

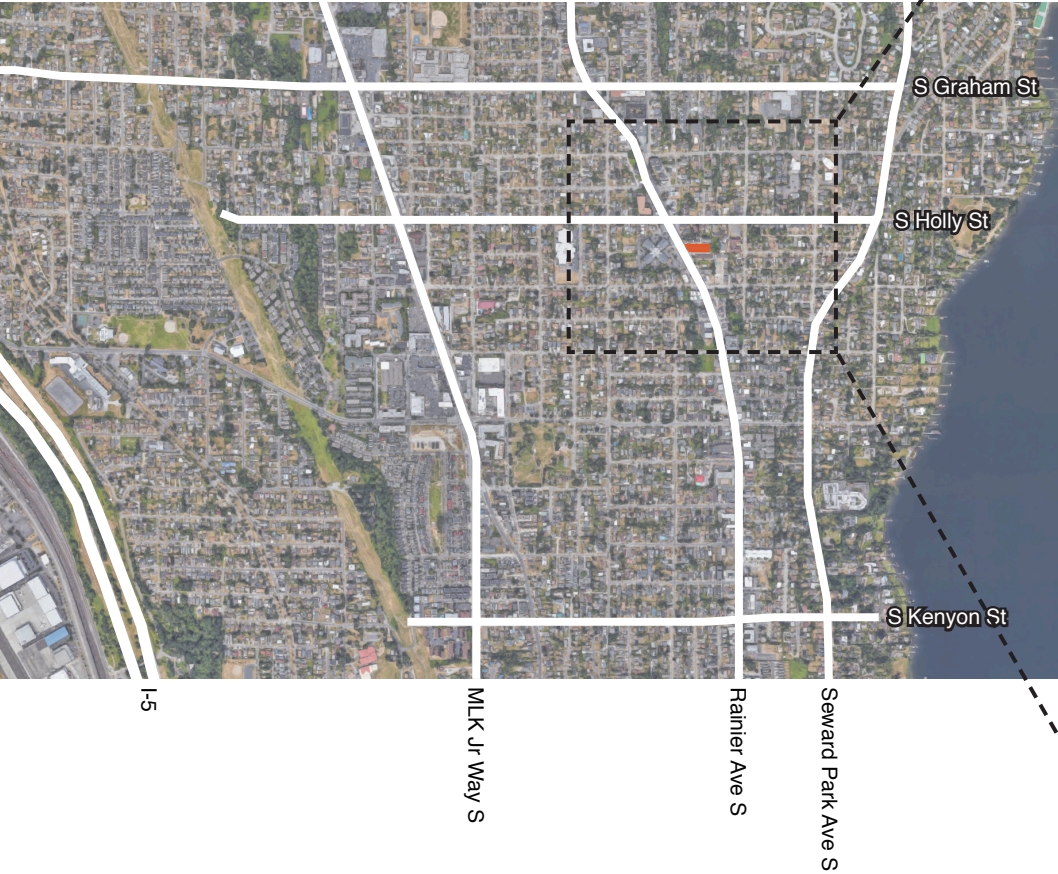


# 6730 Rainier Avenue S

Administrative Design Review (ADR) Packet | Project #3032795 | December 7, 2018



Partial Rainier Valley Key Map





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Rainier Ave S

## OBJECTIVES

Land Use Application to allow nine 3-story townhouse units. Parking will be provided within each unit with access from Rainier Avenue S. Existing structures will be demolished.

Gross Floor Area	---
Number of Units	9
Number of Parking Spaces	9
Number of Bike Parking Spaces	9

Sustainability  
Construct new structure to achieve a 4-Star Built Green certification.

## TEAM

ARCHITECTS	b9 architects
DEVELOPER	Metropolis Properties LLC
STRUCTURAL	--
GEOTECHNICAL	--
LANDSCAPE	--



EDG APPLICATION

PART I: Contact Information

1.	Property address	6730 Rainier Avenue S
2.	Project number	#3032795
3.	Additional related project number(s)	#3032797 (Adjacent site to east)
4.	Owner Name	Metropolis Properties, LLC
5.	Contact Person Name	Bradley Khouri
	Firm	b9 architects
	Mailing Address	610 2nd Avenue
	City, State Zip	Seattle, WA 98104
	Phone	206.297.1284
	Email Address	office@b9architects.com
6.	Applicant's Name	Bradley Khouri
	Relationship to Project	Architect
7.	Design Professional's Name	Bradley Khouri
	Email Address	office@b9architects.com
	Address	610 2nd Avenue
	Phone	206.297.1284

PART II: Site and Development Information

1.

Please describe the existing site, including location, existing uses and/or structures, topographical or other physical features, etc.

*The existing site is located mid-block on Rainier Avenue S, between S Holly Street to the north and S Willow Street to the south. The site's topography descends from east to west. A one-story apartment structure currently exists on the site.*
2.

Please indicate the site's zoning and any other overlay designations, including applicable Neighborhood Specific Guidelines.

*The lot is zoned LR3 and is not located within an Urban Village.*
3.

Please describe neighboring development and uses, including adjacent zoning, physical features, existing architectural and siting patterns, views, community landmarks, etc.

*The immediate neighborhood is residential and consists of predominantly low-rise multi-family and single-family structures, with a number of duplexes and triplexes. Nearby townhouse developments are contextual in their building form and materials, employing gabled roofs and lap siding. A large townhouse development is under construction south of the proposal.*

*Commercial activity is frequent along Rainier Avenue S and increases substantially beginning 3 blocks to the south. There are no substantial views from the site. Immediately adjacent parcels are all zoned LR3. Removed one block to the east is a large swath of SF-5000 zoning. Three blocks to the south NC3-40 zoning is located along Rainier Avenue S.*
4.

Please describe the applicant's development objectives, indicating types of desired uses, structure height (approx), number of residential units (approx), amount of commercial square footage (approx), and number of parking stalls (approx). Please also include potential requests for departure from development standards.

*The development proposal consists of two adjacent sites located at 6724 and 6730 Rainier Ave S. This proposal at 6730 Rainier Avenue S is to construct two three-story structures, comprised of 9 total townhouse units. The existing one-story structure will be removed. The development proposes to provide a minimum of one parking stall per unit. All parking will be located in private garages within the ground floor of the units. Approximate structure height is 30', per SMC 23.45.514A.*

*The three design alternatives represent a design exploration and evolution, resulting in a preferred scheme that is contextual in its volume and prioritizes pedestrian engagement. The design proposes a solution that is respectful of adjacent sites and simultaneously innovative in its massing strategy. The proposal responds to the site's topography, and intends to use materiality and patterning to create rhythm and unit definition.*

*There are no departures requested.*



# ZONING SUMMARY

23.45.510 - FLOOR AREA RATIO (FAR) LIMITS

- 1.1
- 1.3 if the project meets the standards of subsection 23.45.510.C.

23.45.512 - DENSITY LIMITS—LR ZONES

- 1/1,600
- No Limit for townhouse developments that meet the standards of subsection 23.45.510.C. Requirements of 23.45.501.C:
  - Proposed development must meet the green building standard.
  - Parking shall be totally enclosed within the same structure as the residential use.
  - Access to required barrier-free parking spaces may be from either a street or an alley.

23.45.514 - STRUCTURE HEIGHT

- 30 Feet base height
- For all residential uses in LR3 zones, the applicable height limit is increased 4 feet above the height shown on Table A for 23.45.514 for a structure that includes a story that is partially below-grade
- Open railings, planters, greenhouses not dedicated to food production, parapets, and firewalls on the roofs of principal structures may extend 4 feet above the maximum height limit

23.45.517 - MULTIFAMILY ZONES WITH A MANDATORY HOUSING AFFORDABILITY SUFFIX

- No MHA requirements outside urban centers and villages in LR3 zones.

23.45.518 - SETBACKS AND SEPARATIONS

Townhouses:

- Front- 7 average; 5 minimum
- Rear- 7 average; 5 minimum
- Side less than 40 feet- 5 minimum
- Side more than 40 feet- 7 average; 5 minimum

23.45.522 - AMENITY AREA

- The required amenity area in LR3 zones for townhouses is equal to 25 percent of the lot area.
- A min of 50% of the required amenity area shall be provided at ground level.
- Rooftop amenity area per 23.45.510.E.5 may be counted as amenity area provided at ground level.
- All units shall have access to a common or private amenity area.

23.45.524 - LANDSCAPING STANDARDS

- A Green Factor score of 0.6 or greater is required on LR lots with more than one new dwelling unit.
- Street trees are required if any type of development is proposed. Existing street trees shall be retained unless the Director of the Seattle Department of Transportation approves their removal.

23.45.527 - STRUCTURE WIDTH AND FACADE LENGTH LIMITS IN LR ZONES

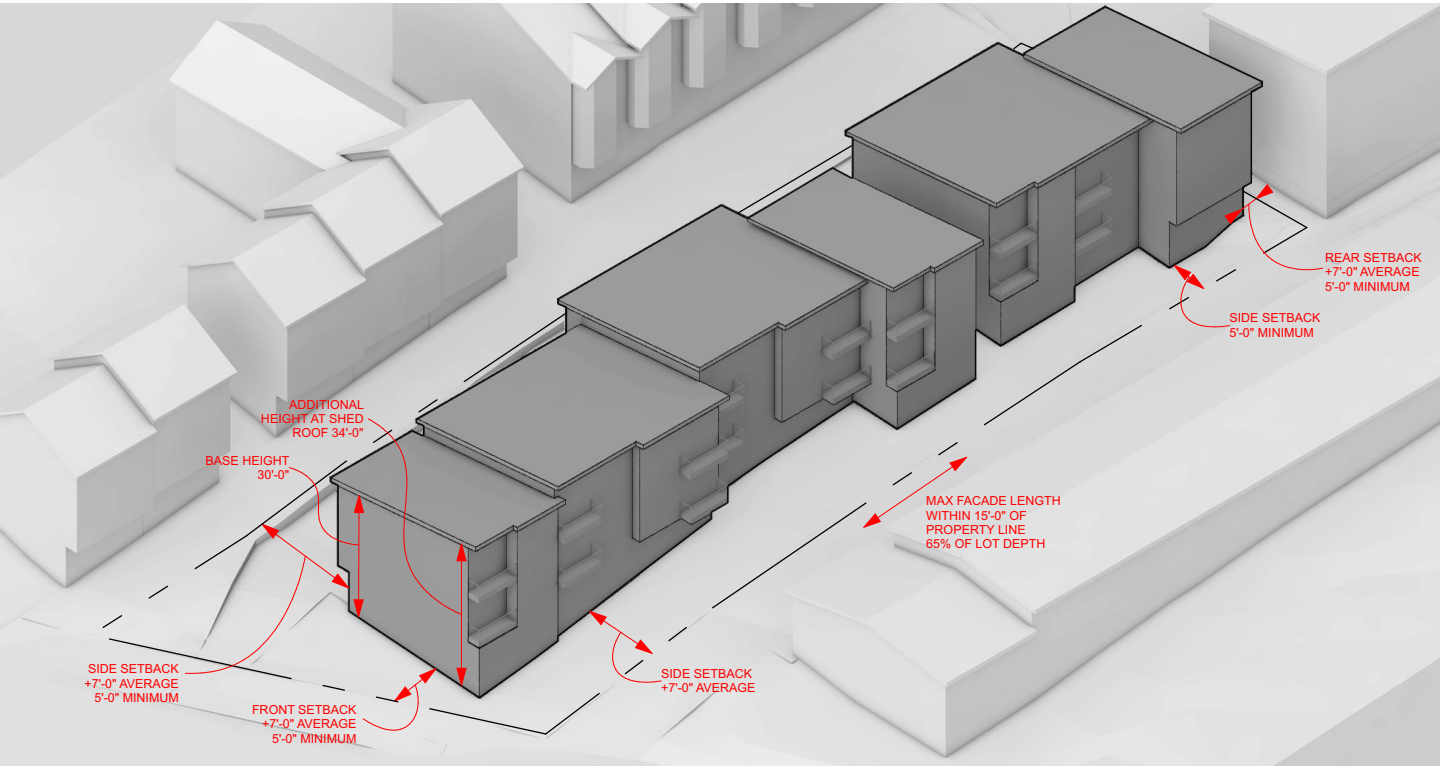
- Maximum Structure Width: 120 for townhouse developments in LR3 lots outside Urban Villages and Centers.
- Maximum Facade Length: 65% of lot line for portions of facade within 15 feet of lot line.

23.45.534 - LIGHT AND GLARE STANDARDS

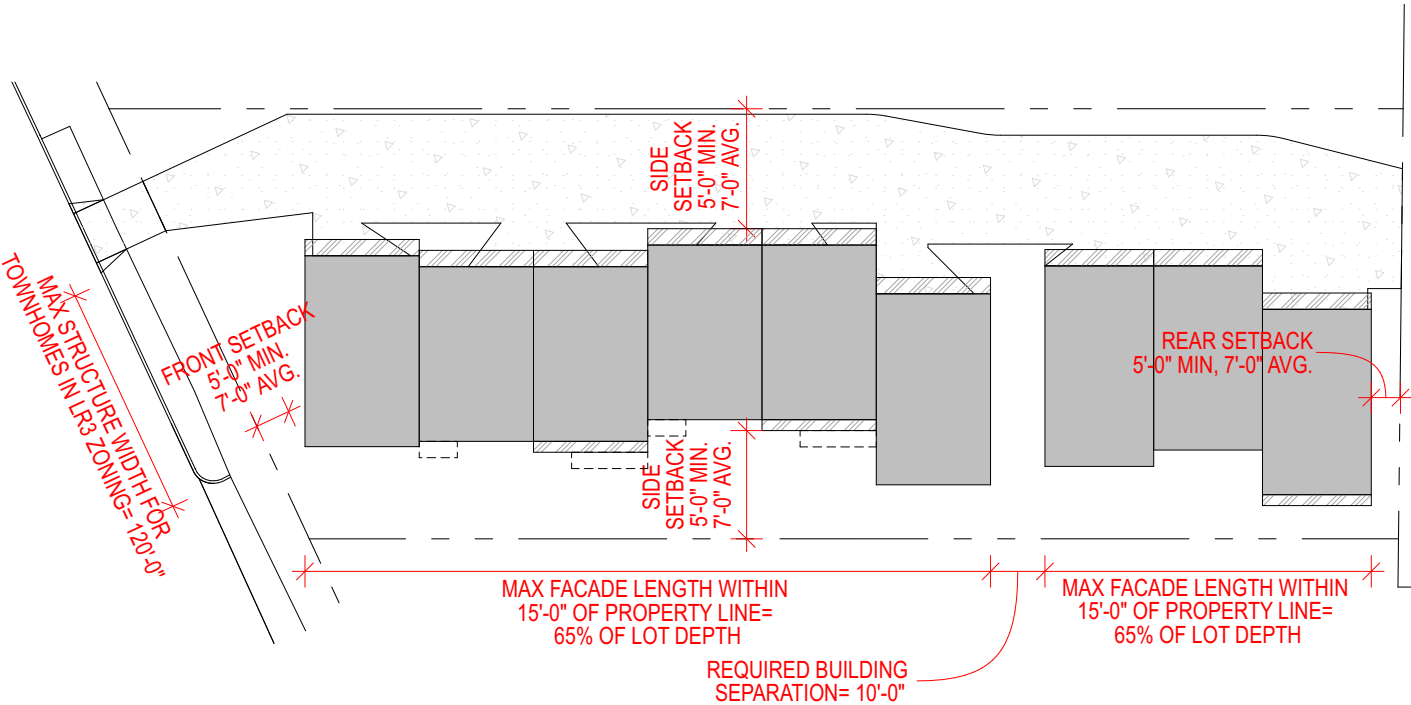
- Exterior lighting shall be shielded and directed away from adjacent properties.
- Interior lighting in parking garages shall be shielded to minimize nighttime glare on adjacent properties.
- Driveways and parking areas for more than two vehicles shall be screened from abutting properties by a fence or wall between 5 feet and 6 feet in height,

23.45.536 - PARKING LOCATION, ACCESS, AND SCREENING

- Parking cannot be located:
  - Between a principal structure and a street lot line;
  - In the required front setback or side street side setback; and
  - Within 7 feet of any street lot line.
- Parking in a structure. Parking may be located in a structure or under a structure, provided that no portion of a garage that is higher than 4 feet above existing or finished grade, whichever is lower.
- Parking shall be screened from direct street view.

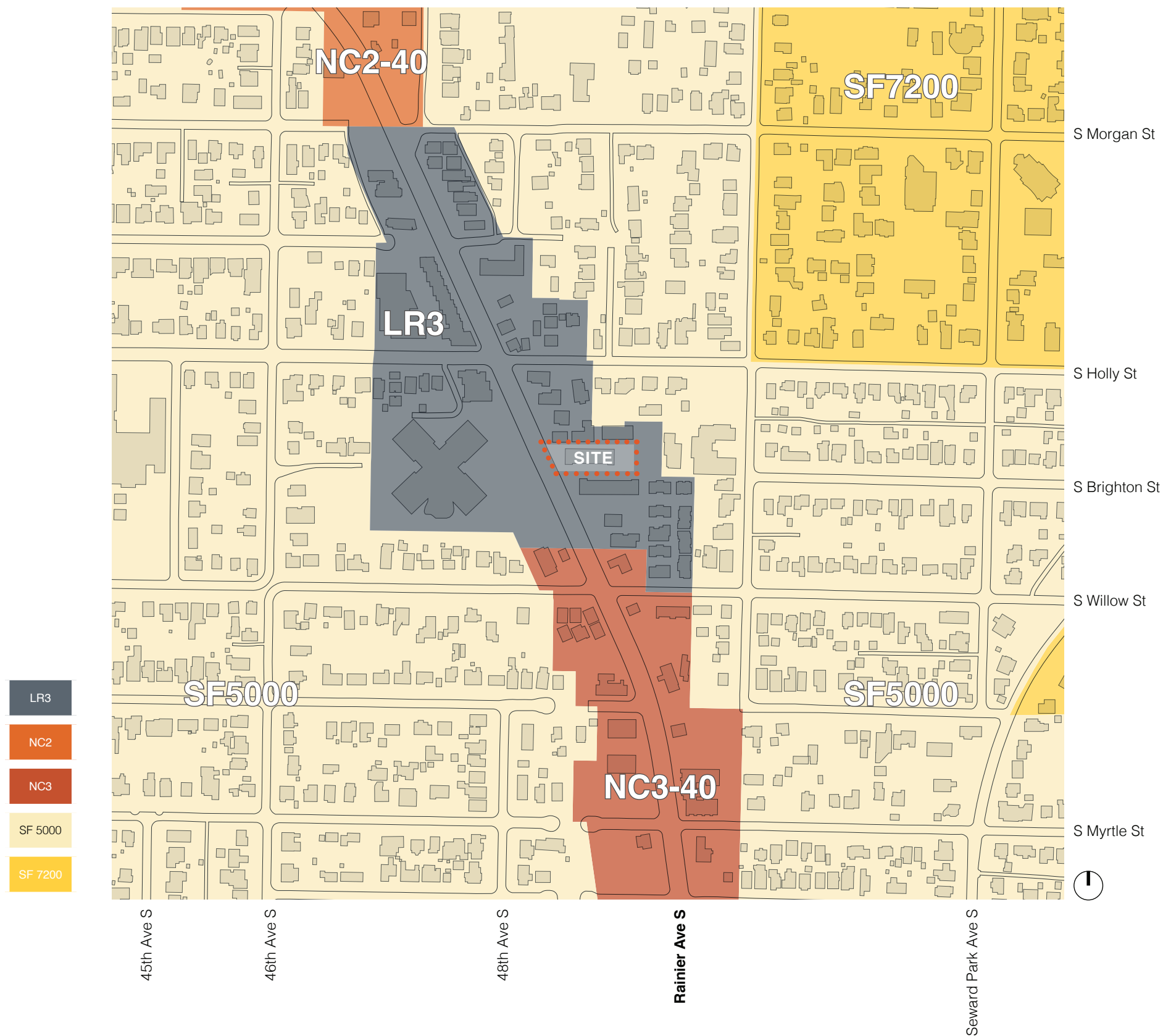


Potential Zoning Envelope



Zoning Plan Diagram





**ADDRESS**

6730 Rainier Avenue S

**PARCEL #**

272404-9050

**LEGAL DESCRIPTION**

That portion of the South half of the North half of the Northeast quarter of the Southeast quarter of the Northeast quarter of Section 27, township 24 North, range 4 East, Willamette Meridian, in King County, Washington, lying easter of Rainier Ave and southerly of a line 75.32 feet south of and parallel to the north line of said subdivision and northerly of a line 154.32 feet south of and parallel to the north line of said subdivision; except the east 320 feet thereof.

**LOT SIZE**

17,302 SF

**ZONE**

LR3

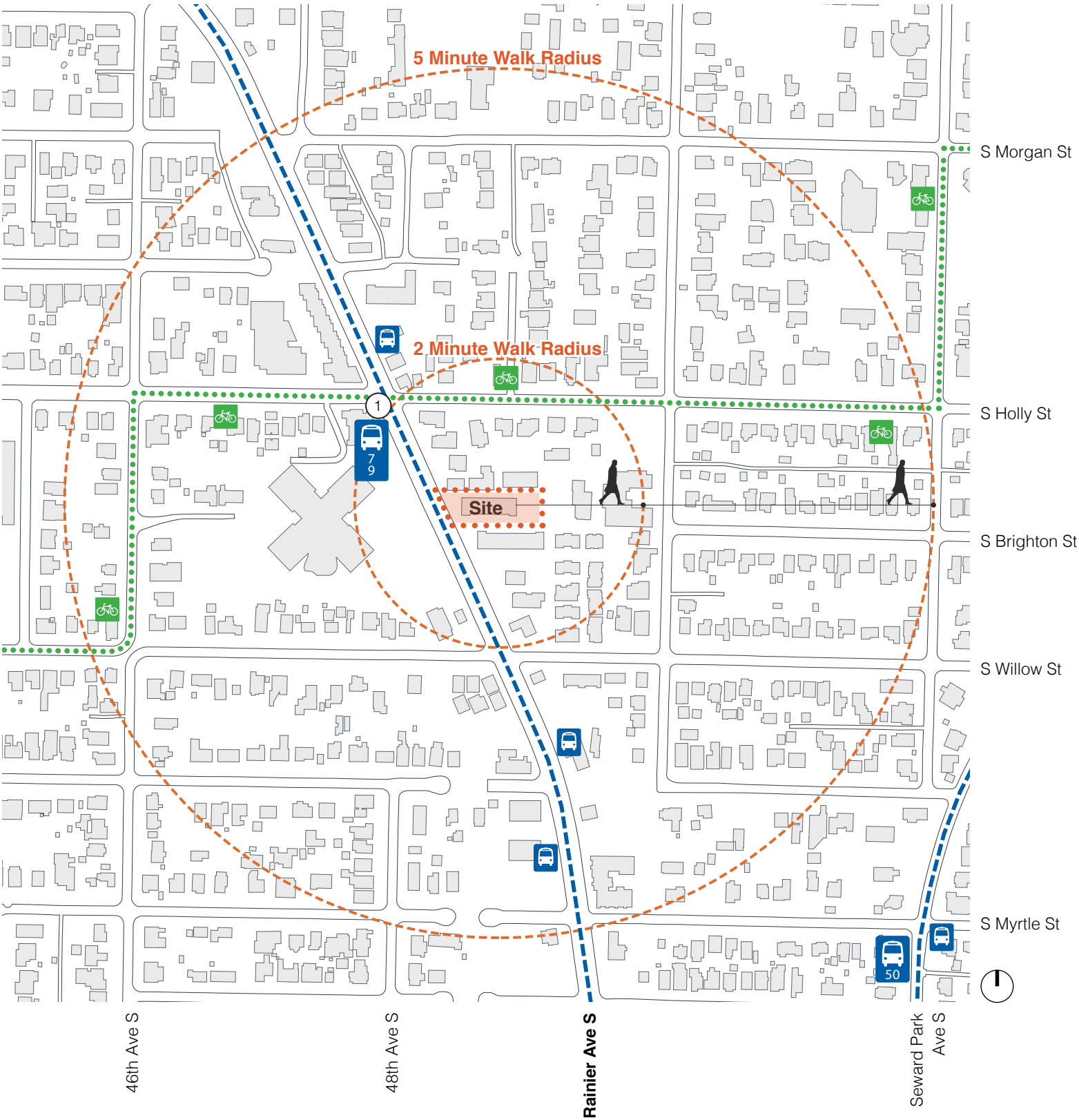


VICINITY MAP





# TRANSIT & ACCESS



This site is well served by several bus lines, including the #7, 9, and 50, facilitating travel to many Seattle neighborhoods, including Downtown, Capitol Hill, Columbia City, Rainier Beach, Beacon Hill, and West Seattle. The Othello link light rail station is within walking distance, roughly 0.7 miles to the West.

Bicycle routes connect the site to neighborhoods to the east and west and link up with bike routes that serve Downtown,

2





MINUTES

Cafe Avole  
Lucky One Food Store

5

MINUTES

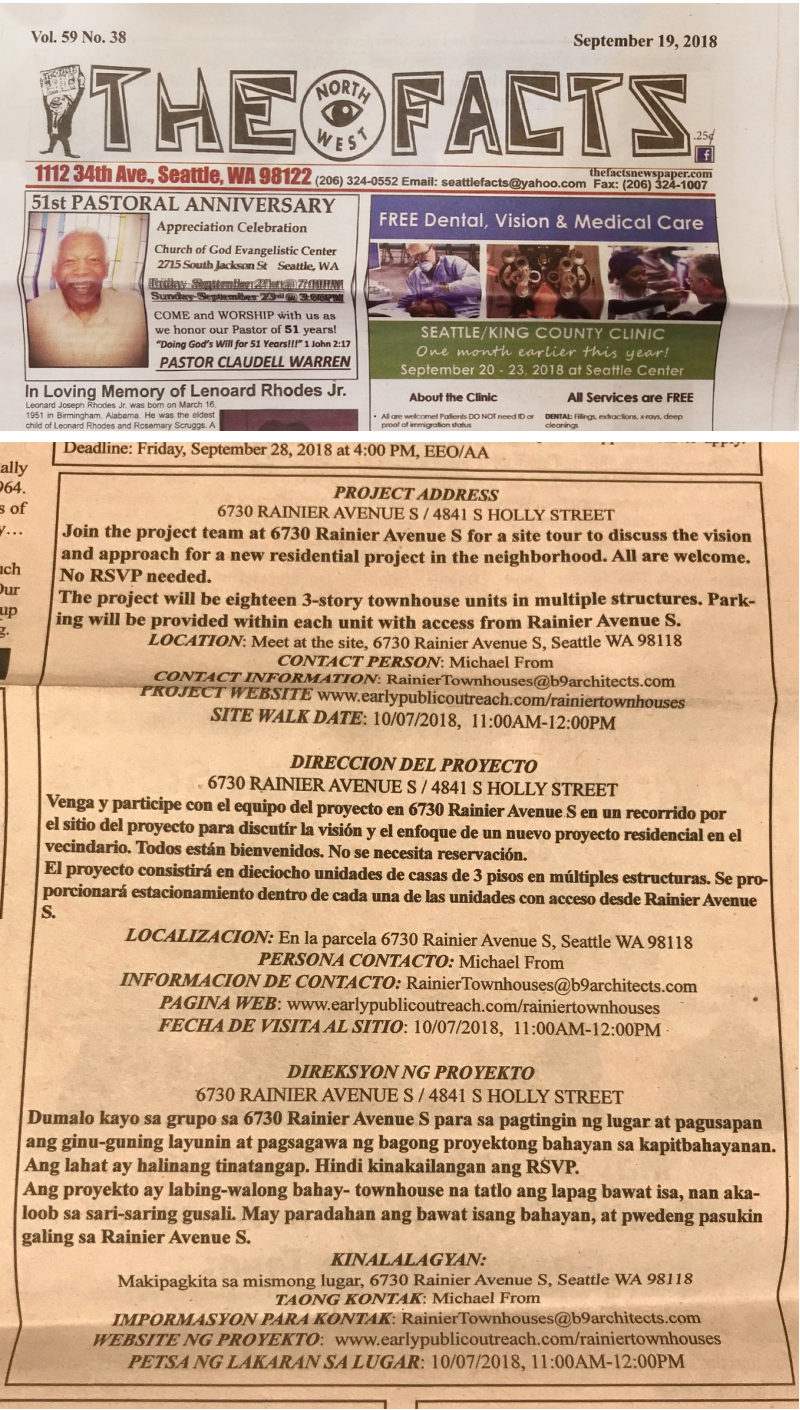
Zest Fast Food  
Shell Gas Station  
Paradise Restaurant

- 
Bike Route

- 
Bus Stop & Route




1 Bus Stop for routes 7 & 9





1 Printed ad in NW Facts



2 Printed 36"x72" sign on site.  
Per community outreach requirements and approved by D.O.N.

## 6730 RAINIER AVENUE S / 4841 S HOLLY STREET

### PROJECT SUMMARY / RESUMEN DEL PROYECTO / BUOD NG PROYEKTO :

Land Use Application to allow eighteen 3-story townhouse units in multiple structures. Parking will be provided within each unit with access from Rainier Avenue S. Existing structures will be demolished.

La Aplicación de Uso permitirá dieciocho unidades de casas de 3 pisos en múltiples estructuras. Se proporcionará estacionamiento dentro de cada unidad con acceso desde Rainier Avenue S. Las estructuras existentes serán demolidas.

Applikasyon sa Paggamit ng Lupa upang payagan ang labing-walong 3-palapag na townhouse units na nakalooob sa sari-saring gusali. Bawat isang unit ay ipag-kakalooban ng paradahan na pwedeng pasukin galing sa Rainier Avenue S. Ang mga dating gusali ay gigibain.

### SITE TOUR / VISITA AL SITIO / PAGLIBOT SA LUGAR:

We will be hosting a site tour at 6730 Rainier Avenue S  
Sunday, October 7, 2018, 11am - 12pm  
All are welcome

Realizaremos un recorrido por la parcel en 6730 Rainier Avenue S  
Domingo, 7 de octubre de 2018, 11am - 12pm  
Todos están bienvenidos

Magpapasagawa kami ng paglibot ng lugar sa 6730 Rainier Avenue S  
Linggo, Oktubre 7, 2018 11am - 12pm  
Ang lahat ay halinang tinatangap.

### CONTACT / CONTACTO / KONTAK:

Michael From  
rainiertownhouses@b9architects.com



3 Interactive project webpage - [www.earlypublicoutreach.com/rainiertownhouses](http://www.earlypublicoutreach.com/rainiertownhouses)  
Approved by D.O.N.



4 Hosted 1-hour community on-site walk with a representative from the Muslim Housing Services



# EARLY COMMUNITY OUTREACH

As the applicant for a proposal at 6724/6730 Rainier Avenue S, b9 architects conducted and completed Early Community Outreach requirements. Outreach included a large sign installed on site, an ad in a local print newsletter, an interactive project website and a site tour/walk. Per DON's guidance regarding Equity Zone regulations, each outreach method was provided in English, Spanish and Tagalog.

Per the SDCI Director's Rule 4-2018/DON Director's Rule 1-2018 VI.E.- Documentation: Early Design Guidance

*Applicants shall include a summary of the design-related feedback they heard during their community outreach as part of their final EDG packet. While collaborative approach is encouraged between the applicant and the community, the applicant is not required to incorporate any specific community feedback into the project's design. Comments and discussion presented at the Design Review meetings should focus on compliance with the established design guidelines. Applicants may, at their discretion, respond directly to the community about any feedback that is not related to Design Review.*

OUTREACH METHOD	DATE IMPLEMENTED	DESIGN-RELATED COMMENTS
1 Printed Outreach Ad in local newsletter (NW Facts)	September 7, 2018	None received
2 Printed Outreach 36"x72" sign on site	September 21, 2018	None received
3 Digital Outreach Interactive project webpage	September 7, 2018	No comments received through digital outreach.
4 In-Person Outreach Hosted 1-hour community on-site walk Invitations to site walk sent to Rainier Valley Historical Society, Muslim Housing Services, SE Seattle Senior Center, and Rainier Valley Community Development Fund	October 7, 2018	One person attended the in-person outreach event. Although no comments were received, the attendee asked whether the units would be rental or for sale.



ARCHITECTURAL CONTEXT



1 Rainier Plaza Apartments  
6800 Rainier Ave S  
Built: 1958, 10 units



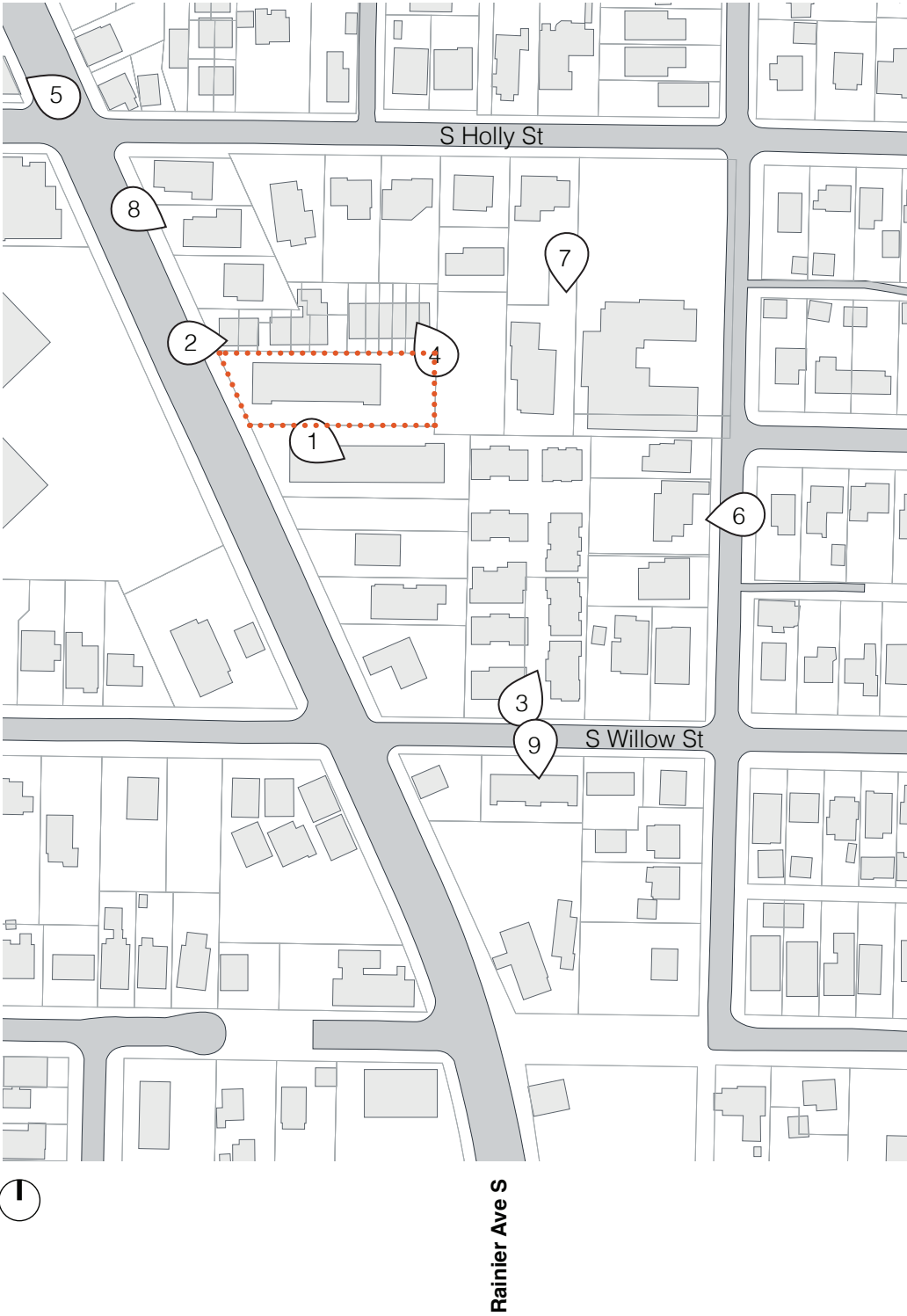
2 Townhouses  
6718-6720 Rainier Ave S  
Built: 2008, 6 townhouse units



3 Townhouses  
4906 - 4922 S Willow St  
Under Construction, 21 townhouse units



4 Townhouses  
6722 Rainier Ave S  
Built: 2004, 6 units





The architectural context in the neighborhood around this site does not represent a unified style and could be characterized as in transition.

The immediate neighborhood is moderately dense with single-family homes and low-rise, multi-family structures. Several lots in the area are currently being redeveloped from single to multi-family.

Single-family homes in the area are predominately one to two stories and are gabled and wood clad. There is diversity exhibited in the local multi-family structures, both in terms of type and architectural style. There are several townhouse structures (see 2 and 4) as well as low-rise apartments (see 5 and 9) nearby.

While there is not a consistent style or structure type in the vicinity, most nearby structures do exhibit standard construction typologies such as gabled roofs, wood and panel siding and ground floor parking.



5 Arches Apartments  
6601 Rainier Ave S  
Built: 1967, 24 units



6 Single Family House  
6807 51st Ave S  
Built: 1949



7 Single Family House  
4837 S Holly St  
Built: 2016



8 Fourplex  
6700 Rainier Ave S  
Built: 1951



9 Willows Apartments  
4911 S Willow St  
Built: 1959, 9 units





## ADJACENT USES



1 Muslim Housing Authority



2 Southeast Seattle Senior Center





③ Lucky One Gas Station and Convenience Store



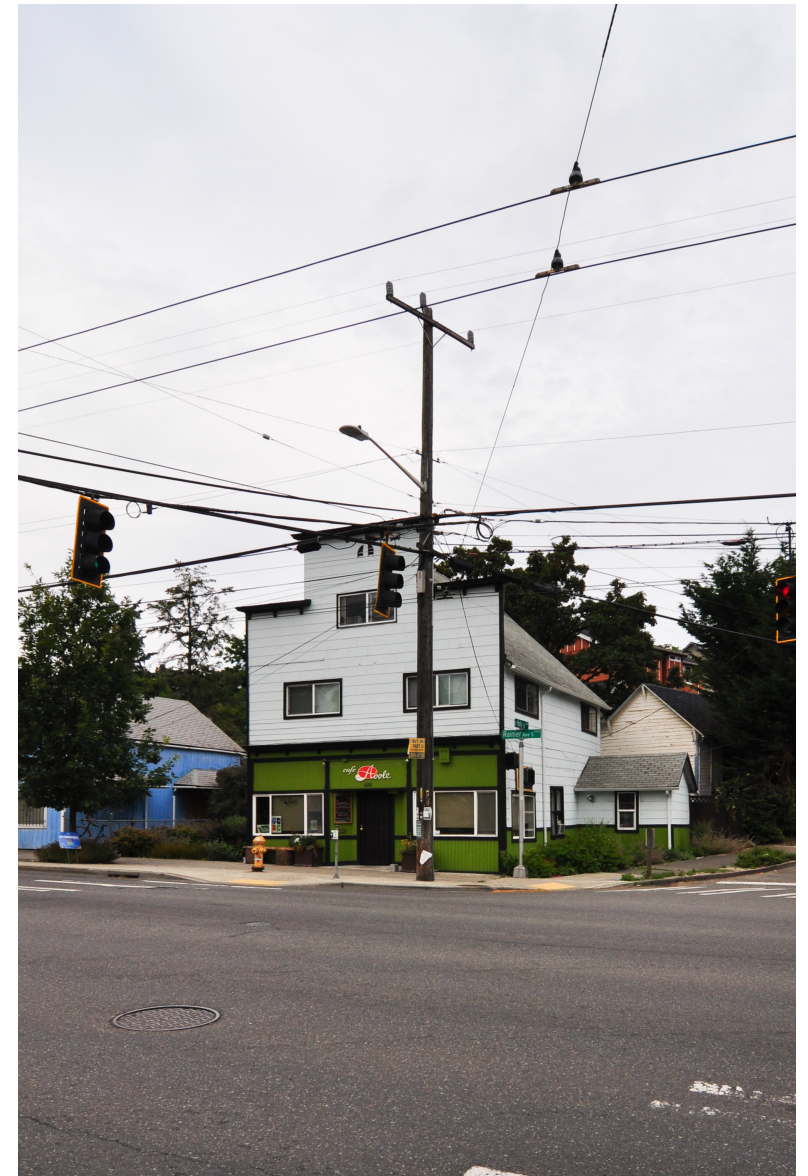
④ Peter Pan Auto Glass Shop



⑤ Lsk Auto Repair and Sales



⑥ Ashreichem Yisrael Orthodox Synagogue



⑦ Cafe Avole

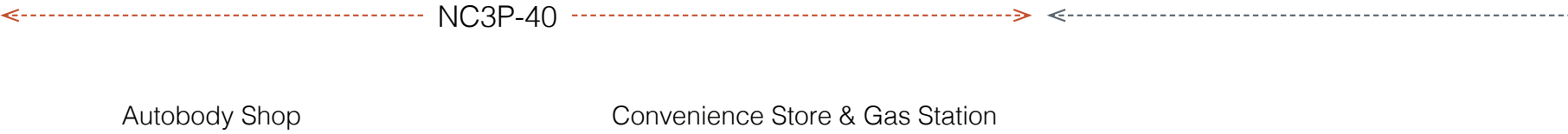




# STREET PANORAMAS



1 East side of Rainier Ave S



2 West side of Rainier Ave S



LR3

NC3P-40

Single Family

Townhouse

10-unit Apartment Building

Duplex

11-unit Apartment Building

Autobody Shop

Restaurant



LR3

124-unit Apartment Building

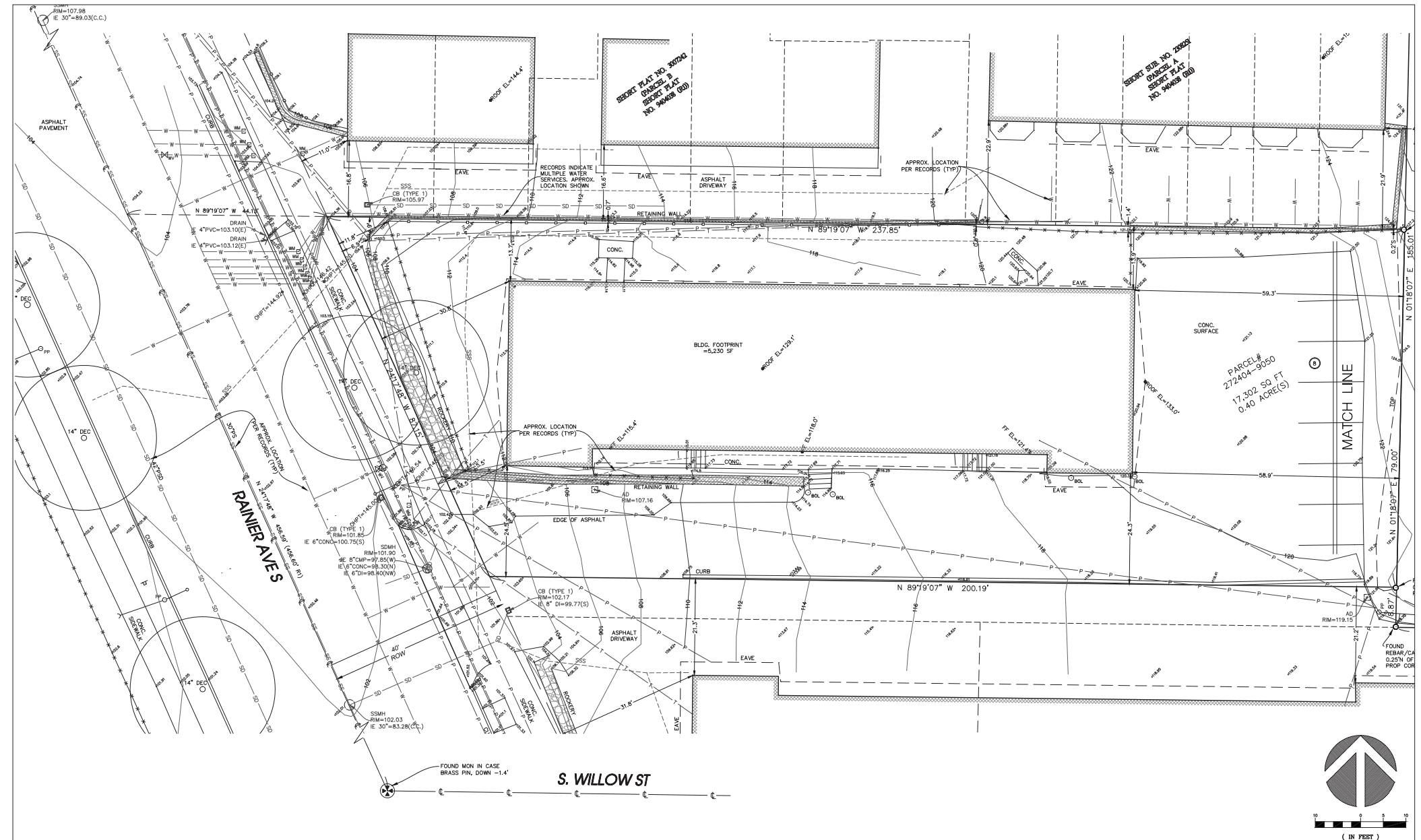
Community Center

24-unit Apartment Building



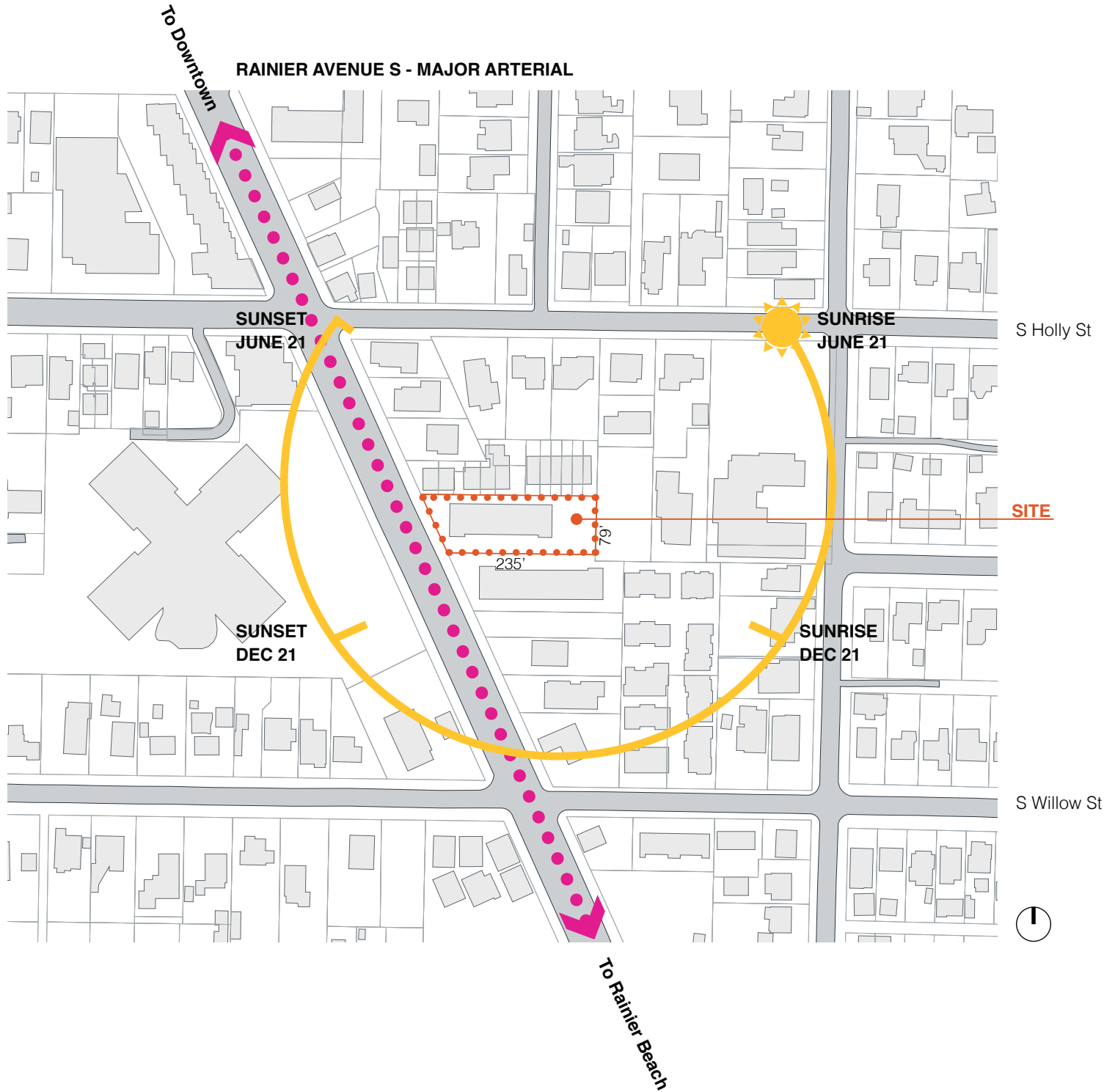


## 6730 Rainier Avenue S | #3032795 | ADR Packet | December 7, 2018





# SITE CONDITIONS & CONSTRAINTS

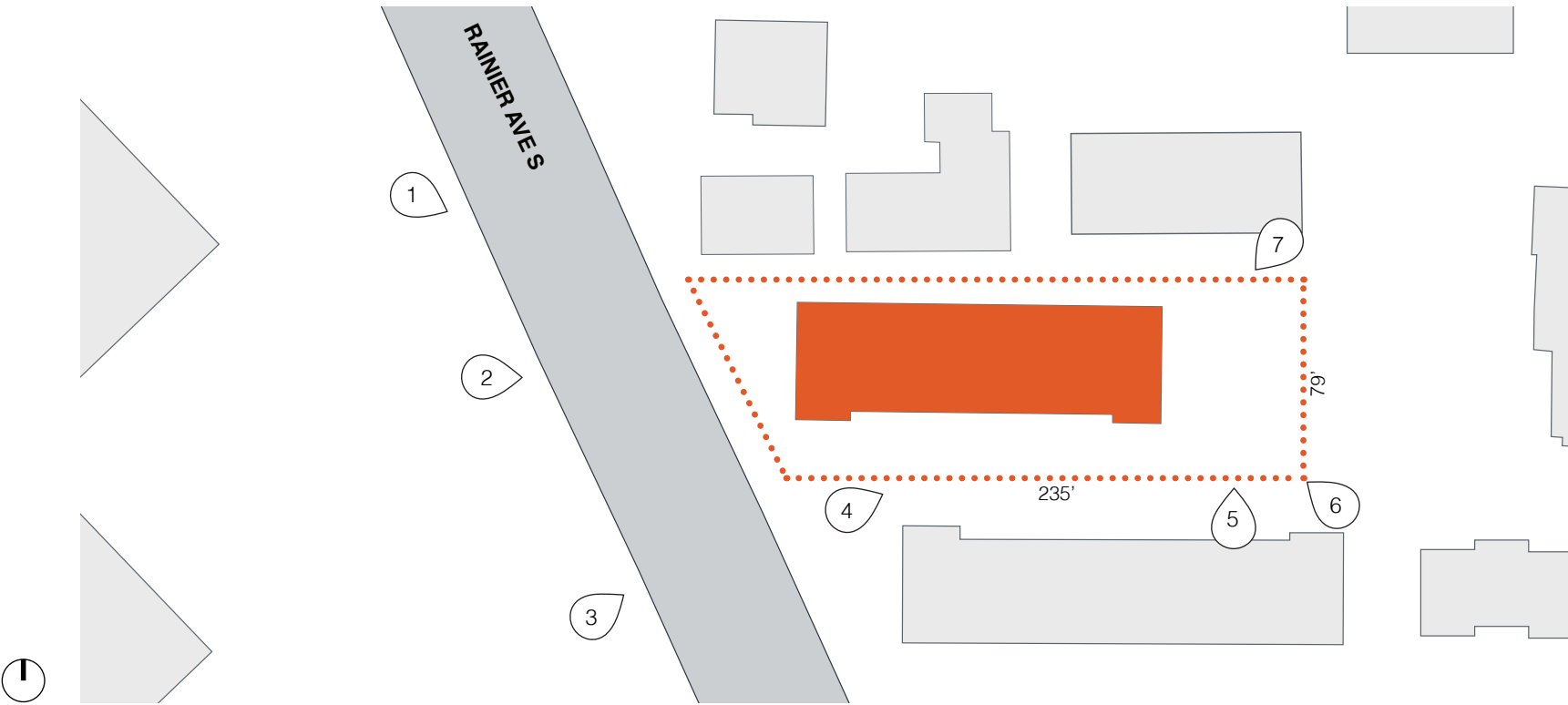


## CONSTRAINTS

- Rainier Avenue S is an Urban Center connector, principal arterial street that provides a direct connection to downtown Seattle and many public transit opportunities.
- The site is located mid-block between S Holly Street and S Willow Street.
- The site is located on the East side of Rainier Avenue S
- The site currently houses a single-story multi-family apartment building and associated driveway, parking aisle and surface parking.
- This infill site dimensions are 79 feet north-south by 235 feet east-west.
- Pedestrian and vehicle access is from Rainier Avenue South.
- The surrounding context is comprised of low-rise multi-family and single-family structures. Directly across Rainier Avenue S is the Muslim Housing Service's Brighton Apartment complex.
- The vacant lot to the east, currently addressed as 4841 Holly Street, will be developed concurrently with this project. Access will be provided through this site to the vacant lot to the east.
- The site has good southern and western exposure due to the low height of adjacent structures and the orientation of the lot's dimensions.

Immediate neighborhood





1 View of site looking Southeast on Rainier Ave S



2 View of site looking East on Rainier Ave S



3 View of site looking Northeast on Rainier Ave S



# EXISTING CONDITIONS

The site is approximately 79 feet wide in the north-south direction by 235 feet deep in the east-west direction. It currently contains a one-story multifamily structure with an associated driveway, parking aisle and surface parking area at the rear of the site. The existing structure and associated pavement is proposed to be removed.

Multi-family structures bound the site to the north and south, with a similar one-story apartment structure to the south and two townhouse projects to the north. To the east is a vacant lot that will be redeveloped concurrently with this project. The site’s topography descends approximately 24 feet from the East property line to Rainier Avenue S, with the majority of the slope closest to Rainier Avenue S.

The site is located on the east side of Rainier Ave S, mid-block between S Holly Street and S Willow Street. The site does not have significant access to views. The site has good access to Othello Playground and Martha Washington Parks each slightly over 0.5 miles from the site.



7 View of site looking Southwest on Rainier Ave S



4 View of site looking Northeast from adjacent property to the south



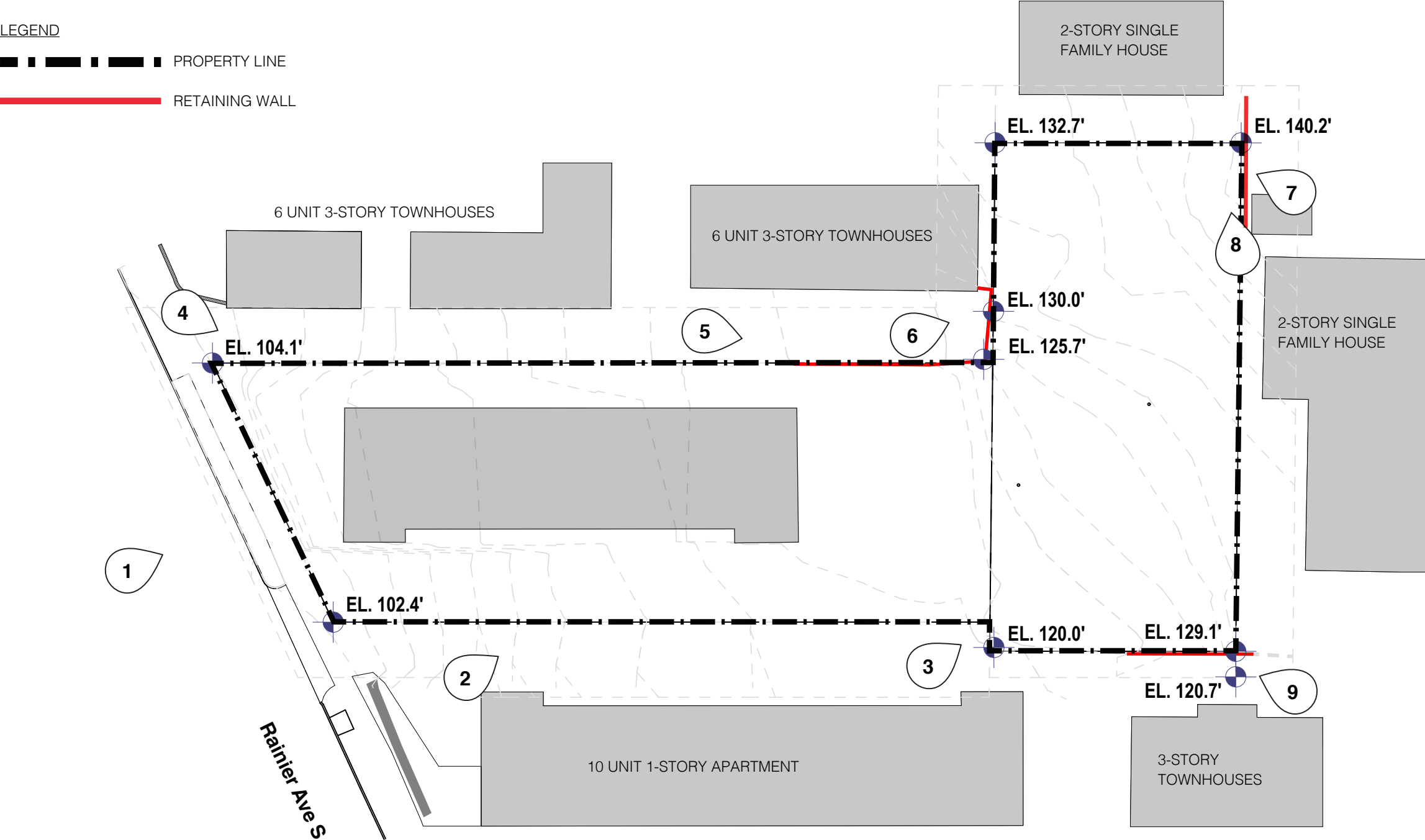
5 View of site looking North from adjacent property to the south



6 View of site looking Northwest from adjacent property to the south



EDGE CONDITIONS







1 View of site looking Northeast on Rainier Ave S



4 View of site looking Southeast from adjacent property to the north



7 View of site looking Northwest from adjacent property to the east



2 View of site looking Northeast from adjacent property to the south



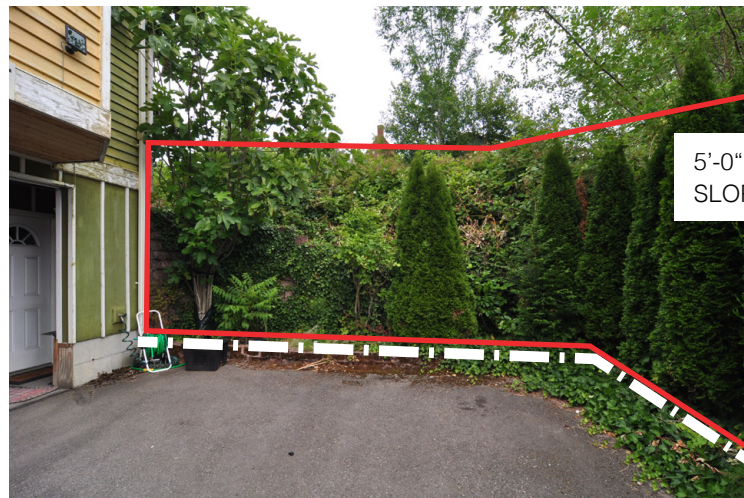
5 View of site looking East from adjacent property to the north



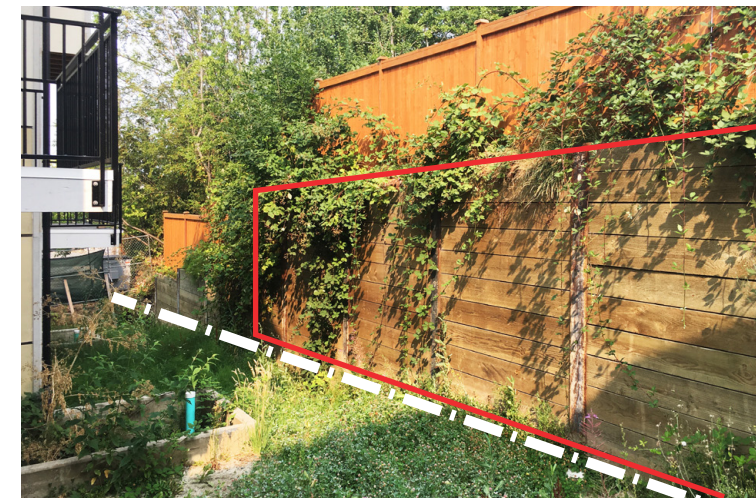
8 View of site looking North



3 View of site looking Northeast from adjacent property to the south



6 View of site looking West from adjacent property to the south



9 View of shoring wall looking Northwest from adjacent property to the south



DESIGN PROPOSAL ALTERNATIVES

ALTERNATIVE 1

Units

- 9 units.
- 9 parking stalls.

Advantages

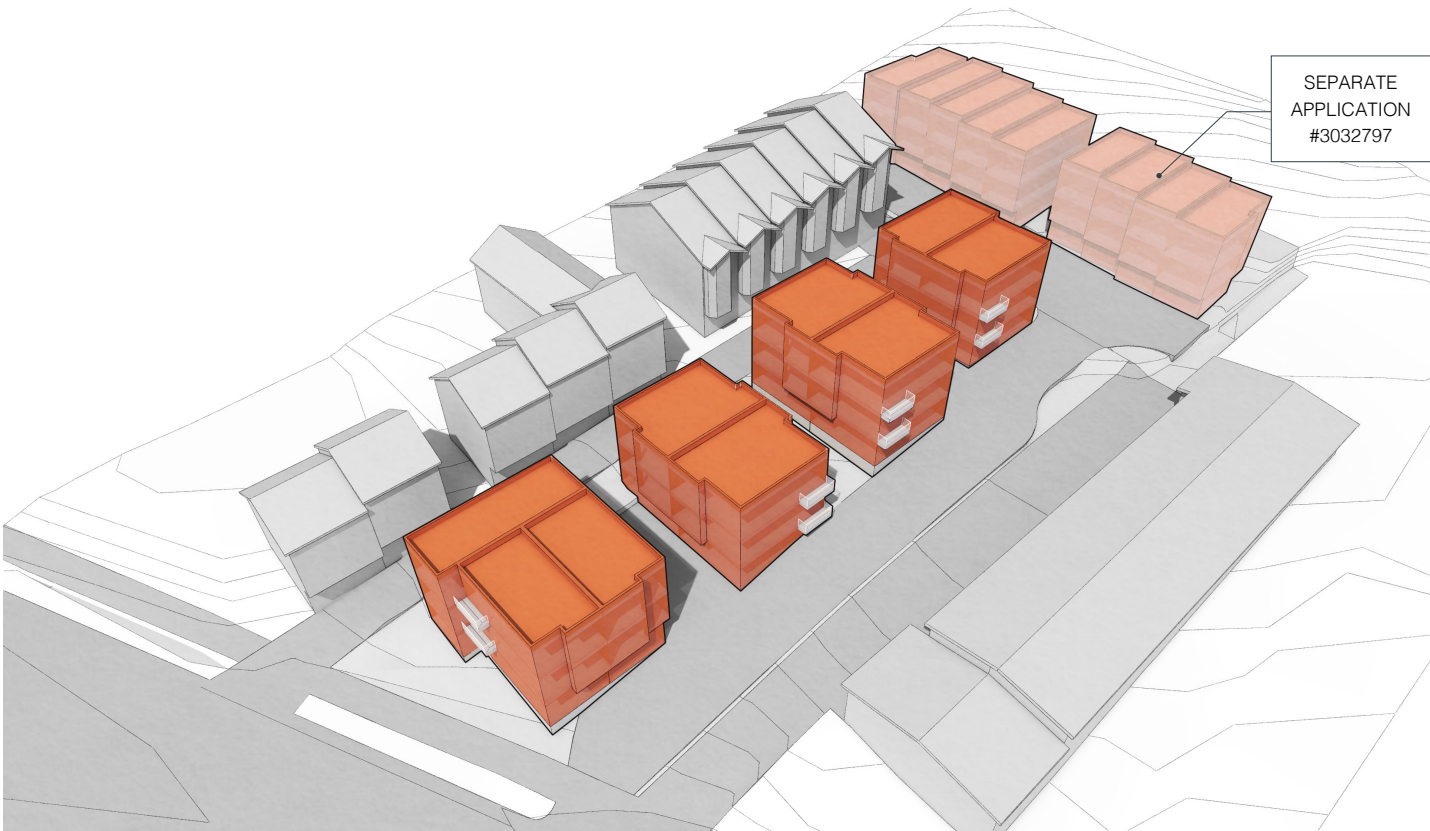
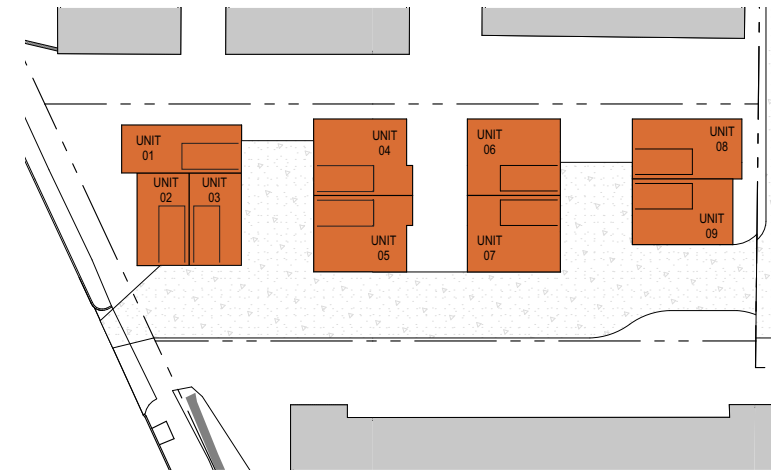
- Code compliant scheme.

Issues

- The parking aisle occupies a large portion of the site due to the spurs that are required for parking access.
- Opportunities for common amenity areas are limited. Reference page 32.
- The parking aisle location on the south side of the proposed development prevents most amenity spaces from having adequate solar access.
- Pedestrian access can only occur within the north setback or within the parking aisle.
- Flat roofs are not a contextual response, neither in terms of adjacent structures or solar access.
- Only five of nine units have southern exposure.

Departures

- No departures requested.

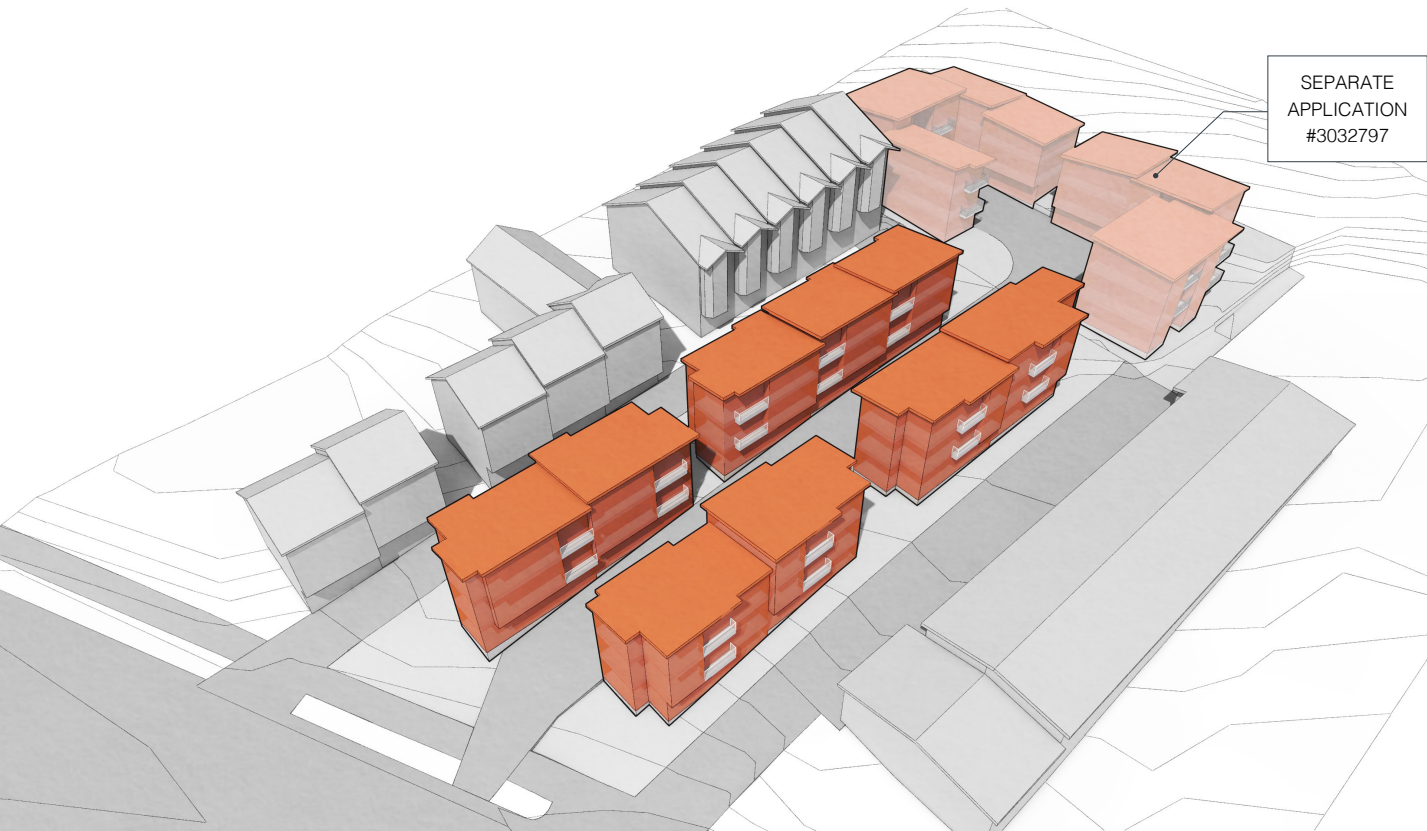
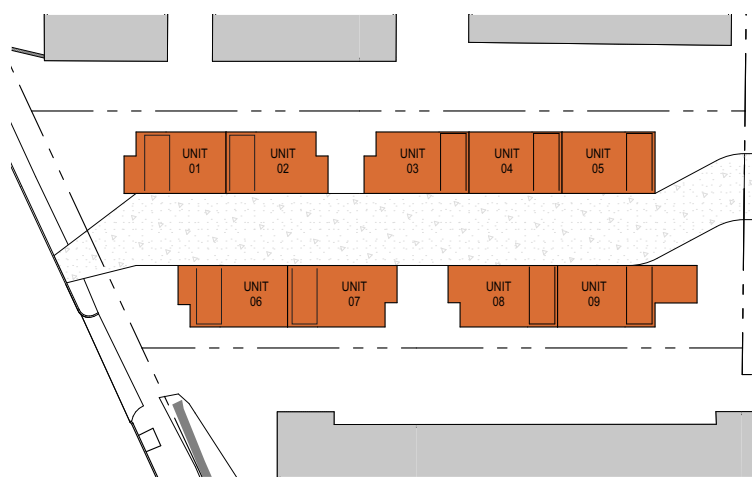


Aerial View looking Northeast



ALTERNATIVE 2

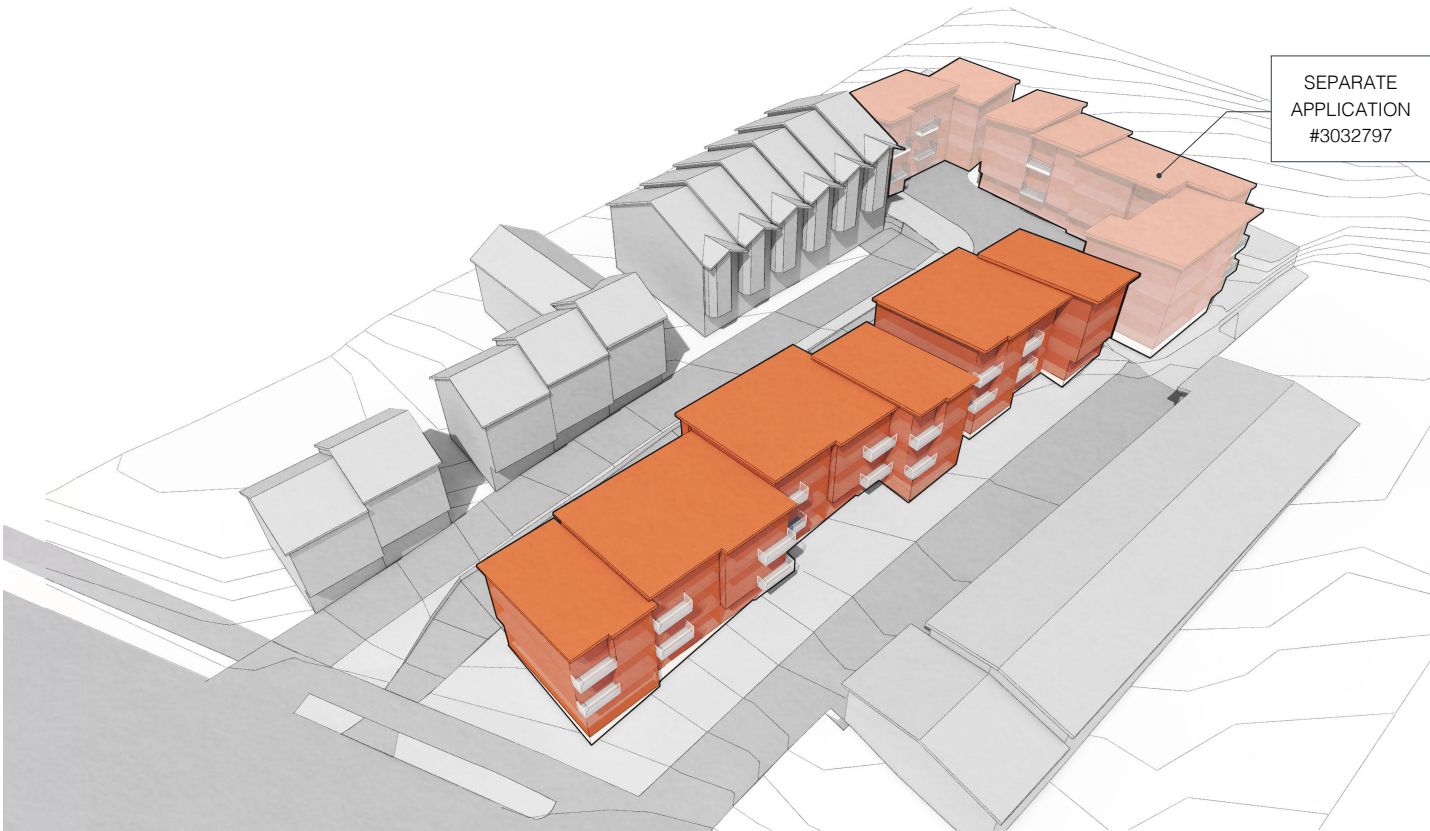
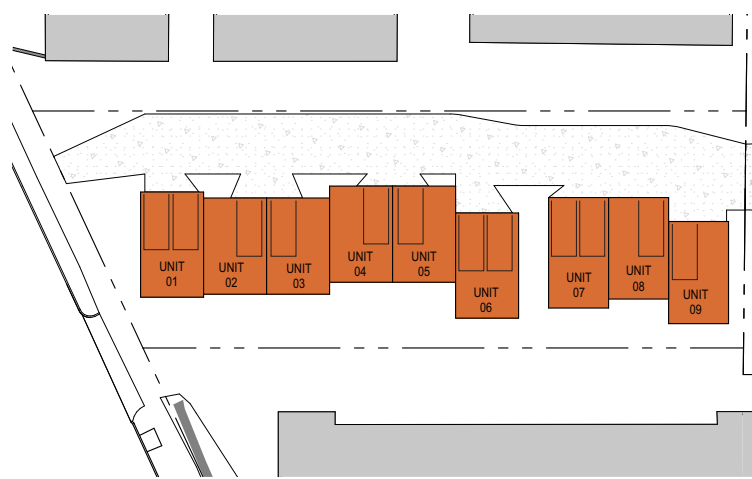
- Units
- 9 units.
  - 9 parking stalls.
- Advantages
- Code compliant scheme.
  - Shed roofs that slope up to the south provide a contextual response to surrounding structures and allow for increased daylighting opportunities.
  - All units have balconies that face south.
- Issues
- The parking aisle occupies the center of the site and makes potential connections between structures difficult.
  - The north and south side setbacks are at their minimums and do not provide relief to adjacent properties.
  - Pedestrian access can only occur within the north or south setback or within the parking aisle.
  - Though all units have southern exposure, the double loaded approach would result in the north five units being shaded.
  - Open spaces are fragmented and only connected through the narrow space between the structure and setbacks.
- Departures
- No departures requested.



Aerial View looking Northeast

ALTERNATIVE 3 (PREFERRED)

- Units
- 9 units.
  - 9 parking stalls.
- Advantages
- Code compliant scheme.
  - All units have southern exposure.
  - All units have balconies that face south.
  - Vehicles and pedestrians are separated.
  - Pedestrian walkway will be provided in the generous south setback, significantly larger than code requirements, which will support a range of activities with southern exposure.
  - Vehicular access and parking is provided to the north, creating a generous north setback, significantly larger than code requirements.
  - Continuous amenity spaces provide a generous open space in aggregate.
  - Shed roofs that slope up to the south, provide a contextual response to surrounding structures and allow for increased daylighting opportunities.
  - Generous north and south side setbacks reduce impacts on adjacent structures.
- Departures
- No departures requested.



Aerial View looking Northeast



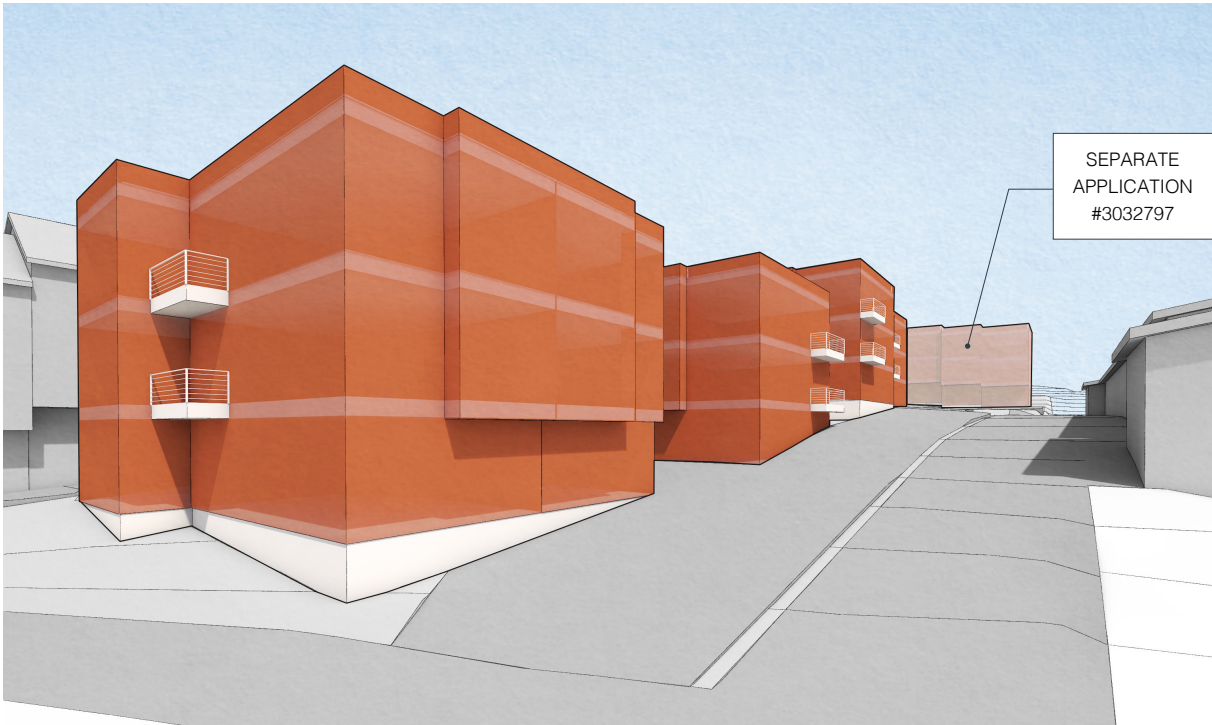
ALTERNATIVE 1

Alternative 1 is a code compliant scheme with two three-story structures, containing a total of (9) townhouse units. The existing one-story apartment building on site is proposed to be removed. All units are parked in private garages at grade in each unit, accessed by a parking aisle on the south side of the site. The garages of units 2-3 are accessed directly from the parking aisle, while units 1 and 3-9 are accessed via parking aisle spurs.

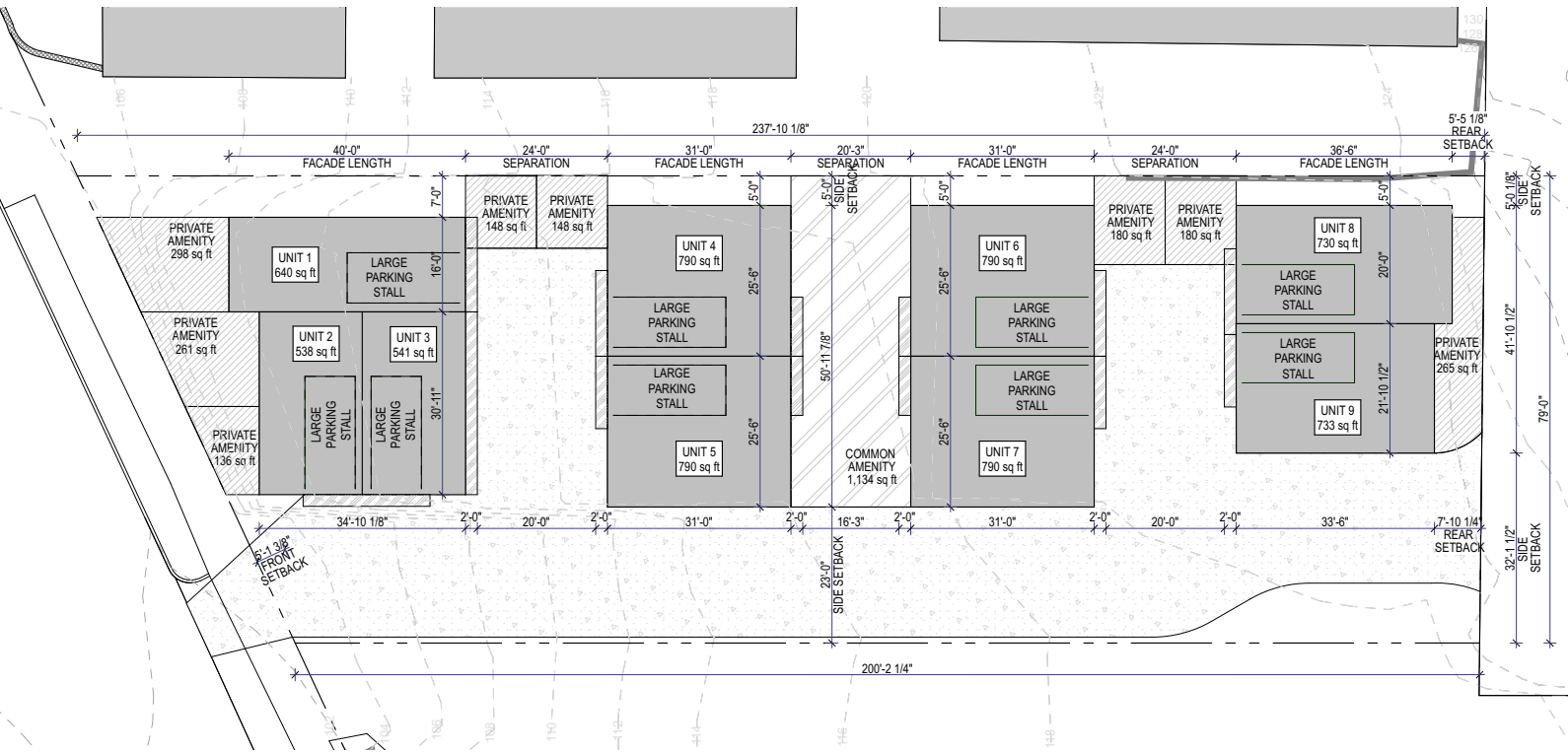
Unit 1 faces Rainier Avenue S and units 2-3 are oriented southward. All other units are oriented toward the interior of the site. Pedestrian access can occur in the north setback, through the common and private amenity areas, or within the parking aisle.

A large common amenity area is provided in the center of the site. Private amenities areas are provided for 6 of the 9 units. Alternative 1 has flat roofs with parapets and provides decks on the south side of units where possible.

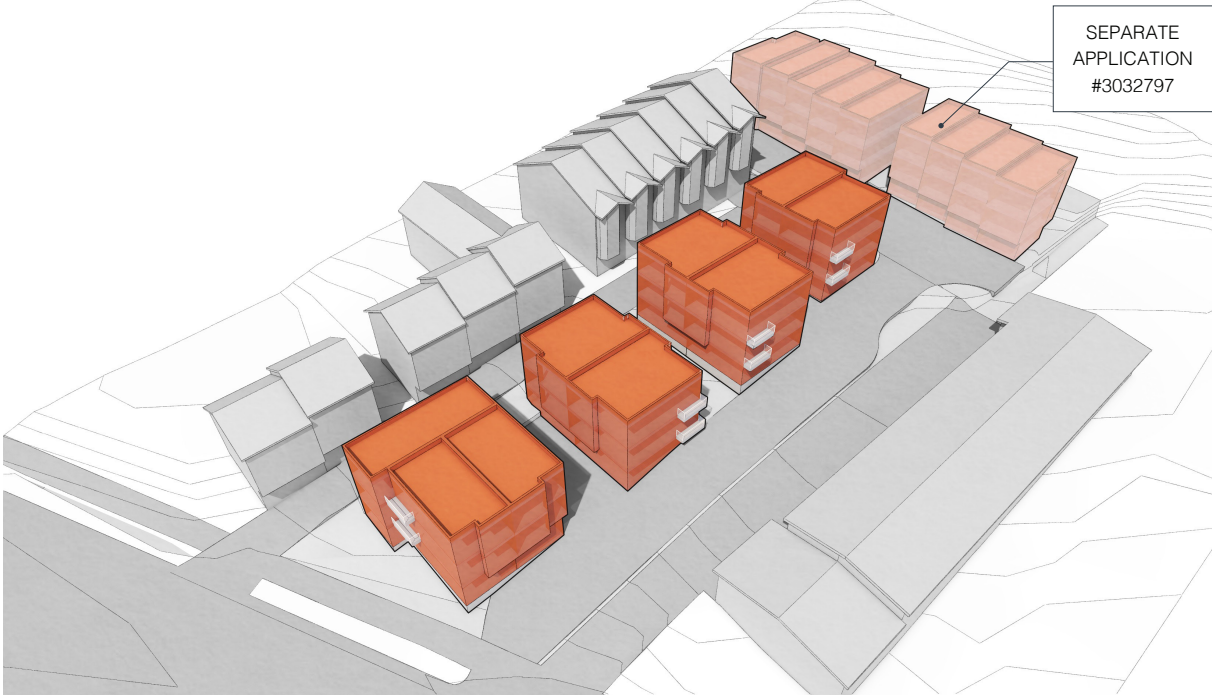
- Units
- 9 units.
  - 9 parking stalls.
- Advantages
- Code compliant scheme.
- Issues
- The parking aisle occupies a large portion of the site due to the spurs that are required for parking access.
  - Opportunities for common amenity areas are limited. Reference page 32.
  - The parking aisle location on the south side of the proposed development prevents most amenity spaces from having adequate solar access.
  - Pedestrian access can only occur within the north setback or within the parking aisle.
  - Flat roofs are not a contextual response, neither in terms of adjacent structures or solar access.
  - Only five of nine units have southern exposure.
- Departures
- No departures requested.



1 View looking Northeast from Rainier Avenue S



1 Site Plan



2 Aerial View looking Northeast

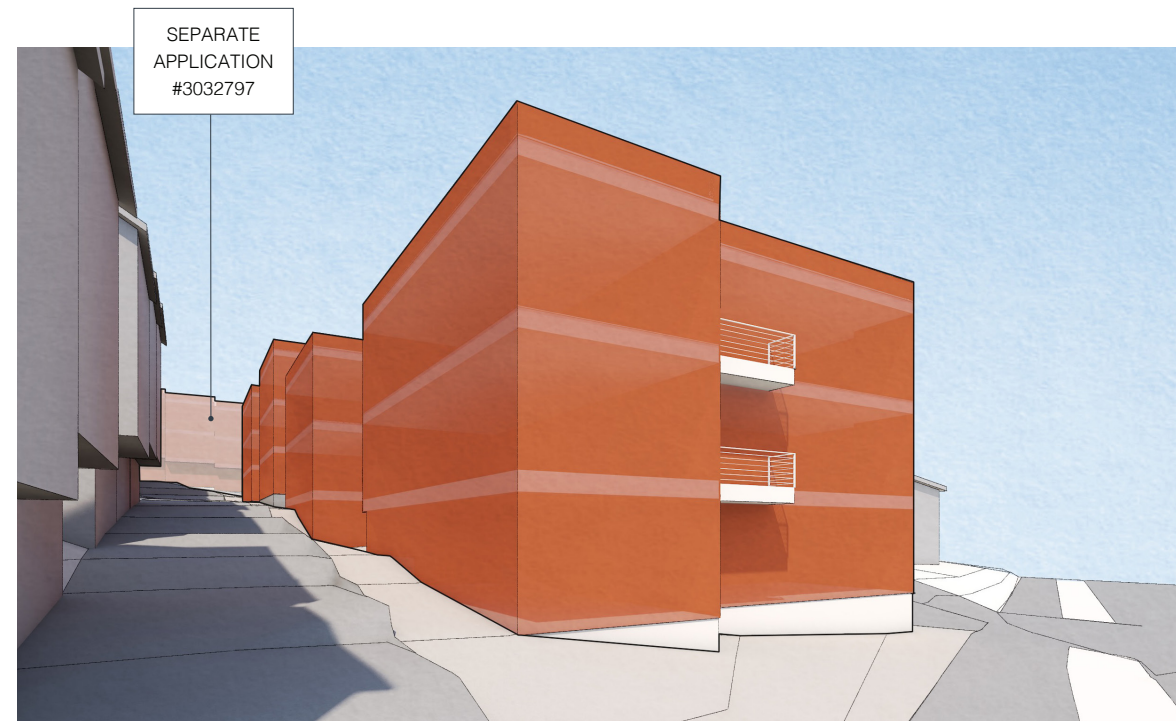




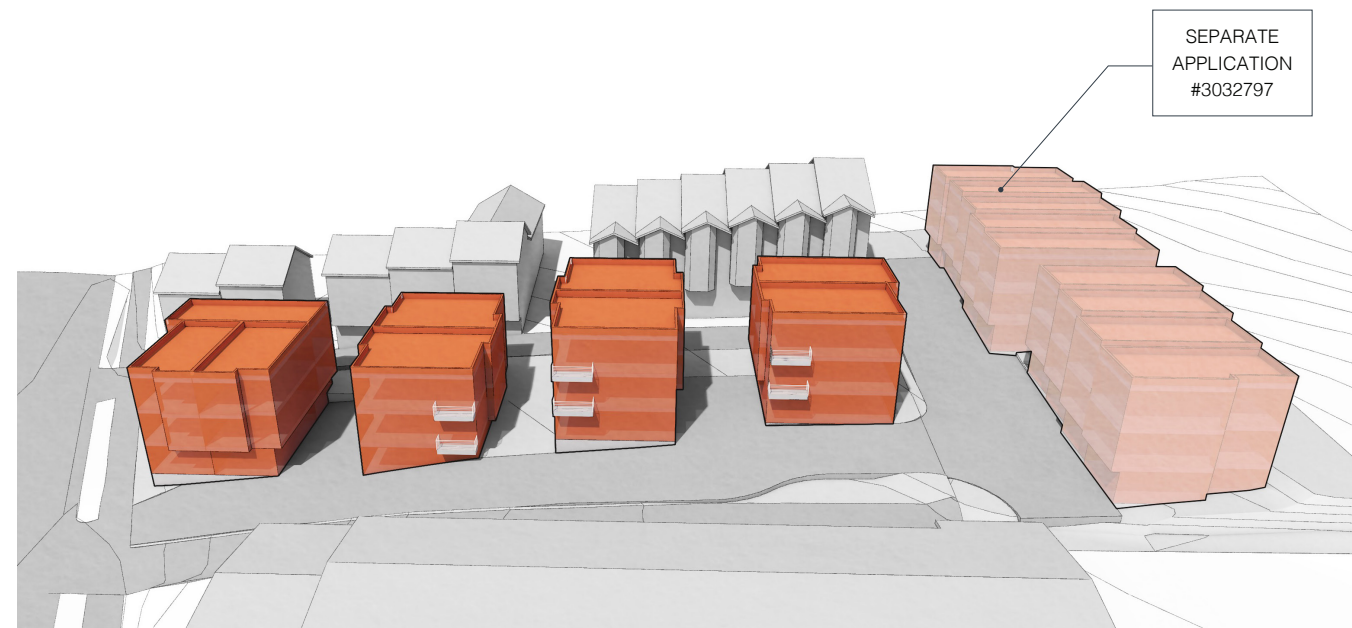
①  
Typical Upper Level Plan



①  
Ground Level Plan



③ View looking Southeast from Rainier Avenue S



④ Aerial View looking North



# ALTERNATIVE 2

Alternative 2 is a code compliant scheme with two three-story structures, containing a total of (9) townhouse units. The existing one-story apartment building on site is proposed to be removed. All units are parked in private garages at grade in each unit, accessed by a parking aisle located at the center of the site. All garages are accessed directly from the parking aisle.

All units in alternative 2 are oriented toward the center parking aisle. Pedestrian access can occur in the north or south side setbacks or within the parking aisle. All units have southern exposure and balconies that face south.

Common amenities areas are located in the four corners of the site. Where allowable, private amenity areas have been provided between units. Alternative 2 has shed roofs that slope up to the south, which provide a contextual response to surrounding structures and allow for increased daylighting opportunities.

Units

- 9 units.
- 9 parking stalls.

Advantages

- Code compliant scheme.
- Shed roofs that slope up to the south provide a contextual response to surrounding structures and allow for increased daylighting opportunities.
- All units have balconies that face south.

Issues

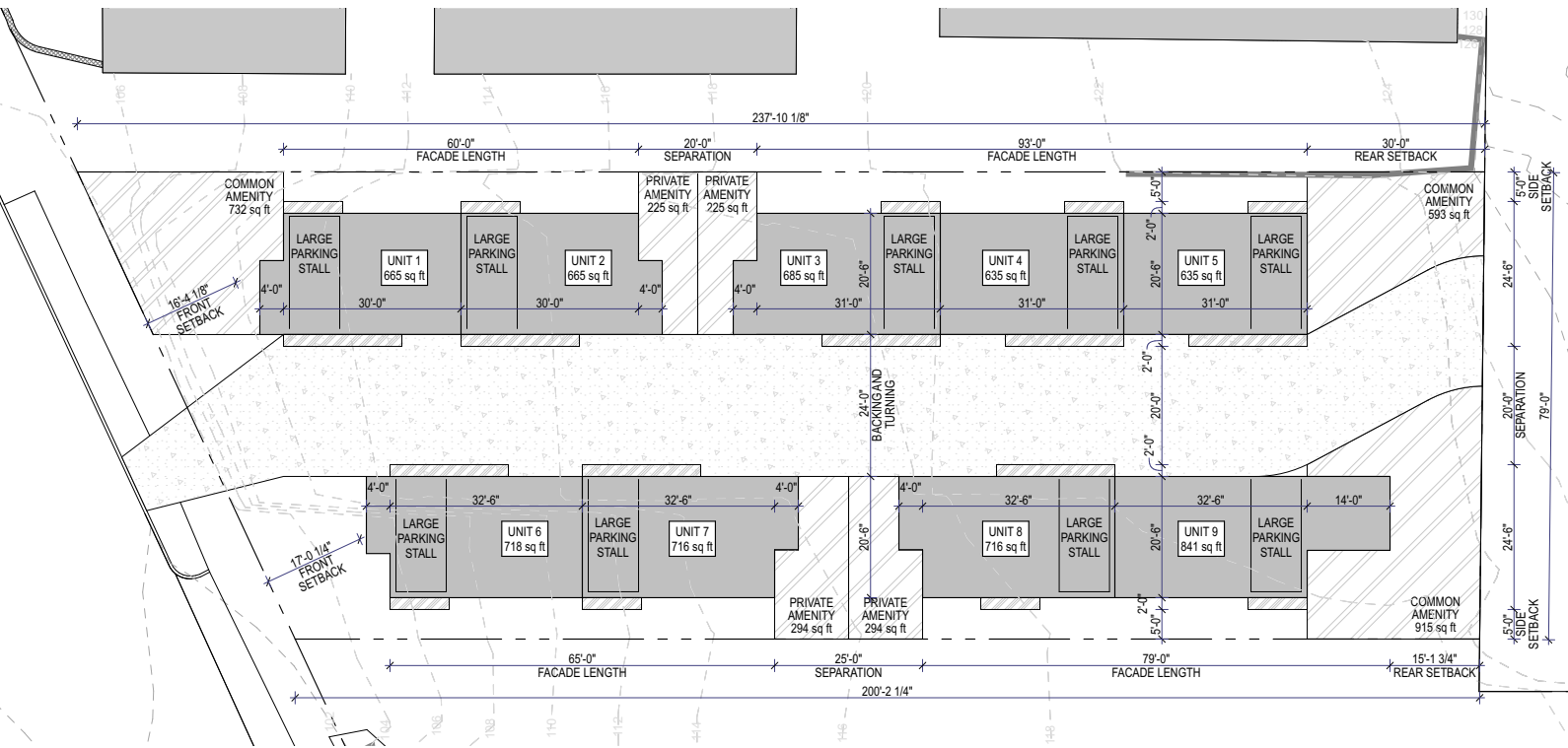
- The parking aisle occupies the center of the site and makes potential connections between structures difficult.
- The north and south side setbacks are at their minimums and do not provide relief to adjacent properties.
- Pedestrian access can only occur within the north or south setback or within the parking aisle.
- Though all units have southern exposure, the double loaded approach would result in the north five units being shaded.
- Open spaces are fragmented and only connected through the narrow space between the structure and setbacks.

Departures

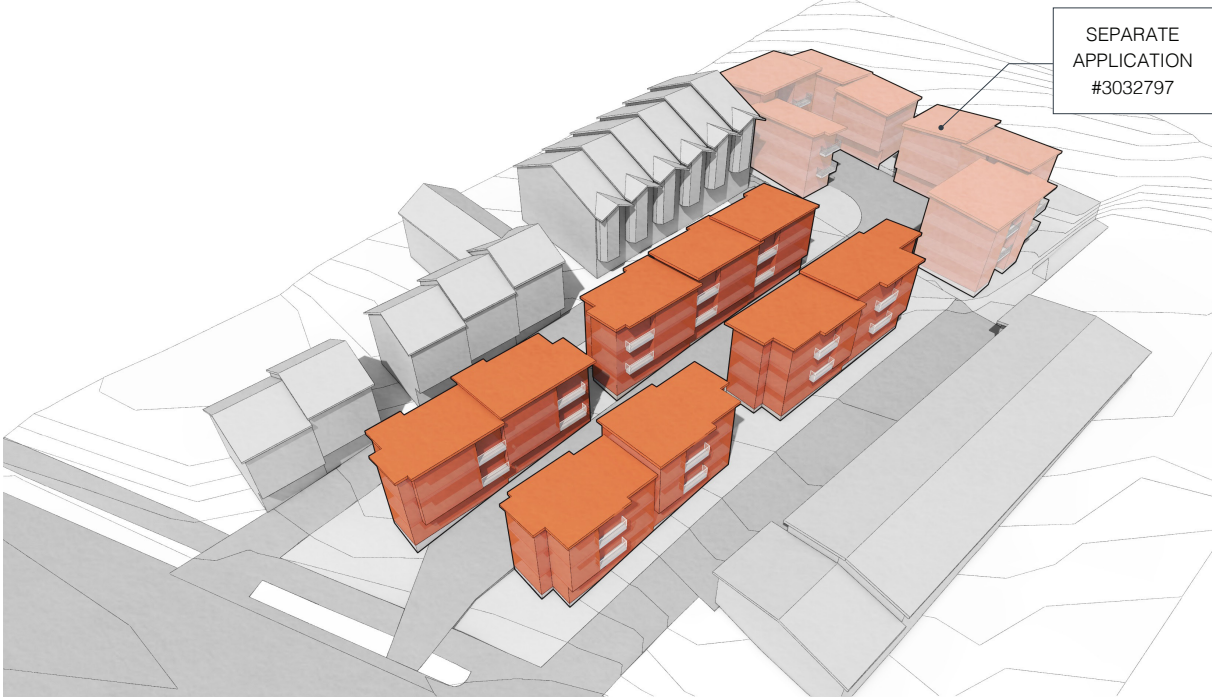
- No departures requested.



1 View looking Northeast from Rainier Avenue S



1 Site Plan

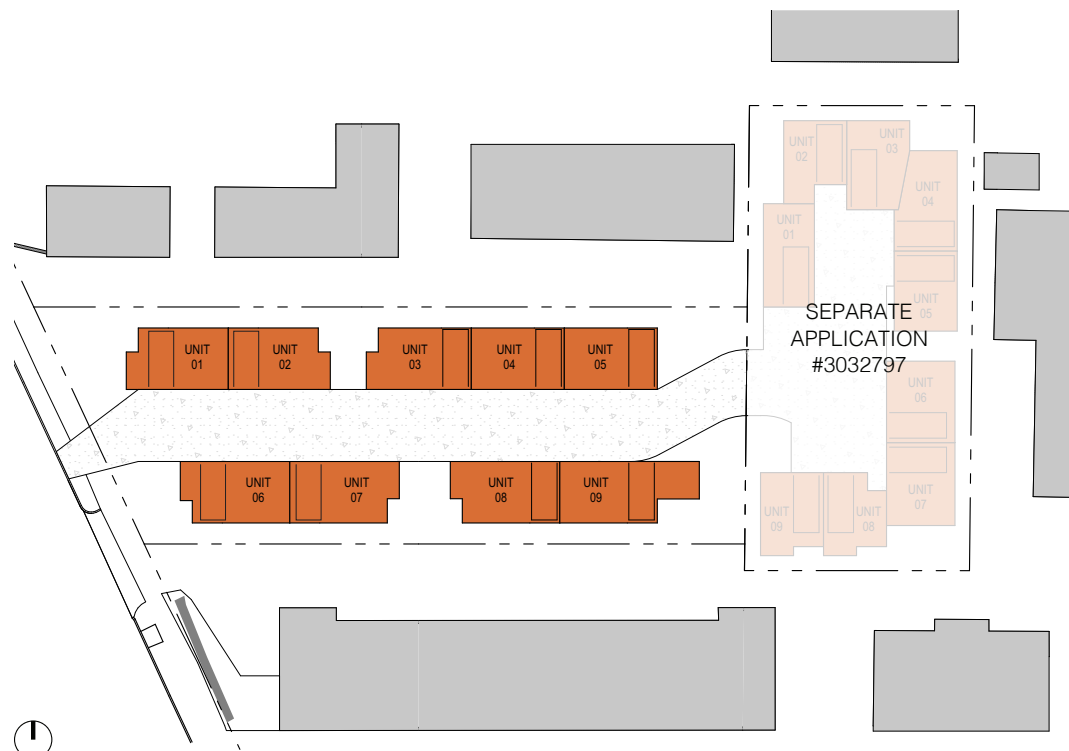


2 Aerial View looking Northeast

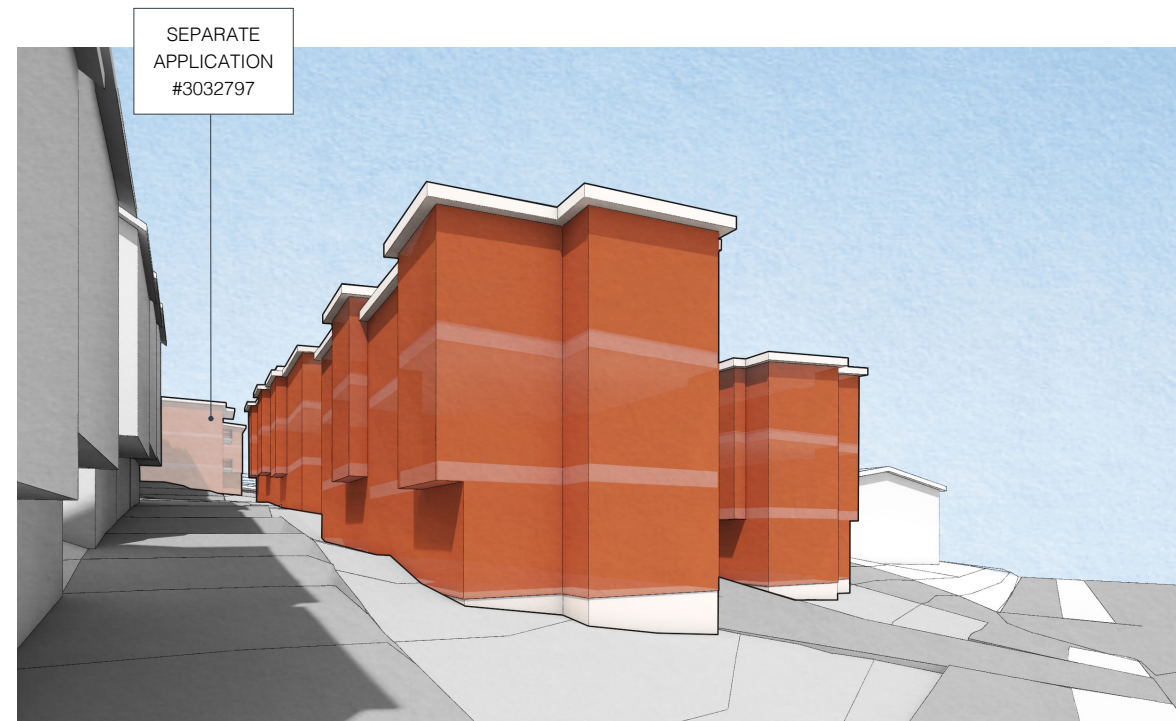




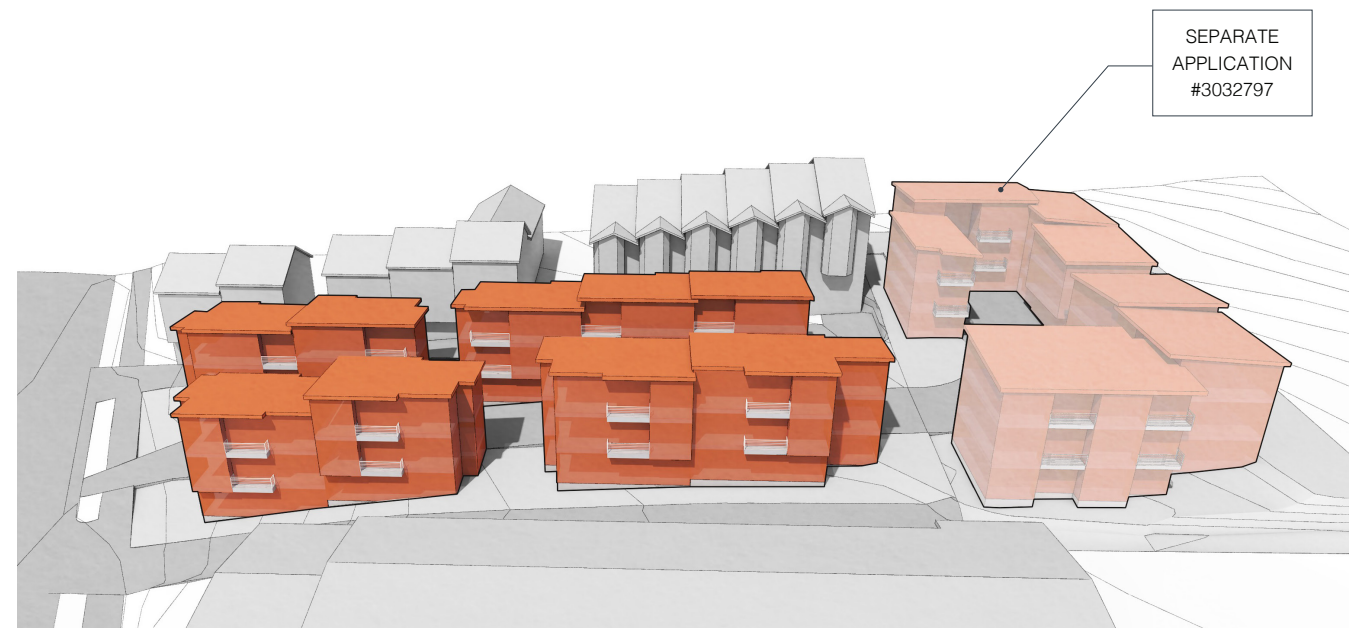
Typical Upper Level Plan



Ground Level Plan



3 View looking Southeast from Rainier Avenue S



4 Aerial View looking North



ALTERNATIVE 3 (PREFERRED)

Alternative 3, our preferred option, is a code compliant scheme with two three-story structures, containing a total of (9) townhouse units. The existing one-story apartment building on site is proposed to be removed. All units are parked in private garages at grade in each unit, accessed by a parking aisle on the north side of the site. All garages are accessed directly from the parking aisle.

The units in Alternative 3 are located in the middle of the site. This location allows for vehicles and pedestrians to be separated. The majority of amenity spaces in Alternative 3 are located on the south side of the structure. These can either be private or common. The amenity spaces are contiguous and provide a generous open space in aggregate. Pedestrian access will occur through these amenity areas and will connect to the adjacent lot to the east, 6724 Rainier Ave S, which is being developed concurrently with this project.

Alternative 3 has shed roofs that slope up to the south, which provide a contextual response to surrounding structures and allow for increased daylighting opportunities.

Units

- 9 units.
- 9 parking stalls.

Advantages

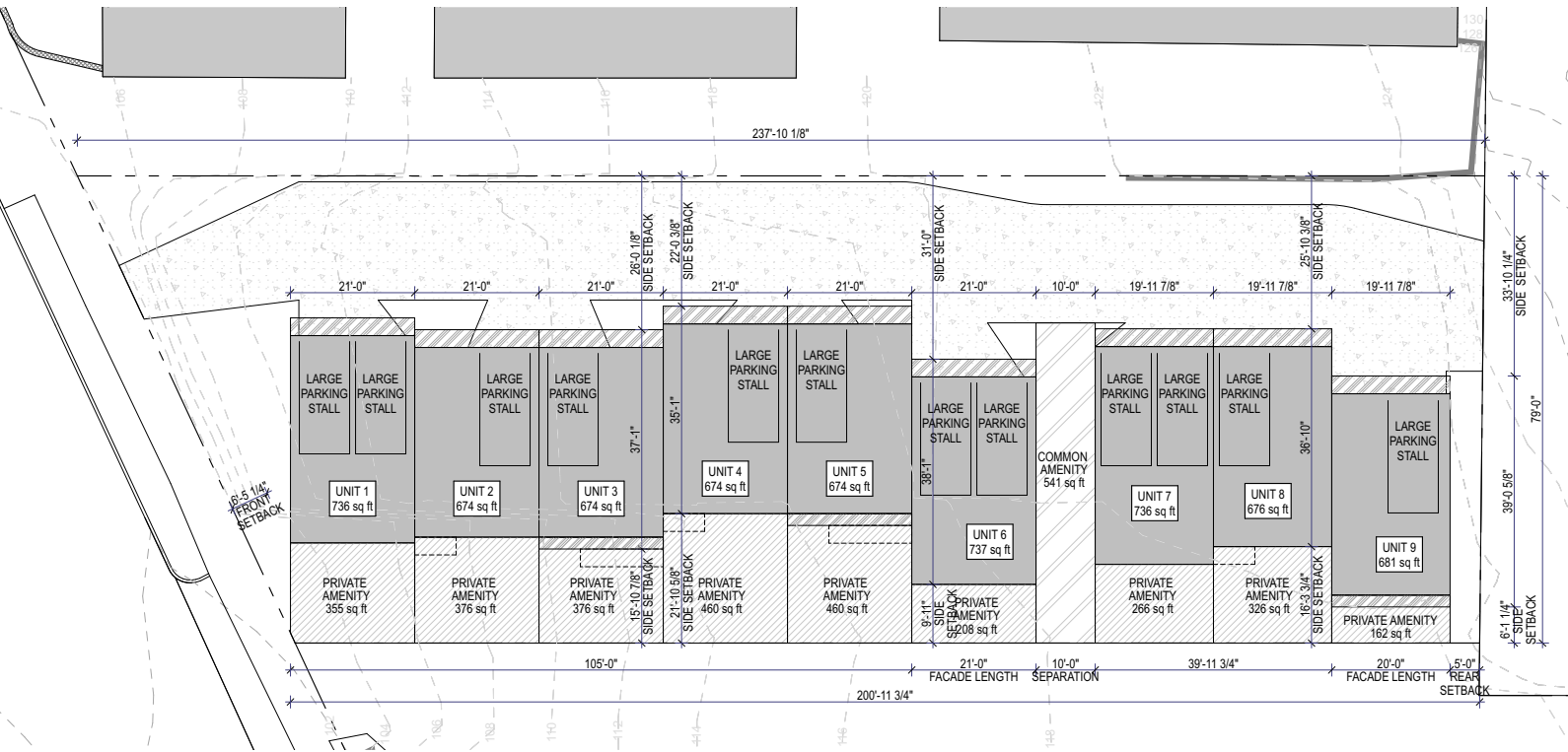
- Code compliant scheme.
- All units have southern exposure.
- All units have balconies that face south.
- Vehicles and pedestrians are separated.
- Pedestrian walkway will be provided in the generous south setback, significantly larger than code requirements, which will support a range of activities with southern exposure.
- Vehicular access and parking is provided to the north, creating a generous north setback, significantly larger than code requirements.
- Continuous amenity spaces provide a generous open space in aggregate.
- Shed roofs that slope up to the south, provide a contextual response to surrounding structures and allow for increased daylighting opportunities.
- Generous north and south side setbacks reduce impacts on adjacent structures.

Departures

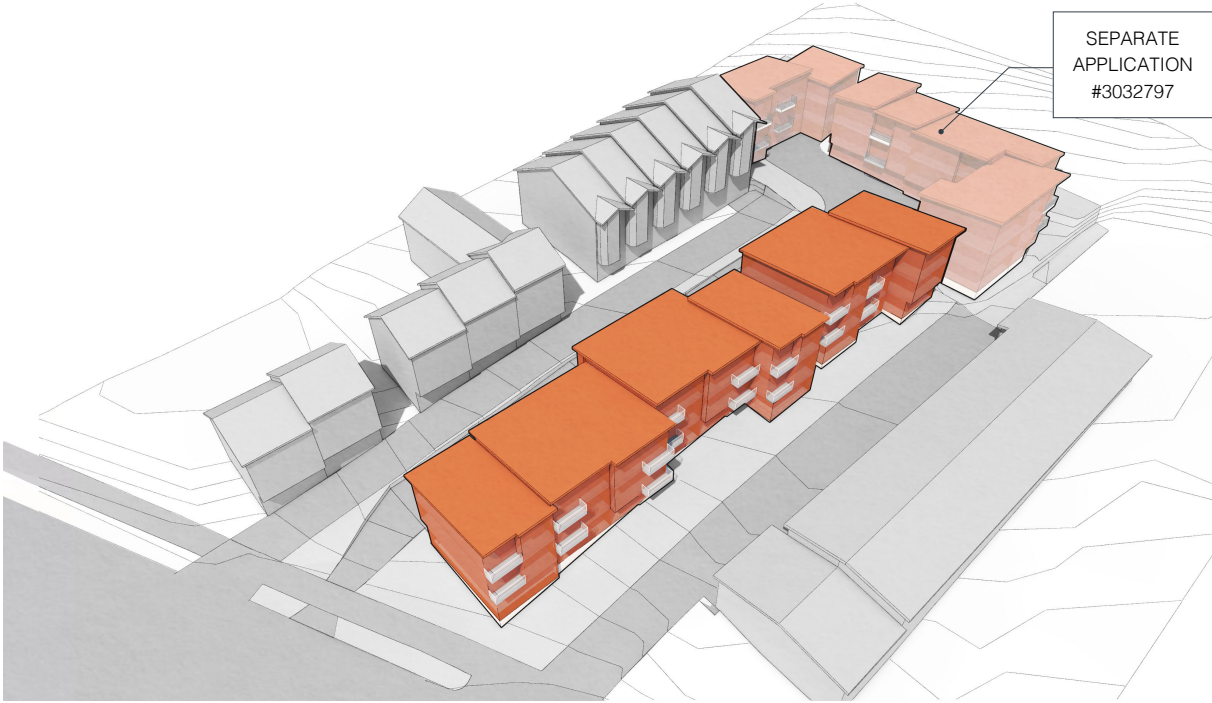
- No departures requested.



1 View looking Northeast from Rainier Avenue S

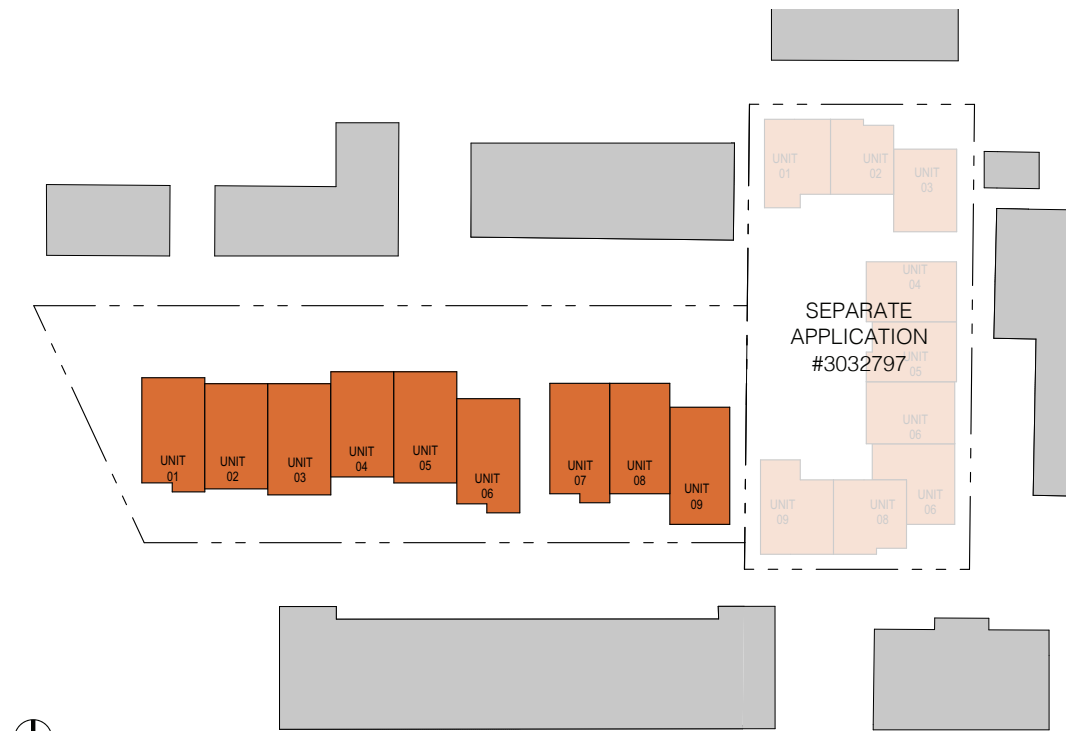


1 Site Plan

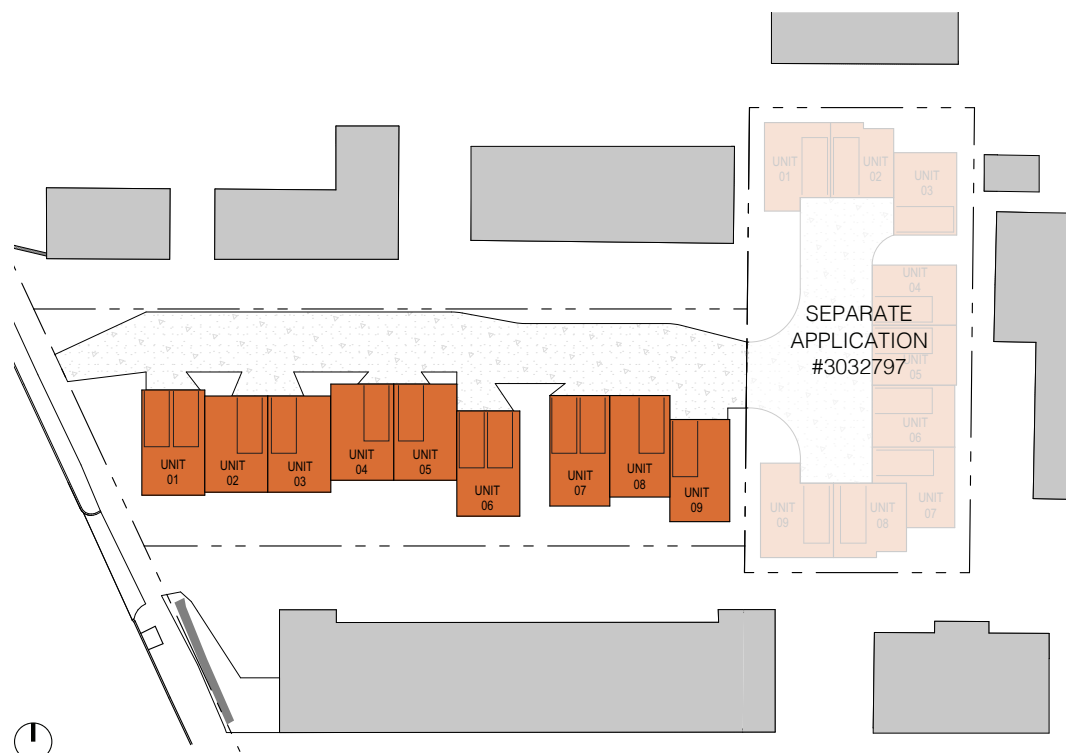


2 Aerial View looking Northeast

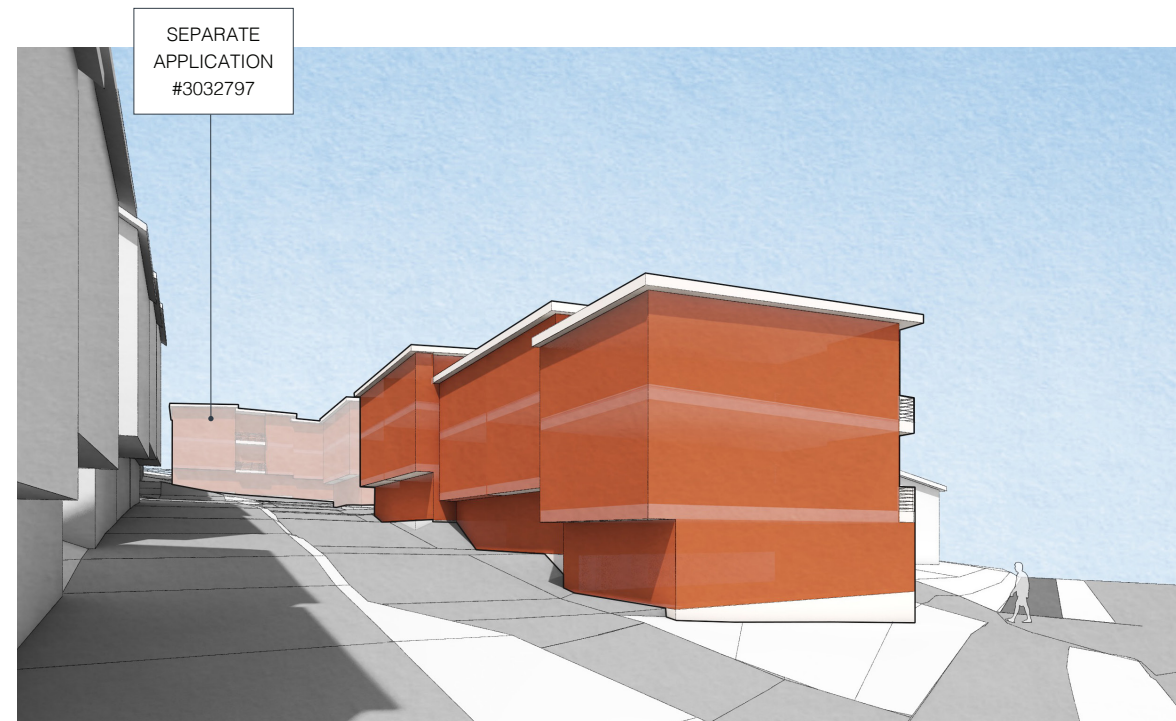




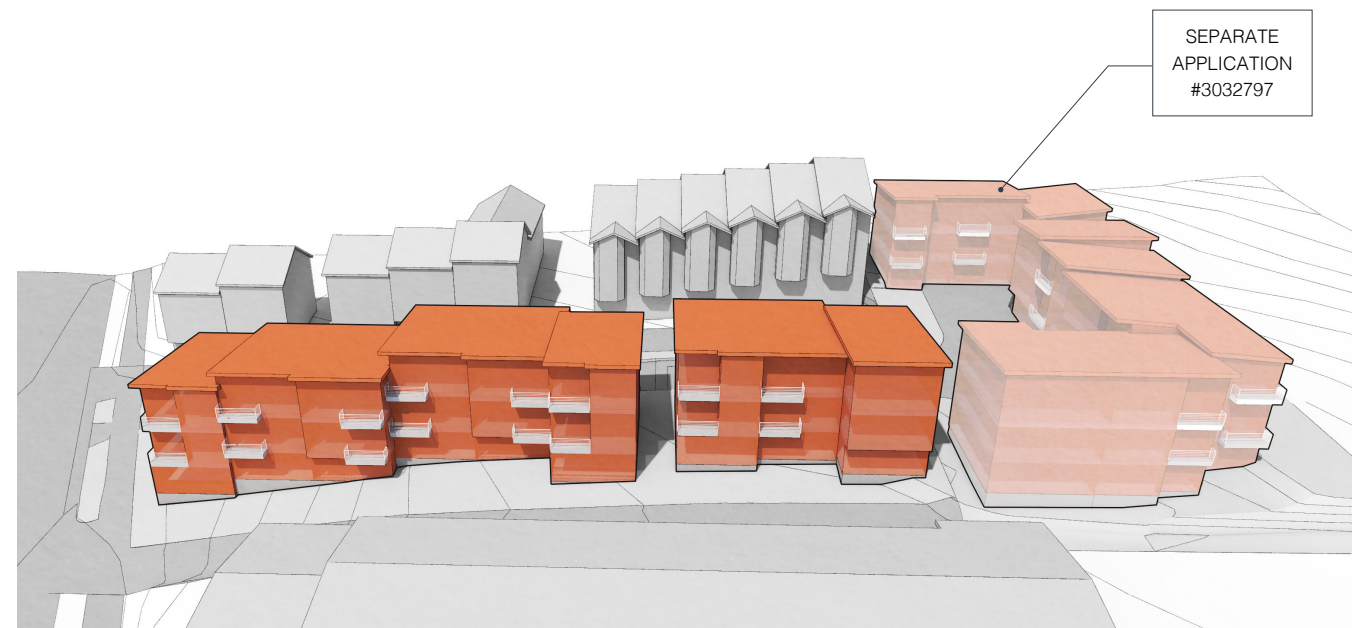
①  
Typical Upper Level Plan



①  
Ground Level Plan



③ View looking Southeast from Rainier Avenue S



④ Aerial View looking North



# OPEN SPACE COMPARISON

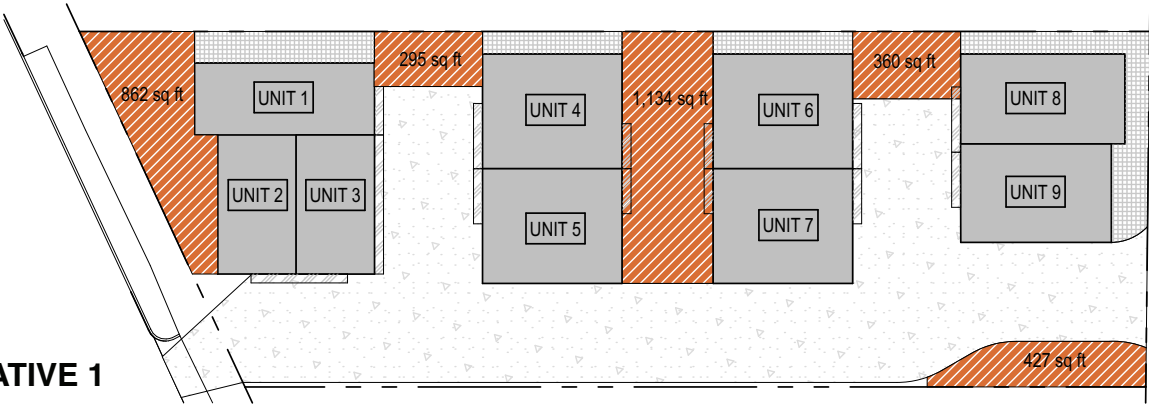
This mid-block site is designed to complement the adjacent site to the east at 6724 Rainier Avenue S. Pedestrian and vehicular access is from Rainier Avenue S, and an access easement will connect to 6724 Rainier Avenue S to the east. The diagrams on the right highlight the open spaces in each of the alternative schemes presented on the previous pages. The areas highlighted in orange are open spaces that are not part of the parking aisle and are not narrow buffers located within setbacks. These spaces can provide common amenity areas or more generous private amenity spaces for individual homes.

This comparison demonstrates the advantage of having the project's units located in the middle of the site, between the parking aisle to the north and large setback with open space to the south. In the Preferred Alternative, the aggregate open space is larger than those in the other alternatives and provides a continuous buffer to for the homes to the site to the south.

LEGEND

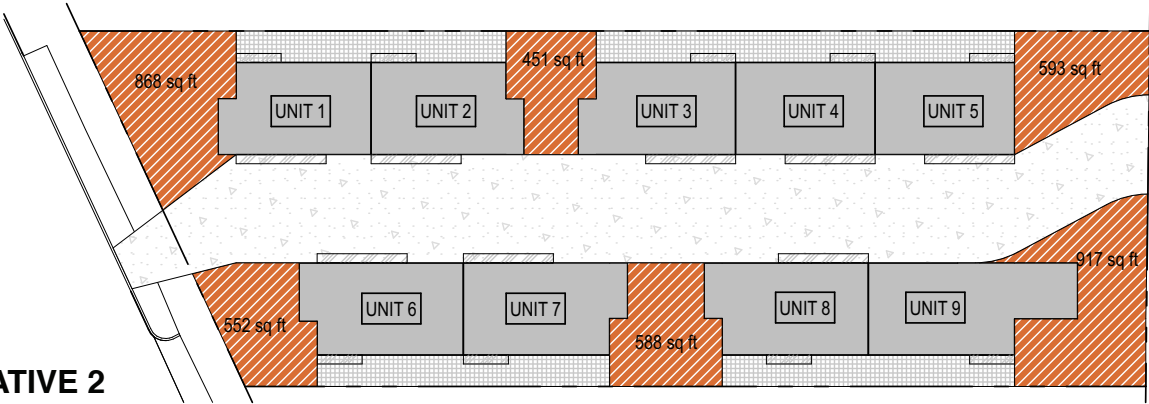
- Unit
- Driveway
- Narrow open spaces at setback
- Open spaces large enough for common amenity area
- Structure Overhang

ALTERNATIVE 1



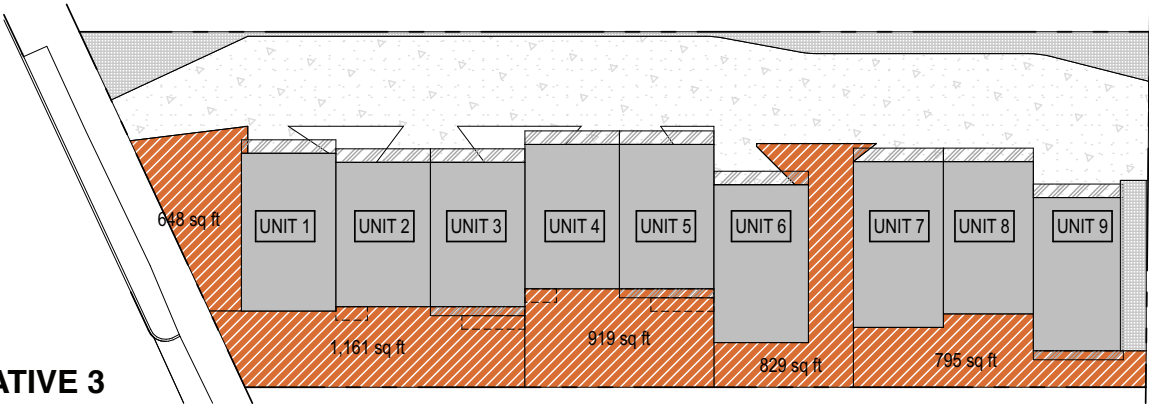
ALTERNATIVE 1 OPEN SPACE	
Area	
295 sq ft	
360 sq ft	
427 sq ft	
862 sq ft	
1,134 sq ft	
<b>3,078 sq ft</b>	

ALTERNATIVE 2



ALTERNATIVE 2 OPEN SPACE	
Area	
451 sq ft	
552 sq ft	
588 sq ft	
593 sq ft	
868 sq ft	
917 sq ft	
<b>3,969 sq ft</b>	

ALTERNATIVE 3  
(PREFERRED)



ALTERNATIVE 3 OPEN SPACE	
Area	
648 sq ft	
795 sq ft	
829 sq ft	
919 sq ft	
1,161 sq ft	
<b>4,352 sq ft</b>	



## OPEN SPACE PRIORITIES:

The site has a unique relationship to the adjacent lot to the east, providing all vehicular and pedestrian access. In response, the design prioritizes a strategy that balances the shared walkway with individual unit entries. The open spaces on site will provide access for all residents and visitors to individual homes as well as provide areas for congregating and private retreat. Below is a list of those priorities:

1. The shared pedestrian walkway will act as the pedestrian access route for all residents from Rainier Avenue S. The walkway will support a pleasant pedestrian experience and act as a wayfinding element for residents and their visitors.

2. Entries and Landscape: Secondary architectural features such as balconies, canopies, and stoops will be used to help define spaces along the shared walkway and in the common amenity area. Planters and landscape areas will help soften areas adjacent to unit entries and highlight locations for different activities to take place.

3. Thresholds: Site features such as arbors will be used to create thresholds on site. Thresholds help define space for residents and will add a sense of security to the development.



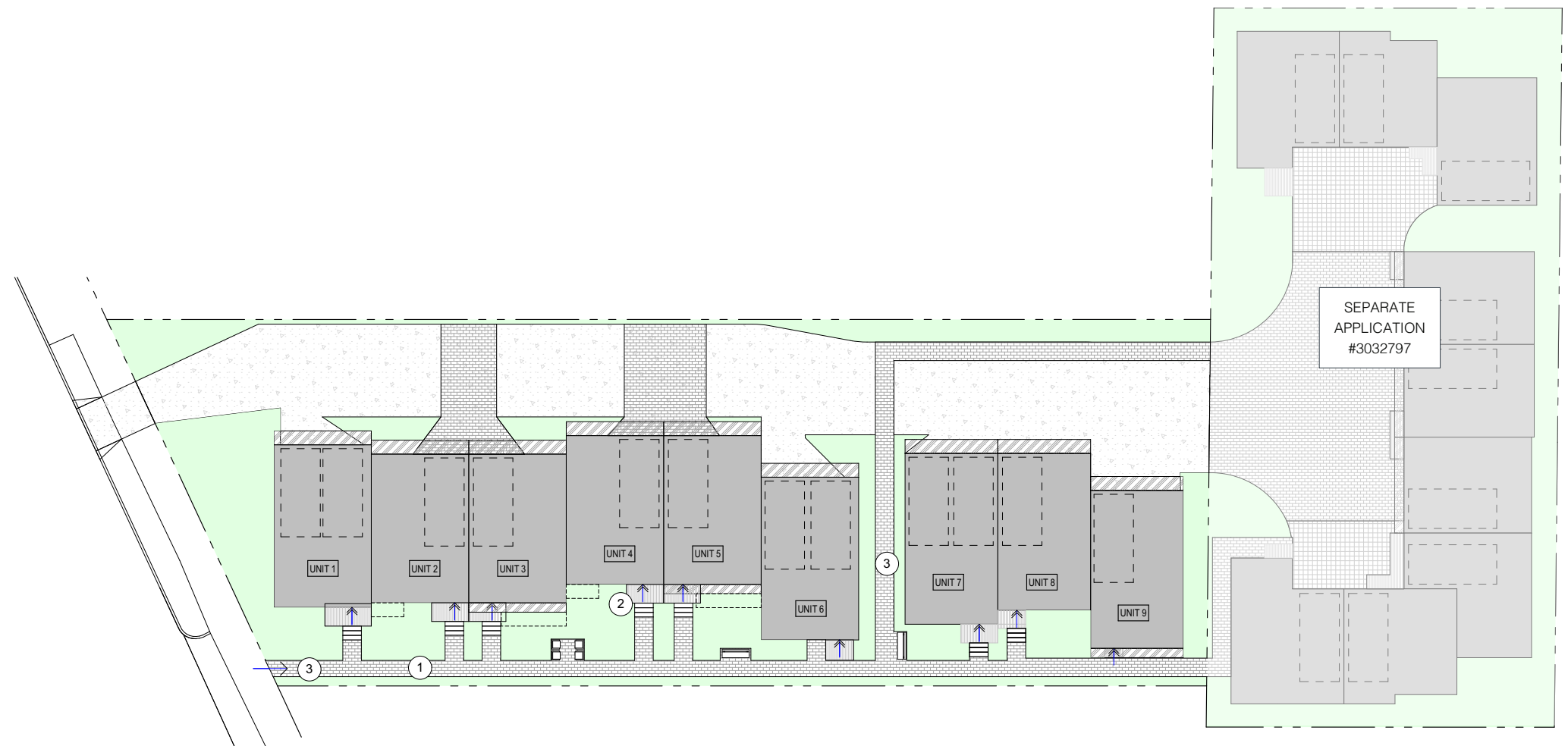
1 Shared Pedestrian Walkway



2 Entry stoops and landscape



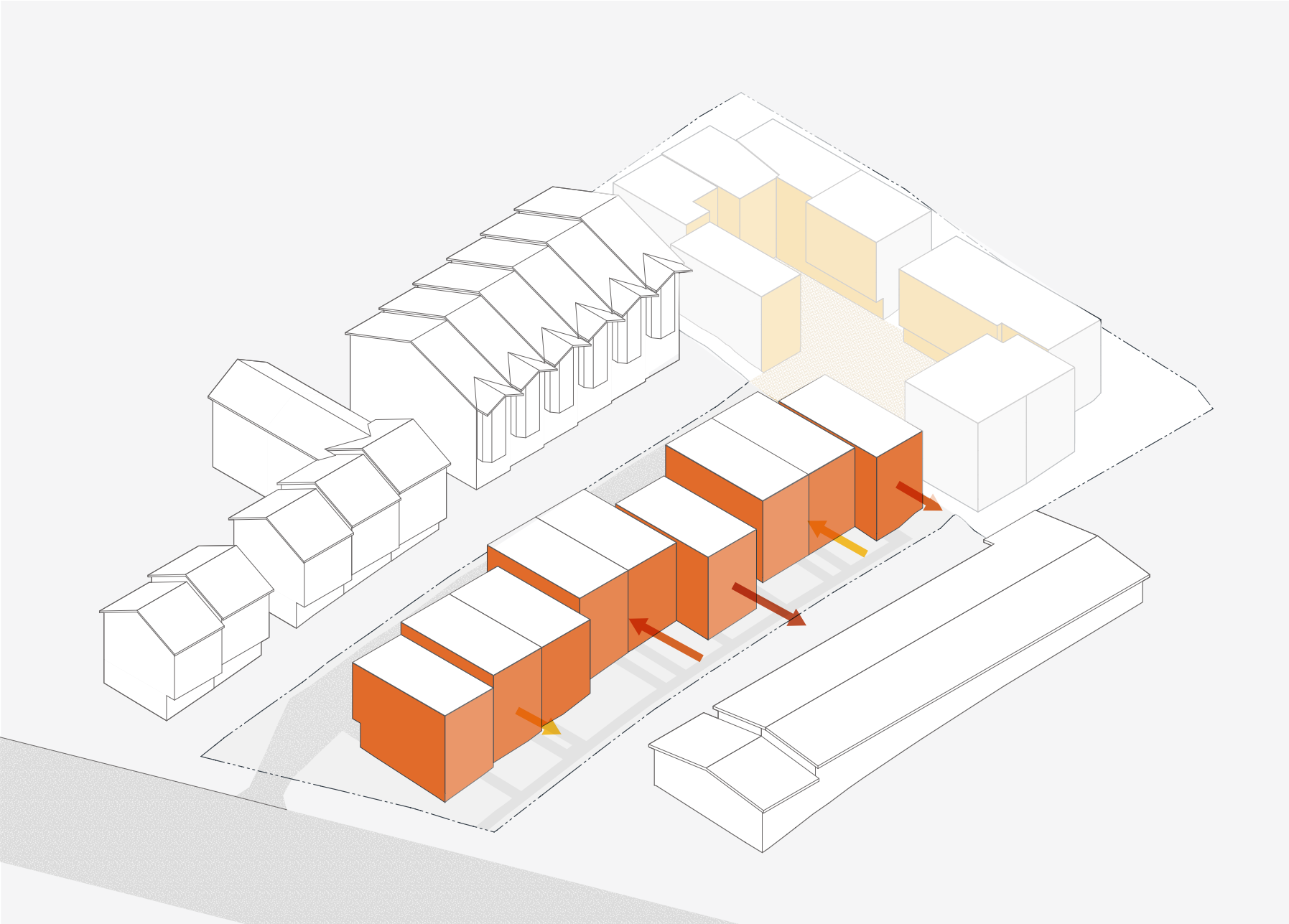
3 Thresholds





DESIGN CONCEPT

MASSING PRIORITIES:



1 Shift units horizontally to create varied experience along pedestrian path.

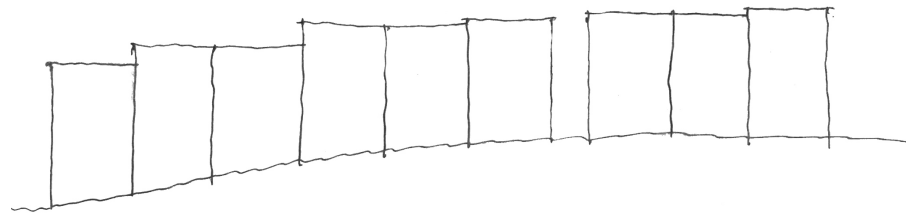


2 Allow topography to be expressed in vertical modulation of structure, emphasizing unit differentiation and informing facade patterning.

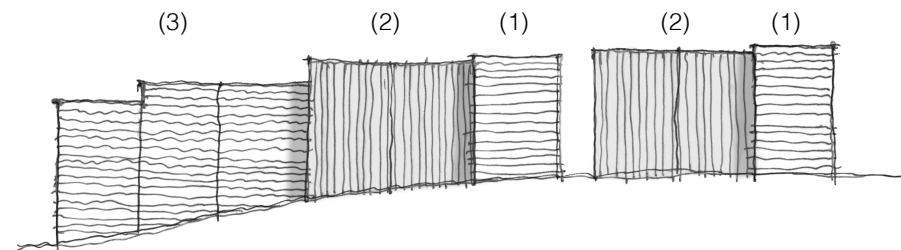


3 Use vertical material changes to break up planar facades, highlight unit entries and inform locations for secondary architectural features.

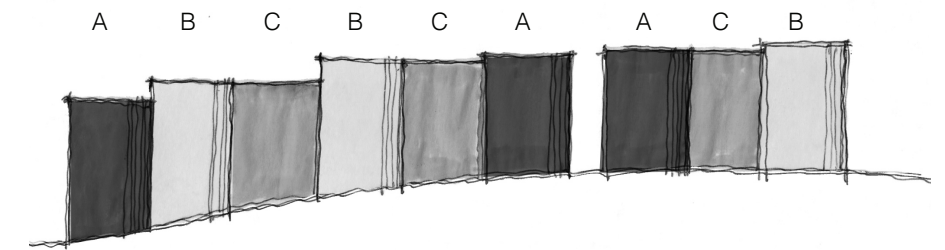




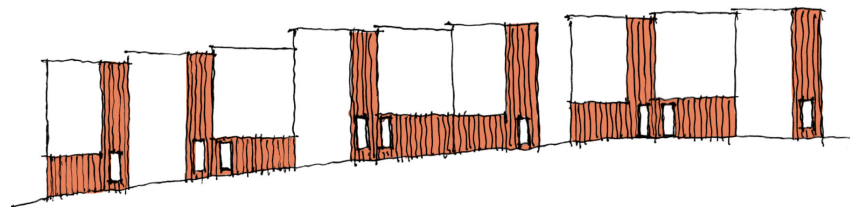
Basic unit massing informed by topography



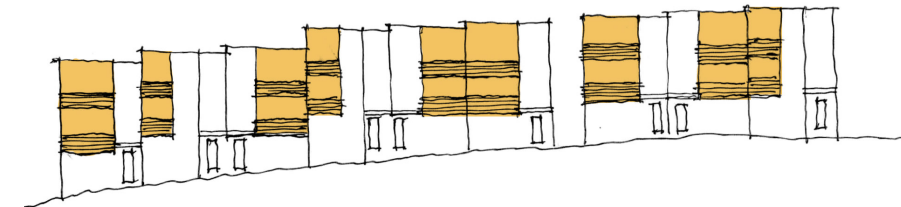
Coplanar facade groupings resulting from horizontal unit shifting



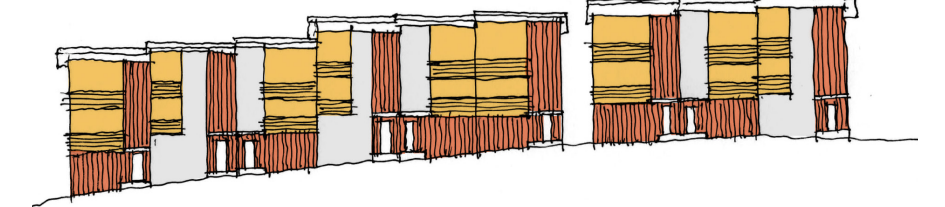
Unit patterning informed by unit type, location in structure and coplanar adjacencies.



Vertical material changes break up planar facades and highlight unit entries



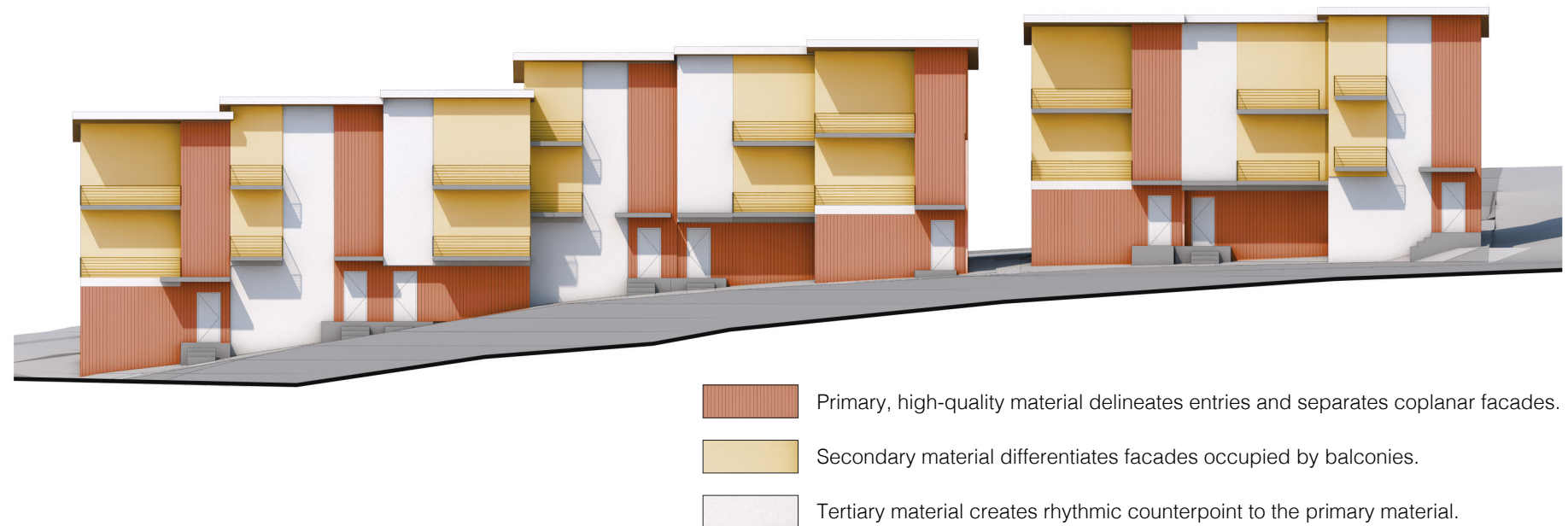
Balcony locations and associated facades



Facade development

## FACADE DEVELOPMENT:

The design concept and subsequent facade development for this project originates in the site plan strategy and response to the site's topography. The site plan organizes the units in a linear approach across the site, oriented east-west, with all units having access to southern and northern exposure. End units will also access eastern or western exposure. Individual units are shifted in the north-south direction to create a more dynamic massing strategy and varied pedestrian experience, limiting the amount of coplanar facades. In addition, the design concept responds to the site's topography by shifting the units vertically along the slope to provide each unit with varying entry and roof heights. This results in a series of volumes, descending with the slope of the site, in which each unit is distinguished. The proposal will build upon this idea by implementing facade elements and massing strategies that highlight individual unit expression and reduce in the perceived scale of the structures.



- Primary, high-quality material delineates entries and separates coplanar facades.
- Secondary material differentiates facades occupied by balconies.
- Tertiary material creates rhythmic counterpoint to the primary material.



LANDSCAPE CONCEPT





# LANDSCAPE CONCEPT



Rose Creek



Paperbark Maple



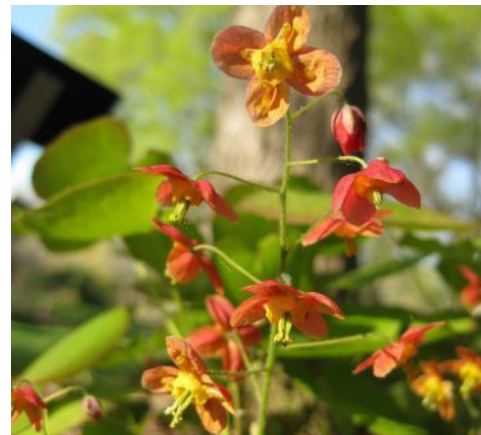
Green Tower



Evergold



Dogwood



Orange Queen



Salal



Red Velvet



Mahonia



Walkers Low



Parrotia



Rhododendron



DESIGN GUIDELINES

CONTEXT AND SITE

CS1 NATURAL SYSTEMS & SITE FEATURES

CS1.B.1 - Sun and Wind

The site plan for the preferred alternative allows all units to receive southern exposure by providing a large buffer between the south facade and adjacent properties. All units in the preferred alternative are oriented north-south and are expected to have large openings on their south sides. Common amenity areas in the preferred alternative have southern exposure as well.

CS1.C.1 - Landform

The conceptual proposal on page 35 references the site's east-west slope as a main factor in the architectural concept. The proposal responds to the site's topography with units set at different elevations to minimize their height. This results in each unit being distinguished as they step along the sloping topography. This relationship to the site's landform informs the pedestrian experience at grade. Entries and stoops are easily identified, resulting in a positive experience along the shared walkway, yet has a sense of privacy.

CS2 URBAN PATTERN & FORM

CS2.A.2 - Architectural Presence

The site's dimensions result in the proposed development having a limited connection to the street. Since the majority of the proposal's facades face a side lot line, the proposal prioritizes a design that responds to the internal conditions of the site. The proposed structures are architecturally contextual, responding to site conditions with units shifting in the north-south direction to activate its internal facades. Roof forms, materials, and detailing will relate to surrounding structures and complement the massing strategy of the project. The proposal will be unique, however, through the organization and patterning of massing, materials, and facade elements as exhibited in the design sketches on page 35.

CS2.B.2 - Connection to the Street

Though the project has limited street frontage, it remains an important aspect of the proposed alternative. The preferred site plan separates vehicular and pedestrian access in order to create a quality pedestrian experience for both resident and passerby. The landscaped buffer between the property line and building will be a priority in design development as it seeks to create a space that both engages the public and provides amenity for residents. The sole building facade facing the street will be composed of durable materials and represent the priorities of the overall design concept. Vehicular access is proposed to be separate from the main pedestrian path in the Preferred Alternative.

CS2 URBAN PATTERN & FORM

CS2.B.3 - Character of Open Spaces

The separation of vehicle and pedestrian access is a primary priority in the Preferred Alternative. This separation allows the open spaces along the south property line to span continuously along the unit entries. The shared walkway sponsors activity and creates the potential for adjacent units to connection while allowing for private experiences as well. This orientation allows the space to be used for many different types of activities: semi-public, private, or a combination thereof. The open spaces have solar access and are easily reached by all residents. Moreover, it allows the project to contribute to the existing pedestrian network in the surrounding neighborhood.

CS2.D.5 - Respect for Adjacent Sites

The proposed site organization locates the units in a linear organization at the center of the site. This location allows for generous north and south side setbacks between the proposed and the adjacent structures in excess of what is required by code. The north side setback will maintain the southern exposure of the existing, adjacent structures to the north. The south side setback provides a landscape buffer and fence to the adjacent site to the south.

CS3 ARCHITECTURAL CONTEXT & CHARACTER

CS3.A.4 - Evolving Neighborhoods

The proposal is located in an evolving neighborhood, which features many older structures as well as lots that are currently under development. This project contributes to a positive context that the evolving neighborhood can build upon. Examples of this include: prioritizing the pedestrian experience in the site plan, creating site-responsive massing through orientation and roof forms and using the landform as a positive influence on the project's design concept.

PUBLIC LIFE

PL1 CONNECTIVITY

PL1.A - Network of Open Spaces

The proposal supports a network of open spaces through the creation of a continuous area containing a mix of private and common amenity areas on the south side of the structure. Its location and continuity allow the amenity area to foster interaction between residents through the adjacency of entries and their connection to the shared pedestrian path. A mix of common and private amenity areas along the shared walkway, promotes a sense of community on-site. Moreover, projected balconies are located above the pedestrian path to further increase the density and activity of the common amenity area.

PL1.B.1 - Pedestrian Infrastructure

The proposed shared pedestrian path is accessed directly from the public sidewalk on Rainier Avenue S. This connection promotes pedestrian activity on the site where it can be seen by residents and in the neighborhood and offers a safe connection to the street for residents and visitors. The primary pedestrian path links Rainier Avenue S to the site to the east that is being developed concurrently with this project.

PL1.C.1 - Selecting Activity Areas

Activity areas have been located on the south side of the structure along the main pedestrian path. These areas are identified as part of the site planning process due to their relationship to adjacent sites and solar exposure.

PL2 WALKABILITY

PL2. B Safety and Security

Exterior walkways and decks will provide lines of sight throughout the project site. Lighting will provide sufficient illumination for pathways and entries.

PL2.C - WEATHER PROTECTION

Overhead weather protection is integrated into each of the building entries through either a recessed entry or overhead canopy projection. Weather protection elements are a part of the patterning implemented on the building facades.

PL2.D - WAYFINDING

Clear and prominent wayfinding is integrated into the project's site plan. The pedestrian path from the street is clearly defined through its materiality and planned lighting. Individual unit entries are highlighted on the facade and unit addressing will be clearly located.

PL3 STREET-LEVEL INTERACTION

PL3.A.2 -ENSEMBLE OF ELEMENTS

Unit entries coordinate several design features that are a part of the overall structure facades. These include: canopies, balconies, entry stoops, unique paving at entries, roof overhangs and planters.

PL3.B.1 - SECURITY AND PRIVACY

Due to the topography of the site, the majority of residential first floors will be slightly elevated. This results in an ensemble of exterior stairs and stoops, which activate the amenity spaces and adjacent shared walkway, while providing a vantage point of security and privacy for the interior living spaces. Additionally, the unit entries follow the existing topography, and in many instances are located at different heights, allowing for a separation in private entry spaces.

PL4 ACTIVE TRANSPORTATION

PL4.A.1 - SERVING ALL MODES OF TRAVEL

The separation of pedestrian access from vehicle access creates a safe experience for pedestrians and bicyclists entering and exiting the site.

PL4.B.2 - BIKE FACILITIES

Bicycle parking will be provided for all the units in an easily accessible location on site.

PL4.C - PLANNING AHEAD FOR TRANSIT

The site is located directly off Rainier Avenue S, a transit corridor connecting to downtown Seattle to the north.



DESIGN CONCEPT

DC1 PROJECT USES & ACTIVITIES

DC.B.1 - Vehicle Access and Circulation - Location

The proposal separates vehicles to the north from pedestrian access to the south. This separation allows for a safe and pleasant experience for pedestrians.

DC.C.2 - Parking and Service Uses - Visual Impact

In order to minimize the effect of the on-site parking all parking will be provided in private garages inside each unit. In response to this, U, the proposed design will emphasize pedestrian elements including unit entries, landscaped planters, and overhead projection. This adds areas of visual interest in order to further balance the visual impact of the required on-site parking.

DC2 ARCHITECTURAL CONCEPT

DC2.A.1 - Massing - Site Characteristics and Uses

The site slopes down to the southwest and is much deeper (235 feet) than it is wide (79 feet). The proposed massing approach responds directly to these site conditions through the implementation of a strategy of stepping heights that follows the sloping topography. The resulting massing is an organization of units that is visually dynamic when viewed from the street or on site.

DC2.B.1 - Architectural and Façade Composition

The design team has worked to generate a well-proportioned composition with special consideration of the fact that the more public facades of the project need to balance vehicular and pedestrian needs. A system of exterior balconies, cantilevers, and massing modulation will be used to highlight certain volumes while maintaining a consistent language throughout the project. Roof forms and varying unit heights add another layer of interest to the composition.

DC2.C - Secondary Architectural Features

The design team is developing a compositional strategy that will employ balconies, roof overhangs, railings, parapets and cantilever treatments to activate the proposed massing and enliven the site. This will occur to varying degrees at all edges of the site.

DC3 OPEN SPACE CONCEPT

DC3.A.1 - Building Open Space Relationships

The design alternatives present different strategies for incorporating open space into the development, reference page 32. The Preferred Alternative, Alternative 3, places the structure at the center of the site, allowing for a large continuous exterior amenity area and circulation space. The quality of this space is strengthened through its relationship to unit entries and to the larger network of shared pedestrian pathways. The primary shared pedestrian path connects this site with the adjacent site to the east and with the sidewalk on Rainier Avenue S.

DC3.B - Open Spaces Uses and Activities

Open spaces will be considered and designed to accommodate a variety of uses. The proposal will incorporate seating and landscape strategies that will activate these spaces. The internal network of entry stoops, private balconies and common walkways provides opportunities for various shared and private uses.

DC4 MATERIALS

DC4.A.1 - Building Materials - Exterior Finish Materials

The proposal will include durable materials consistent with similar project types in the neighborhood. Materials and finishes will create an attractive and inviting street edge. Pattern and rhythm within the façade will be highlighted through variation of materials and finishes as a way of reducing the overall scale of the building and identifying individual units.

DC4.B.2 - Signage - Coordination with Project Design

Signage will be minimal but effective, supporting wayfinding and the design approach of the proposal. The design team proposes an approach to address signage that is incorporated with weather protection and is of a consistent design language as those secondary architectural elements of the proposal.

DC4.C.1 - Lighting - Functions

Lighting will promote safety and security, while also respecting the adjacent properties and units on site. It will add detail to the pedestrian realm and support on-site wayfinding.

DC4.D.1 - Trees, Landscape and Hardscape Materials

The landscape design will incorporate the various open spaces at the edges and center of the site. A buffer will be created to Rainier Avenue S to support a clearly identifiable entry in the Preferred Alternative, and to shelter the units closest to the street. The network of open spaces will utilize high quality materials consistent with this scale of development. These materials and landscape elements will complement the architecture.



COMPLETED WORK b9 ARCHITECTS



Townhomes at 416 19th Avenue E



Townhomes at 416 19th Avenue E



Townhomes at 1623 14th Avenue



Townhomes at 1623 14th Avenue





Rowhouses at 2847 Franklin Avenue E



Rowhouses at 2847 Franklin Avenue E



Rowhouses at 90 E Newton Street



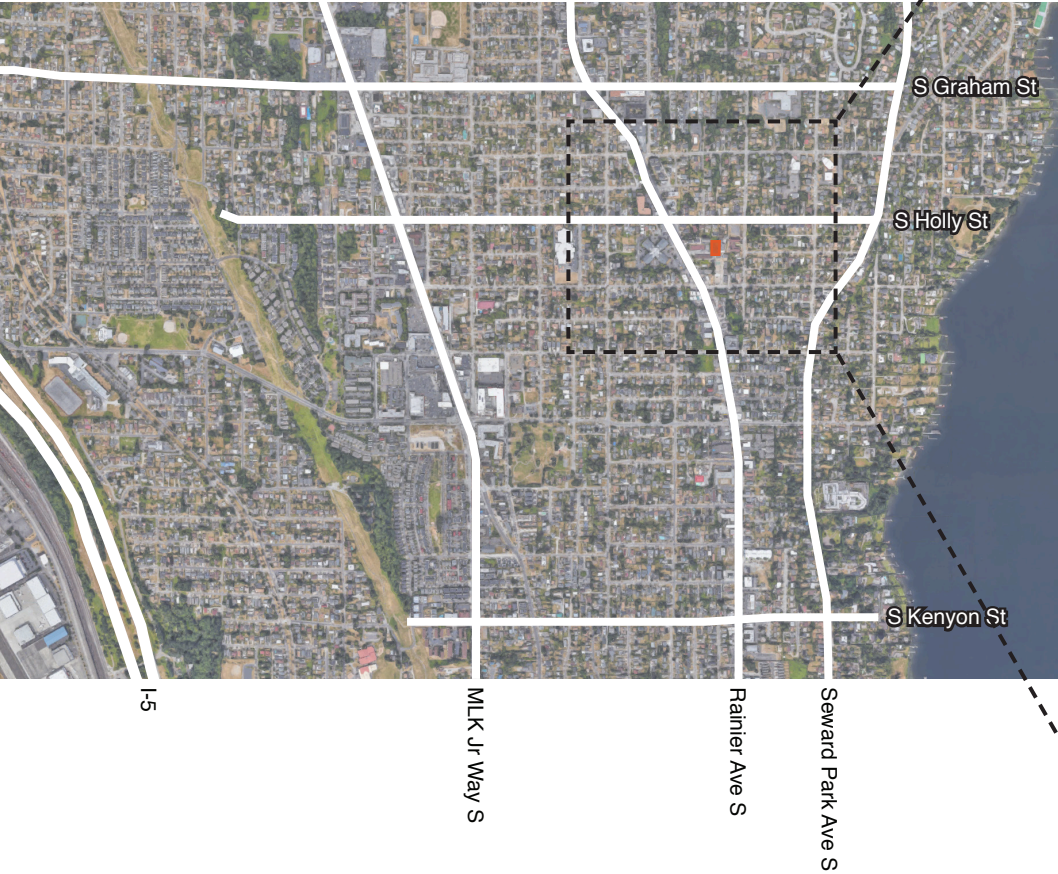


# 6724 Rainier Avenue S

Administrative Design Review (ADR) Packet | Project #3032797 | December 7, 2018



Partial Rainier Valley Key Map





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S Holly St

S Brighton St

Rainier Ave S

51st Ave S







Rainier Ave S



## OBJECTIVES

Land Use Application to allow nine 3-story townhouse units. Parking will be provided within each unit with access from Rainier Avenue S through adjacent parcel 2724049050. Site is functionally dependent on 18TMP079376 (#3032795) for parking No existing structures.

Gross Floor Area	---
Number of Units	9
Number of Parking Spaces	9
Number of Bike Parking Spaces	9

Sustainability  
Construct new structure to achieve a 4-Star Built Green certification.

## TEAM

ARCHITECTS	b9 architects
DEVELOPER	Metropolis Properties LLC
STRUCTURAL	--
GEOTECHNICAL	--
LANDSCAPE	--



EDG APPLICATION

PART I: Contact Information

1.	Property address	6724 Rainier Avenue S
2.	Project number	#3032797
3.	Additional related project number(s)	#3032795 (adjacent property to west)
4.	Owner Name	Metropolis Properties, LLC
5.	Contact Person Name	Bradley Khouri
	Firm	b9 architects
	Mailing Address	610 2nd Avenue
	City, State Zip	Seattle, WA 98104
	Phone	206.297.1284
	Email Address	office@b9architects.com
6.	Applicant's Name	Bradley Khouri
	Relationship to Project	Architect
7.	Design Professional's Name	Bradley Khouri
	Email Address	office@b9architects.com
	Address	610 2nd Avenue
	Phone	206.297.1284

PART II: Site and Development Information

1.	Please describe the existing site, including location, existing uses and/or structures, topographical or other physical features, etc.
	<i>The existing site is located mid-block off Rainier Avenue S, between S Holly Street to the north and S Willow Street to the south. The site, which has no street frontage, is accessed through an access easement on the adjacent lot to the west, 6730 Rainier Avenue S, which is being developed concurrently with this project. The site's topography descends from east to west. The site is currently vacant.</i>
2.	Please indicate the site's zoning and any other overlay designations, including applicable Neighborhood Specific Guidelines.
	<i>The lot is zoned LR3 and is not located within an Urban Village.</i>
3.	Please describe neighboring development and uses, including adjacent zoning, physical features, existing architectural and siting patterns, views, community landmarks, etc.
	<i>The immediate neighborhood is residential and consists of, predominantly low-rise multi-family and single-family structures, with a number of duplexes and triplexes. Nearby townhouse developments are contextual in their building form and materials, employing gabled roofs and lap siding. A large townhouse development is under construction south of the proposal. Commercial activity is frequent along Rainier Avenue S and increases substantially beginning 3 blocks to the south. There are no substantial views from the site. Immediately adjacent parcels to the west are zoned LR3. Immediately adjacent parcels to the east and south are zoned SF-5000. Three blocks to the south NC3-40 zoning is located along Rainier Avenue S.</i>
4.	Please describe the applicant's development objectives, indicating types of desired uses, structure height (approx), number of residential units (approx), amount of commercial square footage (approx), and

number of parking stalls (approx). Please also include potential requests for departure from development standards.

*The development proposal consists of two adjacent sites located at 6724 and 6730 Rainier Avenue S. This proposal at 6724 Rainier Avenue S is to construct two three-story structures, comprised of 9 total townhouse units. The development proposes to provide a minimum of one parking stall per unit. All parking will be located in private garages within the ground floor of the units. Approximate structure height is 30', per SMC 23.45.514A. The three design alternatives represent a design exploration and evolution, resulting in a preferred scheme that is contextual in its volume and pedestrian engagement. The design proposes a solution that is respectful of adjacent sites and simultaneously innovative in its massing strategy. The proposal responds to the site's topography, and intends to use materiality and patterning to create rhythm and unit definition. There are no departures requested.*



ZONING SUMMARY

23.45.510 - FLOOR AREA RATIO (FAR) LIMITS

- 1.1
- 1.3 if the project meets the standards of subsection 23.45.510.C.

23.45.512 - DENSITY LIMITS—LR ZONES

- 1/1,600
- No Limit for townhouse developments that meet the standards of subsection 23.45.510.C. Requirements of 23.45.501.C:
  - Proposed development must meet the green building standard.
  - Parking shall be totally enclosed within the same structure as the residential use.
  - Access to required barrier-free parking spaces may be from either a street or an alley.

23.45.514 - STRUCTURE HEIGHT

- 30 Feet base height
- For all residential uses in LR3 zones, the applicable height limit is increased 4 feet above the height shown on Table A for 23.45.514 for a structure that includes a story that is partially below-grade
- Open railings, planters, greenhouses not dedicated to food production, parapets, and firewalls on the roofs of principal structures may extend 4 feet above the maximum height limit

23.45.517 - MULTIFAMILY ZONES WITH A MANDATORY HOUSING AFFORDABILITY SUFFIX

- No MHA requirements outside urban centers and villages in LR3 zones.

23.45.518 - SETBACKS AND SEPARATIONS

Townhouses:

- Front- 7 average; 5 minimum
- Rear- 7 average; 5 minimum
- Side less than 40 feet- 5 minimum
- Side more than 40 feet- - 7 average; 5 minimum

23.45.522 - AMENITY AREA

- The required amenity area in LR3 zones for townhouses is equal to 25 percent of the lot area.
- A min of 50% of the required amenity area shall be provided at ground level.
- Rooftop amenity area per 23.45.510.E.5 may be counted as amenity area provided at ground level.
- All units shall have access to a common or private amenity area.

23.45.524 - LANDSCAPING STANDARDS

- A Green Factor score of 0.6 or greater is required on LR lots with more than one new dwelling unit.
- Street trees are required if any type of development is proposed. Existing street trees shall be retained unless the Director of the Seattle Department of Transportation approves their removal.

23.45.527 - STRUCTURE WIDTH AND FACADE LENGTH LIMITS IN LR ZONES

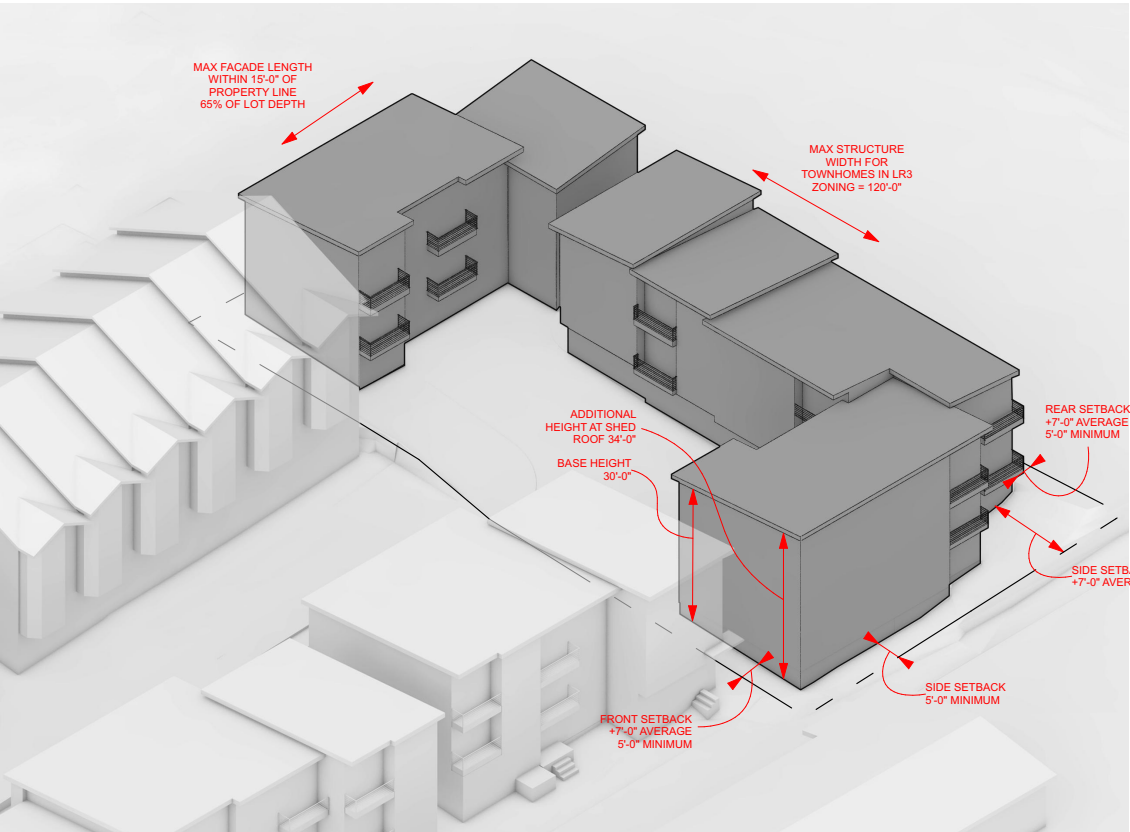
- Maximum Structure Width: 120 for townhouse developments in LR3 lots outside Urban Villages and Centers.
- Maximum Facade Length: 65% of lot line for portions of facade within 15 feet of lot line.

23.45.534 - LIGHT AND GLARE STANDARDS

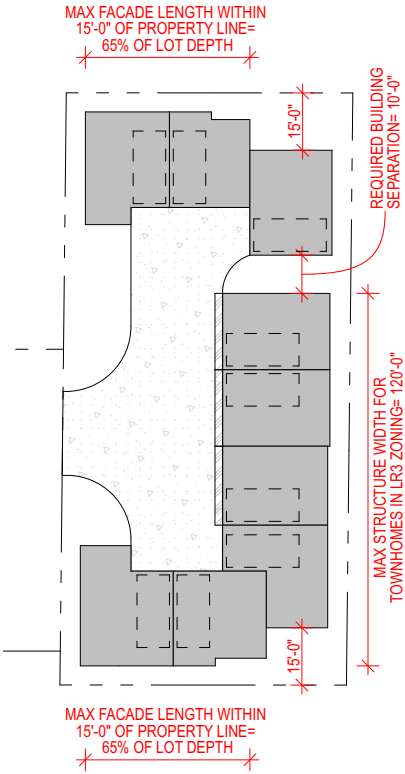
- Exterior lighting shall be shielded and directed away from adjacent properties.
- Interior lighting in parking garages shall be shielded to minimize nighttime glare on adjacent properties.
- Driveways and parking areas for more than two vehicles shall be screened from abutting properties by a fence or wall between 5 feet and 6 feet in height,

23.45.536 - PARKING LOCATION, ACCESS, AND SCREENING

- Parking cannot be located:
  - Between a principal structure and a street lot line;
  - In the required front setback or side street side setback; and
  - Within 7 feet of any street lot line.
- Parking in a structure. Parking may be located in a structure or under a structure, provided that no portion of a garage that is higher than 4 feet above existing or finished grade, whichever is lower.
- Parking shall be screened from direct street view.

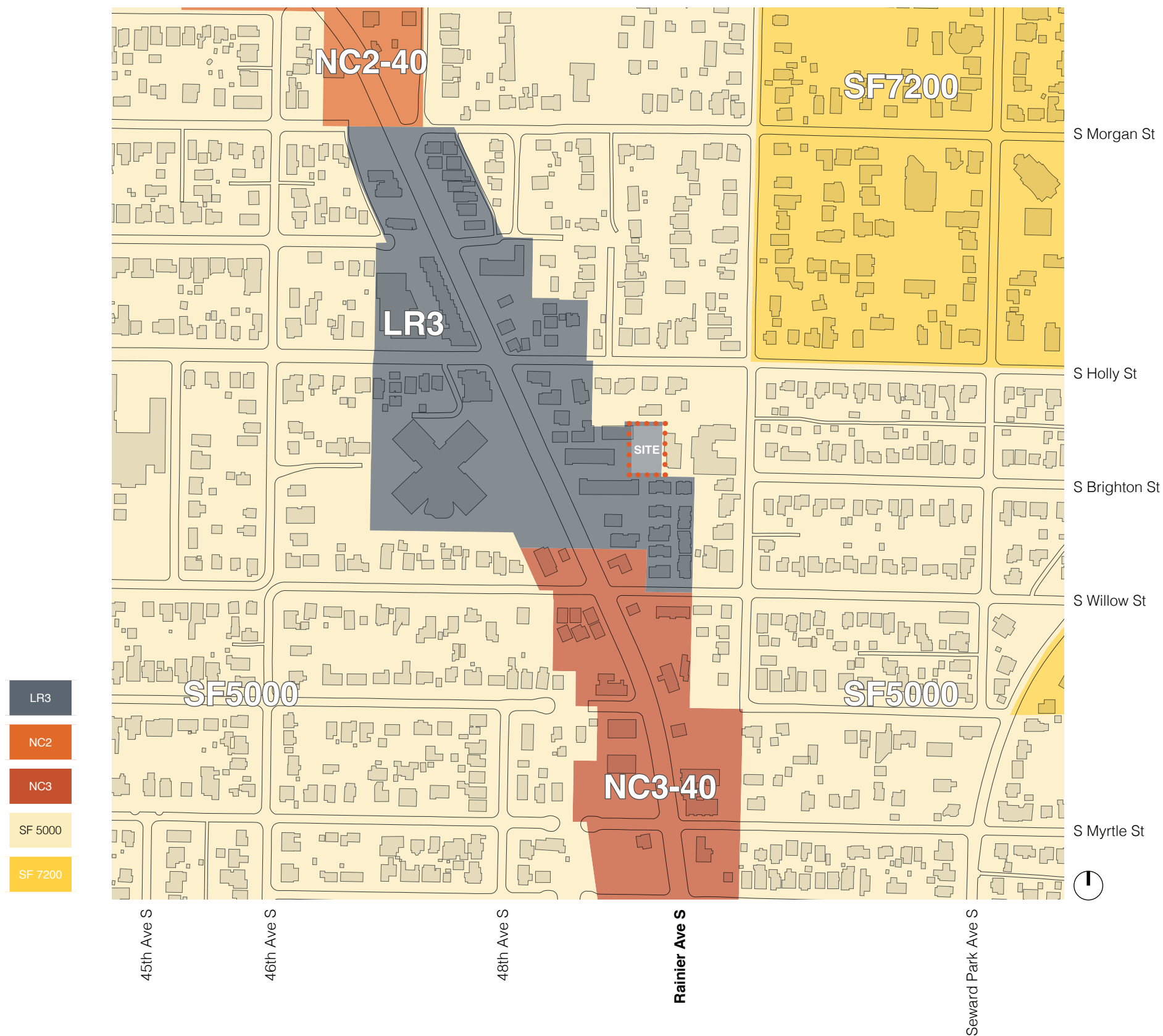


Potential Zoning Envelope



Zoning Plan Diagram





**ADDRESS**

6724 Rainier Avenue S

**PARCEL #**

272404-9180

**LEGAL DESCRIPTION**

The South 155 feet of the West 75 feet of the East 320 feet of the North Half of the Northeast quarter of the Southeast Quarter of the Northeast Quarter, Section 27 township 24 North, Range 4 East, Willamette Meridian, in King County, Washington.

**LOT SIZE**

11,626 SF

**ZONE**

LR3

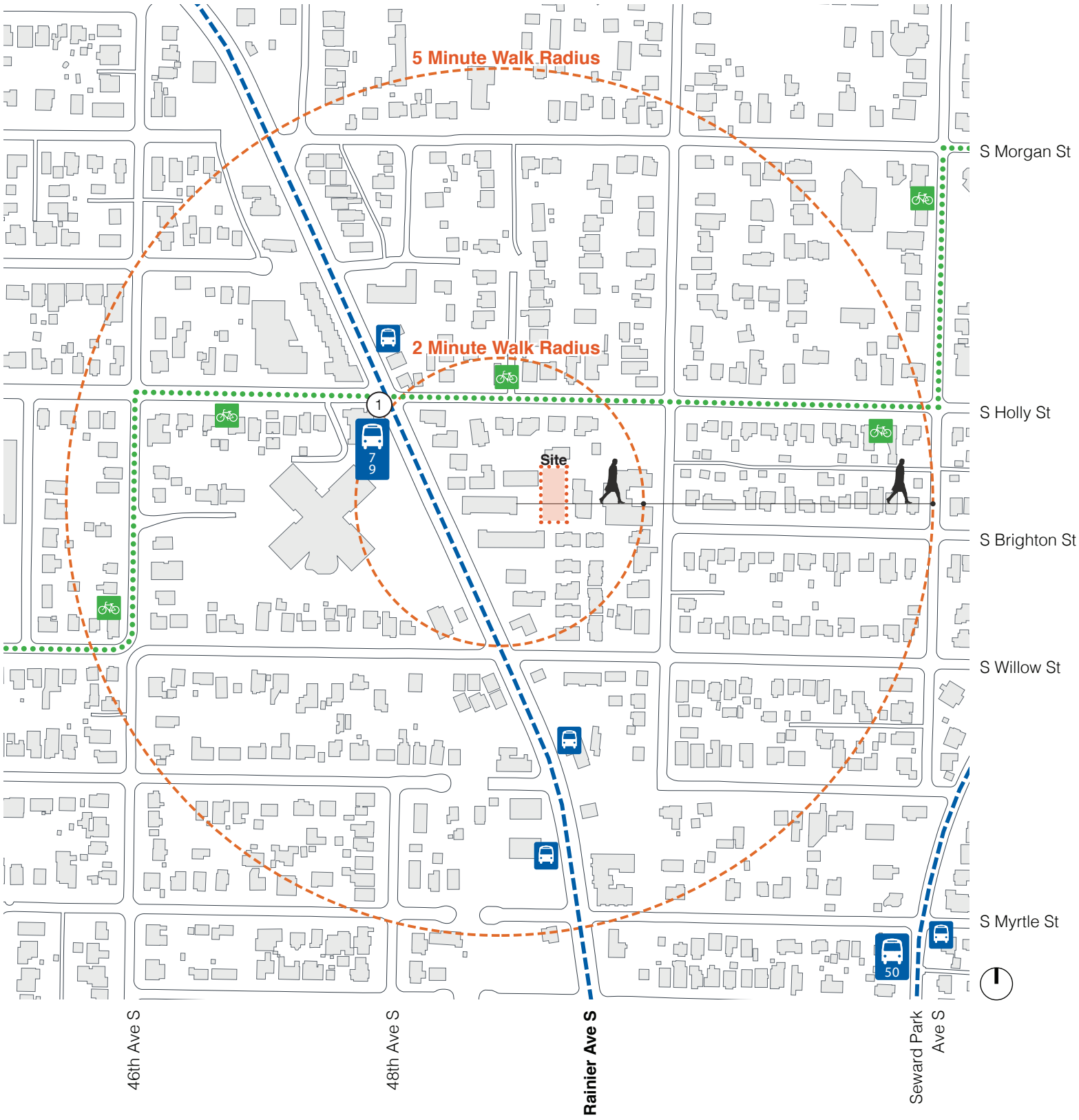


VICINITY MAP





# TRANSIT & ACCESS







This site is well served by several bus lines, including the #7, 9, and 50, facilitating travel to many Seattle neighborhoods, including Downtown, Capitol Hill, Columbia City, Rainier Beach, Beacon Hill, and West Seattle. The Othello link light rail station is within walking distance, roughly 0.7 miles to the West.

Bicycle routes connect the site to neighborhoods to the east and west and link up with bike routes that serve Downtown,

**2 MINUTES**  
Cafe Avole  
Lucky One Food Store

**5 MINUTES**  
Zest Fast Food  
Shell Gas Station  
Paradise Restaurant

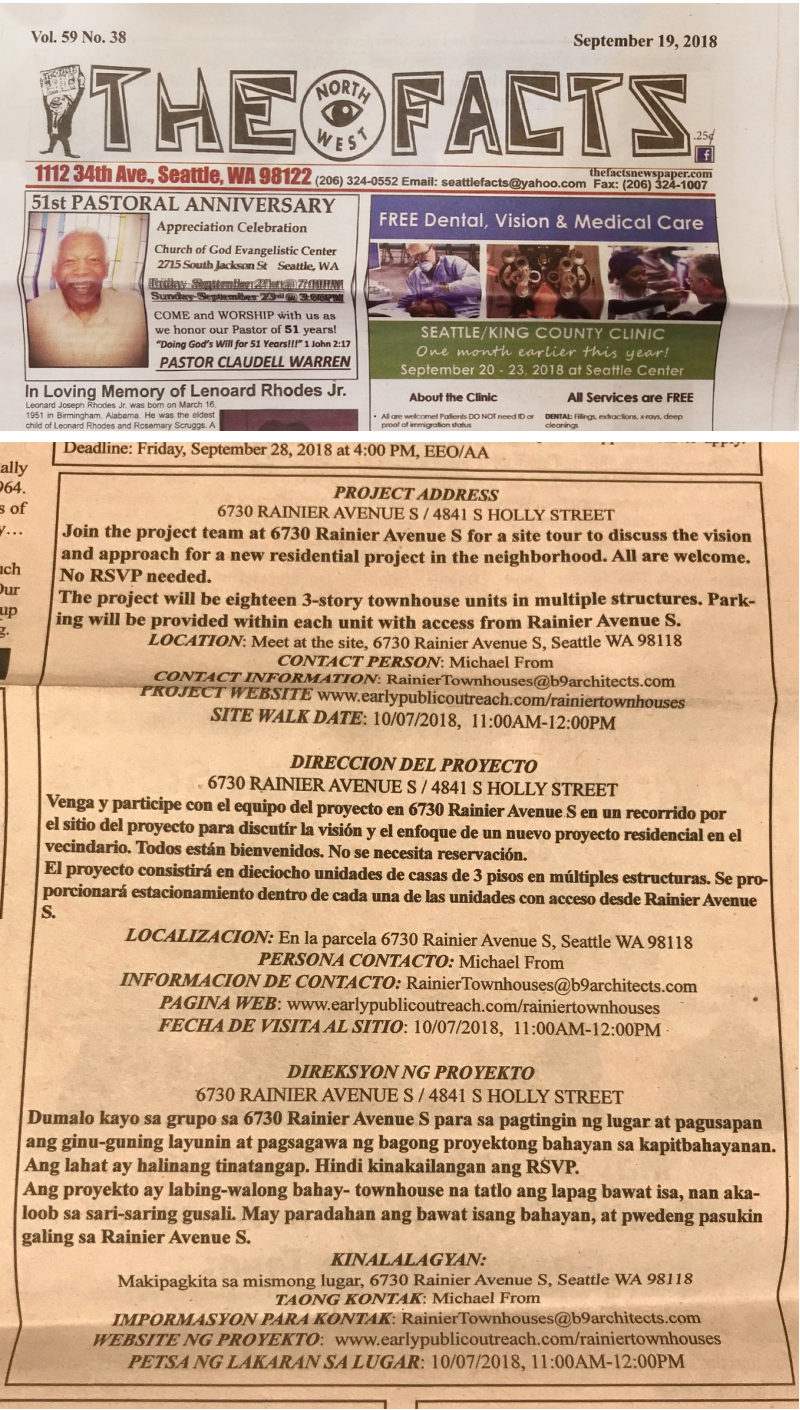
 Bike Route  


 Bus Stop & Route  




1 Bus Stop for routes 7 & 9





1 Printed ad in NW Facts



2 Printed 36"x72" sign on site.  
Per community outreach requirements and approved by D.O.N.

## 6730 RAINIER AVENUE S / 4841 S HOLLY STREET

### PROJECT SUMMARY / RESUMEN DEL PROYECTO / BUOD NG PROYEKTO :

Land Use Application to allow eighteen 3-story townhouse units in multiple structures. Parking will be provided within each unit with access from Rainier Avenue S. Existing structures will be demolished.

La Aplicación de Uso permitirá dieciocho unidades de casas de 3 pisos en múltiples estructuras. Se proporcionará estacionamiento dentro de cada unidad con acceso desde Rainier Avenue S. Las estructuras existentes serán demolidas.

Applikasyon sa Paggamit ng Lupa upang payagan ang labing-walong 3-palapag na townhouse units na nakalooob sa sari-saring gusali. Bawat isang unit ay ipag-kakalooban ng paradhanan na pwedeng pasukin galing sa Rainier Avenue S. Ang mga dating gusali ay gigibain.

### SITE TOUR / VISITA AL SITIO / PAGLIBOT SA LUGAR:

We will be hosting a site tour at 6730 Rainier Avenue S  
Sunday, October 7, 2018, 11am - 12pm  
All are welcome

Realizaremos un recorrido por la parcel en 6730 Rainier Avenue S  
Domingo, 7 de octubre de 2018, 11am - 12pm  
Todos están bienvenidos

Magpapasagawa kami ng paglibot ng lugar sa 6730 Rainier Avenue S  
Linggo, Oktubre 7, 2018 11am - 12pm  
Ang lahat ay halinang tinatangap.

### CONTACT / CONTACTO / KONTAK:

Michael From  
rainiertownhouses@b9architects.com

COMMENT FORM / COMENTARIOS / FORM NG PUNA

LINK TO SDCI / ENLACE A SDCI / LINK SA SDCI



3 Interactive project webpage - [www.earlypublicoutreach.com/rainiertownhouses](http://www.earlypublicoutreach.com/rainiertownhouses)  
Approved by D.O.N.



4 Hosted 1-hour community on-site walk with a representative from the Muslim Housing Services



# EARLY COMMUNITY OUTREACH

As the applicant for a proposal at 6724/6730 Rainier Avenue S, b9 architects conducted and completed Early Community Outreach requirements. Outreach included a large sign installed on site, an ad in a local print newsletter, an interactive project website and a site tour/walk. Per DON's guidance regarding Equity Zone regulations, each outreach method was provided in English, Spanish and Tagalog.

Per the SDCI Director's Rule 4-2018/DON Director's Rule 1-2018 VI.E.- Documentation: Early Design Guidance

*Applicants shall include a summary of the design-related feedback they heard during their community outreach as part of their final EDG packet. While collaborative approach is encouraged between the applicant and the community, the applicant is not required to incorporate any specific community feedback into the project's design. Comments and discussion presented at the Design Review meetings should focus on compliance with the established design guidelines. Applicants may, at their discretion, respond directly to the community about any feedback that is not related to Design Review.*

	OUTREACH METHOD	DATE IMPLEMENTED	DESIGN-RELATED COMMENTS
1	Printed Outreach Ad in local newsletter (NW Facts)	September 7, 2018	None received
2	Printed Outreach 36"x72" sign on site	September 21, 2018	None received
3	Digital Outreach Interactive project webpage	September 7, 2018	No comments received through digital outreach.
4	In-Person Outreach Hosted 1-hour community on-site walk Invitations to site walk sent to Rainier Valley Historical Society, Muslim Housing Services, SE Seattle Senior Center, and Rainier Valley Community Development Fund	October 7, 2018	One person attended the in-person outreach event. Although no comments were received, the attendee asked whether the units would be rental or for sale.



ARCHITECTURAL CONTEXT



1 Rainier Plaza Apartments  
6800 Rainier Ave S  
Built: 1958, 10 units



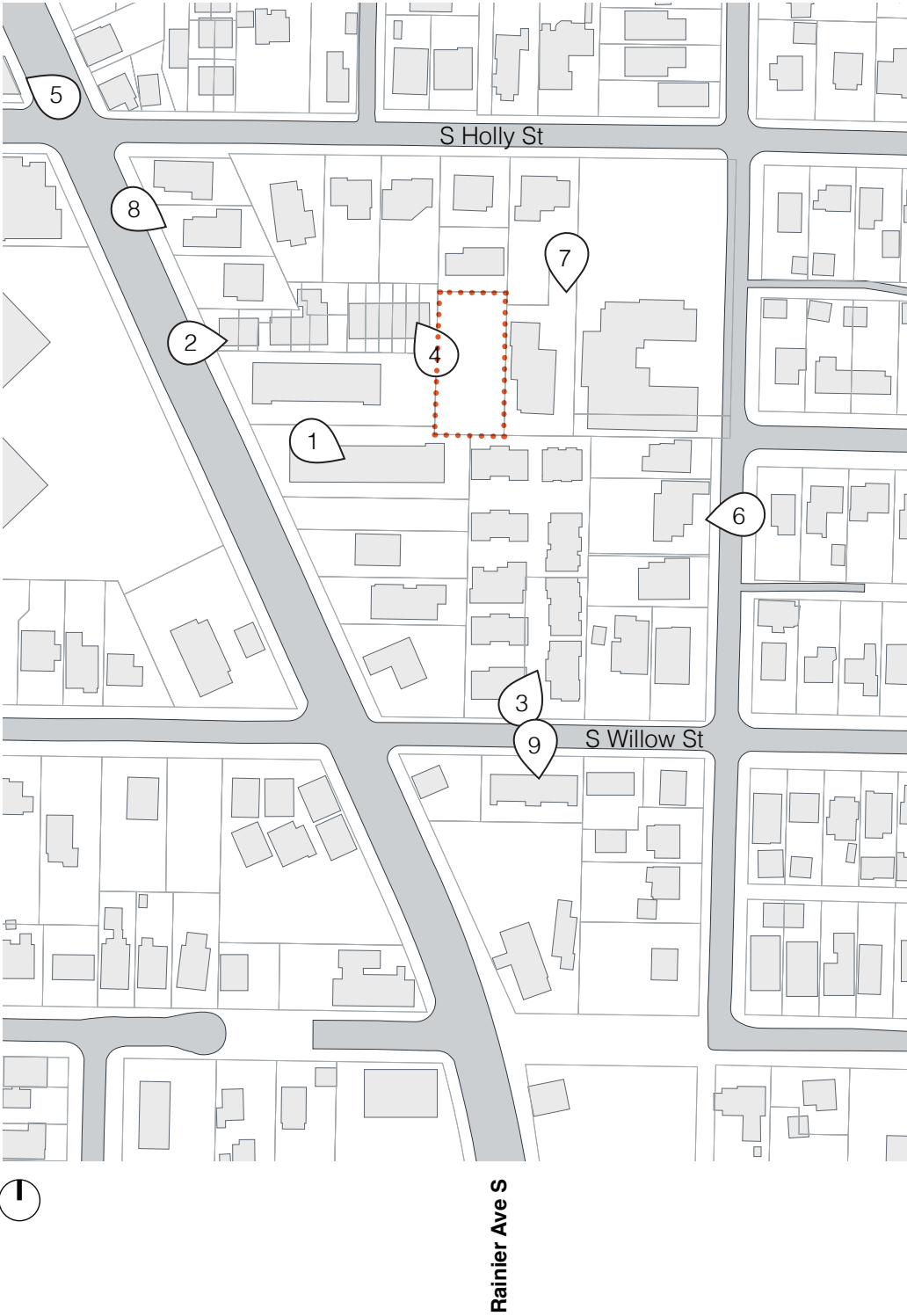
2 Townhouses  
6718-6720 Rainier Ave S  
Built: 2008, 6 townhouse units



3 Townhouses  
4906 - 4922 S Willow St  
Under Construction, 21 townhouse units



4 Townhouses  
6722 Rainier Ave S  
Built: 2004, 6 units





The architectural context in the neighborhood around this site does not represent a unified style and could be characterized as in transition.

The immediate neighborhood is moderately dense with single-family homes and low-rise, multi-family structures. Several lots in the area are currently being redeveloped from single to multi-family.

Single-family homes in the area are predominately one to two stories and are gabled and wood clad. There is diversity exhibited in the local multi-family structures, both in terms of type and architectural style. There are several townhouse structures (see 2 and 4) as well as low-rise apartments (see 5 and 9) nearby.

While there is not a consistent style or structure type in the vicinity, most nearby structures do exhibit standard construction typologies such as gabled roofs, wood and panel siding and ground floor parking.



5 Arches Apartments  
6601 Rainier Ave S  
Built: 1967, 24 units



6 Single Family House  
6807 51st Ave S  
Built: 1949



7 Single Family House  
4837 S Holly St  
Built: 2016



8 Fourplex  
6700 Rainier Ave S  
Built: 1951



9 Willows Apartments  
4911 S Willow St  
Built: 1959, 9 units





## ADJACENT USES



1 Muslim Housing Authority



2 Southeast Seattle Senior Center





③ Lucky One Gas Station and Convenience Store



④ Peter Pan Auto Glass Shop



⑤ Lsk Auto Repair and Sales



⑥ Ashreichem Yisrael Orthodox Synagogue



⑦ Cafe Avole

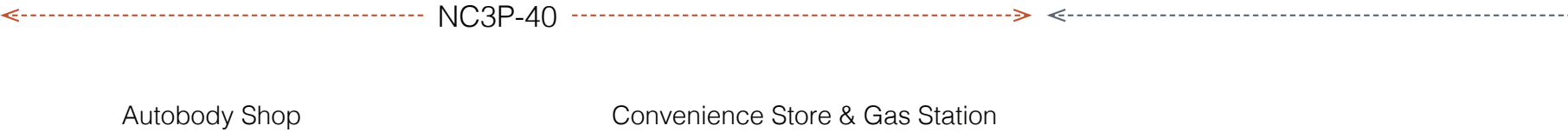




STREET PANORAMAS



1 East side of Rainier Ave S



2 West side of Rainier Ave S



LR3

NC3P-40

Single Family

Townhouse

10-unit Apartment Building

Duplex

11-unit Apartment Building

Autobody Shop

Restaurant



LR3

124-unit Apartment Building

Community Center

24-unit Apartment Building



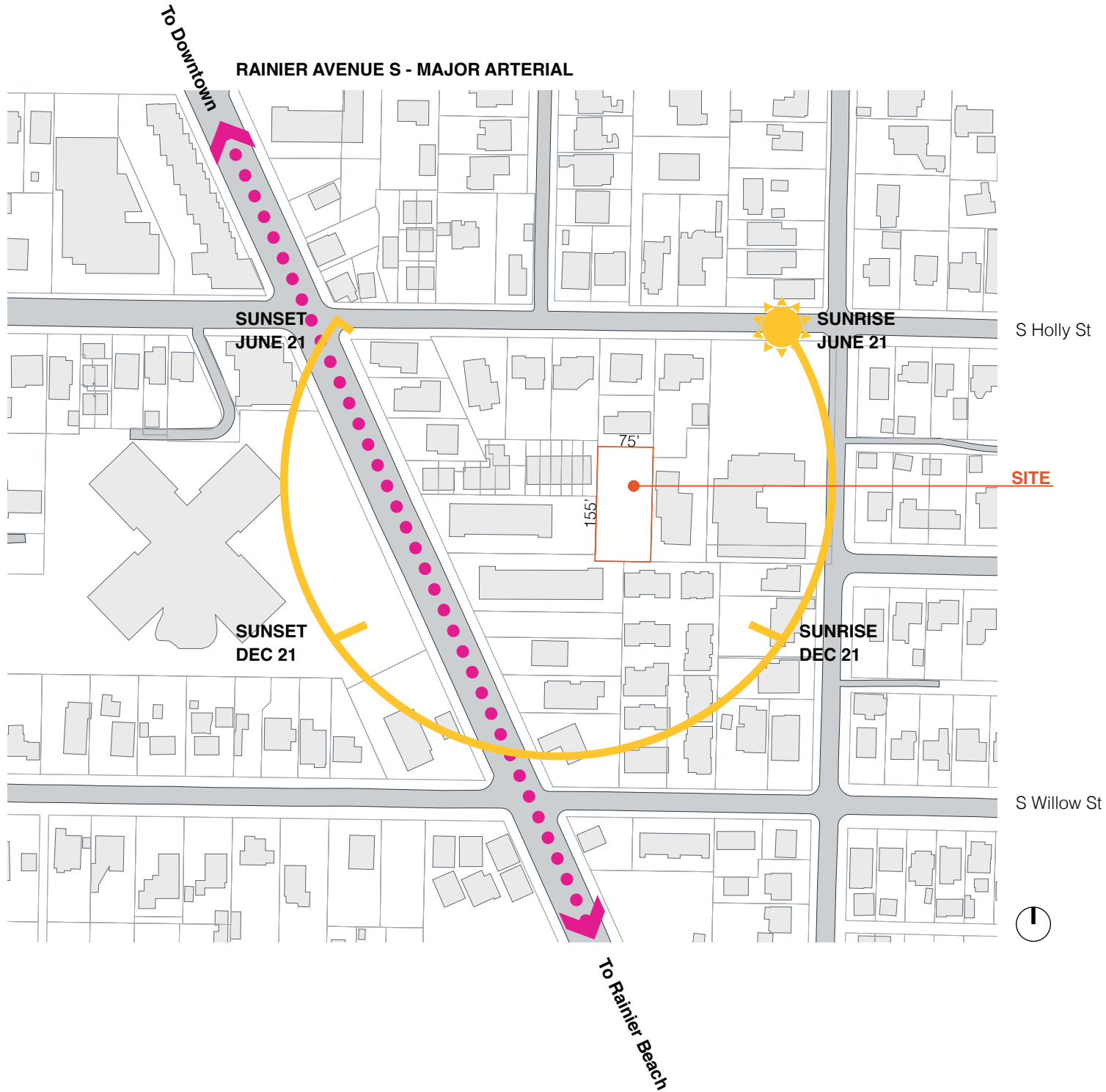


6724 Rainier Avenue S | #3032797 | ADR Packet | December 7, 2018





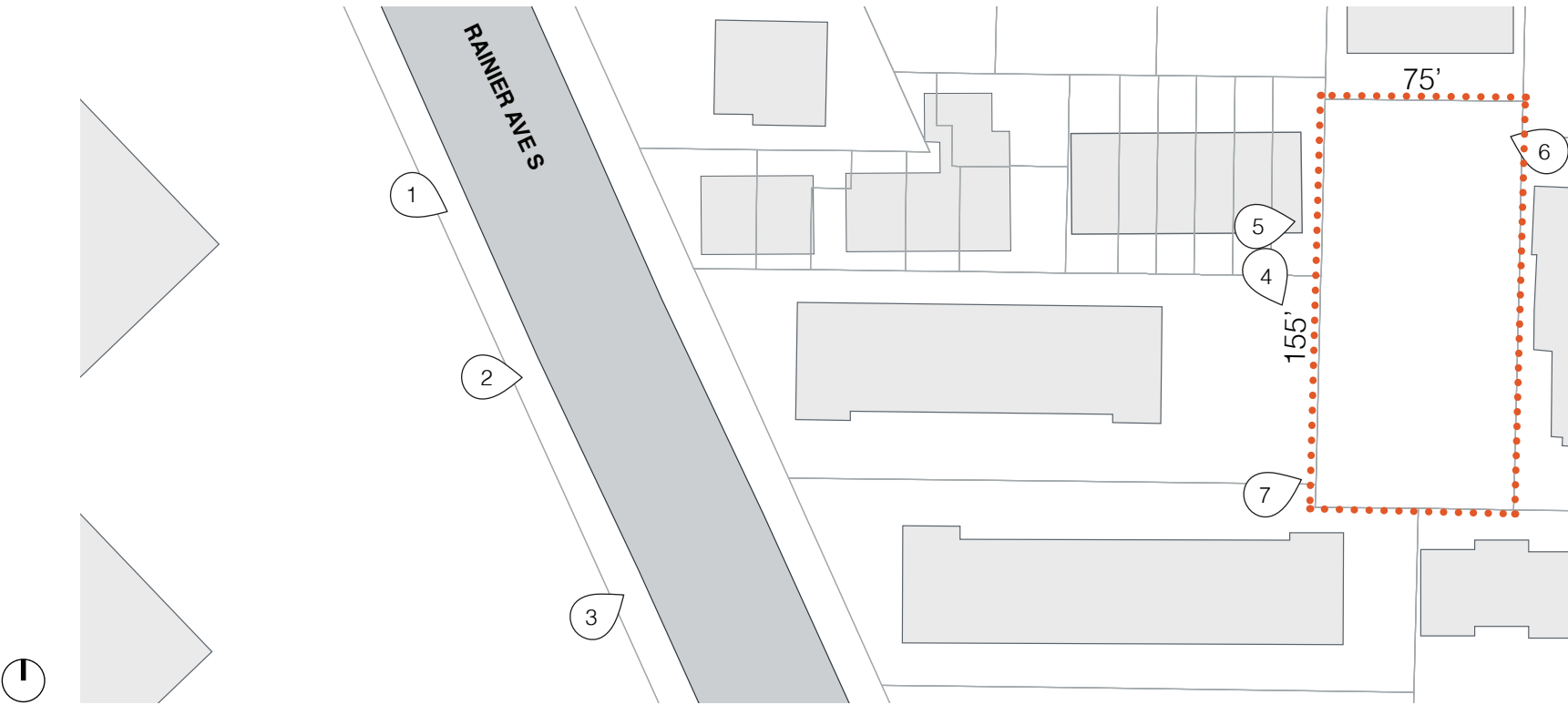
# SITE CONDITIONS & CONSTRAINTS



## CONSTRAINTS

- Rainier Avenue S is an Urban Center connector, principal arterial street that provides a direct connection to downtown Seattle and many public transit opportunities.
- The site is located mid-block between S Holly Street, S Willow Street, and Rainier Avenue S. The site, which has no street frontage, is accessed through an access easement for pedestrians and vehicles on the adjacent site to the west, 6730 Rainier Avenue S (#3032795), which is being developed concurrently to this project.
- The site is located on the East side of Rainier Avenue S
- The site is currently vacant. The landscape is comprised of non-exceptional trees and shrubs.
- This infill site dimensions are 155 feet north-south by 75 feet east-west.
- Due to the site's position without street frontage, the development proposal will be connected to and reliant upon the adjacent site to the west, 6730 Rainier Avenue S (#3032795).
- The surrounding context is comprised of low-rise multi-family and single-family structures. Directly across Rainier Avenue S is the Muslim Housing Service's Brighton Apartment complex.
- The site has good southern and western exposure due to the low height of adjacent structures and the orientation of the lot's dimensions.





1 View of site looking Southeast on Rainier Ave S



2 View of site looking East on Rainier Ave S



3 View of site looking Northeast on Rainier Ave S



# EXISTING CONDITIONS

The site, approximately 155 feet wide in the north-south direction by 75 feet deep in the east-west direction, is currently a vacant lot. The landscape is comprised of non-exceptional trees and shrubs.

Multi-family structures bound the site to the North, East, and South. To the west, 6730 Rainier Avenue S includes a low-rise apartment structure. The site's topography descends approximately 30 feet from the northeast of the property to the southwest. The site currently has no street frontage and therefore cannot be accessed from the right-of-way without connecting through an adjacent site.

The site is located on the east side of Rainier Ave S, mid-block between S Holly Street and S Willow Street. The site does not have significant access to views. The site has good access to Othello Playground and Martha Washington Parks each slightly over 0.5 miles from the site.



7 View of site looking Northeast from adjacent property to the south



4 View of site looking Southeast from adjacent property to the north



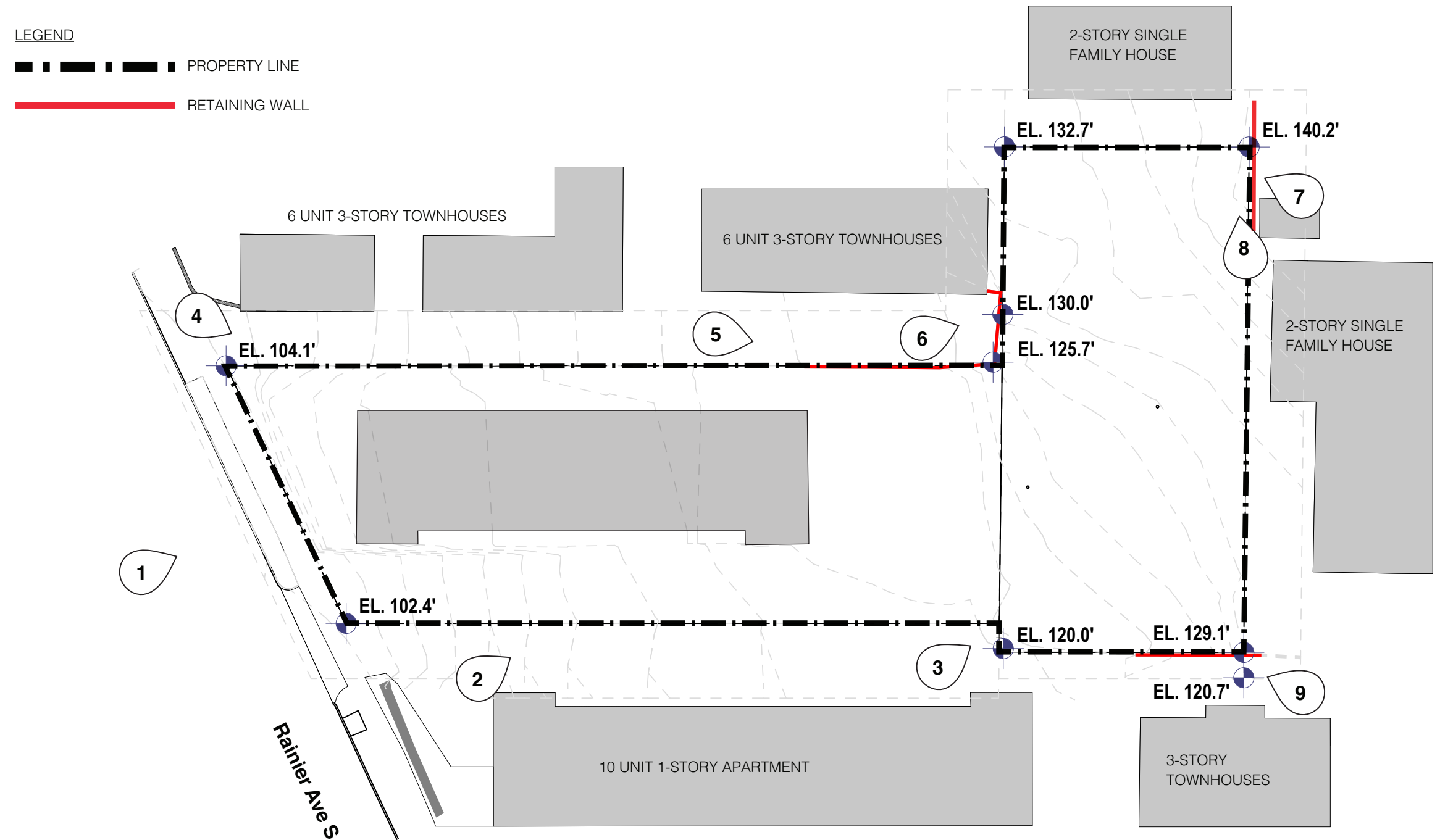
5 View of site looking east from adjacent property to the north



6 View of site looking west from adjacent property to the east



## EDGE CONDITIONS







1 View of site looking Northeast on Rainier Ave S



4 View of site looking Southeast from adjacent property to the north



7 View of site looking Northwest from adjacent property to the east



2 View of site looking Northeast from adjacent property to the south



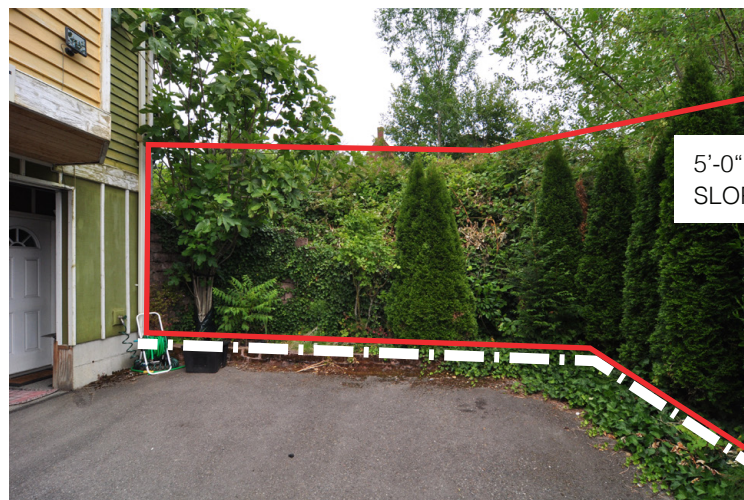
5 View of site looking East from adjacent property to the north



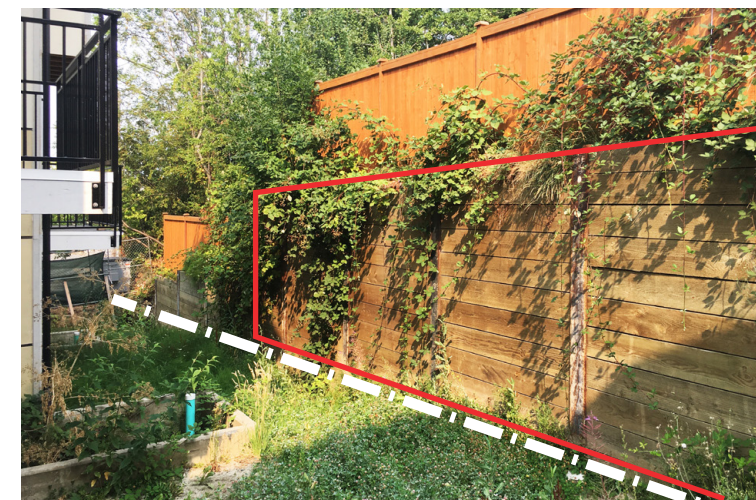
8 View of site looking North



3 View of site looking Northeast from adjacent property to the south



6 View of site looking West from adjacent property to the south



9 View of shoring wall looking Northwest from adjacent property to the south



DESIGN PROPOSAL ALTERNATIVES

ALTERNATIVE 1

Units

- 9 units .
- 9 parking stalls.

Advantages

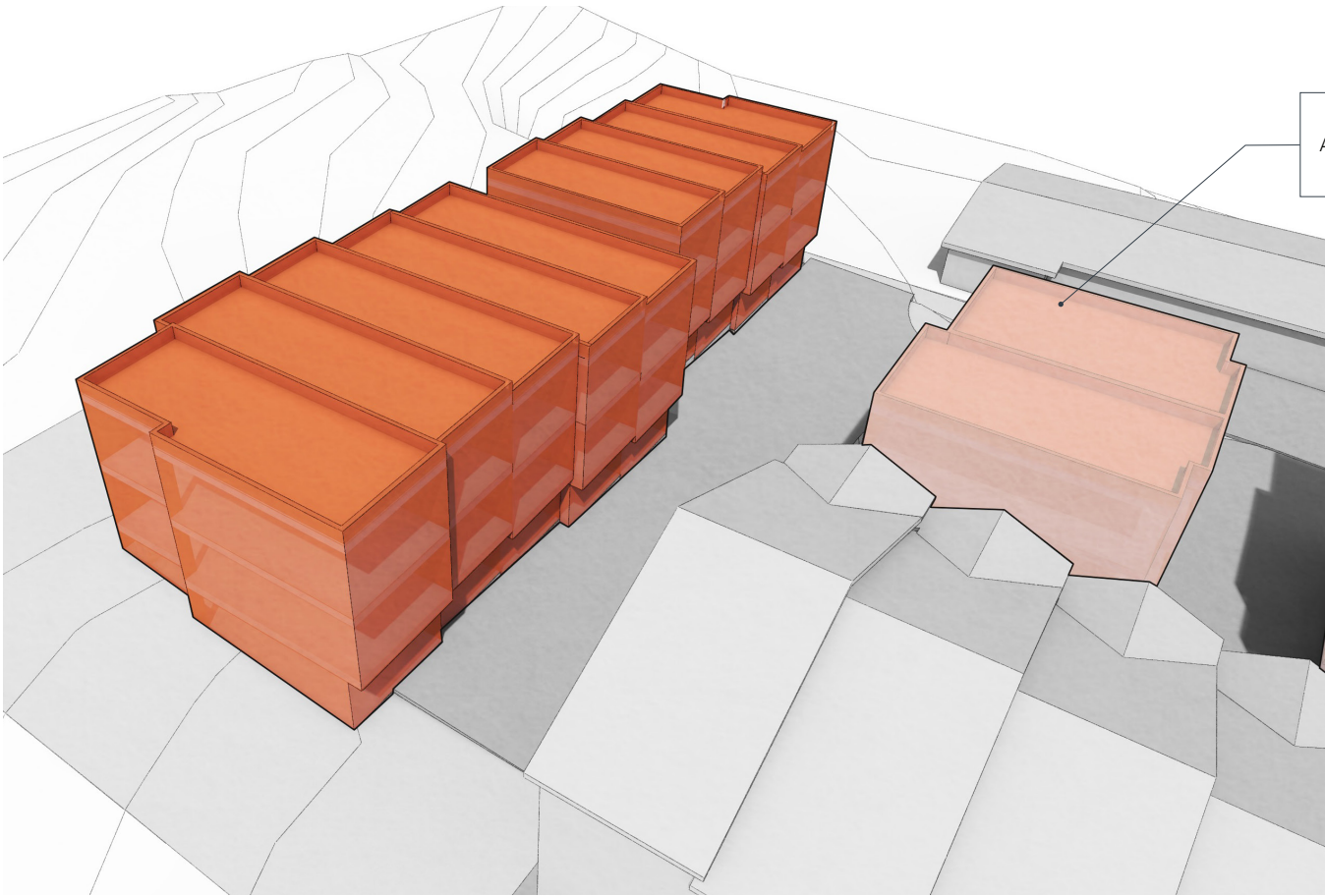
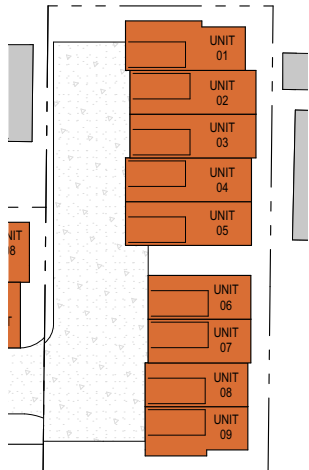
- Code compliant scheme.

Issues

- Units are narrow and long.
- Only one unit has significant southern exposure.
- Opportunities for common amenity areas are limited
- There are a few opportunities for positive pedestrian experiences.
- Flat roofs are not a contextual response, neither in terms of adjacent structures or solar access
- Organization does not support any sense of community or privacy.
- Auto-centric site planning.

Departures

- No departures requested.



Aerial View looking Southeast



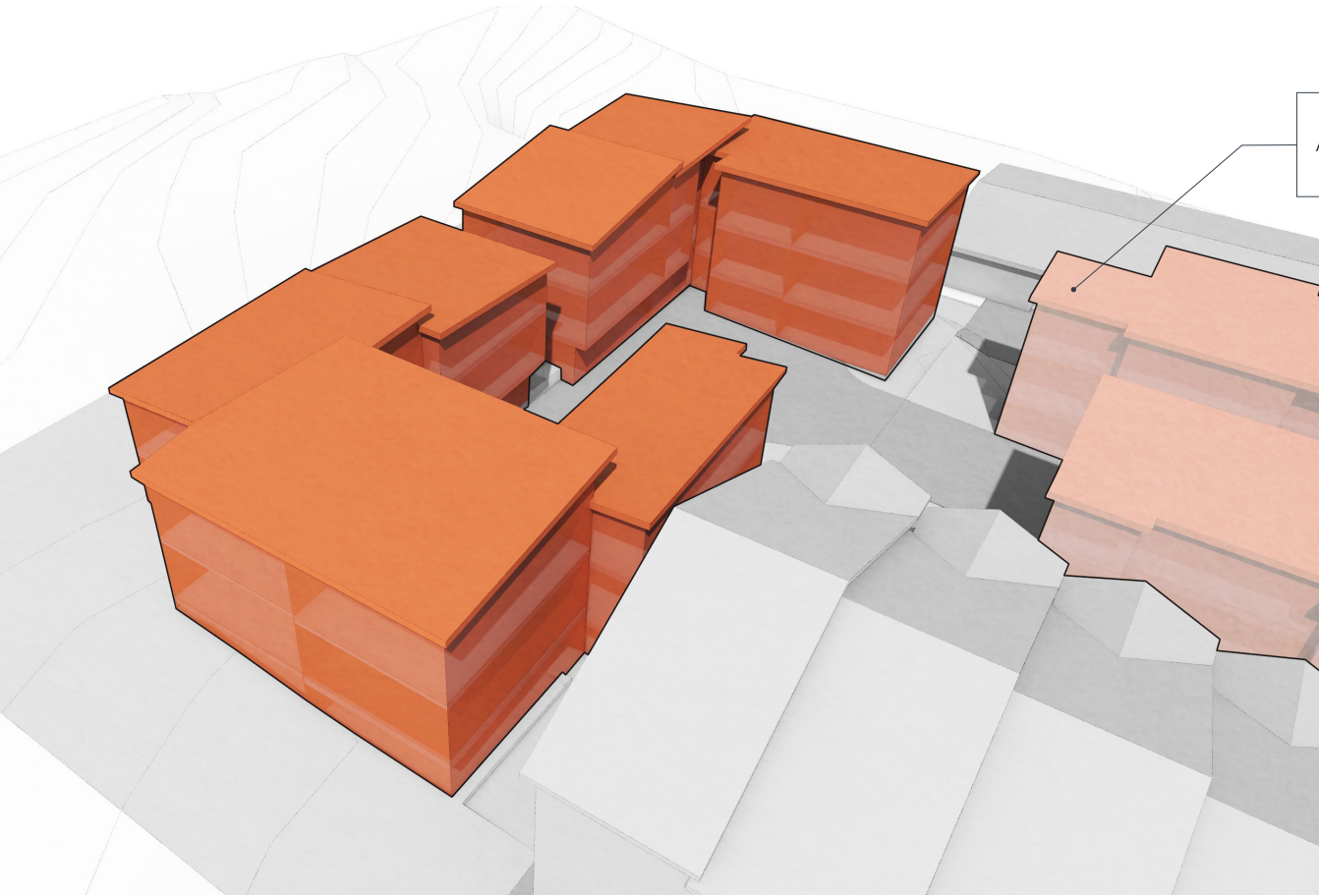
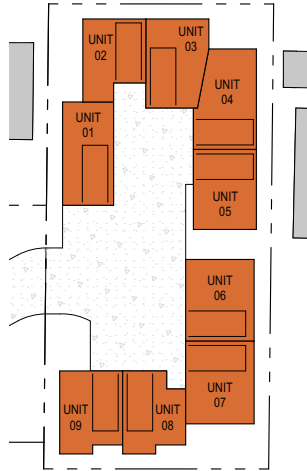
ALTERNATIVE 2

- Units
- 9 units.
  - 9 parking stalls.

- Advantages
- Code compliant scheme.
  - C-shape to the organization of the structures supports a sense of community and of privacy.
  - Six units have southern exposure.
  - Shed roofs are architecturally contextual and responsive to site conditions.

- Issues
- Amenity spaces are generally small and at the rear of each dwelling.
  - Pedestrian access only occurs in the parking aisle.
  - Common space at interior of site is comprised solely of the surface of the parking aisle.

- Departures
- No departures requested.



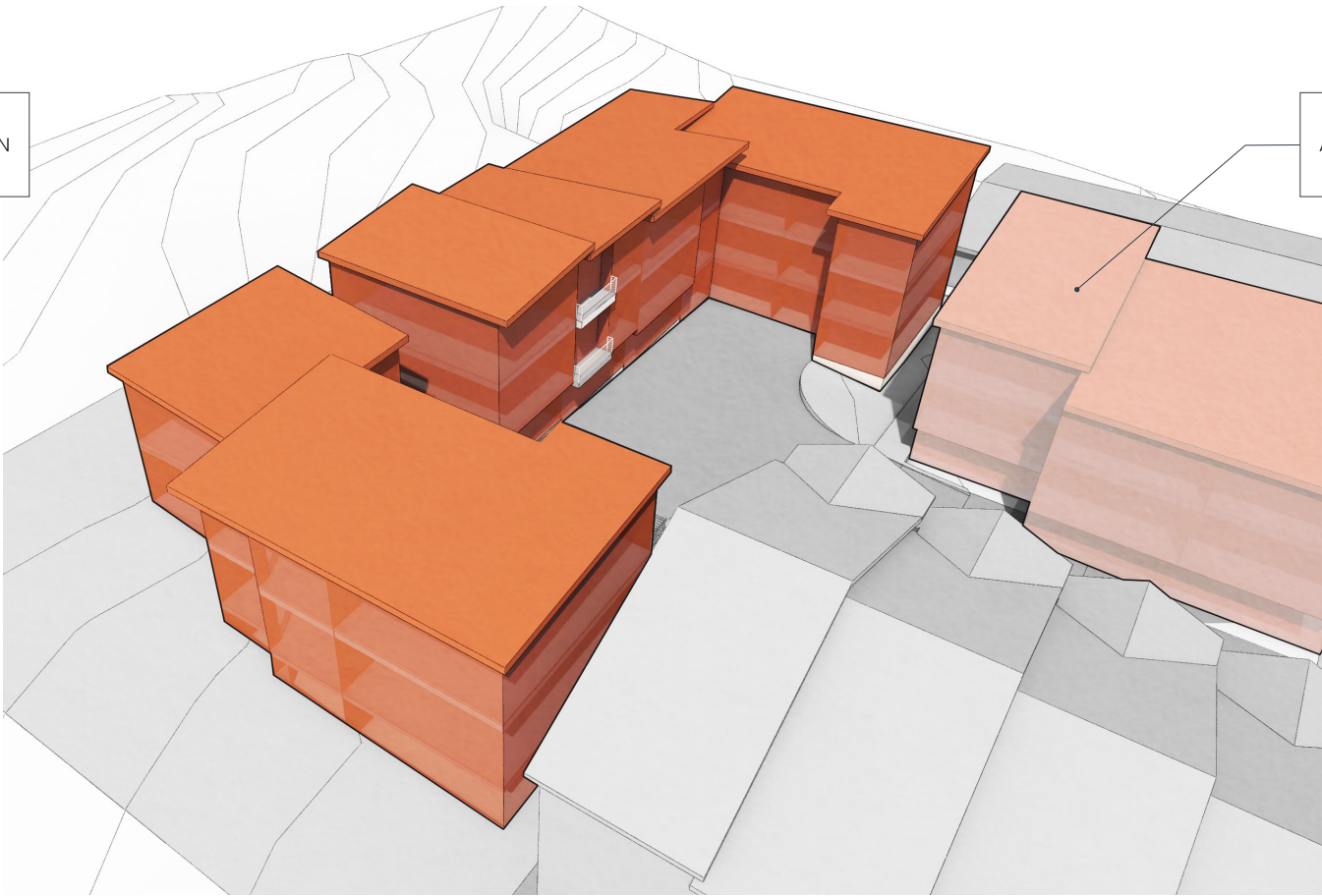
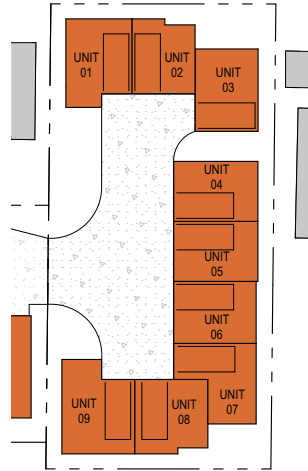
Aerial View looking Southeast

ALTERNATIVE 3 (PREFERRED)

- Units
- 9 units.
  - 9 parking stalls.

- Advantages
- Code compliant scheme.
  - U-shape supports as sense of community while remaining open to the west property line.
  - All units have either southern or western solar access.
  - Shed roofs are architecturally contextual and responsive to site conditions.
  - Interior of site is not dominated by parking aisle.
  - Four unit have south facing balconies.
  - Alternative 3 has the most open space. Reference page 32.

- Departures
- No departures requested.



Aerial View looking Southeast



ALTERNATIVE 1

Alternative 1 is a code compliant scheme with two three-story structures, containing a total of (9) townhouse units. All units are parked in private garages at grade in each unit, accessed by a parking aisle on the west side of the site. The units in Alternative 1 are stacked on the east side of the site and are all oriented east to west.

Pedestrian access will be located within the parking aisle. Common amenity spaces are proposed to be located in the building separation and in the northwest corner of the site. The remaining amenity spaces are private and located behind the units along the east property line. Structures are proposed to have flat roofs in this alternative.

Units

- 9 units.
- 9 parking stalls.

Advantages

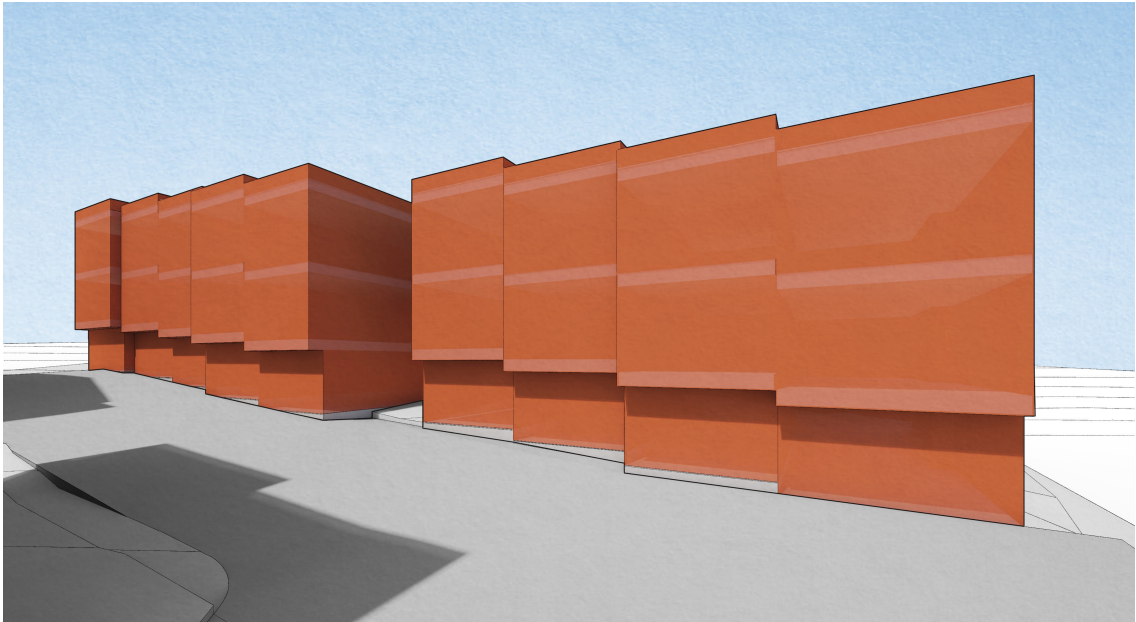
- Code compliant scheme.

Issues

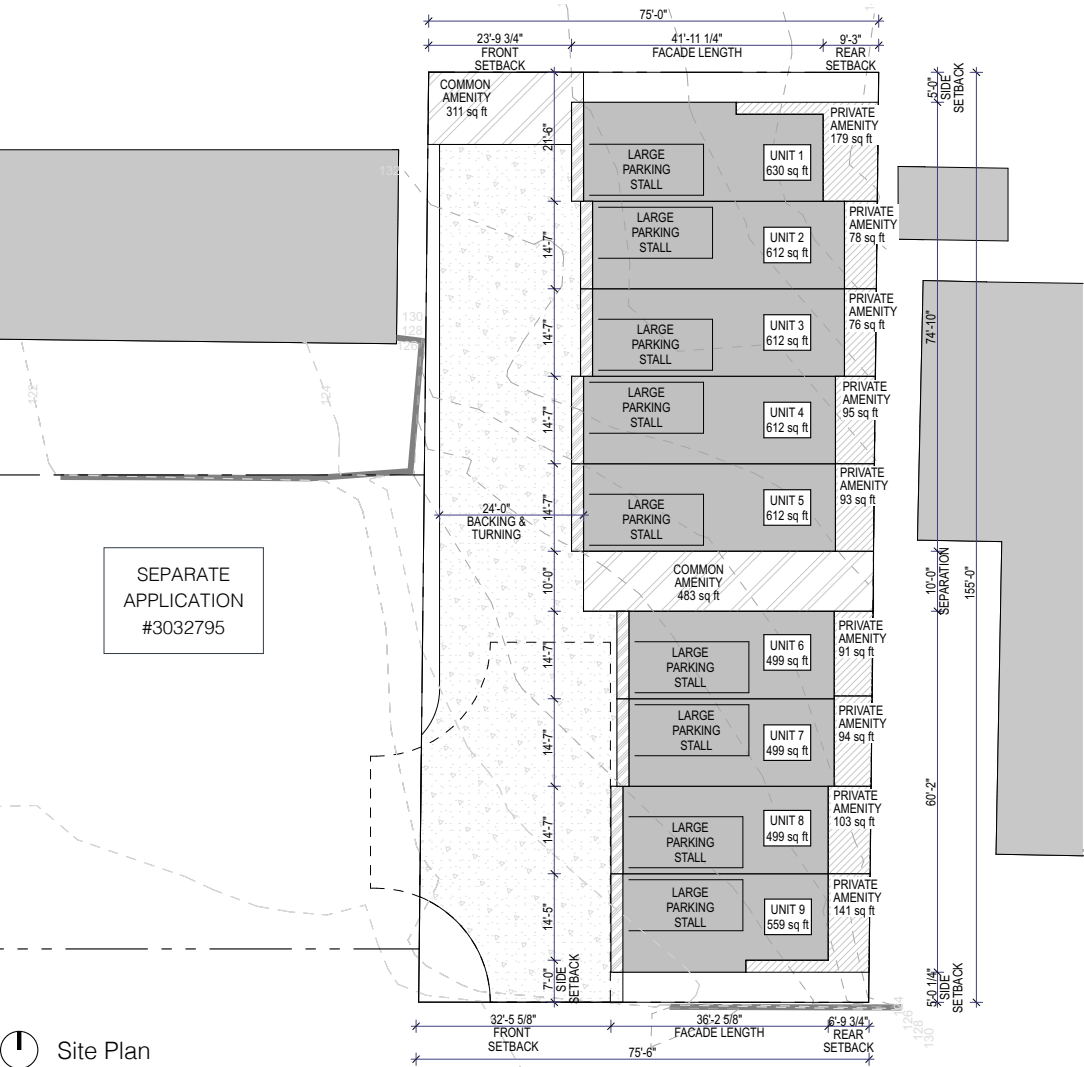
- Units are narrow and long.
- Only one unit has significant southern exposure.
- Opportunities for common amenity areas are limited
- There are a few opportunities for positive pedestrian experiences.
- Flat roofs are not a contextual response, neither in terms of adjacent structures or solar access
- Organization does not support any sense of community or privacy.
- Auto-centric site planning.

Departures

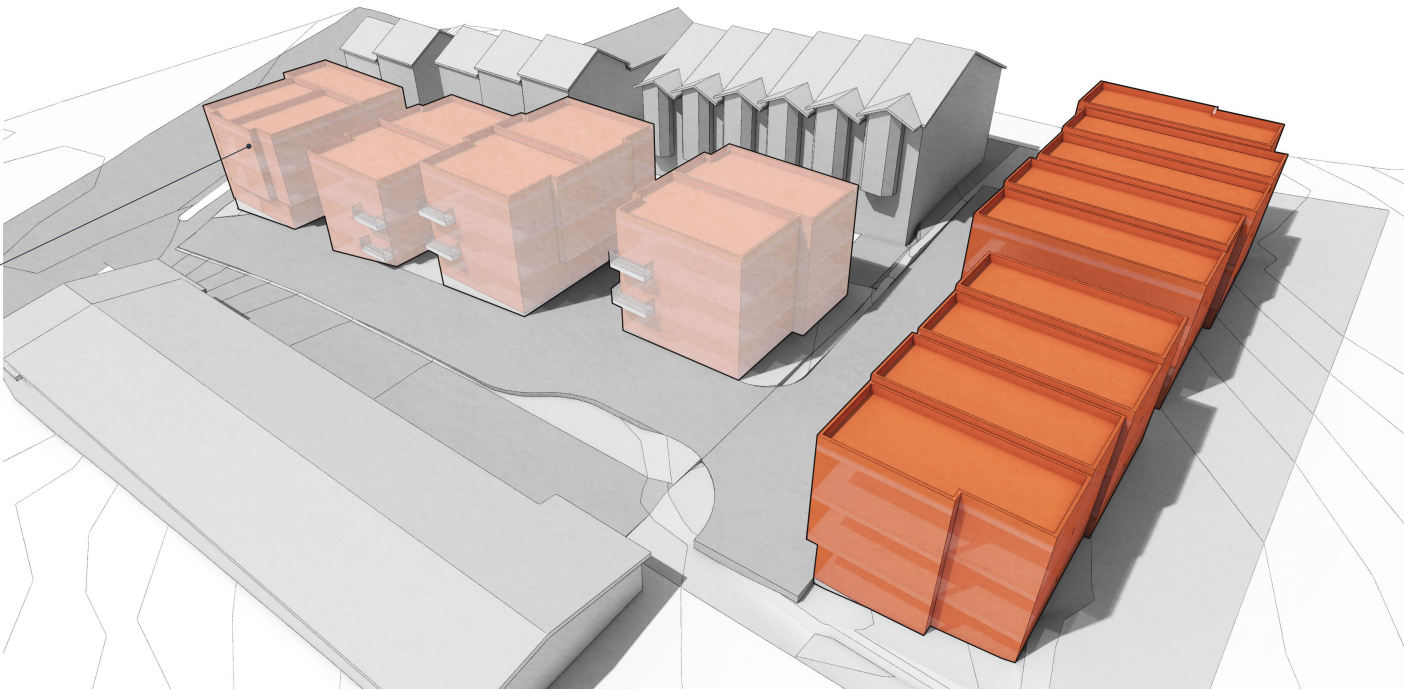
- No departures requested.



1 View looking East

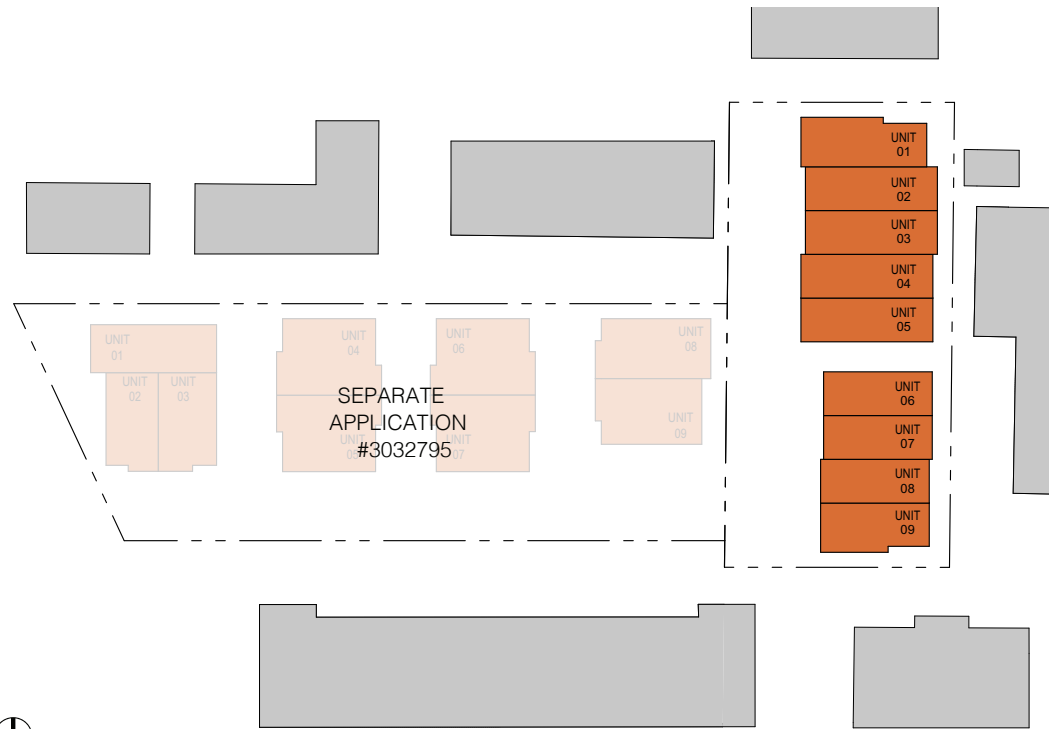


Site Plan

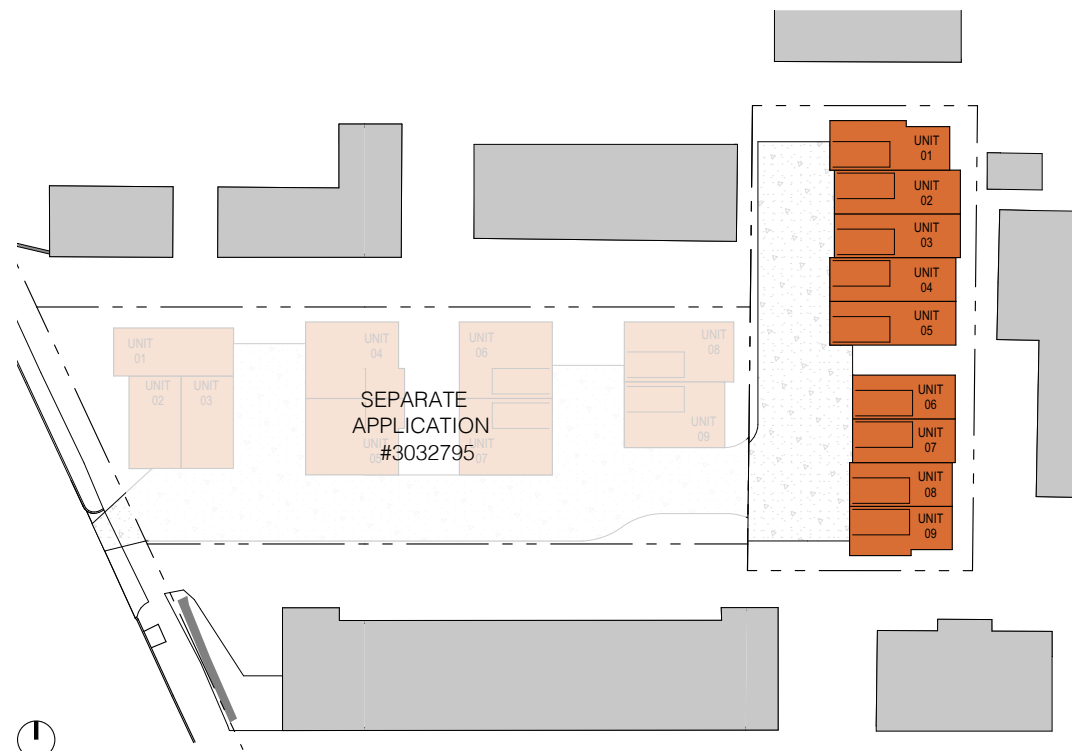


2 Aerial View looking Northwest

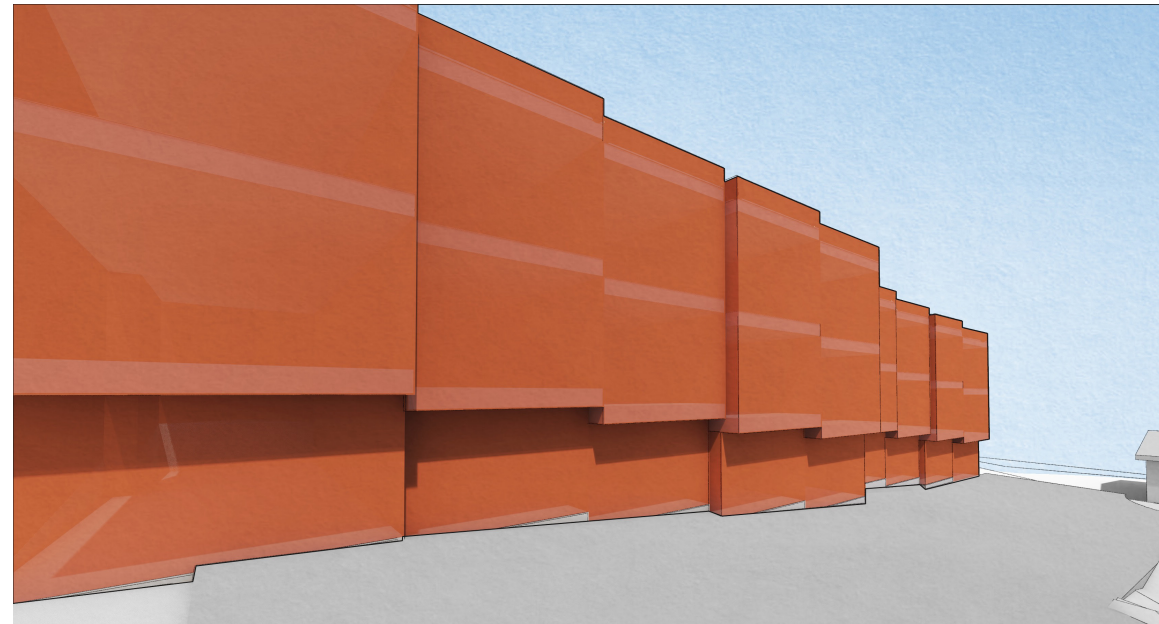




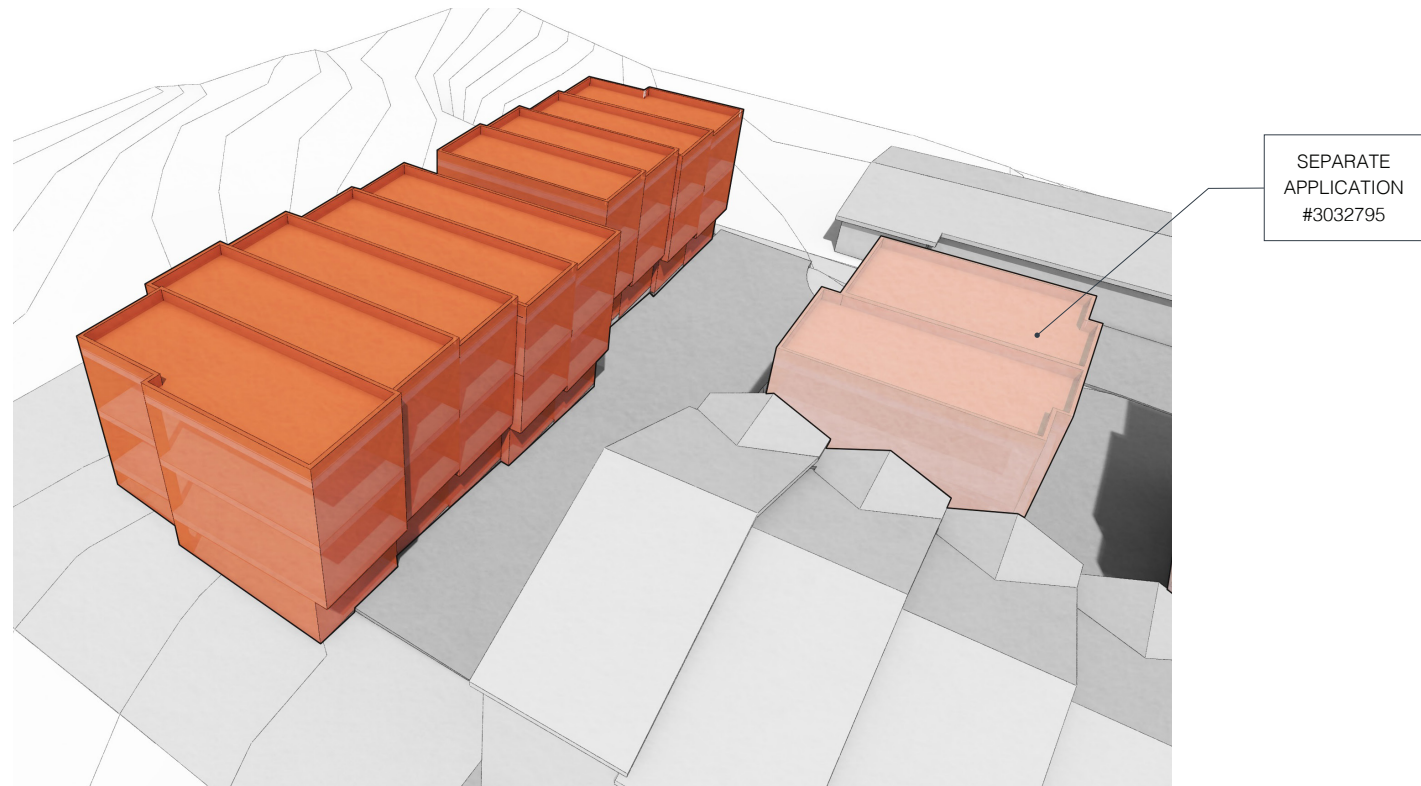
1  
Typical Upper Level Plan



2  
Ground Level Plan



3 View looking Southeast



4 Aerial View looking Southeast

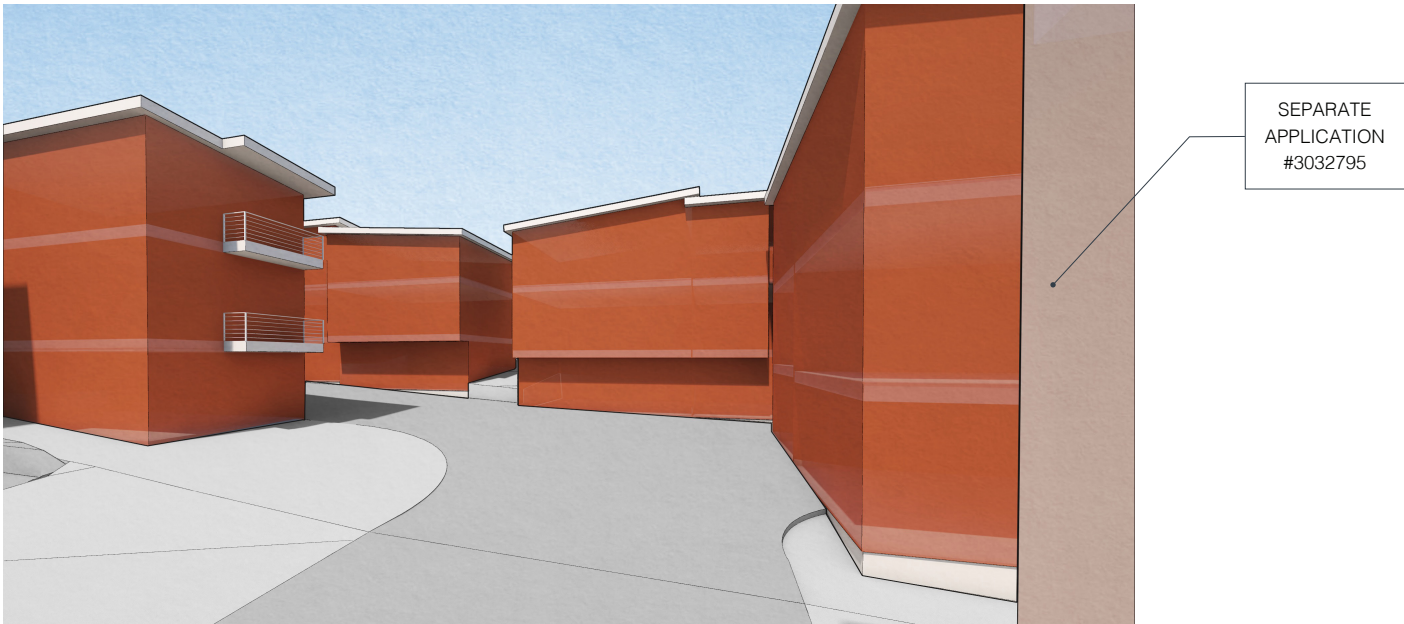
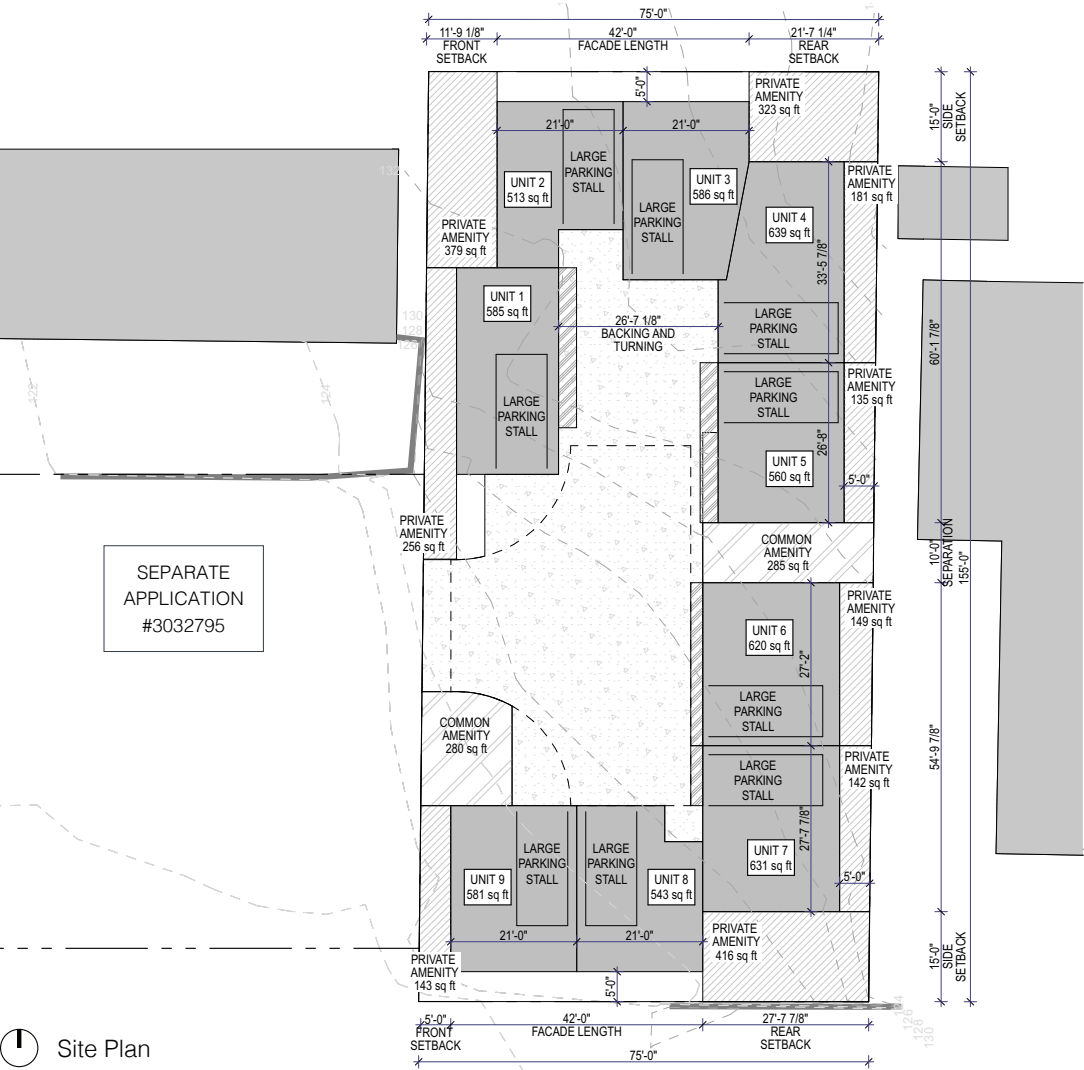


# ALTERNATIVE 2

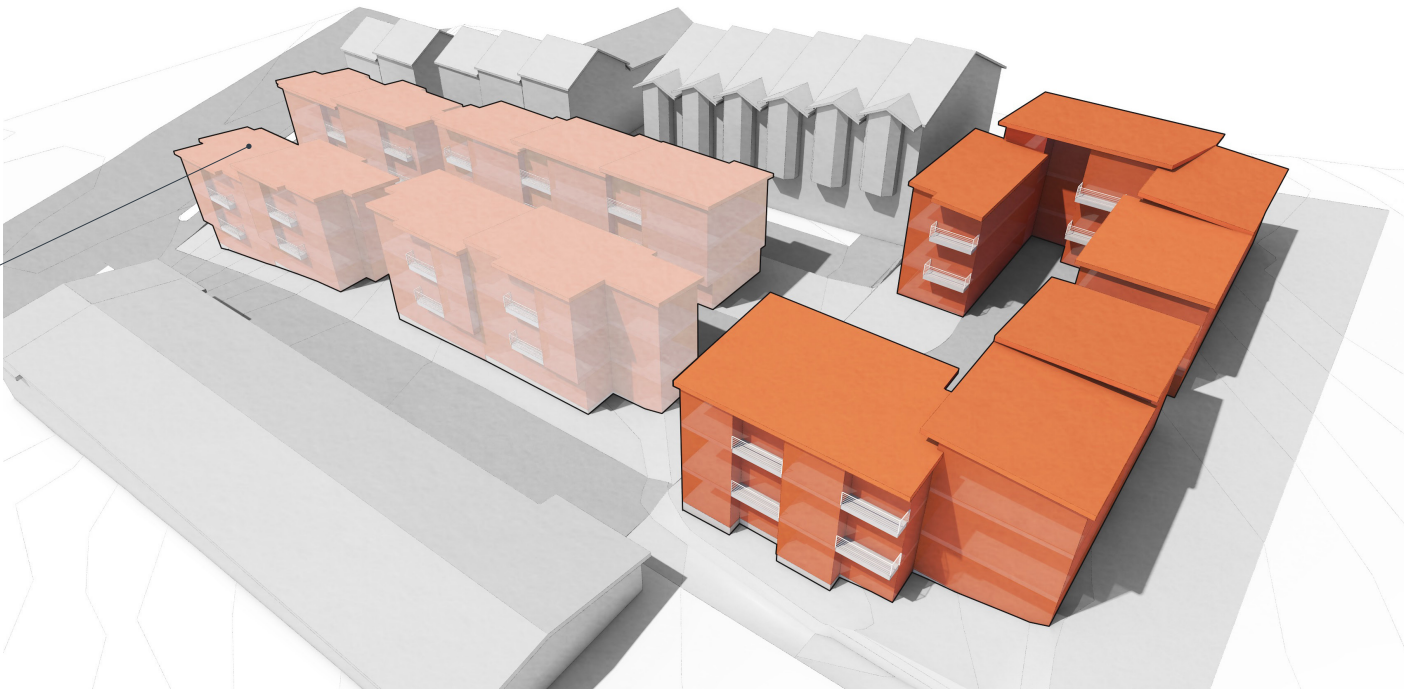
Alternative 2 is a code compliant scheme with two three-story structures, containing a total of (9) townhouse units. All units are parked in private garages at grade in each unit, accessed by a parking aisle in the center of the site. The units in Alternative 2 are organized in a 'C' shaped around the parking aisle. This configuration results in the parking aisle acting as the separating, common space between units.

Amenity spaces are mostly located between the structure and property lines. Pedestrian access for Alternative 2 will be provided in the parking aisle. Alternative 2 has shed roofs that slope up to the south, which provide a contextual response to surrounding structures and allow for increased daylighting opportunities. Four units have with exposure to the south have balconies.

- Units**
  - 9 units.
  - 9 parking stalls.
- Advantages**
  - Code compliant scheme.
  - C-shape to the organization of the structures supports a sense of community and of privacy.
  - Six units have southern exposure.
  - Shed roofs are architecturally contextual and responsive to site conditions.
- Issues**
  - Amenity spaces are generally small and at the rear of each dwelling.
  - Pedestrian access only occurs in the parking aisle.
  - Common space at interior of site is comprised solely of the surface of the parking aisle.
- Departures**
  - No departures requested.

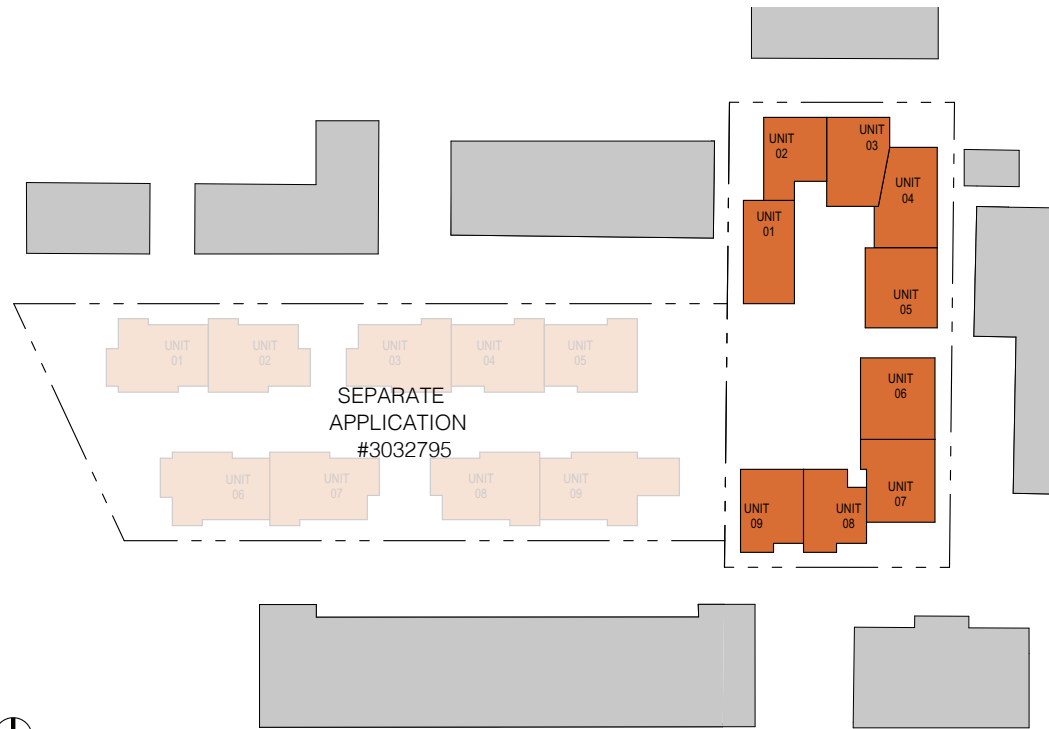


1 View looking East

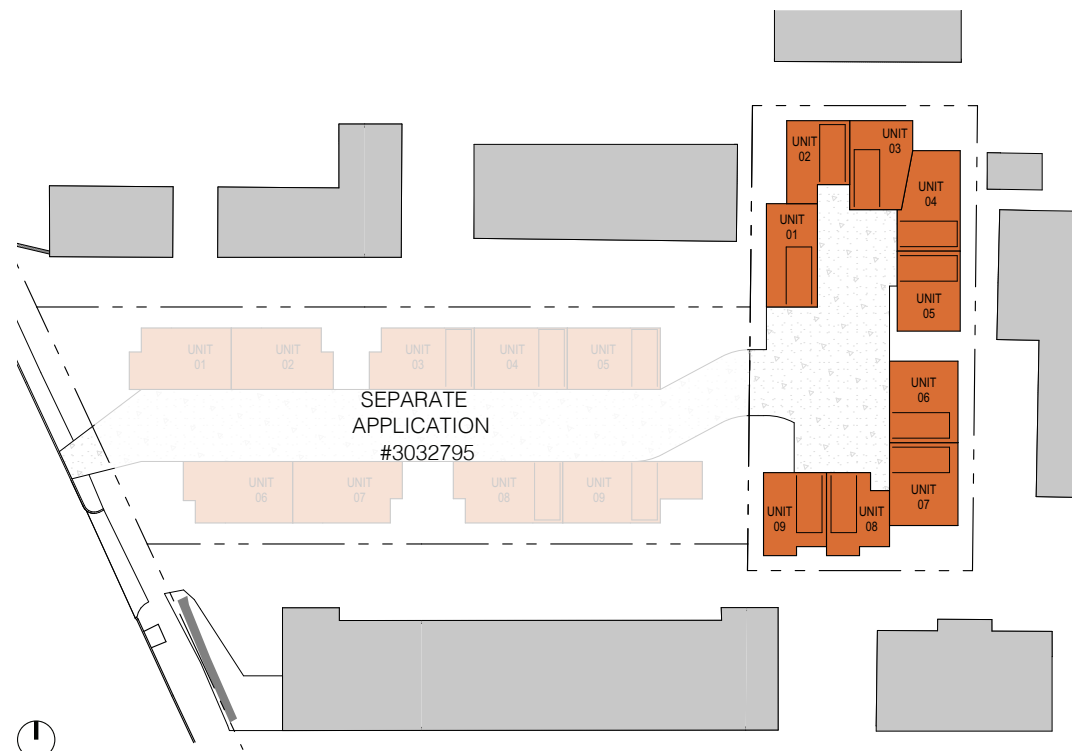


2 Aerial View looking Northwest

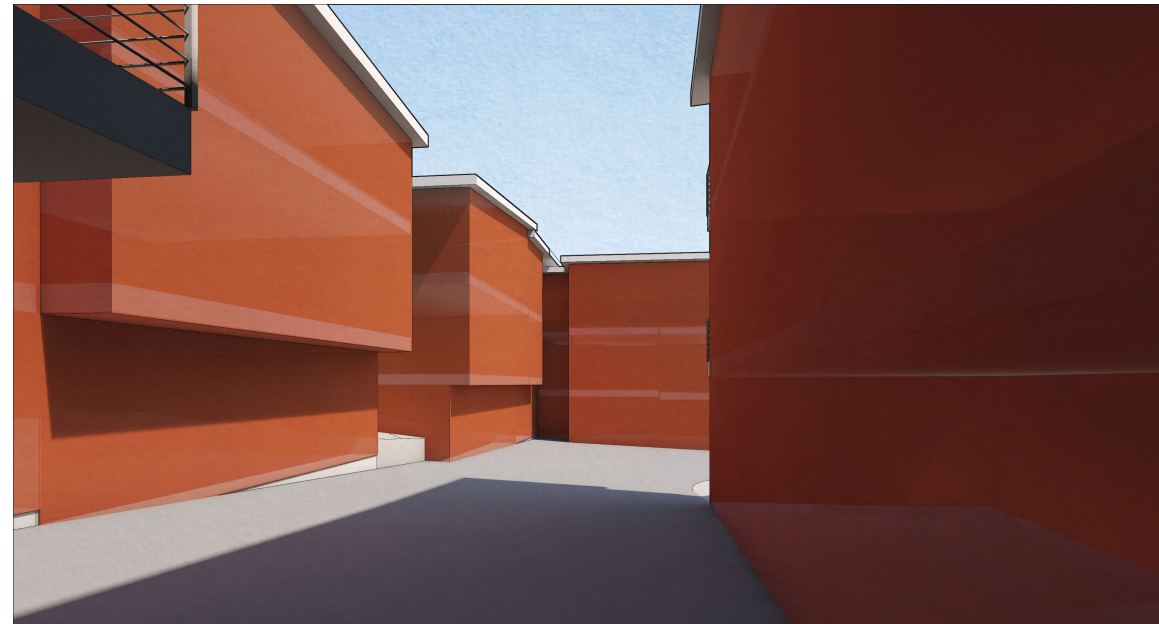




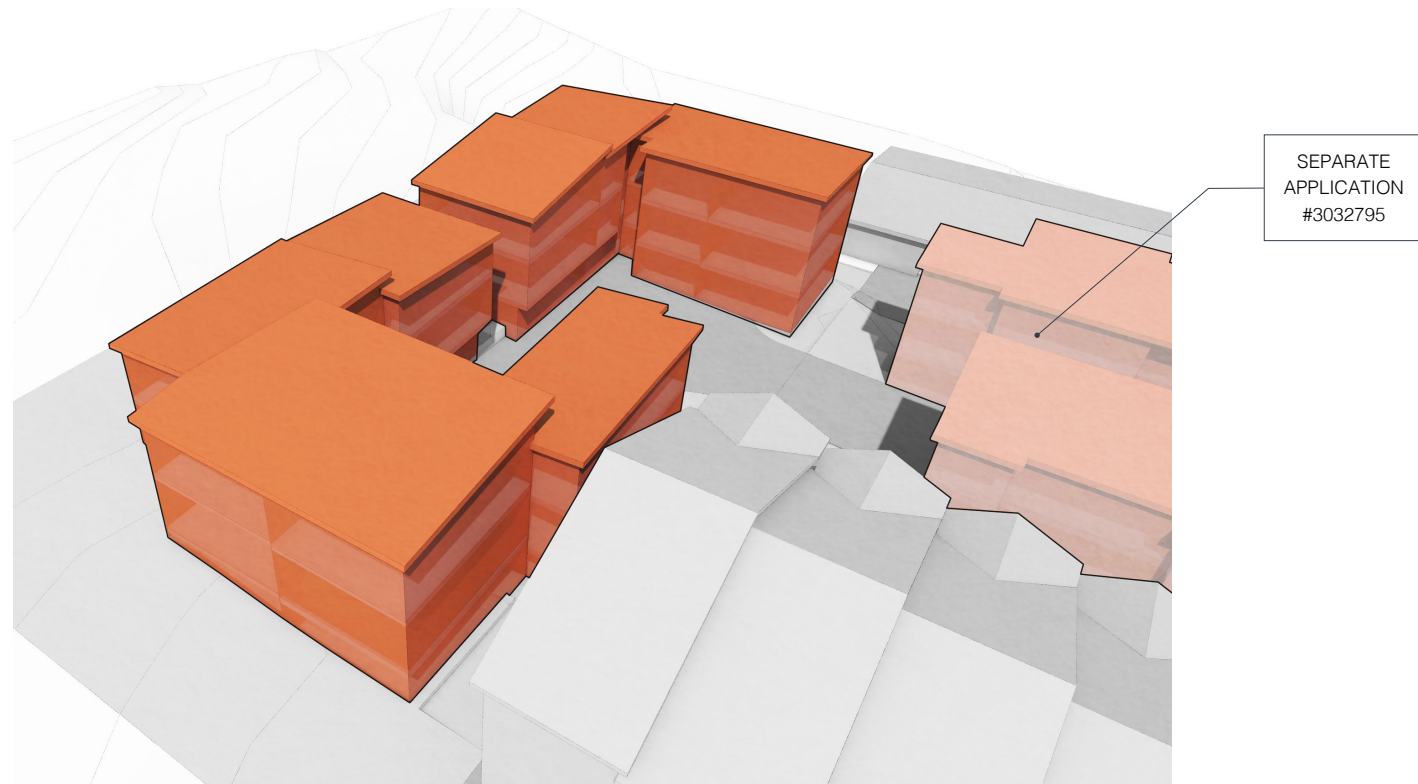
1  
Typical Upper Level Plan



1  
Ground Level Plan



3 View looking Southeast



4 Aerial View looking Southeast



ALTERNATIVE 3 (PREFERRED)

Alternative 3, the preferred alternative, is a code compliant scheme with two three-story structures, containing a total of (9) townhouse units. All units are parked in private garages at grade in each unit, accessed by a parking aisle in the center of the site. The units in Alternative 3 are organized in two structures, that together form a 'U'. This configuration results in a site plan that is contained to the north, south, and east, but open to the west and the site that this being developed concurrently with this project.

Large common amenity areas are located along the west property line, allowing for a sense of openness and increased western solar access. Pedestrian access is able to occur adjacent to common areas and work with the preferred alternative of the adjacent site. Reference page 33.

Units

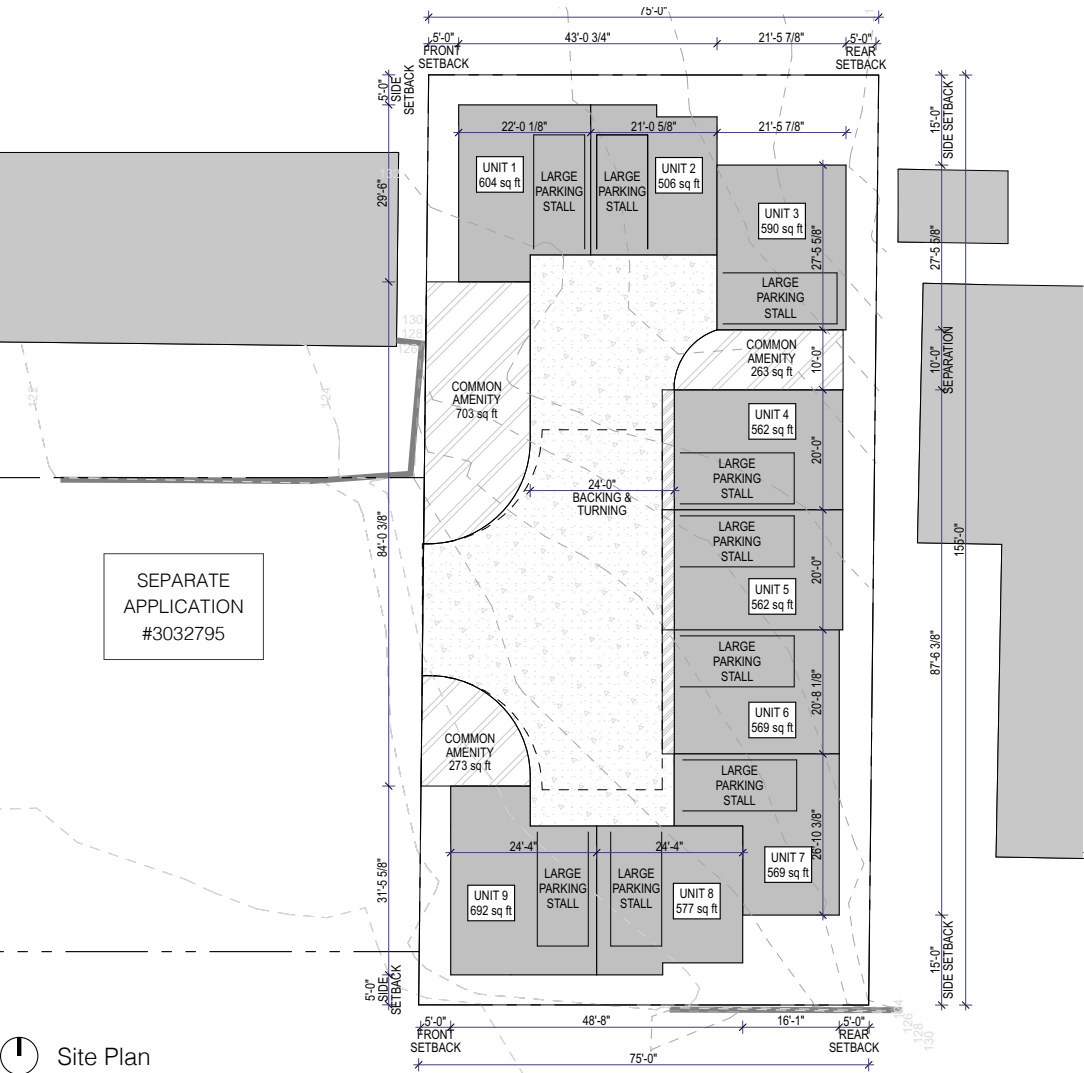
- 9 units.
- 9 parking stalls.

Advantages

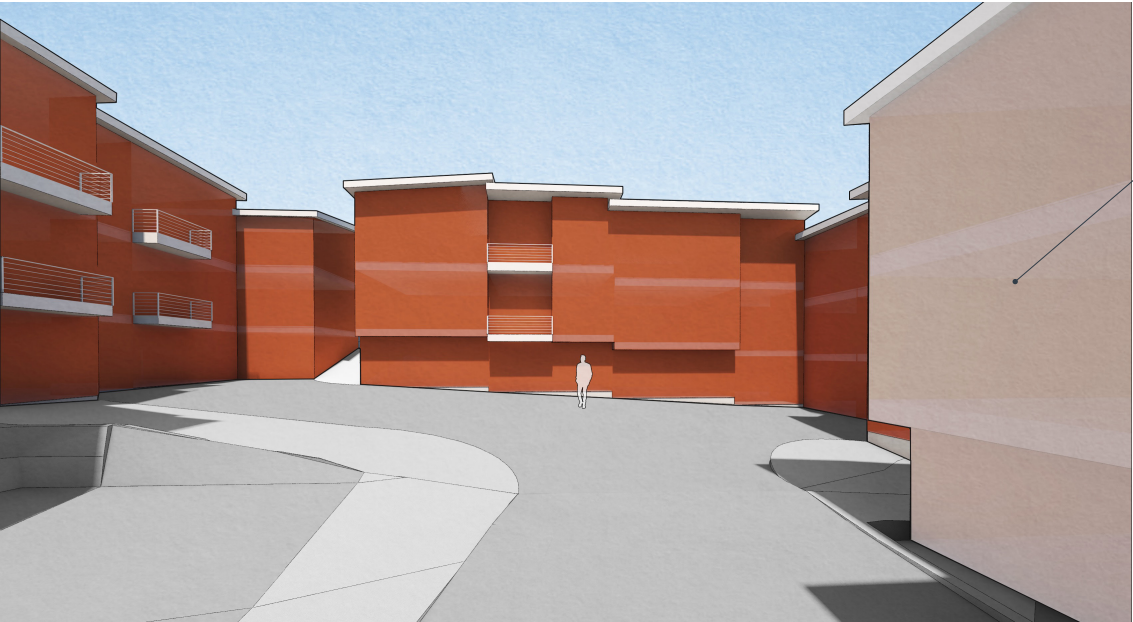
- Code compliant scheme.
- U-shape supports as sense of community while remaining open to the west property line.
- All units have either southern or western solar access.
- Shed roofs are architecturally contextual and responsive to site conditions.
- Interior of site is not dominated by parking aisle.
- Four unit have south facing balconies.
- Alternative 3 has the most open space. Reference page 32.

Departures

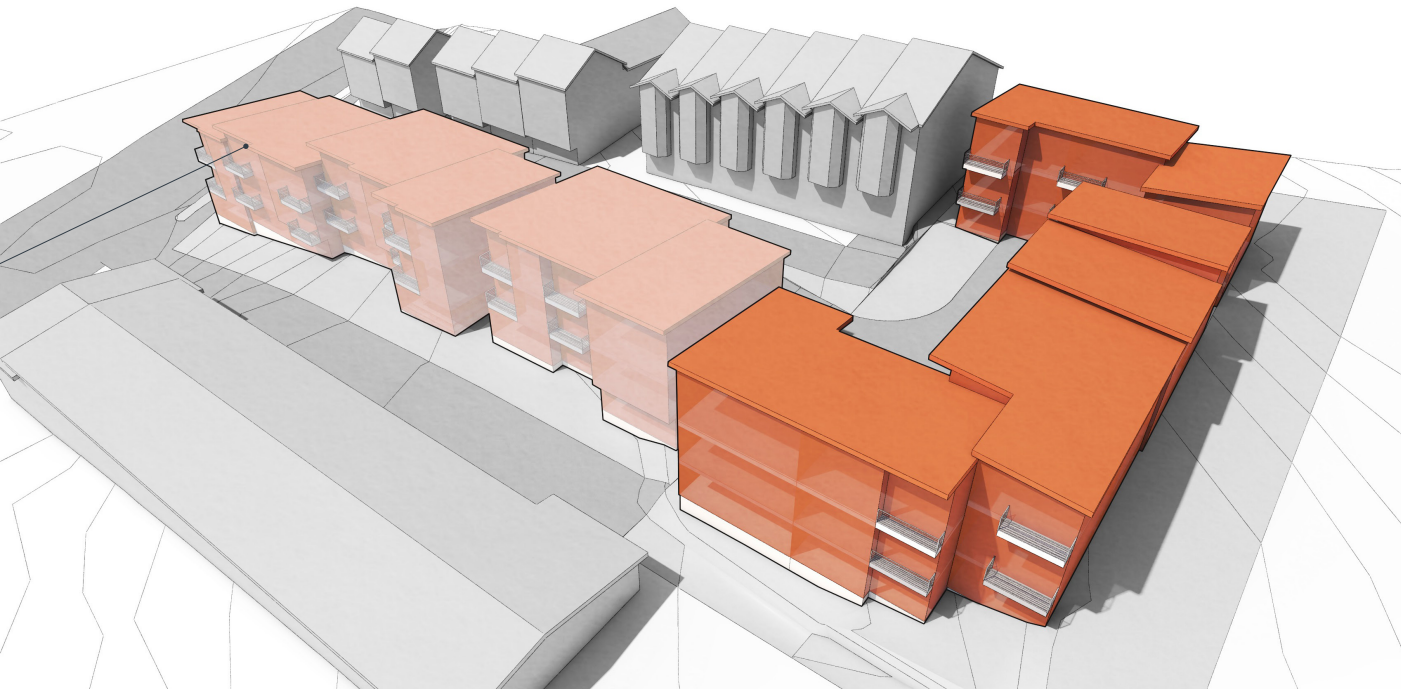
- No departures requested.



1 Site Plan

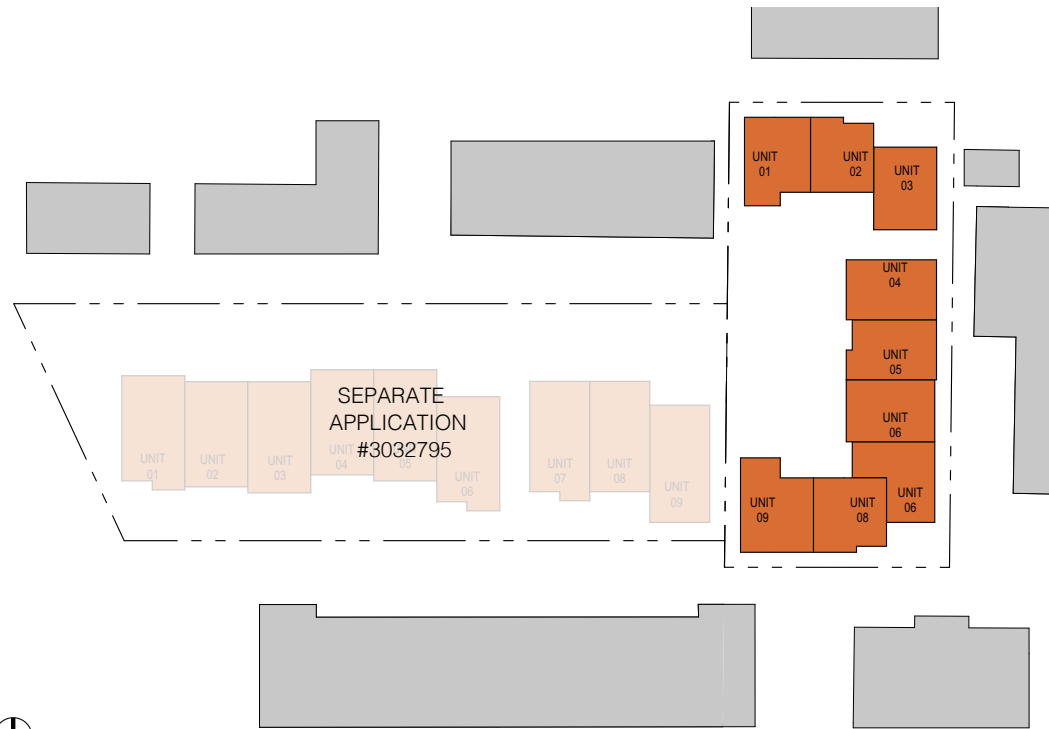


1 View looking East

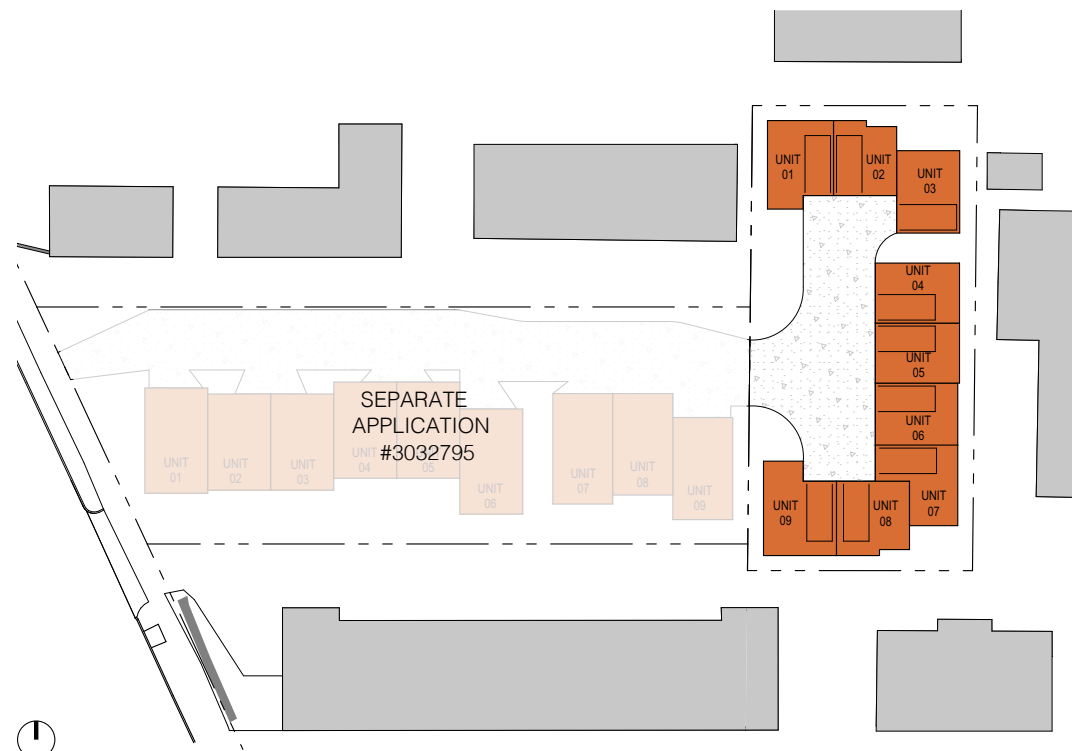


2 Aerial View looking Northwest

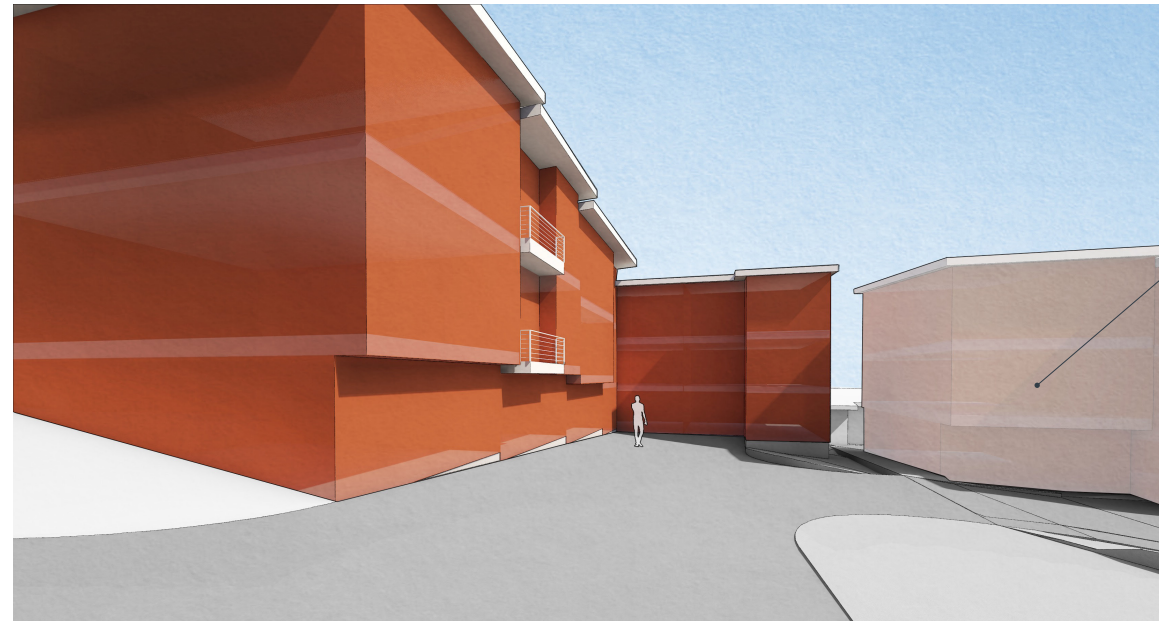




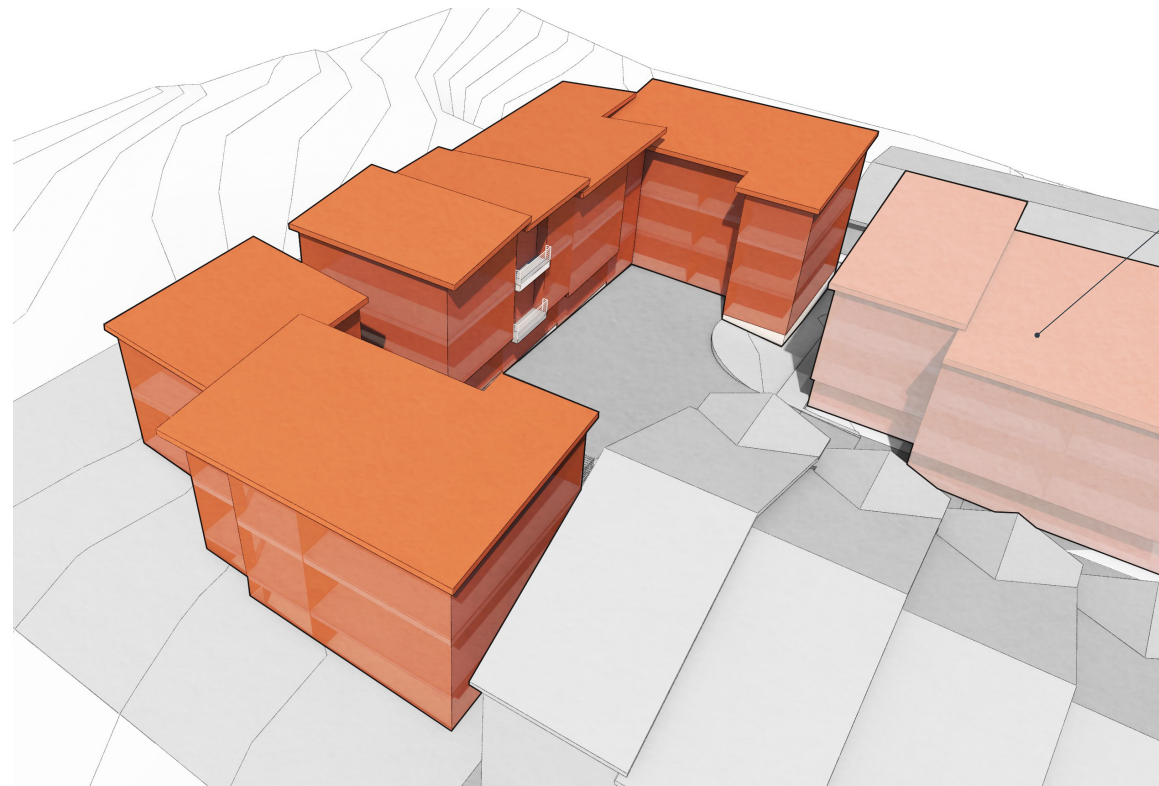
1  
Typical Upper Level Plan



1  
Ground Level Plan



3  
View looking Southeast



4  
Aerial View looking Southeast



OPEN SPACE COMPARISON

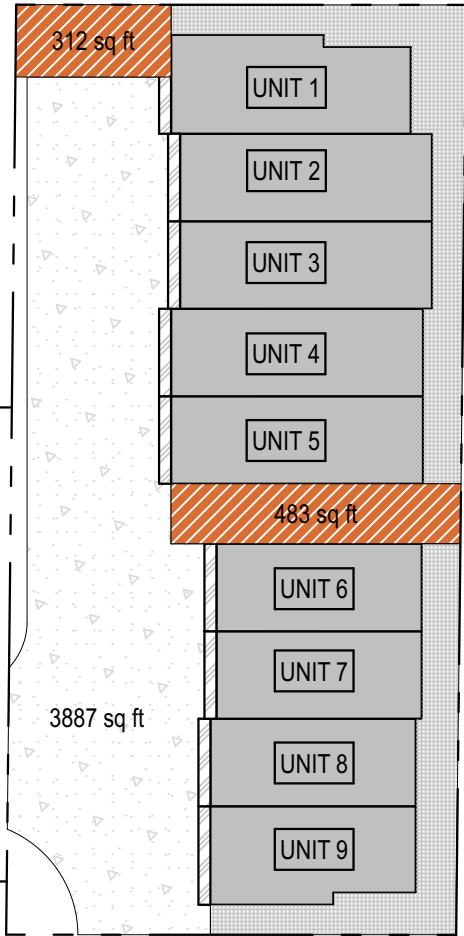
This landlocked parcel is designed to complement the adjacent site to the west at 6730 Rainier Avenue S. Pedestrian and vehicular access is through an access easement on the adjacent site to the west, terminating in a code-required turnaround on this site. The diagrams on the right highlight the open spaces in each of the alternative schemes presented on the previous pages. The areas highlighted in orange are open spaces that are not part of the driveway and are not narrow buffers located within setbacks. These spaces can provide common amenity areas or more generous private amenity spaces for individual homes.

This comparison demonstrates the advantage of having the project's units organized in a 'U' shape. The preferred alternative minimizes the amount of parking aisle required and allows for more private and common amenity area spaces.

LEGEND

- Unit
- Parking Aisle
- Narrow open spaces at setback
- Open spaces large enough for common amenity area or larger private amenity areas
- Structure Overhang

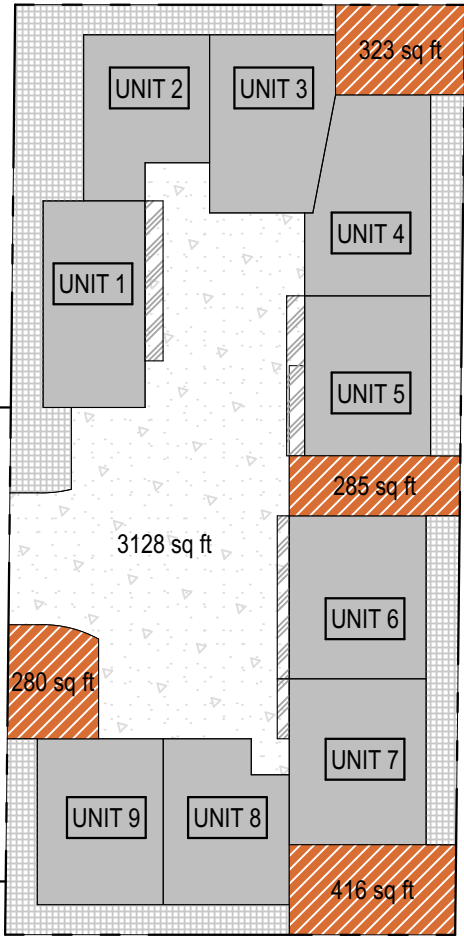
ALTERNATIVE 1



ALTERNATIVE 1 OPEN SPACE	
Area	
312 sq ft	
483 sq ft	
<b>795 sq ft</b>	

ALTERNATIVE 1 PAVING	
Area	
3,887 sq ft	
<b>3,887 sq ft</b>	

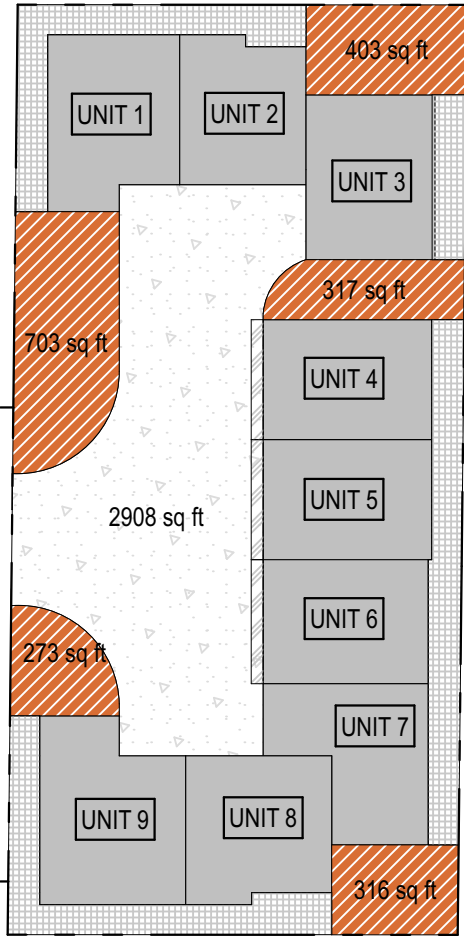
ALTERNATIVE 2



ALTERNATIVE 2 OPEN SPACE	
Area	
280 sq ft	
285 sq ft	
323 sq ft	
416 sq ft	
<b>1,304 sq ft</b>	

ALTERNATIVE 2 PAVING	
Area	
3,128 sq ft	
<b>3,128 sq ft</b>	

ALTERNATIVE 3  
(PREFERRED)



ALTERNATIVE 3 OPEN SPACE	
Area	
273 sq ft	
316 sq ft	
317 sq ft	
403 sq ft	
703 sq ft	
<b>2,012 sq ft</b>	

ALTERNATIVE 3 PAVING	
Area	
2,908sq ft	
<b>2,908 sq ft</b>	



# OPEN SPACE PRIORITIES

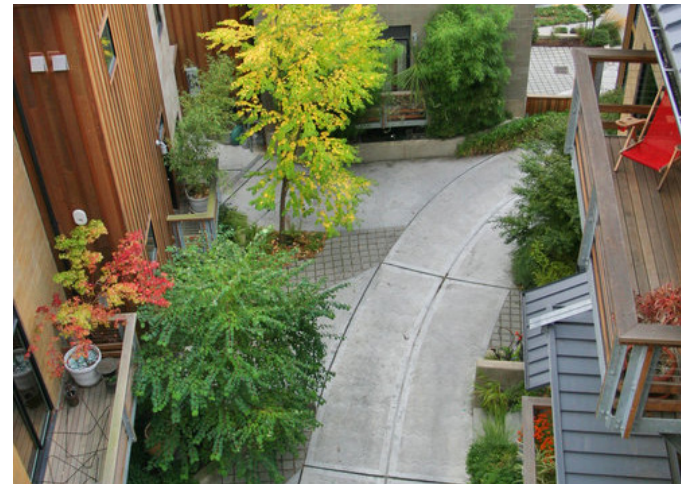
## OPEN SPACE PRIORITIES:

Due to the site's access constraints, the design team has prioritized open space as a primary development strategy. The on-site open spaces will be accessible by all residents and their guests from each residential unit. There will also be spaces for groups to congregate as well as spaces for private gathering. Below is a list of those priorities:

1. Pedestrian Connection to Adjacent Site and Street: The pedestrian experience will be considered for the full length of the distance between the site and Rainier Avenue S. This experience will vary along the path, however, it will be treated consistently in material and will relate to the adjacent open spaces on site. The shared pedestrian walkway will alternate between being separated from and within the driveway/parking aisle. In both cases, the pedestrian walkway will be differentiated through the use of distinct materials and assist in site way-finding.

2. Woonerf: Because the majority of the space that comprises the site's interior is devoted to the parking aisle, the design team is exploring ways in which this space can be used for other activities in addition to vehicle maneuvering. The spaces will rely on aspects of the European woonerf with varied paving surfaces, site furniture and pedestrian path variation to create opportunities for pedestrians and vehicles to co-exist in the space typically just for the vehicles.

3. Common and Private Amenity Areas: The common and private amenity areas on the site will provide spaces for gathering and retreat as well as provide buffers to and from adjacent sites add warmth and character and humanize the scale of the ground-level. The amenity areas will be a mix of hard surface and natural landscape.



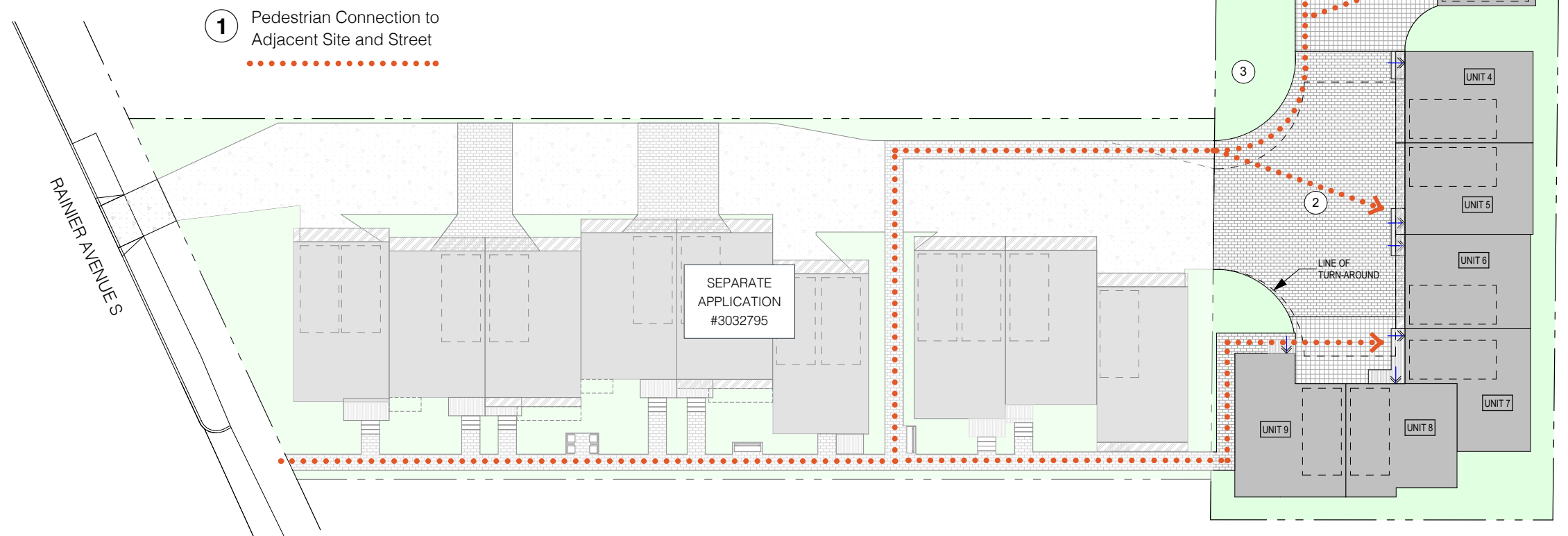
① Shared Pedestrian Walkway



② Woonerf



③ Common Amenity Areas

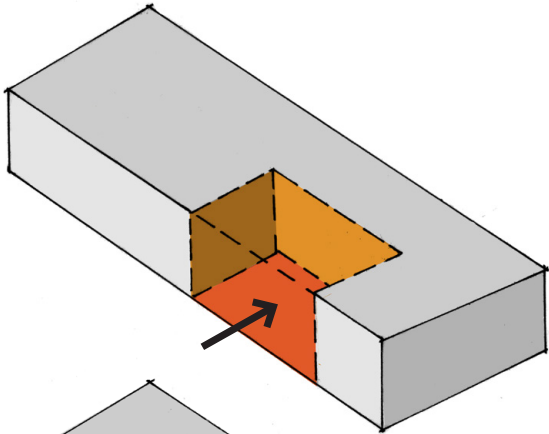




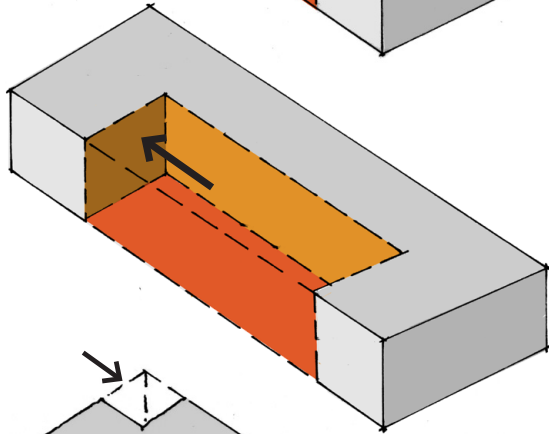
DESIGN CONCEPT

MASSING PRIORITIES:

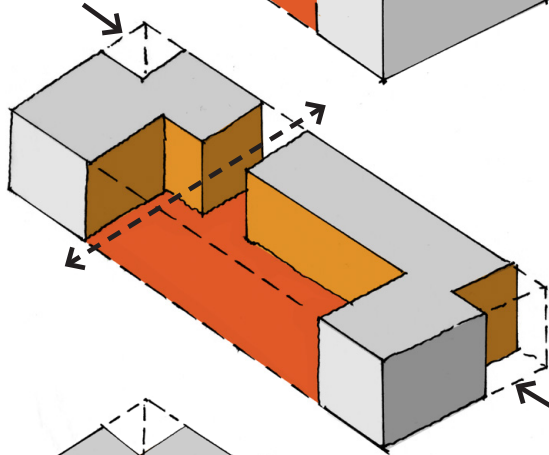
**A**  
Create U-shaped center woonerf around access point.



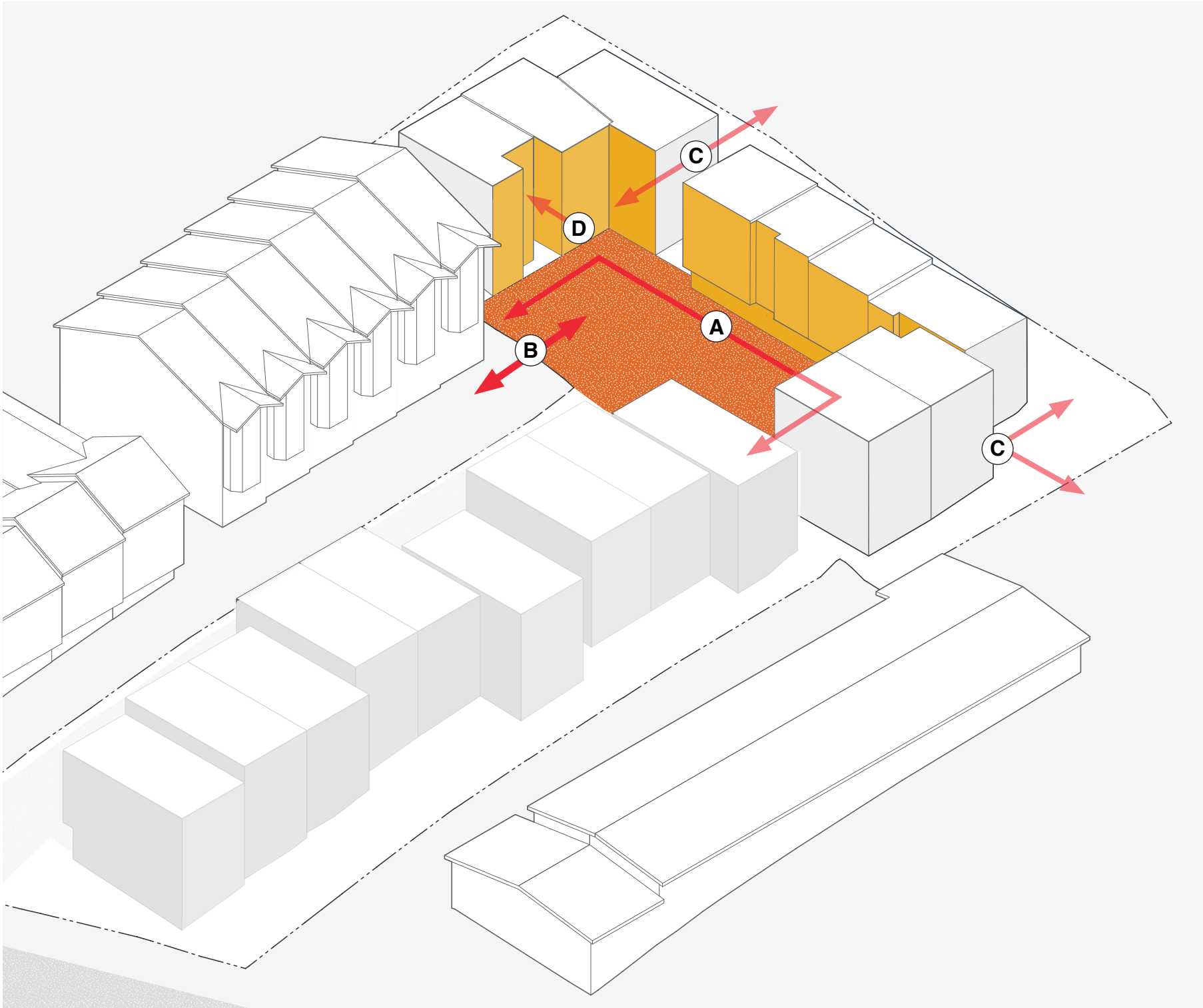
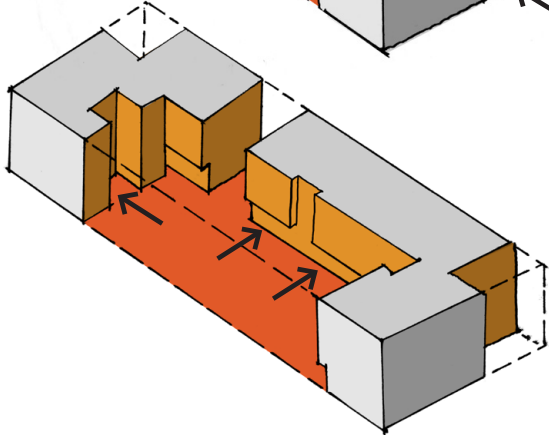
**B**  
Expand center woonerf to allow connection to adjacent property to the Northwest. U shaped woonerf maintained.



**C**  
Remove strategic portions of massing to achieve code compliance with side setbacks and building separation.



**D**  
Highlight entries and enliven facades through secondary massing moves.







## ARCHITECTURAL DEVELOPMENT

The design concept for this project works in unison with the strategies for open space development to reduce the perceived scale of the structure and support the pedestrian experience for residents and guests. Architectural features such as visible entries, planters, and canopies, reduce the scale of the surrounding structure and give proportion to the pedestrian experience. The surface of the driveway/parking aisle will include various paving materials to provide a clear pedestrian pathway to unit entries and common amenity areas. Rhythm and pattern within the massing, supported by secondary architectural features such as balconies and varied roof forms, activate the full extents of the site. The following are key strategies to support the architectural concept:

Rhythm and pattern in massing: the massing is shaped to define three edges of the site, west, north and south. In the center is a shared space for access and gathering. It is defined by the rhythm and pattern of the units that bound its edges.

1. Upper level activity and interest: balconies that face the open space at the center of the site, allow for occupation of the entire height of the structure, supporting the liveliness of the woonerf and increasing the security of the site.

2. Secondary Architectural Features: Distinct unit entries, canopies, and planters reduce the perceived mass of the structure and give proportion to the pedestrian experience.

3. Varied Roofs: A variety of roof slopes and heights allow units to be distinct and prevent the structure from being monolithic.

Preferred Alternative 3 Preliminary Perspective Sketch  
Highlighting Secondary Architectural Features



LANDSCAPE CONCEPT





# LANDSCAPE CONCEPT



Rose Creek



Paperbark Maple



Green Tower



Evergold



Dogwood



Orange Queen



Salal



Red Velvet



Mahonia



Walkers Low



Parrotia



Rhododendron



DESIGN GUIDELINES

CONTEXT AND SITE

CS1 NATURAL SYSTEMS & SITE FEATURES

CS1.B.1 - Sun and Wind

The site plan for the preferred alternative allows all units to receive western or southern exposure through its 'U' organization in site plan. In the 'U' shaped scheme, the structure does not form canyon-like spaces or significantly shade units. Common amenity areas in the preferred alternative have southern exposure as well.

CS1.D.1 - Plants and Habitat - On-site Features

The preferred alternative maximizes on-site green space through the creation of large amenity areas, some of which will be common. These areas will be a mix of landscape and permeable surface. Native planting will used where appropriate. To further highlight and separate unit entries, planters will be provided adjacent to stoops and porches where possible. These will give human proportion to entries and provide a private planting space for residents.

CS2 URBAN PATTERN & FORM

CS2.A.2 - Architectural Presence

The proposed development has no street frontage and is accessed through the adjacent site to the west. As a result, the proposal prioritizes a design that responds to the adjacent site through which it connects to Rainier Avenue S as well as the internal conditions of the site. This produces a design that increases the overall livability for its residents. The proposed structures are architecturally contextual and respond to site conditions. Roof forms, materials, and detailing will relate to surrounding structures and not seek to differentiate the project on their own. The proposed will be unique, however, through the organization and patterning of massing, materials, and facade elements as exhibited in the design sketches on page 35. This approach allows the project to be responsive while remaining contextual.

CS2.B.2 - Connection to the Street

Because the project has no street frontage, the site's connection to Rainier Avenue S is an important aspect of the preferred alternative. The preferred site plan prioritizes a distinct pathway in order to create a pleasant pedestrian experience for both resident and passerby. The landscaped buffers between the west property line and structures are a priority in the design and seek to create a space that engages the public and provides amenity for residents. The sole building facade facing the pedestrian and automobile approach will be composed of high-quality materials and represent the priorities of the design concept. Vehicular access is proposed to be separate from the main pedestrian path in the preferred alternative.

CS2 URBAN PATTERN & FORM

CS2.B.3 - Character of Open Spaces

The primary open spaces at the center of the site will be used as both a driveway and a gathering space for residents. The pedestrian occupation of this space will be encouraged through distinguished surface materials and connections to adjacent landscaped amenity areas. The open spaces have solar access and are easily reached by all residents. Balconies on the upper levels of the structure will add life and visual interest to the open spaces. The primary open spaces are well connected to adjacent sites to the west resulting from the 'U' organization of the structure.

CS2.D.5 - Respect for Adjacent Sites

The proposed site organization will focus the majority of windows and occupiable balconies toward the center of the site, rather than outward toward neighboring structures. Due to the slope of the neighborhood's topography, the portion of the structure adjacent to SF-5000 zoning will be roughly equal in height to the adjacent two-story structures. Views into other units will be minimized and studies for window openings will be conducted to ensure privacy for residents of the proposed structure and adjacent sites.

CS3 ARCHITECTURAL CONTEXT & CHARACTER

CS3.A.4 - Evolving Neighborhoods

The neighborhood in which the proposed is located is home to many older structures as well as existing lots that are currently under development. This project contributes to a positive context that the evolving neighborhood can build upon. Examples of this include: prioritizing the pedestrian experience in the site plan, creating site-responsive massing through orientation and roof forms, and using the landform as a positive influence on design concept.

PUBLIC LIFE

PL1 CONNECTIVITY

PL1.A.2 - Adding Public Life

The proposal supports a network of open spaces through the creation of a continuous common amenity area on the west side of the structure, comprised of landscaped common amenity areas and the driveway woonerf. The location and continuity of these features allows the amenity area to foster interaction between residents both through the adjacency of entries and the adjacency of entries to the main pedestrian path. Moreover, projected balconies are located above the main open space to further increase the density and activity of the common area.

PL1.B.1 - Pedestrian Infrastructure

The proposed primary pedestrian path is accessed directly from the public sidewalk on Rainier Avenue S. It continues through the adjacent site to the west that fronts Rainier Avenue S, and enters the proposed site in two locations along its west (front) property line. This connection promotes pedestrian activity on the site where it can be seen by residents and in the neighborhood and offers a safe connection to the street for residents and visitors.

PL1.C.1 - Selecting Activity Areas

Activity areas are located on the west side of the structure, both in and outside of the woonerf parking aisle. These areas are identified as part of the site planning process due to their relationship to adjacent sites and solar exposure.

PL2 WALKABILITY

PL2. B Safety and Security

Exterior walkways and decks will provide lines of sight throughout the project site. Lighting will provide sufficient illumination for pathways and entries.

PL2.C - Weather Protection

Overhead weather protection is integrated into each of the building entries through either a recessed entry or overhead canopy projection. Weather protection elements are a part of the patterning implemented on the building facades.

PL2.D - Wayfinding

Clear and prominent wayfinding is integrated into the project's site plan. The pedestrian path from the street is clearly defined through its materiality and planned lighting. Individual unit entries are highlighted on the facade and unit addressing will be clearly located.

PL3 STREET-LEVEL INTERACTION

PL3.A.2 -Ensemble of Elements

Unit entries coordinate several design features that are a part of the overall structure facades. These include: canopies, balconies, entry stoops, unique paving at entries, roof overhangs and planters.

PL3.B.1 - Security and Privacy

Due to the topography of the site, several residential first floors will be slightly elevated. This results in an ensemble of exterior stairs and stoops, which activate the amenity spaces and adjacent shared walkway, while providing a vantage point of security and privacy for the interior living spaces. Additionally, the unit entries follow the existing topography, and in many instances are located at different heights, allowing for a separation in private entry spaces.

PL4 ACTIVE TRANSPORTATION

PL4.A.1 - Serving All Modes of Travel

The creation of the woonerf within the parking aisle and access easement, and the use of distinct paving materials for the pedestrian pathways, helps to define safe areas for travel by foot and bike.

PL4.B.2 - Bike Facilities

Bicycle parking will be provided for all the units in an easily accessible location on site.

PL4.C - Planning Ahead for Transit

The site is located directly off Rainier Avenue S, a transit corridor connecting to downtown Seattle to the north.



## DESIGN CONCEPT

### DC1 PROJECT USES & ACTIVITIES

#### DC.B.1 - Vehicle Access and Circulation - Location

The proposal creates a woonerf area within the parking aisle to bring pedestrian life into an area that would conventional be dominated by cars. Distinct paving materials and textures will be used to add character to this area and make it inviting to residents.

#### DC.C.2 - Parking and Service Uses - Visual Impact

In order to minimize the effect of the on-site parking all parking will be provided in private garages inside each unit. In response to this, U, the proposed design will emphasize pedestrian elements including unit entries, landscaped planters, and overhead projection. This adds areas of visual interest in order to further balance the visual impact of the required on-site parking.

### DC2 ARCHITECTURAL CONCEPT

#### DC2.A.1 - Massing - Site Characteristics and Uses

The site slopes to the southwest and is much wider (155 feet) than it is deep (75 feet). The proposed massing approach responds directly to these site conditions through the implementation of a strategy of stepping heights that follows the sloping topography. The resulting massing is an organization of units that is visually interesting.

#### DC2.B.1 - Architectural and Façade Composition

The design team has worked to generate a well-proportioned composition with special consideration of the fact that the more visible facades of the project need to balance vehicular and pedestrian needs. A system of exterior balconies, cantilevers, and massing modulation will be used to highlight certain volumes while maintaining a consistent language throughout the project. Roof forms and varying unit heights add another layer of interest to the composition.

#### DC2.C - Secondary Architectural Features

The design team is developing a compositional strategy that will employ balconies, roof overhangs, railings, parapets and cantilever treatments to activate the proposed massing and enliven the site. This will occur to varying degrees at all edges of the site.

### DC3 OPEN SPACE CONCEPT

#### DC3.B.3 - Connections to Other Open Spaces

The woonerf and related open spaces for the proposed mid-block site rely on its connection to the adjacent site to the west. These relates the site to the street and the immediate neighborhood.

#### DC3.B.4 - Open Spaces Uses and Activities - Multi-family

Open spaces will be considered and designed to accommodate a variety of uses. The proposal will incorporate seating and landscape strategies that will activate these spaces. The internal network of entry stoops, private balconies and common walkways provides opportunities for various shared and private uses.

### DC4 MATERIALS

#### DC4.A.1 - Building Materials - Exterior Finish Materials

The proposal will include durable materials consistent with similar project types in the neighborhood. Materials and finishes will create an attractive and inviting street edge. Pattern and rhythm within the façade will be highlighted through variation of materials and finishes as a way of reducing the overall scale of the building and identifying individual units.

#### DC4.B.2 - Signage - Coordination with Project Design

Signage will be minimal but effective, supporting wayfinding and the design approach of the proposal. The design team proposes an approach to address signage that is incorporated with weather protection and is of a consistent design language as those secondary architectural elements of the proposal.

#### DC4.C.1 - Lighting - Functions

Lighting will promote safety and security, while also respecting the adjacent properties and units on site. It will add detail to the pedestrian realm and support on-site wayfinding.

#### DC4.D.1 - Trees, Landscape and Hardscape Materials

The landscape design will incorporate the various open spaces at the edges and center of the site. Landscaped amenity areas will create a noticeable transition when entering the threshold of this site. A landscape buffer will be created along the west property line to create a desirable connection to the neighbor to the Northwest. The network of open spaces will utilize high quality materials consistent with this scale of development. These materials and landscape elements will complement the architecture.



COMPLETED WORK b9 ARCHITECTS



Townhomes at 1224 5th Avenue N



Townhomes at 1224 5th Avenue N



Courtyard at Townhomes at 3806 Fremont Ave N



Townhomes at 1818 E Yesler Way



# WORK IN PROGRESS b9 ARCHITECTS



Townhouses by b9 architects in West Seattle



Townhouses by b9 architects in West Seattle