



ALKI
BEACH

1116 ALKI AVE. SW.

SEATTLE, WA 98116

Early Design Guidance: 3037493-EG

Submittal date: 03/21/2022

Meeting date: 04/07/2022

M Z A

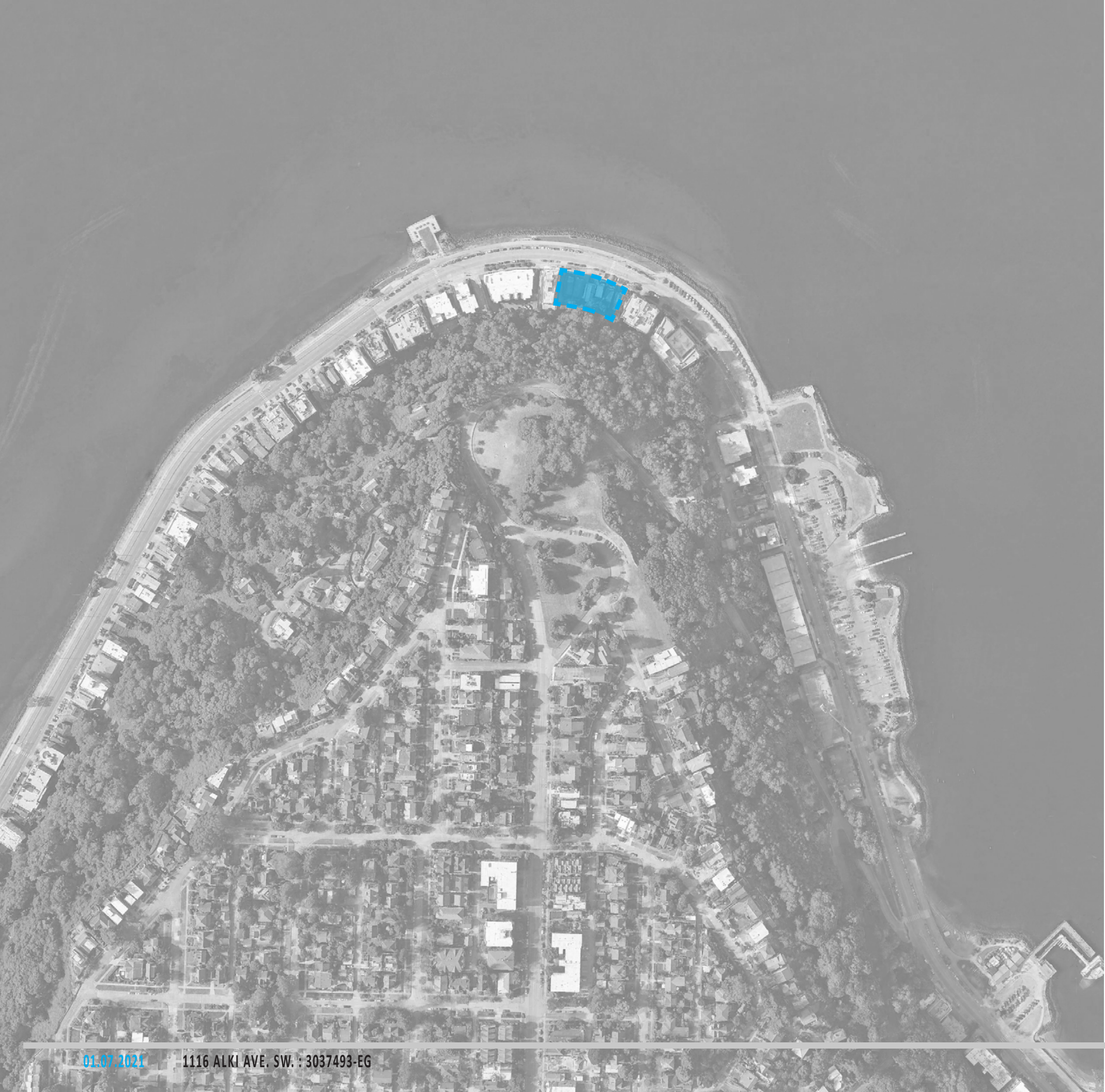


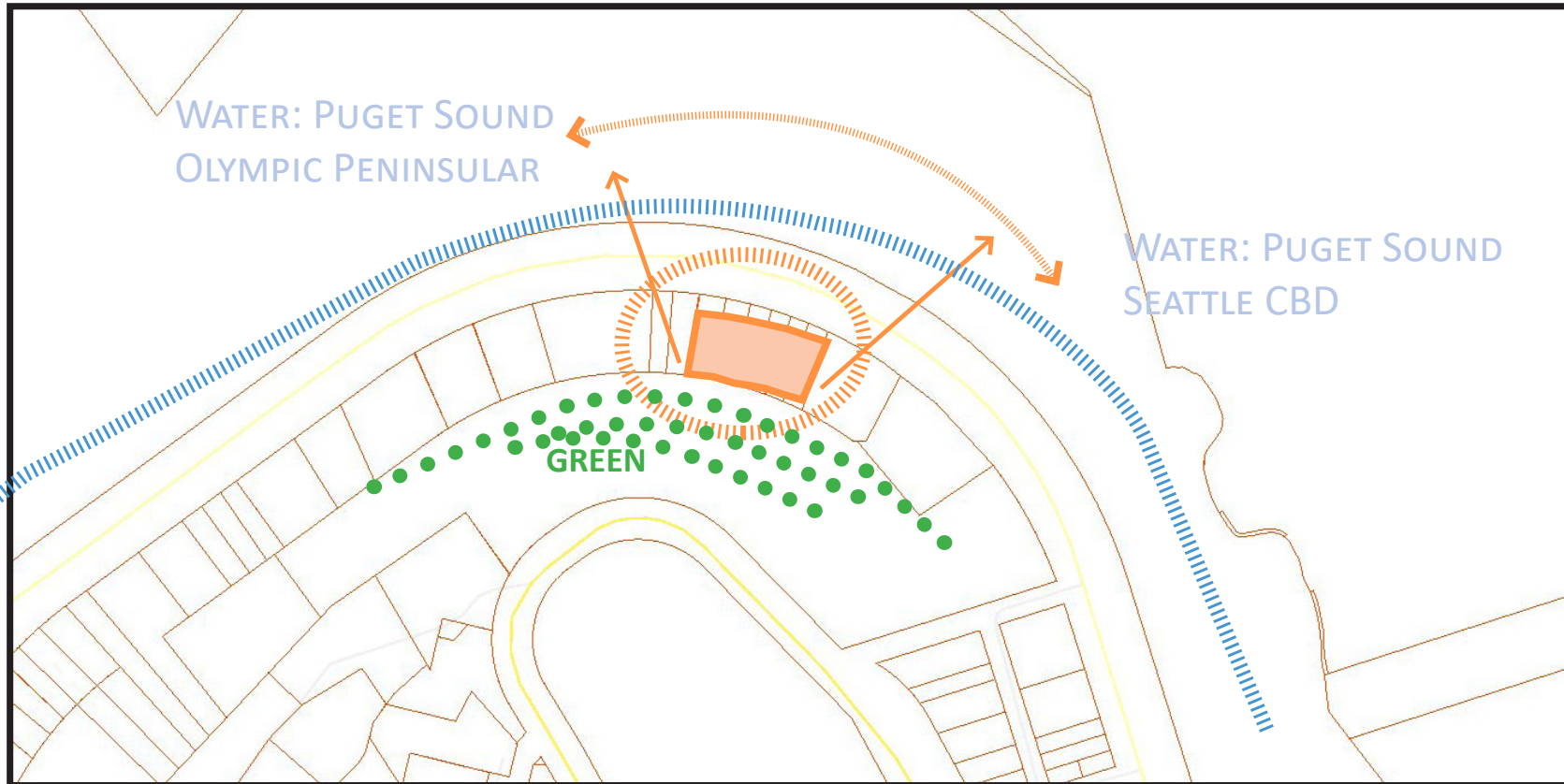
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PROJECT TEAM

Owner/Developer: N & M
Architect: MZA Architecture
Civil Engineer : Latitude-48





EXISTING SITE

The project is located in the Alki neighborhood of West Seattle, at the western side of Duwamish Head. The site is comprised of six parcels along Alki Avenue SW that face Puget Sound. One mile to the Southwest is Alki Beach and three miles to the east is the west Seattle bridge. Behind the site parcels rises a steep hillside with residential neighborhoods above, though there is no direct access from the site itself or within close proximity. Alki Avenue curves along the front of the site, creating a slightly wedge-shaped parcel. The buildings currently on the site consist of 1-2 story wood frame single family structures that are in poor to fair conditions. The site is relatively flat along the Alki Avenue SW frontage at an elevation of approximately 16' and is rise up to 18' to the bottom of a steep forested slope up to California Way SW.

DEVELOPMENT OBJECTIVES

Alki Beach 1116 continues the trend of mid-rise residential development in the area and celebrates the natural beauty of the site, namely Puget Sound, the views of the Seattle skyline, and the densely vegetated steep hillside behind the site. The building's orientation is intended to maximize views of the water and the Seattle skyline NE of the site. At ground level, a private garden viewed from the units above echoes the green, wooded hillside on the south side of the site, and at roof level, a private observation deck offers views of the water and city for all residents to enjoy.

PROPOSAL INFORMATION OF THE PREFERRED DESIGN OPTION

- 58 Residential Units
- Six stories including five levels of residences over a ground floor of lobby space, utility areas, parking and additional residences
- 71 parking stalls for residents at the ground floor level and in a below grade garage
- Total gross floor area is around 84,000 sf

NOTABLE FEATURES

- Generous amenity spaces for residents
- Rooftop sanctuary
- Landscaped entries to level 1 units
- Solar panel array on the rooftop (future)
- Ample bike and recreation equipment storage for residents

Outreach Documentation: 1001-1116 Alki Ave

SUMMARY OF OUTREACH

Cascadia Consulting Group conducted the following outreach activities on behalf of MZA Architecture as part of the Department of Neighborhood's (DON) Early Design Guidance process. Cascadia's approved outreach plan is included as an appendix to this document.

1. Project Webpage (Multi-Prong Method)

The project website launched on April 12, 2021. The website included a project overview, design approach, hotline information, and the feedback survey. The project website had 219 unique visitors who viewed the site a total of 278 times.

2. Project Hotline (High-Impact Method)

The project hotline phone number launched on April 12, 2021 and appeared on web and print materials. We received two voicemails during the project inquiring about the survey. The project email address was also on web and print materials. We used the same address to send emails to community organizations. We received one email from a community organization and one email from a resident in the area requesting technical assistance for the online survey. The voicemails received were from the same community organization and resident that sent emails.

3. Online Survey (High-Impact Method)

The online survey was embedded into the project webpage. It consisted of seven questions with a combination of multiple-choice and open-ended response formats. The survey was open from April 12 to May 4, 2021. We received 70 responses.

4. Email to Community Organizations (Multi-Prong Method)

On April 12, we sent an email to twelve community organizations inviting them to provide feedback on the project. The email included a PDF of the direct-mail flyer.

5. Direct Mailing (High-Impact Method)

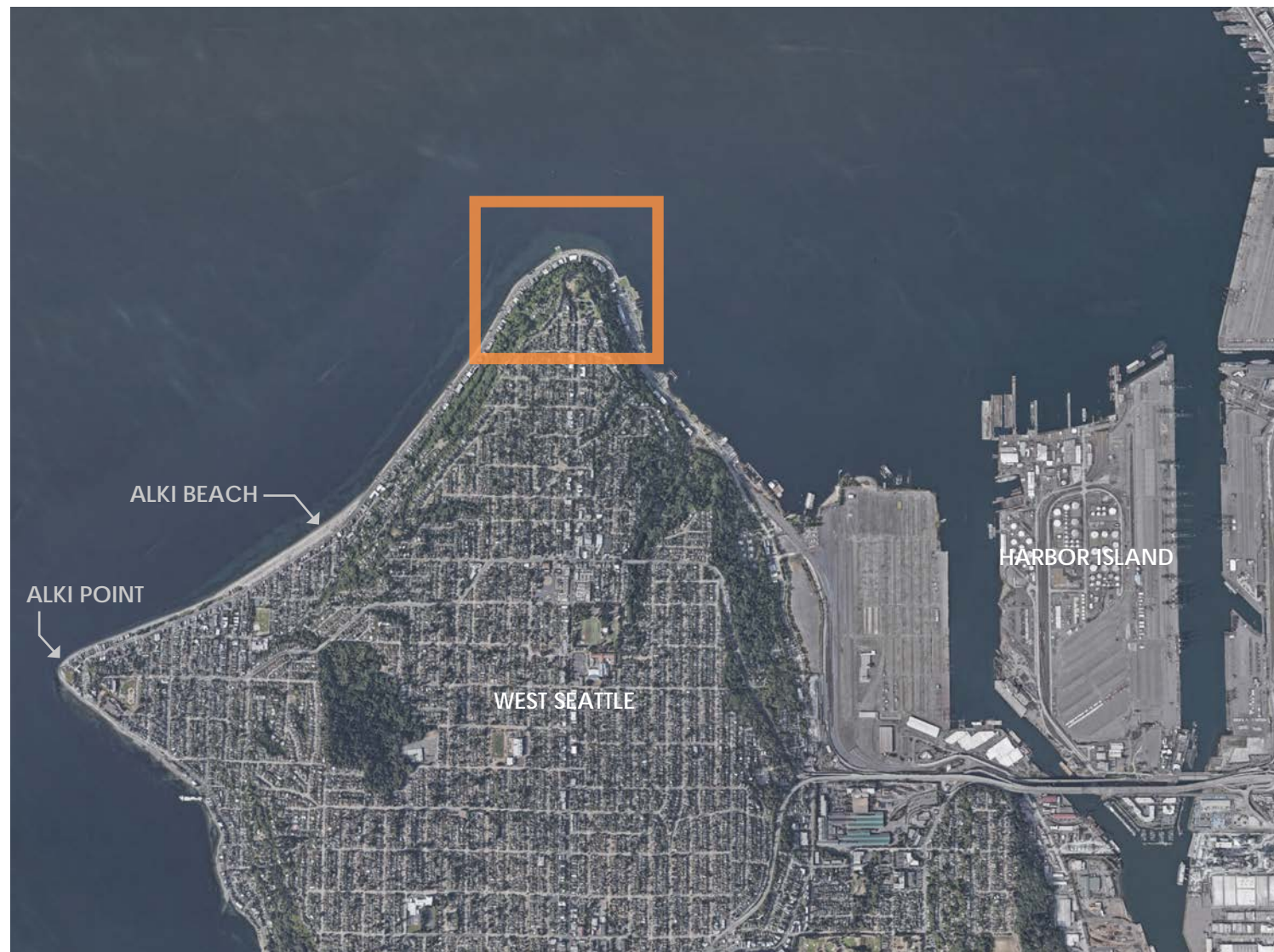
We promoted the project website and the online survey through a direct-mail flyer sent to households within a 500-ft radius of the project site. We sent the mailing to recipients on April 12, 2021.

SUMMARY OF COMMUNITY FEEDBACK

All project feedback was captured through the online survey (see appendices for a detailed response summary and individual responses). Most of the individuals who responded live very close to the proposed project site. The majority of respondents heard about the project through email or "other", which mostly included sources such as West Seattle Blog and HOA meetings. Some of the key themes from the feedback include:

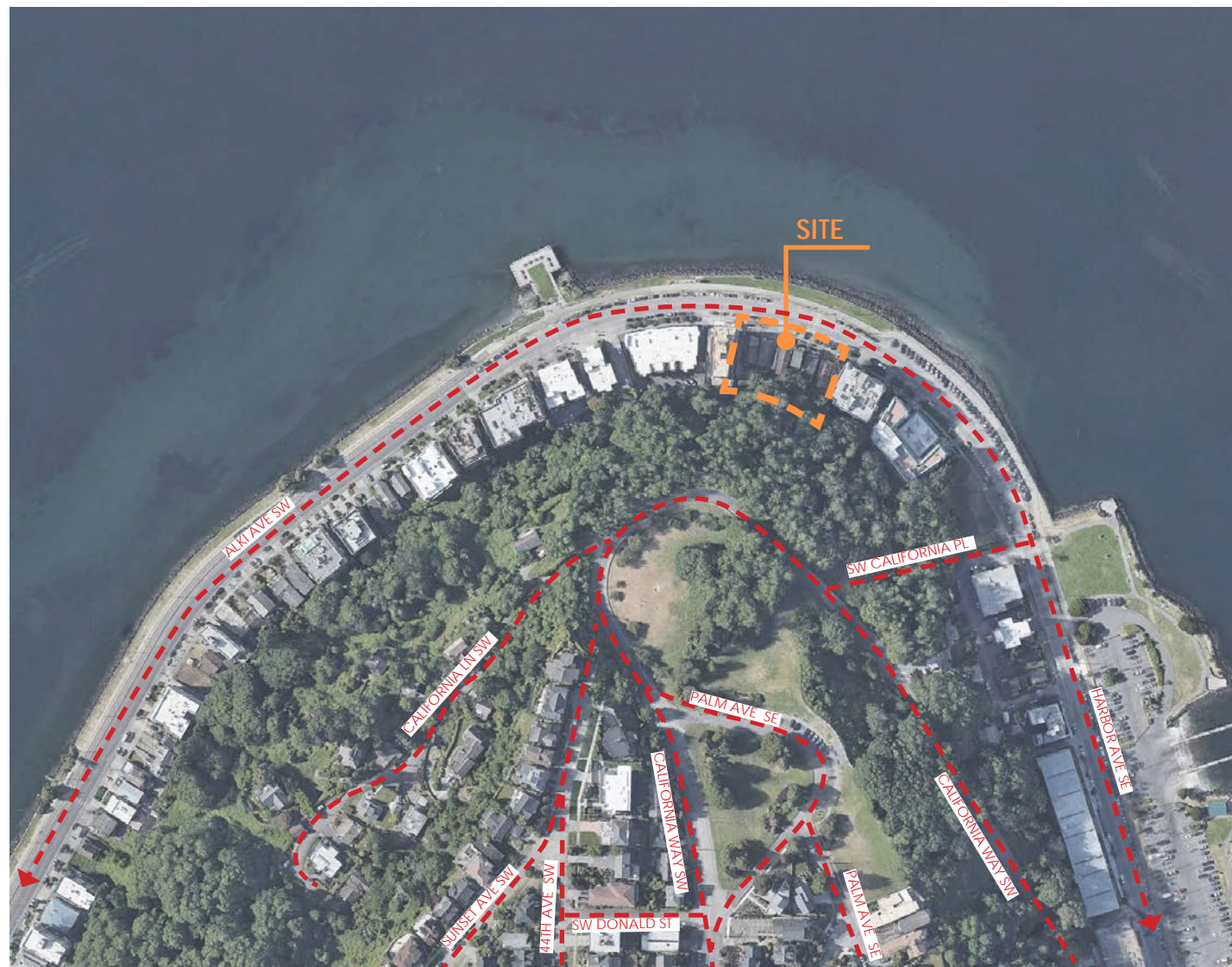
- **Interest in sustainability and aesthetic.** When asked, "What is most important to you about a new building?" one third of respondents (20 out of 58) hoped the property design will consider environmental sustainability and nearly another third of respondents (17 out of 58) hope that it is nice looking. Many respondents noted the importance of adopting the existing Alki aesthetic into the design.

- **Concerns about parking and congestion.** Nearly all respondents noted concern that the building will make driving and parking in the neighborhood more difficult (48 out of 58). Some respondents commented that ample off-street parking should be provided for tenants and others specifically suggested providing two parking spaces per unit. Nearly two thirds of respondents (36 out of 59) noted concern about construction impacts. Respondents pointed out that traffic is already an issue and expressed concern that this development will add to the problem. Several respondents suggested off-street parking or a shuttle for construction crews to minimize impacts to parking and congestion.
- **Concerns of scale and design.** Approximately two thirds (41 of 58) of respondents are concerned that the building may feel out of scale with other buildings nearby and one third (17 of 58) are concerned they will not like the way it looks. Several respondents expressed that will be too high-density for the area and worry that the design will not fit with the neighborhood character by "overwhelming" the current look and feel. Another respondent worried a high-density building will contribute further to parking congestion and noise. Some respondents recommended reducing the total number of units and designing a medium-density building.
- **Preference for tenant-owned units.** When asked about priorities, preferences, and/or concerns about the development, some respondents hope that the units be tenant-owned condominiums, rather than apartment rentals. Several respondents noted that condos would be more fitting with existing neighboring buildings and surrounding community.
- **A focus on green space.** Some respondents mentioned that they would like to see more green space, such as a buffer of trees and lawn between the building and sidewalk. Respondents see Alki Avenue as a beautiful and special area for the community and hope to maintain the natural views and open space. A few respondents mentioned concern for potential landslides behind the building and one respondent suggested leaving as many trees in place as possible to reduce that risk.
- **Interest in including retail and/or amenities space.** Nearly one quarter (13 out of 58) of respondents feel it is important that the development bring new services or amenities to the area. One respondent said the area would greatly benefit from a high-end café and other pointed out that the proximity to Luna Park offers business opportunity associated with the water taxi and boat launch.

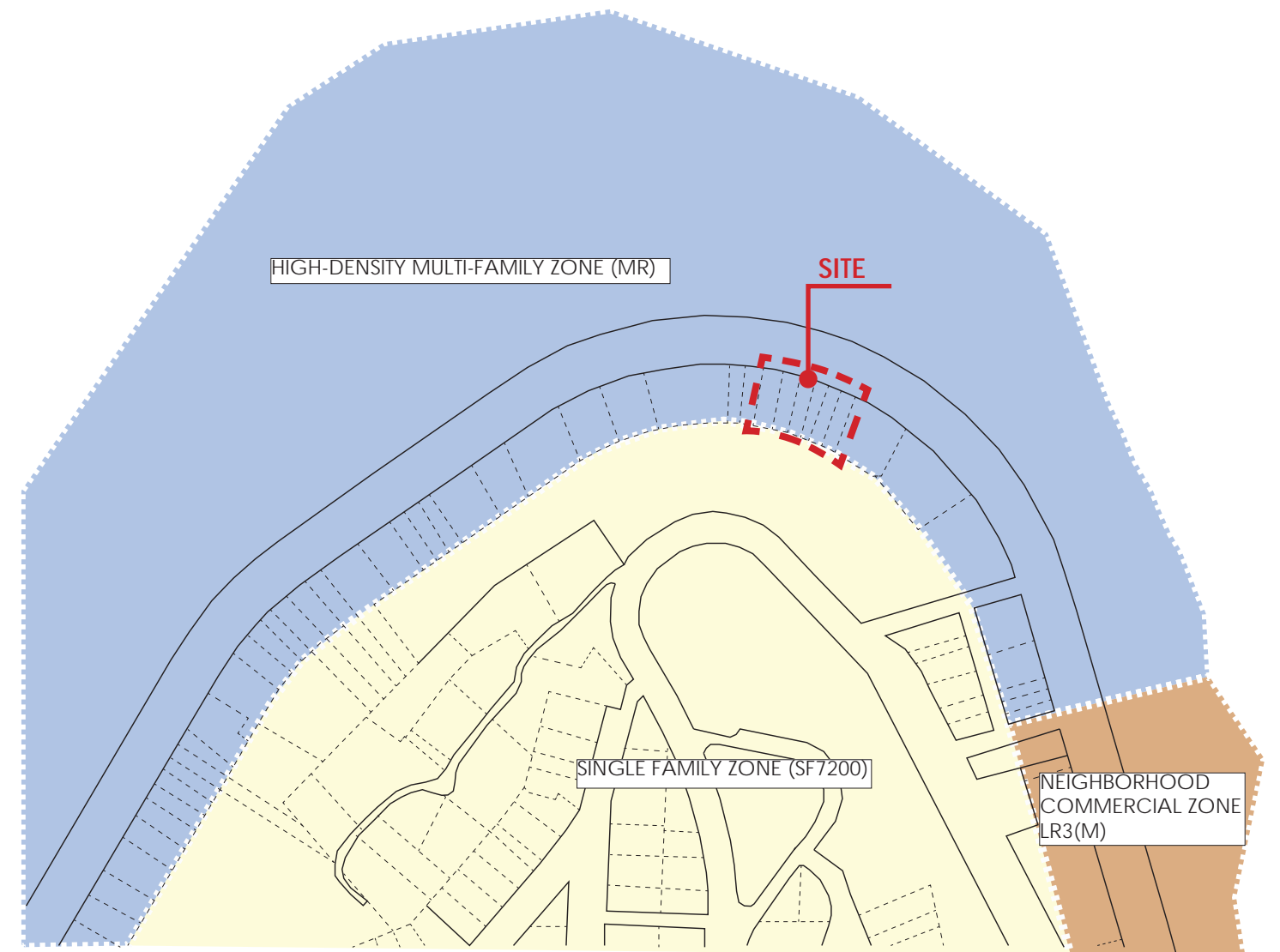


VICINITY MAP

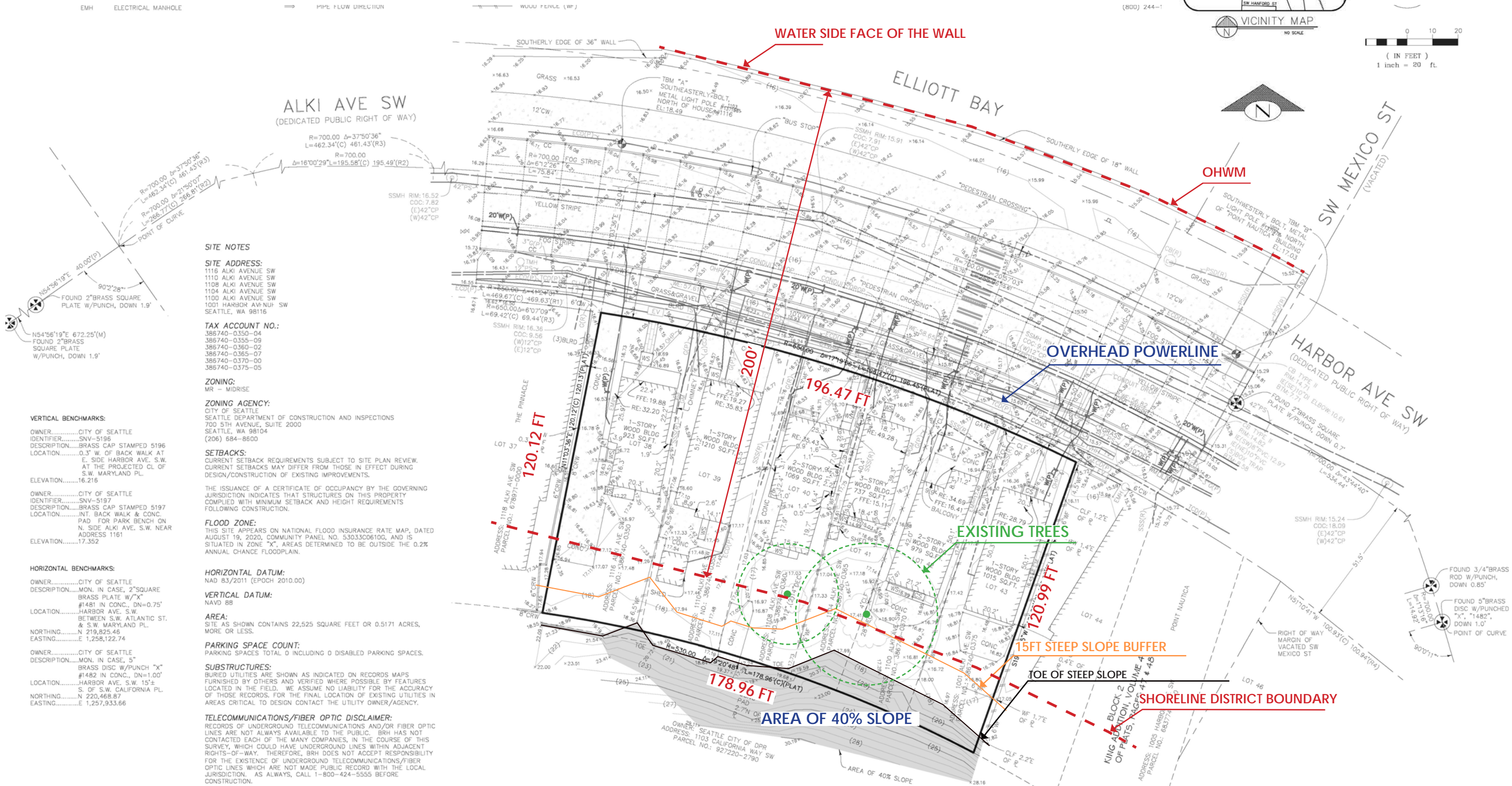




VICINITY MAP



ZONING OVERLAY





Tree Assessment Matrix
MZA Architecture
1116-1001 Alki Ave.
Seattle

Urban Forestry Services | Bartlett Consulting
15119 McLean Road
Mount Vernon, WA 98273

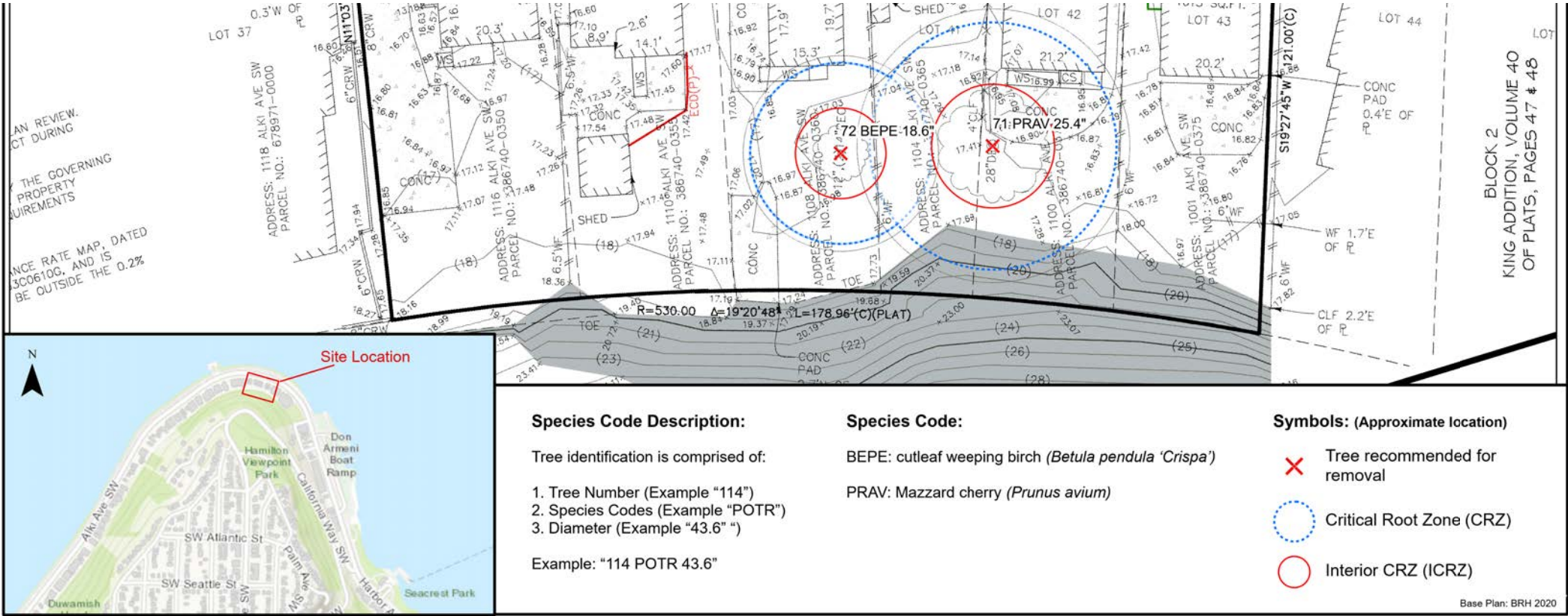


Tree Assessment Matrix
MZA Architecture
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Tree	Species	DBH (in)	Drip Rad.	CRZ (ft)	Vigor	Structure	Risk	Pres Value	Recommendations
71	Mazzard cherry <i>Prunus avium</i>	25.4	18.3	25.4	Fair to Good	Fair	Low	Medium Significant	Remove tree - Direct Construction Impacts
Notes/ Defects	This tree has two stems that originate at roughly 1-foot from the ground but have become intertwined/fused together to make one stem. There is a motorcycle strap attached to the south most stem at roughly 14 feet in height on main stem. the strap is constricting the stem. It is unclear what the strap is for. The crown of the tree is asymmetrical. The tree is within 1 1/2 feet of the fence to the west. The likelihood of complete failure at the main stem or roots or significant branch failure in a three-year time frame for this tree is <i>improbable</i> . The likelihood of the tree or its parts impacting any targets, which include homes to the north is <i>medium</i> . The consequences of failure and impact to a target would be negligible to significant, contingent on the proximity of the target to the tree. Based on these metrics, this tree receives a risk rating of <i>low</i> .								

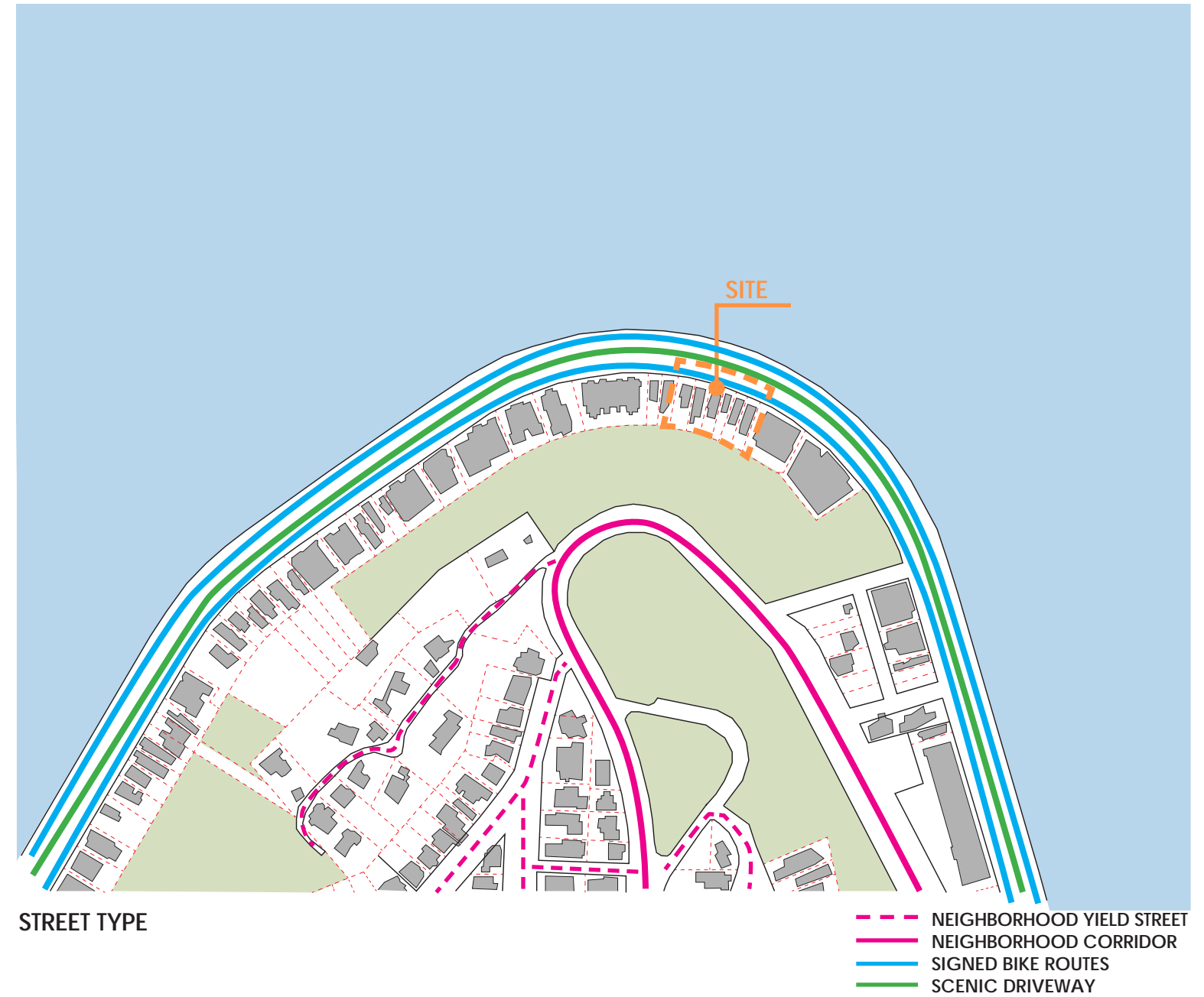
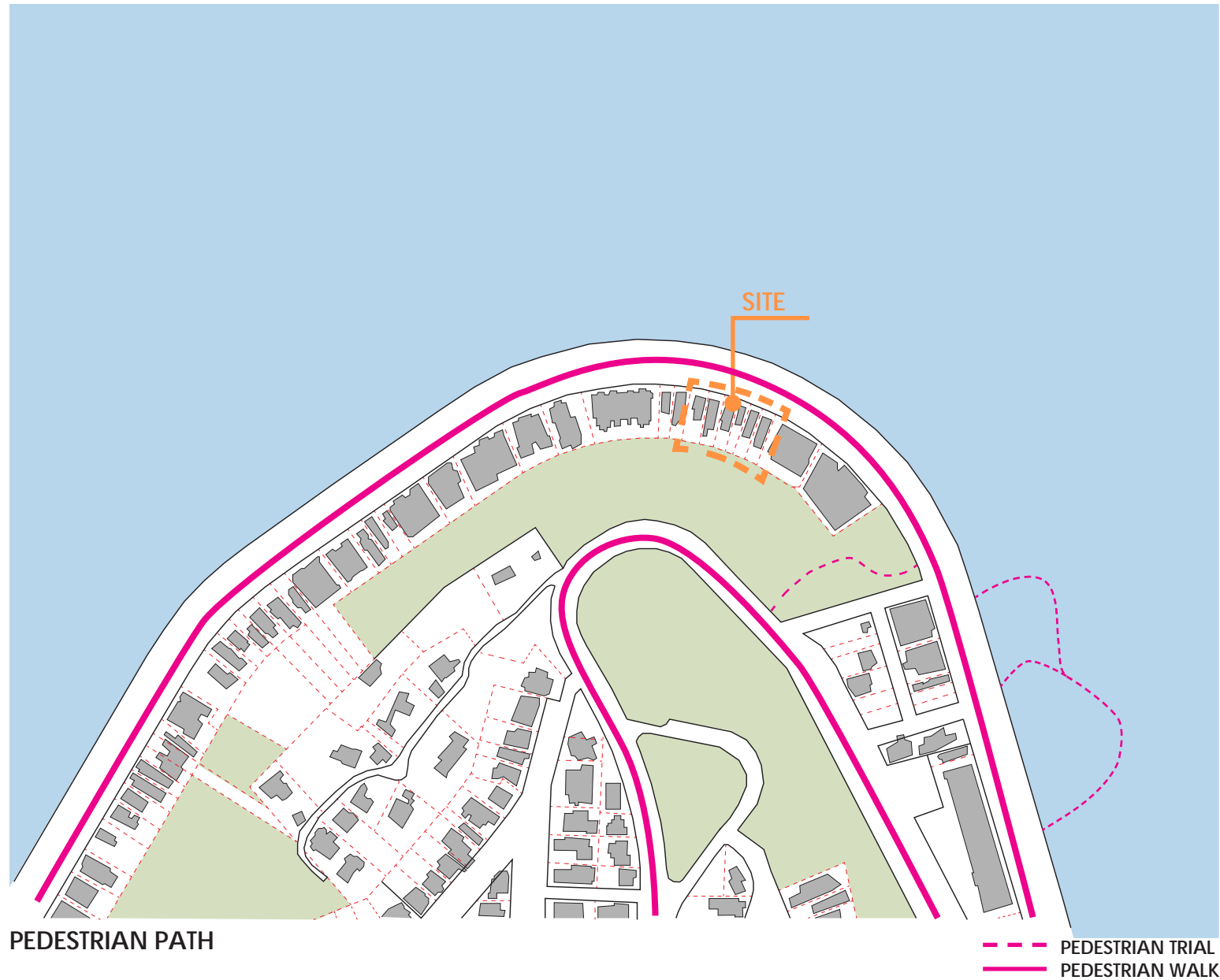
Tree	Species	DBH (in)	Drip Rad.	CRZ (ft)	Vigor	Structure	Risk	Pres Value	Recommendations
72	cutleaf weeping birch (<i>Betula pendula</i> 'Crispa')	18.6	19.5	18.6	Fair to Good	Fair to Good	Low	Medium	Remove tree - Direct Construction Impacts
Notes/ Defects	This cut leaf birch cultivar is Exceptional as defined by the Seattle Directors Rule 16-2008. The tree is a multi-stem specimen consisting of three stems that originate at the ground. This tree contains vigorous interior shoot growth and witches' broom formations. The likelihood of complete failure at the main stem or roots or significant branch failure in a three-year time frame for this tree is <i>improbable</i> . The likelihood of the tree or its parts impacting any targets, which include homes to the north is <i>medium</i> . The consequences of failure and impact to a target would be negligible to significant, contingent on the proximity of the target to the tree. Based on these metrics, this tree receives a risk rating of <i>low</i> .								

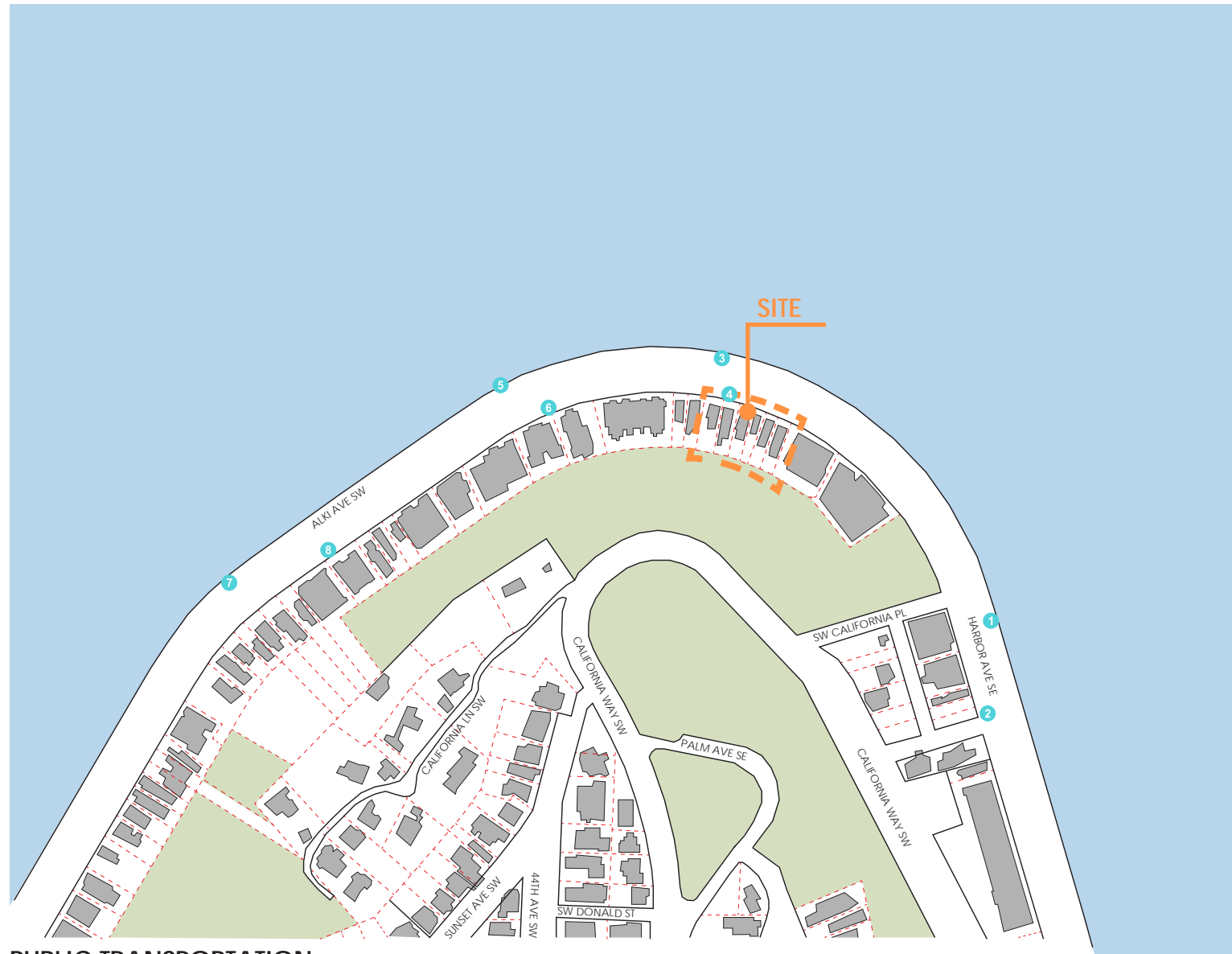




AERIAL







PUBLIC TRANSPORTATION

- 1 Harbor Ave SW & SW Maryland Pl
Bus 37
- 2 Harbor Ave SW & SW Maryland Pl
Bus 37, 775
- 3 Alki Ave SW & Harbor Ave SW
Bus 37
- 4 Alki Ave SW & Harbor Ave SW
Bus 37, 775
- 5 Alki Ave SW & 1000 Block
Bus 37
- 6 Alki Ave SW & 1000 Block
Bus 37, 775
- 7 Alki Ave SW & 1200 Block
Bus 37
- 8 Alki Ave SW & 1200 Block
Bus 37, 775



OUTDOOR PARKS

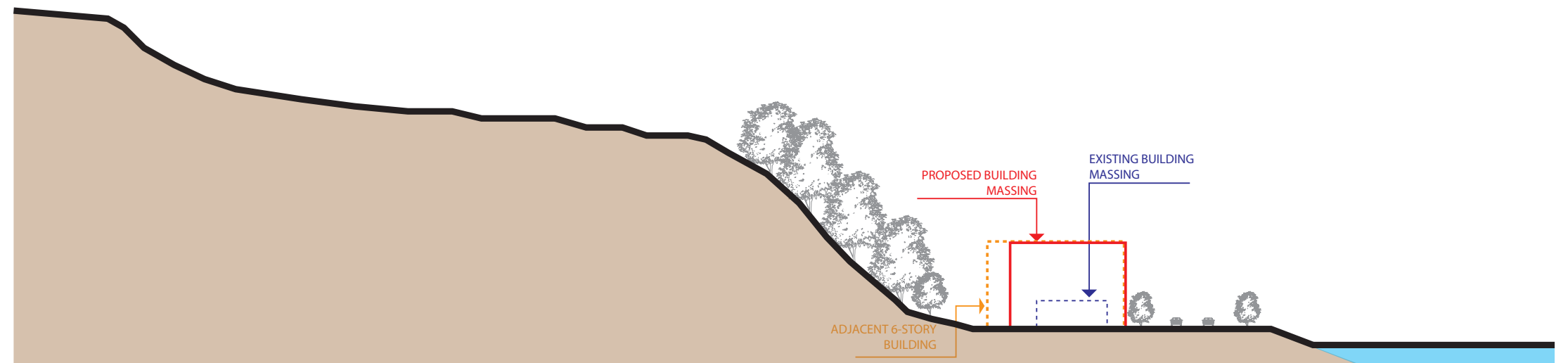
- 1 Alki Beach Park
- 2 Don Ameni Park
- 3 Hamilton Viewport Park
- 4 Duwamish Head Greenbelt
- 5 Luna Park



ALKI DRIVE SW STREETSCAPE



VIEW FROM CALIFORNIA WAY SW



SITE SECTION



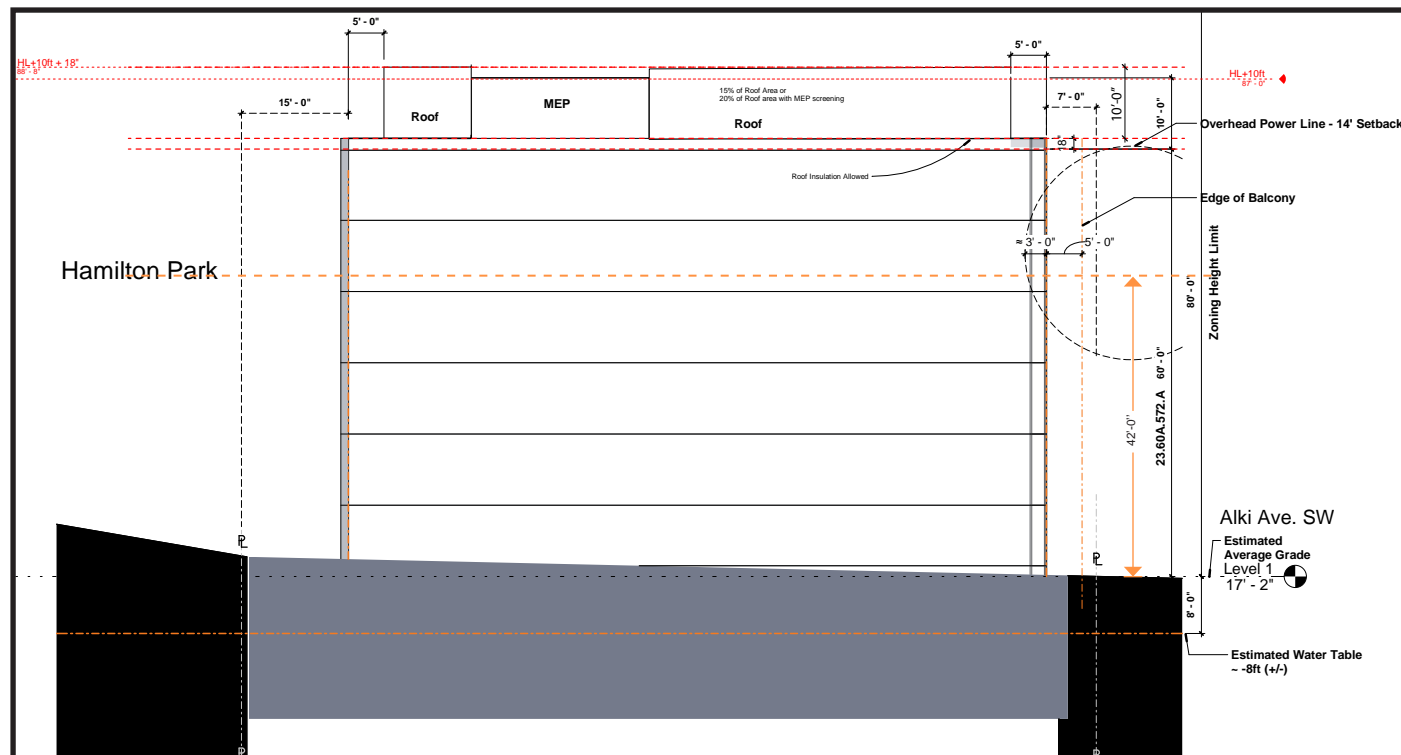
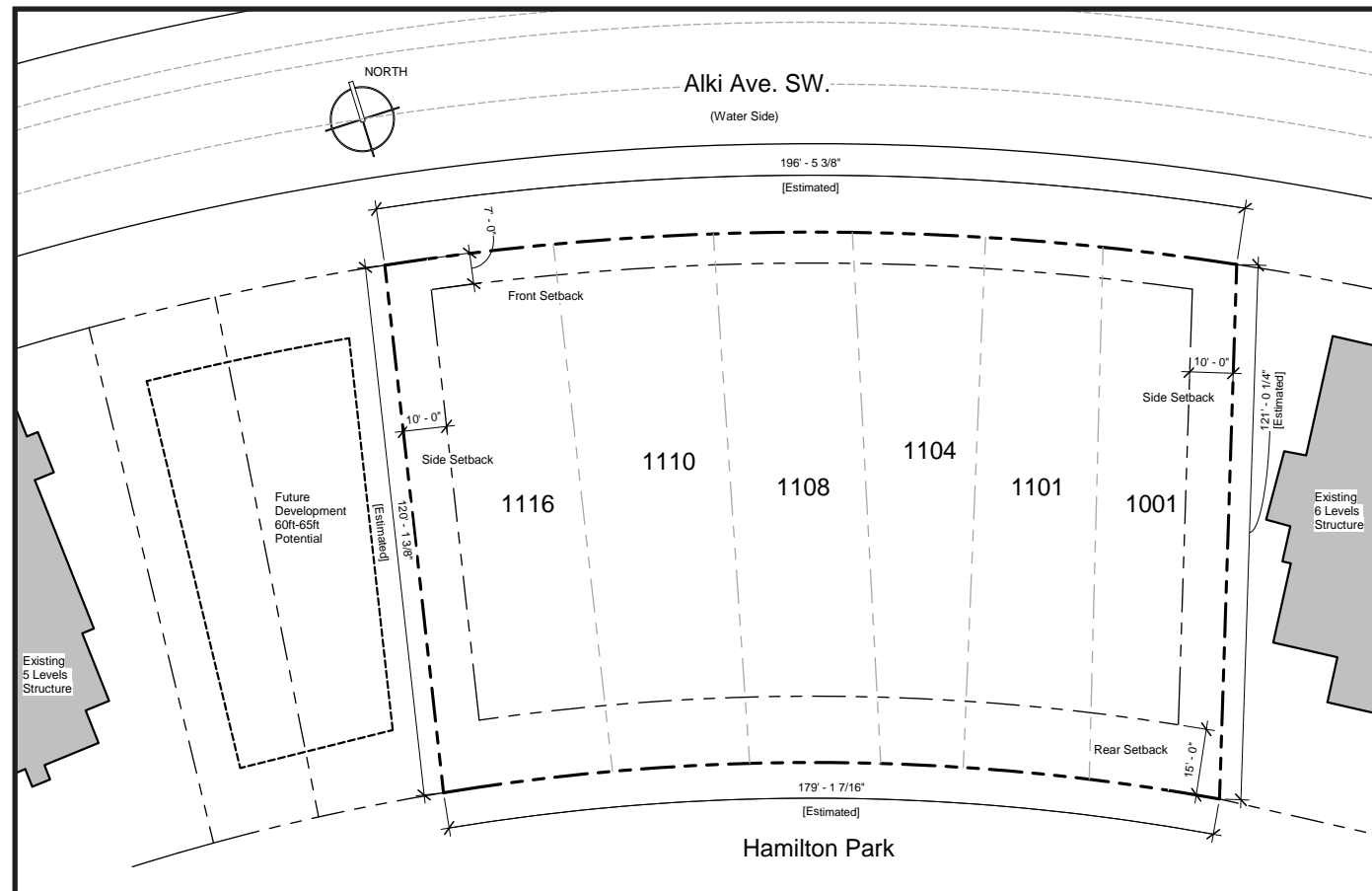
ZONING DATA

Project Name	Alki Multi Family
Client	N & M
Address	1116,1110,1108,1104,1100,1001 Alki Ave. SW
Jurisdiction	City of Seattle
Codes Enforced	SMC Chapter 23
Parcel Number(s)	#3867400350, #3867400355, #3867400360, #3867400365, #3867400370, #3867400375
Zoning	MR(M) - Multifamily Residential Zone - Midrise, MHA applied
Overlay Zoning	UR(Urban Residential) Shoreline Jurisdiction, Alki Parking Overlay(AL)
Land Use Notes	Alki parking district, Airport height overlay - outer transitional surface, Infiltration evaluation required
Environmentally	UR-AL (Urban Residential)
Critical Areas	Potential Slide, Known Slide, Liquefaction, Steep Slope
Site Area	22,525 sf

LAND USE				
Code Section	Title	Zoning Requirements	Proposal / Comments / Questions	LEVEL
23.45	MULTIFAMILY			
23.45.504.E	Permitted and prohibited uses	Ground Floor Commercial Permitted		
23.45.510.A	Gross floor area	In multifamily zones, gross floor area includes exterior corridors, breezeways, and stairways that provide building circulation and access to dwelling units or sleeping rooms. Balconies, patios, and decks that are associated with a single dwelling unit or sleeping room and that are not used for common circulation, and ground-level walking paths, are not considered gross floor area.		
23.45.510.B	FAR limits in LR and MR Zone	MR Zone:3.2 Zones with an MHA suffix: 4.5		
23.45.510.D	Exempt from FAR	1. All stories, or portions of stories, that are underground. 4.Portions of a story that extend no more than 4 feet above existing or finished grade, whichever is lower. 7. As an allowance for mechanical equipment, in any structure more than 85 feet in height, 3.5 percent of the gross floor area that is not otherwise exempt under this subsection 23.45.510.D.	No exempt for MEP	
23.45.514.B	The height limits for MR Zone	MR Zone: 80 ft	Refer to 23.60A.572 - Height in the UR Environment for Height Requirement. 60 ft max under 23.60A.572	
23.45.514.I	Rooftop features	2. Open railings, planters, greenhouses not dedicated to food production, parapets, and firewalls on the roofs of principal structures may extend 4 feet above the maximum height limit. 3. Architectural projections that result in additional interior space, such as dormers, skylights, and clerestories,the projections may extend 4 feet above the maximum height limit if the following requirements are met: a) The total area of the projections is no more than 30 percent of the area of the roof plane; b) The projections are set back at least 4 feet from any street facing facade. 5. In MR and HR zones, the following rooftop features may extend 15 feet above the applicable height limit , if the combined total coverage of all features does not exceed 20 percent of the roof area, or 25 percent of the roof area if the total includes screened mechanical equipment: a) Stair penthouses, except as provided in subsection 23.45.514.I.6; b) Mechanical equipment; 6. Subject to the roof coverage limits in subsections 23.45.514.I.4 and 23.45.514.I.5, elevator penthouses may extend above the applicable height limit up to 16 feet. Stair penthouses may be the same height as an elevator penthouse if the elevator and stairs are co-located within a common penthouse structure.		
23.45.517	Mandatory housing affordability (MHA) in multifamily zones	LR, MR, and HR zones with a mandatory housing affordability suffix are subject to the provisions of Chapters 23.58B and 23.58C.	Refer to 23.58C for residential MHA	

23.45.518.B	Setbacks and separations - MR Zone	1. Front and side setback from street lot line: 7 ft average; 5 ft min Rear setback: 15 ft from a rear lot line, not abut an alley Side setback from interior lot line: 42 ft or less in height - 7 ft average, 5 ft min; Above 42 ft - 10 ft average, 7 ft min		
23.45.518.H	Projections permitted in required setbacks and separations	1. Cornices, eaves, gutters, roofs, and other forms of weather protection may project into required setbacks and separations a maximum of 4 feet if they are no closer than 3 feet to any lot line. 7. Unenclosed decks and balconies may project a maximum of 4 feet into required setbacks if each one is: a) No closer than 5 feet to any lot line; b) No more than 20 feet wide; c) Separated from other decks and balconies on the same facade of the structure by a distance equal to at least 1/2 the width of the projection.		
23.45.522.C	Amenity area required in MR zone	5 percent of the total gross floor area of a structure in residential use		
23.45.522.D	Amenity general requirement	2b.No more than 50 percent of the amenity area may be enclosed, and this enclosed area shall be provided as common amenity area.		
23.45.528	Structure width and depth limits for lot greater than 9000 sf in MR zone	A. The width of principal structures shall not exceed 150 feet. B. Structure depth: 1) Not exceed 80 percent of the depth of the lot, except as provided in subsection 23.45.528.B.2. 2) Exceptions to structure depth limit. To allow for front setback averaging and courtyards as provided in Section 23.45.518, structure depth may exceed the limit set in subsection 23.45.528.B.1 if the total lot coverage resulting from the increased structure depth does not exceed the lot coverage that would have otherwise been allowed without use of the courtyard or front setback averaging provisions.	150' W x 96' D	
23.45.529.C.1	Treatment of street-facing facades - Facade Opening	1a. At least 20 percent of the area of each street-facing facade shall consist of windows and/or doors, except as provided in subsection 23.45.529.C.1		
23.45.529.C.2	Treatment of street-facing facades - Facade Articulation	b. If the street-facing facade of a structure exceeds 750 square feet in area, division of the facade into separate facade planes is required. c. a portion of the street-facing facade shall have a minimum area of 150 square feet and a maximum area of 500 square feet, and shall project or be recessed from abutting facade planes by a minimum depth of 18 inches. d. Trim that is a minimum of 0.75 inches deep and 3.5 inches wide is required to mark roof lines, porches, windows, and doors on all street-facing facades.	Director may allow exceptions for façade opening requirements and façade articulation requirements	
23.45.529.D	Treatment of side facades that are not street-facing	For the purposes of this subsection 23.45.529.D, a side facade that is not street-facing includes all vertical surfaces enclosing interior space, including gables and dormers, as shown in Exhibit A for 23.45.529, if located within 10 feet of a side lot line. 1. If the side facade of a structure that is not street-facing exceeds 1,000 square feet in area, one of the following must be met: a) A portion of the side facade with a minimum area of 250 square feet and a maximum area of 750 square feet shall project or be recessed from abutting facade planes by a minimum depth of 18 inches; or b) The side facade shall include vertical or horizontal variations in building materials or color, covering a minimum of 25 percent of the facade surface. 2.Structures shall be designed to maintain the privacy of dwelling units by minimizing placement of proposed windows where they would directly align with windows on the side facade of a structure on an abutting lot located within 20 of the side property line or by use of fencing, screening, landscaping, or translucent windows to create privacy between buildings.		
23.45.530	Green building standards	FAR thresholds for MR : 3.45	Need to meet the green building standard and shall demonstrate compliance with that commitment, all in accordance with Chapter 23.58D.	
23.45.532.A	Ground floor commercial use	2. The commercial use is permitted only on the ground floor of a structure that contains at least one dwelling unit. 3. The maximum size of use of any one business establishment is 4,000 square feet; except the maximum size of use of a multi-purpose retail sales establishment is 10,000 square feet		

23.45.534	Light and glare standards	A. Exterior lighting shall be shielded and directed away from adjacent properties. B. Interior lighting in parking garages shall be shielded to minimize nighttime glare on adjacent properties. C. To prevent vehicle lights from affecting adjacent properties, driveways and parking areas for more than two vehicles shall be screened from abutting properties by a fence or wall between 5 feet and 6 feet in height, or a solid evergreen hedge or landscaped berm at least 5 feet in height. If the elevation of the lot line is different from the finished elevation of the driveway or parking surface, the difference in elevation may be measured as a portion of the required height of the screen so long as the screen itself is a minimum of 3 feet in height.		
23.45.536.A	Parking location, access, and screening	Off-street parking spaces are required to the extent provided in Chapter 23.54	Refer to 23.54 for parking requirement	
23.45.536.B	Location of parking	2. Surface Parking a. Except as otherwise provided in this subsection 23.45.536. B, surface parking maybe located anywhere on a lot except: 1) Between a principal structure and a street lot line 2) In the required front setback or side street side setback; and 3) Within 20 feet of any street lot line	23.54.020.J.2: for any development requiring 20 or more parking spaces under Section 23.54.015 that provides a space for vehicles operated by a car-sharing program, the number of required parking spaces may be reduced by the lesser of three required parking spaces for each car-sharing space or 15 percent of the total number of required spaces.	
23.45.536.C	Access to parking	2. Street access required. Access to parking shall be from the street if: a) The lot does not abut an alley.		
23.45.536.D	Screen of parking	1. Parking shall be screened from direct street view by: a. The street-facing facade of a structure; b. Garage doors; c. A fence or wall; or d. Landscaped areas, including bioretention facilities or landscaped berms. 2. Screening provided by a fence, wall, or vegetation in a landscaped area shall not be located within any required sight triangle and shall meet the following conditions: a. The fence, wall, or vegetation in the landscaped area shall be at least 3 feet tall measured from the elevation of the curb, or from the elevation of the street if no curb is present. If the elevation of the ground at the base of the fence, wall, or landscaped area is higher than the finished elevation of the parking surface, the difference in elevation may be measured as a portion of the required height of the screen, so long as the fence, wall, or vegetation in the landscaped area is at least 3 feet in height. If located in a setback, the fence or wall shall meet the requirements of subsection 23.45.518.I.7. b. The fence, wall, or vegetation in the landscaped area shall be set back at least 3 feet from the lot line.		



BASE ZONING: MR (MIDRISE RESIDENTIAL):

Residential use permitted outright per SMC 23.45.504, Table A

ZONING OVERLAYS PRESENT:

UR (Urban Residential Shoreline Jurisdiction)
Alki Parking Overlay (AL)

ENVIRONMENTALLY CRITICAL AREA OVERLAYS PRESENT

Steep Slope
Liquefaction Zone
Potential Slide area

OVERALL SITE AREA: 22,200 sf

FLOOR AREA RATIO (FAR):

3.2 base allowed
4.5 MHA allowed
Allowable Floor Area: 71,040 sf base allowed
Allowable Floor Area: 99,900 sf MHA allowed

MAX HEIGHT: 61' - 6"

60' - 0" base for MR zones, capped by UR Shoreline Environmental Overlay, 23.60A.572
1' - 6" additional allowable for roof insulation exceeding code minimum, per 23.60A.572.C.2
10' - 0" additional allowable for stair and elevator penthouses and mechanical

SETBACK: Per SMC23.45.518, Table B

Front Setback: 5' min. 7' average
Side Setback: <42' in height: 5' min, 7' average
Side Setback: >42' in height: 7' min, 10' average
Rear Setback: 15' min

MAX BUILDING DIMENSIONS: Per SMC 23.45.528

Structural Depth: 90' - 0" - 75% of 120' lot depth
Structural Width: 150' - 0"
Front Setback: 5' min. 7' average (Courtyard exception - 23.45.518 applied in option 2)
Side Setback: <42' in height: 5' min, 7' average
Side Setback: >42' in height: 7' min, 10' average

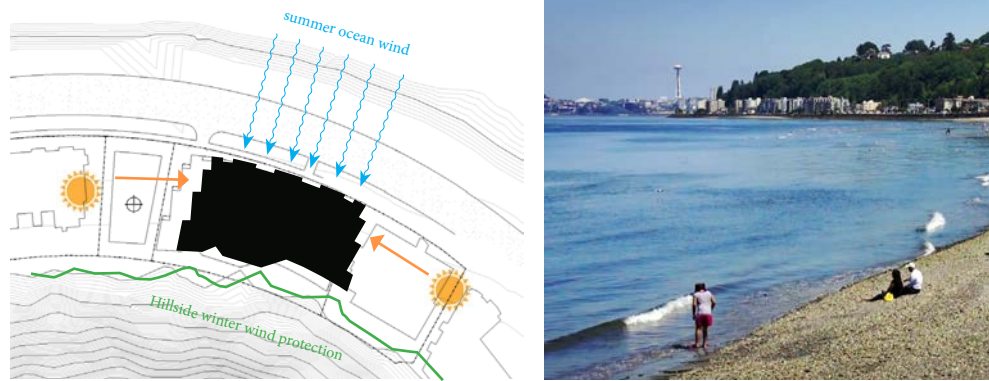
PARKING REQUIRED: 1.5 Space per dwelling unit

Per Part O, Table B for 23.54.015, Alki Parking Overlay

BICYCLE PARKING:

1 space per 4 dwelling units for long term use, per SMC23.54.015, Table D

CS1 Natural Systems and Site Features



CS1 Natural Systems and Site Features

- B1. Sun and Wind
- B2. Daily and Shading

Design Response:

The hillside protects the building from colder winds in winter and the building is able to take advantage of summer winds from the ocean. The massing recess(es) on the south allows access to increased natural ventilation from two sides.

The massing modulation recesses increase the amount of exterior wall available for daylighting so that more units will receive daylighting from two sides. The splay in the building massing opens up the recess to increase those effects. The side yard setbacks have been increased at the street side to allow for more access to daylight for this and neighboring sites.

CS2 Urban Pattern and Form



CS2 Urban Pattern and Form

- A. Location in the City and Neighborhood
- B. Adjacent Sites, Streets, and Open Spaces
- C. Relationship to the Block
- D. Height, Bulk, and Scale

Design Response:

The building mass responds to many existing patterns found in nearby mid-rise development. The ground level is differentiated from floors above with street front recess and parking access. The glazed facade is modulated to a much smaller scale to breakdown mass that directly relates in width and proportion to neighboring properties to continue an established rhythm of form.

The proposed masses play at differing angles to follow the curvature of Alki Avenue and to align with adjacent properties.

CS3 Architectural Context and Character



CS3 Architectural Context and Character

- A. Emphasizing Positive Neighborhood Attributes
 - A1. Fitting old and new together
 - A2. Contemporary design
 - A4. Evolving neighborhoods

Design Response:

The proposed project fits in between two new mid-rise developments with similar height and the flat roof and wrap around decks. The massing aligns with both buildings to create a strong urban edge, with landscaped ground level recess to engage and activate the sidewalk and providing a pause at the existing street crossing.

PL1 Connectivity



PL1 Connectivity

- A. Network of Open Spaces
- B. Walkways and Connections
- C. Outdoor Uses and Activities

PL2 Walkability



PL2 Walkability

- A. Accessibility
- B. Safety and Security
- C. Weather Protection

PL3 Street-Level Interaction



PL3 Street-Level Interaction

- A. Entries
- B. Residential Edges
- C. Retail Edges

PL4 Active Transportation



PL4 Active Transportation

- A. Entry Location and Relationships
- B. Planning ahead for Bicyclists
- C. Planning ahead for Transit

Design Response:

The proposed residential units along Alki Ave SW with front yard entrances provide a buffer zone between the residents and the public sidewalk. Outdoor planters, lighting, overhead weather protection, and landscaping will be provided to create an interactive, pedestrian oriented sidewalk.

Elevating the residential units helps with any possible security or privacy issues and creates a strong residential edge. The primary residential entrance is located on Alki Ave SW so it is easily visible and slightly setback for identification. The top level residential deck promotes a resident connection.

Public transportation is easily accessible from this site, existing bus stop and scooter & bike share stands are right across street from the site. Having the residential lobby on Alki Ave SW will directly connect residents to public transportation.

DC1 Project Uses and Activities



DC1 Project Uses and Activities

- B. Vehicular Access and Circulation
- C. Parking and Service Uses

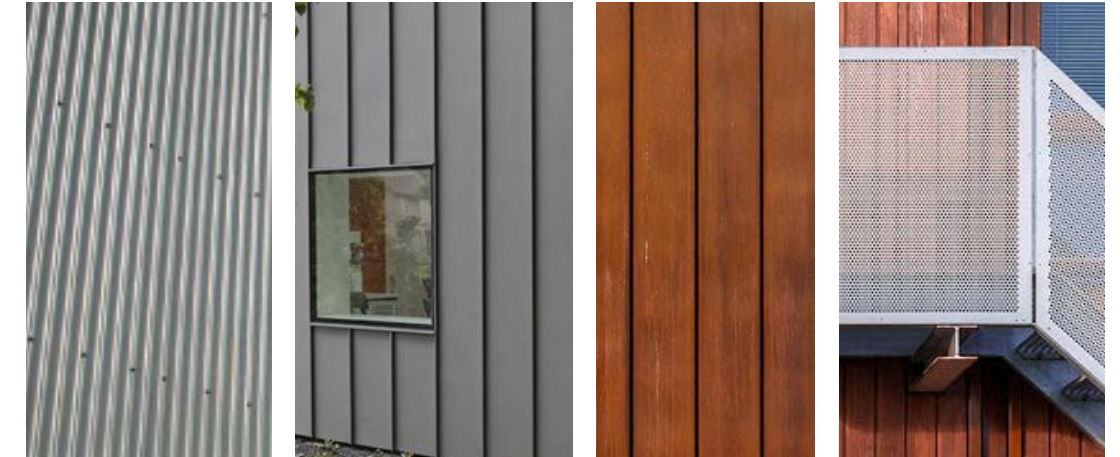
DC2 Architectural Concept



DC2 Architectural Concept

- A. Massing
- B. Architectural and Façade Composition
- C. Secondary Architectural Features
- D. Scale and Texture
- E. Form and Function

DS4 Exterior Elements and Finishes



DC4 Exterior Elements and Finishes

- A. Exterior Elements and Finishes
- D. Trees, Landscape and Hardscape Materials

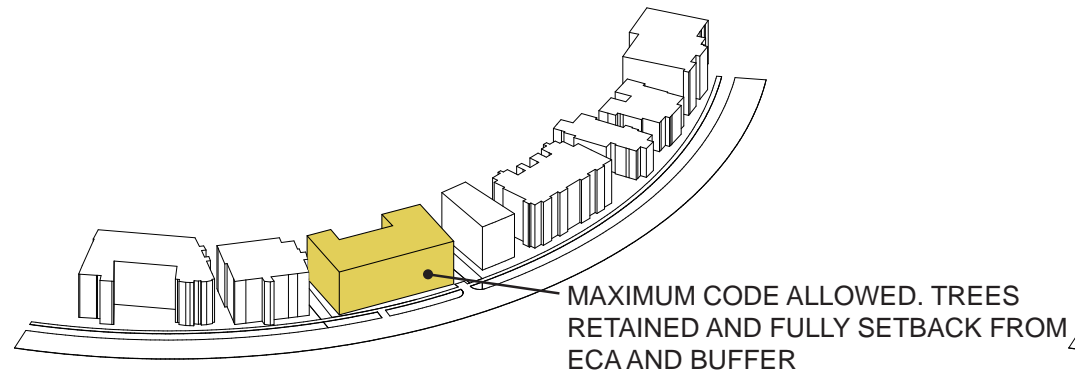
Design Response:

The building is splayed to follow the curve of Alki Avenue and the site. This creates a distinct facade which aligns to the curving street and matches the alignment of neighboring buildings, reinforces the existing streetscape patterns. Interior uses are oriented to connect to the exterior spaces and the water.

The building mass is broken down into smaller volumes by the recessed opening. The resulting forms are informed by the scale and proportion of the adjacent building. Balconies facing the water also help to modulate the distinct facade. and a high degree of transparency and permeation gives the building an open, airy feel.

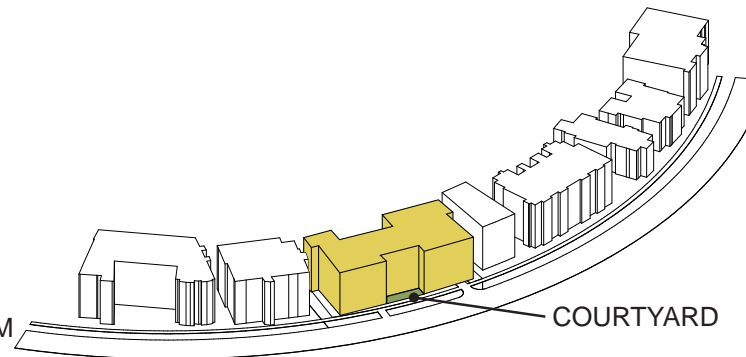
High quality, durable, modern finish materials are intended for the building. Colors will be researched and carefully selected to be compatible with the neighborhood context. Sustainable landscape design with a sensitive selection of vegetation will further enhance the community's outdoor activity space.

OPTION1



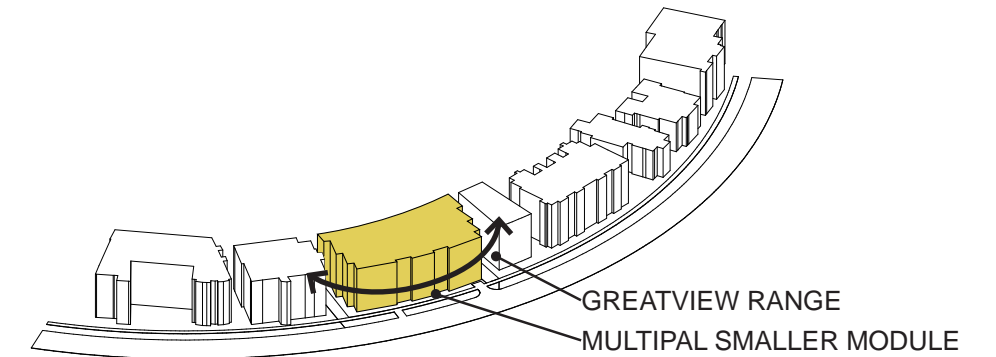
CONCEPT 1 - SLOPE EXTENDED

OPTION2

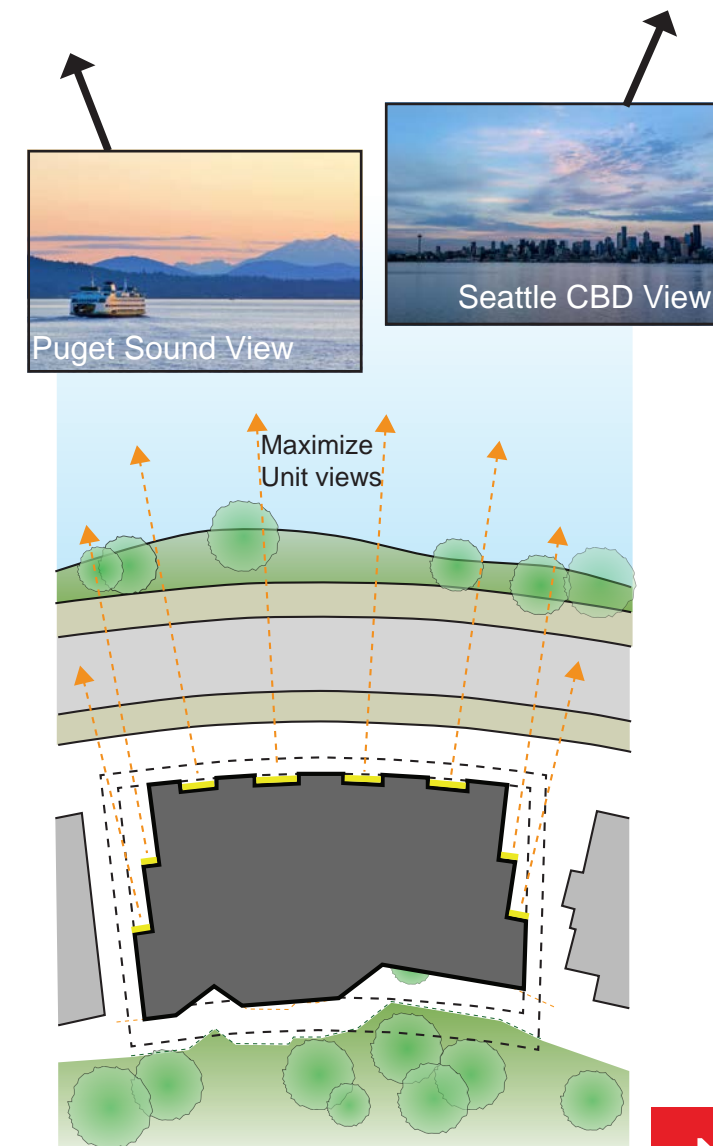
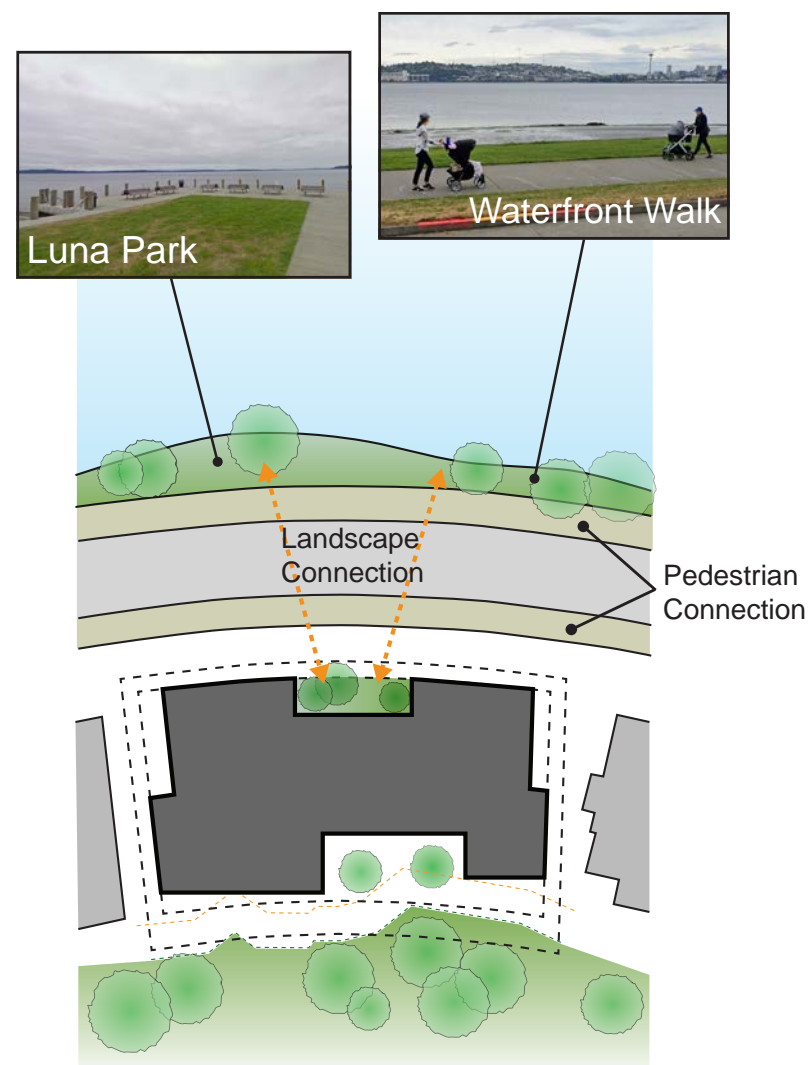
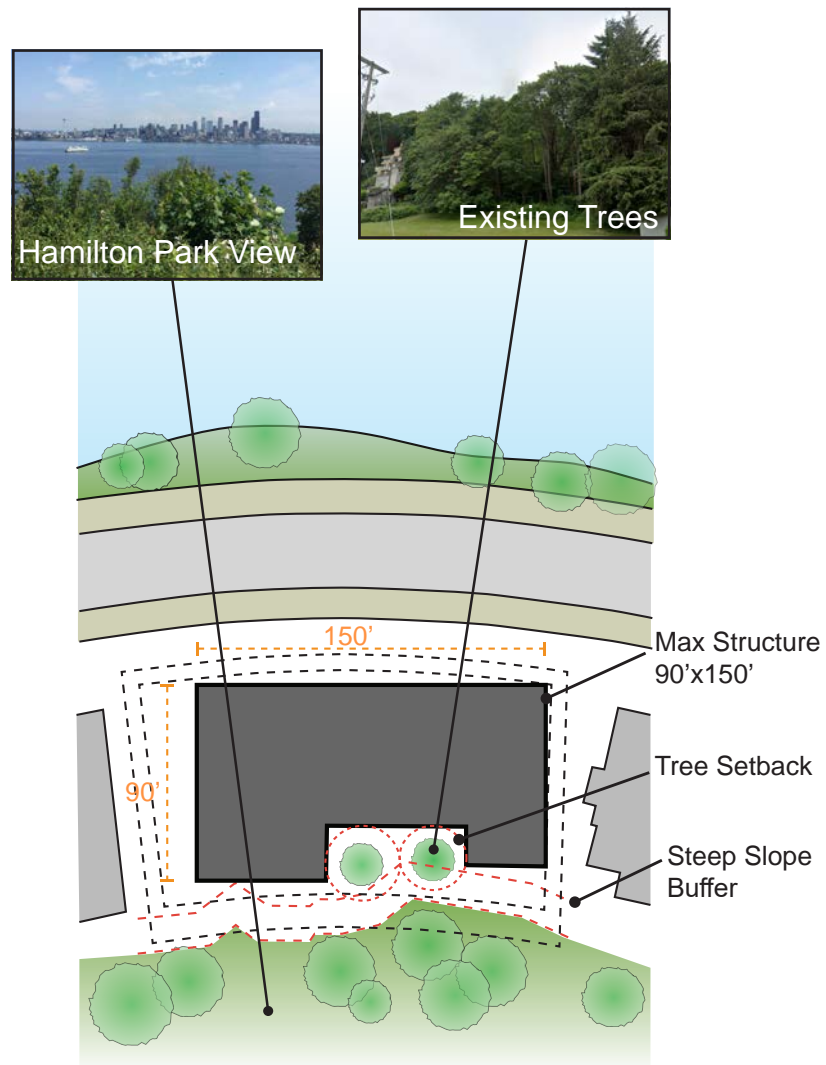


CONCEPT 2 - PARK EXTENDED

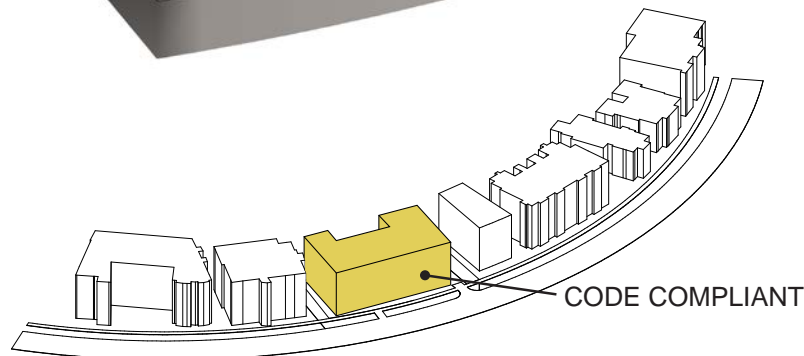
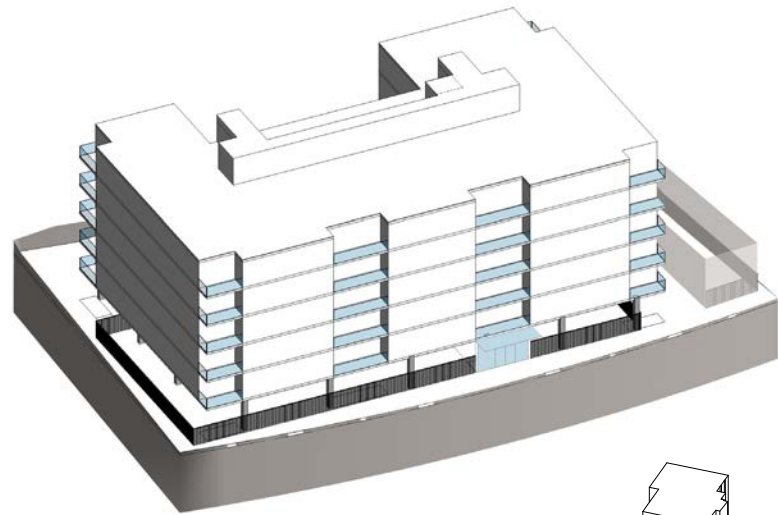
OPTION3



CONCEPT 3 - VIEW EXTENDED



OPTION1



PRO:

- Fully setback from 40% EAC steep slope and buffer zone
- Trees preserved

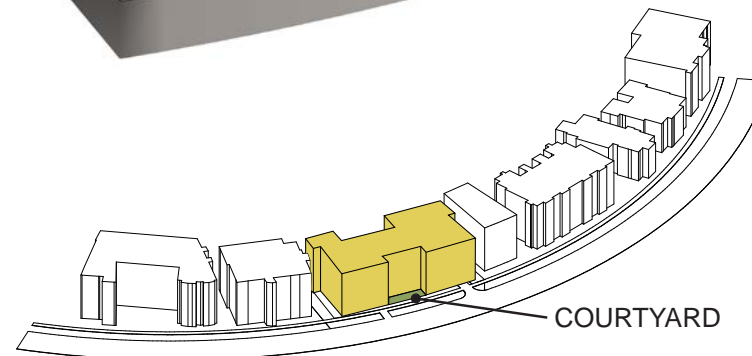
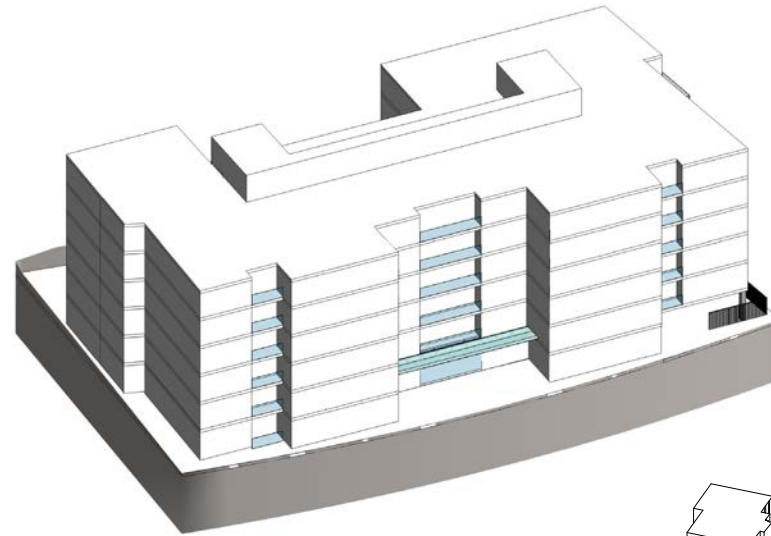
CON:

- Limited units with water view
- Very limited buildable area, parking takes up the entire ground floor
- No modulation, the shape and scale are not responding to the site and surrounding development
- Parking is exposed to the street

DEPARTURE REQUEST:

Not Required

OPTION2



PRO:

- Landscaped court yard provides a pause and viewing point on the pedestrian path
- Smaller mass at street-front for a less perceptive overall massing
- Trees preserved
- Parking is screened by the building

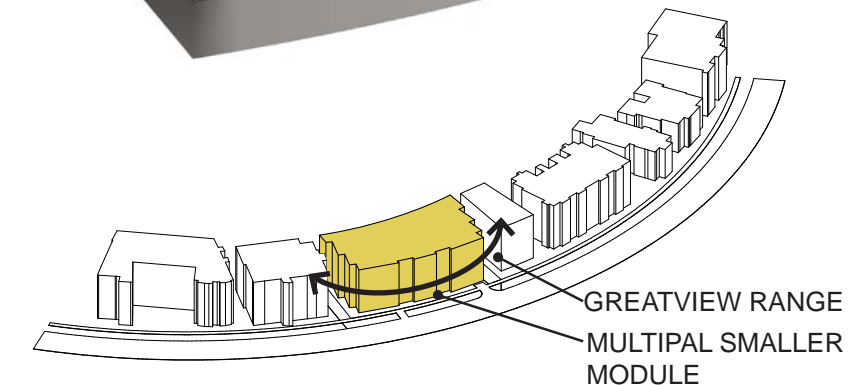
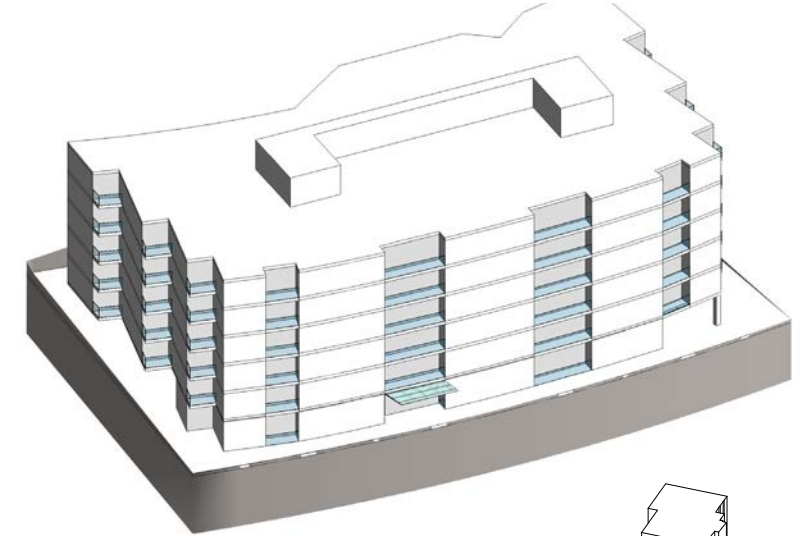
CON:

- No setback at the front lot line
- Massing is pushed to the sides, less air and light opportunities for the resident units on the sides.
- Per SMC 25.11.080.A& B- Parking requirement reduction per tree perserved

DEPARTURE REQUEST:

1. SMC 23.45.528 - Exceed max building dimension: 150' - 0" (W) x 90' - 0" (D)
2. SMC23.45.518, Table B - Minimal Side setback reduced to 2'

OPTION3



PRO:

- Multiple smaller module at street facade with balcony setbacks
- More units with water view
- Massing steps out following the shape of the site - site context
- Increased setback from the neighboring building at the front - perserve / share waterview
- Parking is screened by the building

CON:

- Limited landscaped area at the street front
- Trees Removed

DEPARTURE REQUEST:

1. SMC 23.45.528 - Exceed max building dimension: 150' - 0" (W) x 90' - 0" (D)
2. SMC23.45.518, Table B - Minimal Side setback reduced to 2'

Recorded lot area: **22,200 sf**
Total FAR area above grade: **56,358 sf** (2.52 x FAR)

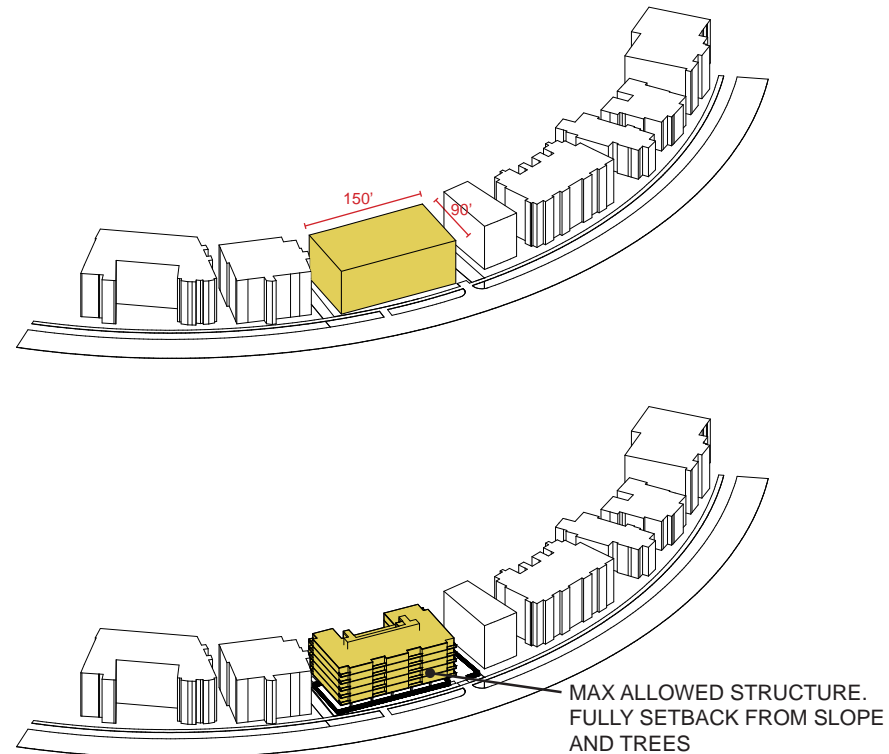
Total residential units: **55 units**
Estimated residential unit area **50,688** (net sf 89.9%)

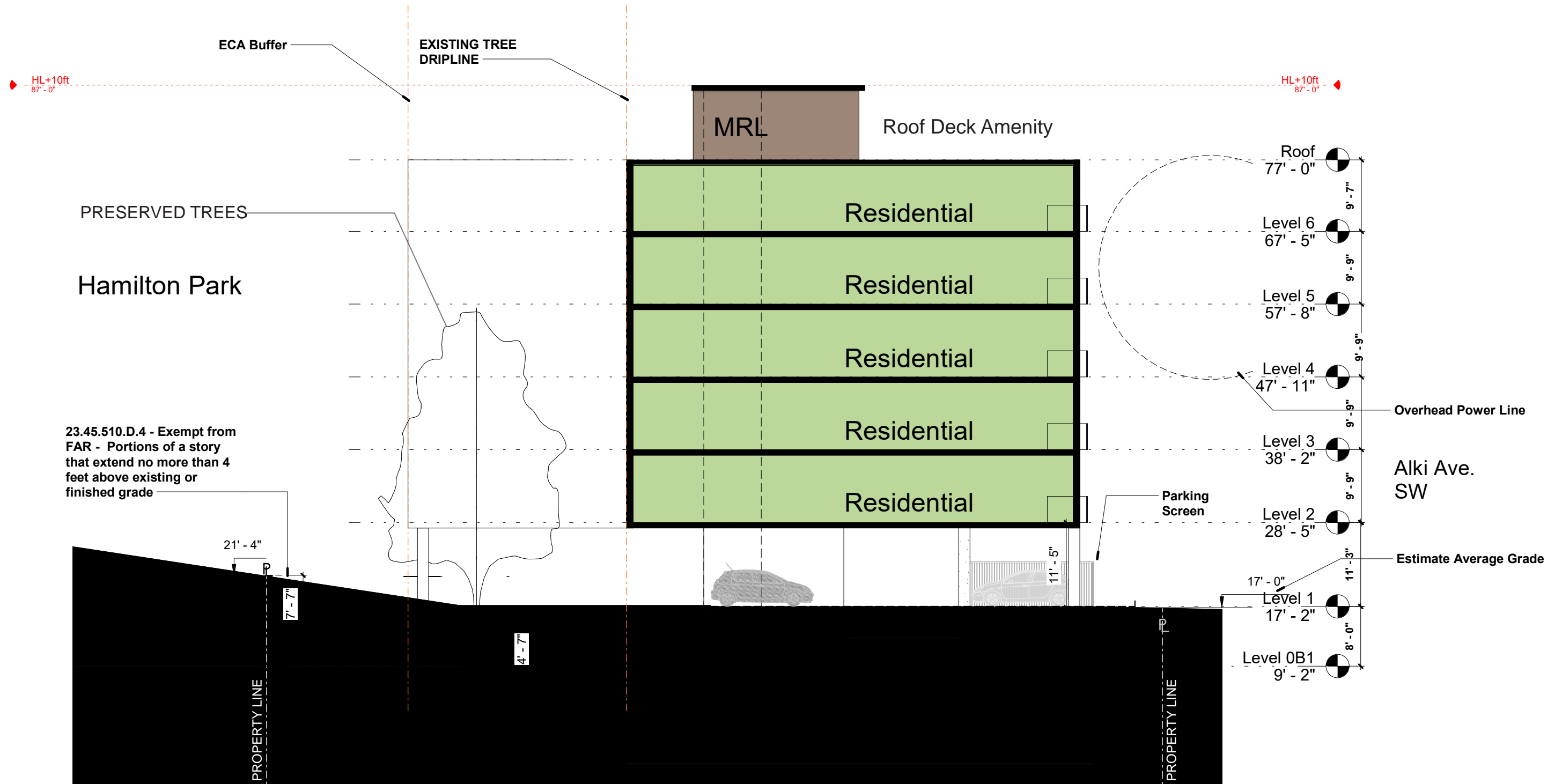
Total Amenity Area Required: **2,534.4 sf**
Total Amenity Area Provided: **3,000.0 sf**

Parking ratio required: **1.5 stall/unit**
Total parking required: **83 stalls**

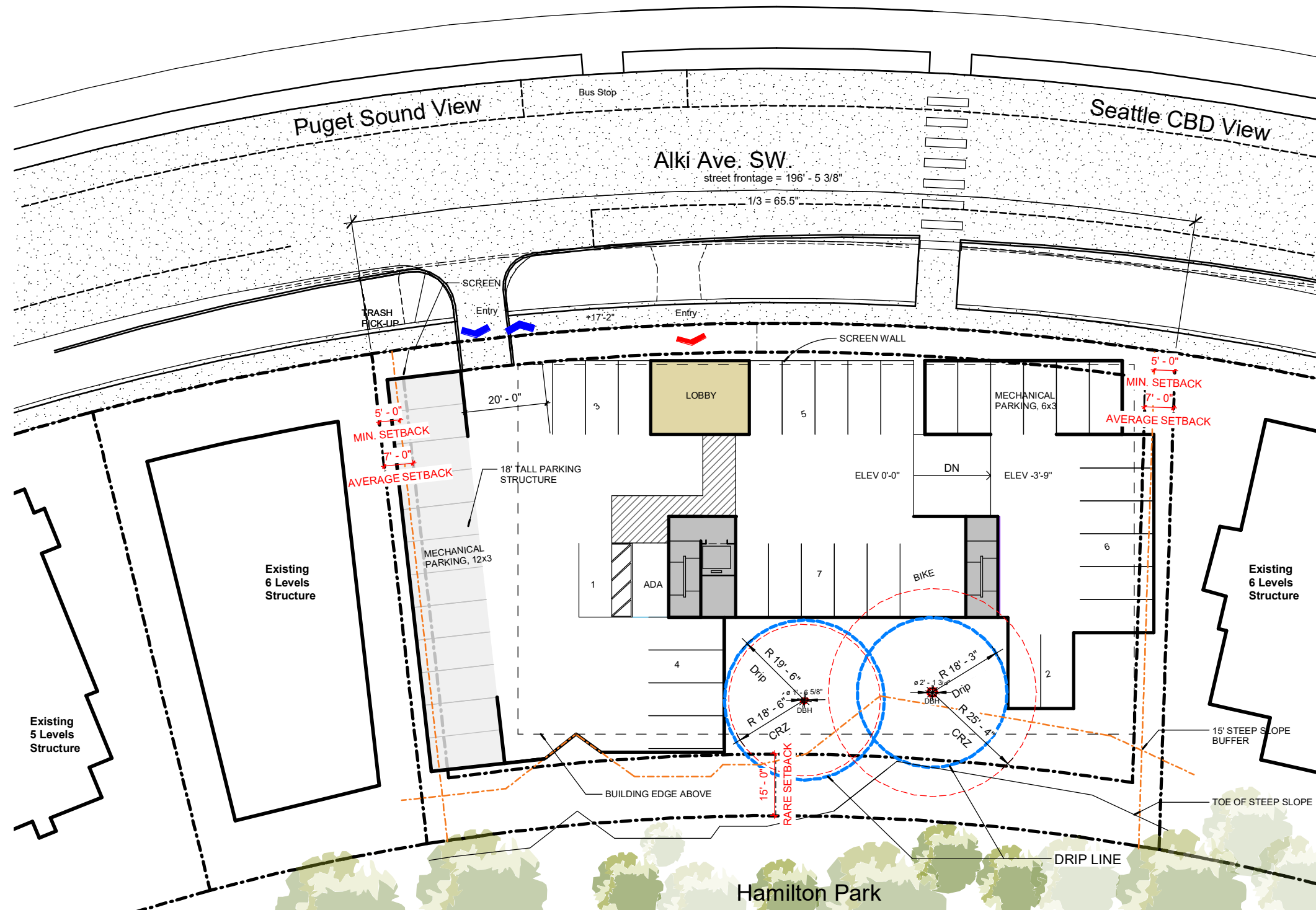
Total parking provided: **83 stalls**

Mechanical parking: **54 stalls**
Ground parking: **29 stalls**

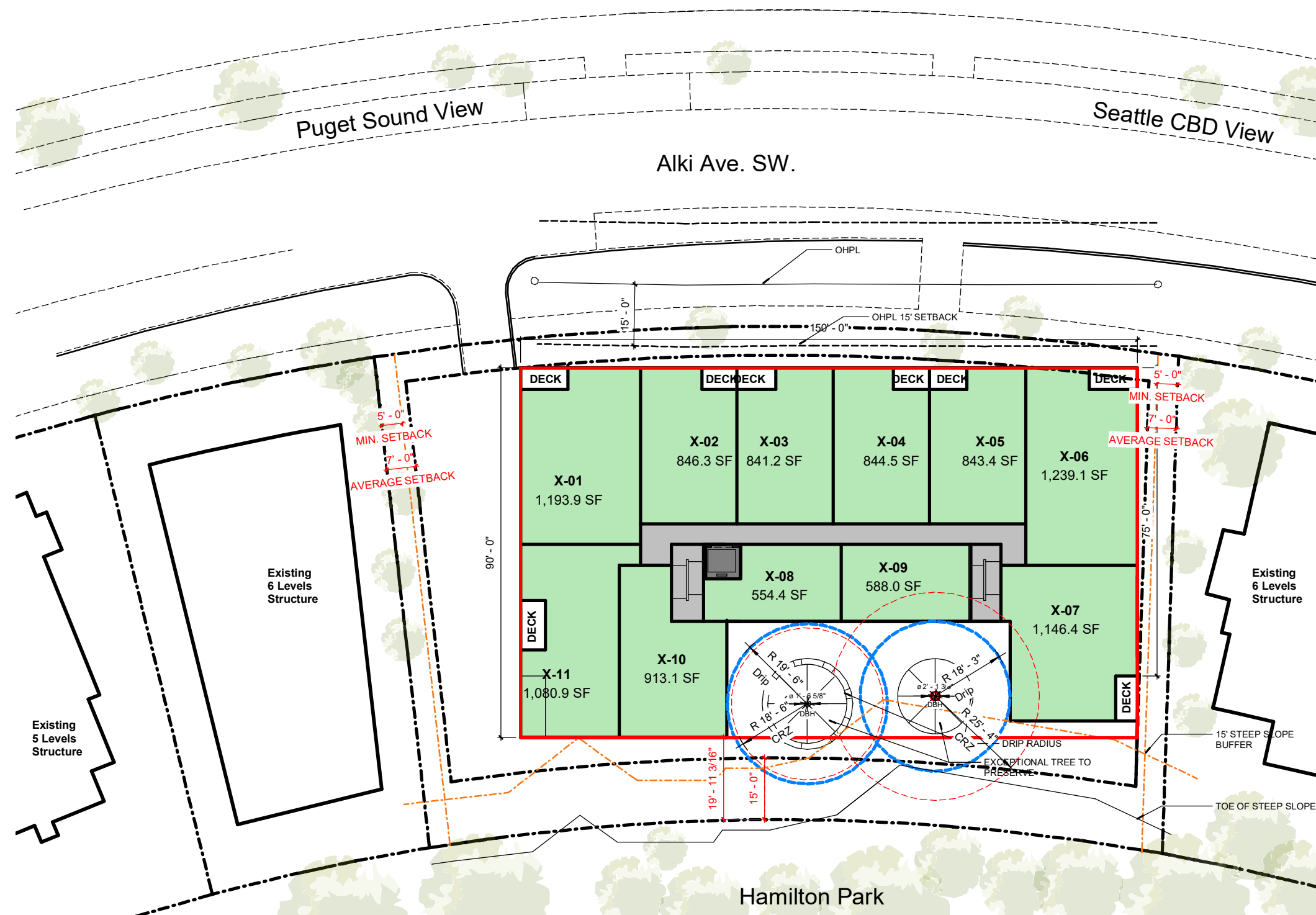




NORTH-SECTION SECTION

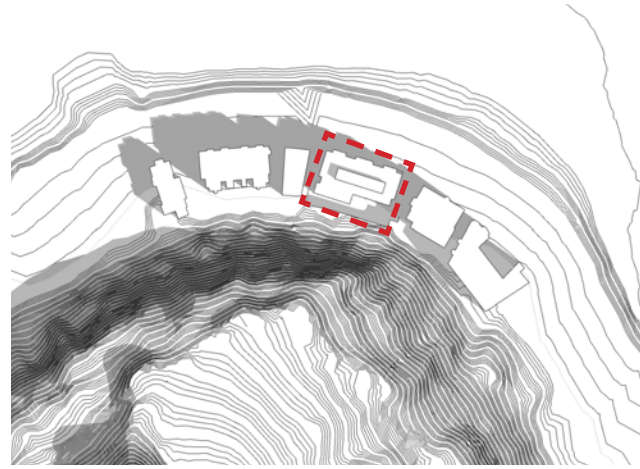


FIRST FLOOR PLAN

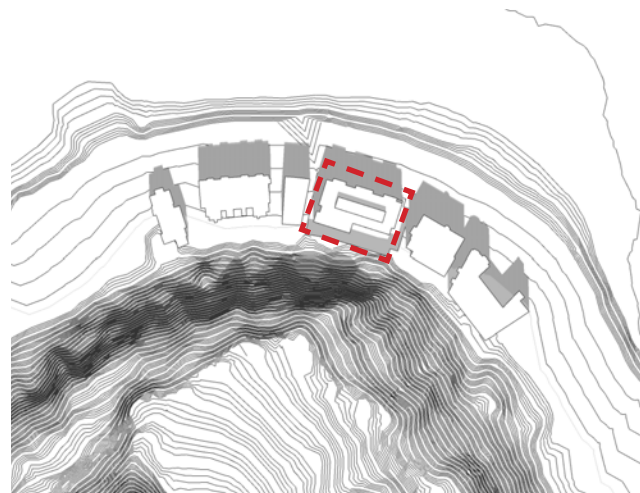


TYPICAL FLOOR PLAN

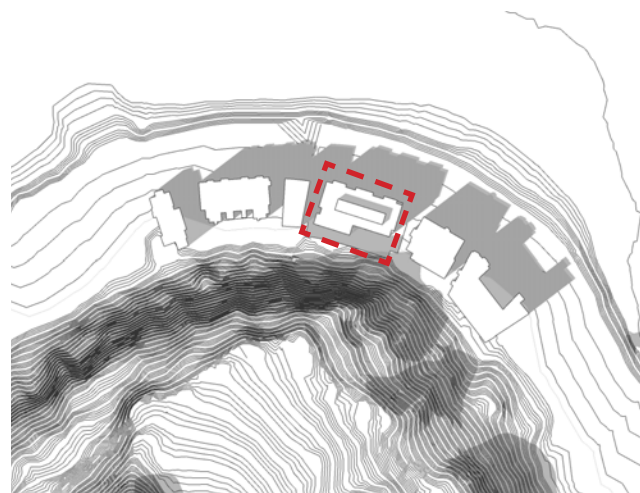
March/Sept 21



9 AM

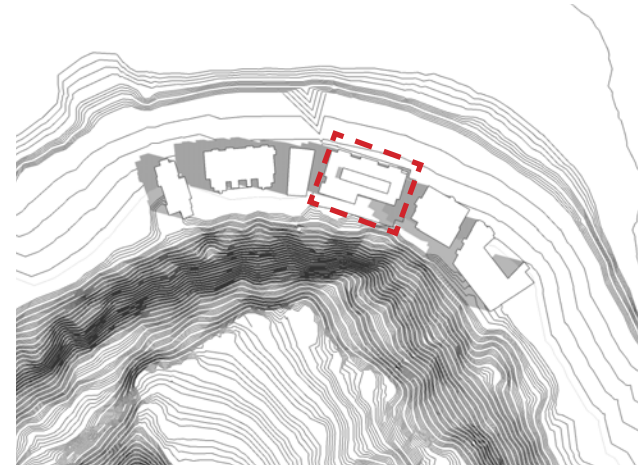


12 PM

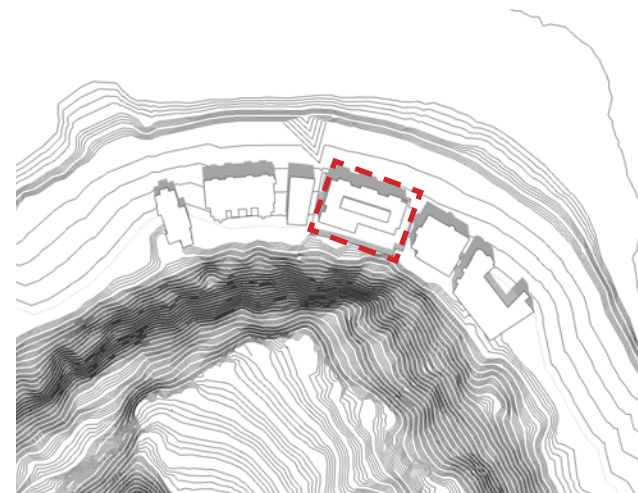


15 PM

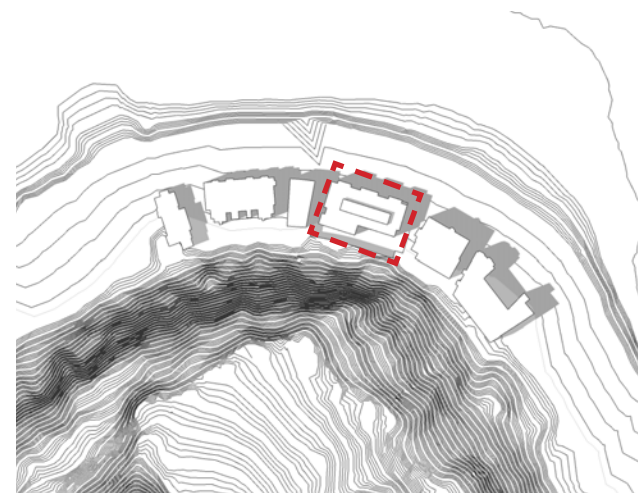
June 21



9 AM

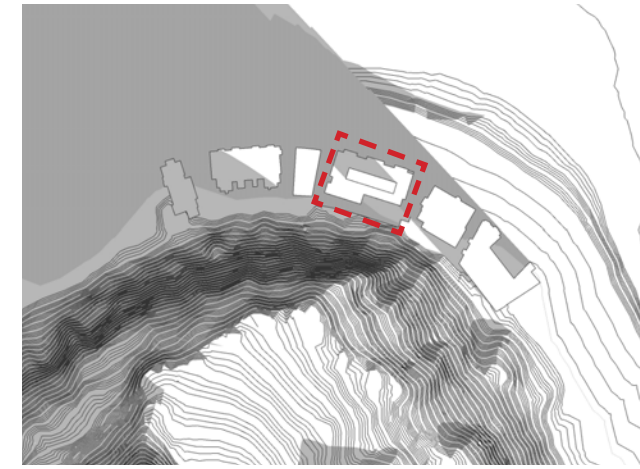


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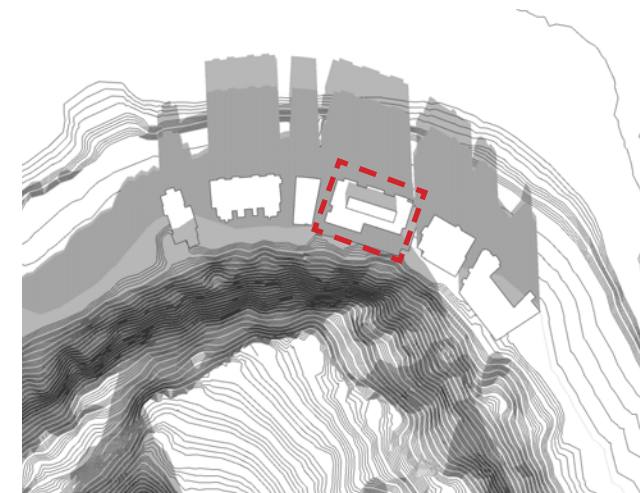


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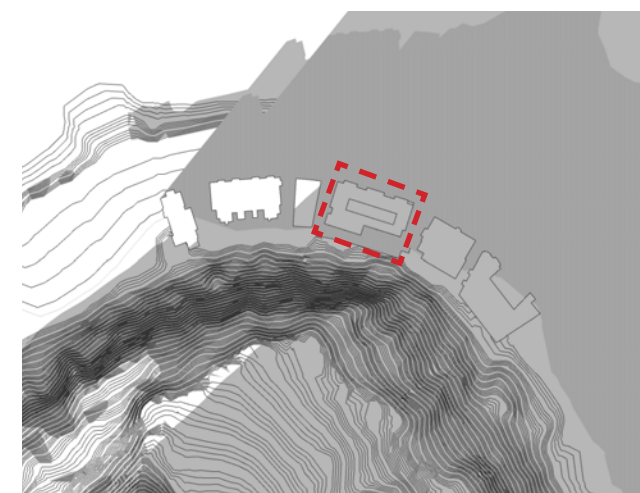
Dec 21



9 AM



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Recorded lot area: **22,200 sf**
Total FAR area above grade: **75,683 sf** (3.35 x FAR)

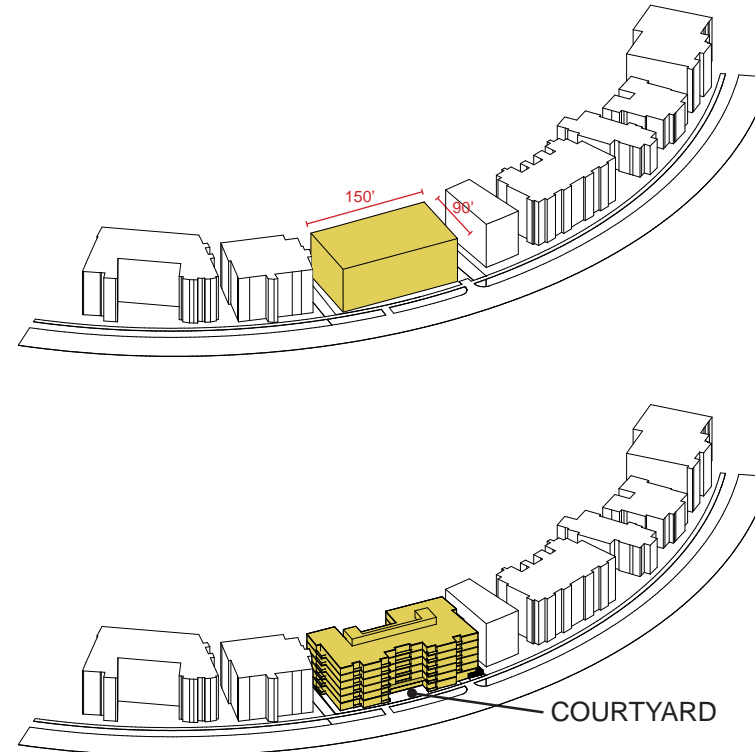
Total residential units: **54 units**
Estimated residential unit area **67,683** (net sf 89.4%)

Total Amenity Area Required: **3,384.2 sf**
Total Amenity Area Provided: **4,000.0 sf**

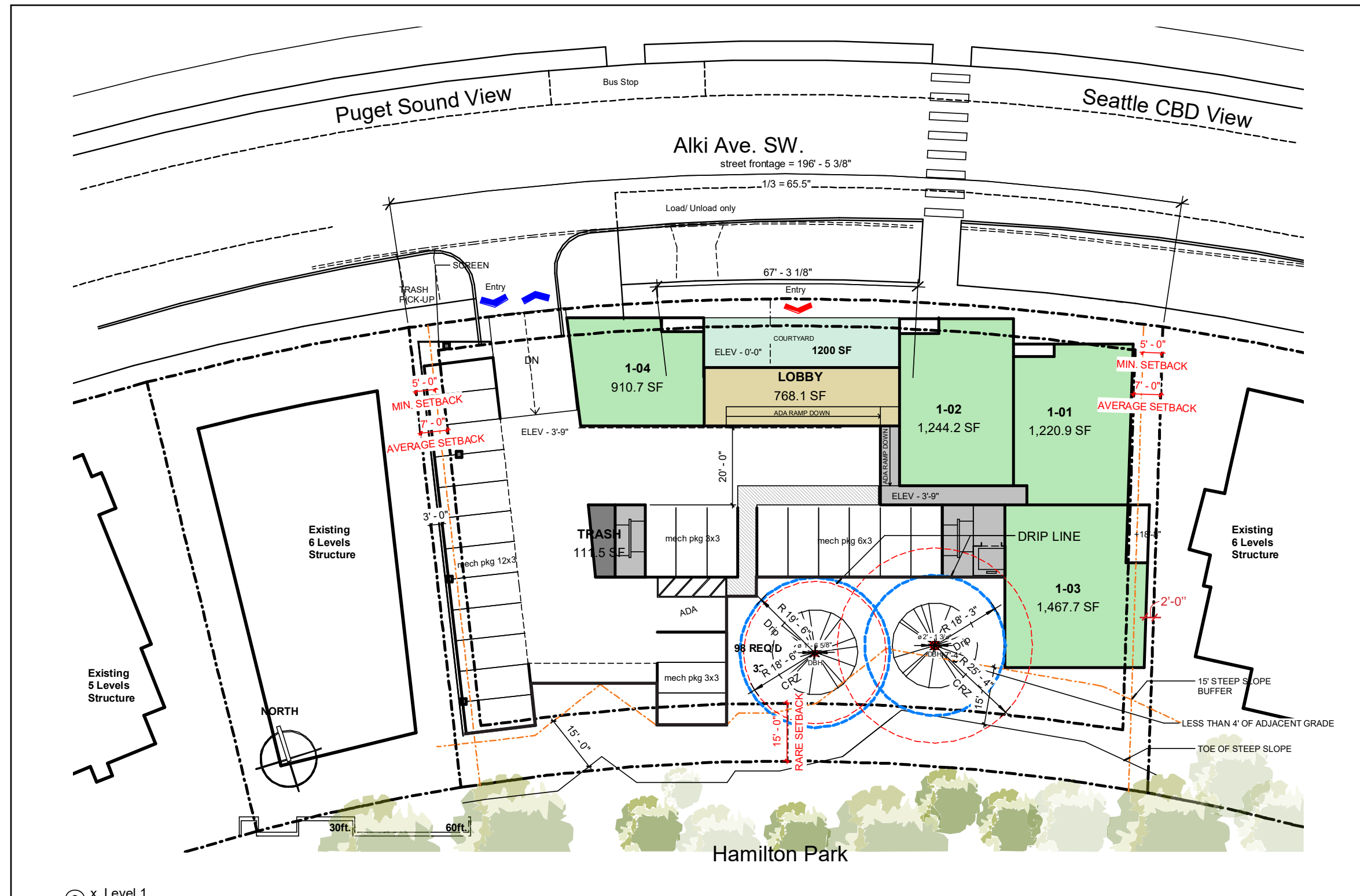
Parking ratio required: **1.5 stall/unit**
Total parking required: **81 stalls**

Total parking provided: **73 stalls**

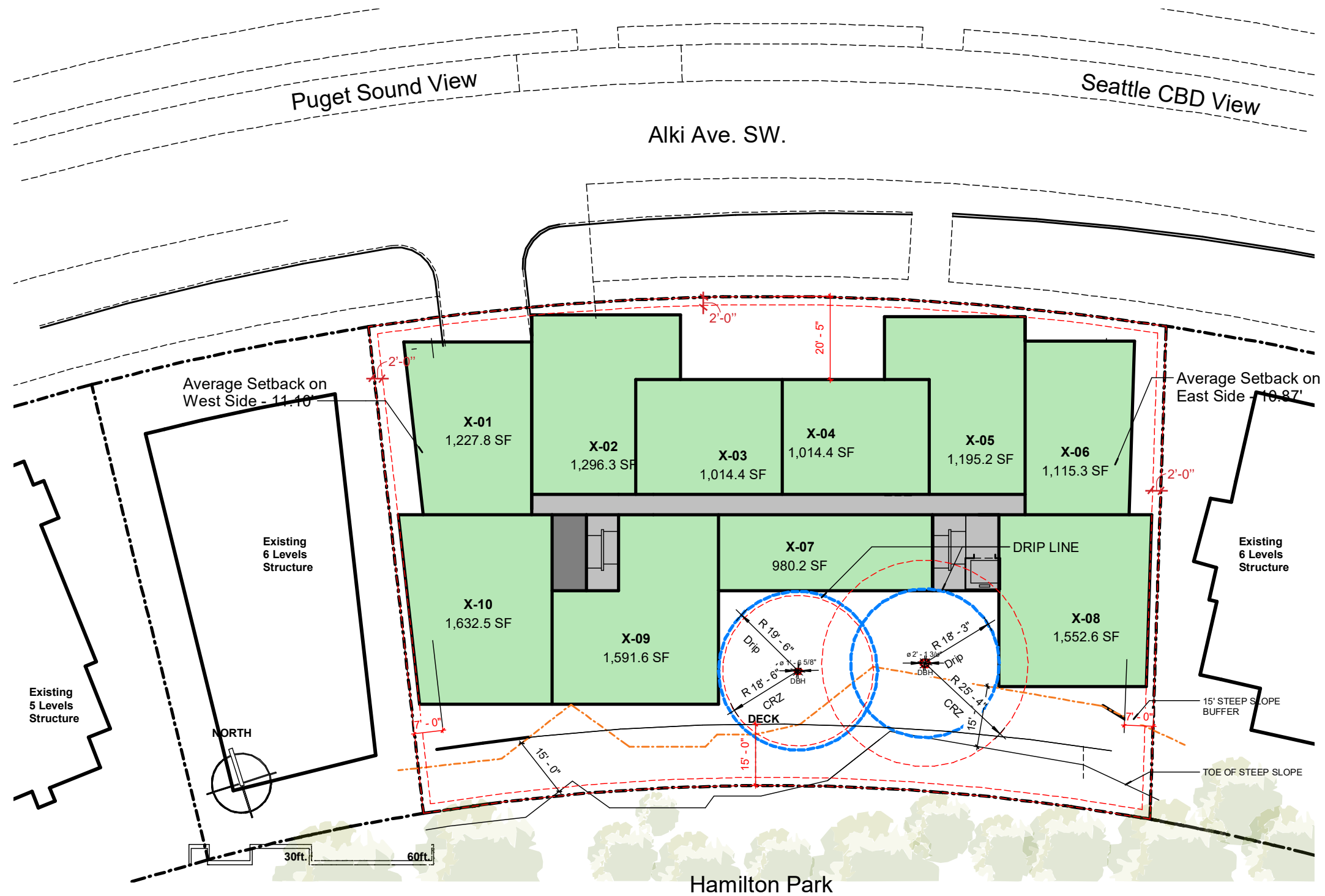
Mechanical parking: **72 stalls**
Ground parking: **73 stalls**



07.30.2021 1116 ALKI AVE. SW. : 3037493-EG



FIRST FLOOR PLAN

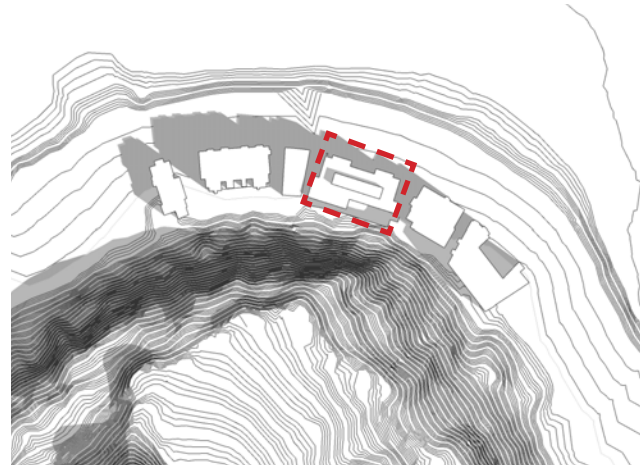


Courtyard expection - 23.45.518
applied in option 2

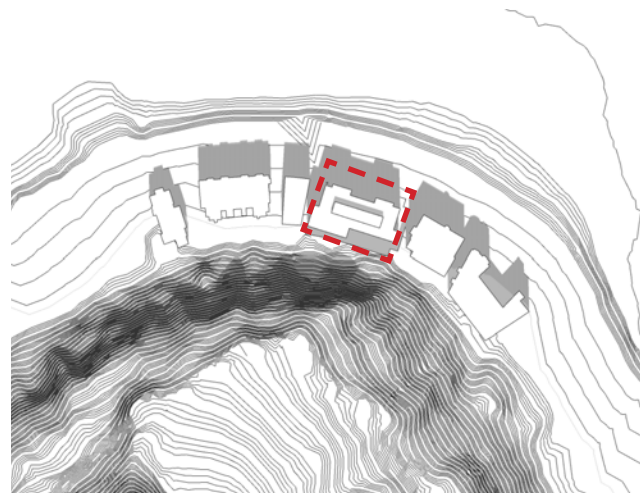
DEPARTURE REQUEST:

1. Exceed MAX BUILDING DIMENSIONS:
150' - 0" (W) x 90' - 0" (D) Per SMC 23.45.528
Proposed Structure: 168' - 0"W by 95 - 6" D

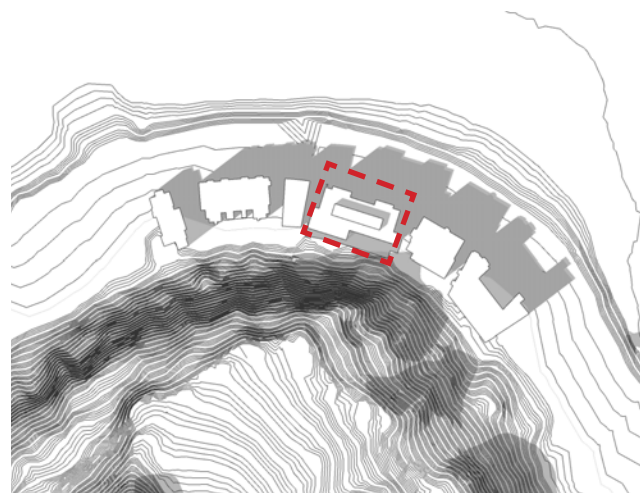
March/Sept 21



9 AM

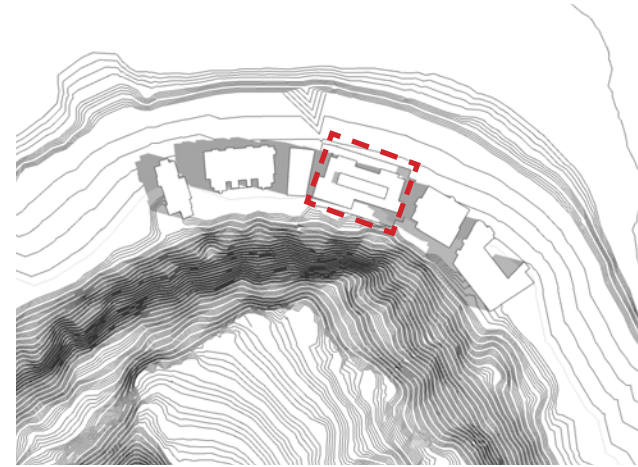


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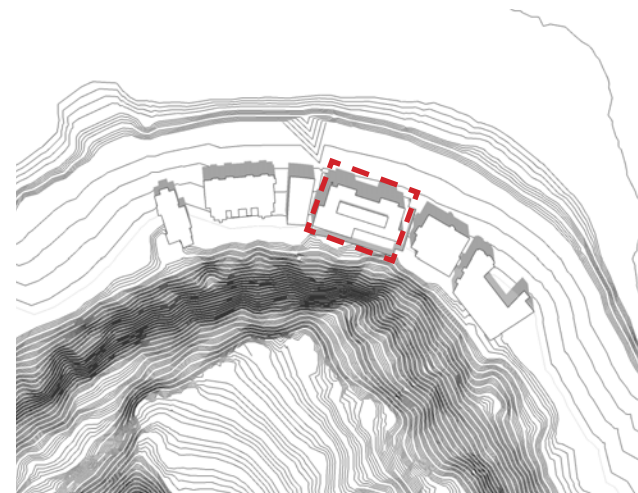


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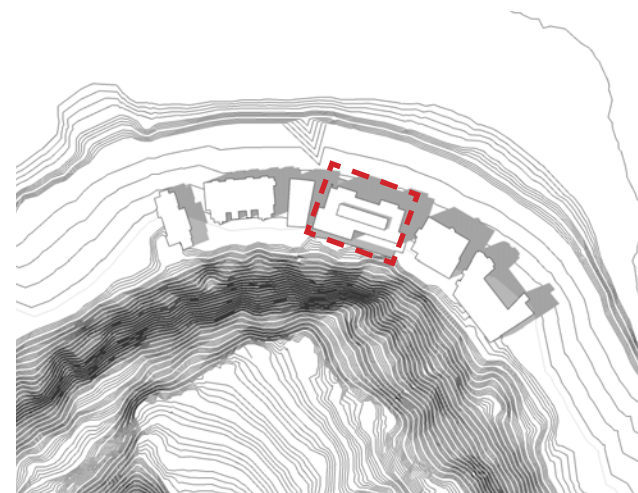
June 21



9 AM

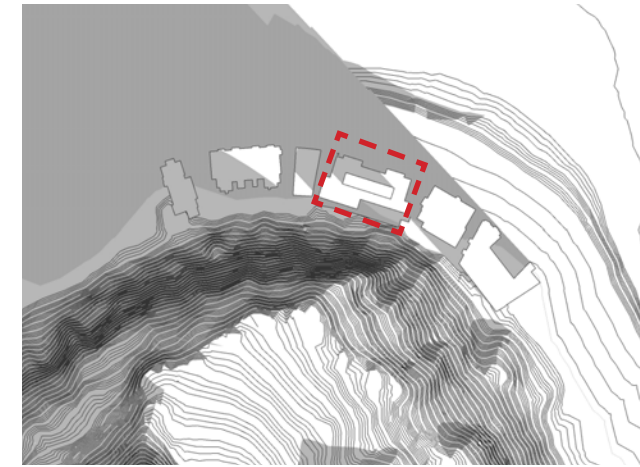


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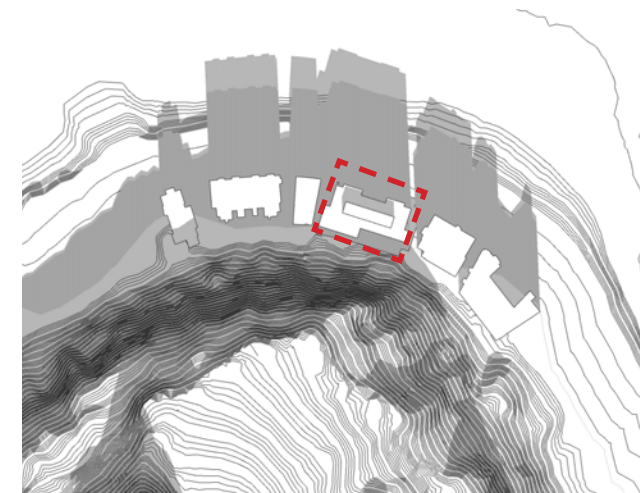


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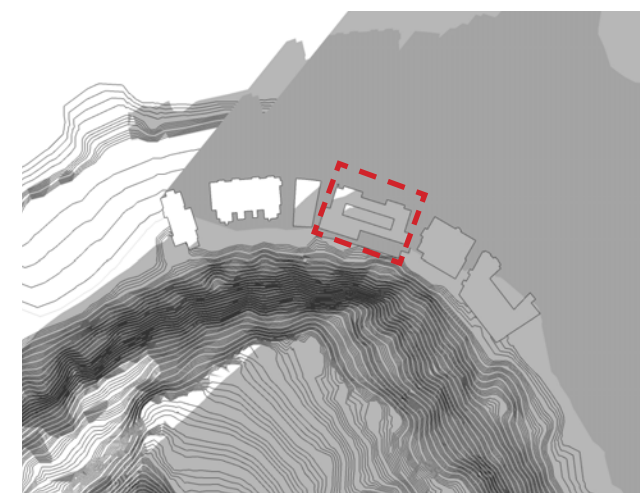
Dec 21



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Recorded lot area: **22,200 sf**
Total FAR area above grade: **82,190 sf** (3.65 x FAR)

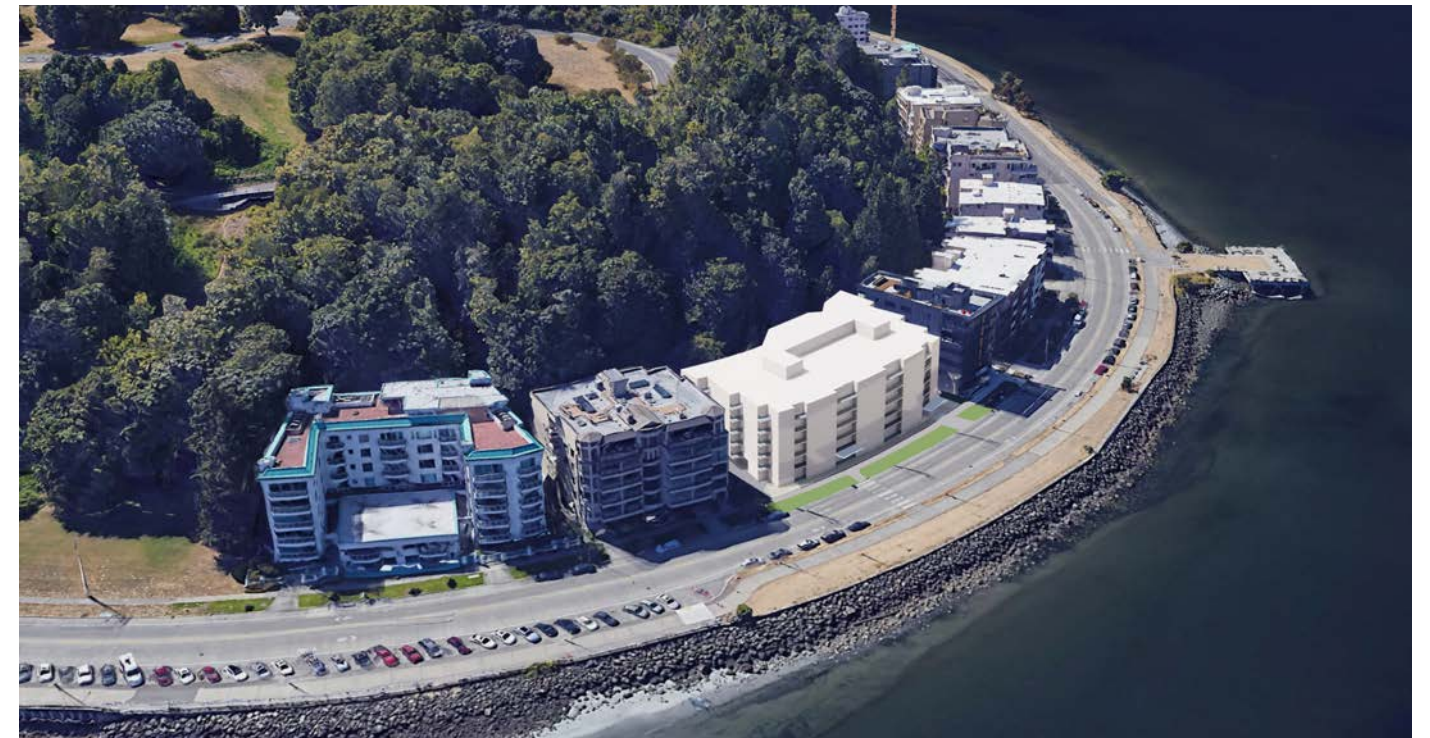
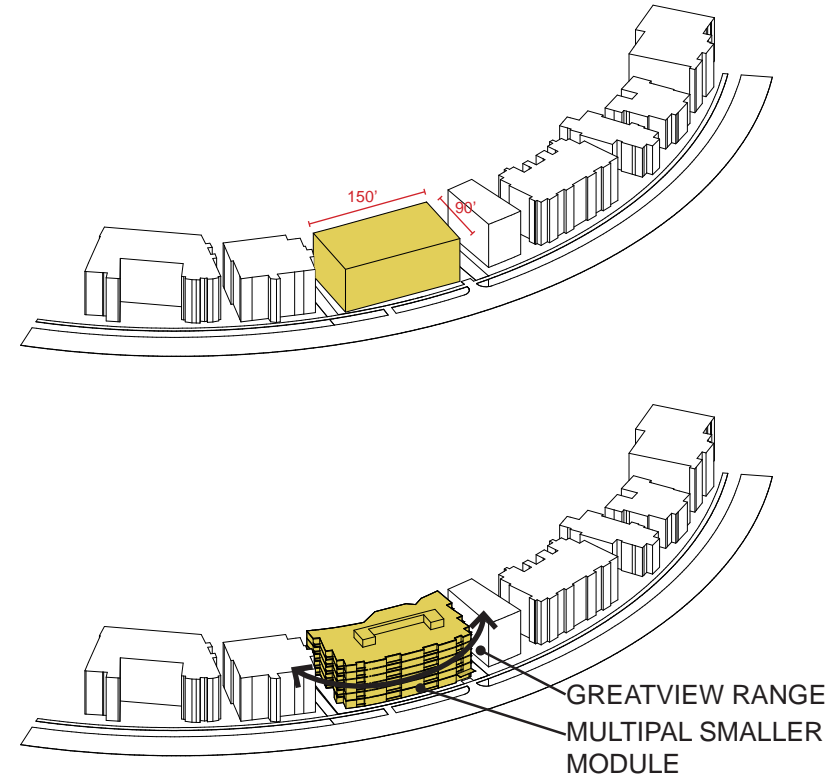
Total residential units: **58 units**
Estimated residential unit area **74,422 sf** (net sf 90.5%)

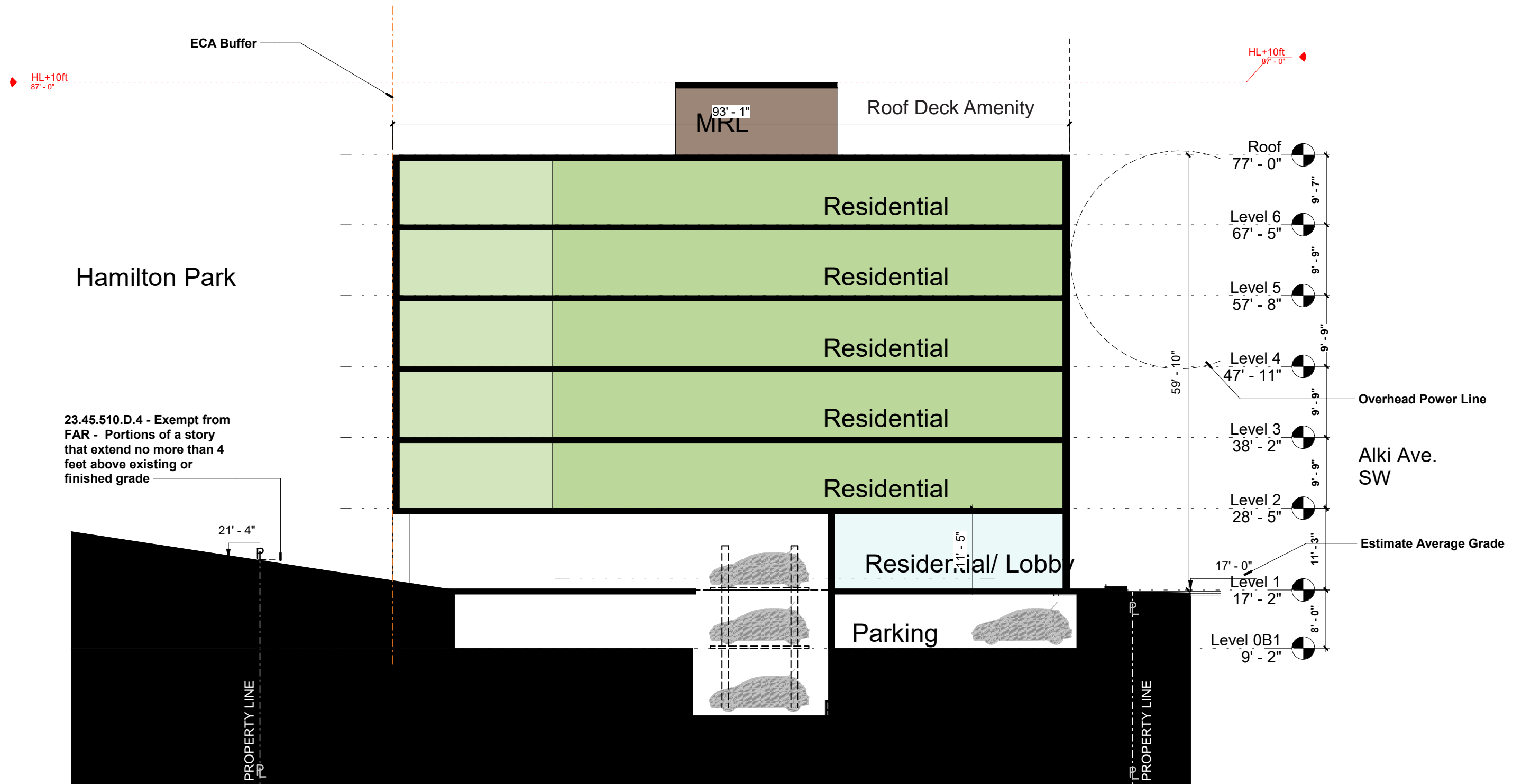
Total Amenity Area Required: **3,721.1 sf**
Total Amenity Area Provided: **4,000.0 sf**

Parking ratio required: **1.5 stall/unit**
Total parking required: **87 stalls**

Total parking provided: **92 stalls**

Mechanical parking: **27 stalls**
Ground parking: **30 stalls**
B1 parking: **35 stalls**





NORTH-SECTION SECTION

07.30.2021 1116 ALKI AVE. SW. : 3037493-EG

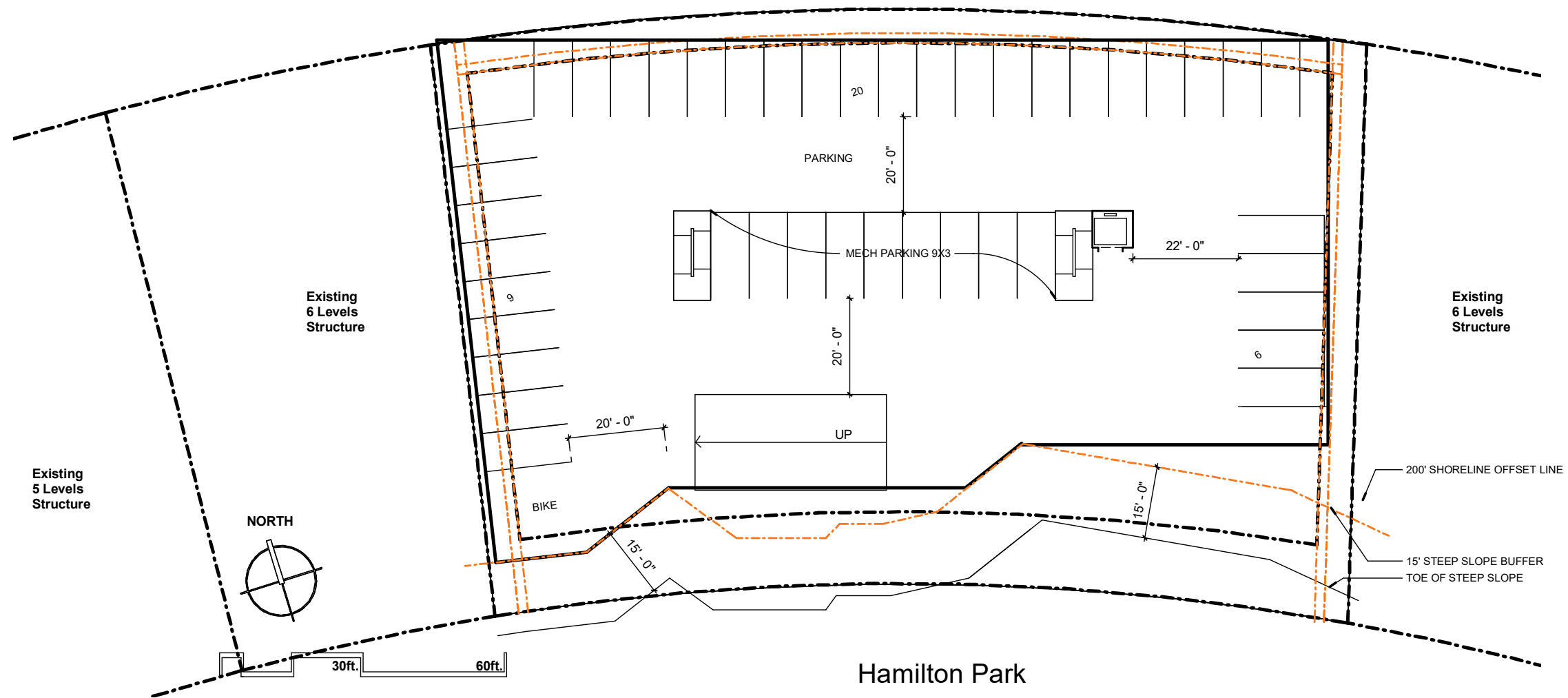


ECA setback reduction applied in option 3

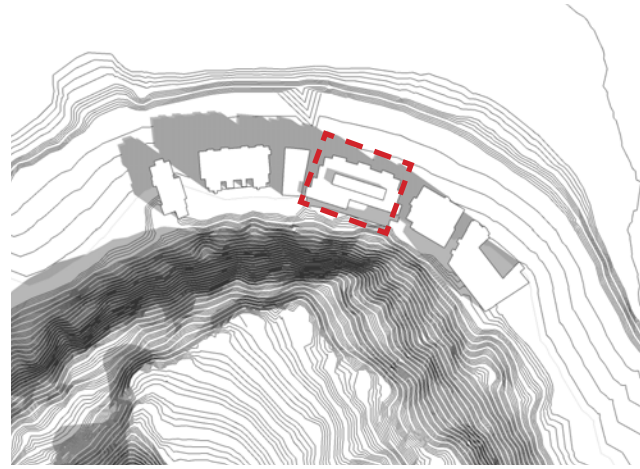
DEPARTURE REQUEST:

1. Exceed MAX BUILDING DIMENSIONS:
 150' - 0" (W) x 90' - 0" (D) Per SMC
 23.45.528
 Proposed Structure: 169'-5" x 105'-0"

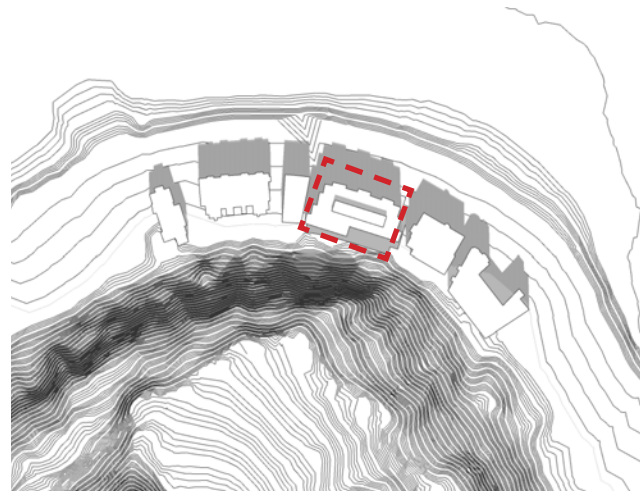
TYPICAL FLOOR PLAN



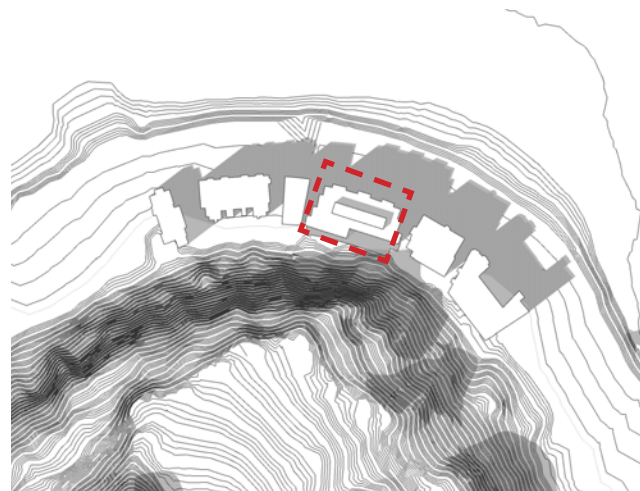
March/Sept 21



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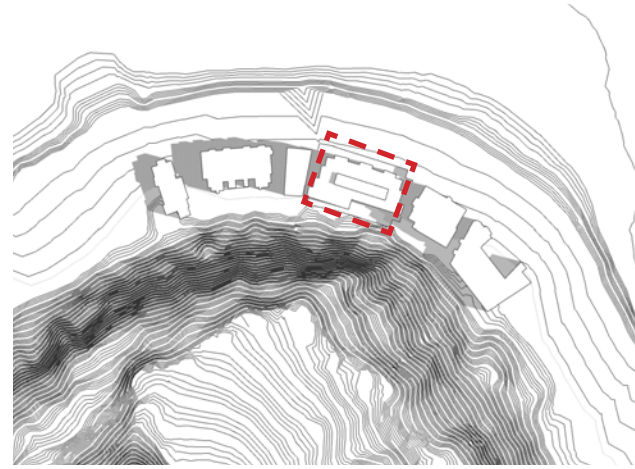


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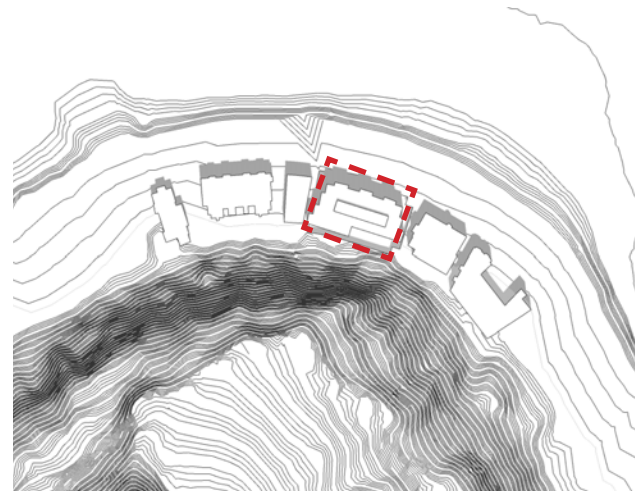


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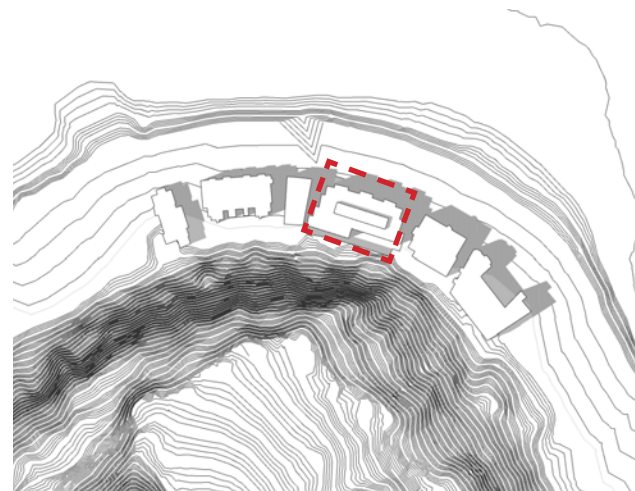
June 21



9 AM

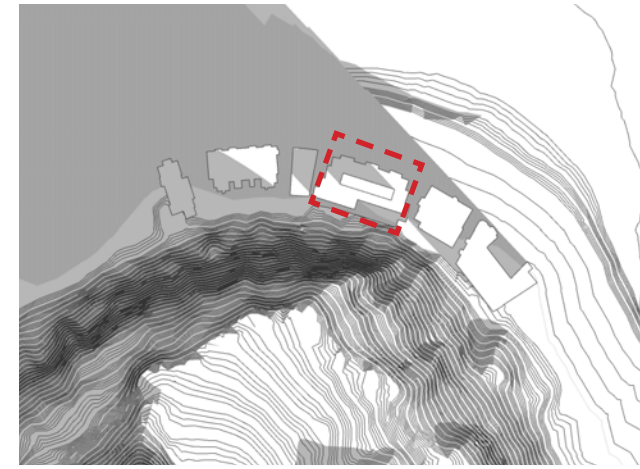


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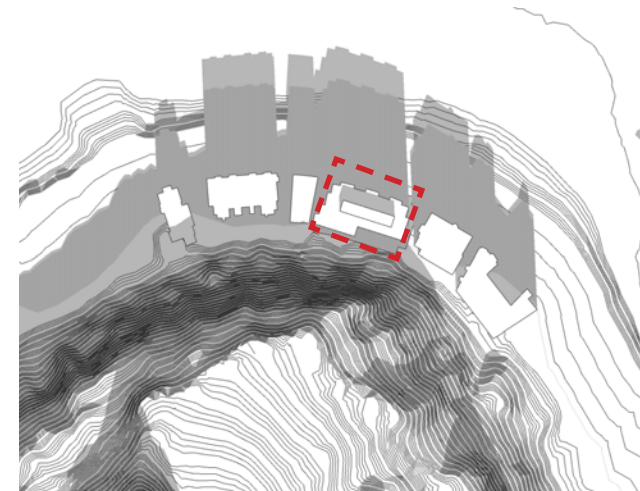


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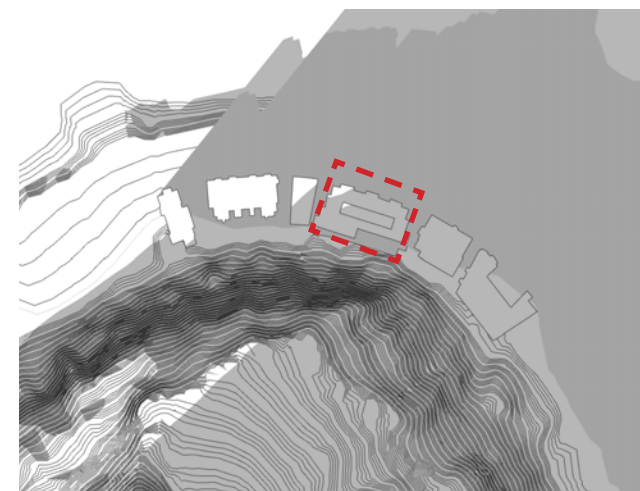
Dec 21



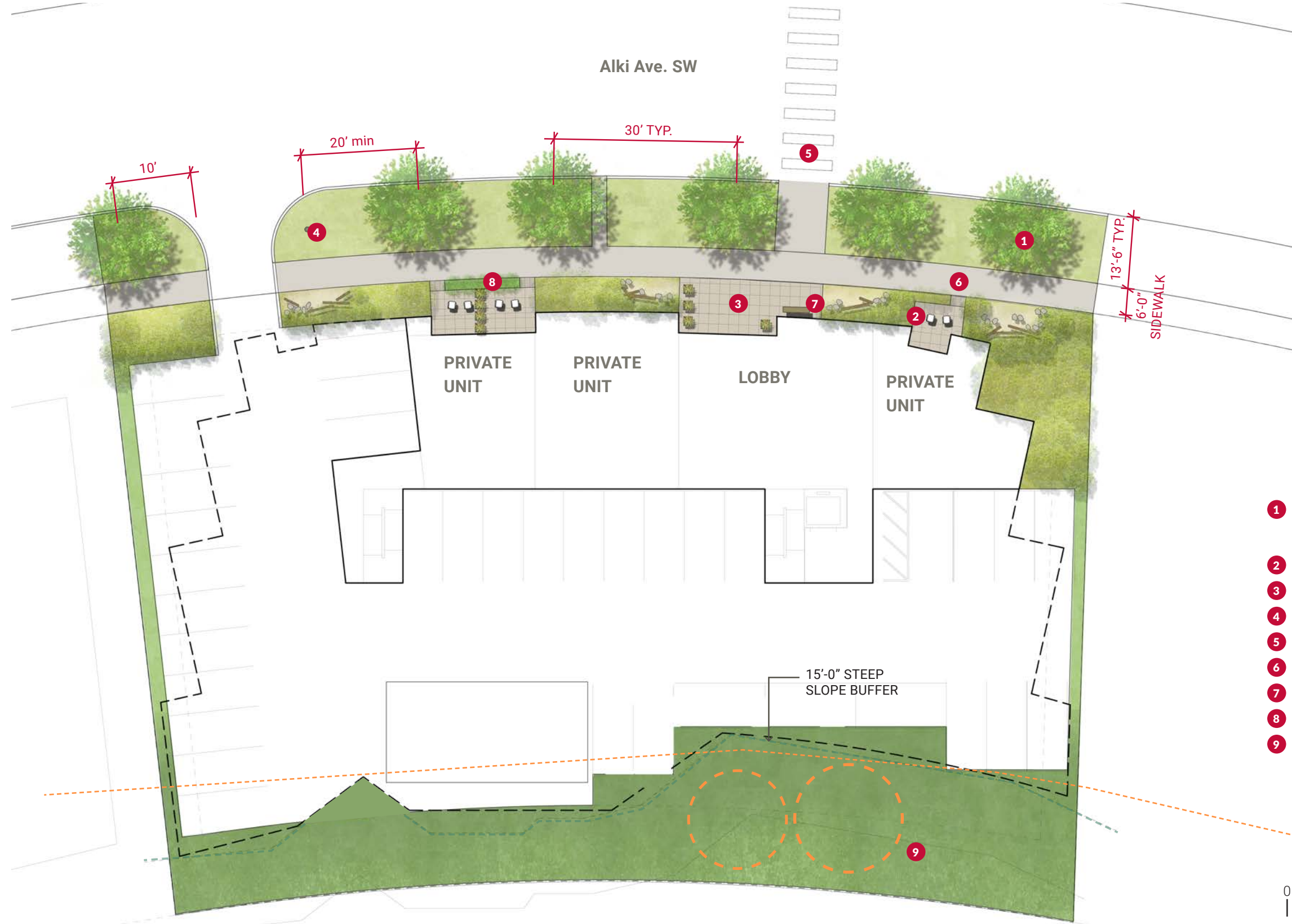
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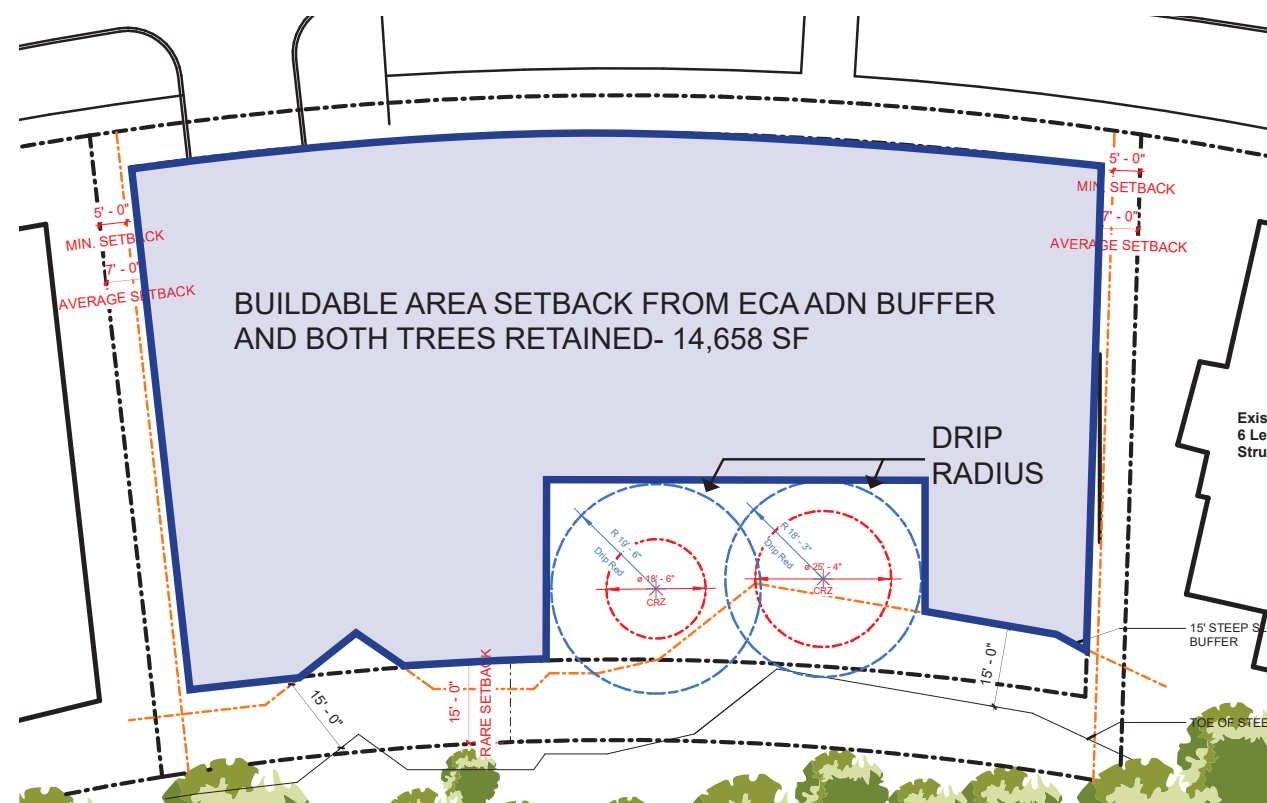
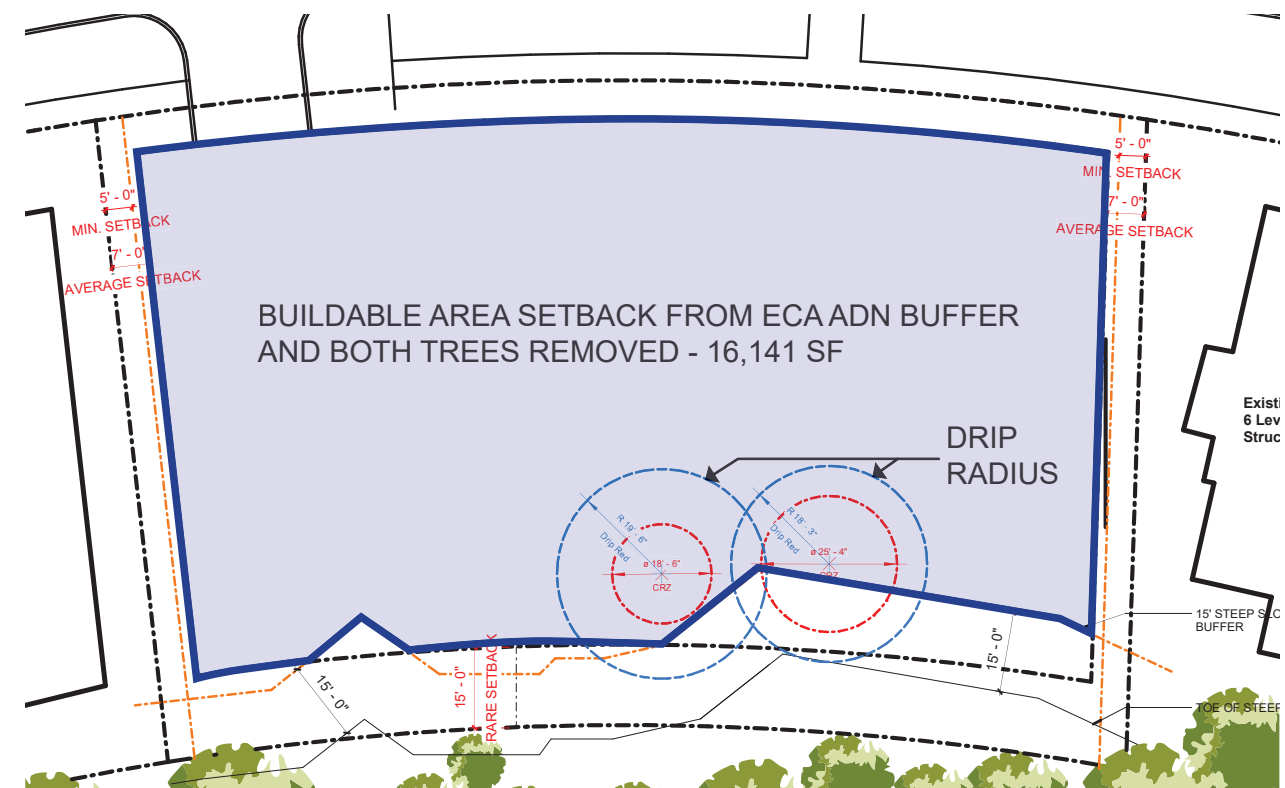
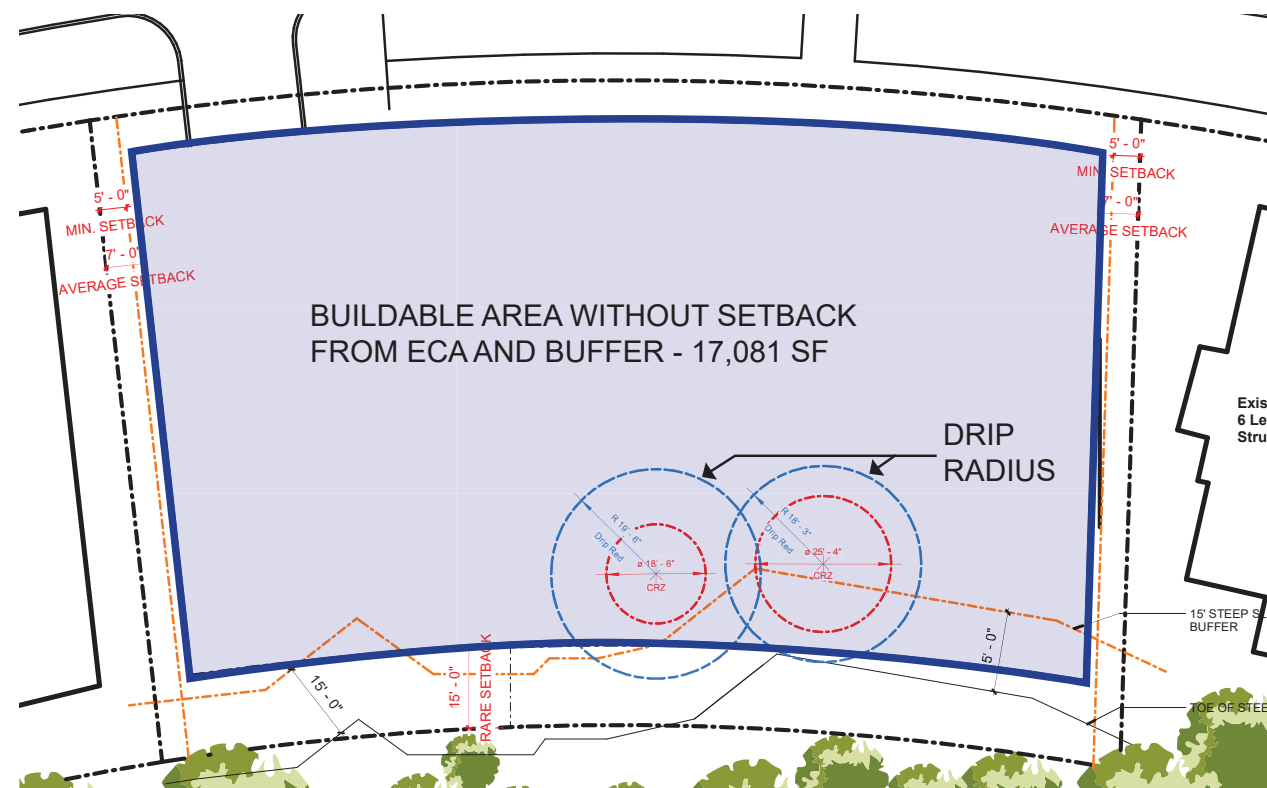
- 1 Potential street trees location, in coordination with SDOT
- 2 Private patio, TYP.
- 3 Entry
- 4 Existing light pole
- 5 Crosswalk
- 6 6' Sidewalk
- 7 Bench
- 8 Privacy planting between patios
- 9 Tree replacement plantings - there will need to be at least 4 trees



FIRST FLOOR LANDSCAPE PLAN



ROOF LANDSCAPE PLAN



Design challenges:

The steep slope and its buffer encroach the project site more than the surrounding parcels, (30' at most)

Two trees (one significant and one exceptional) are inside the shoreline boundary, which further reduce the development potential.

High water table at 10' below grade making it very difficult to meet the parking requirement.

DEPARTURE REQUEST- 1 // MAX BUILDING DIMENSION

DEPARTURE REQUEST #1

The width and depth limits of this Section 23.45.528 apply to lots greater than 9,000 square feet in MR zones.

A. The width of principal structures shall not exceed 150 feet.

B. Structure depth. The depth of principal structures shall not exceed 80 percent of the depth of the lot, except as provided in subsection 23.45.528.B.2.2. Exceptions to structure depth limit. To allow for front setback averaging and courtyards as provided in Section 23.45.518, structure depth may exceed the limit set in subsection 23.45.528.B.1 if the total lot coverage resulting from the increased. The maximum building dimension is 150' W by 96' D per 23.45.528.

PROPOSED:

Option 2 - proposed 185'-5" W x 95'-9" D

To reduce the long street front expression, the proposed design breaks the single massing into two smaller pieces, each with a facade width of less than 63' and separate them with a landscaped courtyard.

Option 3 - proposed 181'-9" W x 108'-5" D

To reduce the long street front expression, the proposed design breaks the single massing into multiple smaller modules at street facade, and stagger them into a trapezoid shape, resulting a 150' wide street facade

RATIONALE:

Option 2

Breaking down the single massing in order to emphasize the scale and characters of the neighborhood **CS2-B.1& D.** reducing perceived Mass **DC2.A.2** and shows respect to Adjacent sites **CS2-D.5**

Courtyard opens to both residents and public to encourage physical activity and social interaction **DC4.B.4**

Option 3 - preferred

Breaking down the single massing into residential scale with added secondary features, the recessed balconies add visual depth and interests. The residential unit entries at the street level create a residential edge and interaction. The further reduced scale fits with neighboring buildings. **DC2.C.1-3, DC2.A.2, PL3.B.2**

Massing steps into trapezoid shape with multiple indents to maximize the water view of both this project and neighboring site, and to maximize the access to light and air. **CS1.B/C**

With trees removed, the propose option will have enough area to get a ramp down to the below grade parking, which reduces the visual impact of surface parking or the cost of mechanical parking **DC1.B&C**



ONE SINGLE FLAT FACADE

CODE COMPLIANT OPTION MASSING



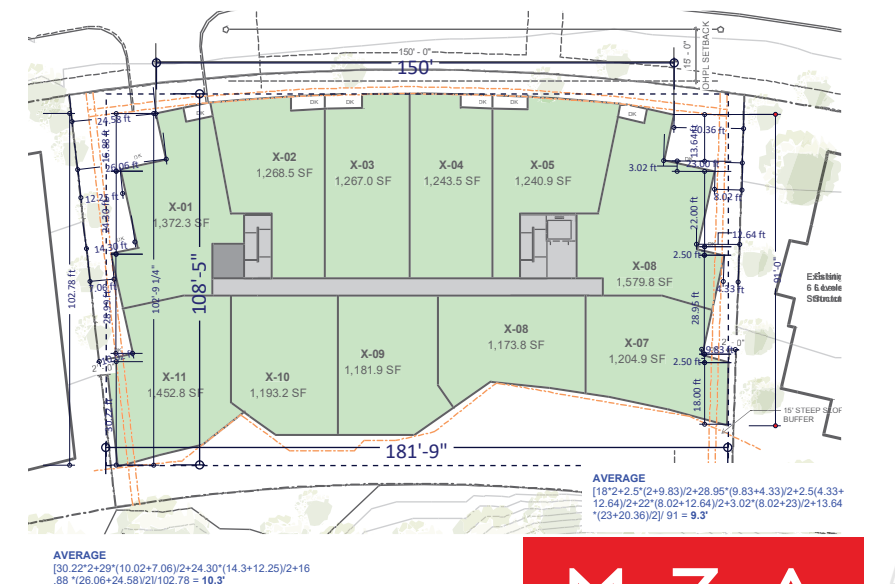
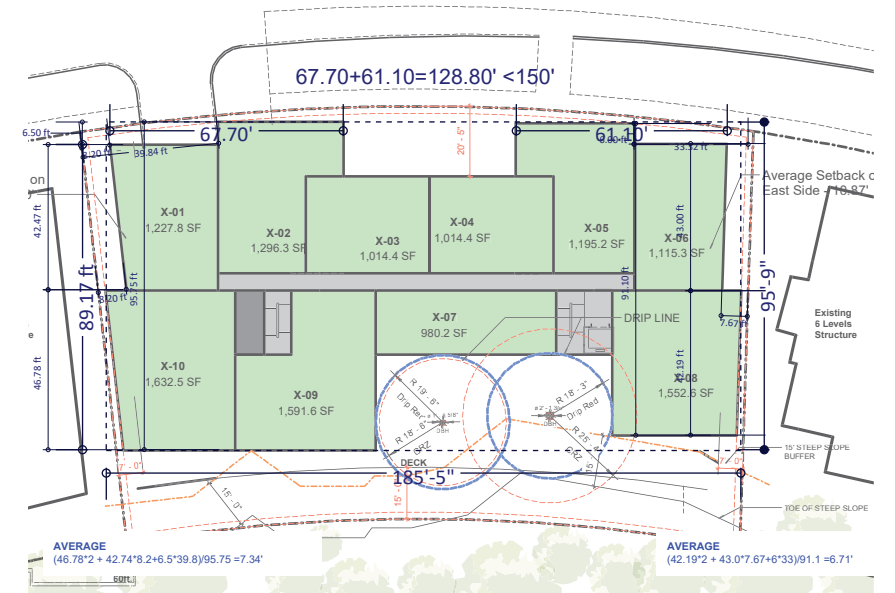
TWO 50FT LONG FACADES

OPTION 2 MASSING



MULTIPLE LAYERS OF SMALLER MODULES

PREFERRED OPTION MASSING



DEPARTURE REQUEST #2: 23.45.518 - Setbacks and separations

STANDARD: 23.45.518.B.1

Minimum setbacks for the MR zone are shown in Table B for 23.45.518 and subsection 23.45.518.B.2

SETBACK: Per SMC23.45.518, Table B

Front Setback: 5' min. 7' average

Side Setback: <42' in height: 5' min, 7' average

Side Setback: >42' in height: 7' min, 10' average

Rear Setback: 15' min

PROPOSED:

Option 2 Front Setback: 2' min. 20' max. at courtyard
 Side Setback <42': 2' min. 8' max. $(6.7+7.34)/2=7'$ average
 Side Setback <42': 2' min. 8' max. 7' average
 Rear Setback: 20' -48'. 33' average

Option 3 - Front Setback: 7' min. 44' max
 Side Setback <42': 2' min. $(9.3'+10.3')/2=9.8'$ average
 Side Setback <42': 2' min. 9.8' average
 Rear Setback: 15' -29'. 25' average

RATIONALE:

Option 2 -

The proposed option sets back from the steep slope & its buffer and with both trees retained, which normally would have earned additional height of 10' and +0.5 FAR, Since the project is mostly located inside the shoreline district, the additional height is not allowed.

To recapture the lost build-able area, the proposed option reduced the side setbacks to 2' min. in order to spread the massing apart to make room for the trees, which results in an interesting "tree-house" like setting for the units on the south. (**CS1-B.2** Maximize the daylight for interior spaces. **CS2-B.1& D.** reducing perceived Mass. Courtyard & backyard open to both residents and public to encourage physical activity and social interaction **DC4.B.4**)

Option 3 -

The proposed option sets back from the steep slope and its buffer, with both trees removed to create a useful size of ground floor that could accommodate a parking ramp to access below-grade parking. Parking is in high demand in the Alki parking overlay area as it is clearly shown in the community outreach survey. (**DC1.B&C** facilities for alternative transportation such as shared vehicles; : Locate parking below grade wherever possible to reduce visual impact, and to serve multiple Uses)

To minimize the impact on the neighboring buildings, the proposed option staged the units into a trapezoid shape to create generous side setbacks at the waterside and only reduced the side setback at the very back of the property, resulting in almost no view loss to the neighboring sites.

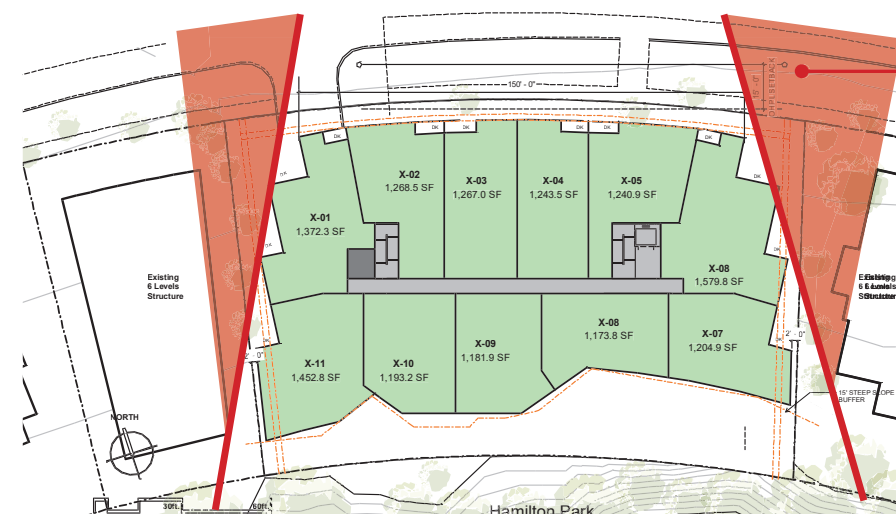
(**DC2.A.1&2:** Arrange the mass of the building taking into consideration the characteristics of the site; Use secondary architectural elements to reduce the perceived mass of larger projects. **DC2.C.3:** Fit With Neighboring Buildings)



CODE COMPLIANT UPPER LEVEL PLAN



OPTION 2 UPPER LEVEL PLAN



PREFERRED OPTION UPPER LEVEL PLAN

VIEW ANGLE

CS1-B.2 Maximize the daylight for interior spaces and minimize solar gain

CS1-C1: Land Form - follow the curve of the Alki ave and the contour of the hill

CS2-D.5 - respect to Adjacent sites