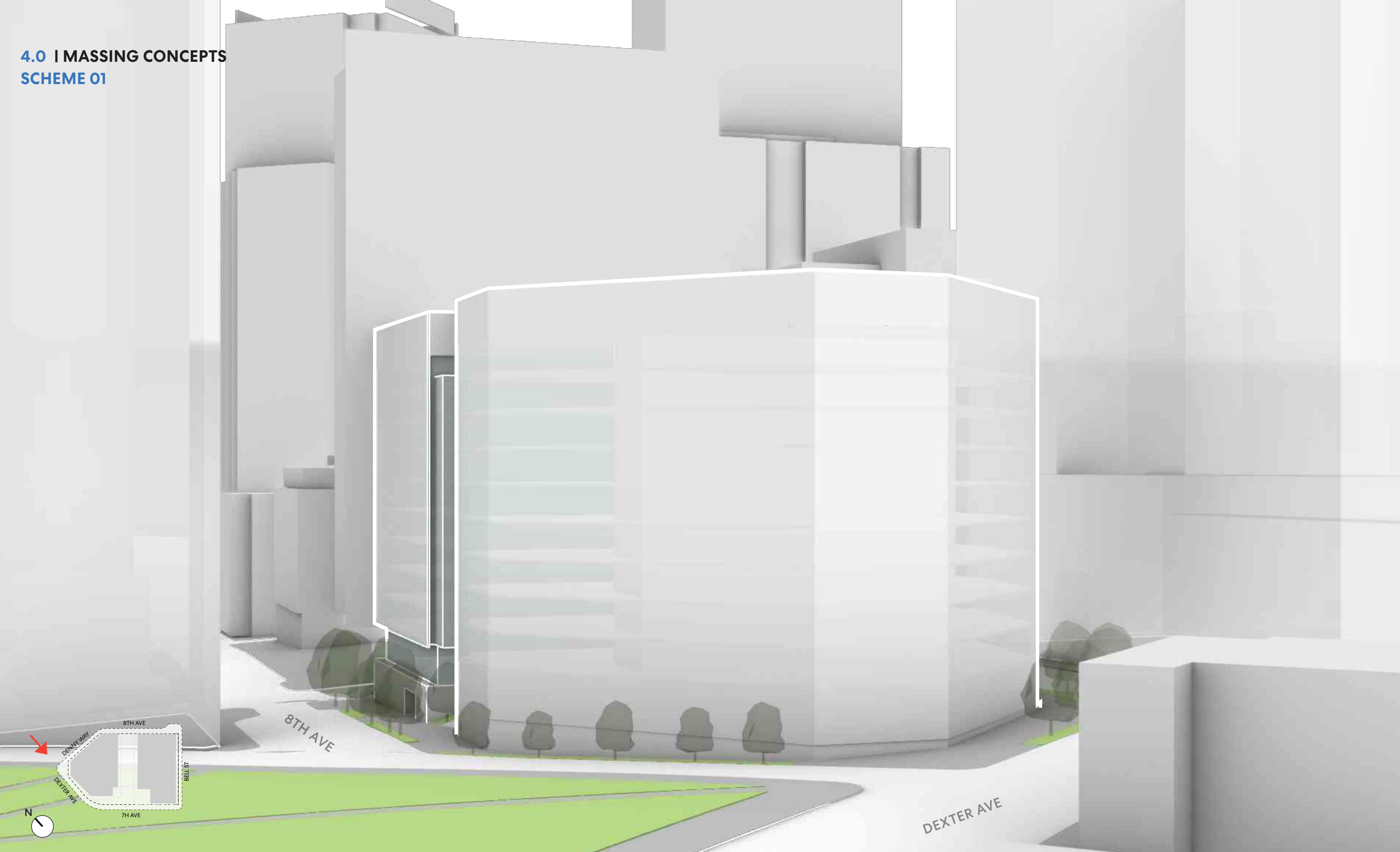
- Pros:**
- Allows for a striking building on a significant corner marking the transition to downtown
 - Opens buildings and site to Denny Park
 - Creates opportunity for two unique plazas
 - Maximizes open space on site
 - Connects to grids at either side of Denny Way
 - Establishes plazas along 7th and 8th which link 'urban arbo-retum' to south with Denny Park to the north
 - Opens up pedestrian sight lines to Denny Park from Dexter
- Cons:**
- Departures required

4.0 | MASSING CONCEPTS
SCHEME 01 | ROTATE

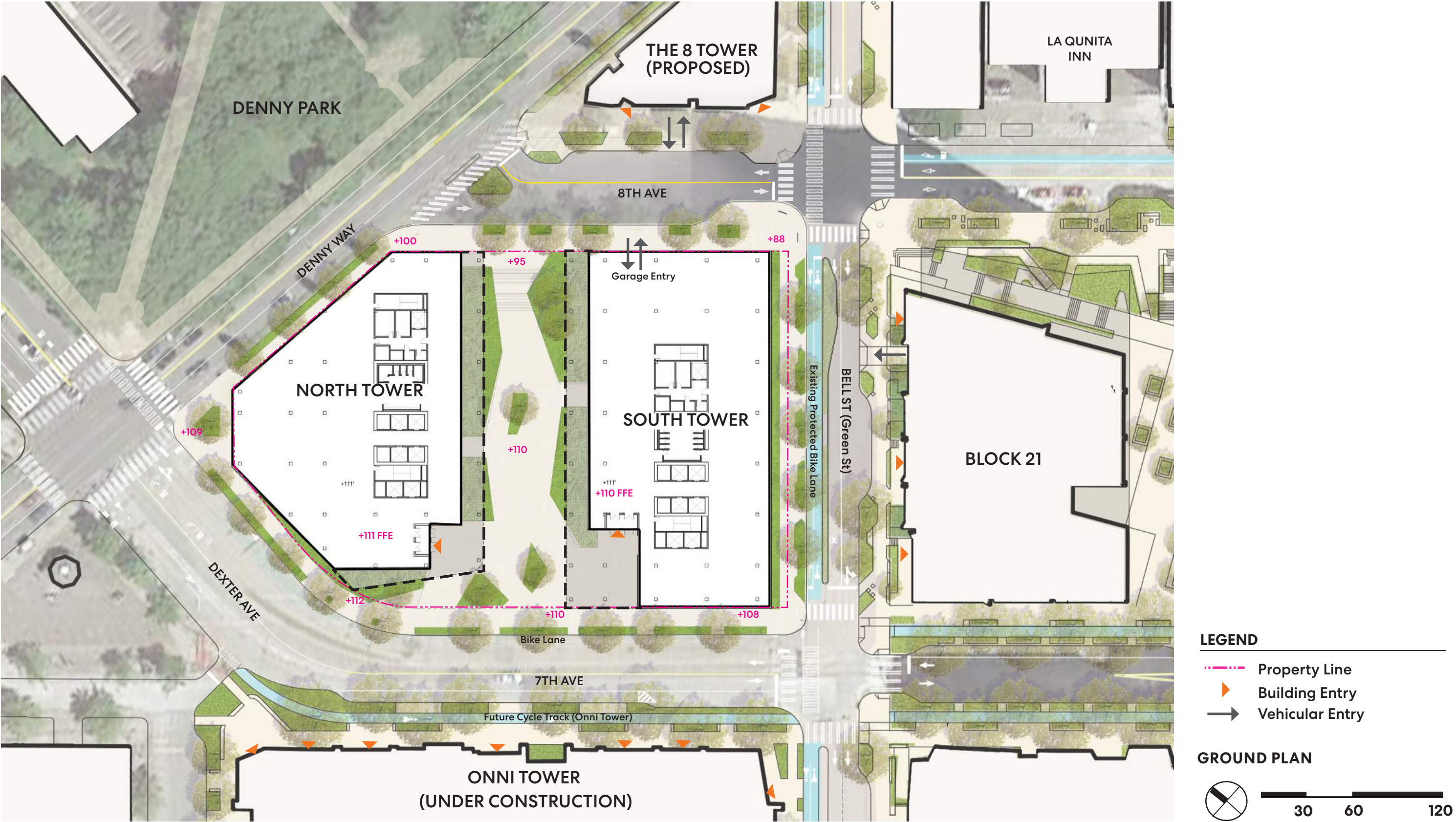
- Eliminate alley
- Rotate buildings parallel with developments to the southeast
- Provide pedestrian through-block connection
- Loading and garage access off 8th Ave
- Ground plane open space 10,350sf



4.0 | MASSING CONCEPTS
SCHEME 01



4.0 | MASSING CONCEPTS
SCHEME 01



4.0 | MASSING CONCEPTS
SCHEME 01

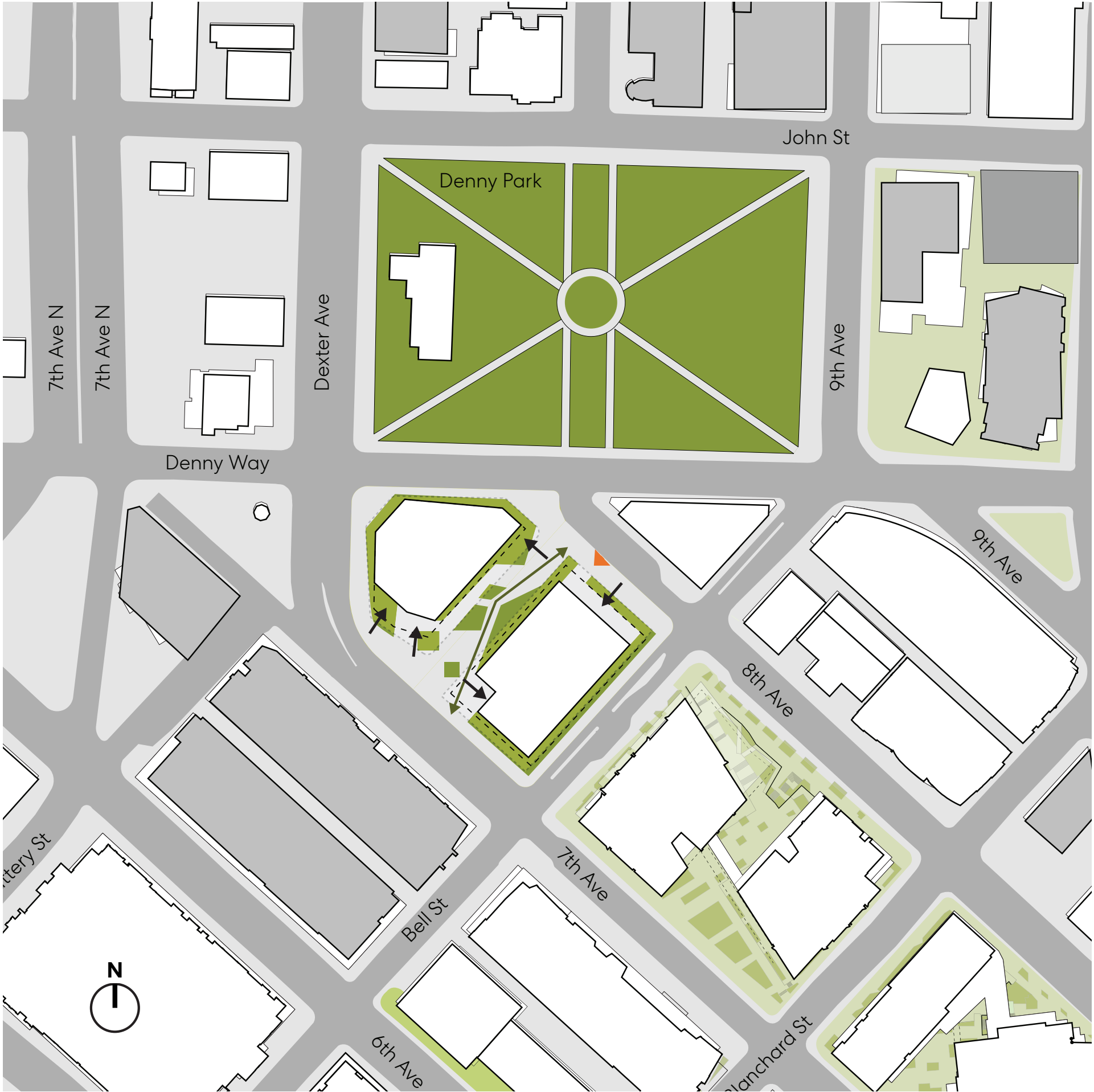


4.0 | MASSING CONCEPTS
SCHEME 01

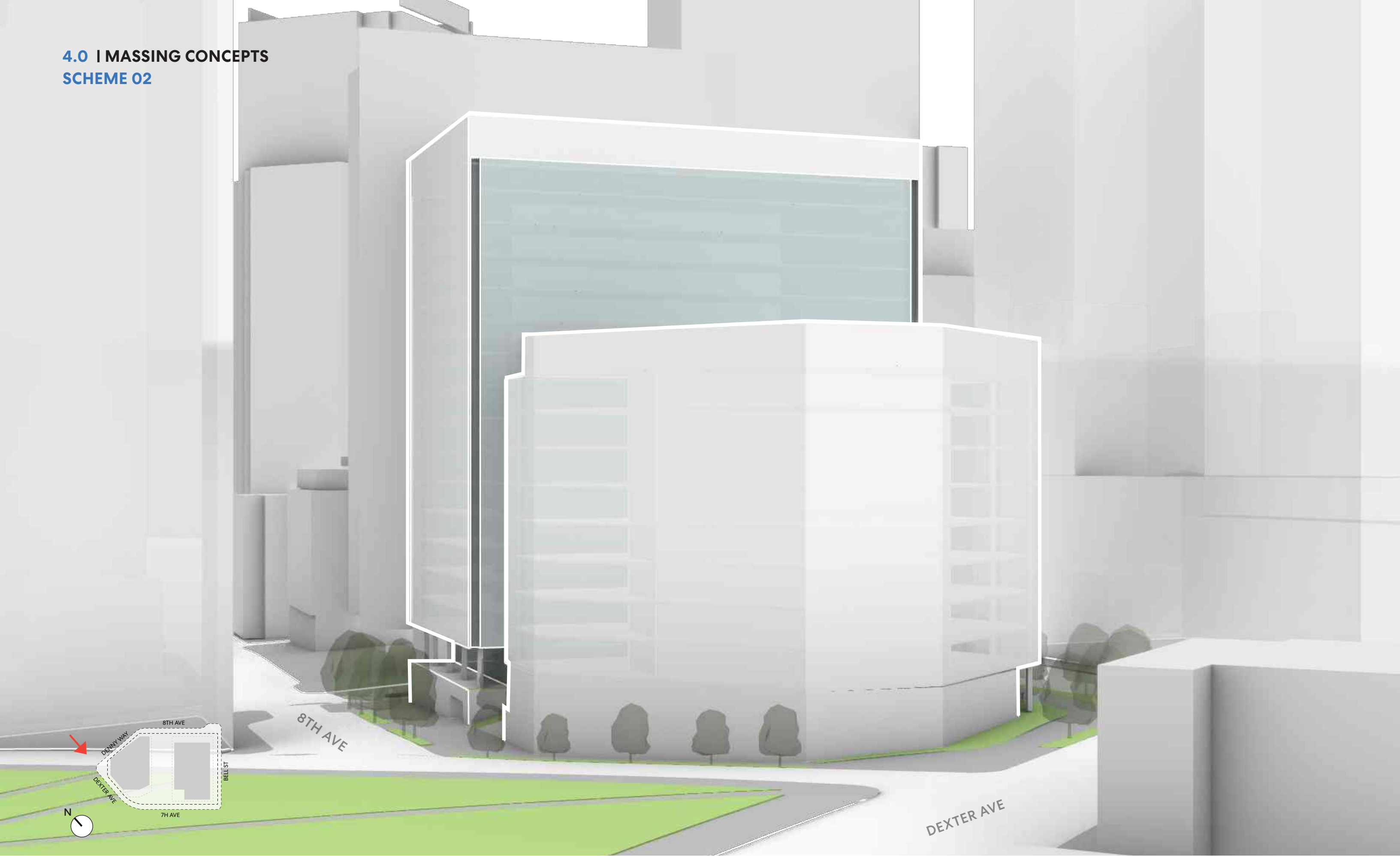


4.0 | MASSING CONCEPTS
SCHEME 02 | INCREASE HEIGHT

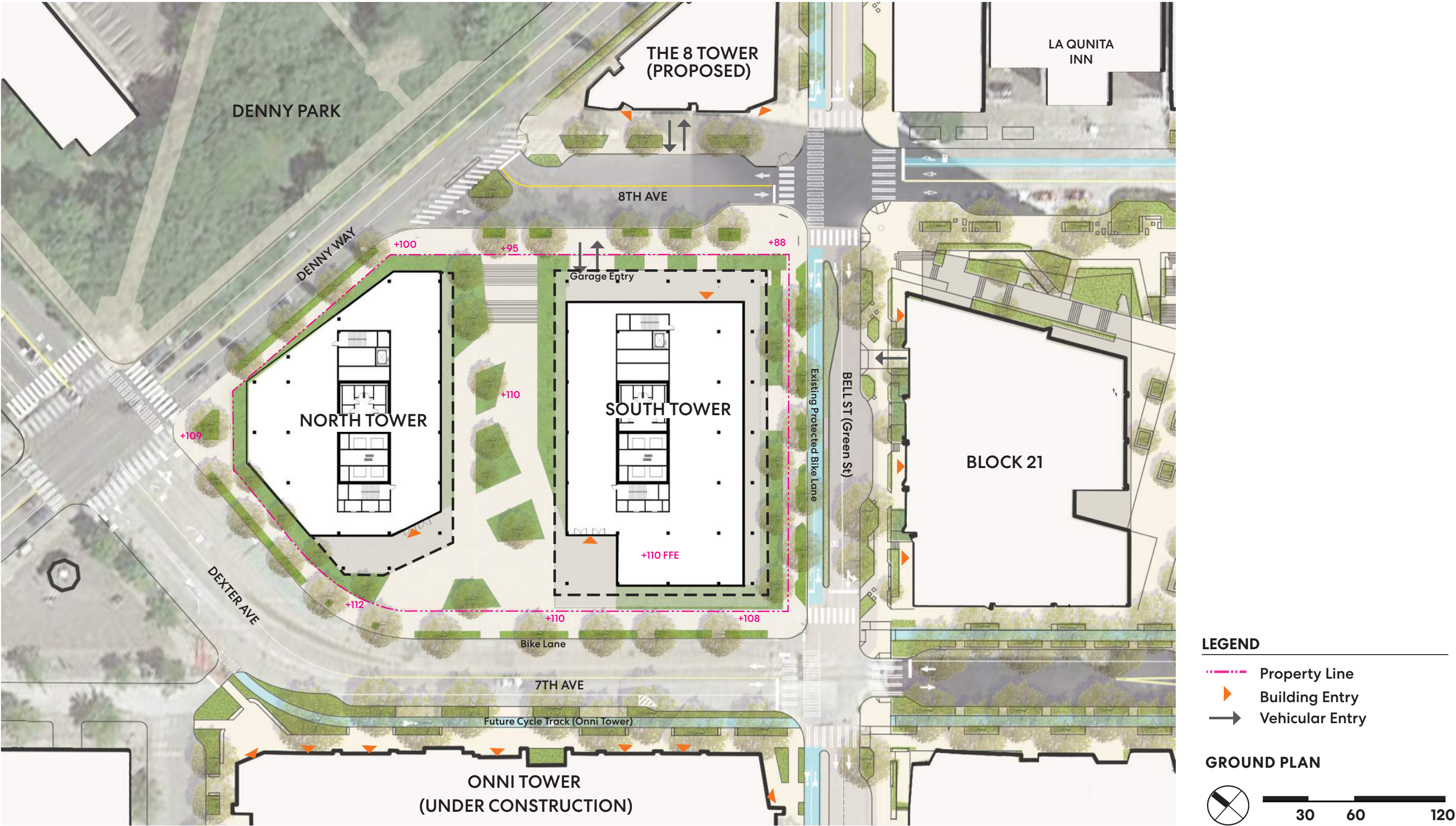
- Increase height of south tower to increase open space at grade
- Provide pedestrian through-block connection
- Loading and garage access off 8th Ave
- Ground plane open space 24,00sf



4.0 | MASSING CONCEPTS
SCHEME 02



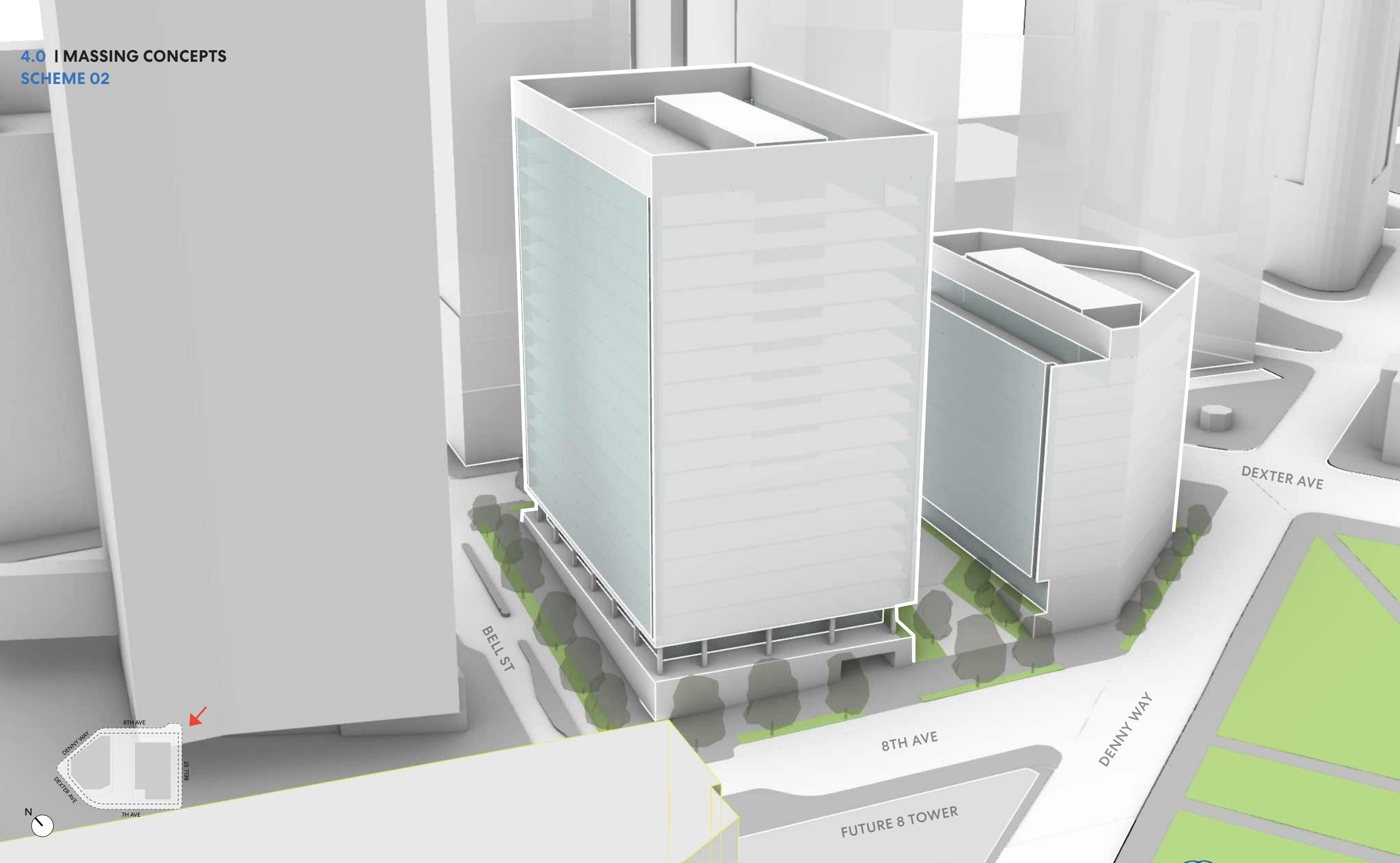
4.0 | MASSING CONCEPTS
SCHEME 02



4.0 | MASSING CONCEPTS
SCHEME 02

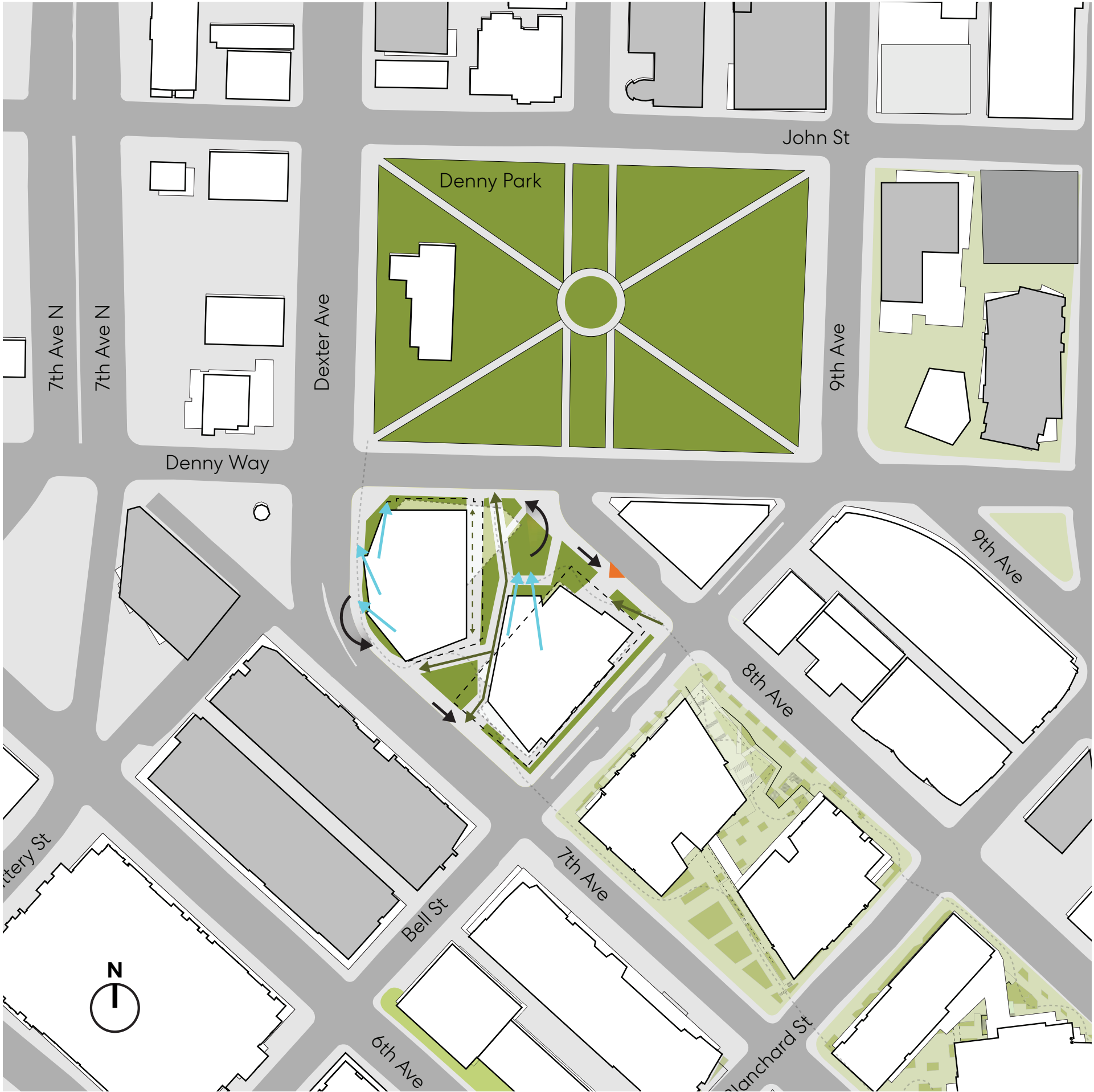


4.0 | MASSING CONCEPTS
SCHEME 02



4.0 | MASSING CONCEPTS
SCHEME 03 | GRID SHIFT

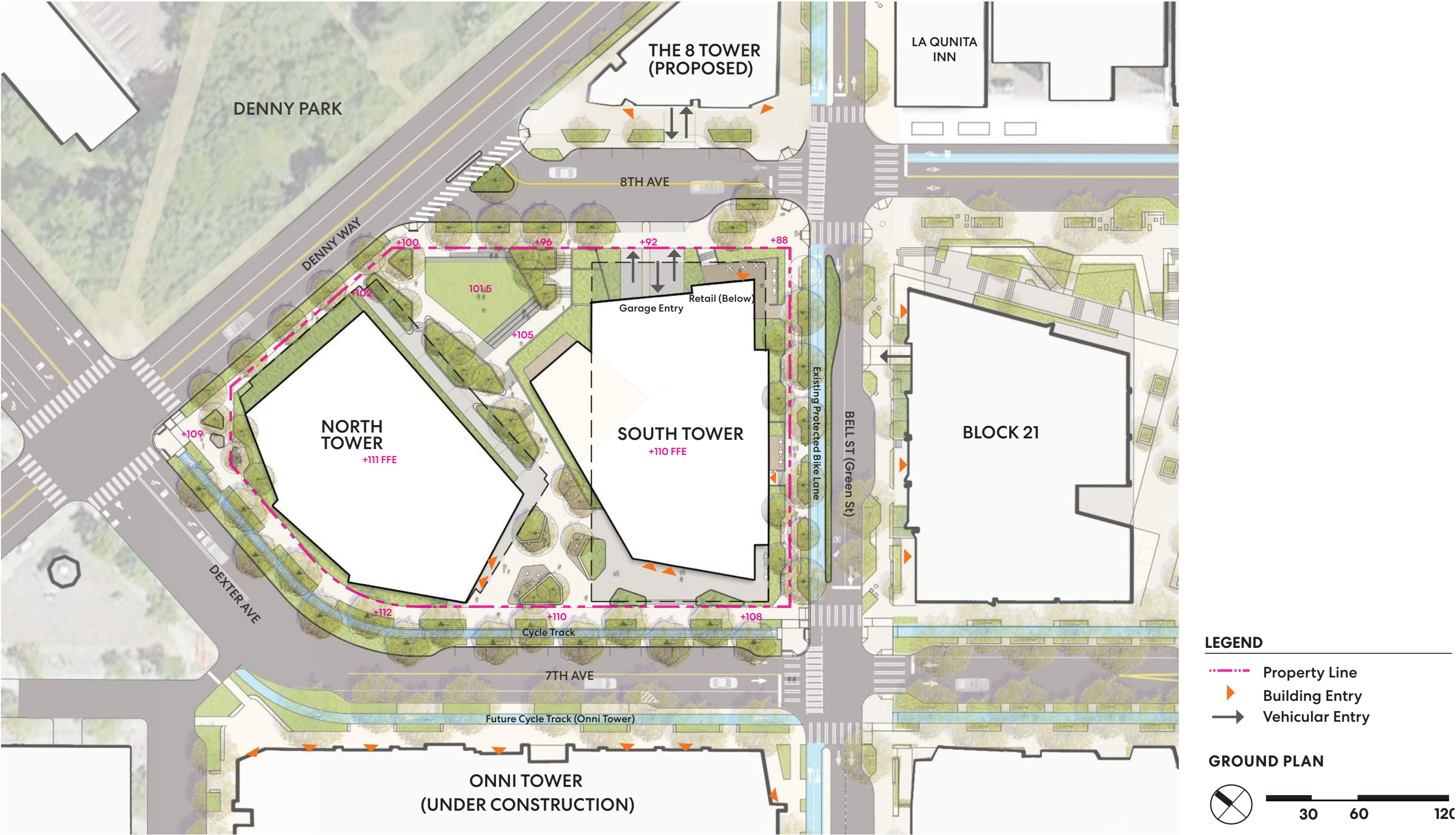
- Make north building the taller tower to create iconic building at grid shift and establish gateway to downtown
- Rotate north building to open up towards Denny Park
- Link site plaza open spaces to language of open spaces to the south. Continue language of 'urban arboretum'.
- Provide pedestrian through-block connection
- Loading and garage access off 8th Ave
- Ground plane open space 24,375SF



4.0 | MASSING CONCEPTS
SCHEME 03



4.0 | MASSING CONCEPTS
SCHEME 03 PREFERRED



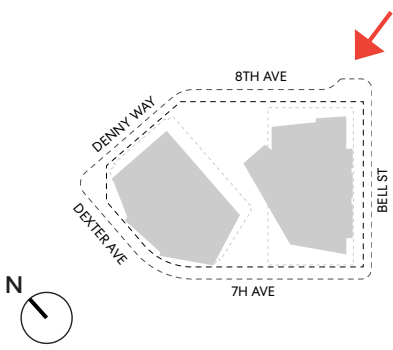
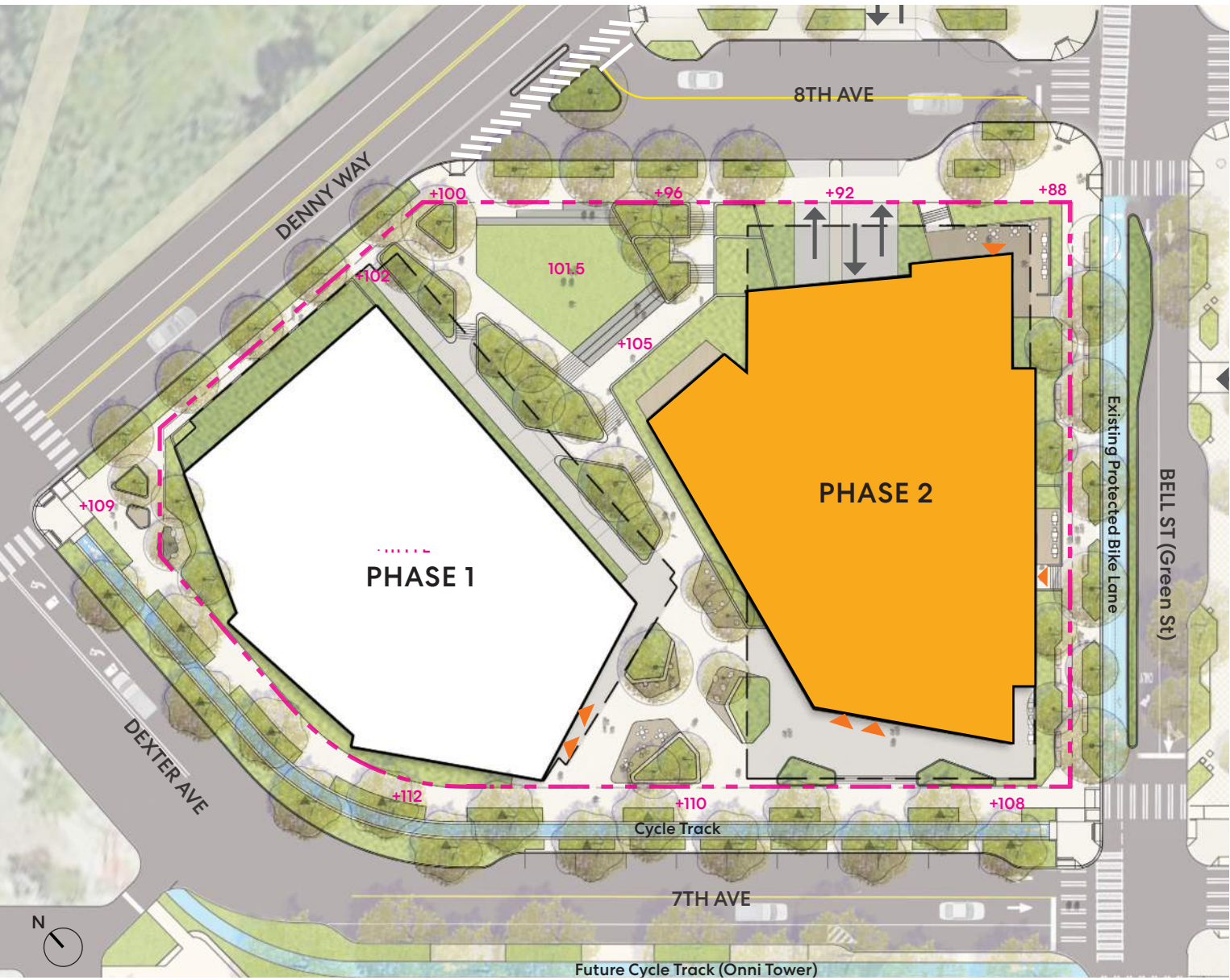
4.0 | MASSING CONCEPTS
SCHEME 03 PREFERRED



4.0 | MASSING CONCEPTS
SCHEME 03 PREFERRED

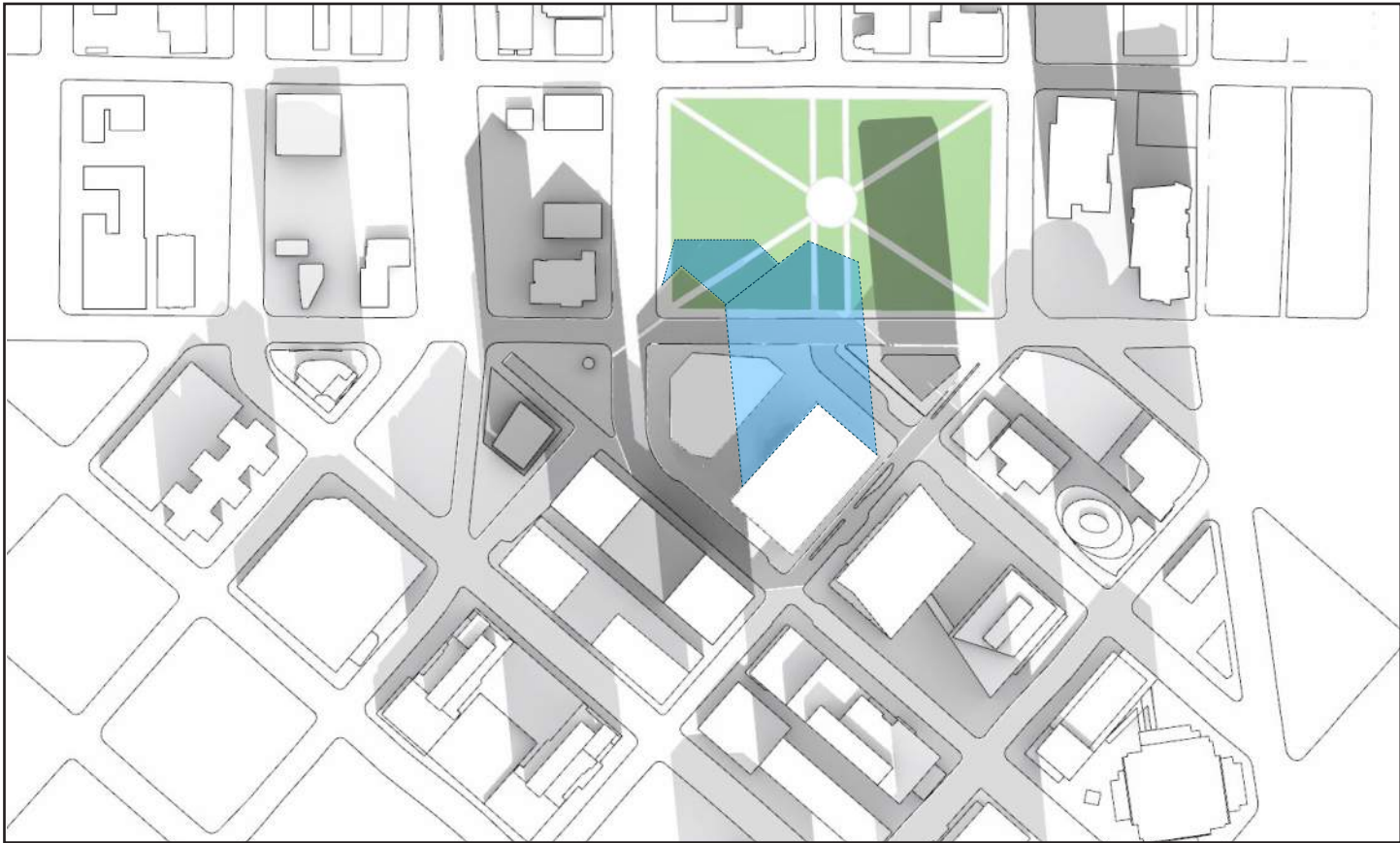


4.0 | MASSING CONCEPTS
CONCEPT OVERVIEW - PHASING

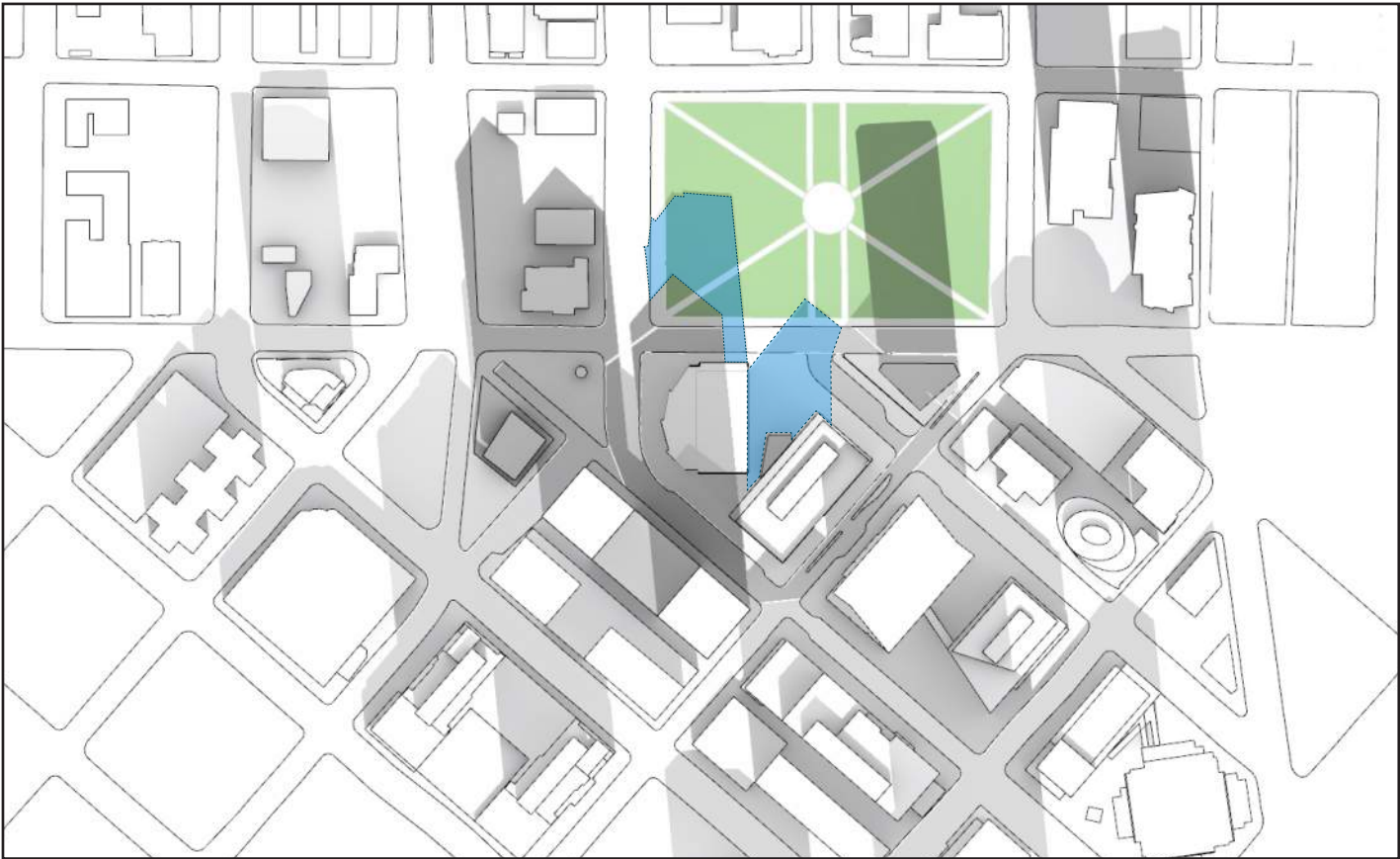


SCHEME 2

12 PM

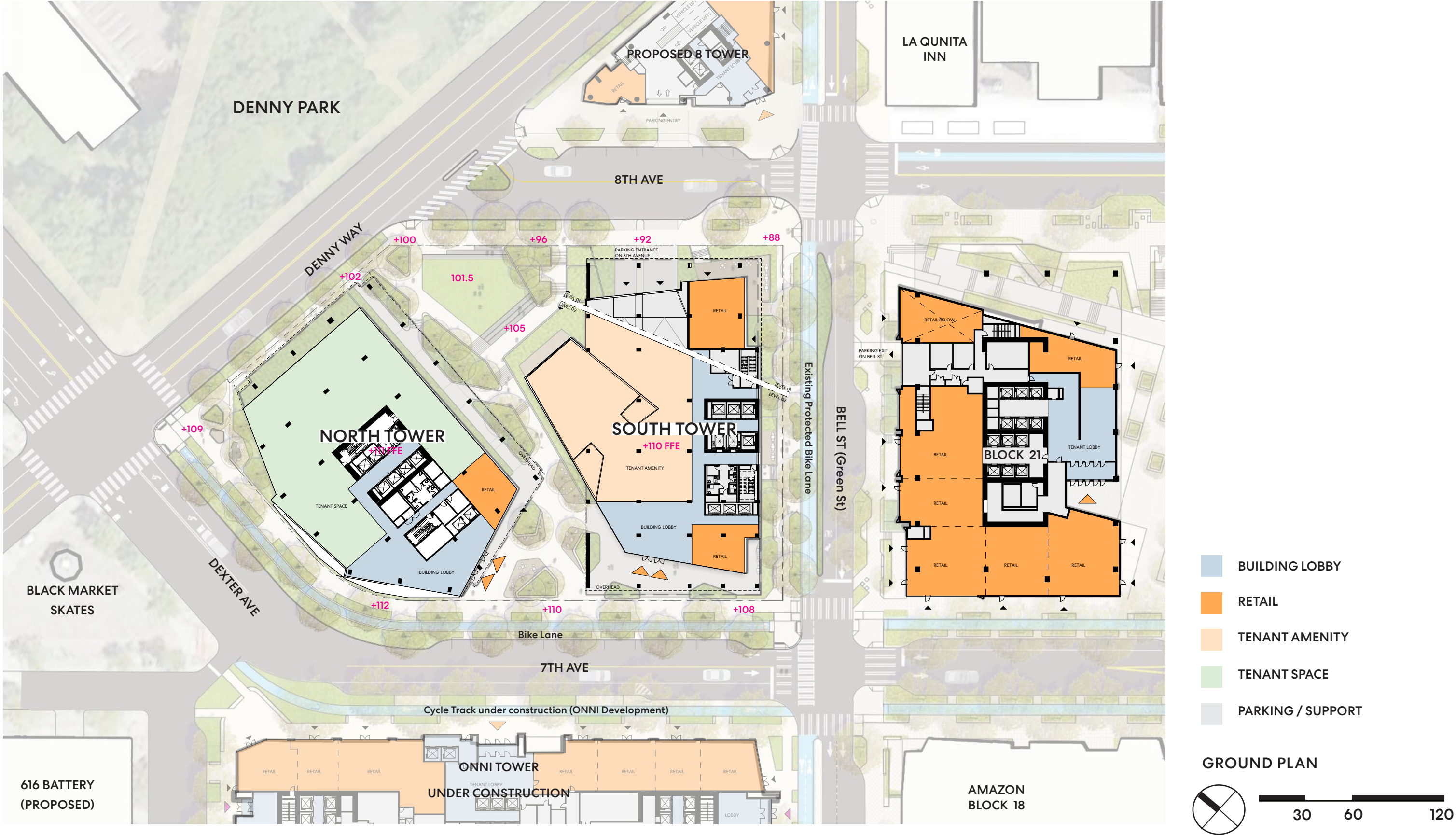


SCHEME 3

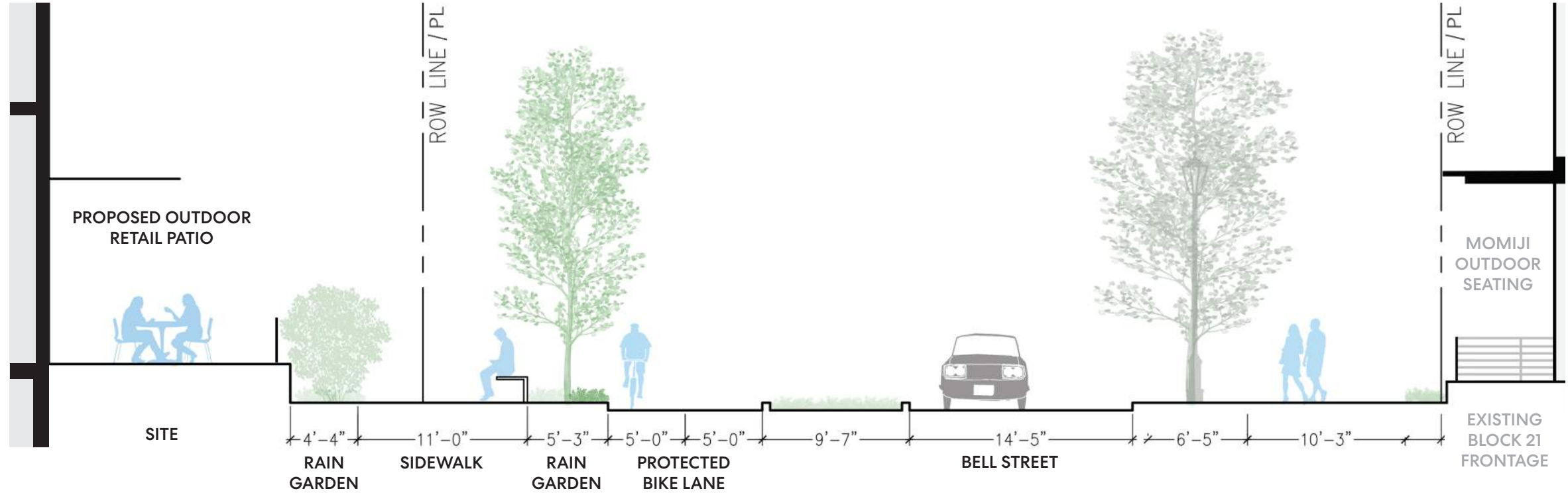
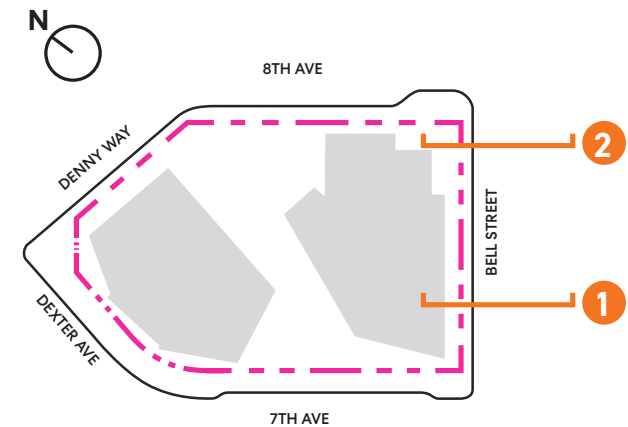
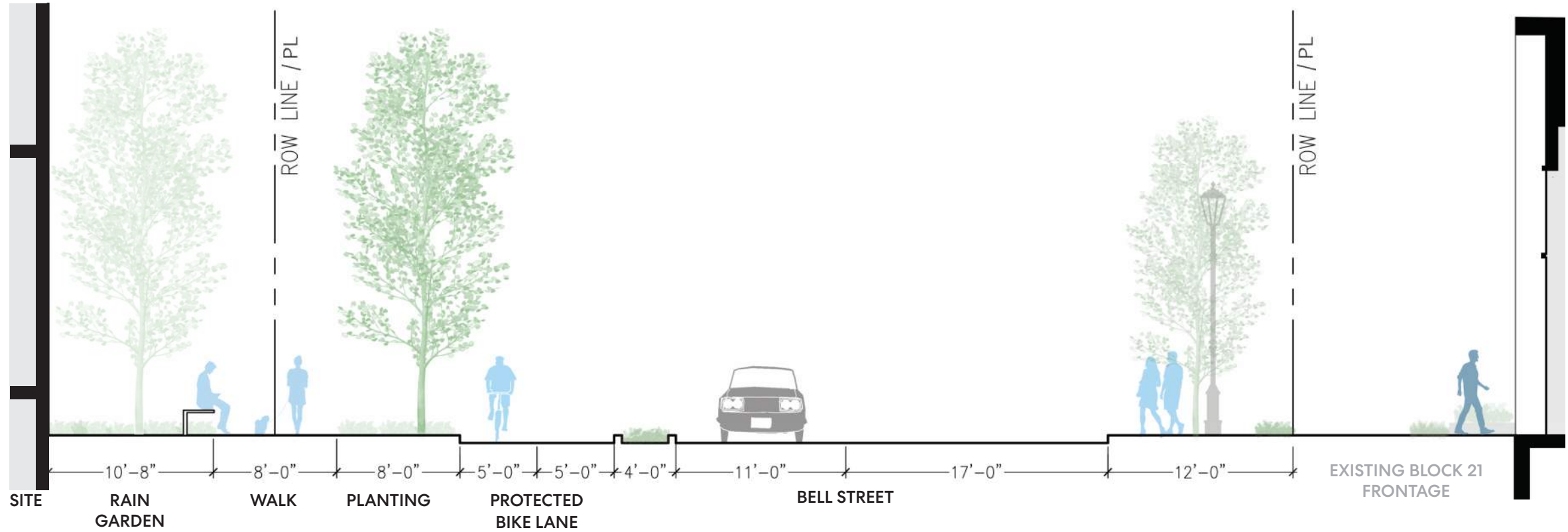


- Shadows from existing buildings
- Shadows from planned development
- Shadows added from proposed development

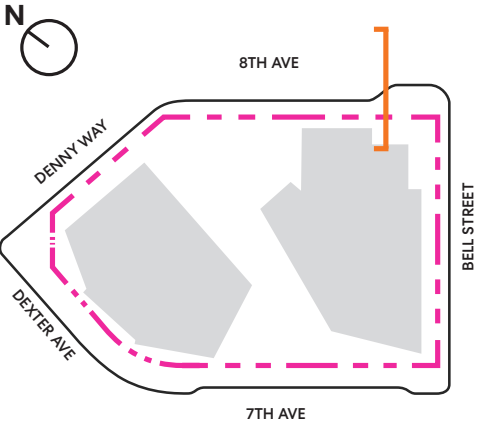
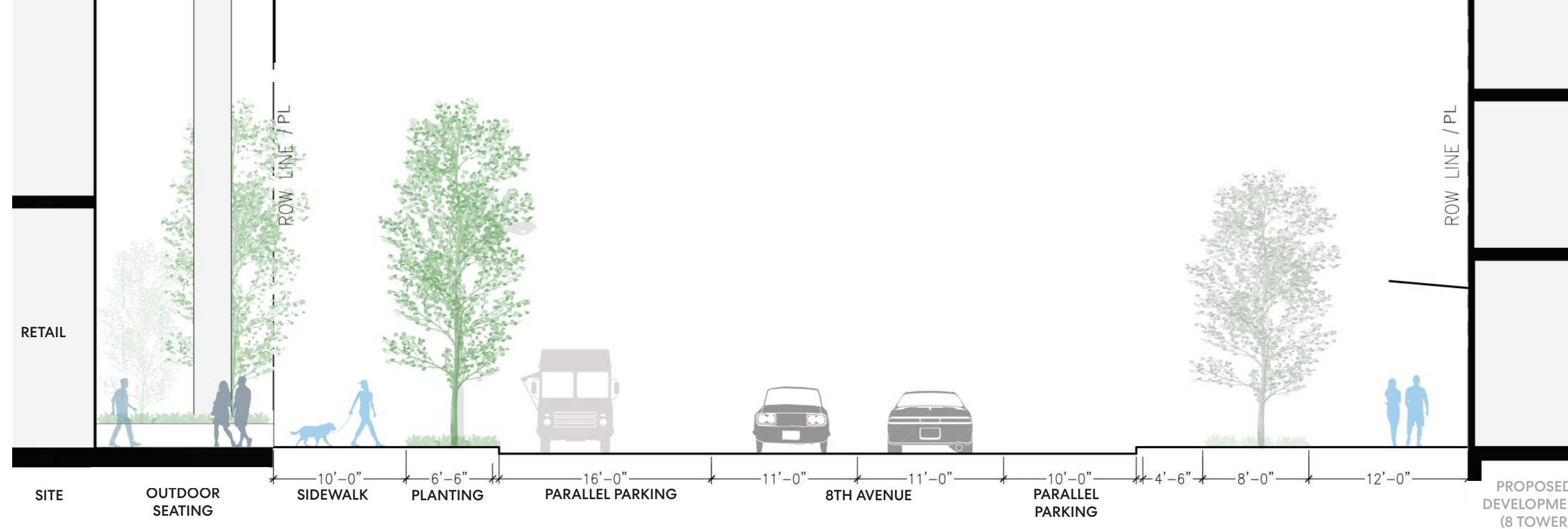
4.0 | MASSING CONCEPTS
GROUND LEVEL ACTIVATION



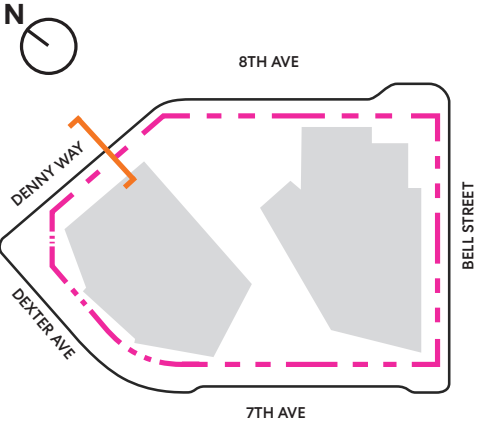
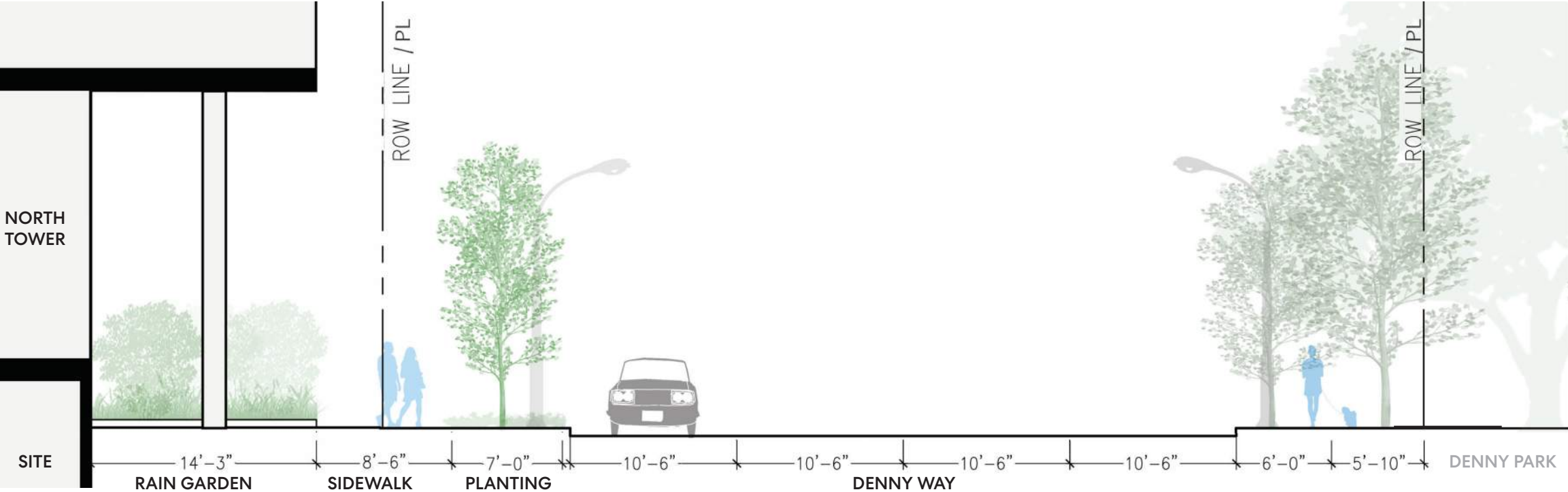
4.0 | MASSING CONCEPTS
PROPOSED STREETScape SECTIONS - BELL STREET (GREEN STREET)



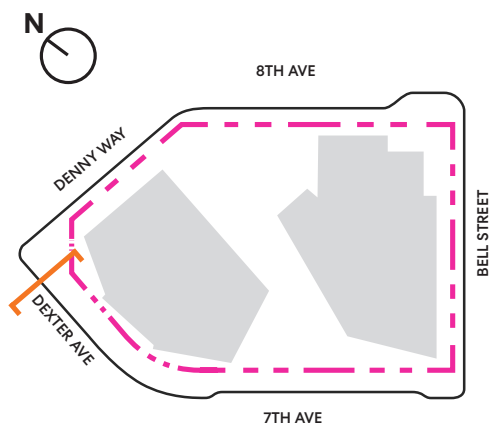
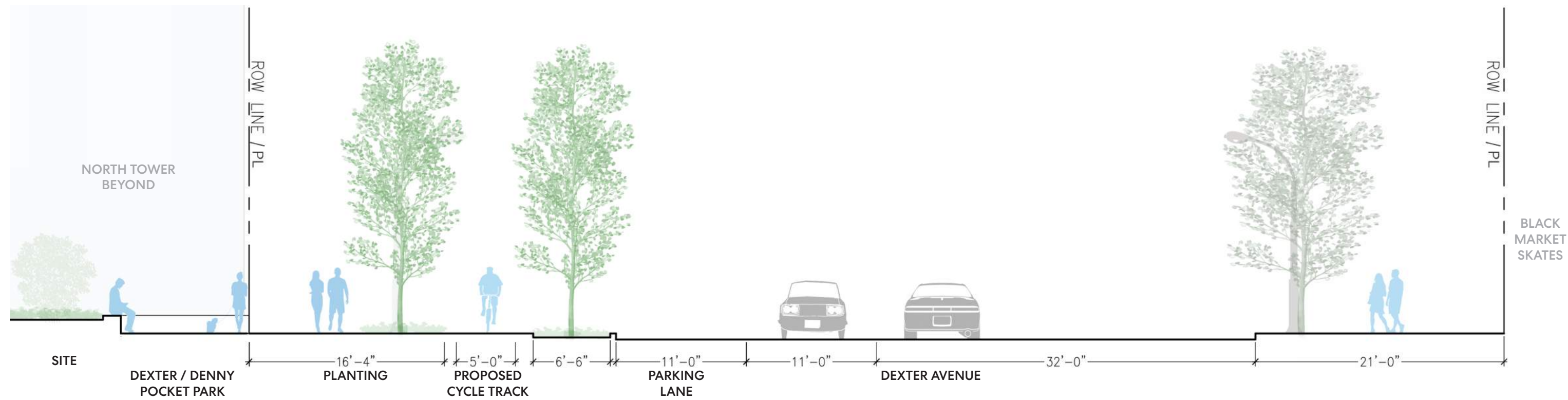
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SCHEME 03 PREFERRED
8TH AVE STREETSCAPE



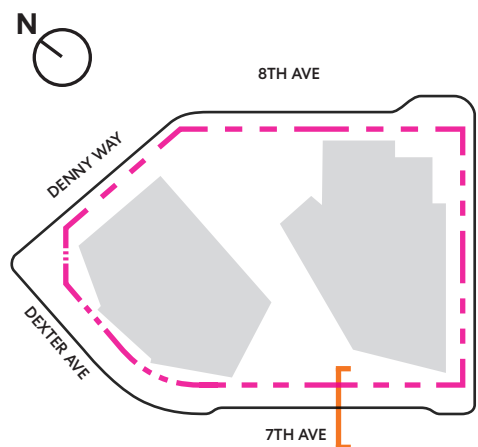
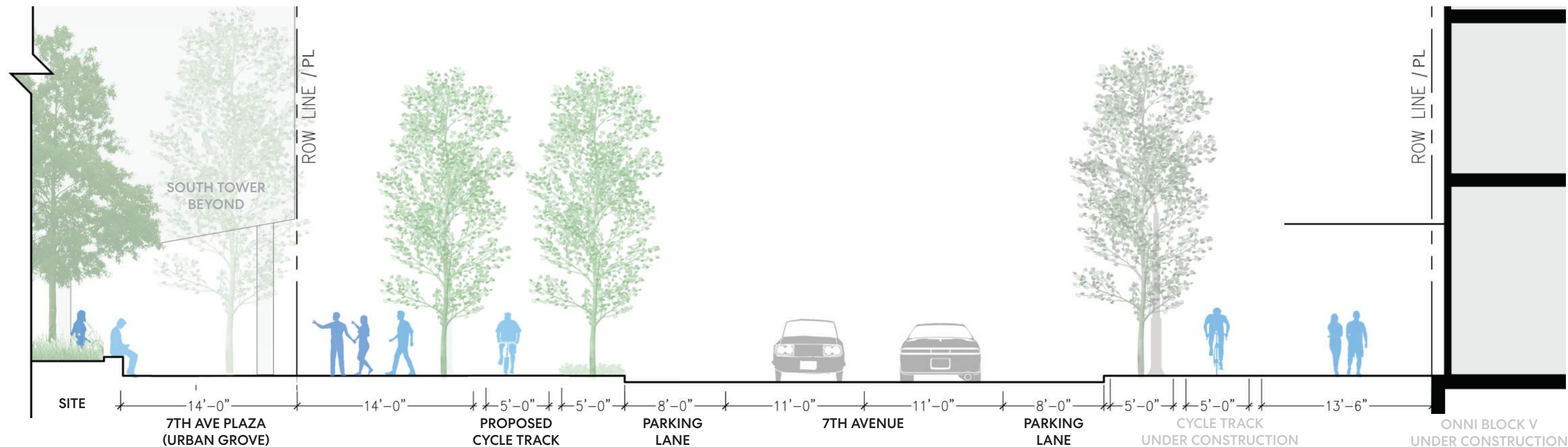
4.0 | MASSING CONCEPTS
 SCHEME 03 PREFERRED
 DENNY WAY STREETSCAPE



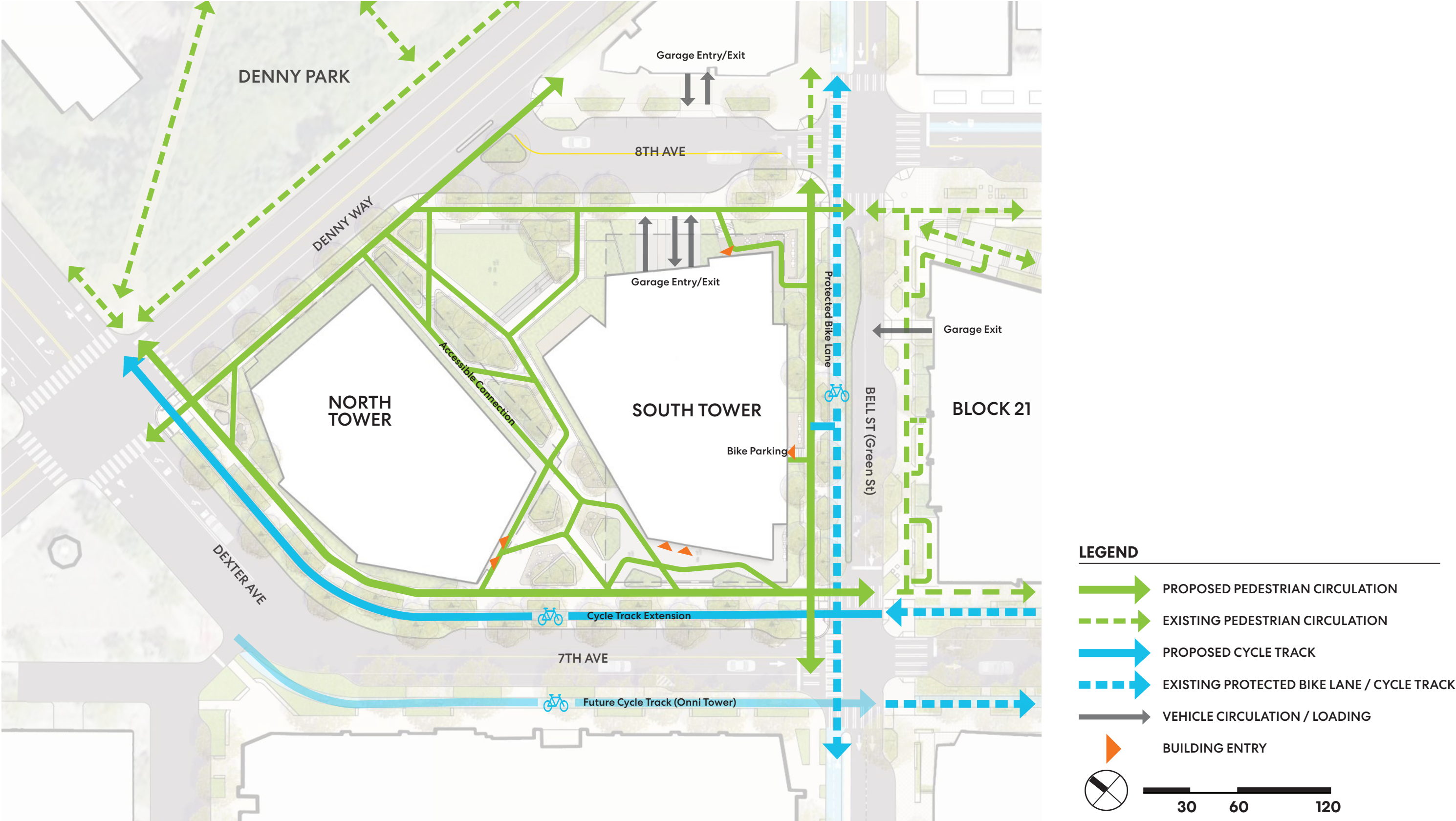
4.0 | MASSING CONCEPTS
 SCHEME 03 PREFERRED
 DEXTER AVENUE STREETScape



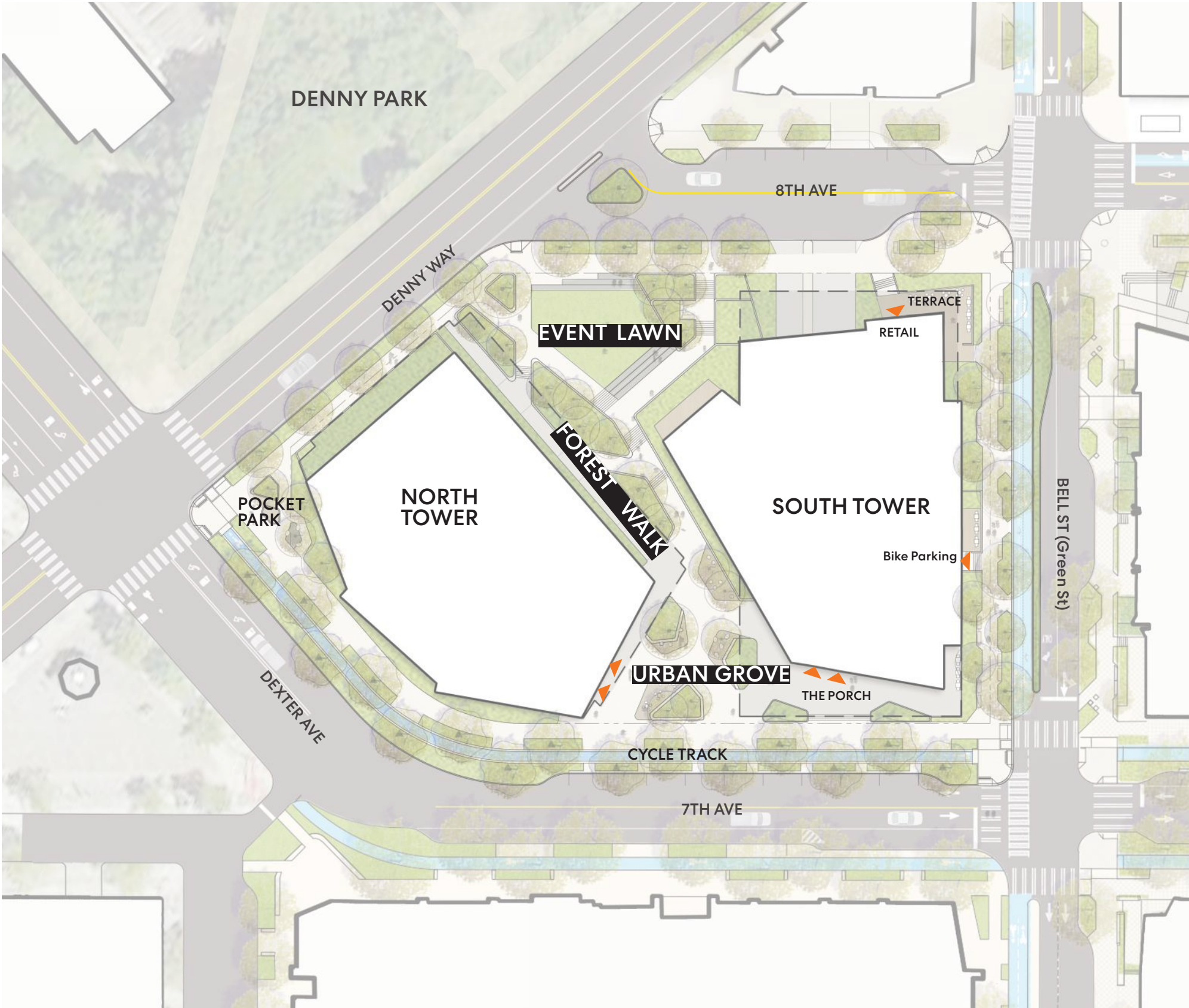
4.0 | MASSING CONCEPTS
SCHEME 03 PREFERRED
7TH AVE STREETSCAPE



4.0 | MASSING CONCEPTS
SCHEME 03 PREFERRED
CIRCULATION DIAGRAM



4.0 | MASSING CONCEPTS
SCHEME 03 PREFERRED
SITE PROGRAMMING



EVENT LAWN



FOREST WALK



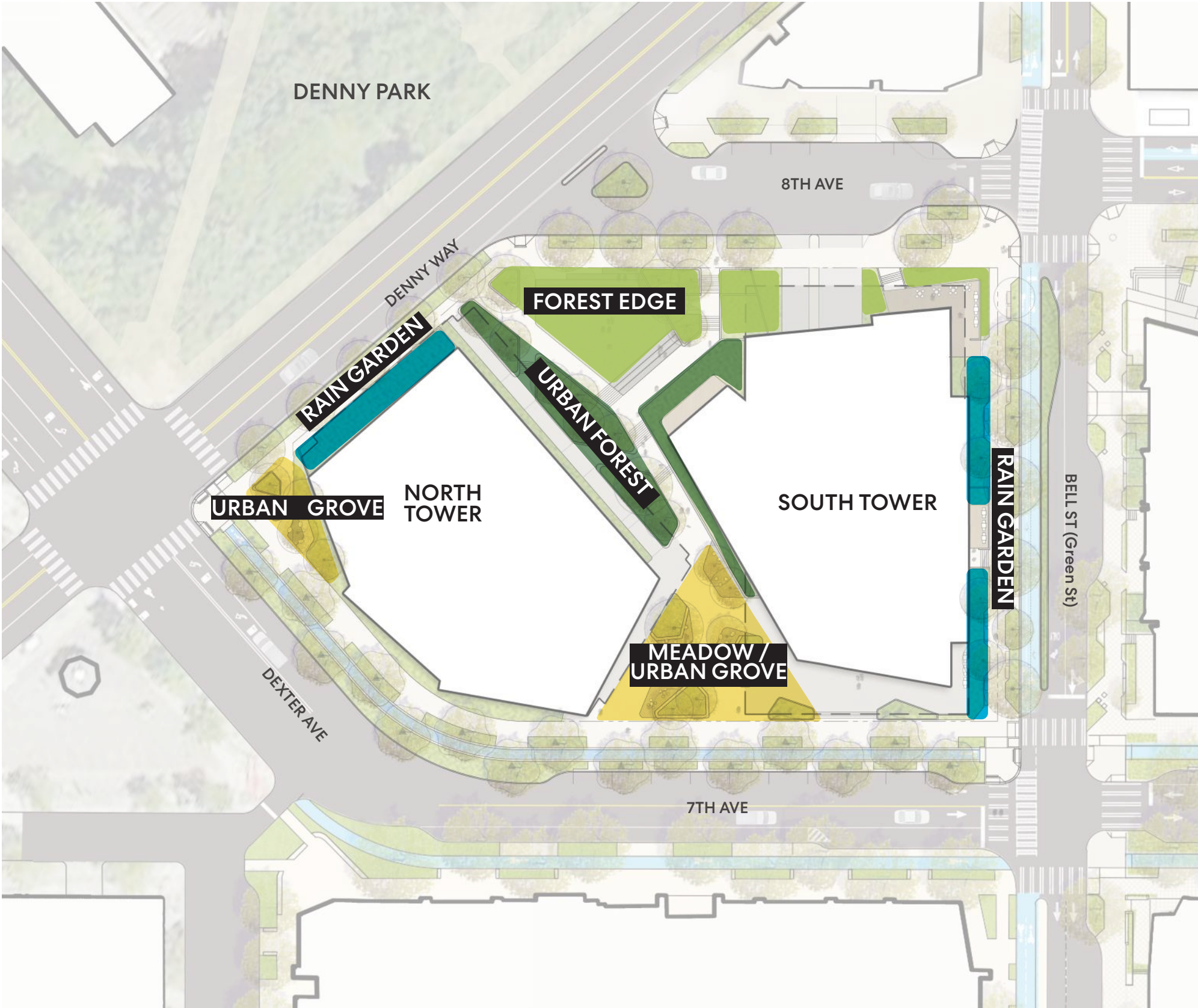
URBAN GROVE/ POCKET PARK



FOOD TRUCKS



4.0 | MASSING CONCEPTS
SCHEME 03 PREFERRED
PLANTING TYPOLOGIES



URBAN GROVE



MEADOW



URBAN FOREST



RAIN GARDEN



4.0 | MASSING CONCEPTS
CONCEPT OVERVIEW

SCHEME 01



- Pros:**
- Allows for better utilization of site at corner of Denny and Dexter
 - Eliminates loading and parking access from a green street
 - Aligns with development pattern to south
 - No departures required

- Cons:**
- Through block connection is constricted
 - Minimal open space provided
 - Does not account for grid shift at Denny Way
 - Does not enhance the skyline
 - Does not respond to neighborhood context
 - Pedestrian experience is not significantly improved
 - Massing does not allow for optimal light and air around buildings
 - Project turns it's 'back' to Denny Park

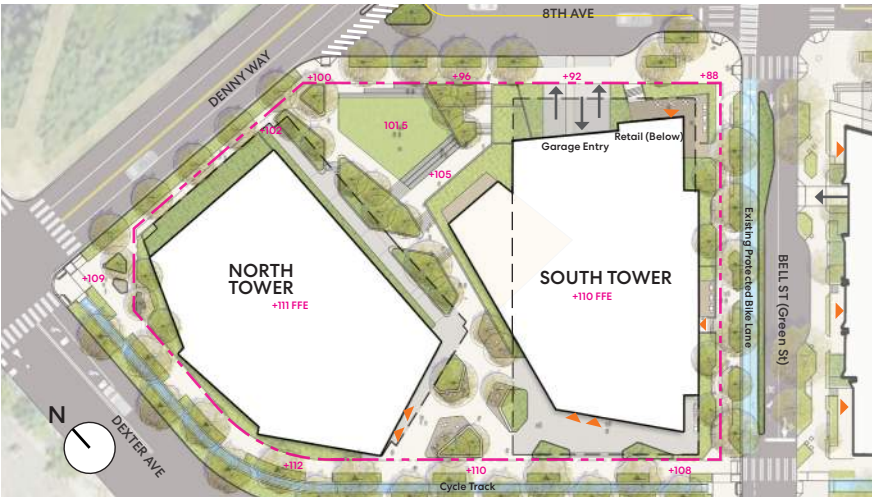
SCHEME 02



- Pros:**
- Allows for more generous through block connection with greater than required open space
 - More light and air around buildings
 - No departures required

- Cons:**
- Does not account for grid shift at Denny Way
 - Lost opportunity for a significant building presence establishing a gateway to downtown at corner of Dexter and Denny
 - Does not allow for modulation of skyline
 - Project turns it's 'back' to Denny Park

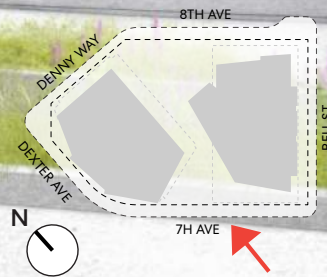
SCHEME 03 (PREFERRED)



- Pros:**
- Allows for a striking building on a significant corner marking the transition to downtown
 - Opens buildings and site to Denny Park
 - Creates opportunity for two unique plazas
 - Maximizes open space on site
 - Connects to grids at either side of Denny Way
 - Establishes plazas along 7th and 8th which link 'urban arbo-retum' to south with Denny Park to the north
 - Opens up pedestrian sight lines to Denny Park from Dexter

- Cons:**
- Departures required

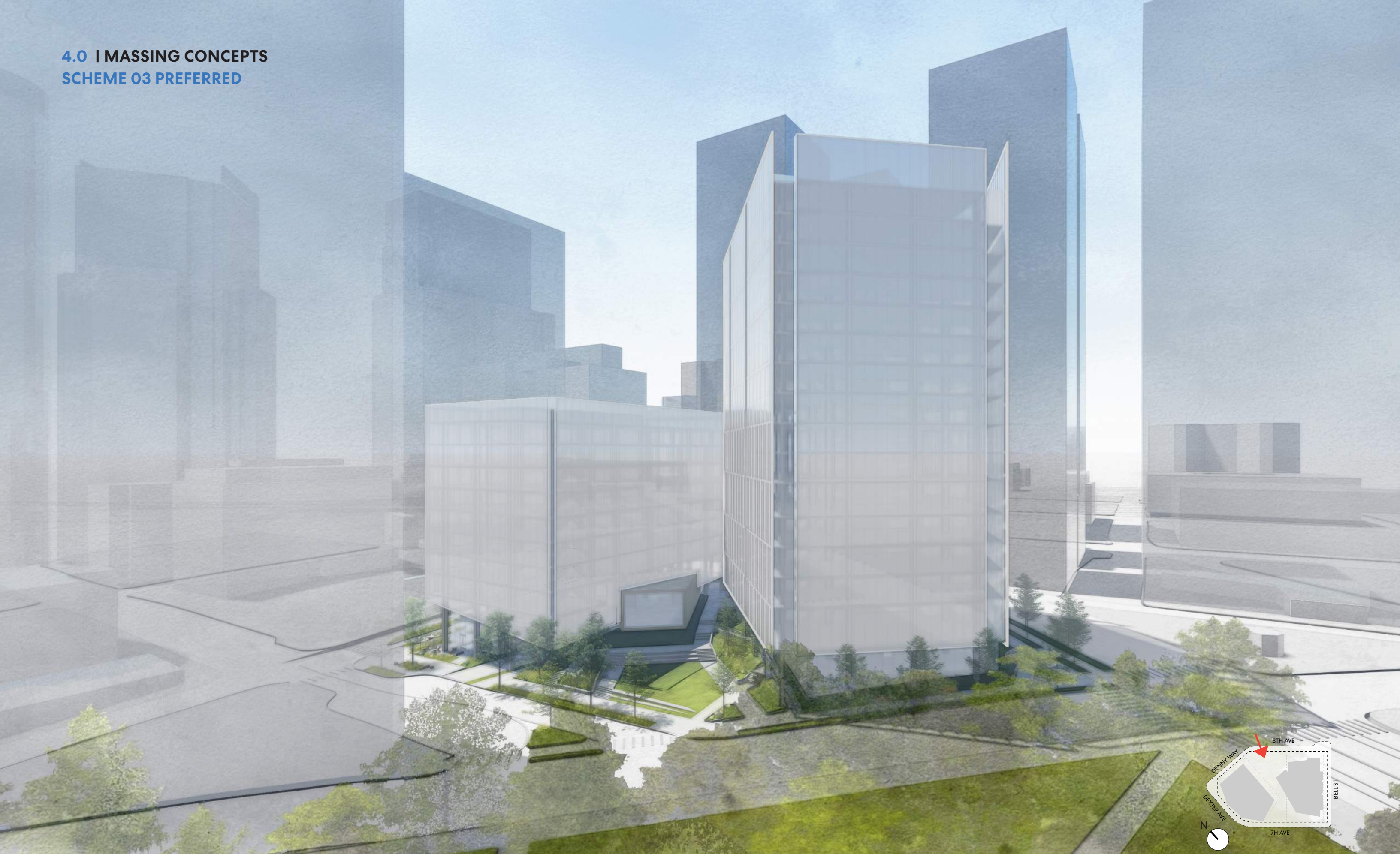
4.0 | MASSING CONCEPTS
SCHEME 03 PREFERRED
URBAN GROVE



4.0 | MASSING CONCEPTS
SCHEME 03 PREFERRED
EAST EVENT LAWN



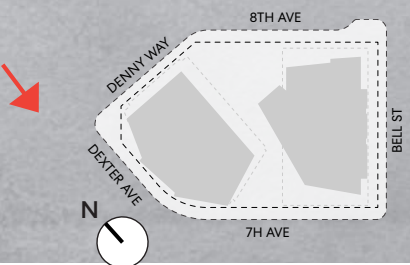
4.0 | MASSING CONCEPTS
SCHEME 03 PREFERRED



4.0 | MASSING CONCEPTS
SCHEME 03 PREFERRED



4.0 | MASSING CONCEPTS
SCHEME 03 PREFERRED

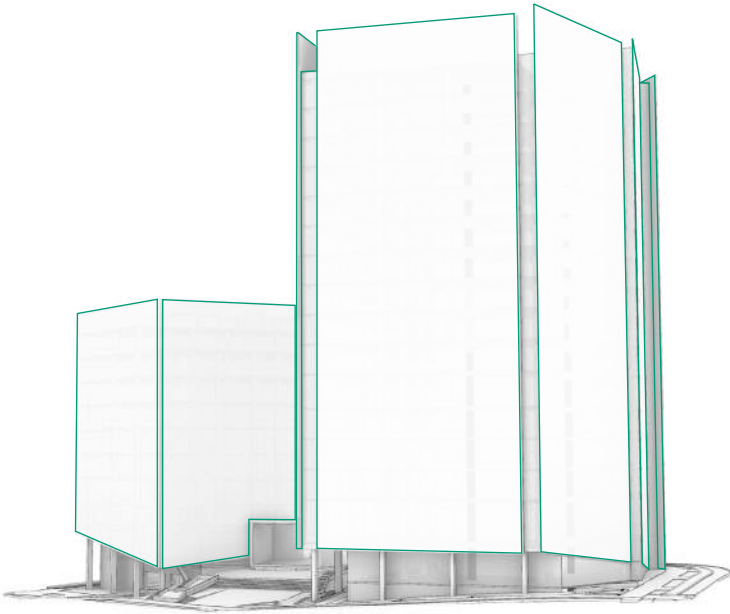
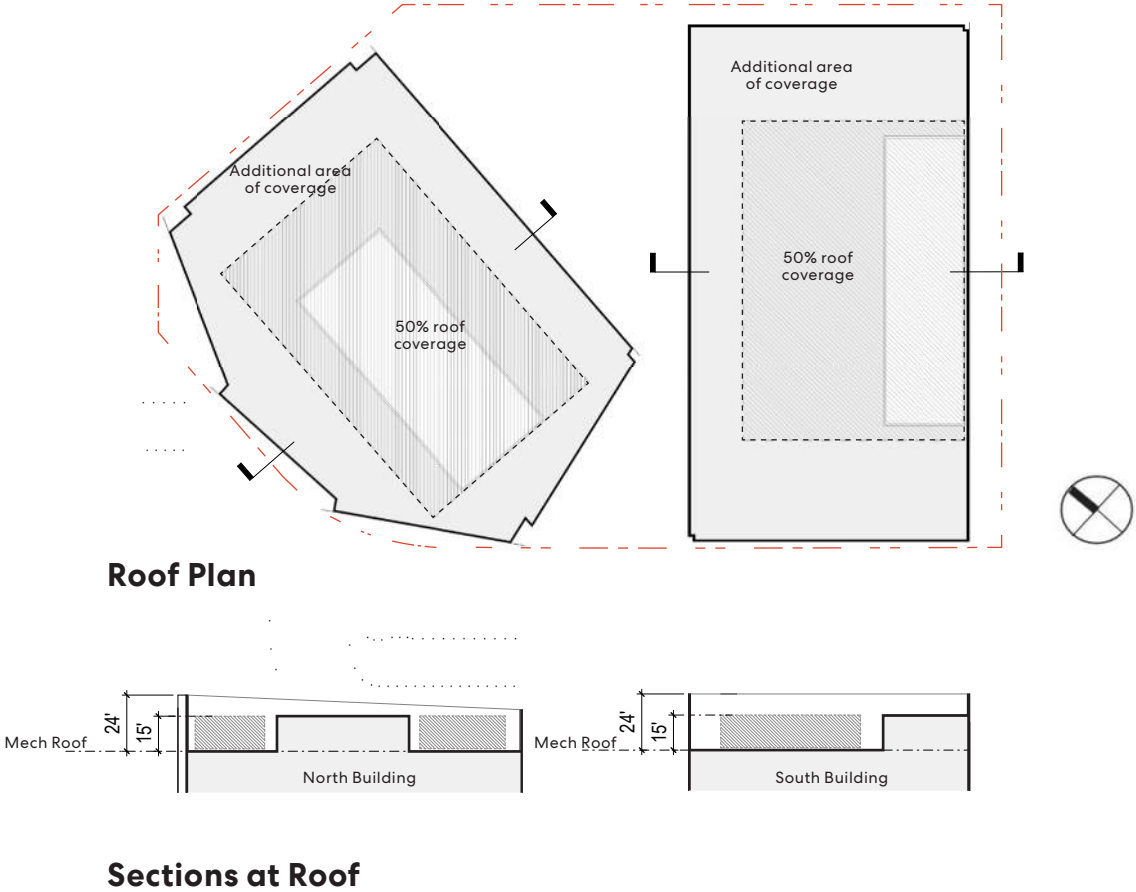


05 DEPARTURES

5.0 | DEPARTURES

ROOF TOP FEATURES

Code Citation & Requirement (Excerpt)	Proposed Departures	Rationale
<p>23.49.008 - Structure height</p> <p>D. Rooftop features</p> <p>2. The following rooftop features are permitted up to the heights indicated below, as long as the combined coverage of all rooftop features, whether or not listed in this subsection 23.49.008.D.2, does not exceed 75% of the roof area for structures that are subject to maximum floor area limits per story pursuant to Section 23.49.058, or 50% of the roof area for other structures.</p> <p>3. Screening of rooftop features</p> <p>b. Except in the PMM zone, the amount of roof area enclosed by rooftop screening may exceed the maximum percentage of the combined coverage of all rooftop features as provided in subsection 23.49.008.D.2.</p>	<ul style="list-style-type: none">Allow for rooftop mechanical equipment in excess of 50% of roof area per 23.49.008.3.b. Equipment to be located behind rooftop screening as illustrated below.	<ul style="list-style-type: none">We propose providing a screening enclosure integrated into the overall building massing, which extends up to a maximum of 24ft above the maximum allowed height (10% of 240 feet). The screening device will hide all mechanical rooftop features from public view. By integrating the rooftop mechanical with the facade we are responding to the Downtown Design Guidelines <i>B-4 Design a well-proportioned and unified building</i> as well as <i>A-2 Enhance the Skyline</i>.With the screening provided by the enclosure we anticipate utilizing more of the rooftop for mechanical. This will eliminate additional blank components in the elevations which would be required to screen mechanical equipment at other locations in the building. Limiting the amount of mechanical equipment on lower floors reduces the amount of louvers in the facade which also responds to <i>B-4 Design a well-proportioned and unified building</i>.



PROPOSED OPTION

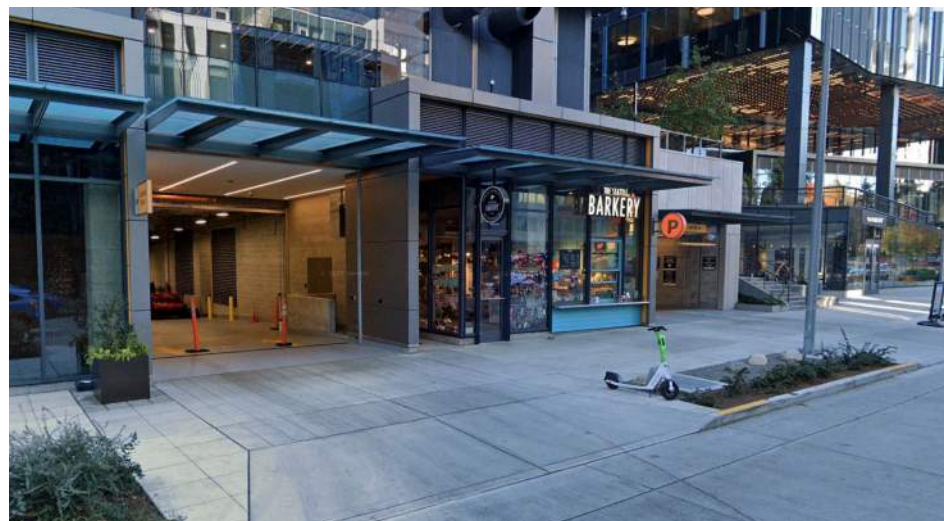
5.0 | DEPARTURES

CURB CUTS

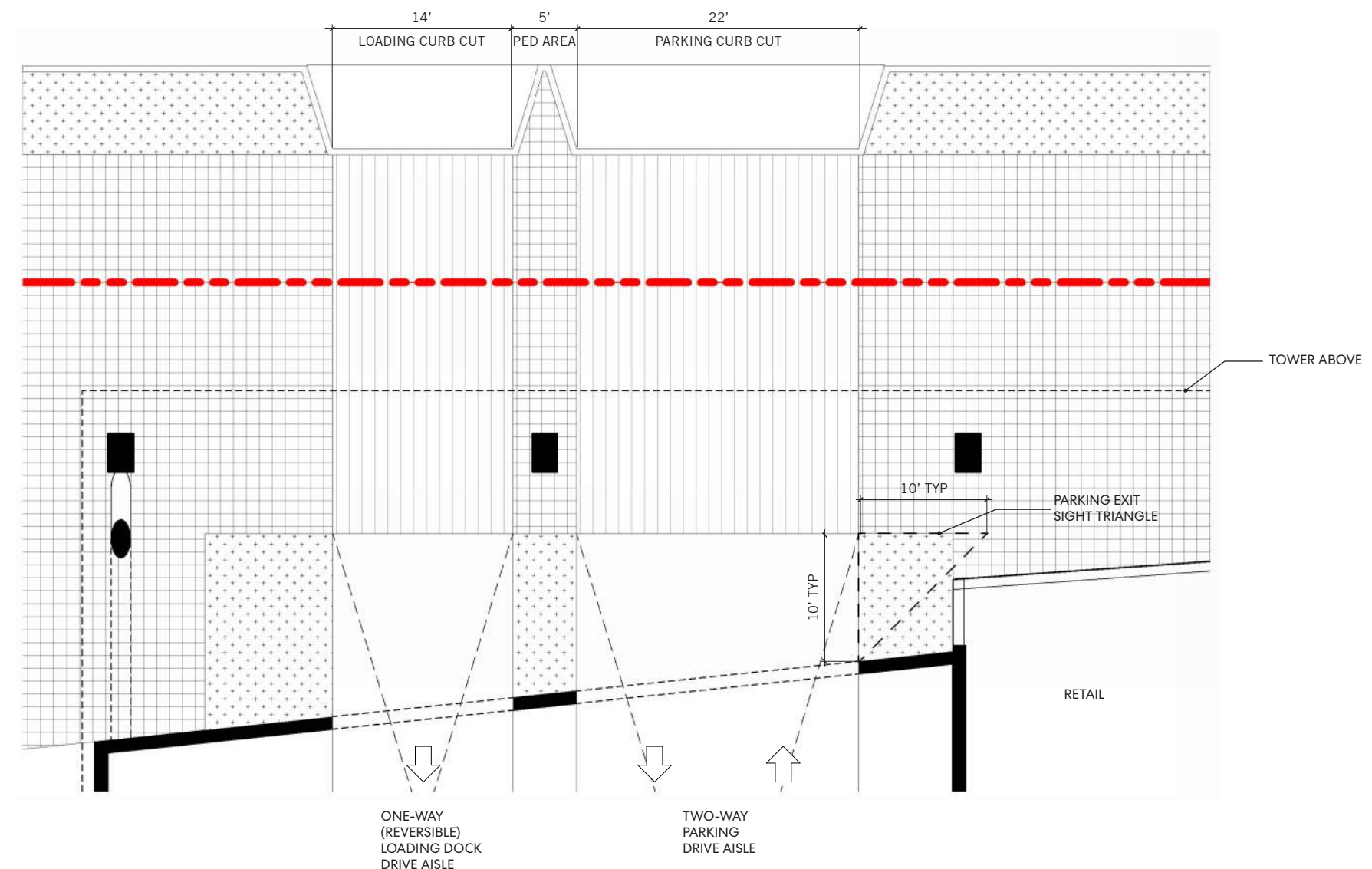
Code Citation & Requirement (Excerpt)	Proposed Departures	Rationale
<p>23.54.030 - Parking Space and Access Standards</p> <p>F. Curb cuts</p> <p>2.a.4. In downtown zones, a maximum of two curb cuts for one-way traffic at least 40 feet apart, or one curb cut for two-way traffic, are permitted on each street front where access is permitted by subsection 23.49.019.H. No curb cut shall be located within 40 feet of an intersection. These standards may be modified by the Director as a Type I decision on lots with steep slopes or other special conditions, to the minimum extent necessary to provide vehicular and pedestrian safety and facilitate a smooth flow of traffic</p>	<ul style="list-style-type: none"> The project proposes (1) two-way curb cut for passenger vehicles and (1) one-way (reversible) curb cut for service vehicles along 8th Ave. The curb cuts will be proximal to each other to provide the sense of a single curb cut. However, a textured pedestrian area between the two driveways will be provided. 	<ul style="list-style-type: none"> In accordance with the Downtown Design Guidelines the proposed design locates all service areas below grade, <i>E-3 Minimize the presence of service areas.</i> Additionally all parking is located below grade per <i>E-2 Integrate parking facilities.</i> Entry to the parking garage and loading dock are set back from the property line and located under the tower above so as to minimize their visual impact. Impacts from curb cuts are addressed by with the provision of safety devices (contrasting/textured specialty paving between driveways and sidewalks, <i>E-1 Minimize curb cut impacts.</i> The proposed configuration provides a safer entry/exit sequence between passenger and service vehicles by allowing the entries to remain separate while appearing connected. The separate entrances also support a functional below grade parking and loading dock configuration.



2118 8th Ave



2202 8th Ave

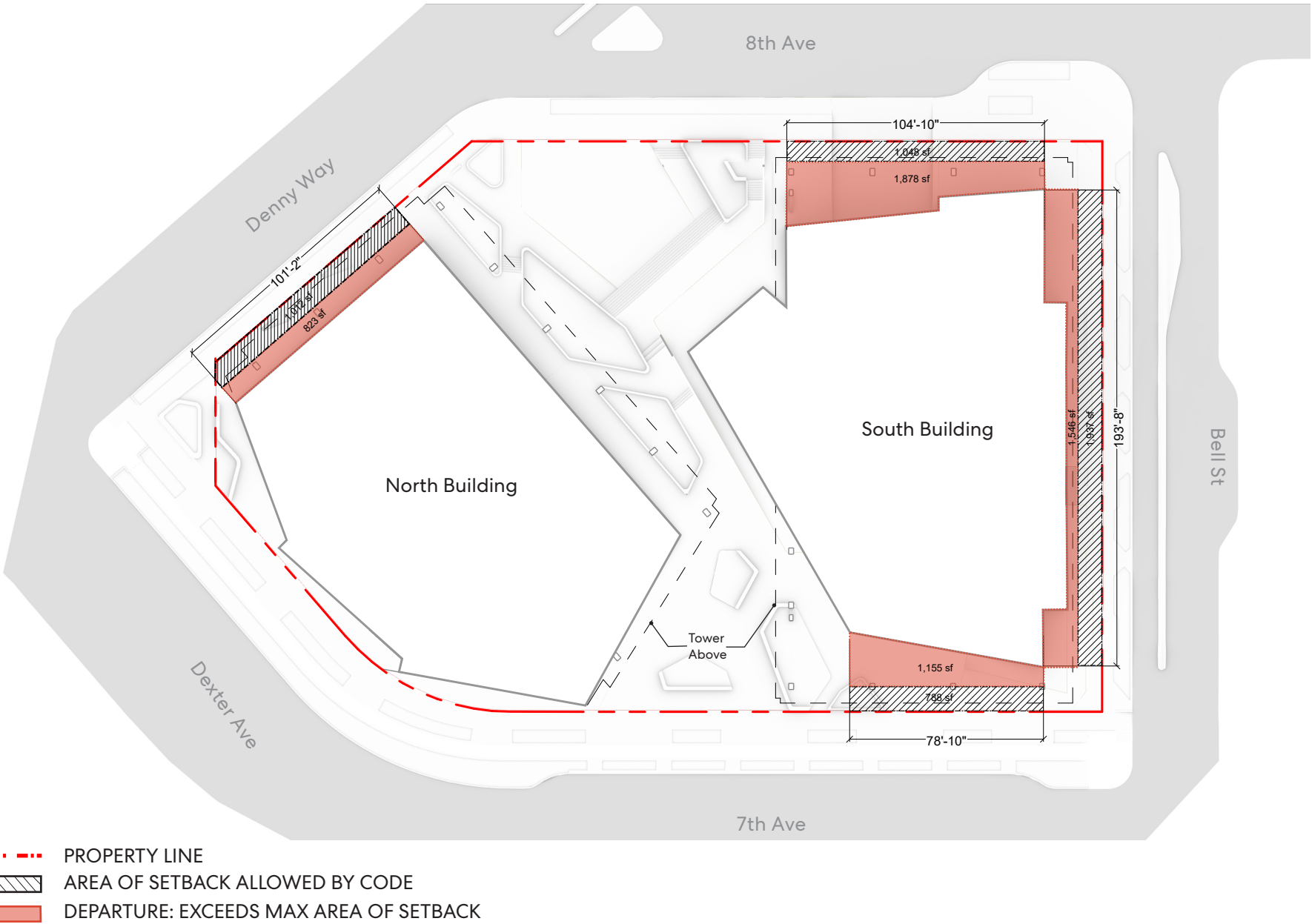


5.0 | DEPARTURES

FACADE SETBACK LIMITS (MAX AREA OF SETBACKS)

Code Citation & Requirement (Excerpt)	Proposed Departures	Rationale
<p>23.49.056 - Downtown Office Core 1 (DOC1), Downtown Office Core 2 (DOC2), and Downtown Mixed Commercial (DMC) street facade, landscaping, and street setback requirements</p> <p>B.Facade setback limits</p> <p>2. General setback limits. The following setback limits apply on streets not requiring property line facades, as shown on Map 1H:</p> <p>a. The portion of a structure subject to setback limits shall vary according to the structure height and required minimum facade height, as follows:</p> <ol style="list-style-type: none"> Except as provided in subsection 23.49.056.B.2.a.3, if the structure is greater than 15 feet in height, the setback limits apply to the facade between an elevation of 15 feet above sidewalk grade and the minimum facade height established in subsection 23.49.056.A and illustrated in Exhibit C for 23.49.056. If the minimum facade height is 15', the setback limits apply to the portion of the street-facing facade that is 15 feet or less in height. <p>b. The maximum area of all setbacks between the street lot line and facade along each street frontage of a lot shall not exceed the area derived by multiplying the averaging factor by the width of the street frontage of the structure along that street (see Exhibit D for 23.49.056). The averaging factor is five on Class I pedestrian streets and ten on Class II pedestrian streets and designated green streets.</p>	<p>Allow for setback departures</p> <ul style="list-style-type: none"> Exceed maximum area of setback limitations along Bell St, Denny Way, 8th Ave and 7th Ave 	<ul style="list-style-type: none"> The ground level setbacks proposed along Bell St, 7th Ave, 8th Ave and Denny Way provide sightlines into the site and draw pedestrians to the open spaces provide, <i>D-1 Provide inviting and useable open space</i>. This pattern of open space, providing open space mid-block and creating a through-block connection, follows the pattern of development that is established to the southeast <i>B-3 Reinforce the positive urban form & architectural attributes of the immediate area</i>. The porous edges and generous landscaped pedestrian circulation paths build upon and enhance the surrounding environments <i>B-1 Respond to the neighborhood context</i>. Additionally, these open spaces and setbacks provide inviting and useable open space that promote pedestrian interaction. Downtown Design Standards C-1.

Street	Class	Averaging Factor	Street Frontage of Structure	Max Setback Area (sf)	Proposed Setback Area (sf)
Bell	Green	10	193'-8"	1,937	3,483
Denny	Class II Ped	10	101'-2"	1,012	1,835
8th	Class II Ped	10	104'-10"	1,049	2,927
7th	Class II Ped	10	78'-10"	788	1,943

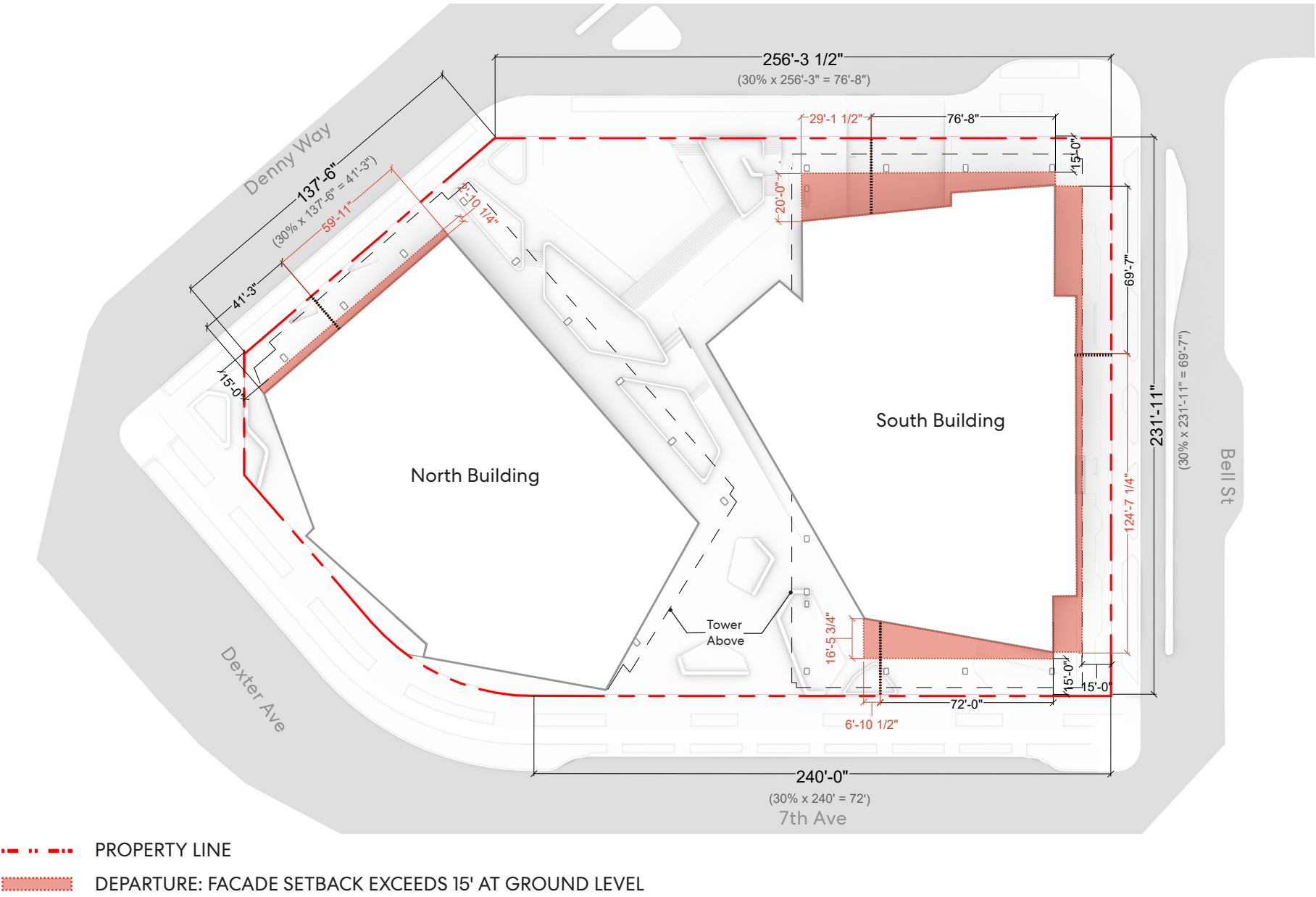


5.0 | DEPARTURES

FACADE SETBACK LIMITS (MAX WIDTH OF SETBACK)

Code Citation & Requirement (Excerpt)	Proposed Departures	Rationale
<p>23.49.056 - Downtown Office Core 1 (DOC1), Downtown Office Core 2 (DOC2), and Downtown Mixed Commercial (DMC) street facade, landscaping, and street setback requirements</p> <p>B.Facade setback limits</p> <p>2. General setback limits. The following setback limits apply on streets not requiring property line facades, as shown on Map 1H:</p> <p>a. The portion of a structure subject to setback limits shall vary according to the structure height and required minimum facade height, as follows:</p> <p>1. Except as provided in subsection 23.49.056.B.2.a.3, if the structure is greater than 15 feet in height, the setback limits apply to the facade between an elevation of 15 feet above sidewalk grade and the minimum facade height established in subsection 23.49.056.A and illustrated in Exhibit C for 23.49.056.</p> <p>3. If the minimum facade height is 15', the setback limits apply to the portion of the street-facing facade that is 15 feet or less in height.</p> <p>c. The maximum width, measured along the street lot line, of any setback area exceeding a depth of 15' from the street lot line shall not exceed 80', or 30% of the lot frontage on that street, whichever is less.</p>	<p>Allow for setback departures</p> <ul style="list-style-type: none">Exceed 15' setback depth at the ground plane along 7th Ave, 8th Ave, and Denny Way.	<ul style="list-style-type: none">The ground level setbacks proposed along Bell St, 7th Ave, 8th Ave and Denny Way provide sightlines into the site and draw pedestrians to the open spaces provide, <i>D-1 Provide inviting and useable open space</i>. This pattern of open space, providing open space mid-block and creating a through-block connection, follows the pattern of development that is established to the southeast <i>B-3 Reinforce the positive urban form & architectural attributes of the immediate area</i>.The porous edges and generous landscaped pedestrian circulation paths build upon and enhance the surrounding environments <i>B-1 Respond to the neighborhood context</i>.Additionally, these open spaces and setbacks provide inviting and useable open space that promote pedestrian interaction. Downtown Design Standards C-1.

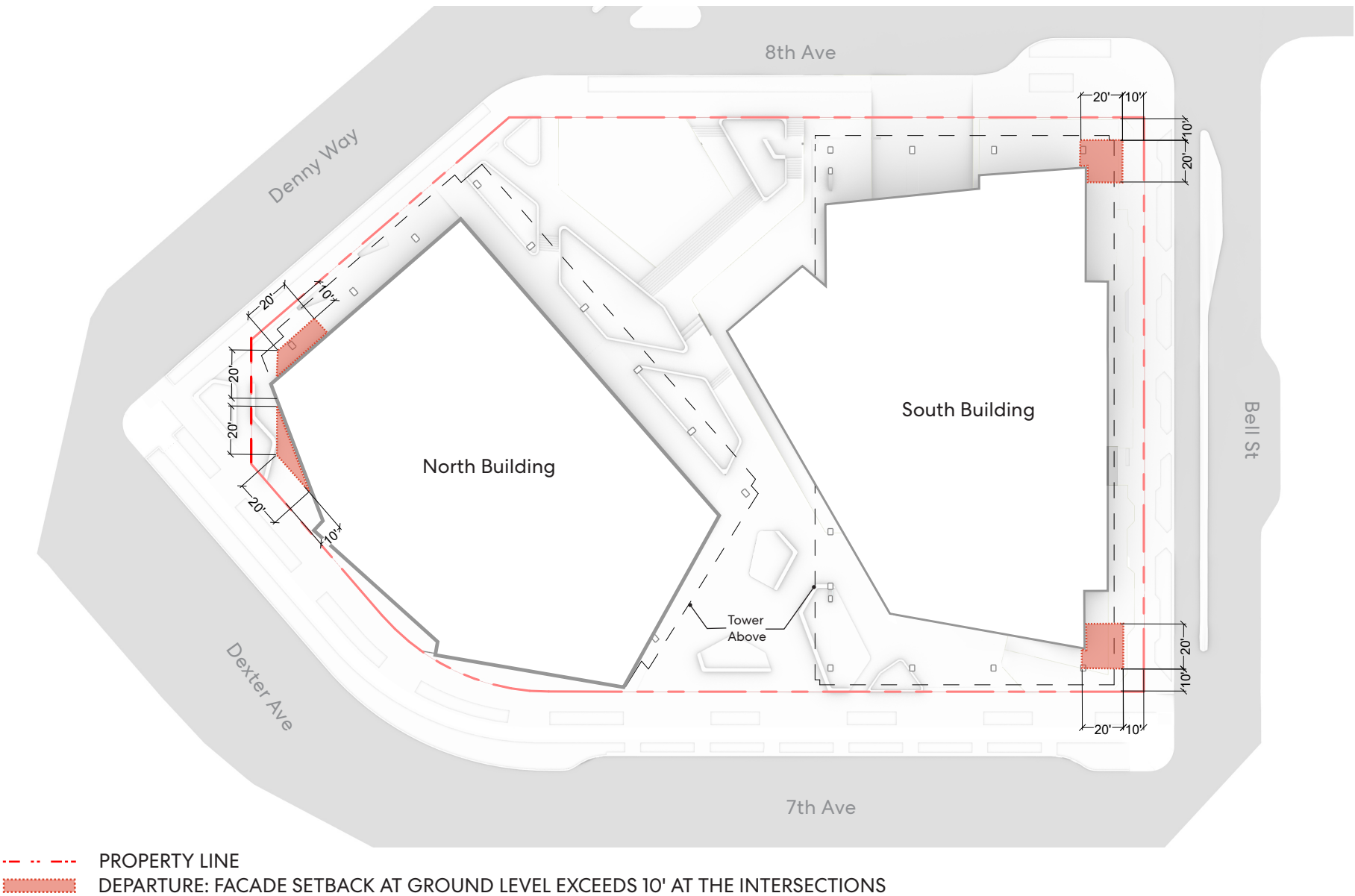
Street	Lot Frontage	x 30%	Max Width for Setbacks > 15' Deep	Proposed Width for Setbacks > 15' Deep
Bell	231'-11"	69'-7"	69'-7"	193'-8"
Denny	137'-6"	41'-3"	41'-3"	101'-2"
8th	256'-3 1/2"	76'-8"	76'-8"	104'-10"
7th	240'	72'	72'	78'-10"



5.0 | DEPARTURES

FACADE SETBACK LIMITS (MAX SETBACK AT INTERSECTIONS)

Code Citation & Requirement (Excerpt)	Proposed Departures	Rationale
<p>23.49.056 - Downtown Office Core 1 (DOC1), Downtown Office Core 2 (DOC2), and Downtown Mixed Commercial (DMC) street facade, landscaping, and street setback requirements</p> <p>B.Facade setback limits</p> <p>2. General setback limits. The following setback limits apply on streets not requiring property line facades, as shown on Map 1H:</p> <p>a. The portion of a structure subject to setback limits shall vary according to the structure height and required minimum facade height, as follows:</p> <p>1. Except as provided in subsection 23.49.056.B.2.a.3, if the structure is greater than 15 feet in height, the setback limits apply to the facade between an elevation of 15 feet above sidewalk grade and the minimum facade height established in subsection 23.49.056.A and illustrated in Exhibit C for 23.49.056.</p> <p>3. If the minimum facade height is 15', the setback limits apply to the portion of the street-facing facade that is 15 feet or less in height.</p> <p>d. The maximum setback of the facade from the street lot lines at intersections is 10 feet. The minimum distance the facade must conform to this limit is 20 feet along each street.</p>	<p>Allow for setback departures</p> <ul style="list-style-type: none">Exceed 10' setback maximum at the Dexter Ave and Denny Way intersection, the 7th Ave and Bell St intersection, and at the 8th Ave and Bell St intersection	<ul style="list-style-type: none">The ground level setbacks proposed along Bell St, 7th Ave, 8th Ave and Denny Way provide sightlines into the site and draw pedestrians to the open spaces provide, <i>D-1 Provide inviting and useable open space</i>. This pattern of open space, providing open space mid-block and creating a through-block connection, follows the pattern of development that is established to the southeast <i>B-3 Reinforce the positive urban form & architectural attributes of the immediate area</i>.The porous edges and generous landscaped pedestrian circulation paths build upon and enhance the surrounding environments <i>B-1 Respond to the neighborhood context</i>.Additionally, these open spaces and setbacks provide inviting and useable open space that promote pedestrian interaction. Downtown Design Standards C-1.



5.0 | DEPARTURES
FACADE MODULATION

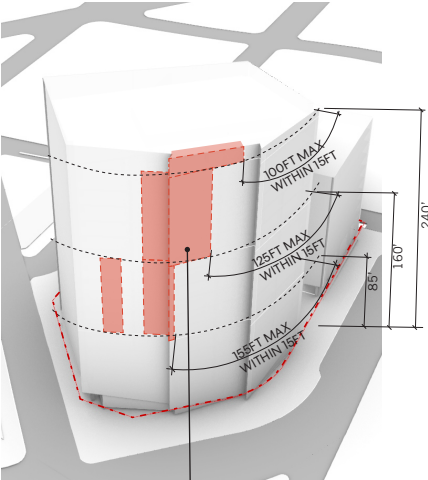
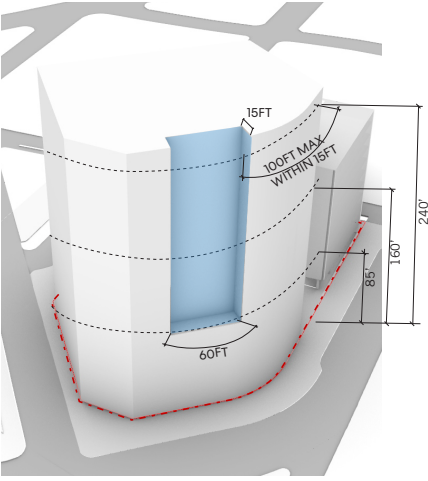
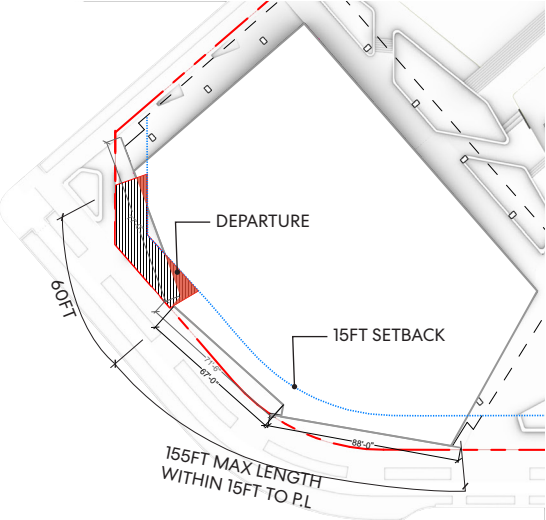
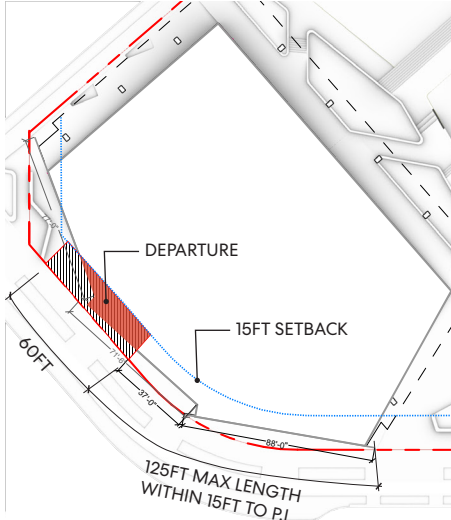
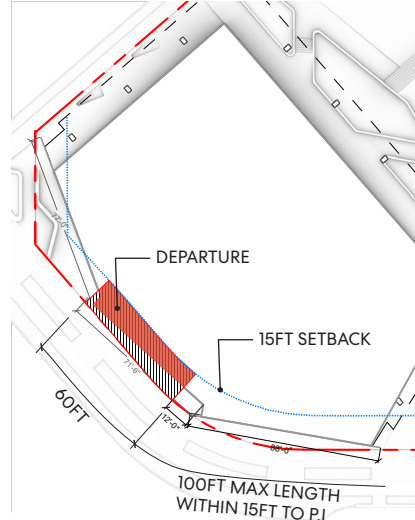

Code Citation & Requirement (Excerpt)	Proposed Departures	Rationale
<p>23.49.058 - Downtown Office Core 1 (DOC1), Downtown Office Core 2 (DOC2), and Downtown Mixed Commercial (DMC) upper-level development standards</p> <p>B. Facade modulation and upper-level width limit</p> <p>2. The following facade modulation requirements apply to structures meeting subsection 23.49.058.B.1:</p> <p>a. In DOC1, DOC2, and DMC zones, except the DMC 170 zone, facade modulation is required above a height of 85 feet above the sidewalk for any portion of a structure located within 15 feet of a street lot line. No modulation is required for portions of a facade set back 15 feet or more from a street lot line.</p> <p>c. The maximum length of a facade without modulation is prescribed in Table A for 23.49.058. This maximum length shall be measured parallel to each street lot line, and shall apply to any portion of a facade, including projections such as balconies, that is located within 15 feet of street lot lines.</p> <p>d. Any portion of a facade exceeding the maximum length of facade prescribed on Table A for 23.49.058 shall be set back a minimum of 15 feet from the street lot line for a minimum distance of 60 feet before any other portion may be within 15 feet of the street lot line.</p>	<ul style="list-style-type: none">Allow for reduction of required modulation depth along Dexter Ave above 160' <div><p>Departure</p><p>Additional portion of facade exceeding the maximum length of unmodulated facade within 15' of the street lot line</p><p>Code Compliant</p></div> <div><p>85ft -160ft above sidewalk</p><p>160ft -240ft above sidewalk</p><p>240ft above sidewalk</p></div> <div><p>Denny Way and Dexter Ave</p></div>	<ul style="list-style-type: none">The design of the façade along Dexter Ave seeks to meet the design intent for façade modulation in a non-traditional way befitting its location at the shift to the downtown grid. Instead of modulating the façade horizontally (as described in the code) the façade is modulated vertically creating a dynamic, shifting, faceted edge as you travel past the building by foot, bicycle, or vehicle. This approach is in direct response to its location at the busy intersection of Dexter Ave and Denny Way, meets the intent of the code and, <i>B-1 Respond to neighborhood context</i>, in the Downtown Design Guidelines.The faceted sawtooth modulation provides interest and dynamism as well as establishing a distinctive building language that defines the place, <i>D-3 Provide elements that define the place</i>.The singular design move of a continuous faceted façade along the edge of Dexter Ave allows for a well-proportioned and unified building, <i>B-4 Downtown Design Guidelines</i>

Table A for 23.49.058 Modulation requirements for DOC1, DOC2, and DMC zones, except DMC 170 zone	
Elevation (in feet)	Maximum length of unmodulated facade within 15 feet of street lot line (in feet)
0 to 85	No limit
Greater than 85, up to 160	155
Greater than 160, up to 240	125
Greater than 240, up to 500	100
Above 500	80
Modulation requirements for DMC 170 zone	
0 to 60	No limit
Above 60	125

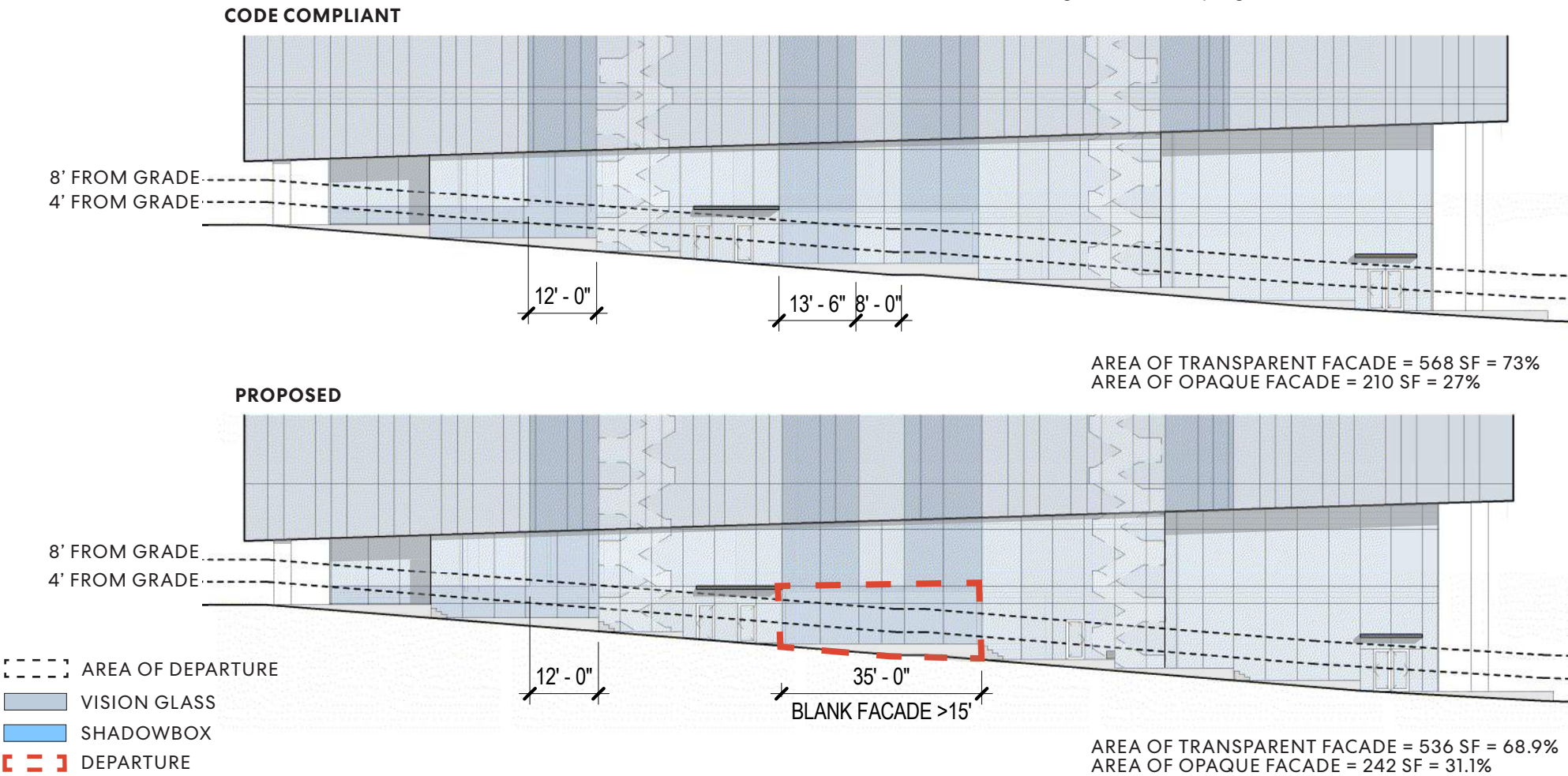
----- Elevation Reference 15' Setback Departure

5.0 | DEPARTURES

GREEN STREET FACADE TRANSPARENCY REQUIREMENTS

Code Citation & Requirement (Excerpt)	Proposed Departures	Rationale
<p>23.49.056 - Downtown Office Core 1 (DOC1), Downtown Office Core 2 (DOC2), and Downtown Mixed Commercial (DMC) street facade, landscaping, and street setback requirements</p> <p>D. Blank facade limits</p> <p>1. General Provisions</p> <p>a. Facade transparency requirements apply to the area of the facade between 2 feet and 8 feet above the sidewalk, except that if the slope along the street frontage of the facade exceeds 7.5 percent, the transparency requirements apply to the area of the facade between 4 feet and 8 feet above sidewalk grade.</p> <p>b. Any portion of a facade that is not transparent shall be considered to be a blank facade.</p> <p>2. Blank facade limits for Class I pedestrian streets and designated green streets</p> <p>a. Blank facades shall be no more than 15 feet wide except segments with garage doors may exceed a width of 15 feet and may be as wide as the driveway plus 5 feet. Blank facade segment width may be increased to 30 feet if the Director in a Type I decision determines that the facade segment is enhanced by features with visual interest such as architectural detailing, artwork, landscaping, or similar features.</p> <p>b. Any blank segments of the facade shall be separated by transparent areas at least 2 feet wide.</p> <p>c. The total width of all blank facade segments, including garage doors, shall not exceed 40 percent of the street-facing facade of the structure on each street frontage, or 50 percent if the slope of the street frontage of the facade exceeds 7.5 percent.</p>	<ul style="list-style-type: none">Allow for blank facade along Bell St in excess of 15 feet	<ul style="list-style-type: none">Bell St, a 'Green Street', is an area of quiet respite along the south face of the building. With the generous 15' setback, additional 12' of public sidewalk and protected bike lane there is ample room for the creation of seating areas and rain garden plantings. These amenities offer the passerby a place of rest or a perch to eat a takeaway lunch and watch the activity of the green street. The landscape and pedestrian amenities along this edge answers Downtown Design Guidelines <i>C-1 Promote pedestrian interaction</i> and <i>B-1 Respond to the neighborhood context</i>. The design strategy complements and reflects the existing condition on the opposite side of the street and is in accordance with the Green Street standards.The facade along Bell Street provides both a neutral backdrop against which seating areas and rain garden plantings can flourish undisturbed but also elements of movement and activity with the fully glazed stair towers and easy entry access to the bike parking and locker rooms. Locating quiet seating areas in front of opaque portions of the façade provide a place for people to relax undisturbed. The transparent façade at areas of movement or entry along with rain garden plantings provide interest for the passerby. The combination of transparency at areas of movement and opacity at areas for seating and plantings respond to Design Guidelines <i>D-1 Provide inviting and useable open space</i>, and <i>D-2 Enhance the building with landscaping</i>.

LANDSCAPE PRECEDENT IMAGES:



5.0 | DEPARTURES
BELL STREET EXPERIENCE



5.0 | DEPARTURES
8TH AVE EXPERIENCE



5.0 | DEPARTURES
BELL STREET EXPERIENCE



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STREETSCAPE - 7TH AVE CYCLE TRACK EXTENSION



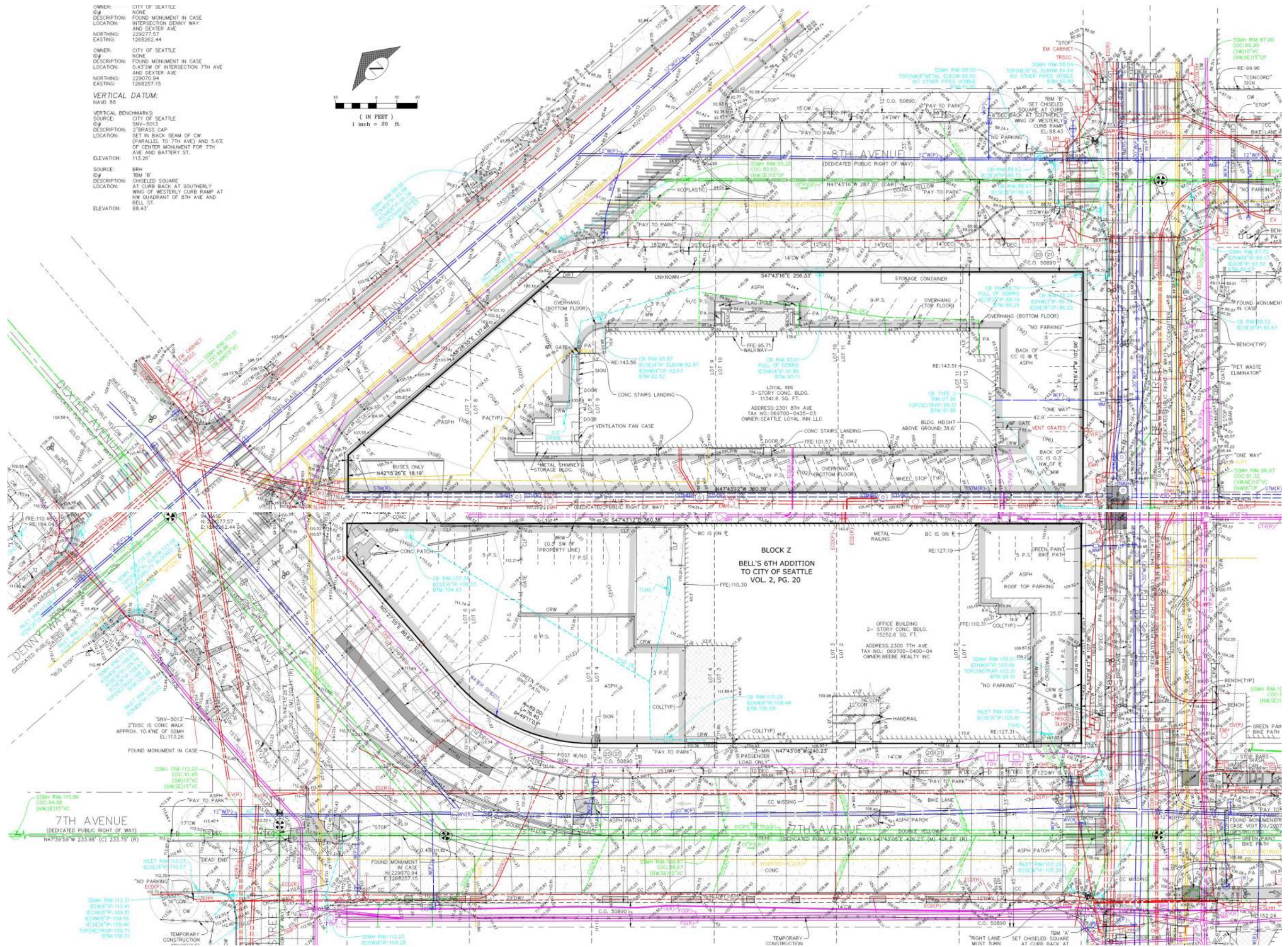
5.0 | DEPARTURES
STREETScape - DEXTER AND DENNY POCKET PARK



THANK YOU!

APPENDIX

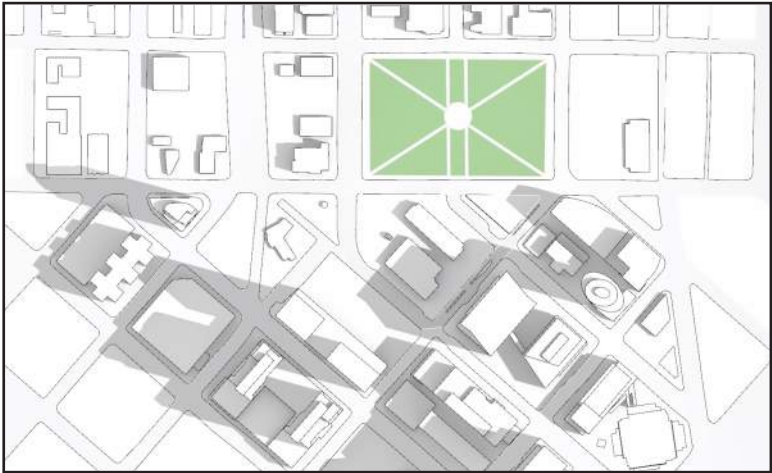
APPENDIX | PROJECT INFORMATION
SURVEY



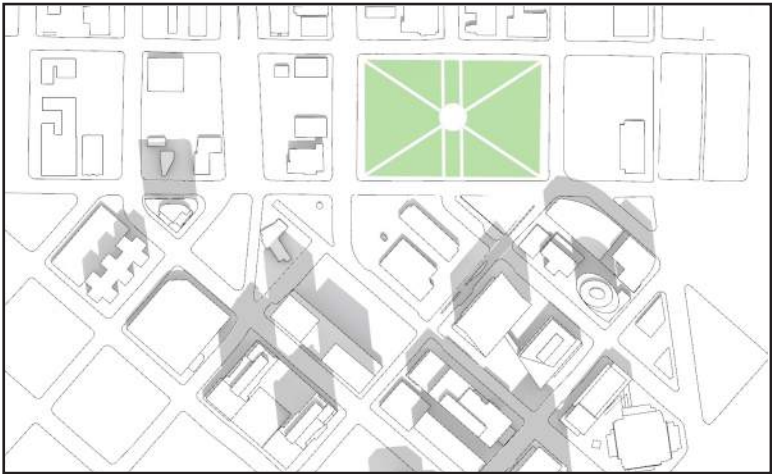
APPENDIX | PROJECT INFORMATION
SOLAR STUDIES - EXISTING SITE

SUMMER SOLSTICE

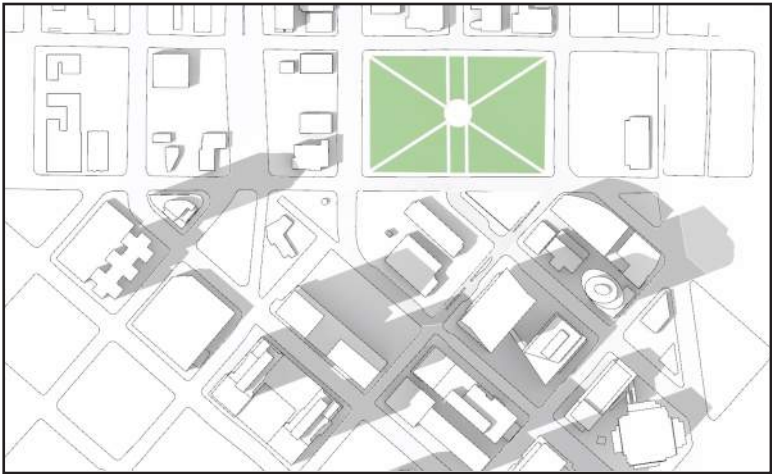
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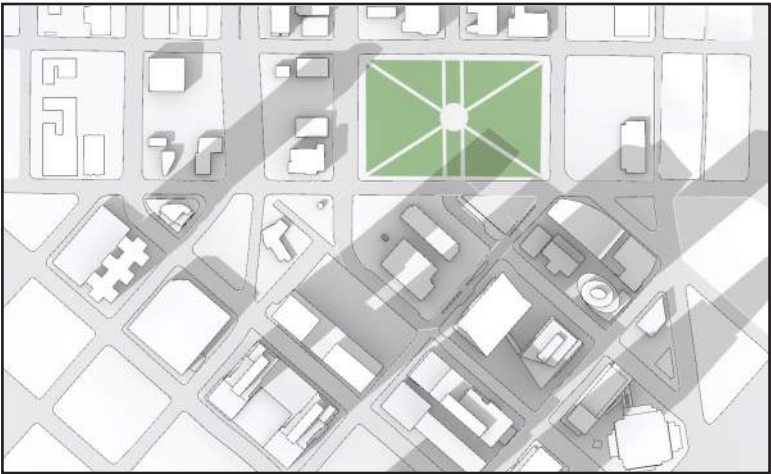
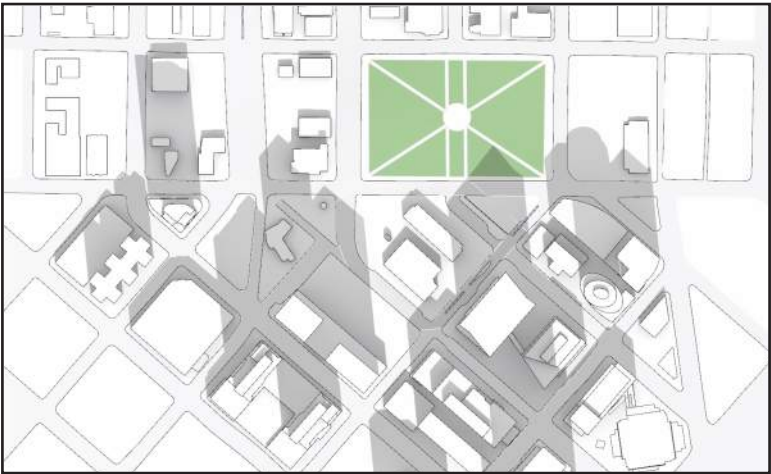
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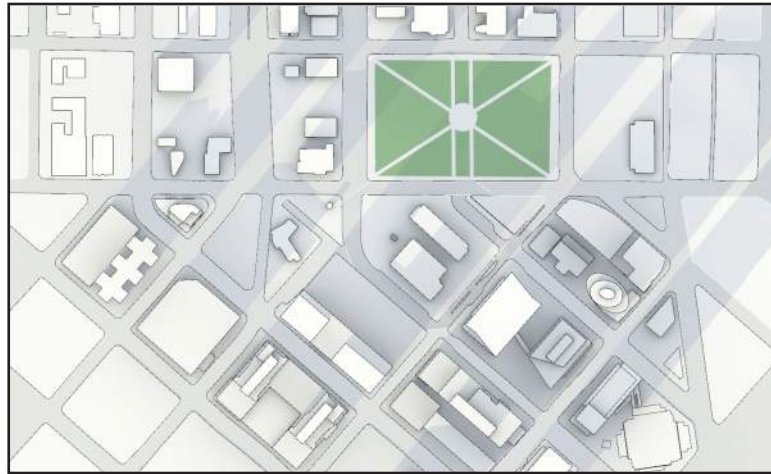
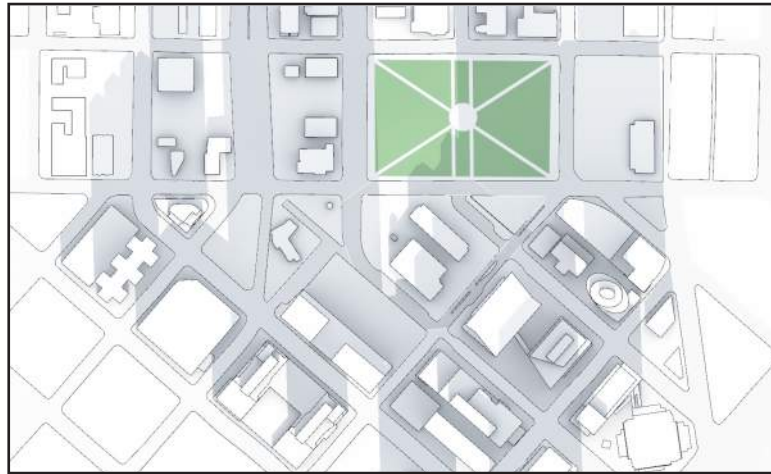
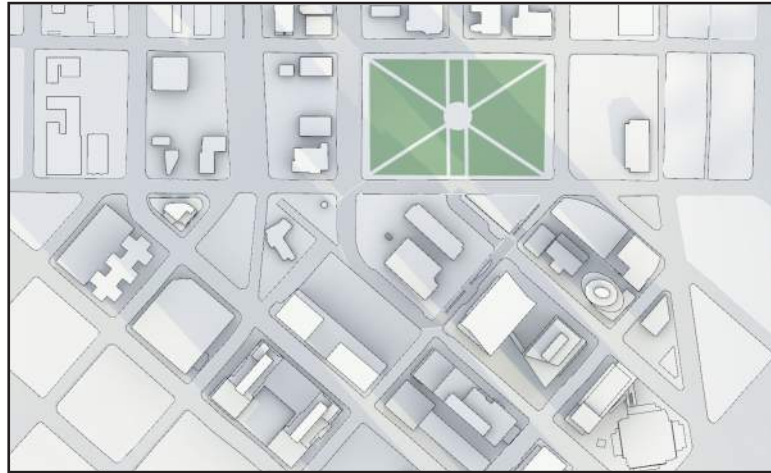
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WINTER SOLSTICE



Shadows from existing buildings

APPENDIX | PROJECT INFORMATION

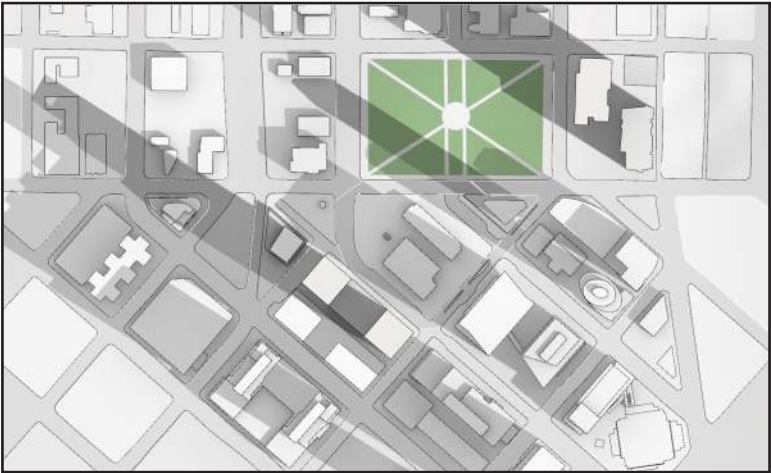
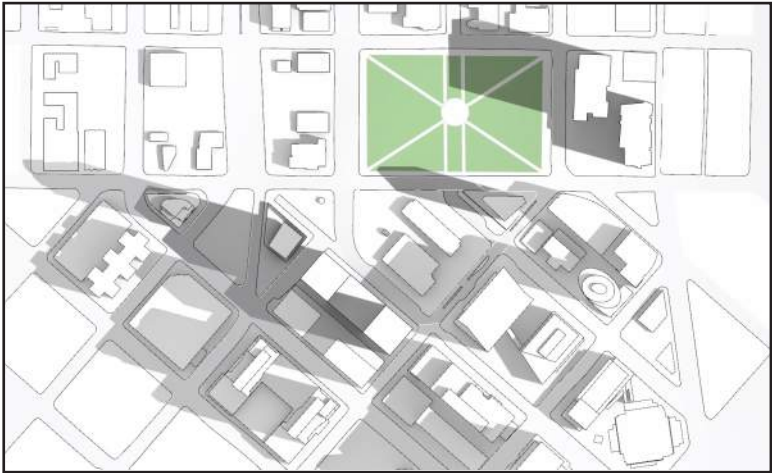
SOLAR STUDIES - EXISTING SITE WITH CURRENT PLANNED DEVELOPMENTS

SUMMER SOLSTICE

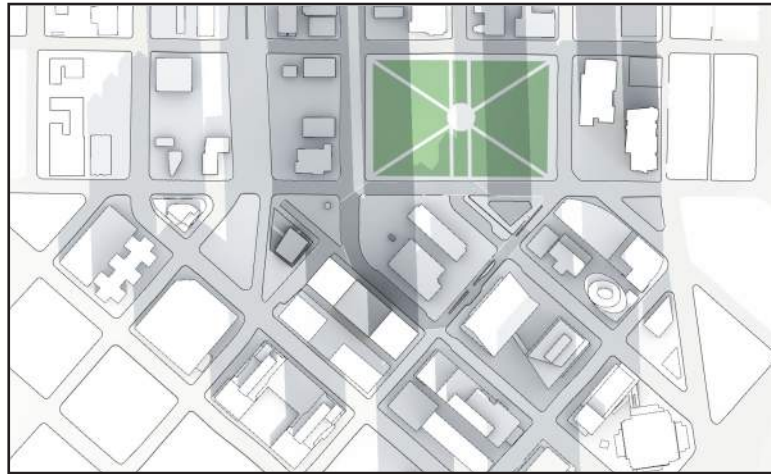
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WINTER SOLSTICE

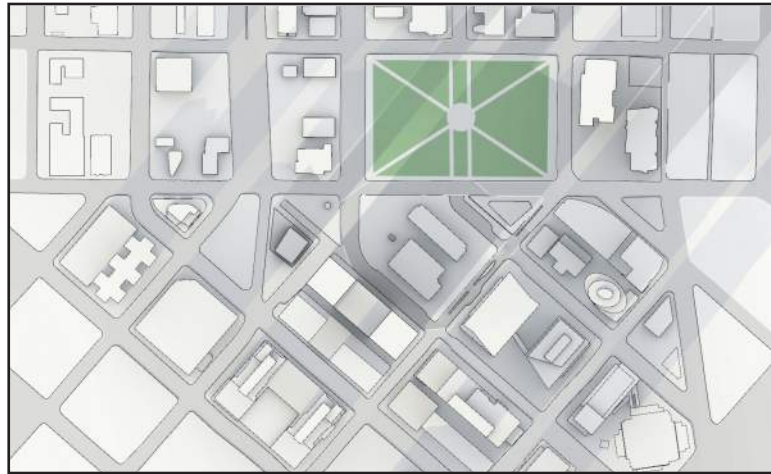
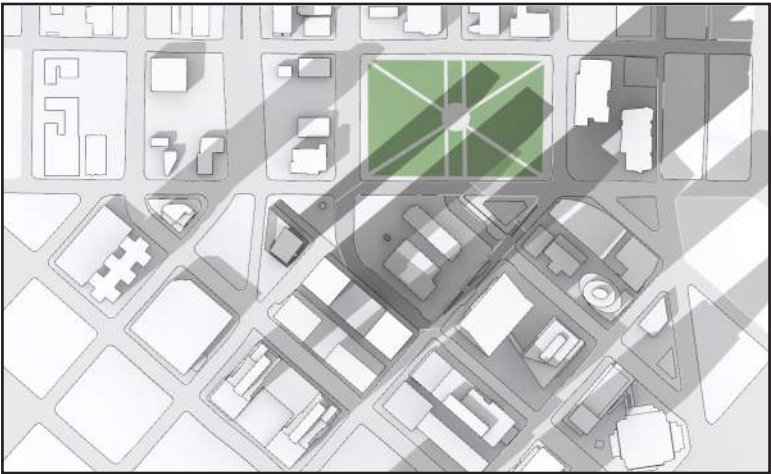
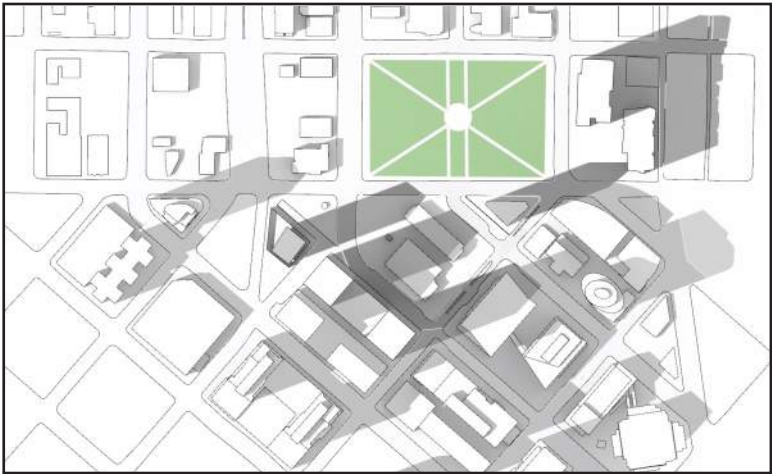
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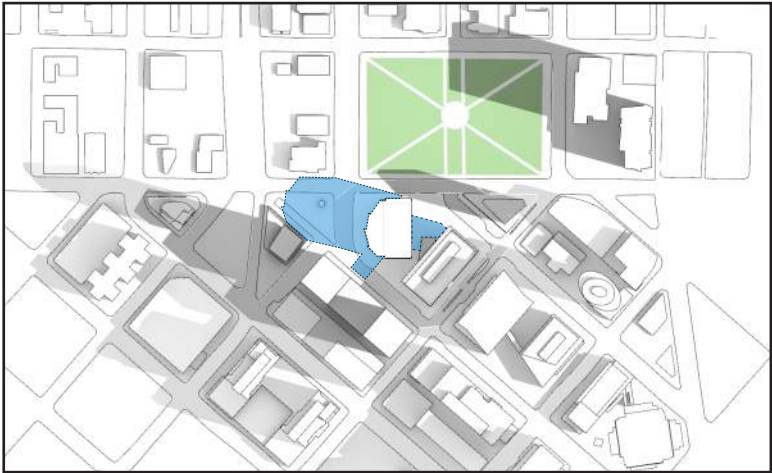


Shadows from existing buildings
Shadows from planned development

APPENDIX | PROJECT INFORMATION
SOLAR STUDIES - PROPOSED DEVELOPMENT

SUMMER SOLSTICE

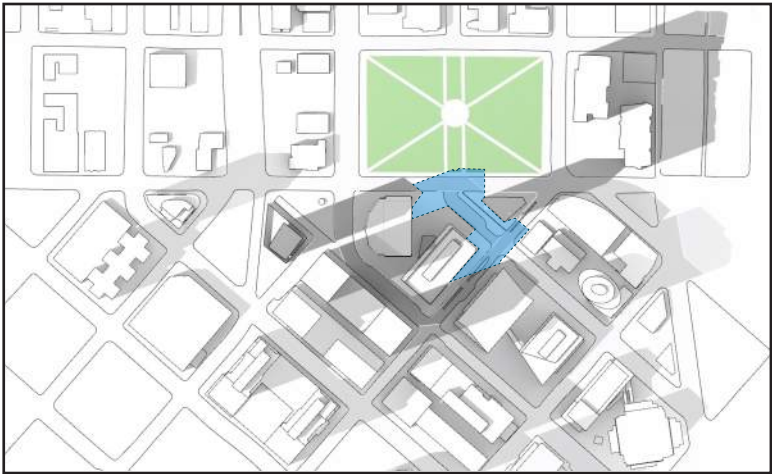
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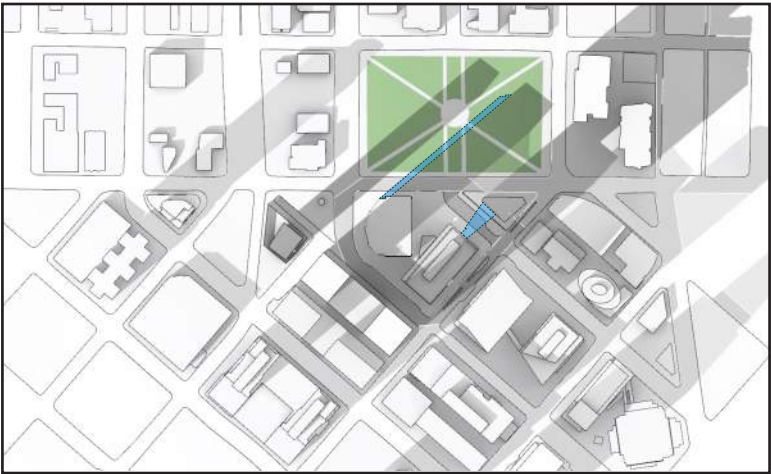
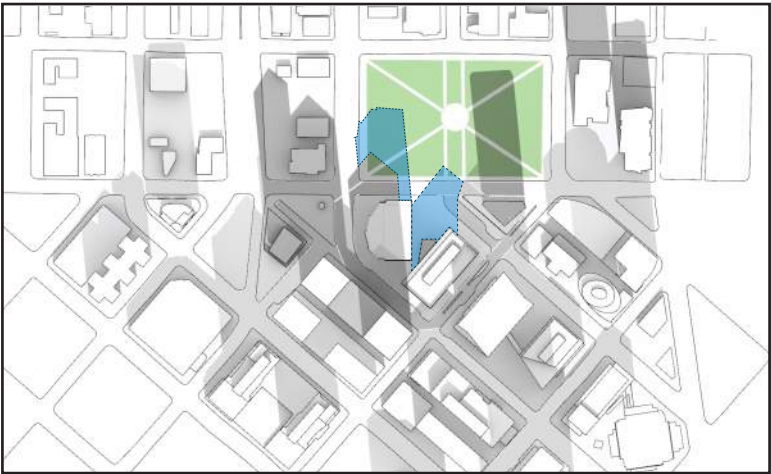
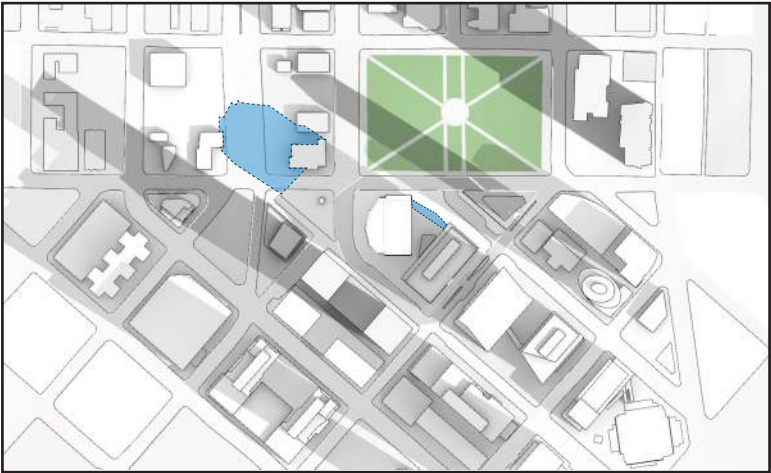
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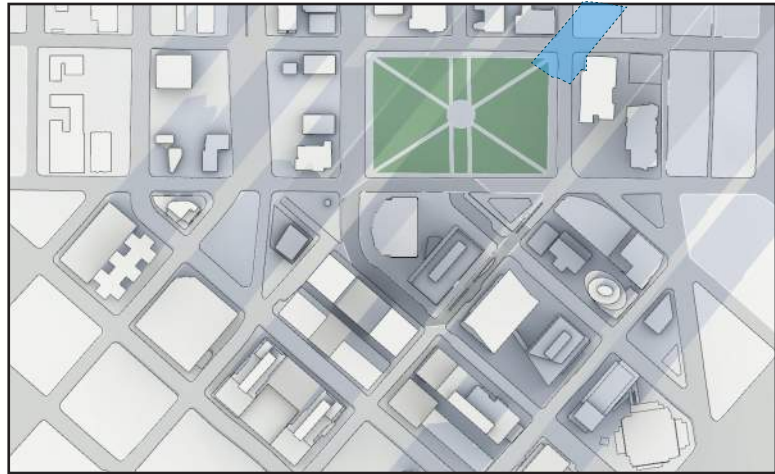
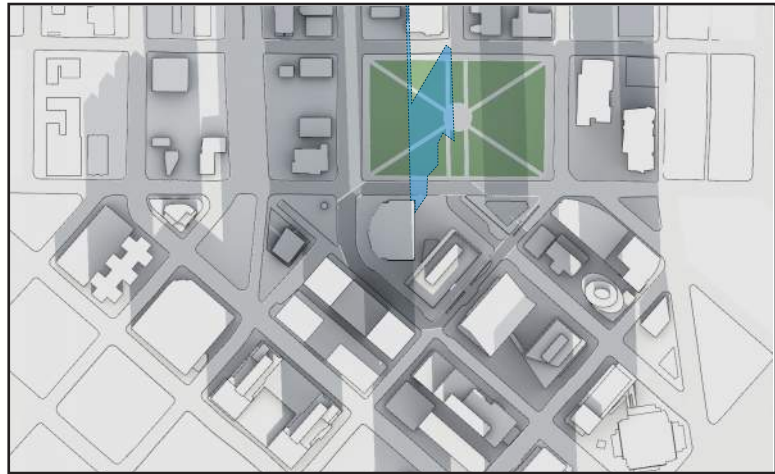
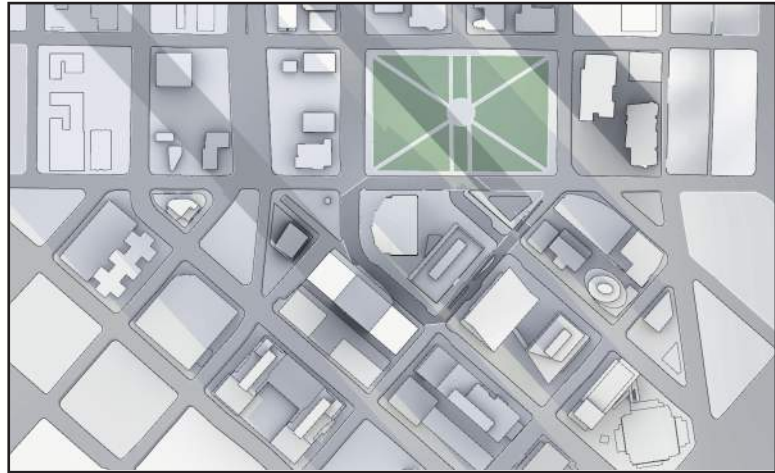
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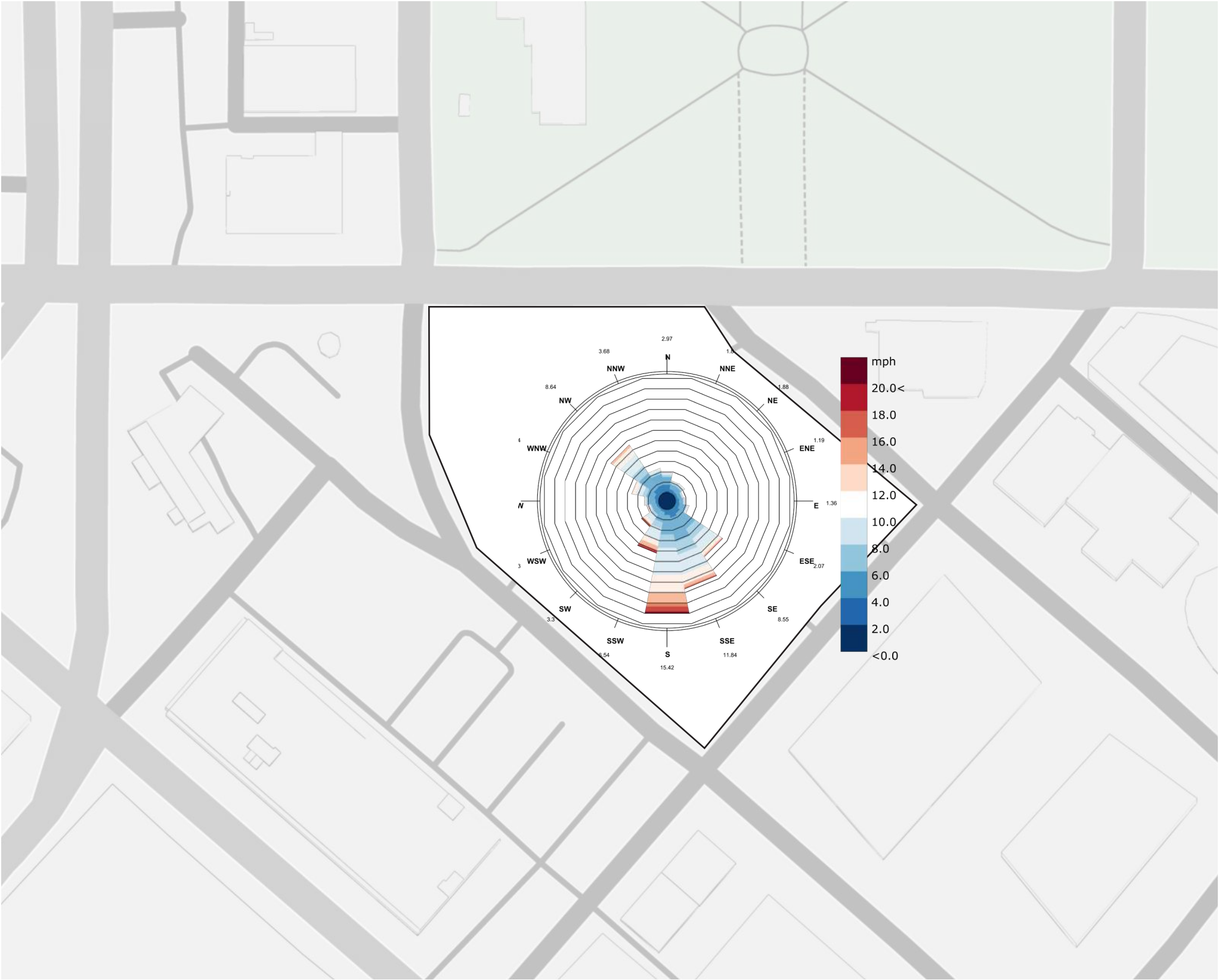
WINTER SOLSTICE



- Shadows from existing buildings
- Shadows from planned development
- Shadows added from proposed development

APPENDIX | PROJECT INFORMATION

SITE WIND ANALYSIS



Understanding the Wind Diagram

