

NIWA APARTMENTS

513 FIRST AVENUE NORTH
DESIGN REVIEW RECOMMENDATION
11.14.18 | MUP # 3029562-LU

PROJECT TEAM

Applicant: SOLTERRA NIWA LLC
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Seattle, WA 98104

Architect: JACKSON|MAIN ARCHITECTURE, P.S.
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Seattle, WA 98104

Landscape Architect: KEN LARGE LANDSCAPE ARCHITECT, INC.
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Sammamish, WA 98074





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OVERVIEW

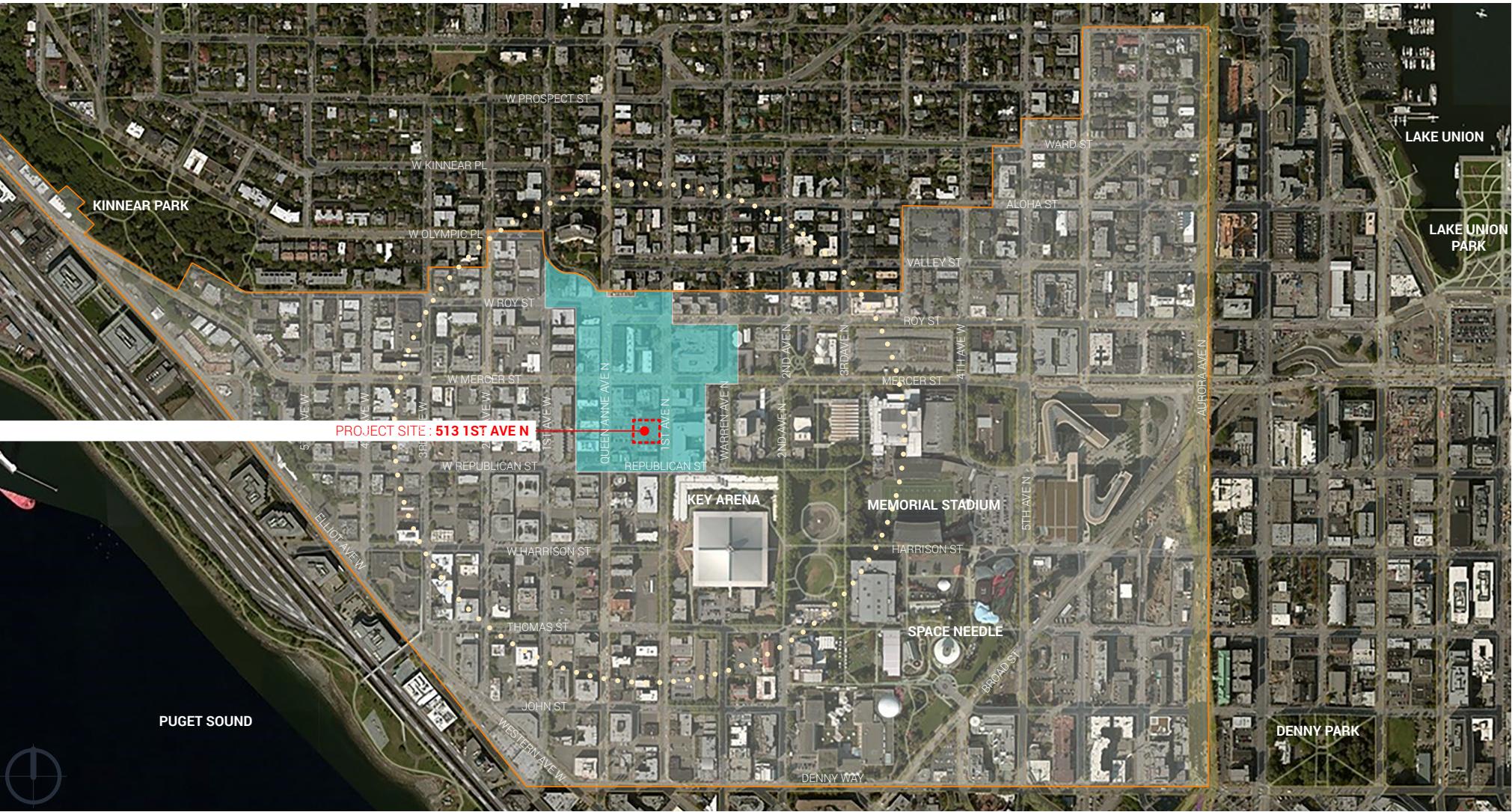


PROPOSAL / PROJECT DESCRIPTION:

The site is located within the Uptown Urban Village, with its address along the west side of 1st Avenue North, bounded by Republican Street the south and Mercer Street to the north. The zoning is SM-UP 85'(M1) with adjacent properties zoned the same. The site is currently occupied by a retail store and restaurant.

This application proposes to build an 8-story mixed use, mid-rise structure containing 127 residential units with ground level commercial, residential lobby, and amenity. Parking is provided for approximately 29 vehicles below grade and accessed from the alley. The building will be 5 levels of Type IIIA construction over 3 levels of Type IA construction, plus a single story basement, for a total building area of approximately 97,211 GSF. The existing structure is to be demolished.

Number of residential units: 127 Units
Number of parking stalls: 29 Parking Stalls
Commercial Area: 5,876 SF



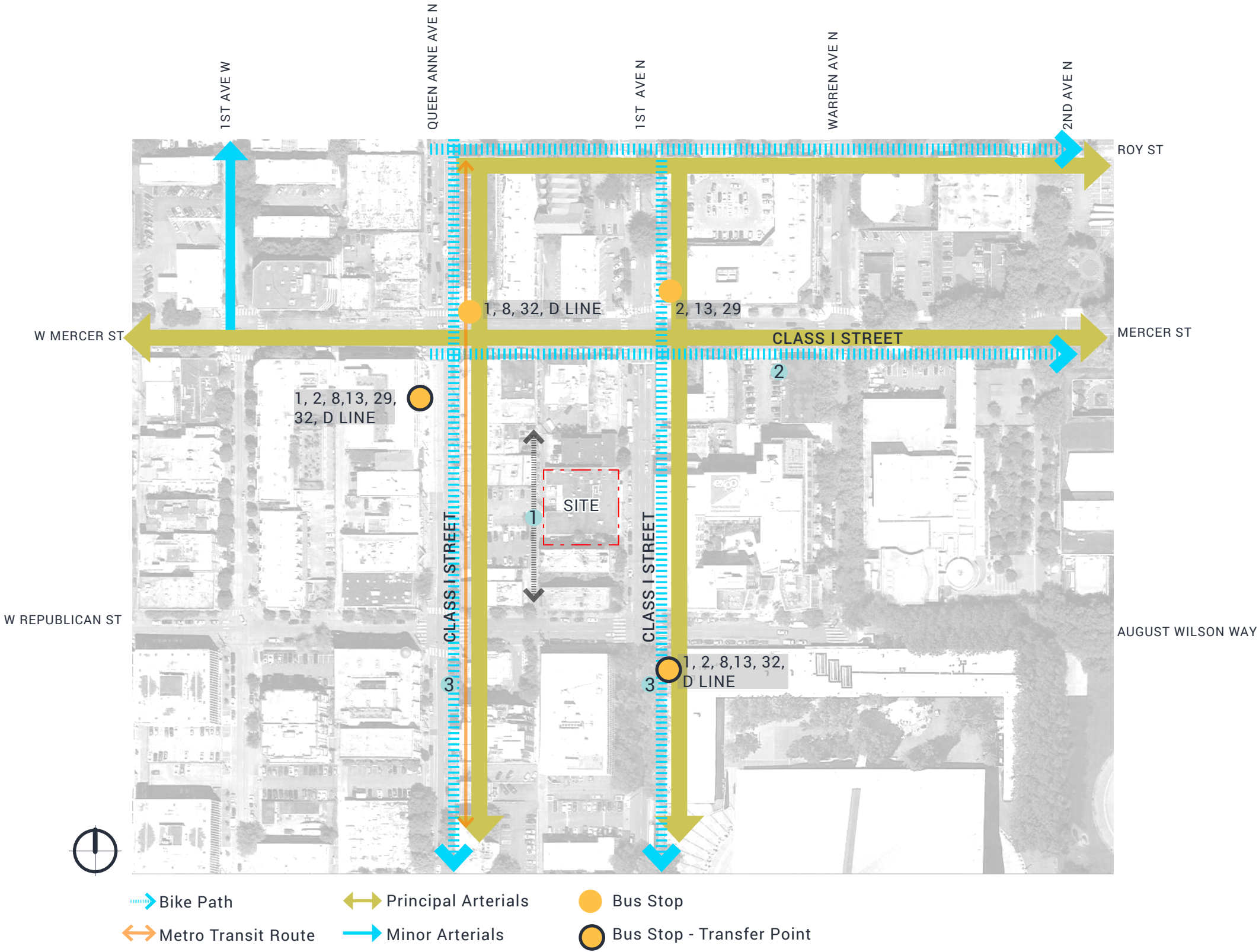
<u>Basement + 8 Stories, Type 3A over Type 1A</u>	
Basement	Parking + Amenity
Level 1	Residential Lobby Commercial Amenity
Levels 2 - 7	Residential Units
Level 8 (Rooftop)	Residential Units + Amenity

- ● ● 5-Min Walking Radius
- ▭ Uptown Urban Village
- ▭ Project Site
- ▭ Pedestrian Overlay

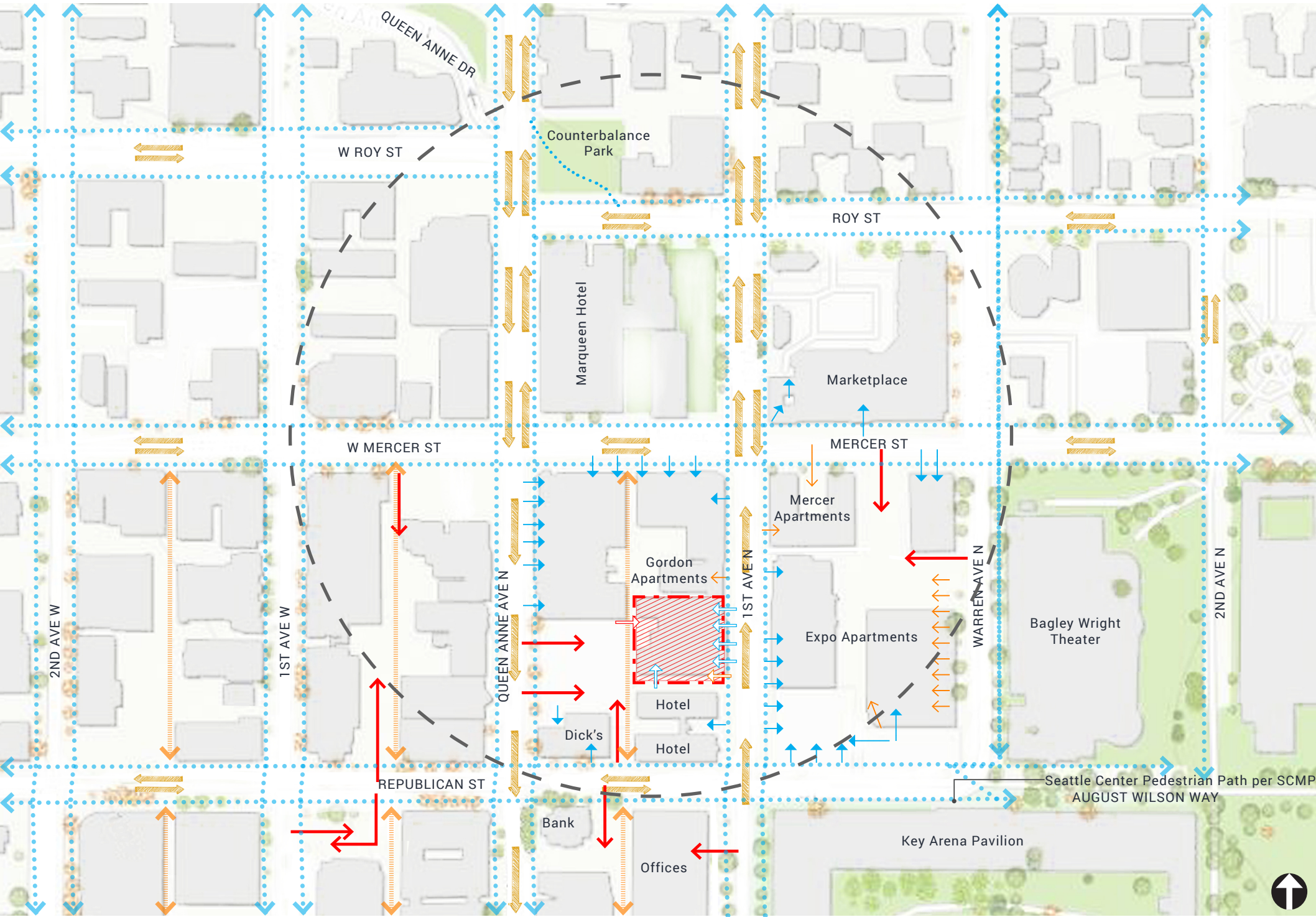
SITE ANALYSIS
TRANSPORTATION AND STREET CLASSIFICATION

ACCESS OPPORTUNITIES
The site is located in Uptown neighborhood in Seattle and a Walker's Paradise so daily errands do not require a car. Transit access is very good on nearby streets, 5 Metro Routes and the RapidRide D Line run past the site. Car sharing is available from Zipcar and RelayRides which means transit is convenient for most trips. The site is moderately sloped and some bike infrastructure is provided.

- 1 Opportunity to activate the underutilized alley and open up the project to the west to offer space to arts community.
- 2 Potential light rail entrance in 2035.
- 3 Long range include "uncoupling" 1st Ave N. and Queen Anne Ave N. First Ave. will become a general purpose street and Queen Anne Ave. N will be the transit street.



SITE ANALYSIS
SURROUNDING SITE CONTEXT INFORMATION



Entries

Retail Entries:
Heart of Uptown areas are encouraged to provide pedestrian friendly environment and streetscapes that promote human interaction on the ground level. 1st Avenue N is identified as a Main Street Commercial Corridor in the Uptown Design Guidelines, characterized by a mix of small shops and a pedestrian oriented streetscape.

Vehicular and Residential Entries:
Throughout Uptown providing opportunities for interaction among residents and neighbors is important. Residential and vehicular entries shall also be designed to offer privacy and emphasize personal safety and security for the building occupants.

- - - Project Site
- Vehicle Route
- Pedestrian Route
- Alley
- Parking Entry
- Commercial/Dining Entry
- Residential Entry
- Preferred Location for Vehicular Entry
- Preferred Location for Commercial Zone/Dining
- Preferred Location for Residential Entry
- Heart of Uptown

ADJACENT USES

The nearby parks include: Counterbalance Park, Kinnear Place Park and South Fountain Lawn.

The site is located in the heart of the Uptown Neighborhood, an area defined by smaller retail establishments and restaurants as well as some 1920s and 1930s era brick apartment structures, and larger scaled contemporary mixed-use development like the Expo (located across the street from the site). This area is considered the vibrant core to the Uptown Neighborhood and is an active node with a focus on pedestrians and transit riders.

Community Facilities

Entertainment

Dining

Hotel

Park

Mixed Use

Multi-Family/Apartment

Single-Family

Office

Medical

Religious

Retail

Service

Proposed Projects

Project Site

Bar & Restaurant

Art Gallery

Market

Bank

Surface Parking



ZONING SUMMARY

Assessor Parcel Number	1988200050
Legal Description	THE NORTH OF 120 FEET OF THE SOUTH 230 FT OF EAST 120 FT OF BLOCK 24 OF D.T. DENNY’S HOME ADDITION TO THE CITY OF SEATTLE, ACCORDING TO THE PLAT THEREOF RECORDED IN VOLUME 3 OF PLATS, PAGE 115, IN KING COUNTY, WASHINGTON; TOGETHER WITH AN EASEMENT FOR INGRESS AND EGRESS OVER THE NORTH 11.43 FEET OF THE SOUTH 110 FEET OF THE WEST, 42.5 FEET OF THE EAST 120 FEET OF SAID BLOCK 24 OF D.T. DENNY’S HOME ADDITION TO THE CITY OF SEATTLE, ACCORDING TO THE PLAT THEREOF RECORDED IN VOLUME 3 OF PLATS, PAGE 115, IN KING COUNTY, WASHINGTON.
Street Address	513 1st Ave N, Seattle, WA 98109
County	King
State	Washington
Authority Having Jurisdiction	City of Seattle
Total Lot Area	14,400 sq. ft. (0.331 Acres)
Present Use	Retail / Wholesale – Convenience market
Building Setbacks	Front Yard: 0’ - below 65’, 10’- above 65’ Side Yard: None Rear Yard: None
Pedestrian Zone	Yes
Overlay Zone	“Uptown” Urban Village.
Environmental Critical Area	None.
Current Zoning	SM-UP 85 (M1)
Proposed Building Height	85’-0”
FAR	Maximum - 5.25 / Minimum – 2
Parking:	None required per table 23.54.015 Nearest Transit Line ~500 feet away (D line bus stop)
Bicycle Parking :	Per table D for 23.54.015 1 per 4 dwelling units or 0.75 per Small Efficiency Dwellings
Driveway:	Per 23.54.030 Driveways less than 100 feet in length that serve 30 or fewer parking spaces shall be a minimum of 10 in width for one or two way traffic
Solid Waste Storage	See in Table A for 23.54.040 Minimum for more than 100 dwelling units = 575sf plus 4 square feet for each additional unit above 100
Landscaping:	Minimum area= None minimum green factor score of .3 or greater. Street trees required per 23.47A.016.B.2 and Section 23.53.015 Retain existing street trees.

SITE PHOTOS



SOUTH PROPERTY LINE ALONG 1ST AVE



VIEW OF EXISTING CONDITION



NORTH PROPERTY LINE ALONG 1ST AVE



SOUTH ADJACENT PROPERTY ALONG 1ST AVE



PERSPECTIVE ALONG ALLEY



PERSPECTIVE ALONG ALLEY

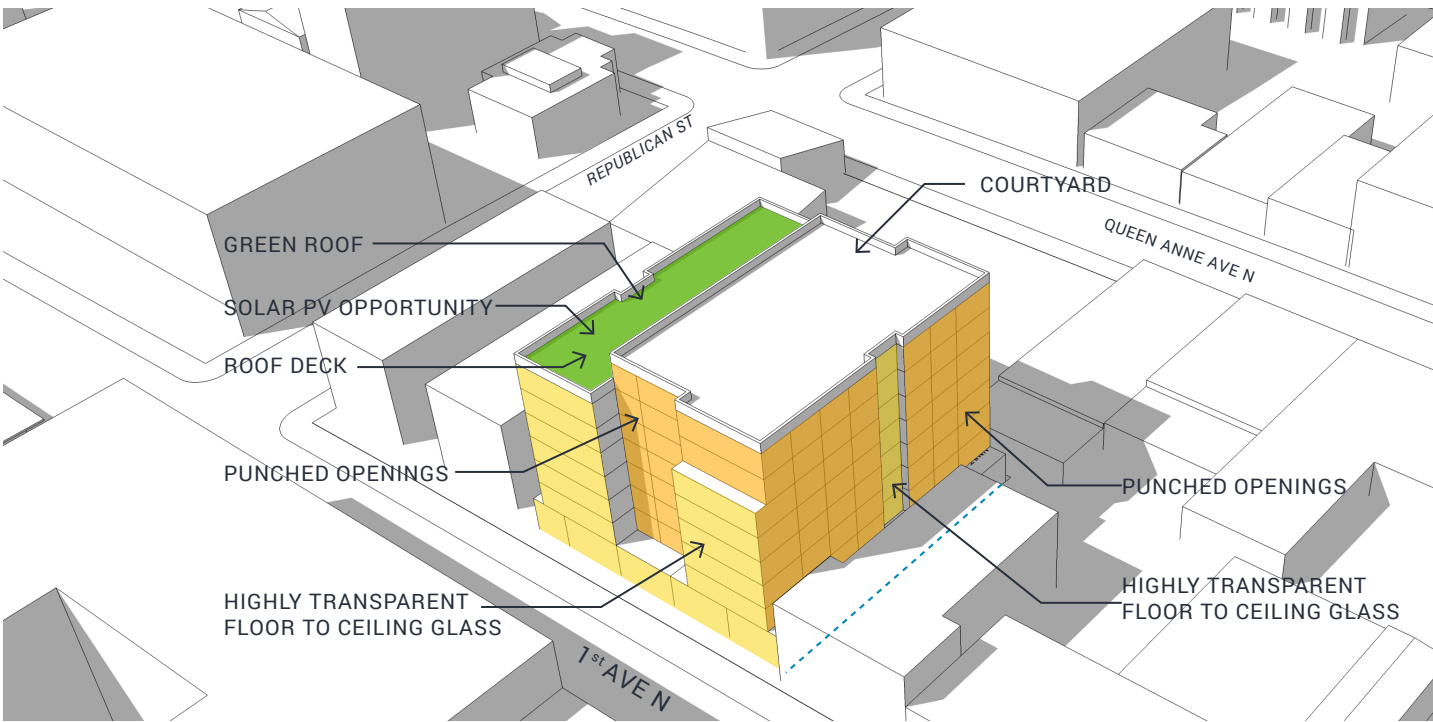
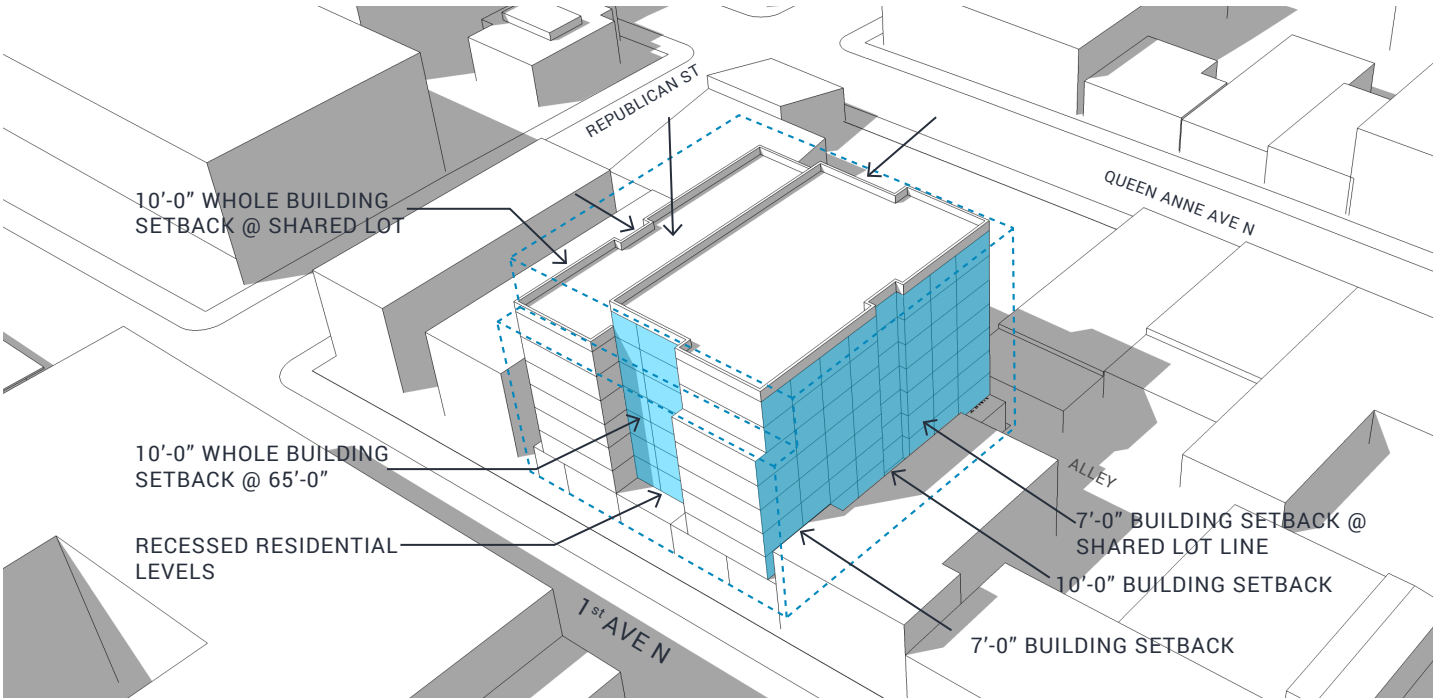


PERSPECTIVE ALONG ALLEY

ARCHITECTURAL MASSING CONCEPT

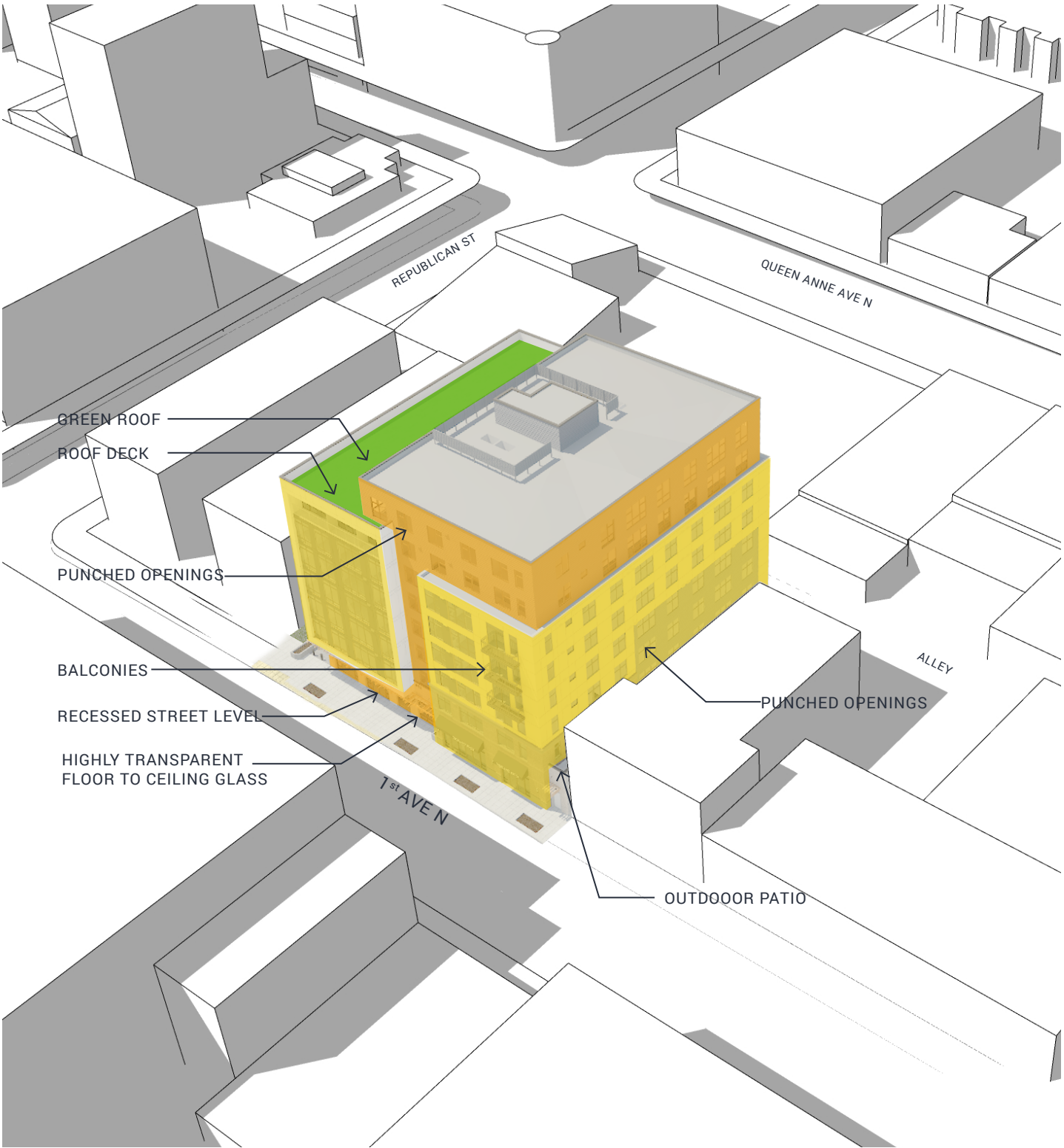
INTRODUCTION TO MASSING OPTIONS

OPTION 03 - MID BLOCK CONNECTION PREFERRED @ EDG



NORTHEAST AERIAL VIEW

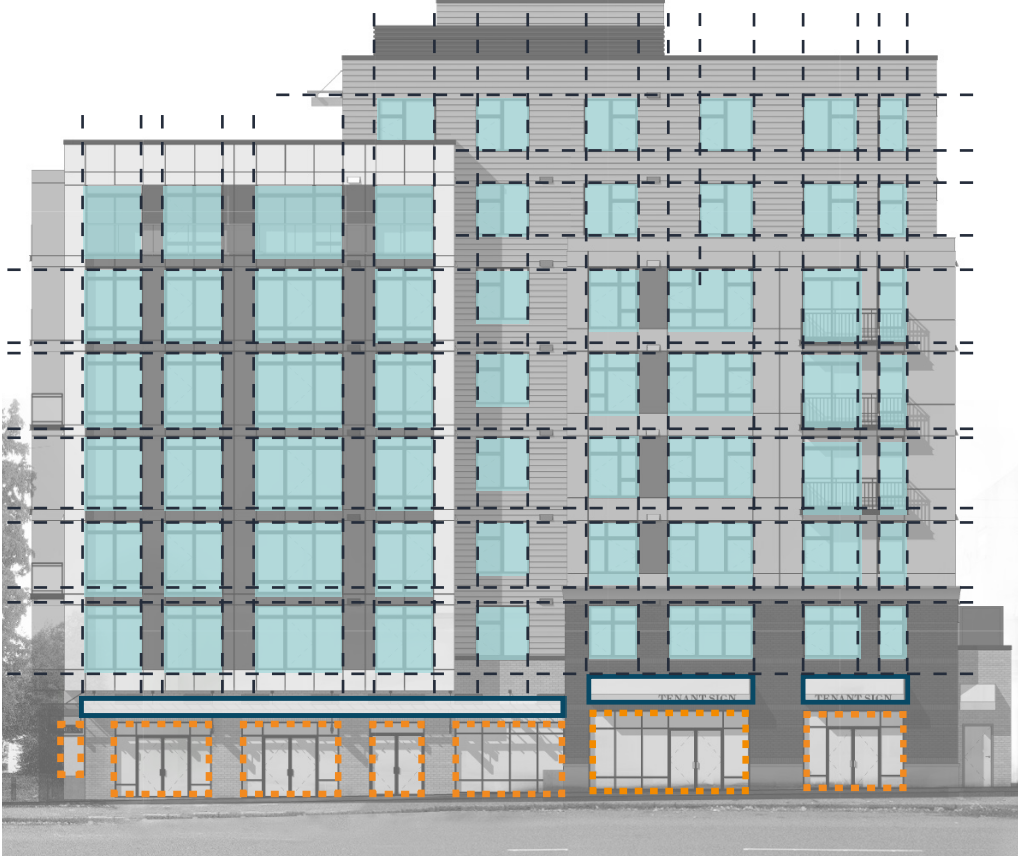
DEVELOPMENT RESPONSE: FOR DESIGN RECOMMENDATION



NORTHEAST AERIAL VIEW

SURROUNDING CONTEXT STUDY

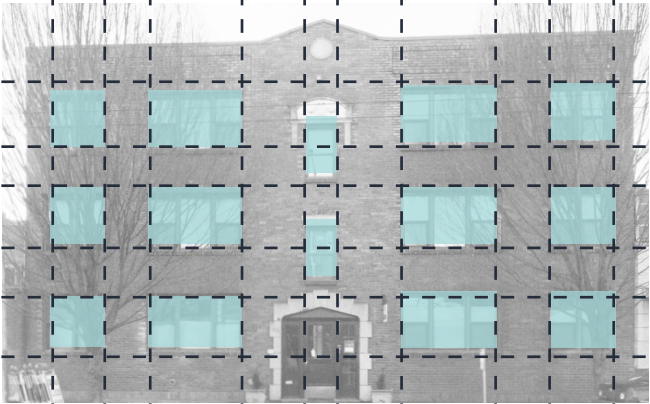
WINDOW CONFIGURATION



PROPOSED DESIGN - EAST ELEVATION



PROPOSED DESIGN - SOUTH ELEVATION



527 FIRST AVENUE



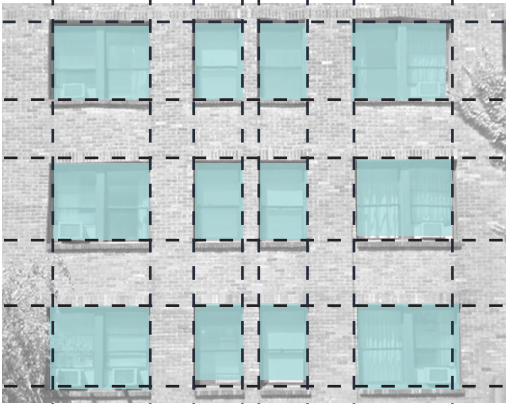
415 FIRST AVENUE



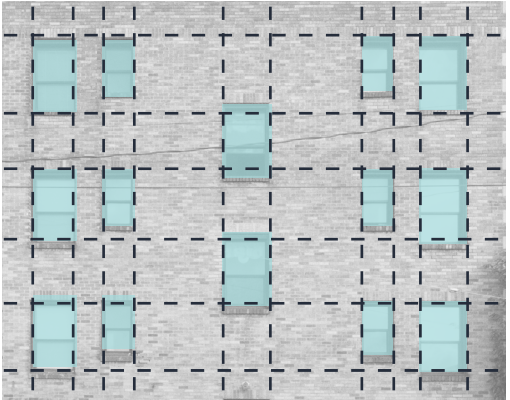
306 QA APARTMENTS



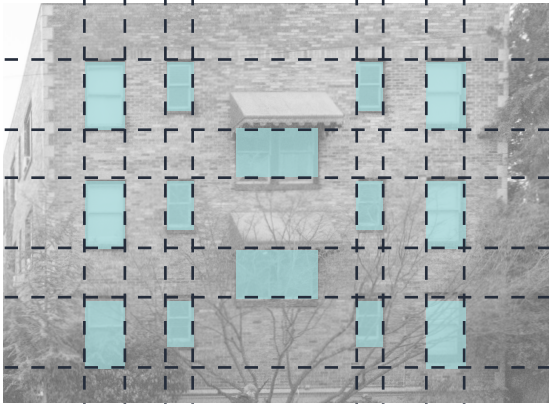
100 REPUBLICAN STREET - EXPO



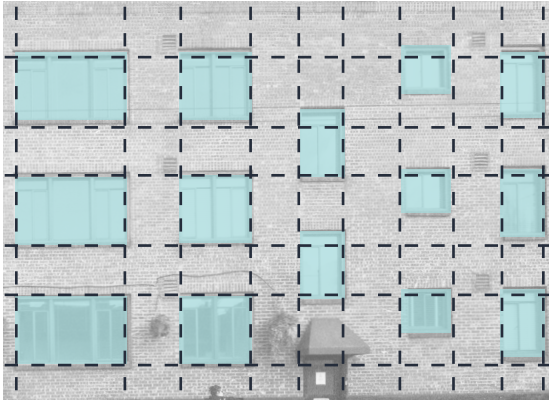
505 FIRST AVENUE - SOUTH VIEW



505 FIRST AVENUE - WEST VIEW



505 FIRST AVENUE - EAST VIEW



105 MERCER STREET

- GRID Configuration
- STOREFRONT WINDOWS
- BAY WINDOWS
- CANOPY/AWNING

The depicted window configuration as highlighted mimics some of the Uptown Neighborhood window characteristics and styles. On the Queen Anne hotel buildings, are a regular window pattern organized on a grid. The patterning is evident on all sides of the buildings’ facades. These configurations are reflected on the proposed design where the windows are organized on a grid system occurring mainly on the upper residential levels, along with the storefront bays on the ground floor retail level. In addition, the east property across 1st Avenue, Expo Apartments, and 306 QA Apartments consist of a regular pattern of punched windows and bay windows. This is also portrayed in the proposed design featuring bay windows of varying heights with an accent color to emphasizing the framed element.

Canopy structure and awnings are incorporated on the ground and street-facing facades of the building to enhance a pedestrian friendly environment. The canopy structure wraps around from the street-facing facade unto the mid-block connector (south elevation) further enhances the pedestrian flow and allowing the public to access the alley from 1st avenue.

SUMMARY OF EDG BOARD RECOMMENDATIONS

MASSING

The Board supported the design team’s preferred Option 3 with its half block passage (see below) and the upper level setbacks at the north and south property lines. The Board supported the breakup of the facade facing 1st Ave N into two distinct masses with different materiality and stated that quality detailing will be important. The Board was concerned that the interior layout of the residential units along 1st Ave N would not activate that street facing façade.
(CS2-C-2, CS2-D-5, DC2-B-1, DC2-III-I)

The following guidance was given:

A) Break up the 1st Ave N façade into two distinct masses, with different material treatments and height if needed. Detailing of the materials is key. The Board indicated support for departures to allow the street facades to extend beyond 45’ in height (up to 65’ height for one massing and up to 75’ for the other massing section), if needed to achieve this guidance. See departures below.] (DC2-B-1, DC2-III-I)

The massing has been further articulated with the use of materials and window fenestration (See Figure 1). This project still seeks a departure for the height / setback required in order to provide better differentiation between the white and yellow masses.

B) The board supported a more contemporary approach to materials and detailing and gave guidance to not provide a cornice, to avoid looking historic (CS3-A-1, CS3-A-2, DC4-A)

More contemporary detailing of materials has been provided. The cornice has been eliminated and upper level elements are planned to have a more contemporary detailing. The dark brick on the lower mass features some more traditional detailing and coursing to help draw ties with the neighborhood at the ground level.

C) Resolve the relationship of the gasket between the two masses and the windows on 1st Ave (DC2-B-1)

The “gasket” element is articulated with the lap siding material above the ground level and uses a different window configuration to help set it apart from the masses to either side. The ground floor has shifted the retail Entry doors so that they do not occur in this “gasket” space and full fenestration glazing can be provided. Additionally landscape elements and street trees are being coordinated to further enforce this articulation.

D) The board supported the deck locations on the south facade and asked to see a dimensioned plan at recommendation (DC2-B-1)

The decks on the south side have been maintained. Additional information about the dimension of these decks can be found on page 39

E) Consider having more residential units face 1st Ave N and providing active spaces that will provide “eyes on the street” and activate the facade (PL2-B-1)

The typical floor plan has been reworked so that there are units facing both the mid block passage, 1st Ave N and the alley (See Figure 2).



Figure 1. Massing modulation at east facade.

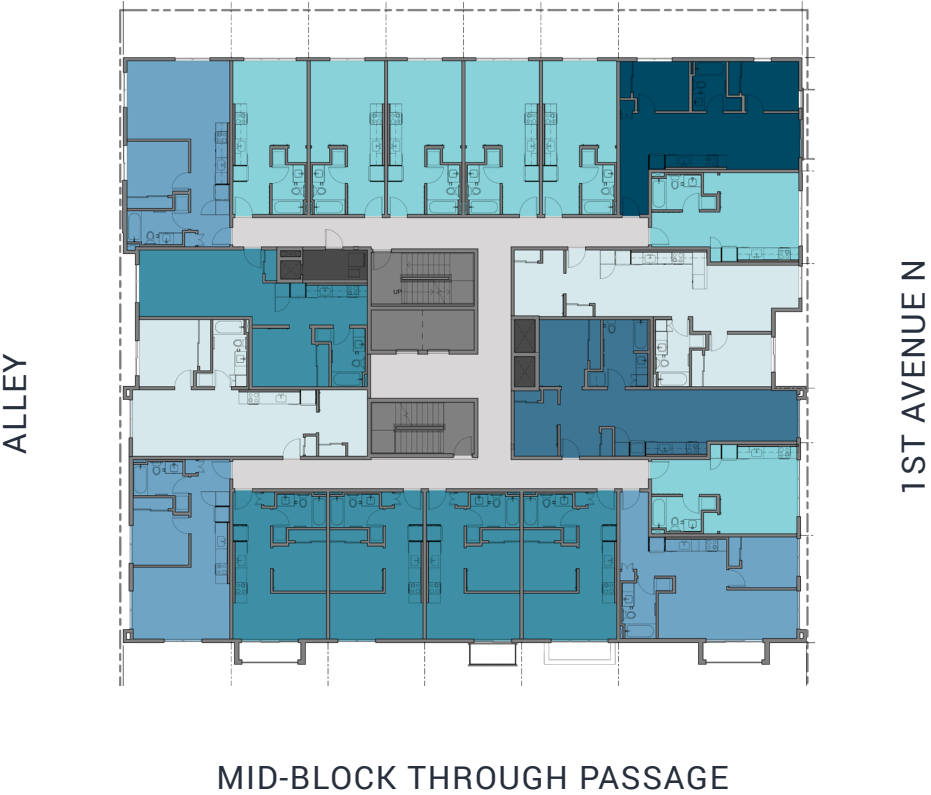


Figure 2. Residential unit views.

SUMMARY OF EDG BOARD RECOMMENDATIONS

MATERIALITY

The Board gave guidance to use contrasting high quality materials and was supportive of material changes between the two street facing masses. (DC4-A)

A) The board supported a more contemporary approach to materials and detailing (CS3-A-2, DC4-A)

The majority of the building feature contemporary materials and detailing. The material palette has been further refined and specific products have been selected as shown on page 29 and 30.

B) Consider incorporating finely textured materials (DC2-D)

The ground floor of the building is primarily masonry and glass. To help create a finer texture, two different brick colors are utilized. Coursing and detailing is articulated differently between the dark brick and the lighter gray brick. The darker brick takes on more traditional coursing, with a wainscot height ledge, sills and headers featuring solidier courses, while the lighter gray color takes on a more modern detailing (See Figure 2). Both provide a finer texture at the pedestrian level.

C) Choose materials to enhance the design concept of the protruding elements on the south facade

The protruding elements on the south facade have been further articulated with a POP of the yellow color (See Figure 1) that is used on the smaller mass along 1st Ave N. The material helps draw a contrast from the more muted field behind and the gray material.



Figure 1.(Top) South facade showing accented window bays.
Figure 2.(Bottom) Southeast corner view of the retail and into the half-block through passage.



SUMMARY OF EDG BOARD RECOMMENDATIONS

HALF BLOCK THROUGH PASSAGE

The Preferred Option 3 showed a pedestrian passageway along the south property line, between the proposed structure and the existing hotel to the south. The applicant noted that to make the proposed 21'5" wide passageway viable, an easement with the hotel property to the south would be needed, as 11'5" of the passage would happen on the hotel site. The board encouraged the design team to work on securing the easement and to have it recorded with King County before the recommendation meeting. The board also gave guidance that if the easement cannot be secured by the time of recommendation, an alternative option should be presented. (PL1-B, PL2 Uptown)

A) Locate the residential entry along this passageway as shown, to help ensure activation of the passageway (PL3-A-2)

The ground floor plan has provided the residential entry off of the half block through passage (See Figure 1). The design and articulation of this entry has been developed to provide a vibrant focal point drawing pedestrians from 1st Ave and the alley. The use of color, glass, and light all serve to provide further way-finding and activate the spaces around the residential entry (See Figure 2,3).

B) Have the two site owners agree to and record an easement agreement with King county to ensure the passageway design (PL1-B-1)

An agreement is being discussed between the property owners and the applicant and conversations continue. The current design has incorporated those discussions and the proposed plan has been developed to not require an easement while still providing access, privacy and activity to create an attractive pedestrian environment (See Figure 1).

C)Provide an alternative passageway design if the easement can't be secured. (PL1-B)

The proposed plan has been developed to not require an easement while still providing access, privacy and activity to create an attractive pedestrian environment (See Figure 1).

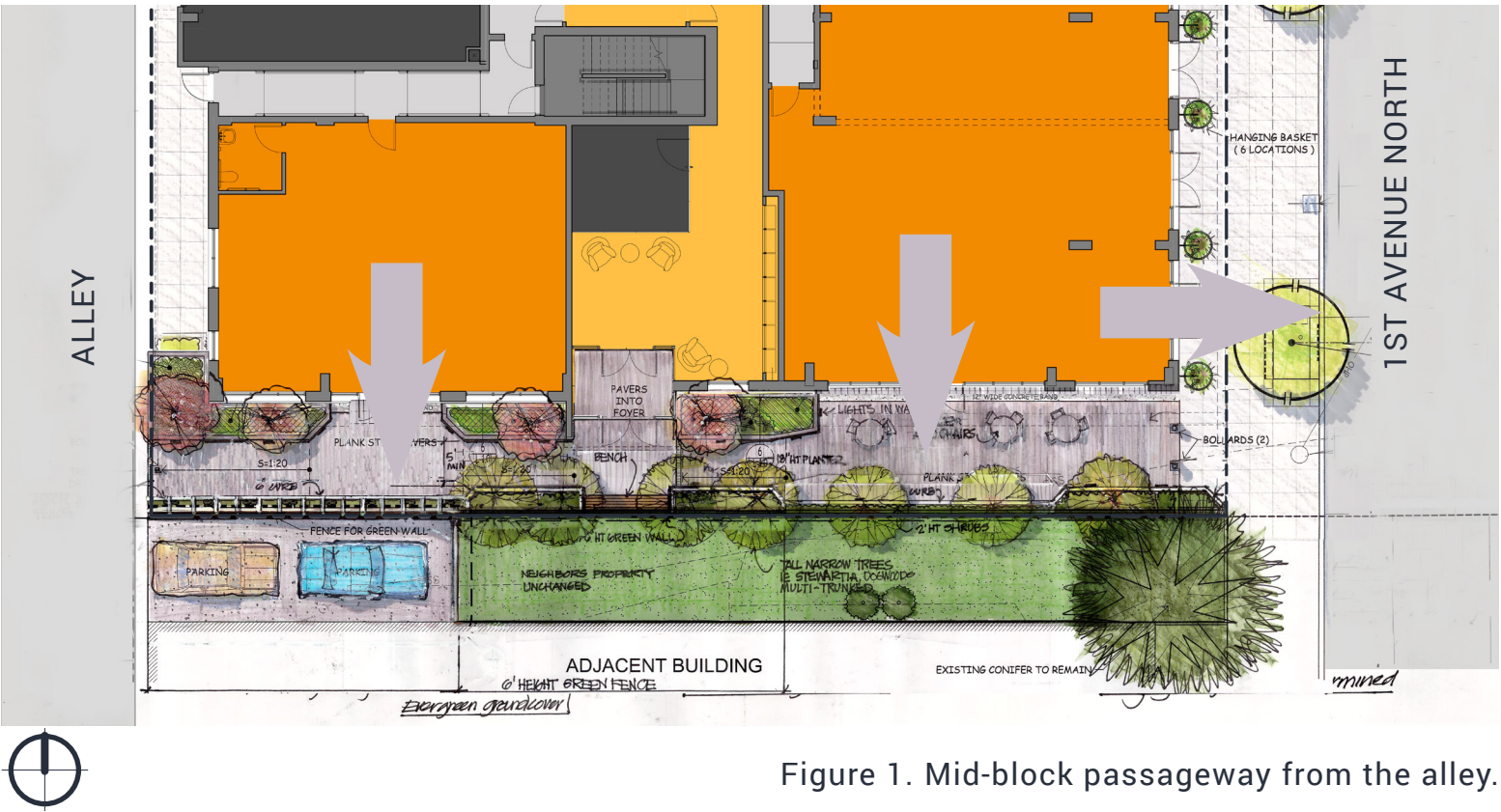


Figure 1. Mid-block passageway from the alley.



Figure 2. Residential entry located at half-block passage.



Figure 3. Mid-block passageway landscape design.

SUMMARY OF EDG BOARD RECOMMENDATIONS

GROUND LEVEL DESIGN

The Board approved of the smaller retail spaces along 1st Ave N and the space at the corner of the half block passage and 1st Ave N, but noted that the entries into these spaces should not look residential. The board asked to see further development of the entry into the half block passage, and the design relationship of the half block passage to the live work units to the alley. (PL1-B, PL2 Uptown, DC1-VI, DC4-B, DC4-C)

A) Show how the entry and half block passage way will be experienced, addressing safety, privacy, signage, lighting and the entry sequence, and the corner retail space. (PL1-B, DC4-B, DC4-C)

Additional images of the entry and half block passageway have been provided. Safety has been addressed by providing bollards at the entrances to the passage as well as a high degree of transparency.

B) Design the relationship of the live/work units to the alley and through block passage, including way-finding, access and privacy for the units. (DC1-VI)

The live/ work units have been replaced with additional retail space to draw more activity to this area, additionally the orientation of this retail space has been revised to face the proposed mid block connection. Signage and some high windows are provided from the retail space to the alley

C) Design the retail space entries to read as commercial entries, not residential. (PL3-A-1)

The retail entries are treated differently from the residential entry. The residential entry located in the center of the block utilizes a large glass mass with a vertical wood accent. The use of glass, the massing of the building above and the wood accent wall highlight the residential entry, while the commercial entries feature a more consistent storefront and awning combination.

D) Consider opening the live/work units and corner retail space into the half block passage (PL3-I-i)

The live/ work units have been replaced with additional retail space to draw more activity to this area, additionally the orientation of this retail space has been revised to face the proposed mid block connection. Signage and some high windows are provided from the retail space to the alley.

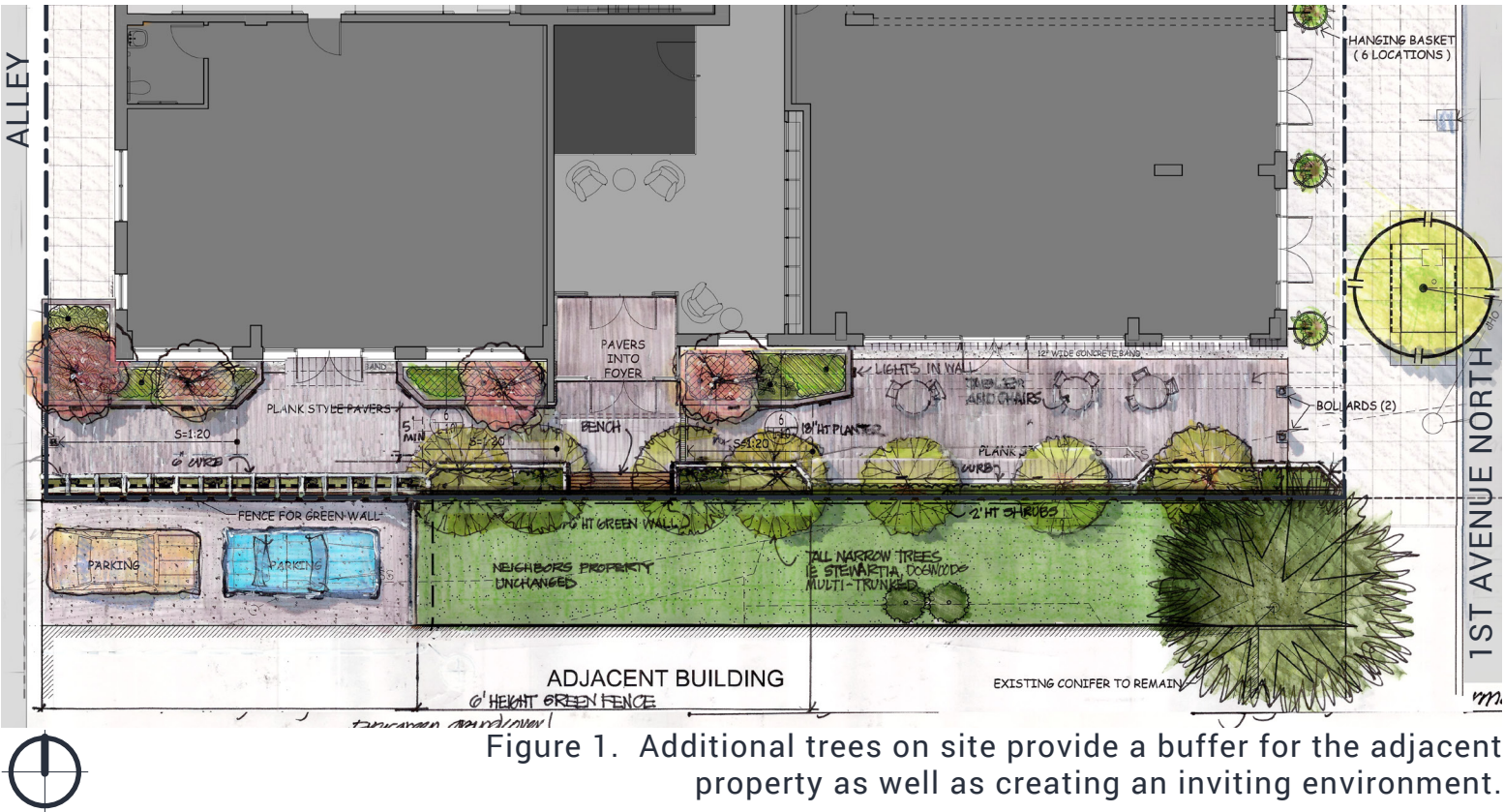


Figure 1. Additional trees on site provide a buffer for the adjacent property as well as creating an inviting environment.



Figure 2 (Left), Figure 3 (Right)

Residential entry (Figure 3) defined in a distinctly different manner than commercial entries (Figure 2). The massing, lighting, and signage provide clarity in wayfinding.

SUMMARY OF EDG BOARD RECOMMENDATIONS

FOR THE RECOMMENDATION MEETING

A) Provide window studies to show the relationship of the design to the existing structures at the north and south lot lines

Window studies of the adjacent buildings to the north and south have been provided and are overlaid with the proposed building elevation on page 17 of this package. Additionally diagrams have been provided on page 18 of this document to show the relationship of the window patterning and fenestration with neighborhood precedents.

B) If needed, provide sections showing the relationship of any upper massing protruding over the street level spaces.

A section through the mid-block through passage indicating the character at the street level. Proposed vegetation and outdoor seating creates an inviting environment.

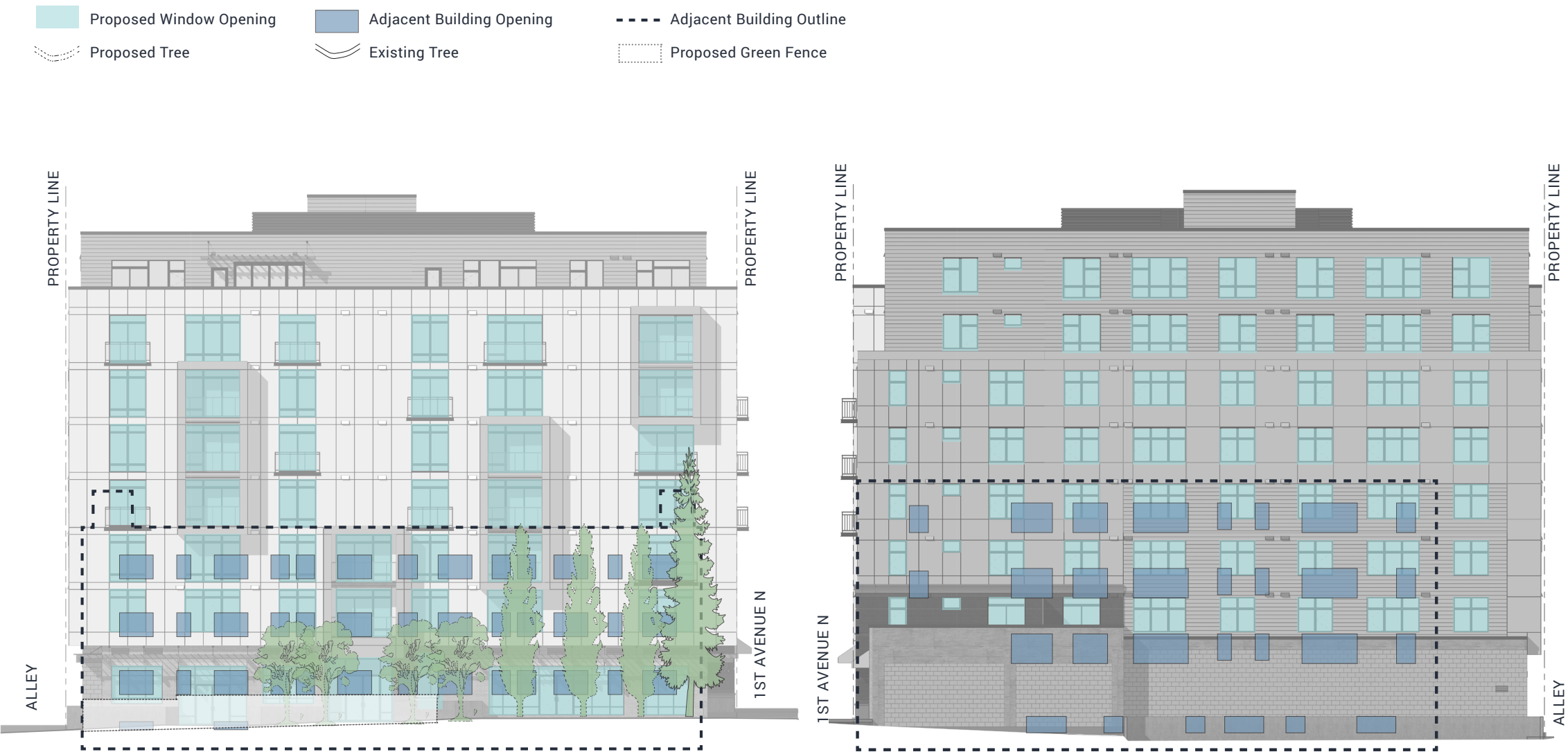


Figure 1. Window overlay at south facade.

Figure 2. Window overlay at north facade.



Figure 3. Section through mid-block passage.

VIEW FROM THE SOUTHEAST



VIEW FROM THE NORTHEAST



VIEW FROM THE SOUTHWEST





PROPOSED LANDSCAPE PLAN
STREET LEVEL LANDSCAPE PLAN



Axonometric Image: Mid-block through passage to the alley street with proposed trees and raised planters to provide screening and separation.

The proposed mid-block through passage serves at the residential entrance to the building and will also include a commercial space opening onto it. Landscaping, seating and lighting are incorporated to invite residents and passers-by to use the space. The new connection helps link the existing mid-block pedestrian crossing on Queen Anne N to 1st Avenue N and improve access to public transportation on these two streets.



[Mid-block Passage, West Seattle]

- ① Plank Pavers
- ② Outdoor Seating



[Mid-block Passage, West Seattle]

- ③ High Board Formed Concrete Planter
- ④ Low Board Formed Concrete Planter
- ⑤ Wood Bench



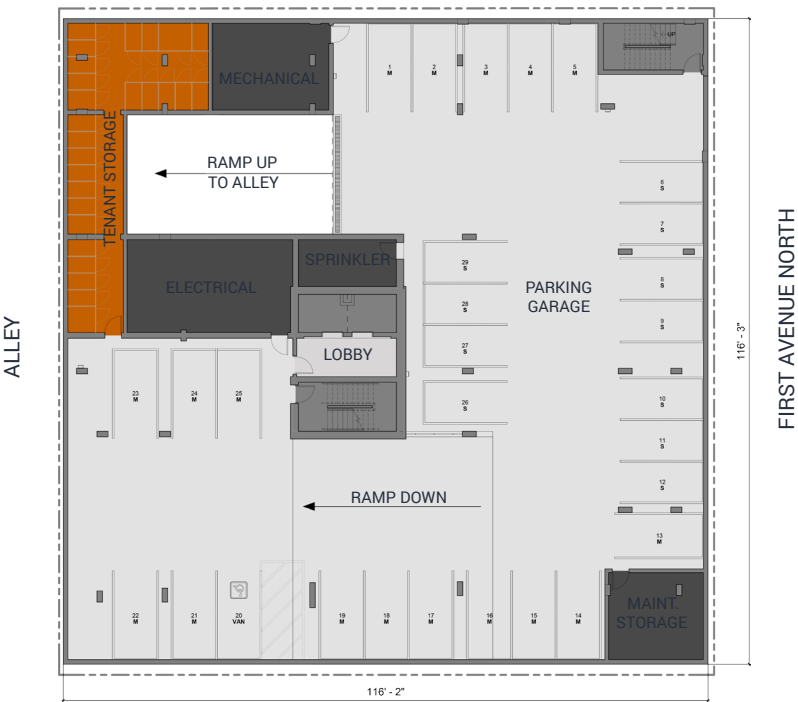
- Property Line
- Japanese Maple 20 Feet
- Japanese Stewartia 30 Feet
- Swedish Poplar 40+ Feet
- European Hornbeam 25 Feet
- 18" High Planter
- 48" High Planter
- 6" High Planter
- 4' - 6' Fence

LEVEL 1 PLAN W/ SITE INFO

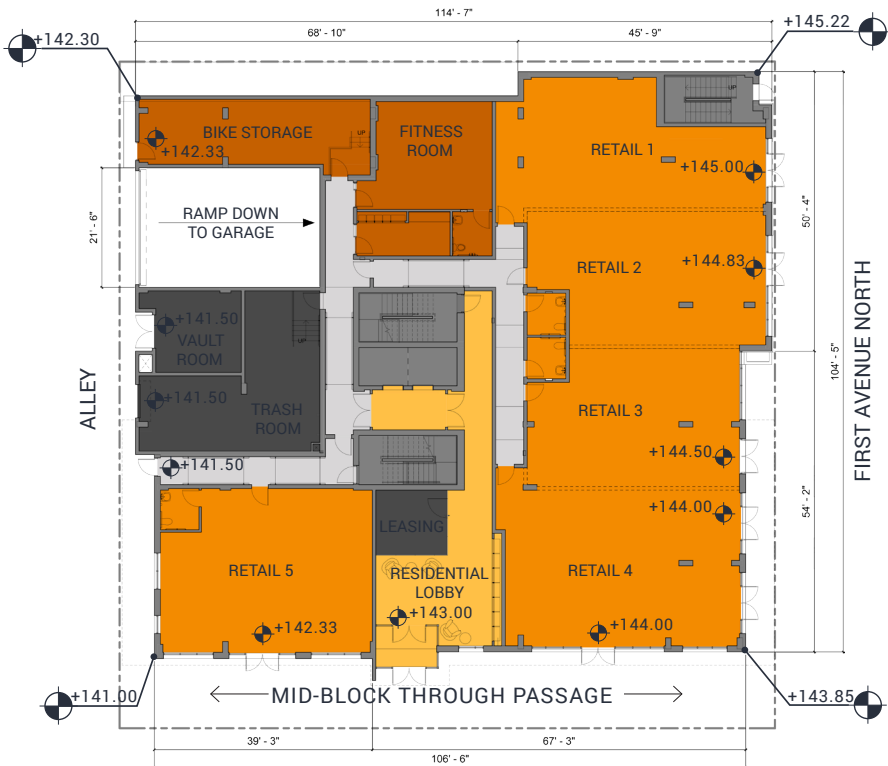


PROPOSED FLOOR PLANS

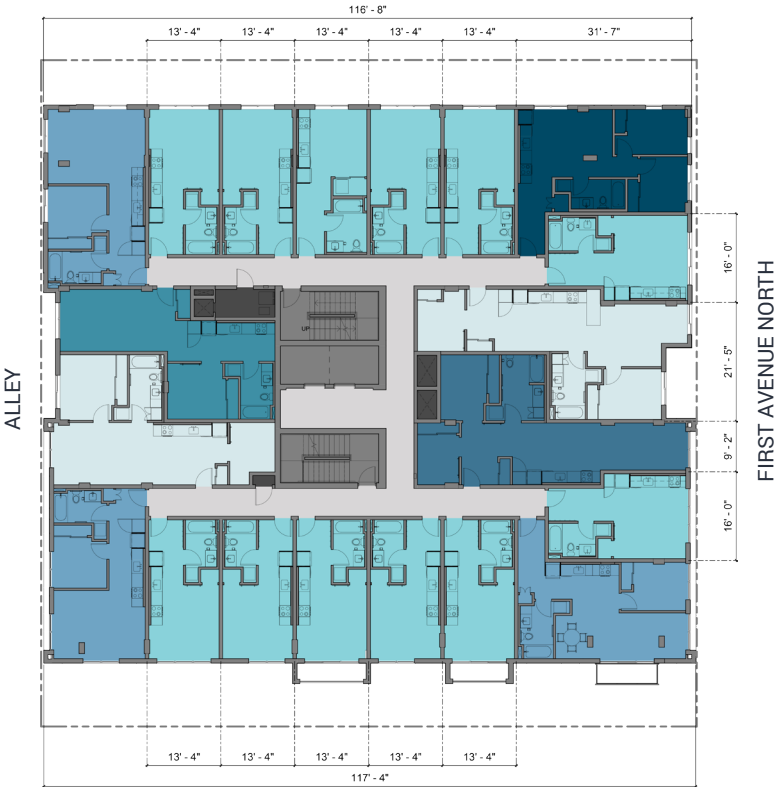
- Circulation
- Corridor
- Two Bedroom
- Open-Two Bedroom
- One Bedroom
- One Bedroom + Open
- Open-One Bedroom
- Studio
- Support / Utility
- Retail
- Residential Lobby
- Amenity



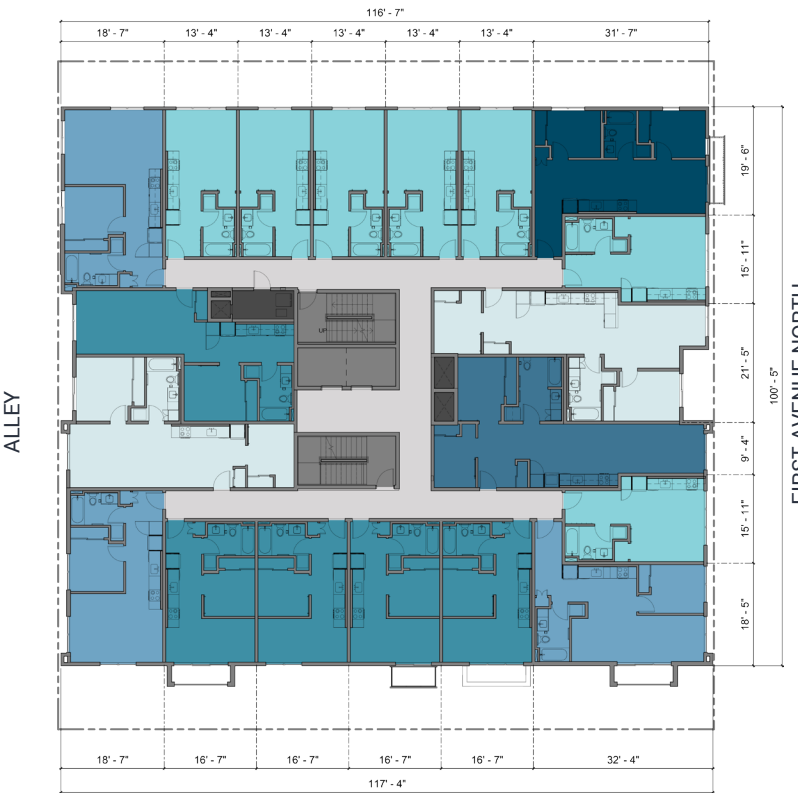
BASEMENT FLOOR PLAN



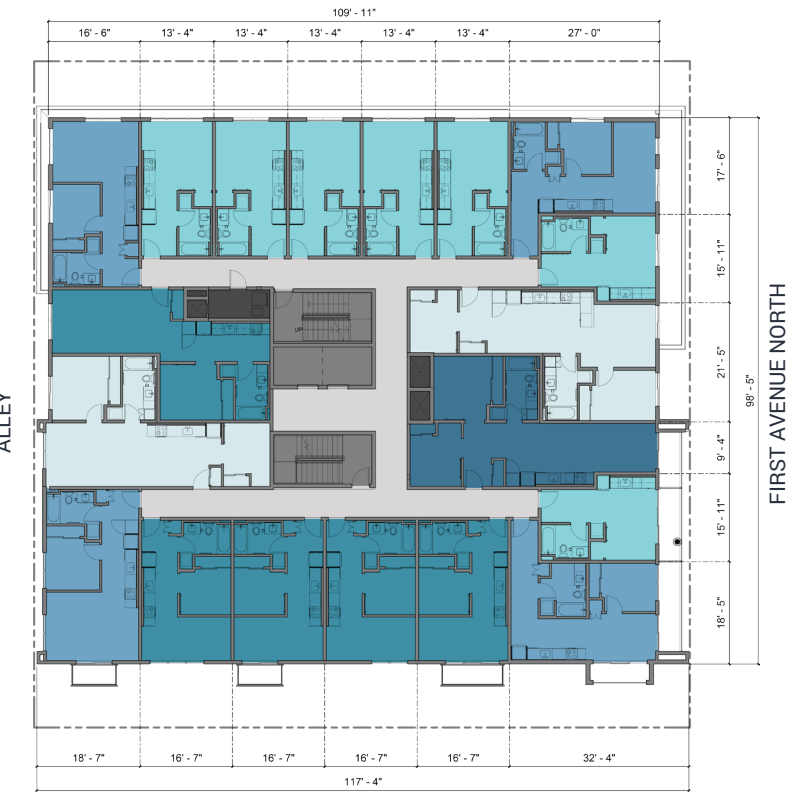
LEVEL 1 FLOOR PLAN



LEVEL 2 - 3 FLOOR PLAN



LEVEL 4 - 6 FLOOR PLAN

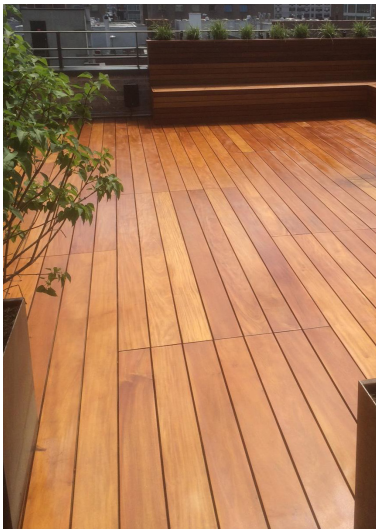
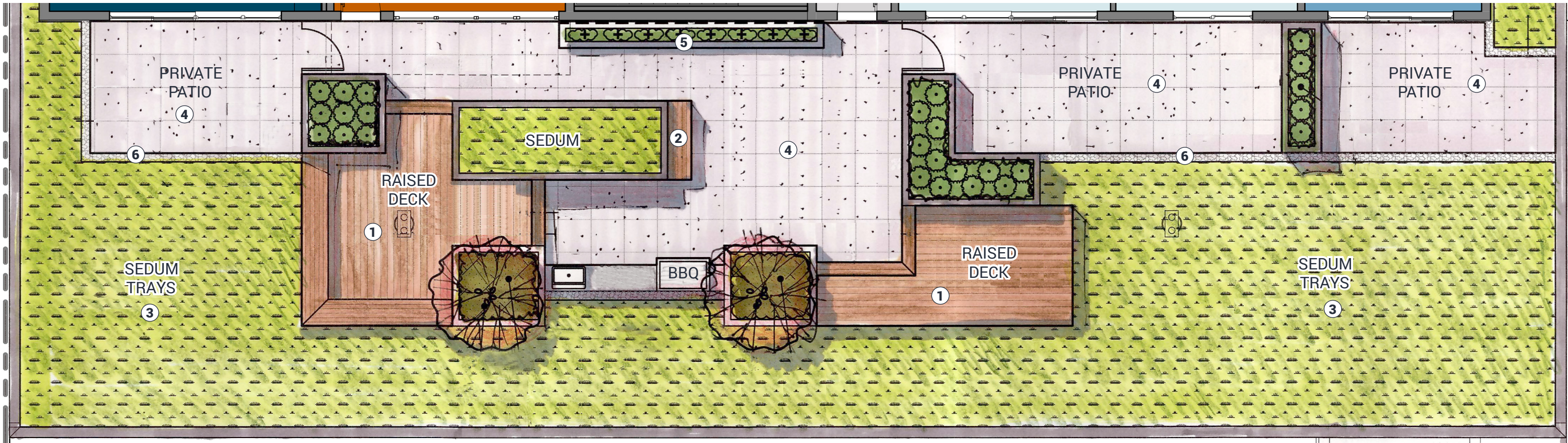


LEVEL 7 FLOOR PLAN



ROOF DECK FLOOR PLAN

PROPOSED LANDSCAPE PLAN
ROOF DECK LANDSCAPE PLAN



① Raised Wood Deck



② Wood Bench



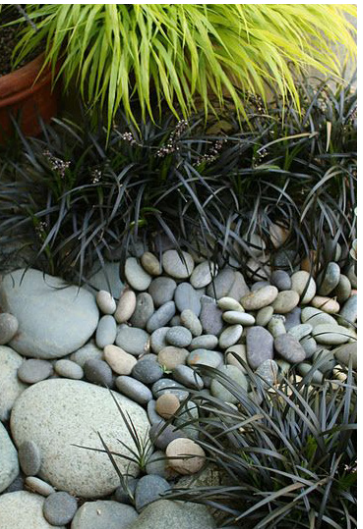
③ Sedum



④ Roof Deck with Concrete Pavers + View of the Space Needle [Expo Apartment]



⑤ Green Wall on Cable Trellis



⑥ River Rock

PROPOSED LANDSCAPE PALETTE

STREET TREES



[POP] Swedish Poplar



[ACERP] Japanese Maple



[CARP] Carpinus Frans Fontaine



[STEW] Japanese Stewartia

BIORETENTION PLANTING



[ASA] Wild Ginger
asarum caudatum



[CARM] *Carex morrowi variegata* ice dance



[CLEM] *Clematis montana rubens*



[ERIC] *eric vagans mrs df maxwell*



[HEBM] Mckean Hebe
hebe mckeannii



[LIRS] Spreading Lirope
lirope spicata



[MAH] Mahonia Charity



[PAO] Peonies



[POLYB] Tassel Fern
polystichum polybleharum



[POLYM] Sword Fern
polystichum munitum



[SAM] Sutherland Gold
sambucus racemosa



[SARC] Sarco
sarcococca ruskifolia

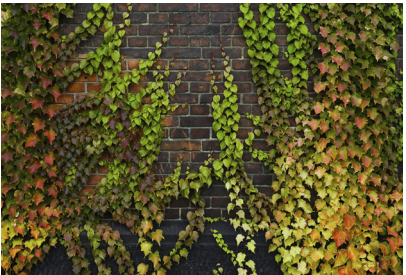
ROOF DECK PLANTING



[LONNH] Evergreen Honeysuckle
lonicera henryi



[TAXBS] Irish Yew
taxus baccata stricta



[PART] Boston Ivy
parthenocissus tricuspidata

MID-BLOCK THROUGH PASSAGE PLANTING



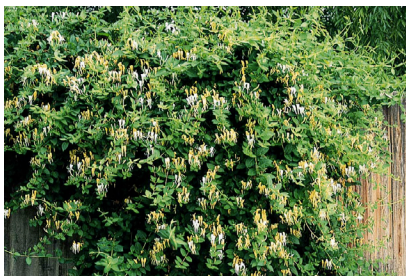
[ACOR] *acorus pusillus minimus aureus*



[AMP] Porcelain Vine
ampelopsis glandulosa



[CALW] Calluna Zoi
asarum caudatum



[LONNC] Evergreen Honeysuckle
lonicera candy swirl Vine



[RAPH] Raphiolepis Umbellata
Minor

ELEVATION + MATERIAL PALETTE

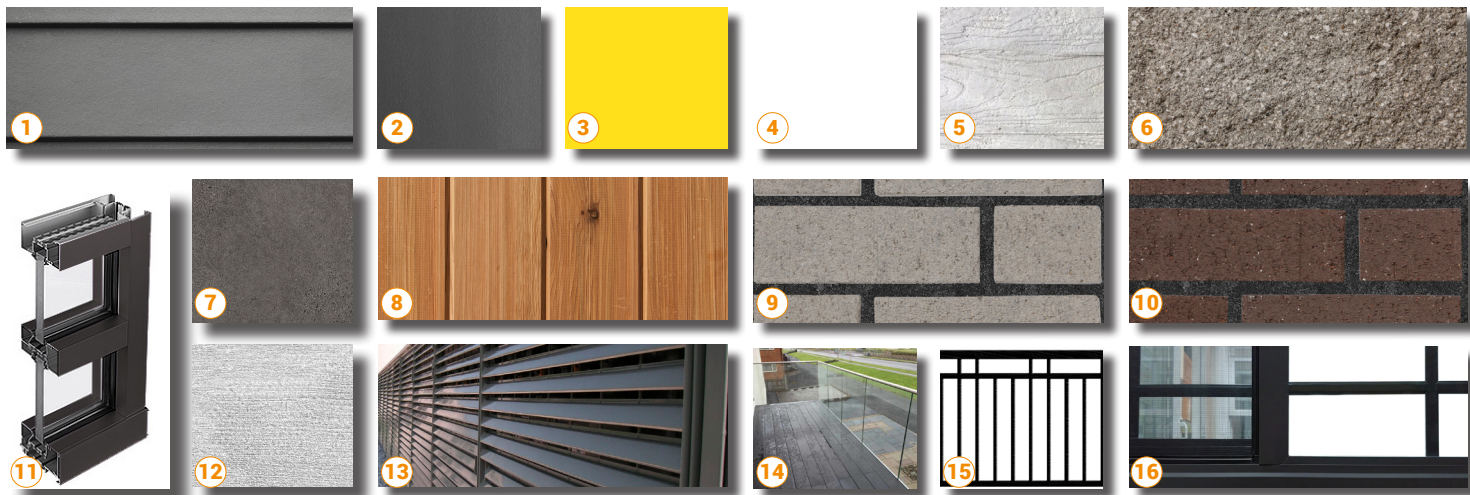


EAST FACADE

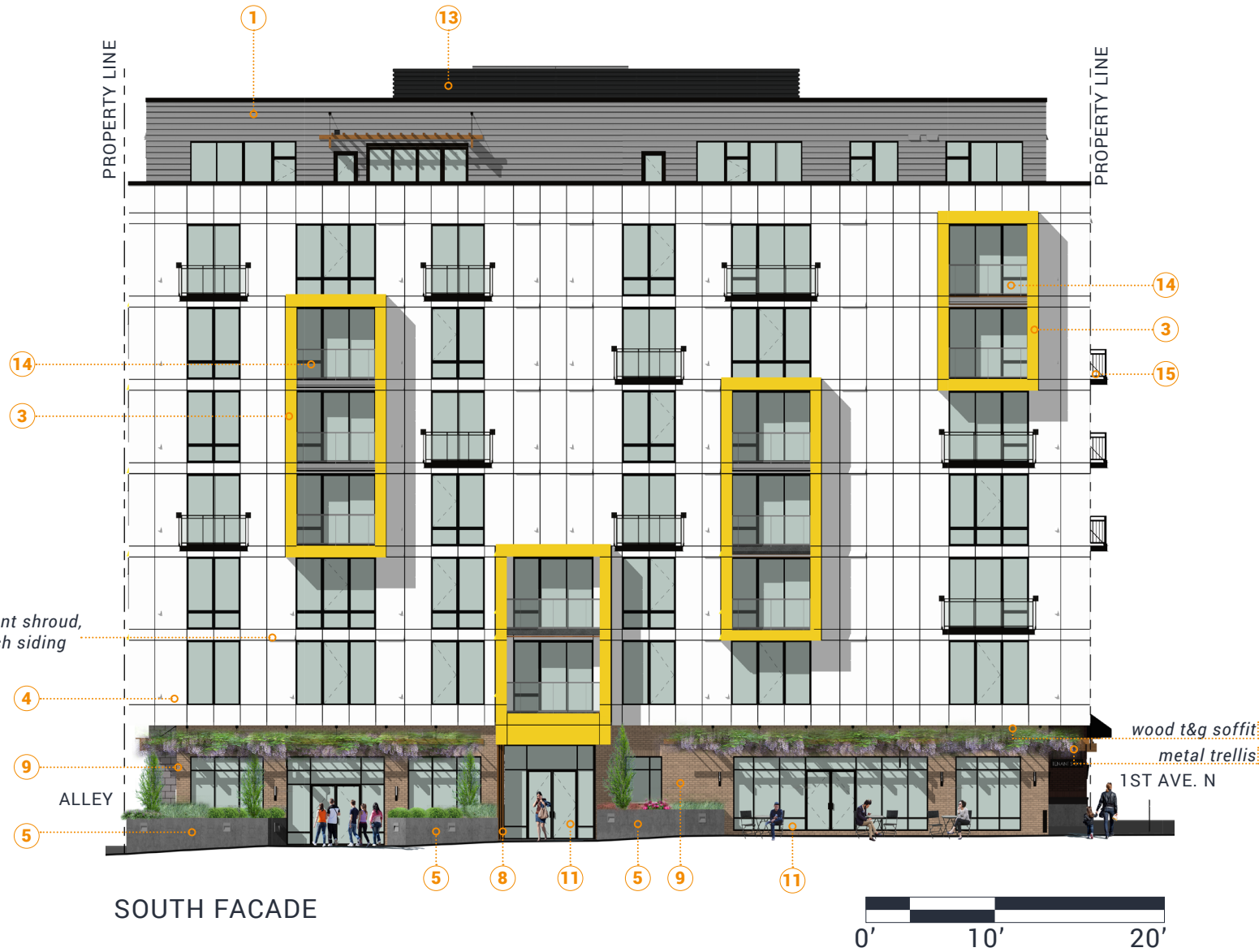
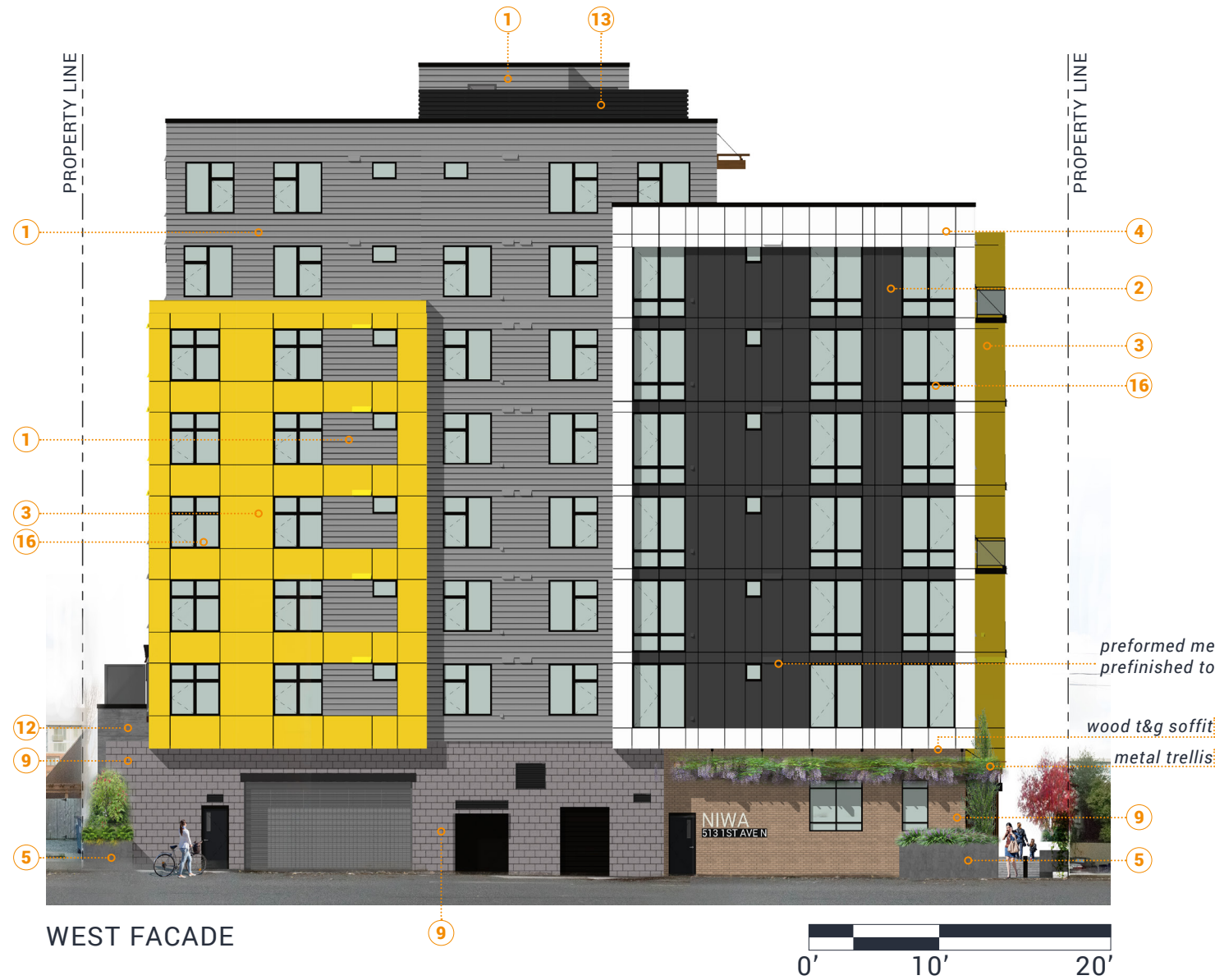
- | | |
|---|---|
| 1 CEMENTITIOUS LAP SIDING
<i>horizontal smooth, james hardie gray slate</i> | 9 MASONRY VENEER
<i>running bond standard face, mission brick pewter</i> |
| 2 CEMENTITIOUS FIBER PANEL
<i>custom smooth, cembrit solid night gray</i> | 10 MASONRY VENEER
<i>running bond standard face, mission brick mauna loa</i> |
| 3 PHENOLIC PANEL
<i>custom panel, stonewood sunray</i> | 11 KAWNEER STOREFRONT
<i>dark bronze</i> |
| 4 PHENOLIC PANEL
<i>custom smooth, stonewood white 5022-AB</i> | 12 CAST-IN-PLACE CONCRETE
<i>light gray</i> |
| 5 BOARD FORMED CONCRETE
<i>light gray</i> | 13 MECHANICAL SCREENING
<i>metal louver</i> |
| 6 CONCRETE MASONRY UNIT
<i>running bond ground face, natural ground face</i> | 14 GLASS BALCONY RAILING
<i>infinity glass with metal top rail</i> |
| 7 CAST-IN-PLACE CONCRETE
<i>dark gray</i> | 15 PICKET BALCONY RAILING
<i>custom, dark bronze</i> |
| 8 WOOD T&G PANEL
<i>woodform cedar</i> | 16 VINYL WINDOW
<i>dark bronze</i> |



NORTH FACADE



ELEVATION + MATERIAL PALETTE



- 1

CEMENTITIOUS LAP SIDING
horizontal smooth, james hardie gray slate
- 2

CEMENTITIOUS FIBER PANEL
custom smooth, cembrit solid night gray
- 3

PHENOLIC PANEL
custom panel, stonewood sunray
- 4

PHENOLIC PANEL
custom smooth, stonewood white 5022-AB
- 5

BOARD FORMED CONCRETE
light gray
- 6

CONCRETE MASONRY UNIT
running bond ground face, natural ground face
- 7

CAST-IN-PLACE CONCRETE
dark gray
- 8

WOOD T&G PANEL
woodform cedar
- 9

MASONRY VENEER
running bond standard face, mission brick pewter
- 10

MASONRY VENEER
running bond standard face, mission brick mauna loa
- 11

KAWNEER STOREFRONT
dark bronze
- 12

CAST-IN-PLACE CONCRETE
light gray
- 13

MECHANICAL SCREENING
metal louver
- 14

GLASS BALCONY RAILING
infinity glass with metal top rail
- 15

PICKET BALCONY RAILING
custom, dark bronze
- 16

VINYL WINDOW
dark bronze



VIEW FROM THE HALF-BLOCK THROUGH CONNECTION



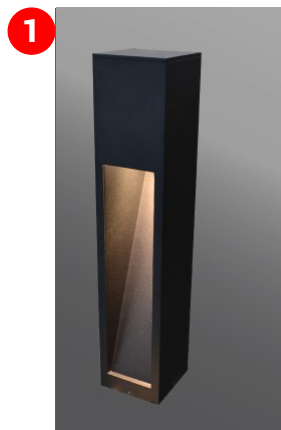
VIEW FROM THE SOUTHEAST



VIEW OF THE MID-BLOCK PASSAGE



SITE PLAN



BOLLARD



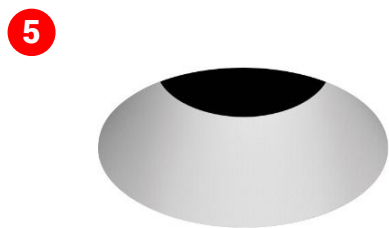
WALL / PLANTER LIGHT @ ALLEY



WALL SCONCE



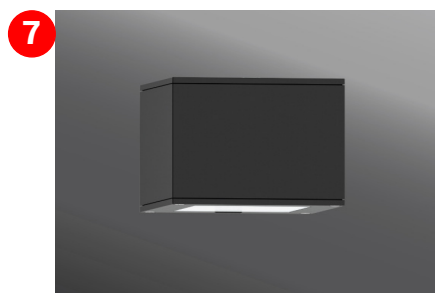
LINEAR LED @ TRELLIS



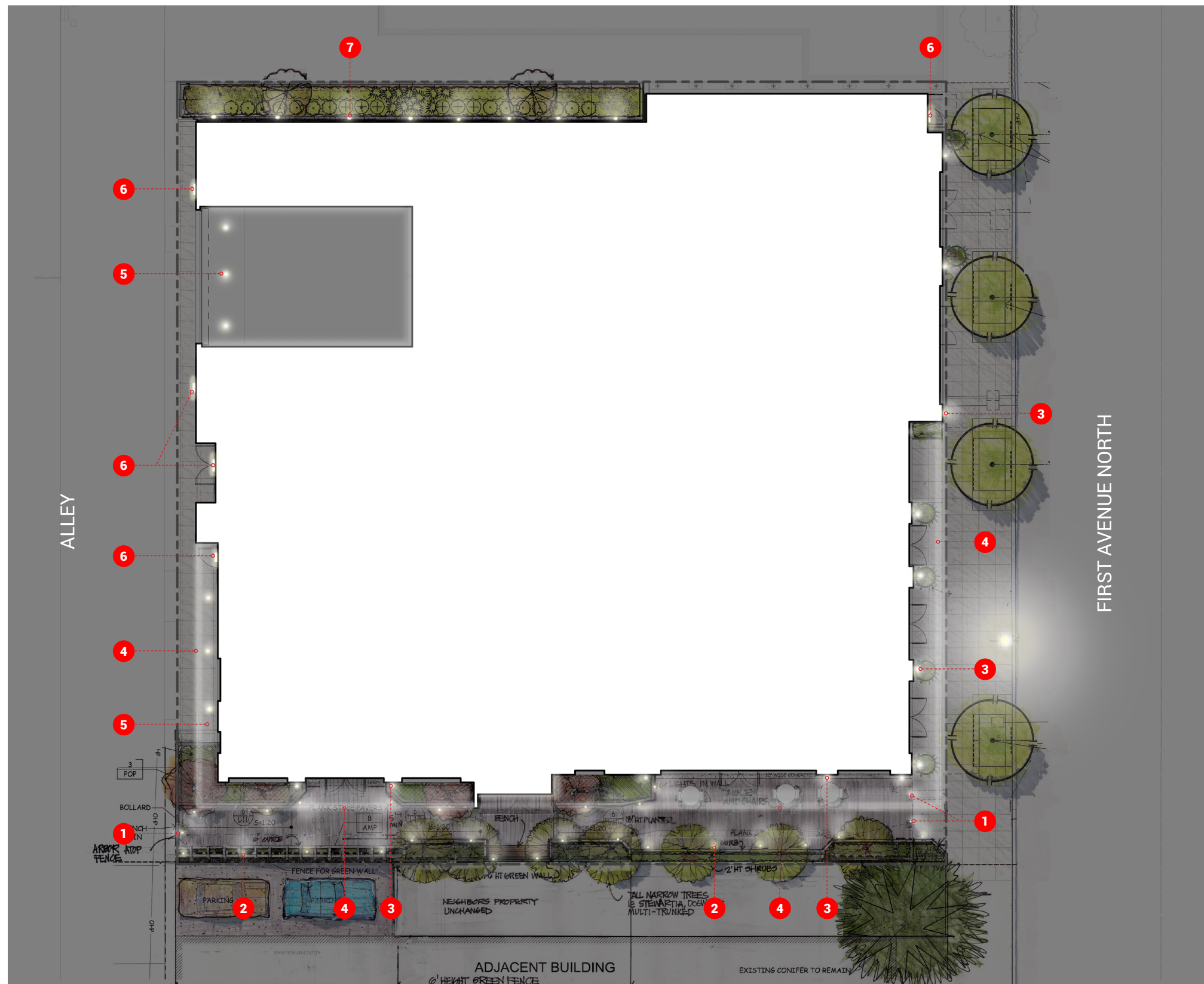
RECESSED DOWN LIGHT



MOUNTED WALL LIGHT



WALL SCONCE @ PLANTER



DESIGN DEVELOPMENT
NIGHT PERSPECTIVE OF MID BLOCK



DESIGN DEVELOPMENT

SIGNAGE LOCATIONS



ILLUMINATED SIGNAGE



PLAQUE SIGNS



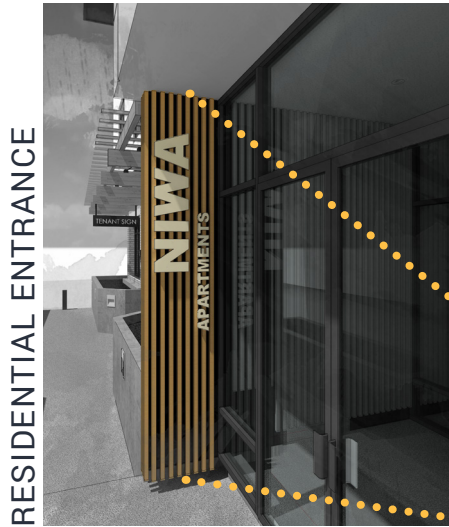
BLADE SIGNS



GLASS
SIGNS



CANOPY SIGN

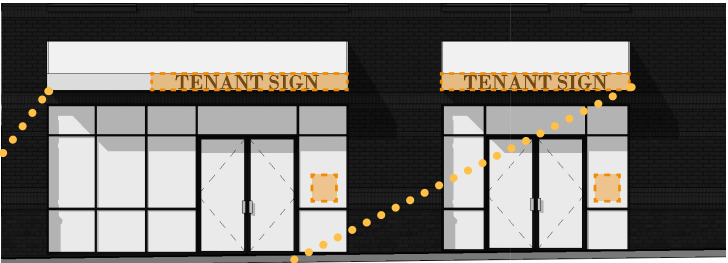


Illuminated building signage will be located on the horizontal wood panel wall at the residential lobby entry.

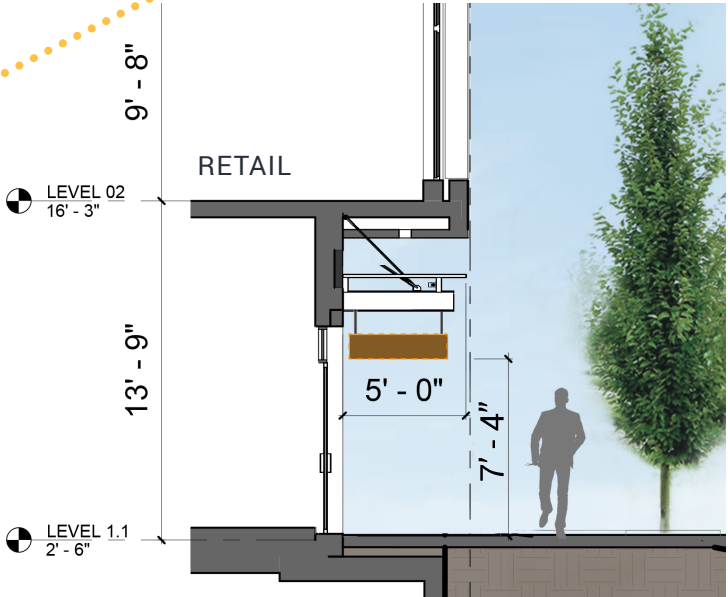
In addition to the blade signage, storefront glazing films will be incorporated at the door and/or near the entryway for a pedestrian scaled signage.



SOUTHEAST VIEW ALONG 1ST AVE

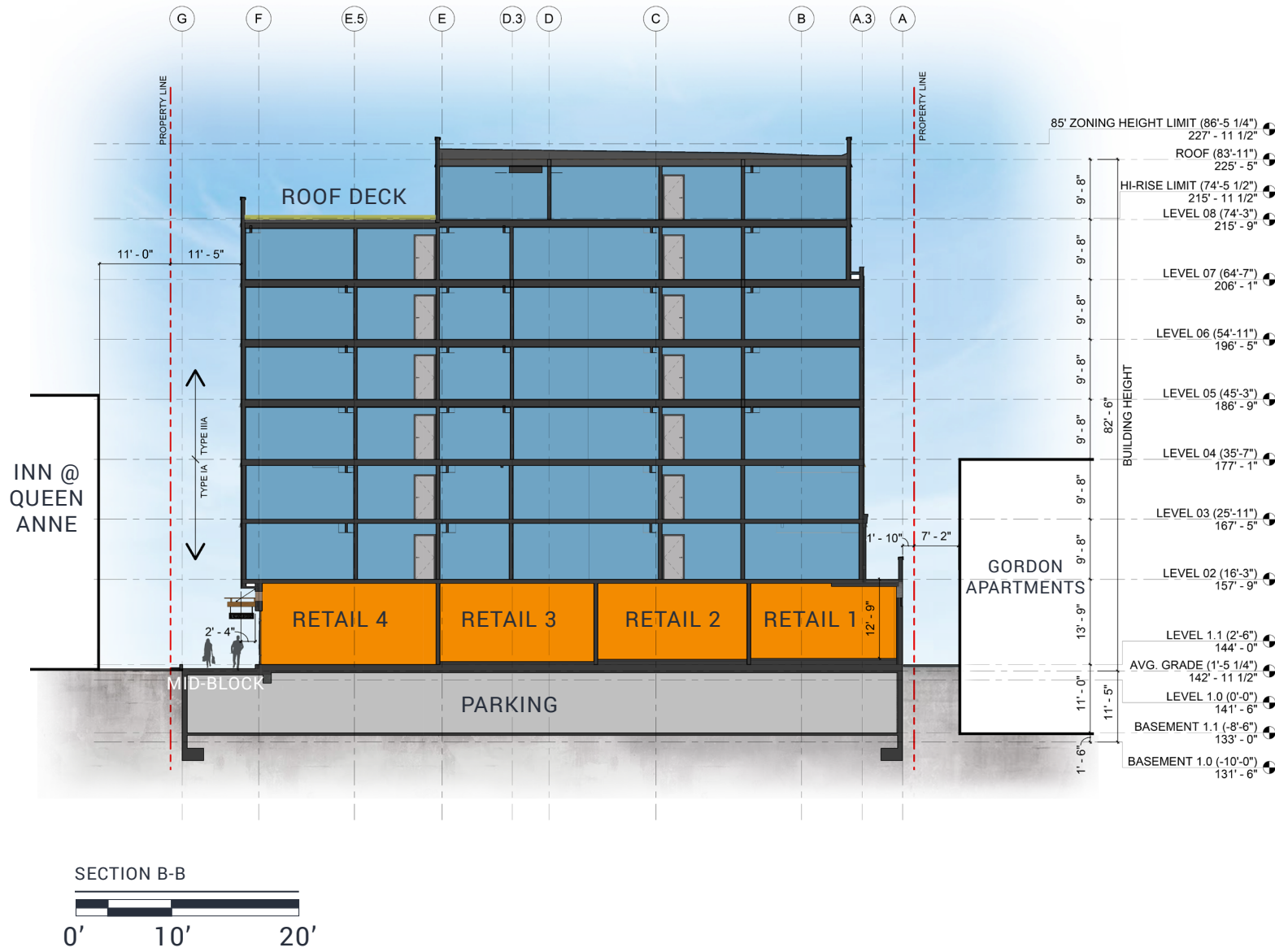
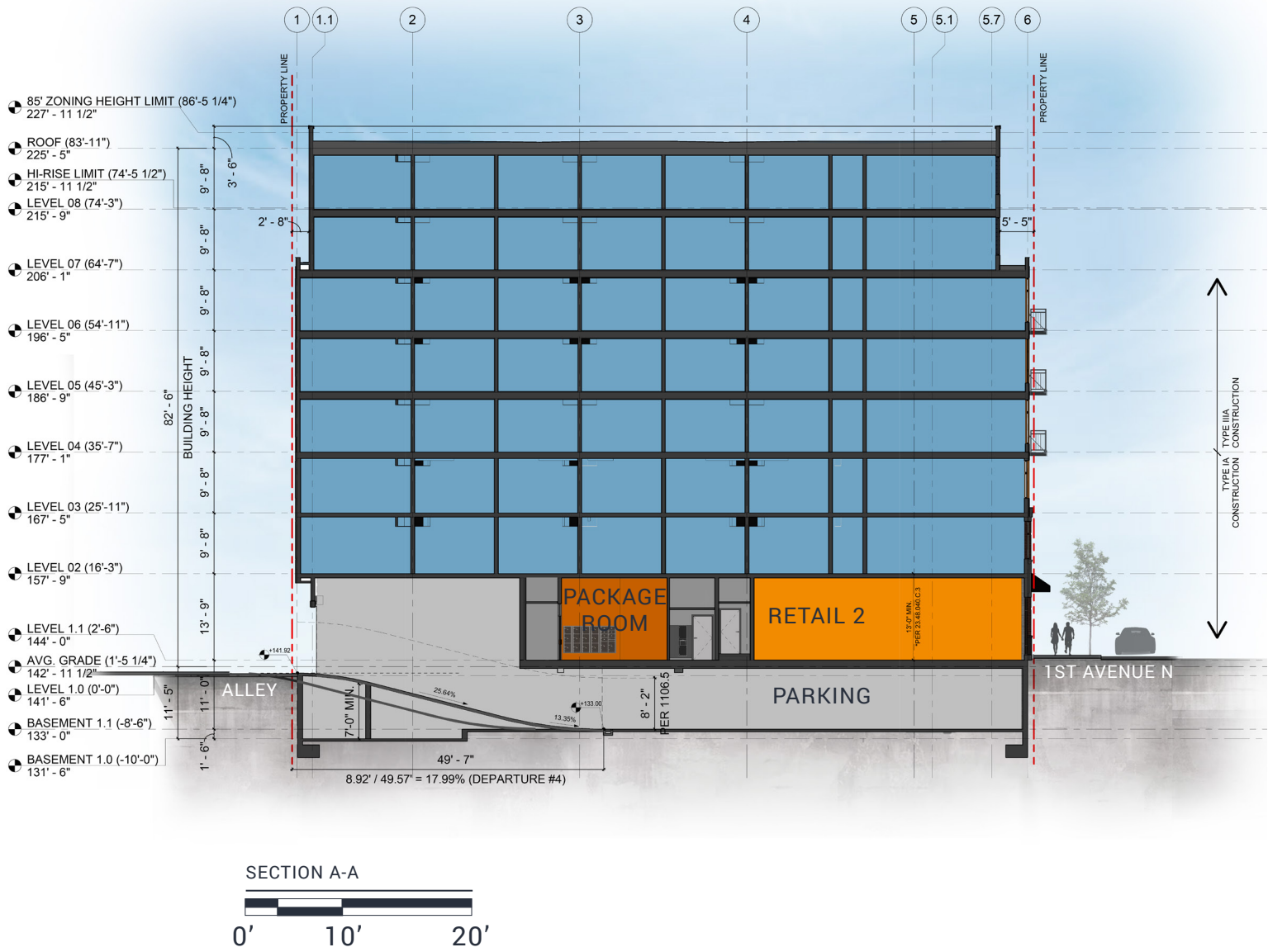


EAST FABRIC CANOPY SIGNAGE



SIGNAGE HEIGHT @ RETAIL

BUILDING SECTIONS
EAST - WEST SECTION



WINDOW BAYS SOUTH FACADE

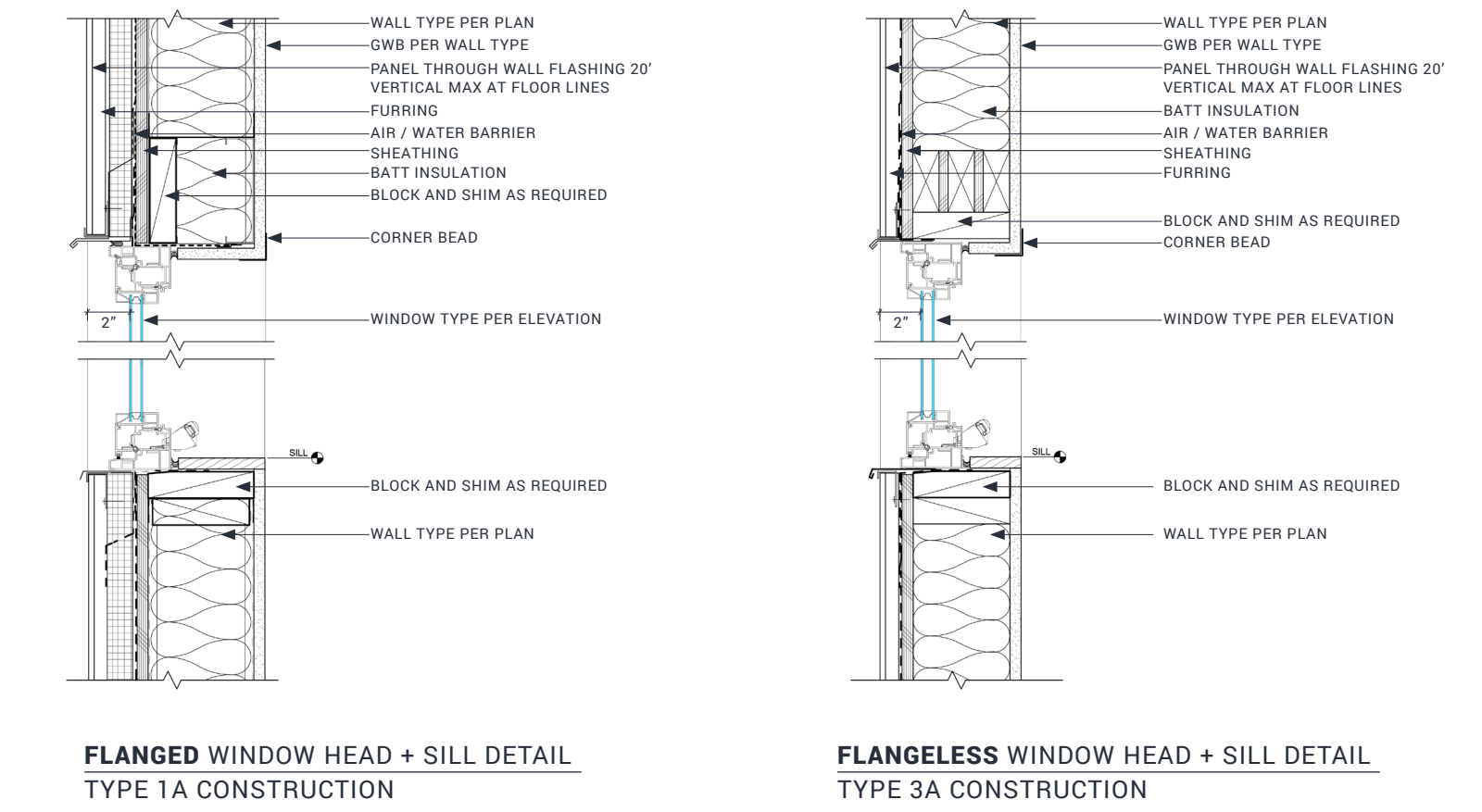


SOUTH ELEVATION FOR REFERENCE



WINDOW BAY DEPTH [2 AND 3 SHOWN]
window bay 1 and 4 similar

SECTION PERSPECTIVE
EAST STOREFRONT



EAST ELEVATION



DEPARTURES

DEPARTURE #1

23.48.735 - UPPER LEVEL SETBACK REQUIREMENTS IN SM-UP ZONES

CODE REQUIREMENT

A. In all SM-UP zones, any portion of a structure greater than 45 feet in height or 65 feet in height must be set back from a lot line that abuts a designated street shown on Map A for 23.48.735. A setback of an average of 10 feet from the lot line is required for any portion of a structure exceeding the maximum height that is permitted without a setback.

Required setback of 10 feet above 65 feet per Map A for 23.48.735 along 1st Avenue North.

REQUESTED DEPARTURE

Request the required 10 foot setback above 65 feet on the 1st Avenue North facade be vacated.

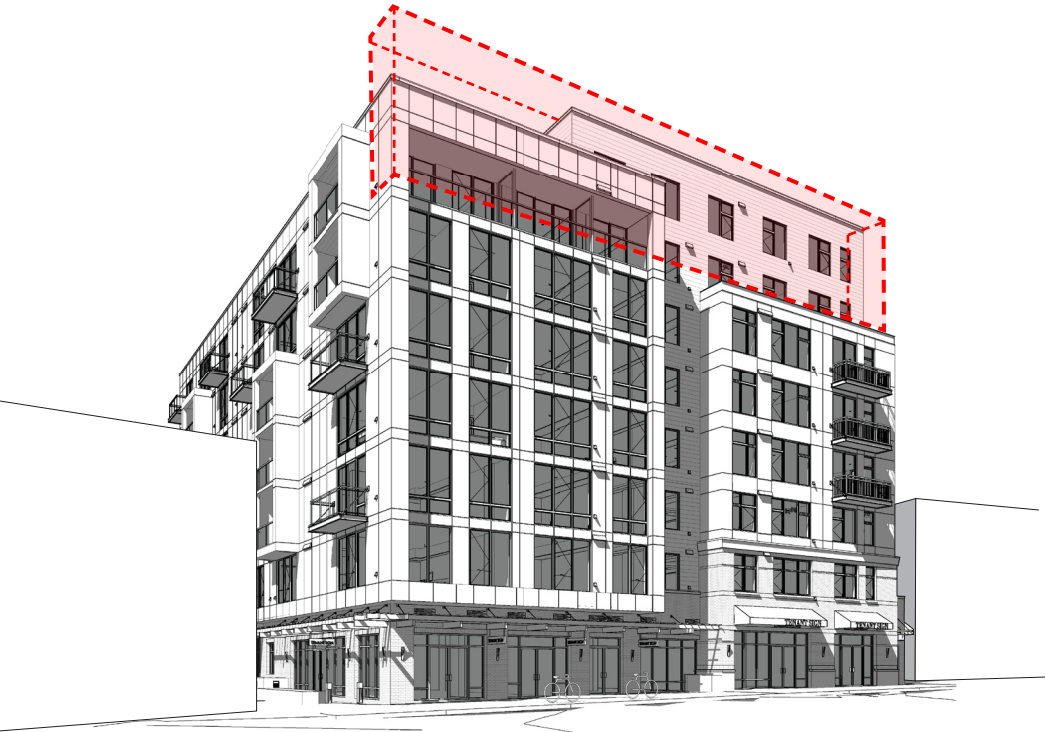
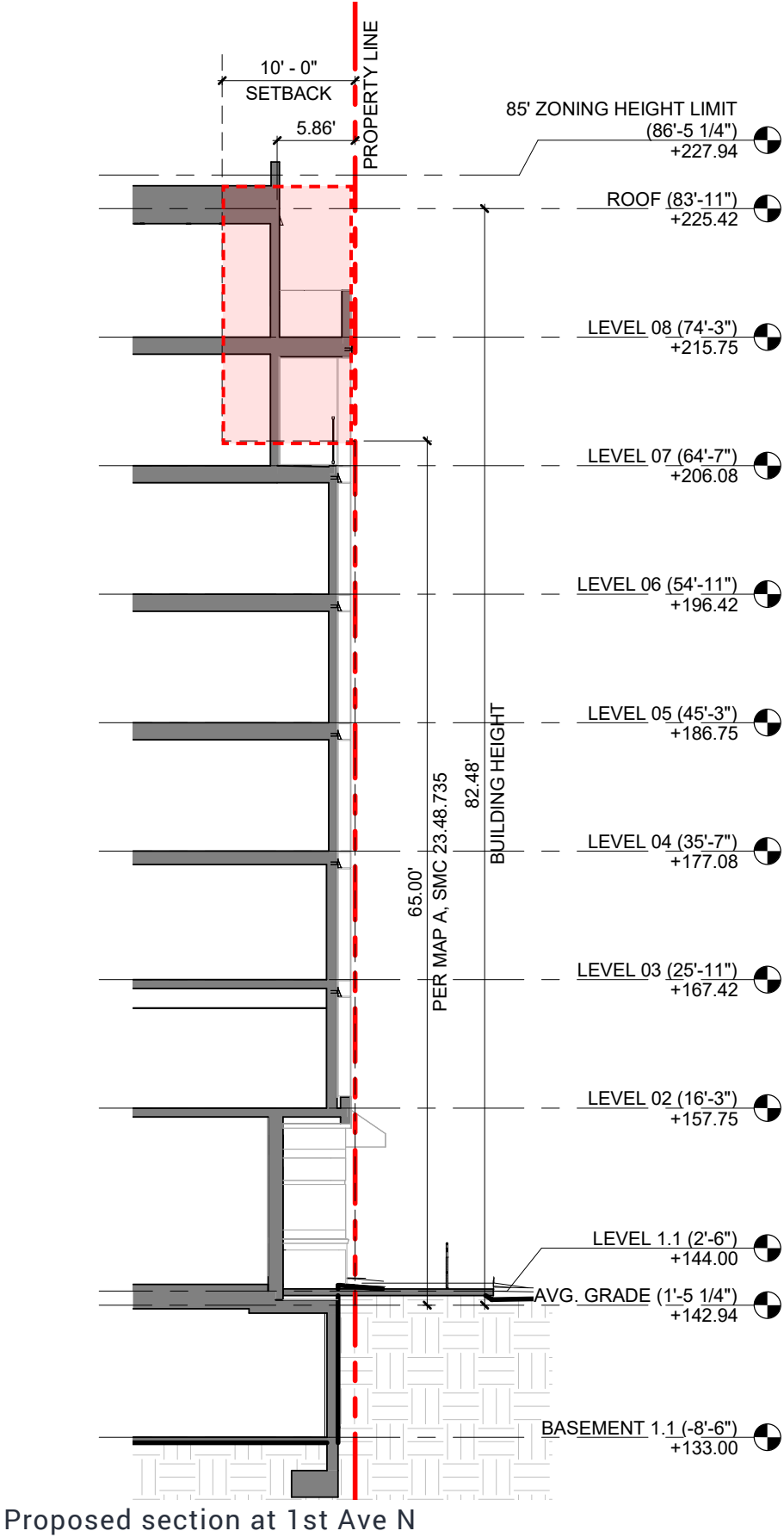
RATIONALE

This departure allows for the two distinct masses to step back at different elevations, which will help break down the large mass of the building along 1st Avenue North (CS2-IV-ii). The north mass is set back 5.86 feet above 64 feet, while the south mass continues up at the property line to an elevation of 74 feet to create more visual interest and clearly communicate that the two masses are distinct (CS2-IV-ii).

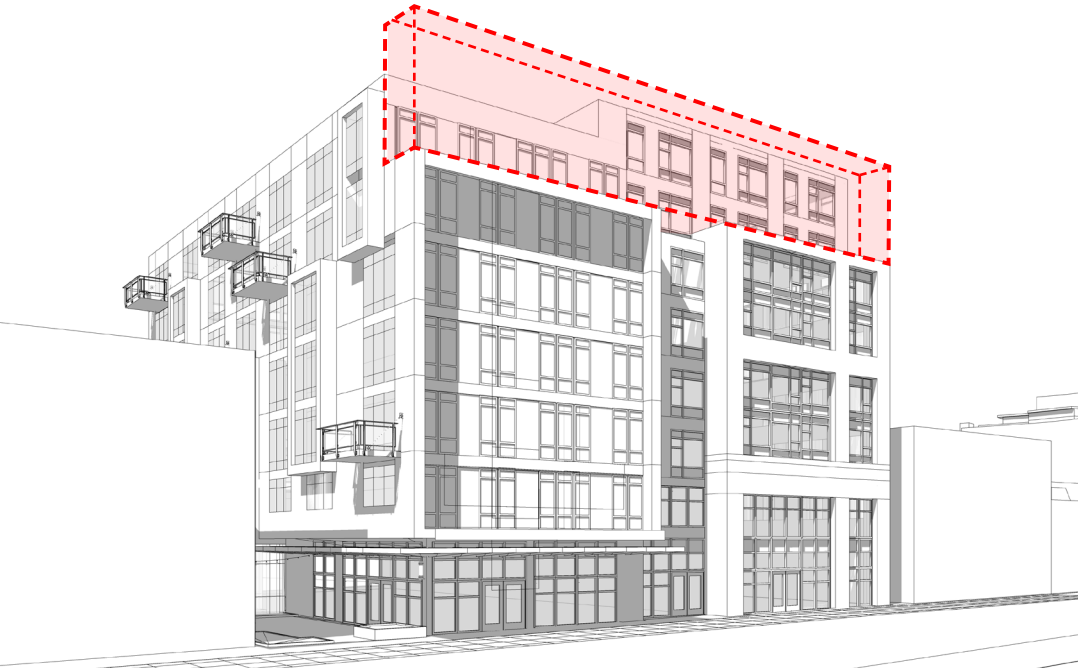
This departure will improve the overall appearance of the building from 1st Avenue North and provide clarity in the design intent.

UPTOWN DESIGN GUIDELINES CITED:

- CS2-IV-ii Facade Composition
- CS2-IV-i Setback of Top Floor

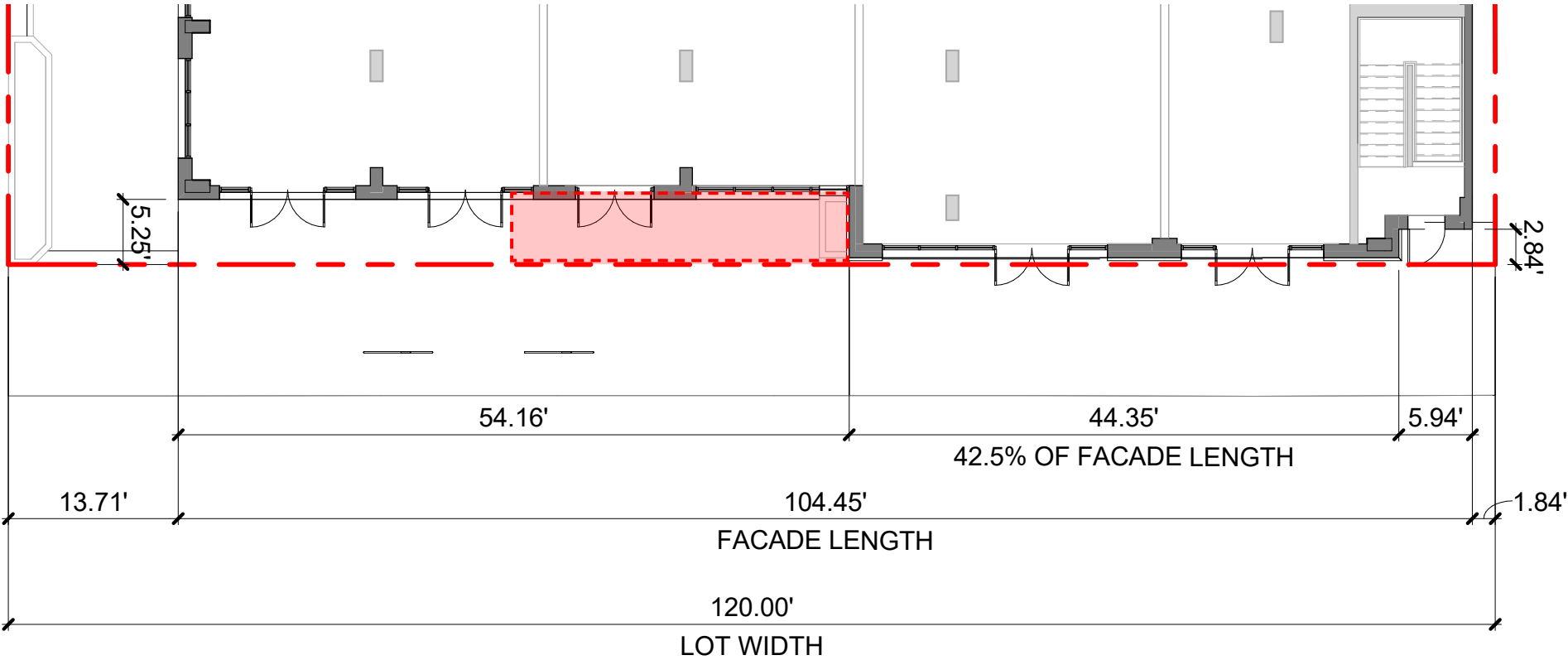


Proposed Massing



Code-Compliant Massing (EDG)

DEPARTURES
DEPARTURE #2



Proposed 1st Floor Plan at 1st Ave N

23.48.740 - STREET LEVEL DEVELOPMENT STANDARDS IN SM-UP ZONES

CODE REQUIREMENT

A. Street-level facade requirements; setbacks from street lot lines
Street-facing facades of a structure are must be built to the lot line except as follows:

- 1. The street-facing facades of structures abutting Class 1 Pedestrian Streets, as shown on Map A for 23.48.740, shall be built to the street lot line for a minimum of 70 percent of the facade length, provided that the street frontage of any required outdoor amenity area, other required open space, or usable open space provided in accordance with subsections 23.48.740.B and 23.48.740.C is excluded from the total amount of frontage required to be built to the street lot line.

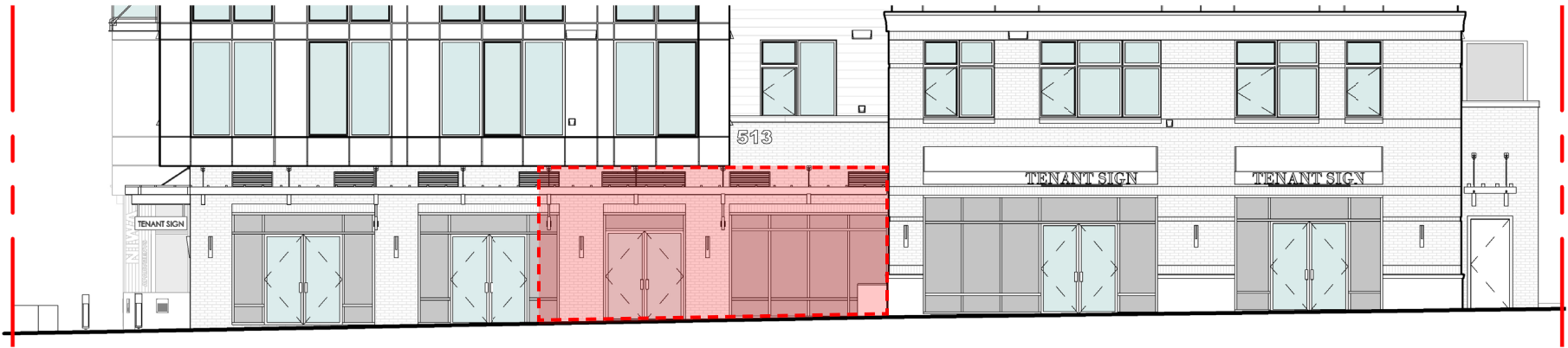
REQUESTED DEPARTURE

Request the 70% facade length on street line requirement be reduced on the 1st Avenue N (east) facade to allow for 42% of facade length.

RATIONALE

This Departure allows for a more pleasing composition of the facade, providing variation in depth of massing of the building (CS2-IV-ii), creating a wider sidewalk space for a more engaged, friendly pedestrian experience (CS2-II-iii) as well as opening up and helping to create relief drawing attention to the mid-block through passage and pedestrian activity in this space.

UPTOWN DESIGN GUIDELINES CITED:
CS2-IV-ii Facade Composition
CS2-II-iii Widened Sidewalk



Proposed 1st Ave N (East) Street Elevation

DEPARTURES

DEPARTURE #3

23.48.040 - STREET LEVEL DEVELOPMENT STANDARDS

CODE REQUIREMENT

A. Street-facing facade requirements. The following street-facing facade requirements apply to facades facing a Class 1, Class 2, or Class 3 Pedestrian Street, Neighborhood Green Streets, and all other streets, as shown on Map A for 23.48.240, Map A for 23.48.440, or Map A for 23.48.740:

- 1. Primary pedestrian entrance. In the SM-SLU, SM-NR, and SM-UP zones, each new structure facing a Class 1 Pedestrian Street is required to provide a primary building entrance for pedestrians from the street or a street-oriented courtyard that is no more than 3 feet above or below the sidewalk grade.

REQUESTED DEPARTURE

Request allowance for the primary pedestrian entrance be located on south facade, on mid-block through passage. Retail entrances to be from Class I street (1st Avenue North) and from mid-block through passage, less than 3 feet below sidewalk grade.

RATIONALE

This departure allows for more active and engaging retail spaces and more retail frontage along the Class I pedestrian street. (PL3-I, PL3-I-ii) it also provides a more appropriate human scaled entrance for the residences and activates the mid-block through passage between the street and alley (DC1-VI-ii, DC2-III-ii).

UPTOWN DESIGN GUIDELINES CITED:

- PL3-I Human Activity
- PL3-I-ii Retail Spaces
- DC1-VI-ii Activation on the “alley” or mid-block through passage
- DC2-III-ii Proportioned Design

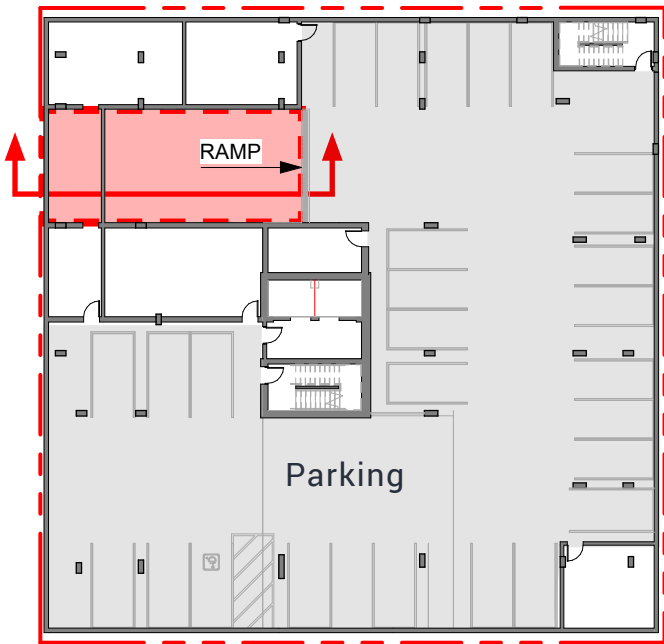
- ➡ Primary Access (1st Ave N)
- ➡ Secondary Access (Alley)



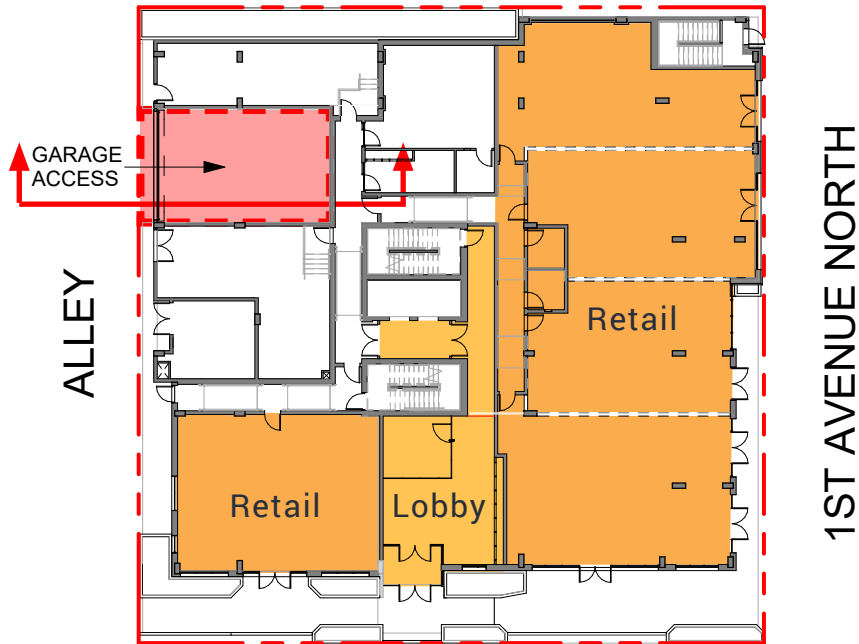
Proposed Site Plan

DEPARTURES

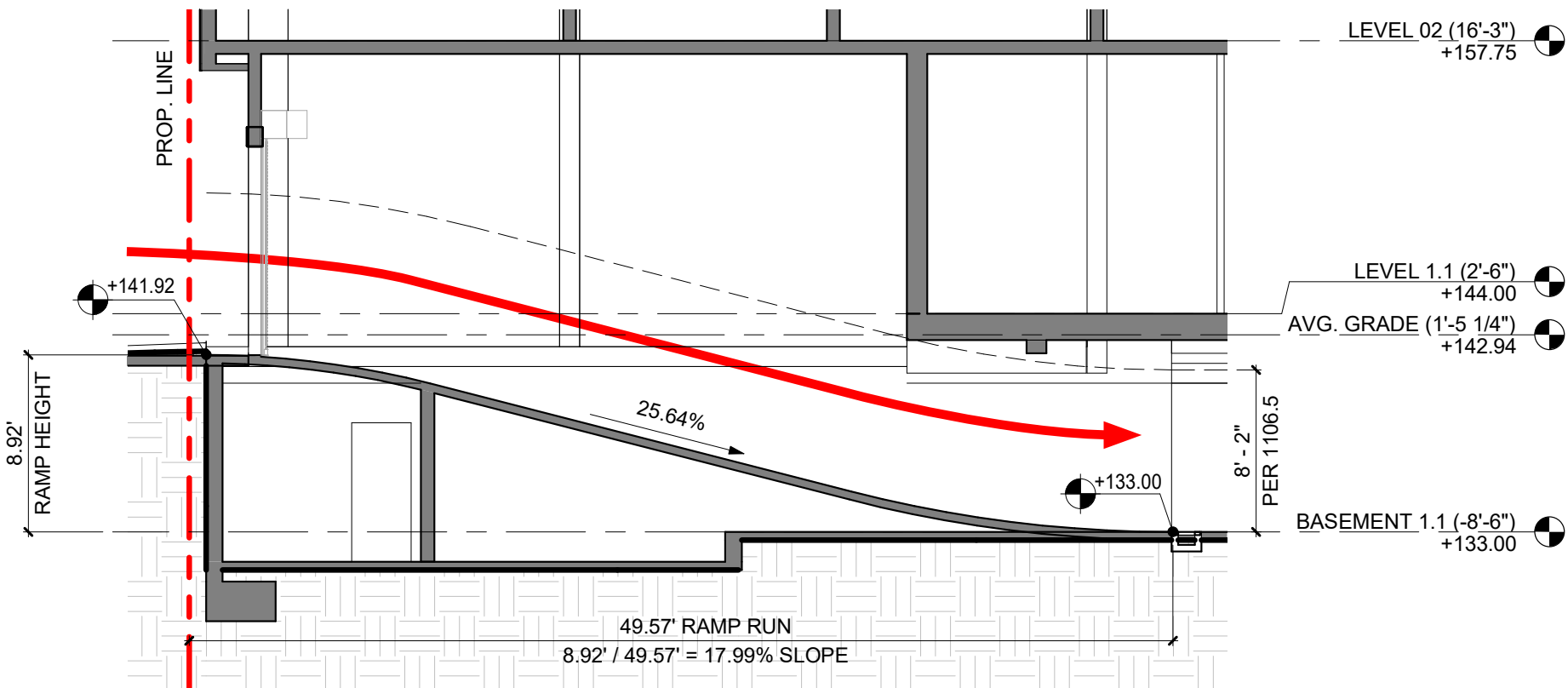
DEPARTURE #4



Basement Plan



First Floor plan



Proposed Garage Ramp Section

23.54.030 - PARKING SPACE AND ACCESS STANDARDS

CODE REQUIREMENT

- A. Driveways
3. Driveway slope for all uses. No portion of a driveway, whether located on a lot or on a right-of-way, shall exceed a slope of 15 percent, except as provided in this subsection 23.54.030.D.3. The maximum 15 percent slope shall apply in relation to both the current grade of the right-of-way to which the driveway connects, and to the proposed finished grade of the right-of-way if it is different from the current grade. The ends of a driveway shall be adjusted to accommodate an appropriate crest and sag. The Director may permit a driveway slope of more than 15 percent if it is found that:
- a. The topography or other special characteristic of the lot makes a 15 percent maximum driveway slope infeasible;
 - b. The additional amount of slope permitted is the least amount necessary to accommodate the conditions of the lot; and
 - c. The driveway is still useable as access to the lot.

REQUESTED DEPARTURE

Request the 15% max driveway slope requirement be waived to allow for 18% average slope, with a maximum of 25.64%

RATIONALE

The ramp location on the site was selected to maximize vehicle parking and allow for functional commercial and amenity space along the south property line. Due to the sloping topography and narrow width of the site a ramp at the required 15% grade would be infeasible as it would require significant floor area on the basement level. At the first floor, due to required overhead clearance to allow for accessible van access circulation and retail depth would be negatively impacted by a 15% slope ramp.

Since parking is being provided for resident convenience and not required by code we ask for a waiver of the 15% maximum slope requirement. A ramp with an average slope of 18% and maximum of 25.64% is the safest slope that will allow for better circulation and flow on the first floor of the building.