



800 ALASKAN

800 Alaskan Way S, Seattle, WA

Project# 3033083-LU

Design Review Board Recommendation Meeting

November 5, 2019

Project Intention

Development Summary

Located on Alaskan Way across from Colman Dock, 800 Alaskan is at the center of downtown Seattle's waterfront. The site offers unobstructed views of Elliott Bay and is connected via pedestrian bridge to the ferry terminal.

800 Alaskan is a 519,000 SF, 14-story + penthouse high-rise structure at 190 ft tall consisting of approximately:

- 278 below grade parking stalls on three levels
- 16,700 SF street level retail space
- 209,000 SF commercial office space on seven levels
- 136,000 SF residential space on six levels
- 106 residential units
- Rooftop residential interior and exterior amenity space
- Exterior terrace at level 5 for office tenants

Development Objectives

Create an iconic gateway to the city and offer new connections to the waterfront.

Reactivate all four street frontages and redefine the new waterfront experience along Alaskan Way.

Offer varied, delightful spatial experiences for tenants of the building and pedestrians.

Respond to urban context through the scale of massing components and simple elegant skin.



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Section 01 / Background and Context



Section 01 / Background and Context



Section 01 / Board Recommendations & Priorities

EDG Summary

The Design Review Board evaluated three massing concepts at the Early Design Guidance meeting on January 22, 2019 and the board recommended that the project move forward to MUP application with the preferred "Erode" scheme. "Those who supported the [Erode] massing expressed support for the **dynamic, interesting, iconic, and elegant form.**"

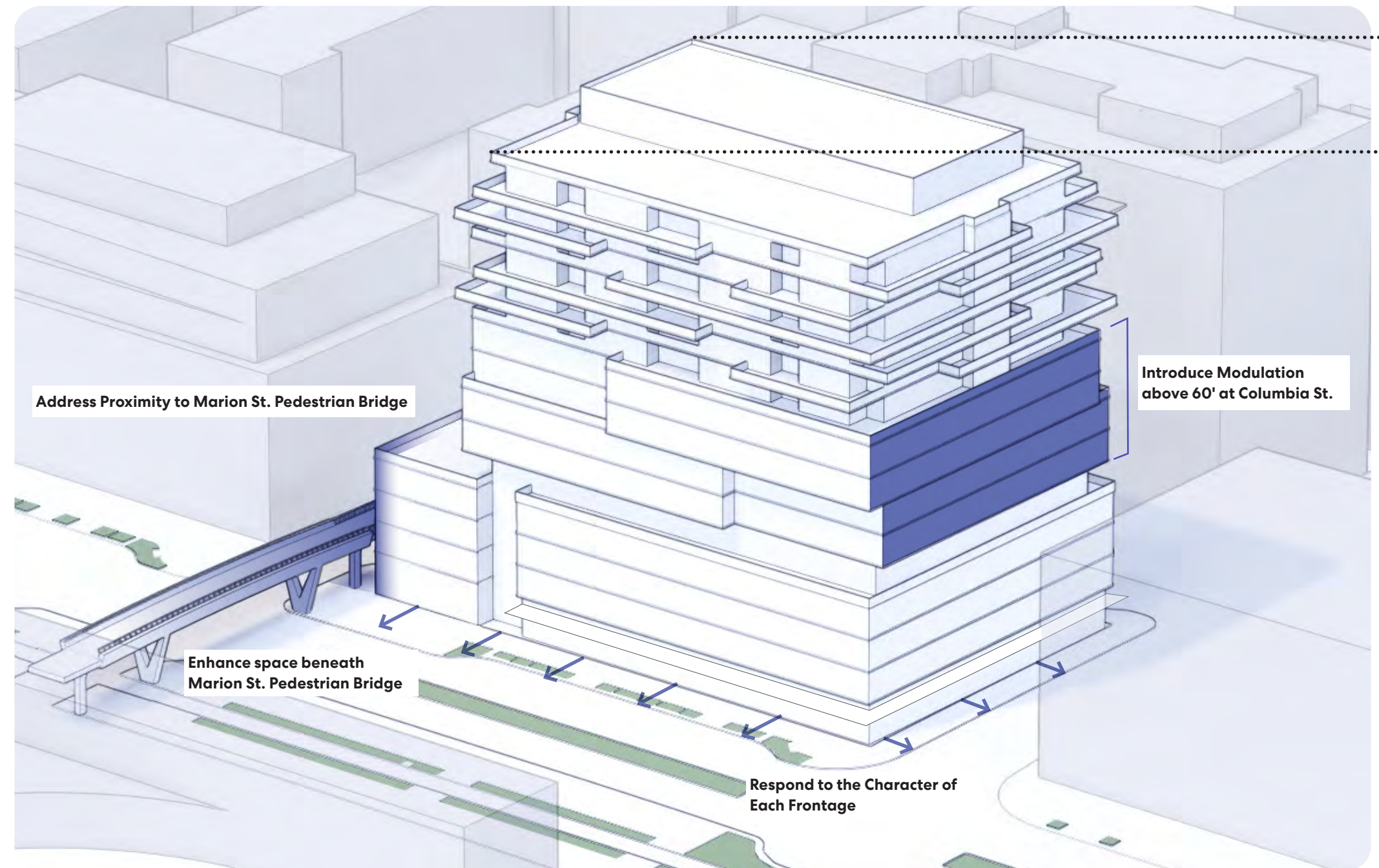
Architectural Concept, Massing and Materials

- 1. Study Context and Proportion.** Study of how the immediate context has informed building modulation. Studies are provided on pages 8-9.
- 2. Develop the Idea.** Provide elevations, sections, and vignettes demonstrating materials, material detailing and material transitions at all levels of the structure. Requested drawings are provided on pages 12-27.
- 3. Illustrate Office Expression.** Provide further detail on how the office use is articulated at the exterior of the building. See section perspectives on pages 25 and 27.
- 4. Develop the Fifth Facade.** Include composite hardscape/landscape plans demonstrating the roof has been developed as a fifth building facade. See aerial rendering and landscape proposal on pages 28-33.

Street Level Design

- 1. Address Proximity to Marion St. Pedestrian Bridge.** Demonstrate how pedestrians will be drawn from the bridge into ground level retail space and provide information about the relationship of the space adjacent to the bridge. See pages 20-23.
- 2. Enhance space beneath the Marion St. Pedestrian Bridge.** Show how space beneath the bridge has been developed as a Green Street maximizing pedestrian comfort and safety and study overhead weather protection at entries. See pages 18-23, 31, and 45.
- 3. Respond to the Character of each Frontage.** Demonstrate how each street scape responds to the unique character of that street and provide drawings that remonstrate materials, material detailing and transitions. See pages 18-21 and 36-41.

EDG Massing

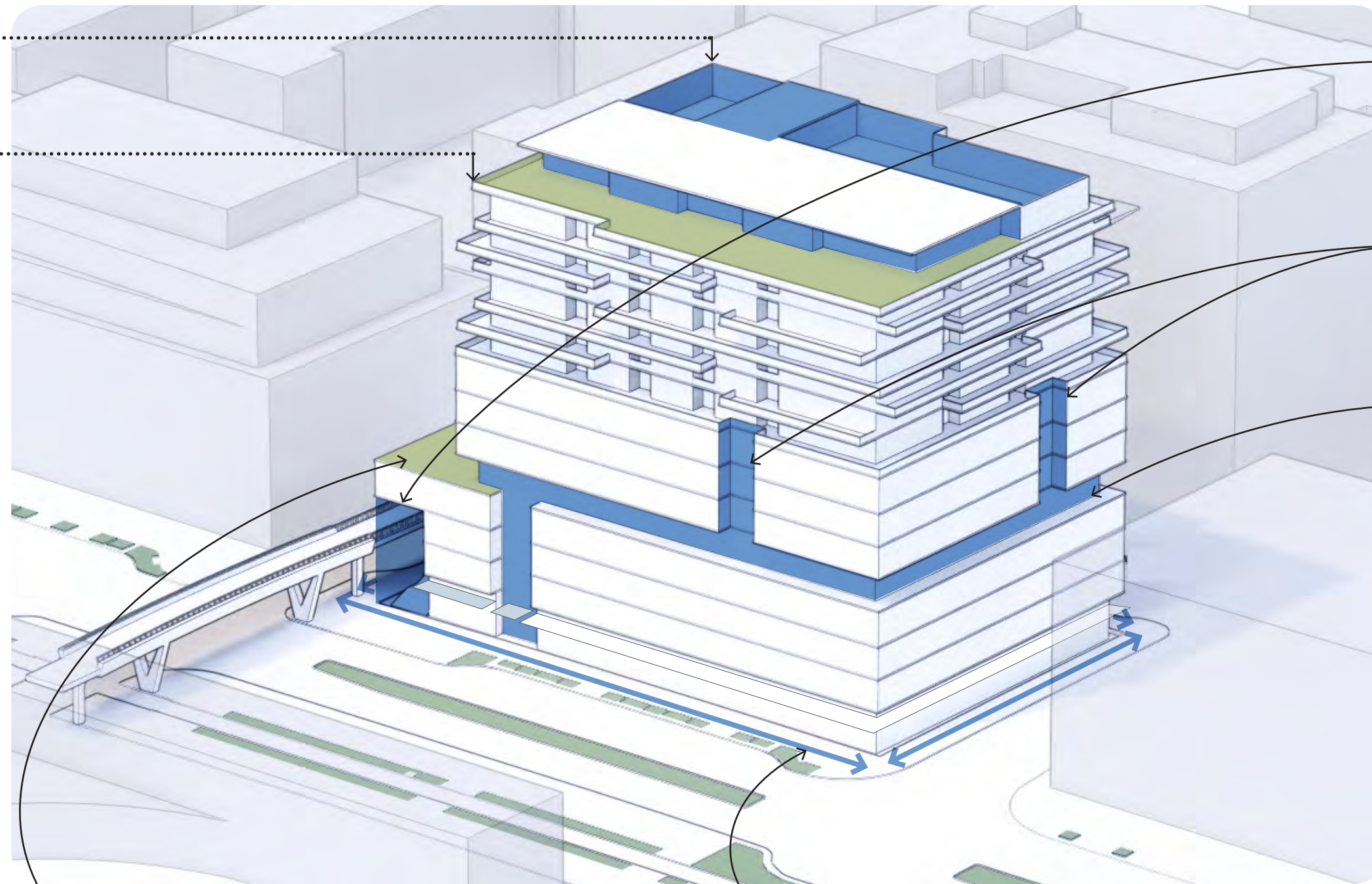


Departures

- 1. Facade Modulation SMC 23.49.058 B2.** The board indicated early support for departure along Alaskan Way and Western Ave, but did not support departure along Columbia St. See page 51 for updated request.
- 2. Overhead Weather Protection SMC 23.49.018.** The board indicated early support for a departure from overhead weather protection along Marion Street and the northern portions of Western and Alaskan Way as long as weather protection is provided at entries along Marion Street. See page 50 for updated request.

Section 01 / Board Recommendations & Priorities

Current Massing - Response to EDG



Design Development

Massing changes are summarized below. Other design refinements are described by other drawings and illustrations provided in this package.

Address Proximity to Marion St. Pedestrian Bridge. A public stair and elevator now provide access between the Marion St. Pedestrian Bridge and street level at Marion St. and Alaskan Way. Retail is proposed beneath the stair where feasible. See pages 18-21.

Introduce Modulation above 60' at Columbia St. The method of modulating the office floors has evolved to further breakdown the scale of the mass and address the board's response to the departure request for unmodulated facade at this frontage. See page 49 for updated request.

Program Adjustment. The interstitial residential floor at level 5 was removed resulting in the building being 10' shorter.

Priority Design Guidelines

Downtown Design Review Guidelines

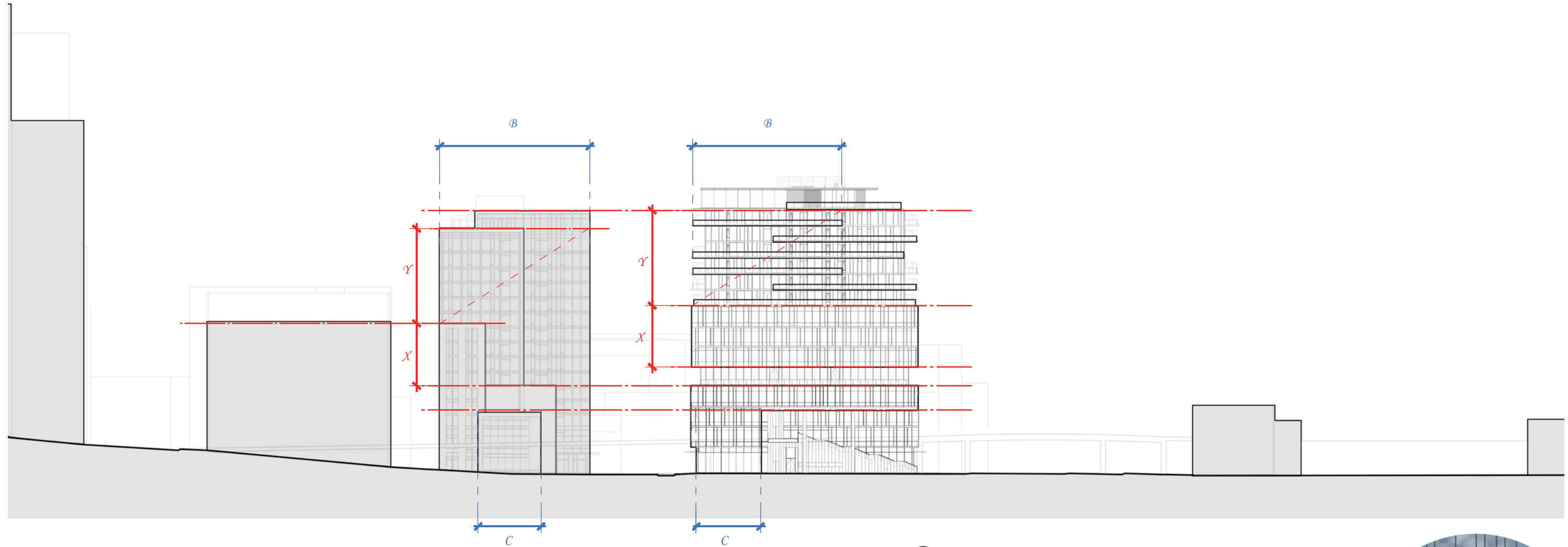
- A2 Enhance the Skyline (2)
- B1 Respond to the Neighbourhood Context (2 & 3)
- B2 Create a Transition in Bulk & Scale (1,2 & 3)
- B3 Reinforce the Positive Urban Form & Architectural Attributes of the Immediate Area (2 & 3)
- B4 Design a Well-Proportioned & Unified Building (2 & 3)
- C1 Promote Pedestrian Interaction (1)
- C2 Design Facades of Many Scales (2 & 3)
- C4 Reinforce Building Entries (1)
- E1 Minimize Curb Cut Impacts
- E3 Minimize Presence of Service Areas

Develop the Fifth Facade. All mechanical equipment is either screened or fully enclosed and overall the building has become 10' shorter due to program adjustment described below. The amenity volume has been refined to be more consistent with the expression of the floors below. Terraces and planting proposed at level 5 and penthouse rooftops. See aerial rendering and landscape proposal on pages 28-31.

Respond to the Character of each Frontage. See street elevations, plans and section perspectives on pages 18-21 and 36-41. Entries have been made more legible with framing elements, seating, planting and lighting have been incorporated as appropriate at each frontage.

Section 02 / Early Design Guidance & Response

Contextual Modulation: North Elevation (South Elevation sim. above 60')



Study Context and Proportion. The proportions of the proposed building are sympathetic to those found in the immediate context.

At EDG, the board agreed that the office podium was appropriately scaled to the existing Alaskan Way context. Additionally, the board was supportive of the massing relief between the podium and tower as well as the placement of the tower justified to the south, which maintains the view corridor along Marion.

A-2 Enhance the Skyline
The eroded form of the tower is unique along the waterfront.

B-1 Respond to the Neighborhood Context
The scale of the modulation becomes smaller as the building ascends. Larger volumes at the base relate to the simple, historic forms of the adjacent buildings along Alaskan Way.

B-2 Create a Transition in Bulk and Scale
The building is composed of parts sympathetic in scale to the surroundings as shown in the analysis above.

B-3 Reinforce the Positive Urban Form & Architectural Attributes of the Immediate Area

The blocks to the north and south of the site share a similar built footprint to the proposed project and are characterized by buildings that have the same treatment at all frontages (no apparent front or back). Similarly, all four of the 800 Alaskan street facing elevations are proposed to be unified in expression.

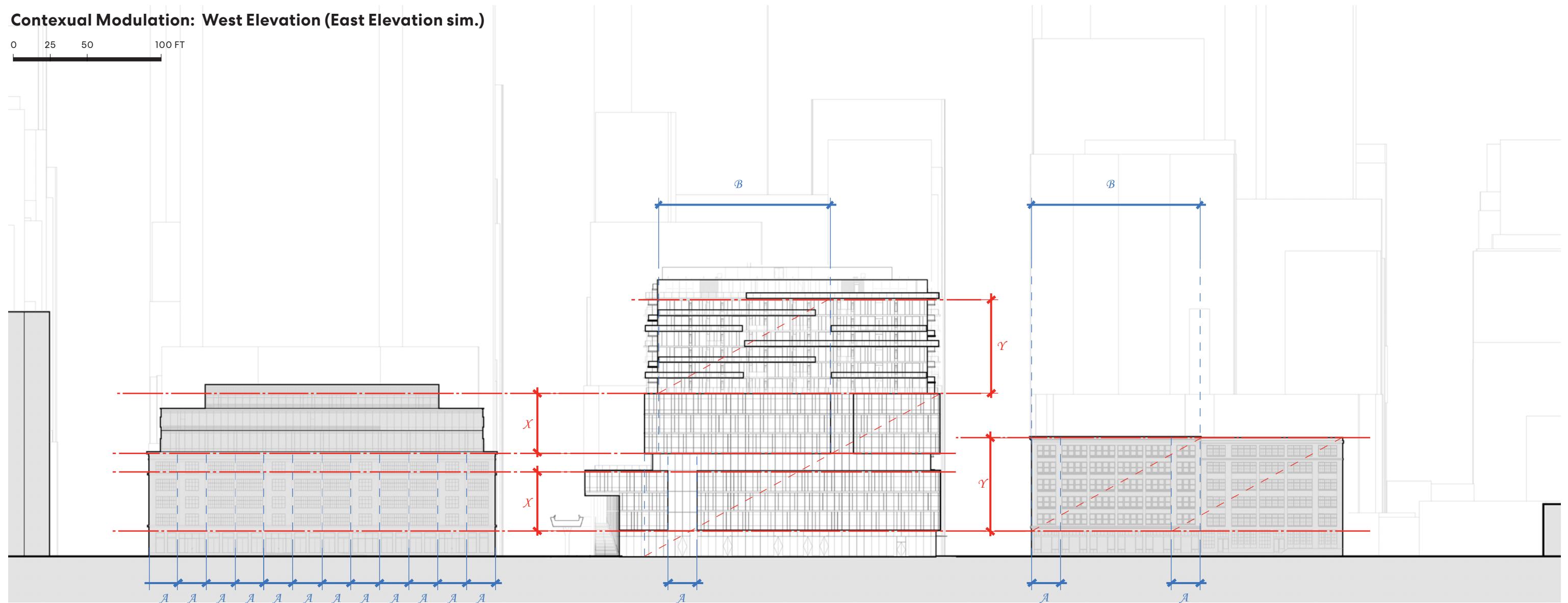
The facades of the adjacent buildings rationally convey internal structure. To optimize views, a highly transparent envelope with a syncopated but regular rhythm that reinforces the horizontal is proposed.



Section 02 / Early Design Guidance & Response

Contextual Modulation: West Elevation (East Elevation sim.)

0 25 50 100 FT



B-4 Design a Well-Proportioned & Unified Building

The outer most extents of the balconies align with the office volume below and the expression of the horizontal slab edge unites the form while honestly conveying the program within.

C-1 Promote Pedestrian Interaction

Retail space with a high level of transparency and 18-foot floor to floor height is proposed along all street frontages to the greatest extent possible. The Marion St. Pedestrian stair engages commuters with the building.

C-4 Reinforce Building Entries

Entries are reinforced through building form and an interior atrium. The entry is clearly identifiable and visible from Alaskan Way (similar at Western Avenue). Sufficient lighting and weather protection is proposed at entries to ensure security and positive pedestrian experience. The primary retail entries respond to contextual patterns.

Section 02 / Early Design Guidance & Response

EDG Massing



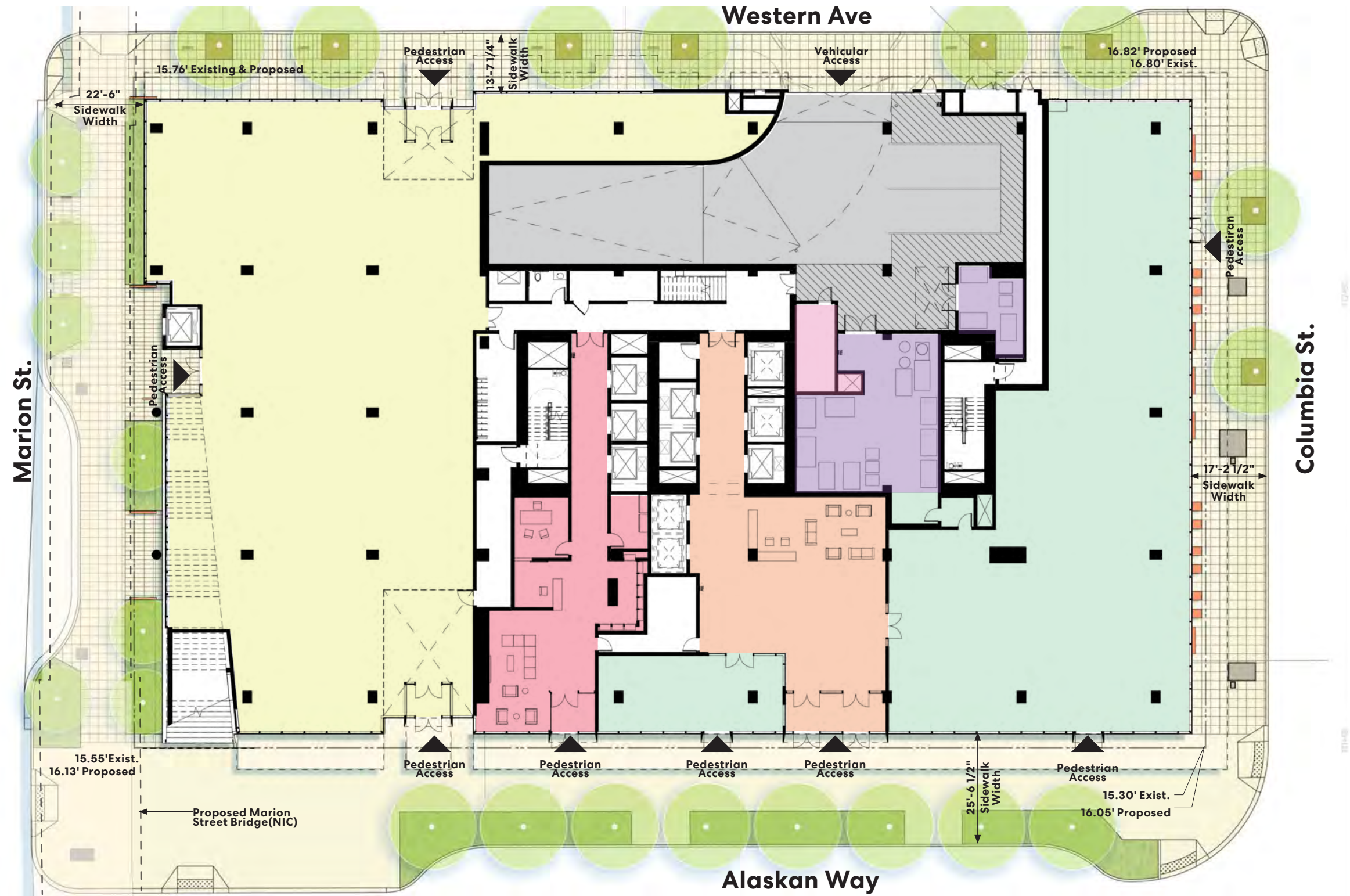
Section 02 / Early Design Guidance & Response

Current Massing



Section 02 / Ground Floor Plan

- Retail / Food Hall
- Retail
- Office Lobby
- Residential Lobby
- Electrical Room
- Trash & Recycling Storage
- Loading & Parking Entry



Section 02 / Ground Floor Program

Food Hall



Central Market - Denver, CO



Auction House Market - New Orleans, LA



400 Fairview - Seattle, WA



Union Market - Washington, DC

Section 02 / Elevation: Alaskan Way



+210' - 5" T.O. ELEV. PENTHOUSE

+202' - 5" ROOF

+190' - 5" PENTHOUSE

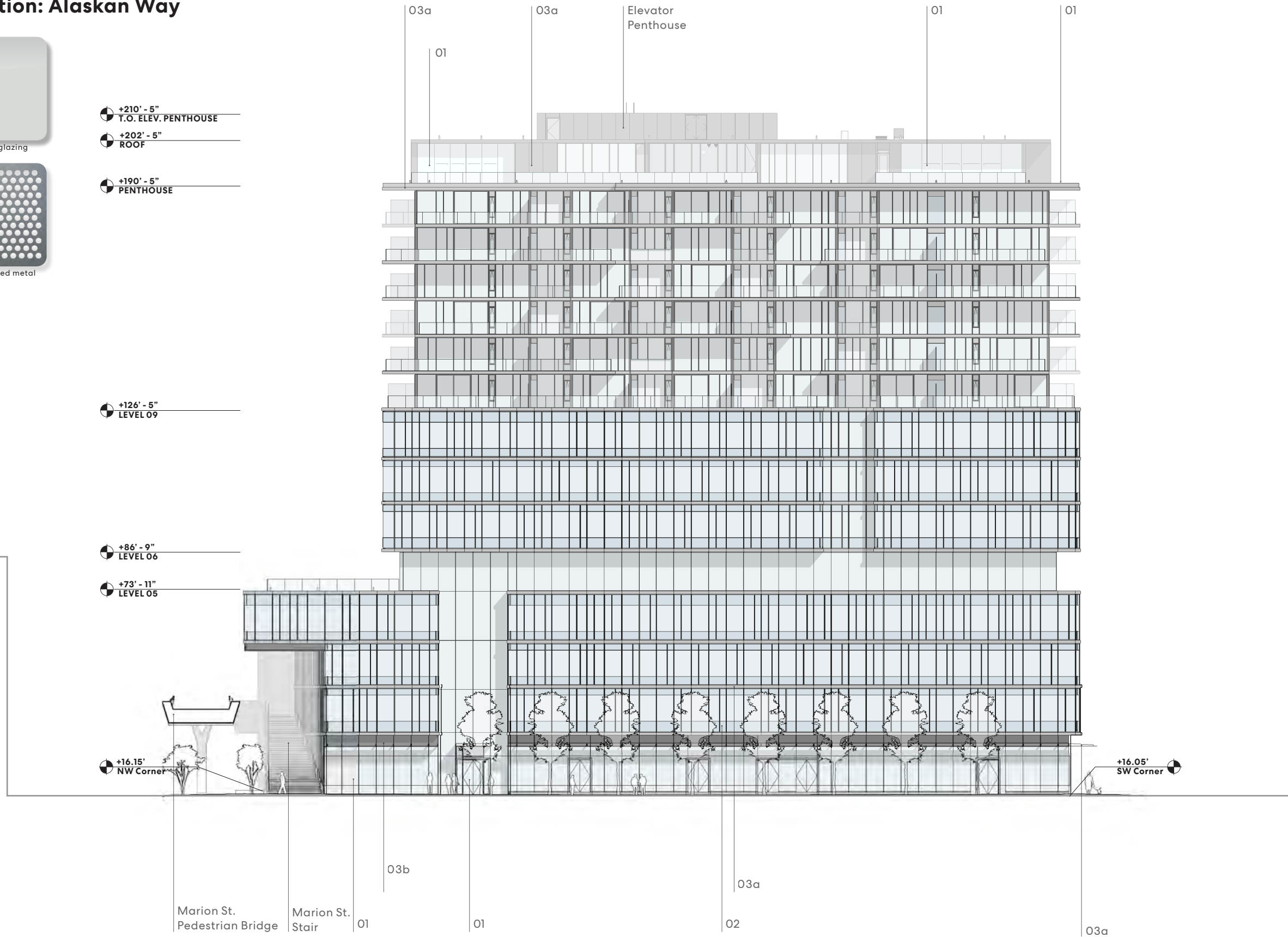
+126' - 5" LEVEL 09

+86' - 9" LEVEL 06

+73' - 11" LEVEL 05

+16.15' NW Corner

+16.05' SW Corner



Section 02 / Elevation: Marion St.

+210' - 5"
T.O. ELEV. PENTHOUSE

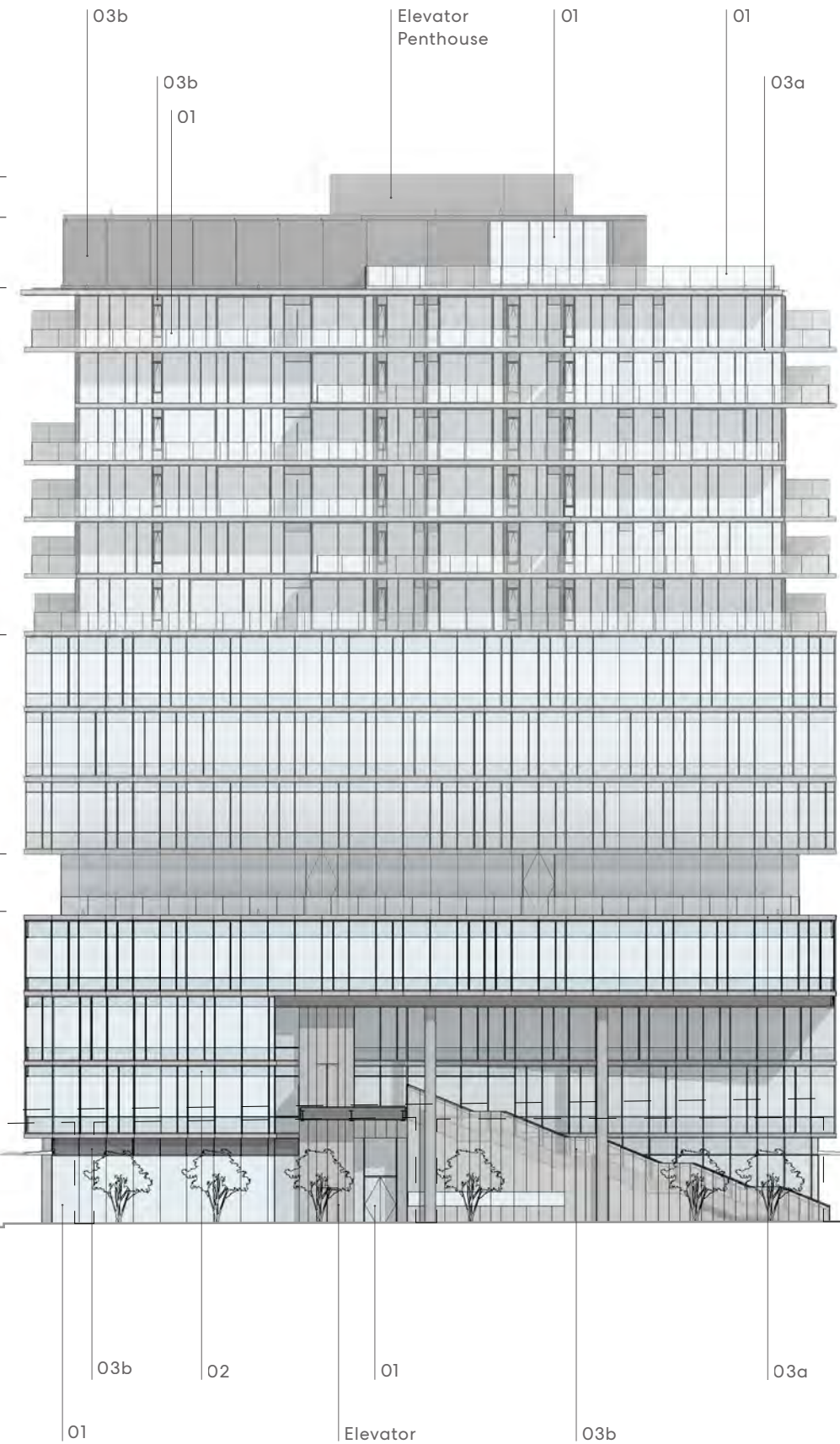
+202' - 5"
ROOF

+190' - 5"
PENTHOUSE

+126' - 5"
LEVEL 09

+86' - 9"
LEVEL 06

+73' - 11"
LEVEL 05



Section 02 / Elevation: Western Ave.



+210' - 5"
T.O. ELEV. PENTHOUSE

+202' - 5"
ROOF

+190' - 5"
PENTHOUSE

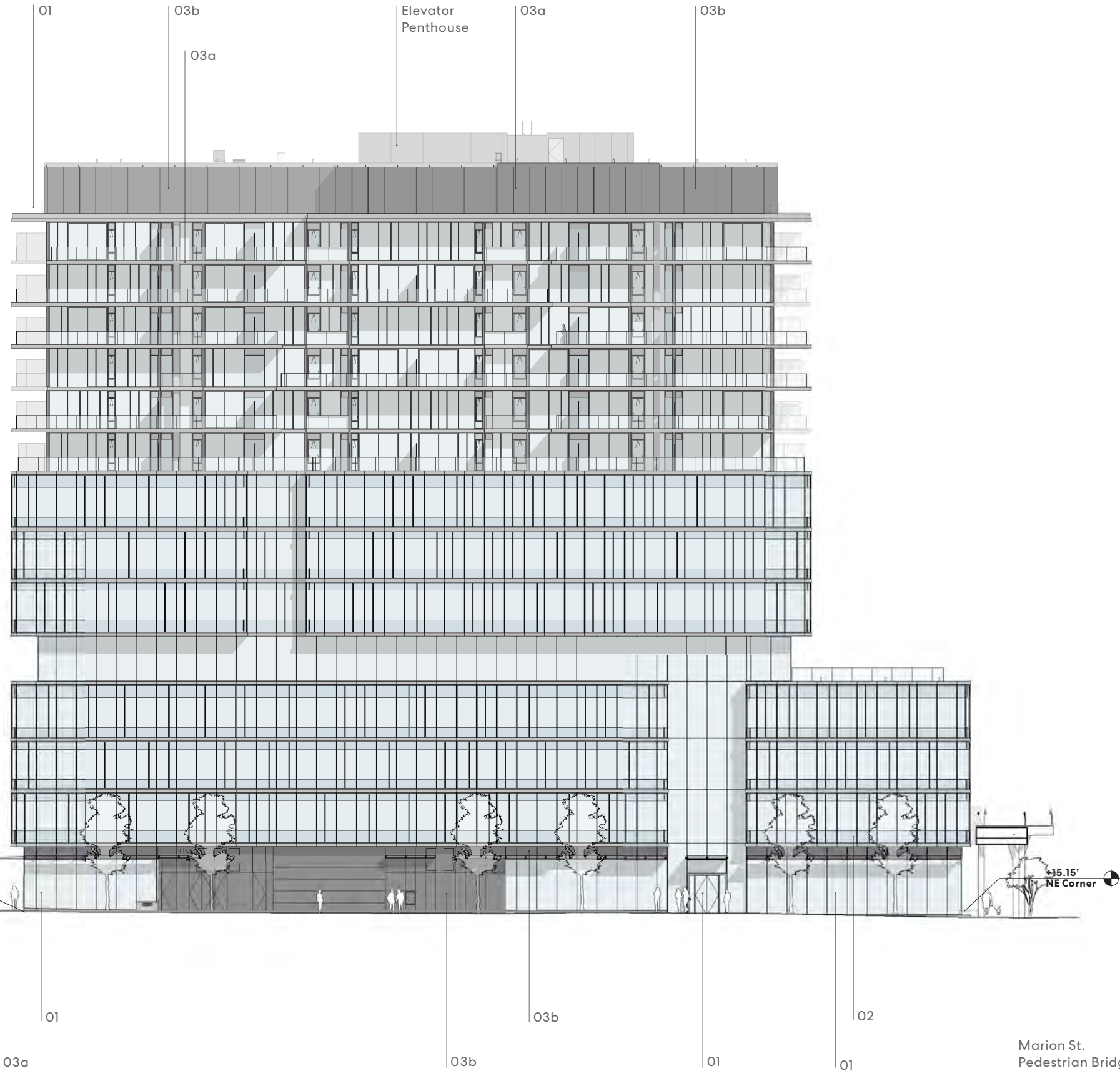
+126' - 5"
LEVEL 09

+86' - 9"
LEVEL 06

+73' - 11"
LEVEL 05

+16.76'
SE Corner

+15.15'
NE Corner



Section 02 / Elevation: Columbia St.



● +210' - 5"
T.O. ELEV. PENTHOUSE

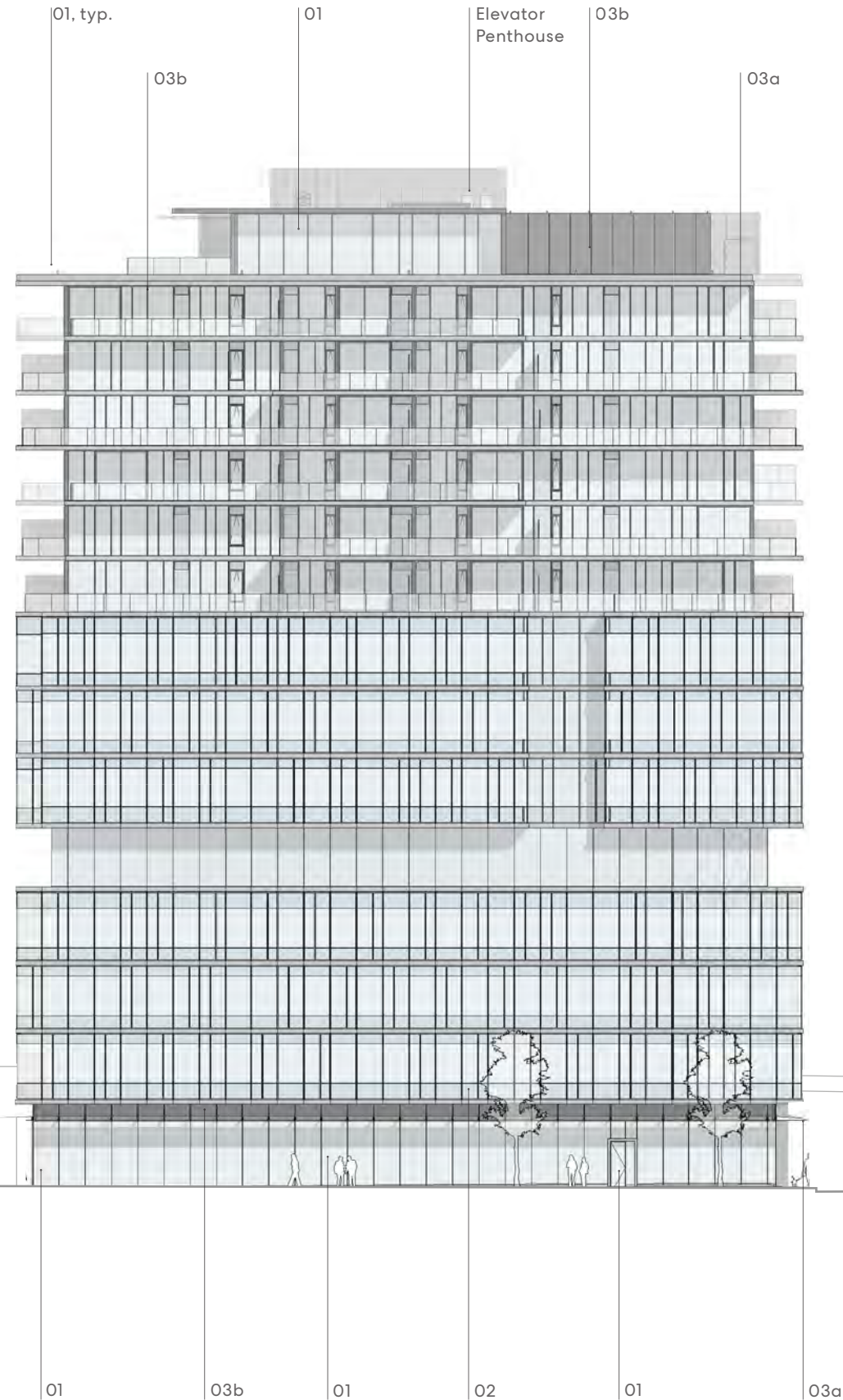
● +202' - 5"
ROOF

● +190' - 5"
PENTHOUSE

● +126' - 5"
LEVEL 09

● +86' - 9"
LEVEL 06

● +73' - 11"
LEVEL 05



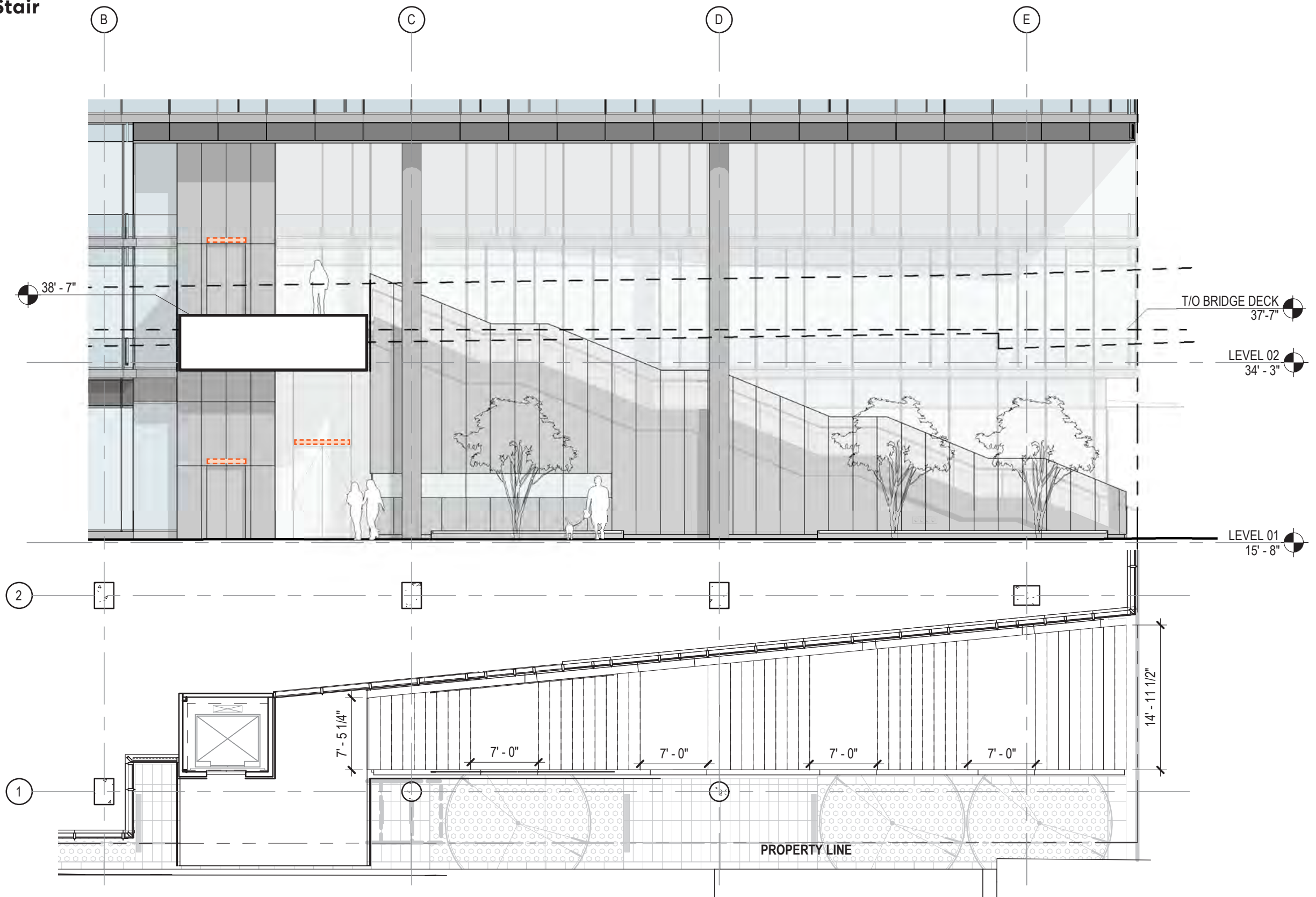
Section 02 / Marion St. Stair

Address Proximity to Marion St. Pedestrian Bridge. The incorporation of a public stair and elevator has provided the link that was absent at EDG between the ground level retail space and the pedestrian bridge.

A much shorter portion of the building runs parallel to the bridge and is set back 5' to provide visual relief. Please see below for previous studies of the building's relationship to the bridge below with varying set backs.



Section 02 / Marion St. Stair



 potential signage locations

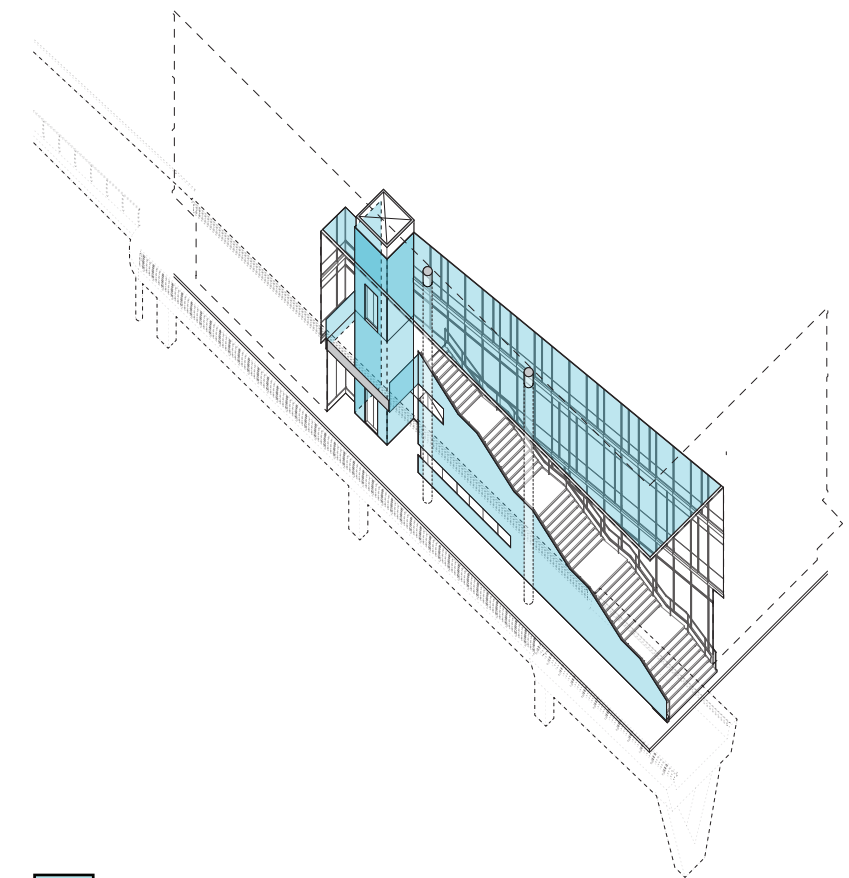
Marion Street - the Green Street



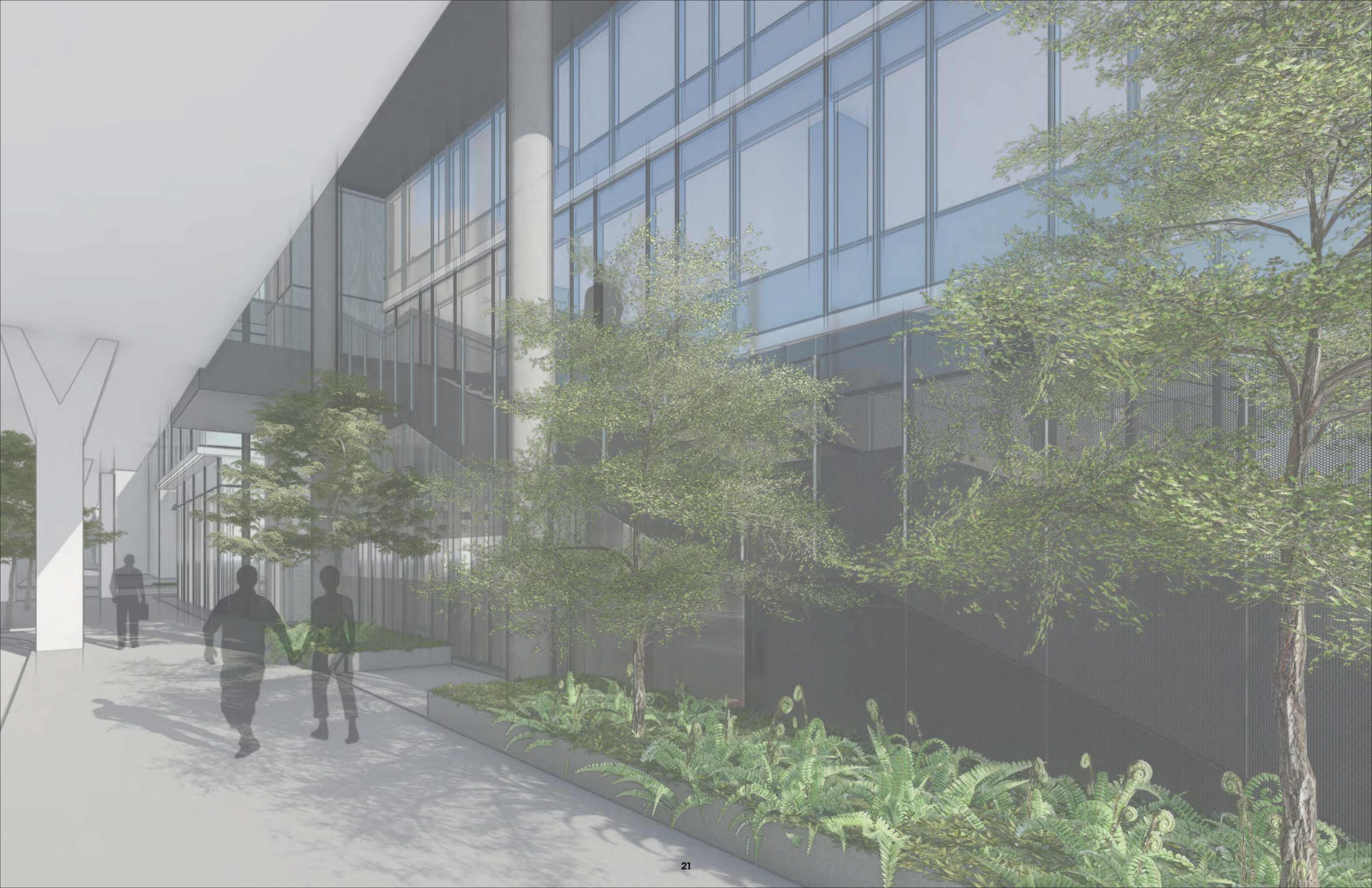
Section 02 / Marion St. Stair



Enhance space beneath the Marion St. Pedestrian Bridge. A green buffer softens the Marion St. pedestrian experience adjacent to the public stair. A highly transparent perforated metal scrim unifies the stair, elevator and soffit above.



 Perforated Metal Scrim



Section 02 / Material



1

1

3a

1

3a

2

3a

Section 02 / Material

The material expression of the addition takes cues from the restrained palette of the historic building.



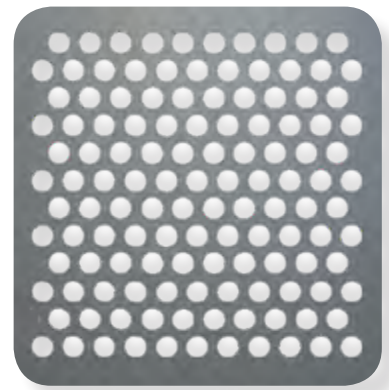
1 vision glazing



2 spandrel glazing



3a metal



3b perforated metal



Section 02 / Material

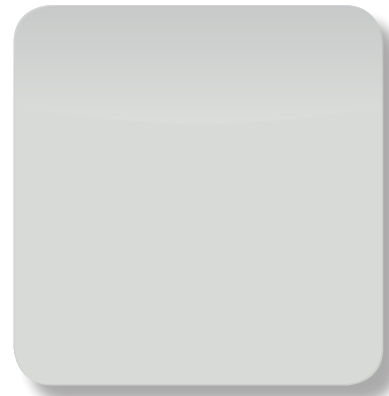


Section 02 / Material

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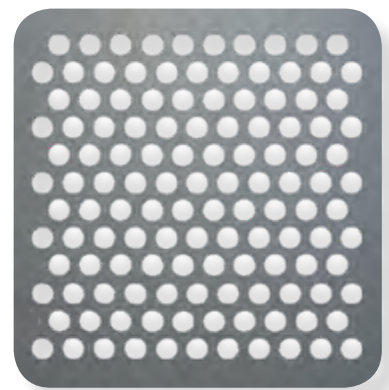
1 vision glazing



2 spandrel glazing



3a metal



3b perforated metal



Section 02 / Material



3a

1

1

3a

2

1

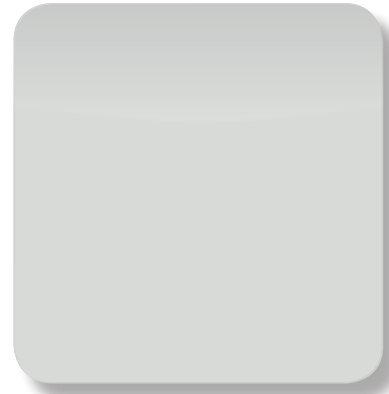
2

Section 02 / Material

The material expression of the addition takes cues from the restrained palette of the historic building.



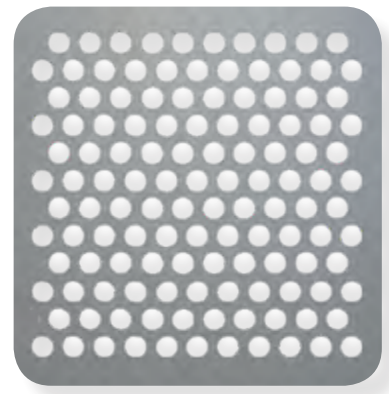
1 vision glazing



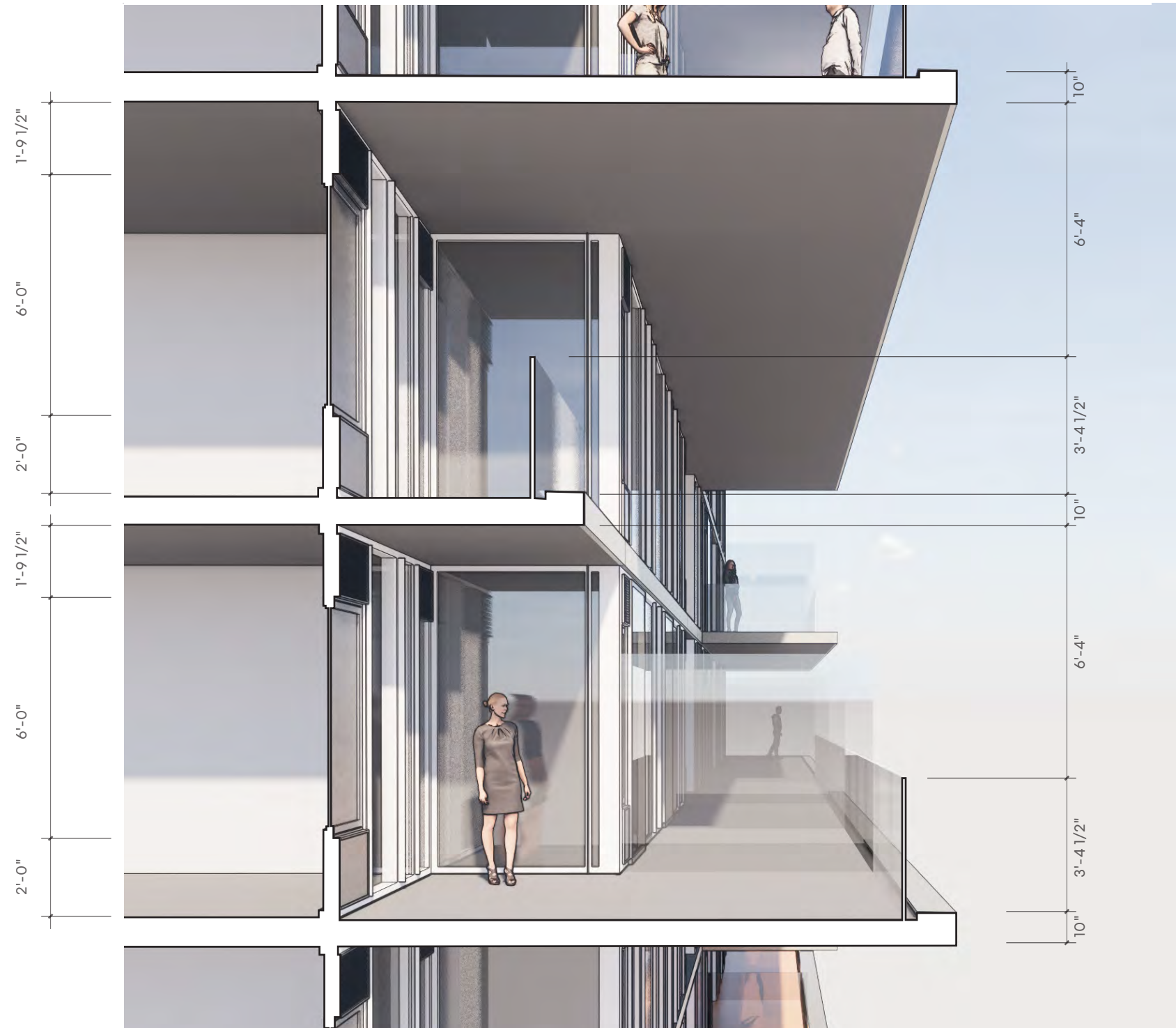
2 spandrel glazing



3a metal



3b perforated metal



Section 02 / Landscape

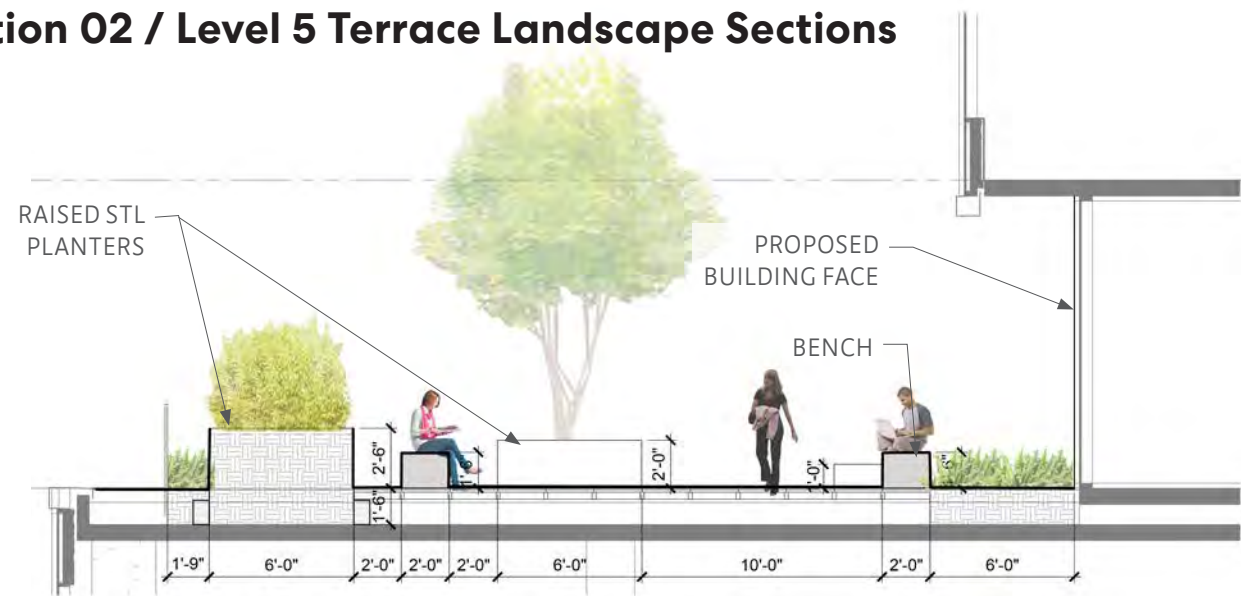


MARITIME BUILDING

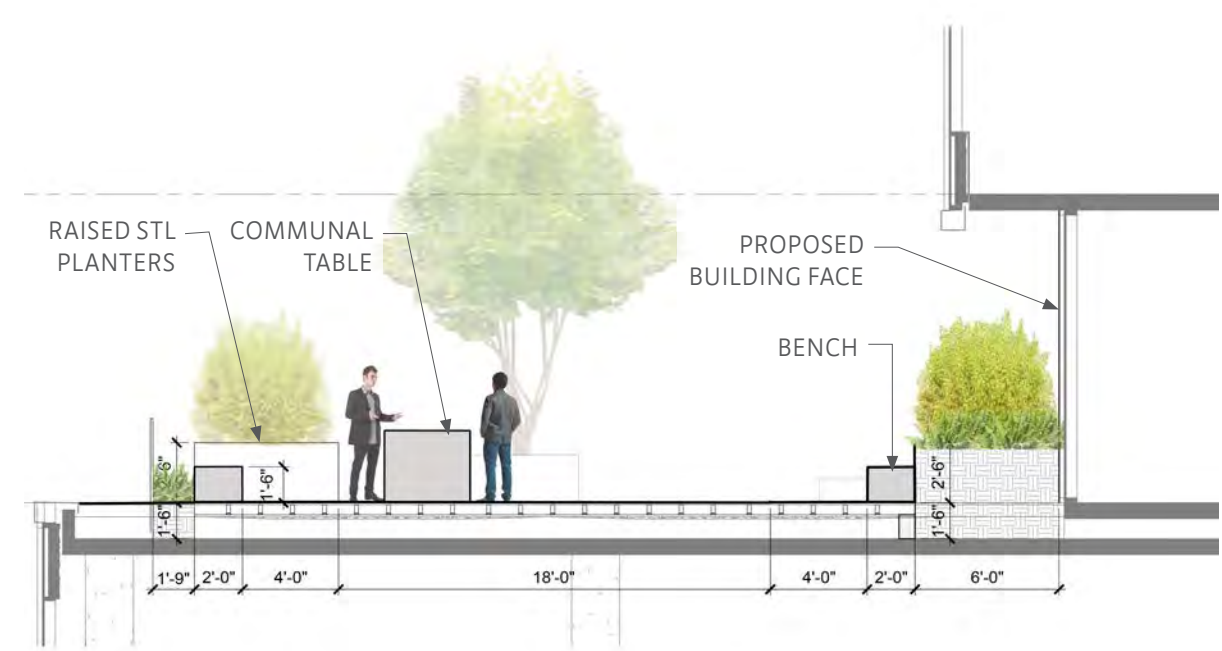
Section 02 / Level 5 Terrace Landscape Plan



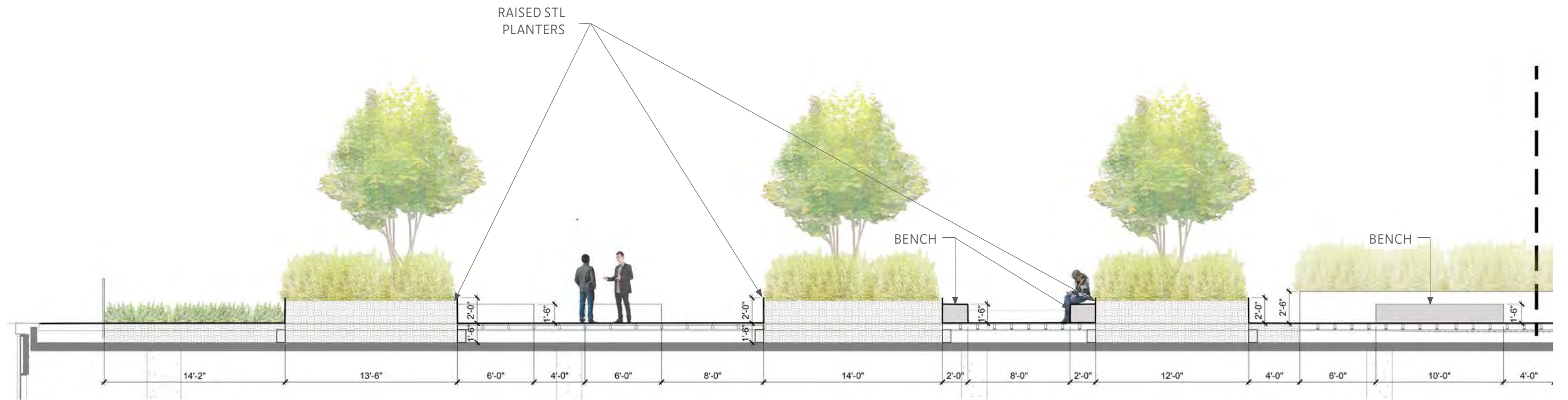
Section 02 / Level 5 Terrace Landscape Sections



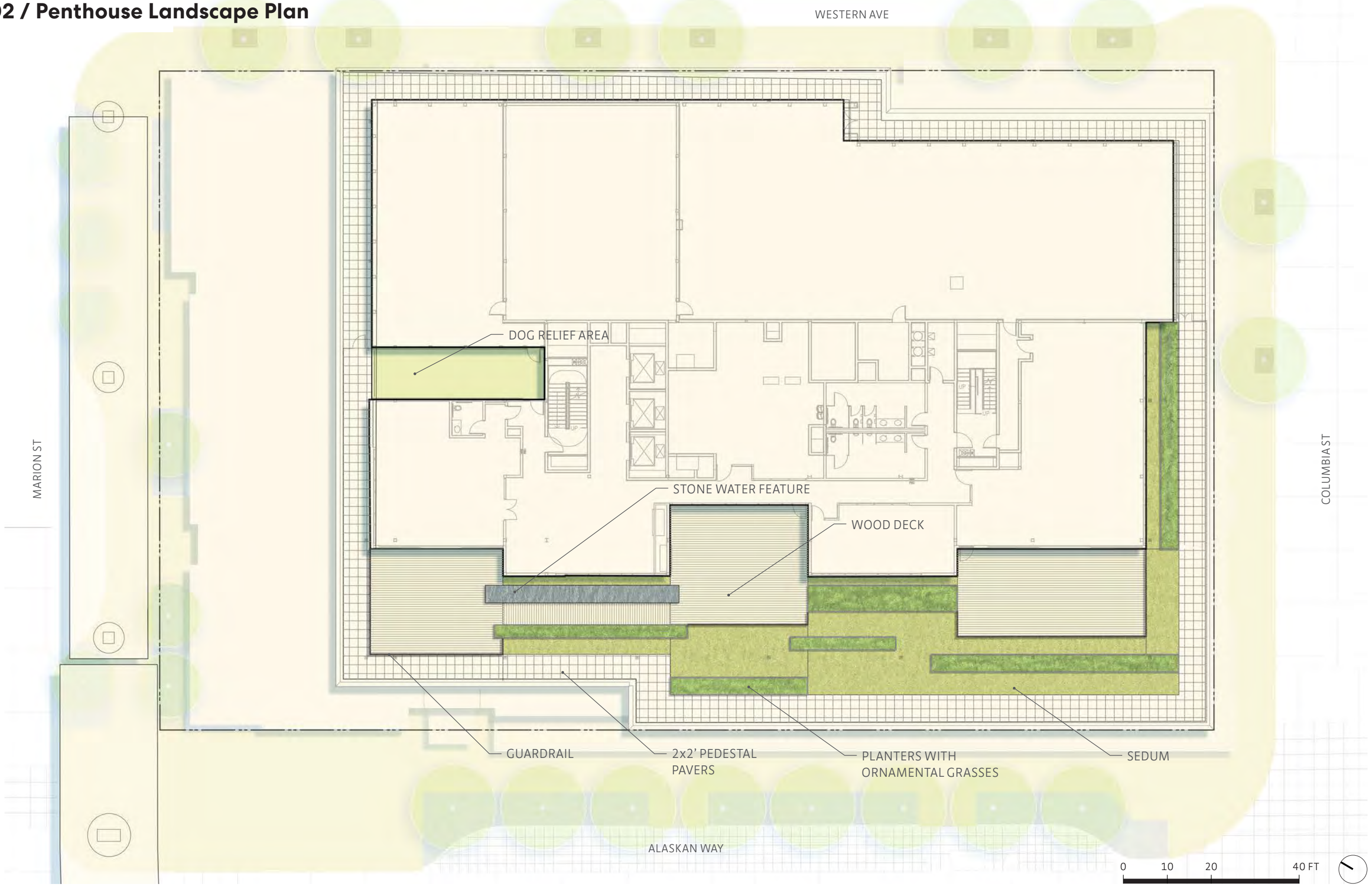
LEVEL 5 TERRACE SECTION AT NORTHEAST SEATING AREA



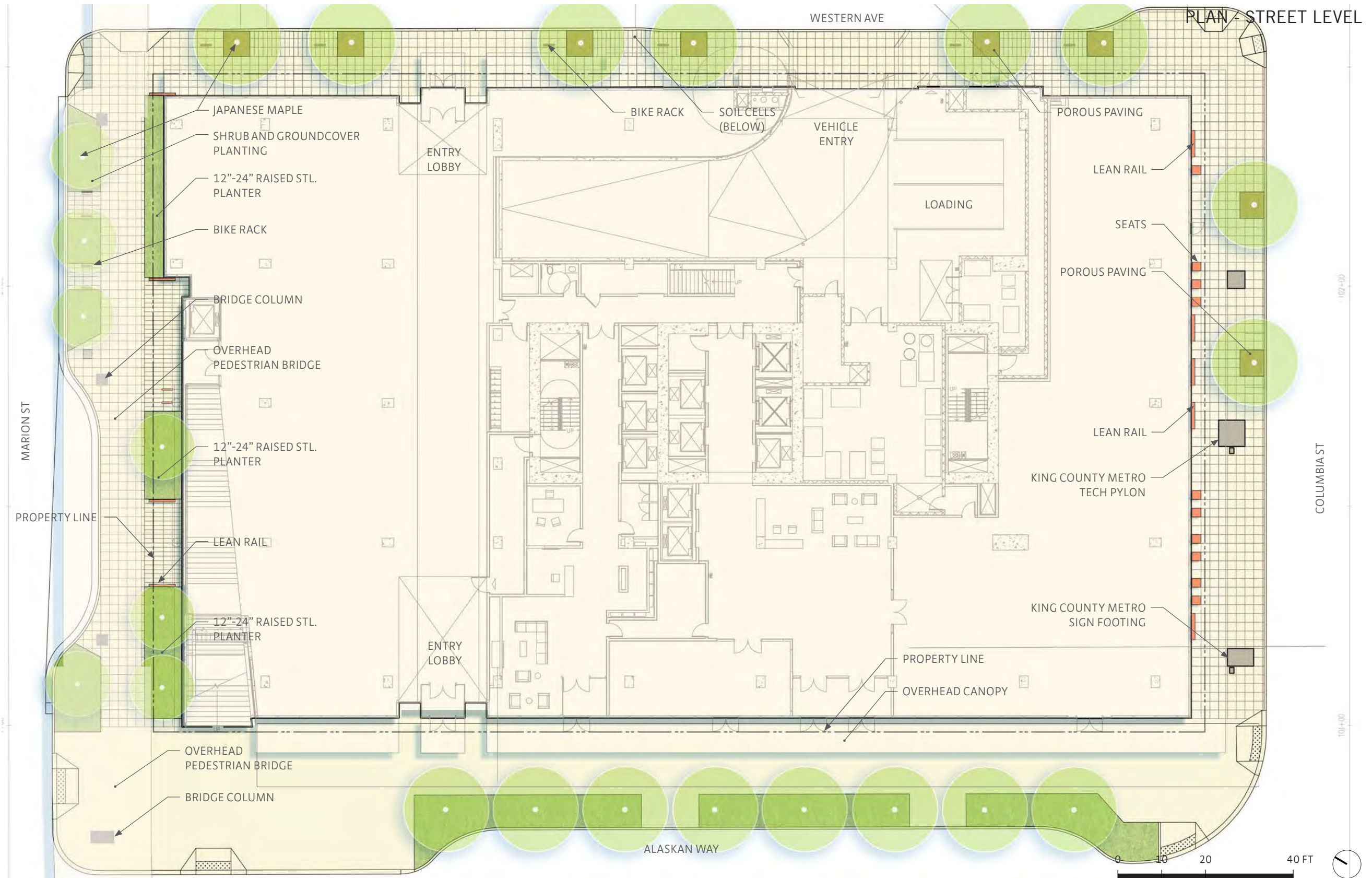
LEVEL 5 TERRACE SECTION AT SOUTHWEST DECK



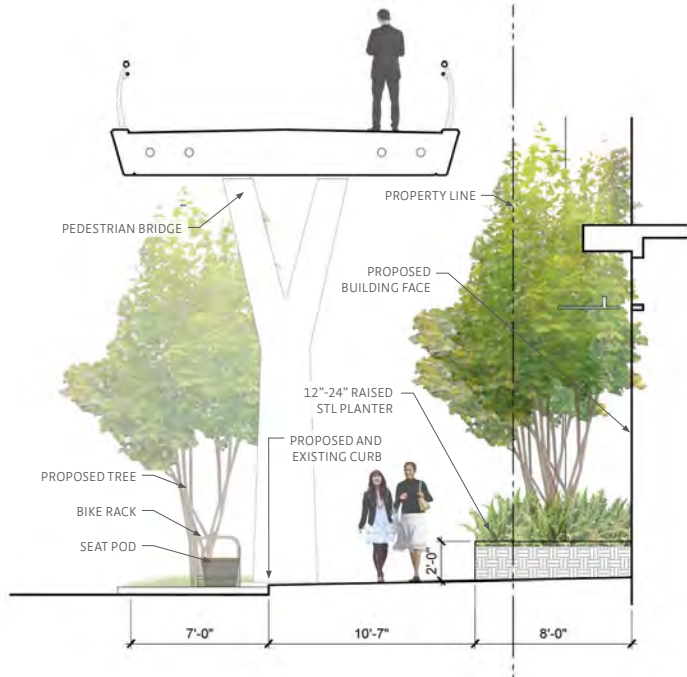
Section 02 / Penthouse Landscape Plan



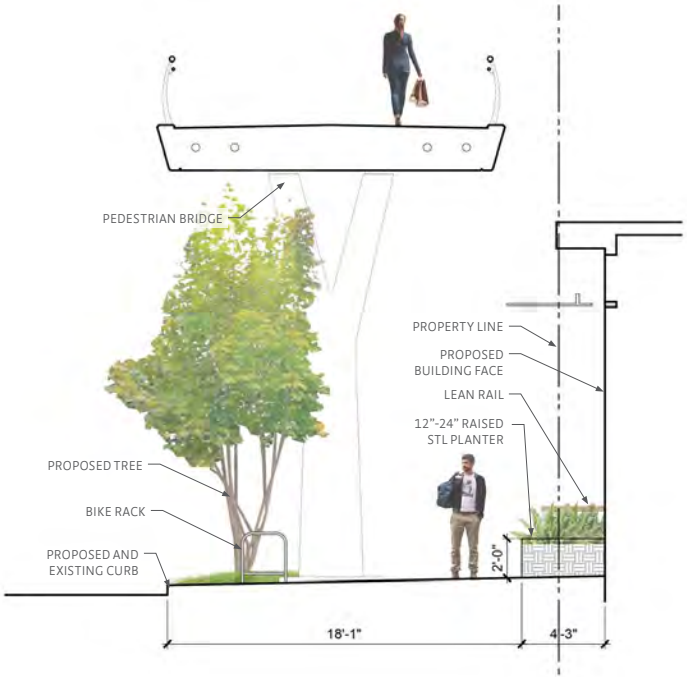
Section 02 / Ground Level Landscape Plan



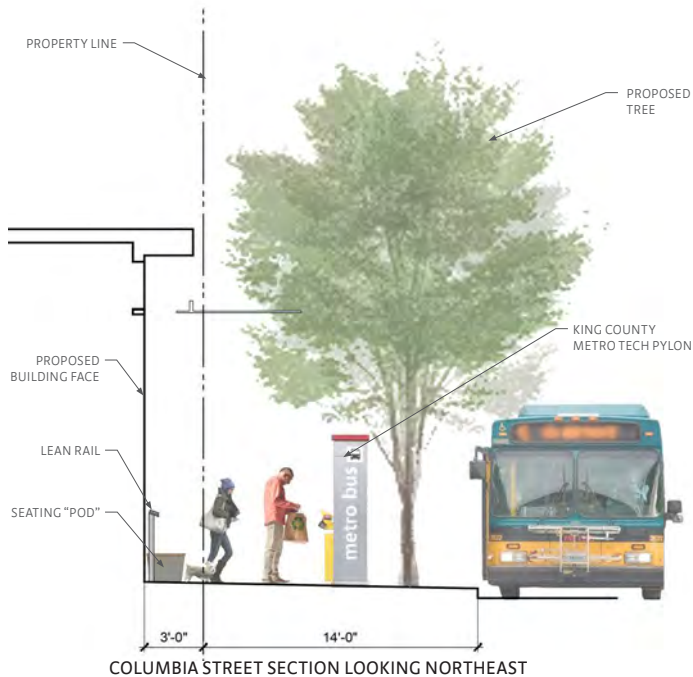
Section 02 / Ground Level Landscape Right of Way Sections



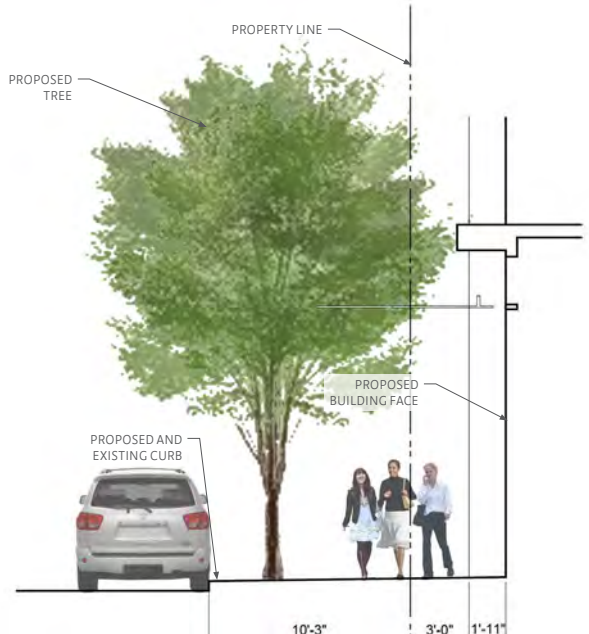
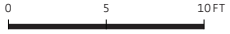
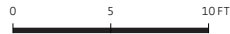
MARION ST SECTION NEAR ALASKAN WAY LOOKING NORTHEAST



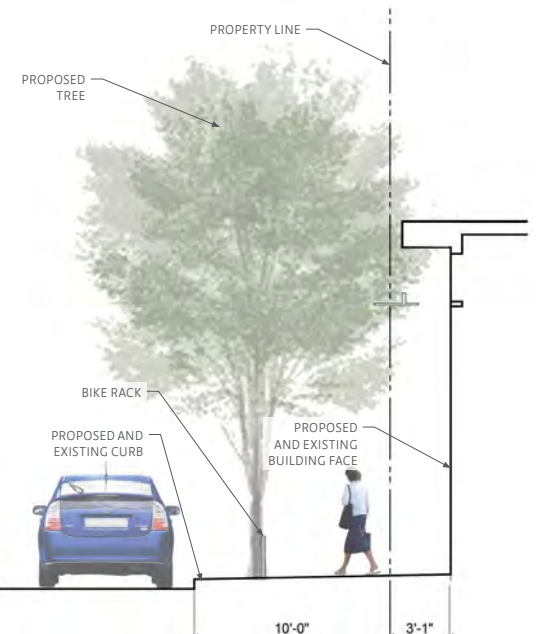
MARION ST SECTION NEAR WESTERN AVE LOOKING NORTHEAST



COLUMBIA STREET SECTION LOOKING NORTHEAST



WESTERN AVENUE SECTION NEAR COMCOLUMBIA ST LOOKING SOUTHEAST



WESTERN AVE SECTION NEAR MARION ST LOOKING SOUTHEAST

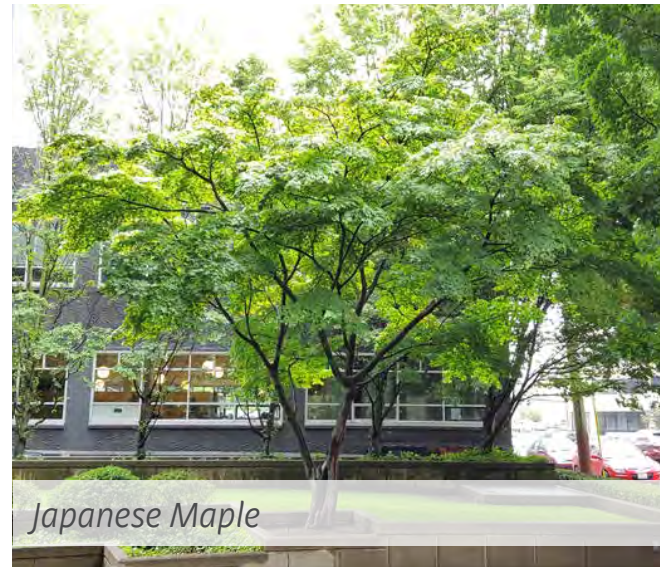


Section 02 / Landscape - Plantings

WESTERN AVE AND
COLUMBIA STREET
TREES



Accolade Elm



Japanese Maple



Japanese Maple

MARION STREET
SITE PLANTERS



Sword Fern



Asian Saber Fern



Japanese Tassel Fern



Maidenhair Fern



Salal



Beach strawberry



Small-fruited Bulrush



Swordleaf Rush

Section 02 / Landscape - Plantings

SHRUBS AND GROUNDCOVERS



Dwarf Northblue Highbush Blueberry



Beach strawberry



Beach Strawberry

GRASSES



Gold Tufted Hair Grass



Hameln Dwarf Fountain Grass



Autumn Moor Grass



Autumn Moor Grass

SEDUM



Sedum Color Mix

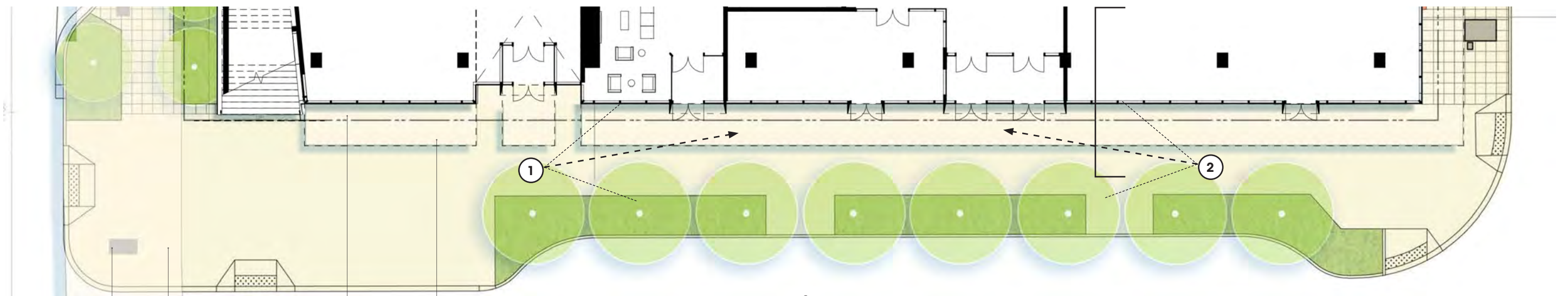
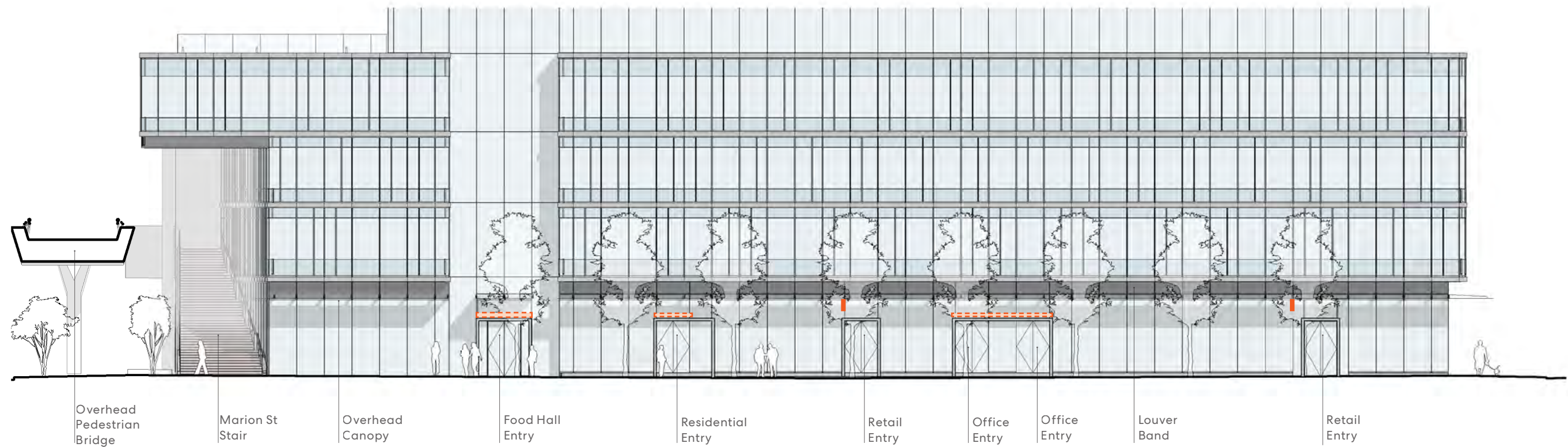


Sedum Color Mix



Sedum Color Mix

Section 02 / Street Level Design: Alaskan Way



Alaskan Way - the Waterfront

Bridge Column
Overhead Pedestrian Bridge
Property Line
Overhead Canopy

 potential signage locations



Section 02 / Street Level Design: Alaskan Way



 potential signage locations

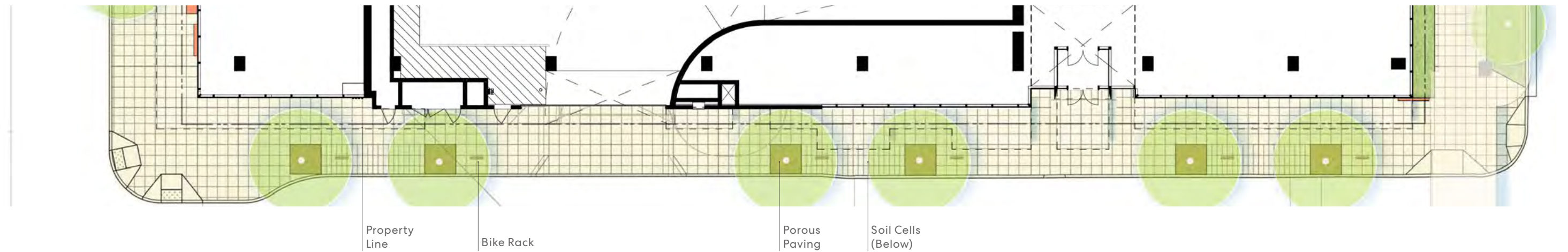
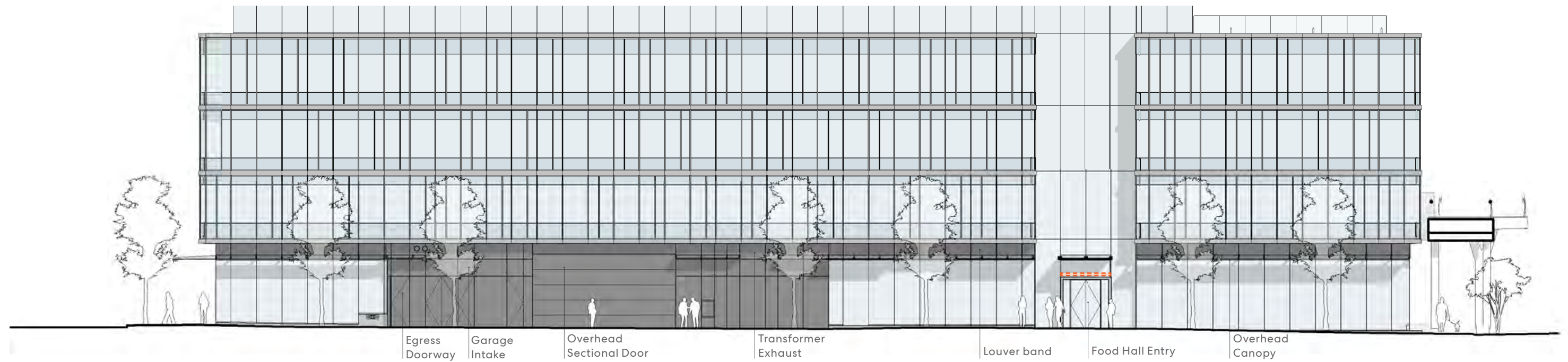


① View looking towards Marion St



② View looking towards Columbia St

Section 02 / Street Level Design: Western Ave



Western Avenue - the "Quiet" Retail Street

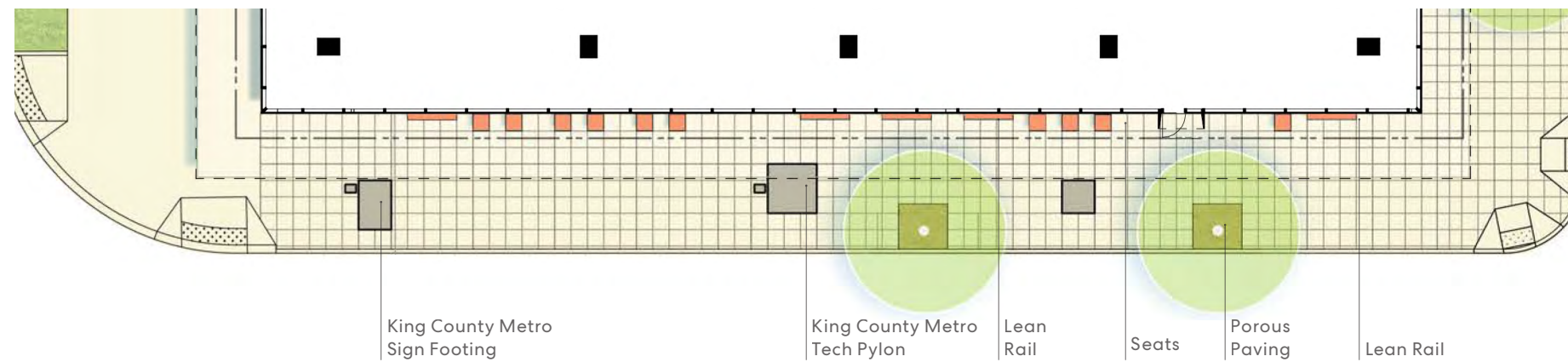
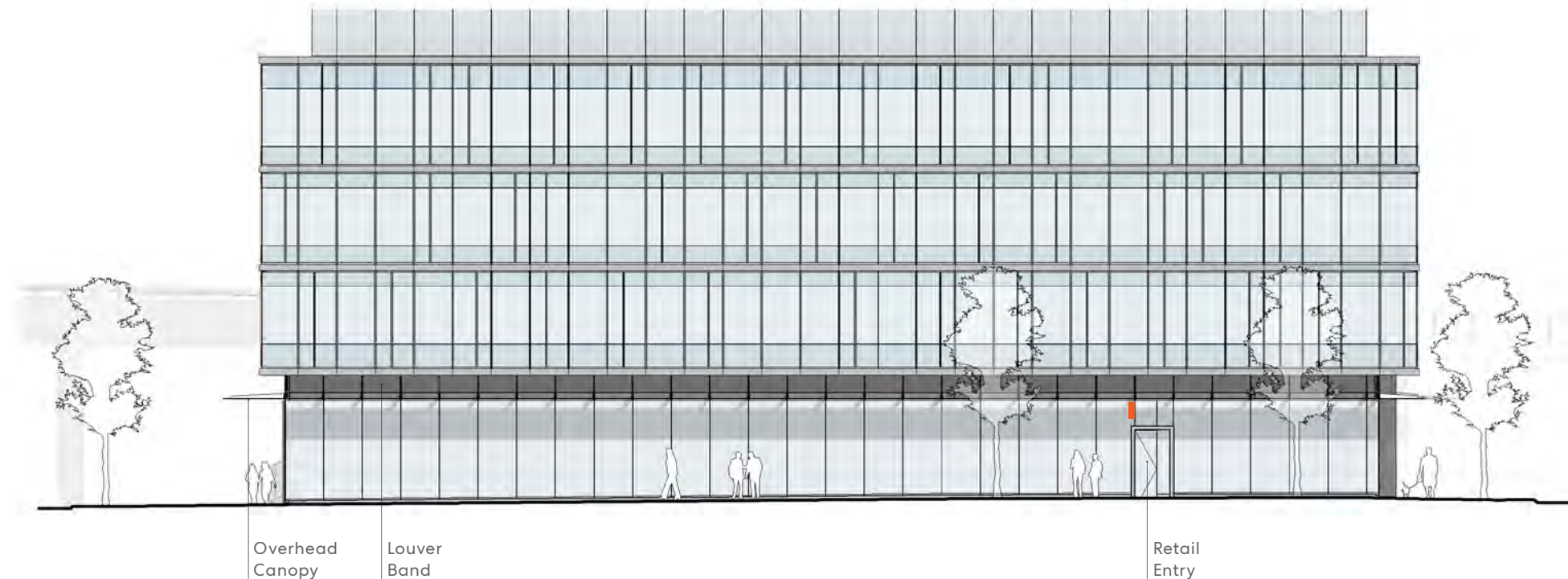
 potential signage locations



Section 02 / Street Level Design: Western Ave



Section 02 / Street Level Design: Columbia st.



Columbia Street - A Bustling Mobility Hub

 potential signage locations



Section 02 / Street Level Design: Columbia St.



 potential signage locations

Section 02 / Exterior Lighting

1.0 Lighting Goals

This chapter outlines broader goals and strategies for the lighting design approach for the functional, landscape and façade elements of the project. This chapter primarily addresses specifics related to lighting for exterior environment which influences luminaire optical selections, lamping, and final layouts. Lighting solutions proposed here are expected to meet quality, quantity, and operational criteria as set forth by the project's design team.

While the proposed lighting will be supportive of the anticipated functions and tasks, it will also be within the power and energy limits as per prevailing codes and fit within night-environment compliance.

Luminaires specified for the project will have full cut-off optics. Lighting Power Density (LPD) and the Lighting Controls for the project-wide Lighting will comply local codes. Wherever possible, light sources will be concealed within architectural and landscape features such as hand-rails, benches, canopies, etc. with a design goal to softly illuminate surfaces without revealing their location. Areas concerning public safety, security and way-finding, a more pronounced lighting emphasis will be provided.

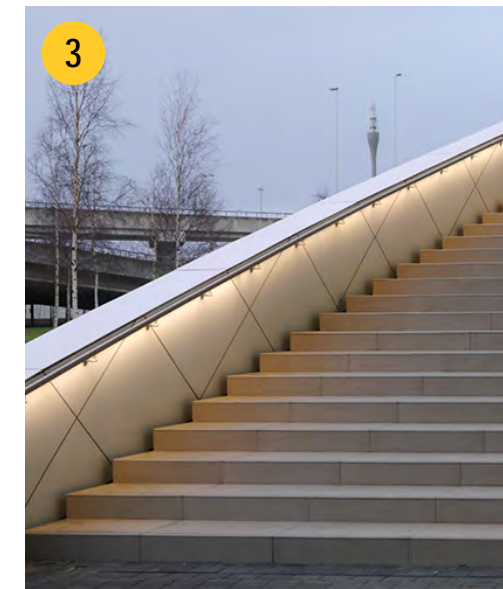
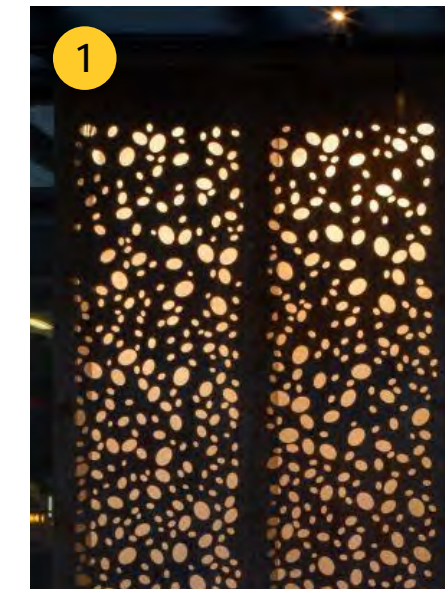


1.1.1 Level 01 Lighting Plan

- 1 Back-lit Public Art Installation for Visual interest and aid Wayfinding
- 2 Ceiling integrated down-lights
- 3 Railing integrated light
- 4 Canopy integrated general lighting for building perimeter illumination
- 5 Entry frame integrated lighting.
- 6 Wall mounted luminaire for areas illumination at the vehicular entry/exit point
- 7 Downlight at the Vestibules
- 8 Screen Wall integrated light for pedestrians on Marion St.

Section 02 / Exterior Lighting

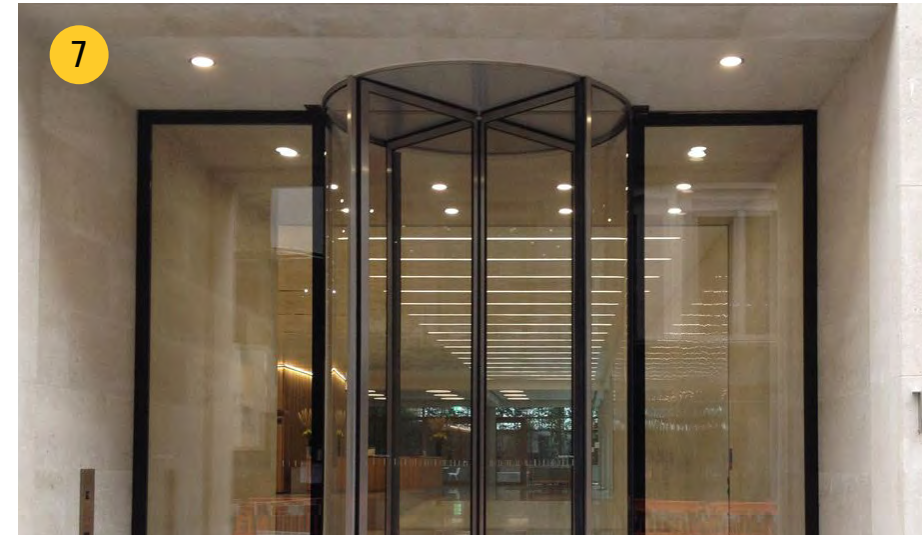
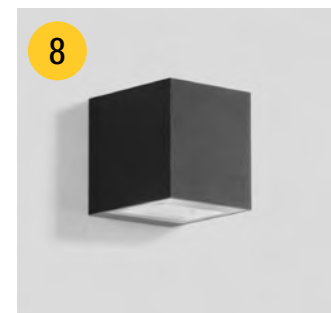
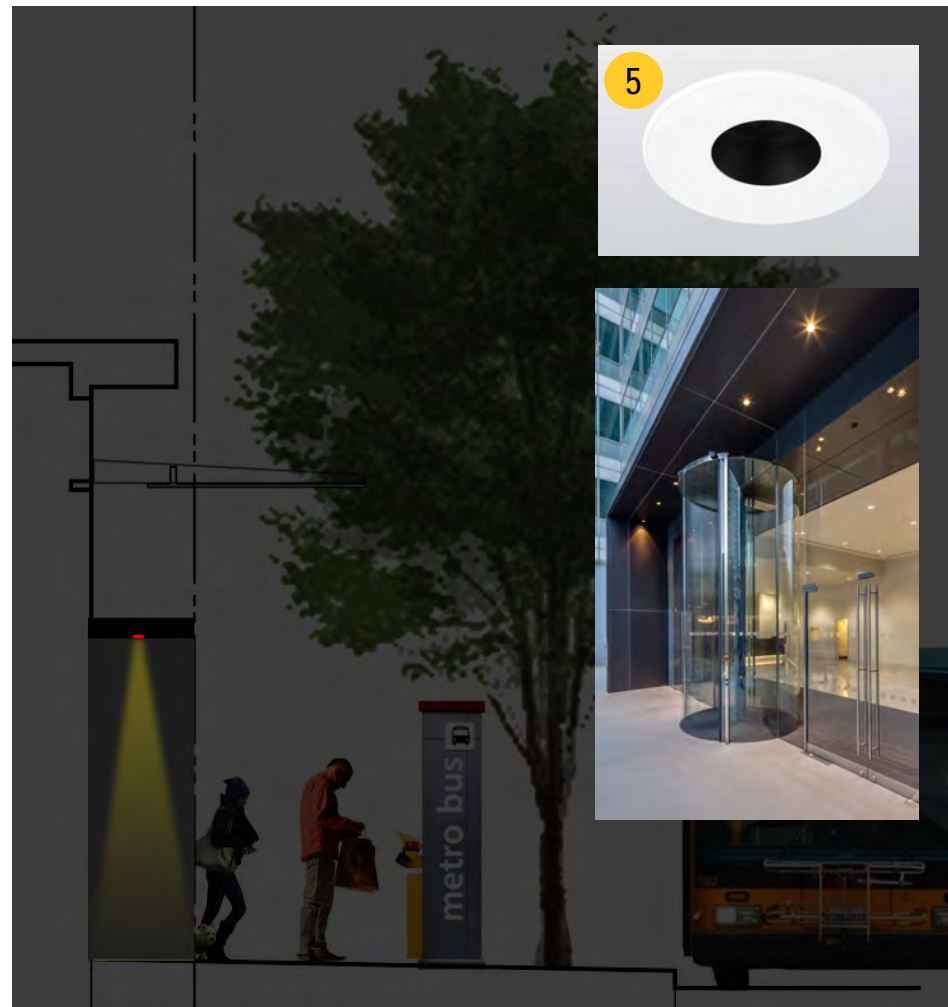
1.1.2 Level One - Grand Stair



Design Intent:

Being a public part of the development, this space must serve multiple visual needs. From the public safety standpoint, the space will need adequate illumination at the stairs. From an urban design standpoint, the space will complement the vibrant and dynamic aspects of the city life. While serving these needs, the lighting has to meet the stringent environmental and energy standards of light trace-pass and optimized energy consumption.

Section 02 / Exterior Lighting



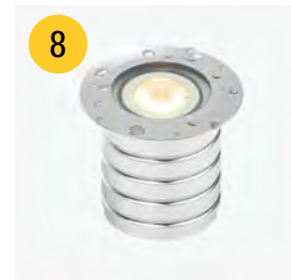
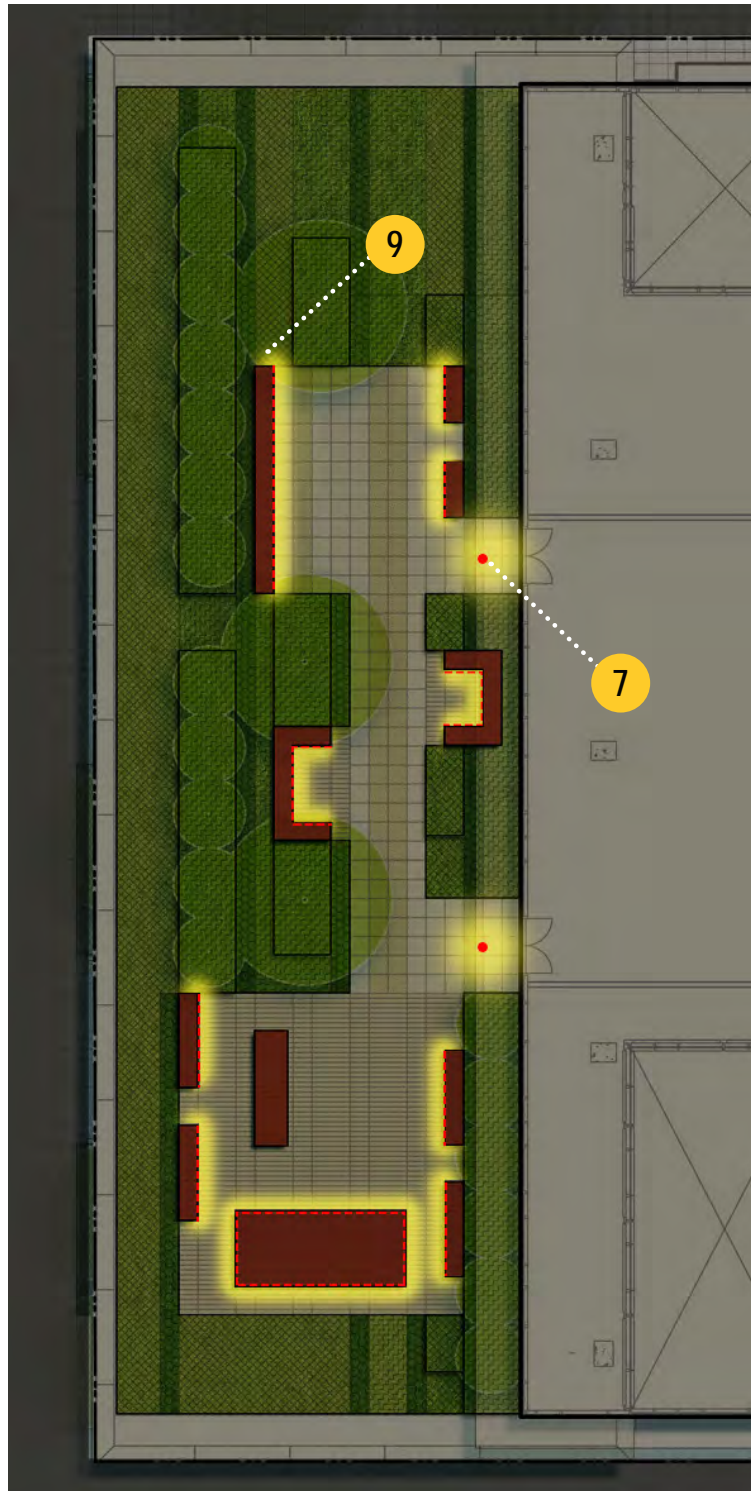
1.1.3 Level One - Perimeter

Design Intent:

Being a public part of the development, this space must serve multiple visual needs. From the public safety standpoint, the space will need adequate illumination at the building perimeter, while providing way-finding destinations to various entry/exit points. Lighting also has to meet the stringent environmental and energy standards of light trace-pass and optimized energy consumption.

Light element (4) will provide a general layer of ambient illumination all across the perimeter of the building. Luminaire will be visually concealed and integrated with the support brackets of the canopy. Light element (5) will serve as a marker-light at the entry/exit points while providing functional and higher levels of illumination at the floor plane. Light element (6) is a functional area luminaire that will provide higher illumination at the Vehicular Entry/Exit point of the building. Light element (7) will provide a general layer of ambient illumination all across the vestibules. On the Marion St., downlight (5) will be installed in the soffit on east side of the building, and surface mounted fixtures (8) integrated with the screen wall of the stair.

1.2.1 Level Five - Landscape

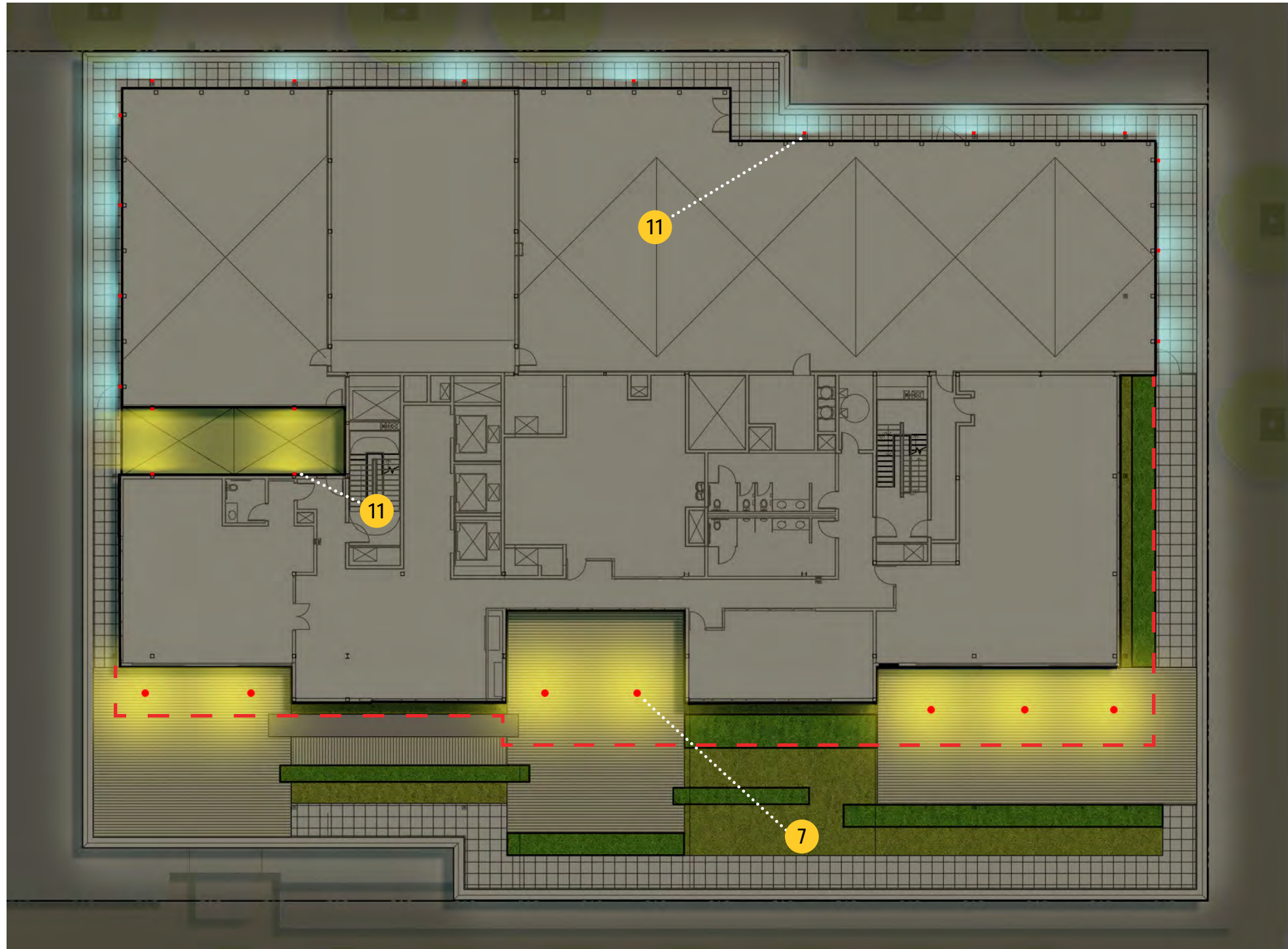


Design Intent:

Landscape lighting will be provided to preserve nocturnal experience and reduce light pollution. It will be characterized by low levels of soft illumination, mostly confined to floor level with light sources concealed from normal view of sight.

Light element (9) will be seamlessly integrated with the seating bench with a purpose to wash the floor surface, that serves as a general illumination for the space. Ceiling recessed downlight (7) will provide general illumination at the entry points of the building. Light element (8) is proposed at the Residential balconies, as a low intensity uplight integrated with the paving.

Section 02 / Exterior Lighting



1.3.1 Level 15 Penthouse

Design Intent:

Lighting for the tenant terrace will be integrated with the soffit to provide low level of general illumination to the space. More functional lighting will be provided to the service corridor of this floor that could be turned on only when required by the facilities personal.

Light element (7) is a low-glare , ceiling recessed luminaire. Where as light element (11) is a utilitarian wall mounted luminaire with full cut-off optics.



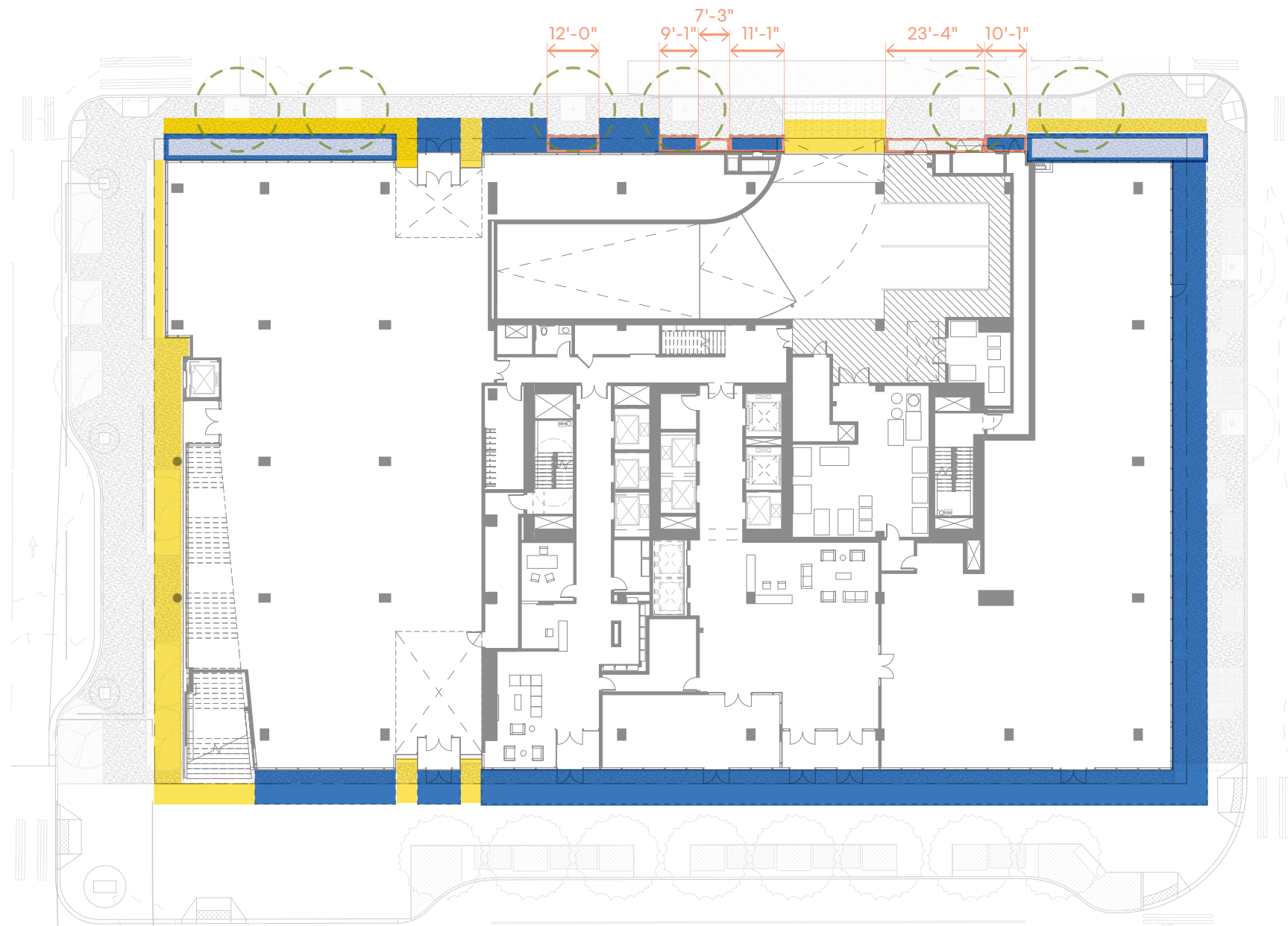
Section 02 / Exterior Lighting

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Section 02 / Departures

Overhead Weather Protection

Code Citation	Code Requirement	Proposed Design Departure	Rationale
23.49.018.B	Overhead weather protection shall have a minimum dimension of eight (8) feet measured horizontally from the building wall or must extend to a line two (2) feet from the curb line, whichever is less.	Along Western Avenue, overhead weather protection with a minimum depth of 8 feet from building face is required. At limited locations, overhead weather protection is eliminated where it would otherwise be required or a 4'-2" depth is proposed. Please see below for locations where a departure is requested and for areas where overhead weather protection is not required due to setbacks or landscape buffers per SMC 12.49.018B. The total length of facade where a departure is requested from overhead weather protection depth requirements is 42'-3". The total length of facade where a departure is requested from overhead weather protection all together is 30'-7".	Overhead weather protection less than 8 feet deep accommodates required street trees along Western that enhance the pedestrian environment per Downtown Design Guideline B3.3 . Trees are required to have a 5-foot wide canopy clearance, which would otherwise conflict with the overhead weather protection. In order to reduce conflicts between required overhead weather protection and tree canopies, portions of the facade have been set back in order to be exempted from overhead weather protection requirements, or the overhead weather protection depth has been reduced. There are two locations where overhead weather protection has been eliminated in order to accommodate SCL transformer exhaust louvers and garage intake louvers. The site is not abutted by an alley and efforts have been made to consolidate and reduce the impact of all building services along the street frontage per Downtown per Downtown Design Guideline E3.1 .



- 8'-0" overhead weather protection depth
- 5'-11" overhead weather protection depth
- 4'-2" overhead weather protection depth (departure requested)
- No overhead weather protection (departure requested)
- No overhead weather protection required per SMC 23.49.018A. The following portions of facade are exempt from overhead weather protection: areas located farther than five feet from street property line, areas separated from the street property line by a landscaped areas at least two feet in width, driveways into structures or loading docks

Section 02 / Departures

Facade Modulation

Code Citation	Code Requirement
23.49.058.B.2.c	The maximum length of a facade without modulation is prescribed in Table A for 23.49.058. This maximum length shall be measured parallel to each street lot line, and shall apply to any portion of a facade, including projections such as balconies, that is located within 15 feet of street lot lines.
	Table A for 23.49.058
Modulation requirements for DMC 170 zone	
0 to 60	No limit
Above 60	125

Proposed Design Departure

The code states that unmodulated lengths of facade within 15 feet of the property line cannot exceed 125 feet in length above 60 feet.

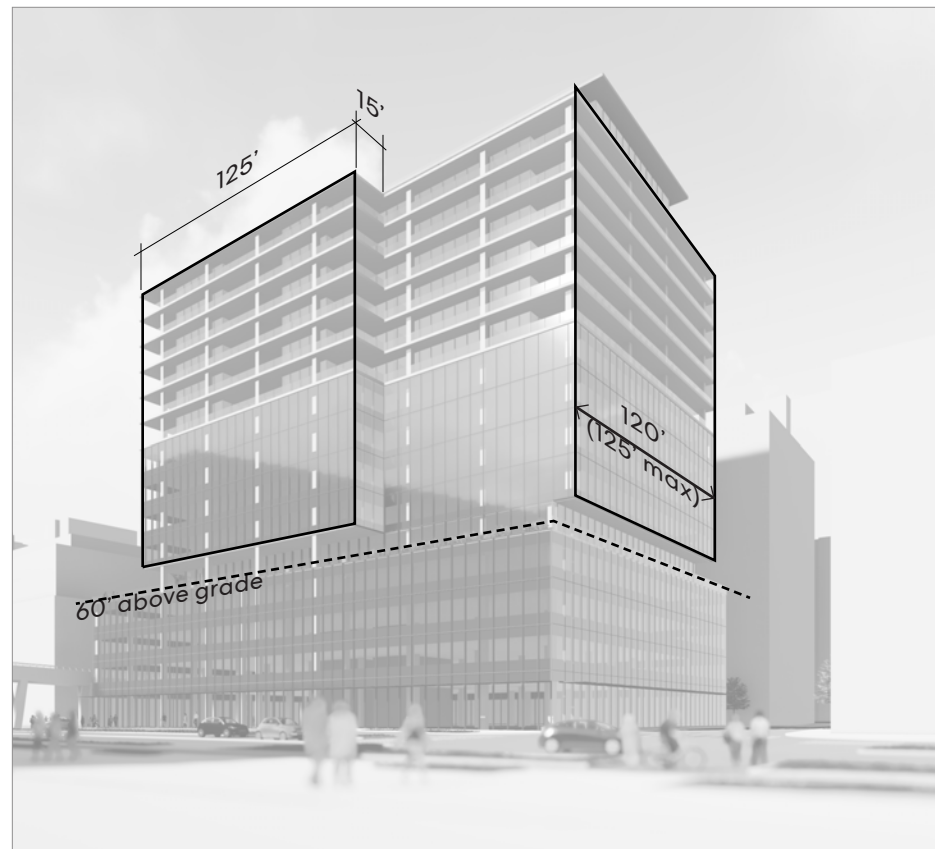
Street	Elevation	Length of Unmodulated Facade
Alaskan Way	60'; 158'; 190'	188'; 130'; 130'
Western Ave	60'; 147'; 168'	188'; 130'; 130'
Columbia St	60'; 158'; 190'	136'; 147'; 147'

Depths of modulations are shown on design plans in section 03.

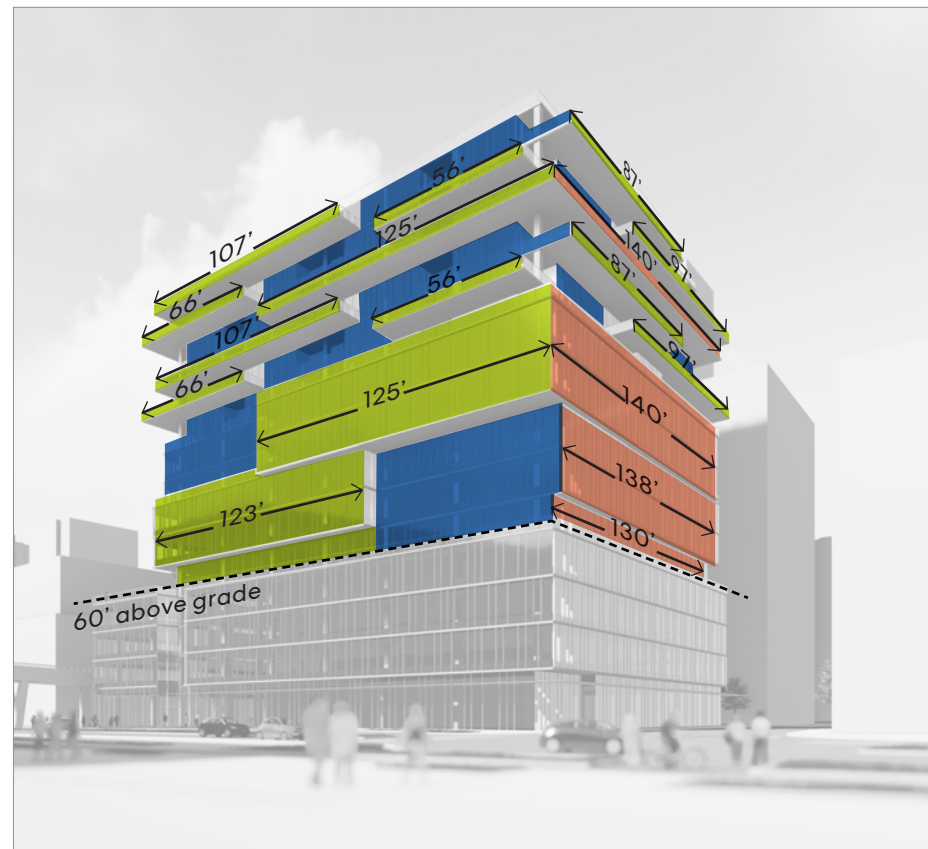
Rationale

The highly modulated form meets the intent of the land use code and **Downtown Design Guidelines A2 and B4** through creation of a dynamic and unique facade. Columns are located inboard of the skin at the office volumes which creates the opportunity for more modulation than an extruded form provides.

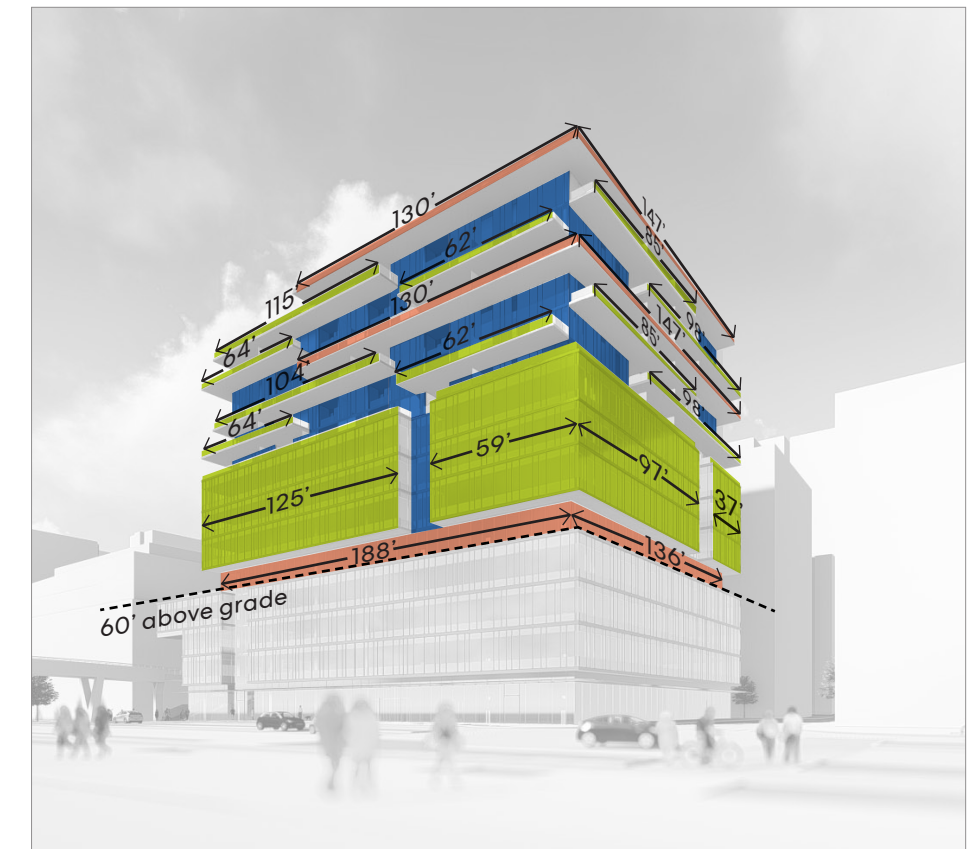
Some of the zones of non-compliance are marginally above the 125' length limit due to alignments with other elements of the facade in the interest of **Downtown Design Guideline B4**. Overall, the highly modulated form meets the intent of the upper level development standards and creates more visual interest than shown in the code compliant form.



Code compliant form



EDG form with departures (Western Ave elevation is similar to Alaskan Way elevation)



Proposed form with departures (Western Ave elevation is similar to Alaskan Way elevation)

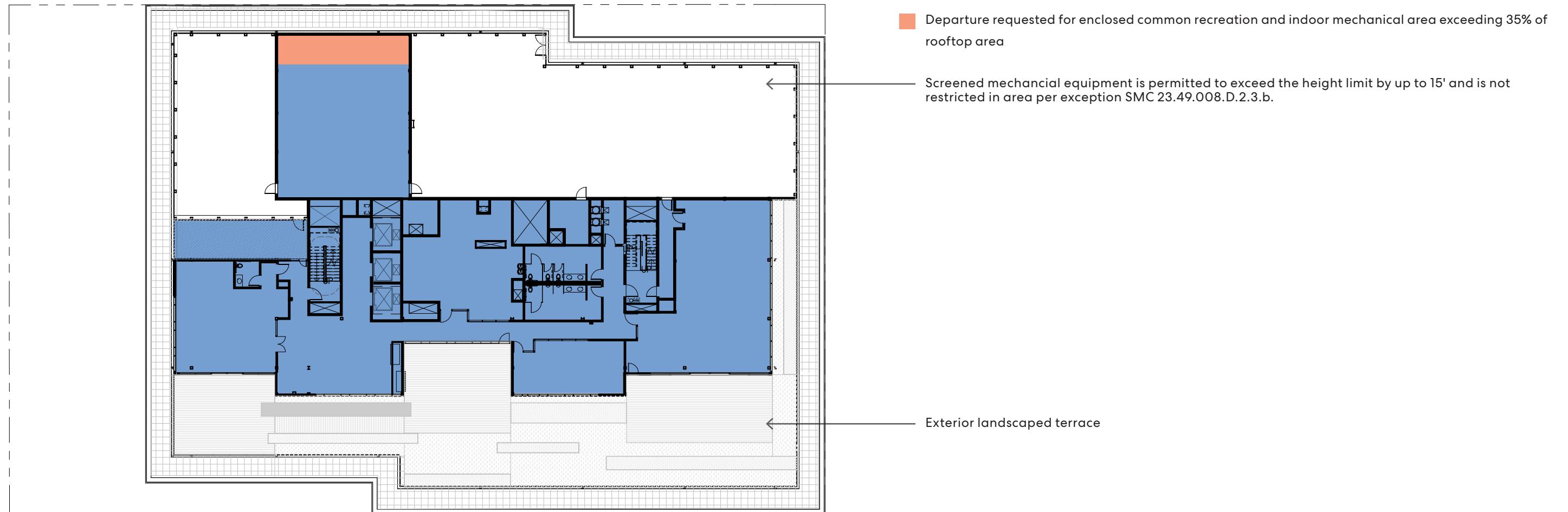
- Length of unmodulated facade exceeds maximum of 125'
- Modulated areas
- Length of unmodulated facade is less than maximum of 125'

Section 02 / Departures

Rooftop Coverage

Code Citation	Code Requirement	Proposed Design Departure	Rationale
23.49.008.D.2	<p>The following rooftop features are permitted up to the heights indicated below, as long as the combined coverage of all rooftop features, whether or not listed in this subsection 23.49.008.D.2, does not exceed... 35 percent of the roof area for other structures: solar collectors, stair penthouses, play equipment, covered or enclosed common recreation, mechanical equipment, and wind turbines. Elevator penthouses are permitted up to 23-25 feet above the applicable height limit depending on elevator cab height.</p> <p>Screened mechanical equipment is permitted to exceed the maximum percentage of the combined coverage of all rooftop features as provided in subsection SMC 23.49.008.D.3.b.</p>	<p>Enclosed common recreation and indoor mechanical area occupies 37% of rooftop area, which exceeds the 35% allowed.</p> <p>Total Rooftop Area = 28,651 sf</p> <p>Rooftop Area Permitted to Exceed Height Limit (35%) = 10,028 sf</p> <p>Proposed Enclosed Rooftop Area Exceeding Height Limit (37%) = 10,601 sf</p> <p>Area for which Departure is requested (2%) = 573 sf</p>	<p>The HVAC and plumbing system requirements for a mixed-use building are greater than would be required for either a single-use office or single-use residential building. Therefore, the rooftop space required for indoor mechanical equipment is larger.</p> <p>Outdoor mechanical equipment is screened and indoor mechanical area is consolidated to the greatest extent possible in order to enhance the skyline (Downtown Design Guideline A2) and organize the fifth facade.</p>

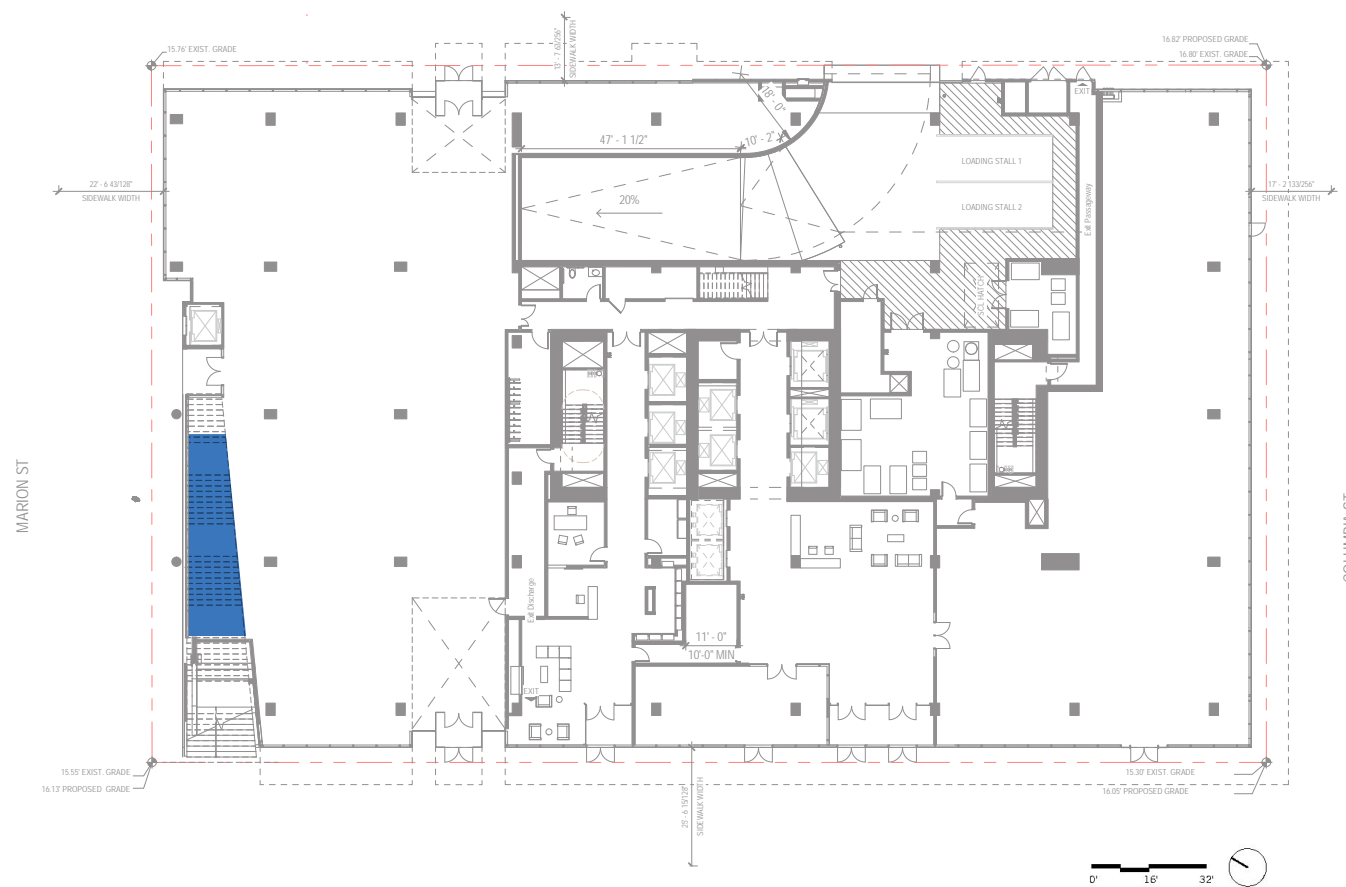
Rooftop Coverage Diagram - Penthouse Level



Section 02 / Departures

Exemptions from FAR Calculations

Code Citation	Code Requirement	Proposed Design Departure	Rationale
23.49.011.B.1.b.1)	The street level of the structure containing the exempt space has a minimum floor-to-floor height of 13 feet, except that in the DMC 170 zone the street level of the structure containing the exempt space has a minimum floor-to-floor height of 18 feet;	A minimum 18 foot floor-to-floor height is required at FAR exempt street level use areas in the DMC-170 zone. 464 sf of retail space beneath the proposed green street improvement public stair that provides pedestrian access from the marion street bridge to the sidewalk at Alaskan Way has a floor-to-floor height of between 7'-6" and 17'-11". A departure is proposed to make this space with less than 18'-0" floor-to-floor exempt from FAR.	The Marion Street Stair and elevator are public amenities providing connection between the pedestrian bridge and the sidewalk that promote pedestrian interaction at the ground level (Downtown Design Guideline C1). Retail space beneath the stair is maximized, but due to the presence of the stair, has a floor to floor of less than 18 feet. The retail space meets the intent of creating engaging space at grade.



■ Floor-to-floor height is between 7'-6" and 17'-11"
(Departure requested from SMC 23.49.011.B.1.B.1)



Section 02 / Departures

Blank Facade

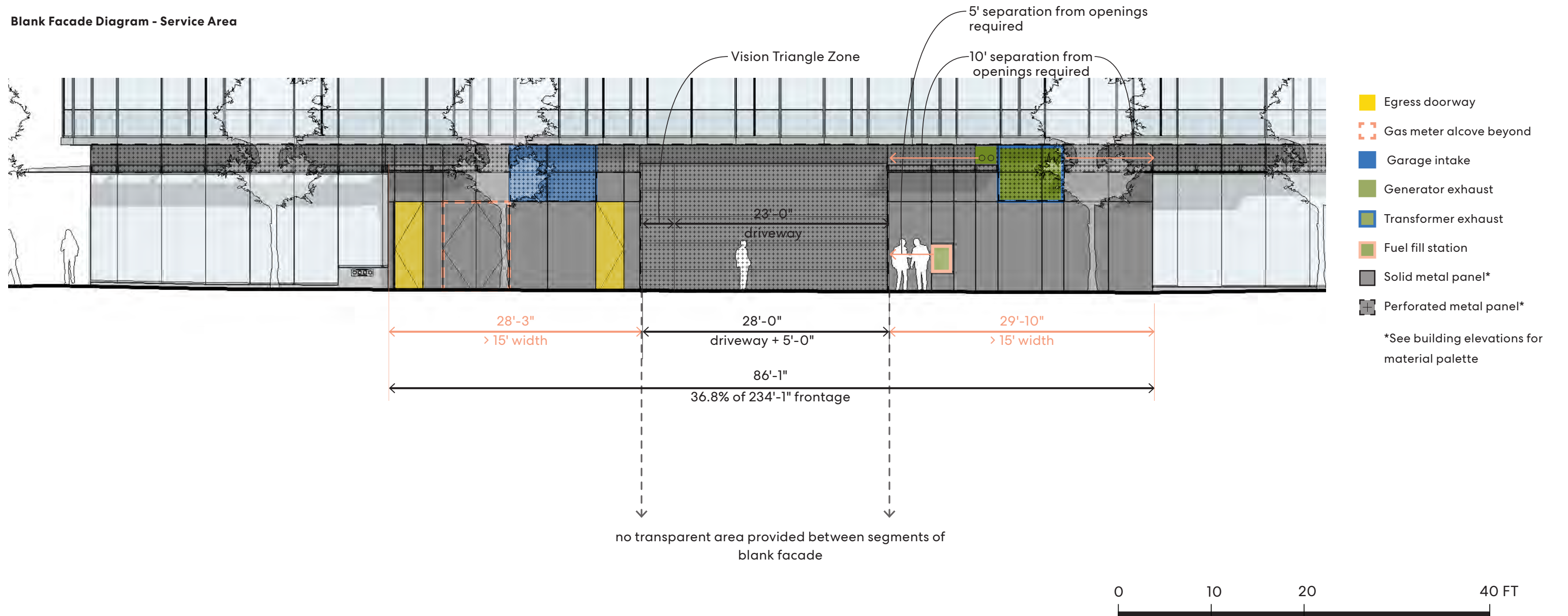
Code Citation	Code Requirement
23.49.056.D.2.a	Blank facades shall be no more than 15 feet wide except segments with garage doors may exceed a width of 15 feet and may be as wide as the driveway plus five feet. Blank facade segment width may be increased to 30 feet if a director in a Type I decision determines that the facade segment is enhanced by features with visual interest such as architectural detailing, artwork, landscaping, or similar features. All portions of blank facade shall be separated by transparent areas at least 2 feet wide. The total length of blank facade including garage doors shall not exceed 40% of the facade length.

Proposed Design Departure
Alaskan Way and Western Ave are Class I Pedestrian Streets, and Marion is Green Street, therefore blank facades along these streets shall be no more than 15 feet and must be separated by transparent areas at least 2 feet wide. A departure is requested from the requirement for transparent areas at least 2 feet wide.

Requested Type I Decision
Alaskan Way and Western Ave are Class I Pedestrian Streets, and Marion is Green Street, therefore blank facades along these streets shall be no more than 15 feet. Two segments of blank facade without a garage door along Western Avenue exceed 15 feet in length at 28'-3" in length and 29'-10" in length. A Type I Decision to increase blank facade segment width to 30 feet is requested.

Rationale
Blank facade segments along Western Avenue accommodate required building services, and cannot be reduced further in size due to required clearances (**Downtown Design Guideline E3**). Efforts have been made to consolidate these services so as to minimize impacts to adjacent retail space and the pedestrian experience along Western Ave. To minimize negative visual impact, the blank facade segment itself will be comprised of an articulated panelized system that integrates with the material palette of the larger building (**Downtown Design Guidelines B4,C2**). The placement of the blank facade segment along Western Avenue is necessitated by the lack of an alley on the block, and in order to minimize curb cut impacts (**Downtown Design Guidelines E1**).

Blank Facade Diagram - Service Area

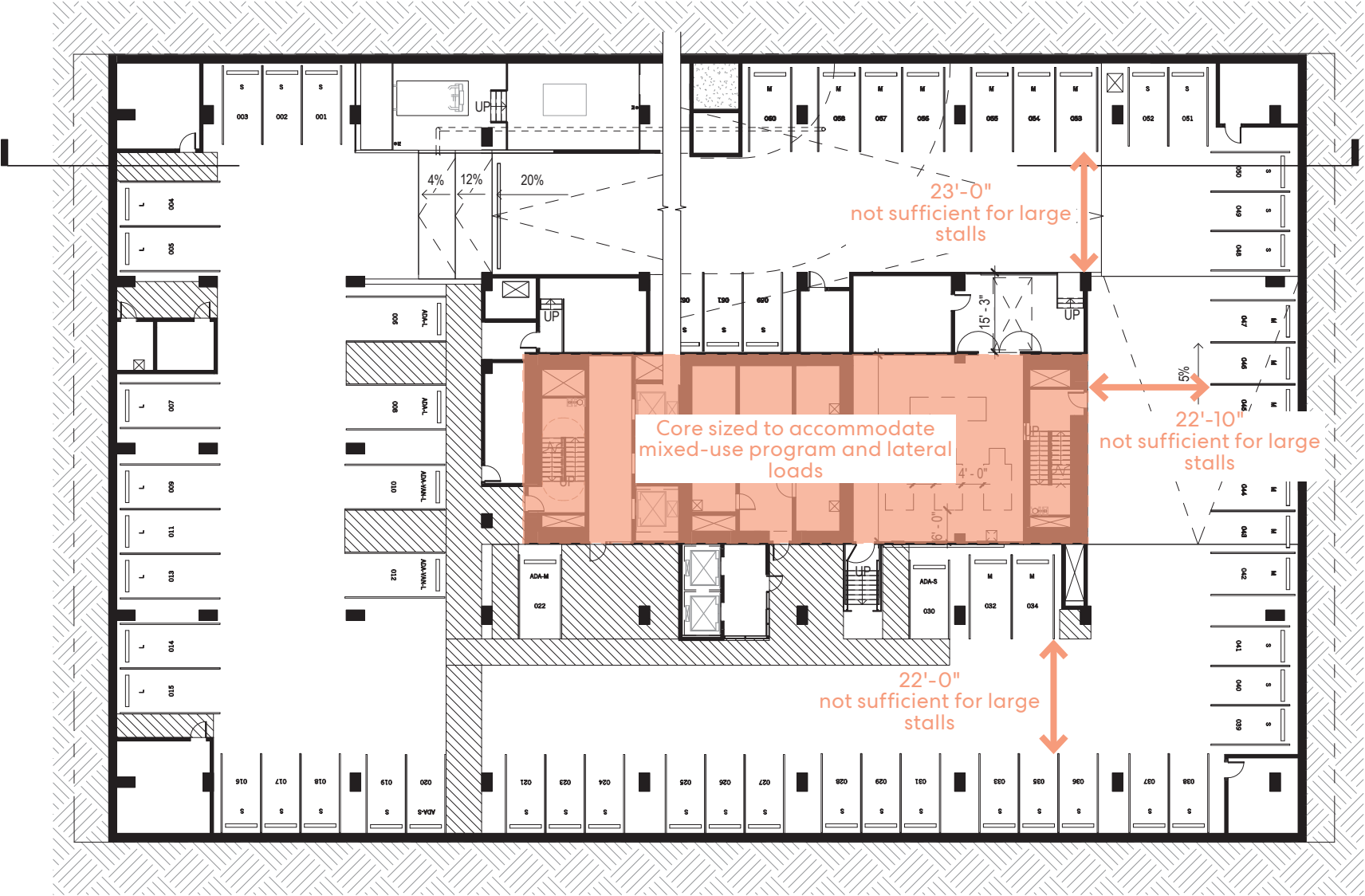


Section 02 / DEPARTURES

Parking Space and Access Standard: Stall Ratio

Code Citation	Code Requirement	Proposed Design Departure	Rationale
23.54.030.B.2.c	When 20 or more parking spaces are provided, a minimum of 35 percent of the parking spaces shall be striped for small vehicles. The minimum required size for small parking spaces shall also be the maximum size. A maximum of 65 percent of the parking spaces may be striped for small vehicles. A minimum of 35 percent of the spaces shall be striped for large vehicles.	The project provides 43 large vehicle spaces where 60 spaces are required. The project also proposes 47 medium vehicles and 83 small vehicle spaces as part of the nonresidential parking mix.	Per the Seattle Municipal Code, no parking stalls are required to be provided. The stalls that are provided are not in the ratios of small, medium and large per SMC 23.54. Due to the configuration of the garage as a result of mitigation of complex below grade conditions, the size of the core needed for structural purposes, and to accommodate a mixed-use building activating the neighborhood (Downtown Design Guideline B1.2), there is not sufficient depth at all sides of the garage to provide the required number of large stalls.

Parking Space Departure - P1 Parking Plan



Section 03 / Updated Massing



SW Corner



SE Corner



NW Corner

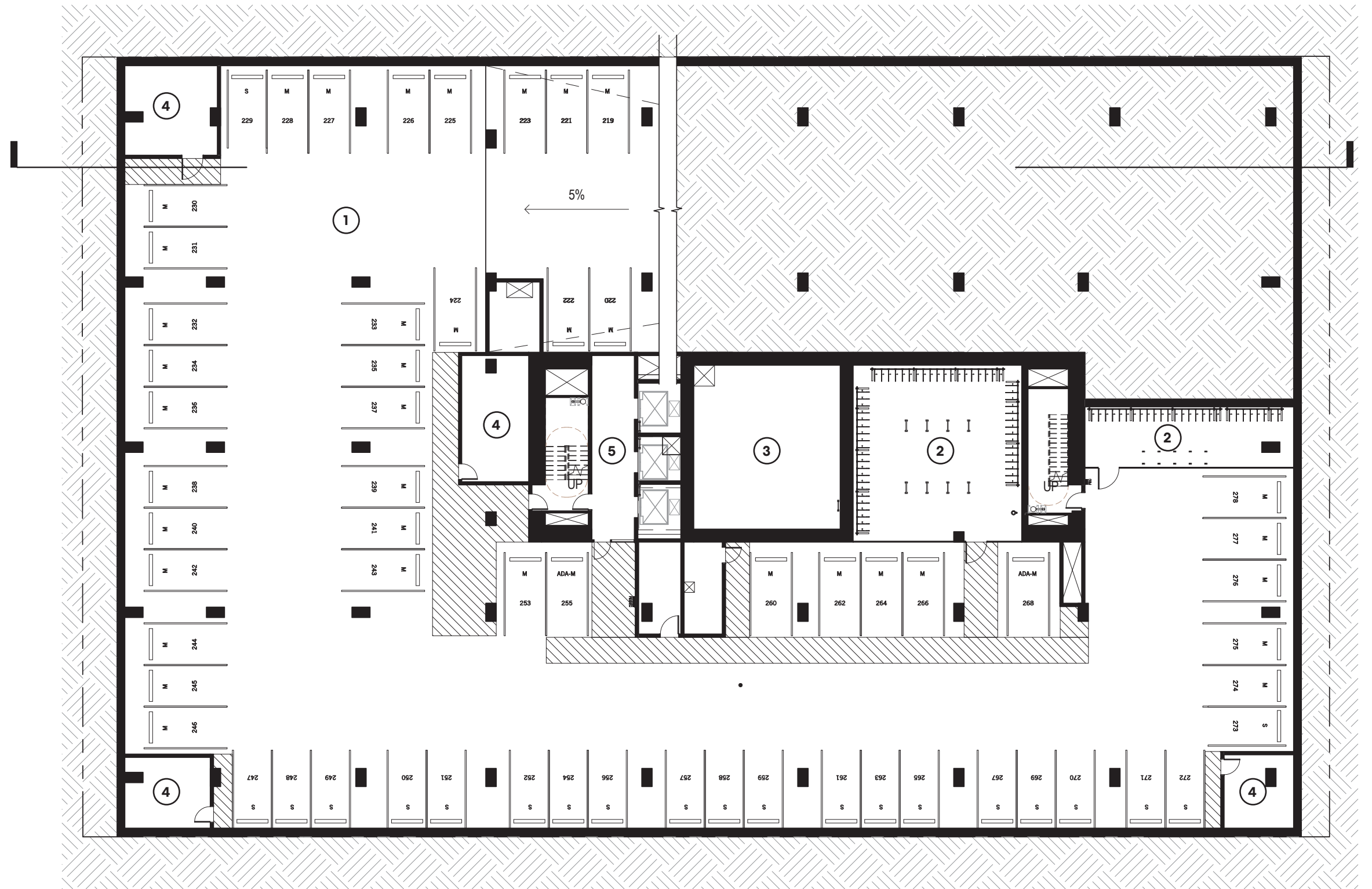


NE Corner

Section 03 / Plans

Floor Plan - P4

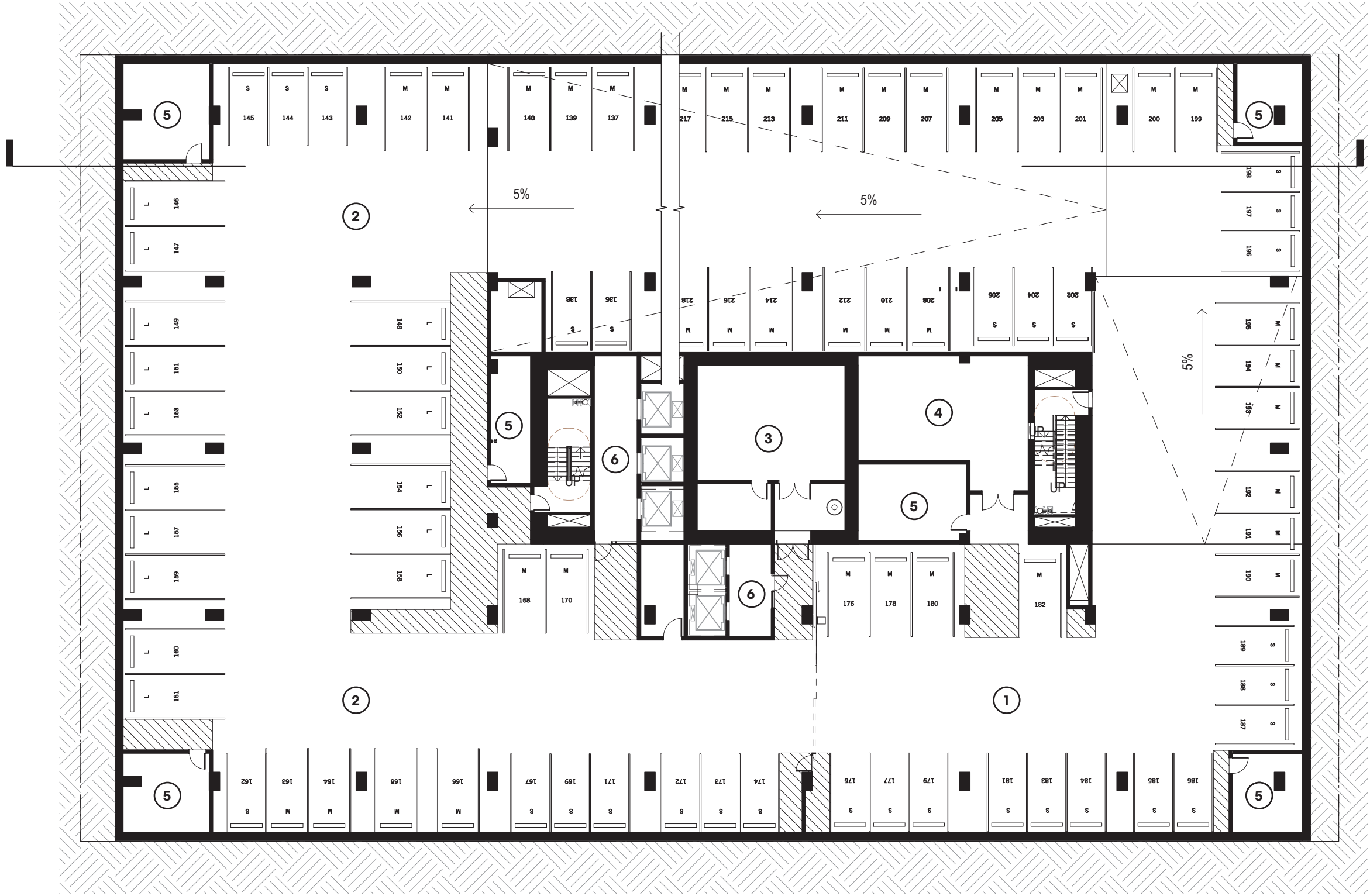
- 1 Residential Parking
- 2 Residential Bike Storage
- 3 Fire Tank
- 4 Storage
- 5 Elevator Lobby



Section 03 / Plans

Floor Plan - P3

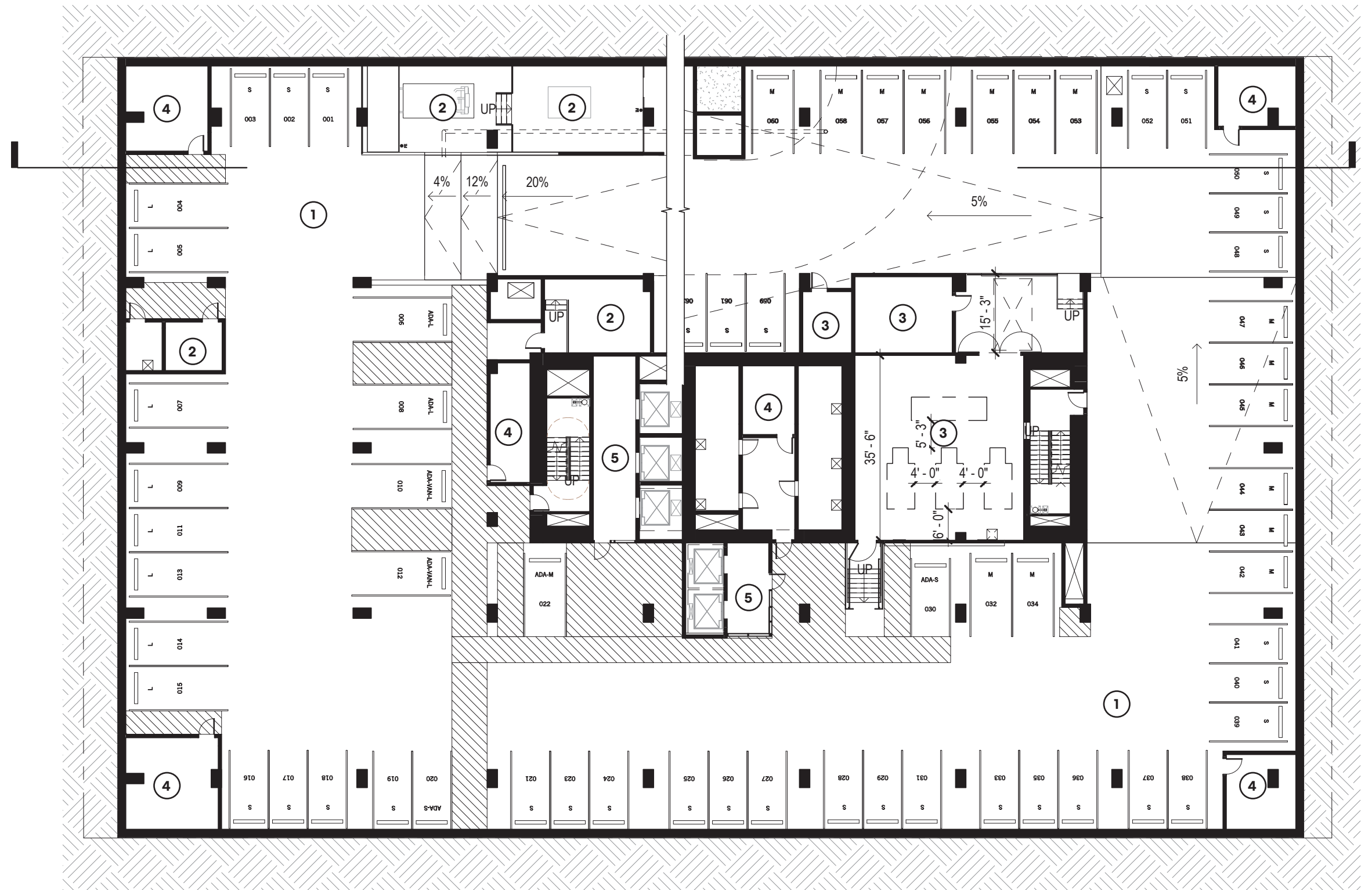
- 1 Residential Parking
- 2 Non-Residential Parking
- 3 Fire Pump Room
- 4 Mechanical Room
- 5 Storage
- 6 Elevator Lobby



Section 03 / Plans

Floor Plan - P1

- 1 Non-Residential Parking
- 2 Mechanical Room
- 3 Electrical Room
- 4 Storage
- 5 Elevator Lobby



Section 03 / Plans

Floor Plan - L01

- 1 Retail Entry
- 2 Office Entry
- 3 Residential Entry
- 4 Retail
- 5 Retail Lobby
- 6 Office Lobby
- 7 Residential Lobby
- 8 Electrical Room
- 9 Trash & Recycling Storage
- 10 Loading

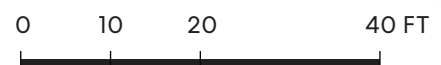


15.76' EXIST. GRADE

16.82' PROPOSED GRADE
16.80' EXIST. GRADE

15.55' EXIST. GRADE
16.13' PROPOSED GRADE

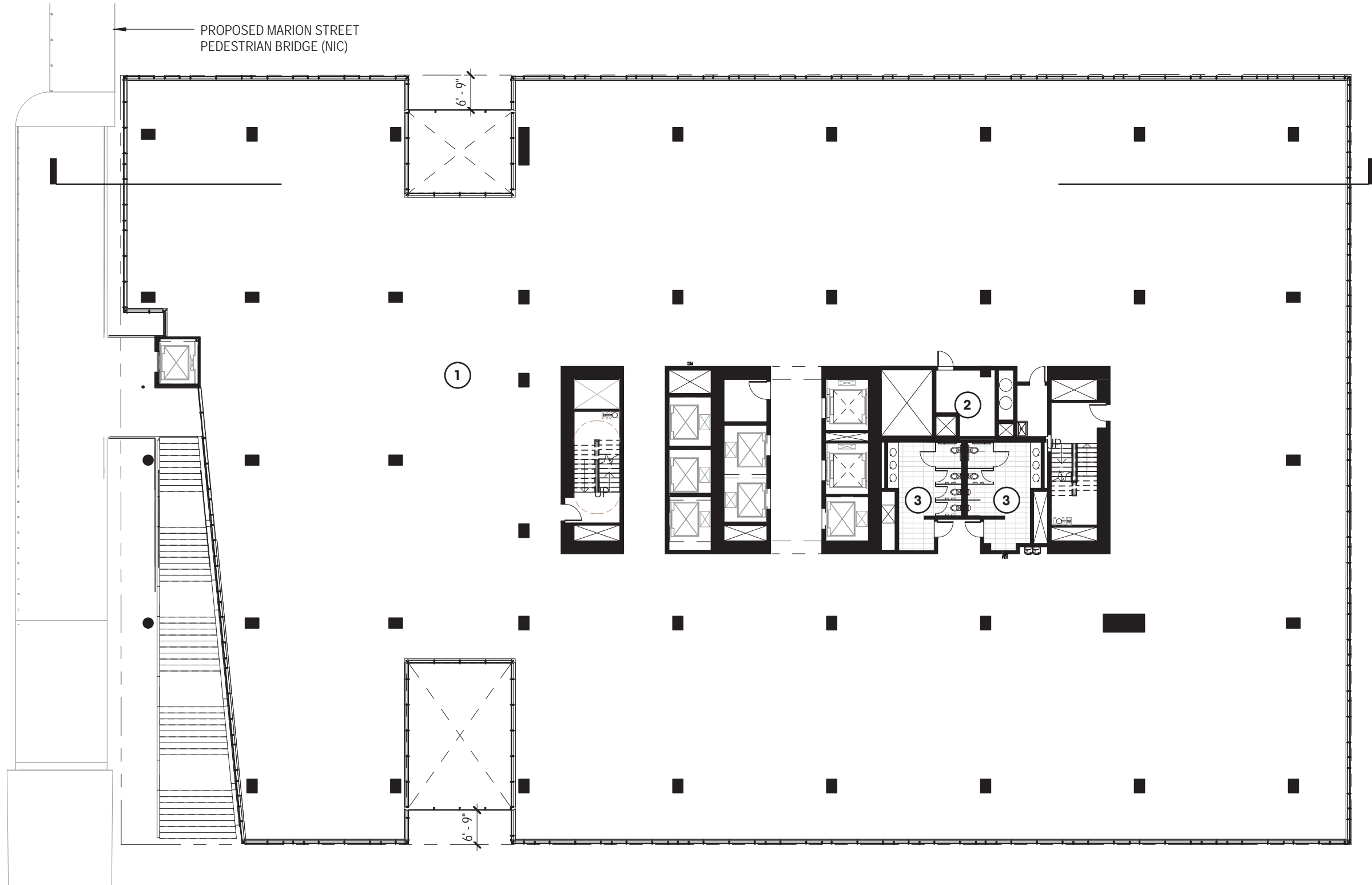
15.30' EXIST. GRADE
16.05' PROPOSED GRADE



Section 03 - Plans

Floor Plan - L02

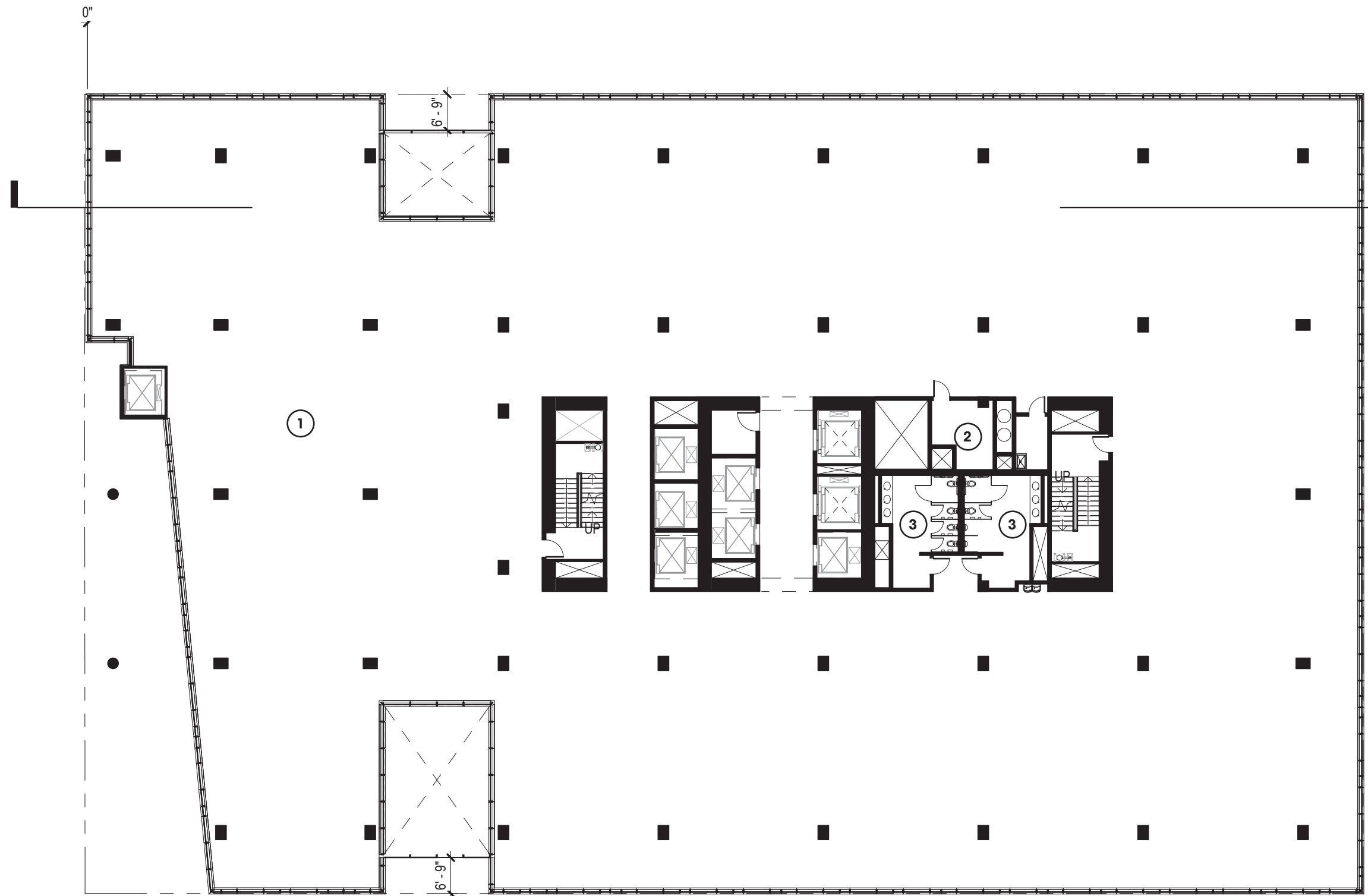
- 1 Office
- 2 Electrical Room
- 3 Restrooms



Section 03 / Plans

Floor Plan - L03

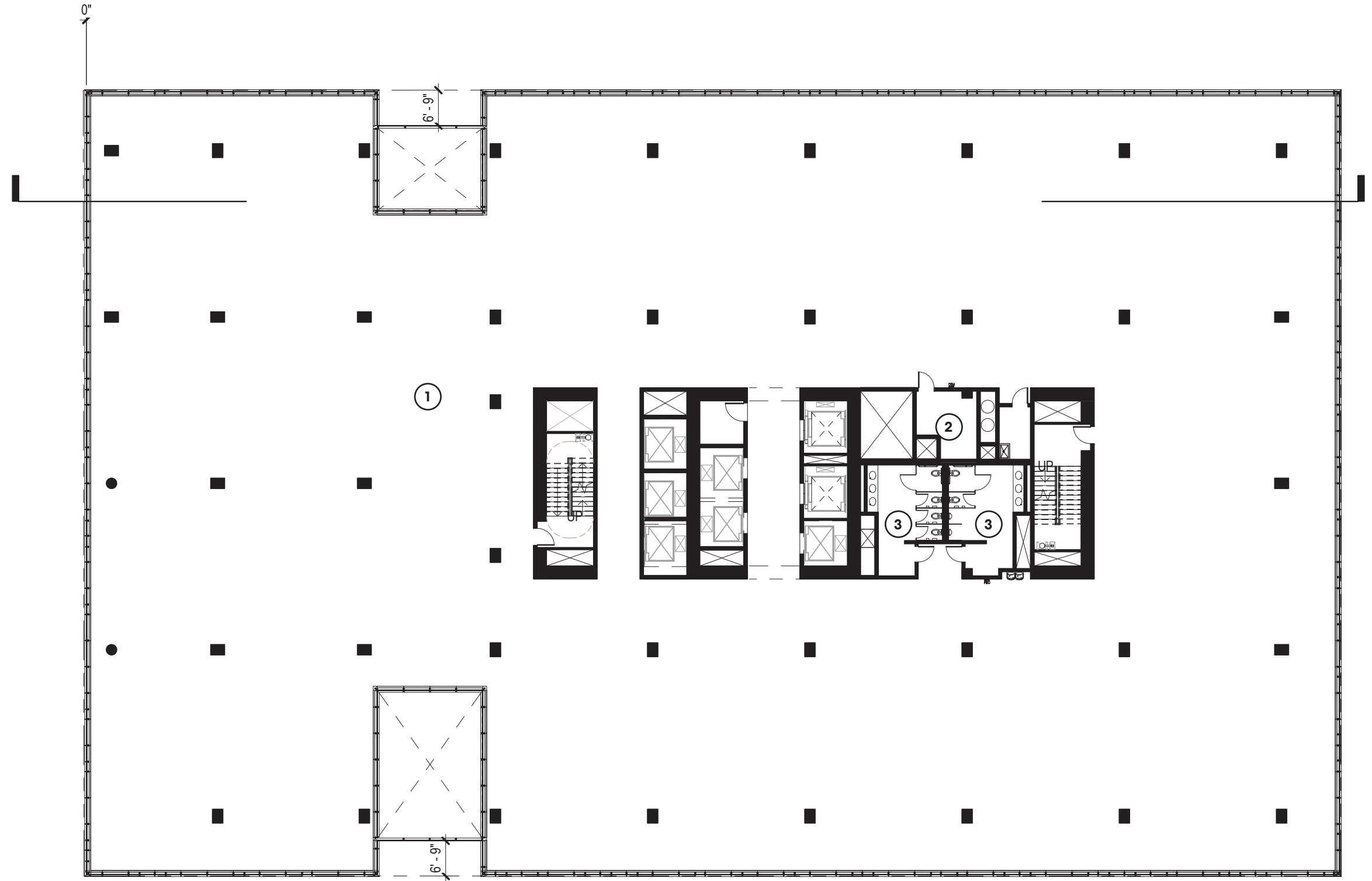
- 1 Office
- 2 Electrical Room
- 3 Restrooms



Section 03 / Plans

Floor Plan - L04

- 1 Office
- 2 Electrical Room
- 3 Restrooms



Section 03 / Plans

Floor Plan - L05

- 1 Office
- 2 Electrical Room
- 3 Restrooms
- 4 Terrace



Section 03 / Plans

Floor Plan - L06/L07/L08

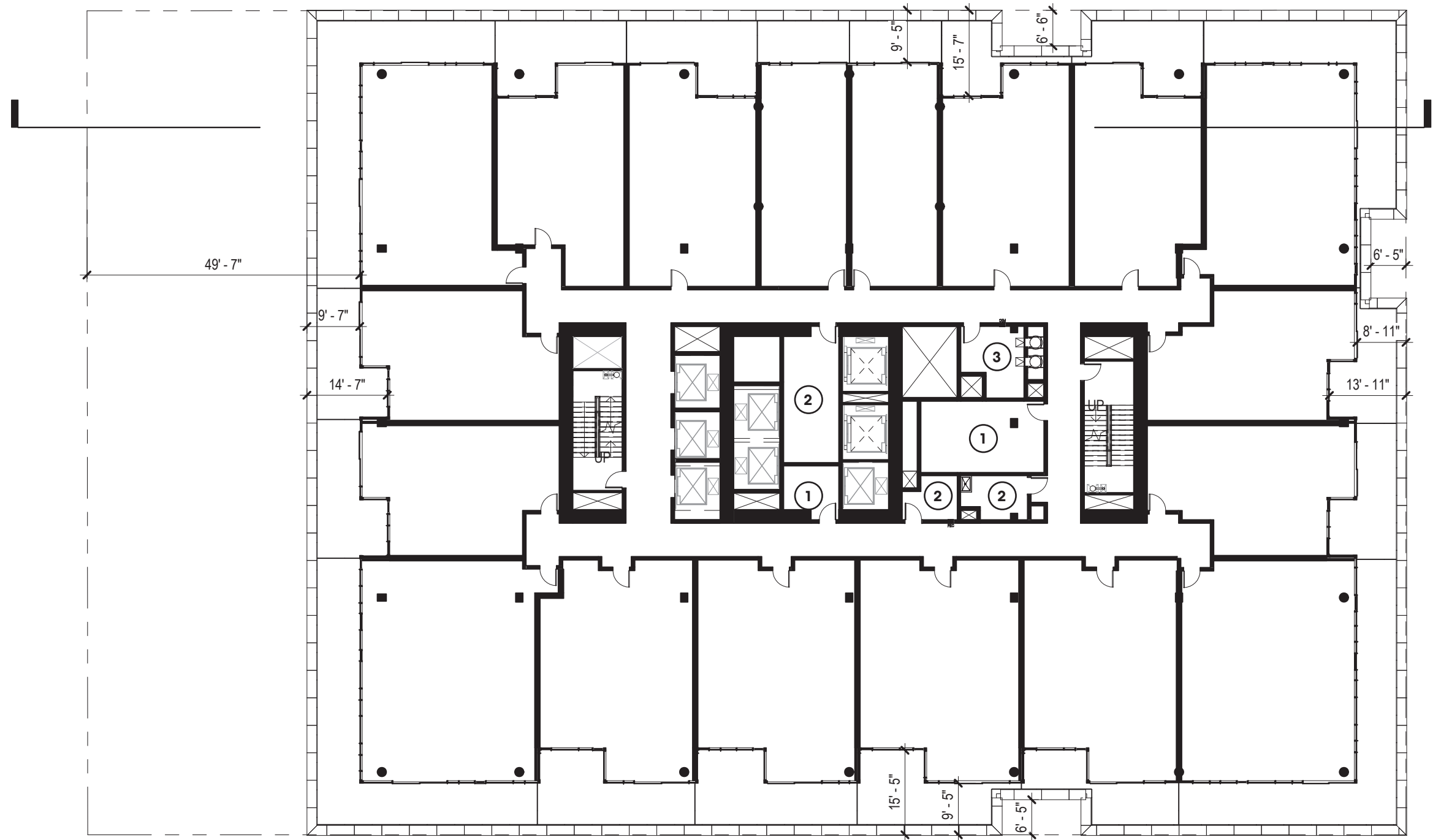
- 1 Office
- 2 Electrical Room
- 3 Restrooms



Section 03 / Plans

Typ. Residential Floor Plan - L09-L14

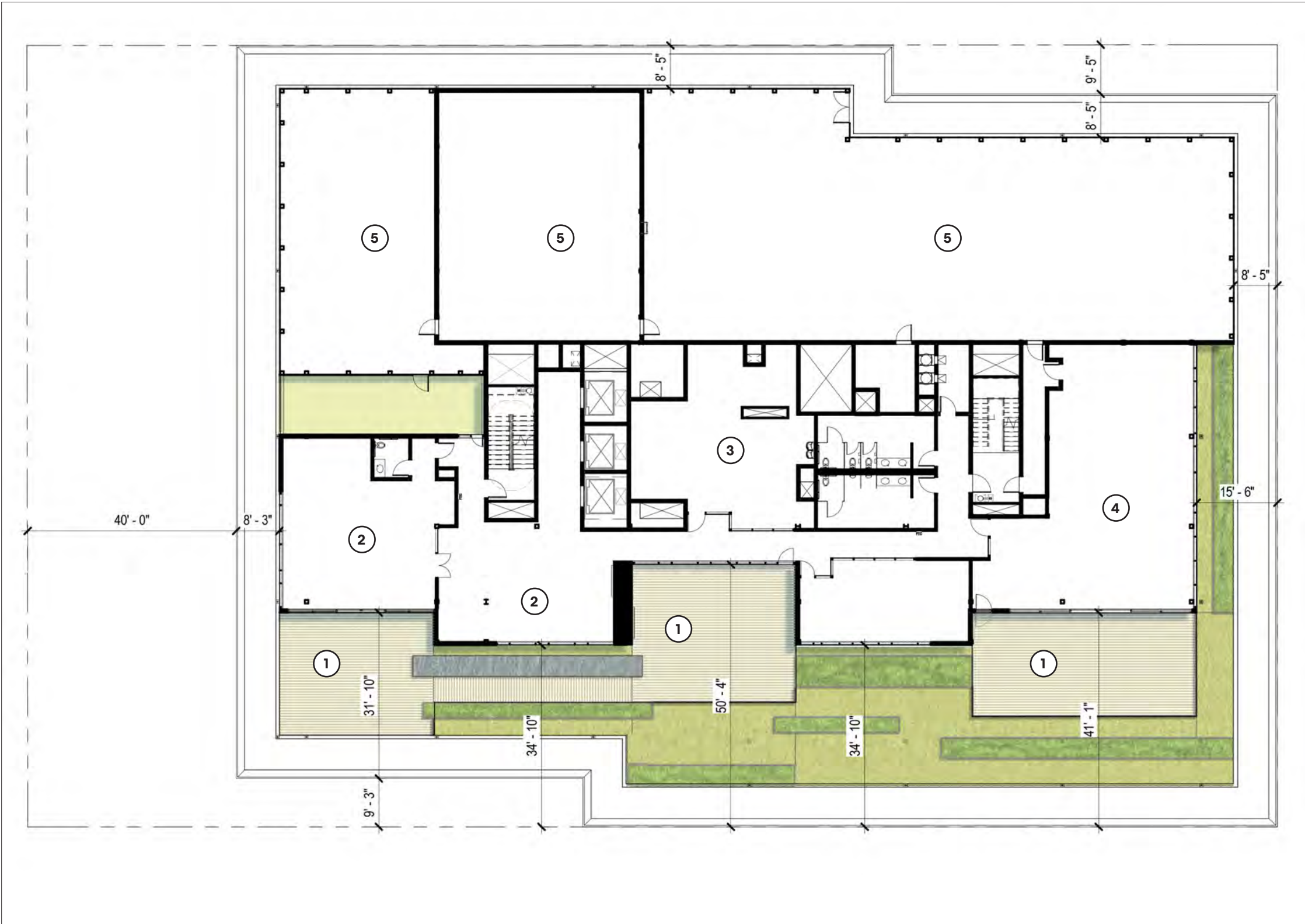
- 1 Electrical Room
- 2 Storage
- 3 Trash Room



Section 03 / Plans

Floor Plan - Penthouse Level

- 1 Deck
- 2 Lounge
- 3 Fitness
- 4 Event Space
- 5 Mechanical



Section 03 / Building Section

North - South

