

Administrative Design Guidance Application



OWNER:
MRN Homes LLC
7556 12th Ave NE
Seattle, WA 98115

PROJECT:
NW 85th St Rowhouses
1302 NW 85th St
Seattle, WA 98117

ARCHITECT: Novion Group Inc.
8634B 3rd Ave NW
Seattle, WA 98117
rod@noviongroup.com (206.361.6133)

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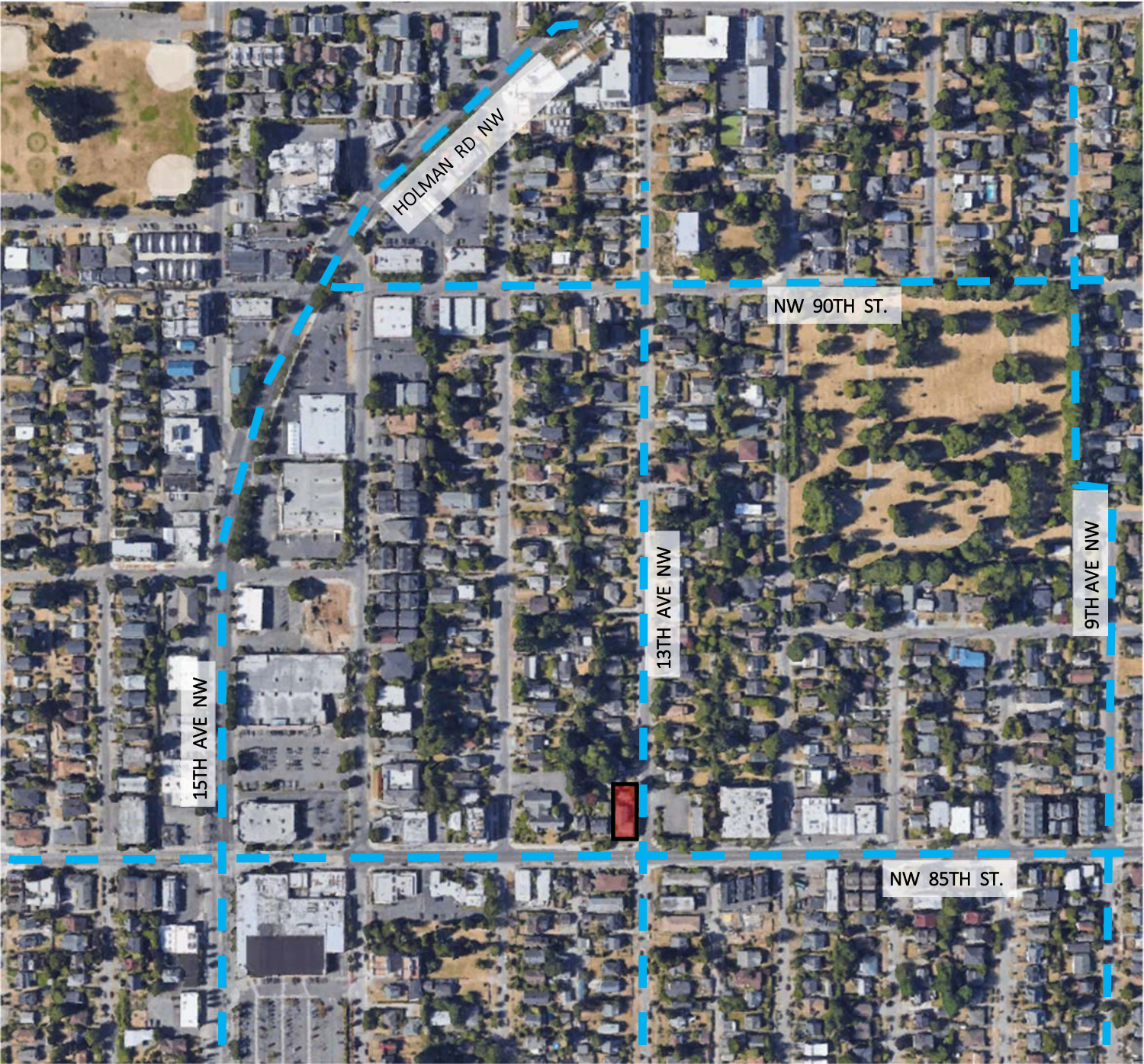
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DEVELOPMENT OBJECTIVES

PROJECT TEAM

Owner: MRN Homes LLC
7556 12th Ave NE
Seattle, WA 98115
Contact: Michael Nelson

Architect & Applicant: Novion Group Inc.
8634B 3rd Ave NW
Seattle, WA 98117
Contact: Shaun Novion

Landscape Architect: GHA Landscape Architects
1417 NE 80th St
Seattle, WA 98115
Contact: Neil Buchanan

SDCI Project #3033782-EG
Contact: Colin Vasquez

Existing Site:

Address: 1302 NW 85th St
Location: East of 15th Ave NW and West of 8th Ave NW.
Site Area: 5,760 sf
Existing Development: Residential Triplex

Project Proposal:

Number of Residential Units: 8 new rowhouse units
Number of Parking Stalls: 6 garage stalls & 2 surface parking stalls
Gross Floor Area of Residential Use: 9,402 sf

Developmental Objective:

To provide more multifamily housing in a neighborhood with a plan to increase the density of underutilized Lowrise lots. The proposal is to provide 8 new townhouse units in a location where the residents will benefit from the easy connection to the neighborhood amenities and public transportation, as well as have access to the many public sites that encourage community interaction.



PROJECT DESCRIPTION

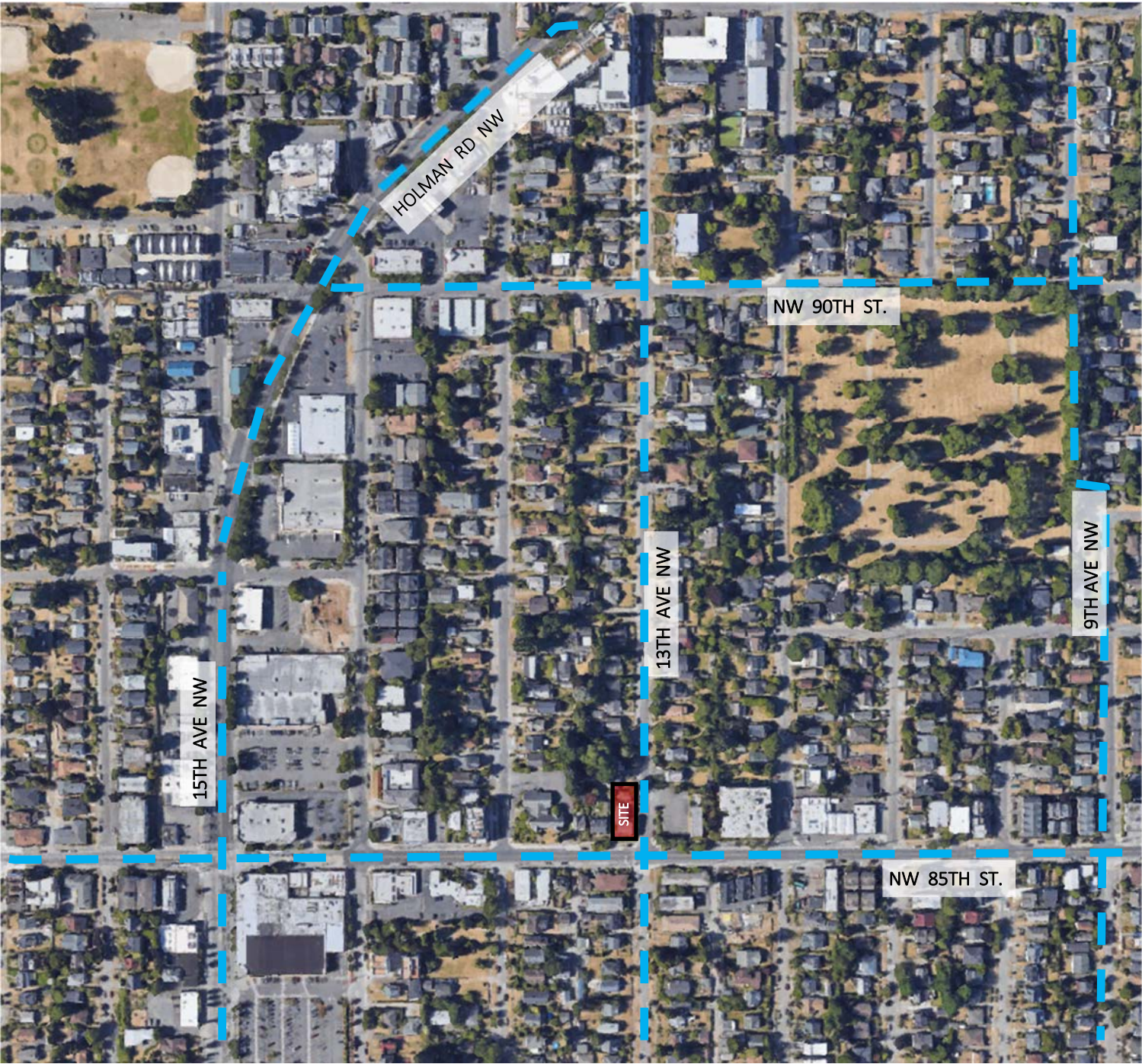
The subject site is currently a residential triplex structure on an LR3 RC zoned lot. It is located near the intersection of NW 85th St and 15th Ave NW. The site is part of the Crown Hill/Ballard Neighborhood Plan.

With a proposed expansion of the Crown Hill Residential Urban Village and an increased need for housing per desired density limits, this proposal aims to develop 8 new modern influenced townhouse units available for purchase, on a corner lot with frontage along NW 85th St and 13th Ave NW.

Site Information:

City: City of Seattle
Existing Zoning: LR3 RC
Site Area: 5,760 sf
APN Number: 2776600015

Comprehensive Plan Land Use: Crown Hill/Ballard Neighborhood Plan
Neighborhood Planning Area: Crown Hill—Whittier Heights
Urban Village: Crown Hill Residential Urban Village

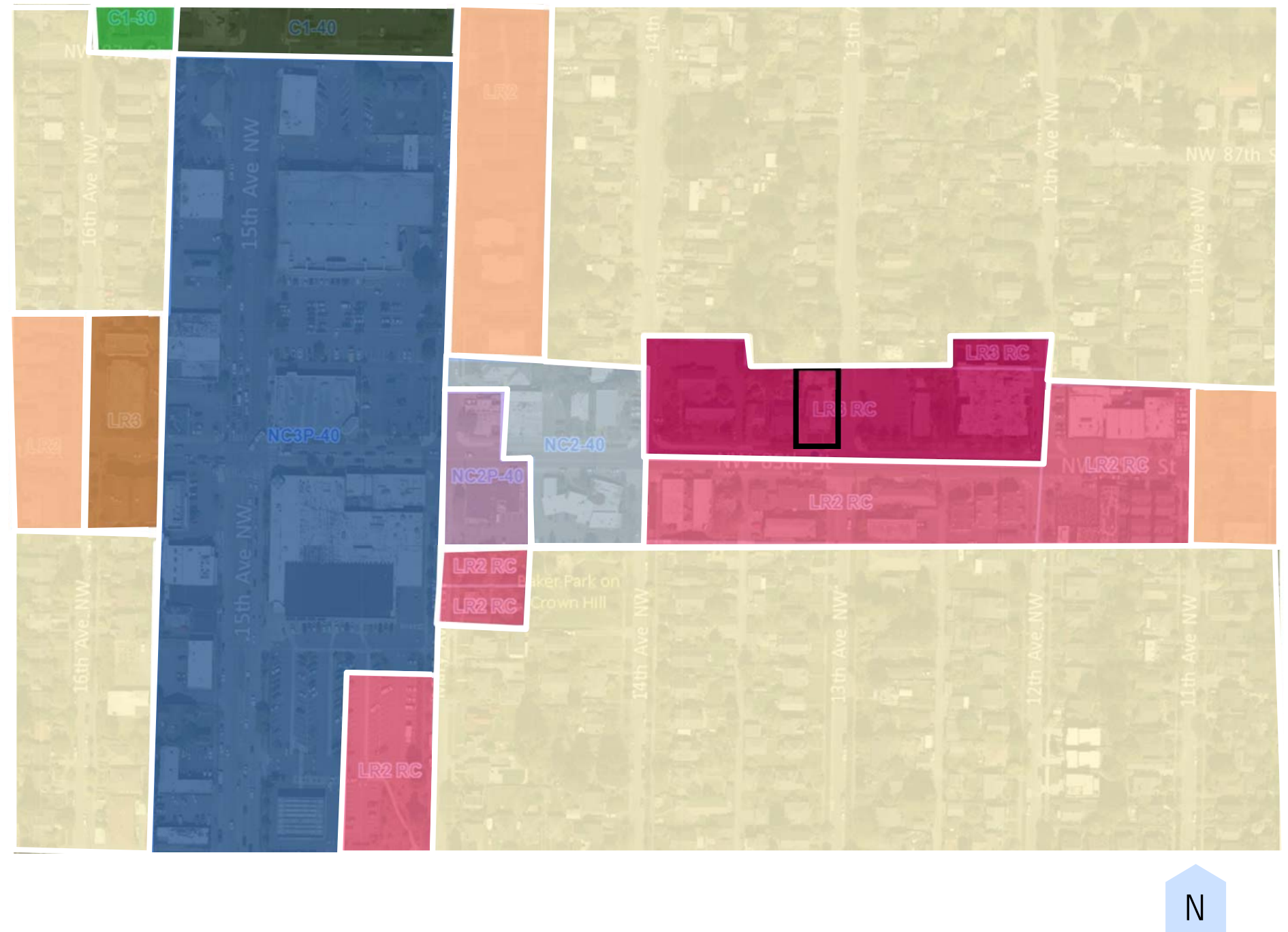
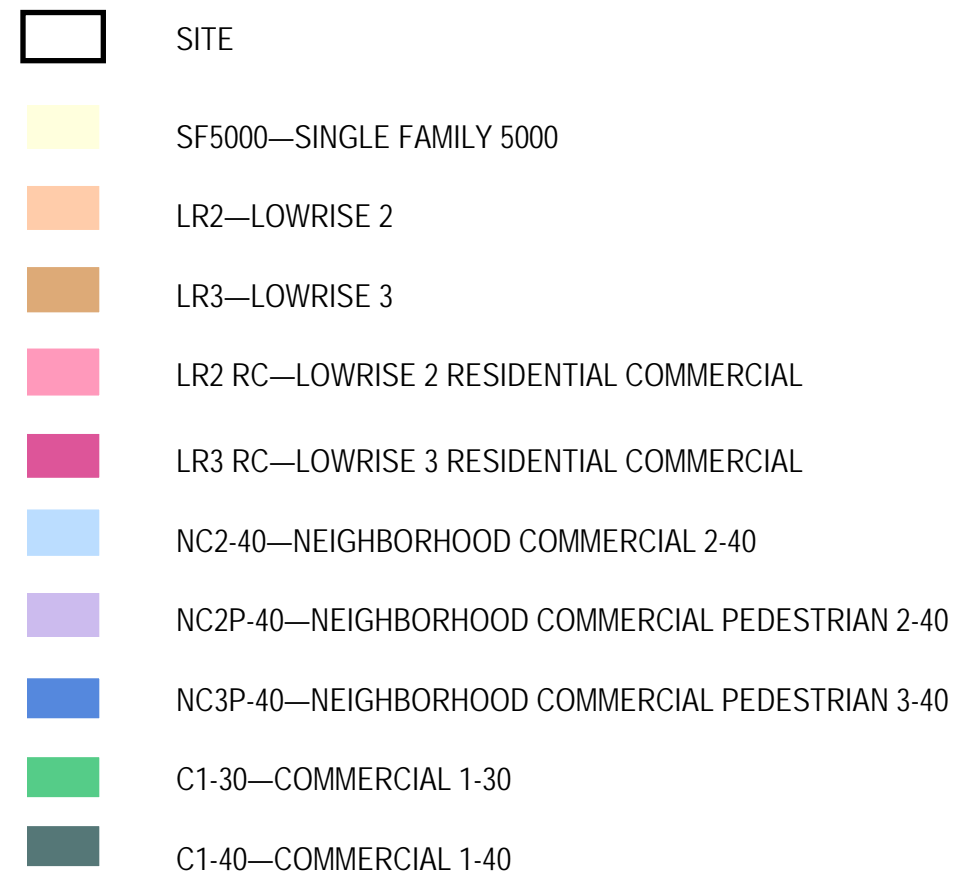


ZONING MAP

The area directly north of the subject site is zoned SF5000. Directly south, across NW 85th St, is LR2 RC; one block further south is more single family residential. To the west is predominantly commercial in nature with a pedestrian overlay at the intersection of NW 85th St and 15th Ave NW. To the east, along NW 85th St is a medley of Lowrise Residential Commercial, flanked by single family to the north and south of that arterial.

There is a planned effort to continue defining the neighborhood, through growth and a cohesive mix of commercial and residential uses. The goal is to create a walkable town center with increased connectivity through safe non-vehicular routes to parks, schools, transit and engaging public spaces that foster a sense of community.

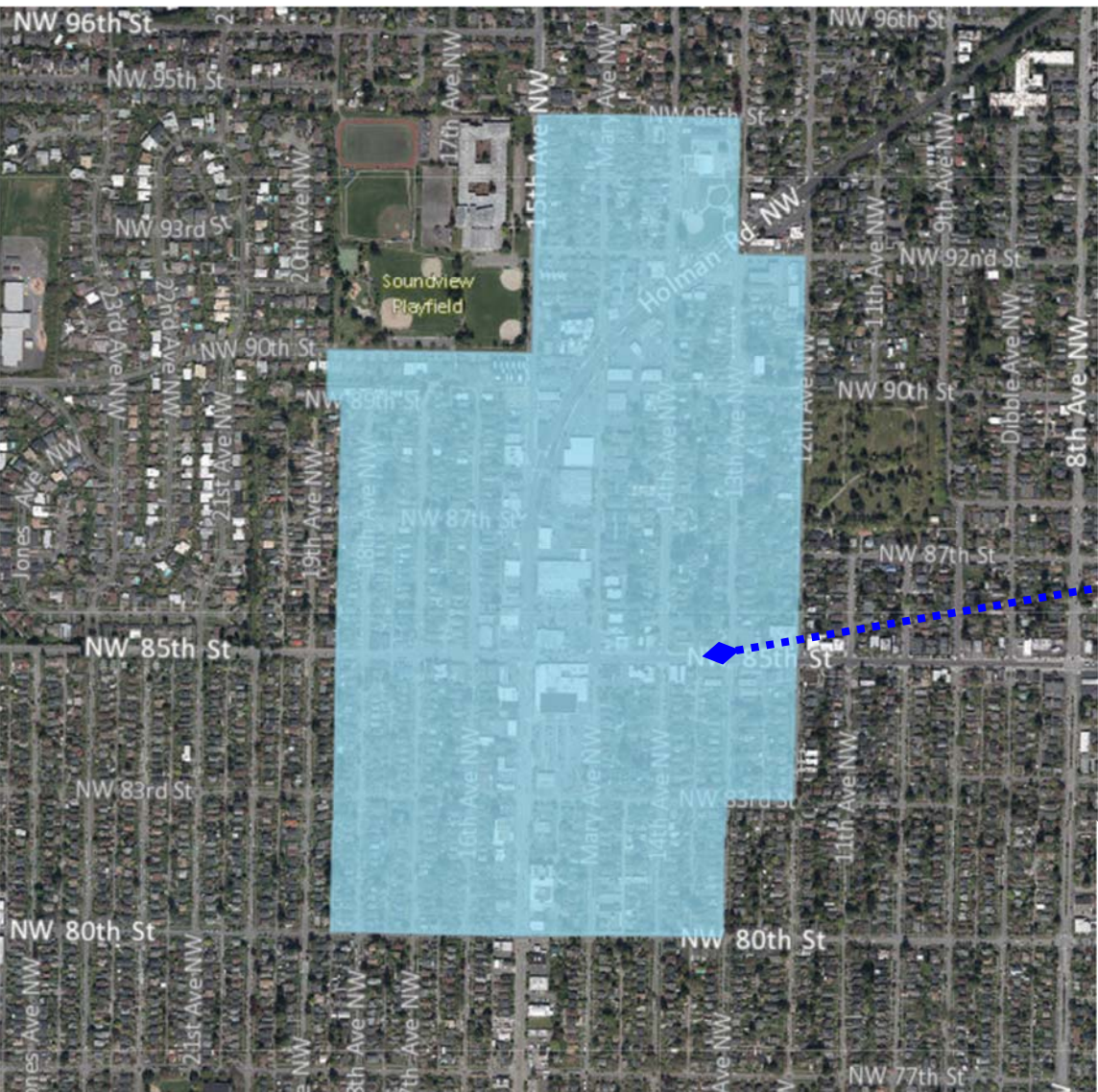
LEGEND: CURRENT ZONING



NEIGHBORHOOD ANALYSIS

CROWN HILL RESIDENTIAL URBAN VILLAGE

The Crown Hill Residential Urban Village is primarily centered around the intersection of NW 85th St and 15th Ave NW. It's current footprint extends North to NW 95th St, East to 11th Ave NW, South to NW 80th St, and West to 18th Ave NW. There are proposals to increase the Urban Village footprint, to incorporate parts of adjacent neighborhoods like the North portions of Loyal Heights & Whittier Heights, and the South portions of North Beach & Blueridge. 15th Ave NW is a major arterial where businesses large and small coexist, with engaging public spaces and streetscapes mixed in. In the Crown Hill Neighborhood Plan, most recently updated in 2013, the community identified the areas they wanted the OPCD to focus on. These include, creating a vibrant town center, increased pedestrian friendly routes to public spaces and an improved stormwater infrastructure. These elements with the plan to enlarge the Urban Village footprint, are laid out with the ambition to nurture a community orientated neighborhood.

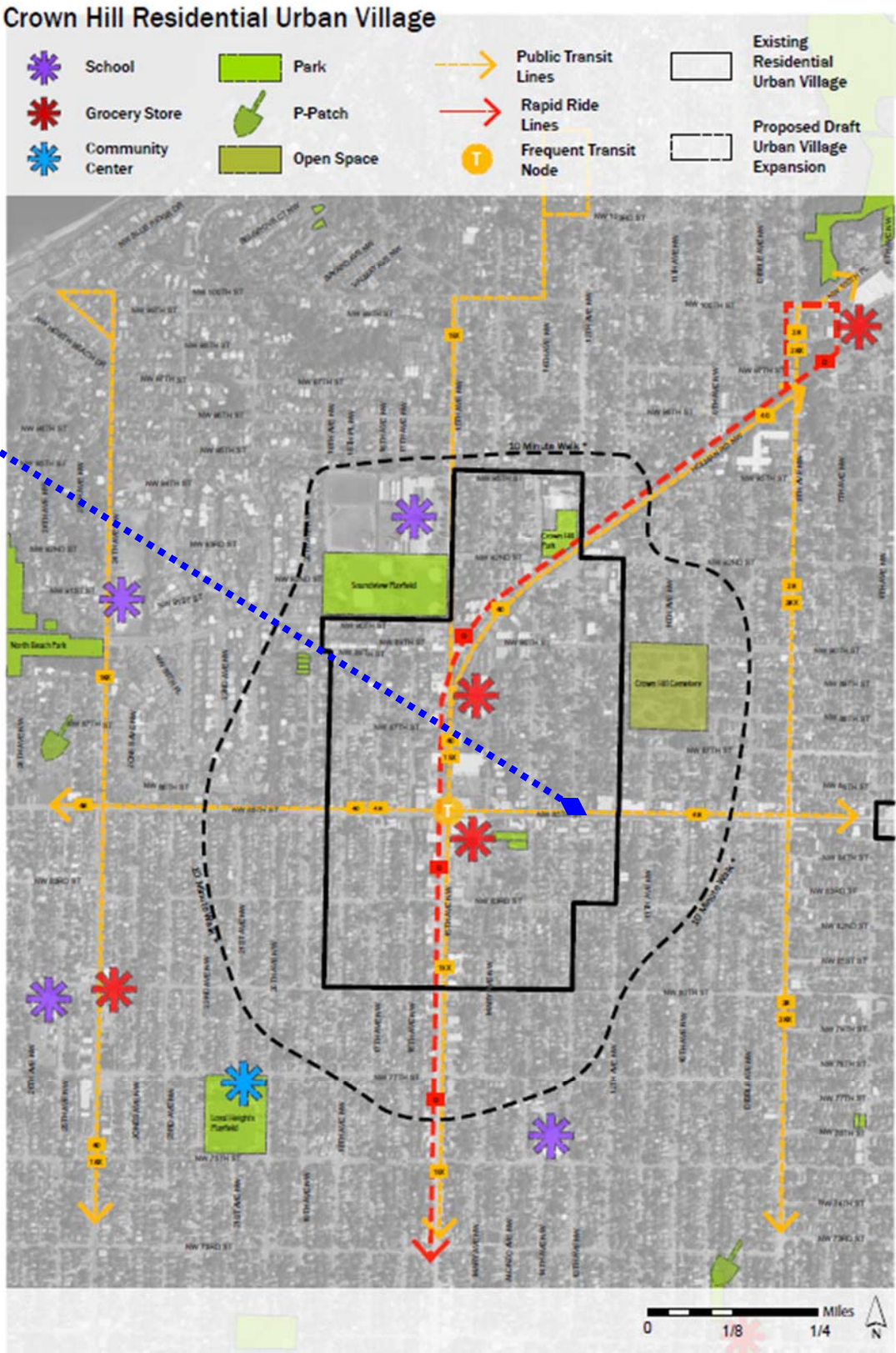


SUBJECT SITE

CROWN HILL RESIDENTIAL URBAN VILLAGE

SUBJECT SITE

CURRENT BORDERS & FUTURE PLANS FOR THE:



VICINITY MAP

TRANSIT

There are multiple Metro transit stops within blocks of the subject site. The routes that run north/south along 15th Ave NW & 8th Ave NW, connect Downtown (#15, 28 & D) to Crown Hill. And, the routes that run east/west along NW 85th St connect Northgate (#9) and the University District (#45) with Crown Hill.









CYCLING

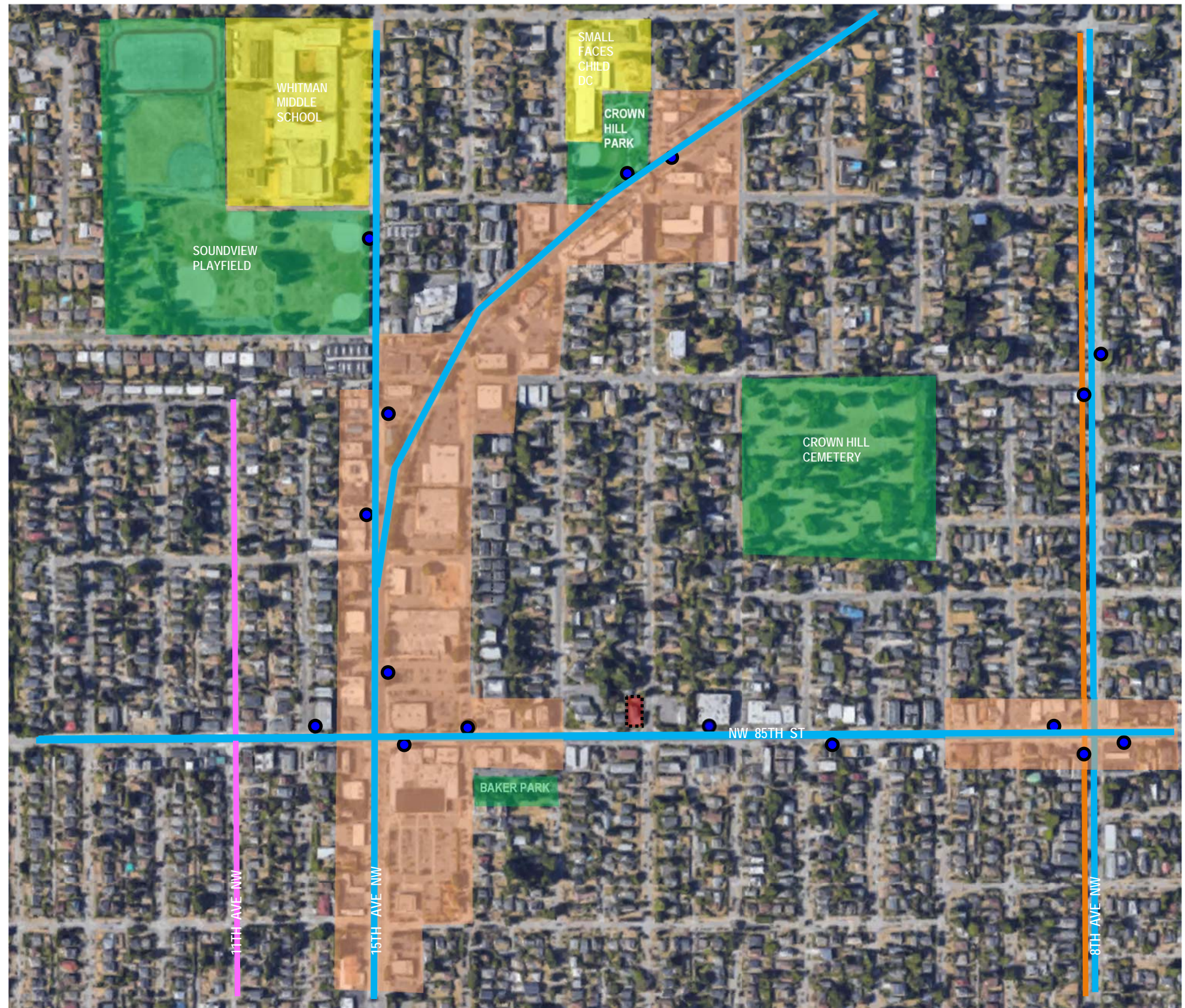
There is a bike lane/climbing lane on 8th Ave NW. At NW 85th St it changes to a protected bike lane. There is a bike friendly neighborhood greenway along 11th Ave NW, south of NW 90th St. The Master Bike program has plans under construction on along NW 90th St.

RECREATION

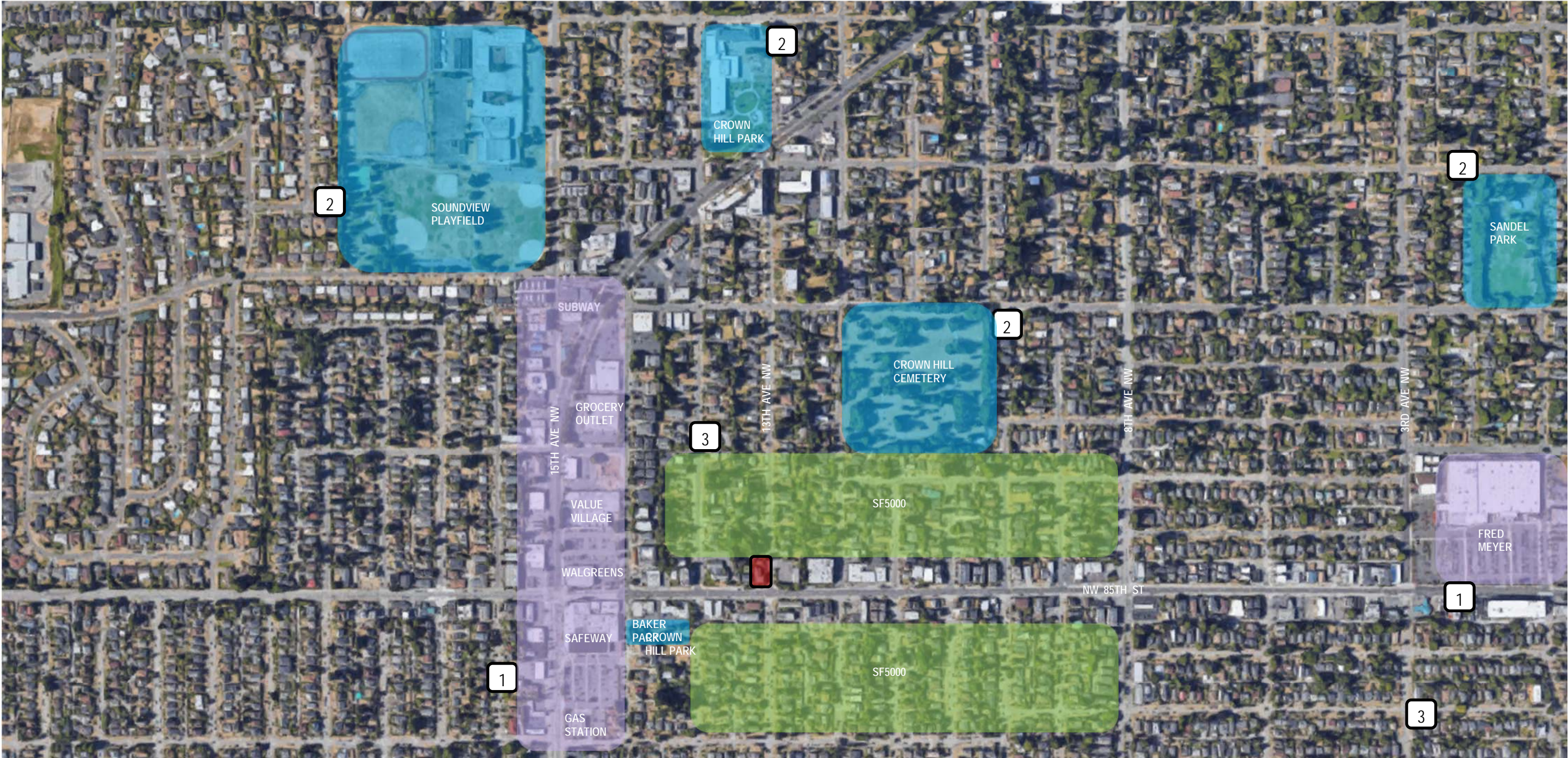
The subject site is conveniently located near, and a short travel distance to many retail stores and public recreational amenities. These include grocery stores, restaurants, banks, pharmacies, parks, playfields, and pedestrian friendly streets. The opportunity for future residents to participate and engage with the community will be easily achieved with an already established presence of nearby amenities

LEGEND

-  SITE
-  METRO TRANSIT STOPS
-  METRO TRANSIT ROUTES
-  BIKE LANE
-  NEIGHBORHOOD GREENWAY
-  PARKS & PUBLIC SPACES
-  RETAIL
-  SCHOOLS



EXISTING BUILDINGS IN IMMEDIATE CONTEXT



1—COMMERCIAL

The majority of the buildings to the West of the subject site, along 15th Ave NW are commercial in nature. The zones are a mix of NC and NCP. There are numerous services located a short walk from the subject site. Ample parking at these locations provides more street parking for neighborhood residents

2—COMMUNITY BUILDINGS & SPACES

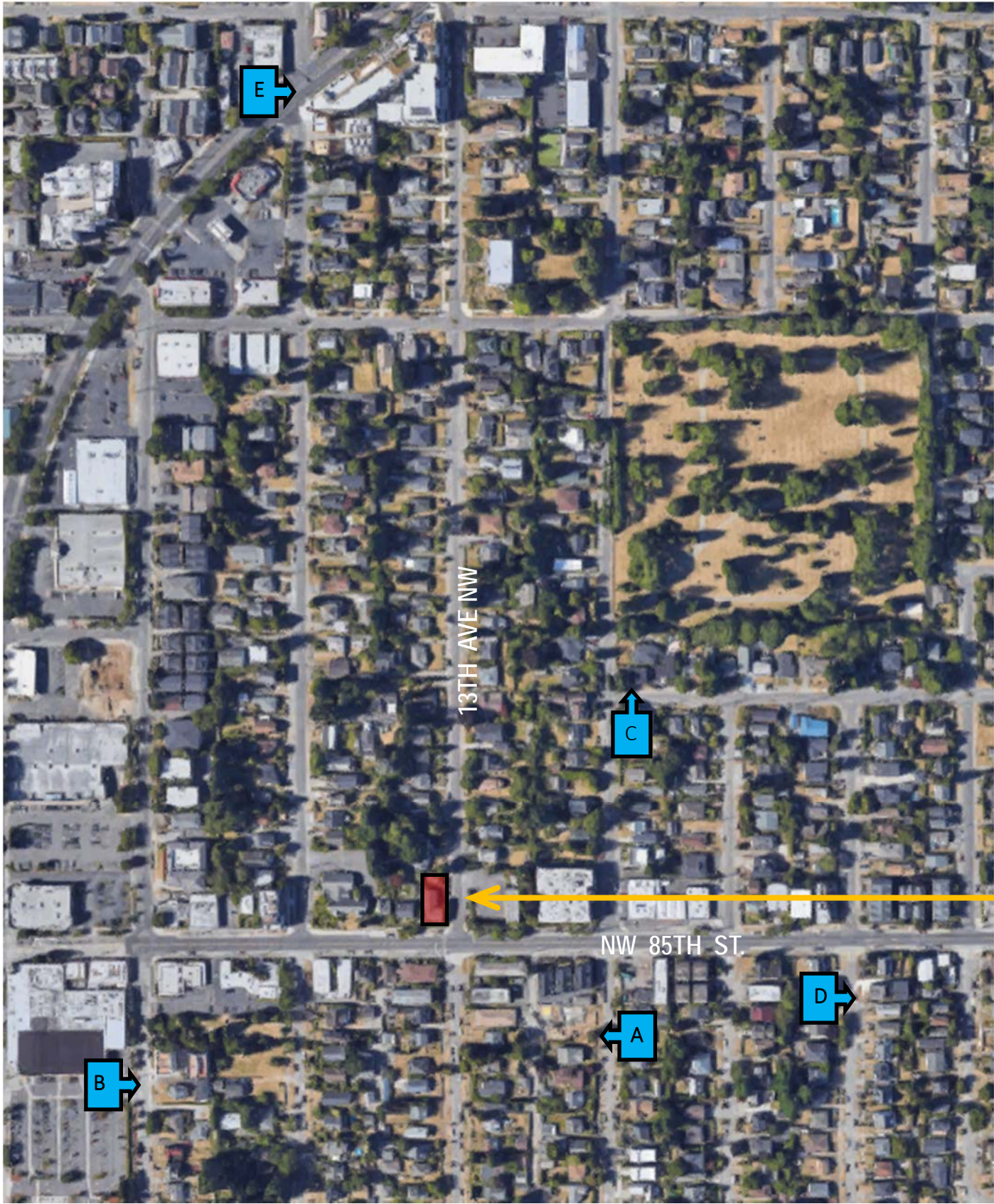
There is a great deal of public facilities within walking distance of the subject site. To the North and South there are public schools, play fields, green spaces, and pedestrian friendly greenways. A far reaching network of transit routes allows Crown Hill residents easy access to other public facilities in surrounding neighborhoods

3—RESIDENTIAL

Directly adjacent the subject site to the North is SF5000 zone and predominantly single family homes. South of NW 85th St is more SF5000 zoning. Along NW 85th St, East of 15th Ave NW is a mix of Lowrise Residential Commercial with a healthy share of multifamily structures.



EXISTING NEIGHBORHOOD STYLE AND CONTEXT



A—8347 12TH AVE NW



B—8342 MARY AVE NW



C—8700 12TH AVE NW



D—8354 10TH AVE NW



E—9176 HOLMAN RD NW

EXISTING NEIGHBORHOOD STYLE AND CONTEXT



F—8354 17TH AVE NW



G—1590 NW 90TH ST (NOVION GROUP)



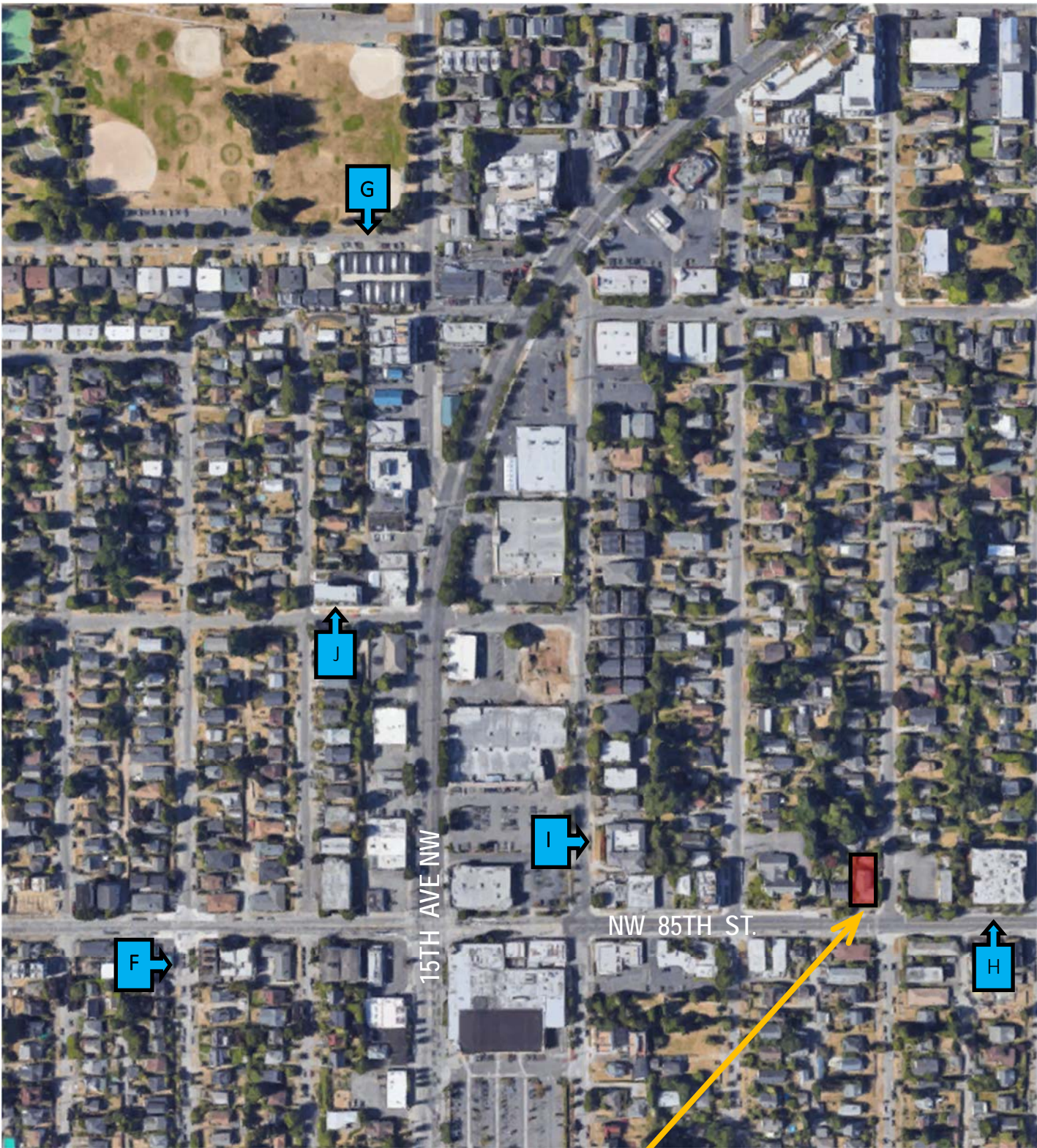
H—1209 NW 85TH ST



I—8514 MARY AVE NW



J—1598 NW 87TH ST



SUBJECT SITE

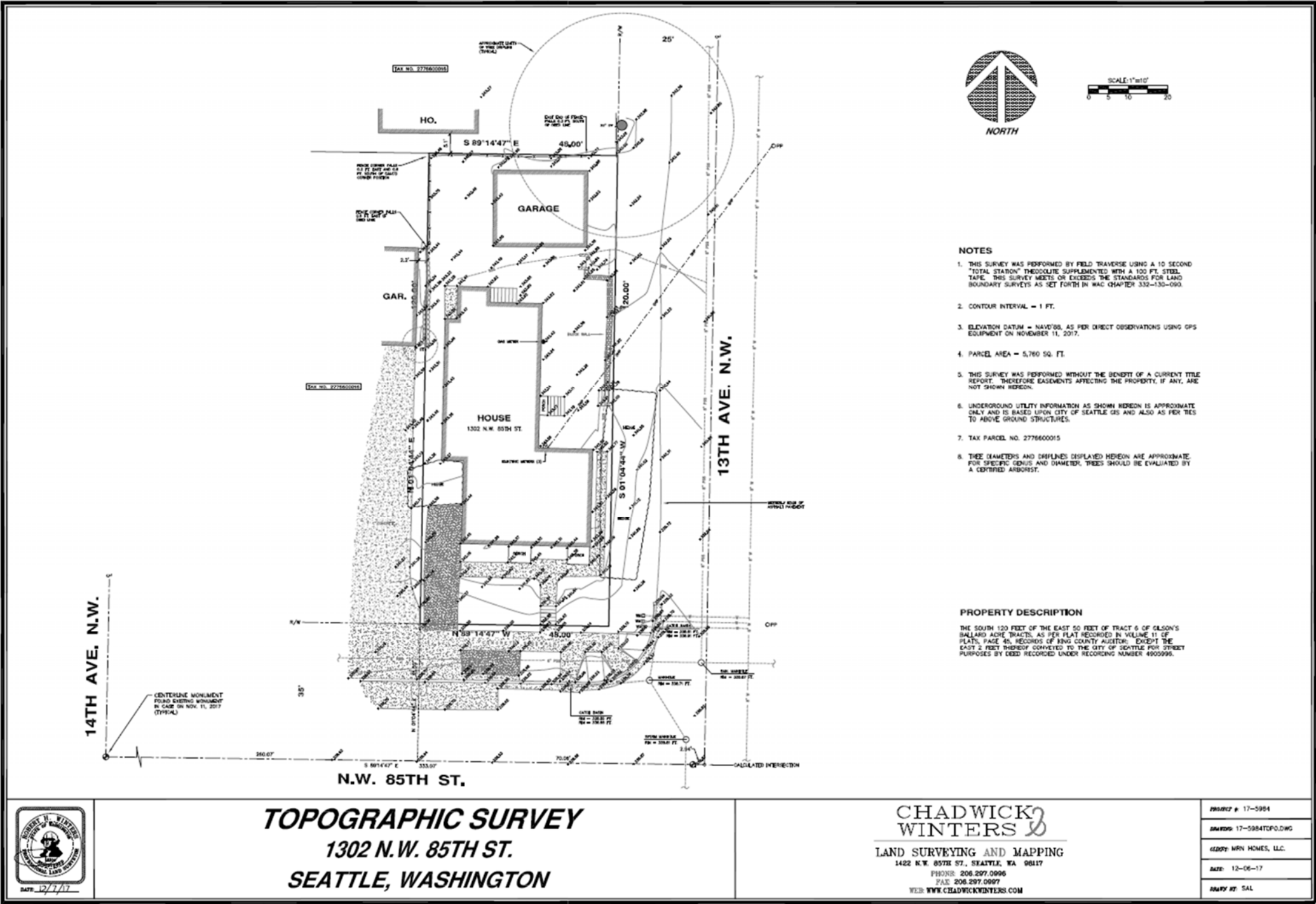
SITE SURVEY

The subject site is a mostly flat, rectangular shaped parcel with approximately 10% slope on the southern quarter of the lot, climbing up from the ROW. The site is a corner lot, fronting both NW 85th St and 13th Ave NW, three blocks East of the major arterial 15th Ave NW, and located within the limits of the Crown Hill Residential Urban Village

- Existing triplex structure centrally located on subject lot to be removed
- Existing hardscapes to be removed and replaced
- Gradually sloping lot on southern quarter, towards the ROW
- Existing curbs & sidewalks on NW 85th St
- SIP for new curb, sidewalk and planting strip along 13th Ave NW in process

Legal Description:

The South 120 feet of the East 50 feet of Tract 6 of Gilson's Ballard Acre Tracts as per plat recorded in Volume 11 of Plats, Page 45, Records of King County Auditor; Except the East 2 feet thereof conveyed to the City of Seattle for street purposes by deed recorded under recording number 4905996.

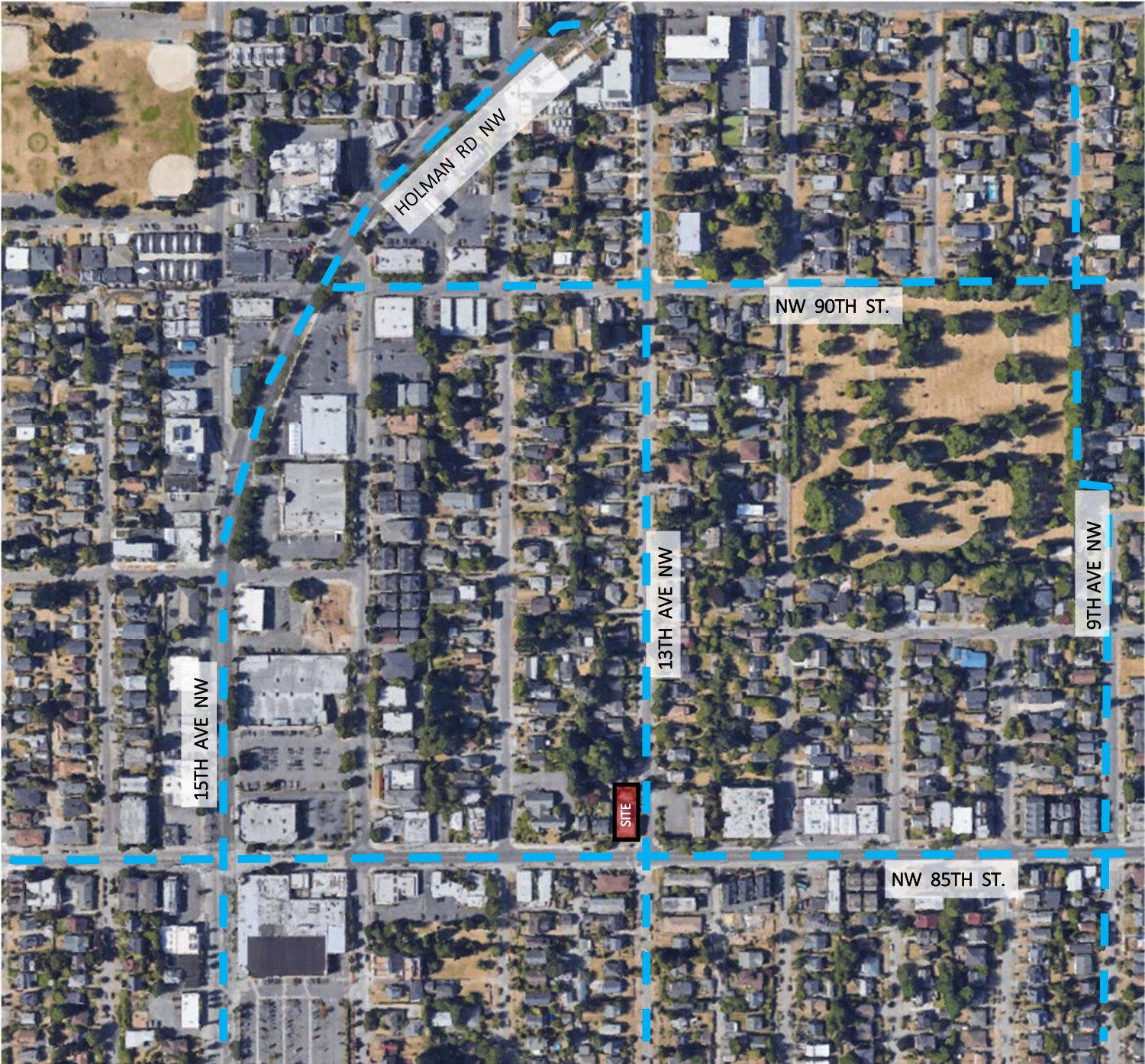


SITE ANALYSIS

The proposed project will be developed on what is presently being used as a triplex, on the corner of NW 85th St and 13th Ave NW, in the Crown Hill Residential Urban Village. Directly East across 13th Ave NW is the Finnish Lutheran Church. South, across NW 85th St is a mid-century triplex. To the West, the lot is presently being used as a single family residence, although the zoning would allow for more residential units. To the North is SF5000 zone, and comprised of a mix of early and mid-century structures. Along NW 85th St is a mix of commercial, multi-family and single family uses.

The subject lot does not presently consist of any notable site features to be retained. The existing foliage will be removed and replaced with new native plants and trees. A new curb, sidewalk and planting strip are being proposed on 13th Ave NW, fronting the subject site. The most notable feature of the subject site would be it's location in the Crown Hill Residential Urban Village. It is conveniently located on a major arterial, with frequent transit stops a short distance away. The site sits about 4' higher than the ROW on NW 85th St. The proposed roof decks should provide an expansive territorial view of the neighborhood.

The subject site is on NW 85th St, three blocks East of 15th Ave NW, both major arterials. There is frequent transit service on both streets that connect Crown Hill with neighborhoods all over Seattle. They also provide connections to many amenities in closer proximity.



13TH AVE NW ELEVATIONS—LOOKING WEST



Project Site (Looking West)



13TH AVE NW ELEVATIONS—LOOKING EAST

Across 13th Ave NW from proposed project site



Looking East from proposed project site



ZONING SYNOPSIS

SITE OVERVIEW	
Zoning	LR3 RC — The surrounding lots are a mixture of residential and commercial, with a high emphasis of pedestrian friendliness. There are a large number of new multi-family developments, with a steady stream of new construction being proposed.
Overlay	Crown Hill Residential Urban Village
Lot Area	5,760 sf
COMPLIANCE SUMMARY	
Requirement	Permitted Uses — SMC 23.45.504 Table A
	Uses permitted include Residential, Live-Work, limited Commercial, Institutions, Uses in existing or former schools, Parks and Playgrounds
Provided	Residential rowhouses with attached garages are proposed.
Requirement	FAR (Floor Area Ratio) — SMC 23.45.510 Table A
	The maximum permitted FAR for rowhouses is 1.2 or 1.4 (Project must comply w/ SMC 23.45.510.C for higher FAR)
Provided	The proposed FAR for the preferred option is 1.4
Requirement	Density — SMC 23.45.512 Table A
	There is no density limit for rowhouses.
Provided	The proposed density for the preferred option is 8 new rowhouse units
Requirement	Structure Height — SMC 23.45.514 Table A
	The maximum permitted height limit for rowhouses is 30' from Avg. grade (plus an additional 10' for stair penthouses)
Provided	The proposed height limit is 30' to the roof, 34' to parapet, from FG.

COMPLIANCE SUMMARY	
Requirement	Setbacks — SMC 23.45.518 Table A
	The Required setbacks for rowhouses: Front— 5' Minimum Rear— 7' Average, 5' Minimum Side— 3.5' Min. (5' min. from SF), for facades less than 40' 3.5' Min. (7' min. from SF), for facades greater than 40'
Provided	All options meet or exceeds all the required setbacks
Requirement	Amenity Area — SMC 23.45.522
	The required amount of amenity area for Lowrise zoned lots is 25% of the lot area, with 50% of it located at the ground level. Amenity area can be public or private (Min. area dimensions required in certain setbacks)
Provided	All options propose ground level amenity areas as well as private roof top decks, meeting the required area and dimensions.
Requirement	Landscaping Standards — SMC 23.45.524
	The required amount of landscaping shall meet a Green Factor of .6
Provided	The preferred option proposes a Green Factor of .606 and uses new trees, new ground cover and shrubs, and permeable pavement surfaces
Requirement	Structure Width & Façade Length — SMC 23.45.527
	There are is no maximum structure width for rowhouses The maximum façade length for portions of structures within 15' of a side lot line is 65% of the length of the lot line
Provided	All options meet the width and length requirements.
Requirement	Design Standards — SMC 23.45.529
	The required street facing façade standards include at least 20% of the street facing façade be transparent glass, and façade articulation is to be applied to certain sized facades. For rowhouses, additional standards apply including wayfinding, landscaping and architectural expression.
Provided	All options meet the required design standards for rowhouses.

ZONING SYNOPSIS

COMPLIANCE SUMMARY	
Requirement	Required Parking — SMC 23.54.015 Table B & D
	No vehicular parking is required because the subject site is located in an Urban Village and a Frequent Transit Service area. The required amount of long-term bicycle parking is 1 stall/unit, and The required amount of short-term bicycle parking is 1 stall/20 units
Provided	The preferred option proposes 6 garage stalls & 2 surface parking stalls located behind all structures at the back of the lot, per SMC 23.45.510.C; the required bicycle parking is proposed in the proposed garages and at the back of the lot.
Requirement	Solid Waste & Recycle — SMC 23.54.040
	Storage requirements for 8 residential units is either individual 2'x6' solid waste storage areas or 84sf of shared storage area
Provided	The preferred option is proposing individual 2'x6' solid waste storage areas, locate in the proposed garages and at the back of the lot, where they will be screened from street view.

DESIGN GUIDANCE RESPONSES

CS1 Natural Systems & Site Features	
B) Sunlight & Natural Ventilation	The subject site is located on the corner of NW 85th St and 13th Ave NW. The proposed building will benefit from the absence of structures adjacent its East and South facing facades. The preferred option's interior layouts will take advantage of Easterly and Westerly facing windows, allowing for natural sunlight and ventilation pathways. The proposed building footprint is allowing for an 11' setback from the single family lot line, to the north, and a 14' setback from the multifamily lot line, to the West. These setbacks will help reduce the amount of shade cast by the proposed building structure, as well as protect the subject sites access to natural light from potential future developments on these adjacent lots.
CS2 Urban Pattern & Form	
A) Location in the City and Neighborhood –Architectural Presence B) Adjacent Sites, Streets and Open Spaces	The preferred option proposes a similar building footprint and connection to the street as many of the new developments on corner lots located on the north side of NW 85th St do. The proposed rowhouse will have all the unit entries facing 13th Ave NW. The building exterior design will be a mix of color and material that matches the existing and changing style of Crown Hill. The existing mid-century single family structures that occupy most of the lots north and south of the subject site, will contribute inspiration for color palettes and materials, complimentary to the visual flow of the neighborhood. The proposal will also draw from the new modern designed developments in the neighborhood, incorporating architectural features and increased fenestration. Street improvements along 13th Ave NW will add landscaping and safe pedestrian pathways. Landscaping onsite will also reduce the bulkiness of the proposed structure. The neighborhood plan is to increase the Urban center feel with more residential units, and safe connectivity for these new developments
CS2 Height, Bulk & Scale Compatibility	
D) Height, Bulk, and Scale —Zone Transitions —Zone edges	The subject site is located on the corner of NW 85th St and 13th Ave NW. The zoning is the same to the West and East. It transitions at the north property line to SF5000. In an effort to reduce the effects of the zone transition, the preferred option proposes a driveway on the north side of the lot, increasing the required setback to 11.5'. This setback will aid in reducing the shadow cast by the new building, on the neighboring property to the North. The preferred option aims to propose exterior colors and materials that will flow with the existing structures. There are existing trees and landscaping buffering the existing single family structure to the north from the proposed development, which will help limit the contrast in structures. The preferred option is not seeking any departures, and will build to the allowable height set out by the zoning.

CS2 Street Pattern & Structure Orientation	
Continuity & Orientation	The subject site is located on the corner of major arterial, with a zone change one block north. The preferred option is to maximize the number of allowable units on site, but also maintain a residential feel of the proposed structure. The site orientation and configuration are similar to many new multi-family developments along NW 85th St. Orientating the proposed units entries towards 13th Ave NW increases the residential feel and access, similar to the single family structures further north on 13th Ave NW. The neighborhood plan is to increase the urban center feel and connectivity of the residential units to the community spaces. The preferred option considers the residential appeal and changing characteristics of the Crown Hill neighborhood.
CS3 Architectural Context & Character	
A) Emphasizing Positive Neighborhood Attributes —Established Neighborhood —Compatibility - Existing Patterns	The subject site is located in the established Crown Hill neighborhood. There are a large mix of residential houses and commercial properties, especially along NW 85th St. The existing construction is predominantly mid-century designs; 1 to 2 story single family structures with a wide range of color palettes, materials and rood construction. A lot of existing properties utilize brick or stone in their facades. This proposal will draw from these existing features when designing the exterior of the new development. Some existing, and most new, multi-family structures will provide inspiration through modern designs and flat roof compositions. The neighborhood plan encourages additional residential units, where allowed, and providing a connection for those units to the urban amenities close by.
PL1 Open Space Connectivity	
B) Walkways and Connections	The preferred option proposes all unit entries face 13th Ave NW. There will be individual pedestrian pathways, surrounded by year round landscaping, leading to each unit. The entry paths and the entries themselves will be very identifiable, properly illuminated and provide addressing signage, for ease of wayfinding. The street improvements proposed for 13th Ave NW will provide new sidewalks and landscaping parallel to the East property line. This will divert pedestrian traffic away from the major arterial, NW 85th St, and contribute to a residential feel.

DESIGN GUIDANCE RESPONSES

PL2 Walkability	
A) Accessibility B) Safety and Security C) Wayfinding	The preferred option proposes all entries face 13th Ave NW. The individual units will have illuminated pedestrian pathways that lead directly to the entries, from the new sidewalk along 13th. Unit street addresses will be predominantly displayed adjacent the entry door, and illuminated at night, for easy wayfinding. The majority of the windows will also face 13th Ave NW which will provide additional forms of security for the residents, with increased views of street activity. The proposed garages will provide additional security for vehicular and bicycle parking. There will be windows proposed facing West, over the driveway, providing additional visual security of the site
PL3 Street Level Interaction	
A) Entries C) Residential Edges	The preferred option proposes all the unit entries face 13th Ave NW, away from the main arterial of NW 85th St. The entries will be clearly identifiable with street address signage and illuminated pathways and entry porches. The proposal raises the entries above the ROW, contributing to the residential edge transition from street to individual unit. There will also be landscaping proposed between the units and ROW that will promote a buffer for the residents from pedestrian traffic on 13th Ave NW. Although conformity of a rowhouse design will be utilized, the individual entries will be designed with security and wayfinding as a top consideration.
PL4 Active Transit	
A) Entry Locations and Relationships B) Planning Ahead for Bicyclists C) Planning Ahead for Transit	The preferred option proposes pedestrian entries and vehicular access of 13th Ave NW, a non-arterial street. With street improvement plans in place for a new sidewalk on 13th Ave NW, pedestrian access to the new units will benefit from the safety and security of an improved designated walkway. The proposal has 6 garages, which will allow storage of long-term bicycle parking, beneficial to the individual units. There is additional areas in the drive court that will provide temporary bicycle parking. The subject site is conveniently located on a major arterial with multiple Metro transit stops and routes. A short walk West to 15th Ave NW also increases the accessibility of public transit options. There are dedicated bike lanes in the area, as well as an established network of greenways and sidewalks, optimal for pedestrians and cyclists.

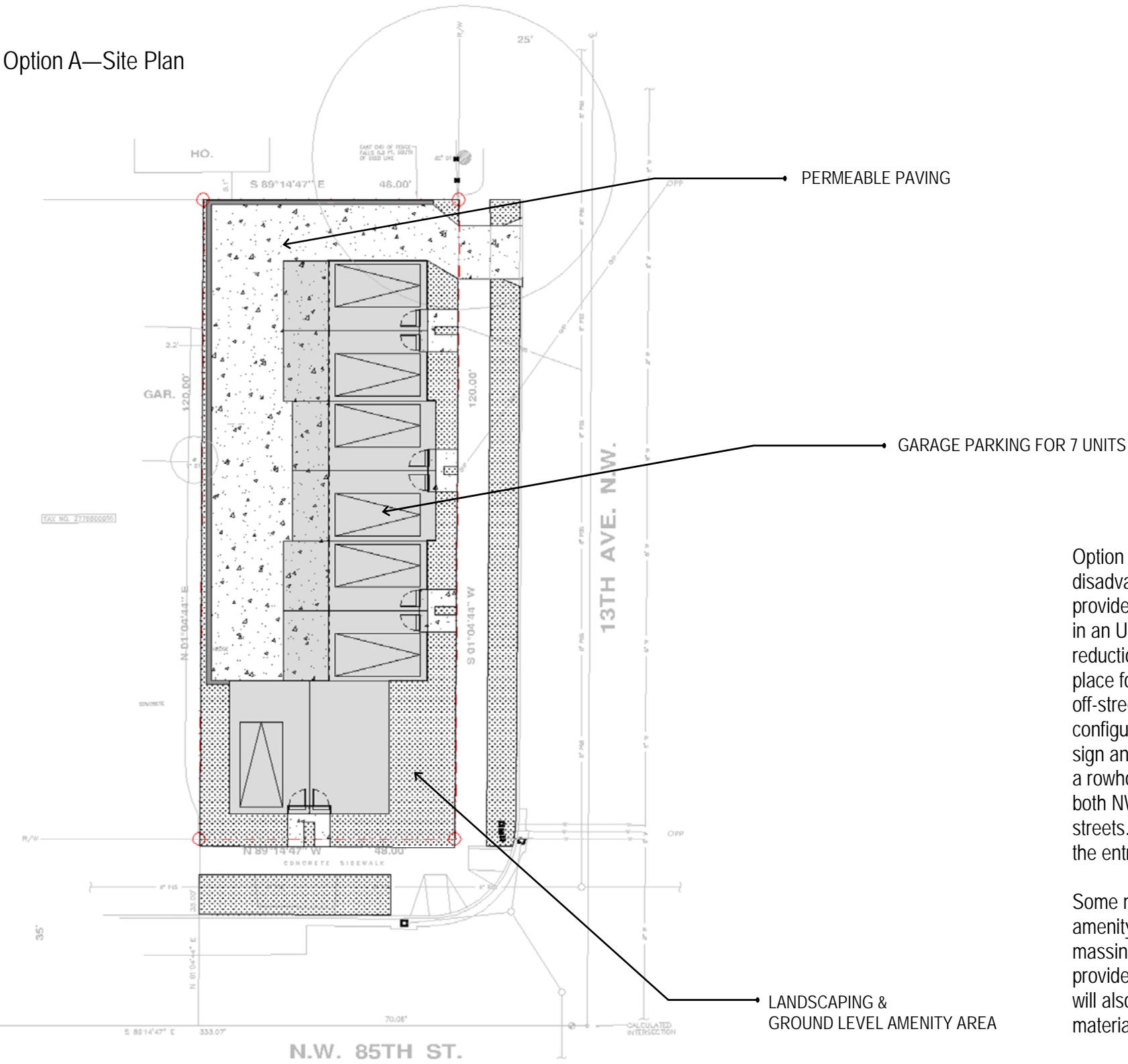
DC1 Project Uses and Activities	
A) Arrangement of Interior Uses B) Vehicular Access and Circulation C) Parking and Service Uses	The preferred option proposes the unit entries face 13th Ave NW. Although these are being proposed as residential units, there is potential to convert the lower level into commercial space, creating Live-work units, if the neighborhood continues it's plan towards a more urban center density layout. Vehicular access to the lot will be off 13th Ave NW as well. The proposal allows for maneuvering and turnaround on site so vehicles won't have to back-up into the ROW. The vehicular and bicycle parking is proposed in a basement, away from street view. This location will lessen the impact of hardscape caused by the driveway and parking surfaces.
DC2 Architectural Concept	
A) Massing B) Architectural and Façade Composition C) Secondary Architectural Features D) Scale and Texture E) Form and Function	The preferred option proposes a typically conforming rowhouse layout. In an attempt to reduce the perceived massing, the building is setback from the North and West lot lines, to reduce the affects on the neighboring properties. Architectural features, a transition in exterior finishes and colors will also help reduce the bulkiness of the proposed structure. The proposed design will consider the street facing facades composition so as to reduce blank exteriors and increase a visually appealing structure from the ROW. The proposal will utilize similar exterior finishes and colors as the existing structures nearby. As the proposed design is boxy in nature, architectural features will be utilized to aid in façade articulation. The unit entries are proposed to be recessed from the upper floors to create transitions in building facades and provide weather protection for residents. A range of finishes from Hardie board, repurposed wood, and stone, will all draw inspiration from surrounding properties.
DC3 Open Space Concept	
C) Design	There is a street improvement plan in place to add curbs, sidewalks and a planting area along the 13th Ave NW, fronting the subject lot. This improvement will add landscaping with ground cover, shrubs and new street trees. The preferred option provides small individual garden spaces near the entries, along 13th Ave NW. It also proposes a larger green space in the Southwest corner to provide additional landscaping, within eyesight of passers by on NW 85th St. The proposed development will utilize roof decks as the main location for individual amenity areas.

DESIGN GUIDANCE RESPONSES

DC4 Exterior Elements and Materials	
A) Exterior Elements and Finishes C) Lighting D) Trees, Landscape and Hardscape Materials	The preferred option proposes exterior finishes that will be installed to maintain durability throughout Seattle's weather cycles. The colors and materials chosen will be modern in nature, but attempt to match the existing color palettes and textures of surrounding structures. Alternate colored pavers will be used for the vehicular and pedestrian access, not only for their distinction and drainage purposes, but as well as a play off the extensive use of brick in surrounding structures. Adequate signage for wayfinding will be visually prominent. Illumination for pathways, entries, landscaping and security will be installed to aid residents as well as reduce glare on adjacent parcels. New landscaping is proposed on all areas not considered hardscape. Trees, green screens, bio-planters, shrubs and ground cover are proposed and designed to provide annual vegetation. Additional landscaping from the street improvements will help reduce the visual appearance of the new hardscapes.
Site Reconnaissance	
ROW Improvements Existing Trees Existing Structures Parking Space & Access Standard	There is currently a street improvement plan in the works for 13th Ave NW, adjacent the subject site. The improvements include curbs, sidewalks, and a new planting area. An arborist report will be provided for all existing trees that meet the measurement threshold and overhang the site from neighboring properties. The existing structure originally built in the early 20th century, is currently being utilized as a triplex. A thorough historical analysis of the existing structure will be provided at a later date. The proposed parking stalls will conform to the required space dimensions and locations per SMC 23.54.030 and SMC 23.45.510.C. Vehicular access to the site will be from 13th Ave W. An adequate maneuver and turnaround area will be provided on site, adding to the safety of accessibility.

OPTION A—SITE LAYOUT AND DESCRIPTION

Option A—Site Plan



Option A — Code Compliant

Proposal:
8-unit rowhouse structure
(8 units total, at approximately 1,080sf per unit)

Advantages:
Less Impervious Area—More Landscaping
Off-street Parking for 7 of the units
Garages for solid waste storage and bicycle parking
Strong Street frontage presence

Disadvantages:
No Vehicular parking for one of the units
Large contiguous building footprint
Some units will face an arterial

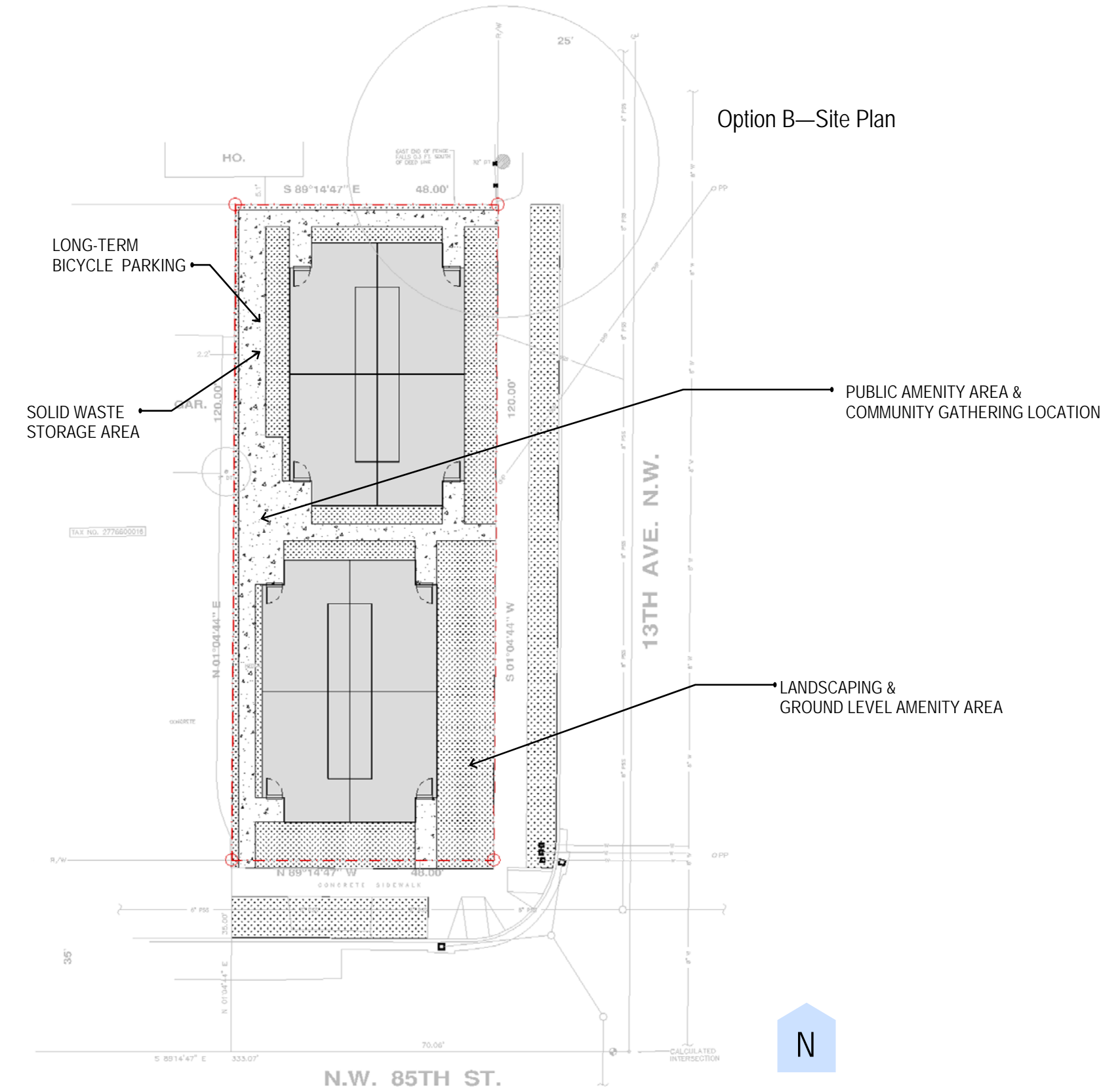
Option A is a code compliant design, and does offer some advantages, the biggest disadvantage though, is the lack of parking for one of the units (only 7 off-street parking stalls provided with this layout). The subject site is located in a parking flexibility area because it is in an Urban Village and considered to be near frequent transit, which does allow for a reduction in the number of parking stalls required. There are street improvement plans in place for 13th Ave NW, that will provide additional street parking areas, but the lack of off-street parking for one of the units is a downside for any new development. The building configuration follows many other new construction structures in the area with it's modern design and layout. Rotating the south two units to have their entries face NW 85th St, maintains a rowhouse feel as well as taking advantage of a corner lots street appeal. Having entries off both NW 85th St and 13th Ave NW will create a more engaging street façade facing both streets. Having two units face NW 85th St could also be seen as a disadvantage; orientating the entries towards an arterial

Some more advantages of this option are a greater ground level landscaping and public amenity area at the Southeast corner of the lot. Reducing the amount of hardscape, and massing of the structure will provide a more visually pleasing look from the ROW as well as provide for additional community interaction of residents on site. The modulation of the units will also help to break up the bulk of the building by allowing for a easy transition of siding materials and colors, as well as reduce the bulkiness of the new the structure.

OPTION A—MASSING STUDY (LOOKING SOUTHWEST)



OPTION B—SITE LAYOUT AND DESCRIPTION



Option B — Code Compliant

Proposal:
Two 4-unit townhouse structures
(8 total units at approximately 1,080 sf per unit)

Advantages:
Less Impervious Area—More Landscaping
Less shared walls
More public amenity area

Disadvantages:
No Vehicular parking
Less connection to the street
Weak Street Frontage Presence

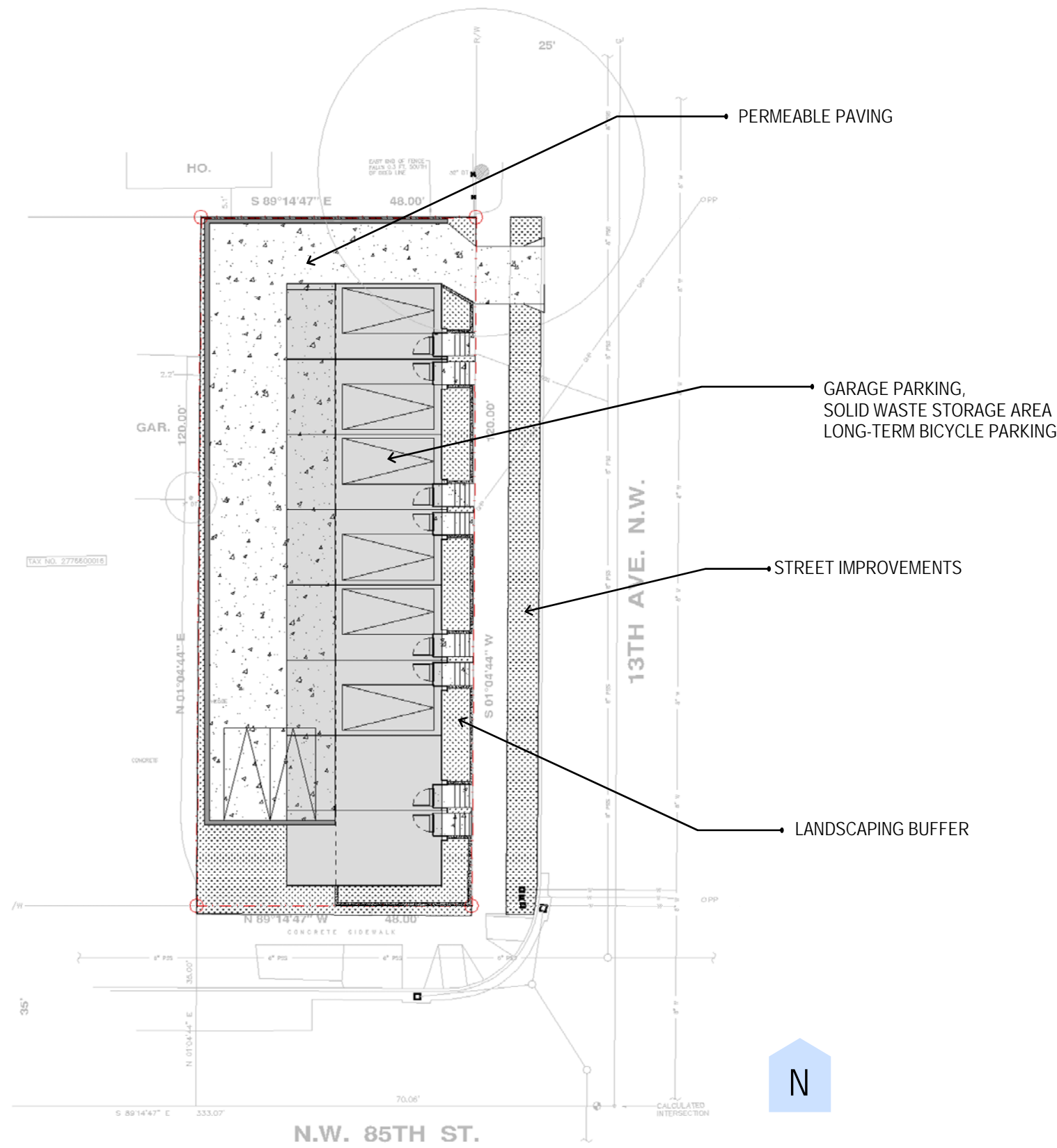
Option B has a large disadvantage in that there is no off-street parking provided. This is allowed per the subject site's location in an Urban Village and proximity to frequent transit. There is also a disadvantage to this site layout in that the buildings do not provide a strong connection to the street. The entries are not all orientated towards the ROW, and could impact wayfinding. Another disadvantage to this layout is the proposed setbacks from the adjacent lots. This proposal places the new buildings at the minimum setbacks from the shared lot lines. It will cast a larger shadow to the north and west, and could create more window overlap for less privacy. Two of the units will face NW 85th St, a major arterial, and could be seen as a disadvantage having the entries face this street.

Option B does have its advantages when considering the amount of landscaping and vegetation that is proposed on-site. The lack of vehicular parking reduces the hardscape and allows for more plantings around the building footprint. This proposed layout creates a courtyard area located in the middle of the lot, between the buildings, and setback from the ROW. Here the residents can congregate around a shared bbq or other landscaping features. They can also store their bicycles in a shared location, increasing the opportunity for community engagement. Since parking is not proposed, there would not be a need to excavate for a basement, reducing the impact on neighboring properties during construction. Proposing two structures also has the advantage of less shared walls, and more opportunity for natural light and ventilation in all the units.

OPTION B—MASSING STUDY (LOOKING SOUTHWEST)



OPTION C (PREFERRED OPTION)—SITE LAYOUT AND DESCRIPTION



Option C — Preferred Option

Proposal:
One 8-unit rowhouse structure
(8 units total, approximately 1,080 sf per unit)

Advantages:
8 Vehicular Parking Stalls
Garages for solid waste & bicycle storage
Entries orientated along a non-arterial
Increased setbacks from adjacent properties

Disadvantages:
Less Community congregation areas
Amount of excavation for basement parking

Option C—the Preferred Option
The biggest advantage to this site layout is the proposal for 8 parking stalls (1 per unit). Although the lot is in an Urban Village and Frequent transit service area, the proposal for 8 parking stalls on site, will reduce the need for off-site parking. The proposed garages will also provide solid waste and recyclable storage area, which will reduce the noise and odor impacts on adjacent parcels. The garages will also provide long-term bicycle parking for the residents. This preferred layout orientates all the entries facing 13th Ave NW, which can be seen as an advantage that none of the entries face the major arterial NW 85th St. This layout has a strong rowhouse appeal, and mimics the building orientation of many new developments on similar sized lots along NW 85th St. The units will take advantage of natural light from the East and West, where the majority of the proposed windows will face. Providing parking also has the advantage of the required access and maneuver distances, which help contribute to a larger setback from the adjacent parcels. The increase setback will reduce the shadow cast by the new structure and add to the sense of privacy with more separation between neighboring windows. The unit entries are identifiable by the individual pedestrian pathways for easy wayfinding. The units will also benefit from a landscaping buffer between the entries and ROW. The disadvantage of this layout is less public amenity area and congregational space that encourages community interaction. Although the design is boxy in nature, the addition of architectural features, a transition in material and color, and the amount of fenestration proposed, will all aid in decreasing the bulkiness and massing the building imposes. The preferred option meets many of the Neighborhood plan goals; by adding residential units, providing vehicular & bicycle parking, and improving the street on 13th Ave NW with new curbs, sidewalks and planting areas, that'll make pedestrian related actives easier and safer.

OPTION C (PREFERRED OPTION)—MASSING STUDY (LOOKING SOUTHWEST)



OPTION C (PREFERRED OPTION)—ADDITIONAL MASSING STUDY VIEWS



Preferred Option C Massing Study—Looking Northwest

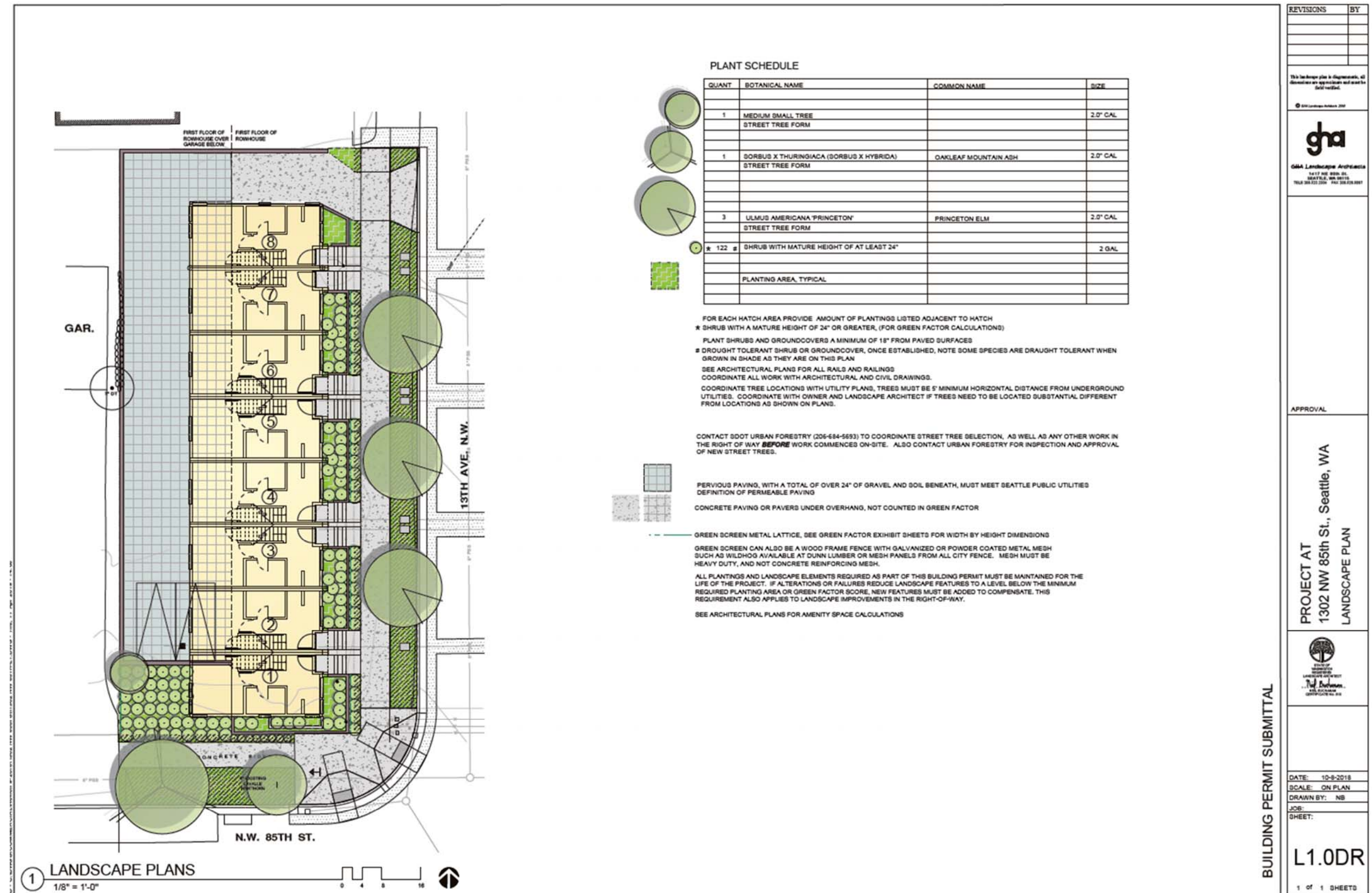


Preferred Option C Massing Study—Looking Northeast

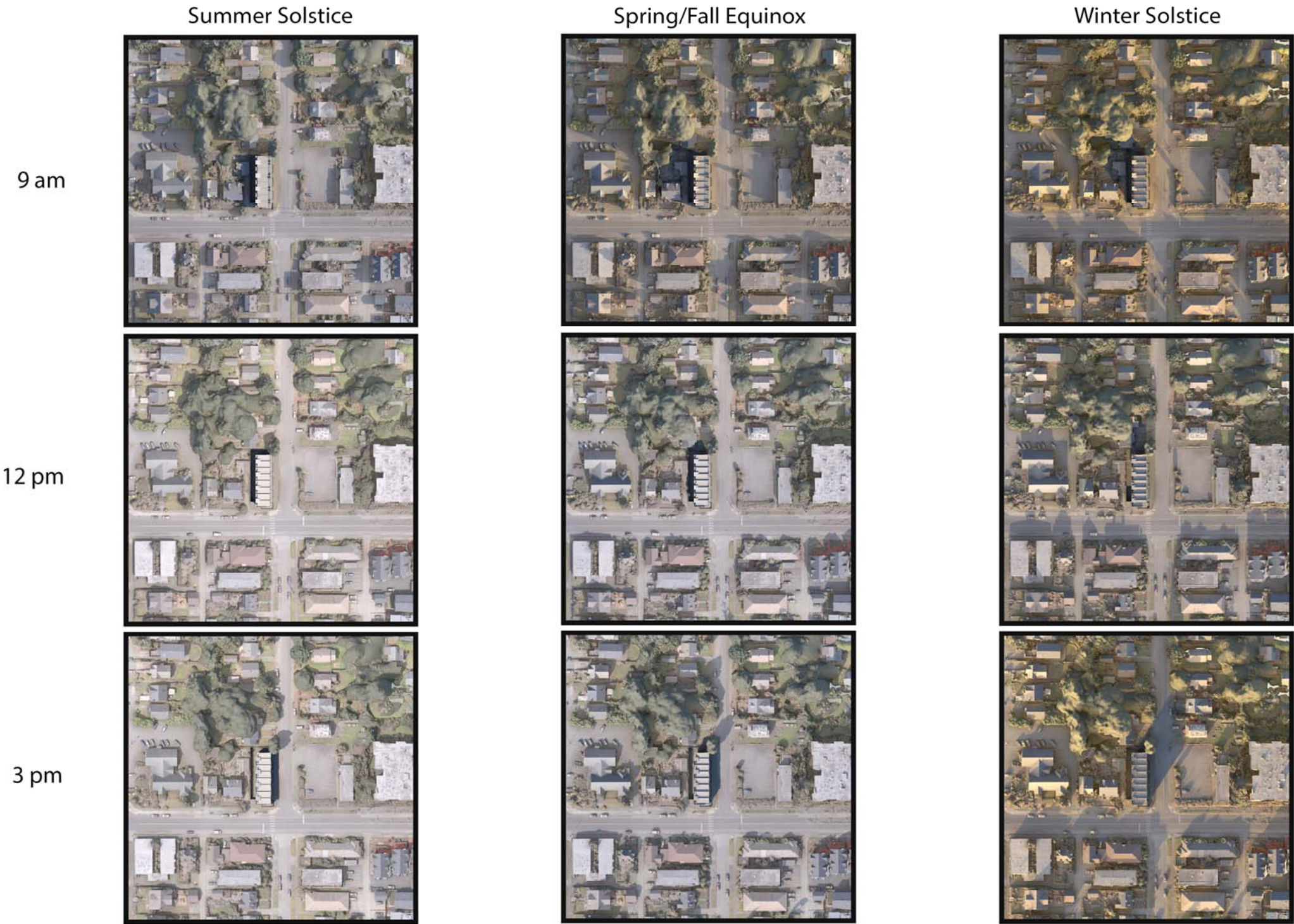


Preferred Option C Massing Study—Looking Southeast

OPTION C (PREFERRED OPTION)—LANDSCAPE PLAN



OPTION C (PREFERRED OPTION)—SHADOW STUDIES



COMMUNITY OUTREACH MATERIALS

EARLY COMMUNITY OUTREACH REGARDING PROPOSED PROJECT GOING THROUGH DESIGN REVIEW

Brief summary of the proposal;

Demolish Existing Structure with Detached Garage and Construct one new, eight unit row house structure for a total of 8 units

Project/Property Address(s): 1302 NW 85th Street, Seattle, WA
SDCI Project Number(s): 3033782-EG
Applicant/Contact Person: Andrew Novion – NOVION GROUP INC.
Website: <http://www.noviongroup.com>

Feedback can be submitted:

Directly to the Applicant via **email.** office@noviongroup.com
Through our **interactive website.** <http://www.noviongroup.com/design-review/4594542438>
Please label your emailed feedback and/or comments as “Feedback for 3033782-EG” in the subject line.
Feedback and Comments will be **accepted through April 10th, 2019.**

Additional information about the project can be found:

SDCI - Seattle Services Portal <https://cosaccela.seattle.gov/portal/welcome.aspx>
Seattle Department of Neighborhood <https://www.seattle.gov/neighborhoods>

A **Community meeting** open to the general public will be held at the **Greenwood Library Branch**, of the Seattle Public Library, Meeting room on **Tuesday, April 9th, 2019 from 4:00PM-5:00PM**. Space is limited, **RSVP** is appreciated.

Comments and discussion presented at the Community meeting will focus on compliance with the established design guidelines. Applicants may, at their discretion, respond directly to the community about any feedback that is not related to Design Review.

Retention of Public Records. All outreach materials and information sent or gathered as part of this Community Outreach effort are public records subject to the Washington Public Records Act, and may be subject to disclosure to a third-party requestor through the City of Seattle.

Para obtener información sobre el proyecto en español, visite nuestro sitio web.

If you would like to request an interpreter at the Community Meeting, please reach out to us via email no later than Tuesday, April 2nd, 2019. Note that emails are returned within 1-2 business days.

Si desea solicitar un intérprete en la Reunión comunitaria, comuníquese con nosotros por correo electrónico a más tardar el martes 2 de abril de 2019. Tenga en cuenta que los correos electrónicos se devuelven dentro de 1-2 días hábiles.

Community Outreach Feedback

Synopsis of the feedback generated from the mailer, website and neighborhood meeting held at the Greenwood Library

A copy of the mailer that was sent to residences within 500’ is shown here. This mailer was translated into Spanish as required by DON. This information was also included on the website link for this particular project outreach. Nobody attended the community meeting held at the Greenwood Library

The only feedback we received was through a survey provided on the website. It was provided by someone identifying themselves as someone who lives very close to the project site. Their concerns are that the final design fit in with the neighborhood “look”. They’re also concerned with the impact of construction related activities (noise & debris). They are also concerned that the proposed structure will feel out of scale with the buildings nearby; that they won’t like the look of the exterior. They want to make sure the proposal will adequately provide off-street parking. They mention they like the neighborhood feel and dislike the amount of cars parked on the street, lack of curbs, gutters and sidewalks. They identify themselves as middle aged, and a neighborhood resident of 15 years.

OTHER MODERN DESIGNS BY ARCHITECTURAL FIRM

