

## 3803 SOUTH WARSAW STREET

SDCI #3037570  
EDG-2 06.17.2021

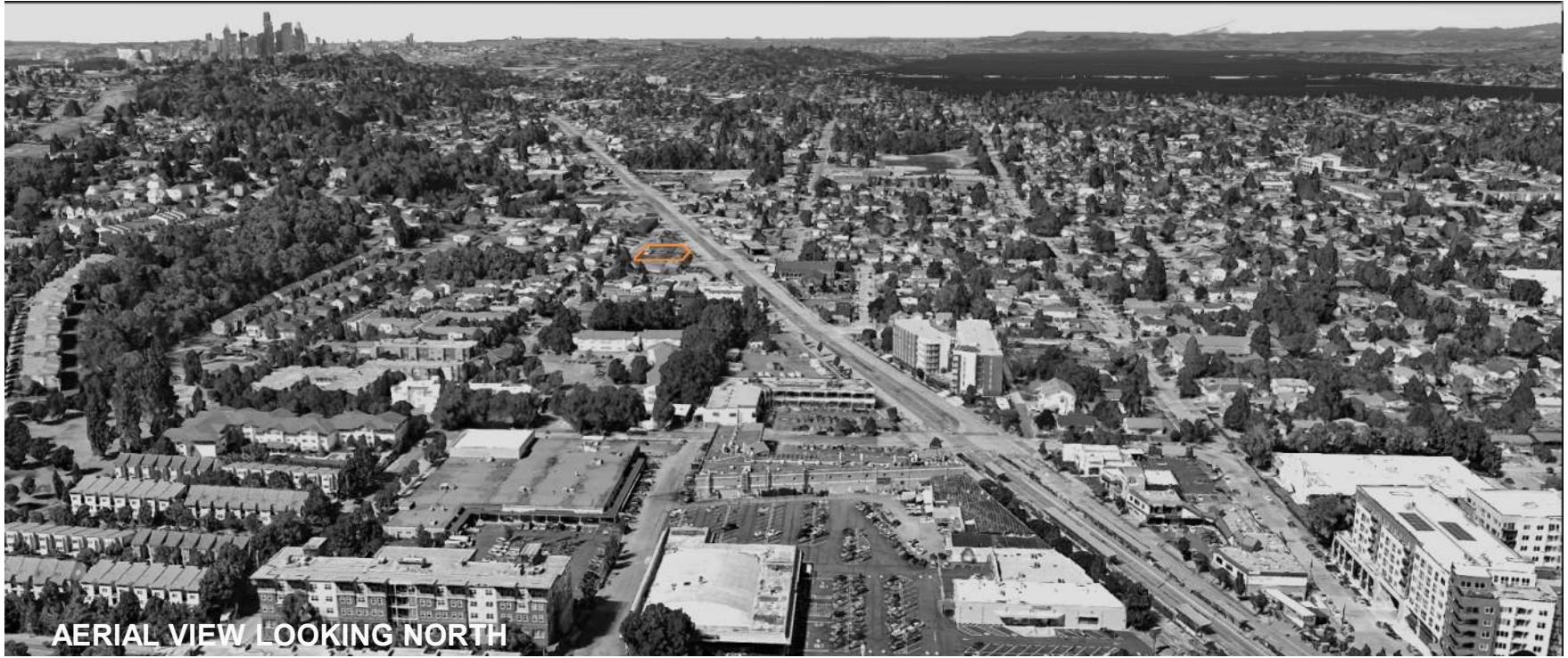
## SEATTLE, WA

PREPARED BY: KÖZ DEVELOPMENT

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# 1 DESIGN OVERVIEW

PROJECT TEAM

## REPRESENTATIVE PAST PROJECTS



EvCC Student Housing Everett, WA



Kōz on North Killingsworth Portland, OR



1554 Market Street Tacoma, WA



Kōz on Thirteenth Portland, OR



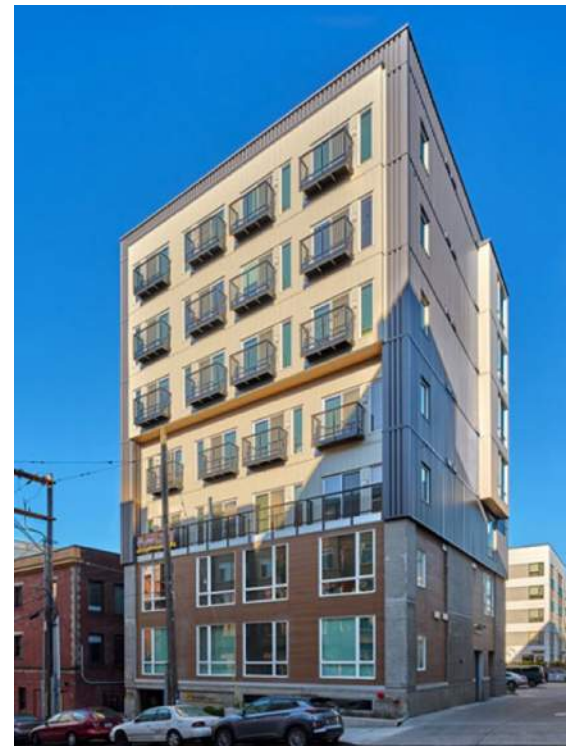
Kōz on Weaver Rd. Snohomish, WA



Kōz on Yamhill Portland, OR



Derby Slabtown Portland, OR



Derby SLU Seattle, WA

**kōz** cose|kōz  
: to make oneself cozy : be cozy  
Architecture | Development | Property Management

### DEVELOPER AND ARCHITECT:

KOZ DEVELOPMENT, LLC  
1830 BICKFORD AVENUE  
SUITE 201  
SNOHOMISH, WA 98290

Kōz Development LLC provides affordable, innovative living, in highly sought after urban neighborhoods and near universities. Our housing is accessible to students and individuals in the workforce, providing a high quality housing option in a building near work, school, and local amenities such as restaurants, coffee shops, bars, etc. Our projects are intentionally located near transit, requiring little to no dependence on an automobile for transportation. Kōz communities require limited land and are built as environmentally-friendly structures adding vibrancy to the neighborhood.



A mixed use, affordable housing project derived from the local context, with an emphasis on urban living and public transportation; a catalyst for urban revitalization.



picture credit: Carolyn Bick

#### DESIGN GOALS

##### Urban Catalysts

- Dense and affordable urban living with walkable public transportation
- Bring additional population to the area thereby reinvigorating local business
- Create vibrancy along the MLK corridor

##### Connection with the local context

- Pedestrian connection and buffer along single family (west) side of site
- Extension of the successful Othello Station area further north with similarly scaled and patterned building model to other recent transit oriented development

#### PROPERTY SUMMARY

##### Residential

- Approximately 55,000 sqft of residential space, including 131 units
- 2000 sqft of amenity space

##### Commercial

- 2000 sqft of storefront space
- 100 linear feet of 10' high storefront along MLK

##### Support

- 4000 sqft of mechanical and building support
- No onsite parking





# 1 DESIGN OVERVIEW

## LOCAL PRECEDENTS



**230 Broadway**  
Seattle, WA - SRM Development  
- Prevalent and well appointed private courtyard



**Odin Apartments**  
Seattle, WA - Runberg Architecture  
- Smaller, intimate outdoor spaces for resident connection



**Odin Apartments**  
Seattle, WA - Runberg Architecture  
- Breaking large masses into separate forms



**Axle Apartments**  
Seattle, WA - Will Austin  
- Simple, deliberate modulation and form



**Ride at RINO**  
Denver, CO - Will Austin  
- Addressing the commercial strip with public facing entrance



**Ride at RINO**  
Denver, CO - Will Austin  
- Sparing use of color and architectural ornamentation





## Koz on 13th

Portland, OR - Kōz Development

- pedestrian connection along residential community
- neighborhood scale achieved through use of materials and breaking up of facade

## 304 Puyallup Ave

Tacoma, WA - Kōz Development

- Urban affordable living
- Courtyards as a forefront for residential areas/separation from commercial spaces

## Chloe on Madison Apartments

Seattle, WA - Ankrom Moisan

- facades aligned according to street geometry
- angled entrance off main street

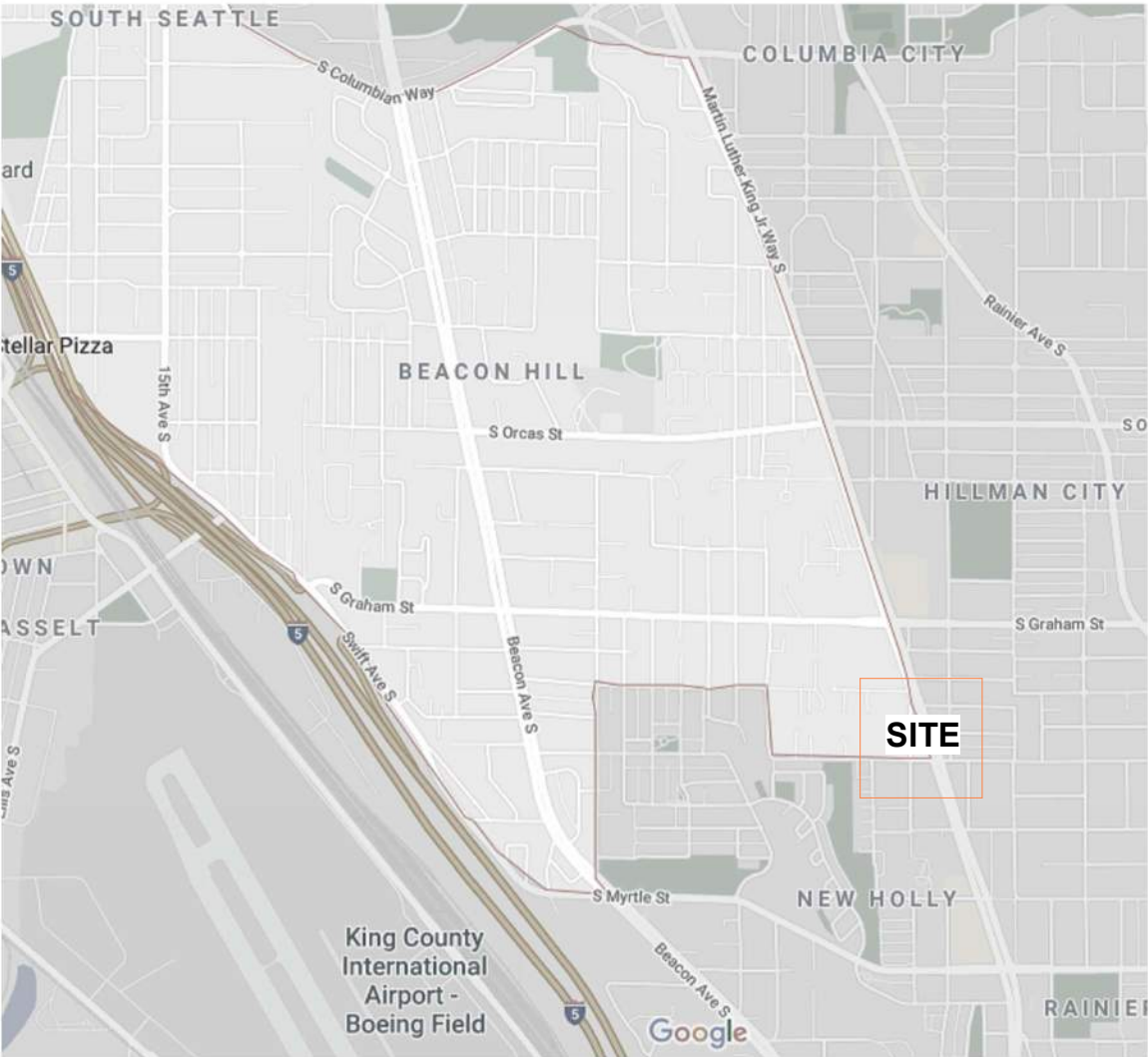


2 CONTEXT & SITE ANALYSIS

CONTEXT AREA



-  BUS STOP
-  LIGHT RAIL STATION
-  MAIN ACCESS CORRIDOR
-  BIKE FRIENDLY ROADS
-  DEDICATED BIKE PATH
-  WALKABILITY RADIUS



Location and Vicinity

The site consists of 4 parcels totaling 22,101 sqft. It is located at the lower end of the Beacon Hill neighborhood, at the edge of New Holly, Hillman City and Rainier Valley Neighborhoods.

The larger site area is classified as a Hub Urban Village and is an area quickly developing due to easy access to mass transit.



## 2 CONTEXT & SITE ANALYSIS

### ZONING ANALYSIS

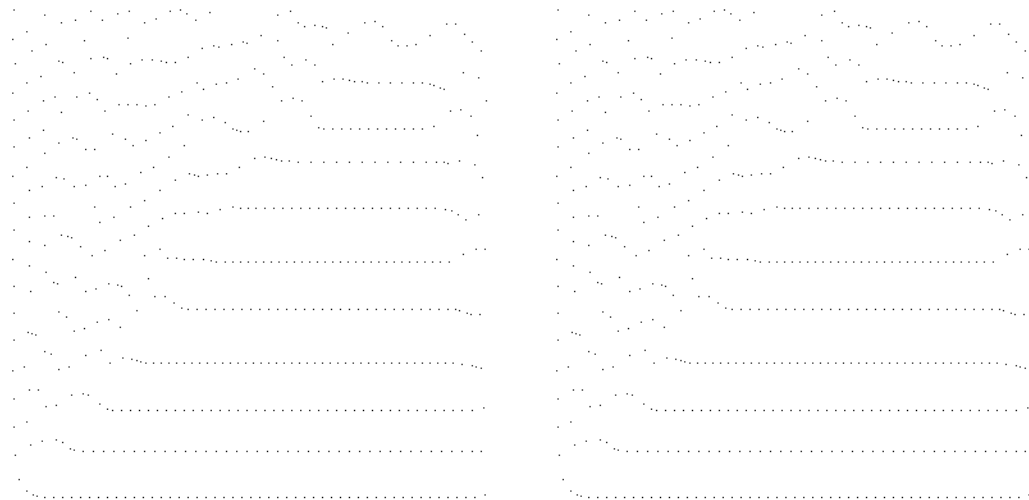
ZONING CLASSIFICATION  
NC2-40

#### Neighborhood Overlay (per SDCI GIS maps)

1. Mandatory Housing Affordability Zone
2. Opportunity Zone
3. South-east Seattle Reinvestment Area
4. Hub Urban Village

#### Topography

Site Survey shows a difference of about 4' across the site.  
Access to the site from the northeast (corner of MLK and Warsaw) is about two and a half feet lower than access from the west (38th Ave)



Seattle Zoning Code Summary

Address3803 S Warsaw Street, Seattle WA 98118

Parcel Number333300-1115, 333300-1126, 333300-1140, 333300-1150

Lot Area22,101 SF or 0.50 acres

ZoningNeighborhood Commercial 2 / NC2-55 (M)

OverlaySoutheast Seattle Reinvestment

Permitted UsesAll uses proposed permitted outright

Street Level Development Standards 23.47A.005	<div>-Blank segments of the street-facing facade between 2 feet and 8 feet above the sidewalk may not exceed 20 feet in width.</div> <div>-The total of all blank facade segments may not exceed 40% of the width of the facade of the structure along the street.</div> <div>-Street level, street-facing facades shall be located within 10 feet of the street lot line, unless wider sidewalks, plazas, or other approved landscaped or open spaces are provided.</div> <div>-Non-residential street level requirements transparency: a 60% of the street facing between 2 feet and 8 feet above the sidewalk shall be transparent.</div> <div>-Height provisions: Non-residential uses at street level shall have a floor-to-floor height of at least 13 feet.</div> <div>-Additional provisions:</div> <div>-Overhead weather protection are required along at least 60% of the street frontage of a structure on a principal pedestrian street. Covered area shall have a min. width of 6 ft. unless there is a conflict.</div>	<div>The proposed design of the Martin Luther King Jr. Way facade will have storefront glazing at street level exceeding minimum requirements of blank facade and transparency provisions.</div> <div>The proposed design of the 38th Ave. S. will have residential living units meeting or exceeding transparency percentage.</div> <div>The site is not located along a principal pedestrian street.</div>
Structure Height 23.47A.012	<div>-Max. Allowable Height: 55 feet.</div> <div>-Open railings, planters, skylights, parapets and firewalls permitted up to 4 feet above height limit.</div> <div>-Roof top decks and other similar features may exceed the max. height by up 2 feet.</div> <div>-Solar collectors may extend 4 feet above height limit and rooftop features such as mechanical equipment, minor communication utilities, stair and elevator penthouse my extend up to 16 feet.</div>	<div>The proposed building will not exceed 55 feet in height and will provide 5 stories.</div>
Floor Area Ratio 23.47A.013	<div>-FAR of 3.75 per Table A</div>	<div>FAR for 22,101 SF is 82,878.75 SF</div>
Setback requirements 23.47A.014	<div>-Rooftop features are not allowed in setbacks, except that for upper-level setbacks: open railings and parapets. Parapets may extend up to 2 feet above the height at which the setback begins.</div> <div>-Setback is required for lots abutting or across the alley from residential zones. The required setback forms a triangular area, where two sides of the triangle extend along the street lot line and side lot line 15 feet from the intersection of the residentially zoned lots.</div>	<div>The site is not abutting or across the alley from a residential zone so no additional setback or upper level setback is required.</div>
Amenity Area 23.47A.024	<div>-Amenity areas are required in an amount equal to 5 percent of the total gross floor area in residential use. B.Required amenity areas shall meet the following standards, as applicable: 1. All residents shall have access to at least one common or private amenity area. 2.Amenity areas shall not be enclosed. 4.Common amenity areas shall have a minimum horizontal dimension of 10 feet, and no common amenity area shall be less than 250 square feet in size. 5.Private balconies and decks shall have a minimum area of 60 square feet, and no horizontal dimension shall be less than 6 feet.</div>	<div>Amenity area will be provided exceeding 5% of total gross floor area in residential use.</div>
Landscaping 23.47A.016	<div>-A <i>Landscaping requirements.</i> Item 2: Landscaping that achieves a Green Factor score of 0.3 or greater.</div> <div>-B <i>Street tree requirements.</i> Item 1. Existing street trees shall be retained unless the Director of Transportation approves their removal. The Director, in consultation with the Director of Transportation, will determine the number, type and placement of street trees to be provided: 3.When an existing structure is proposed to be expanded by more than 1,000 square feet, one street tree is required for each 500 square feet over the first 1,000 square feet of additional structure, up to the maximum number of trees that would be required for new construction.</div>	<div>Landscape will be provided to meet or exceed landscaping requirements. Street trees will be planted per SDOT requirements. Walkable surfaces, planters and other features will be integrated with landscape design.</div>
Light and Glare 23.47A.022	<div>-A. Exterior lighting must be shielded and directed away from adjacent uses. D.Height. 1.Exterior lighting on poles is permitted up to a maximum height of thirty (30) feet from finished grade. In zones with a forty (40) foot or greater height limit, exterior lighting on poles is permitted up to a height of forty (40) feet from finished grade.</div>	<div>Proposed building will comply with light and glare standards.</div>
Parking 23.54.015	<div>Table B, II.L. All residential uses within urban centers or within the Station Area Overlay District. No minimum requirement.</div>	<div>No onsite parking proposed.</div>
Solid Waste & Recyclable Materials Storage and Access 23.54.040	<div>-Shared storage spaced for solid waste containers per Table A: More than 100 dwelling units, 575 SF plus 4 square feet for each additional unit above 100.</div> <div>-*For developments with more than 100 dwelling units, the required min. area for storage space may be reduced to 15 percent if the area provided as storage space has a min. horizontal dimension of 20 feet.</div>	<div>Approximately 125 dwelling units proposed: 575 SF for 100 units; 4 SF for ea. additional unit is 4*25=100 SF Total SF required for solid waste and recyclable materials storage and access is 675 SF*</div>



2

CONTEXT & SITE ANALYSIS

URBAN CONTEXT ANALYSIS - 9 BLOCK AXON



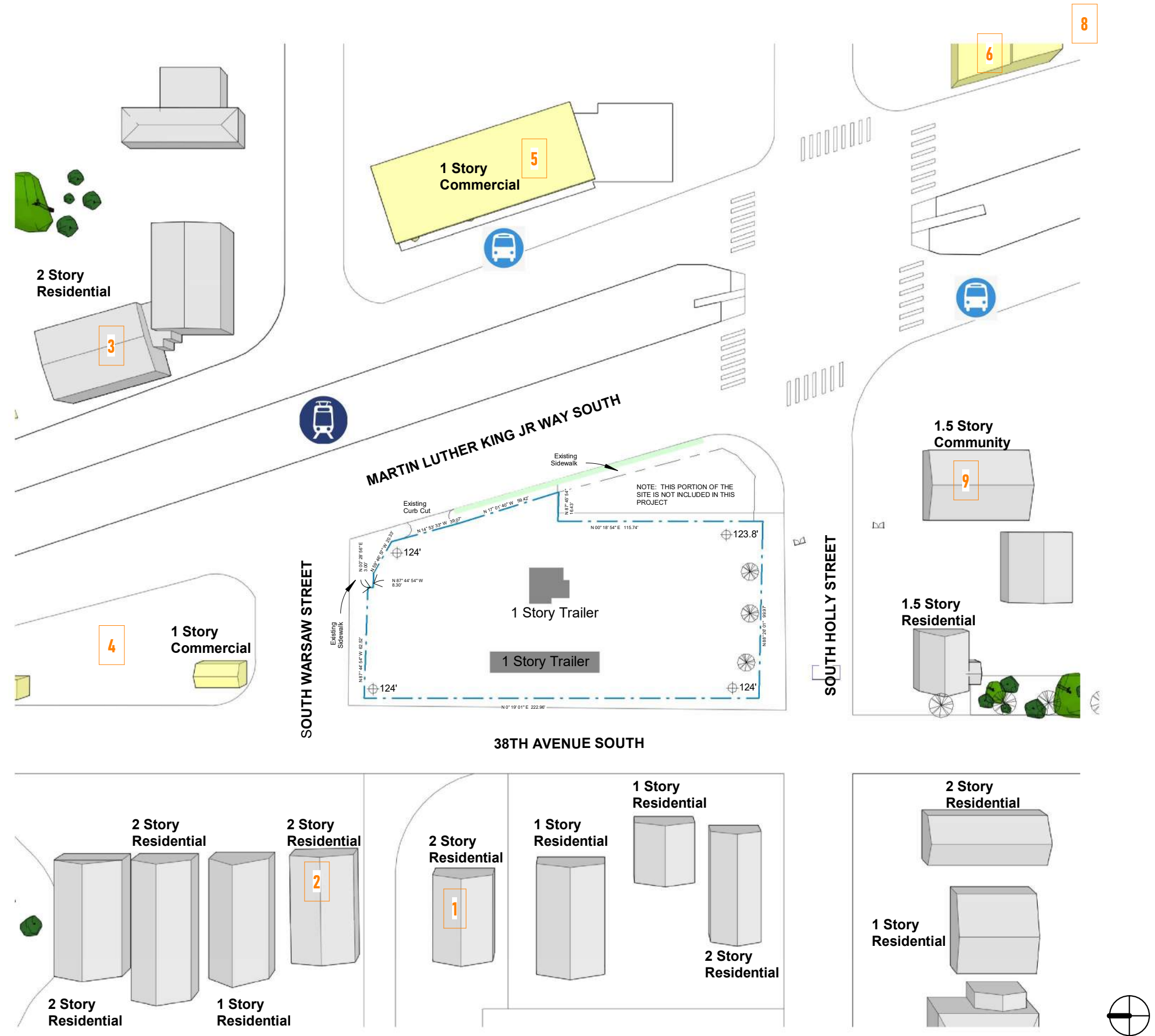
2 CONTEXT & SITE ANALYSIS

SITE PLAN

3803 SOUTH WARSAW STREET  
Seattle, WA

The site consists of eight lots. It is bound by roads on all sides; MLK Jr Way on the east, Warsaw street to the north, 38th Ave S to the west and S Holly St. to the south. The Site is sandwiched between the commercial corridor along MLK way which is heavily trafficked, from the light rail stopping at the nearby Othello Station to the multiple bus stops across the street from the site.

Currently the site is vacant, with a small dilapidated structure and a trailer currently occupying a small portion of it.





## ARCHITECTURAL CHARACTER

**Residential-** The area has mostly single family residential with the exception of the two story apartments diagonally across from the site on MLK. Siding and stucco are the most commonly used materials but these buildings have no overreaching architectural character. The proposed project will have the opportunity to be a catalyst for the area.

**Commercial/Office -** These buildings do not offer much in terms of architectural character for this area.

**Public and Community -** Othello Station has some modern elements and the Religious /Community structures have some cultural details.



1. Two story home with an attached garage and driveway facing 38th St.



2. Two story home with an attached garage and driveway facing 38th St.



3. Two-Story Apartments across the street from the site on MLK Jr way.



4. Single story car dealership with parking lot north of the site



5. Gas Station on MLK Jr Way S



6. Dental office on MLK



7. Othello Station



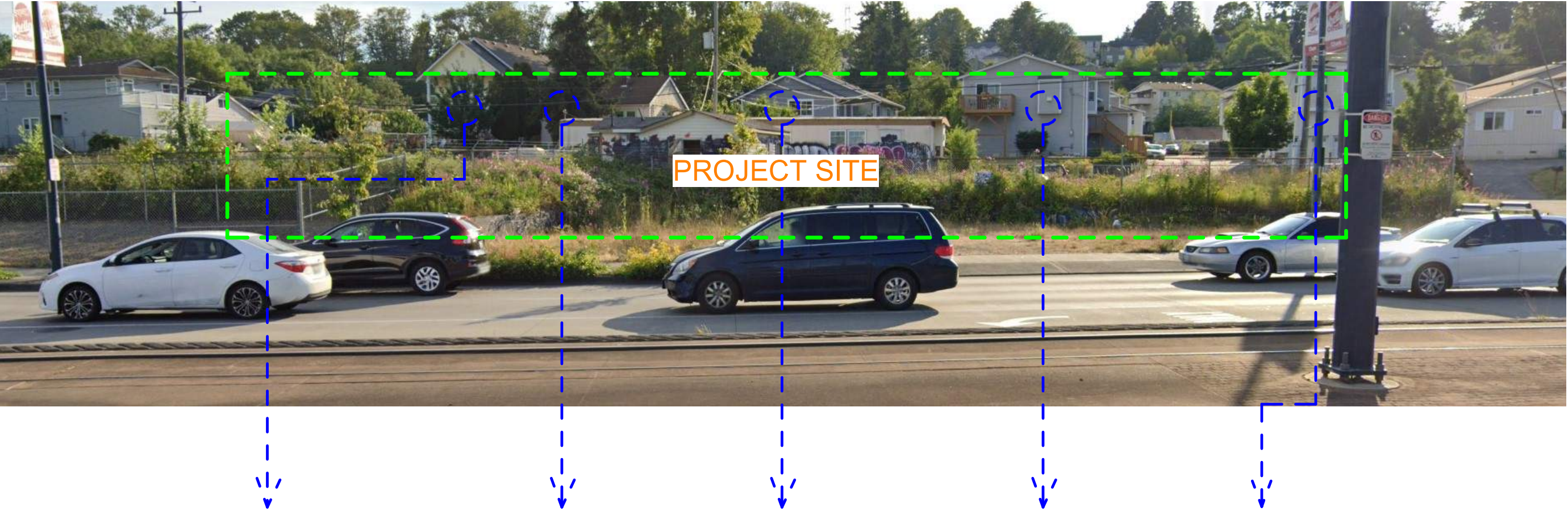
8. Church on MLK



9. Community Center to the north of the site

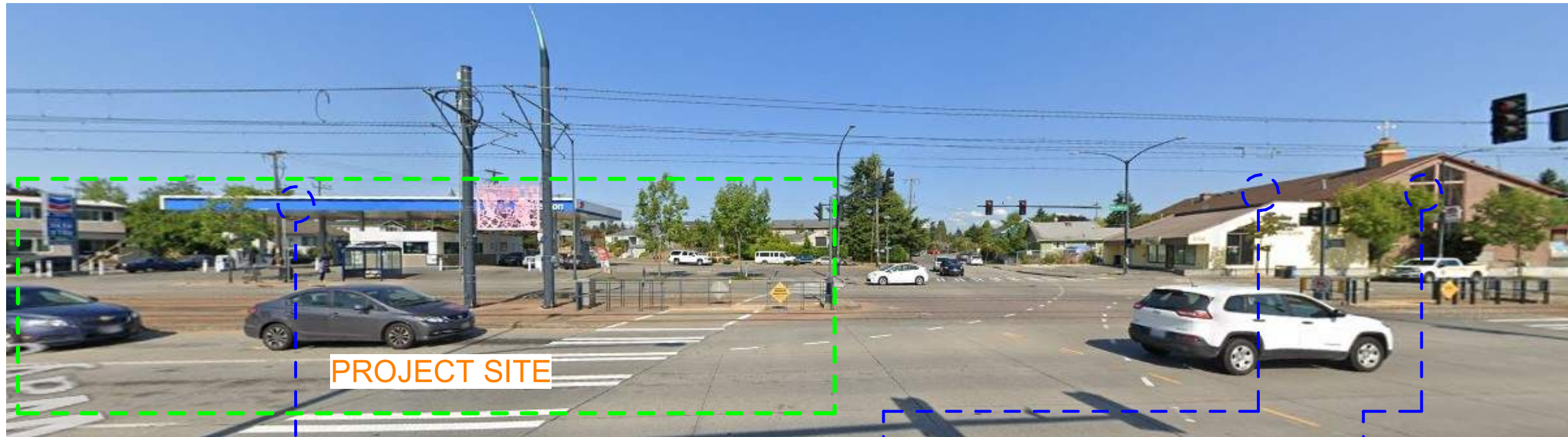


2 CONTEXT & SITE ANALYSIS  
STREET ELEVATIONS



38TH ST - Street Elevation





## MLK JR WAY - Street Elevation



## 2 CONTEXT & SITE ANALYSIS

### STREET ELEVATIONS



S WARSAW ST - Street Elevation



S HOLLY ST - Street Elevation





1. 2226 38TH AVE S



2. 2226 38TH AVE S



3. 3939 OTHELLO ST



4. 7343 MLK JR WAY S



5. 7350 MLK JR S



6. 7339 43RD AVE S

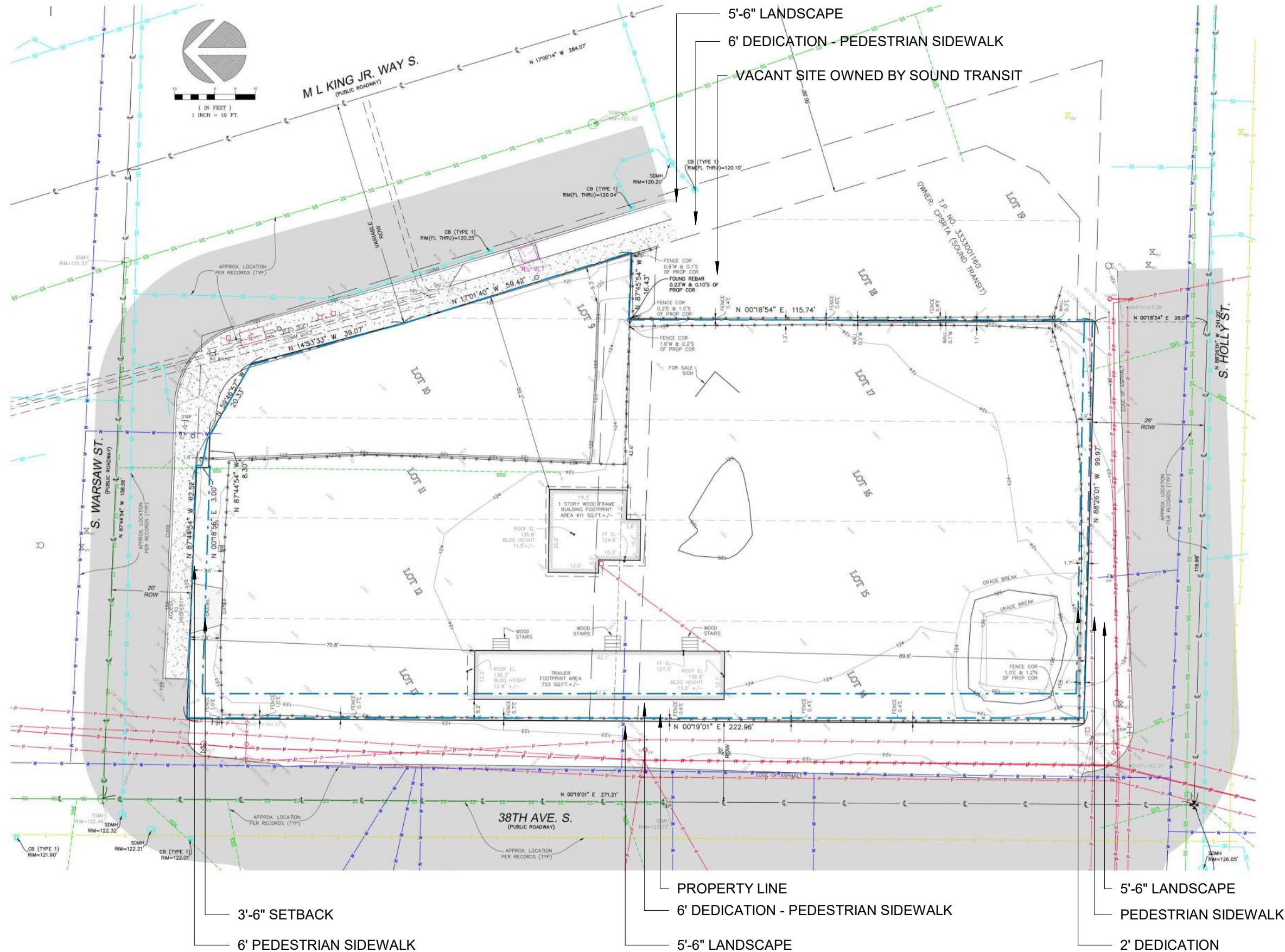
Similar Design context to be utilized:

- 1- Ground floor retail with strong visibility from main street
- 2- Panel cladding on the upper residential floors
- 3- Large windows

4- Roof Decks

- 2- Facade projections and the use of secondary architectural design elements





## PROJECT SITE

Located on MLK Jr. Way S and S. Warsaw St.  
S. Holly St. South of the site and 38th Ave. S West of the site.  
Site area = 21,101 SF, rectangular shaped lots measuring approx. 110' x 25'  
2 existing wood frame structures to be demolished.

## ALLOWABLE BUILDING AREA

NC2-55 (M) 3.75 FAR = 82,878.75 SF

## TOPOGRAPHY

Less than 1 percent.

## LEGAL DESCRIPTION

LOTS 9 THROUGH 17, BLOCK 6, HILLMAN CITY ADDITION TO THE CITY OF SEATTLE, DIVISION NO. 6, ACCORDING TO THE PLAT THEREOF RECORDED IN VOLUME 11 OF PLATS, PAGE 23, IN KING COUNTY, WASHINGTON;  
TOGETHER WITH THAT PORTION OF VACATED ALLEY ADJOINING, WHICH UPON VACATON, ATTACHED TO SAID PREMISES BY OPERATION OF LAW;  
EXCEPT THAT PORTION THEREOF LYING WITHIN EMPIRE WAY SOUTH; AND EXCEPT THAT PORTION CONVEYED TO THE CENTRAL PUGET SOUND REGIONAL TRANSIT AUTHORITY BY DEED RECORDED NUMBER UNDER RECORDING NUMBER 20041222000730.

## PARCEL NUMBERS

333300-1115, 333300-1126, 333300-1140, 333300-1150

## TRAFFIC CIRCULATION

Site is primarily served by the principal arterial MLK Jr. Way S.



# 3 DESIGN APPROACH

EDG2 DESIGN APPROACH

## ARCHITECTURAL CONCEPT

The conceptual design is based on expressing the shift (transition) in function, form, texture, look and feel between both the commercial and residential zoning edges that shaped the project site. Utilizing the site's shape, the architectural concept is to provide a commercial space facing MLK JR, private outdoor spaces for residents facing 38th St, gathering spaces connected to the sidewalk at Holly St (a future Greenway) with respect to the neighborhood existing and future character and scale.

## BUILDING MASSING

DC2.A,C,D,E, DC4D Massing, Scale and Texture, Form and Function, Facade Composition

- Building orientation and massing on both MLK way and 38TH Avenue S will be created to provide a strong visual emphasis on urban pattern.
- Exterior facades will be composed of a variation of cladding materials, patterns and texture, and storefront glazing which will convey a sense of permanence, human scale and proportion which responses to both the commercial and residential corridors.
- Large windows will be strategically designed and placed to maximize daylight for interior spaces.
- Active uses have been positioned along the major arterial - MLK Jr. Way- including Retail, Lobby, and Mail Room to activate the street level and provide views into those spaces.

## STREET LEVEL INTERACTION

PL3,A-2, A-3, A-4, A-5 Common Entries, Individual Entries, Streetscape Compatibility, Human Activity

- Entry to residence lobby and outdoor seating share same exterior space along the building's East side accessed from MLK Way
- Shallow setbacks and no vertical grade separations along MLK way are provided where the commercial areas are proposed.
- Connection of on-site pedestrian walkways with existing public and private pedestrian infrastructure will be provided to support pedestrian connections within and outside the project along with outdoor seating, lighting, large storefront windows and year round landscaping.
- The street level glazing will maximize opportunities to engage the pedestrians and enable an active and vibrant streetfront, especially as a corner lot building.
- Overhead weather protection (canopies) will be used along the commercial use on MLK way to provide pedestrian comfort
- Setbacks with landscape areas will be provided along 38TH Avenue S to create a buffer between uses.

## WALKABILITY + COMMUNITY CONNECTIVITY

PL2, B-1, B.3 Safety and Security, Street level Transparency

- Improvements to the pedestrian environment at the street level on MLK Way, 38th Avenue S, Holly and Warsaw Streets will be provided.
- A variety of features such as planters, decks, groves of trees and street trees using a combination of monochromatic and colored hardscape as well as plantings will be created to shape open spaces and articulate a transition from private to public at both MLK way and 38th ST
- Lines of sight will be provided for a safe environment encouraging natural surveillance through strategic placement of doors and other features like pedestrian entry lighting and security lighting.
- A landscape buffer ( flower beds, variety of sidewalk patterns and lighting fixtures) will be provided between the private open courtyards and entrances and public spaces to create a symbolic transitional boundary between zones.
- Privacy and security will be provided for residents by providing a setback from the public sidewalk, a recessed entranceway and a range of exterior finishes on the facade such as planter boxes or pots and vertical modulation that articulates the location of residential entries.

## COMMERCIAL CORRIDOR



## RESIDENTIAL CORRIDOR







#### DC2.D.2 Texture

Use of distinctive material at the ground floor on all streets and differentiate between the building material and the character of the building on both MLK way and 38th ST to express commercial and residential corridors.

#### PL3. I.i. Street-level Interaction

Overhead weather protection along the sidewalk on MLK JR Way for pedestrian comfort, canopies and awnings are encouraged



#### DC2.A.2 Massing

Use canopies -Secondary architectural element- to reduce the perceived mass on MLK way. Create recesses or indentations in the building envelope.

#### DC2.D.1 Human Scale

Incorporate architectural features, elements and details into the building facade, both retail and residential entries and outdoor spaces to maximize opportunities to engage pedestrian and to enable a vibrant feel at MLK way





### 3 DESIGN APPROACH

EDG2 DESIGN APPROACH



#### PL3.A Main Residential Entries

Recessed lobby entry to provide a strong transition from commercial to residential services

#### PL3.B Residential Edges

Private residential outdoor spaces facing 38th ST to improve residential character

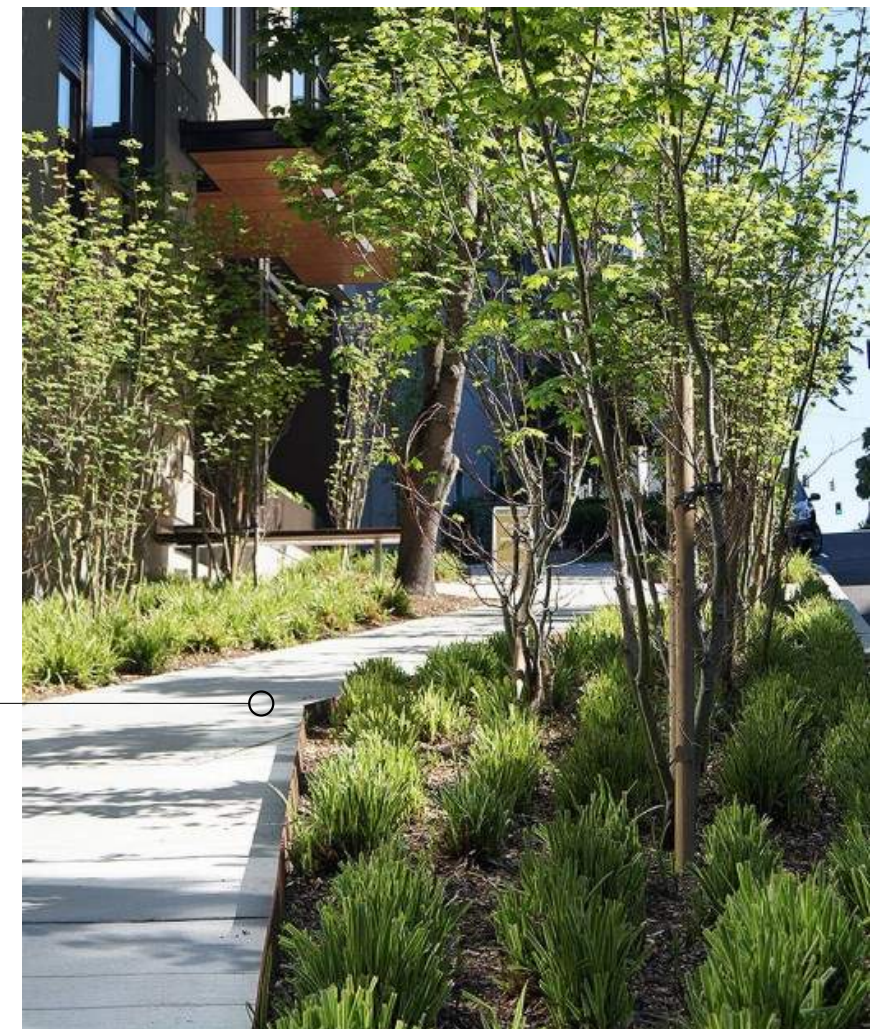


#### PL1-A-2, PL1-B Walkability, Public life

Connecting onsite open spaces to public sidewalk at Holly St and S Warsaw ST

#### DC1.C.2 Visual Impacts

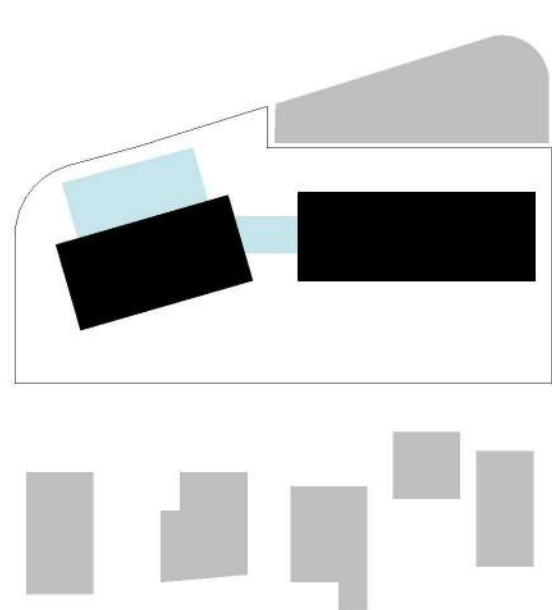
Landscape buffer near the solidwaste driveway at 38th ST





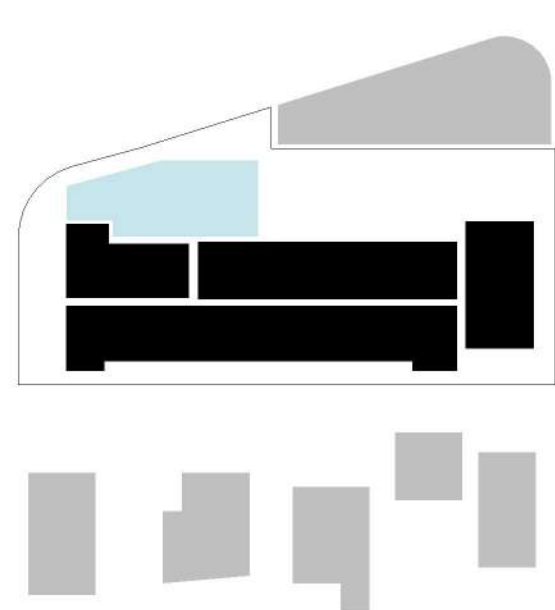
MASSING OPTIONS

The new hybrid option presented below was selected from studying both options 2 and 3 and with the feedback from the design review board. The relationship of multifamily and single family uses, alignment of streets, and engagement with the transit core were all considerations that were weighed in the development of this option.



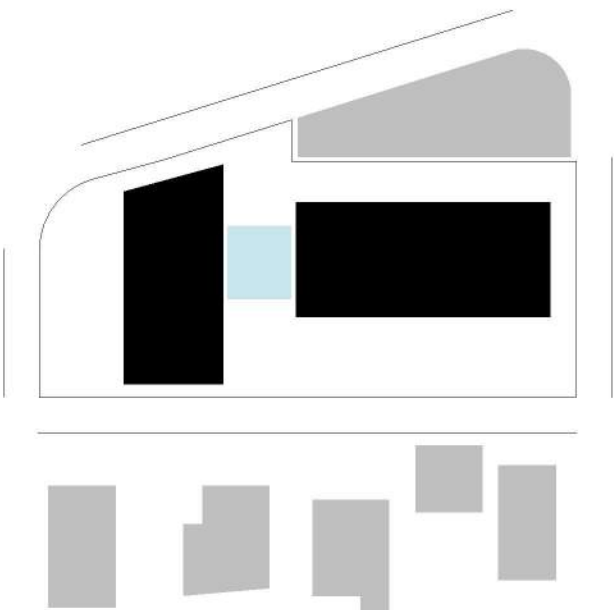
HYBRID OPTION : PARALLEL SHIFT-**PREFERRED**

An equally presented two masses which shift in orientation to connect both commercial and residential zoning areas. A commercial mass is aligned with MLK JR Way. The residential mass will be aligned with 38th ST



OPTION 1 : PARALLEL RESIDENTIAL

Primary mass parallel and more closely associated with 38th Avenue South.



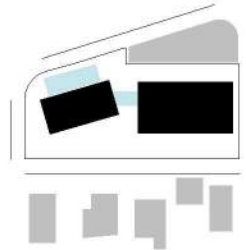
OPTION 2: TRANSITIONING MASSES

Two dominant masses, each responding to the primary axes of the site. Additional buffering space between single family and multifamily uses.



## SHIFTED PARALLEL - PREFERRED

This approach considers the benefits and impacts of aligning the primary masses of the building with the orientation of MLK WAY on one side and 38TH St on the residential side as a new hybrid option between earlier options 2 & 3 in the EDG-1



### Overview

- Approx Total area +/- 54,000 SF
- 125 Residential Units
- Commercial Space +/- 14,000 SF
- Bike Parking Stalls +/- 75

### Pros

- The massing responds to the site lot line along MLK more closely associating the project with other transit oriented developments in the area.
- The main residential entry on the northeast side of the site increases pedestrian activity along MLK.
- The massing creates two interesting yard spaces along MLK JR and 38th ST which take advantage of the southwest orientation and provides buffering next to the adjacent single family uses

### Cons

- Building orientation limits outdoor spaces serving commercial space.
- Massing creates large facades along east and west edges of the site.



NW BIRDSEYE VIEW



SE BIRDSEYE VIEW



# 4 MASSING OPTIONS -PARALLEL SHIFT

## STREET PERSPECTIVES



**A** RESIDENTIAL OUTDOOR SPACE



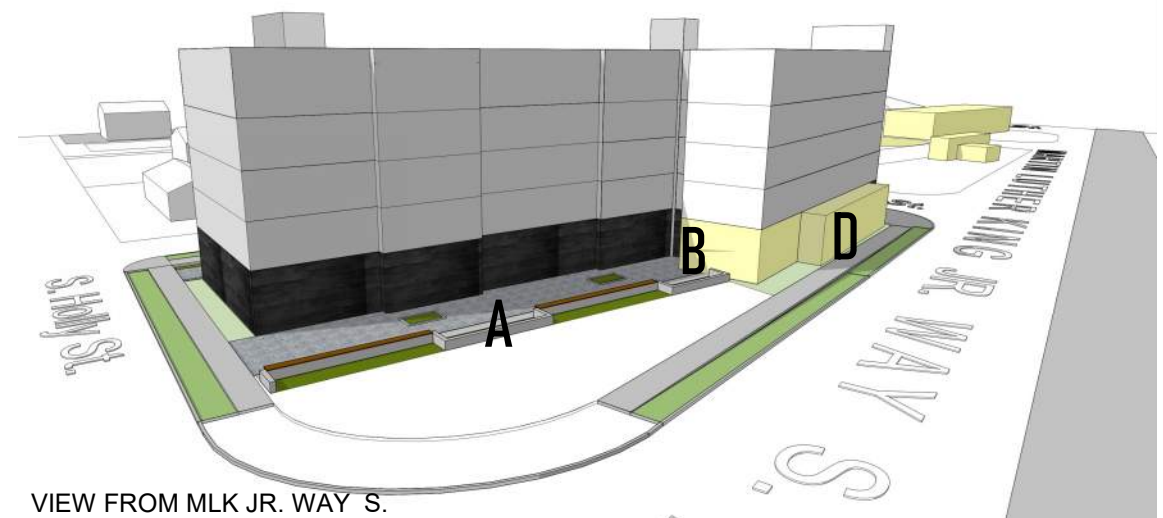
**B** LOBBY MAIN ENTRANCE



VIEW FROM 38TH AVE. S.



VIEW S. WARSAW ST.



VIEW FROM MLK JR. WAY S.



**C** RESIDENTIAL FACADE AT 38TH STREET



**D** COMMERCIAL STREET FRONTAGE



## 4

COMPOSITE SITE PLAN



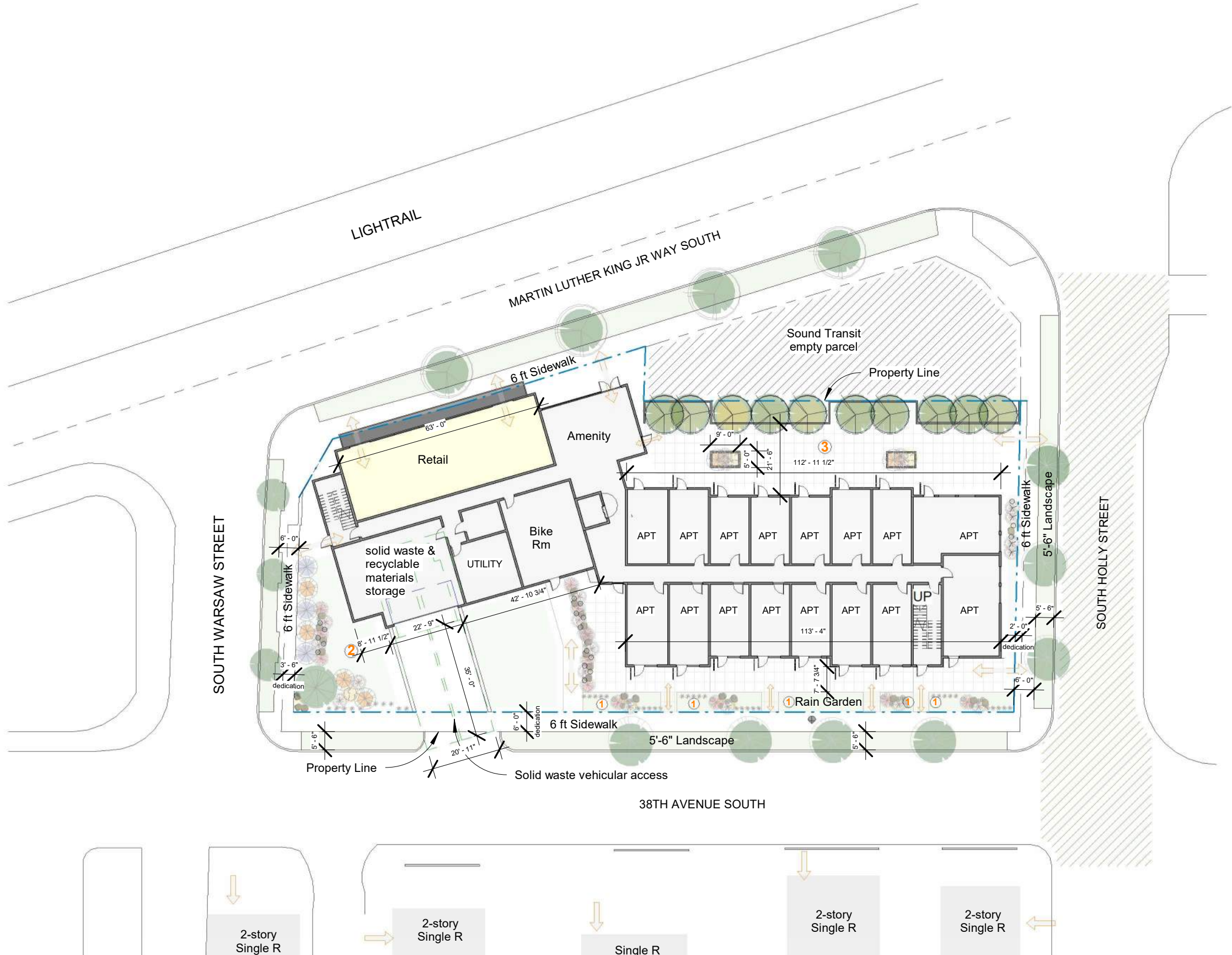
## 1. Rain Garden at residential units- 38ST



## 2. Landscape Buffer at S Warsaw ST

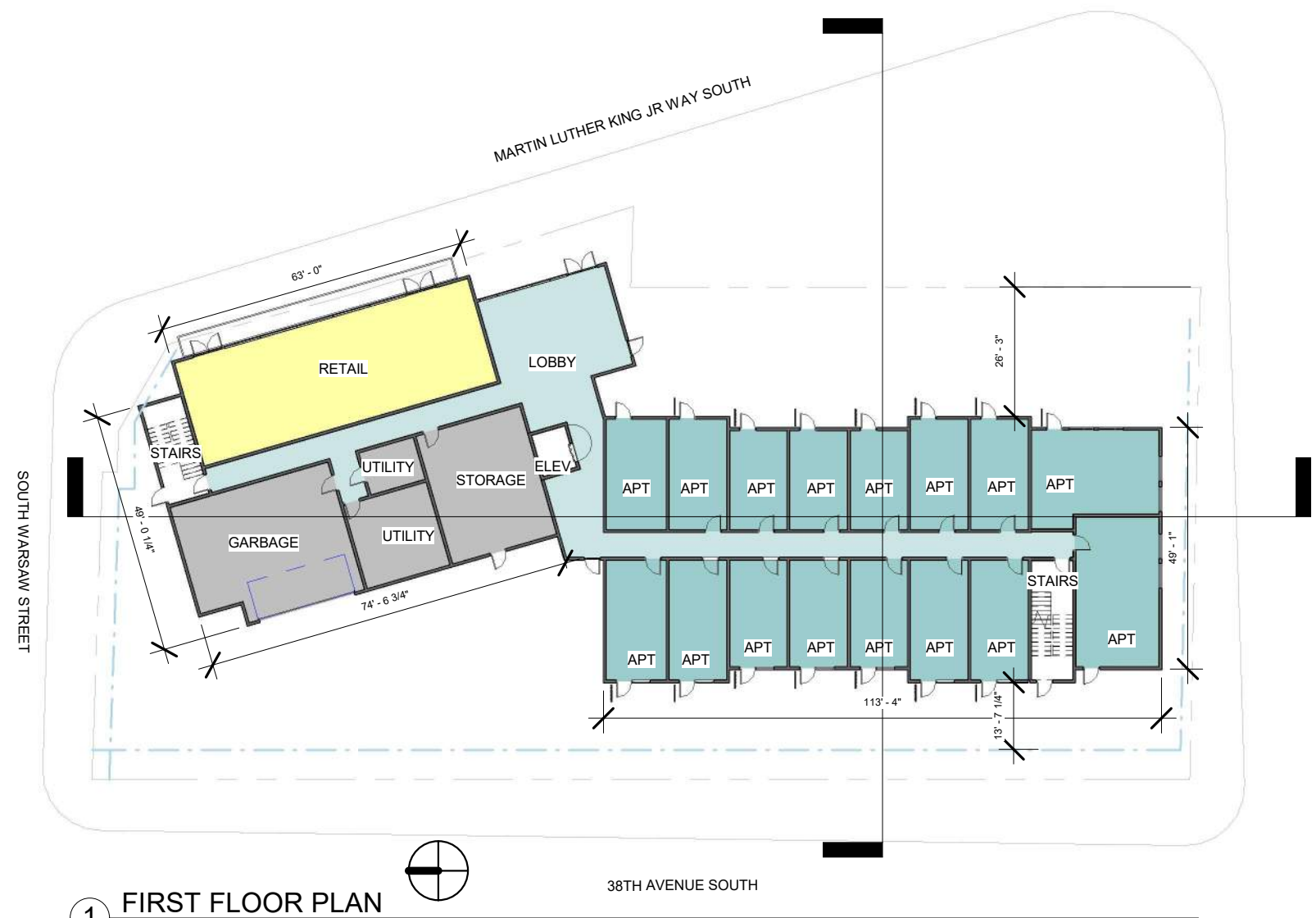


### 3. Residential Courtyard facing MLK JR Way



1 SITE PLAN  
1/32" = 1'-0"



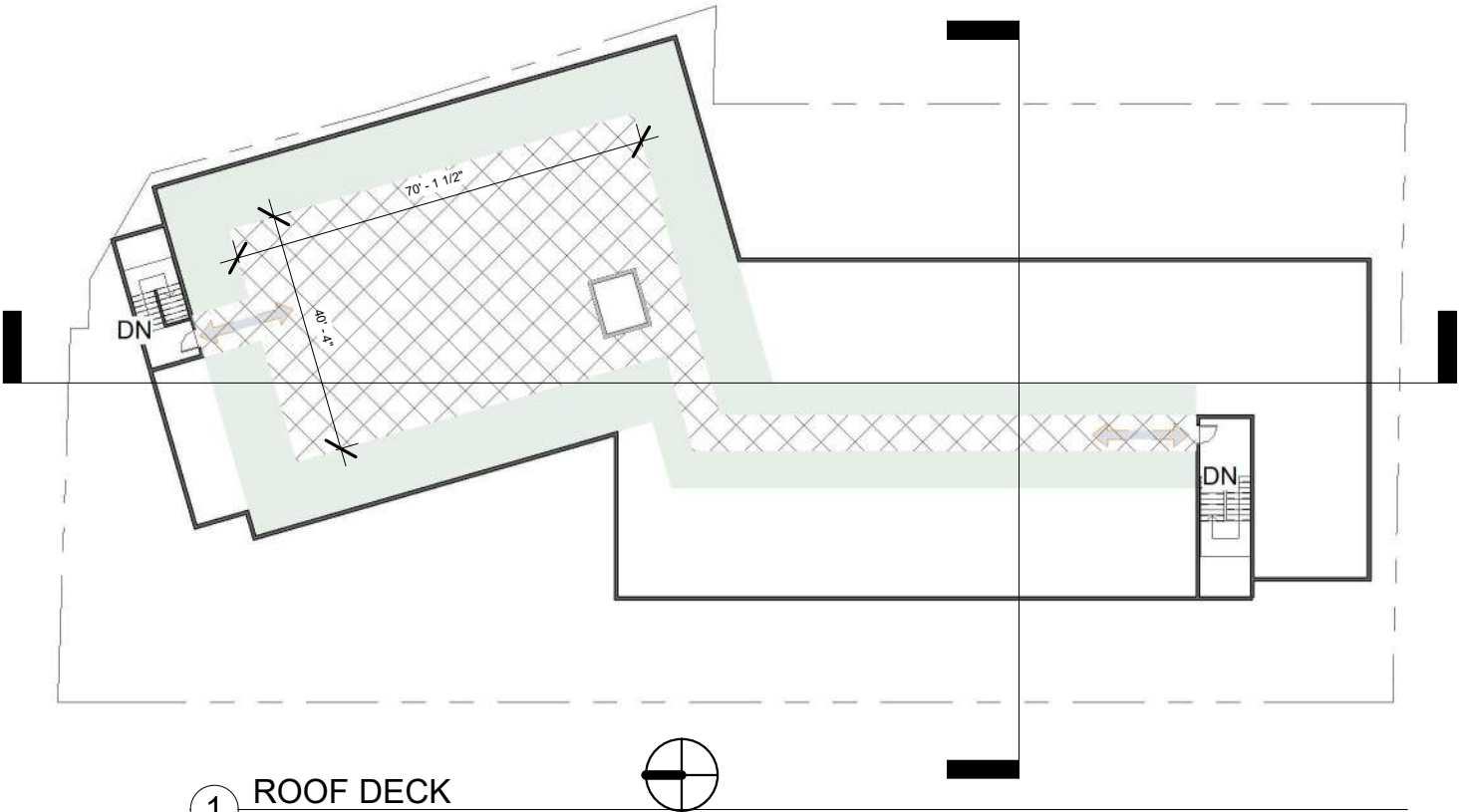


- Lobby & Amenity
- Commercial
- Residential Units
- Utilities & services
- Corridor & Vertical circulation



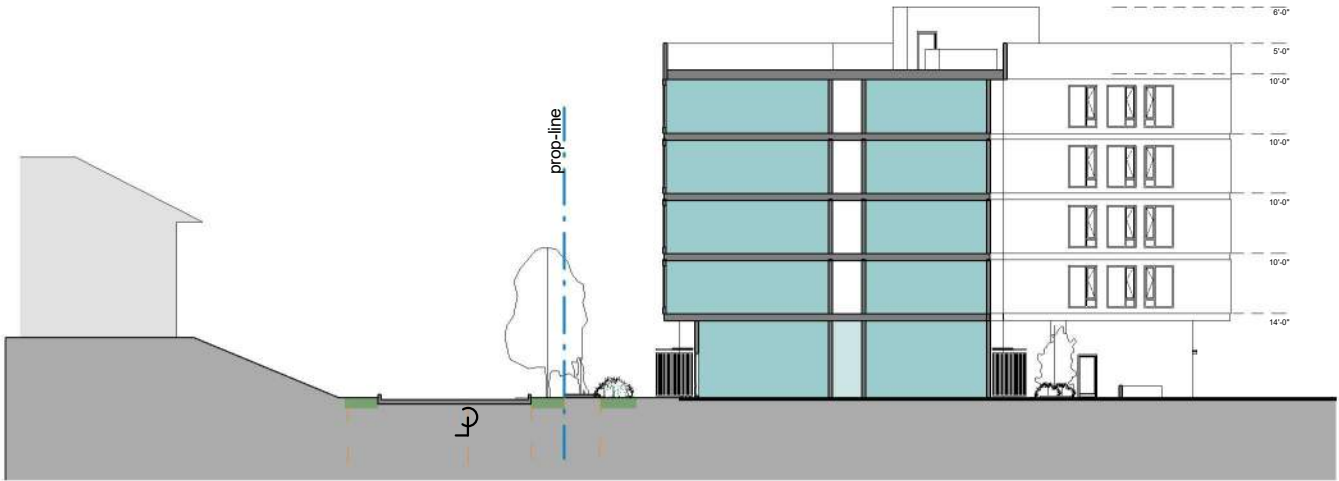
4

MASSING OPTIONS -PARALLEL SHIFT  
 FLOOR PLANS + SECTIONS



1

ROOF DECK  
 1/32" = 1'-0"



2

NORTHEAST- NORTHWEST SECTION  
 1/32" = 1'-0"



3

PARALLEL NORTHWEST-SOUTHEAST  
 1/32" = 1'-0"

- Lobby & Amenity
- Commercial
- Residential Units
- Utilities & services
- Corridor & Vertical circulation





DEC 21, 10 AM



SEP/MARCH 21, 10 AM



JUNE 21, 10 AM



DEC 21, 12 PM



SEP/MARCH 21, 12 PM



JUNE 21, 12 PM



DEC 21, 2 PM



SEP/MARCH 21, 2 PM



JUNE 21, 2 PM





- Outdoor indoor transition at the Commercial space



- Fibercement plank & panel cladding facing 38th ST



- Private entry and outdoor space for groundfloor residential units



- Fibercement and metal cladding facing commercial streets



- Outdoor courtyard at the residential lobby



- Metal railing balconies.



- Street front glazing at commercial spaces



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EDG1 - PUBLIC COMMENT

**Concerned with the number of parking spaces to residential units being provide. Would like to see more parking.**

We intentially chose to design in a transit oriented zoning site without providing on site parking to encourage the use of public transporatation and walkability. We will look into mapping public parking services in proximity to the site as an option to residents with cars.

EDG1 - SDCI STAFF COMMENTS

**1. SDOT and SPU support solid waste access via 38th Ave S, as the other three frontages are unsuitable for solid waste collection. The MLK Jr Way S frontage is a busy arterial, while S Holly St is a planned future neighborhood greenway, and the Warsaw Ave S frontage is too short to safely accommodate solid waste collection.**

Solid waste access is proposed to be via 38th Ave S in the three design options per SDOT recommendation.

**2. This project will require a Street Improvement Permit (SIP) for the required frontage improvements on MLK Jr Way S, S Warsaw St, 38th Ave S, and S Holly St. These frontage improvements include sidewalks, planting strips with new street trees, new curbs where not currently existing, and curb ramps at the currently unimproved site corners. SDOT will continue to work with the applicant through the SIP process.**

Consultants have been engaged to start with the SIP application.

**3. SPU supports collection from 38th Ave S. Services planned from 38th Ave S. will require the use to 2 cubic yard dumpsters and smaller as right-of-way staging is unavailable due to the absence of curb lane parking along the 38th Ave S. frontage of this site. SPU supports a waste access ramp in accordance with SMC Land Use 23.54.040.J to transport dumpsters to the collection location. A waste access ramp must be reviewed and approved by SDOT prior to SPU approval. To ensure the solid waste storage room does not overly exceed the required space outlined in SMC Land Use 23.54.040 Table A, SPU supports twice weekly collection for residential recycle and garbage services.**

Solid waste access is proposed to be via 38th Ave S in the all design options per SPU design recommendation. A ramp will be provided on site to avoid right-of-way staging.

EDG1 - BOARD RECOMMENDATIONS

**1. Massing Options.** The Board discussed the massing options noting preference of option 2 and 3 with additional guidance as follows:

a. **The Board stated they could be comfortable with either option 2 or 3, or perhaps a hybrid massing option. The Board clarified at the next meeting, all presented options should be developed and presented with equal amount of detail. (CS2-D-1. Existing Development and Zoning, CS2-D-3. Zone Transitions, CS3-A-4. Evolving**

We chose to create a hybrid option " The shift parallel" a merge options 2 and 3 . The new option combines the appropriate design features and main ideas from both previous designs and taking into consideration all of the boards feedback and requests.

b. **The Board supported the concept of breaking down the massing into two volumes with potential for two distinct façade treatments in both options 2 and 3. (CS2-D-1. Existing Development and Zoning, CS2-D-3. Zone Transitions, CS3-A-4. Evolving Neighborhoods, CS2-A Location in the City and Neighborhood.**

The Hybrid option is designed with the intent to keep the approved concept of breaking down the masses into two distict volumes. Please see pages 25-31

c. **Option 2**

- **Supported the commercial edge being pushed to the street to create a stronger urban edge and noted it may be helpful to study the longer commercial frontage as shown in Option 3. (CS2-A Location in the City and Neighborhood, CS2-B-2. Connection to the Street)**
- **However, the Board noted pulling back the massing to allow for the entry plaza could provide an interesting outdoor area and relief from MLK in the entry sequence, as well as noting this could be a shared space for retail and residential activation. (CS2-A Location in the City and Neighborhood, CS2-B-2. Connection to the Street, PL3-II Pedestrian Open Spaces and Entrances)**
- **The Board expressed a slight preference for the orthogonal relationship of the massing volume to 38th Ave S and the residential uses across street. (CS2-D-1. Existing Development and Zoning, CS2-D-3. Zone Transitions)**

d. **Option 3**

- **The Board also supported the angled massing volume along 38th Ave S, noting this could result in an interesting play with light and shadow along this façade. (DC2-B Architectural and Facade Composition)**
- **The Board appreciated the longer commercial frontage along MLK Jr. Way South. However, they questioned if this would be more successful if pushed closer to the street edge as done in Option 2. (CS2-A Location in the City and Neighborhood, CS2-B-2. Connection to the Street)**

This design of the hybrid option responds to the board's recommendations and feedback on options 2 & 3 as follow:

- A commercial edge of the building is pushed as close as possible to MLK way to create a more defined and stronger urban edge.
- The residential entrance is pulled back away from MLK JR Way (a major commercial arterial street) to provide an outdoor area, yet maintain visibility.
- The placement of the massing facing 38th St in the hybrid option is responding to and improving the residential uses across the street by stepping the building back, creating a private outdoor space for residents on the ground level, and providing a landscape buffer while maintaining a pedestrian access to 38th ST

e. **Moving forward. Provide studies further developing massing option 2 or 3, or a hybrid option, which takes further consideration of:**

1. **The commercial condition and character along MLK Jr. Way South. Explore the longer massing and pushing the massing to the street. DC2-A-1. Site Characteristics and Uses)** Please see the plans of the preferred option " The shift parallel".

2. **Study the context's surrounding street edge condition to assist in determining what is the appropriate street wall or plaza condition. How will this project fit into the larger context? (DC2-A-1. Site Characteristics and Uses)** A further study of the larger street context and the project's masses helped shape the site and landscape design. See Composite Site plan.

3. **Further refine and clarify siting of the massing at residential zone transition across 38th Ave S. (CS2-D-3. Zone Transitions, CS2-IV Height Bulk and Scale Compatibility)** See sheets 27-29

2. **Site plan and landscaping. The Board requested more detail and clarification as follows:**

a. Further consider the future greenway along South Holly St and how the proposed landscape plan will support and add to the greenway. (PL1-A-2. Adding to Public Life, PL1-B Walkways and Connections)



2. **Site plan and landscaping.** Overall, the Board was intrigued by the proposed landscaping sketches for both options 2 and 3 However, the Board requested more detail and clarification as follows:

a. **Further consider the future greenway along South Holly St and how the proposed landscape plan will support and add to the greenway. (PL1-A-2. Adding to Public Life, PL1-B Walkways and Connections)**

See the composite site plan and the conceptual landscape design

b. **Provide a composite landscape plan / first floor plan which clarifies entries, setbacks, occupiable vs. passive spaces, and circulation.(DC4-D Trees, Landscape, and Hardscape Materials, DC3-A Building-Open Space Relationship)**

Please see composite siteplan

c. **Provide precedent images or character sketches clarifying the residential scale and character intended for the first floor, especially, along South Holly St and 38th Ave S. (PL2-B Safety and Security, PL3-A Entries, PL3-B Residential Edges)**

See new design guideline responses where precedent images and character sketches have been provided clarifying the residential scale and character intended for the first floor.

d. **Begin to study how the garbage entry and drive will be designed to mitigate visual impacts. (DC1-C-2. Visual Impacts)**

The preferred option provides the most appropriate solution to solid waste collection where an on-site ramp is designed per SPU guidelines. The ramp will be separated from the residential outdoor spaces with a curb and elevated vegetation area. Collecting the solid waste on-site will ease the burden of right-of-way use and mitigate visual impact on the residents across the street.

e. **Consider and explore how residential common entry and stoops could improve the residential character and scale along 38th Ave S. (PL3-A Entries, PL3-B Residential Edges)**

The composite site plan and inspiration design images provided in the design guidelines for the new design present the intent of the feel and look of the common residential entry and the entries of the individual units on the ground floor.



6 APPENDIX A  
COMMUNITY OUTREACH SUMMARY

Summary of Outreach Methods:

KOZ Development LLC has recently conducted the following outreach activities for the early design guidance process in order to gain community input for the proposed project development at 3803 S Warsaw ST, Seattle, WA. The project is located in an Equity area. Specific methods have been used to reach out as many groups as possible based on the instructions stated in Seattle Neighborhoods.

Below is a list of chosen methods.

Electronic/Digital Outreach

**Department of neighborhoods Blog Post:** Information was provided to (DON) for the community blog. Project was posted to the blog on December 18, 2020.  
**Project website and email (High impact):** A project website and email was established on December 01, 2020 to provide information about the project and to collect comments and or suggestions. Both email and website are both active as of today's date. A few emails have been received requesting to be put on a list for future notifications.  
Project email: <mailto:kozonMLK@gmail.com>  
Project Website: <https://Koz on MLK.com>

**Online Survey (High Impact):** An online survey was created on December 01, 2020 to collect information for the proposed project. The survey could be found by either an address provided on the posted flyers or from a link provided on the project web site. The survey is still active as of today's date. We didn't receive any responses from this survey so we sent out direct emails with a link to the website and survey to community Groups in the area shown on Othello Neighborhood page.  
<https://www.surveymonkey.com/r/MJY6MQG>

Printed Outreach

**Outreach posters (High Impact):** Roughly 50 posters presented in six languages (English, Spanish, Somali, Mandarin Chinese, Cantonese Chinese, and Vietnamese) were placed in various locations in and around the site area on 12/14/20 (15 days in advance of the meeting) in an attempt to reach out to people and neighbors that might use, live, or work in the area. Posters were hung in high traffic areas on poles as well as off-path intersections, businesses, bus stops, community boards, and other locations likely to attract attention. In addition, posters were handed out to several small business owners and community residents around the area. The posters provided information about the project as well as contact information, website, survey, and the date of a community outreach event. Many of these posters are still visible as of today's date.

In-Person Outreach

**Community meeting (High Impact):** An open community informational meeting was held via virtual "Zoom" meeting on Monday, January 11, 2021 from 4:00-5:00. The meeting was held to inform attendees about the project, address any questions or concerns about the project, and collect input from the community. 1 attendee from the area joined the meeting and provided a lot of great discussions, and useful comments.

**Local neighborhood group meeting (High Impact):** Prior to the outreach process KOZ met and presented to the Othello Station Community Action Team (OSCAT) on January 07, 2021 from 7:00-8:00 with roughly 13 attendees. This meeting was very helpful and yielded some great discussions and useful feedback.

Comments:

lower level Program:

- The attendee asked if retail will be part of the project?
- The attendee expressed that a commercial feel at the base with flexible common space would be good. And commented that street level engagement is important and if retail is not proposed a plan for activating the street level should be given consideration.

Parking:

- The attendee commented that tenants will be stressed to find parking in the area and suggested if there is a possibility to partner with or help providing parking in public parking lots with discounted rate.

Design:

- The attendee commented that an activated base is very important.
- Another comment was expressed about the need for common area space, and what the plan will be for this building?

Building program:

- These are the comments presented from the attendee:
- Asked about the occupancy and demographic of the units? Longer term residents are more desirable than short term residents.
  - The bike parking should be secure.
  - Suggested to have projections in the massing, roof decks and balconies if possible

EARLY COMMUNITY OUTREACH PUBLIC EVENT  
SỰ KIỆN CỘNG ĐỒNG TIẾP CẬN CỘNG ĐỒNG SỚM  
EVENTO PUBLICO DE ALCANCE COMUNITARIO TEMPRANO  
BULSHADA HORREEYA DHACDOOYINKA DADWEYNAHA  
早期社区外展公众活动  
早期社區外展公眾活動  
01-11-21 from 4:00 - 5:00pm

PLEASE JOIN US FOR A COMMUNITY MEETING  
HÃY THAM GIA VỚI CHÚNG TÔI ĐỂ HỌP CỘNG ĐỒNG  
POR FAVOR, ÚNASE A NOSOTROS PARA UNA REUNIÓN DE LA COMUNIDAD  
FADLAN NAGU SOO BIIR SHIRKA BULSHADA  
請加入我們的社區會議  
加入我区的社区会议

PROPOSAL SUMMARY  
TÓM TẮT ĐỀ XUẤT  
RESUMEN DE LA PROPUESTA  
SOO KOOBAN KU SAABSAN  
建議摘要  
建区摘要

THE PROJECT PROPOSES A 125 UNIT 5-STORY APARTMENT BUILDING PRIMARILY  
CONSISTING OF STUDIOS WHICH WILL RANGE FROM 258 SQFT TO 357 SQFT.  
ALONG WITH A FEW 1 AND 2 BEDROOM UNITS  
RANGING FROM 448 SQFT TO 634 SQFT. NO PARKING IS PLANNED.  
Website: <https://6609mlk.webnode.com>

ADDITIONAL INFORMATION  
THÔNG TIN BỔ SUNG  
INFORMACIÓN ADICIONAL  
MACLUUMAAD DHEERAAD AH  
其他資訊  
其他信息

Site: 3803 S Warsaw ST, Seattle, WA  
Website: <https://6609mlk.webnode.com>  
Email: [kozonMLK@gmail.com](mailto:kozonMLK@gmail.com)  
Survey: <https://survey.typeform.com/to/q06nln8o>

EVENT INFORMATION  
THÔNG TIN SỰ KIỆN  
INFORMACIÓN DEL EVENTO  
MACLUUMAAD DHACDO  
活動資訊  
活区信息

Date: January 11, 2021  
Time: 4:00 - 5:00 pm  
Location: <https://us02web.zoom.us/j/82267057804?pwd=SjNyQ0dEdk9qdItRc2ZuY05ialBYQT09>

35 Koz Development I JUNE 2021 I EDG-2 I SDCl # 3037570 3803 S Warsaw ST



## 7 DESIGN APPROACH

### PRIORITY DESIGN GUIDELINES

#### OTHELLO NEIGHBORHOOD DESIGN GUIDELINES

#### OTHELLO NEIGHBORHOOD DESIGN GUIDELINES

#### PROJECT USES AND ACTIVITIES

-Active uses have been positioned along the major arterial - MLK Jr. Way- including Retail, Lobby, Bike Room and Mail Room to activate the street level and provide views into those spaces.

-The proposed building massing steps down and employs modulation and secondary elements such as balconies, and canopies to minimized the perceived mass.

-The street level glazing will maximize opportunities to engage the pedestrians and enable an active and vibrant street front, especially as a corner lot building.

-Large windows will be strategically designed and placed to maximize daylight for interior spaces.

#### A. SITE PLANNING

##### A-2 STREETSCAPE COMPATIBILITY

- Shallow setbacks and no vertical grade separations along MLK way are provided where the commercial areas are proposed.

-Connection of on-site pedestrian walkways with existing public and private pedestrian infrastructure will be provided to support pedestrian connections within and outside the project along with outdoor seating, lighting, large storefront windows and year round landscaping.

##### A-4 HUMAN ACTIVITY

-Universal design will be provided in a manner that is fully integrated into the project design.

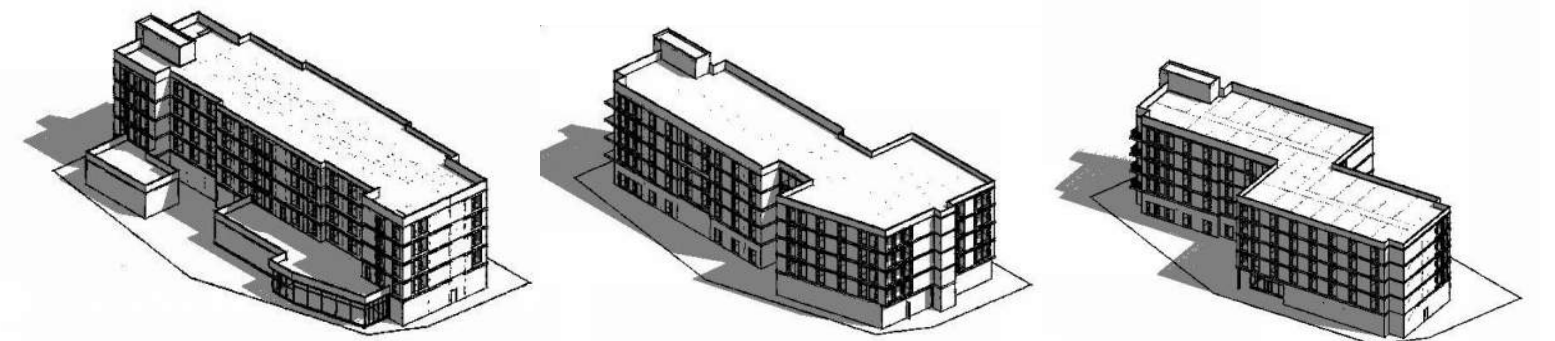
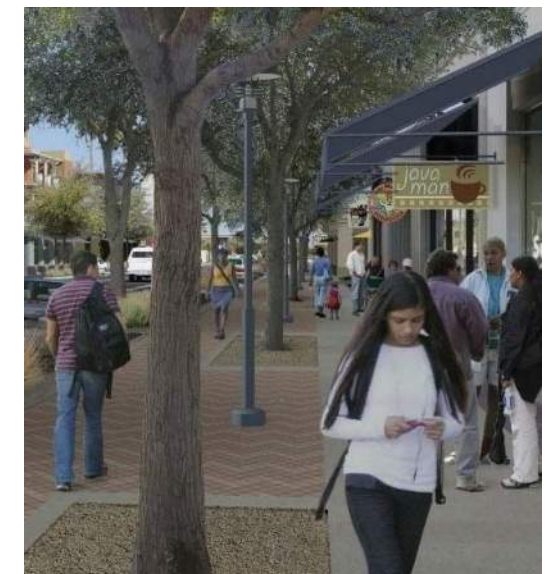
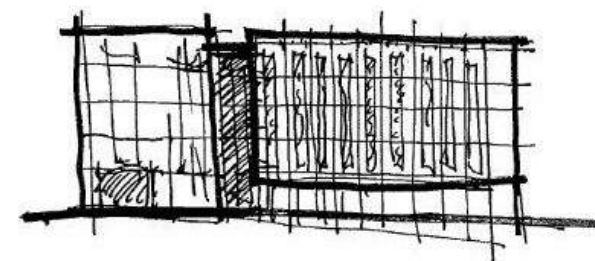
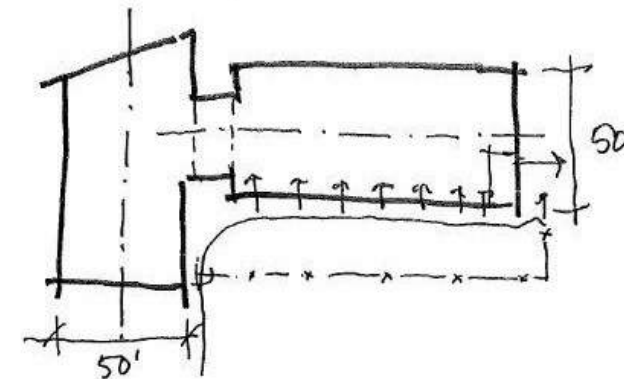
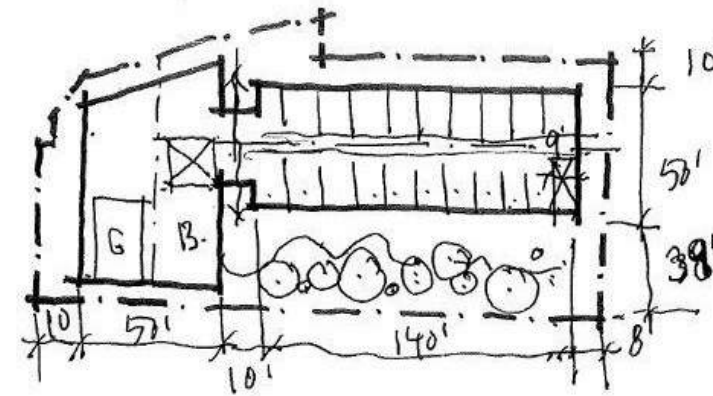
-Large glazed areas will be utilized along MLK way to provide visibility into the building interior and merchandise display at commercial areas.

-Privacy and security will be provided for residents by providing a setback from the public sidewalk, a recessed entranceway and a range of exterior finishes on the facade such as planter boxes or pots and vertical modulation that articulates the location of residential entries.

-Overhead weather protection (canopies) will be used along the commercial use on MLK way to provide pedestrian comfort

##### A-5 RESPECT TO ADJACENT SITES

- Setbacks with landscape areas will be provided along 38TH Avenue S to create a buffer between uses.







## B. HEIGHT, BULK AND SCALE COMPATIBILITY

-Building orientation and massing on both MLK way and 38TH Avenue S will be created to provide a strong visual emphasis on urban pattern.

## C. ARCHITECTURAL ELEMENTS AND MATERIALS

-Building cladding will be durable and maintainable high quality materials suited to our Pacific Northwest climate.

-Exterior facades will be composed of a variation of fibercement panels, metal cladding, and storefront glazing which will convey a sense of permanence, human scale and proportion

-Lighting will be used to provide safety, security and interest to outdoor spaces and building features such as entrances and feature walls.

-Attractive native low maintenance species will be emphasized and mark edges and create points of interest at the pedestrian level and for street level private residential entrances.

## D. PEDESTRIAN ENVIRONMENT

### D-1 PEDESTRIAN OPEN SPACES AND ENTRANCES

-Improvements to the pedestrian environment at the street level on MLK Way, 38th Avenue S, Holly and Warsaw Streets will be provided.

-A variety of features such as planters, decks, groves of trees and street trees using a combination of monochromatic and colored hardscape as well as plantings will be created to shape open spaces and articulate a transition from private to public.

### D-7 PERSONAL SAFETY AND SECURITY

-Lines of sight will be provided for a safe environment encouraging natural surveillance through strategic placement of doors and other features like pedestrian entry lighting and security lighting.

-A landscape buffer ( flower beds, variety of sidewalk patterns and lighting fixtures) will be provided between the private open courtyards and entrances and public spaces to create a symbolic transitional boundary between zones.

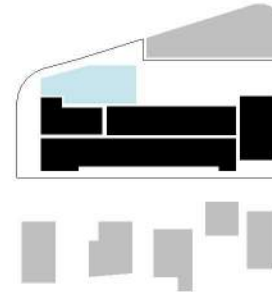




## 7 MASSING OPTIONS -PARALLEL RESIDENTIAL

### MASSING

This approach considers the benefits and impacts of relating the primary mass of the project more closely to the residential area to the west and providing for commercial development along MLK.



#### Overview

- Approximate Gross Area +/- 59,000 SF
- 125 Residential Units
- Commercial Space +/- 2300 SF
- 2nd Level Roof Deck Amenity space +/- 1350 SF
- Bike Parking Stalls +/- 75

#### Pros

- Commercial space added on the east side of the site along MLK increases connectivity to the business area and provides additional space for economic ventures associated with the transit core.
- The massing allows a more public yard close to MLK to be utilized by the commercial spaces and their customers.
- The massing creates a private 2nd floor plaza for residents with views over MLK.
- Individual units' entries on the ground floor from 38th St increase pedestrian activity and improves residential area adjacent to the site

#### Cons

- The massing consolidated to the west limits available space for landscaping adjacent to the single family use across 38th Street South
- The massing approach creates a larger, blockier form, the avoidance of which was requested by at least one member of the community during outreach.
- The massing bulk is in stark contrast to the scale of structures to the west of 38th Street South.



NW BIRDSEYE VIEW



SE BIRDSEYE VIEW

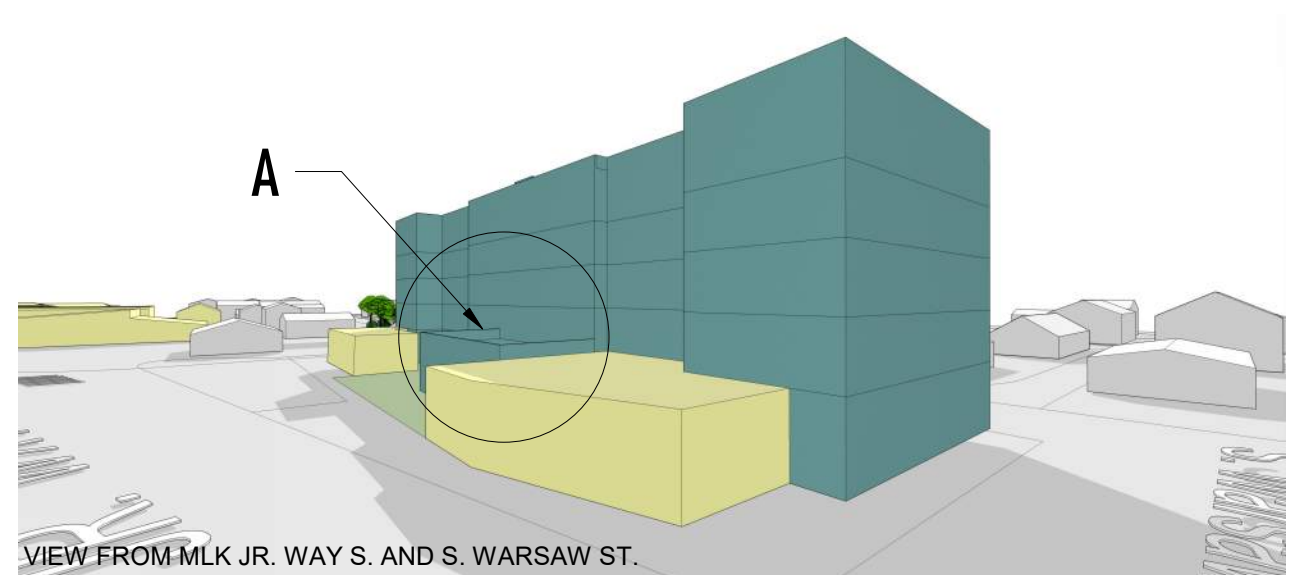




**A** SECOND FLOOR ROOF DECK



**B** COMMERCIAL ENTRANCE



VIEW FROM MLK JR. WAY S. AND S. WARSAW ST.



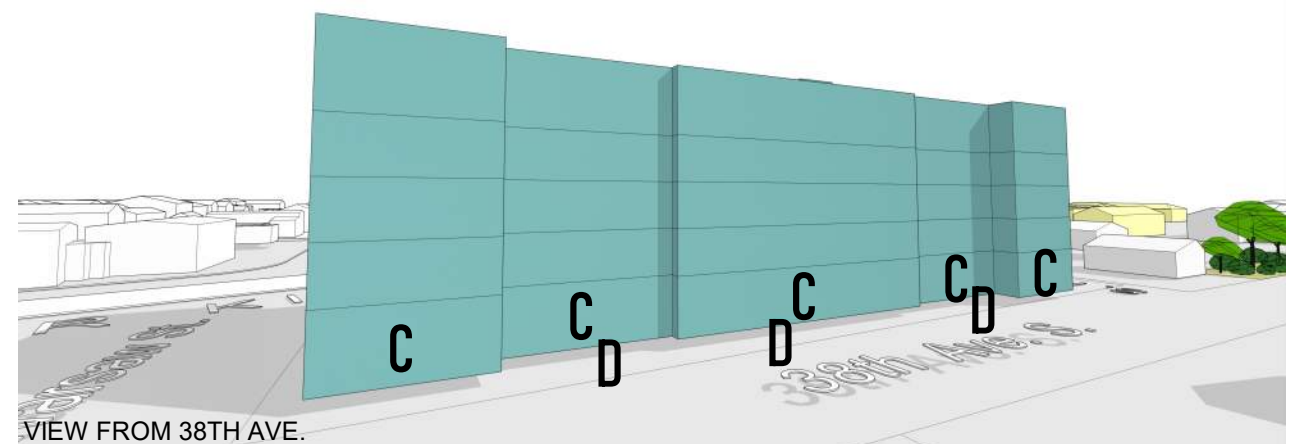
**C** RESIDENTIAL ENTRY



**D** RESIDENTIAL FRONT GARDEN

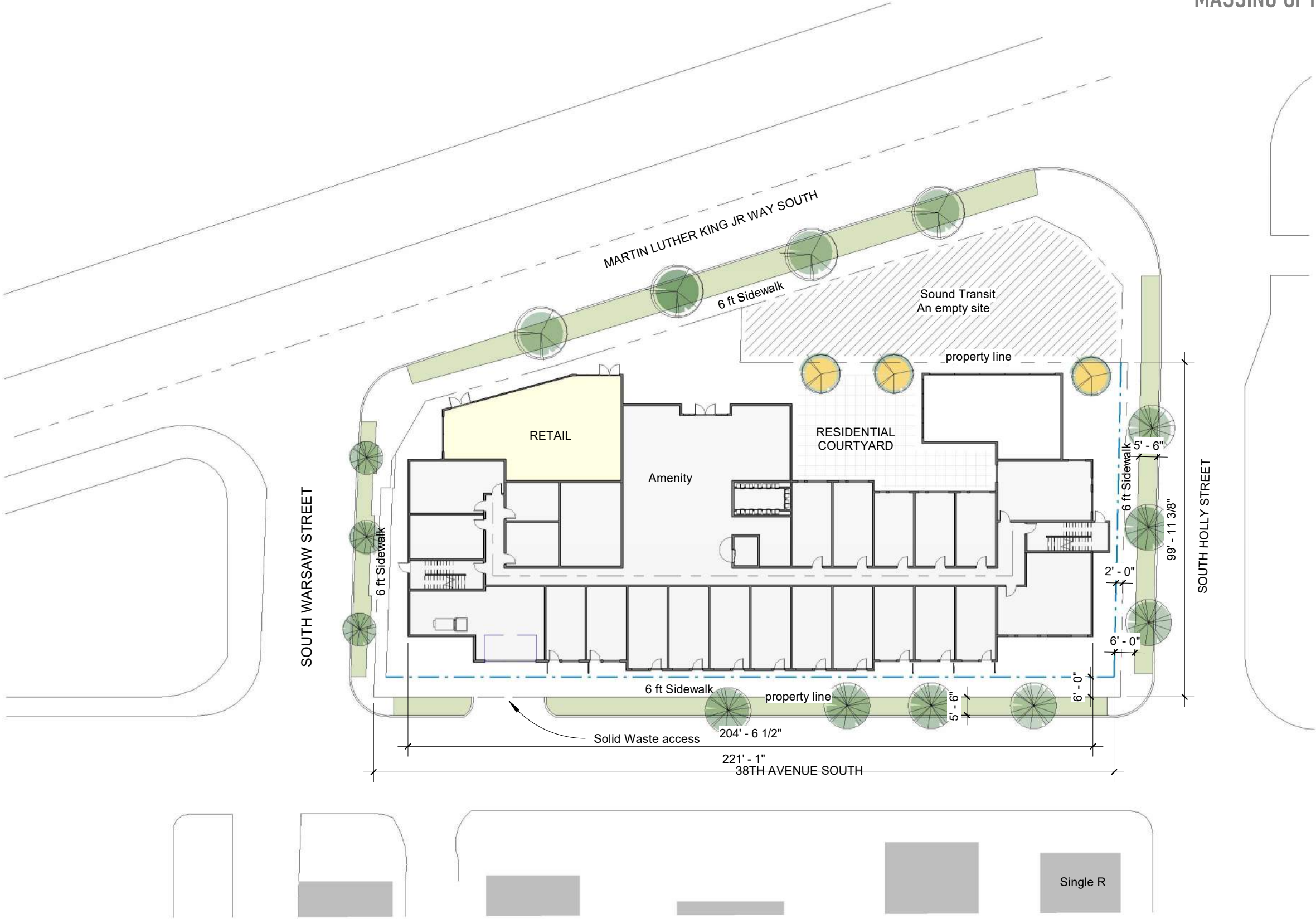


VIEW FROM MLK JR. WAY S. AND S. HOLLY ST.



VIEW FROM 38TH AVE.

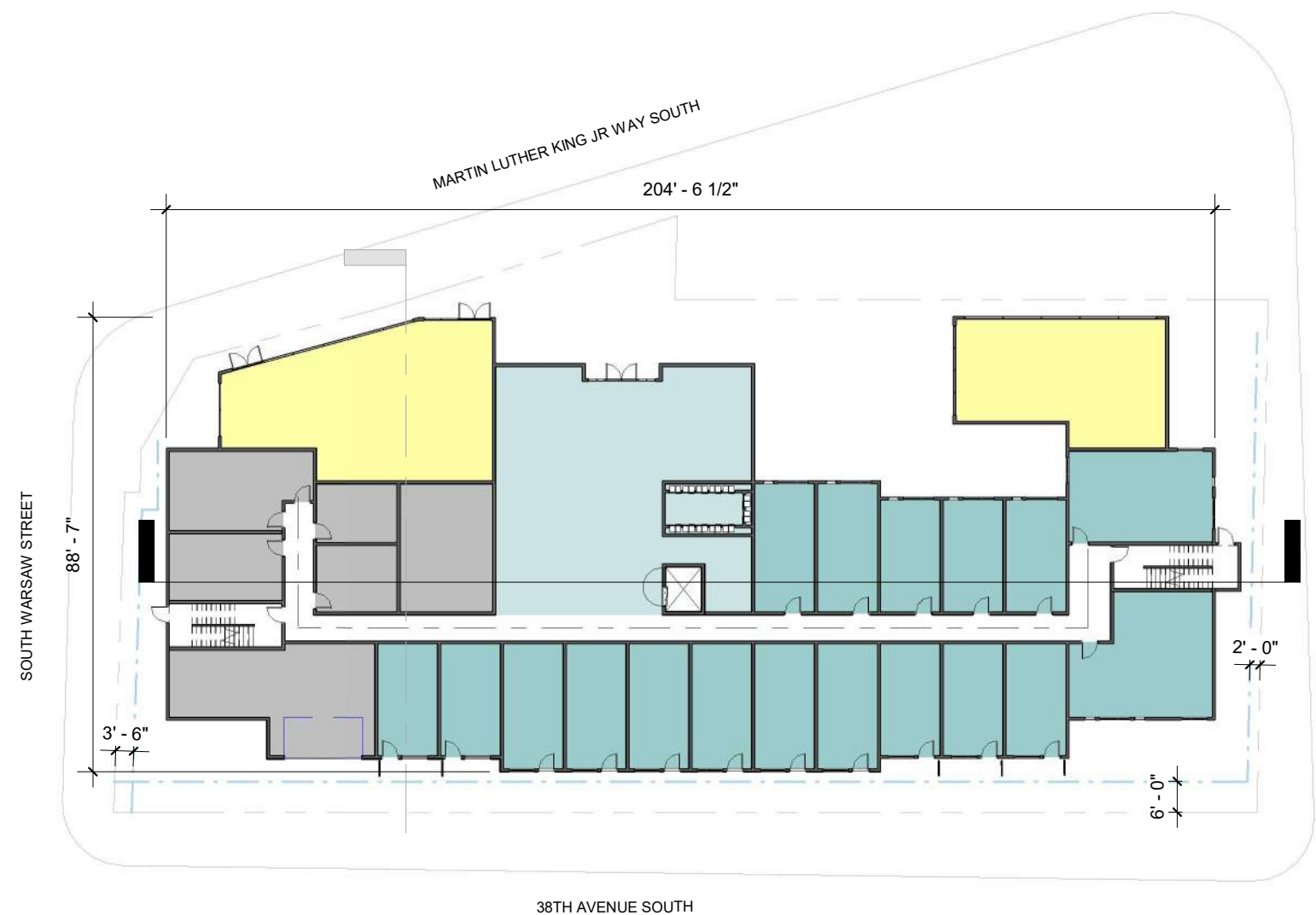




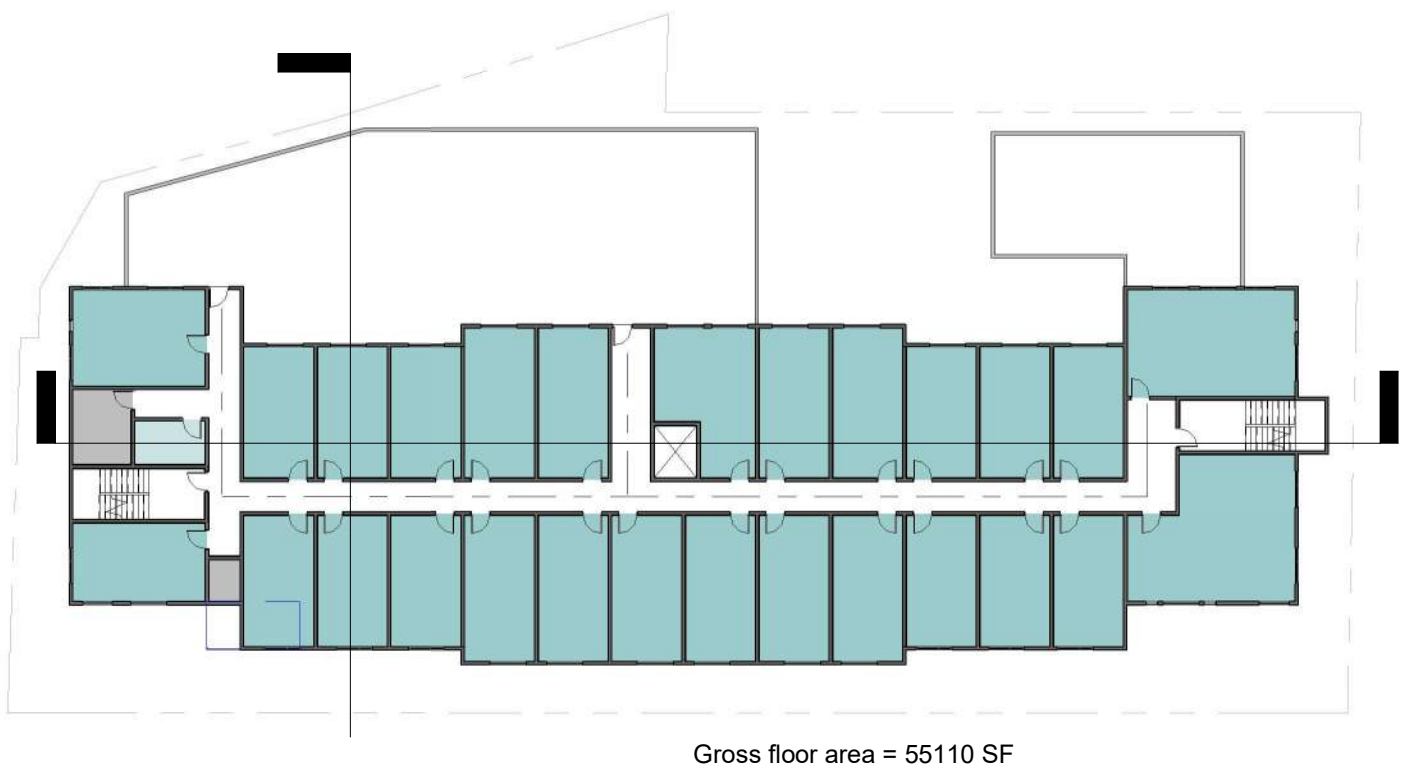
1 COMPOSITE SITE PLAN  
1/32" = 1'-0"



7 MASSING OPTIONS -PARALLEL RESIDENTIAL  
FLOOR PLANS + SECTIONS



1 FIRST FLOOR PLAN  
1/32" = 1'-0"

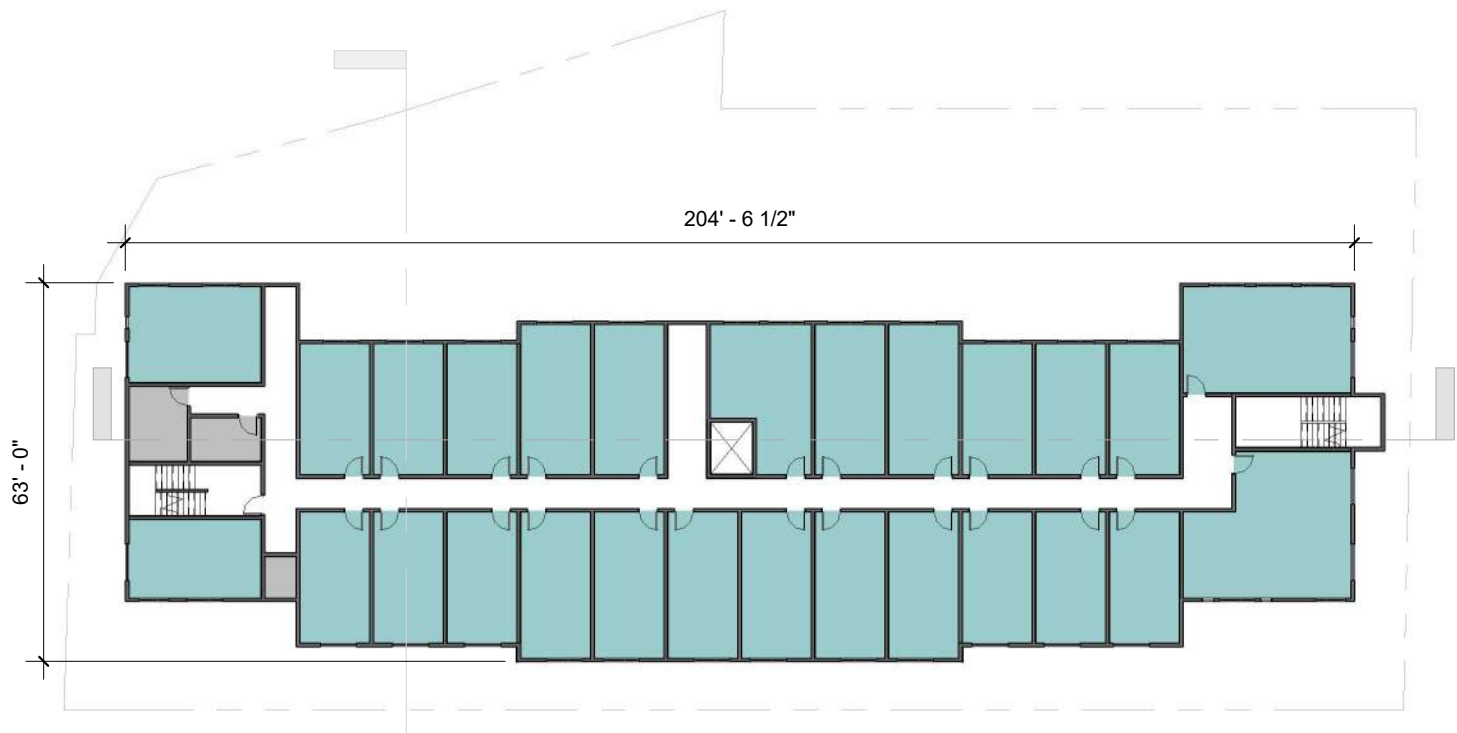


2 SECOND FLOOR PLAN  
1/32" = 1'-0"

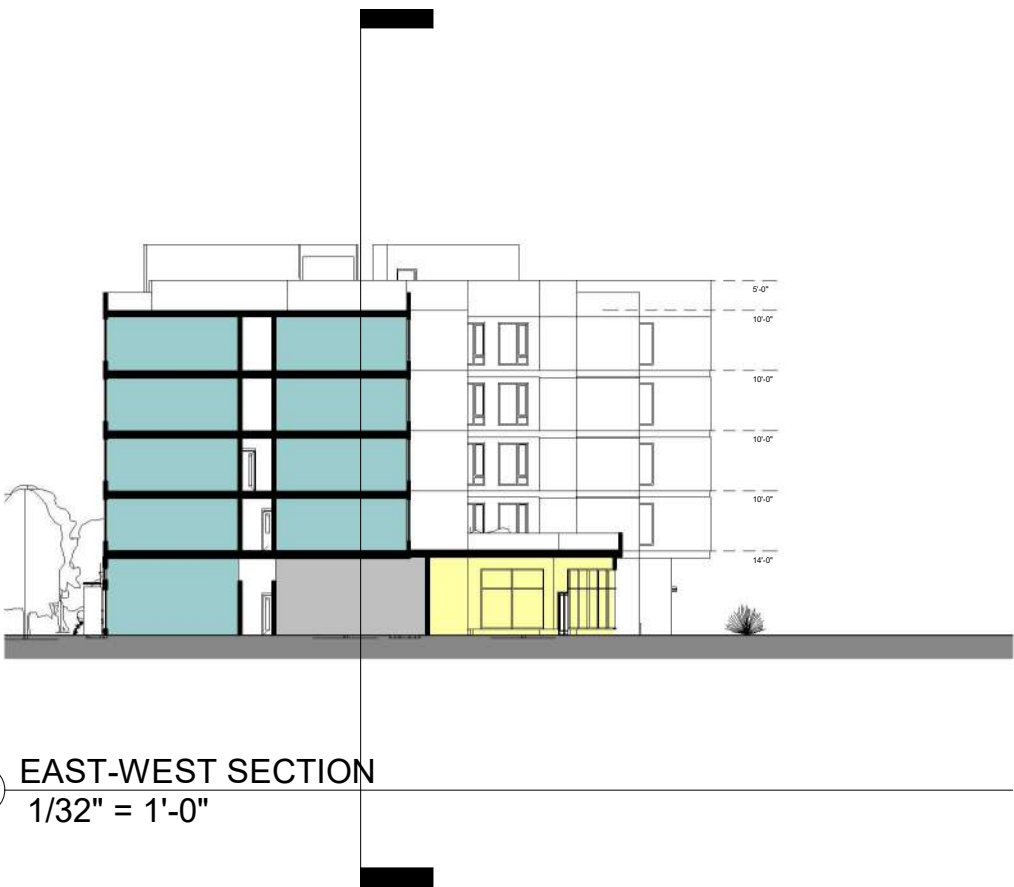
Gross floor area = 55110 SF

- Lobby & Amenity
- Commercial
- Residential Units
- Utilities & services
- Corridor & Vertical circulation





1 THIRD TO FIFTH FLOOR PLAN  
1/32" = 1'-0"



2 EAST-WEST SECTION  
1/32" = 1'-0"



3 NORTH-SOUTH SECTION  
1/32" = 1'-0"

- Lobby & Amenity
- Commercial
- Residential Units
- Utilities & services
- Corridor & Vertical circulation



7 MASSING OPTIONS -PARALLEL RESIDENTIAL  
SHADING STUDIES



DEC 21, 10 AM



SEP/MARCH 21, 10 AM



JUNE 21, 10 AM



DEC 21, 12 PM



SEP/MARCH 21, 12 PM



JUNE 21, 12 PM



DEC 21, 2 PM



SEP/MARCH 21, 2 PM



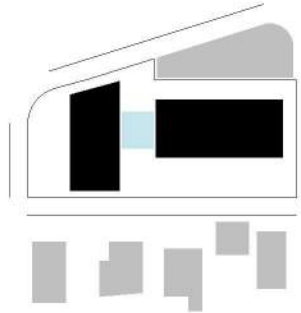
JUNE 21, 2 PM



## 7 MASSING OPTIONS - TRANSITIONING MASSES

### MASSING

This approach considers the benefits and impacts of separating the building into two main masses and "gasketing" them with a more transparent circulation core



### Overview

- Approx Total area +/- 59,000 SF
- 125 Residential Units
- Commercial Space +/- 1470 SF
- Bike Parking Stalls +75

### Pros

- The massing allows the building's form to respond to both axes of the surrounding streets.
- The commercial space added on the east side of the site along MLK increases connectivity to the business area and provides additional space for economic ventures associated with the transit core.
- The massing creates a large yard along 38th Avenue S which takes advantage of the South West orientation and buffers impacts to the adjacent single family uses.
- The breakdown in masses responds to the larger neighborhood contexts of smaller buildings.
- The massing creates a private 2nd floor plaza for residents with views over MLK.
- Building footprint provides a variety of outdoor spaces and landscaping opportunities.

### Cons

- The massing limits amenity areas on the main level.
- The massing reduces the outdoor space along MLK.

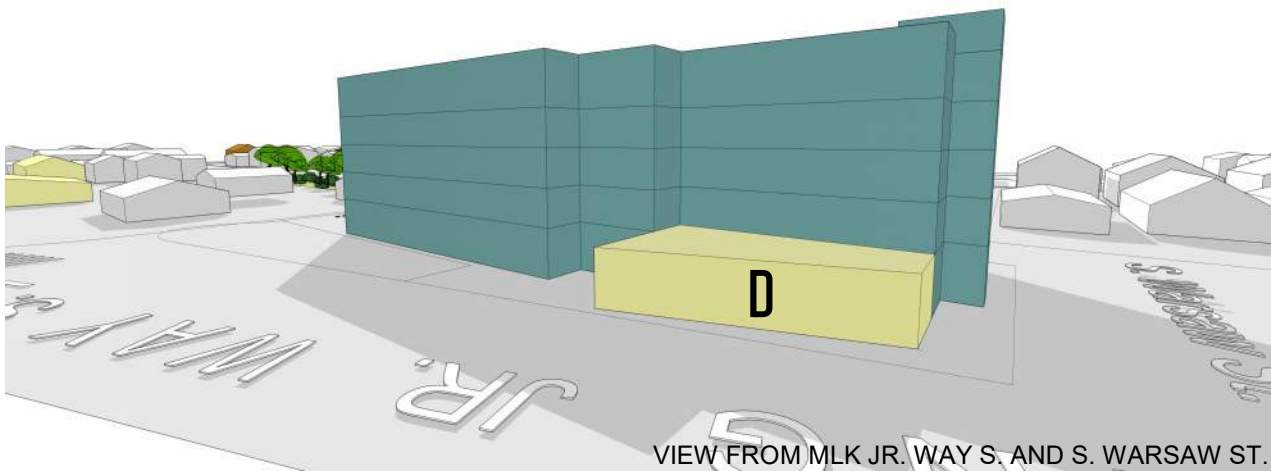


NW BIRDSEYE VIEW

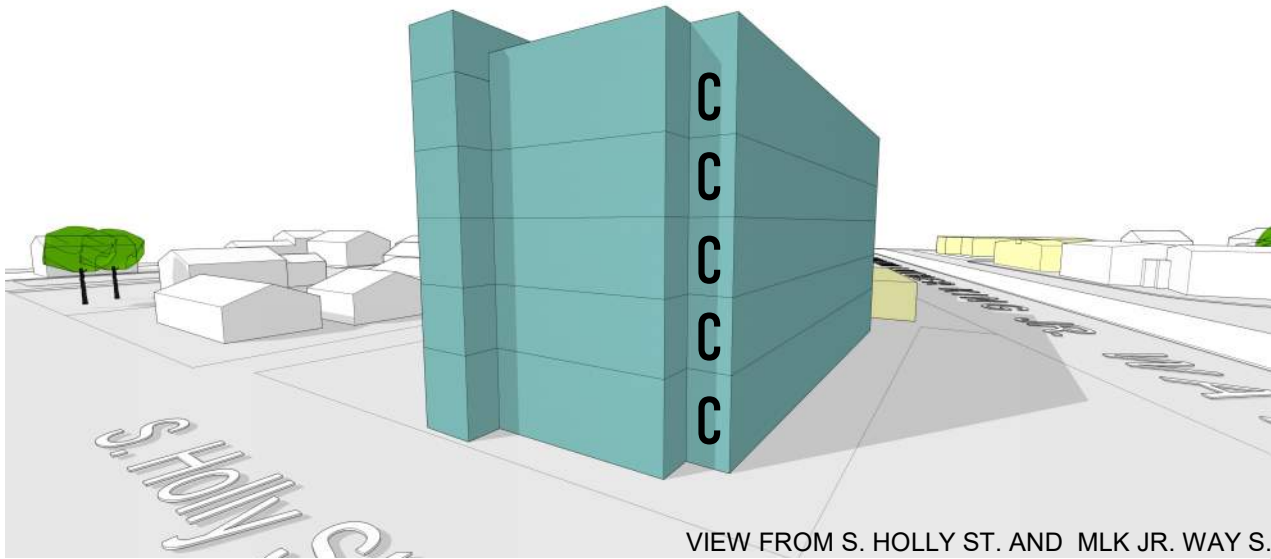


SE BIRDSEYE VIEW

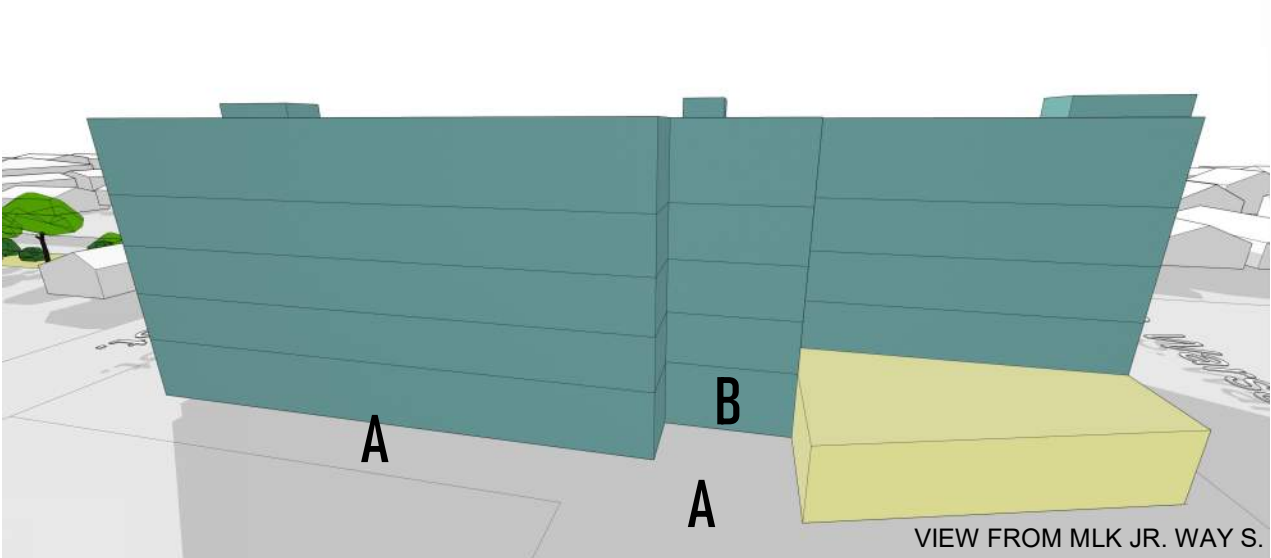




VIEW FROM MLK JR. WAY S. AND S. WARSAW ST.



VIEW FROM S. HOLLY ST. AND MLK JR. WAY S.



VIEW FROM MLK JR. WAY S.



A RETAIL + RESIDENTIAL OUTDOOR SPACE



B LOBBY ENTRY

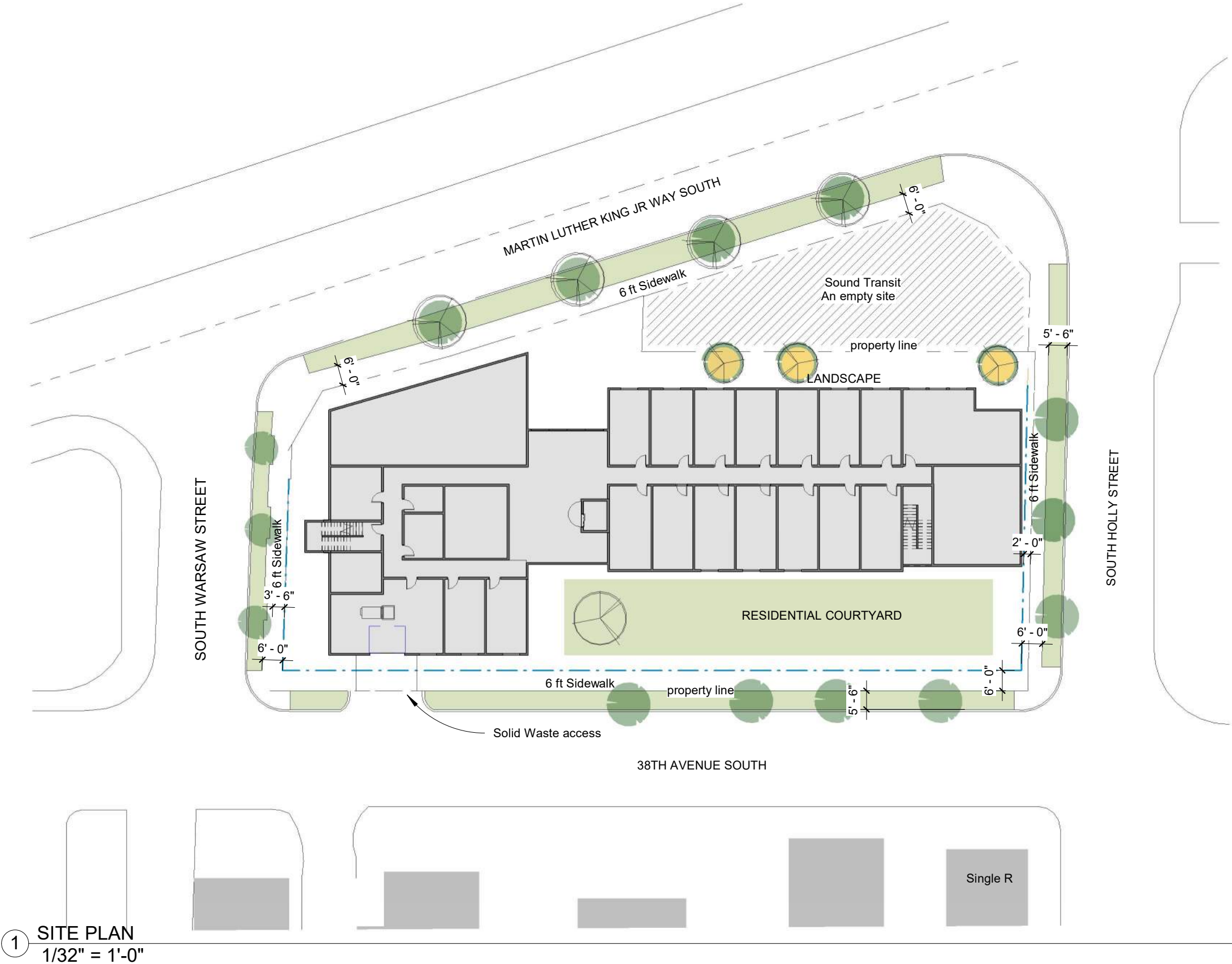


C LIVING UNIT BALCONIES



D COMMERCIAL ENTRANCE





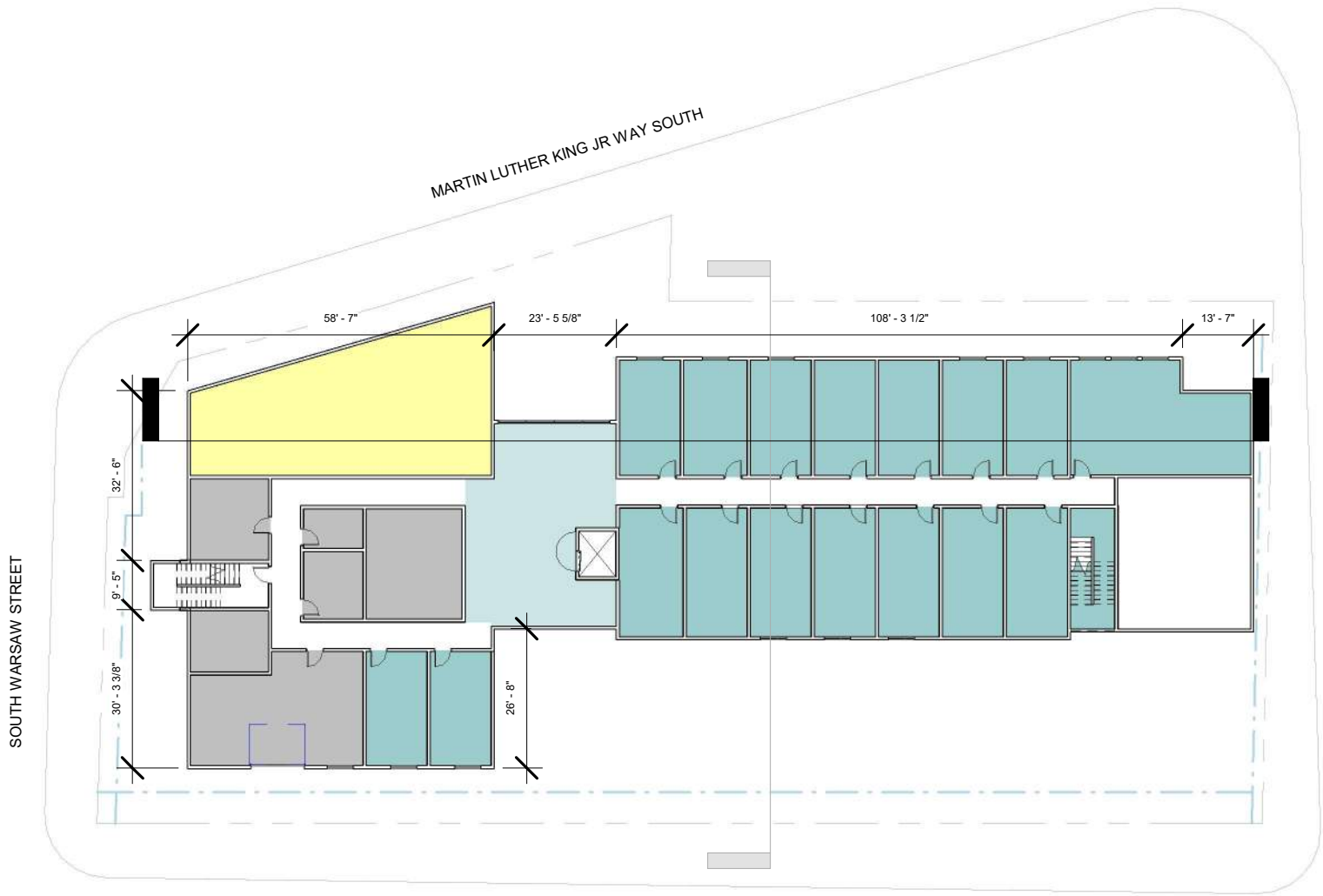
1 SITE PLAN  
1/32" = 1'-0"



# 7

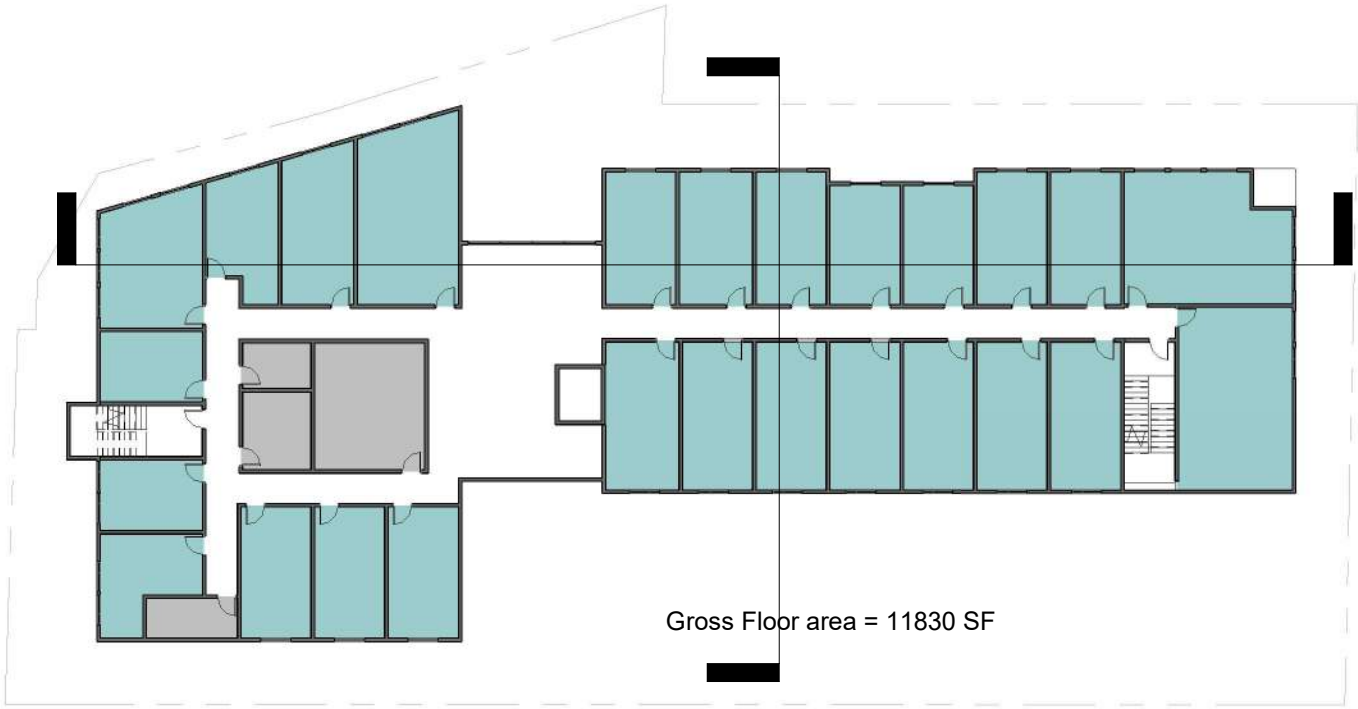
## MASSING OPTIONS - TRANSITIONING MASSES

PLANS + SECTIONS



1 FIRST FLOOR PLAN  
1/32" = 1'-0"

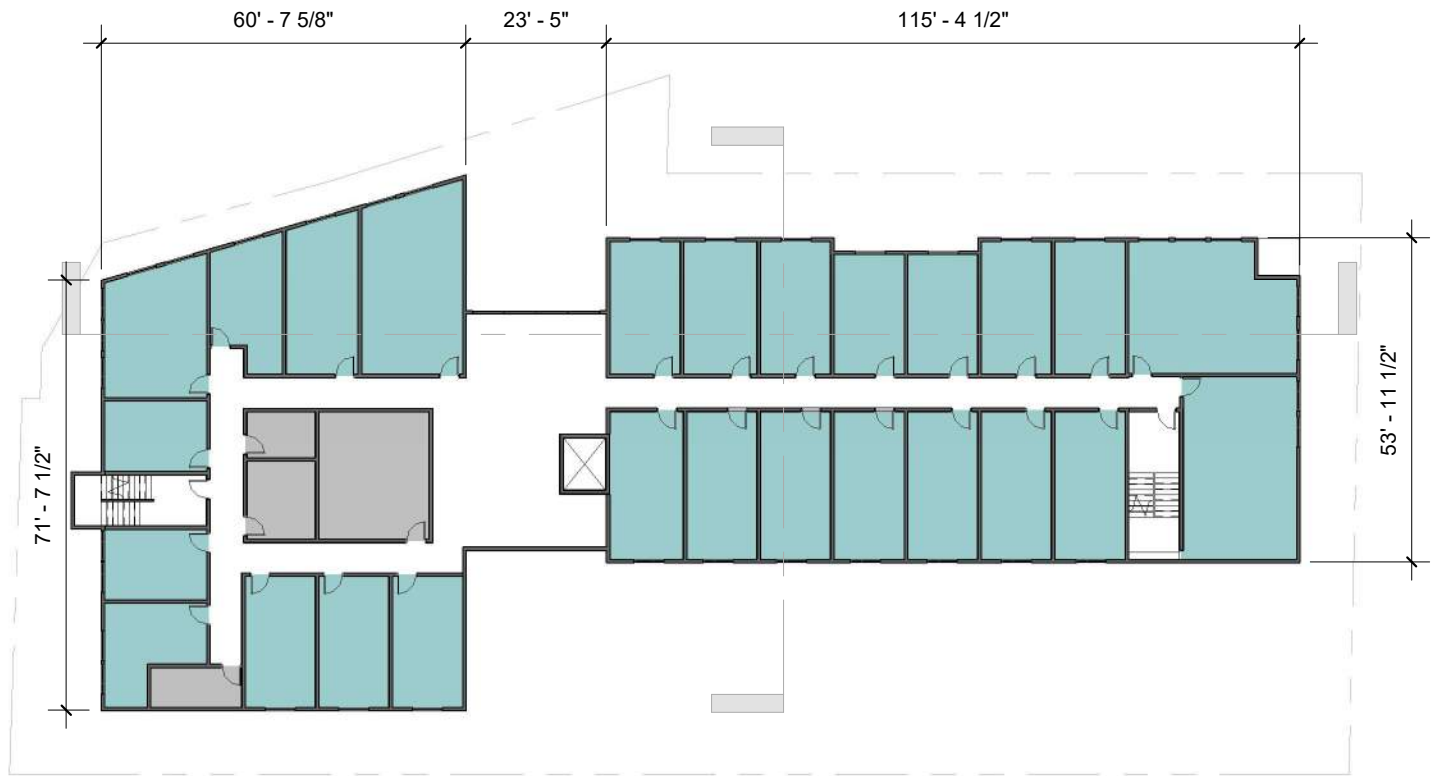
38TH AVENUE SOUTH



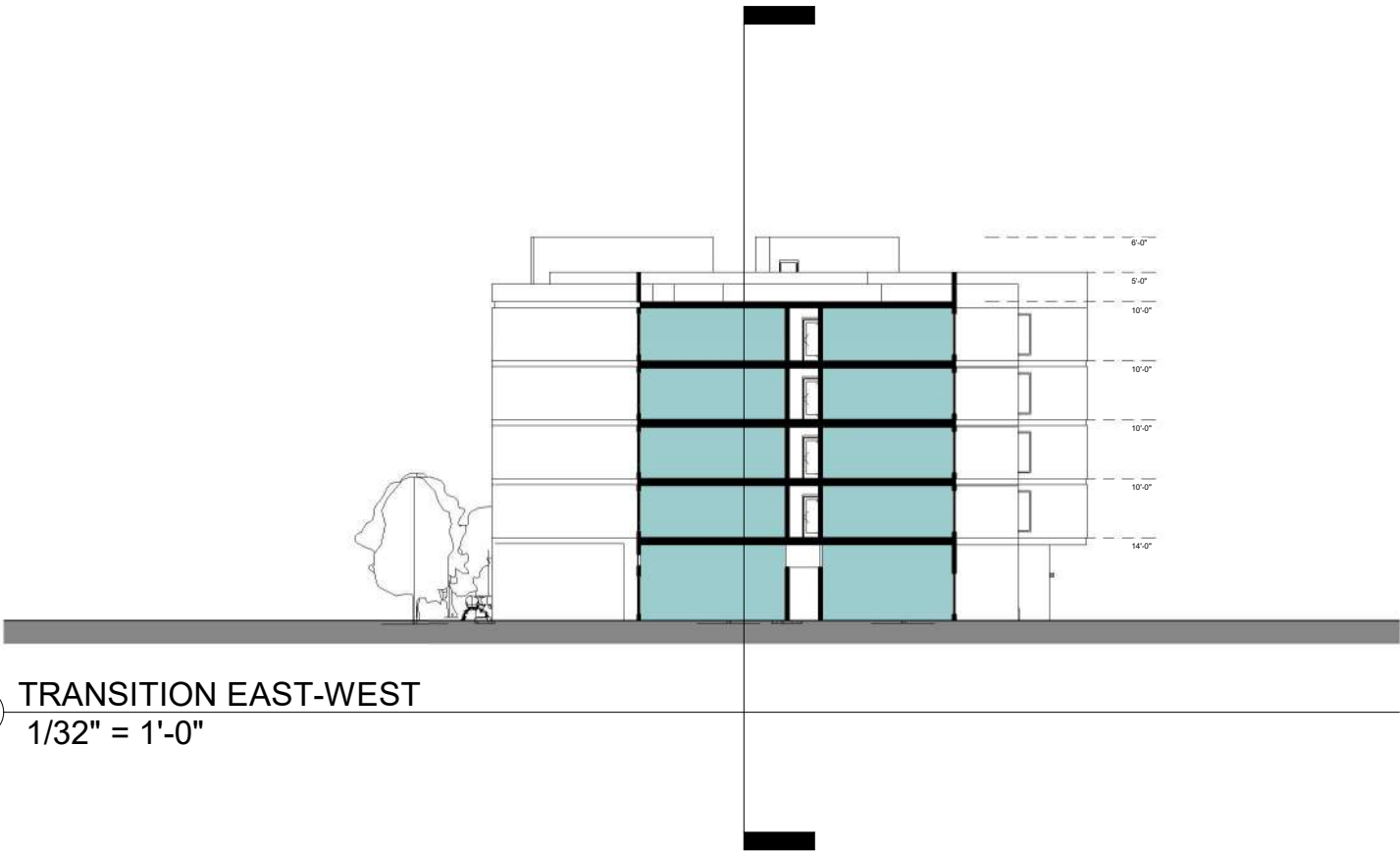
2 SECOND FLOOR PLAN  
1/32" = 1'-0"

- Lobby & Amenity
- Commercial
- Residential Units
- Utilities & services
- Corridor & Vertical circulation





1 THIRD TO FIFTH FLOOR PLAN  
1/32" = 1'-0"



2 TRANSITION EAST-WEST  
1/32" = 1'-0"



3 TRANSITION SOUTH-NORTH  
1/32" = 1'-0"

- Lobby & Amenity
- Commercial
- Residential Units
- Utilities & services
- Corridor & Vertical circulation



7 MASSING OPTIONS - TRANSITIONING MASSES  
SHADING STUDIES



DEC 21, 10 AM



SEP/MARCH 21, 10 AM



JUNE 21, 10 AM



DEC 21, 12 PM



SEP/MARCH 21, 12 PM



JUNE 21, 12 PM



DEC 21, 2 PM



SEP/MARCH 21, 2 PM



JUNE 21, 2 PM



## PARALLEL COMMERCIAL - PREFERRED

This approach considers the benefits and impacts of aligning the primary masses of the building with the orientation of MLK.

### Overview

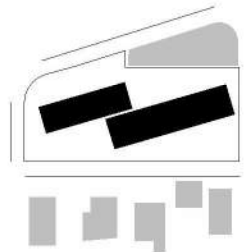
- Approx Total area +/- 54,000 SF
- 125 Residential Units
- Commercial Space +/- 14,000 SF
- Bike Parking Stalls +75

### Pros

- The massing responds to the site lot line along MLK more closely associating the project with other transit oriented developments in the area.
- The main residential entry on the northeast side of the site increases pedestrian activity along MLK.
- The massing creates two interesting yard spaces along 38th Avenue S which takes advantage of the southwest orientation and provides buffering next to the adjacent single family uses.

### Cons

- The massing does not match the orientation of buildings west of it.



NW BIRDSEYE VIEW



SE BIRDSEYE VIEW



## 7 MASSING OPTIONS -PARALLEL COMMERCIAL

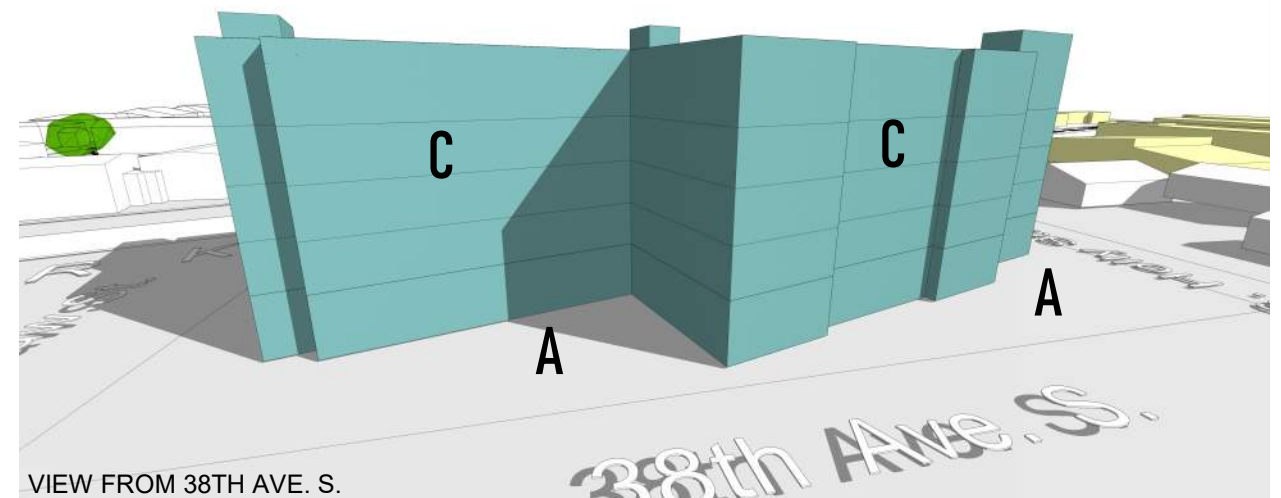
### STREET PERSPECTIVES



**A** RESIDENTIAL OUTDOOR SPACE



**B** LIVING UNIT BALCONIES



VIEW FROM 38TH AVE. S.



**C** RESIDENTIAL FACADE AT 38TH STREET



**D** LOBBY MAIN ENTRANCE



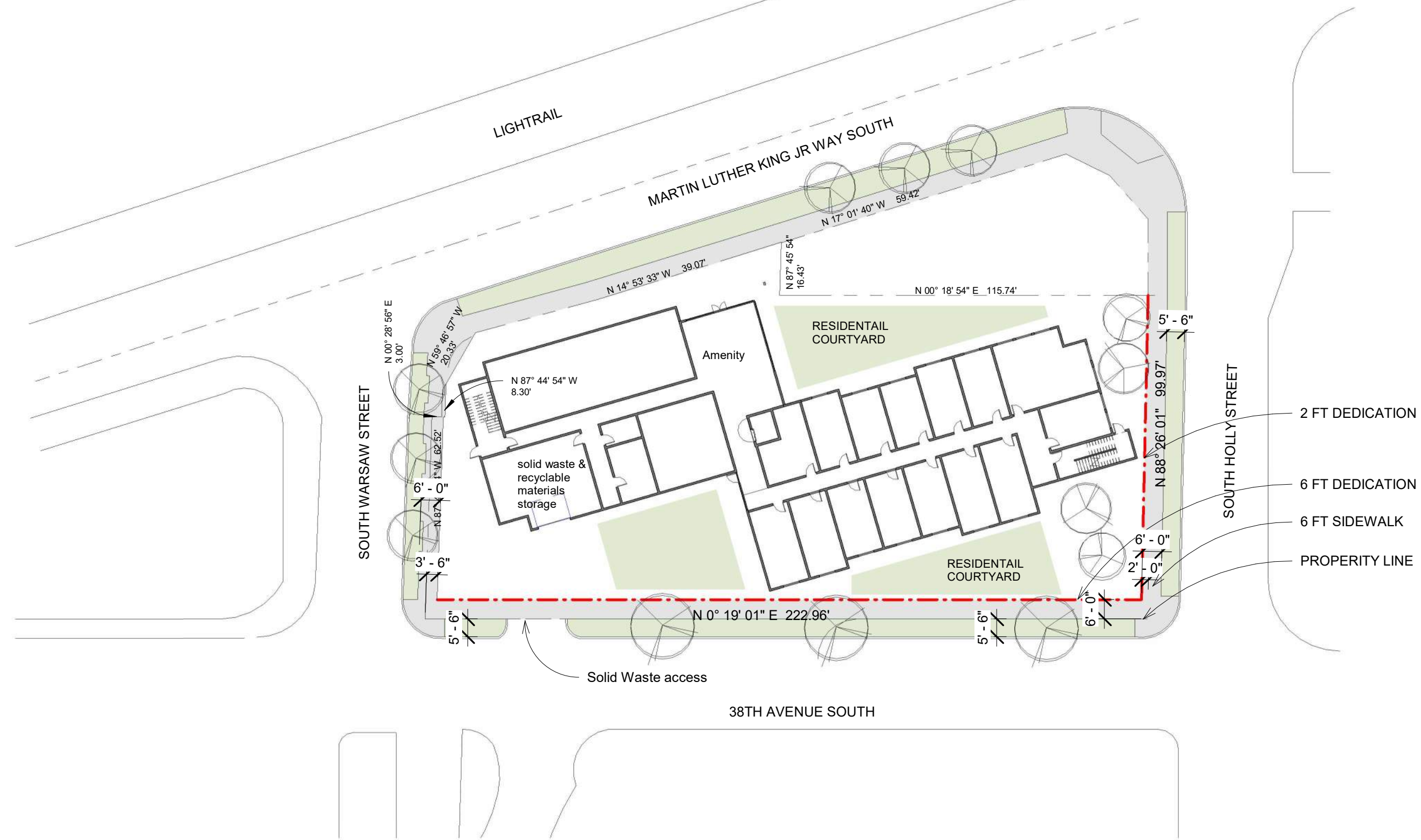
VIEW S. WARSAW ST. AND MLK JR. WAY S.



VIEW FROM MLK JR. WAY S.



7 MASSING OPTIONS -PARALLEL COMMERCIAL  
COMPOSITE SITE PLAN

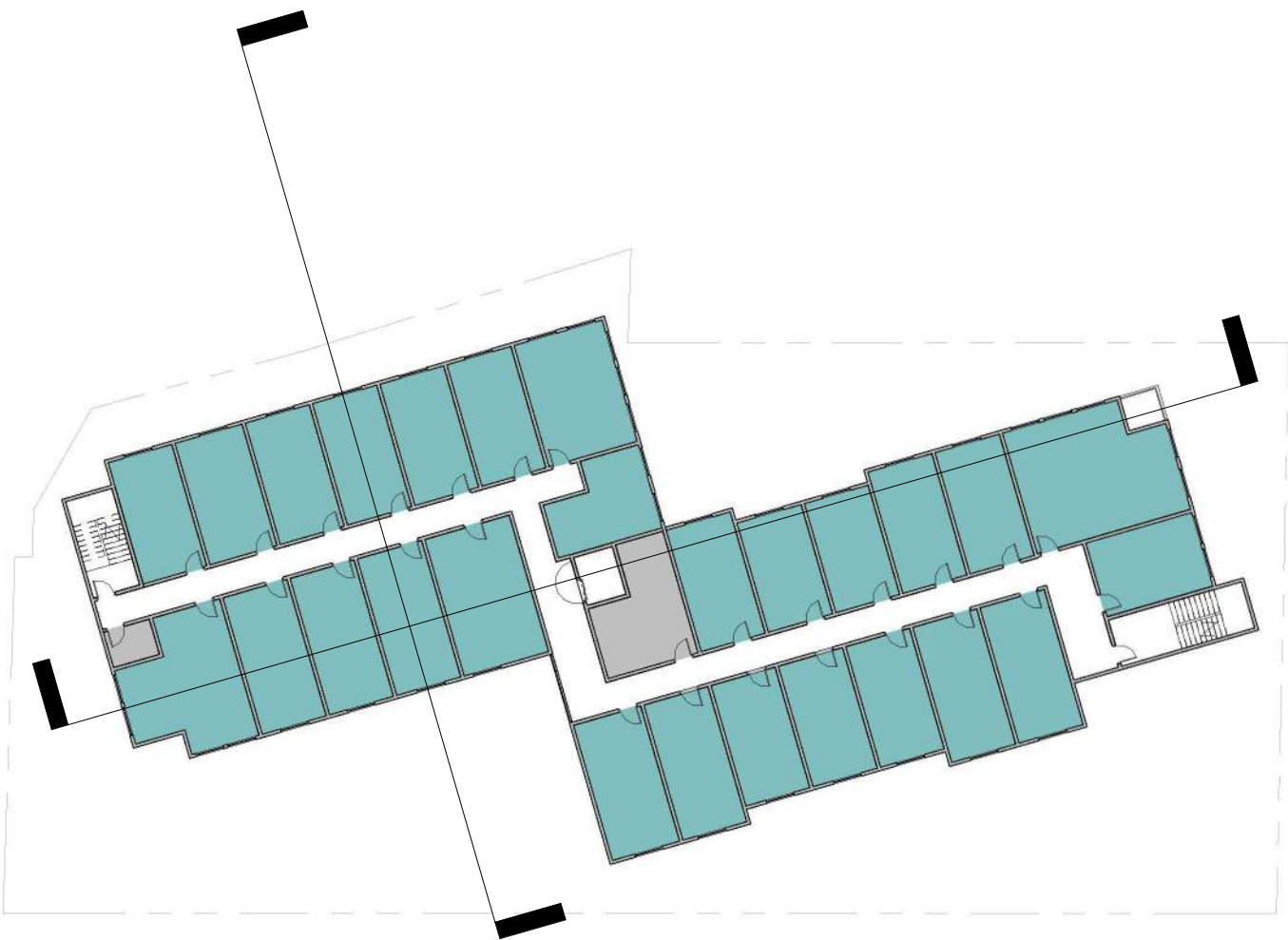


1 SITE PLAN  
1/32" = 1'-0"





1 FIRST FLOOR PLAN  
1/32" = 1'-0"

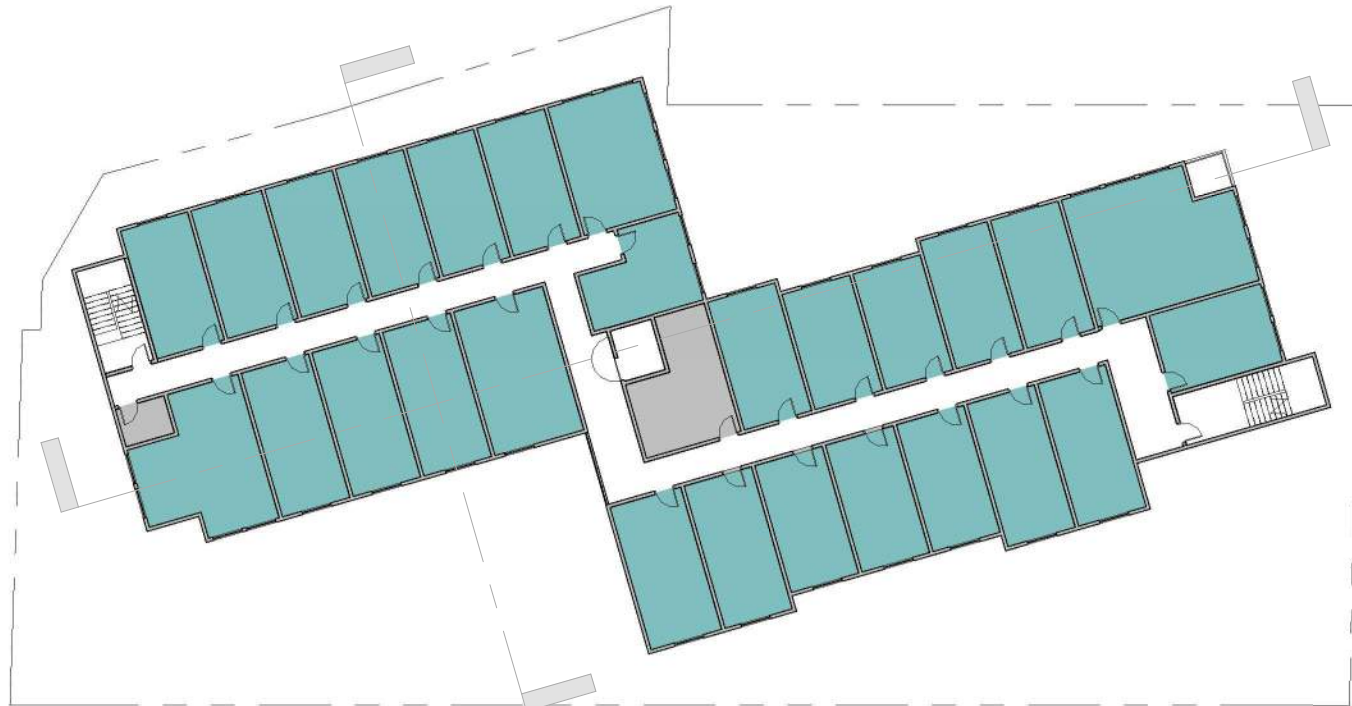


2 SECOND FLOOR PLAN  
1/32" = 1'-0"

- Lobby & Amenity
- Commercial
- Residential Units
- Utilities & services
- Corridor & circulation



7 MASSING OPTIONS -PARALLEL COMMERCIAL  
FLOOR PLANS + SECTIONS



① THIRD TO FIFTH FLOOR PLAN  
1/32" = 1'-0"



② NORTHEAST- NORTHWEST SECTION  
1/32" = 1'-0"



③ PARALLEL NORTHWEST-SOUTHEAST  
1/32" = 1'-0"

- Lobby & Amenity
- Commercial
- Residential Units
- Utilities & services
- Corridor & circulation





DEC 21, 10 AM



SEP/MARCH 21, 10 AM



JUNE 21, 10 AM



DEC 21, 12 PM



SEP/MARCH 21, 12 PM



JUNE 21, 12 PM



DEC 21, 2 PM



SEP/MARCH 21, 2 PM



JUNE 21, 2 PM