

Amalfi

APARTMENTS



12337 30th Ave. NE

REC Meeting #2 | SDCI #303584-LU

06/28/2021

PROJECT INFORMATION

ADDRESS

12337 30th Ave NE
Seattle, WA 98125

PARCELS

344800-0135 & 344800-0140

SITE AREA

18,655 SF (0.428 ACRES)

ZONING

NC3-75 (M)
Hub Urban Village, Lake City Hub

Overlay District

DESCRIPTION

Construction of a 7-story building consisting of one above-grade structure with 130 apartment units and one story of below-grade parking.

PROJECT TEAM

Architect

Tiscareno Associates
1200 6th Ave Ste. 605
Seattle, WA 98101
(206) 325-3356

Developer

EPIC Asset Management
219 E Garfield St Ste 600
Seattle, WA 98102
(206) 505-8000

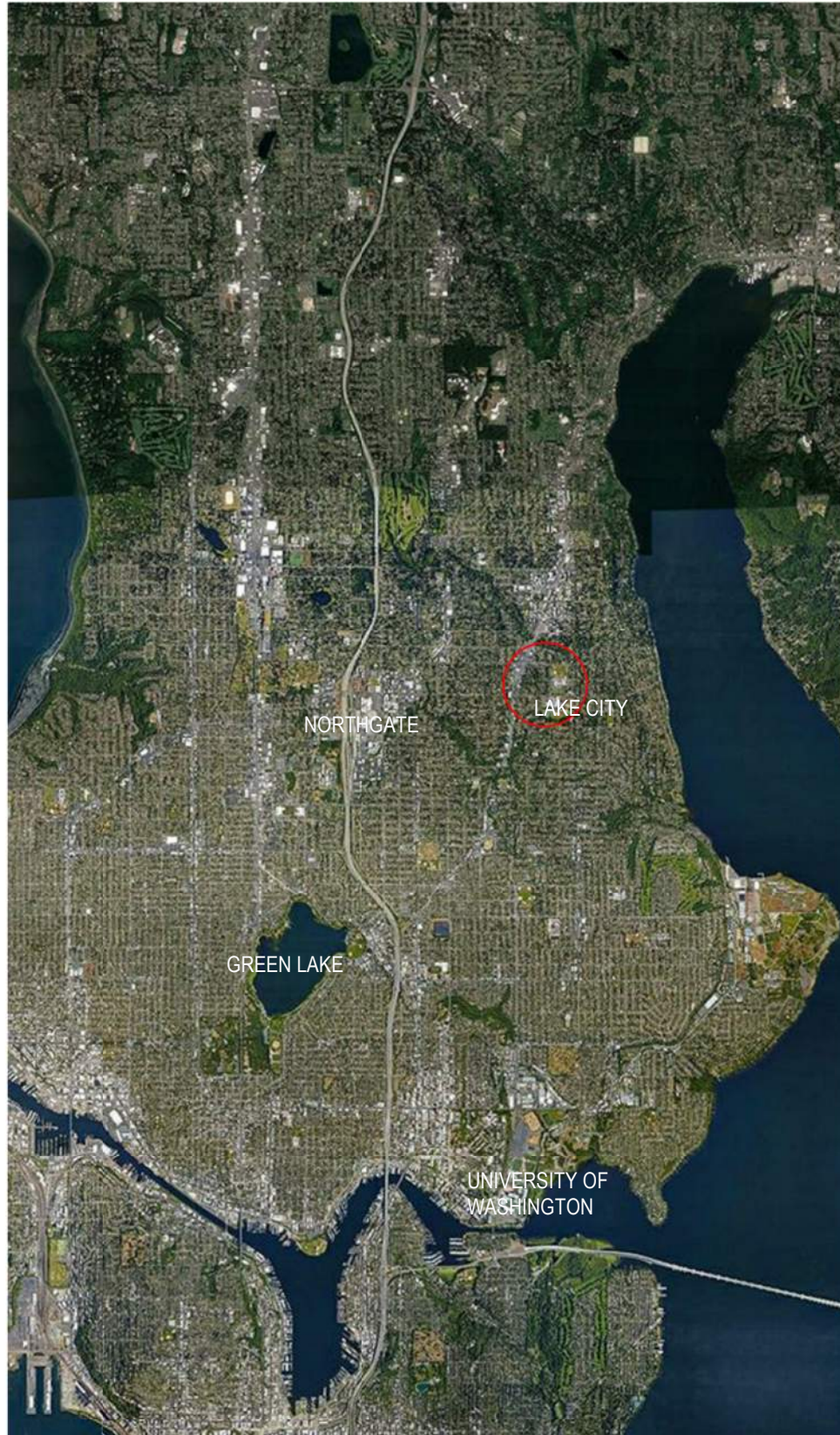
Landscape Architect

BlueLine Group
25 Central Way, Ste. 400
Kirkland, WA 98033
(425) 216-4051

CONTENTS

- 01 Project Introduction
- 02 Zoning Summary
- 03 Existing Site Plan
- 04.1 Board Direction and Response
- 04.2 Prior Design
- 05 Design Response - East / South
- 06 Design Response - East Elevation (30th Ave)
- 07 Design Response - South Elevation
- 08 Design Response - East Bay Study
- 09 Design Response - 30th Streetscape
- 10 Design Response - Color Studies
- 11 Design Response - North
- 12 Design Response - North Elevation
- 13 Design Response - Bike Entry
- 14 Lighting
- 15 Design Response - West (Alley)
- 16 Materials Board
- 17 Courtyard Elevations
- 18 Views
- 19 Site Plan
- 20 Floor Plans
- 21 Floor Plans
- 22 Floor Plans
- 23 Building Section
- 24 Landscape
- 25 Landscape
- 26 Signage
- 27 Closer

Project Introduction



DEVELOPMENT OBJECTIVES

The proposed project is a 7 story residential building with 130 units. Several below grade parking spaces will be provided on site as well as some on-grade parking accessed by the alley.

The size and massing are complementary to the Urban Hub vision and fit in with the 5, 6, and 7 story developments in the Urban Hub.

Proposed Uses: Multi-Family
Structure Height: Approx. 75 feet
Number of Residential Units: 130
Number of Live-Work Units: 0
Commercial Area: 0 SF
Building Area Approx.: 173,000 SF
Parking Stalls: 51
Bicycle Spaces: 117

EXISTING SITE AND CONTEXT

The project is located in the Lake City Urban Hub, a neighborhood in Northeast Seattle. This area is developing its unique identity as Seattle continues to urbanize. Several new projects have been constructed that are of similar size and scale to proposed building.

The site consists of two parcels, midblock between NE 125th Street and NE 123rd Street. There are currently two single story office buildings on the site. There is a grade change of 4'- 2" feet along the 30th Ave NE face of the site. The most notable immediate landmark is to the south, Dick's Drive in. There are no trees on the site.

The surrounding buildings in the Urban Hub are a mix of 1-story commercial, low rise and mid-rise multifamily, as well as several civic buildings. The architectural character of this neighborhood is still developing. However, the once automobile-dominant planning is giving way to a pedestrian-friendly urban environment.

ZONING

Zoning for the project and adjacent sites are NC3-75. The Lake City Urban Hub is a mix of NC3, NC3P, LR, and MR.



Lake City Gateway proposal from 1992. Notable for acknowledging Lake City Way's auto-oriented history. Excerpt from Lake City Urban Design Framework

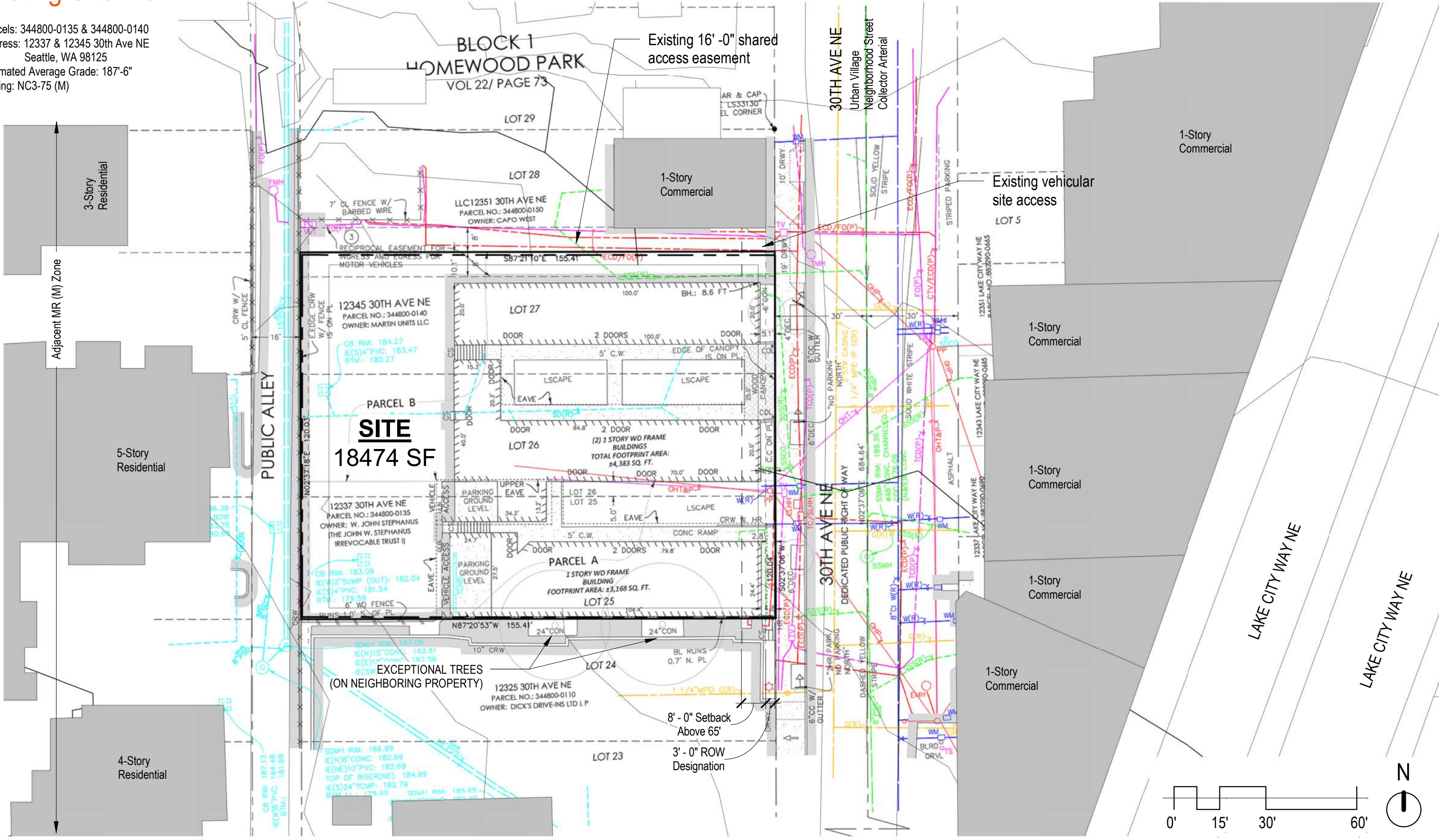
Zoning Summary

SITE OVERVIEW		
Address		12337-12345 30th Avenue NE, Seattle, WA 98125
Parcel Numbers		344800-0135 & 344800-0140
Lot Size		18,655 sf
ZONING OVERVIEW		
Zone	23.47A	NC3-75 (M)
Adjacent Zones		MR (M)
Zoning Overlay	23.47A.009.E	Hub Urban Village, Lake City Hub
ROW Designation	ROW 2.9	30th Avenue NE: Urban Village Neighborhood, Collector Arterial Minimum ROW: 66', Existing ROW: 60'
	23.53.030.D Table C	Alley: Minimum ROW: 20'
Permitted Uses	23.47A.004 Table A	Permitted outright: eating and drinking establishments, sales and services, institutions, live-work units, residential, offices, medical services
ECA		None
MHA	23.47A.017	M suffix, low MHA area
Design Review Equity Area		Yes
DEVELOPMENT POTENTIAL		
Allowable FAR	23.47A.013	Base FAR: 5.5 Min FAR: 2
		Area exempt from FAR limits: stories that are underground, portions of a story that extend no more than 4 feet above the lower of finish or existing grade, rooftop greenhouses, bicycle commuter facilities, required bicycle parking for small efficiency dwelling units or congregate residence sleeping rooms
Structure Height	23.47A.012	75 ft
	23.47A.012.C.2	4' above base height for clerestories, parapets, and railings, greenhouses, solariums, and firewalls
	23.47A.C.4-5	16' above base height for stair and elevator penthouses 15' for mechanical equipment, solar collectors, play equipment, communication utilities, and greenhouses dedicated to food production
Setbacks	23.47A.014 .C.1	Street: Avg 8' upper level setback for portions above 65'
	23.47A.014.B.2	Alley: No setback required. Side: No setback required.
		Projections in required setbacks: Decks with open railings, uncovered unenclosed pedestrian bridges, detached solar collectors, dumpsters and other trash receptacles, green stormwater infrastructure
Landscaping	23.47A.016.A.2	Green factor score 0.3 or greater pursuant to 23.86.019
	23.47A.016 Table A	Landscaping required for surface parking areas: 20-25 spaces: 18 sf / parking space 52-99 spaces: 25 sf / parking space 100+ spaces: 35 sf / parking space
Amenity Area	23.47A.024	Minimum 5% of gross floor area in residential use. Rooftop areas excluded.
		Required amenity areas shall not be enclosed. Common amenity: minimum 10' and 250 sf Private balconies and decks: minimum 6' and 60 sf

STREET-LEVEL STANDARDS			
Blank Facades	23.47A.008	May not exceed 20' in width between 2'-8' above the sidewalk on the street-facing façade May not exceed 40% of the width of the façade of the structure along the street	
Non-Residential Uses	23.47A.008.B.2	Transparency:	Spaces greater than 600 sf shall extend an avg depth of at least 30' and min depth of 15' from the street-level street-facing façade
	23.47A.008.B.3	Depth:	Spaces greater than 600 sf shall extend an avg depth of at least 30' and min depth of 15' from the street-level street-facing façade
	23.47A.008.B.4	Height:	Minimum floor-to-floor height: 13'
Residential Uses	23.47A.008.C.1	Entry:	Prominent pedestrian entry on street-level street-facing façade
	23.47A.008.C.2	Floor:	Min 4' above or below sidewalk grade, or be setback min 10' from sidewalk
Live-Work	23.47A.008.D.1	Areas:	Min 300 sf between street and residential portion of live-work. Non-residential portion shall extend the width of the street-level street-facing façade, and min 15' deep
	23.47A.008.D.2	Pedestrian Entry:	Each unit must have a visually prominent entry located on street-facing façade, and provide direct access to non-residential portions
	23.47A.008.D.3	Signage:	Must include exterior sign
PARKING REQUIREMENTS			
Automobiles	23.47A.015	Residential in urban village: No minimum req	
	23.47A.032.A.1.a	Parking access required from existing alley	
	23.27A.032.B.1	a. Shall not be between a structure and a street lot line. b. Within a structure, street-level parking shall be separated from street-level street-facing facades by another use. c. Parking to the side of a structure shall not exceed 60' of street frontage	
	23.54.026	Flexible-use parking area	
Bicycles	23.54.015 Table D	Multi-Family Residential:	Long Term: 1 per dwelling unit Short Term: 1 per 20 dwelling units
		Commercial: Offices:	Long Term: 1 per 2,000 sf Short Term: 1 per 10,000 sf

Existing Site Plan

Parcels: 344800-0135 & 344800-0140
Address: 12337 & 12345 30th Ave NE
Seattle, WA 98125
Estimated Average Grade: 187'-6"
Zoning: NC3-75 (M)



Board Direction and Response

EDG Summarized Staff

Recommendations with Response

1. Massing, Design & Street Level Activation:

a. *The applicant will need to include front façade and street level studies to further inform the massing. The study will need to demonstrate how the front façade and street level design effectively breaks down the building mass from the street. The street level study will need to demonstrate how the lobby, amenity space, and street level residential unit are distinct, separate spaces along the street. (DC2.A.2, CS3.A, PL3.A, PL3.B)*

Response: Design diagrams have been included which show the evolution of the basic massing and character on four sides of the building. The residential unit has been removed, and the lobby and amenity space remain. The lobby has been accentuated with a canopy and color and is distinct from the amenity edge, which has a stepping planter to soften the sidewalk.

b. *Staff supports the bump outs shown along 30th Avenue NE, however more detail is needed. The applicant will need to provide character sketches and material callouts for the bump outs to further assist staff in understanding how the bump outs work with the overall building design. In addition, the recommendation packet will need to demonstrate how the bump outs, secondary elements, and massing moves on the north and south sides of the building are applied to create a clear design concept to all four sides of the building. (DC2.B, DC2.C)*

Response: Design diagrams per the previous comment have been included as well as material callouts.

3. Blank walls:

b. *Staff has concerns about the north and south walls of the building being potentially blank walls, noting that presently both the north and south walls will be highly visible to the public and residents in the area. Staff encourages the applicant to introduce secondary elements on both the north and south walls to assist in breaking down the massing. (DC2.B)*

Response: Particular attention is being paid to the north and south facades and how they integrate into the overall design. The design diagrams express this integration and development.

REC Summarized Board Recommendations with Response

1. Architectural Concept/Massing

a. *...The Board noted the application of color, especially with the bays, needed further study and better color integration...Board recommended the applicant simplify the accent colors and study which accent colors may work best on each façade. The Board also recommended the applicant identify one expression on each façade and maintain that expression instead of combining multiple expressions.*

Response: The use of an accent color has been significantly reduced. A wood/wood-like material that is more subtle than the accent color and residential in feel has been added. With the addition of the art to the south façade, we felt that a more muted south façade would allow more flexibility with the art and not compete with it for attention. The resulting design of the south facade uses less direct references to the east. For the sake of consistency, the north façade utilizes the same design approach as the south.

b. *...The Board suggested disassociating the tops and bottoms of each of the (East) bays ...The Board encouraged the applicant to explore changes to the design of the main entry to make it more distinct.*

Response: The vertical extension of the bays on the east façade has been studied. Breaking the planes at Level 2 and Level 7 with extensions have the negative effect of adding inconsistencies into the bay proportions, such as high foreheads at Level 7, without compensatory benefits in strengthening the scheme. At the entry, the continuous planter and lack of any other entrances along 30th make the entrance easily identifiable to any passerby. The canopy has been enlarged and changed to a bright color to further emphasize the entrance. At a distance, the wood/wood-like material used on the bay above the entry as well as the additional signage makes the entry very distinct from the remainder of the façade.

c. *The Board noted the architectural language on the north façade should be clarified by simplifying the façade...*

Response: The north facade has been simplified and muted to be consistent with the south facade. Panels affixed to the balconies have been changed to the wood/wood-like material to add consistency with the east facade.

d. *The Board commented that the bay window concept does not carry through on the west façade. The Board questioned how the cascading concept comes into play...The Board thought the west façade had two competing design logics currently expressed...*

Response: The design of the west façade has been completely revised. The scheme is much more simplified and uses one expression rather than a mixing of two.

e. *...The Board recommended the applicant incorporate civic art...on the south façade.*

Response: The design of the south façade has been revised. See prior note.

2. Entries

a. *...The Board recommended the applicant explore how to create a more meaningful entry at the corner that is prominent and easily identifiable as the main entry.*

Response: The canopy at the entrance has been enlarged and additional signage has been added.

b. *...The Board recommended the applicant explore ways to further highlight the bike entry from the street...*

Response: Graphics will be added to the north wall of the project that connect the bike entrance to the sidewalk.

3. Materials

a. *...The Board also noted the metal siding should be a gauge that would not move or introduce oil canning in longer runs.*

Response: Much of the fiber cement board on the east façade has been changed to an alternate material. The metal siding has been revised to minimize oil canning.

4. Landscaping/Lighting

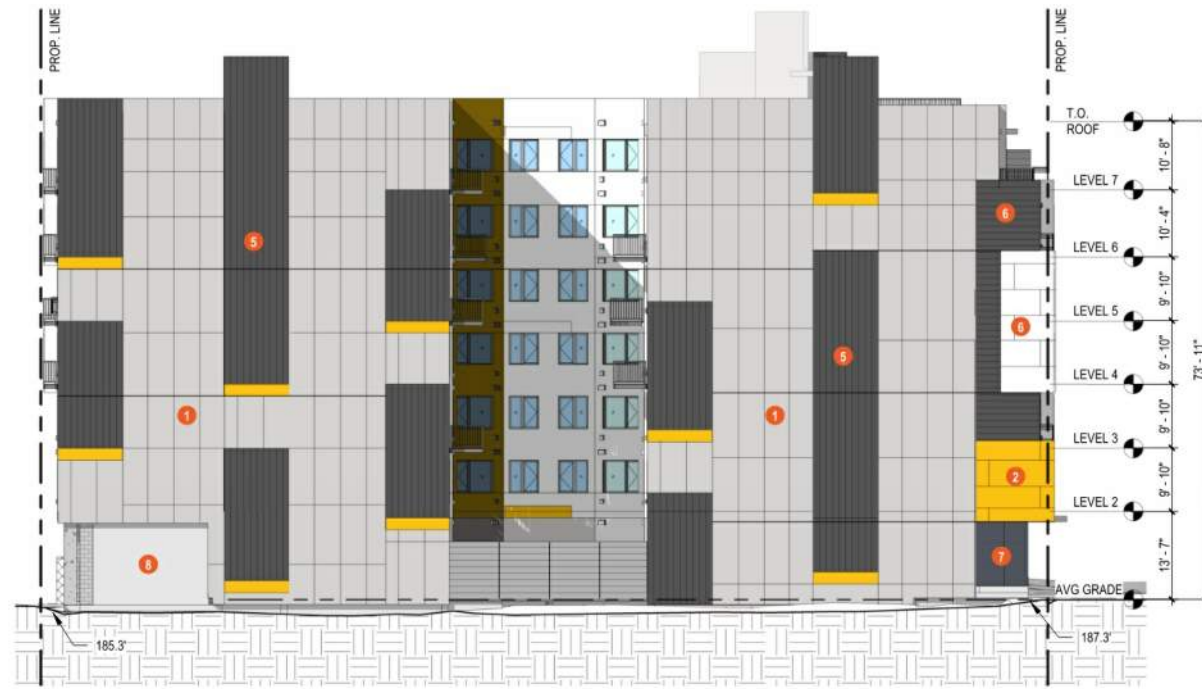
b. *...The Board suggested the applicant improve the lighting in the alley..The Board went further with the bike entry and recommended additional lighting.*

Response: Lighting has been added to the alley and between the bike entry and the sidewalk

Prior Design



30th Ave (SE)



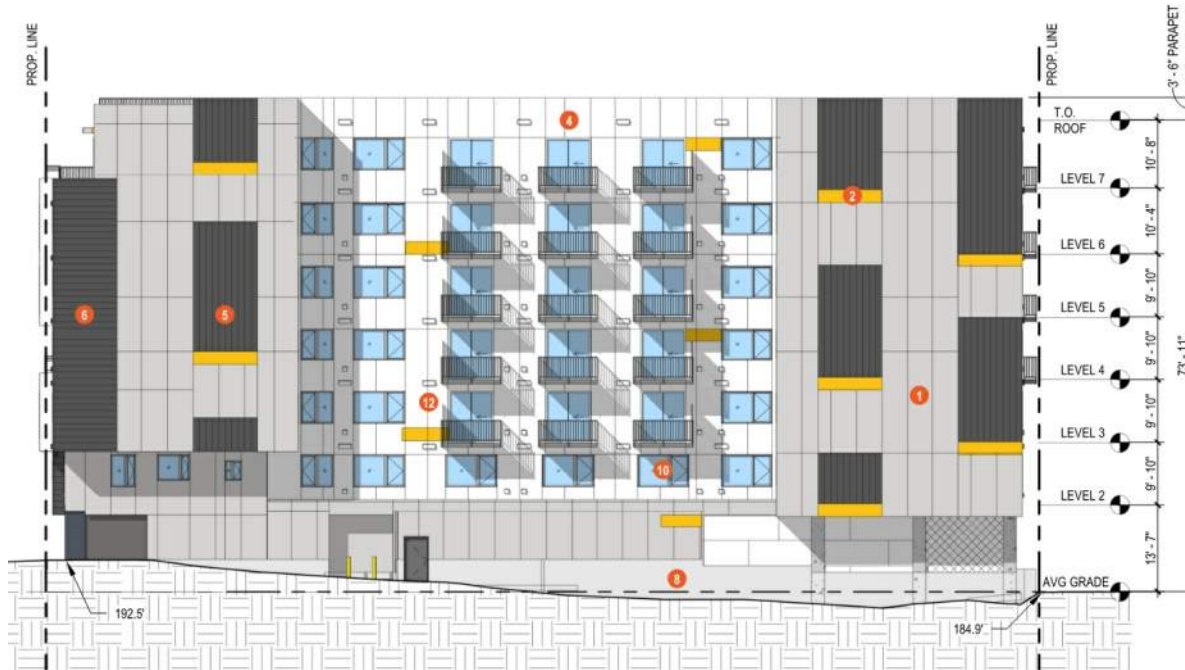
South



East (30th Ave)



30th Ave (NE)



North



West (Alley)

Design Response - East / South



① Previous Design - View at 30th

Comment:...The Board noted the application of color, especially with the bays, needed further study and better color integration...Board recommended the applicant simplify the accent colors and study which accent colors may work best on each façade. The Board also recommended the applicant identify one expression on each façade and maintain that expression instead of combining multiple expressions.

Comment:...The Board recommended the applicant incorporate civic art...on the south façade.

Response: The use of an accent color has been significantly reduced. A wood/wood-like material that is more subtle than the accent color and residential in feel has been added. With the addition of the art to the south façade, we felt that a muted, simplified south façade would allow more flexibility with the art and not compete with it for attention. The resulting design of the south facade uses modules related to the east facade doesn't reference the cascade expression.

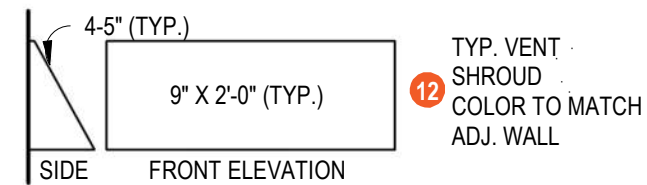
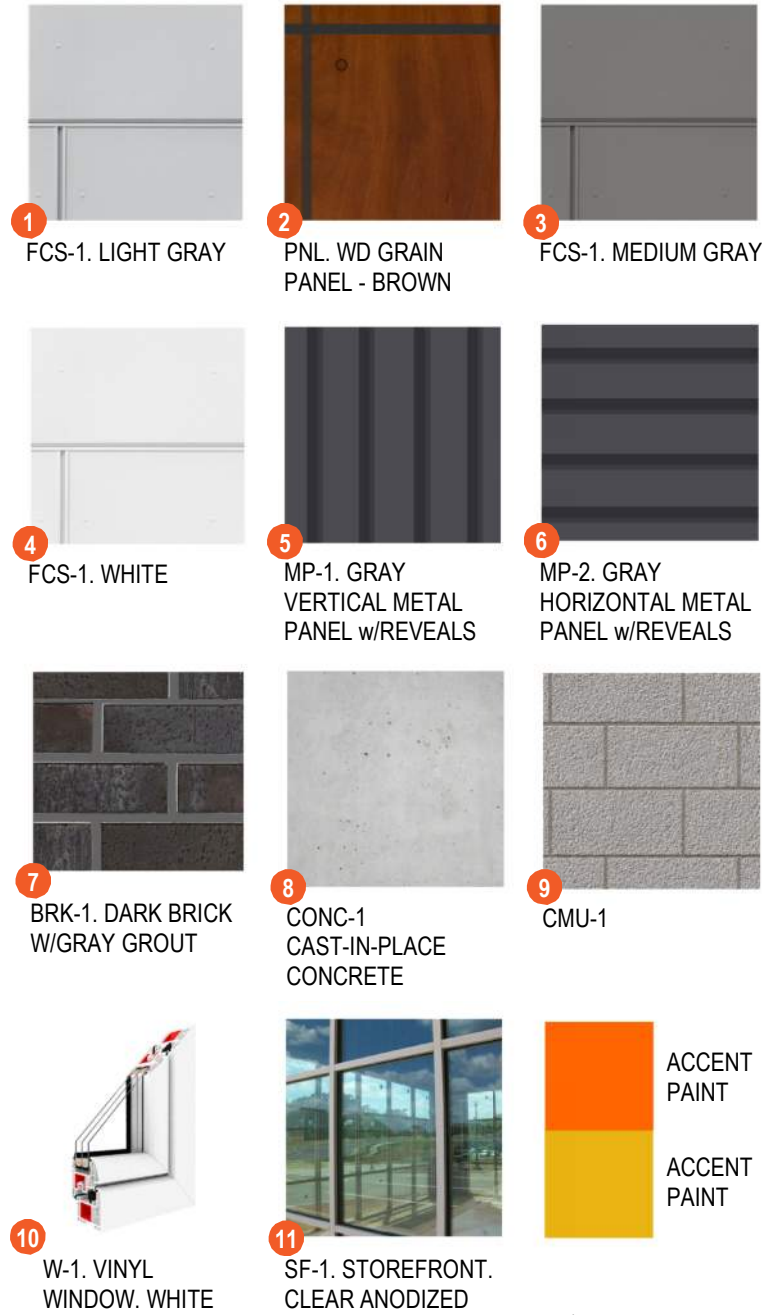
The southern, double bay return has been expanded along the south face to create a space for the art.



② Revised Design - View at 30th

NOTE: Art shown is only representative. Final art will be determined prior to CofO.

Design Response - East Elevation (30th Ave)

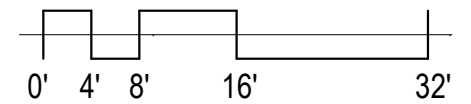


- FCS: FIBER CEMENT SIDING. 5/16" THICK. EXPOSED FASTENERS PAINTED TO MATCH
- METAL PANEL. CONCEALED FASTENERS. 22 GA FOR LENGTHS > 22' AND 24 GA. FOR LENGTHS < 22'. REVEALS ARE APPROX 1" WIDE EVERY 6"
- PNL: WOOD GRAIN HPL PANEL. COLOR MATCH EXPOSED FASTENERS

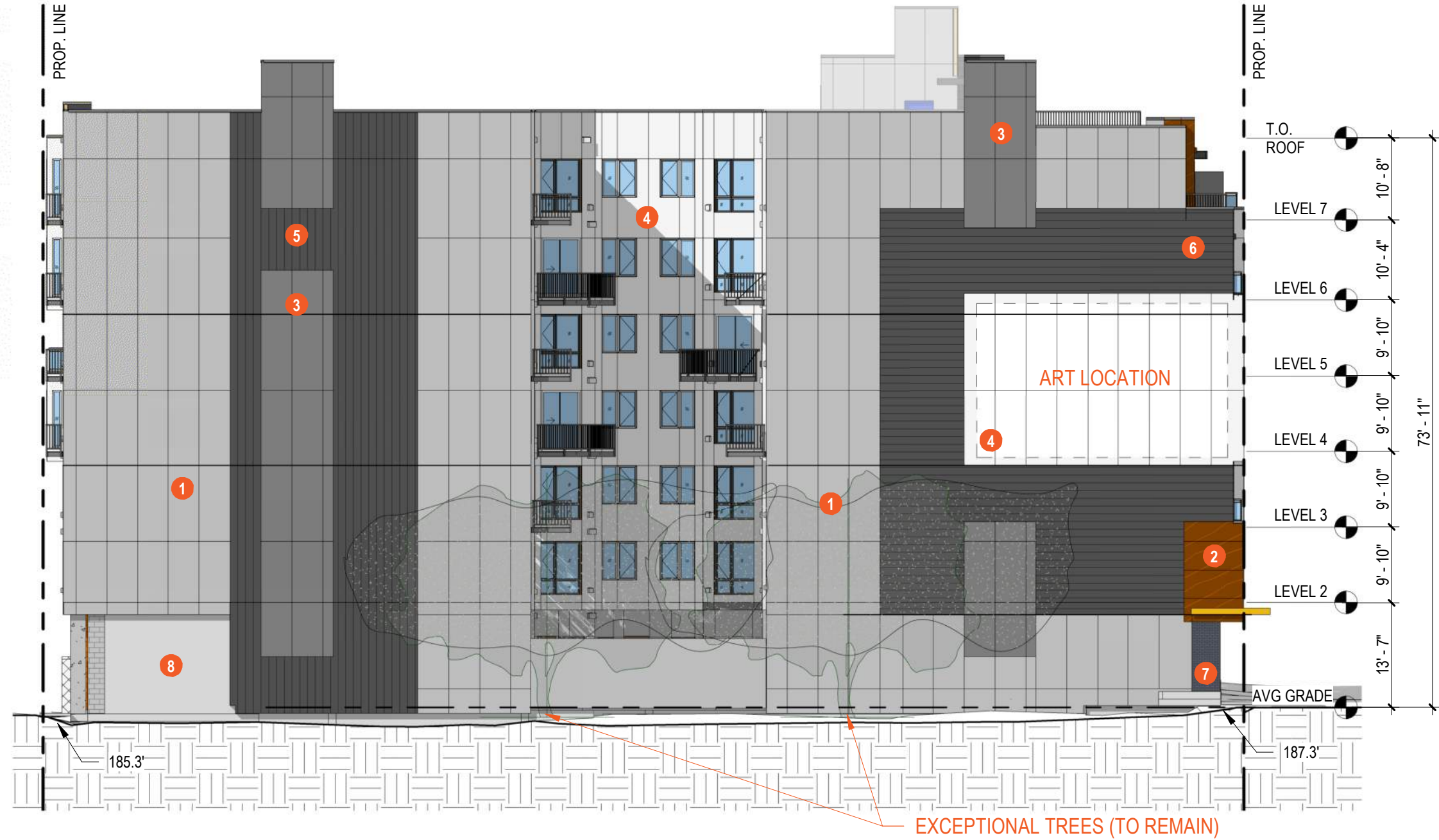
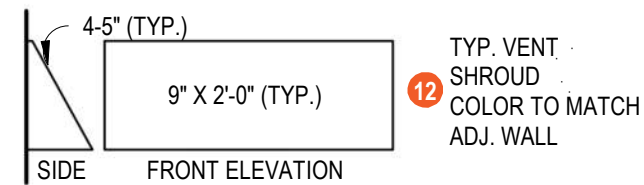
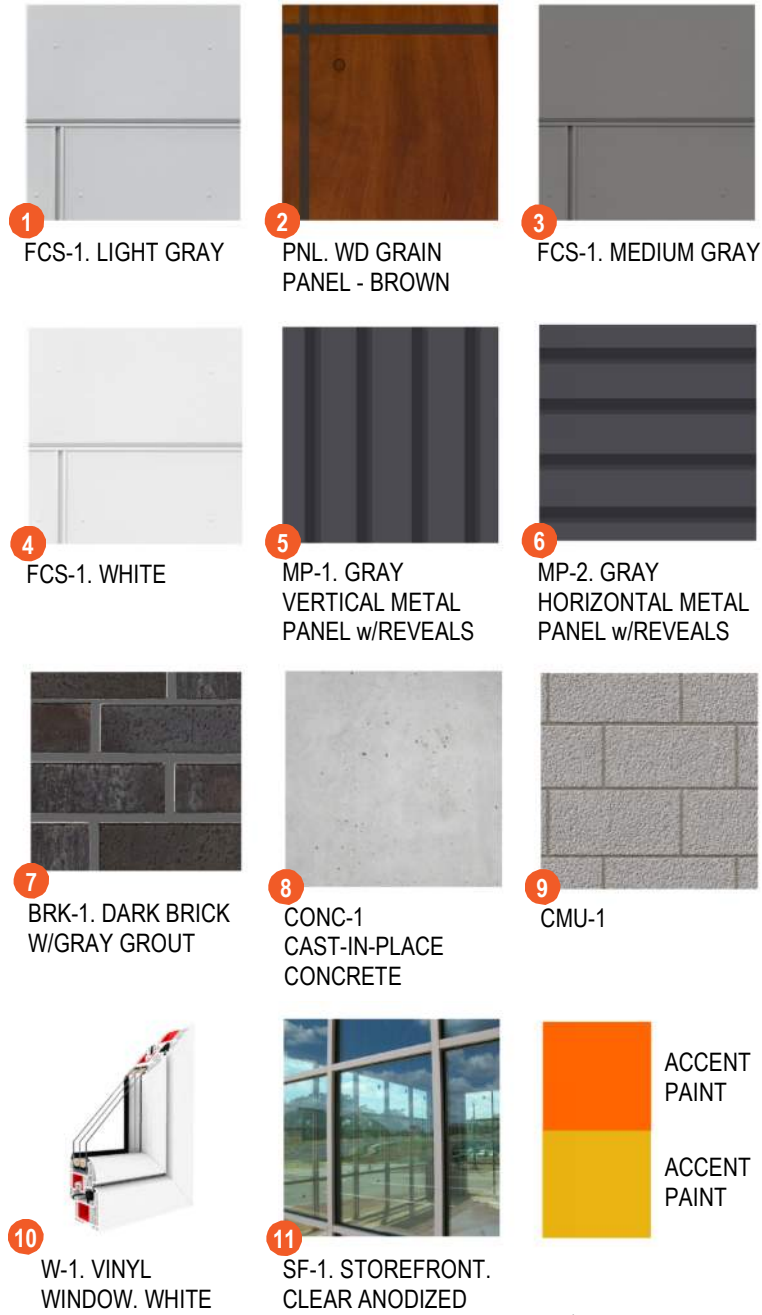


EAST ELEVATION - 30TH AVE

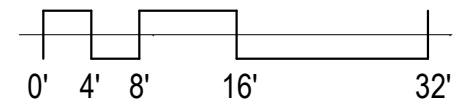
1/16" = 1'-0"



Design Response - South Elevation



SOUTH ELEVATION
1/16" = 1'-0"



- FCS: FIBER CEMENT SIDING. 5/16" THICK. EXPOSED FASTENERS PAINTED TO MATCH
- METAL PANEL. CONCEALED FASTENERS. 22 GA FOR LENGTHS > 22' AND 24 GA. FOR LENGTHS < 22'. REVEALS ARE APPROX 1" WIDE EVERY 6"
- PNL: WOOD GRAIN HPL PANEL. COLOR MATCH EXPOSED FASTENERS

Design Response - East Bay Study

Comment:...The Board suggested disassociating the tops and bottoms of each of the (East) bays

Response: The vertical extension of the bays on the east façade has been studied. Breaking the planes at Level 2 and Level 7 with extensions have the negative effect of adding inconsistencies into the bay proportions, such as high foreheads at Level 7, without compensatory benefits in strengthening the scheme.



Design Response - 30th Streetscape



EAST ELEVATION - 30TH AVE

1/8" = 1'-0"

2. Entries

a. ...The Board recommended the applicant explore how to create a more meaningful entry at the corner that is prominent and easily identifiable as the main entry.

Response: The canopy at the entrance has been enlarged and additional signage has been added.



Design Response - Color Studies



DARKER TONE / ENTRY ALTERNATIVE

WHITE BAY OVER THE ENTRY IS A PURER EXPRESSION OF THE BAY LANGUAGE BUT DOES NOT PUNCTUATE THE ENTRANCE AS MUCH

USE OF WOOD AT ENTRY CREATES WARMTH, BUT WOULD BE AN OVERUSE OF THE MATERIAL IF USED ON THE BAY ABOVE AS WELL



GRAYSCALE SCHEME

USE OF A MEDIUM GRAY COLOR CREATES LESS CONTRAST THAN THE PANEL IN THE PREFERRED SCHEME BUT IS LESS WARM



PROPOSED DESIGN

Design Response - North

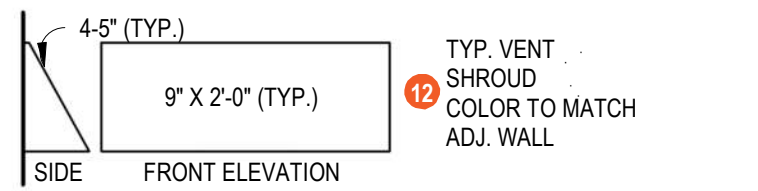
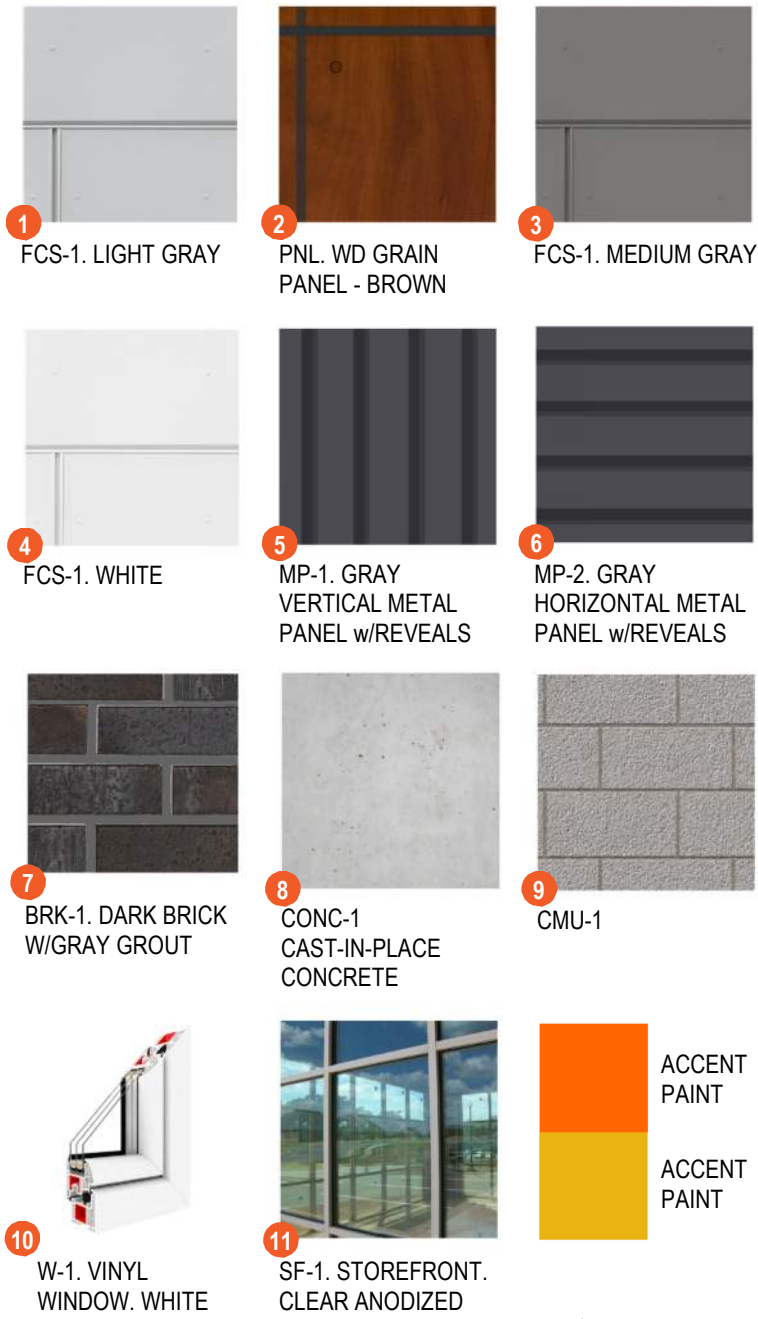
Comment: *The Board noted the architectural language on the north façade should be clarified by simplifying the façade...*

Response: The north facade has been simplified and muted to be consistent with the south facade. Panels affixed to the balconies have been changed to the wood/wood-like material to add consistency with the east facade.



① VIEW SOUTH FROM 30TH

Design Response - North Elevation



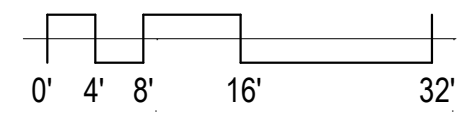
- FCS: FIBER CEMENT SIDING. 5/16" THICK. EXPOSED FASTENERS PAINTED TO MATCH
- METAL PANEL. CONCEALED FASTENERS. 22 GA FOR LENGTHS > 22' AND 24 GA. FOR LENGTHS < 22'. REVEALS ARE APPROX 1" WIDE EVERY 6"
- PNL: WOOD GRAIN HPL PANEL. COLOR MATCH EXPOSED FASTENERS



NORTH ELEVATION
1/16" = 1'-0"

Comment: The Board noted the architectural language on the north façade should be clarified by simplifying the façade...

Response: The north facade has been simplified and muted to be consistent with the south facade. Panels affixed to the balconies have been changed to the wood/wood-like material to add consistency with the east facade.



Design Response - Bike Entry



Comment:...The Board recommended the applicant explore ways to further highlight the bike entry from the street...

Response: Graphics will be added to the north wall of the project that connect the bike entrance to the sidewalk.

Comment:...The Board suggested the applicant improve the lighting in the alley..The Board went further with the bike entry and recommended additional lighting.

Response: Lighting has been added to the alley and between the bike entry and the sidewalk

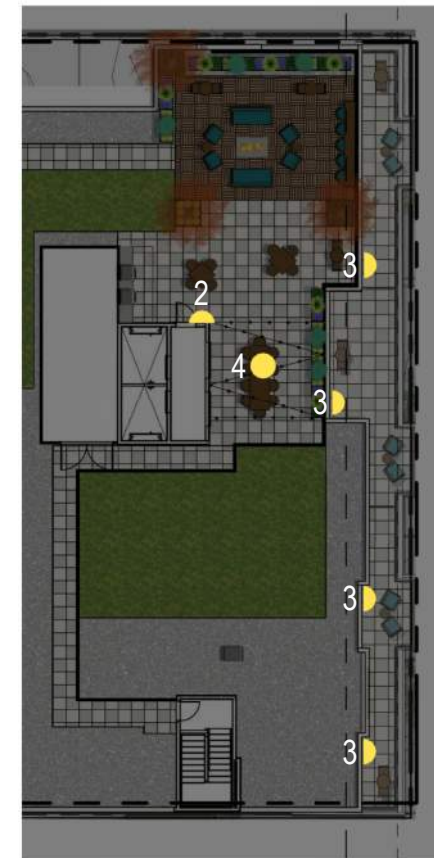
Lighting



Lighting Plan - Level 1
1" = 30'-0"

Comment:...The Board suggested the applicant improve the lighting in the alley..The Board went further with the bike entry and recommended additional lighting.

Response: Lighting has been added to the alley and between the bike entry and the sidewalk



Lighting Plan - Roof
1" = 30'-0"



1 Under Canopy Lights



2 Wall Light



3 Wall Light - Terrace



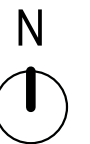
4 String Lights



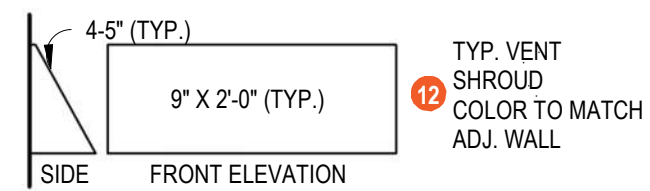
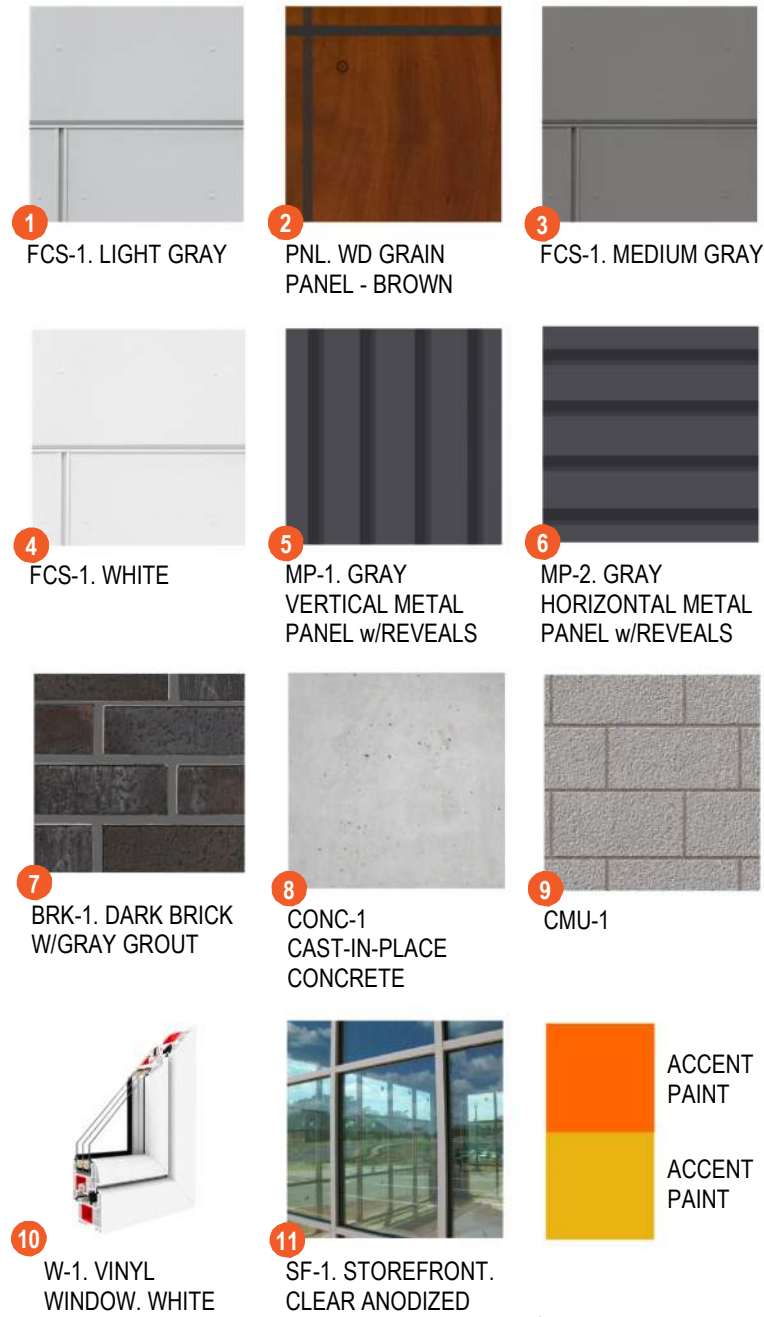
5 Soffit Light



6 Emergency Light



Design Response - West (Alley)



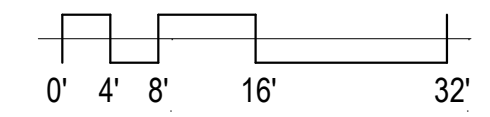
- FCS: FIBER CEMENT SIDING. 5/16" THICK. EXPOSED FASTENERS PAINTED TO MATCH
- METAL PANEL. CONCEALED FASTENERS. 22 GA FOR LENGTHS > 22' AND 24 GA. FOR LENGTHS < 22'. REVEALS ARE APPROX 1" WIDE EVERY 6"
- PNL: WOOD GRAIN HPL PANEL. COLOR MATCH EXPOSED FASTENERS



WEST ELEVATION - ALLEY
1/16" = 1'-0"

d. The Board commented that the bay window concept does not carry through on the west façade. The Board questioned how the cascading concept comes into play...The Board thought the west façade had two competing design logics currently expressed...

Response: The design of the west façade has been completely revised. The scheme is much more simplified and uses one expression rather than a mixing of two.



Materials Board

Comment:...The Board also noted the metal siding should be a gauge that would not move or introduce oil canning in longer runs.

Response: Much of the fiber cement board on the east façade has been changed to an alternate material. The metal siding has been revised to minimize oil canning.

FCS PANEL
WHITE

FCS PANEL
MEDIUM GRAY

REVEAL

MTL PANEL
SLATE GRAY - VERTICAL

WD PANEL
BROWN

ACCENT PAINT

MTL PANEL
SLATE GRAY - HORIZONTAL

REVEAL 2"

4"

2"

4"

2"

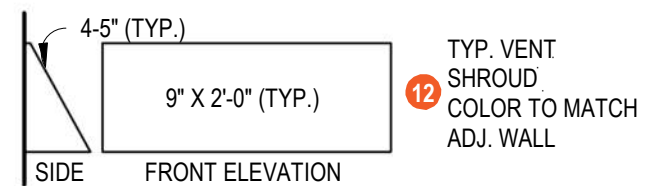
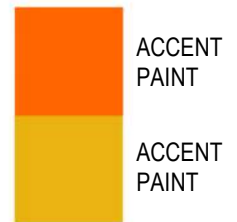
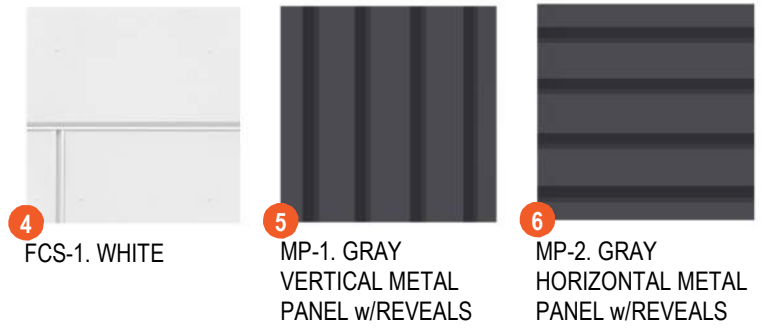
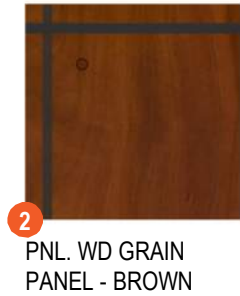
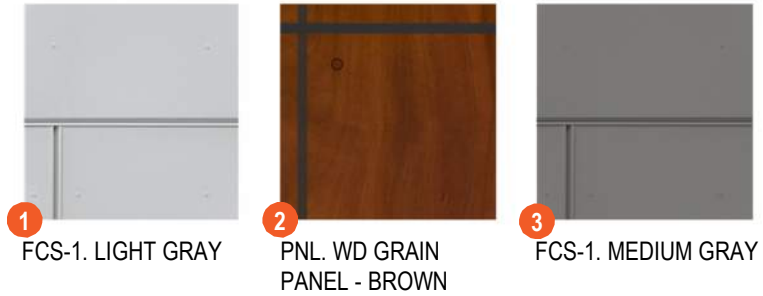
BRICK
COAL GRAY

FCS PANEL
LIGHT GRAY

CONCRETE

CMU

Courtyard Elevations



EAST ELEVATION - COURTYARD
1/16" = 1'-0"

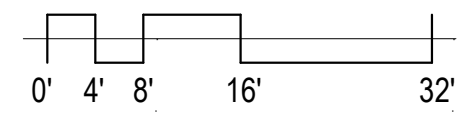


SOUTH - COURT
1/16" = 1'-0"



WEST ELEVATION - COURTYARD
1/16" = 1'-0"

- FCS: FIBER CEMENT SIDING. 5/16" THICK. EXPOSED FASTENERS PAINTED TO MATCH
- METAL PANEL. CONCEALED FASTENERS. 22 GA FOR LENGTHS > 22' AND 24 GA. FOR LENGTHS < 22'. REVEALS ARE APPROX 1" WIDE EVERY 6"
- PNL: WOOD GRAIN HPL PANEL. COLOR MATCH EXPOSED FASTENERS



Views



VIEW SOUTH FROM ALLEY

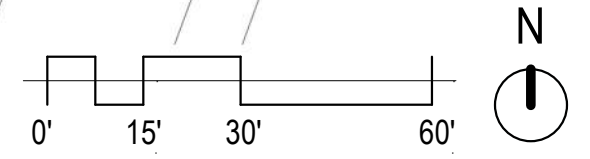
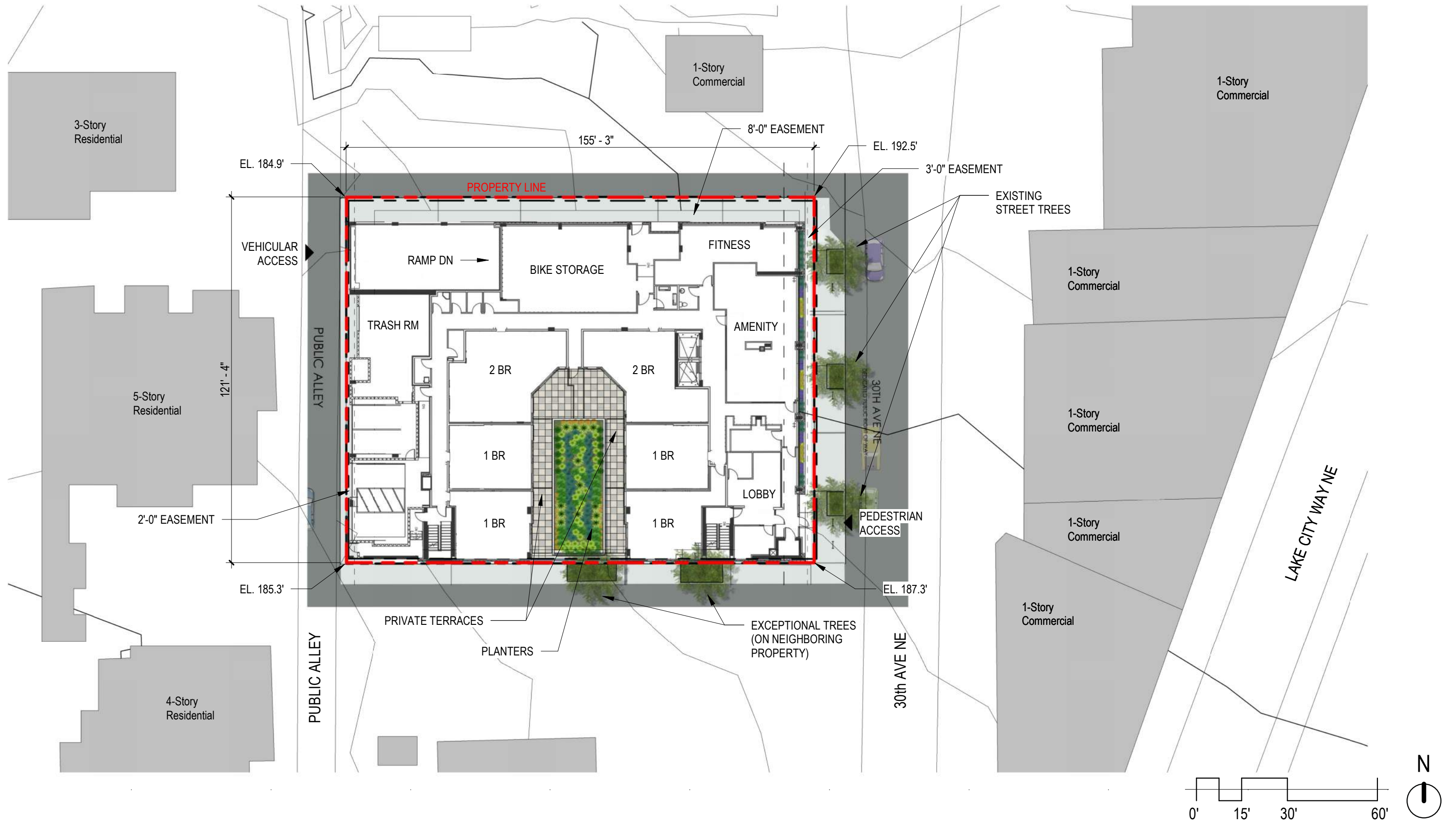


AERIAL VIEW

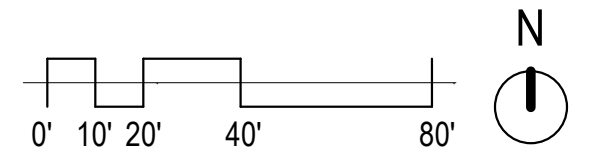
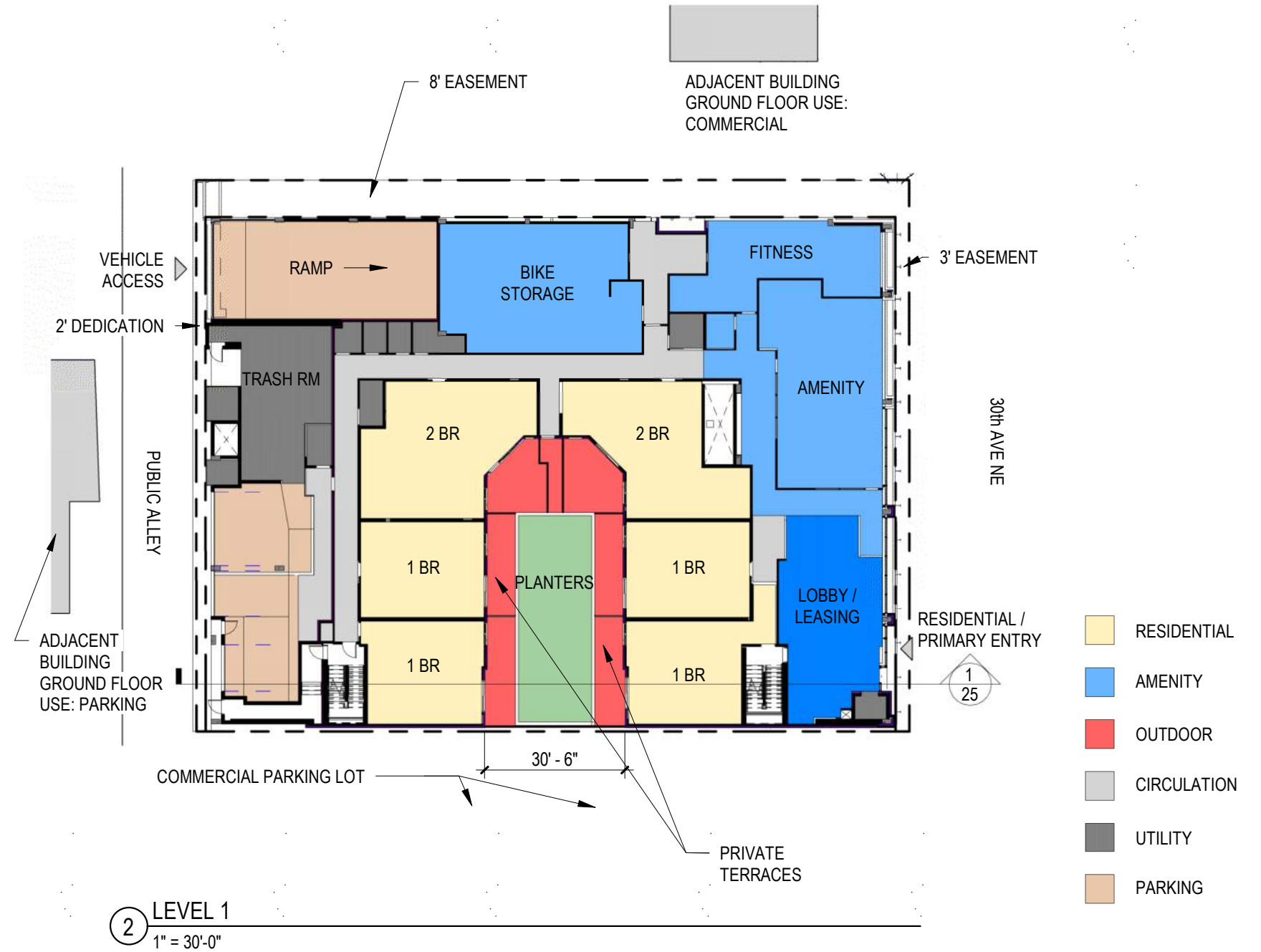
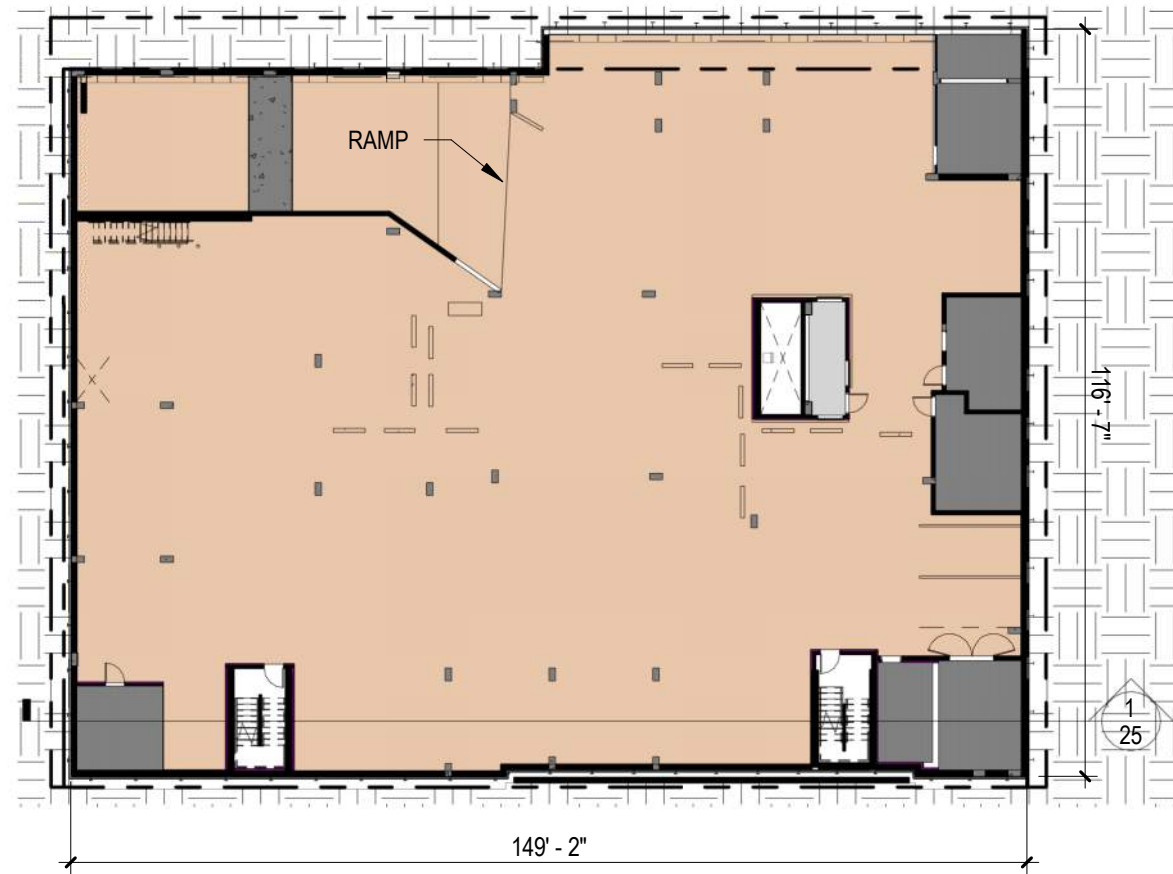


COURTYARD

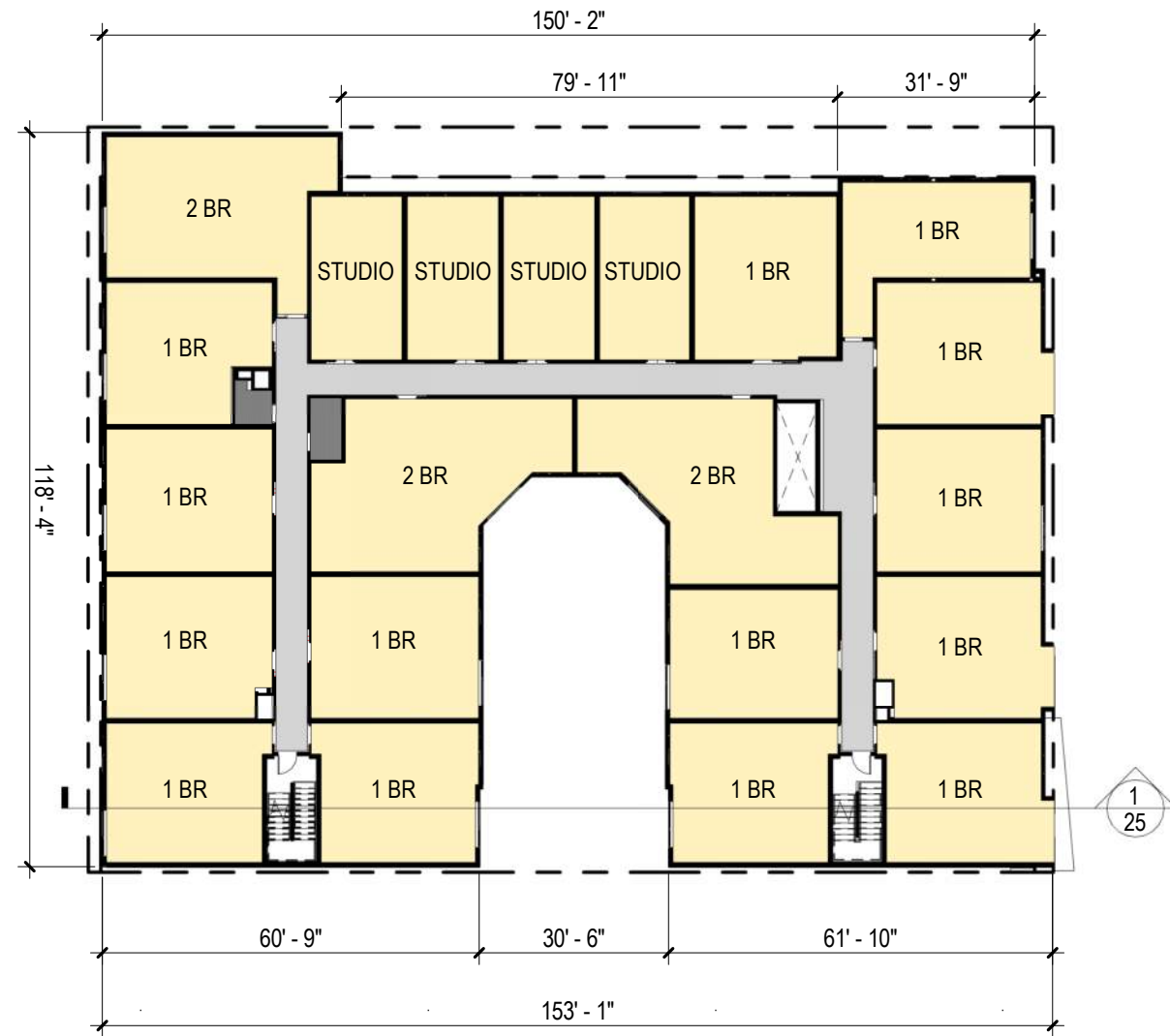
Site Plan



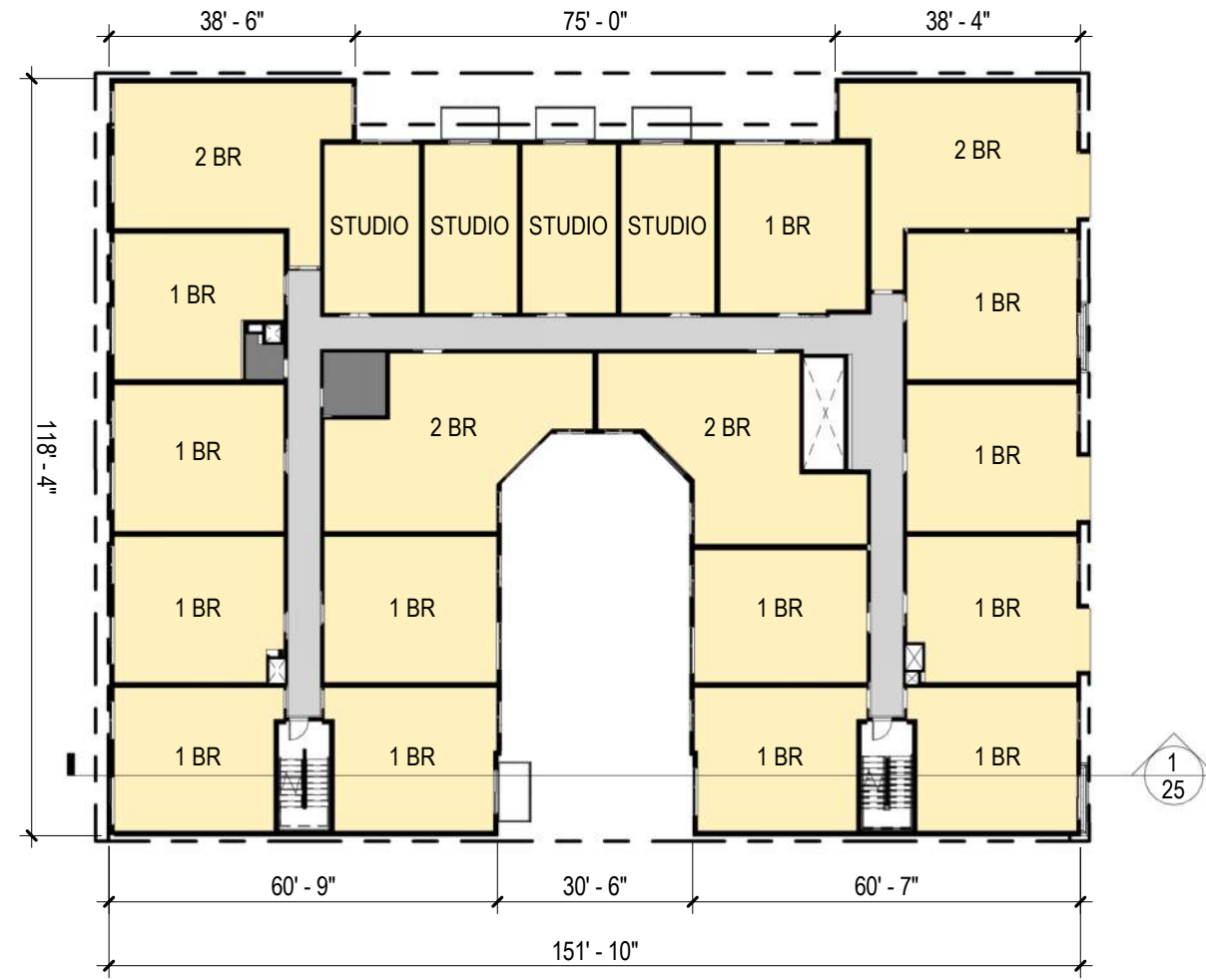
Floor Plans



Floor Plans

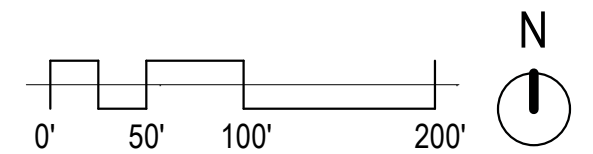


1 LEVEL 2
1" = 30'-0"

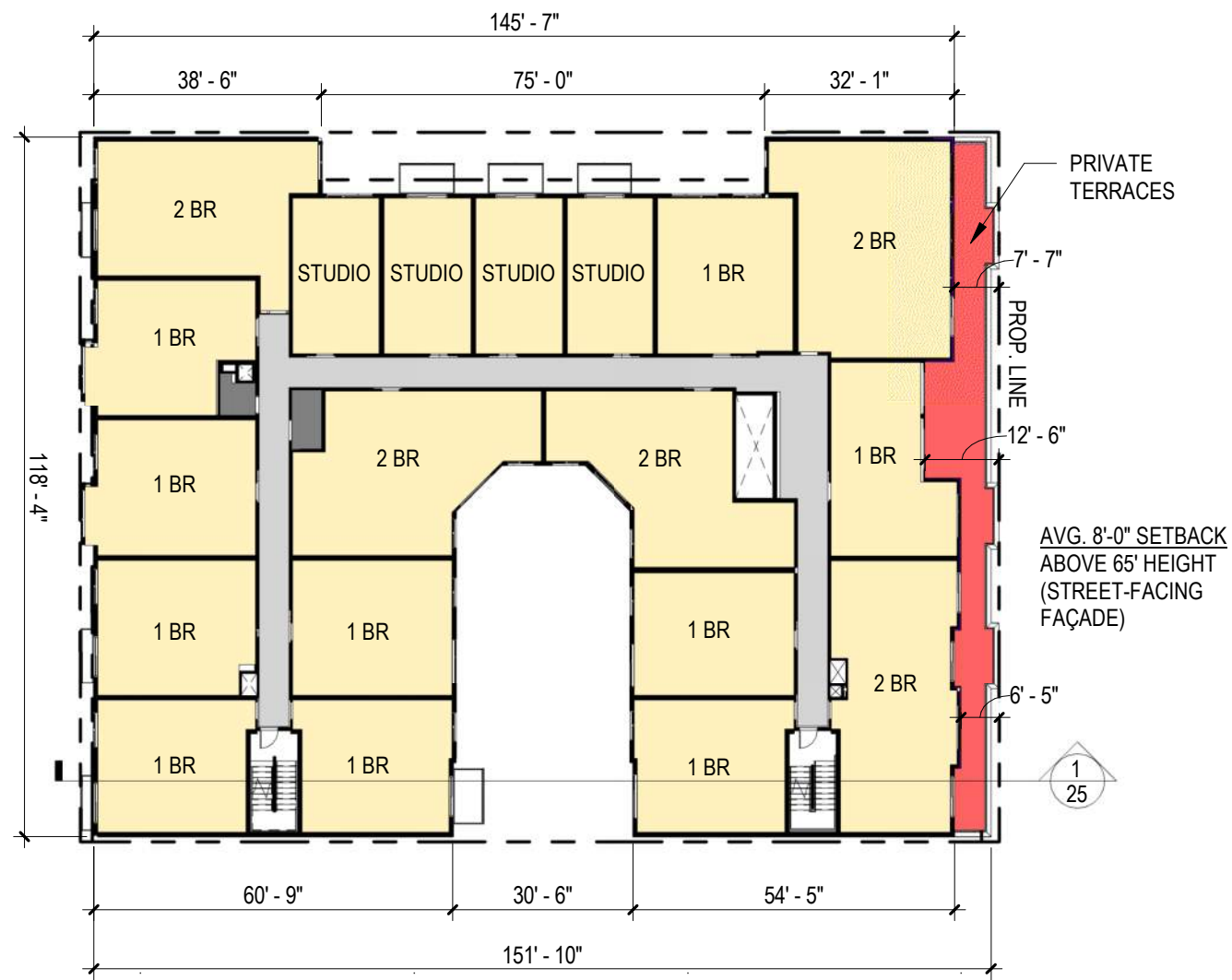


2 LEVEL 3-6
1" = 30'-0"

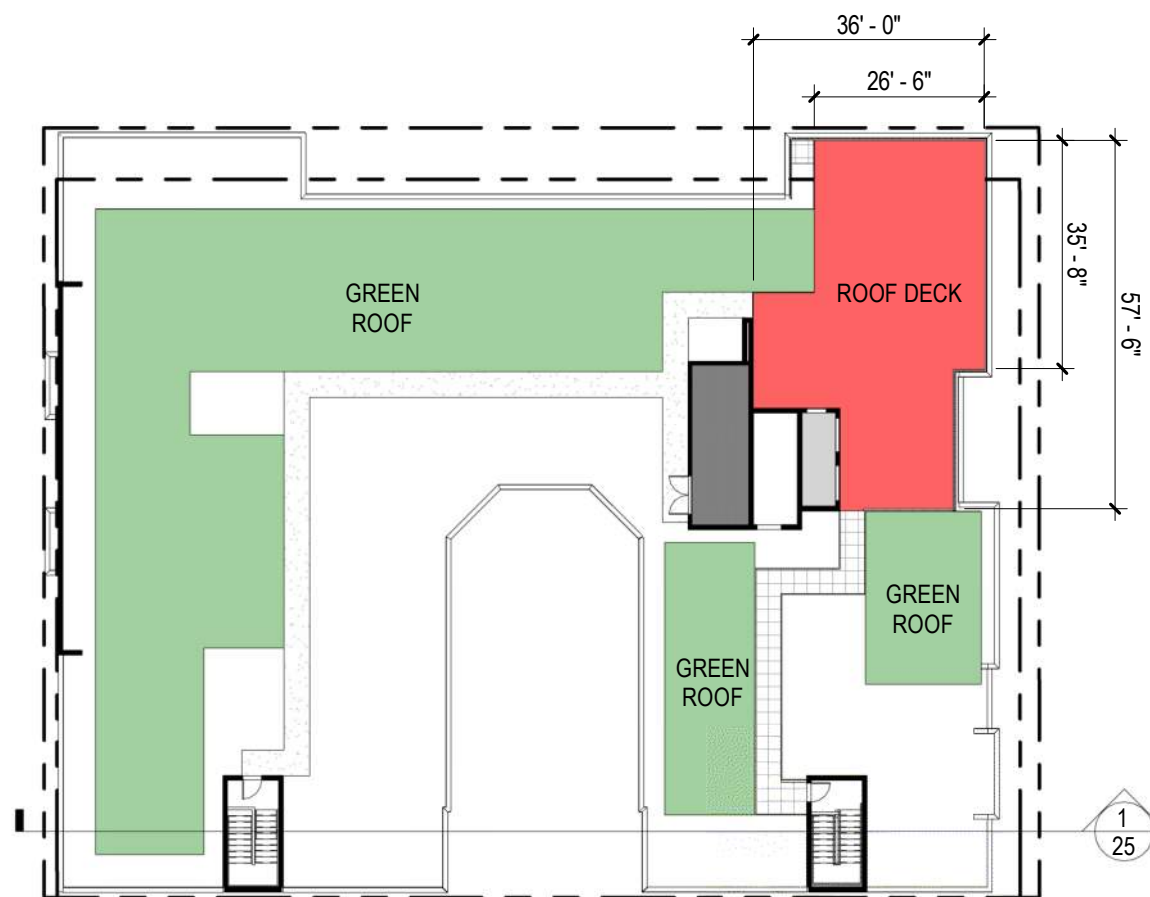
- RESIDENTIAL
- AMENITY
- OUTDOOR
- CIRCULATION
- UTILITY
- PARKING



Floor Plans

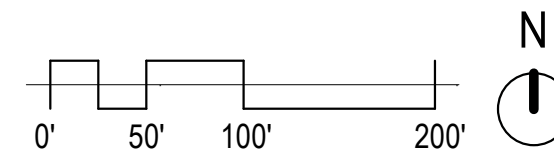


① LEVEL 7
1" = 30'-0"

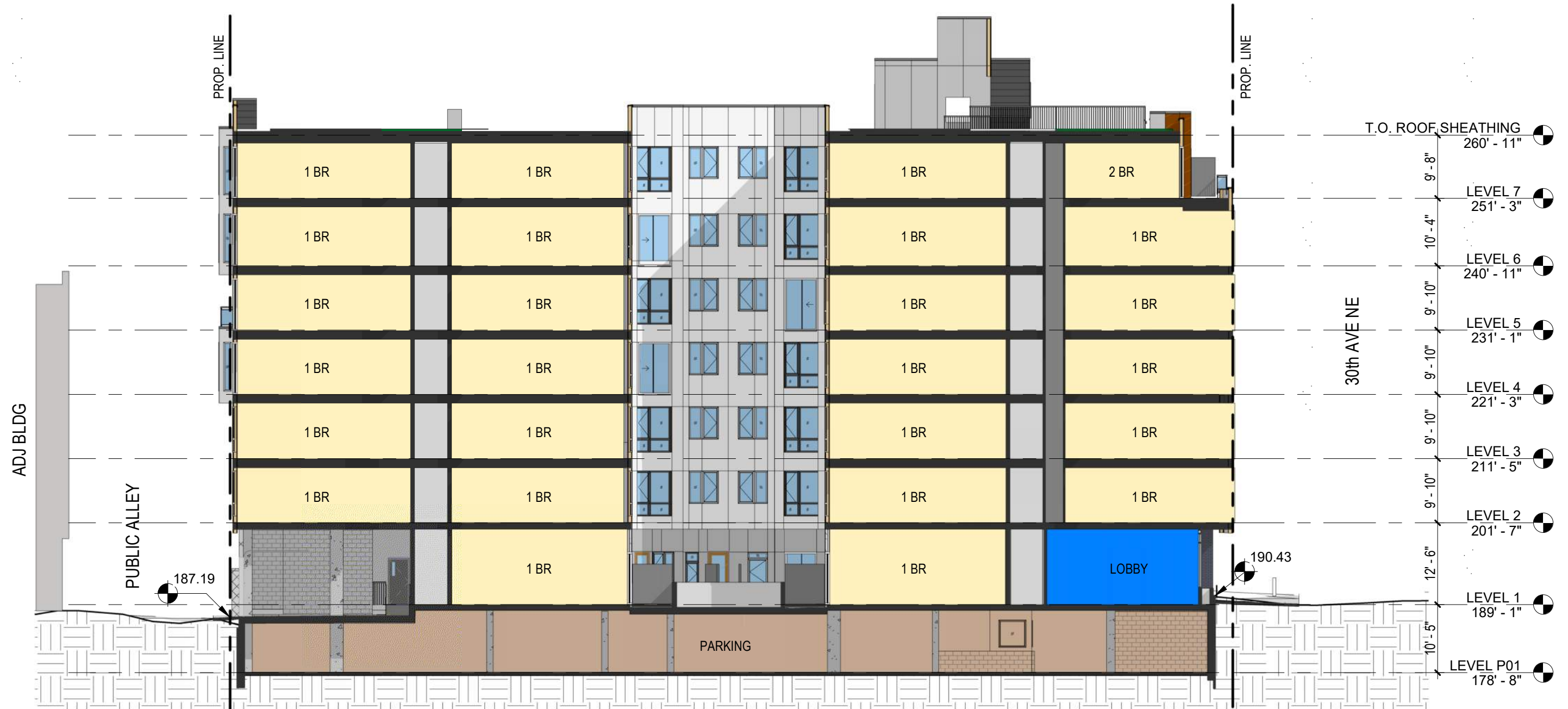


② ROOF
1" = 30'-0"

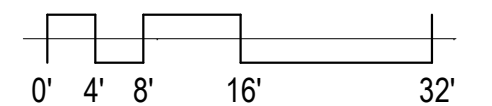
- RESIDENTIAL
- AMENITY
- OUTDOOR
- CIRCULATION
- UTILITY
- PARKING



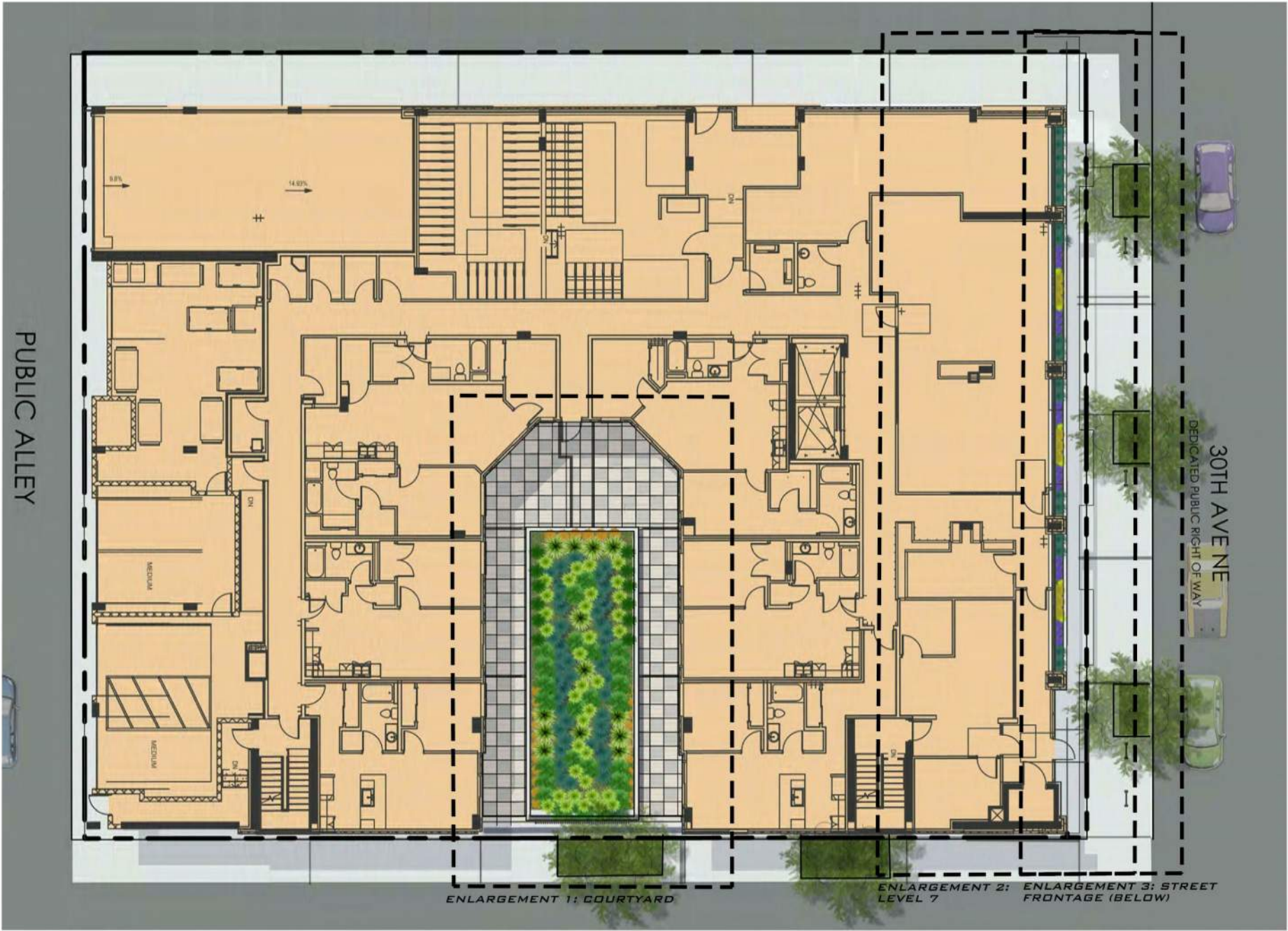
Building Section



EAST-WEST SECTION
1/16" = 1'-0"



Landscape



RED TWIG DOGWOOD



RUSHES



GOLDEN CAREX GRASS



3'10" BIORETENTION PLANTERS



SWORD FERN



HOLLY FERN



PENSTEMON



NEW ZEALAND SEDGE

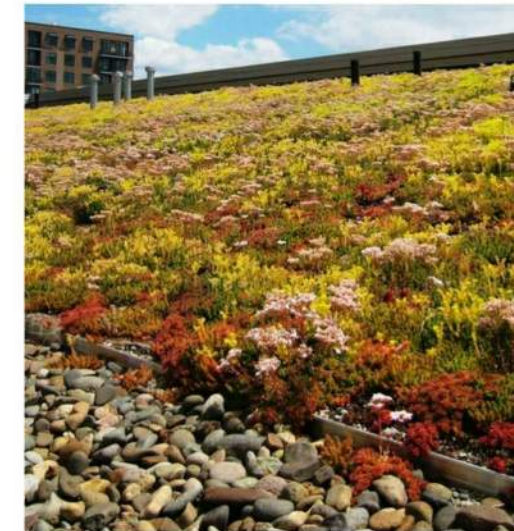


EVERGREEN CLEMATIS



SEDUM MIX

Landscape



EXTENSIVE GREEN ROOF



CASUAL SEATING, PEDESTAL PAVERS



FIBERGLASS PLANTERS



OUTDOOR GRILL/FOOD PREP AREA



FIRE TABLE



NEW ZEALAND SEDGE



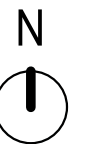
RED EDGE HEBE



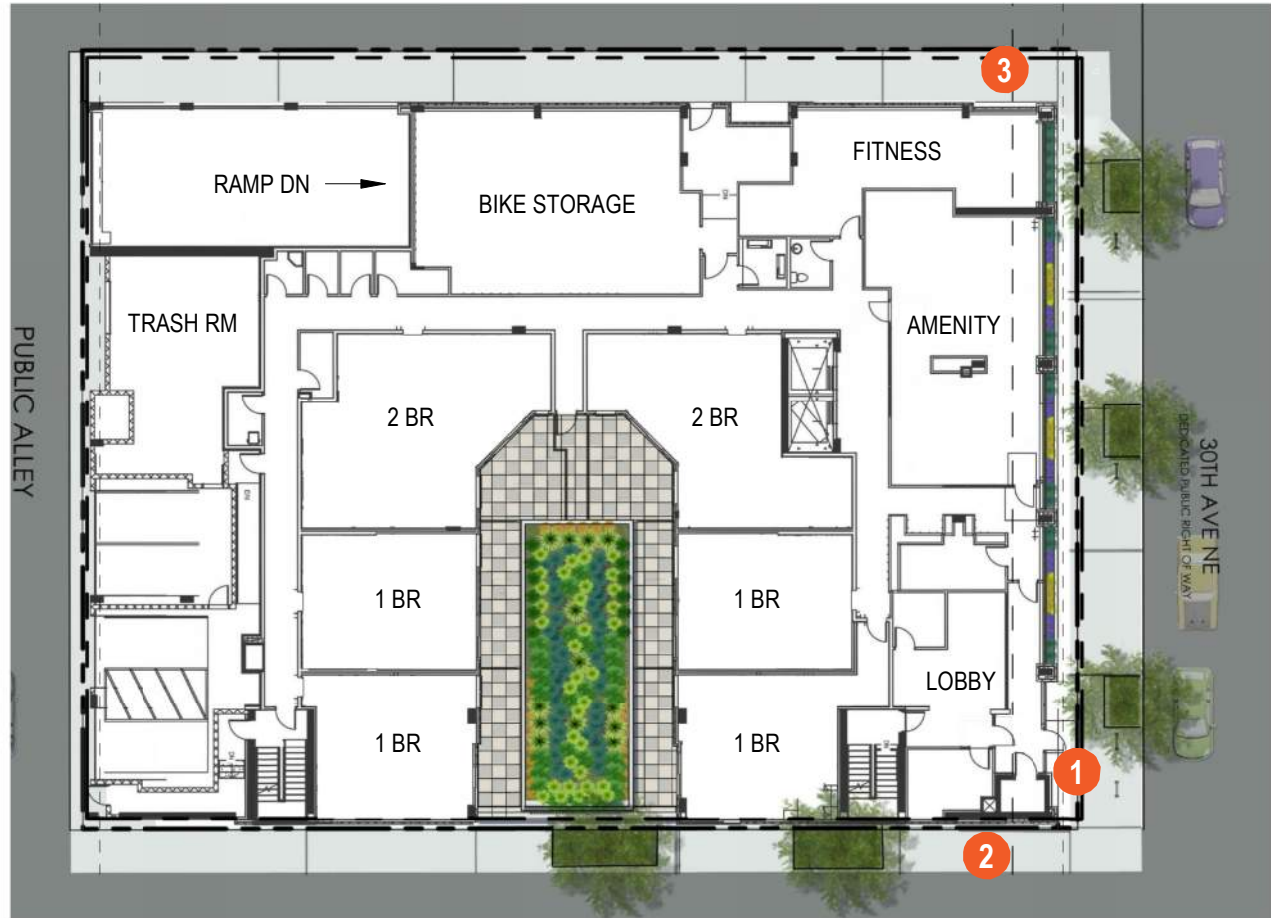
WIRE VINE



JAPANESE MAPLE TREE



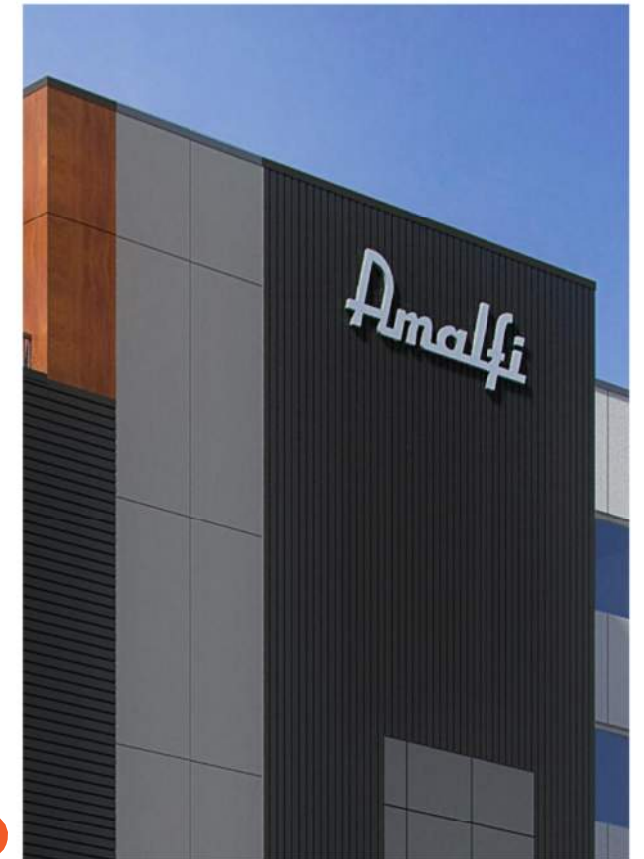
Signage



Signage Plan
1" = 30'-0"



2
MAIN BUILDING SIGN - SOUTH



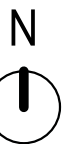
3
MAIN BUILDING SIGN - NORTH



1
MAIN ENTRY SIGN/GRAPHIC



BACKLIT SIGN EXAMPLE





THANK YOU