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1700 21st Ave S. Mixed-Use Building

SDCI #3035541-EG

Early Design Guidance

March 5, 2020

PUBLIC47ARCHITECTS
UP LAKE UNION PARTNERS

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DEVELOPMENT OBJECTIVES

The western edge of Seattle's Atlantic Neighborhood is rapidly changing. What has traditionally been an eclectic mix of low-rise industrial, commercial and residential buildings along the upper Rainier Valley corridor is expected to transform in response to the upcoming construction of a new Light Rail Station, and major zoning changes intended to address housing affordability. The new mixed-use building proposed at 1700 21st Ave S seeks to bring vitality and enhanced habitability to this evolving neighborhood through the addition of a thoughtfully-designed and well executed-building. With numerous up-coming developments planned for the immediate vicinity, this project has the opportunity to set a tone for how new construction can positively contribute to a changing neighborhood.

Development Objectives:

- Provide ±160 Apartment Units (6% Affordable per MHA Performance Method)
- Provide Affordable Units via MHA Performance Method
- Provide ±4 High-Functioning Live Work Units
- Provide ±3,000-sf of Street-Level Commercial Space
- Provide parking for ±65 vehicles



COMMUNITY ENGAGEMENT PLAN

Printed Outreach [High Impact]:

- Direct mailings to the residencies and business within 500ft radius of proposed site. Letters were written in following languages: Mandarin, Japanese, Vietnamese, Spanish, English
- Lake Union Partners posted translated flyers of community event & meeting of local community centers. We will be doing the 500ft letters
- Translated fliers were posted at apartment sites to notify residents of the community meeting and the coming project

Digital Outreach [High Impact]:

- Project hotline (information & Voicemail in the languages identified above) Project info added to DON's "Early Outreach for Design Review Blog. Project added to DON's "Outreach for design review calendar"
- Recorded information included all meeting dates, time, locations & understandable project information
- Email to all community stakeholders located on the neighborhood snapshot provided by the city of Seattle, noting that outreach can be used for Washington Public Records

In- Person Outreach [High Impact]:

- Community meeting of at least one hour of presentation/discussion of project hosted on December 16, 2019 at 6pm. Translators of certain neighborhood languages were present during community meeting to help neighbors truly understand the proposed project and be able to ask good and meaningful questions and voice concerns.



SUMMARY OF COMMUNITY FEEDBACK FROM THE PUBLIC OUTREACH MEETING

Preferred types of Commercial

Attendees expressed interest in the following types of retail:

- Trader Joes
- Craft Beer establishment to bridge a gap between Columbia City and more established neighborhoods to the north.
- Bubble Tea Shop
- Fitness Center / Gym
- Pet Grooming
- Tutoring Center
- Hair Salon
- Coffee Shop
- One attendee suggested vaping and cannabis shops should be avoided

Thoughts on Community Spaces

- One attendee suggested a safe spot for teens—possibly combined with an appropriate retail use, such as a bubble tea shop
- One attendee would like to see a community space (indoor or outdoor) that would not require spending money
- One attendee expressed interest in courtyard spaces
- One attendee expressed interest in high-volume (double height) gathering space or lobby similar to the type of spaces being built in South Lake Union
- Several attendees responded positively to the sidewalk fronting outdoor spaces at the Rooster—indicating an appreciation for expanded sidewalk with overhead coverage and a certain degree of vertical height.
- One attendee suggested a roof-top garden

Types of uses that would bring people to the site

- More businesses and shops

Influence of cars and bikes

- General feeling that cars have a large influence in this area and convenient parking is a desirable amenity
- One attendee noted that it's tough to make left and right turns in the area
- One attendee noted that underground parking is a good idea

Features that could contribute to a successful building

- Metal work adds character
- High ceilings at lobby / public spaces
- Artwork / sculpture / fountains / murals
- Visibility to landscaping and amenities in the building
- Use of a quality and timeless material, such as brick

COMMUNITY MEETING

• WHEN:

- Monday, December 16th, 6 – 7pm

• WHERE:

- Mt. Baker Village Community Room
- 2580 S. 29th Street, Seattle WA 98144

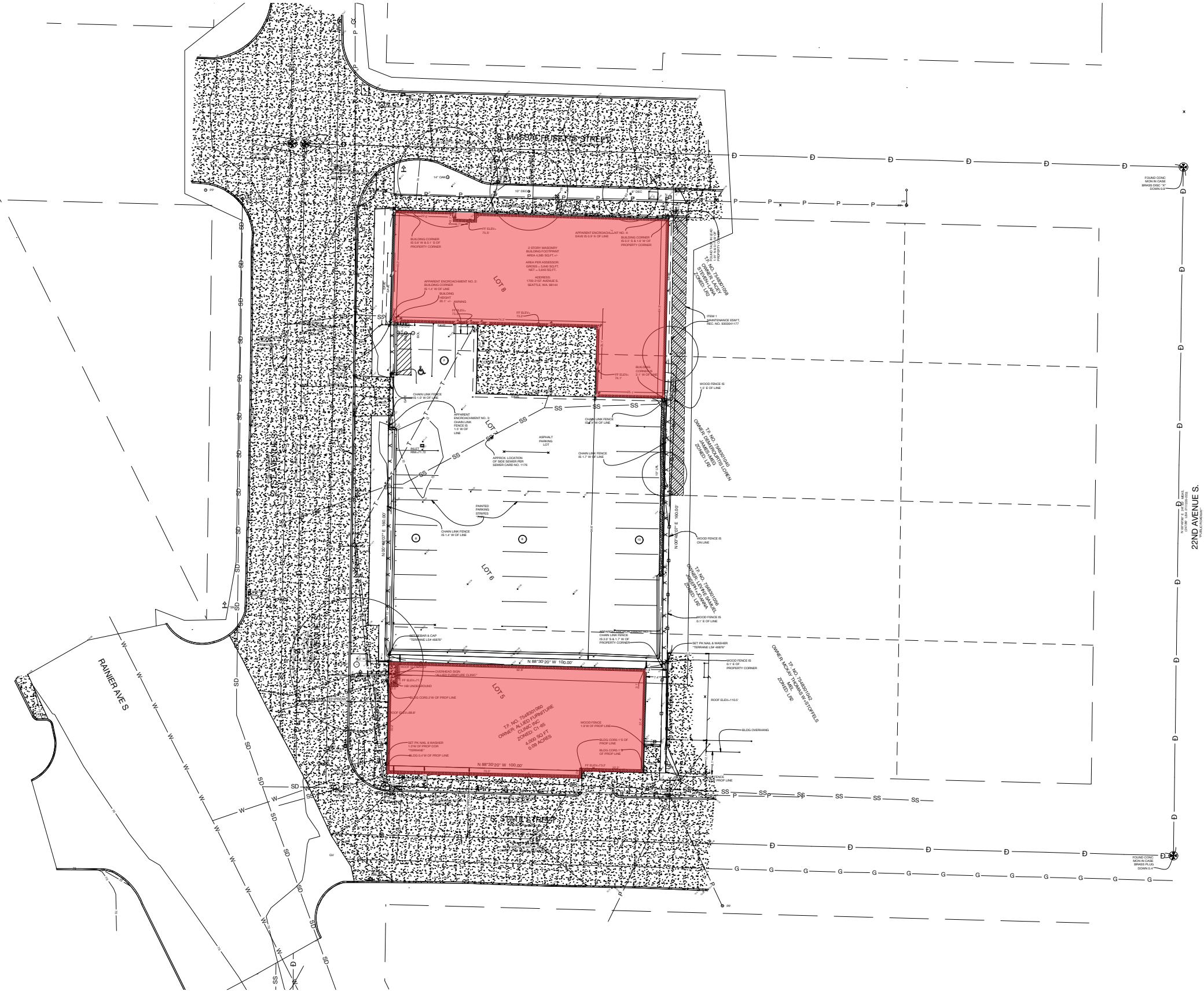
- WHAT: Discussion of future development at 1700 and 1716 21st Street South

Come to learn and share your thoughts about our proposed development at 1700 and 1716 21st Avenue S. The design and development team will be present to discuss the project schedule and solicit community feedback on the design.

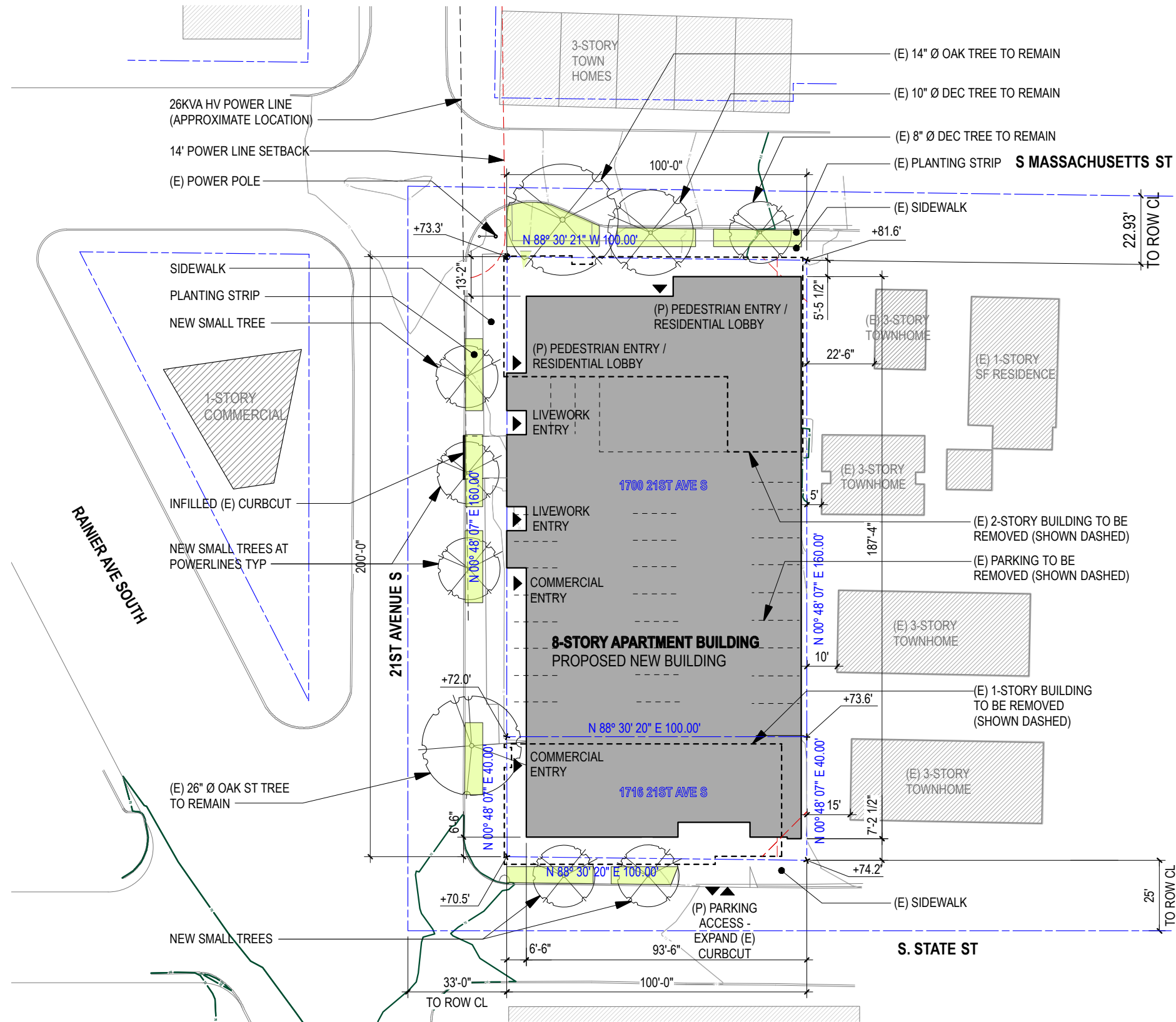
• MORE INFORMATION:

- 206-278-9071
- info@lakeunionpartners.com

TOPOGRAPHIC SURVEY



PRELIMINARY SITE PLAN



OWNER
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SITE ADDRESS
 1700 & 1716 21st Avenue South
 Seattle, WA 98144

PARCEL NUMBER
 1700 21st Ave South: #7548301085
 1716 21st Ave South: #7548301060

LEGAL DESCRIPTION
 1700 21ST AVE S:
 THE NORTH 10 FEET OF LOT 5, ALL OF LOTS 6, 7, & 8, IN BLOCK 25, SANDER'S SUPPLEMENTAL PLAT, ACCORDING TO THE PLAT THEREOF RECORDED IN VOLUME 1 OF PLATS, PAGE 210, IN KING COUNTY, WASHINGTON.

1716 21ST AVE S:
 THE SOUTH 40 FEET OF LOT 5, BLOCK 25, SANDER'S SUPPLEMENTAL PLAT, ACCORDING TO THE PLAT THEREOF RECORDED IN VOLUME 1 OF PLATS, PAGE 210, IN KING COUNTY, WASHINGTON.



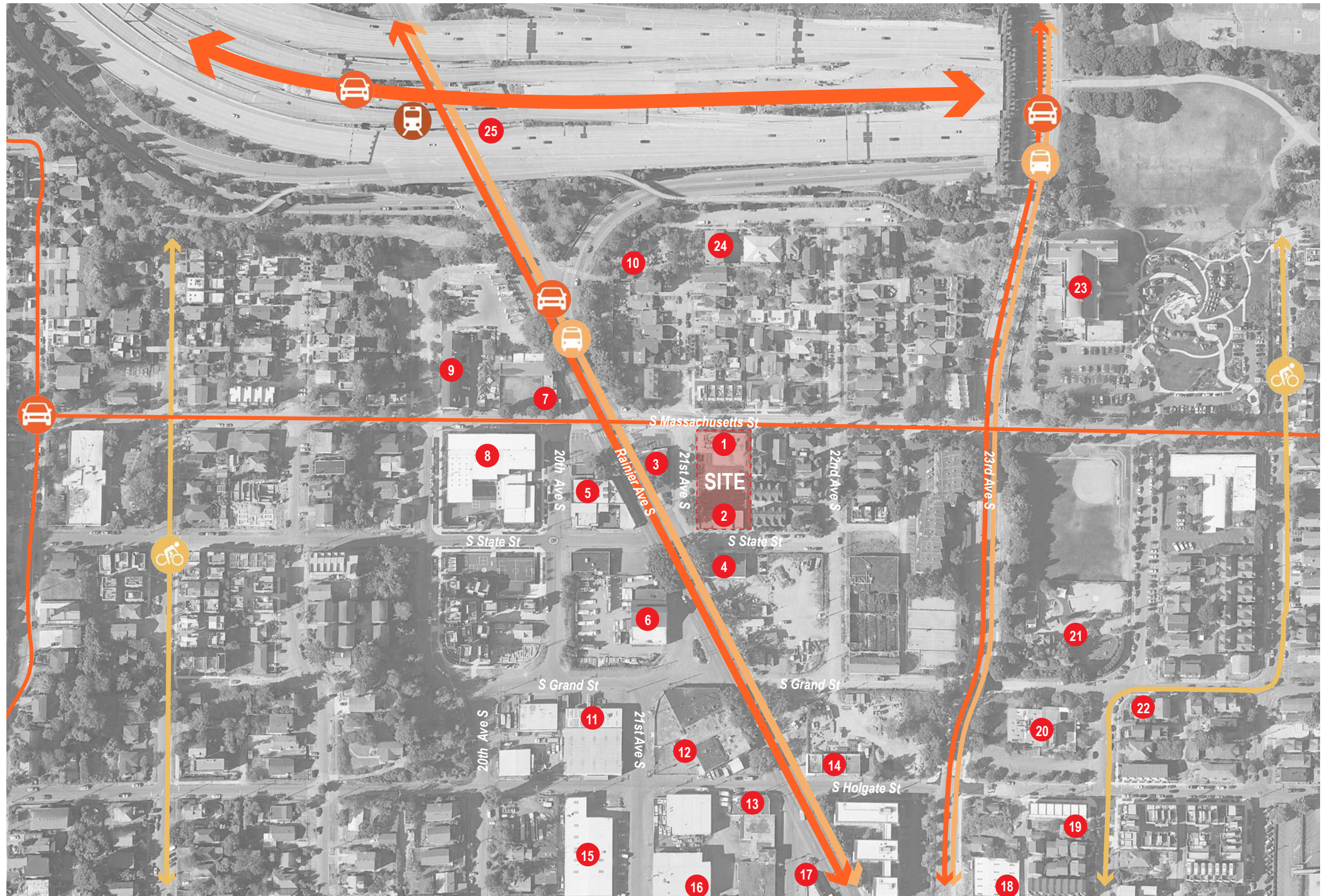
URBAN DESIGN ANALYSIS • NEIGHBORHOOD CONTEXT

Orientation

The subject property is located in Seattle's North Rainier Valley, also known as the Atlantic Neighborhood. It is bound on the north by South Massachusetts Street, which is a busy east-west arterial. 21st Avenue South and South State Street, which define the western and southern edges of the site respectively are more tranquil streets. The site is made up of two parcels, 1700 and 1716 21st Ave S. Two commercial buildings separated by a surface parking lot currently occupy the property. In addition to the commercial presence on the block, there are numerous single-family, multi-family and industrial buildings on adjacent blocks. The subject property is shielded to the west by a triangular-shaped parcel providing a barrier to the major arterial, Rainier Ave S. To the North Interstate-90 slices through the neighborhood, tunnelling under Mount Baker en route to Mercer Island.

- 1 Image Luxury Hair Studio (SITE)
- 2 Seattle Kettlebell Club (SITE)
- 3 Toshio's Teriyaki
- 4 Seattle Collision Center
- 5 Oberito Sausage Co. *TBD*
- 6 Stewart Lumber and Hardware Co.
- 7 Valley Gear & Transmission, Inc
- 8 Hamlin Robinson School
- 9 Eritrean Hall Community Center
- 10 Atlantic Street Park
- 11 Davis Door Service, Inc
- 12 A-1 Towing & Roadside Service *TBD*
- 13 Saffron Spice
- 14 Dere Auto
- 15 Smart Foodservice Warehouse Stores
- 16 Skeeter's Auto Rebuild
- 17 Dixon's Furniture
- 18 Autosalon at Leschi Seattle
- 19 Heroclip
- 20 Japanese Presbyterian Church
- 21 Seattle Children's Playgarden
- 22 My Mac Hero
- 23 Northwest African American Museum
- 24 Atlantic Street Center
- 25 Future Judkins Park Light Rail Station

TBD- to be developed



Vehicle Route
 Light Rail Station
 Bus Route
 Bicycle Route



URBAN DESIGN ANALYSIS • NEIGHBORHOOD CONTEXT

Use Map

- Institutional
- Commercial
- Future Mixed-Use
- Multi-Family
- Single Family



Zoning Map

- Commercial
- Neighborhood Commercial
- High-Density Multi-Family
- Low-Rise Multi-family
- Residential Small Lot



URBAN DESIGN ANALYSIS • CULTURAL + HISTORICAL CONTEXT

In 1891, the City of Seattle authorized the construction of an electric railway that would connect downtown Seattle and Renton. The Rainier Avenue Electric Railway established the right of way that would eventually become Rainier Avenue South and provided direct access from farms and saw mills in the southern areas to downtown Seattle, which was still rebuilding after the 1888 fire. The railway spurred development in Rainier Valley with communities like Hillman City, Columbia City and Atlantic City emerging as thriving neighborhoods.

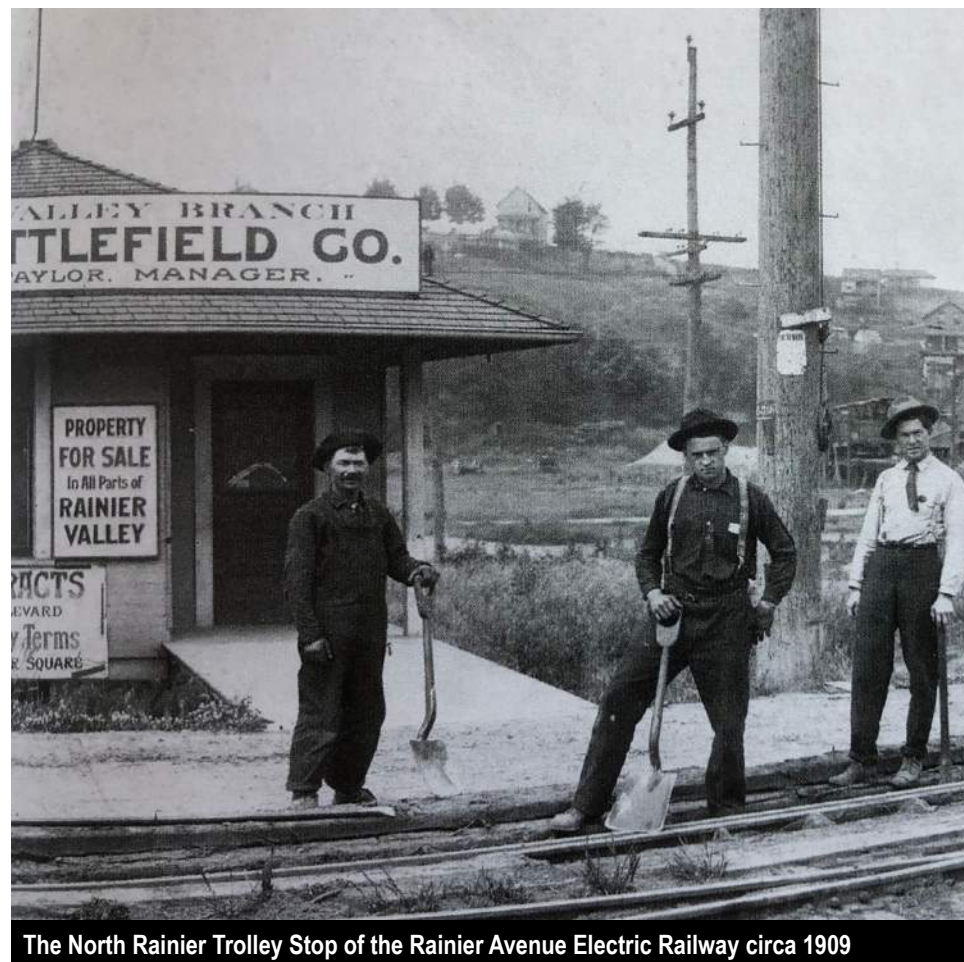
In the early part of the 20th Century, North Rainier Valley was attracted Italian immigrants, many of whom were drawn to the area for mining jobs. Their agricultural talents were put to use with a receptive market for produce and the area was nicknamed “Garlic Gulch”. The imprint of the Italian American community remains intact with such neighborhood icons as Oberto’s Sausage Company, Remo Borracchini’s Bakery, and the Atlantic Street Center, which was established in 1910 as a settlement house serving Italians in the area, but grew into an inclusive social services non-profit agency serving families of all ethnicities.

In 1940, Interstate-90 cut through Atlantic, tunnelling under the Mount Baker ridge en route to Mercer Island. The construction and subsequent widening of the I-90 corridor was devastating, permanently severing what had previously been a cohesive neighborhood.

In the 1930’s, Japanese immigrants, who had traditionally settled in the International District began moving south into North Rainier Valley. Their stay was cut short in 1942 with Executive Order 9066 which evacuated Japanese families to internment camps. While many never returned after this sad chapter in history, the Japanese community still has a strong presence in the neighborhood. The Japanese Presbyterian Church, which was established in 1907 and previously located at 9th and Weller prior to the construction of Interstate-5, was rebuilt at 24th Avenue and Grand Street in 1963 by celebrated Northwest architect, Paul Kirk. It continues to be an important gathering place for the Japanese American community and the neighborhood.

The African American community had traditionally settled in the Central District to the North of Atlantic. Discriminatory housing practices and racial covenants became common after the Great depression making it difficult for minority groups to buy or rent homes anywhere in the city except for specific “redlined” areas. With these policies in place, neighborhoods to the north and west were off limits to African Americans, forcing them to move south into Rainier Valley. By the 1990 there were more African American residents in Rainier Valley than the Central District. In 2008, the Northwest African American Museum opened in the neighborhood with the mission to “spread knowledge, understanding, and enjoyment of all histories, arts and cultures of people of African descent for the enrichment of all.” The museum occupies the first floor of the Coleman School with 25 affordable housing units above.

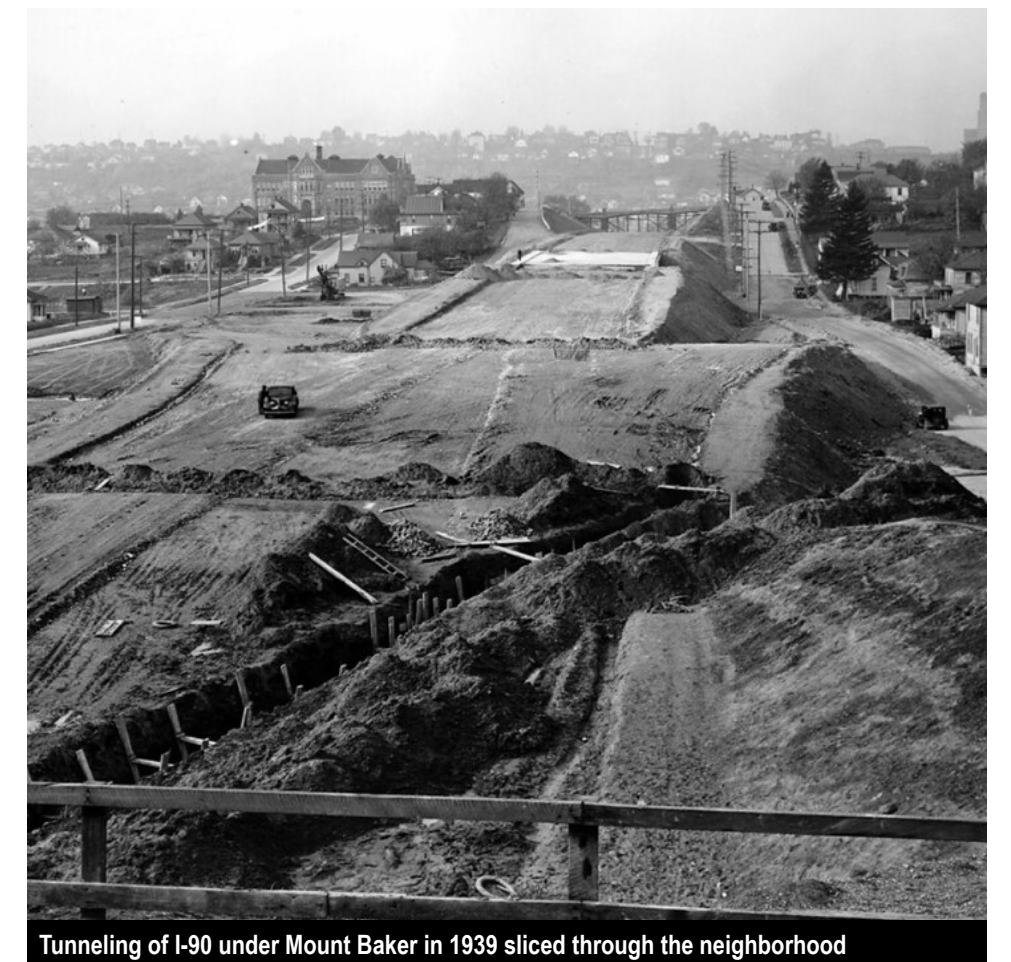
In recent decades, the multi-cultural aspect of North Rainier Valley has been enriched by waves of immigrants from Southeast Asia, Latin America, and Africa.



The North Rainier Trolley Stop of the Rainier Avenue Electric Railway circa 1909



Italian American Farmers in “Garlic Gulch” circa 1940. Note Sick Stadium beyond.



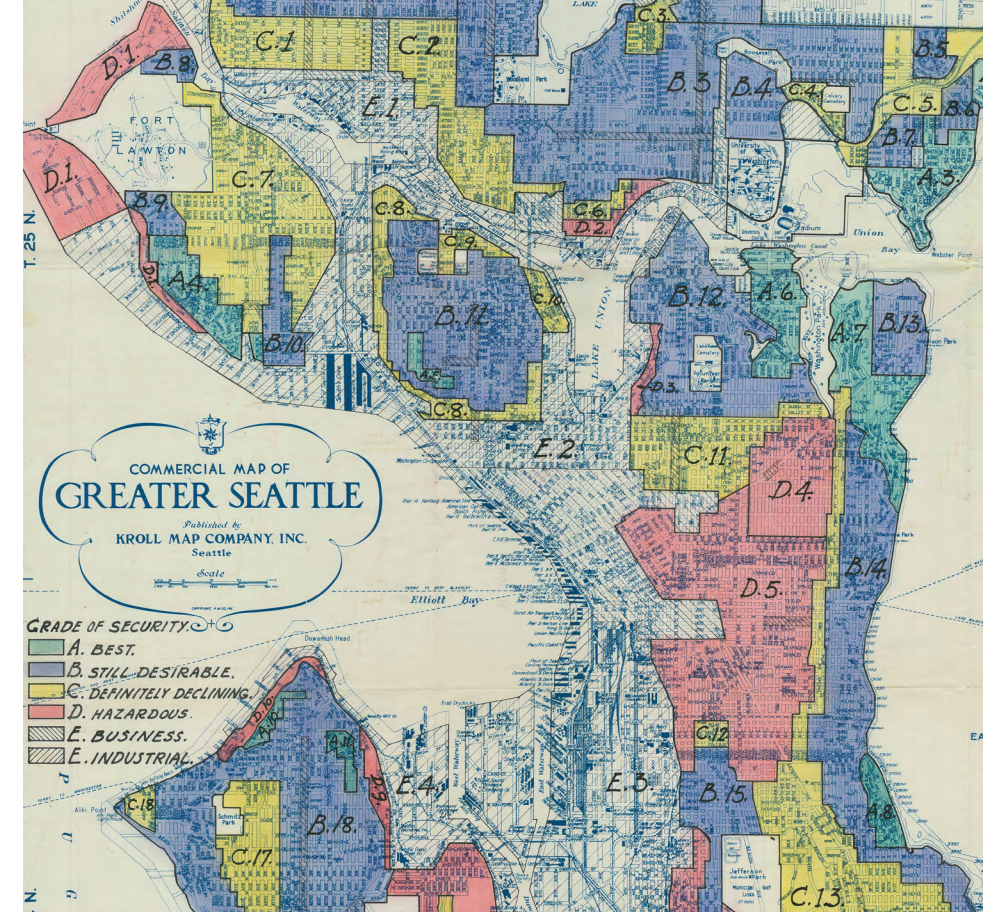
Tunneling of I-90 under Mount Baker in 1939 sliced through the neighborhood



The Atlantic Street Center was a cornerstone of the evolving community



The Japanese Presbyterian Church (1801 24th Ave) built in 1963



Redlining restricted housing options for African Americans and other minority groups



The Charlotte Howland Building has been home to the Atlantic Street Center since 1928



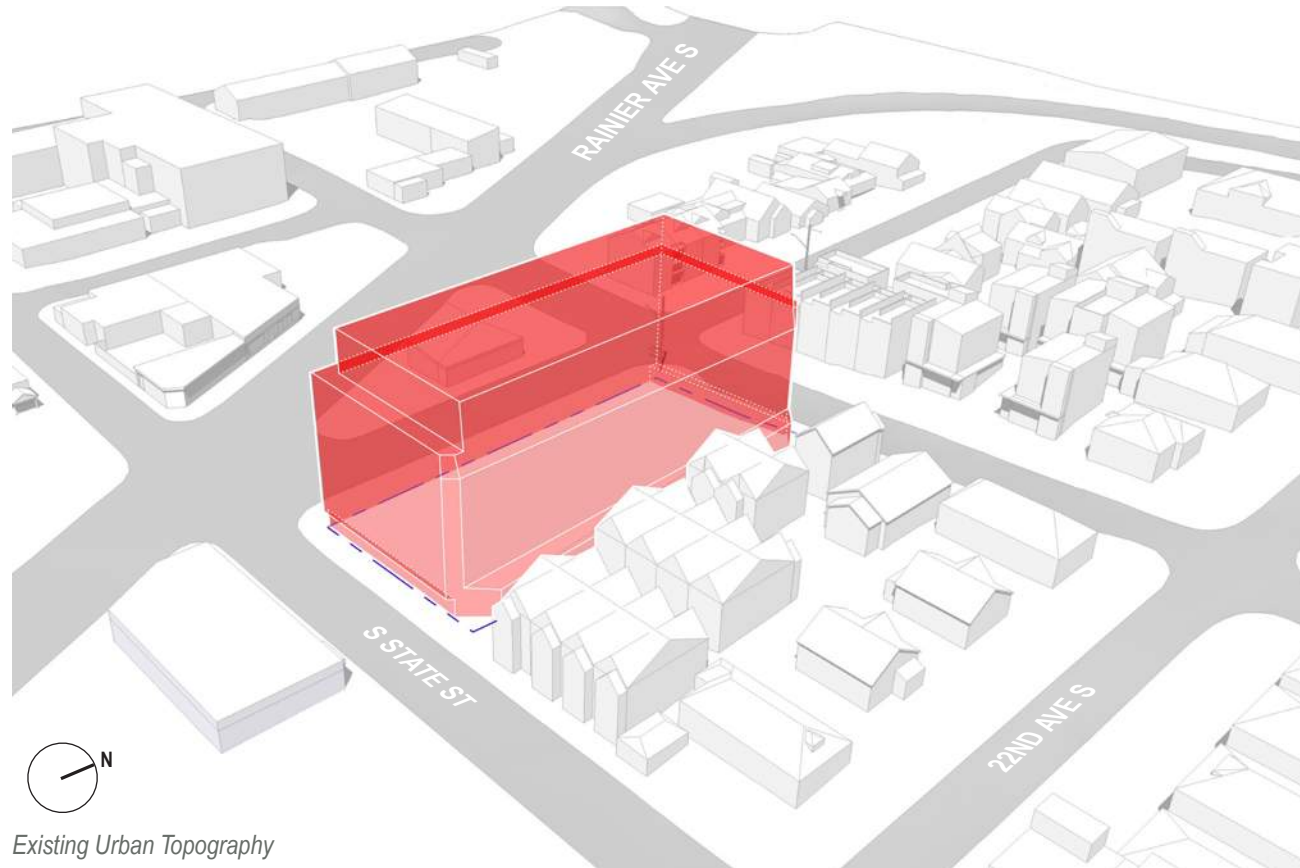
Japanese Americans were abruptly relocated from Seattle to internment camps in 1942



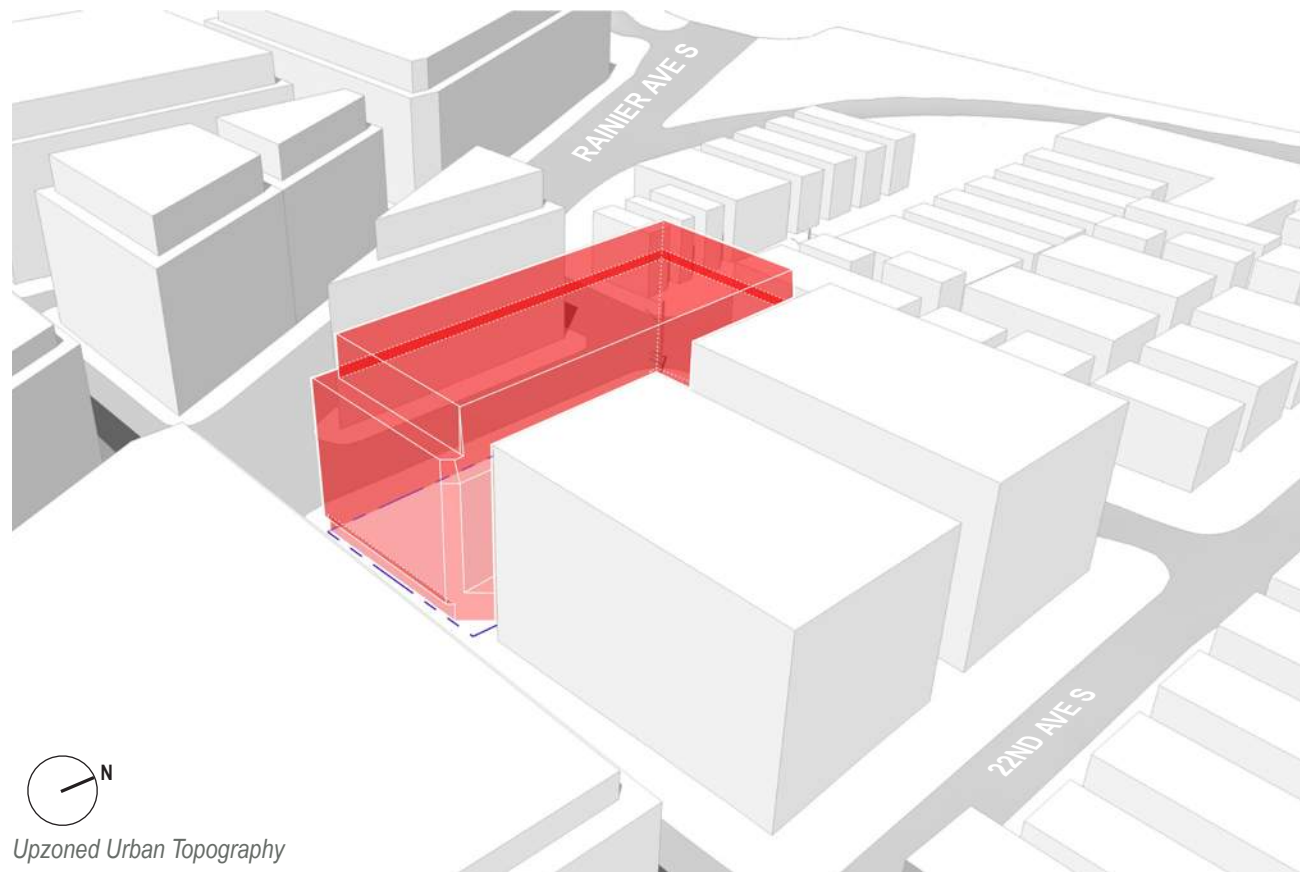
The Northwest African American Museum opened in the Coleman School Building in 2008

URBAN DESIGN ANALYSIS • UPZONING + UPCOMING DEVELOPMENT

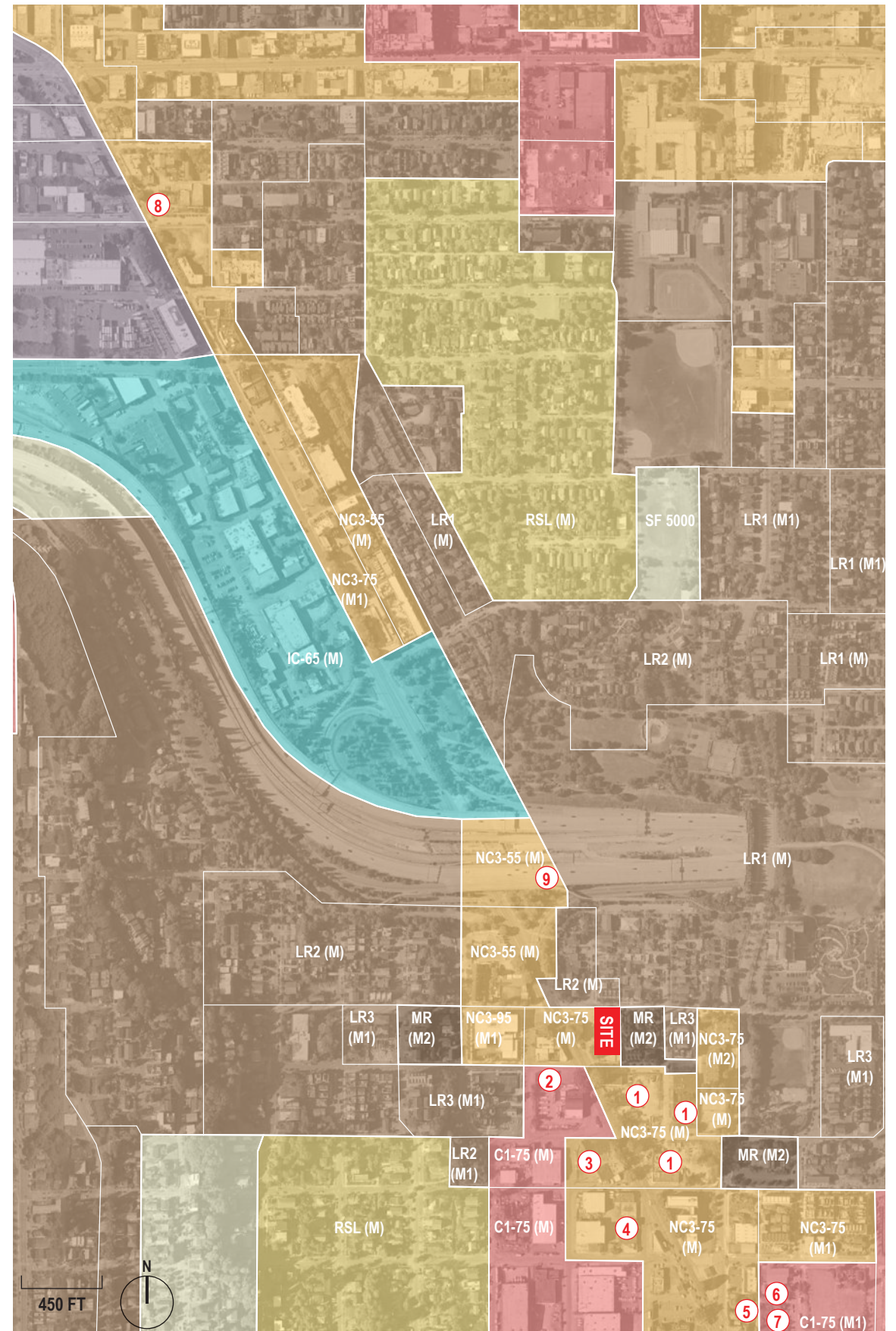
The recently adopted Mandatory Housing Affordability (MHA) legislation requires new buildings to provide affordable housing or contribute to the Seattle Office of Housing fund to support the development of affordable housing. The adoption of MHA is associated with changes to zoning that generally increase density, allowable structure height, and Floor Area Ratio. The up-zoning of the subject property and surrounding blocks has already stimulated redevelopment of the area. The construction of the nearby Judkins Park Light Rail Station further encourages the inevitable transformation of the neighborhood. The project has an opportunity to set a tone for how new construction can positively shape the urban experience.



Existing Urban Topography



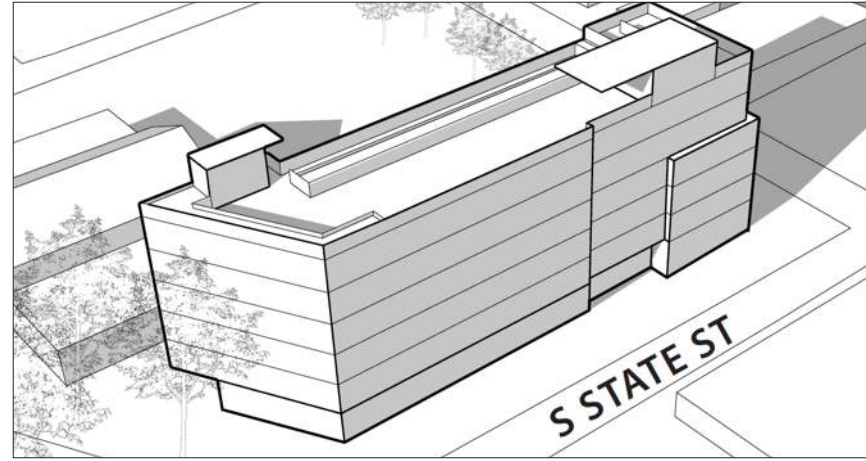
Upzoned Urban Topography



URBAN DESIGN ANALYSIS • UPZONING + UPCOMING DEVELOPMENT



1 Grand Street Commons Proposed 7-story mixed-use apartment blocks (3)



2 2007 S. State St Proposed 6-story apartment building



3 1801 Rainier Ave S. Proposed 7-story apartment building



4 1911 22nd Ave S. Proposed 6-story Mixed-use building



5 1911 22nd Ave S. Proposed 6-story Mixed-use building



6 2016 23rd Ave S. Proposed 6-story apartment building



7 2019 24th Ave S. Proposed 4-story apartment building



8 622 Rainier Ave S. Proposed 7-story Mixed-use building

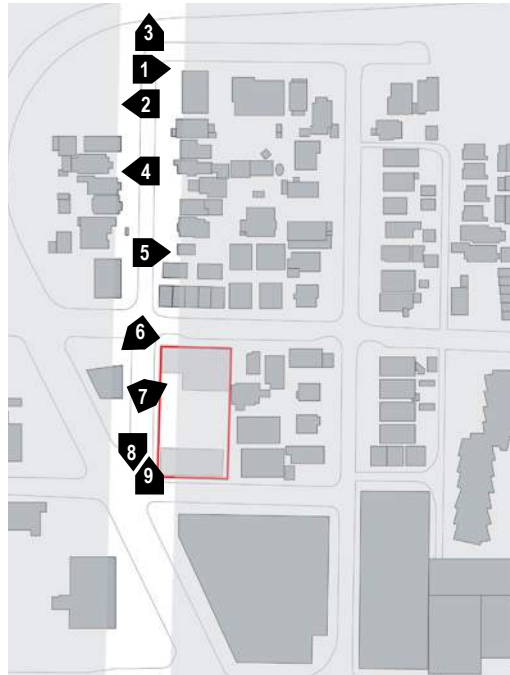


9 Judkins Park Light Rail Station

URBAN DESIGN ANALYSIS • STREET CONTEXT

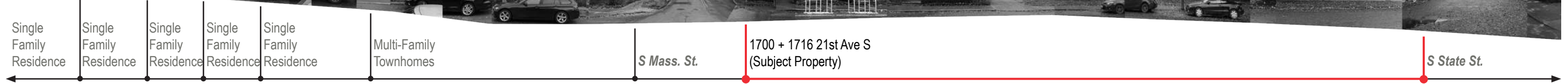
21st Avenue South

21st Avenue S is a relatively tranquil street that is abruptly terminated at a five-way intersection with Rainier Avenue and State Street. The wedge-shaped block between 21st and Rainier houses Toshio's Teriyaki and serves to buffer the subject property from the noise and activity of Rainier. As a result, 21st Avenue South has the opportunity to be a pedestrian friendly commercial stretch. North of the property 21st Avenue becomes decidedly more residential in nature with a mix of contemporary townhouses and single family residences. The Atlantic Street Center which was built in 1910 to serve the Italian community anchors the north end of 21st before it is abruptly terminated by I-90.

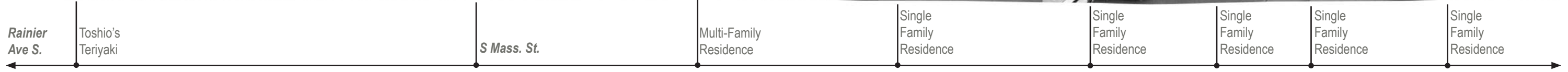


- 1 Atlantic Street Center
- 2 Atlantic Street Playground
- 3 Interstate-90 abruptly terminates 21st Ave
- 4 Single Family Residences
- 5 New Townhouses
- 6 Toshio's Teriyaki
- 7 Image Luxury Hair Studio (SITE)
- 8 5-way intersection at Rainier + State
- 9 Looking North up 21st Ave





21ST AVENUE SOUTH PHOTO-MONTAGE LOOKING EAST



21ST AVENUE SOUTH PHOTO-MONTAGE LOOKING WEST

URBAN DESIGN ANALYSIS • STREET CONTEXT

South Massachusetts St

The subject property is bound on the north by South Massachusetts Street, which is a major east-west arterial. The traffic on this road is relatively constant with increases at rush hour. East of Rainier, South Massachusetts is predominantly residential in nature with townhouses adjacent to and across from the subject property giving way to single family residences to the east. West of Rainier, uses are predominantly commercial and industrial.



- 1 S Massachusetts is a busy arterial
- 2 Single Family Residences
- 3 Single Family Residences
- 4 Single Family Residences + Townhouses
- 5 Single Family Residences with stoops
- 6 Image Luxury Hair Salon (SITE)
- 7 Townhouses
- 8 Townhouses
- 9 Looking West towards Rainier Ave S



SOUTH MASSACHUSETTS STREET PHOTO- MONTAGE LOOKING NORTH

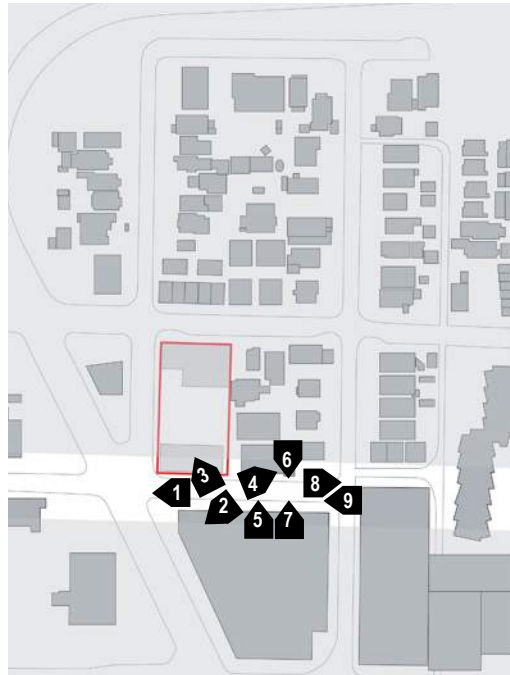


SOUTH MASSACHUSETTS STREET PHOTO- MONTAGE LOOKING SOUTH

URBAN DESIGN ANALYSIS • STREET CONTEXT

South State Street

The subject property is bound to the south by South State Street. Seattle Kettlebell Club, which currently occupies the site, sits on the north side of the Street with Seattle Collision on the south side. Both of these buildings will be demolished to make way for new mixed-use buildings. The development to the south will extend from the intersection of State and Rainier a full two blocks to the east, replacing industrial buildings and vacant tracts of land with street-level commercial uses and the promise of an enhanced pedestrian environment. To the east of the subject property, 3-story townhouse developments populate the north side of the street, however recent zoning changes have increased the height limit of these properties to 80', which may impact future patterns of development.



- 1 Looking West towards Rainier Ave S
- 2 Seattle Collision Center - to be redeveloped
- 3 Seattle Kettlebell Club (SITE)
- 4 Townhouses to the East of Site
- 5 Minimal 5' setback at mid-block townhouses
- 6 Vacant land - future Grand Street Commons
- 7 Townhouses and Single Family Residence
- 8 East termination of State Street
- 9 Looking West down State Street



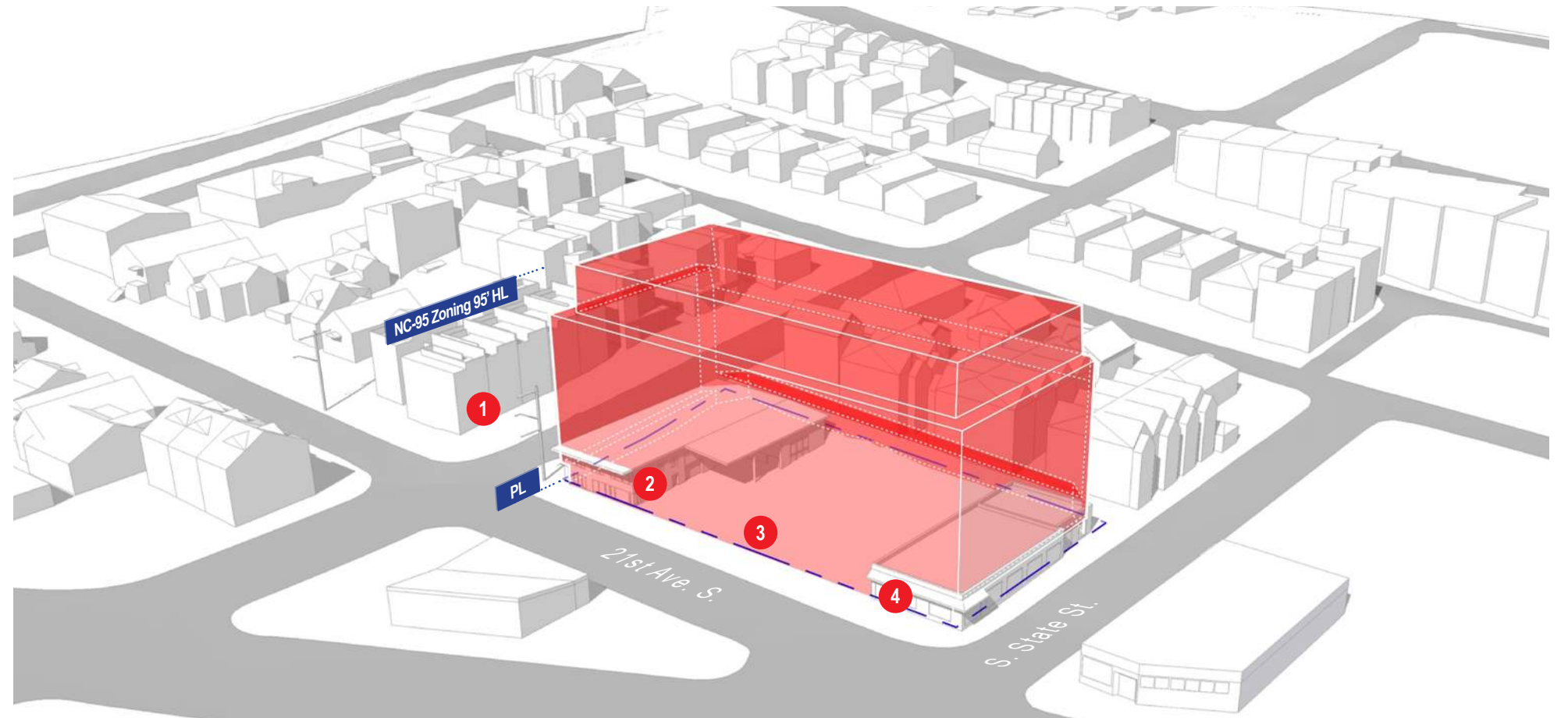
SOUTH STATE STREET MONTAGE LOOKING NORTH



SOUTH STATE STREET MONTAGE LOOKING SOUTH



1. Pole and HV Power Line (requires 14' power line setback shown in diagram)



Zoning Envelope 



2. Image Luxury Hair Studio + Commercial



3. Surface Parking Lot



4. Seattle Kettlebell Club

PRIORITY DESIGN GUIDELINES

CONTEXT + SITE

*Denotes Central Area Neighborhood Guideline

CS2 Urban Pattern + Form

Strengthen the most desirable forms, characteristics, and patterns of the streets, block faces and open spaces in the surrounding area.

CS2.A1 Sense of Place: ...create a sense of place where the physical context is less defined.
CS2.A2 Architectural Presence: Evaluate the degree of visibility or architectural presence that is appropriate or desired given the context.

CS2.B2 Connection to the Street: Identify opportunities for the project to make a strong connection to the street and carefully consider how the building will interact with the public realm.

CS2.C1 Corner Sites: Corner sites can serve as gateways or focal points

CS2.D3 Zone Transitions: For projects located at the edge of different zones, provide an appropriate transition or complement to adjacent zone.

CS2.D5 Respect for Adjacent Sites: Respect adjacent properties with the design and site planning...

CS3 Architectural Context + Character

Contribute to the architectural character of the neighborhood.

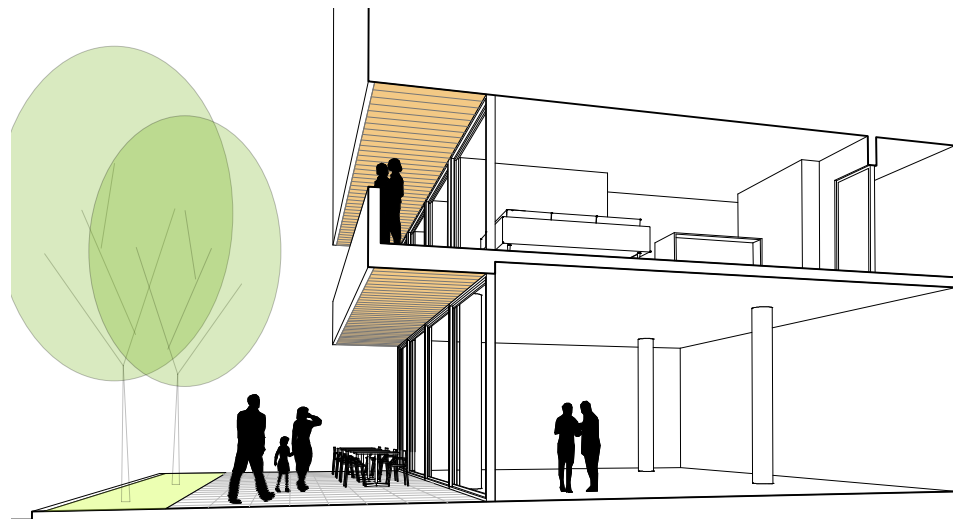
CS3.A4 Evolving Neighborhoods: In neighborhoods where architectural character is evolving or otherwise in transition, explore ways for new development to establish a positive and desirable context for others to build upon in the future.

***CS3.1 Neighborhood Context:** c) Include high ceilings in ground level spaces...

RESPONSE:

The immediate context of the site is in a state of transition, with numerous new buildings in early planning stages. Accordingly, the project will be designed to set a high-bar for future development in the neighborhood. The southwest corner of the site is especially prominent and, in conjunction with the upcoming development to the south, has an opportunity to form a gateway to a new shopping district. 21st Ave and State Street offer strong potential to host pedestrian friendly retail. Massing along these sides will set back at street-level to create a widened sidewalk zone. The preferred alternative elevates this set back to the second story, creating a volumetric extension of the sidewalk which can host outdoor seating, vending, and other pedestrian-oriented activities.

The site abuts an MR zone occupied by 3-story townhouses to the east. The preferred massing proposes a pair of courtyards that triples the required setback, breaks up the length of the east façade, and provides opportunity for lush landscaping to buffer the properties from one another.



Double-height Setback at Street Level expands public realm (CS2, CS3, PL3)

PUBLIC LIFE

*Denotes Central Area Neighborhood Guideline

PL1 Connectivity

Complement and contribute to the network of open spaces around the site and the connections among them.

PL1.A.2 Adding to Public Life: Seek opportunities to foster human interaction through an increase in the size and/or quality of project related open spaces available for public life.

PL1.C.3 Year-Round Activity: Where possible, include features in open spaces for activities beyond daylight hours and throughout seasons of the year.

***PL1.1 Accessible Open Spaces:** a) Provide safe and well connected open spaces; b) Larger projects around important neighborhood nodes should create generous recessed entries, corner plazas, and more usable open space adjoining the streets.

PL2 Walkability

Create a safe and comfortable walking environment that is easy to navigate and well-connected to existing pedestrian walkways and features.

PL2.B1 Eyes on the street: Seek opportunities to foster human interaction through an increase in the size and/or quality of project related open spaces available for public life.

PL2.B3 Street level transparency: Ensure transparency of street level uses (for uses such as nonresidential uses or residential lobbies), where appropriate...

PL2.C1 Weather Protection Locations and Coverage: Overhead weather protection is encouraged and should be located at or near uses that generate pedestrian activity...



Integrated Balconies with Visual Connection to Sidewalk (PL2, PL3, DC2)

PL3 Street-Level Interaction

Encourage human interaction and activity at street-level with clear connections to building entries and edges.

PL3.A1 Entries: Design primary entries to be obvious, identifiable, and distinctive... Common entries to multi-story residential buildings need to provide privacy and security for residents, but also be welcoming and identifiable to visitors.

PL3.B3 Buildings with Live/Work Uses: Maintain active and transparent facades in the design of live/work residences that are required to orient the non-residential portions of the unit toward the street.

PL3.B4 Interaction: Provide opportunities for interaction among residents and neighbors.

PL3.C1 Porous Edge: Engage passerby with opportunities to interact visually with the building interior using glazing and transparency...make a physical and visual connection between people on the sidewalk and retail activities in the building.

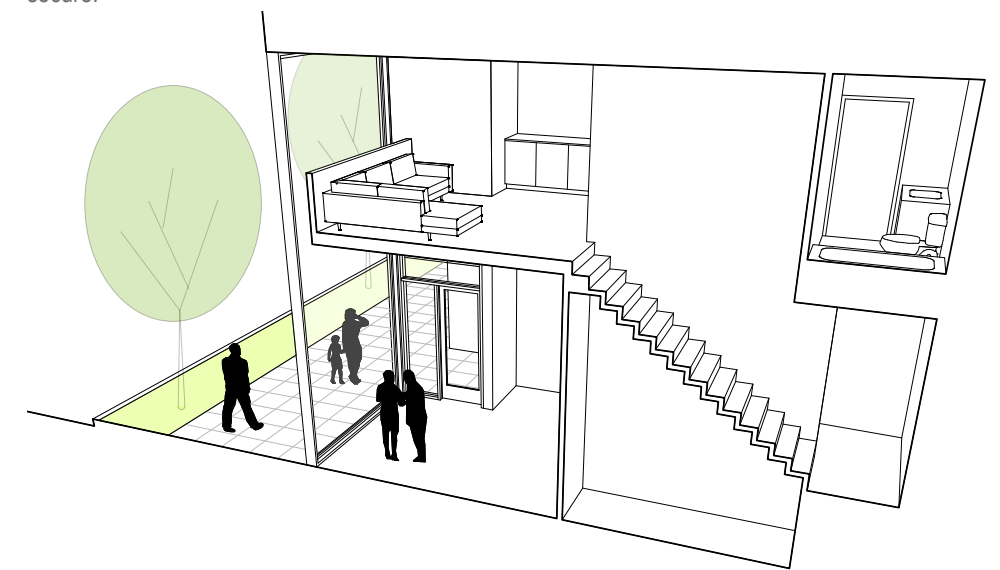
PL3.C3 Ancillary Activities: Allow spaces for activities such as sidewalk vending, seating, and restaurant dining to occur. Consider setting structure back from street...

***PL3.1 Frontages:** c) Promote transparency and "eyes on the street"; f) Live/work spaces should be designed to activate street frontage

***PL3.1 Streetscape Treatment:** c) ...provide overhead weather protection at all non-residential frontages; g) Provide voluntary space abutting the sidewalk right-of-way for businesses to utilize; h) Encourage a safe, comfortable environment for pedestrians...

RESPONSE:

The design will provide widened sidewalks that tuck under the upper-level building massing, providing expanded and weather-protected publicly accessible space. The preferred alternative locates indoor and outdoor amenity spaces at the second level, overlooking the sidewalk and connecting visually with street-activity. Outdoor common areas at the second level are covered, which will extend their use into the rainy seasons. Integrated balconies at the residential levels provide additional "eyes on the street" and opportunities to interact with sidewalk activity. Commercial spaces will be glassy and opportunities for operable glazing features that allow interior activities to spill out onto the sidewalk will be explored during design development. 2-level live/work units will afford ample street-level transparency without compromising the privacy for the residential component. The residential entry will be designed to be identifiable, welcoming, and secure.



2-Level Live/Work Units Facilitate Street-Level Transparency (PL3)

DESIGN CONCEPT

*Denotes Central Area Neighborhood Guideline

DC2 Architectural Concept

Develop an architectural concept that will result in a unified and functional design that- fits well on the site and within its surroundings.

DC2.A Massing: 1) Arrange mass of the building taking into consideration the characteristic of the site and proposed uses of the building and its open spaces; 2) Use secondary architectural elements to reduce the perceived massing of larger projects.

DC2.B Architectural and Façade Composition: 1) Design all building façades considering the composition and architectural expression of the building as a whole

DC2.C1 Visual Depth and Interest: Add depth to facades where appropriate by incorporating balconies, canopies, awnings, decks, or other secondary elements into the façade design.

DC2.D Scale and Texture: 1) Incorporate architectural features, elements, and details that are of human scale into the building façade... 2) Design the character of the building, as expressed in the form, scale and materials, to strive for a fine-grained scale, or “texture”...

***DC2.1 Building Layout and Massing:** a) Project concepts should be intelligible and clear; d) Appropriately scale building so that they relate to the scale and form of the adjacent public realm; e) Consider all sides of the building and the impact each façade has on its immediate neighboring context....

DC3 Open Space Concept

Integrate open space design with the design of the buildings so that each complements the other.

DC3.A1 Interior/Exterior Fit: Develop an open space concept in conjunction with the architectural concept to ensure that interior and exterior spaces relate well to each other

DC3.B3 Connection to Other Open Space: Site and design project-related open spaces to connect with, or enhance the uses and activities of other nearby public open spaces...

***DC3.1 Common Open Spaces:** a) Where possible, provide common courtyards and yards that are publicly visible and accessible

DC4 Exterior Elements + Finishes

Use appropriate and high-quality elements and finishes for the building and open spaces.

DC4.A1 Exterior Finish Materials: Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged.

***DC4.1 Building Details and Elements:** a) Provide operable windows in a way that promotes natural ventilation; b) Incorporate building materials that reflect human scale and craftsmanship of the building process; d) Façades should exhibit a rhythm of fenestration, and transparency of the inside program out to the public realm.

RESPONSE:

The proposed building will present a cohesive, singular image for the prominent SW corner of 21st and State Street. Thoughtfully detailed, high-quality materials will wrap the street façades of the building and a glass articulated base will enclose the commercial base, providing visual connection between interior activity and sidewalk life. A layered strategy with integrated balconies will provide porosity and depth to the street-facing elevations. The preferred alternative proposes a two-story street-level setback for much of the building, expressing the concrete construction of the lower levels distinctly from the wood-framed construction of the upper levels.

The preferred scheme provides common amenity rooms at the second level that spill out onto covered outdoor plazas. These plazas connect to the adjacent courtyards around which the building is organized. The sequence of covered and open outdoor spaces at the second level will be visually accessible from the sidewalk. Opportunities to reveal landscaping in the courtyard to the building exterior will be pursued.

High quality materials that provide a sense of scale and texture, such as brick or corrugated metal panel will be explored during design development.



Well-Defined Entry (PL3.A1) + Exterior Material with Texture (DC4.A1)



Building Wholeness, Façade Composition, Layering + Depth (DC2)



Material + Texture: Brick (DC4)



Material + Texture: Metal Panel (DC4)



Upper-Level Landscape + Open Space Visually Accessible from Street Level (DC3)

PRIORITY DESIGN GUIDELINES

ADDITIONAL GUIDANCE

*Denotes Central Area Neighborhood Guideline

*A.1: Character Areas

Contribute to architectural and placemaking character with local history and cultural references.

*A1.1 History and Heritage:

- a) Provide design features to express the African and Black American presence in the neighborhood. Create 'pockets of culture' to represent both the Black American identity within the Central District, as well as other heritages that have had a large impact on the Central Area's past.
- b) Consider including visual arts as an integral part of the design concept along main street building façades, within highly trafficked pedestrian areas, and within open spaces.
- c) Use any resulting blank walls and surfaces for the visible expression of art that references the history, heritage, and culture of the community.
- d) Include interpretive opportunities (through visual art, signage, markers, etc.) that tell the story of the neighborhood's history in engaging ways.

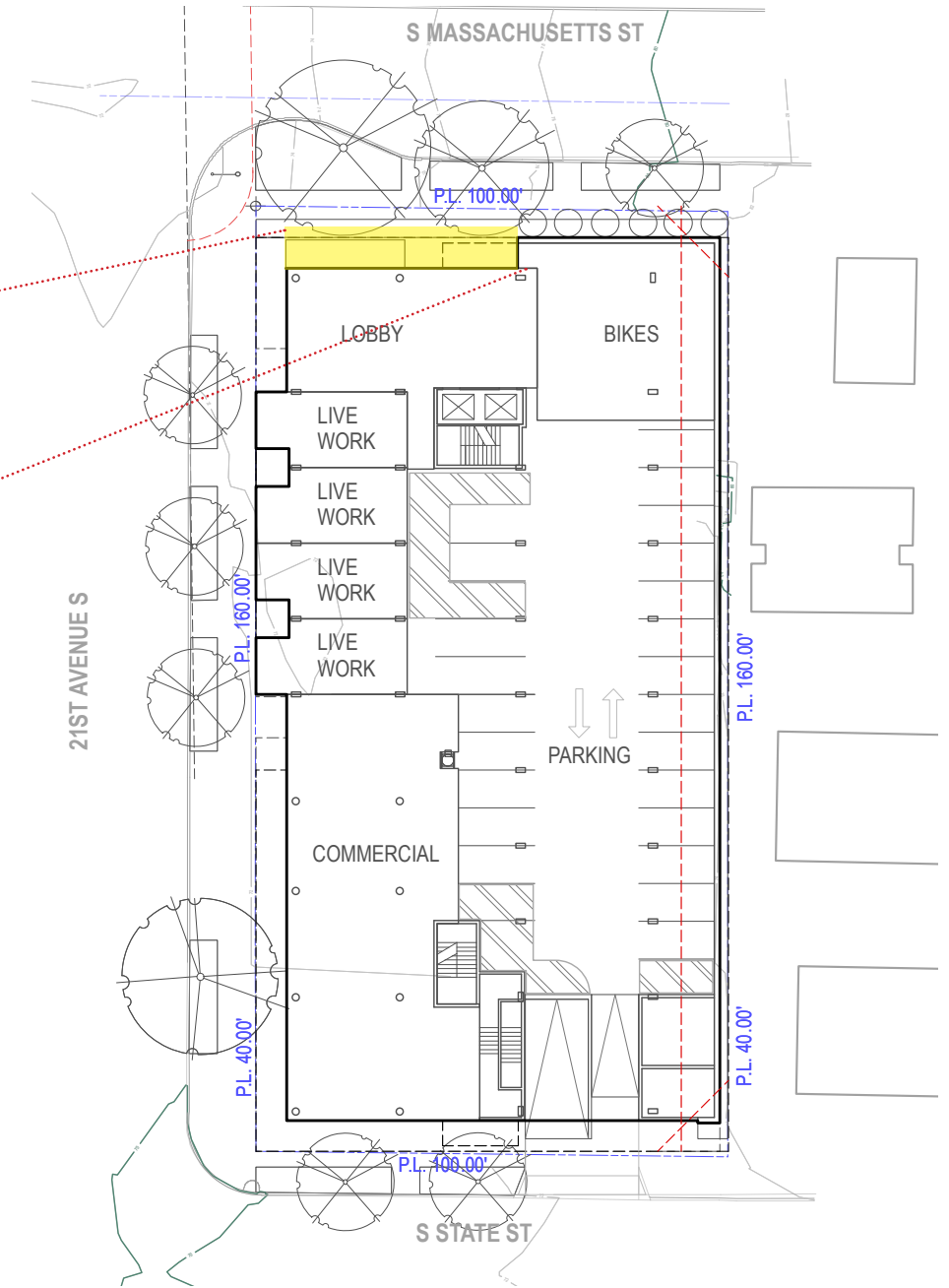
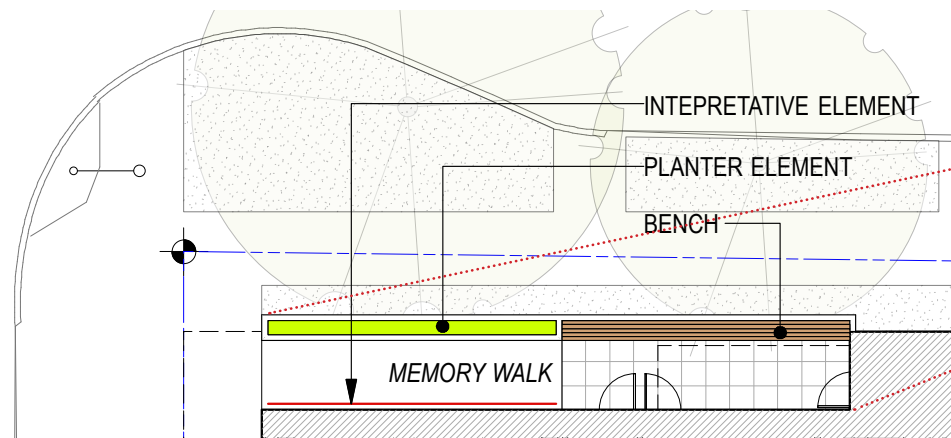
RESPONSE:

The Rainier Valley is one of the most racially and culturally diverse neighborhoods in the United States. North Rainier Valley, which was historically known as the Atlantic Neighborhood, was home to culturally rich communities of immigrants and marginalized populations, including Italian Americans in the early part of the 20th Century, Japanese Americans prior to 1942, and African Americans whose housing options were limited by discriminatory housing policies. The once cohesive neighborhood was irreparably severed by the construction of I-90 in 1940. But recent changes in the zoning code and the construction of the new Judkins Park Light Rail Station promise to bring new vitality.

Inspired by the community-oriented spirit of the populations that have made the Atlantic Neighborhood their home, the project prioritizes the creation of a vibrant and safe pedestrian

realm. The preferred design promotes a high level of openness between interior and exterior spaces through ample glazing, the integration of balconies into a layered façade, and larger communal outdoor spaces that overlook the sidewalk. This strategy takes "eyes on the street" a step further, encouraging interaction between home-life and public-life.

The project will also pursue an opportunity to honor the history of the Atlantic Neighborhood through the inclusion of a "memory walk" at the north side of the building, along the Massachusetts sidewalk. Landscape elements such as benches and planters will define a contemplative space. An interpretive element will serve as a focal point, telling the story. Textural materials and paving can also be used to create an evocative experience that reinforces the narrative.



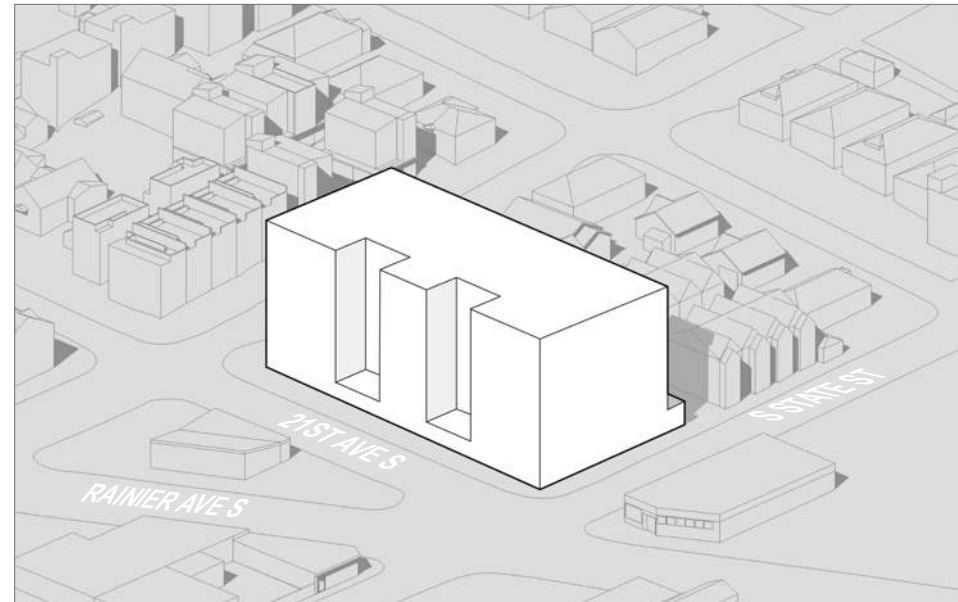
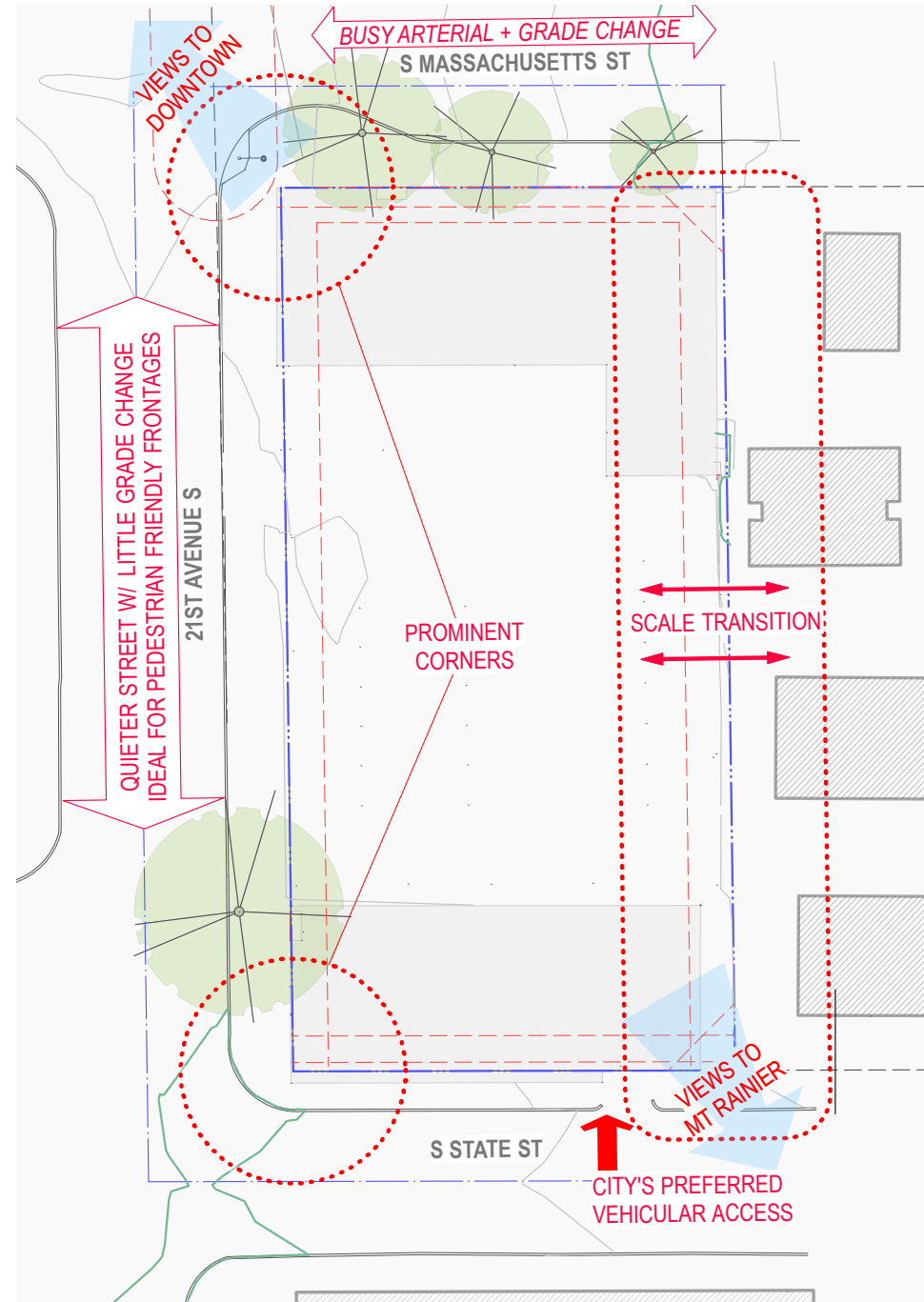
Integrated Interpretive Element (A1.1)



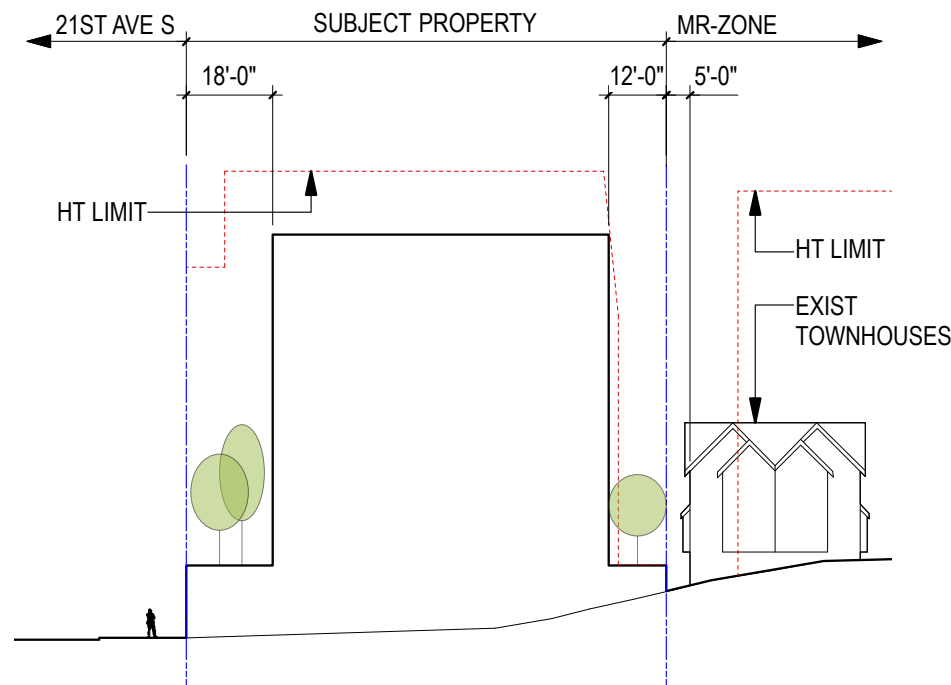
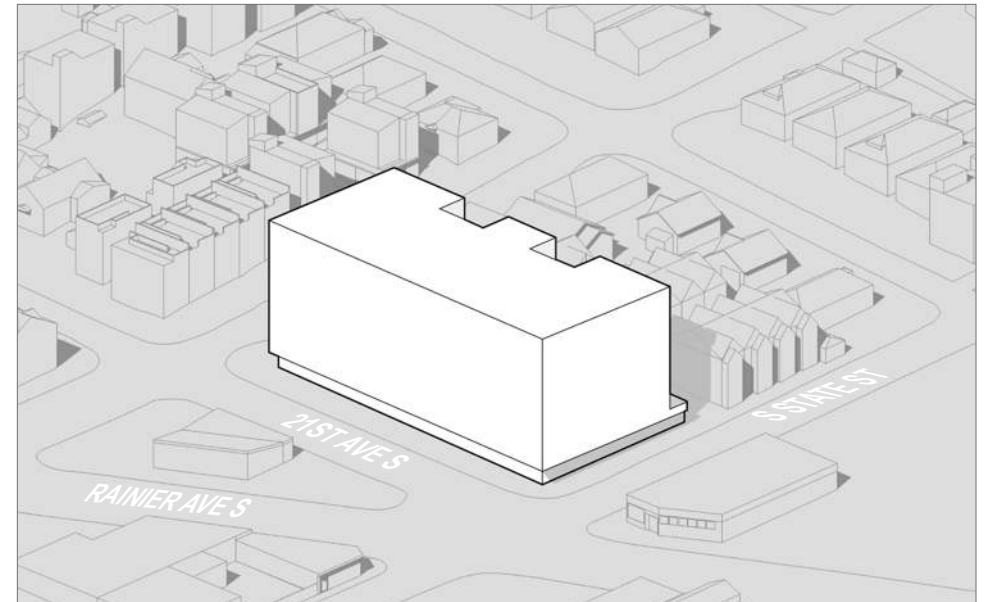
Evocative Garden Space defined by Planting, Texture and Paving (A1.1)

21st Avenue South is a relatively tranquil and flat street and offers the greatest length of frontage for pedestrian oriented uses. Because it is west facing, it has good solar exposure and the adjacent triangular block provides some buffering from busy Rainier Avenue South. All of the proposed schemes assume this to be the "front" of the site. The northwest and southwest corners are very prominent. Important programmatic elements should anchor the base of these corners and attention should be given to how the building facades wrap around them.

S Massachusetts is more residential in nature than the other streets, but because it is a busy arterial with significant grade change it is not an ideal location for primary building entrances or vehicular access. S. State Street is a more appropriate location for services and parking access, but these uses should be biased towards the east in order to maximize the potential for a commercial corner presence.

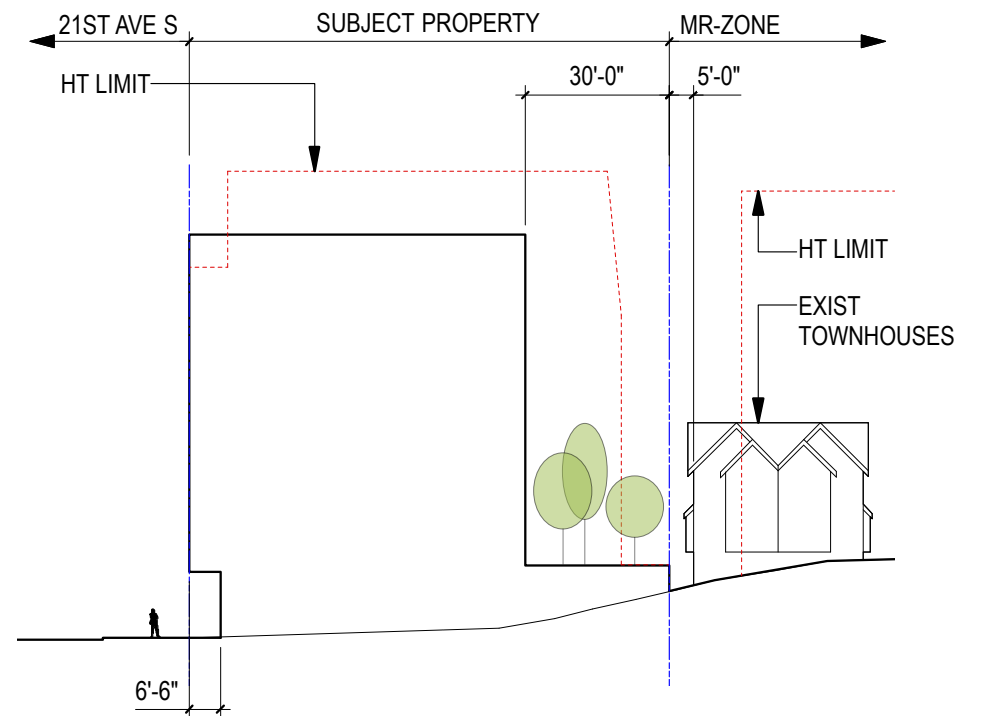


MASSING CONCEPTS • INITIAL SITE RESPONSES



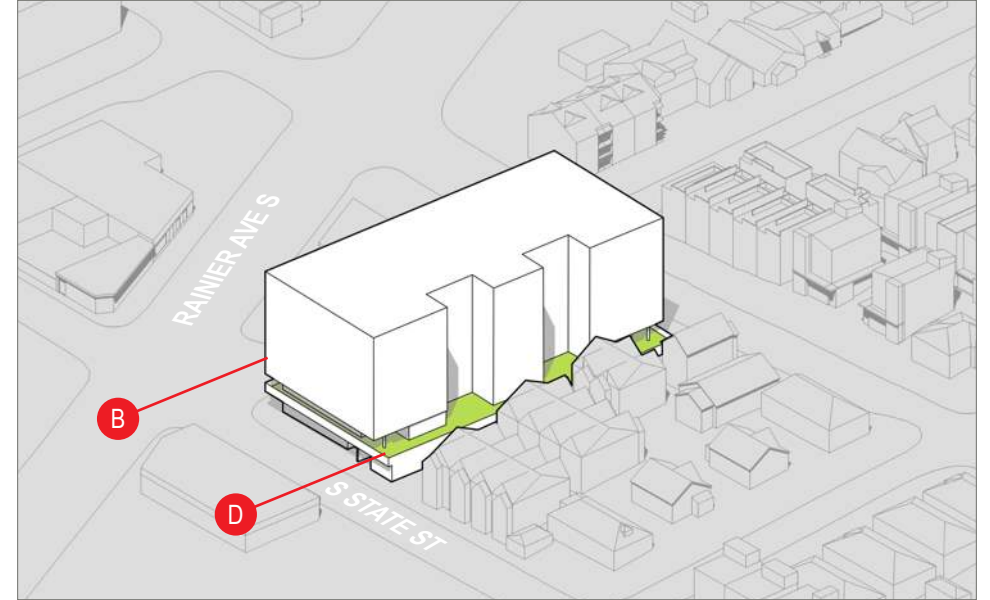
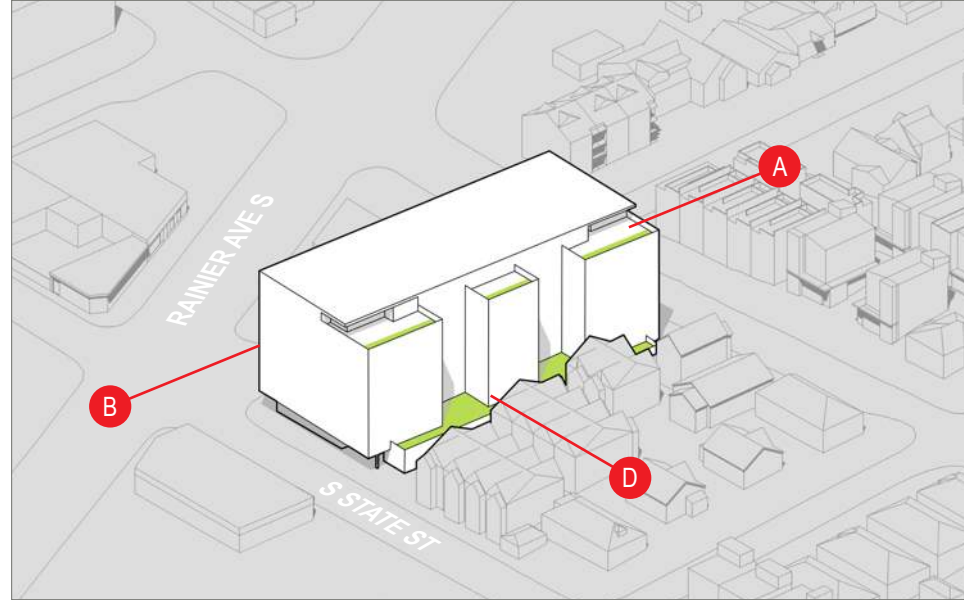
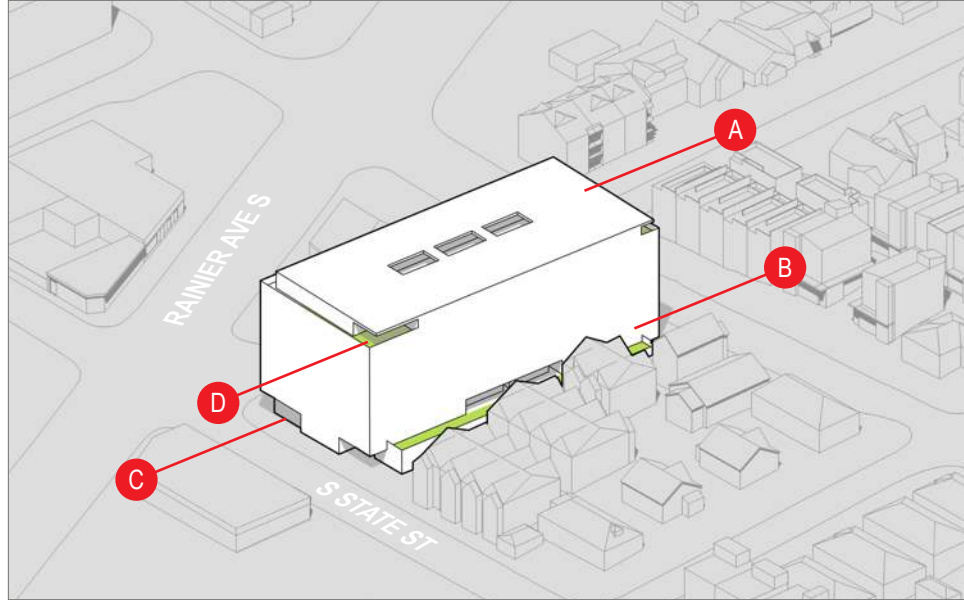
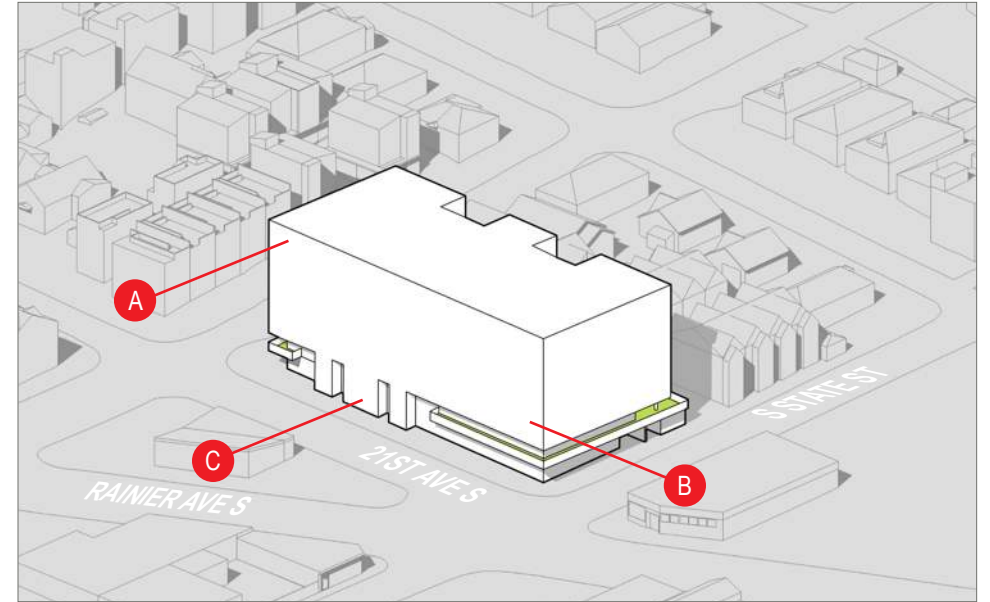
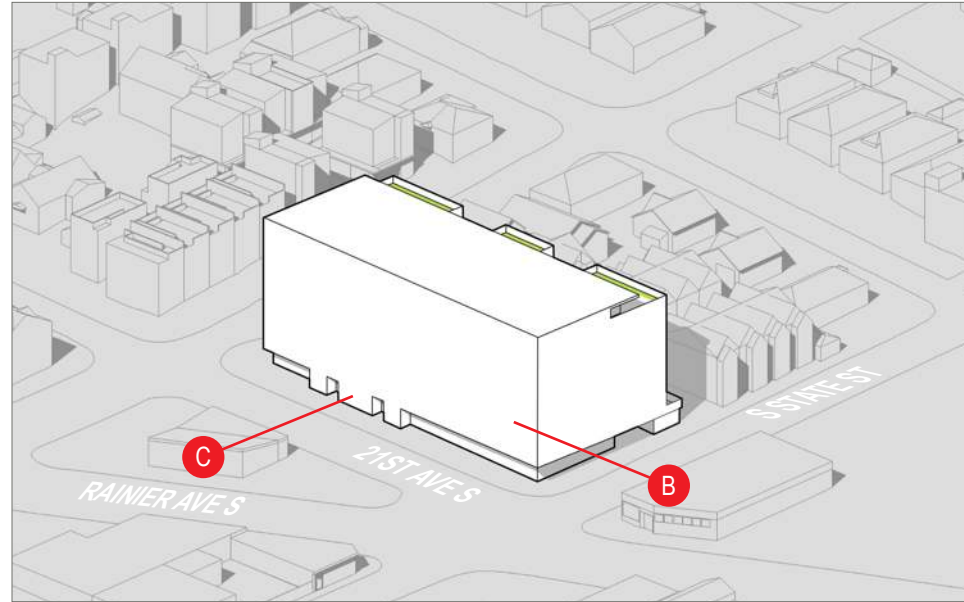
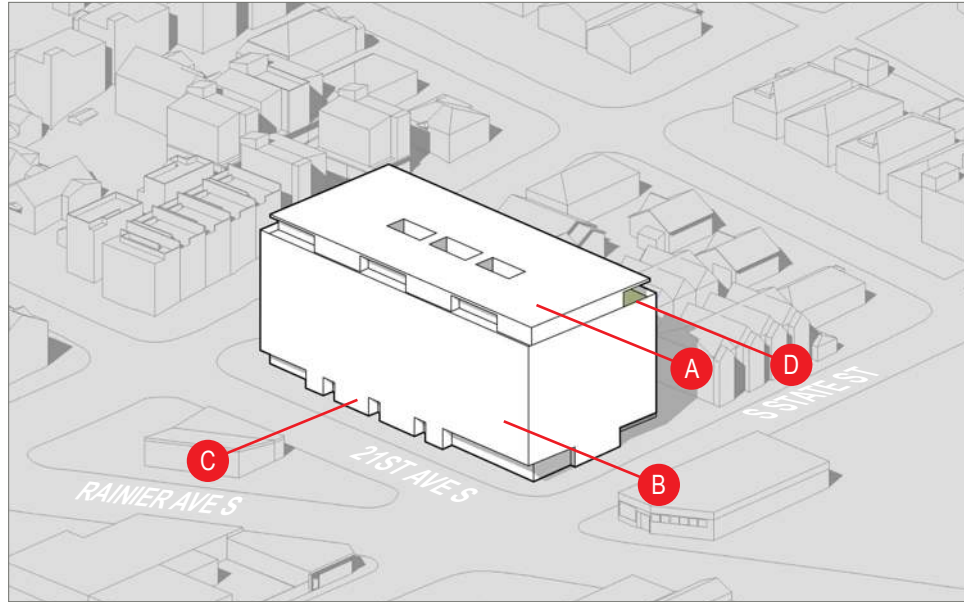
The east property line marks a zoning transition between NC3-95 (M) and MR (M2). Although the building height limit differs by only 15', the existing townhouses were constructed under a previous LR2 zoning. While redevelopment of the neighboring townhouses site in the future is incentivized by the significant upzone, consideration for the existing scale shift is appropriate. In addition, one set of townhouses is only setback 5' from the property line.

A massing strategy that erodes along the west façade breaks up the length of building façade and provides good solar exposure to the carved courtyards along that side, but this approach does little to soften the bulk along the east property line where the transition in zoning and scale occurs.



An alternate approach locates courtyard erosions along the east façade where they are directly beneficial to the neighboring properties. The locations of the east-facing courts can be specifically calibrated to address the pinch-point where the existing townhouses are only 5' from the property line. Holding a more continuous mass along the west façade presents a stronger streetwall. Insetting the base allows for expanded sidewalks and integrated rain coverage.

MASSING CONCEPTS • COMPARISON OF SCHEMES



Alternative 1 (Code Compliant Scheme)

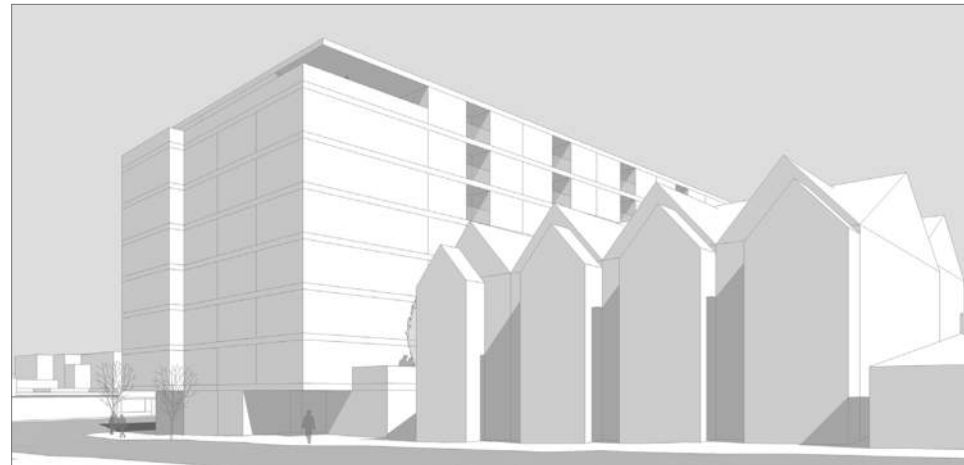
- A** Penthouse-styled top level with private balconies facing west and patios facing east on level 2. Three open-air light wells extrude through the core. Roof deck amenities are placed on level 8 with southeast views.
- B** Mass extends fully to buildable envelope to the east and west.
- C** Live-work units are integrated into the west facade and read as single-height. Level 1 commercial space is recessed from the west and south.
- D** Greenery is planted at the top level, while a majority remains on level 2.

Alternative 2

- A** Penthouse-styled top level facing east. Two amenity spaces are located on the top level. One private balcony is supplied to a single unit.
- B** Mass is biased to the west to follow the main street facade along 21st Ave S. "E" shaped scheme allows more breathing space to neighboring townhouses to the east.
- C** Live-work units are integrated into the west facade and read as single-height. Level 1 commercial space is recessed from the west and south.
- D** Greenery is planted at the top level, while a majority remains on level 2.

Alternative 3 (Preferred Scheme)

- A** No rooftop level. All amenities are relocated to level 2 for greater interaction with the public.
- B** Mass is biased to the west to follow the main street facade along 21st Ave S. "E" shaped scheme allows more breathing space to neighboring townhouses to the east. Entire mass exposes level 2 in a gasket scheme with levels 3-8 placed on top.
- C** Live-work units are integrated into west facade and level 1 commercial space. Transparent levels 1 & 2 have a double-height read along 21st Ave S.
- D** Greenery is planted on level 2 allowing for public visibility entirely around the gasket



Alternative 1 (Code Compliant Scheme)

Description

Alternative 1 proposes an 8-story building composed of street level commercial with apartments above. The upper level floor plans are organized around 3 light courts, which allows the building to maximize the number of true one-bedroom units. The scheme respects all required setbacks, holding back a consistent 12' from the east property line above the first floor, and maintaining the required average setback of 8' above 75' for street-facing facades. The north façade jogs to minimize the impact of the powerline setback and the south façade follows suit avoiding the 15' triangular setback at the SE corner of the site. The residential lobby is located off of S Massachusetts Street, which is more residential in nature than the other two streets. Access to a single level of parking is provided from S State. Two-level live-work units clearly place work spaces at street level with residential uses located at the second floor. Balconies are integrated into the major facades to break up the massing and provide private amenity space for units. Amenity space is provided at the 8th floor with a large covered common roof deck.

Program

- 156 Apartments (6 Live-work Units)
- 2,302-sf of Commercial
- Parking for 24 vehicles

Advantages

- No departures required
- Optimal unit mix with maximum number of one-bedroom units
- Opportunity for top floor penthouse units
- View opportunity for 8th floor Amenity Spaces
- Light Courts provide daylight for corridors

Challenges

- Tall façade along east property line presents a stark change in scale to neighboring townhouse units
- Light Courts significantly increase building perimeter which will add cost
- Top floor setback diminishes "building wholeness"
- Shift in construction type from concrete to wood at 3rd Floor does not align with shift in program at 2nd Floor
- Limited parking



Alternative 2

Description

Alternative 2 also proposes an 8-story building composed of street level commercial with apartments above. Apartment are organized around two east-facing courtyards. The courtyards afford additional buffer space to the neighboring townhouses, providing nearly triple the required setback. In lieu of the street-side top floor set back, the east façade is pulled back at the top level, further softening the edge abutting the neighboring mid-rise zone. Roof-top amenity spaces are located at the outdoor spaces created by this setback. As with Alternative 1, the residential lobby is located off of S Massachusetts Street, and access to parking is provided from South State Street. In this scheme, a lower level of parking is provided, requiring two ramps and a widened curb cut along State. As with the other scheme, two-level live-work units are provided and balconies are integrated into the major facades to break up the massing and provide private amenity space for units.

Program

- 161 Apartments (4 Live-work Units)
- 3,378-sf of Commercial
- Parking for 65 vehicles

Advantages

- Courtyard along east façade break up the mass and create green buffers to neighboring townhouse developments
- Top level step back on east side provides view oriented amenity spaces and reduces apparent height of east façade
- Unit count is maximized
- Increased parking count
- Cohesive building expression for main street facades

Challenges

- Departures required
- Shift in construction type from concrete to wood at 3rd Floor does not align with shift in program at 2nd Floor



Alternative 3 (Preferred Scheme)

Description

Like Alternative 2, Alternative 3 organizes the apartments around a pair of east facing courtyards, providing direct benefit to the neighboring property. Instead of providing upper-level set-backs, this scheme increases the street level setback to extend to the underside of the third floor. This allows for a more volumetric expansion of the sidewalk and allows for a direct expression of the shift of construction from concrete to wood at the third floor. Amenity spaces are located at the second level and covered outdoor spaces connect the north and south street facades to the internal courtyards, creating a breezeway condition and providing visibility into the project. By shifting the major massing erosion to the lower levels, rather than the top level, a more dynamic sidewalk condition is created. Alternative 3 is otherwise similar to Alternative 2, providing two-level live/work units, integrated balconies, and two levels of enclosed parking.

Program

- 160 Apartments (4 Live-work Units)
- 3302-sf of Commercial
- Parking for 65 vehicles

Advantages

- Courtyard along east façade break up the mass and create green buffers to neighboring townhouse developments
- Articulation of double height base enlivens sidewalk activity and allows for expression of shift in program and construction type at 3rd floor
- Maximizes commercial space
- Amenity Spaces provide eyes on the street and exterior common spaces overlook sidewalk
- Increased parking count
- Cohesive building expression for main street facades

Challenges

- Departures required

MASSING CONCEPTS • ALTERNATIVE 1 (CODE COMPLIANT SCHEME)

Description

Alternative 1 proposes an 8-story building composed of street level commercial with apartments above. The upper level floor plans are organized around 3 light courts, which allows the building to maximize the number of true one-bedroom units. The scheme respects all required setbacks, holding back a consistent 12' from the east property line above the first floor, and maintaining the required average setback of 8' above 75' for street-facing facades. The north façade jogs to minimized the impact of the powerline setback and the south façade follows suit avoiding the 15' triangular setback at the SE corner of the site. The residential lobby is located off of S Massachusetts Street, which is more residential in nature than the other two streets. Access to a single level of parking is provided from S State. Two-level live-work units clearly place work spaces at street level with residential uses located at the second floor. Balconies are integrated into the major facades to break up the massing and provide private amenity space for units. Amenity space is provided at the 8th floor with a large covered common roof deck.

Program

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- 2,302-sf of Commercial
- Parking for 24 vehicles

Advantages

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- Opportunity for top floor penthouse units
- View opportunity for 8th floor Amenity Spaces
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Challenges

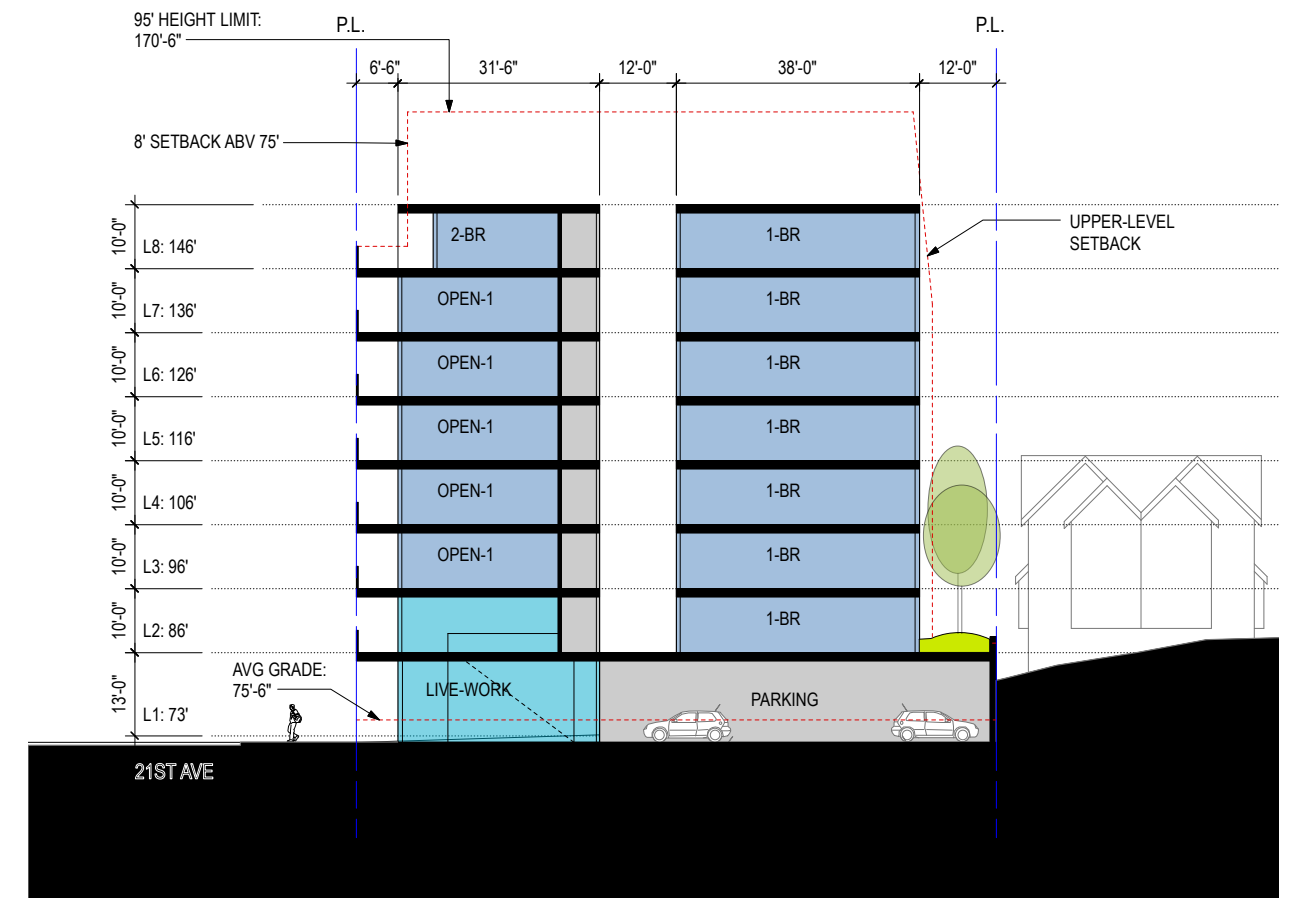
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- Limited parking



SW Street View



SW Aerial View



MASSING CONCEPTS • ALTERNATIVE 2

Description

Alternative 2 also proposes an 8-story building composed of street level commercial with apartments above. Apartments are organized around two east-facing courtyards. The courtyards afford additional buffer space to the neighboring townhouses, providing nearly triple the required setback. In lieu of the street-side top floor set back, the east façade is pulled back at the top level, further softening the edge abutting the neighboring mid-rise zone. Roof-top amenity spaces are located at the outdoor spaces created by this setback. As with Alternative 1, the residential lobby is located off of S Massachusetts Street, and access to parking is provided from South State Street. In this scheme, a lower level of parking is provided, requiring two ramps and a widened curb cut along State. As with the other scheme, two-level live-work units are provided and balconies are integrated into the major facades to break up the massing and provide private amenity space for units.

Program

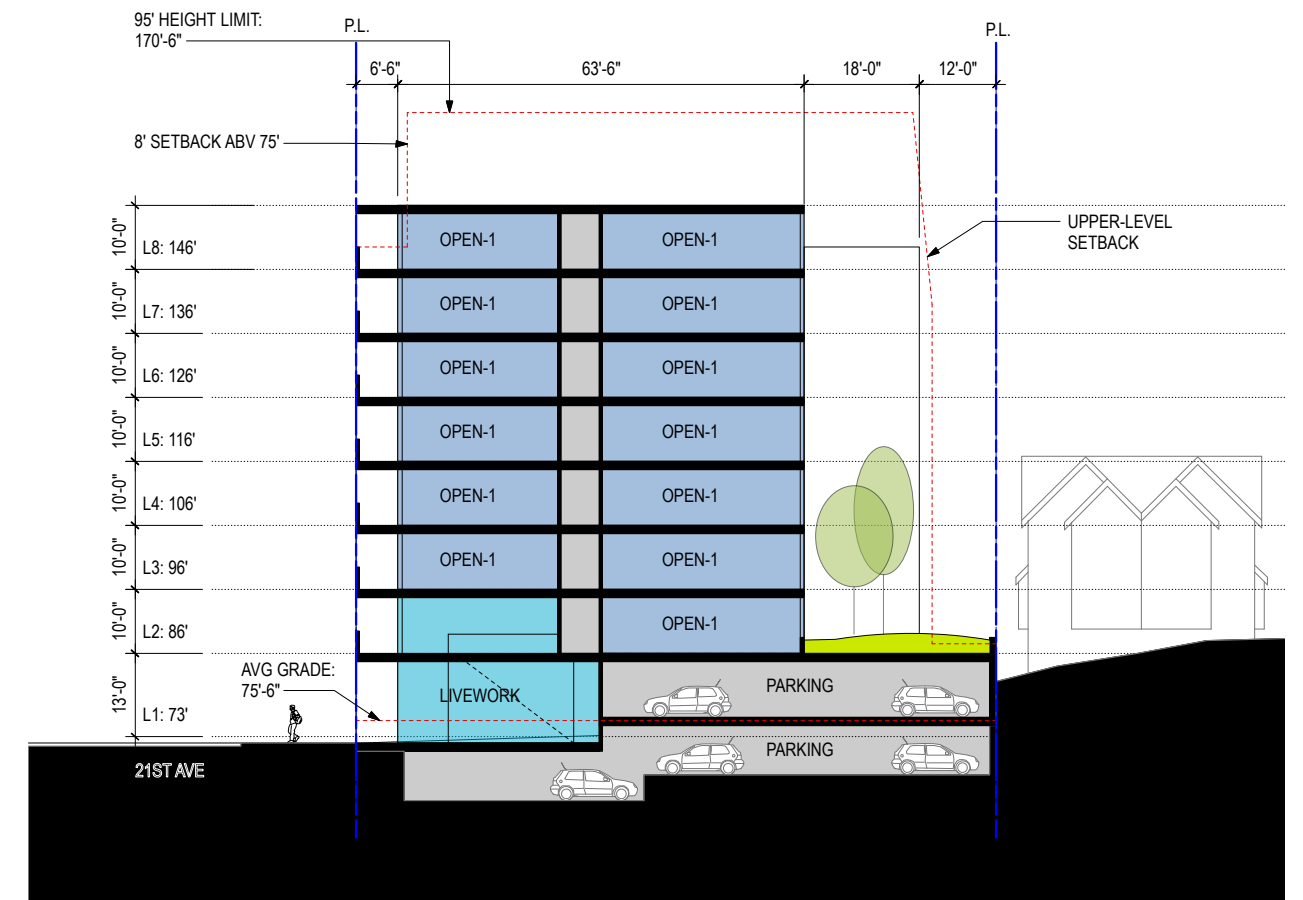
- 161 Apartments (4 Live-work Units)
- 3,378-sf of Commercial
- Parking for 65 vehicles

Advantages

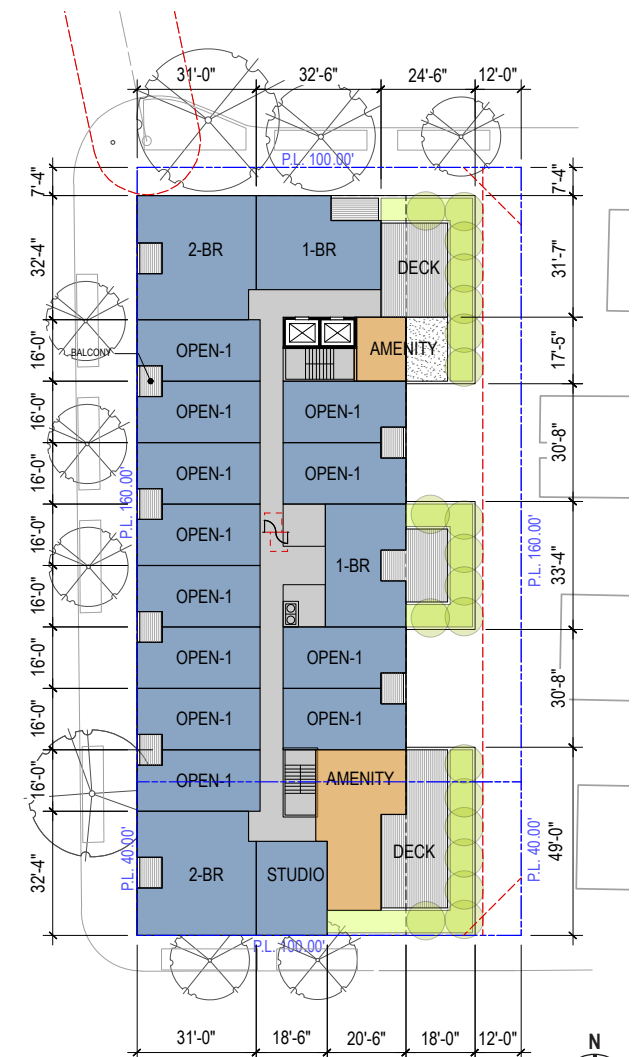
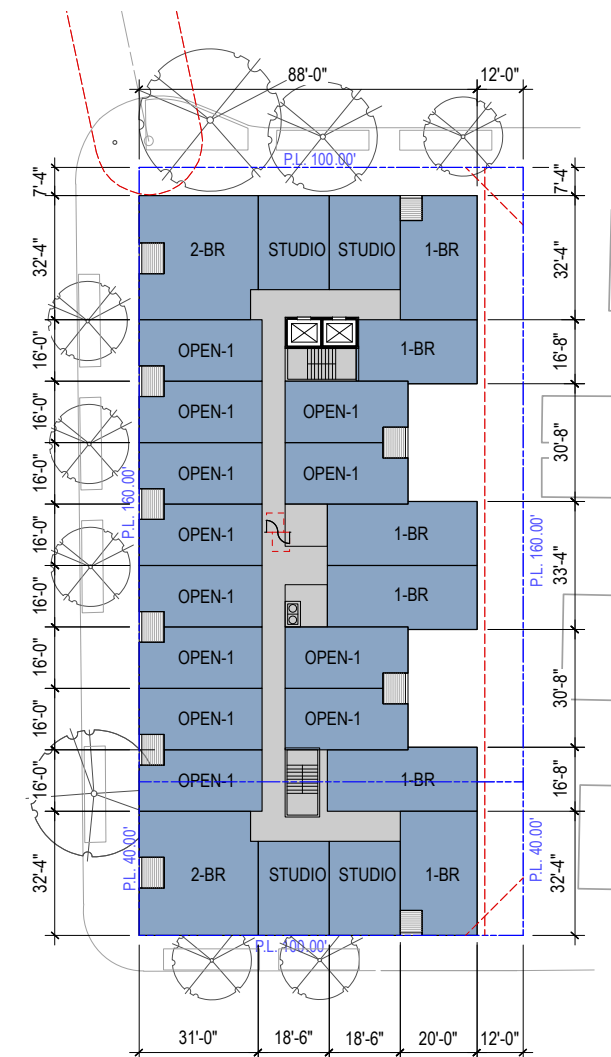
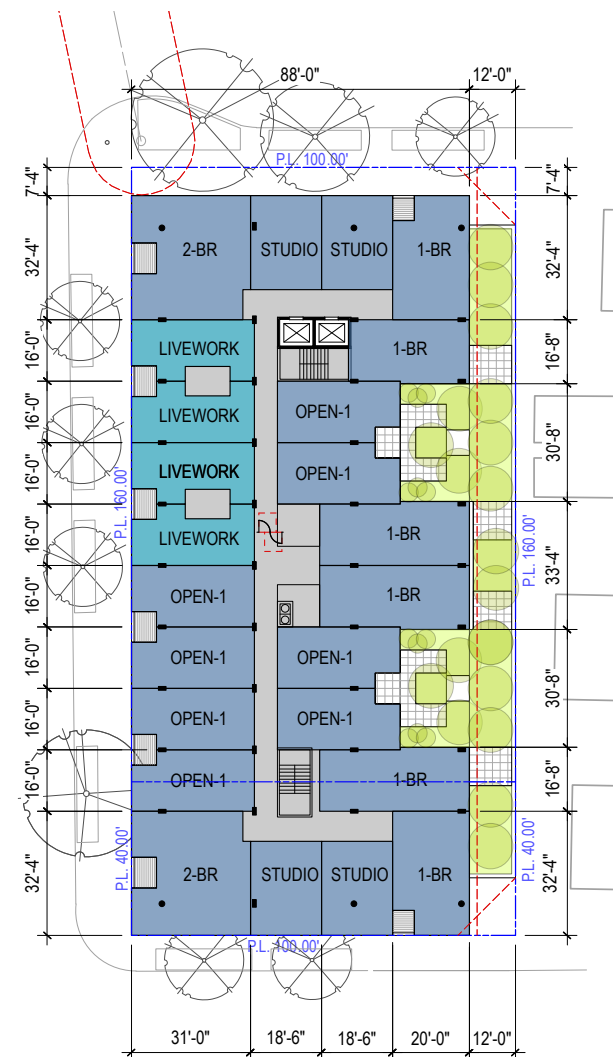
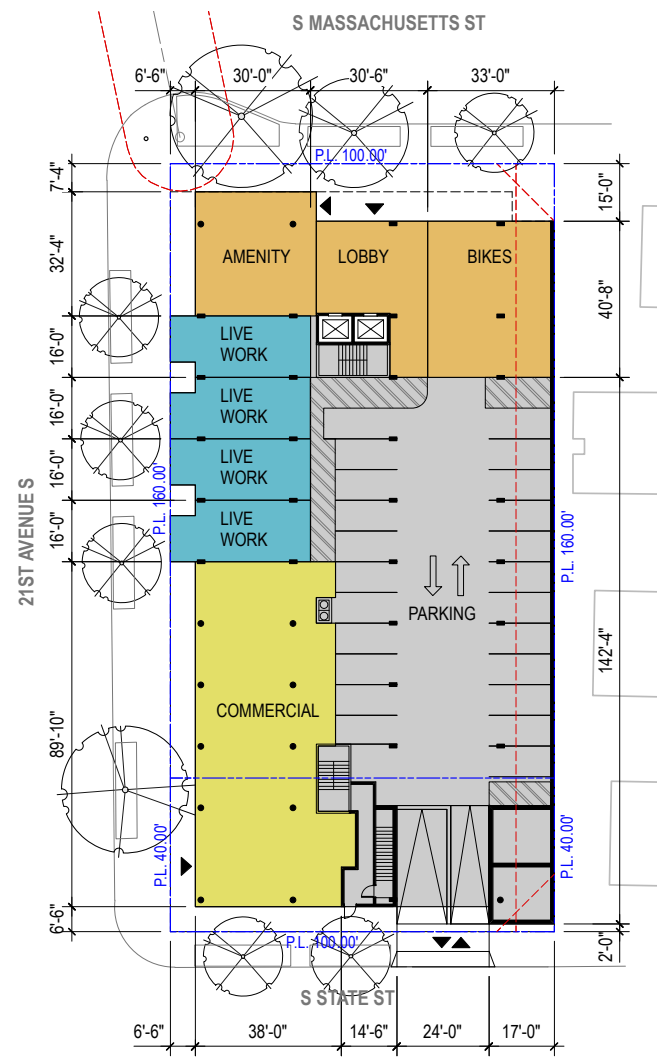
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- Unit count is maximized
- Increased parking count
- Cohesive building expression for main street facades

Challenges

- Departures required
- Shift in construction type from concrete to wood at 3rd Floor does not align with shift in program at 2nd Floor



MASSING CONCEPTS • ALTERNATIVE 2



MASSING CONCEPTS • ALTERNATIVE 3 (PREFERRED SCHEME)

Description

Like Alternative 2, Alternative 3 organizes the apartments around a pair of east facing courtyards, providing direct benefit to the neighboring property. Instead of providing upper-level set-backs, this scheme increases the street level setback to extend to the underside of the third floor. This allows for a more volumetric expansion of the sidewalk and allows for a direct expression of the shift of construction from concrete to wood at the third floor. Amenity spaces are located at the second level and covered outdoor spaces connect the north and south street facades to the internal courtyards, creating a breezeway condition and providing visibility into the project. By shifting the major massing erosion to the lower levels, rather than the top level, a more dynamic sidewalk condition is created. Alternative 3 is otherwise similar to Alternative 2, providing two-level live/work units, integrated balconies, and two levels of enclosed parking.

Program

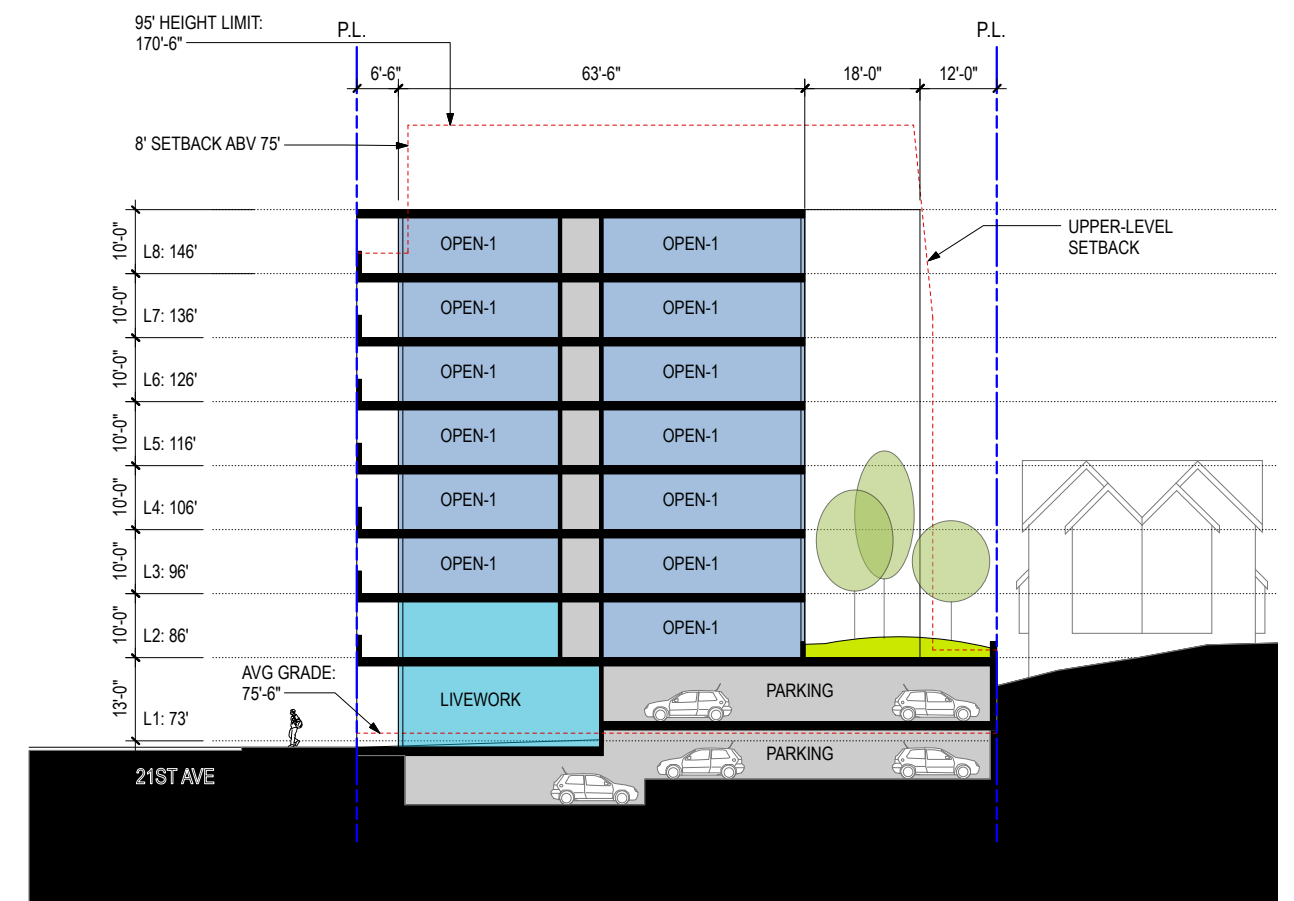
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- 3302-sf of Commercial
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Advantages

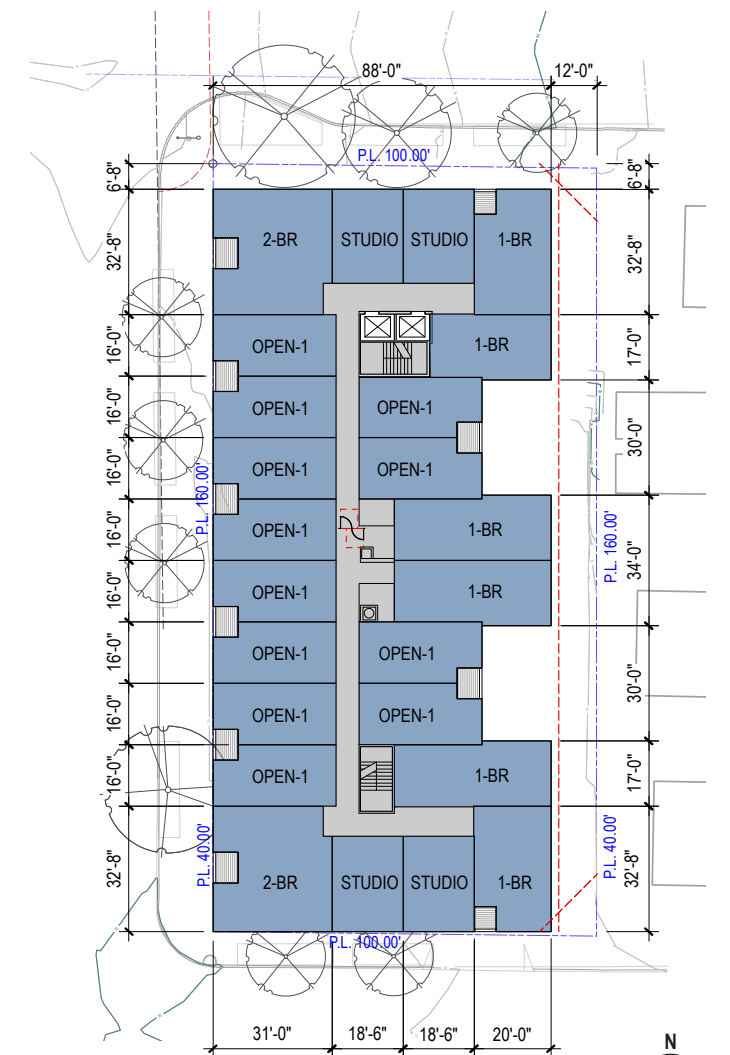
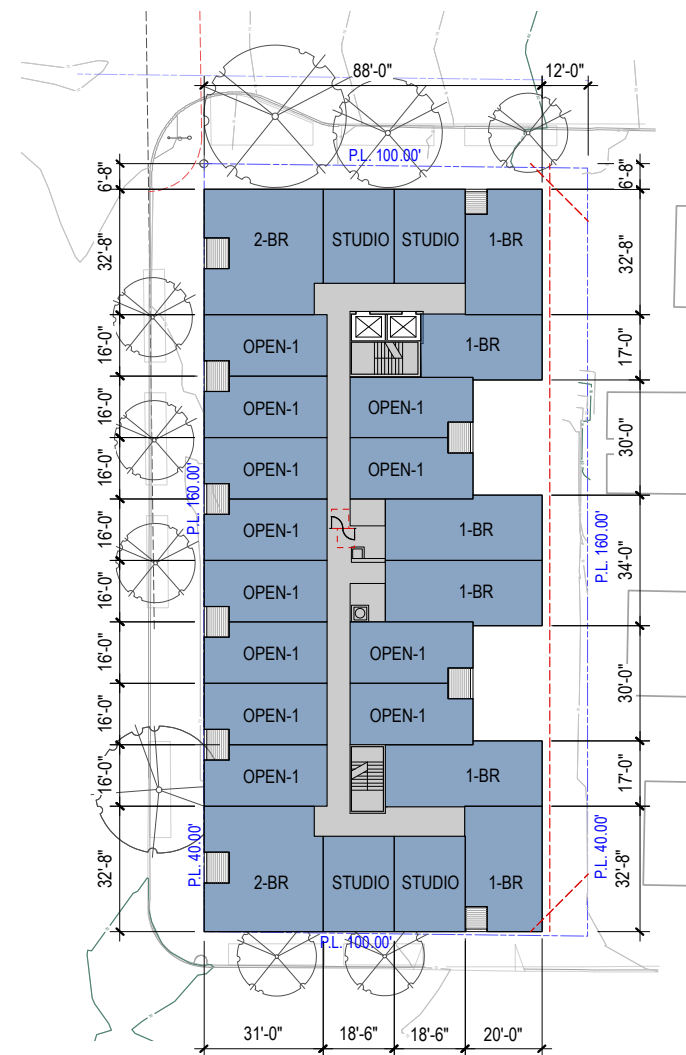
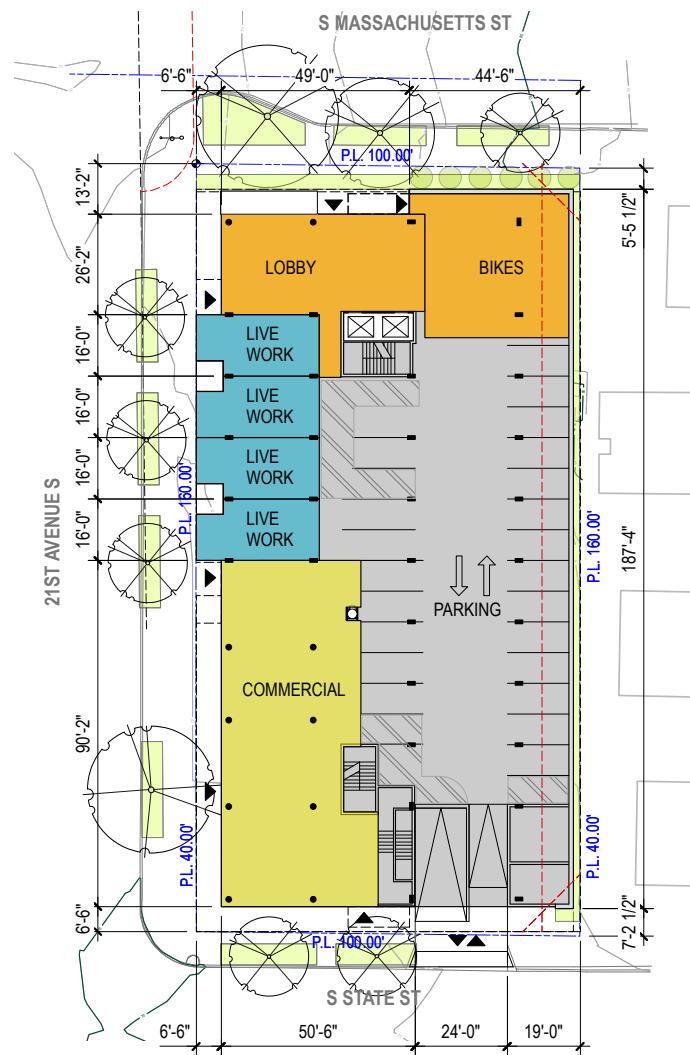
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- Increased parking count
- Cohesive building expression for main street facades

Challenges

- Departures required



MASSING CONCEPTS • ALTERNATIVE 3 (PREFERRED SCHEME)

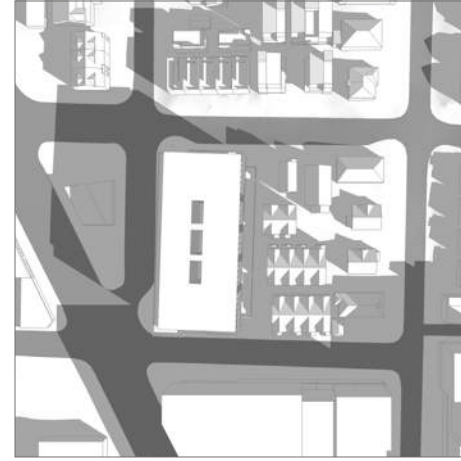


MASSING CONCEPTS • SHADOW ANALYSIS

ALTERNATIVE 1



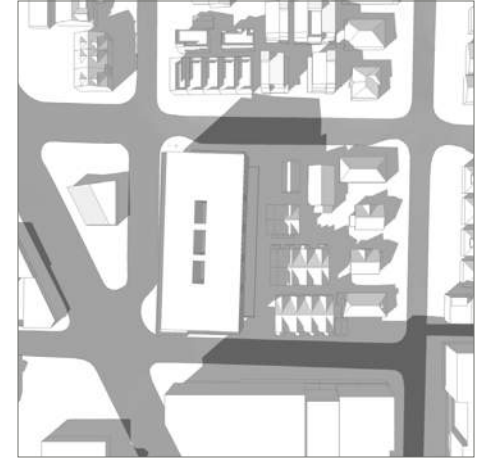
MARCH/ SEPTEMBER 21
9 AM



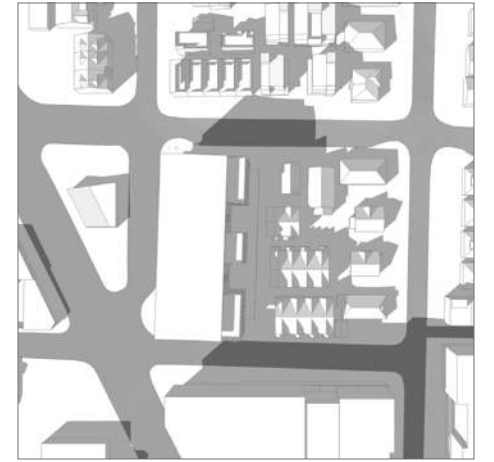
MARCH/ SEPTEMBER 21
12 PM



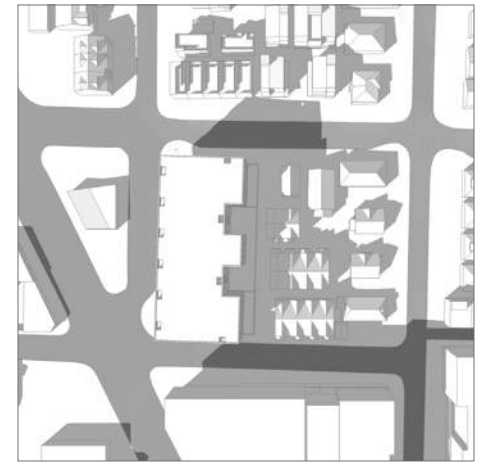
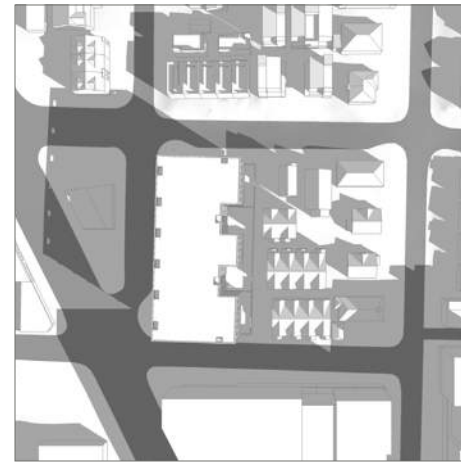
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ALTERNATIVE 2

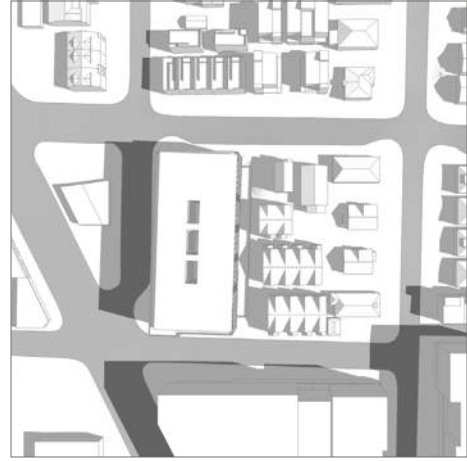


ALTERNATIVE 3



MASSING CONCEPTS • SHADOW ANALYSIS

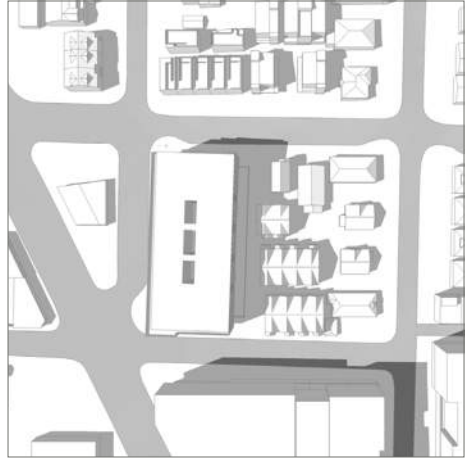
JUNE 21
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12 PM



JUNE 21
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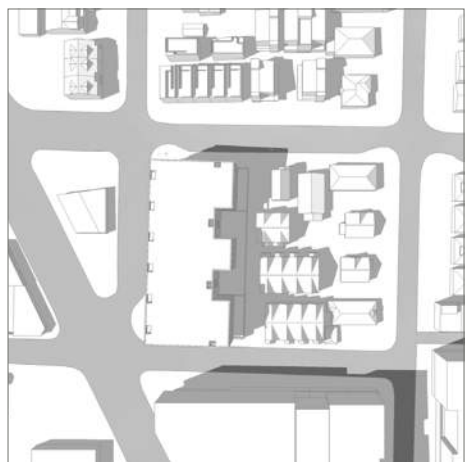
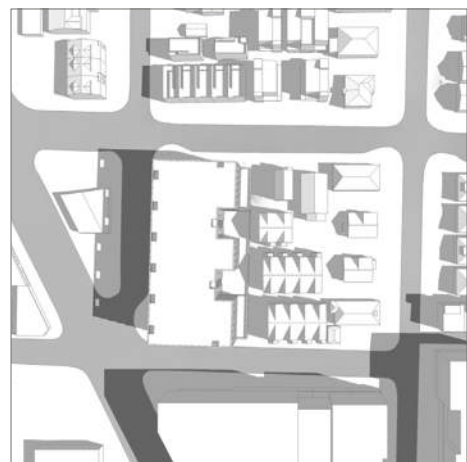
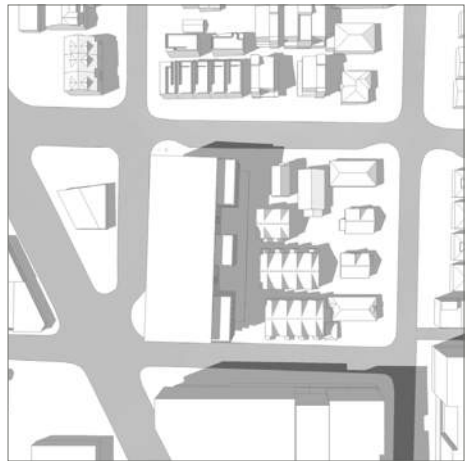
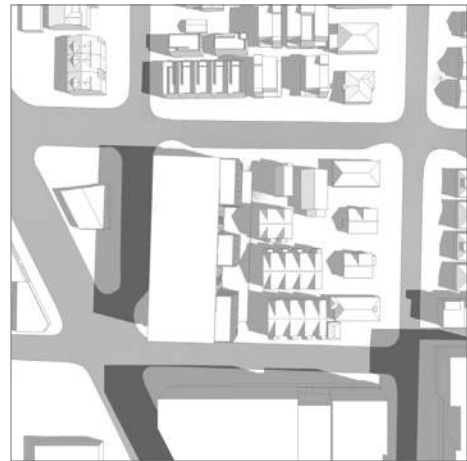
DECEMBER 21
9 AM



DECEMBER 21
12 PM



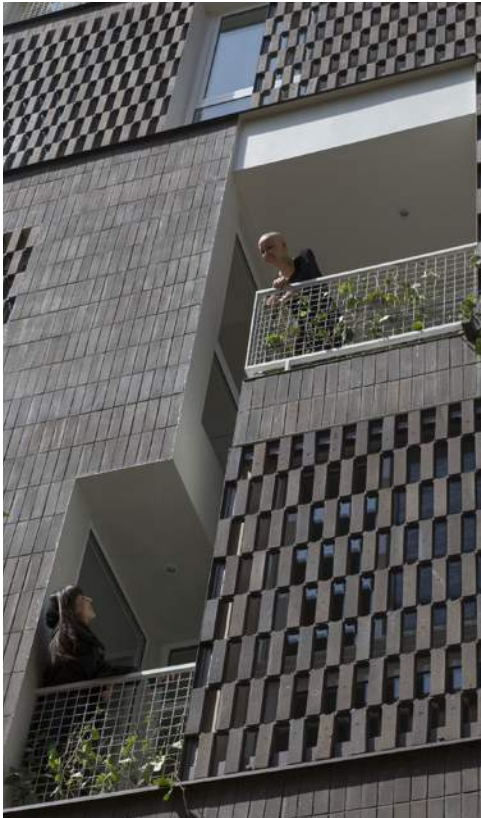
DECEMBER 21
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MASSING CONCEPTS • PRECEDENTS + DESIGN OPPORTUNITIES



Quality Materials + Detailing - Brick



Balconies + Human Interaction



Layered Façade, Integrated Balconies, Dynamic Opening Pattern



Corner Treatment + Detailing



Elevated Landscape Visible from the Street



Integrated Balconies + Visual Connection



Well Defined Residential Entry

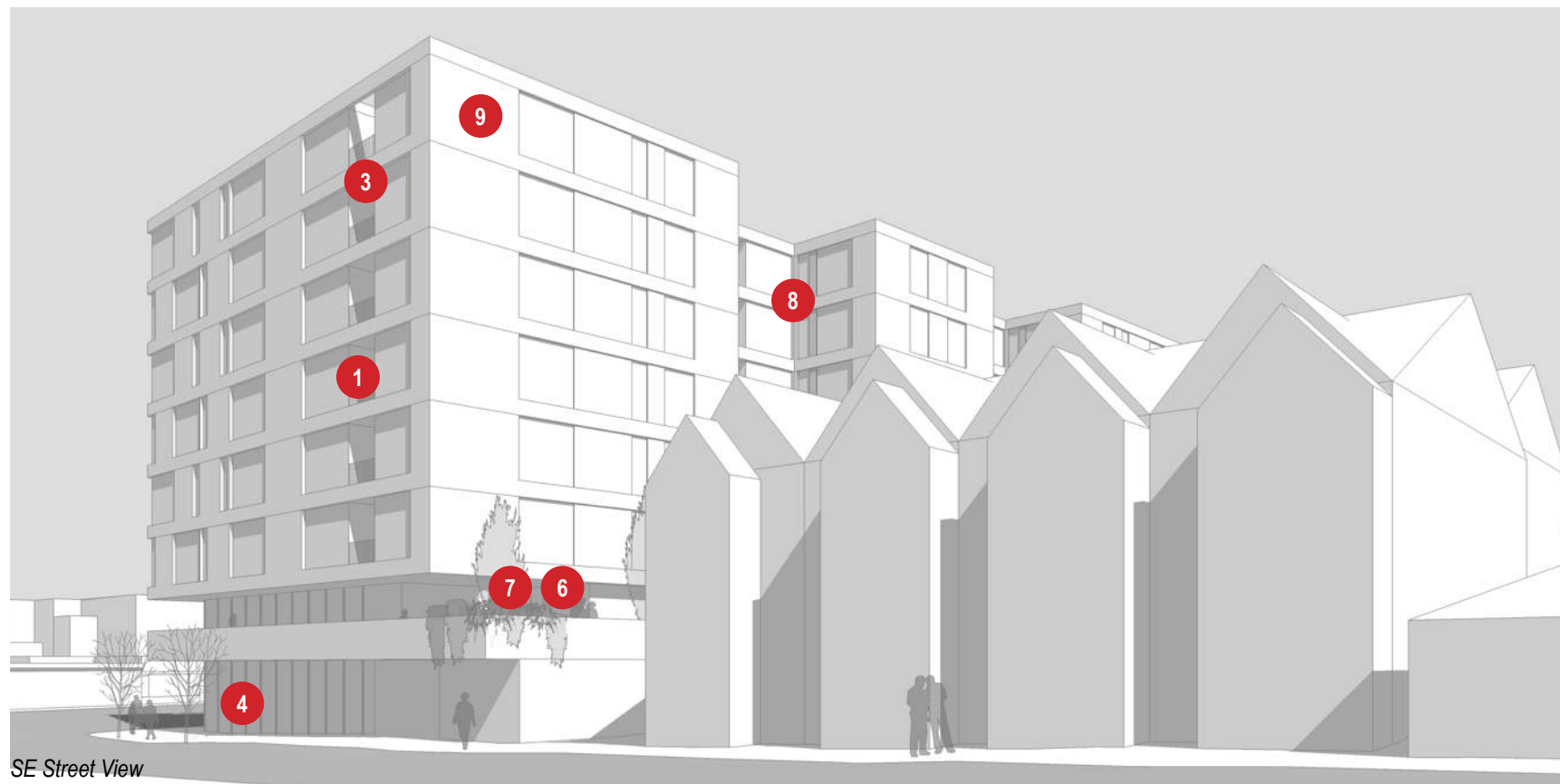
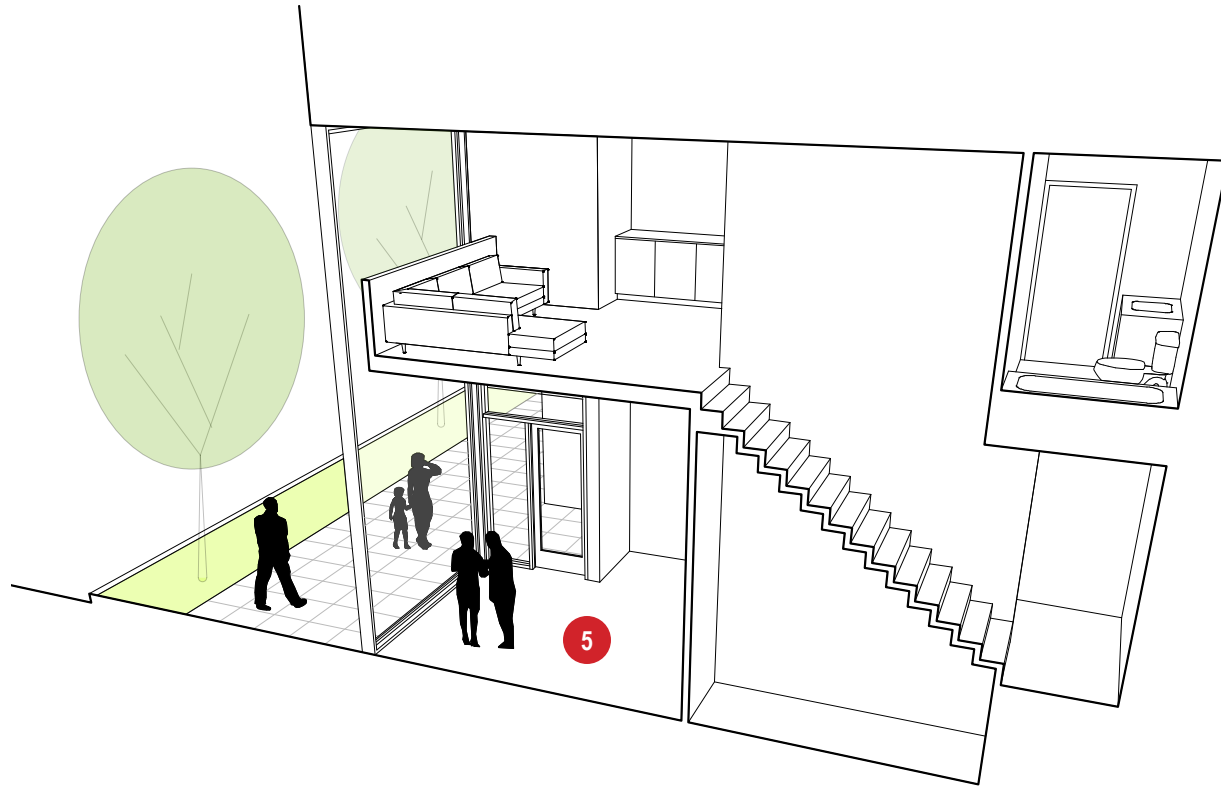


2-Level Live / Work



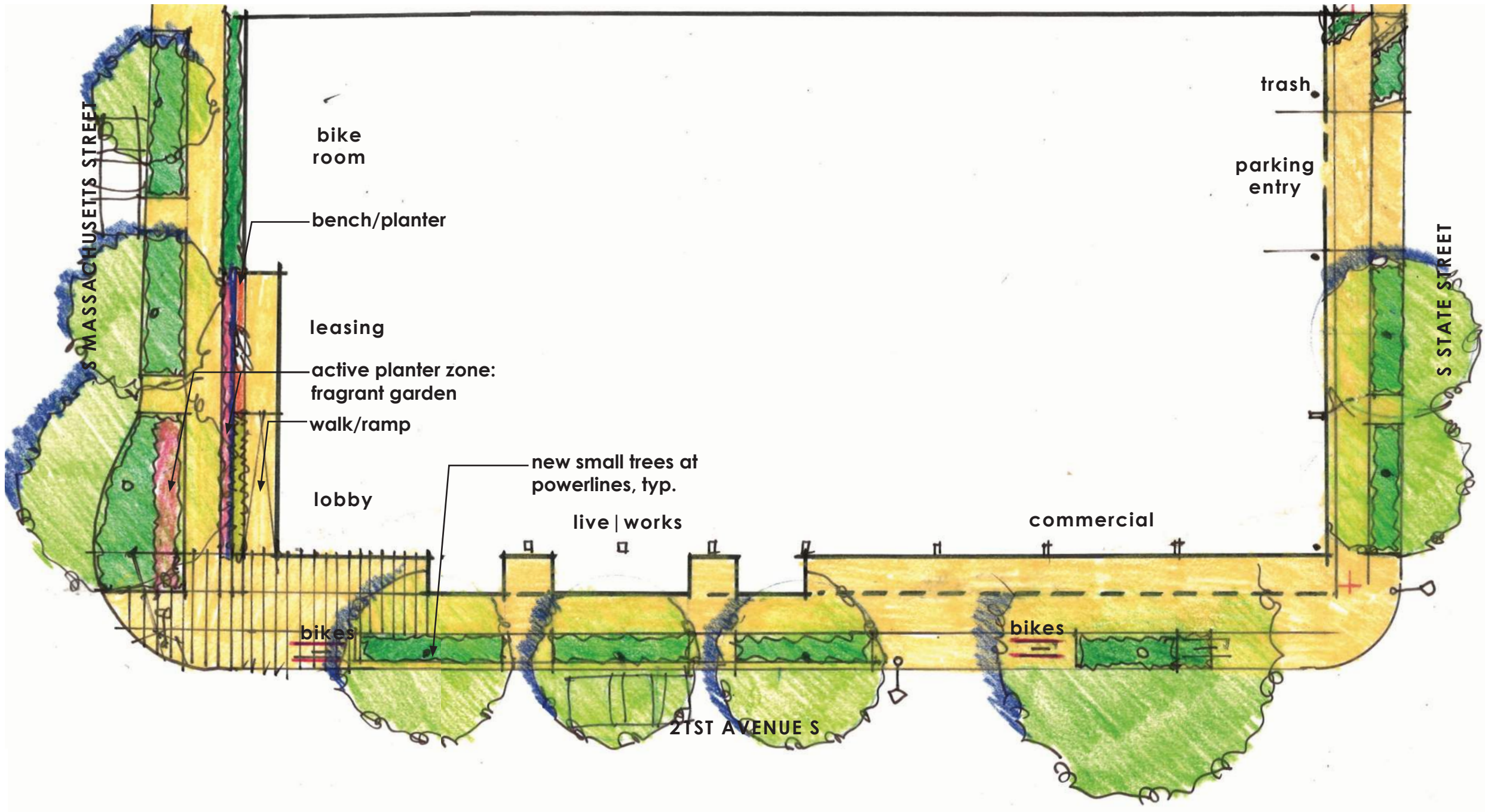
Street level with Expanded Public Zone, Integrated Rain Cover + Transparency

MASSING CONCEPTS • PRECEDENTS + DESIGN OPPORTUNITIES



- 1 Integrated Balconies (PL2.B1, DC2.C1)
- 2 Corner Windows + Detailing (CS2.C1, DC4.1)
- 3 Layered Facade + Dynamic Opening Pattern (DC2.C1 DC4.1)
- 4 Street Level w/ Expanded Public Zone + Integrated Rain Cover (CS2.B2, PL1.A2, PL2.C1, PL3.C1 PL3.C3)
- 5 2-Level Live Work Units w/ Street Level Transparency (PL3.B3)
- 6 Elevated Landscape visible from Street (DC3.B3, DC3.1)
- 7 Common Amenity Space Visually Connected to Sidewalk (DC3.A1, DC3.B3 DC3.1)
- 8 Reduced Massing with Courtyards Open to East (CS2.D3, CS2.D5, DC2.A)
- 9 High Quality Material with Texture (DC4.A1)

MASSING CONCEPTS • STREET LANDSCAPE PLAN



planter at ramp

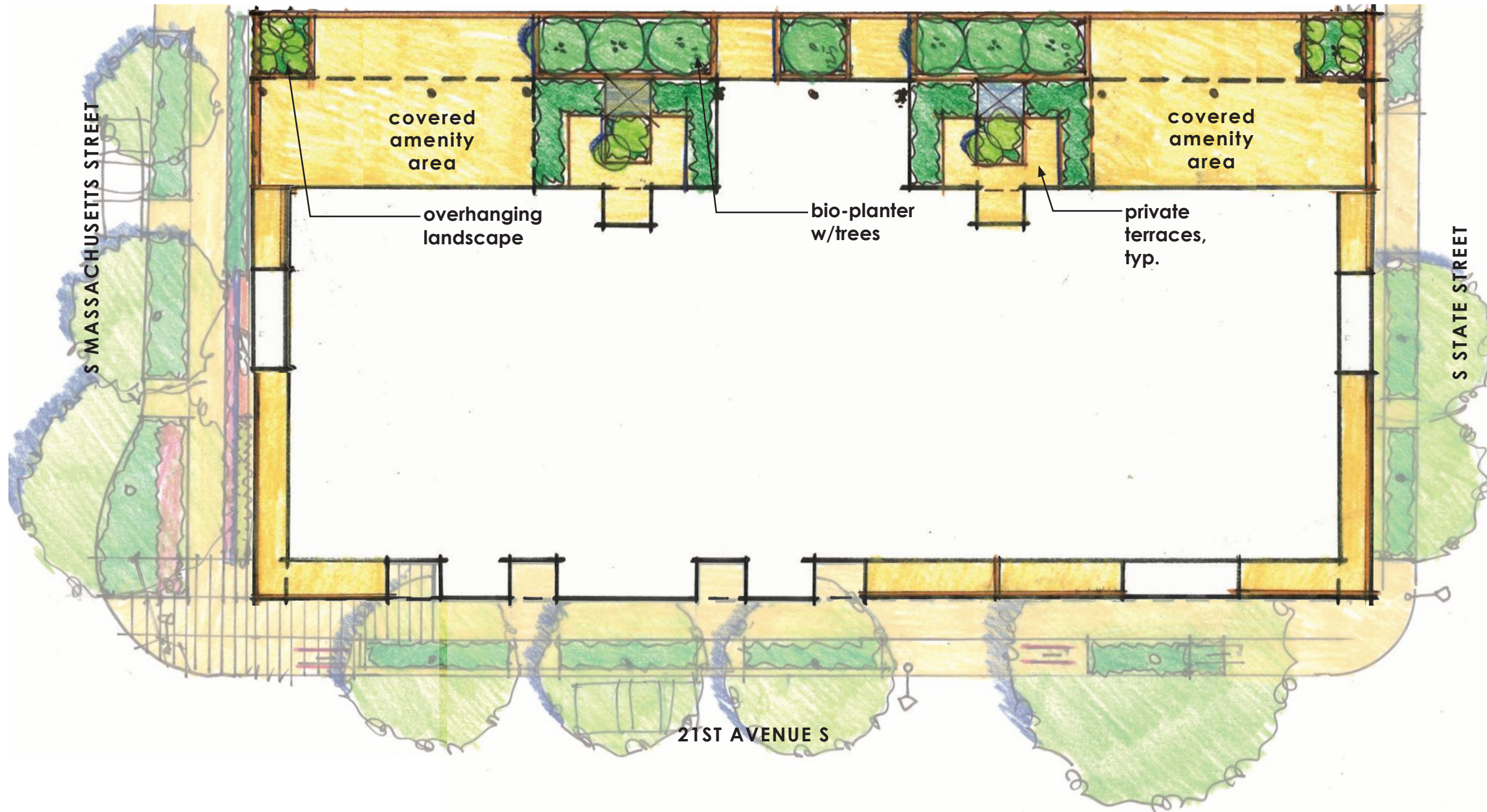


fragrant garden



live/works





overhanging landscape



bio-planter with trees



covered amenity area

0 10 20 < N

DEPARTURE SUMMARY

Departure Request #1: SMC 23.47A.014 B1 Setback for Lots Abutting Residential Zones

Standard:

A setback is required where a lot abuts the intersection of a side lot line and front lot line of a lot in a residential zone... The required setback forms a triangular area. Two sides of the triangle extend along the street lot line and side lot line 15 feet from the intersection of the residentially zoned lot's front lot line and the side lot line abutting the residentially zoned lot. The third side connects these two sides with a diagonal line across the commercially zoned lot.

Proposed:

Allow portions of the building mass to encroach in this triangular zone at both the northeast and southeast locations.

Rationale:

The preferred scheme provides additional set back area well beyond the code minimum by locating two open-ended courtyards along the east property line. As Diagram 1 illustrates, the volume of additional setback provided is more than fifty times the volume of reduced setback as a result of this departure. This extra setback provides a valuable buffer to the townhouse properties to the East, pursuant to Guideline CS2.D3 (zone transitions) and CS2.D5 (respect for adjacent sites). Strict compliance with the triangular setback results in an awkward massing erosions that would be at odds with Guideline DC2.

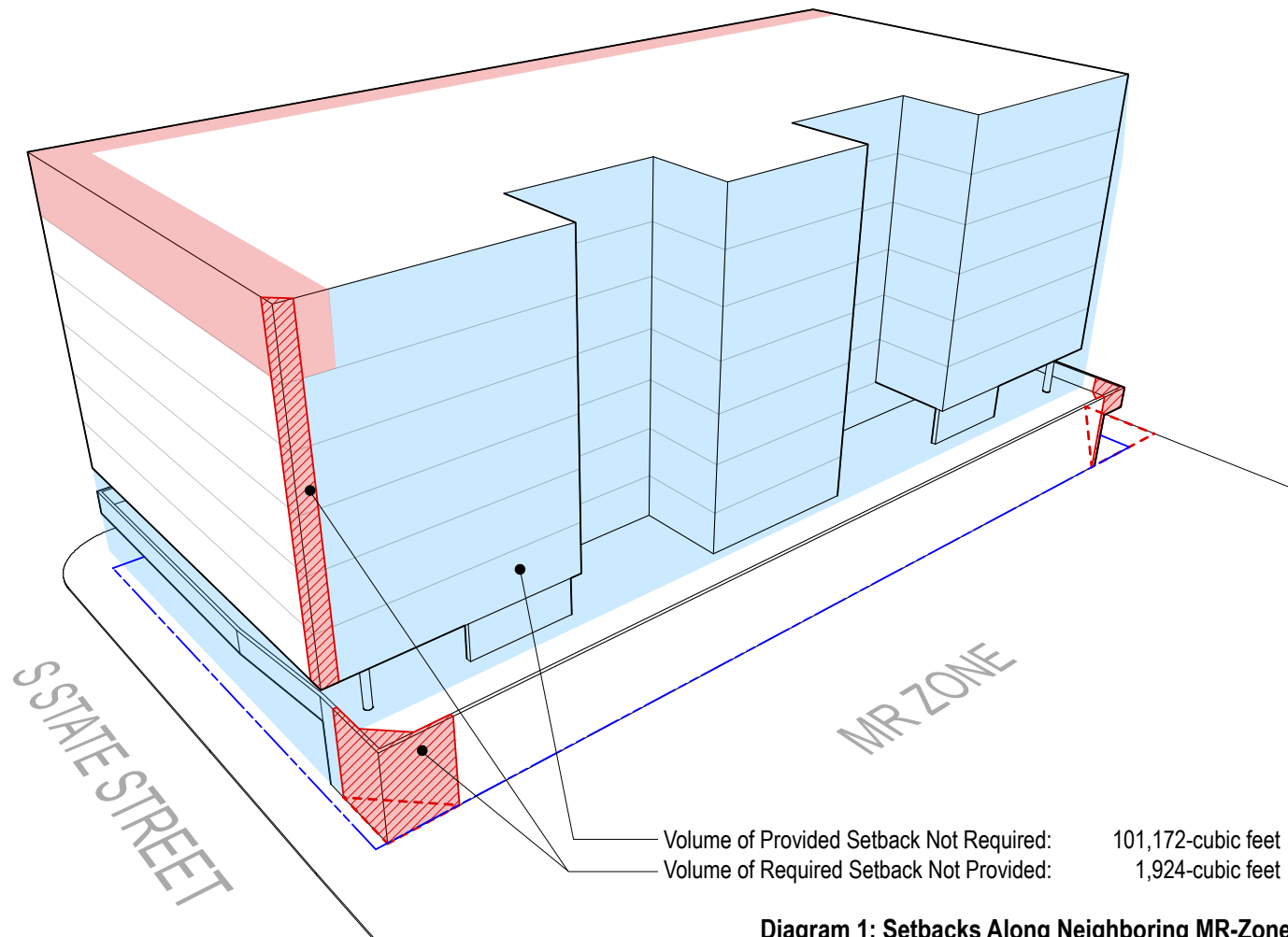


Diagram 1: Setbacks Along Neighboring MR-Zone

Departure Request #2: SMC 23.47A.014 C2 Upper-Level Setbacks for Street Facing Façades

Standard:

For zones with a height limit of 85 feet or 95 feet, portions of structure above 75 feet must be set back from the front lot line by an average of 8 feet.

Proposed:

Allow full height of building to be built to the property line in exchange for additional setback at street level and along the east property line.

Rationale:

The preferred massing alternative takes the approach that additional set-back area is more beneficial at the lower levels where it directly influences the public realm, which better meets Design Guidelines DC3.B3 (Connection to Other Open Space) and DC3.1 (Common Open Spaces). As Diagram 2 illustrates, the volume of additional setback provided at the first two levels exceeds the volume of required setback above 75'. Without the upper-level setback, the massing is more expressive of the programmatic and structural shifts that occur between the first two levels and the upper levels, pursuant to Design Guideline DC2.1 (Building Layout and Massing).

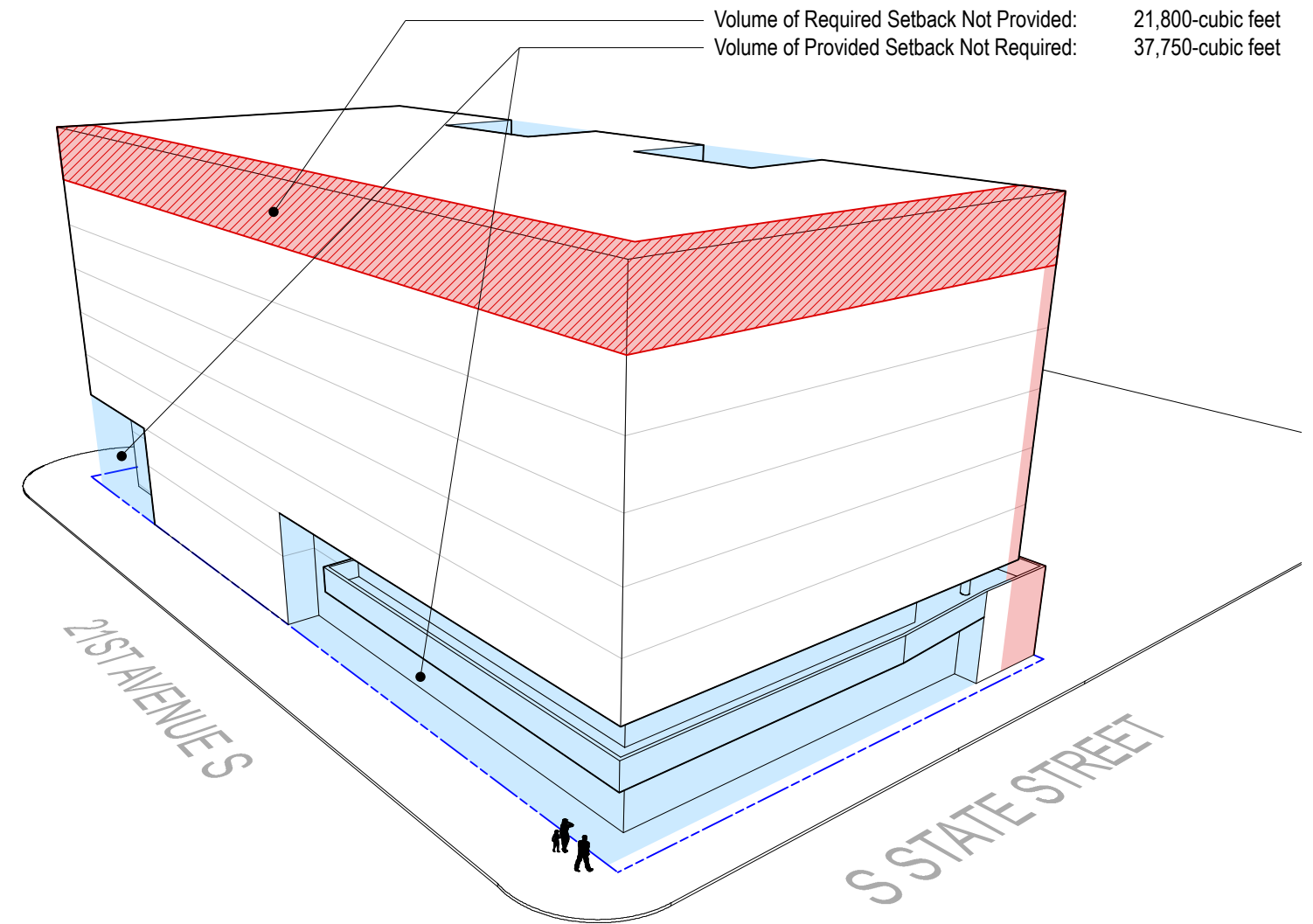


Diagram 2: Setbacks Along Street Facing Façades

Departure Request #3: SMC 23.54.030 D1.c Driveways

Standard:
Driveway widths of any length that serve more than 30 parking spaces shall be at least 10 feet wide for one way traffic and at least 20 feet wide for two-way traffic.

Proposed:
Allow the width of a two-way driveway serving 43 parking spaces to be reduced to 13 feet wide.

Rationale:
Due to the geometry of the site, the upper and lower parking decks must be served by separate driveways, but the SMC limits the number of curb cuts to one. Maintaining a combined driveway width of 24' or less will allow for a more efficient parking layout and structural column grid. Since both driveways will be accessed from a single curb cut, reducing the driveway width will minimize the required width of the curb cut. This is consistent with Design Guideline DC1.B1b, which states that the width and number of curb cuts should be minimized as much as possible. Refer also to Departure Request #4.

Departure Request #4: SMC 23.54.030 F1.b: Curb Cut Width

Standard:
Curb cut shall not exceed a maximum width of 10 feet except that: 4) If subsection 23.54.030.D requires a driveway greater than 10 feet in width, the curb cut may be as wide as the required width of the driveway.

Proposed:
Allow for a 24 foot wide curb cut to accommodate the two driveways serving the building.

Rationale:
A wider curb cut will minimize the required width reduction for the driveway serving the lower parking level and avoid the need for a second curb cut, pursuant to Design Guideline DC1.B1b.

Departure Request #5: SMC 23.54.030 G1 Sight Triangle

Standard:
For exit only driveways and easements, and two way driveways and easements less than 22 feet wide, a sight triangle on both sides of the driveway or easement shall be provided, and shall be kept clear of any obstruction for a distance of 10 feet from the intersection of the driveway or easement with a driveway, easement, sidewalk or curb intersection, if there is not sidewalk.

Proposed:
Allow for alternate means of driveway safety, such as visual alert and change in paving texture, in lieu of the 10-foot sight triangle.

Rationale:
Reduction or elimination of the sight triangle helps reduce the visual impact of the parking garage entrance pursuant to Design Guideline DC1.C2 (reduce visual impact of parking entrances).

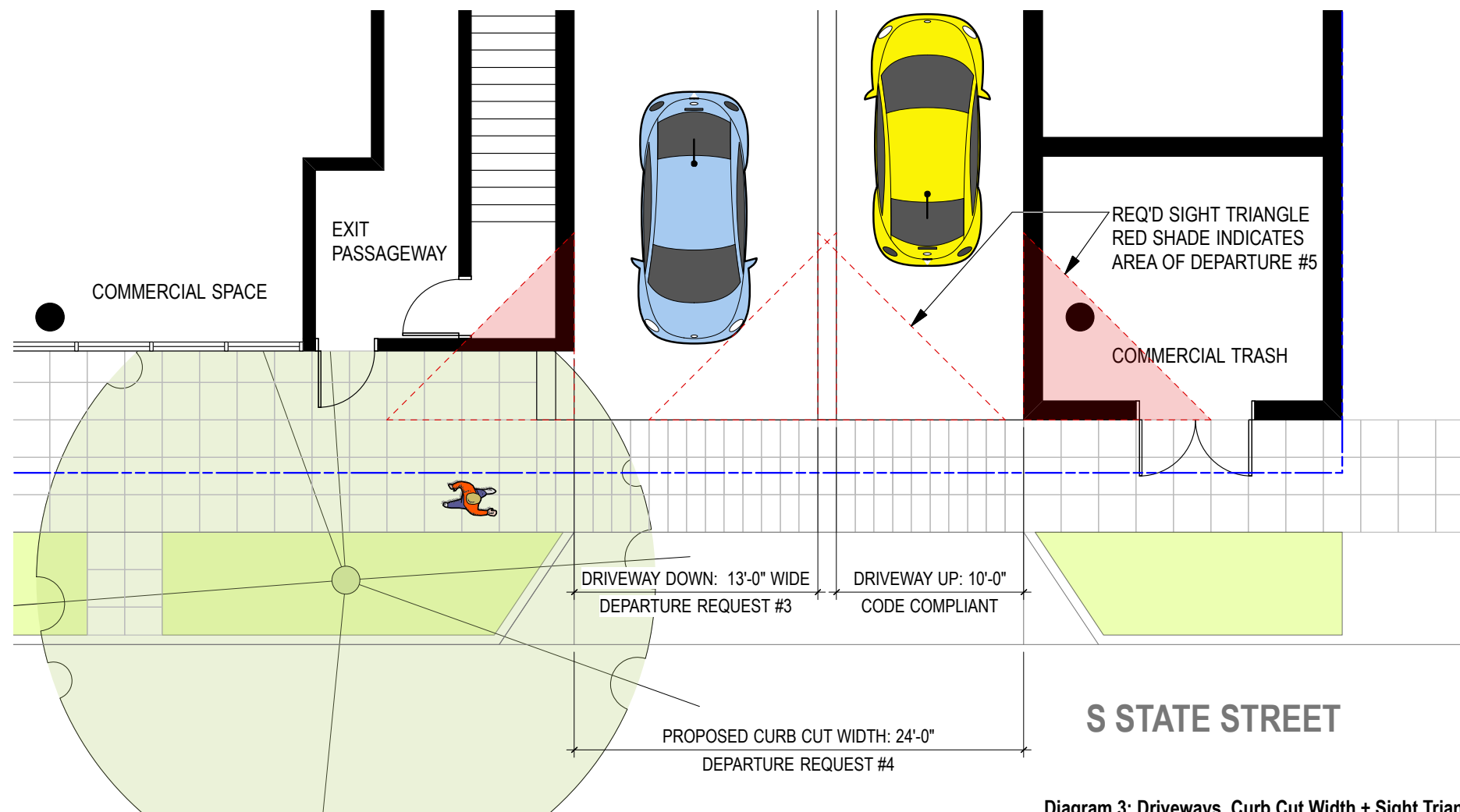


Diagram 3: Driveways, Curb Cut Width + Sight Triangle

EXAMPLES OF PAST WORK • PUBLIC47 ARCHITECTS



Henry Apartments - Seattle, WA



Shelton Apartments - Seattle, WA



Anhalt Apartments - Seattle, WA



Saxonia Renovation - Seattle, WA



The Shea - Seattle, WA



Eastside Prep TALI Hall - Kirkland, WA

EXAMPLES OF PAST WORK • LAKE UNION PARTNERS



East Union - Seattle, WA



The State - Seattle, WA



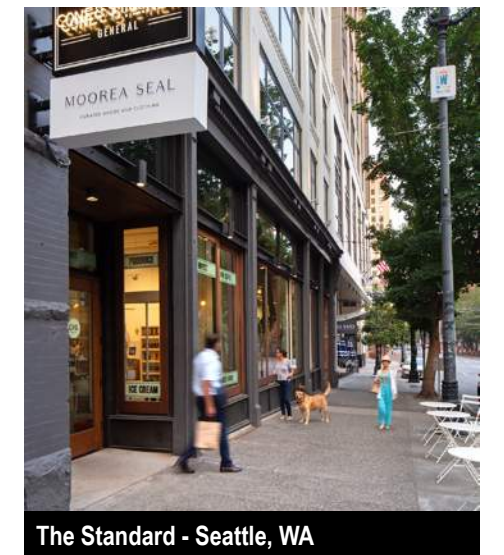
The Rooster - Seattle, WA



The Stencil - Seattle, WA



19th + Mercer - Seattle, WA



The Standard - Seattle, WA