

EARLY DESIGN GUIDANCE - 2ND MEETING



UW 19th Ave, LLC
4726 + 4730 19th Avenue NE Seattle, WA

EARLY DESIGN GUIDANCE - 2ND MEETING
December 12, 2018
PROJECT #3031329

DEVELOPMENT OBJECTIVES

PROJECT DESCRIPTION
Located on a block between 19th Ave NE & 20th Ave NE, the project will build upon existing urban fabric in the University District Northwest. 14 units of 3-story Townhomes will include basements and outdoor amenities. The project proposes to meet the Green Building Standard for sustainability and better building performance. The development objectives for this project are as follows (all values are approximate):

Number of Townhomes:	14 Townhomes
Number of Rooms:	94
Number of Parking Stalls:	0

PROJECT INFORMATION

Zoning / Lot Size:	LR3 / 4,632 SF (4726) + 6,930 SF (4730) = 11,562 SF
Overlay:	University District Northwest (Urban Center Village)
Pedestrian Zone:	No
Parking Flexibility Area:	Yes
Infiltration Evaluation not Req'd:	Yes
Major Pedestrian St:	19th Avenue NE
Codes:	Seattle Land Use Code (current edition)

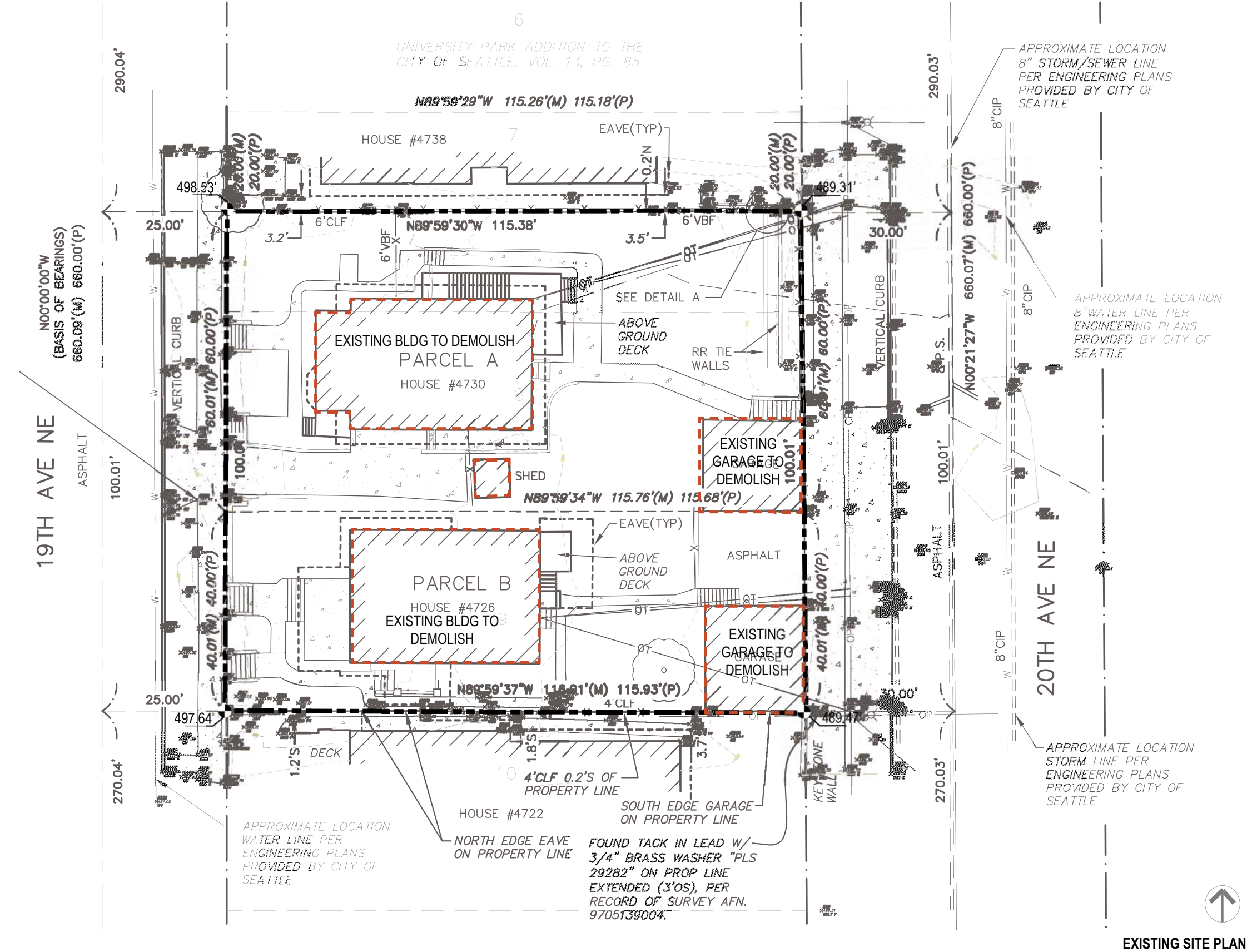
PROJECT TEAM

Applicant:	UW 19th Ave, LLC 9675 SE 36th St, Ste 105 Mercer Island WA 98040
Architect:	GROUPARCHITECT 1735 Westlake Ave. N. Suite 200 Seattle, WA. 98109
Landscape Architect:	TBD

LEGAL DESCRIPTION

UNIVERSITY PARK ADD (4726)
UNIVERSITY PARK ADD S 1/2 OF 7 & ALL 8 (4730)

UW 19th Ave, LLC
4726 + 4730 19th Avenue NE Seattle, WA 98105



EXISTING SITE PLAN

UNIVERSITY DISTRICT CONTEXT

The site is located within the University District Northwest Area of the Urban Center Overlay District. University District is changing with new developments and the Brooklyn light rail station planned for NE 43rd and Brooklyn Ave NE. The site is within a 10 minute walk area from the Brooklyn light rail station, and a predominantly area is in congregate housing, primarily catering to students.

IMMEDIATE NEIGHBORHOOD CONTEXT

The predominate architecture of the neighborhood is 1-2 story single family housings and rooming homes that create fully residential streets. The Northeastern part of the University District Northwest Urban Village is creating a home of fraternity and sorority houses.

ADJACENT AND NEARBY STREETS

The site fronts on 19th Avenue NE and the back of the site is facing 20th Avenue NE. 19th Avenue NE is one-way road towards North, and 20th Avenue NE is two-way road with bike lanes. Bike lanes on 20th Avenue continues to University of Washington campus on South, and to Ravenna Park on North. A shared bike lane is marked in both directions. The major intersections of NE 45th Street and 20th Avenue NE is located approximately one and a half block to the South. Continuous sidewalks are present along both 19th Avenue NE & 20th Avenue NE.

VIEWS AND AMENITIES

Anticipated views will mostly be territorial to the North over a mature treed neighborhood with the possibility of views toward the Cascades on the uppermost residential levels. The University Village is located within 10 min walk distance, and Brooklyn Station is within half-mile distance as a major public transportation.

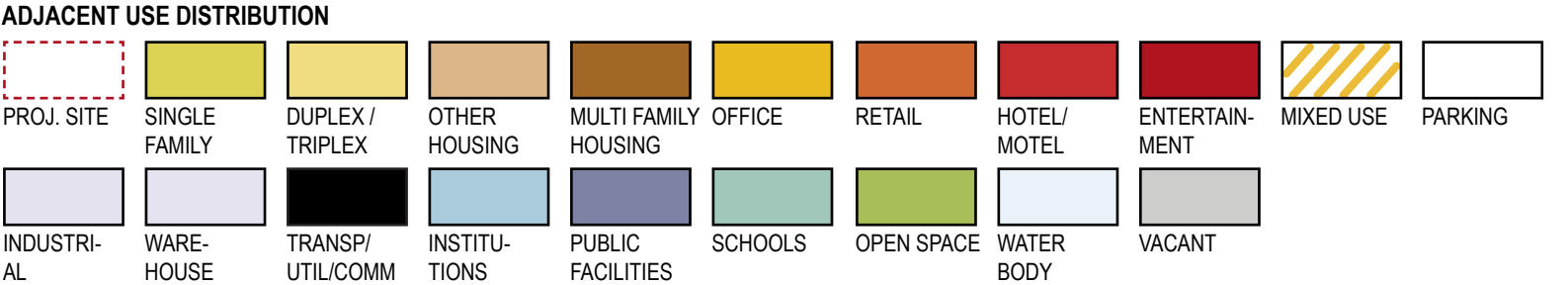
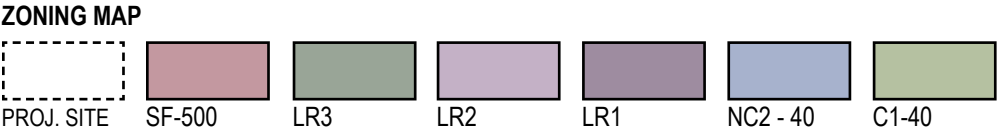
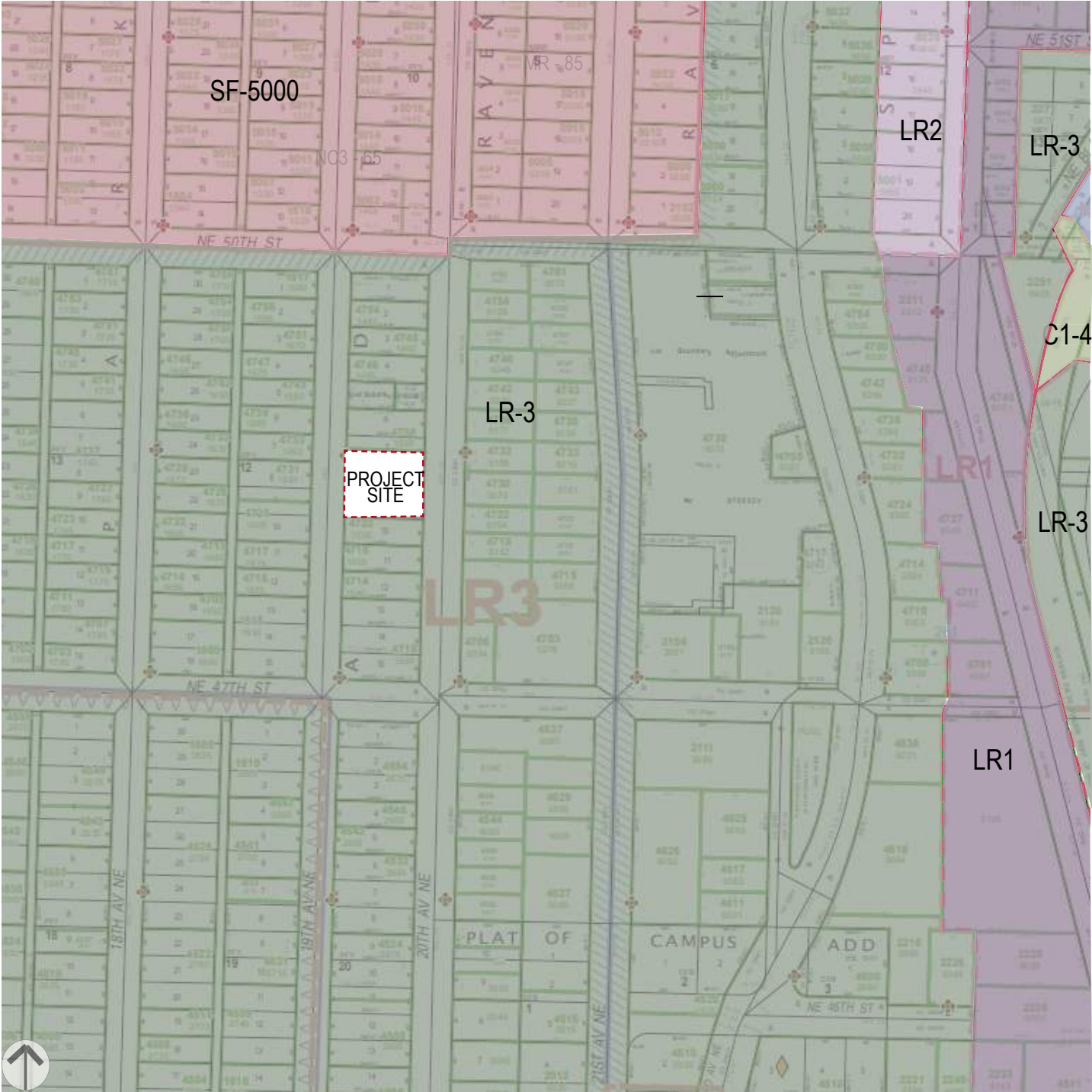
FUTURE PROJECTS

According to city records, the parcel to the Southeast of the site (4710 20th Avenue NE) is currently under construction with a multi-family structure containing 52 dwelling units. Many of the surrounding parcels are under developed by current land use code. The density in the neighborhood will increase as these parcels are developed.



UNIVERSITY DISTRICT NORTHWEST NEIGHBORHOOD

URBAN DESIGN ANALYSIS: ZONING & SURROUNDING USES



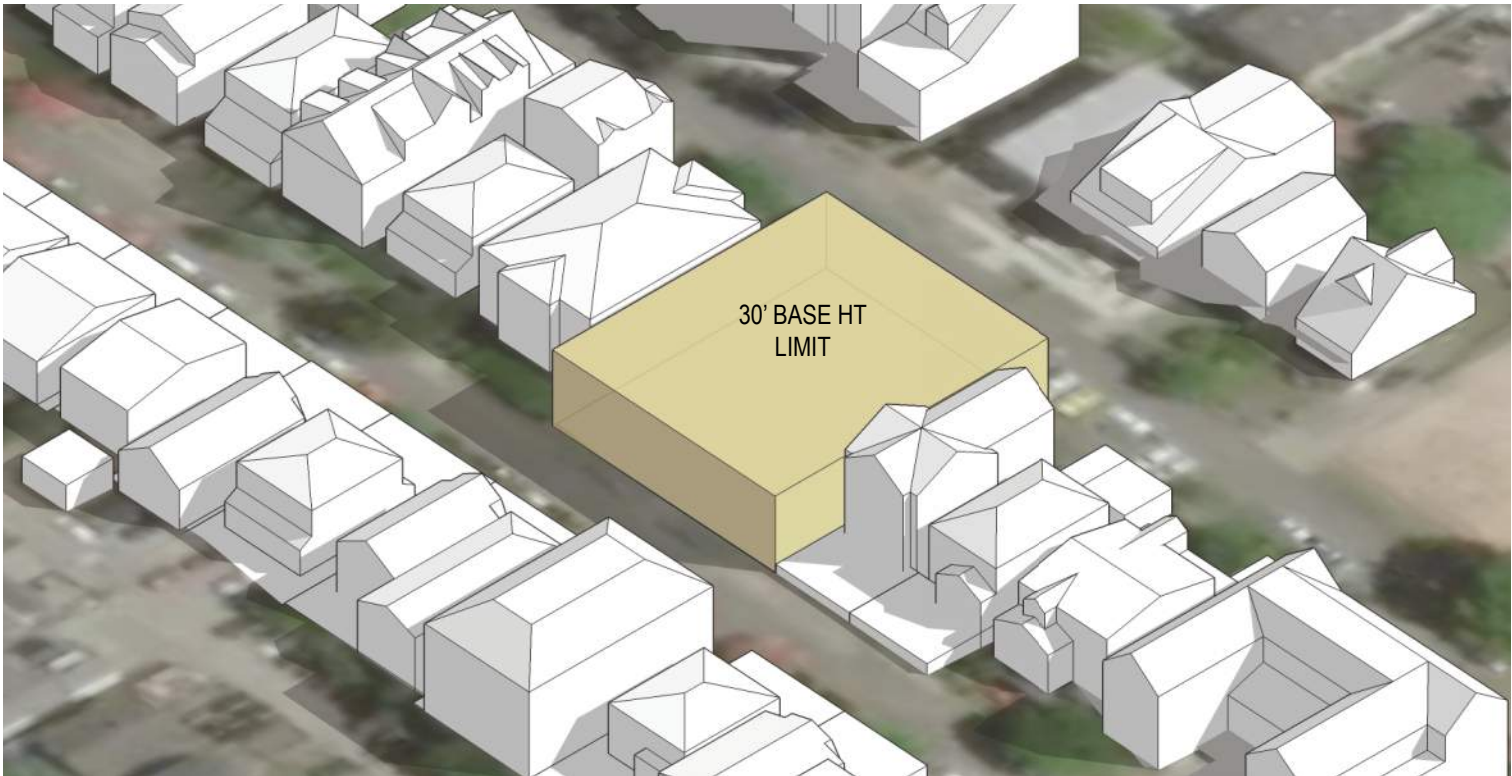
ZONING ANALYSIS: SEATTLE MULTIFAMILY (23.45) - LR3 ZONING

- 23.45.504 Uses Allowed - Residential Uses Allowed (Congregate residence prohibited, 9 bedrooms or more per structure)
- 23.45.510 FAR Limits - Townhomes, LR3 Zoning: 1.4 FAR Max**
- 23.45.510E Portions of Townhomes below grade or meeting exceptions are exempt from FAR calculations
- 23.45.512 Density - No limit for Townhomes in LR3 Zoning**
- 23.45.514 Structure Height - 40' Max (Inside urban center / Village) + allowable height exceptions under 23.45.514
- 23.45.517 Multifamily Zones With Mandatory Housing Affordability Suffix

** Max density requires both use of Green Building provisions under 23.45.510C and mandatory housing participation

Setbacks: Front - 5 ft min
Side - 5 ft min / 7 ft avg

Amenity Area: 11,562 X 25% = 2,890 sf min required (Min 50% at grade; 1445 sf)



ZONING ENVELOPE



OPTION A: NARROW ALLEY CONCEPT

PROS:

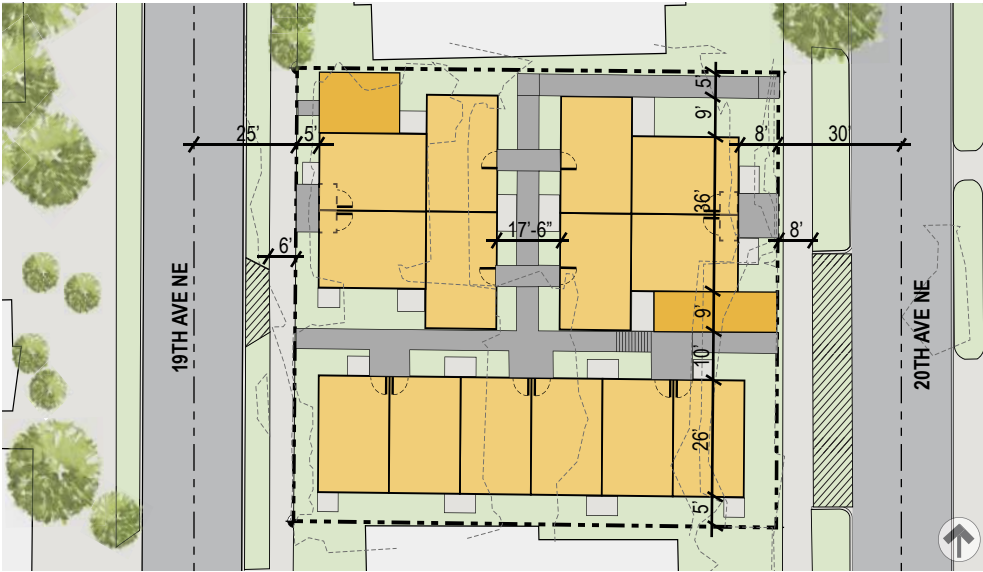
- Siting allows for possibility of pedestrian connection to inner courtyard for unit entries
- Open courtyard to allow light in
- North property line modulation varies, maximizes daylight to neighbor
- Interior circulation path allows unit access from site interior for majority of units
- Roof deck open space provided for all units

CONS:

- Some units have minimum separation between windows due to building placement
- Massing orientation maximizes bulk / massing along both ROW frontages
- South building's south facade unmodulated, increasing apparent bulk / massing and further requires departure as shown
- Site configuration places bike storage and refuse/recycle at site interior adjacent to units, diminishing common open space and landscaping

DEVELOPMENT OBJECTIVES:

Number of residential units -	14 units
Total number of rooms -	94 rooms



OPTION A - L1 PLAN



OPTION B: WIDE ALLEY CONCEPT

PROS:

- North-south central courtyard balances daylight with internal circulation space, mid-building massing
- Roof deck open space provided for all units

CONS:

- Some internal courtyard units have reduced density due to lack of ability to provide windows
- Long facade creates unmodulated east ROW massing
- Substantial amount of massing at north & south setbacks (similar to Option A)
- No opportunity to place refuse/recycle storage along 20th Ave ROW frontage
- Site configuration requires use of north & south setbacks for individual unit entries
- Bulky massing / lack of street modulation
- Least similar option to existing building pattern on 19th

DEVELOPMENT OBJECTIVES:

Number of residential units -	14 units
Total number of rooms -	90 rooms



OPTION B - L1 PLAN



OPTION C: COURTYARD CONCEPT (PREFERRED/CODE COMPLIANT)

PROS:

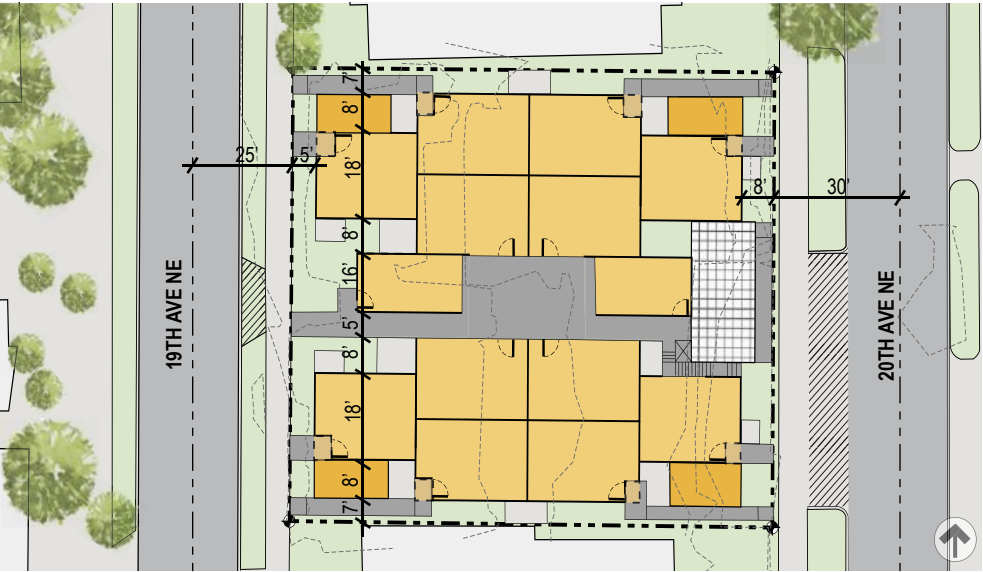
- Maximum building modulation along ROW frontage to reduce bulk/scale
- North and south building modulation varies, pulling back at corners to reduce hard edges
- Modulation along 20th Ave allows for optimum placement of bicycle parking and refuse/recycle
- Roof deck open space provided for all units
- Most similar massing / street modulation to existing building pattern on 19th
- Open Patio along 20th Ave as an amenity space for tenants

CONS:

- Site configuration requires use of north & south setbacks for individual unit entries

DEVELOPMENT OBJECTIVES:

Number of residential units -	14 units
Total number of rooms -	94 rooms



OPTION C - L1 PLAN

OPTION B: WIDE ALLEY CONCEPT

- PROS:
- North-south central courtyard balances daylight with internal circulation space, mid-building massing
 - Roof deck open space provided for all units
- CONS:
- Some internal courtyard units have reduced density due to lack of ability to provide windows
 - Long facade creates unmodulated east ROW massing
 - Substantial amount of massing at north & south setbacks (similar to Option A)
 - No opportunity to place refuse/recycle storage along 20th Ave ROW frontage
 - Site configuration requires use of north & south setbacks for individual unit entries
 - Bulky massing / lack of street modulation
 - Least similar option to existing building pattern on 19th

DRB COMMENTS

- MASSING: Most closely resembles the simple forms found on surrounding multifamily developments.
- PROJECT CONCEPT / DESIGN: The Board recommended to have a similar 'apartment block' concept for the project.
- OPEN SPACE: Common amenity spaces consist primarily of landscaped pedestrian circulation throughout the site with no usable space.
- SITE CIRCULATION: The Board suggested to have dedicated circulation separate from open space to have desirable and usable space.
- STREETSCAPE: The Board recommended to contribute to nearby context with a pattern of front yards and porch.



OPTION C: COURTYARD CONCEPT
(PREFERRED / CODE COMPLIANT)

- PROS:
- Maximum building modulation along ROW frontage to reduce bulk/scale
 - North and south building modulation varies, pulling back at corners to reduce hard edges
 - Modulation along 20th Ave allows for optimum placement of bicycle parking and refuse/recycle
 - Roof deck open space provided for all units
 - Most similar massing / street modulation to existing building pattern on 19th
 - Open Patio along 20th Ave as an amenity space for tenants
- CONS:
- Site configuration requires use of north & south setbacks for individual unit entries

DRB COMMENTS

- MASSING: The board felt cramped and cluttered.
- PROJECT CONCEPT / DESIGN: The Board suggested to utilize cohesive, singular expression for exterior facades.
- OPEN SPACE: Common amenity spaces consist primarily of landscaped pedestrian circulation throughout the site with no usable space.
- SITE CIRCULATION: The Board suggested to have dedicated circulation separate from open space to have desirable and usable spaces.
- STREETSCAPE: The Board recommended to contribute to nearby context with a pattern of front yards and porch.



BOARD COMMENTS - HEIGHT, BULK, AND SCALE:

1 The Board stated that the proposed townhouse layout feels cramped and cluttered. The proposal would work much better as a large, singular building. If townhouse typology is pursued, the massing should be consolidated into one or two simple forms that take inspiration from surrounding multifamily developments. (CS3-A-3)

RESPONSE: Additional site plan alternatives with simple massing and larger continuous open spaces were investigated in response to DRB feedback. The alternative massing schemes all found ways to be compatible with the architectural style and siting patterns of neighborhood buildings while allowing for a “singular” building design to be implemented.

2 The Board did not identify a preferred massing scheme, instead asking the applicant to return with a new proposal and additional massing alternatives. However, the Board was most supportive of Option B as it most closely resembles the simple forms found on surrounding multifamily developments. (CS3-A)

RESPONSE: Option B was further developed in response to DRB comments while other design options were investigated, drawing on similar solutions to the challenges proposed. Revised options focused on open space integration, street frontage modulation / setbacks in consideration of local context. Options are labeled based on similarity to prior design schemes as well as a new direction proposed, herein labeled Option D.

BOARD COMMENTS - LANDSCAPE AND OPEN SPACE:

3 The Board was concerned about the usability of proposed common open space across all three schemes. The proposed common amenity spaces consist primarily of landscaped pedestrian circulation throughout the site, with no real usable space. Consolidate open space to create amenity areas that are usable and desirable, with programming that anticipates future users’ needs and encourages social interaction. (DC3)

RESPONSE: All of the revised option schemes pushed the site designs to maximize social interaction, daylighting, and opportunities for continuous open space for residents. Unit entries were placed directly off the common courtyards wherever possible. See exterior massing scheme concept sketches for studies of open space distribution.

4 The Board recommended that a quality open space concept inform the project’s overall design. (DC2-A-1)

RESPONSE: Investigation of the site plan schemes yielded a new Option D which placed interior, private resident open space as the primary site planning objective, while Option B.1 and others refined the prior placement of open space and attempted to improve upon the prior concept.

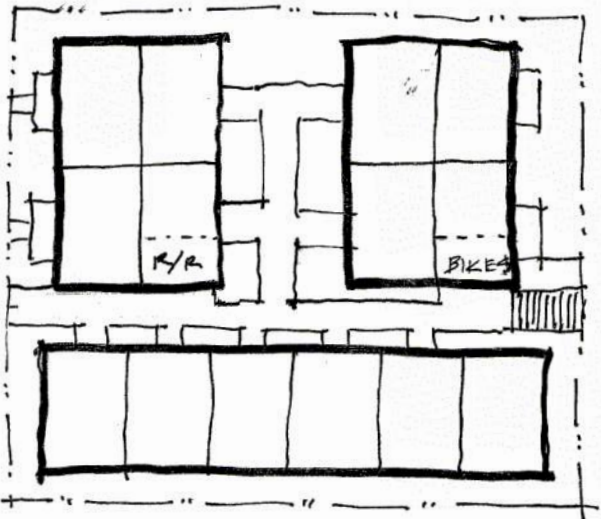
OPTION A.1

PROS:

- Centralized open space
- Recessed buildings from sidewalk, allows for front porches on 19th Avenue / private elevated patios on 20th Avenue frontages
- Locates solid waste storage to interior of site
- Bike storage room / accessible lift located at 20th Avenue grade in interior building w/ access to 19th Avenue street level

CONS:

- Departure required; some unit entries not facing open space or street
- Minimal departure likely at north PL
- Orientation of townhomes on northern half limits distance between these buildings and townhomes along south PL (approx. 15’ total)
- Retains bulky massing along south PL (departure required)



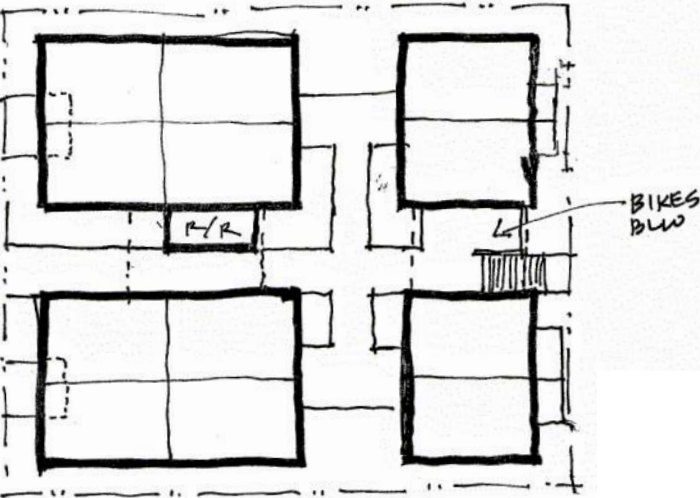
OPTION B.1

PROS:

- Recessed buildings from sidewalk, allows for front porches on 19th Avenue / private elevated patios on 20th Avenue frontages
- All units front on streets or common usable open space
- Bike storage room / accessible lift located at 20th Avenue grade in interior building w/ access to 19th Avenue street level
- Locates solid waste storage to interior of site

CONS:

- Minimal departures required at north and south PL to provide consolidated open space mid-site



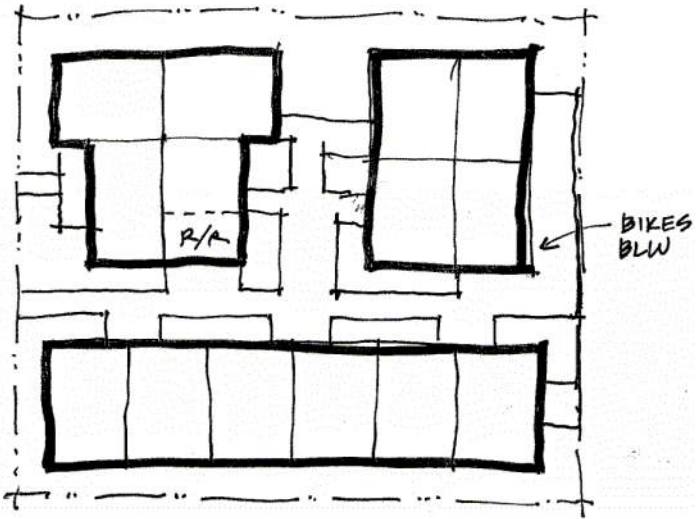
OPTION A.2

PROS:

- Combines front porch of units facing 19th Avenue to better replicate scale of porches in context
- Wider project entry walk / open space on 19th Avenue (approx. 20’ total) than option A.1 as result of porch combination
- Combines private elevated patios on 20th Avenue frontage with common amenity room and outdoor space
- Locates solid waste storage to interior of site
- Bike storage room / accessible lift located at 20th Avenue grade in interior building w/ access to 19th Avenue street level
- Locates solid waste storage to interior of site

CONS:

- Departure required – some unit entries not facing open space or street
- Departure required at north PL (structure depth)
- Retains bulky massing along south PL (departure required)



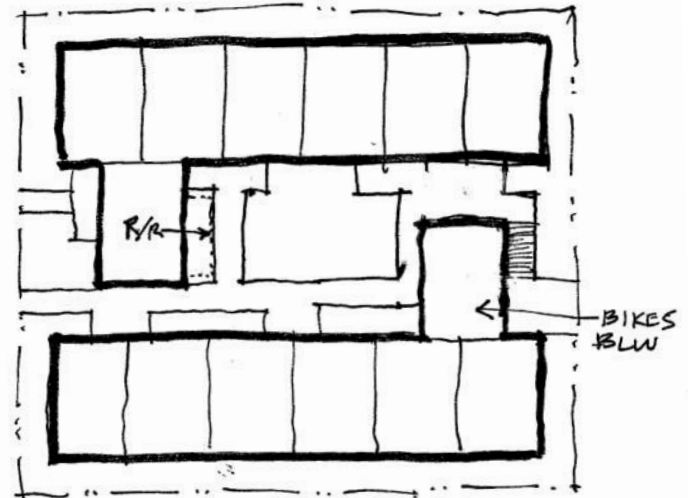
OPTION D

PROS:

- Largest common open space of all new options
- Combines front porch of units facing 19th Avenue to better replicate scale of porches in context
- Uses grade change to develop 20th Avenue frontage for bikes / integrate into pattern of street use
- Bike storage room / accessible lift located at 20th Avenue grade in interior building w/ access to 19th Avenue street level
- Locates solid waste storage to interior of site

CONS:

- Retains bulky massing along south PL (departure required)
- Adds bulky massing along north PL (additional substantial departure required)
- Limited to no rooftop unit decks due to common open space



BOARD COMMENTS - URBAN PATTERN AND FORM

5 The Board stated that the proposed massing schemes do not effectively respond to the existing neighborhood context. Study surrounding multifamily developments and provide a massing scheme that reflects its unique location in the city. (CS2)

RESPONSE: The refinement of the massing schemes and site plan concepts began with an analysis of the surrounding multi-family & single-family structures in the neighborhood to document and study the existing patterns. These studies have been explored and documented on the following sheets. Street level massing and reference studies added on this page document observations of existing street character and building scales. Summary findings included recessed front porches and consistent datum lines at varying levels (porches, garages, ridge lines etc). Exterior materials were also studied on later pages with observations noted. Alternative massing schemes were developed as a response to these studies to reflect the characteristics and patterns of the existing neighborhood. See pages 10 to 13.

6 The Board observed that ‘apartment blocks’ makeup the majority of surrounding multifamily developments and recommended that a similar massing option be pursued. Utilize a cohesive, singular expression for exterior facades. Individual expression on interior facing facades may be an interesting design concept worth investigating. (CS2, CS3)

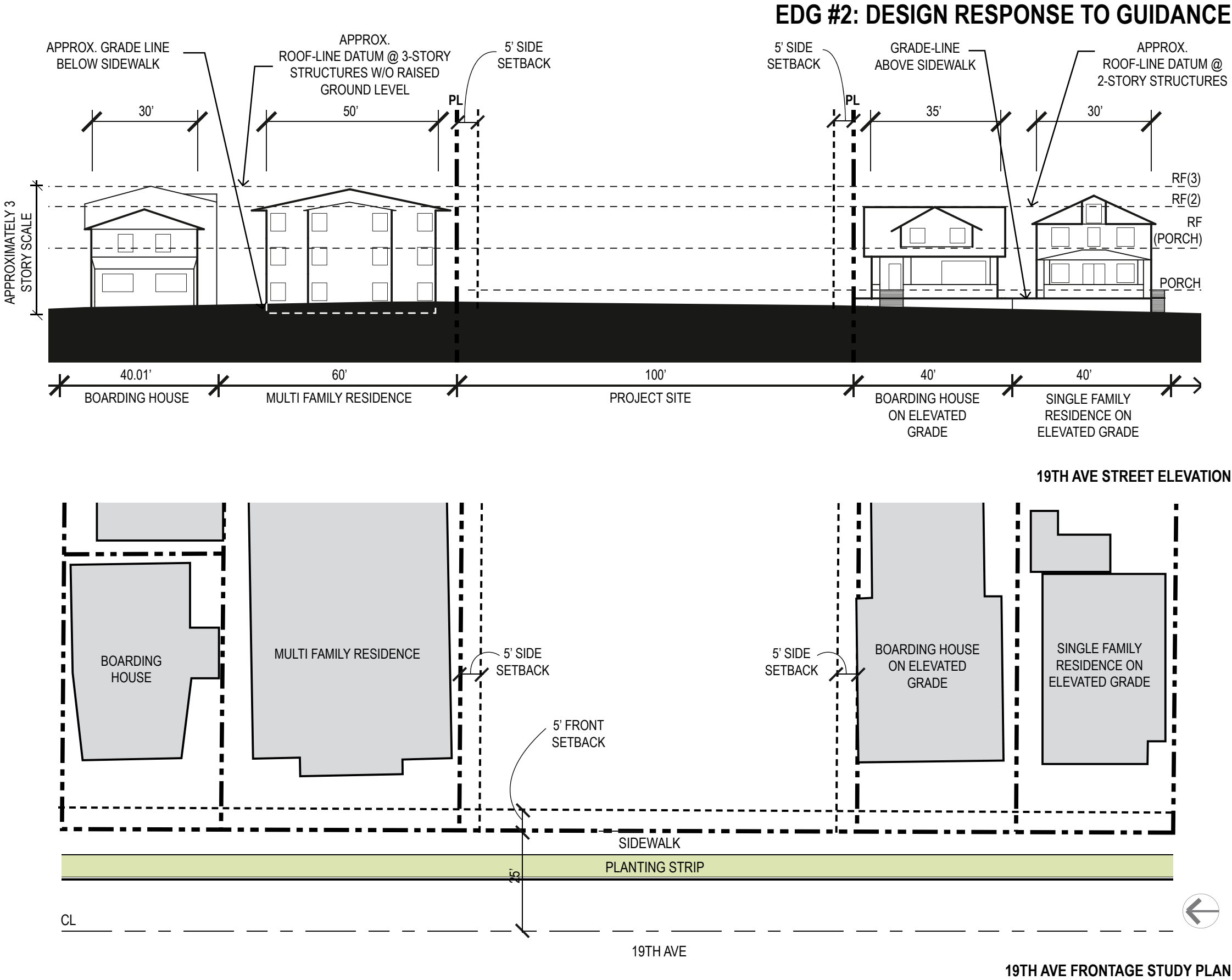
RESPONSE: The “single building” approach to the exterior design was taking as documented in the following revised building massing studies. Alternate design concepts based on each massing are provided for DRB comment.

7 The Board noted the existing nearby context with a pattern of front yards and recommended that the project be designed to respect and contribute to this pattern as much as possible. (CS2-A-1, CS3-A-3, PL1-A-1)

RESPONSE: Front yard patterns and recessed building masses were noted as prevalent in our streetscape and frontage studies as documented in this packet. The massing for each design concept strove to incorporate the front yard / front porch pattern into the respective scheme while still responding to existing streetscape features such as neighboring datum lines, building scale, etc. See pages 18-19 & 28-29.

8 Angled roof forms are prevalent in the neighborhood and are used on every building on the subject block. The Board recommended introducing angled roof forms to further connect the proposal to the existing context. (CS3-A)

RESPONSE: Angled roof forms indeed are prevalent as noted in our streetscape studies documented in this packet. The massing for each design concept strove to incorporate these into their design while still responding to existing streetscape features such as neighboring datum lines, building scale, etc. See pages 18-19 & 28-29.



BOARD COMMENTS - MATERIALS:

9 The Board recommended that the number of different materials be limited in service of a more cohesive expression. Use materials to increase the scale of the building as opposed to highlighting individuality. (DC2-B-1, DC4)

RESPONSE: See exterior massing scheme concept sketches for examples of proposed cladding materials, transitions, and distribution on the buildings. Each scheme has been approached as a comprehensive design / building site rather than individual townhomes to unify the project's identity and respond to neighborhood context.

10 The Board stated that any material changes should correspond to a change in plane. (DC2)

RESPONSE: See exterior massing scheme concept sketches for examples of proposed cladding materials, transitions, and distribution on the buildings.

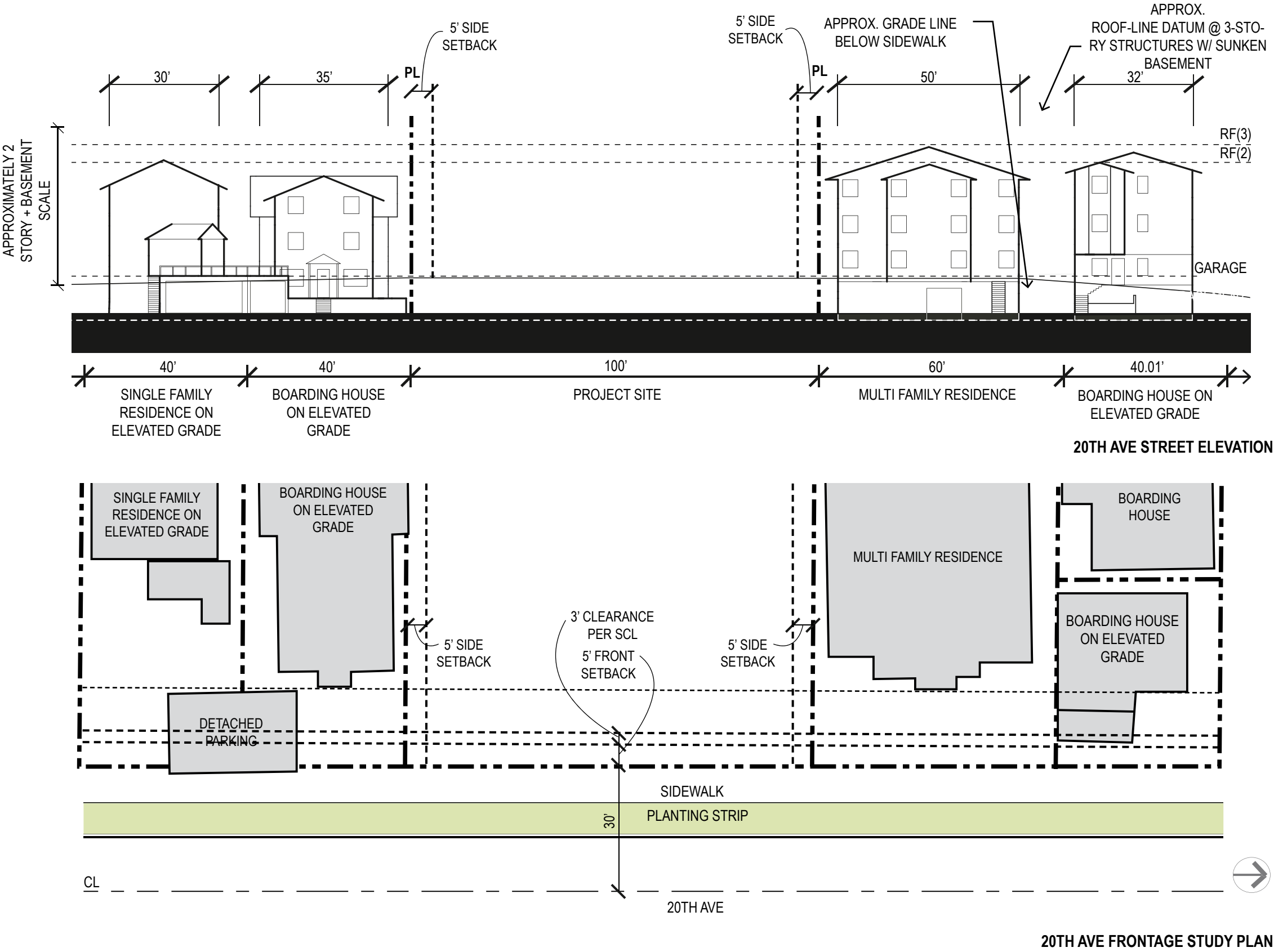
11 The Board recommended that material choices be representative of the unique character of the University District. Refer to surrounding development and to the University District neighborhood guidelines for inspiration. (DC4-I)

RESPONSE: Exterior material studies included on the following sheets. Two different exterior concepts (one more traditional / contextual, and one with more modern detailing and materials) have been included for DRB comment.

BOARD COMMENTS - SUNLIGHT AND NATURAL VENTILATION:

12 Due to the arrangement, orientation, and number of townhouse units proposed, the Board was concerned about access to light and air, particularly for basement-level rooms. Site planning should be revisited to maximize access to light and air for all users at all levels. Consider utilizing a building typology that will allow for better access. (CS1-B-2)

RESPONSE: Site planning and daylighting access were taken into account with the development of the updated massing schemes shown earlier. Where possible, basement-level units receiving daylight via area wells were removed and relocated to attic level locations in each option. These moves also reduced conflicts where units were at risk for privacy intrusion due to location next to public areas such as sidewalks and internal walkways.

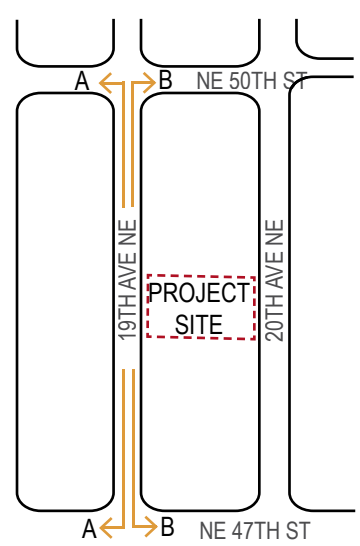


EDG #2 RESPONSE: CONTEXT ANALYSIS + STREET ELEVATIONS

ANALYSIS Of 19TH AVE STREET ELEVATIONS :

Existing architectural context of 19th Avenue is composed of single family housing / boarding houses with multi-family structures interspersed between them. Many of the single family structures are elevated from sidewalk level with planting areas and stoop entries and porches, but single family structures level to the sidewalk are intermittently observed. Traditional gable or hip roof on both single family and multi-family buildings occur as well as dormers from gable roofs are present on single family structures to maximize interior space and daylight. Primary entries to housings are along 19th Ave on both sides of the street. Single family structures are mostly 2-stories, and multi-family structures are 3-stories on top of a parking/basement podium. Due to single family structures elevated above ground and multi family structures sunken from ground level, the height of structures on street is consistent. Front yard setbacks from 19th Avenue range from 8' to 12', wider than required minimum setback (5'), and often have entry stoop and entry porch. Bike storage nor trash enclosure is not visible from 19th Ave. Predominant building material is brick and lap siding, and brick is often used as an accent material on base/porch. Neutral color finish is predominant with some accent colors.

STREET ELEVATION DIAGRAM



1 Wider front setback with planting area and stoop entries on elevated ground level at single family homes



2 Sunken multi-family housing, instead of elevated from ground level to maximize density while reducing perceived massing



3 Planting area flat to the sidewalk with entry porch at single family homes



4 Gable or hip roof with dormer at upper floor at single family homes



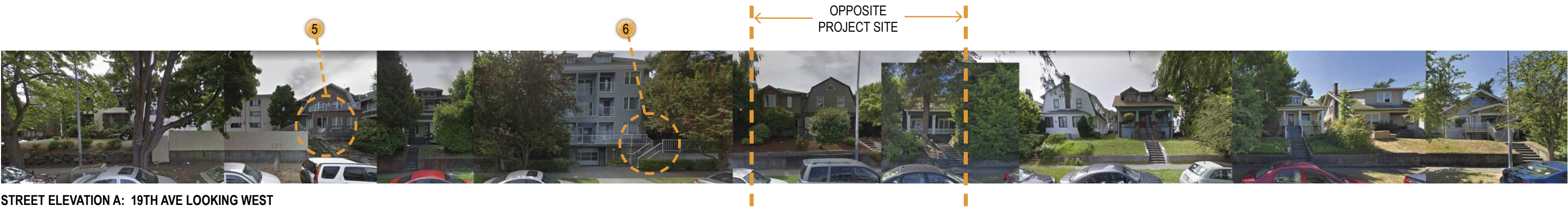
5 Elevated deck facing street



6 Access to residential lobby with stairs from street



STREET ELEVATION B: 19TH AVE LOOKING EAST



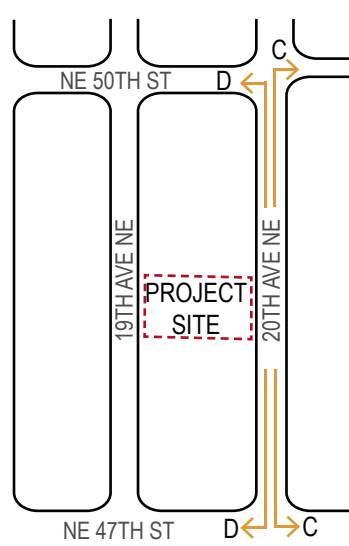
STREET ELEVATION A: 19TH AVE LOOKING WEST

EDG #2 RESPONSE: CONTEXT ANALYSIS + STREET ELEVATIONS

ANALYSIS OF 20TH AVE STREET ELEVATIONS :

Unlike from 19th Avenue, 20th Avenue shows different scales and characters on recently-built multi-family structures to the East with introducing contemporary materials, shed roofs and bold finish colors. The west side of the street has strong transportation connections with garages/on grade parking and bike lane. Most lots have a detached terraced garages with roof top open space on west side of street. Setback line on 20th Avenue west is not as distinctive as on 19th avenue, and detached garages are commonly positioned at the edge of the property line of each parcel, providing shortest distance from the street to the garage entry. On 20th Avenue east, front setback varies from 5' to 12'. Recently built multi-family housings have 10'~12' setback with ramps, trash enclosure/ open space/mailboxes for tenants from 20th Avenue. Width of each residential structures are fairly consistent except for congregate housings along NE 47th Avenue, and a few multi-family structures. There are multi-family structures with 4-stories and a basement on 20th Avenue, showing various building heights. Brick and stone material is visible on 20th Avenue on East at main entries and porches, but concrete base for garages are commonly found on 20th Avenue West. Cement board panels and lap sidings with saturated and bold colors were used on recently built multi family structures.

STREET ELEVATION DIAGRAM



1 Garage access from street, patio on top of garage



2 Stepped planters with stair to residential level at a multi-family housing



3 Shed roof at multi-family housings



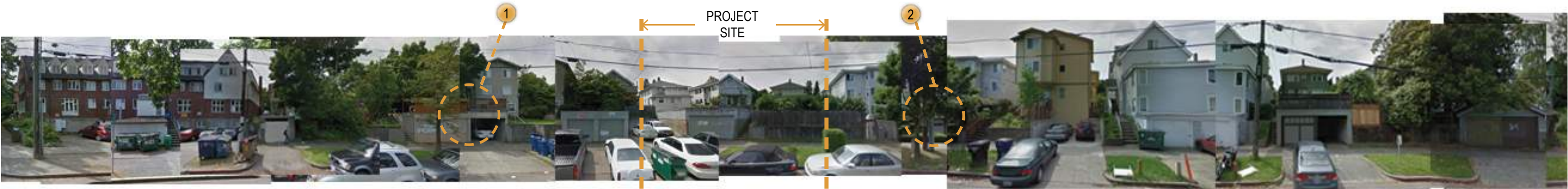
4 Outdoor space with benches trellis



5 Sunken massing with window wells



6 Detached garages along 20th Ave on West with parking strip

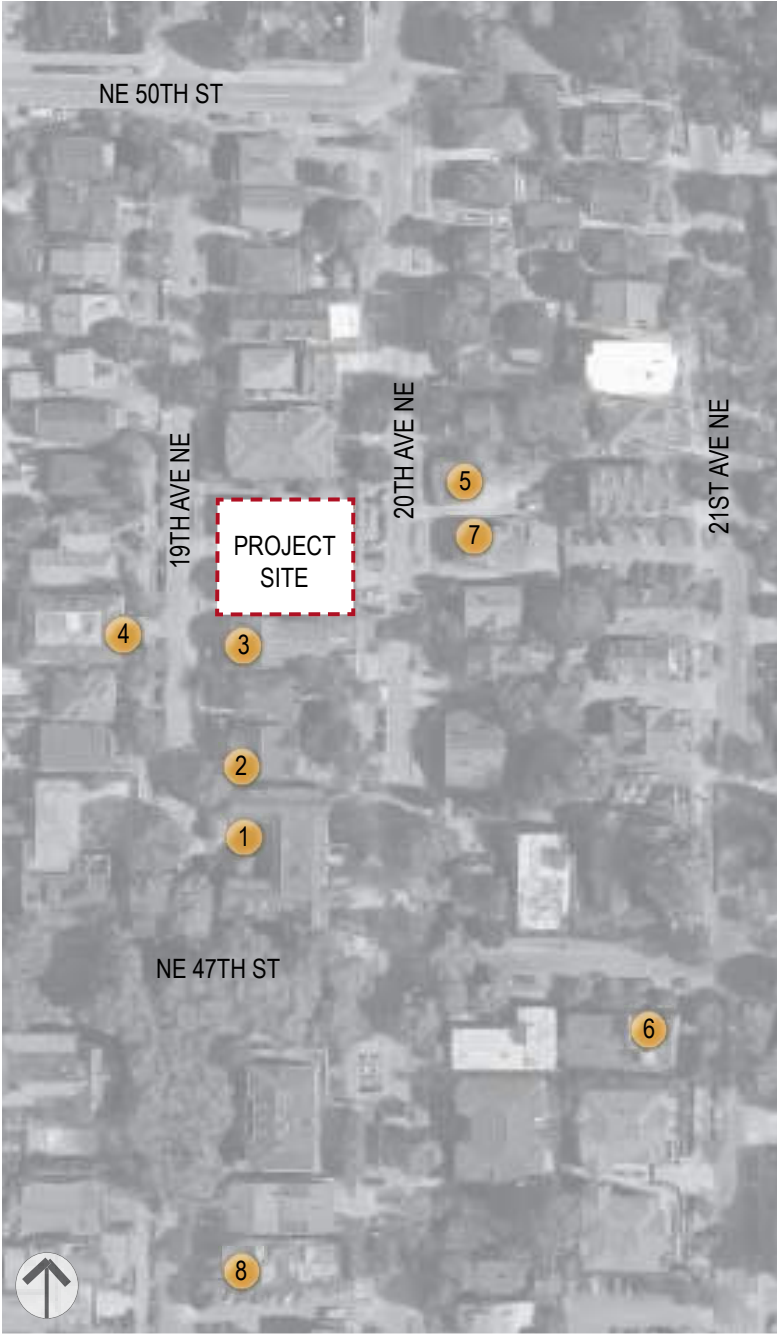


STREET ELEVATION D: 20TH AVE LOOKING WEST



STREET ELEVATION C: 20TH STREET LOOKING EAST

EDG #2 RESPONSE: MATERIAL & NEIGHBORHOOD CHARACTER STUDY



EXISTING PATTERN LOCATIONS

- 1. 4710 19th Ave NE - Mid-rise congregate housing
- 2. 4714 19th Ave NE - Boarding house
- 3. 4720 19th Ave NE - Boarding house
- 4. 4721 19th Ave NE - Mid-rise apartment
- 5. 4742 20th Ave NE - Mid-rise congregate housing
- 6. 4637 21st Ave NE - Mid-rise congregate housing
- 7. 4732 20th Ave NE - Mid-rise congregate housing
- 8. 4542 19th Ave NE - Mid-rise apartment

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ROOFS:

While gable roofs are predominant on single family buildings in the neighborhood, multi-family and congregate tend to have different roof forms as shed roof, flat roof, gable roof and cross hipped roof. Dormers commonly on single family homes, but more limited and intermittent on multi-family and congregate structures.

MATERIALS:

Brick is commonly used on both single family and multi-family structures on base/porch except on congregate residences, where the brick was used on entire facades. Brick at the ground level with lap siding above is predominant siding combinations. Cement board panels and lap siding is commonly found on recently built multi-family buildings. Other materials as stone veneer, shakes and stucco are present in the neighborhood as well. Exposed concrete base/podium at multi-family housing is commonly found.

FINISH COLORS:

Neutral paint finish colors are the most common on the majority buildings within the neighborhood, but bright and saturated colors are also used on recently-built structures.



CONGREGATE HOUSING CROSS GABLED ROOF WITH DORMERS, RED BRICK FACADE WITH LAP SIDING.



BOARDING HOUSE GABLE ROOF WITH DORMERS, STONE EXTERIOR MATERIAL WITH LAP SIDING.



BOARDING HOUSE GABLE ROOF WITH DORMER, SHAKE & PAINTED BRICK WITH SAME COLOR.



APARTMENT CROSS HIPPED ROOF WITH DORMERS, CONCRETE AT BASE STUCCO SIDING ABOVE.



CONGREGATE HOUSING SHED ROOF, CEMENT BOARD PANEL AND LAP SIDING.



CONGREGATE HOUSING FLAT ROOF WITH ROOF DECK, BRICK AT BASE AND LAP SIDING ABOVE.



BOARDING HOUSE GABLE ROOF, SHED ROOF W/ AT ENTRY, CB PANEL AND LAP SIDING & STONE CLADDING AT COLUMN BASE.



APARTMENT CROSS GABLED ROOF W/ ROOF DECK ON TOP STUCCO AT BASE, CB PANEL & LAP SIDING ABV.



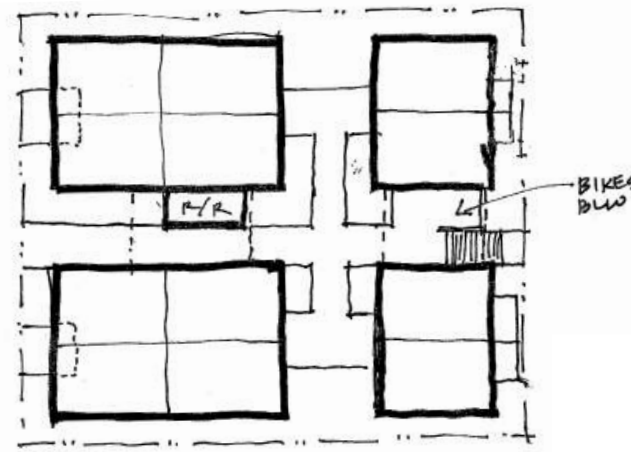
OPTION B.1

PROS:

- Recessed buildings from sidewalk, allows for front porches on 19th Avenue / private elevated patios on 20th Avenue frontages
- All units front on streets or common usable open space
- Bike storage room / accessible lift located at 20th Avenue grade in interior building w/ access to 19th Avenue street level
- Locates solid waste storage to interior of site

CONS:

- Minimal departures required at north and south PL to provide consolidated open space mid-site



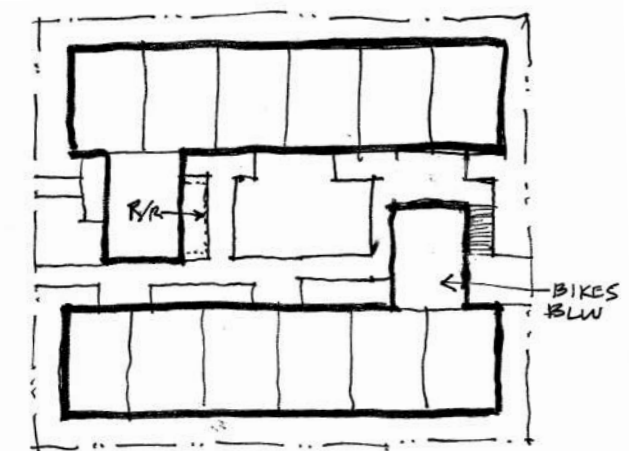
OPTION D

PROS:

- Largest common open space of all new options
- Combines front porch of units facing 19th Avenue to better replicate scale of porches in context
- Uses grade change to develop 20th Avenue frontage for bikes / integrate into pattern of street use
- Bike storage room / accessible lift located at 20th Avenue grade in interior building w/ access to 19th Avenue street level
- Locates solid waste storage to interior of site

CONS:

- Retains bulky massing along south PL (departure required)
- Adds bulky massing along north PL (additional substantial departure required)
- Limited to no rooftop unit decks due to common open space



OPTION B.1

PROS:

- Recessed buildings from sidewalk, allows for front porches on 19th Avenue / private elevated patios on 20th Avenue frontages
- All units front on streets or common usable open space
- Bike storage room / accessible lift located at 20th Avenue grade in interior building w/ access to 19th Avenue street level
- Locates solid waste storage to interior of site

CONS:

- Minimal departures required at north and south PL to provide consolidated open space mid-site

ZONING CALCS

Refuse / Recycling: Unit # (12) / 10 = cubic yards of each
Food Waste Collection: 5 - 50 Unit building = (1) 96 gallon cart
Enclosure: 9 - 15 Dwelling units provide min. 150 sf
= 150 sf storage area required

Provided: Recycle = (2) 2 cu. yd container (min 3.5 cu. yd required per SPU)
Refuse = (2) 2 cu. yd container (min 3.5 cu. yd required per SPU)
Food Waste = (2) 96 gallon cart (min 2 containers required per SPU)
Enclosure = 260 sf

Bike Parking: Residential (Multi-family) = 1 space / dwelling unit
= 12 total required

Provided: 24 spaces provided at Bike Storage off 20th Ave
(exceeds code requirement by 100%)

Amenity Area: Minimum 25% of lot area = 2,890.5 sf
Minimum at grade = 1,450 sf
(50% of required amenity area)

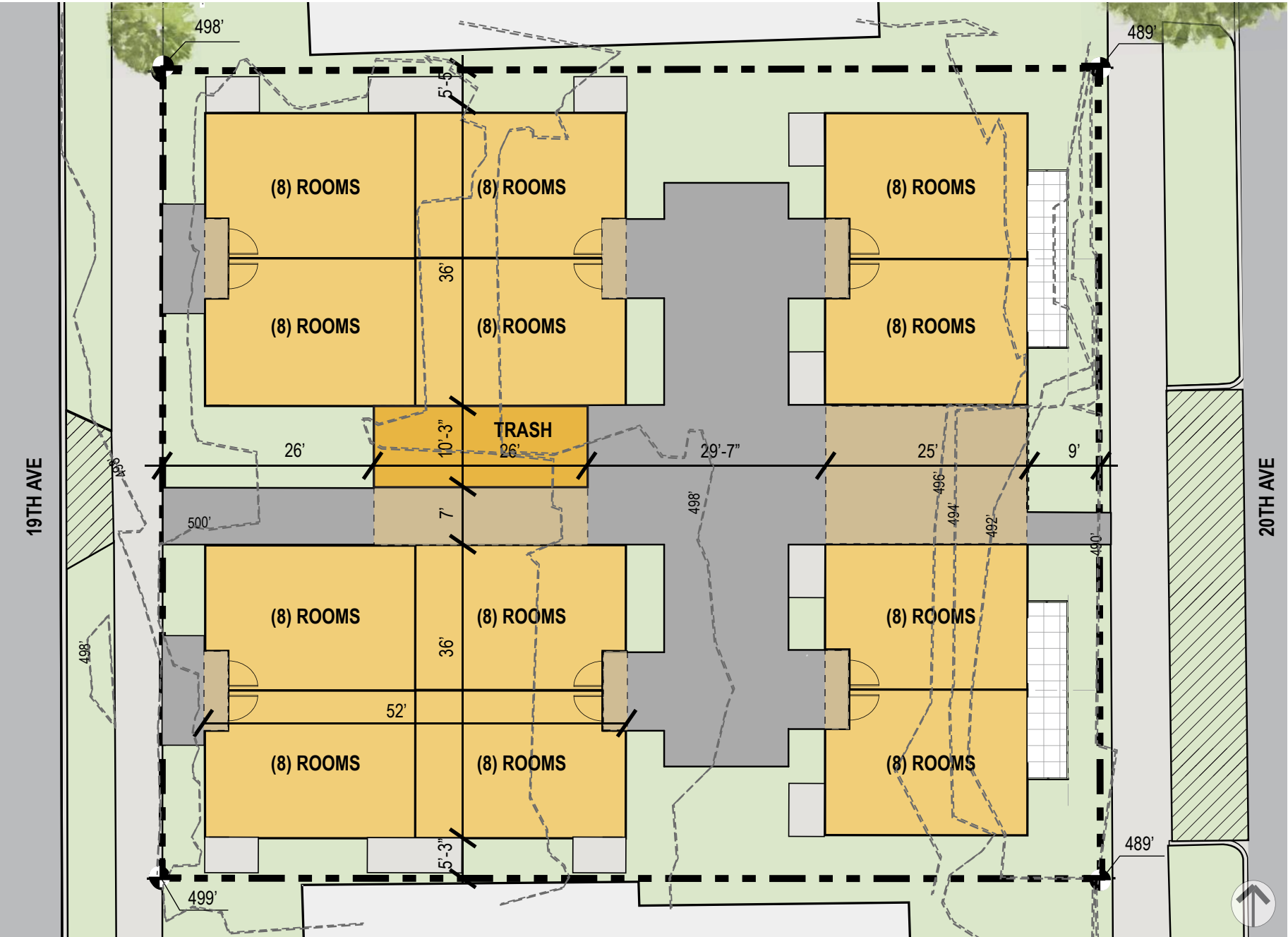
Provided: Ground level - 3,271 sf
Roof deck - 1,886 sf
Total : 5,157 sf (exceeds code requirement by 78%)

SITE PLAN

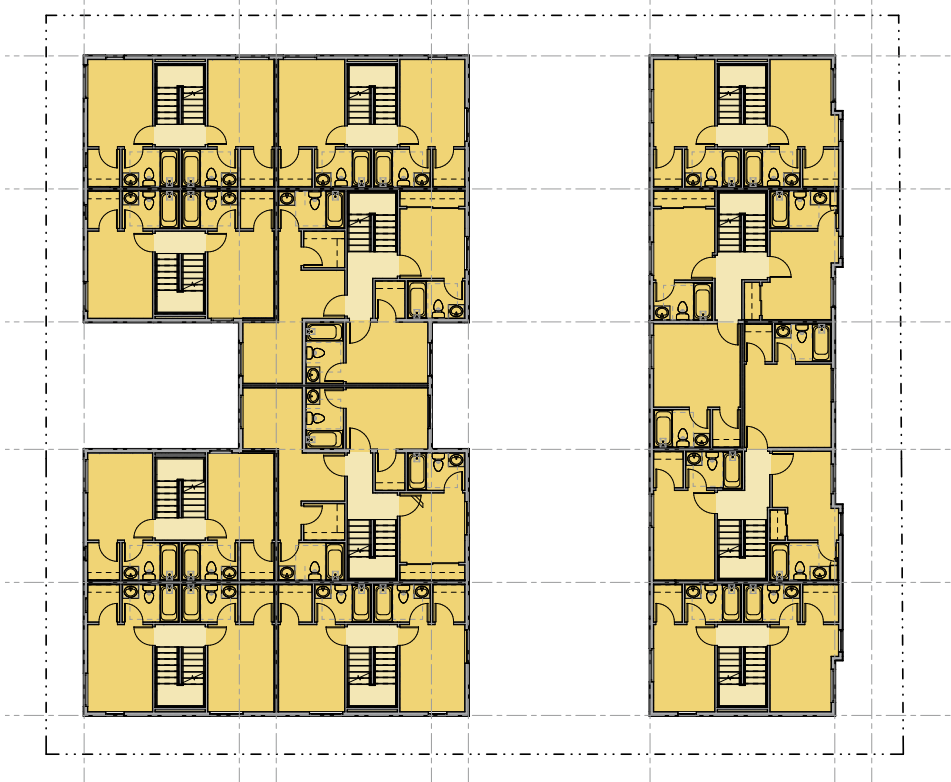
UW 19th Ave, LLC
4726 + 4730 19th Avenue NE Seattle, WA 98105

DEVELOPMENT OBJECTIVES (DPD# 3031329)

Number of residential units: 12 Units
Total number of rooms: 96 Rooms



L1 PLAN (GROUND LEVEL)



LEVEL 2 & 3 FLOOR PLAN



ROOF PLAN



BASEMENT FLOOR PLAN



L1 PLAN (GROUND LEVEL)



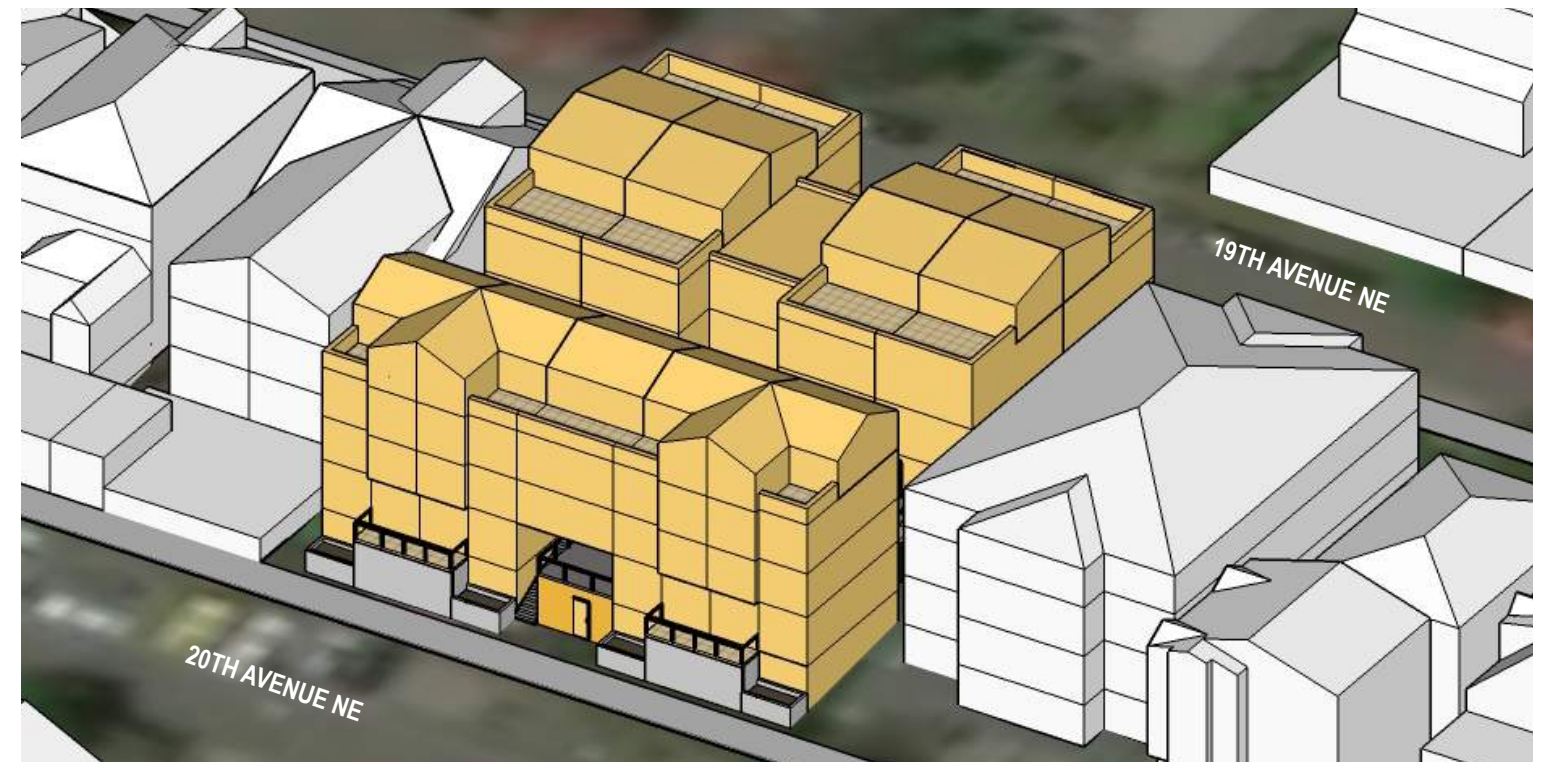
VIEW FROM 19TH AVENUE NE - VIEW FROM NORTH LOOKING EAST



VIEW FROM 20TH AVENUE NE - VIEW FROM SOUTH LOOKING WEST



VIEW LOOKING SOUTHWEST



VIEW LOOKING NORTHEAST

EDG #2 ARCHITECTURAL CONCEPTS : OPTION B.1 - DESIGN DEVELOPMENT CONCEPT



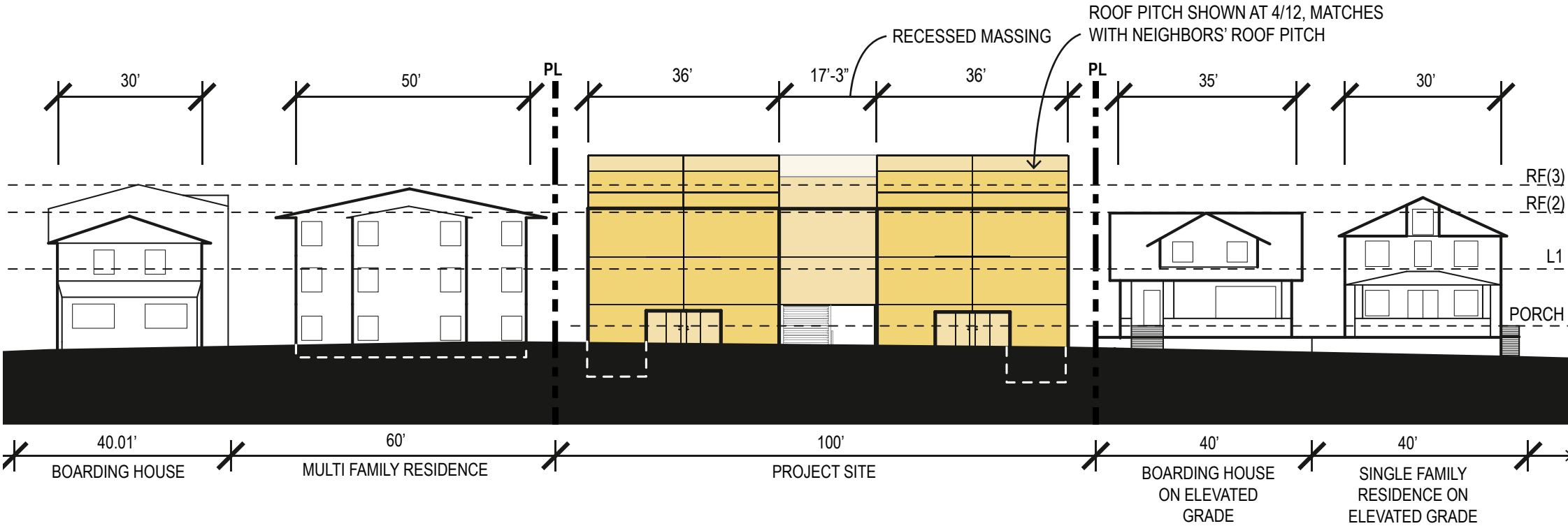
OPTION B.1 - CONCEPT (19TH AVE NW)



OPTION B.1 - CONCEPT (19TH AVE SW)

OPTION B.1: MASSING RESPONSE - 19TH AVENUE

Option B.1 takes its design cues from the adjacent streetscape by breaking up the facade widths into two major massing elements to better match adjacent building facade widths. The upper level roof is stepped back at the top floor to match existing streetscape roof heights. Paired residential entries facing the street pay homage to the existing street pattern while taking into account the project's requirement to meet ADA accessibility requirements for unit access.



19TH AVE STREET ELEVATION

EDG #2 ARCHITECTURAL CONCEPTS : OPTION B.1 - DESIGN DEVELOPMENT CONCEPT



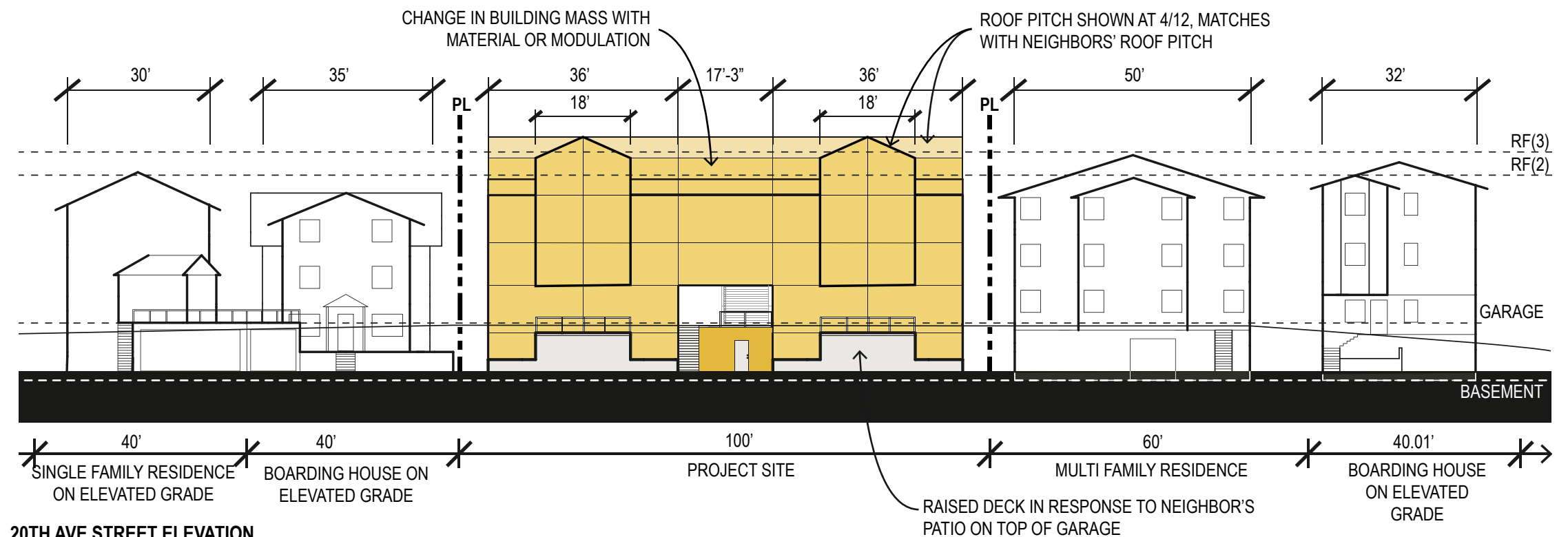
OPTION B.1 - CONCEPT (20TH AVE SE)



OPTION B.1 - CONCEPT (20TH AVE NE)

OPTION B.1: MASSING RESPONSE - 20TH AVENUE

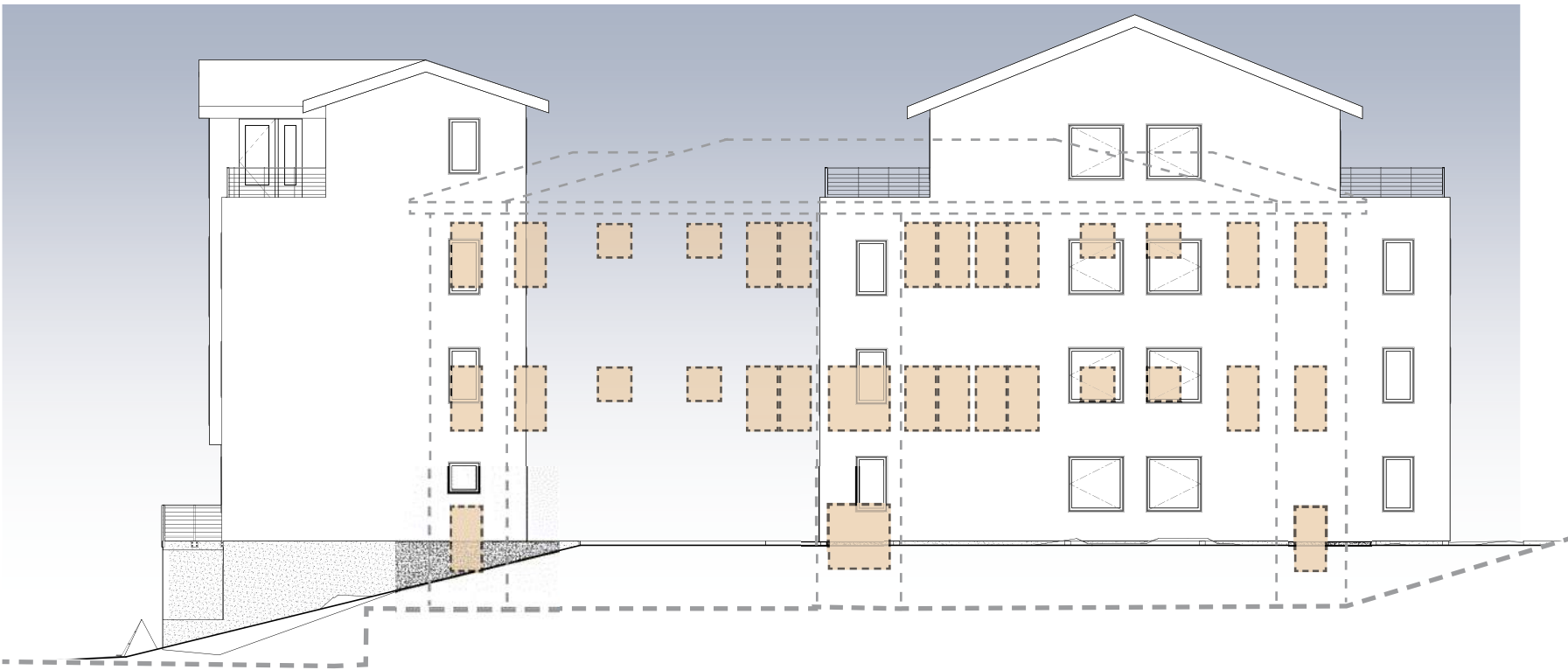
Option B.1 takes similar design cues from the adjacent streetscape by keeping the main project level flush to 19th Avenue, creating a raised porch / patio condition along 20th Avenue to match the existing terrace garage pattern. Sloped roofs have been added to match existing roof pitches and eave lines along 20th Avenue. The upper level facade has bay windows with sloped roofs to match existing roof pitches and eave lines. The project's bike storage room has ground level access from 20th Avenue to connect to the multi-modal pathways in the street.



20TH AVE STREET ELEVATION



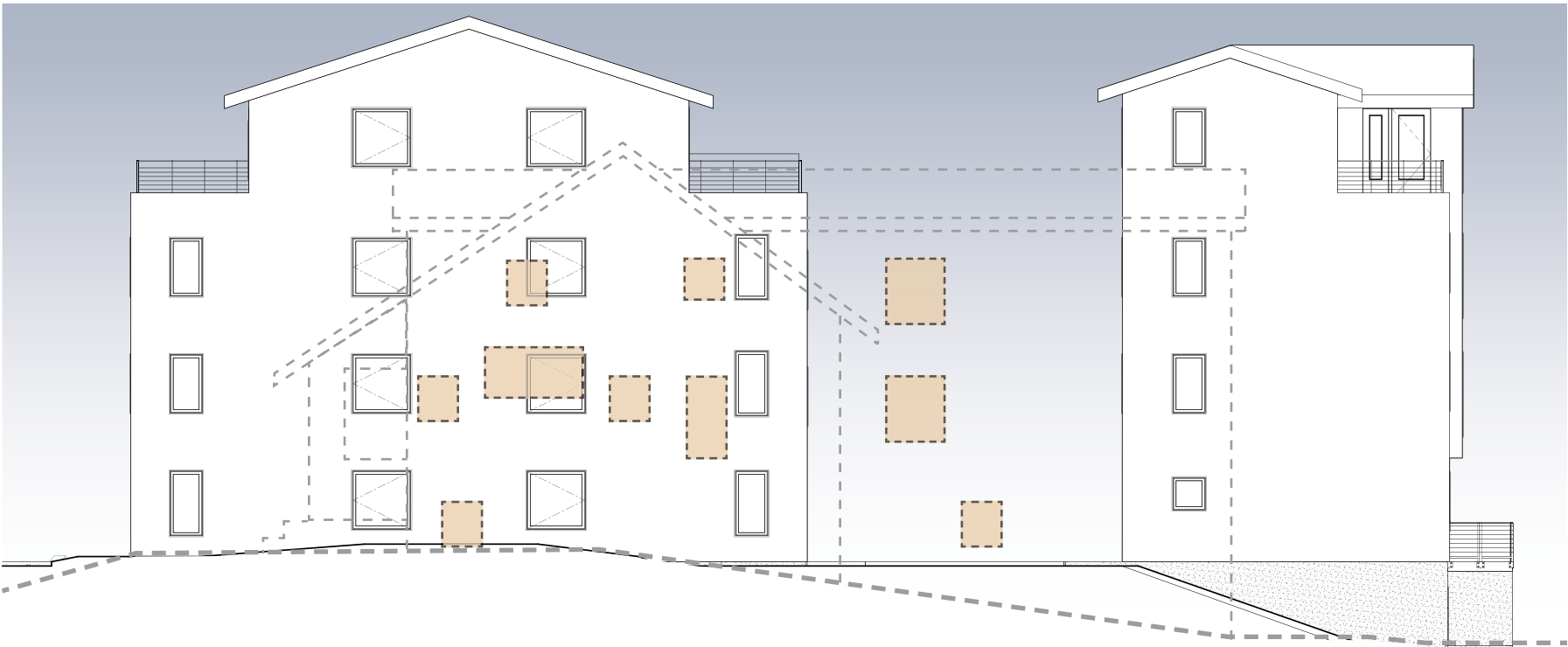
ADJACENT PROPERTY TO THE NORTH



NORTH ELEVATION: WINDOW LOCATION OVERLAY WITH ADJACENT STRUCTURE

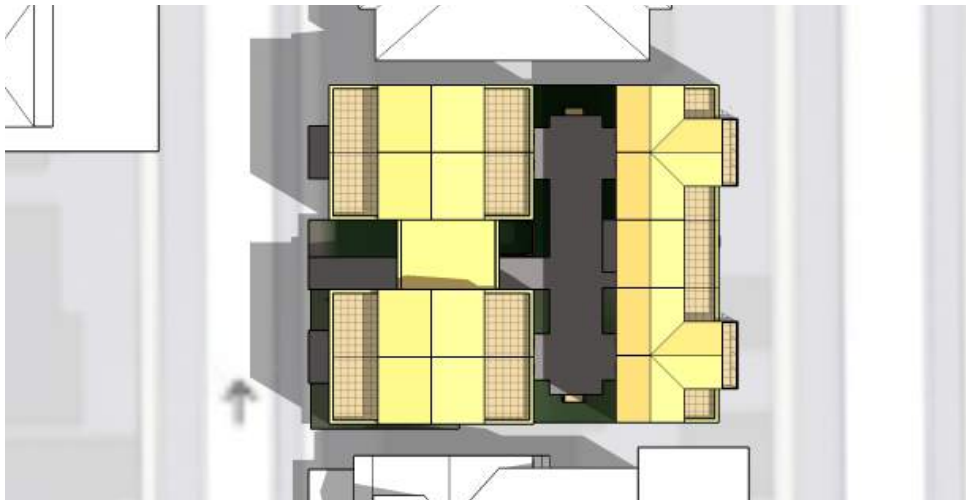


ADJACENT PROPERTY TO THE SOUTH

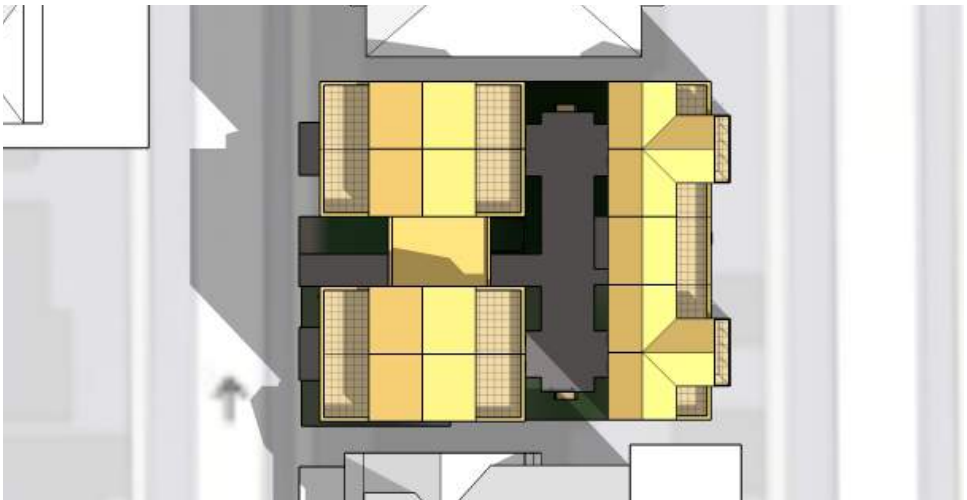


SOUTH ELEVATION: WINDOW LOCATION OVERLAY WITH ADJACENT STRUCTURE

EDG #2 ARCHITECTURAL CONCEPTS : OPTION B.1 - SUN & SHADOWS STUDY



10AM - SUMMER SOLSTICE



10AM - EQUINOX



10AM - WINTER SOLSTICE



NOON - SUMMER SOLSTICE



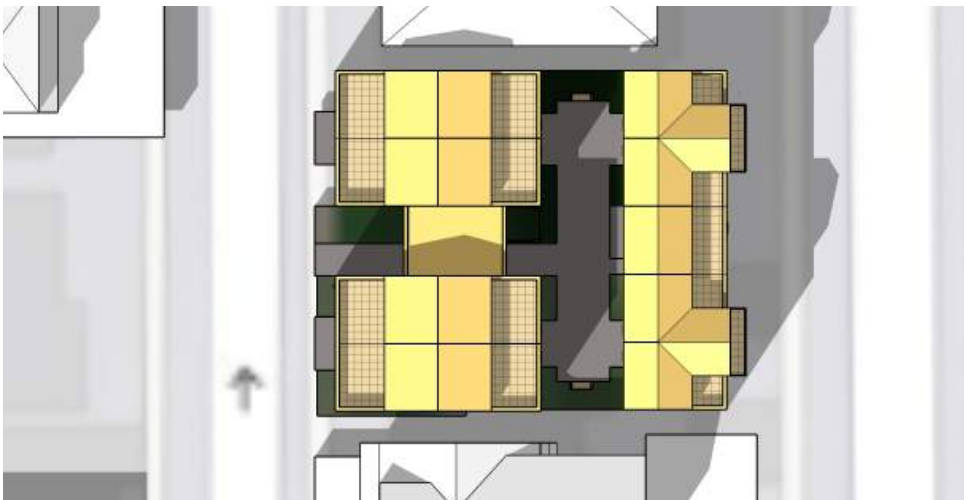
NOON - EQUINOX



NOON - WINTER SOLSTICE



2PM - SUMMER SOLSTICE

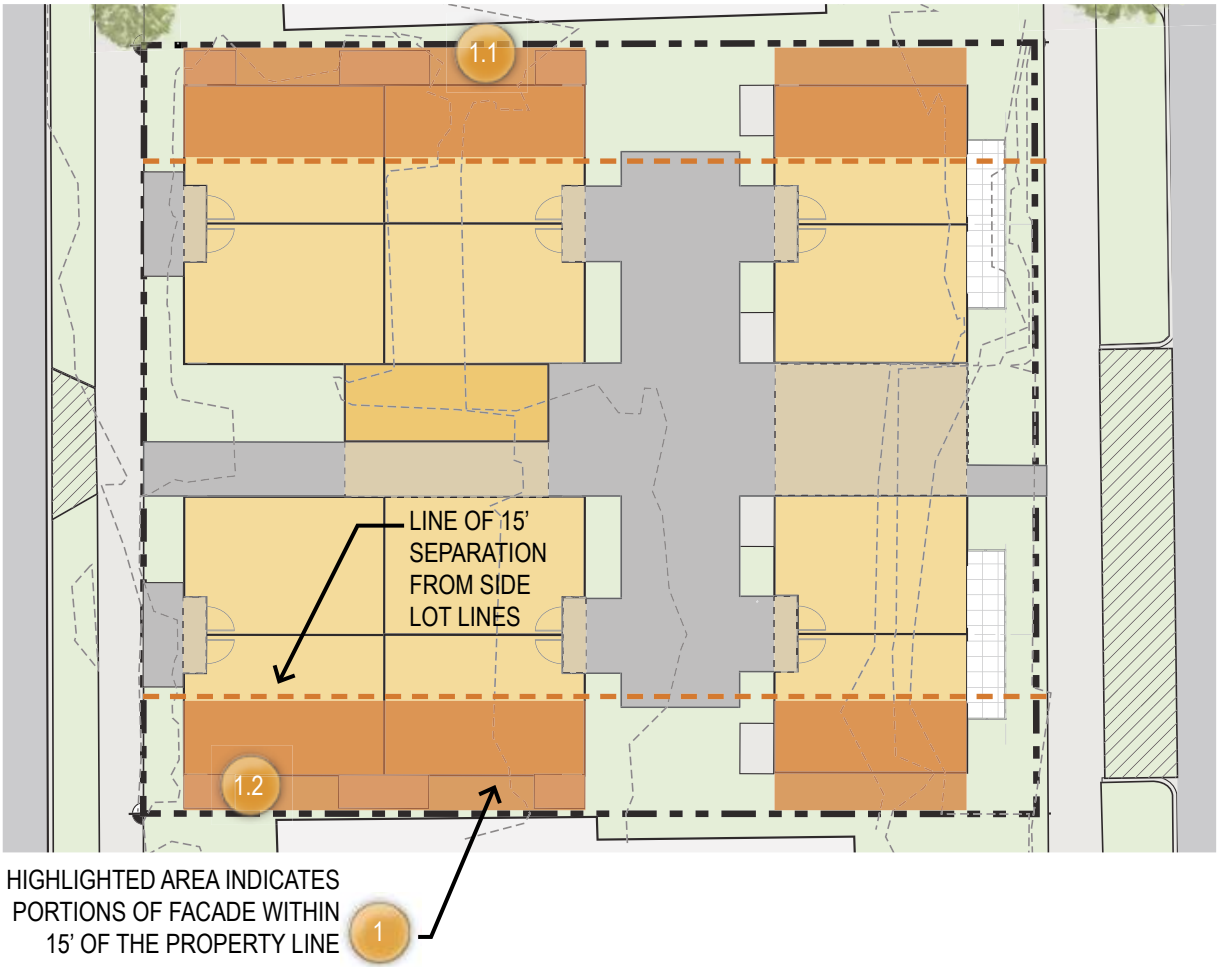


2PM - EQUINOX

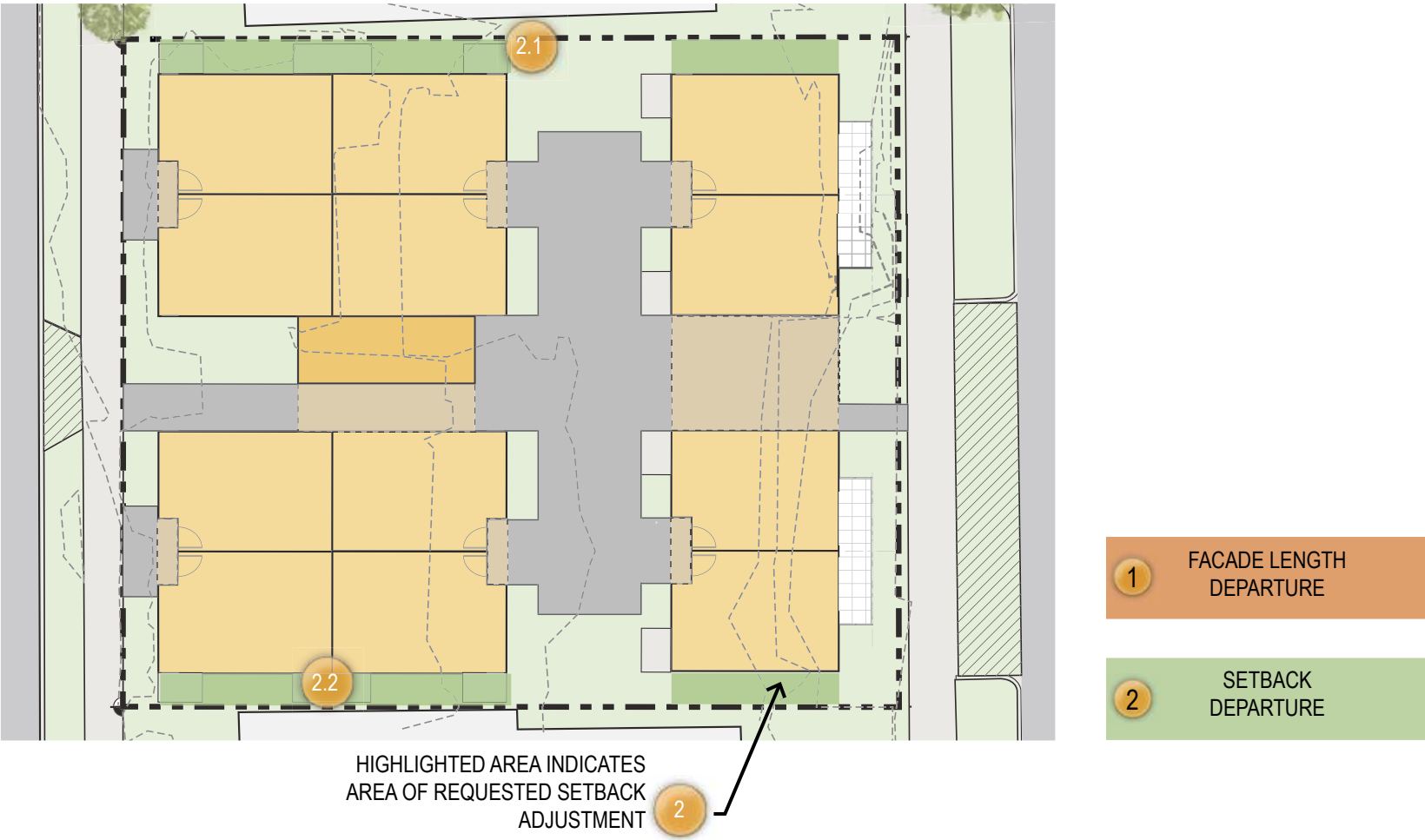


2PM - WINTER SOLSTICE

FACADE LENGTH DIAGRAM



SETBACK DEPARTURE DIAGRAM



DEPARTURE REQUESTS ANTICIPATED

DEPARTURE NUMBER	LAND USE CODE SECTION	CODE REQUIREMENT	DEPARTURE REQUIRED	DESIGN RATIONALE
1	23.45.527.B.1	The maximum combined length of all portions of façades within 15 feet of a lot line that is neither a rear lot line nor a street or alley lot line shall not exceed 65 percent of the length of that lot line. (115.38' x 65%= 74.9' allowed on the North lot line and 116.01' x 65%= 75.41 allowed on the South lot line)	Project proposes 102.25' facade length on both side lot lines. 1.1: North facade length proposed = 77.5' (max 65% allowed, 3.5% increase in allowable facade length proposed) 1.2: South facade length proposed = 77.5' (max 65% allowed, 2.7% increase in allowable facade length proposed)	Project utilizes central site plan concept to optimize resident open space for quality, usefulness, daylighting and security per DRB comments at EDG #1. Design allows for continuous open space from north to south boundary, increasing daylight and reducing privacy impacts on adjacent buildings to north and south of project site. Departure requested for minimal relief, with proposed buildings not encroaching into minimum side yard setbacks.
2	23.45.518 Table A	Required side setbacks for the LR zones: Townhouse developments: 5' minimum, 7' average required	Project proposes a setback minimum of 5' along both side setbacks. 2.1: North facade side setback average proposed = 5' (min 7' average required, reduced by 28.6%) 2.2: South facade side setback average proposed = 5' (min 7' average required, reduced by 28.6%)	

PLANT SCHEDULE

TREES

BOTANICAL / COMMON NAME

- ACER CIRCINATUM
VINE MAPLE
- ACER GRISEUM
PAPERBARK MAPLE
APPROVED BY
LANDSCAPE ARCHITECT
- GLEDITSIA TRIACANTHOS 'DRAVES'
HONEY LOCUST
- GYMNOCLADUS DIOICA
KENTUCKY COFFEE TREE
- KOELREUTERIA PANICULATA 'JFS--SUNLEAF'
SUMMERBURST GOLDENRAIN TREE
- NYSSA SYLVATICA 'BLACK TUPELO'
SOUR GUM

SHRUBS

BOTANICAL / COMMON NAME

- AZALEA KURUME HYBRID 'CORAL BELLS'
KURUME AZALEA
- AZALEA X 'EVEREST'
EVEREST AZALEA
- CLEMATIS ARMANDII
EVERGREEN CLEMATIS
- FRAGARIA CHILOENSIS
COAST STRAWBERRY
- HELIOTRICHON SEMPERVIRENS
BLUE OAT GRASS
- HYDRANGEA QUERCIFOLIA 'SNOW QUEEN'
SNOW QUEEN OAKLEAF HYDRANGEA
- LAVANDULA STOECHAS
SPANISH LAVENDER
- LIRIOPE SPICATA 'BIG BLUE'
CREEPING LILY TURF
- LONICERA JAPONICA 'HALL'S PROLIFIC'
HALL'S PROLIFIC HONEYSUCKLE
- NANDINA DOMESTICA
HEAVENLY BAMBOO
- PHORMIUM TENAX 'JACK SPRATT'
NEW ZEALAND FLAX
- POLYSTICHUM MUNITUM
WESTERN SWORD FERN
- RIBES SANGUINEUM
RED FLOWERING CURRANT
- SKIMMIA JAPONICA
SKIMMIA
- SPIRAEA JAPONICA 'MAGIC CARPET'
MAGIC CARPET SPIREA
- TAXUS X MEDIA 'DENSIFORMIS'
DENSE YEW
- VIBURNUM DAVIDII
DAVID VIBURNUM
- VIBURNUM PLICATUM TOMENTOSUM
DOUBLEFILE VIBURNUM

19TH AVE NE



20TH AVE NE

PLANT & AMENITY CHARACTER IMAGES - OPTION B.1



KURUME AZALEA



EVEREST AZALEA



EVERGREEN CLEMATIS



COAST STRAWBERRY



DOUBLEFILE VIBURNUM



VINE MAPLE



PAPERBARK MAPLE



HONEY LOCUST



KENTUCKY COFFEE
TREE



SUMMERBURST GOLD-
ENRAIN TREE



BLACK TUPELO



BLUE OAT GRASS



SNOW QUEEN OAKLEAF HYDRAN-
GEA



SPANISH LAVENDER



CREeping LILY TURF



DAVID VIBURNUM



HALL'S PROLIFIC HONEY-
SUCKLE



HEAVENLY BAMBOO



WESTERN SWORD FERN



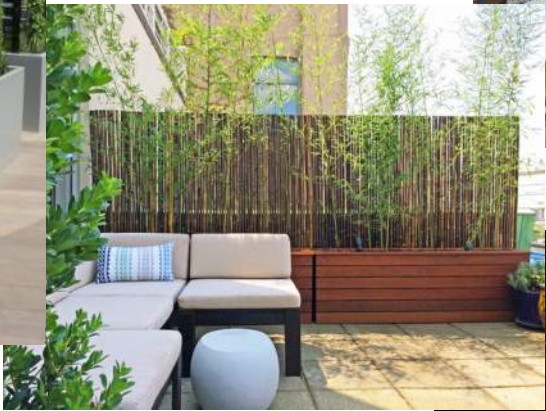
RED FLOWERING CUR-
RANT



MAGIC CARPET SPIREA



SKIMMIA



OPTION D

PROS:

- Largest common open space of all new options
- Combines front porch of units facing 19th Avenue to better replicate scale of porches in context
- Uses grade change to develop 20th Avenue frontage for bikes / integrate into pattern of street use
- Bike storage room / accessible lift located at 20th Avenue grade in interior building w/ access to 19th Avenue street level
- Locates solid waste storage to interior of site

CONS:

- Retains bulky massing along south PL (departure required)
- Adds bulky massing along north PL (additional substantial departure required)
- Limited to no rooftop unit decks due to common open space

ZONING CALCS

Refuse / Recycling:	Unit # (14) / 10 = cubic yards of each
Food Waste Collection:	5 - 50 Unit building = (1) 96 gallon cart
Enclosure:	9 - 15 Dwelling units provide min. 150 sf = 150 sf storage area required

Provided:

Recycle =	(2) 2 cu. yd container (min 3.5 cu. yd required per SPU)
Refuse =	(1) 2 cu. yd container (min 3.5 cu. yd required per SPU)
Food Waste =	(2) 96 gallon cart (min 2 containers required per SPU)
Enclosure =	195.5 sf

Bike Parking: Residential (Multi-family) = 1 space / dwelling unit
= 14 total required

Provided: 55 spaces provided at Bike Storage off 20th Ave (exceeds code requirement by 292%)

Amenity Area:	Minimum 25% of lot area	=	2,890.5 sf
	Minimum at grade	=	1,450 sf
	(50% of required amenity area)		

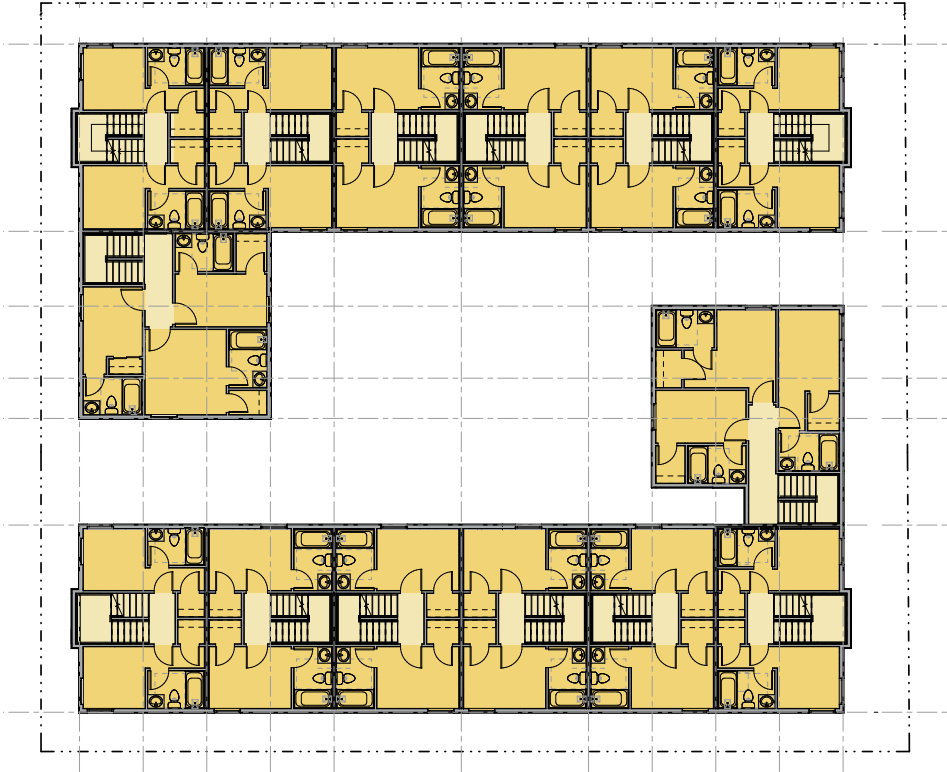
Provided:	Ground level -	2,891 sf
	Total :	2,891 sf

DEVELOPMENT OBJECTIVES (DPD# 3031329)

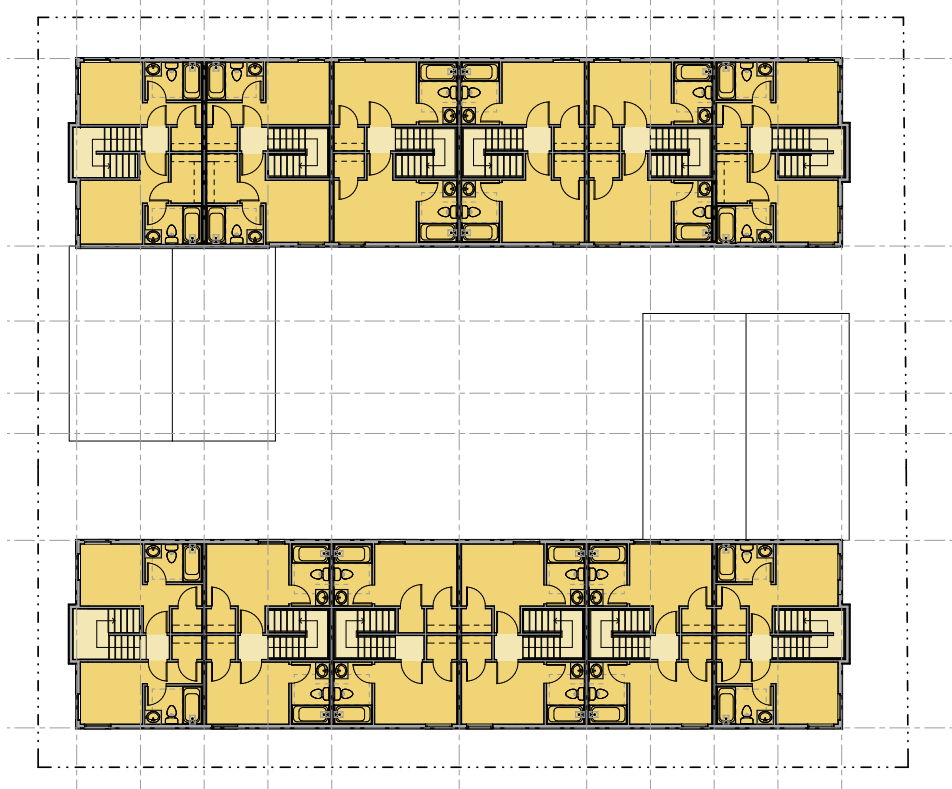
Number of residential units: 14 Units
Total number of rooms: 108 Rooms



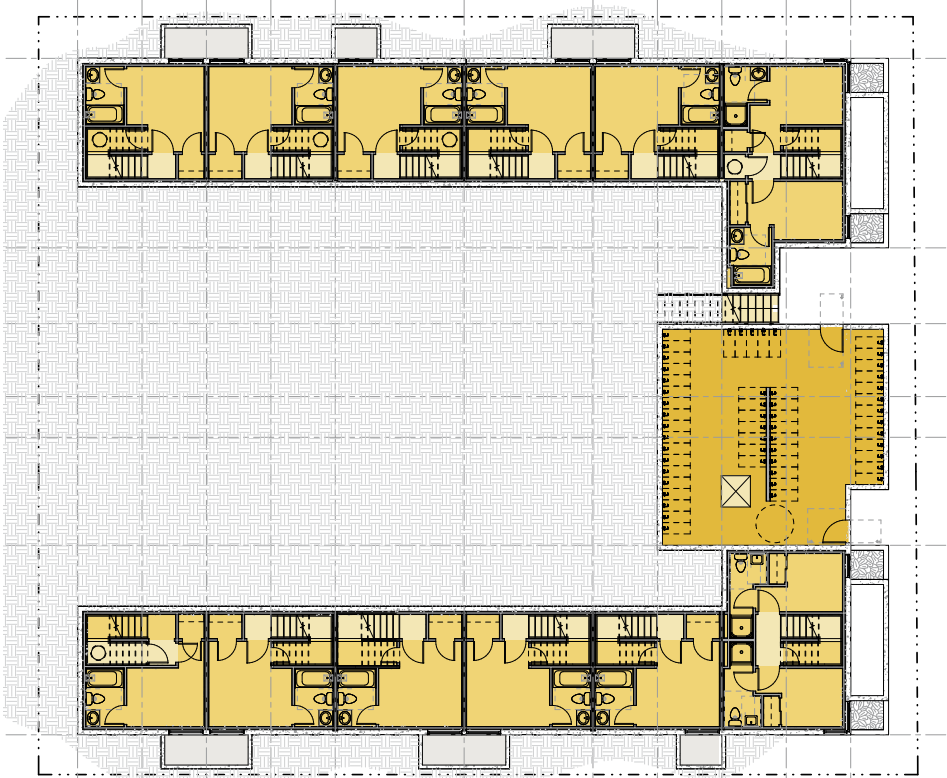
L1 PLAN (GROUND LEVEL)



LEVEL 2 & 3 FLOOR PLAN



LEVEL 4 FLOOR PLAN



BASEMENT FLOOR PLAN



L1 PLAN (GROUND LEVEL)



VIEW FROM 19TH AVENUE NE - VIEW FROM NORTH LOOKING EAST



VIEW FROM 20TH AVENUE NE - VIEW FROM SOUTH LOOKING WEST



VIEW LOOKING SOUTHWEST



VIEW LOOKING NORTHEAST

EDG #2 ARCHITECTURAL CONCEPTS : OPTION D - DESIGN DEVELOPMENT CONCEPT



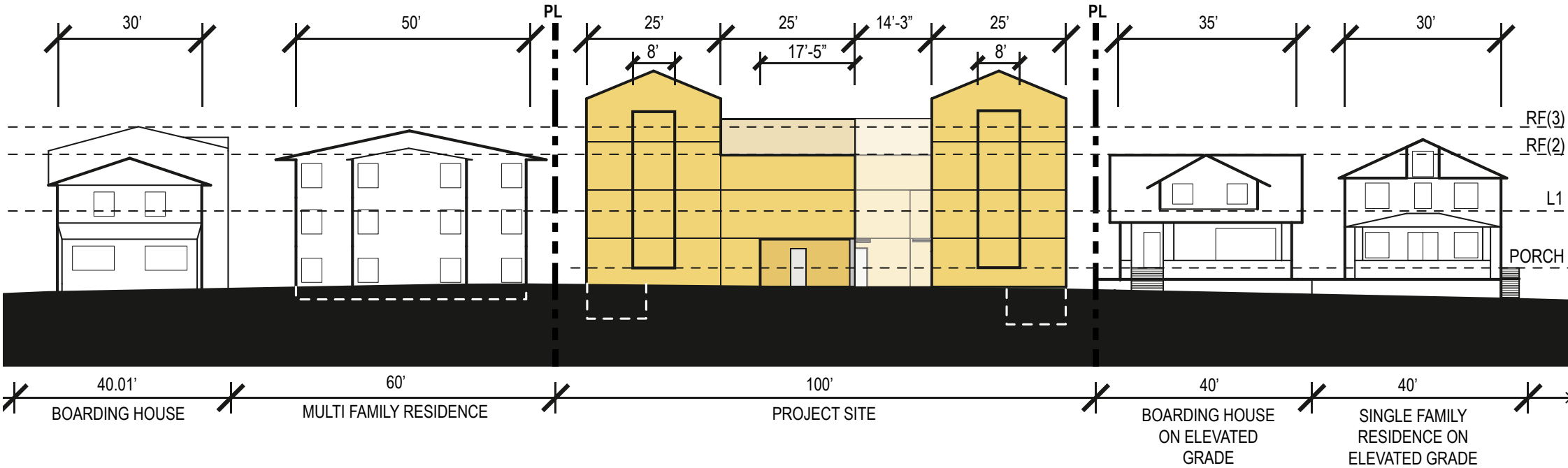
OPTION D - CONCEPT (19TH AVE VIEW FROM NW)



OPTION D - CONCEPT (19TH AVE VIEW FROM SW)

OPTION D: MASSING RESPONSE - 19TH AVENUE

Option D takes its design cues from the adjacent streetscape by breaking up the project massing into two distinct structures, each with major street-facing modulation widths approximating the width of existing single family structures in the neighborhood. Sloped roofs have been added to match existing roof pitches and eave lines along 19th Avenue, with bay windows facing the street capped in height to reflect the existing roofscape scale. A common site entry and large unit front porch face 19th Avenue, to respect the existing street pattern while taking into account the project's requirement to meet ADA accessibility requirements for unit access. Additional story height at the top floor is proposed to offset bedroom loss at courtyard at high activity area. No interior basement bedrooms / area wells proposed this option as a result.



19TH AVE STREET ELEVATION

EDG #2 ARCHITECTURAL CONCEPTS : OPTION D - DESIGN DEVELOPMENT CONCEPT



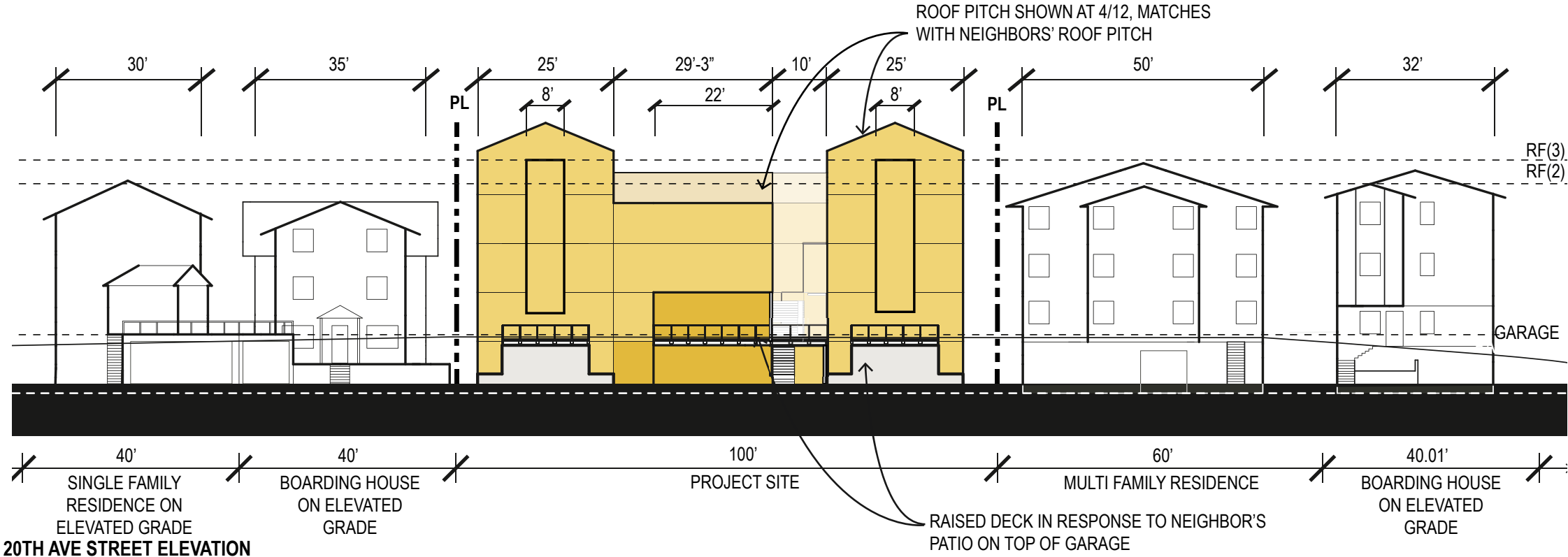
OPTION D - CONCEPT (20TH AVE VIEW FROM NE)



OPTION D - CONCEPT (20TH AVE VIEW FROM SE)

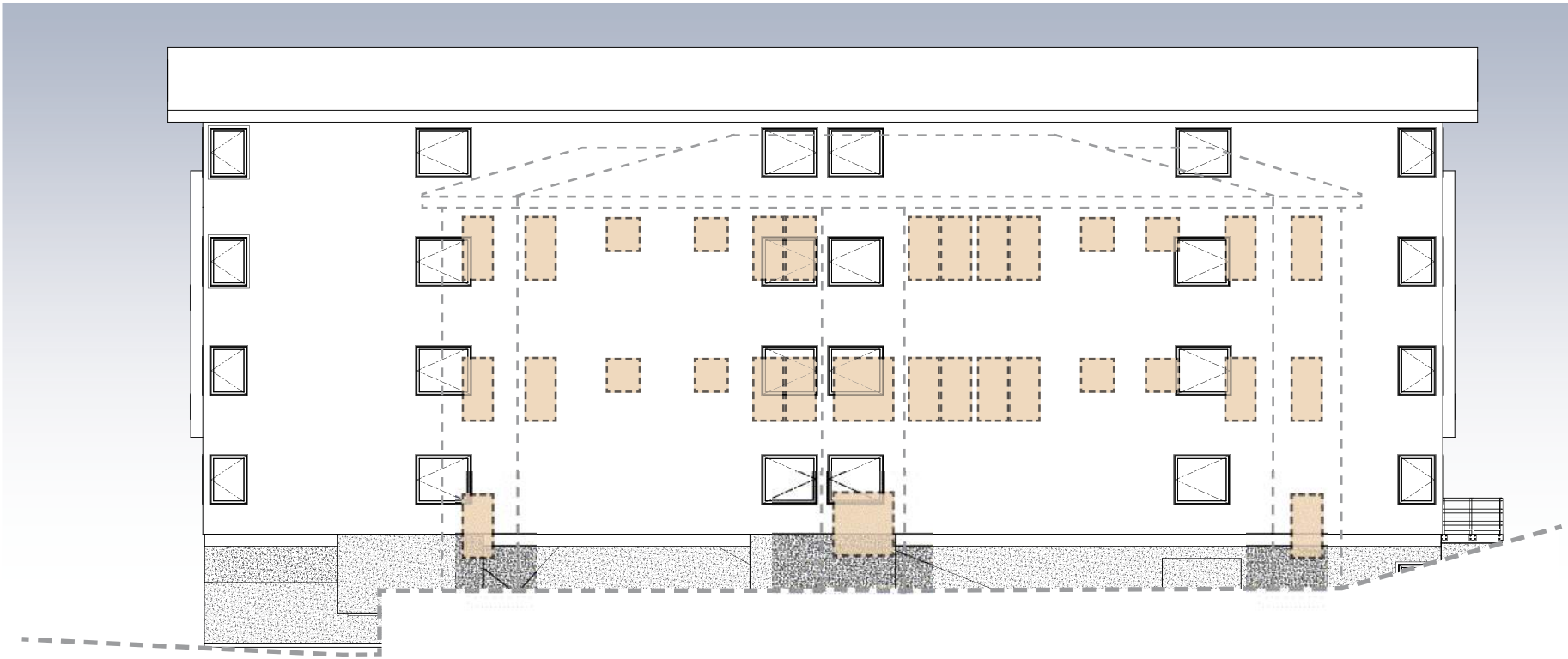
OPTION D: MASSING RESPONSE - 20TH AVENUE

Option D takes similar design cues from the adjacent streetscape by keeping the main project level flush to 19th Avenue, creating a raised porch / patio condition along 20th Avenue to match the existing terrace garage pattern. The project's bike storage room has ground level access from 20th Avenue to connect to the multi-modal pathways in the street.





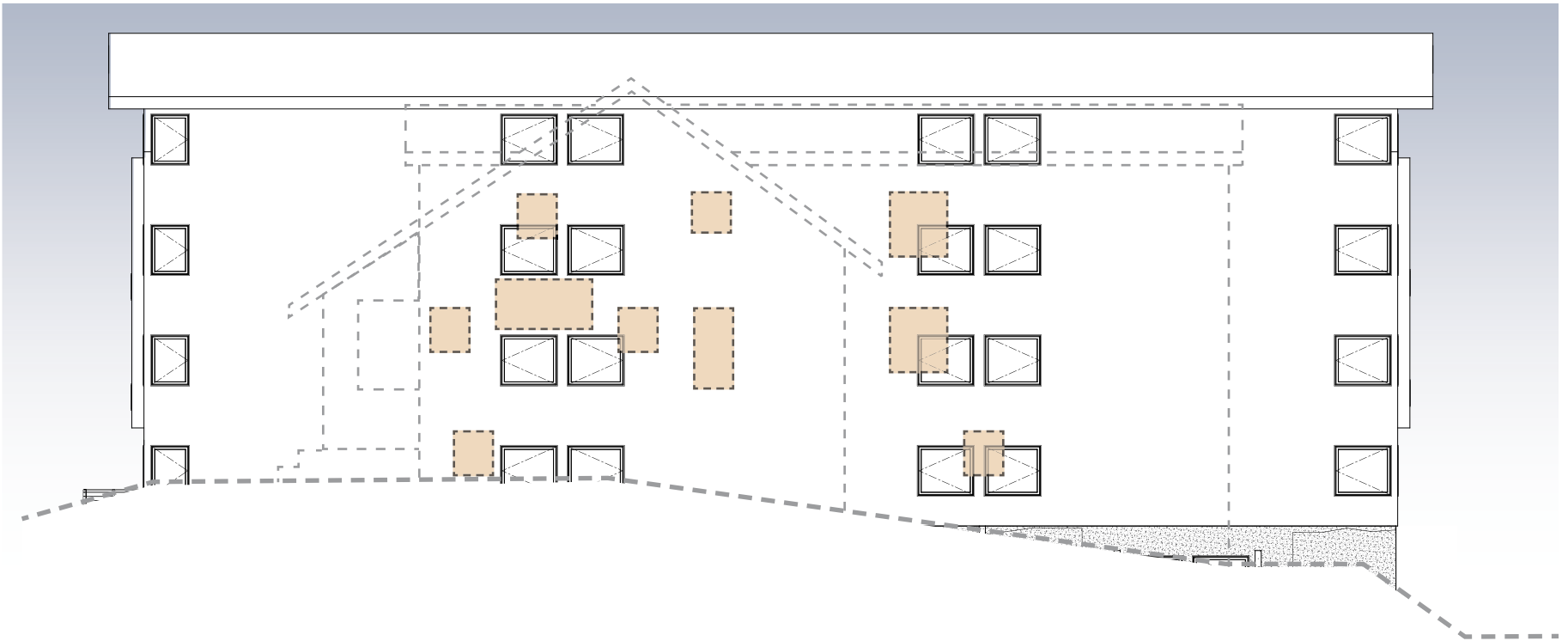
ADJACENT PROPERTY TO THE NORTH



NORTH ELEVATION: WINDOW LOCATION OVERLAY WITH ADJACENT STRUCTURE

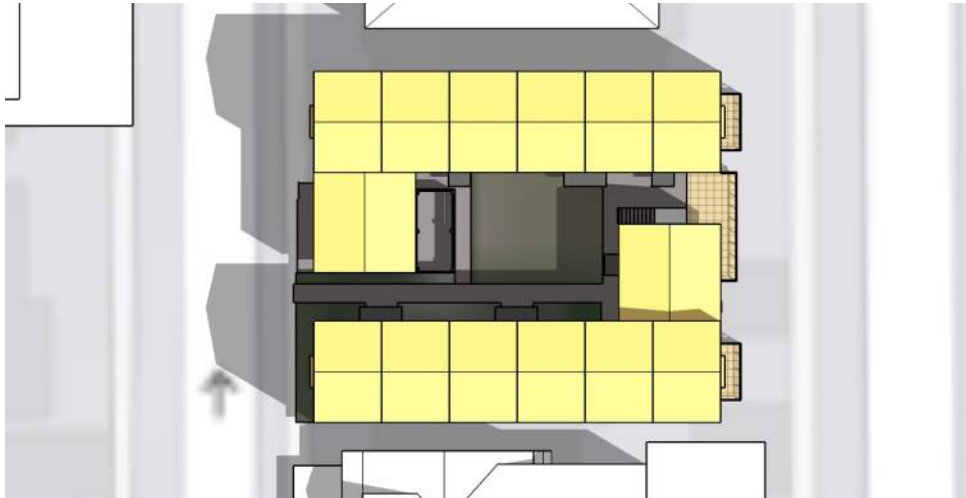


ADJACENT PROPERTY TO THE SOUTH



SOUTH ELEVATION: WINDOW LOCATION OVERLAY WITH ADJACENT STRUCTURE

EDG #2 ARCHITECTURAL CONCEPTS : OPTION D - SUN & SHADOWS STUDY



10AM - SUMMER SOLSTICE



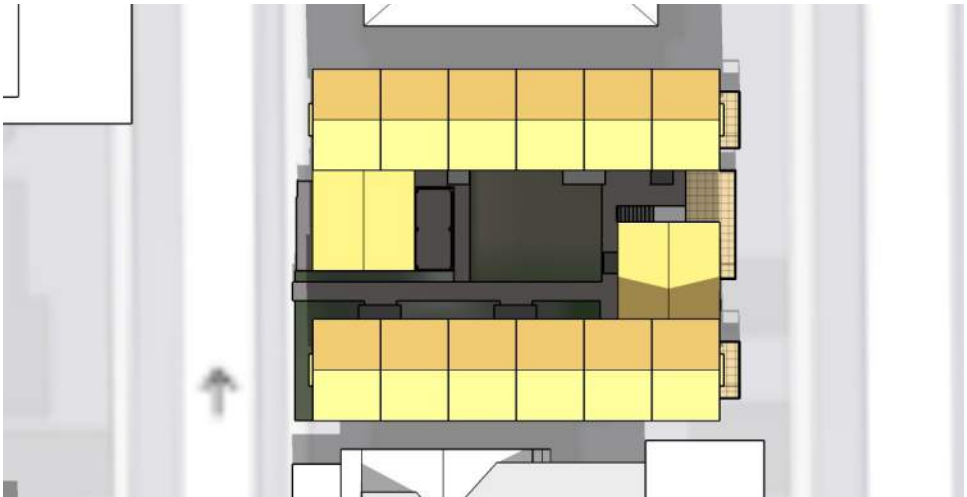
10AM - EQUINOX



10AM - WINTER SOLSTICE



NOON - SUMMER SOLSTICE



NOON - EQUINOX



NOON - WINTER SOLSTICE



2PM - SUMMER SOLSTICE

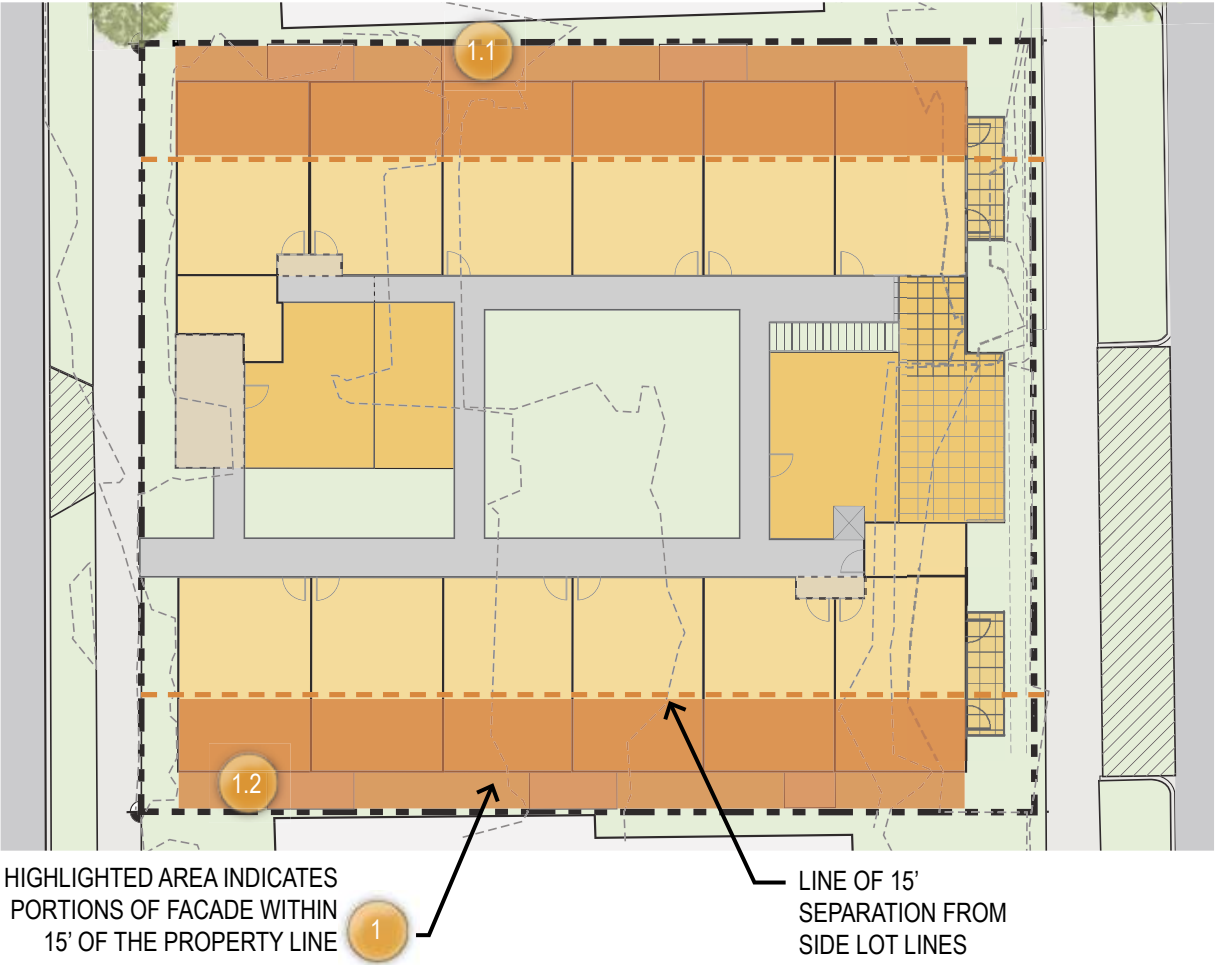


2PM - EQUINOX

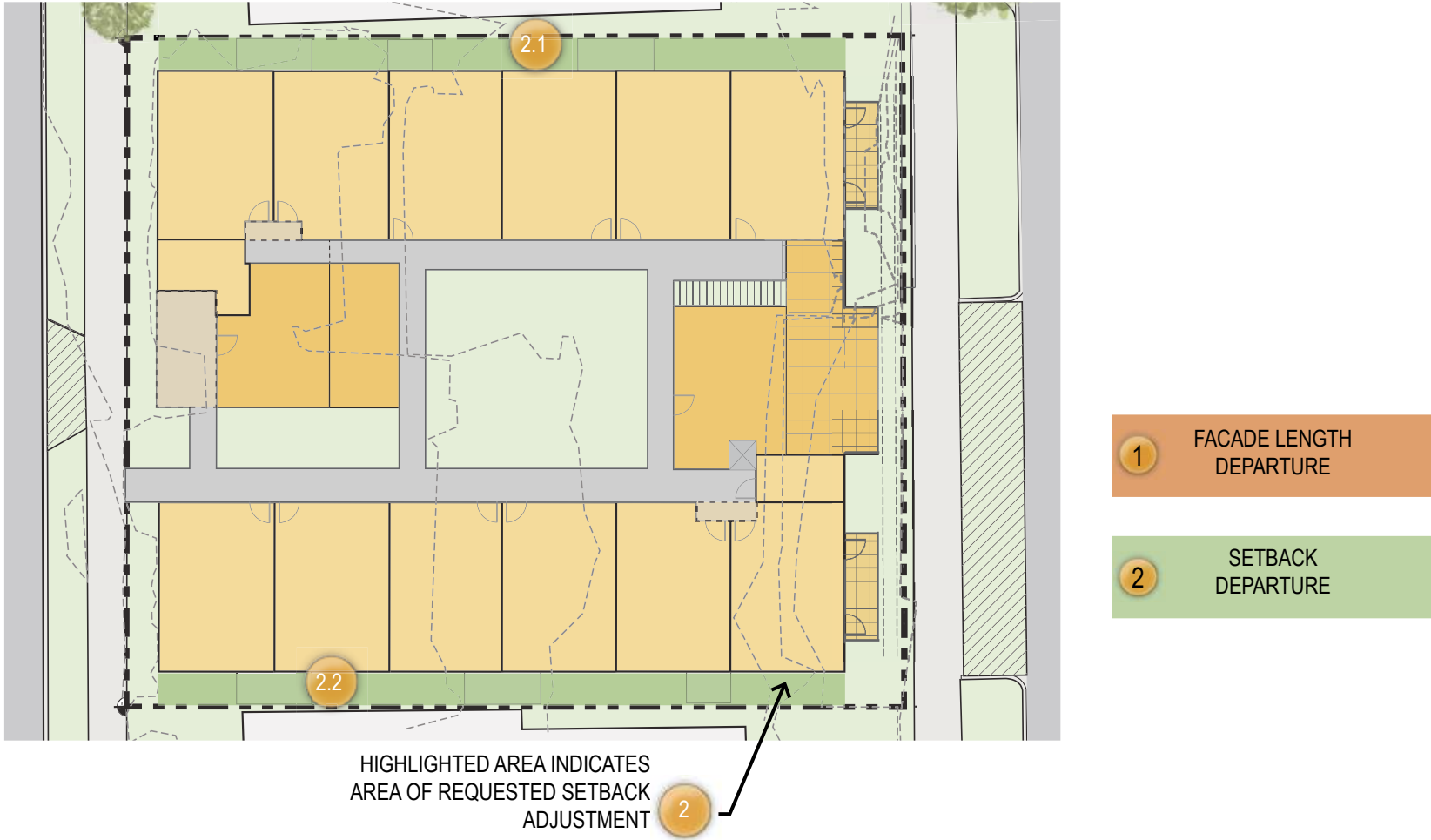


2PM - WINTER SOLSTICE

FACADE LENGTH AND AMENITY AREA DIAGRAM



SETBACK DEPARTURE DIAGRAM



DEPARTURE REQUESTS ANTICIPATED

DEPARTURE NUMBER	LAND USE CODE SECTION	CODE REQUIREMENT	DEPARTURE REQUIRED	DESIGN RATIONALE
1	23.45.527.B.1	The maximum combined length of all portions of façades within 15 feet of a lot line that is neither a rear lot line nor a street or alley lot line shall not exceed 65 percent of the length of that lot line. (115.38' x 65%= 74.9' allowed on the North lot line and 116.01' x 65%= 75.41 allowed on the South lot line)	Project proposes 102.25' facade length on both side lot lines. 1.1: North facade length proposed = 102.25' (max 65% allowed, 36.5% increase in allowable facade length proposed) 1.2: South facade length proposed = 102.25' (max 65% allowed, 35.5% of increase in allowable facade length proposed)	Project utilizes central site plan concept to optimize resident open space for quality, usefulness, daylighting and security per DRB comments at EDG #1. Massing as proposed allows for better modulation for front porches, etc in response to neighborhood patterns per site analysis.
2	23.45.518 Table A	Required side setbacks for the LR zones: Townhouse developments: 5' minimum, 7' average required	Project proposes a setback minimum of 5' along both side setbacks. 2.1: North facade side setback average proposed = 5' (min 7' average required, reduced by 28.6%) 2.2: South facade side setback average proposed = 5' (min 7' average required, reduced by 28.6%)	

PLANT SCHEDULE

TREES

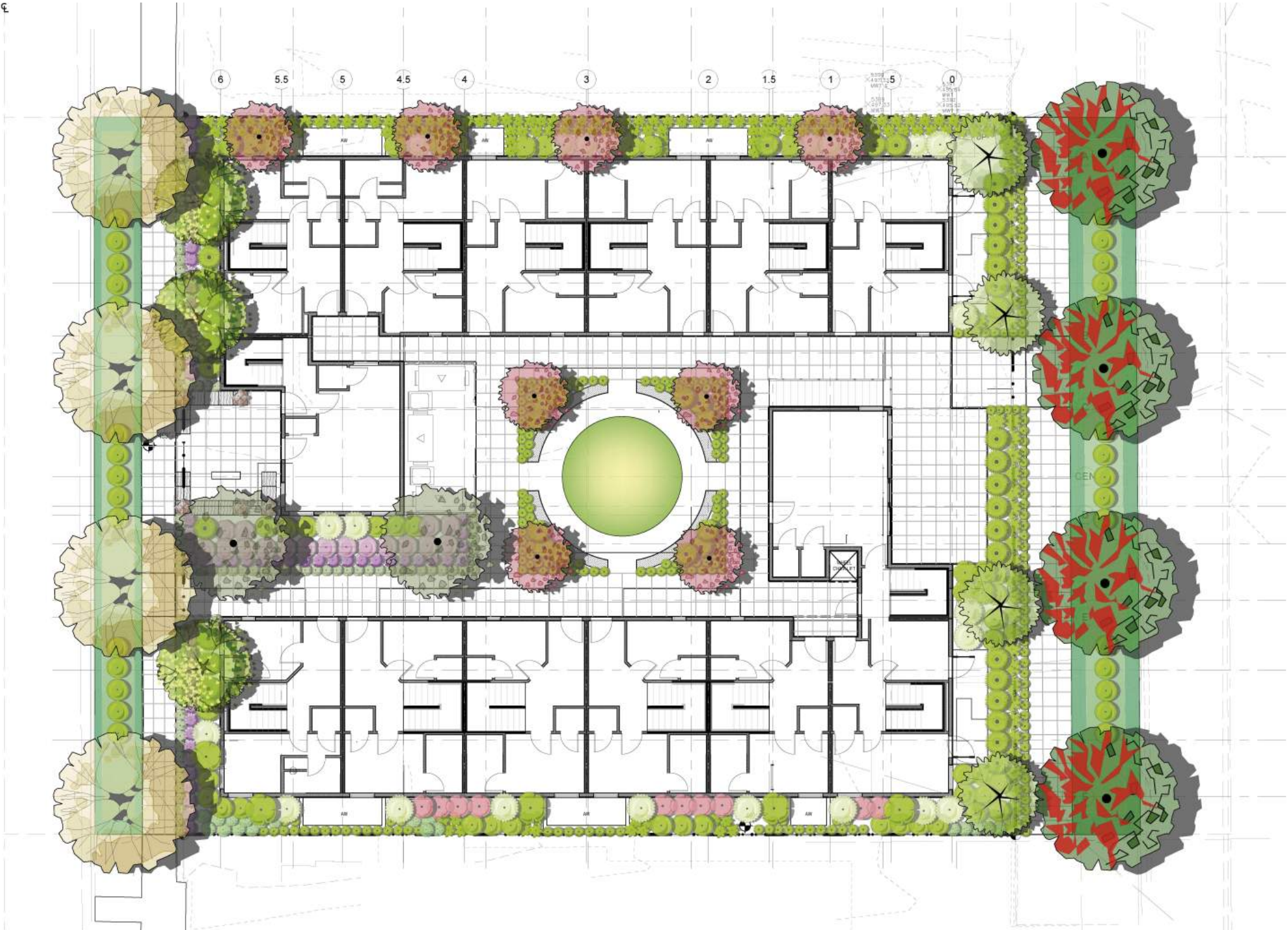
BOTANICAL / COMMON NAME

- ACER CIRCINATUM
VINE MAPLE
- ACER GRISEUM
PAPERBARK MAPLE
APPROVED BY
LANDSCAPE ARCHITECT
- GLEDITSIA TRIACANTHOS 'DRAVES'
HONEY LOCUST
- GYMNOCLADUS DIOICA
KENTUCKY COFFEE TREE
- KOELREUTERIA PANICULATA 'JFS-SUNLEAF'
SUMMERBURST GOLDENRAIN TREE
- NYSSA SYLVATICA 'BLACK TUPELO'
SOUR GUM

SHRUBS

BOTANICAL / COMMON NAME

- AZALEA KURUME HYBRID 'CORAL BELLS'
KURUME AZALEA
- CLEMATIS ARMANDII
EVERGREEN CLEMATIS
- FRAGARIA CHILOENSIS
COAST STRAWBERRY
- HELIOTRICHON SEMPERVIRENS
BLUE OAT GRASS
- HYDRANGEA QUERCIFOLIA 'SNOW QUEEN'
SNOW QUEEN OAKLEAF HYDRANGEA
- LAVANDULA STOECHAS
SPANISH LAVENDER
- LIRIOPE SPICATA 'BIG BLUE'
CREEPING LILY TURF
- LONICERA JAPONICA 'HALL'S PROLIFIC'
HALL'S PROLIFIC HONEYSUCKLE
- NANDINA DOMESTICA
HEAVENLY BAMBOO
- PHORMIUM TENAX 'JACK SPRATT'
NEW ZEALAND FLAX
- POLYSTICHUM MUNITUM
WESTERN SWORD FERN
- RIBES SANGUINEUM
RED FLOWERING CURRANT
- SKIMMIA JAPONICA
SKIMMIA
- SPIRAEA JAPONICA 'MAGIC CARPET'
MAGIC CARPET SPIREA
- TAXUS X MEDIA 'DENSIFORMIS'
DENSE YEW
- VIBURNUM DAVIDII
DAVID VIBURNUM
- VIBURNUM PLICATUM TOMENTOSUM
DOUBLEFILE VIBURNUM



PLANT & AMENITY CHARACTER IMAGES - OPTION D



KURUME AZALEA



EVEREST AZALEA



EVERGREEN CLEMATIS



COAST STRAWBERRY



DOUBLEFILE VIBURNUM



VINE MAPLE



PAPERBARK MAPLE



HONEY LOCUST



KENTUCKY COFFEE
TREE



SUMMERBURST GOLD-
ENRAIN TREE



BLACK TUPELO



BLUE OAT GRASS



SNOW QUEEN OAKLEAF HYDRAN-
GEA



SPANISH LAVENDER



CREeping LILY TURF



DAVID VIBURNUM



HALL'S PROLIFIC HONEY-
SUCKLE



HEAVENLY BAMBOO



WESTERN SWORD FERN



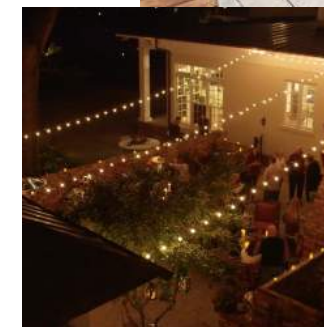
RED FLOWERING CUR-
RANT



MAGIC CARPET SPIREA

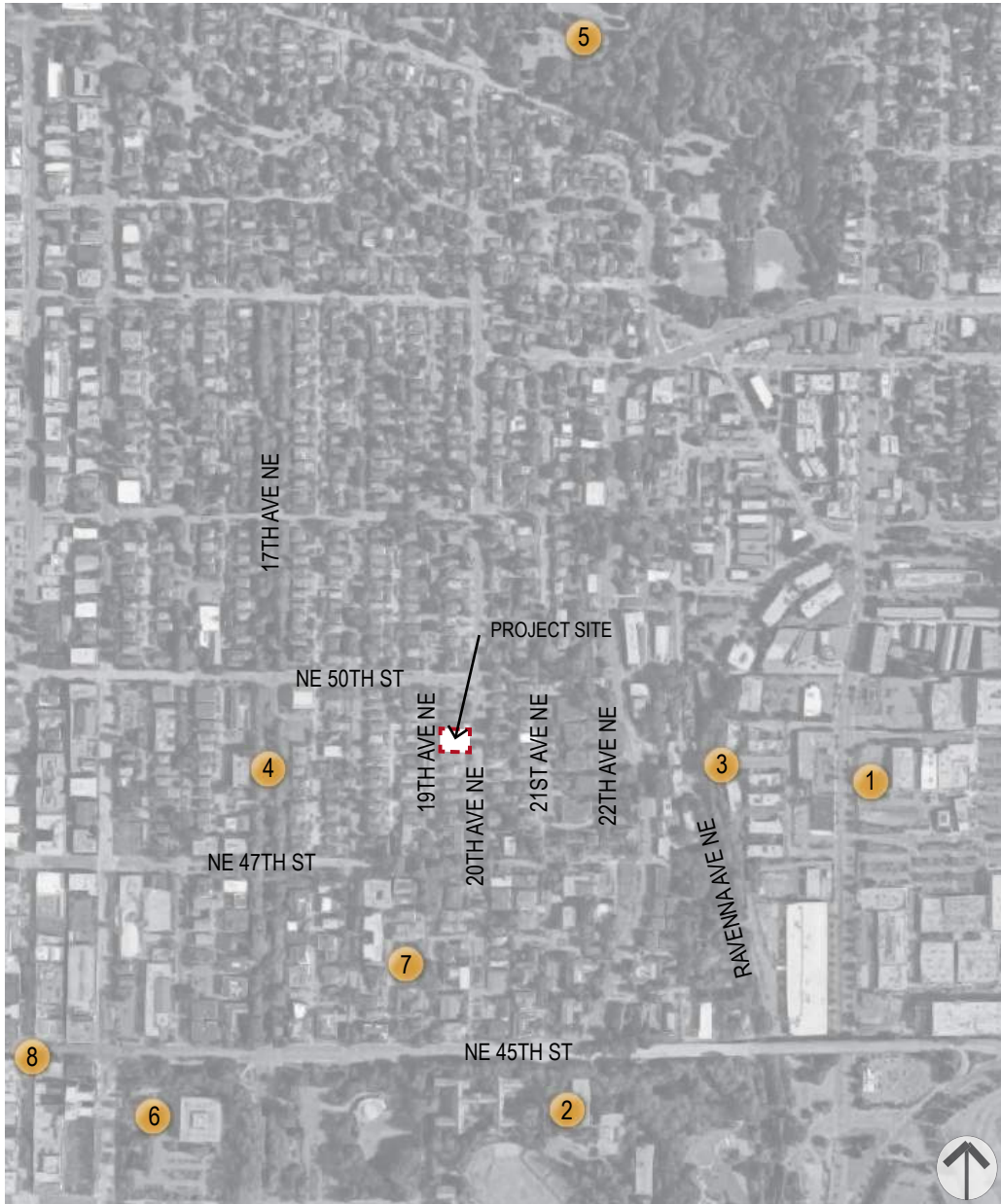


SKIMMIA



APPENDIX

CONTEXT ANALYSIS: COMMUNITY NODES & POINTS OF INTEREST



POINTS OF INTEREST

1. University Village
2. University of Washington
3. Burke Gilman Trail
4. 17th Ave NE Centerstrip
5. Ravenna Park
6. Burke Museum
7. UW Greek Row
8. University way (The Ave)



URBAN DESIGN ANALYSIS: EXISTING NOTABLE ARCHITECTURAL & ENTRY PATTERNS



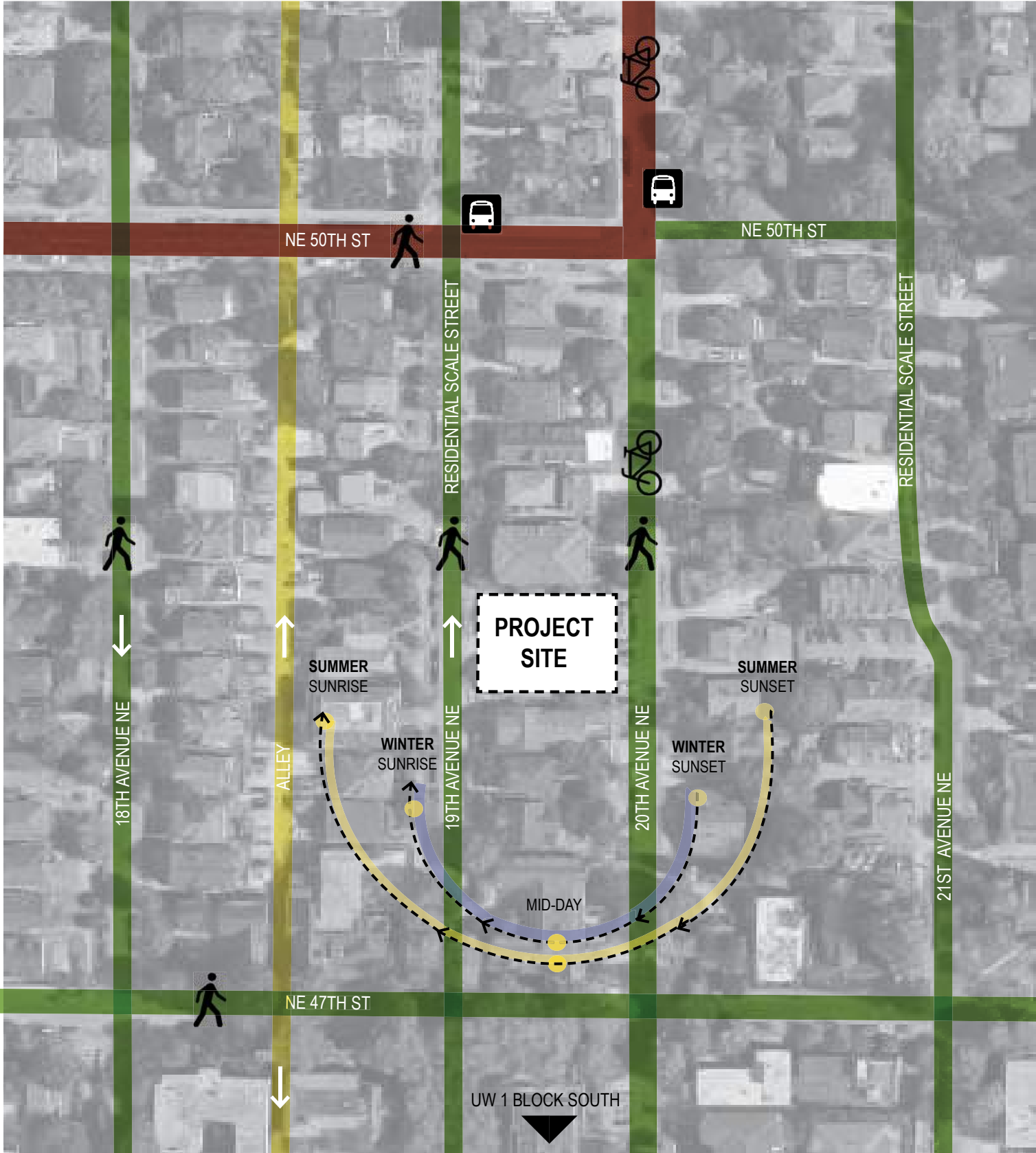
EXISTING PATTERN LOCATIONS

- 1. 4743 & 4735 21st Ave NE - Mid rise residential streetscape
- 2. 4740 20th Ave NE - Mid rise residential streetscape
- 3. 4730 21th Ave NE - Mid rise residential streetscape
- 4. 4754 18th Avenue NE - Mid rise residential entry / accessible way
- 5. 4731 18th Ave NE - Student housing entry / accessible way
- 6. 4721 19th Ave NE - Leasing office entry along access road
- 7. 4537 NE 47th St - Flat roof with Roof deck
- 8. 4738 19th Ave NE - Stepped Platers with grade change

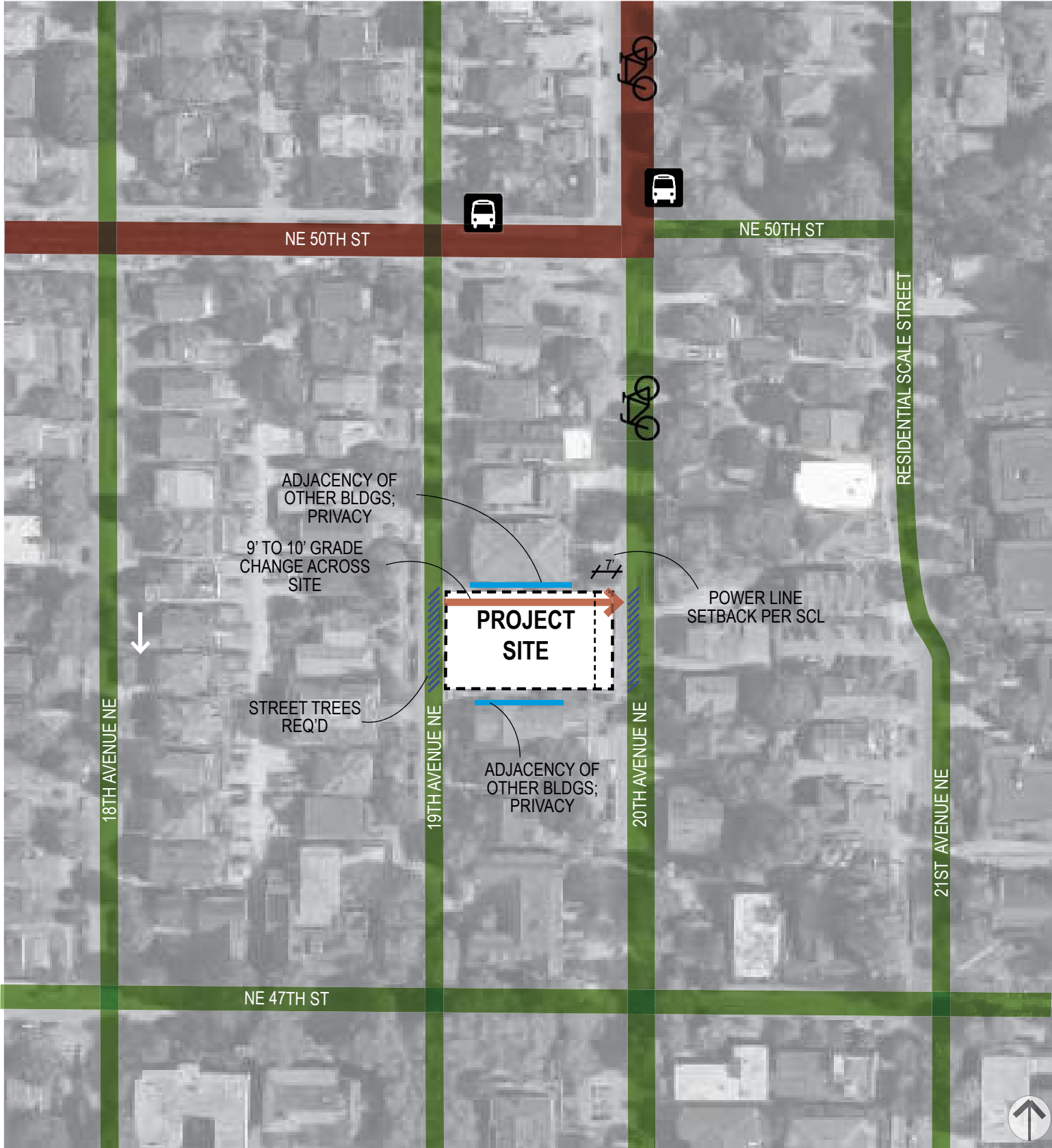
DESIGN CUES

Residential scale emphasized with street setbacks
Human scale residential covered entries
Open plaza areas at street intersections
Stairs / ramps for grading changes at entry





OPPORTUNITIES



CONSTRAINTS

- A 19TH AVE FRONTAGE FROM SOUTH**
Site frontage long 19th Avenue NE has buildings with recessed entry porches, walk up stoops and raised front yards adjacent to sidewalks.
- B 19TH AVE FRONTAGE FROM NORTH**
- C NORTH PROPERTY & NEIGHBOR BUILDING ON NORTH**
North property line is just south of adjacent apartment building with limited windows and blank wall areas facing project site. Ground level landscaping provides pedestrian-level privacy.
- D SOUTH PROPERTY & NEIGHBOR BUILDING ON SOUTH**
South property line is just north of adjacent boarding house in old SFR. Adjacency buildings on sites in prior SF zoning. No security fencing or ground level landscaping present to provide privacy between both properties.
- E LOOKING WEST FROM 20TH AVE NE**
Street frontage along 20th Avenue NE is predominantly terrace garages and recessed driveways with elevated buildings and rear yards as well as refuse/recycling storage. Parking is provided in many locations both in garage as well as surface stalls. Mature trees in east yard of project site currently provides for property to south.
- F NEIGHBOR BUILDING ON SOUTH FROM 20TH AVE**
- G ADJACENT PROPERTY FROM 20TH AVE NE**
- H NORTH PROPERTY LINE FROM 20TH AVE NE**



A 19TH AVE FRONTAGE FROM SOUTH



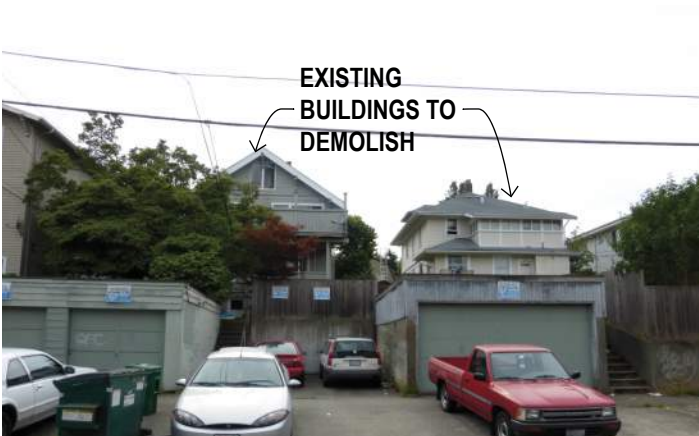
B 19TH AVE FRONTAGE FROM NORTH



C NORTH PROPERTY & NEIGHBOR BUILDING ON NORTH



D SOUTH PROPERTY LINE & NEIGHBOR BUILDING ON SOUTH



E LOOKING WEST FROM 20TH AVE NE



F NEIGHBOR BUILDING ON SOUTH FROM 20TH AVE NE



G ADJACENT PROPERTY FROM 20TH AVE NE



H NORTH PROPERTY LINE FROM 20TH AVE NE