



OLYMPIC TOWER

EARLY DESIGN GUIDANCE
EAST DESIGN REVIEW BOARD
20 DECEMBER 2017
DPD #3028357



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PROJECT INFORMATION

ADDRESS: 715 8TH AVE. SEATTLE
DPD PROJECT : #3028357

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DEVELOPMENT OBJECTIVES

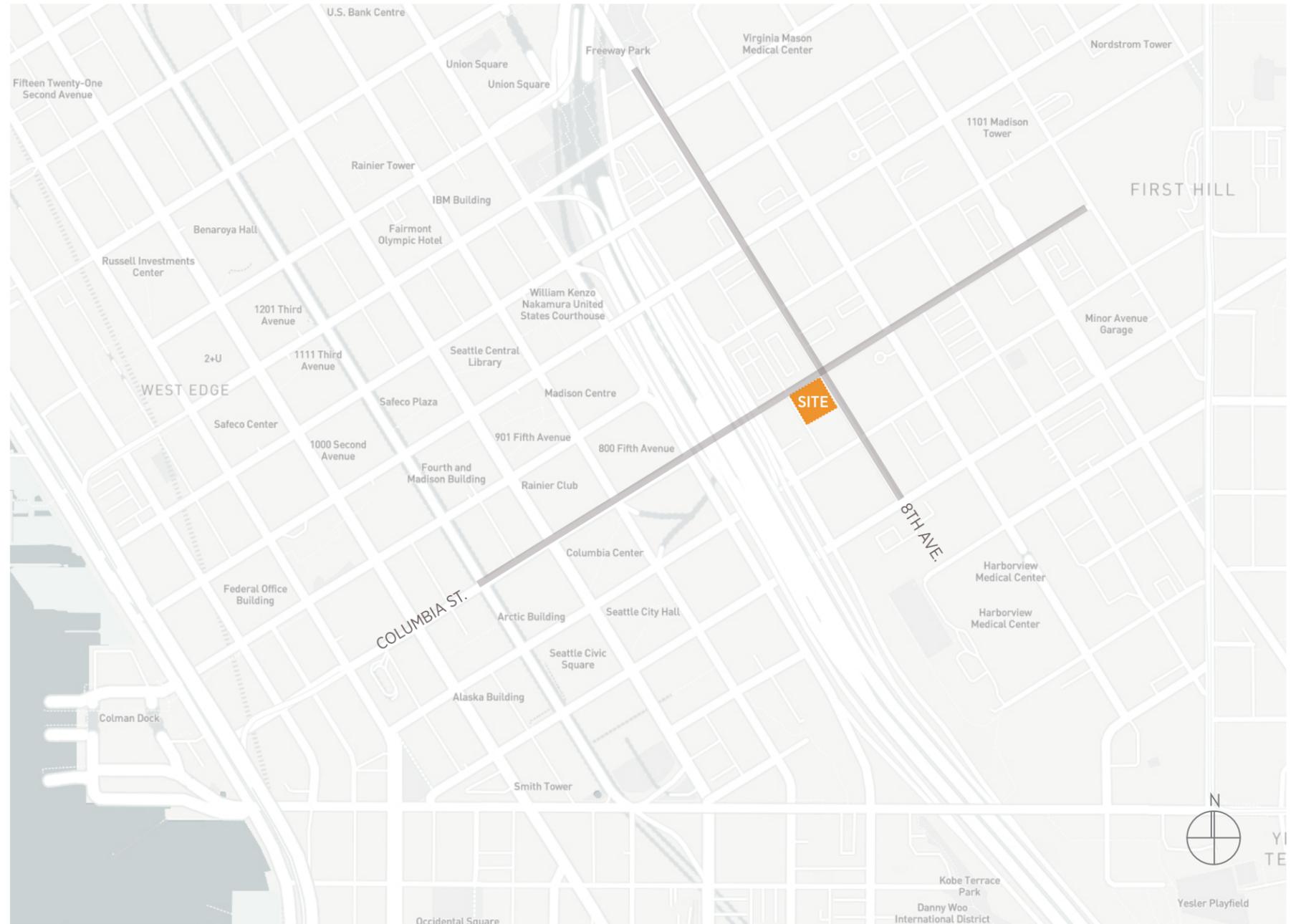
The proposed project is a 21-story independent senior living community with underground parking.

The basic program includes:

- 77 residential units
- 186,127 SF gross square footage
- 23,000 SF amenity area
- 76 parking stalls

PROJECT GOALS

Create a vibrant new senior living community that contributes to the existing Skyline residences in First Hill. Provide safe connections to the existing community and respond to the scale, context and character of the neighborhood.



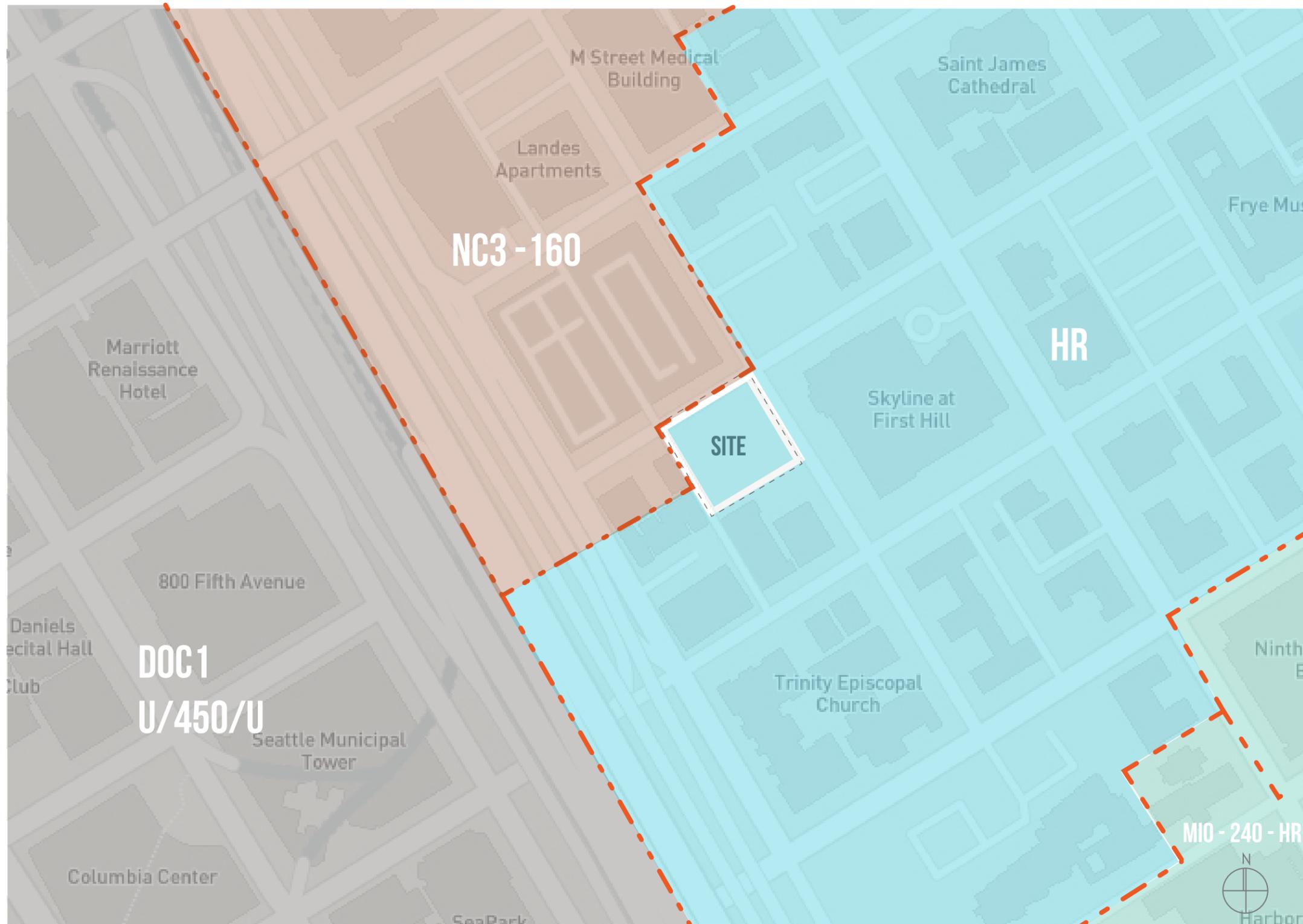
VICINITY MAP

The project is located in the First Hill neighborhood, “a rapidly growing, urban neighborhood with an exciting and diverse mix of business and residents” including residential, medical, historical, and cultural uses. The site is located in the block just above Interstate 5 at the edge of the Commercial Core at the corner of Columbia Street and 8th Avenue.



ZONING DESIGNATIONS

The site is located within the High Rise Zone with the NC3-160 zone adjacent to the north and west. The site is bounded by Columbia Street to the north and 8th Avenue to the east and is also located within the First Hill Urban Center Village.



URBAN DESIGN ANALYSIS

URBAN DESIGN ANALYSIS

TRANSPORTATION:

- Streetcar: Broadway
- Bus lines: 2, 3, 4, 9, 12, 13, 43, 49, 60, 63, 64, 193, 303, 309, 630

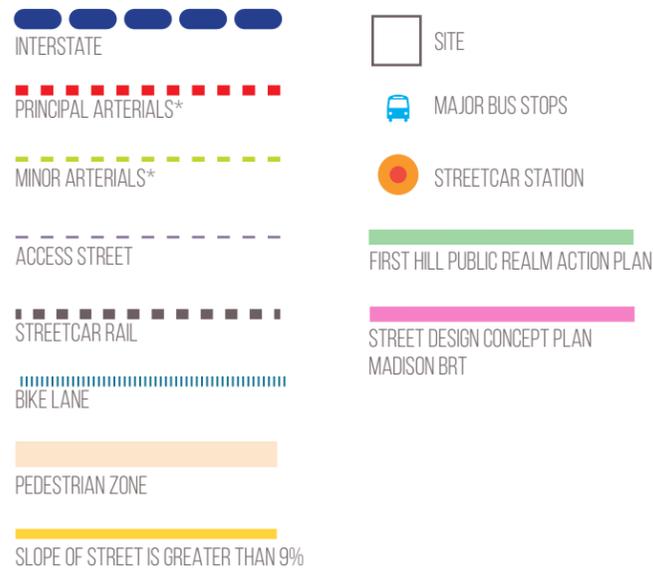
STREET CHARACTERISTICS:

8TH AVENUE:

- Access Street - provides access from principal arterials
- Narrow street mostly used by local residents
- Neighborhood Greenway designation - pedestrian friendly, large amount of tree canopy coverage

COLUMBIA STREET:

- Access Street - provides access from minor arterials
- Narrow, cobblestone street used mostly by local residents
- Steeply sloped - 15% grade



*Per Seattle Arterial Classifications Planning Map
 **Per SDOT Seattle Transit Master Plan

PARK TO PARK PATH: WALKING LOOP

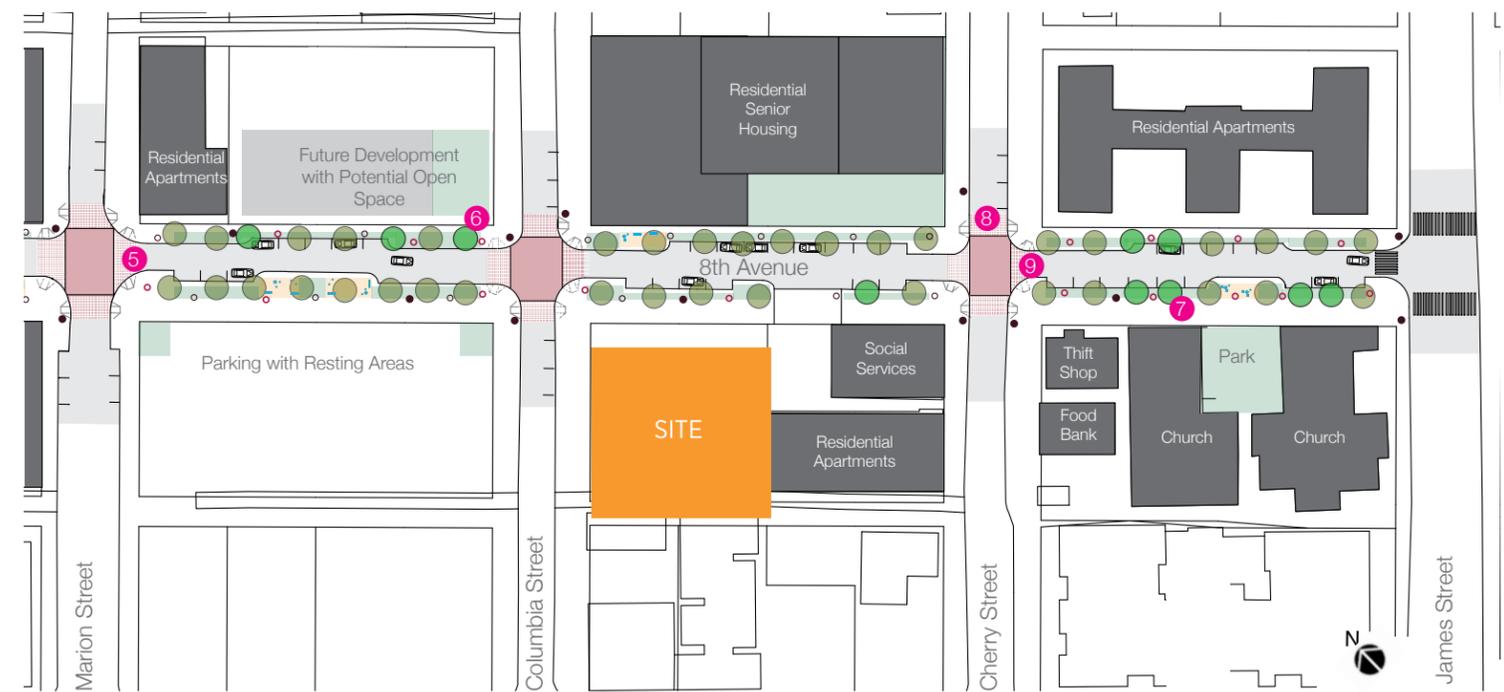
“First Hill remains short of our parks and open space goals by several acres, making every square foot of the public realm and important part of creating a healthy, green, and lush environment for all.” The Park to Park Path aims to connect Freeway Park with the soon to be completed Yesler Terrace Park along neighborhood greenways “creating a beautiful and safe walking route” which showcases the many amenities unique to the neighborhood.



*From First Hill Improvement Association

FIRST HILL PUBLIC REALM ACTION PLAN: 8TH AVENUE

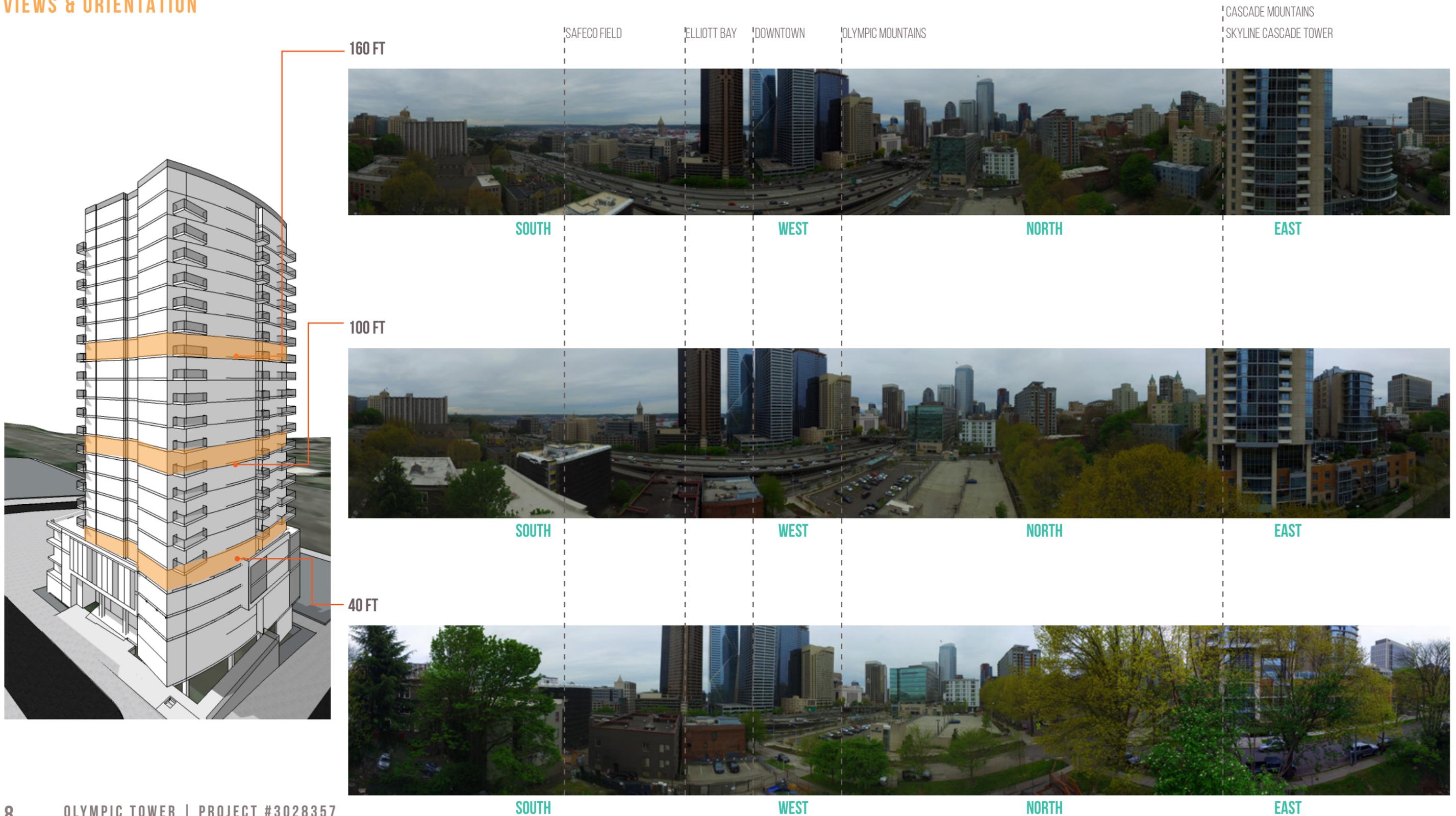
In 2015, the First Hill Public Realm Action Plan was adopted by the City of Seattle as a new approach to consider the right of way as a way to connect parks and public space throughout First Hill to serve working and residential populations in the neighborhood. In the Plan 8th Avenue is designated as a Neighborhood Greenway, a residential street with low volume of slower-moving vehicles which integrates a series of design elements that enable pedestrians and cyclists to feel safe and comfortable.



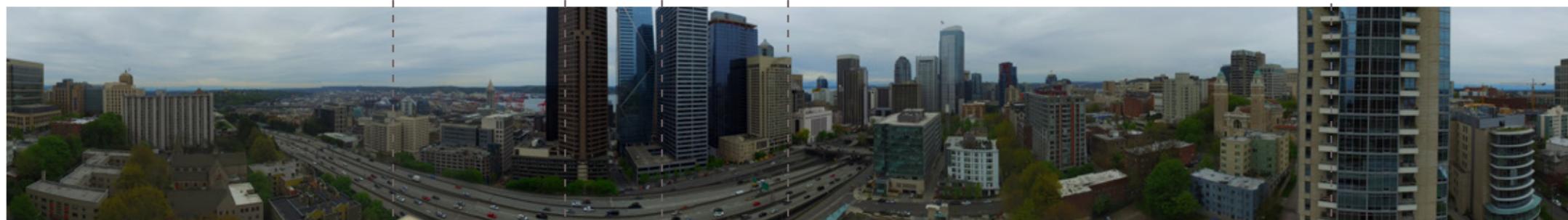
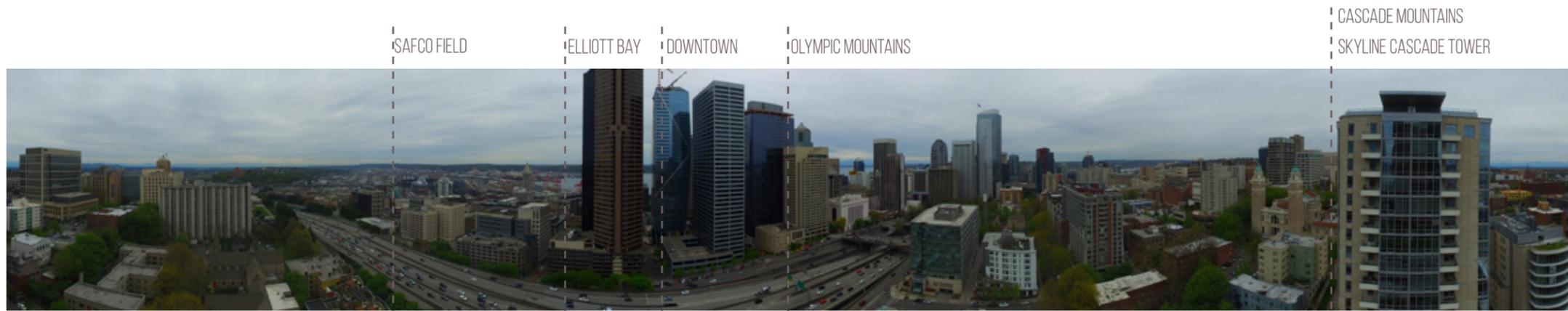
- 5 22' street at intersection throughout
 - 6 Enhanced landscape and extended sidewalk plantings
 - 7 Additional trees of similar species to fill canopy
 - 8 Painted and textured crosswalks and ADA ramps at all intersections
 - 9 ADA accessible elements consistent throughout street
- Existing Street Trees
 - Existing Streetlights
 - Existing Pedestrian Streetlights
 - Proposed Street Trees
 - Proposed Pedestrian Streetlights
 - Active Zones

*From First Hill Public Realm Action Plan

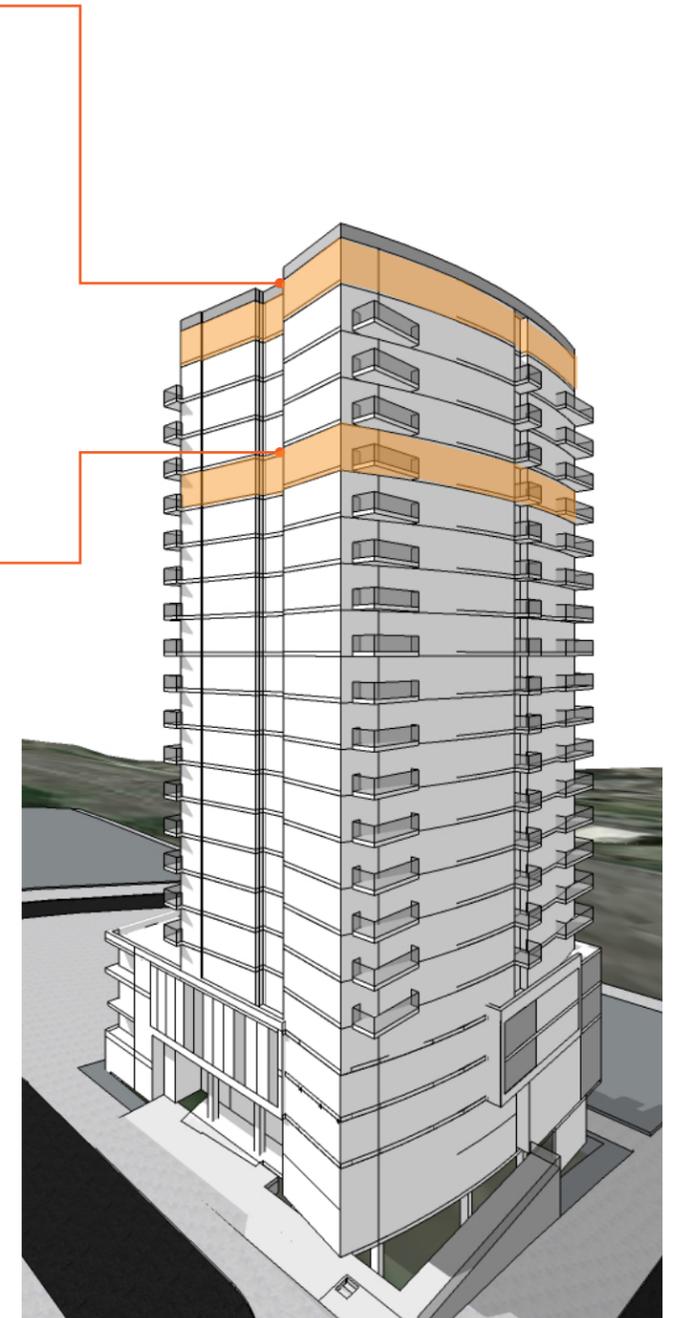
VIEWS & ORIENTATION



VIEWS & ORIENTATION



200 FT



URBAN DESIGN ANALYSIS

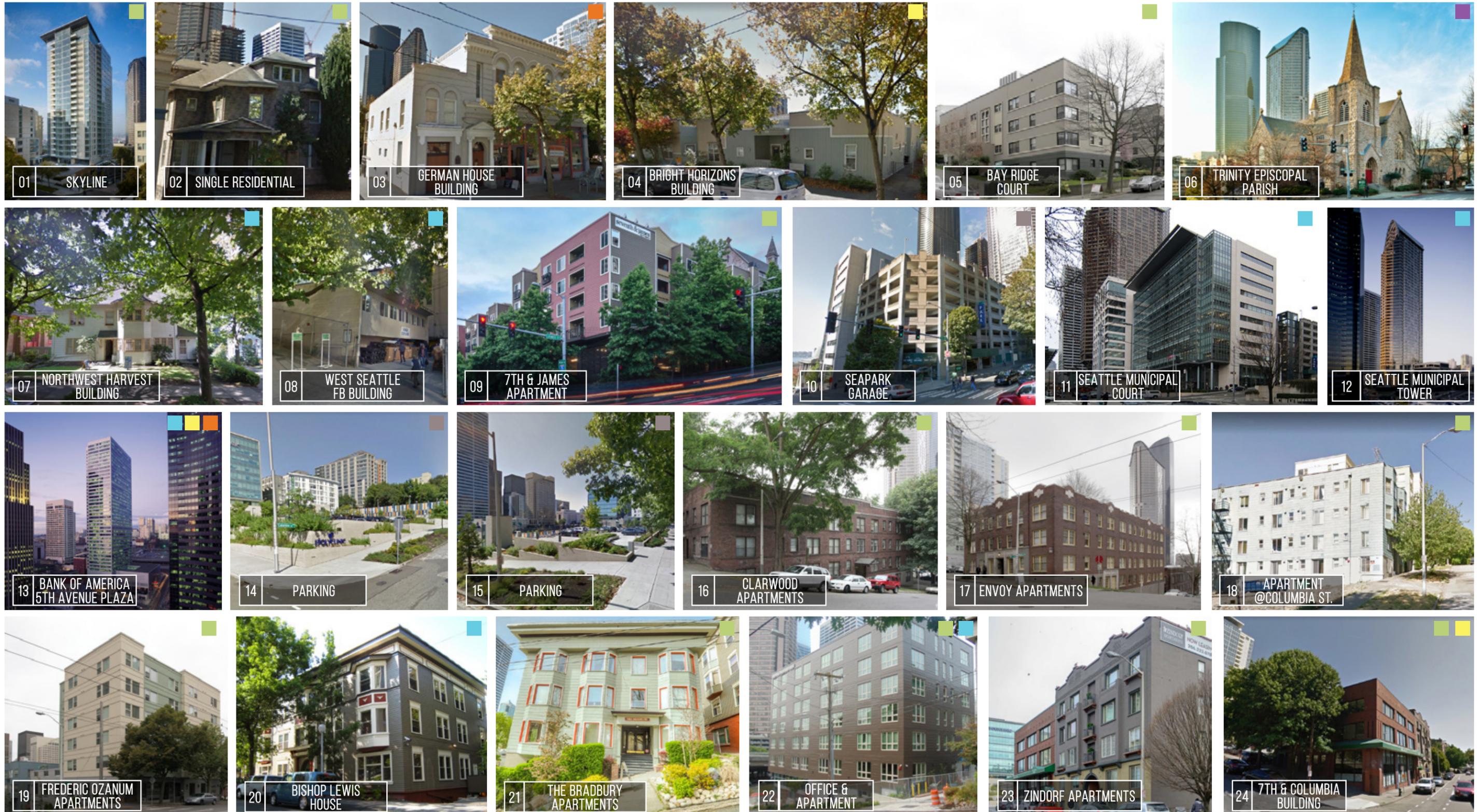
SURROUNDING USES & STRUCTURES

1. SKYLINE
2. SINGLE RESIDENTIAL
3. GERMAN HOUSE BUILDING
4. BRIGHT HORIZONS BUILDING
5. BAY RIDGE COURT
6. TRINITY EPISCOPAL PARISH
7. NORTHWEST HARVEST BUILDING
8. WEAT SEATTLE FB
9. 7TH AND JAMES APARTMENT
10. SEAPARK GARAGE
11. SEATTLE MUNICIPAL COURT
12. SEATTLE MUNICIPAL TOWER
13. BANK OF AMERICA FIFTH AVENUE PLAZA
14. PARKING
15. PARKING
16. CLARWOOD APARTMENTS
17. ENVOY APARTMENTS
18. APARTMENT AT COLUMBIA ST.
19. FREDERIC OZANAM HOUSE
20. BISHOP LEWIS HOUSE
21. THE BRADBURY APARTMENTS
22. UNDER CONSTRUCTION OFFICE & APT.
23. ZINDORF APARTMENTS
24. 7TH & COLUMBIA BUILDING

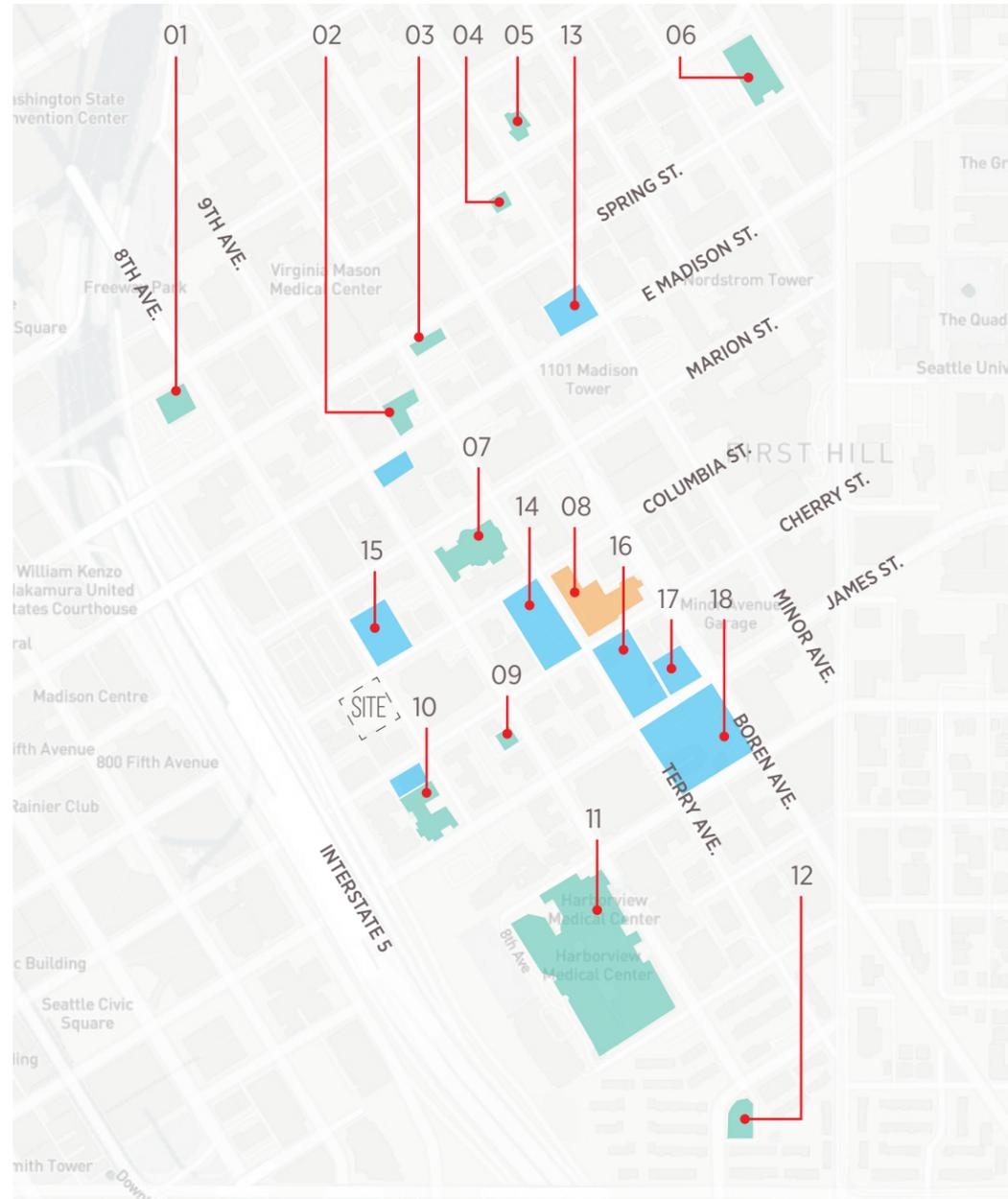
LEGEND

- RESIDENTIAL
- OFFICE
- RETAIL
- RESTAURANT / CAFE
- RELIGIOUS INSTITUTION
- PARKING





LANDMARKS AND NEW DEVELOPMENT



- HISTORIC LANDMARKS
*Per Map of Designated Landmarks
- ARTS AND CULTURE
*Per First Hill Improvement Association
- PROPOSED OR NEW DEVELOPMENT
*Per First Hill Improvement Association



LANDMARKS, ART AND CULTURE



01 - TOWN HALL SEATTLE / FOURTH CHURCH OF CHRIST, SCIENTIST



02 - HOTEL SORRENTO



03 - BARONESS APARTMENT HOTEL



04 - DEARBORN HOUSE



05 - STIMSON / GREEN HOUSE



06 - SEATTLE FIRST BAPTIST CHURCH

PROPOSED/NEW DEVELOPMENT



07 - ST. JAMES CATHEDRAL RECTORY / SITE



08 - FRYE MUSEUM



13 - 1001 MINOR AVE



16 - 620 TERRY AVE



09 - U.S. ASSAY OFFICE / GERMAN HOUSE



10 - TRINITY EPISCOPAL CHURCH



14 - 707 TERRY AVE



17 - 1050 JAMES ST



11 - HARBORVIEW MEDICAL CENTER



12 - YESLER TERRACE STEAM PLANT



15 - 800 COLUMBIA ST



18 - 1001 JAMES ST

DESIGN CUES



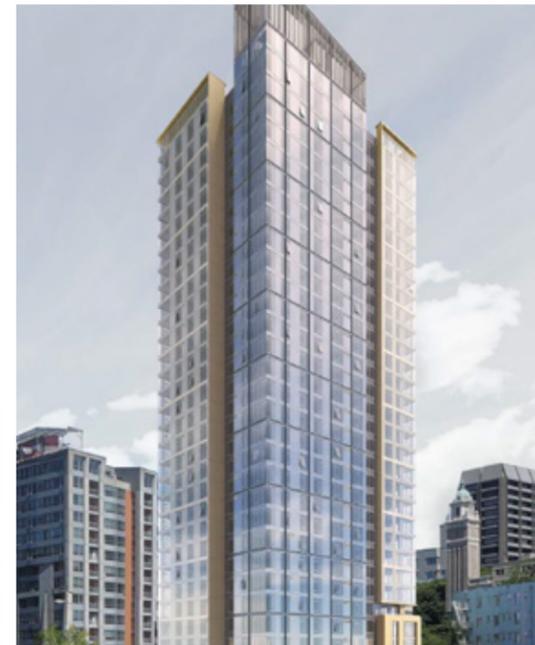
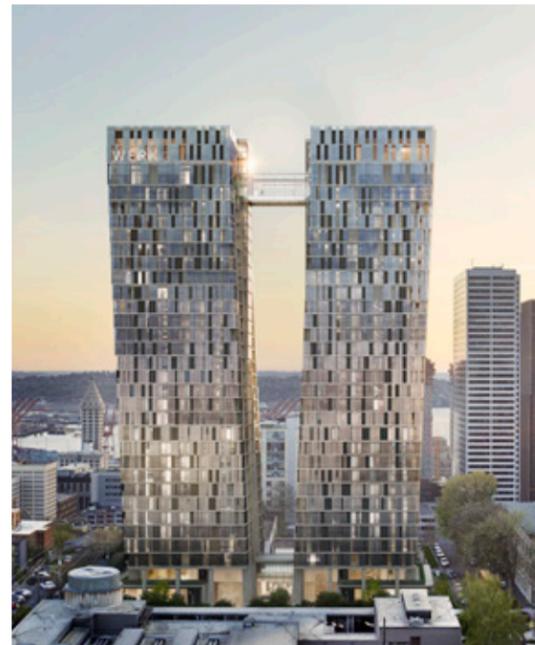
TOWER

PODIUM

PATTERN OF PODIUM/TOWER MASSING RELATIONSHIP IN SITE VICINITY

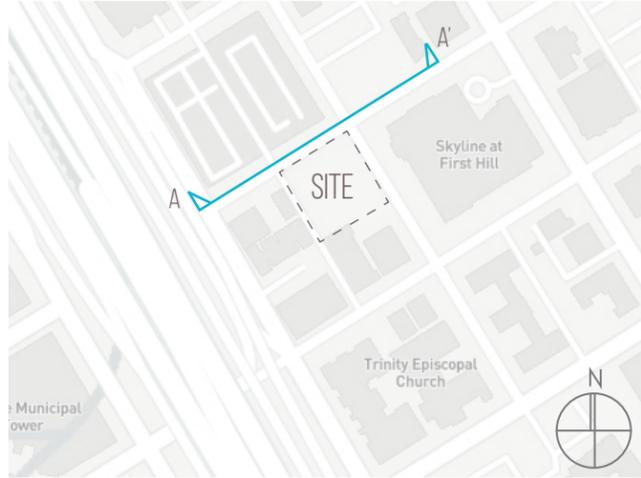


SCALE OF NEIGHBORHOOD MULTIFAMILY AND RETAIL INFORMS PODIUM MASSING AND SCALE

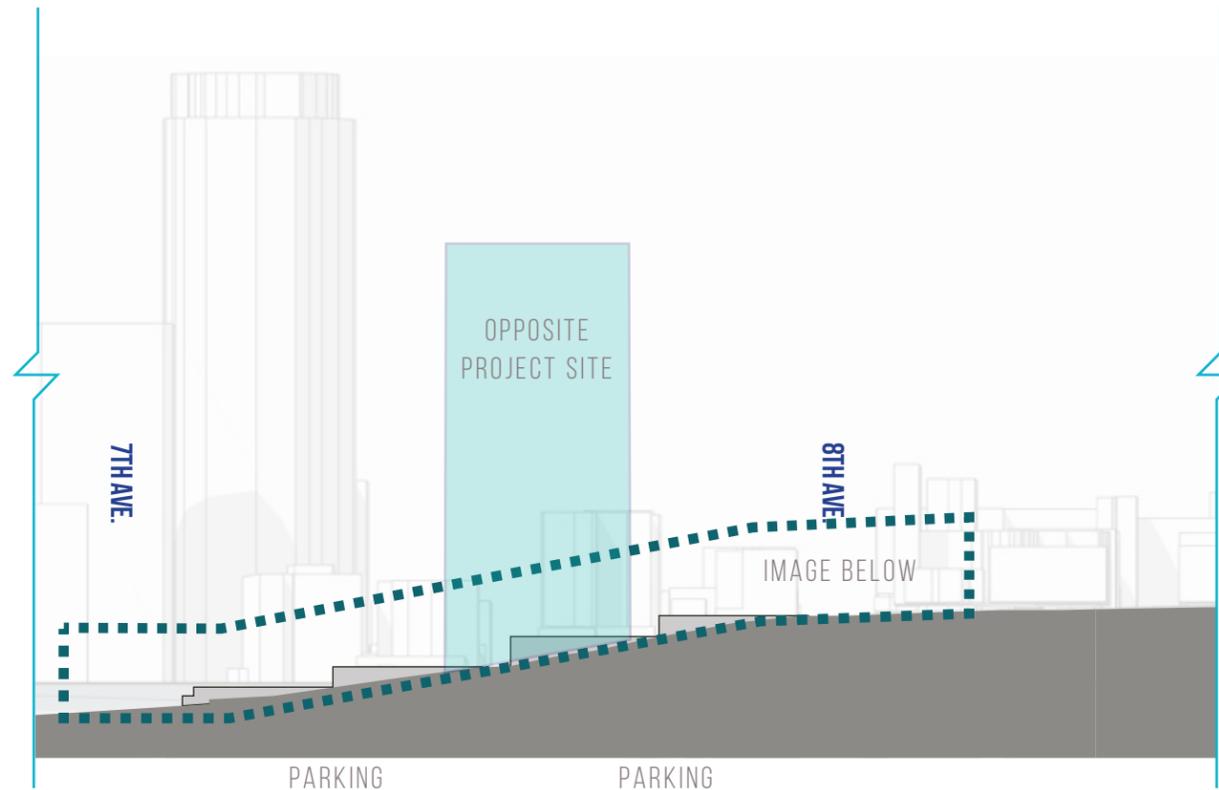


PROPOSED NEIGHBORHOOD EMPHASIZES GLAZED TOWERS ON MASS PODIUMS

STREETSCAPES



1: COLUMBIA ST. FACING TO NORTHWEST



A - A' SECTION

7TH AVENUE

PARKING

PARKING

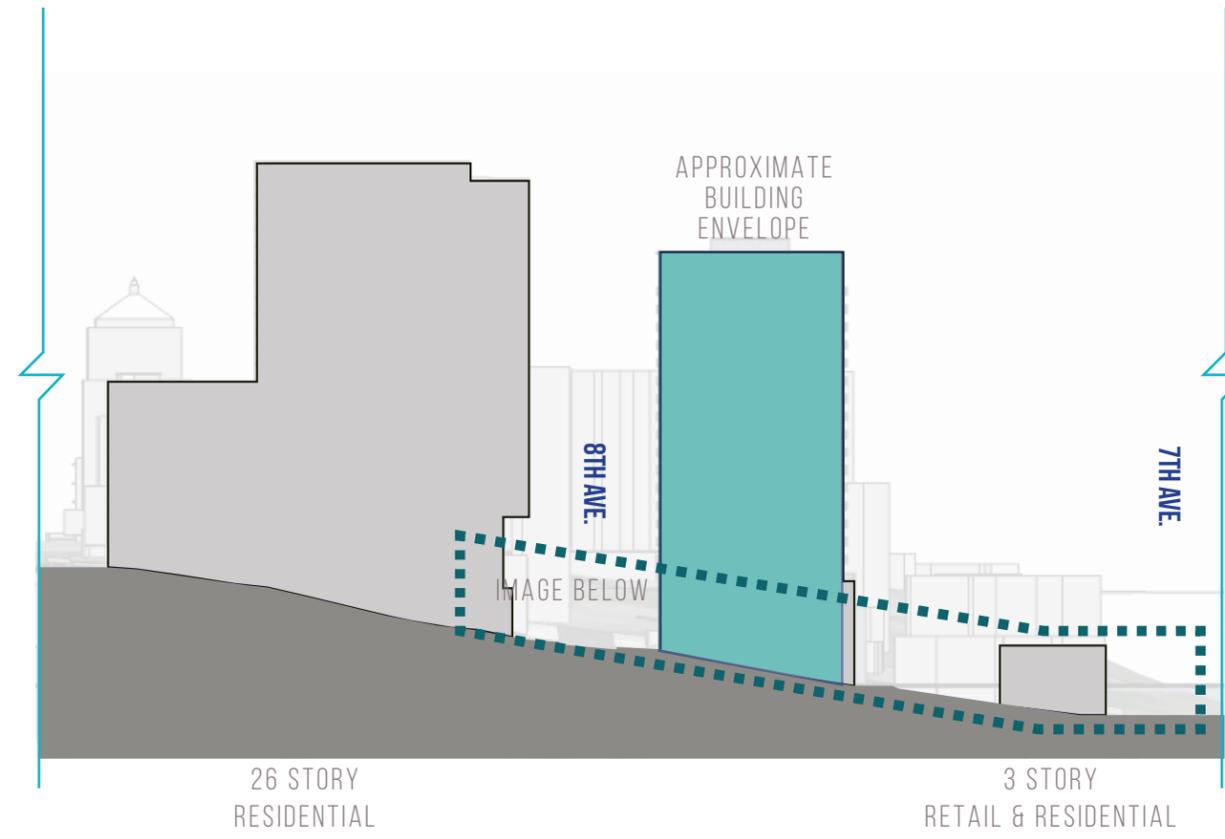
8TH AVENUE



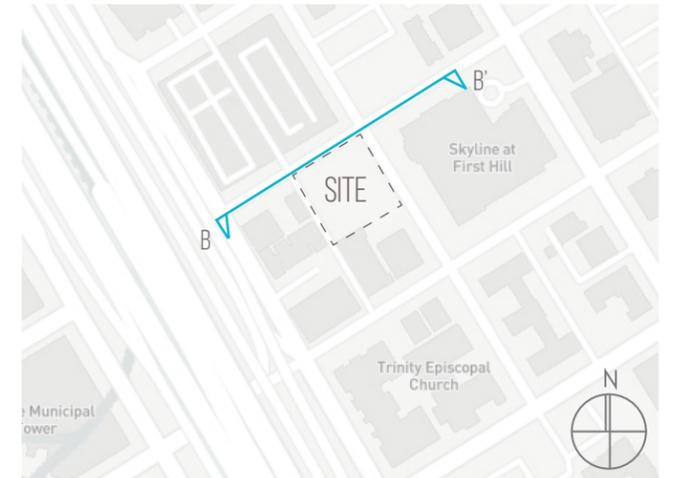
PROJECT SITE BEHIND

STREETSCAPES

2: COLUMBIA ST. FACING TO SOUTHEAST



B - B' SECTION



26 STORY RESIDENTIAL

8TH AVENUE

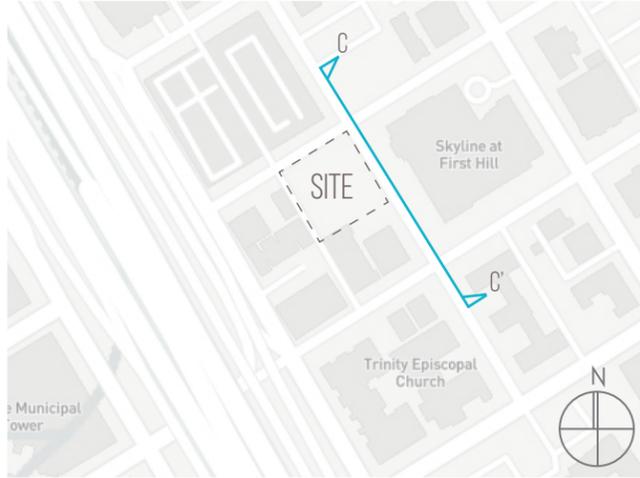
3 STORY RETAIL & RESIDENTIAL

7TH AVENUE

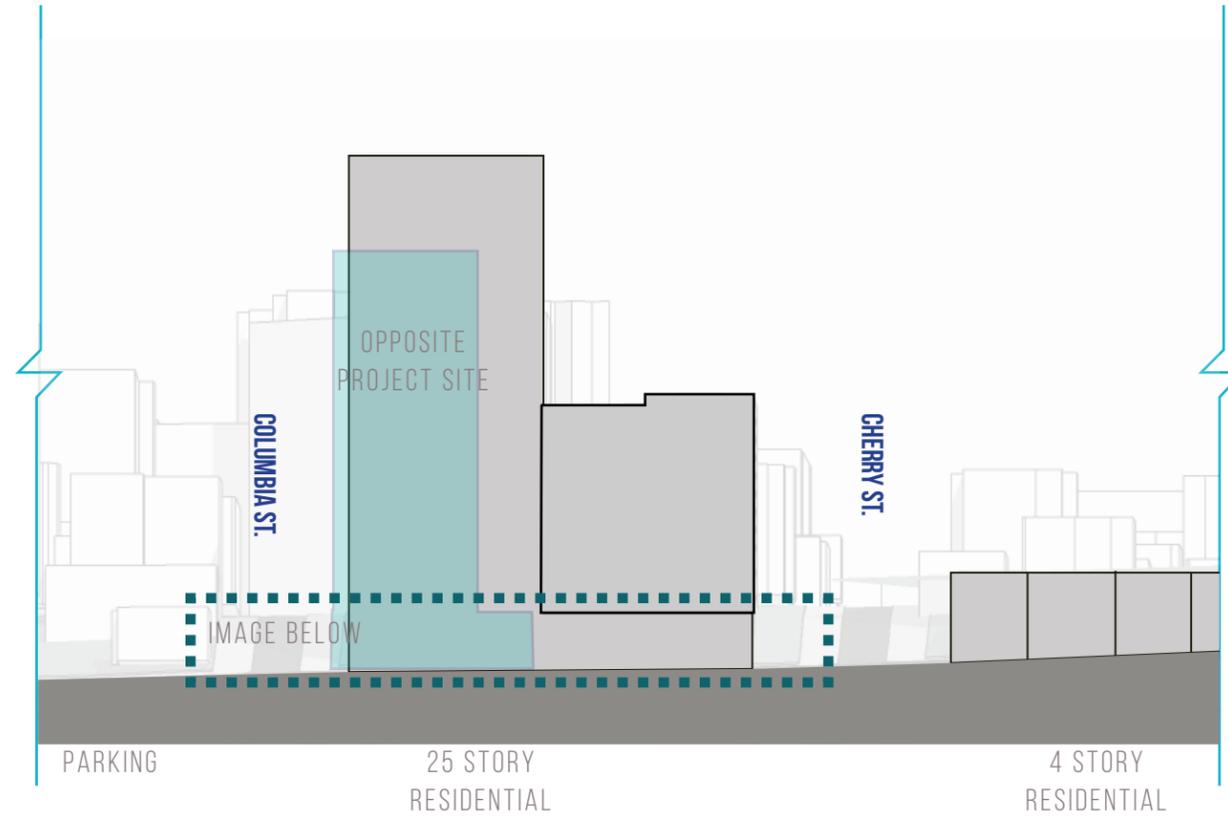


PROJECT SITE

STREETSCAPES



3: 8TH AVE. FACING TO NORTHEAST



COLUMBIA STREET

CHERRY STREET

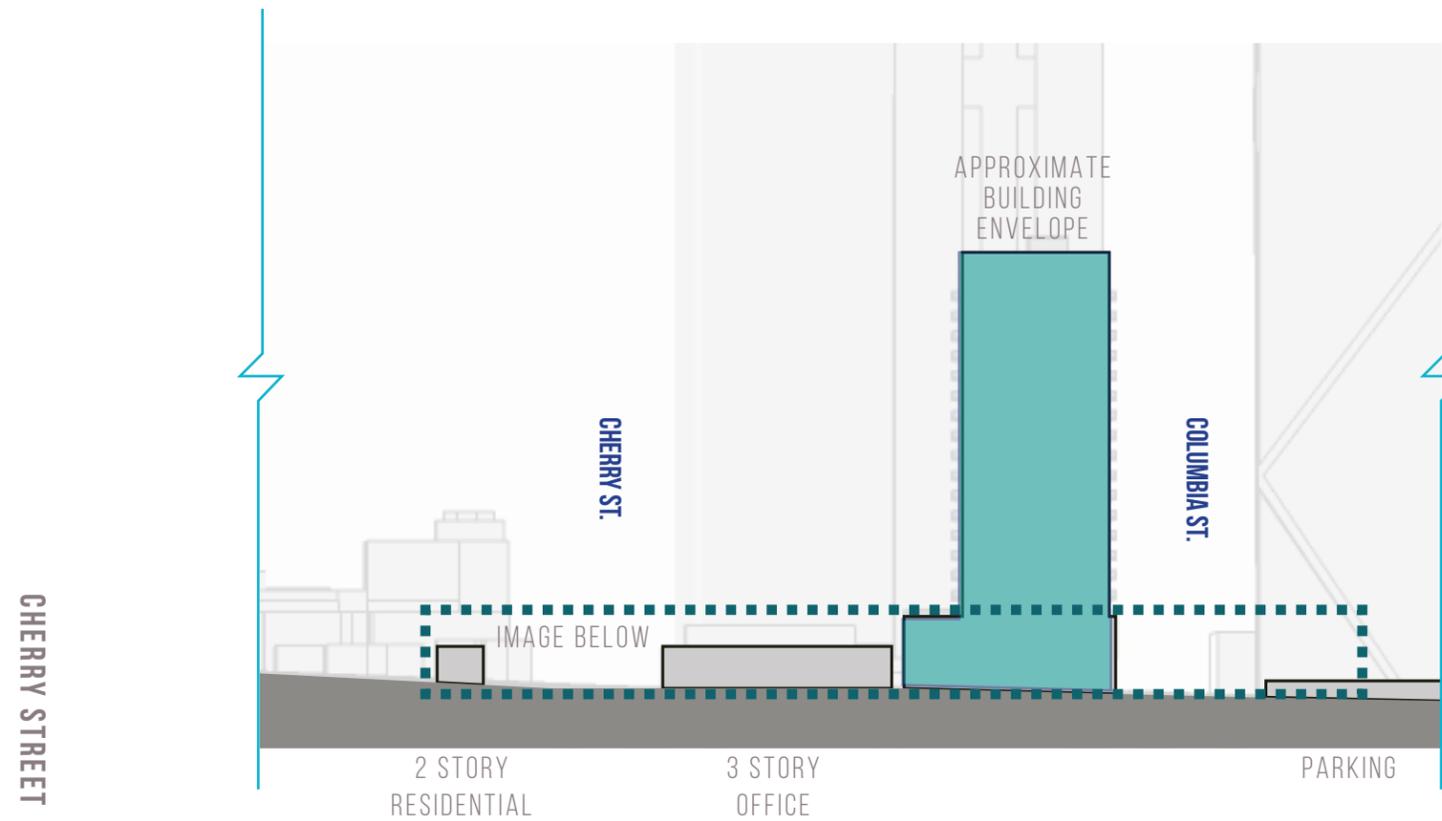
C - C' SECTION



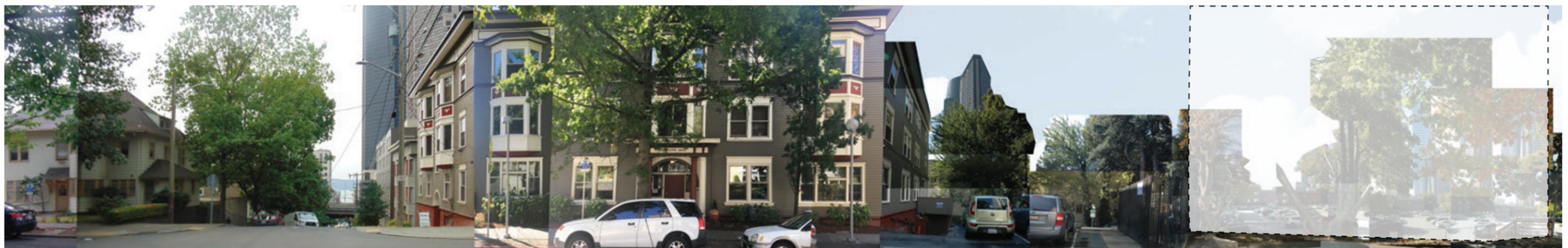
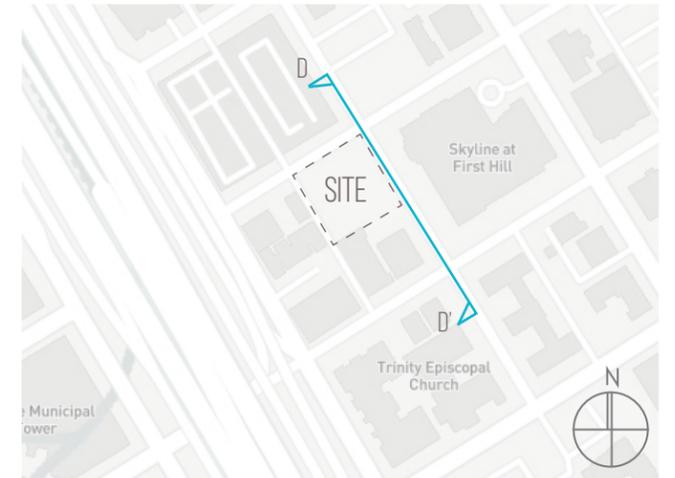
PROJECT SITE BEHIND

STREETSCAPES

4: 8TH AVE. FACING TO SOUTHWEST



D - D' SECTION



EXISTING SITE CONDITIONS

EXISTING SITE CONDITIONS

The site is currently a surface parking lot enclosed with a steel fence located at the corner of Columbia Street and 8th Avenue. There is a slight slope along 8th Avenue of two feet. Columbia Street is steeply sloping with a grade change of nearly 20 feet.

There are no trees on the site, but there are existing street trees. Street trees will be preserved to the extent possible.

Vehicle access to the site currently occurs in two places along the alley connecting Columbia Street and Cherry Street. There is wood stair providing pedestrian access to the site at northeast corner.

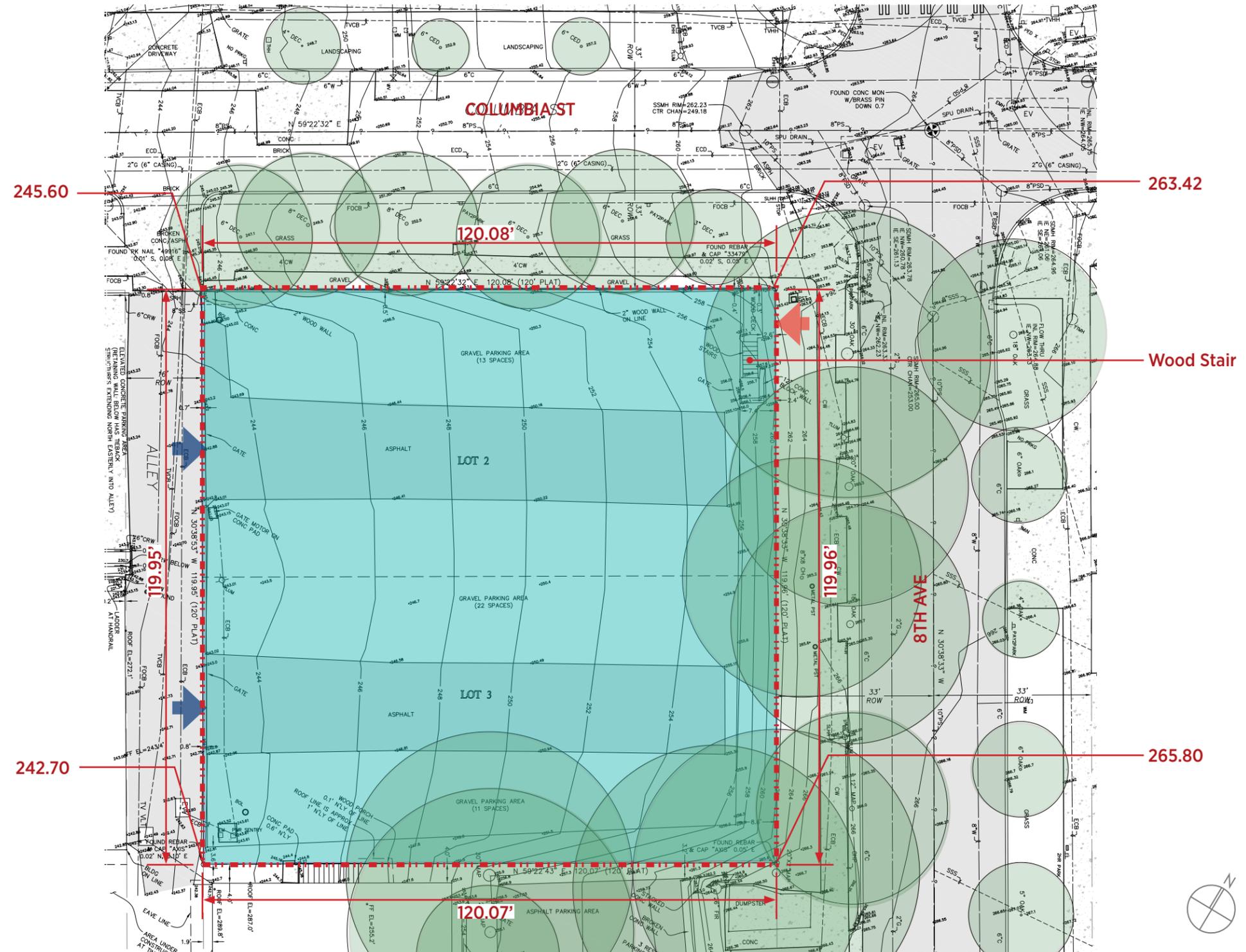
OPPORTUNITIES:

- 8th Avenue is a designated Neighborhood Greenway
- Alley allows for vehicular access that does not affect the character of the streetscape
- Proximity to existing Skyline community creates ability to activate streetscape through design of public space connection between the buildings
- Steep slope of Columbia Street allows back of house and parking uses to be tucked below grade

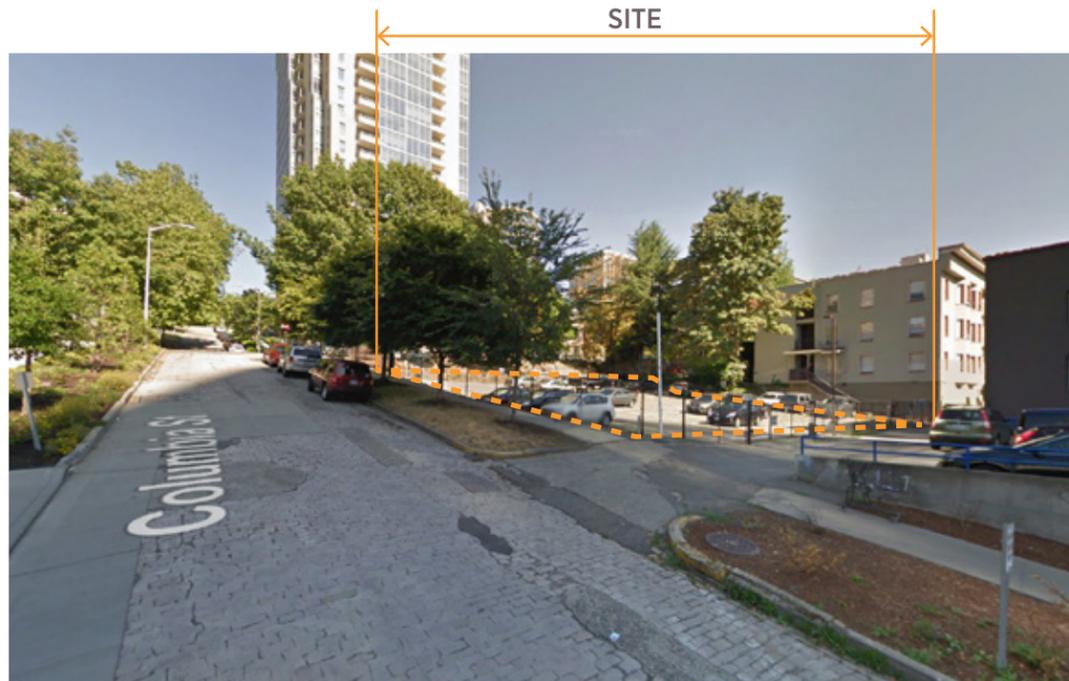
CONSTRAINTS:

- Steep slope of Columbia Street creates challenge to activate street edge through program organization and streetscape treatment
- Creating connection between Skyline communities for less-ambulatory residents

-  EXISTING VEHICULAR ACCESS
-  EXISTING PEDESTRIAN ACCESS



SITE PHOTOS



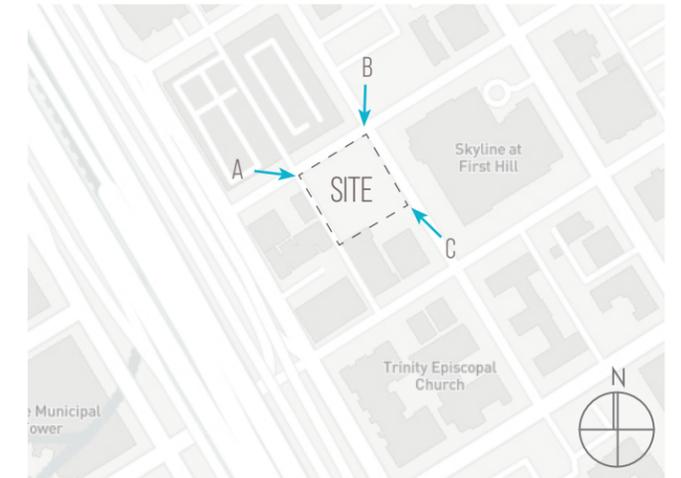
A | VIEW TOWARDS NORTHEAST



B | VIEW TOWARDS SOUTHEAST



C | VIEW TOWARDS SOUTHWEST



SITE PLAN

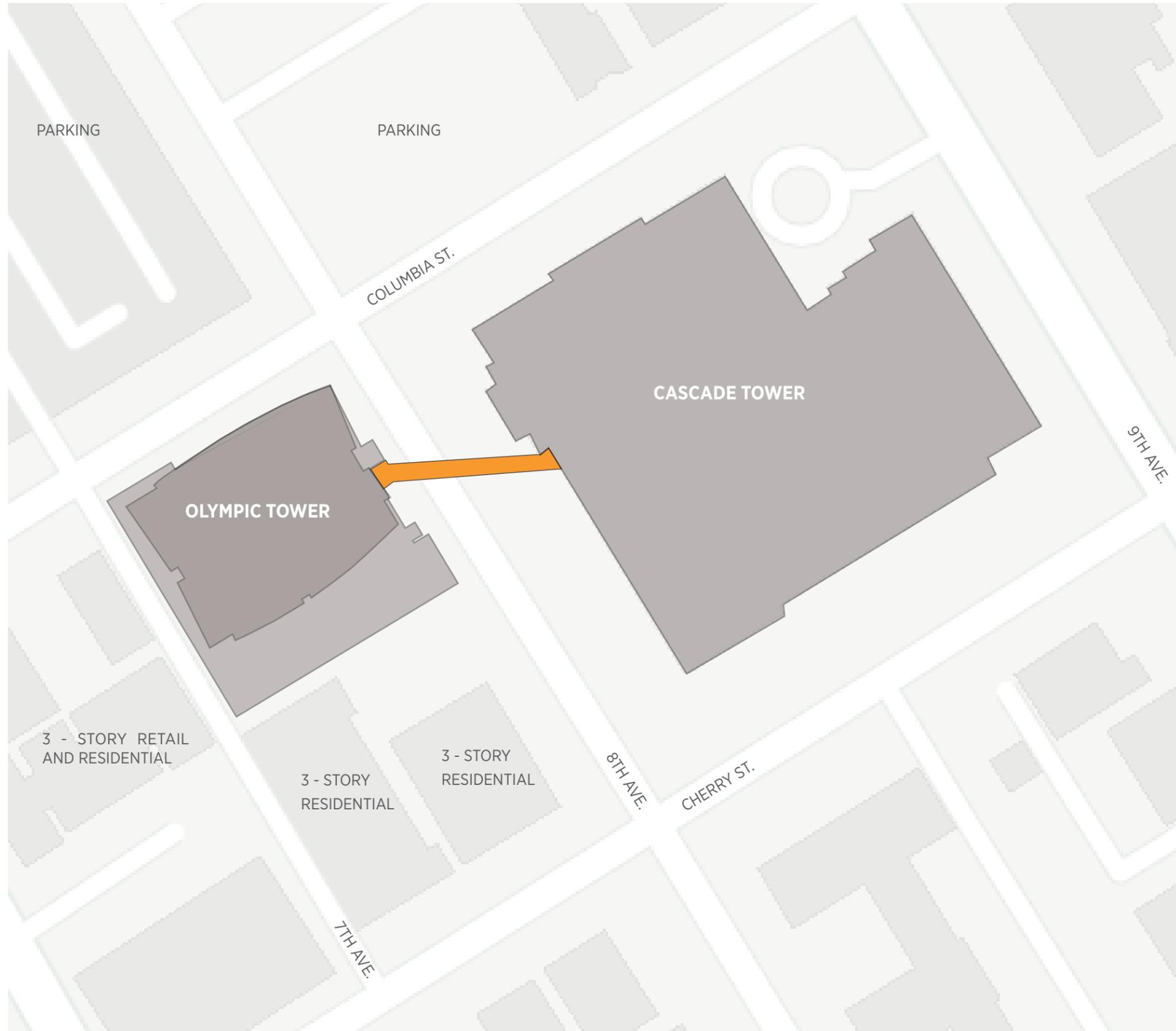
PRELIMINARY SITE PLAN

Columbia Street is a steeply sloped street. The proposal has an outdoor seating area along the building and cascading planters to treat storm water.

8th Avenue is a tree-lined pedestrian friendly street and designated as a neighborhood greenway. The main entry is located along this street, as well as an outdoor seating and gathering area near the intersection of 8th and Columbia. The ground floor is active uses for the residents and is setback generously from the street to provide an active, attractive street amenity to enliven the street. 8th Avenue is also the street between the two parts of the Skyline Campus. Proximity to existing Skyline community creates ability to activate streetscape through design of public space connection between the buildings.

Vehicular access and loading areas are located off the alley so these do not affect the character of the streetscape spaces.





SKYBRIDGE MERIT

- Need to integrate our campus to save lives
- First responder for all medical emergencies – life or death. Currently occurs 7x/month, 86x/yearly.
- Provides safe access to medical services between the two buildings
- Prevents Pedestrian Vehicular accidents
- Residents with Impairments - chronic illness/co-morbidity's, wheel chair dependent, assistance with ambulation/walkers and canes, dementia, hearing, vision.
- This project will add vibrancy to the street, large percentage of population will still use the at-grade crossing

PUBLIC BENEFIT SUMMARY

- Voluntary setbacks at ground level
- Enhanced right - of - way improvements at sidewalk and intersections
- Park to park loop

LEGEND



ZONING DATA

ZONING CODE SUMMARY

King county parcel numbers:
8590400455

Site Area: 114,400 SF

Base zone: HR

Urban Village Overlays: First Hill Urban Village

Environmentally Critical Area: YES 40% steep slope

SMC SECTION	DESCRIPTION	COMPLIANCE
CHAPTER 23.41 - DESIGN REVIEW		
23.45.502	Multifamily Scope of Provisions, Highrise (HR)	
23.45.504 TABLE A	Permitted Uses: Residential Use	Independent Senior Living is a residential use. Ground floor resident amenities on 8th Ave.
23.45.510	Floor area ratio (FAR) limits All gross floor area not exempt under subsection 23.45.510.E, including the area of stair penthouses with enclosed floor space, counts toward the maximum gross floor area allowed under the FAR limits	
23.45.510 TABLE B	8 on Lots 15,000 Square Feet or Less Maximum FAR pursuant to 23.58A & 23.45.516: 13 for 240' height or less 14 for greater than 240' height	Lot is 14,400 square feet Base FAR of 8 = 115,200 Proposed height = 235': FAR of 13 = 187,200 Proposed FAR = 135,803 = 9.4 complies
23.45.510.E	Exempt Areas: 1. Underground Stories; 4. Portions of a story no more than 4' above existing or finished grade, whichever is lower; c. within multi-family structures in HR zone 6. Enclosed common amenity; 7. As allowance for mechanical space, 3.5% of gross floor area; 8. In HR zones, ground floor commercial uses meeting the requirements of Section 23.45.532, if the street level of the structure containing the commercial uses has a minimum floor to floor height of 13 feet and a minimum depth of 15 feet.	
23.45.514 TABLE B	Structure height base max is 160' Maximum height limit if extra residential floor area is gained under Chapter 23.58A and Section 23.45.516 is 240 or 300	See 23.45.516.C.2
23.45.514.J.6	Elevator penthouses may extend above the applicable height limit by 16'	
23.45.514.J.11	A structure may exceed the applicable height limit in the HR zone as follows: a. If the applicable height limit is 240 feet, the height of the structure may be increased by 30 feet if the area bounded by the facades of the portion of the structure above 240 feet is no greater than 6,500 square feet, or if the area bounded by the facades at an elevation that is halfway between 240 feet and the height of the structure is no greater than 50 percent of the area bounded by the facades at a height of 240 feet.	The area bounded by the facade is less than 6,500SF. so, the maximum height is 270 feet.

SMC SECTION	DESCRIPTION	COMPLIANCE
23.45.516.C.1	<p>Extra residential floor area may be gained in accordance with 23.58A; Up to 100% of extra may be through affordable housing per 23.58A.014; Up to 40% may be through any combination of</p> <ul style="list-style-type: none"> b. Neighborhood open space or payment in lieu c. Neighborhood green street setback if allowed per 23.45.516.F 	<p>24,549 SF additional FAR needed Affordable housing option 40% of 24,549 SF = 9,820 SF</p> <ul style="list-style-type: none"> b. Yes c. Yes
23.45.516.C.2	<p>Multifamily Scope of Provisions, Highrise (HR) Structure height a. The applicable height limit is 240' if extra floor area is allowed</p>	<p>a. Proposed height is 240'</p>
23.45.516.F MAP A	<p>Neighborhood green street setback. Floor area may be gained for a neighborhood green street setback according to the provisions of Chapter 23.58A by development on lots abutting one of the streets or street segments within the First Hill Urban Village shown on Map A for 23.45.516.</p>	
23.45.518 TABLE C	<p>Setbacks for HR greater than 85 feet Lot line abutting a street: 45' high or less, 7' average, 5' min 0' if there are street level uses with direct entry from the street Greater than 45', 10' min;</p> <p>Alley Lot Line; 45' or less, none Greater than 45', 10' min;</p> <p>Neither Street Nor Alley 45' or less, 7' average, 5' min, except no setback abutting an existing structure built to the abutting lot line Greater than 45', 20' min;</p> <p>E. Additional structure setbacks may be required in order to meet the provisions of Chapter 23.53, Requirements for streets, alleys and easements.</p>	<p>8th street: Up to 45': 0' required - street level uses with street entry provided Over 45': 0' required - street level uses with street entry provided</p> <p>Columbia Ave: Up to 45': Min 5' / Ave >7' provided Over 45': Departure requested</p> <p>Alley: Up to 45': 2' provided</p> <p>Over 45': Departure requested</p> <p>Lot line abutting adjacent property: Neighboring structure is 4' from lot line. Up to 45': 4' provided; Departure requested Over 45': 30' min provided</p>
23.45.518.H	<p>Unenclosed decks and balconies may project a maximum of 4 feet into required setbacks if each one is:</p> <ul style="list-style-type: none"> 1.no closer than 5 feet to any lot line; 2.no more than 20 feet wide; and 3.separated from other decks and balconies on the same facade of the structure by a distance equal to at least ½ the width of the projection. 	<p>Proposed balconies comply with all requirements</p>

SMC SECTION	DESCRIPTION	COMPLIANCE
23.45.520.A	<p>HR zone width and floor size limits In HR zones, for structures over 85 feet in height, portions of structures above a height of 45 feet are limited to a maximum facade width of 110 feet. 1. A maximum facade width of 130 feet is permitted, provided that the average gross floor area of all stories above 45 feet in height does not exceed 10,000 square fee</p>	<p>Complies Portion of building above 45' high: 100' along Columbia 100' along 8th</p>
23.45.522	<p>Amenity Area: C. Amount of amenity area required in MR and HR zones. The required amount of amenity area in MR and HR zones is equal to 5 percent of the total gross floor area of a structure in residential use D. General requirements. Required amenity areas shall meet the following conditions 1. All units shall have access to a common or private amenity area 2b. Enclosed amenity areas: In MR and HR zones, no more than 50 percent of the amenity area may be enclosed, and this enclosed area shall be provided as common amenity area. 4.Private amenity areas a. There is no minimum dimension for private amenity areas, except that if a private amenity area abuts a side lot line that is not a side street lot line, the minimum horizontal dimension measured from the side lot line is 10 feet. 6. Parking areas, vehicular access easements, and driveways do not qualify as amenity area</p>	<p>Residential area: 125,077 SF 5% 6,523 SF 11,192 SF provided</p> <p>1. Complies 2. Complies: 4,964 SF outdoor common amenity. Using 50% max, total amenity = 9,928</p>
23.45.524	<p>Landscaping standards</p>	
23.45.524.A	<p>Green Factor requirement Landscaping that achieves a Green Factor score of 0.5 or greater, determined as set forth in Section 23.86.019, is required for any lot within an MR or HR zone if construction of more than one new dwelling unit or a congregate residence is proposed on the site.</p>	<p>Will comply Landscape to verify</p>
23.45.536.B	<p>Parking in a structure. Parking may be located in a structure or under a structure, provided that no portion of a garage that is higher than 4 feet above existing or finished grade, whichever is lower, shall be closer to a street lot line than any part of the street-level, street-facing facade of the structure in which it is located;</p>	<p>Complies</p>
23.45.536.C	<p>Access to parking Alley access required. Except as otherwise expressly required or permitted in subsections 23.45.536.C or 23.45.536.D, access to parking shall be from the alley if the lot abuts an alley and one of the conditions in this subsection 23.45.536.C.1 is met. a. The alley is improved to the standards of subsection 23.53.030.C; (Alley needs to be 12' ROW width Per table B 23.53.030)</p>	<p>Alley access is provided</p> <p>a. Alley is 16' currently. 12' min</p>
23.54.015	<p>Required parking</p>	
23.54.015.K TABLE D	<p>Bicycle parking. (LT) long term (ST) short term D residential uses: LT- 1 per 4 units = 19 required ST - none Footnote 3. For congregate residences that are owned by a not-for-profit entity or charity, or that are licensed by the State and provide supportive services for seniors or persons with disabilities, the Director shall have the discretion to reduce the amount of required bicycle parking if it can be demonstrated that residents are less likely to travel by bicycle.</p>	<p>19 spaces are provided</p>

SMC SECTION	DESCRIPTION	COMPLIANCE
23.54.015.A TABLE B	<p>Parking required No Minimum Required - Use M. All residential uses in commercial and multifamily zones within urban villages that are not within urban center or the Station Area Overlay District, if the residential use is located within 1,320 feet of a street with frequent transit service, measured as the walking distance from the nearest transit stop to the lot line of the lot containing the residential use. Table A says the same - Use K</p>	<p>No parking required 76 spaces provided Parking will be valet with semi-automated parking lifts</p>
23.54.030	<p>Parking space standards Residential use more than 5 spaces requires medium stall size: 8'-0"x16'-0" with 22'-0" aisle</p>	<p>Departure requested for aisle width</p>
23.54.035A	<p>Quantity The minimum number of off-street loading berths required for specific uses shall be set forth in Table A</p>	<p>Proposal is under the threshold for loading berths and exempt per 23.45.532B ground floor uses</p>
23.54.035.C	<p>Standards for Loading Berths 1.Width and Clearance. Each loading berth shall be not less than ten (10) feet in width and shall provide not less than fourteen (14) feet vertical clearance. 2.Length b.Low- and Medium-demand Uses. Each loading berth for low- and medium-demand uses, except those uses identified in subsection C2d, shall be a minimum of thirty-five (35) feet in length unless reduced by determination of the Director as provided at subsection C2c.</p>	<p>Loading berth is not required. 1. Complies. 10' wide and clear above</p>
23.54.040	<p>Solid waste and recyclable materials storage and access</p>	
23.54.040 TABLE A	<p>Minimum area for shared space: Residential: 51-100 dwelling units 375 square feet plus 4 square feet for each additional unit above 50</p> <p>76 units 26 x 4 = 104 SF + 375 SF = 479 SF</p>	
23.54.040.F	<p>Access for service providers to the storage space from the collection location shall meet the following requirements: 2. For containers larger than 2 cubic yards and all compacted refuse containers: a. Direct access shall be provided from the alley or street to the containers; b. Any gates or access routes for trucks shall be a minimum of 10 feet wide;</p>	<p>a. Complies b. Complies</p>
23.58A.014	<p>Bonus residential floor area for affordable housing</p>	
23.58A.014.C	<p>Payment option. Must be more than 85 feet high and approved by the director</p>	<p>This option will be used to gain extra residential floor area.</p>
23.58A.040	<p>Bonus floor area for open space amenities</p>	
23.58A.040.C.3	<p>Performance options 3. Bonus ratio. Unless otherwise specified in the provisions of the zone, amenities may be used to gain bonus floor area according to the following ratios b.For a green street setback, area (5:1). c. For a green street improvement area (5:1).</p>	<p>Per 23.45.516.C.1 40% max allowed b. and c. will be used to gain extra residential floor area.</p>

PRIORITY DESIGN GUIDELINES

CS1: NATURAL SYSTEMS AND SITE FEATURES

Use natural systems and features of the site and its surroundings as a starting point for project design.



PROJECT DESIGN RESPONSES

- Shallow floor plates allow for maximum daylighting and solar gain as well as expansive views within units (CS1.B)
- Large units in shallow floor plates allow for good natural cross ventilation (CS1.B)
- Existing trees will be retained as much as possible and provide shade and buffer between the street and the building (CS1.D)
- Rain water will be collected and conveyed down to planters along Columbia St. The planters will treat storm water and create a green edge for pedestrians. (CS1.E)
- 8th Ave is relatively flat and is ideal for a seating area at the corner and landscaped area towards the mid block. Columbia is steeply sloped and ideal for stepping storm water planters. (CS1.C)

CS2: URBAN PATTERN AND FORM

Strengthen the most desirable forms, characteristics, and patterns of the streets, block faces, and open spaces in the surrounding area.



PROJECT DESIGN RESPONSES

- The 21 story building is shorter than the existing building to the east, but similar footprint. The tower has a thin floor plate and is situated closer to the street than the interior lot line. This allows maximum views from the neighboring properties. (CS2.D)
- A strong street edge is created by both the tower and the base. The base creates a strong street edge, and evokes a feeling of a solid base, but is primarily glass allowing interaction with the street. The corner of the tower continues from the street level all the way to the top creating a dramatic corner presence. At the street level, the building is set back from the property line to provide seating and a generous entry. A canopy and seating will bring a pedestrian scale to the corner. (CS2.C)
- 8th Ave is a neighborhood green street. The base of the building is set back generously from the street to allow for generous planting, seating, trees, and sidewalks. Two story windows into the main lobby and resident amenities will create a visual interaction with the street. (CS2.B)
- The building is set back from the property line along steeply sloped Columbia St to create seating areas and step planters (CS2.B)
- The building is an extension of the existing project to the east. Similar scale and bases of the towers relate to each other. (CS2.D)

PL1: CONNECTIVITY

Complement and contribute to the network of open spaces around the site and the connections among them.



PROJECT DESIGN RESPONSES

- Street setbacks are generous to allow for wider sidewalks, landscape and other amenities. Curb bulbs are provided at the corners. (PL1.A)
- Heavy pedestrian traffic is expected between the existing building to the east and the new building. wide sidewalks along with a crossing at the intersection are proposed. (PL1.B)
- Along 8th Ave, pedestrian amenities are proposed. A mix of seating, planting, lighting and large windows into the main lobby and amenity space (PL1.B)
- The double height ground floor is highly porous, providing a strong visual connection to the interior as pedestrians walk along both 8th and Columbia. Moreover pedestrian amenities, such as seating, lighting landscaping encourage people both inside and outside for social interactions. (PL1.B)

PL2: WALKABILITY

Create a safe & comfortable walking environment that is easy to navigate and well-connected to existing pedestrian walkways and features.



PROJECT DESIGN RESPONSES

- Stepped planters will allow for resting points along the steeply sloped Columbia Street. (PL2.A)
- Large windows at the ground floors, residential windows on 3rd floor and a terrace on the 4th floor allow for surveillance of the street level activity. (PL2.B)
- Weather protection elements are integrated into the facade in two ways; Using the mass of the building for overhangs and through canopies attached to the building. Canopies at the corner along with paving and seating will create a human scale element at the dramatic corner element. (PL2.C)

PL3: STREET-LEVEL INTERACTION

Encourage human interaction and activity at the street-level with clear connections to building entries and edges.



PROJECT DESIGN RESPONSES

- The building frontage are set back significantly from the property line in order to provide a more expansive sidewalk environment and publicly accessible open space at the street. As shown in the landscape concept sketches, an integral design approach including planters, paving, and other appurtenances will create a dynamic space with strong indoor-outdoor connections. (PL3.A)
- The main entry is for residents and enters into the main two story lobby. The entry is identifiable through a large canopy and a continuous paved pathway from the curb to the entry. It is flanked by landscape on one side and hardscape with seating and amenities on the other side. The reception area is directly in front of the entry. A secondary entry along Columbia is provided with access to the seating area at the corner. (PL3.A)
- Private residential units are on the upper floors with resident amenities on level 1 and 2. This will encourage strong interaction with the street. (PL3.B)

DC2: ARCHITECTURAL CONCEPT

Integrate open space design with the design of the building so that each complements the other.



PROJECT DESIGN RESPONSES

- The mass of the building is a solid base that houses primarily resident amenities and a slender glass tower housing residential areas. (DC2.A)
- The mass of the tower is broken up with recesses that break up the mass into 4 quadrants. This is representative of the 4 units per floor on the typical floor plate. Balconies also break up the mass of the tower. (DC2.C)
- The massing of the base is primarily rectilinear in response to the streets, but the tower continues to the ground to provide a strong corner. (DC2.B)
- The base is highly transparent at the street facades on the lower two floors and more solid on the residential 3rd floor and facing the interior lot lines and the alley. (DC2.B)
- Canopies will break up the mass at the ground floor (DC2.D)

MASSING OPTIONS

OPTION 01: SQUARE TOWER
CODE COMPLIANT

Total Gross 188,000 square feet

OPPORTUNITIES

- Efficiency - Maximizing floor plates creates the same overall square footage in two less stories.
- Simple form and strong entry.

CONSTRAINTS

- Larger floor plates create bulky mass for the tower.
- Building is closer to the neighbors to the south.
- Larger tower massing blocks the most views and light from the neighboring buildings.
- Units are deeper and less desirable.
- Facade is monolithic
- Podium creates a strong street edge, but no areas for seating.

DEPARTURES: No



OPTION 02: ANGLED TOWER
TOWER + BASE

Total Gross 187,200 square feet

OPPORTUNITIES

- Tower shape is taller and more slender and blocks less light and views.
- Tower is set back 30 feet from neighboring property line providing buffer.
- Distinct podium and tower elements

CONSTRAINTS

- Larger podium
- Tower is not visually connected to the street level.

DEPARTURES: Yes



OPTION 03: CURVED TOWER (PREFERRED)
TOWER MERGE INTO BASE

Total Gross 186,000 square feet

OPPORTUNITIES

- Curved tower shape appears the most slender and provides maximum daylight access.
- The tower has both a strong visual connection to the street as well as a distinct base.
- The base is pulled back to provide seating and gathering areas at street level to activate the street.
- Strong and dramatic corner presence.
- Tower is set back 30 feet from neighboring property line providing buffer.
- Typical floors have 4 corner units.

CONSTRAINTS

- More complex building form

DEPARTURES: Yes

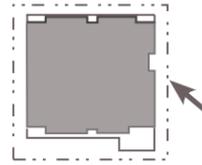


ARCHITECTURAL MASSING CONCEPTS

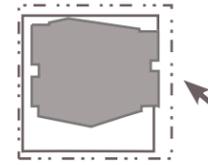
OPTIONS COMPARISON

IMAGES **WITHOUT** SKYBRIDGE FOR EACH OPTION

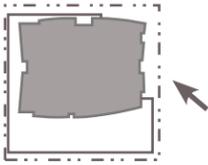
OPTION 01: **SQUARE TOWER**
CODE COMPLIANT



OPTION 02: **ANGLED TOWER**
TOWER + BASE



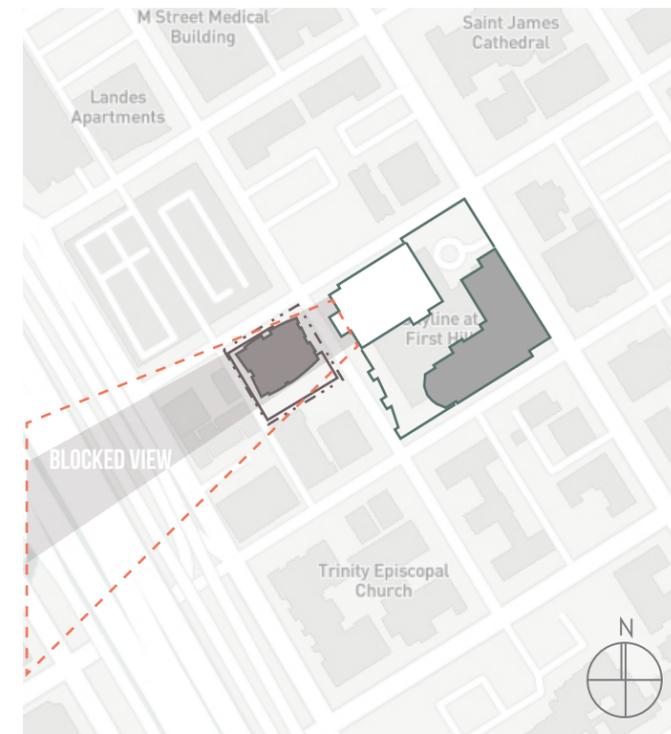
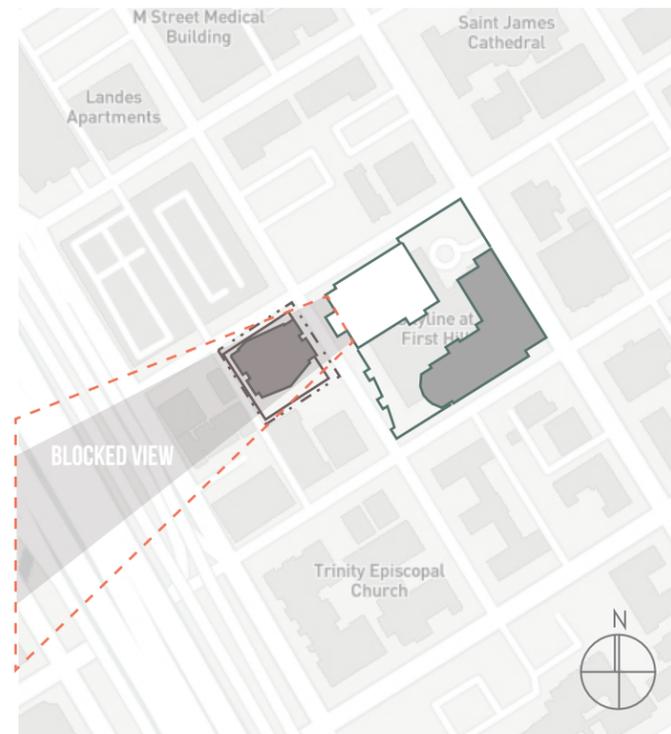
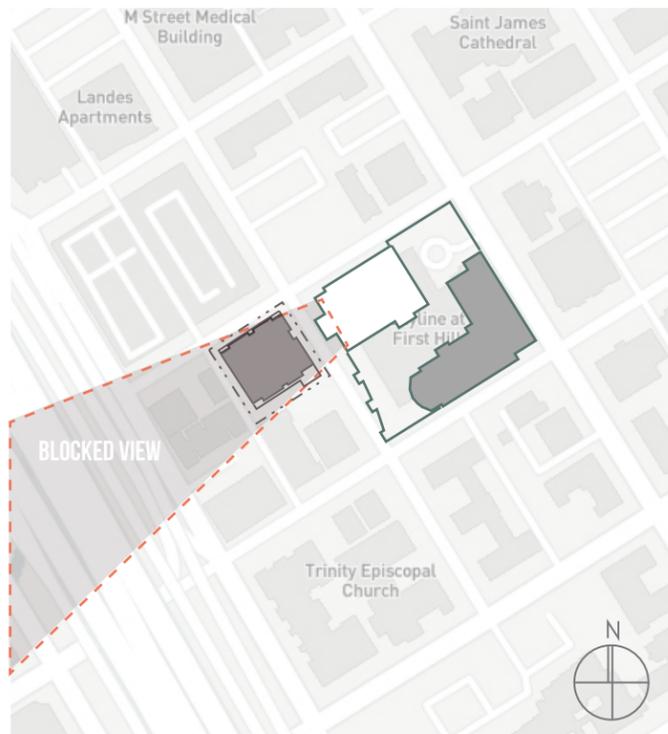
OPTION 03: **CURVED TOWER (PREFERRED)**
TOWER MERGE INTO BASE



OPTION 1: STRAIGHT

OPTION 2: ANGLED

OPTION 3: CURVED



ARCHITECTURAL MASSING CONCEPTS

CONCEPT 01: SQUARE TOWER CODE COMPLIANT

CONCEPT PODIUM

OPPORTUNITIES

- Efficiency - Maximizing floor plates creates the same overall sf in 2 less stories.
- Simple form.
- Strong entry.

CONSTRAINTS

- Larger floor plates create more bulky mass for the tower.
- Building is closer to the neighbors to the south.
- Larger tower massing blocks the most views and light from the neighboring buildings.
- More units per floor, but units are deeper.
- Facade is monolithic
- Podium creates a strong street edge, but no areas for seating.

DEPARTURES

- No

Height: 220 FT
Total: Gross Area: 188,000FT
Unit Count: 76
Parking Count:76



CONCEPT 01: SQUARE TOWER
CODE COMPLIANT

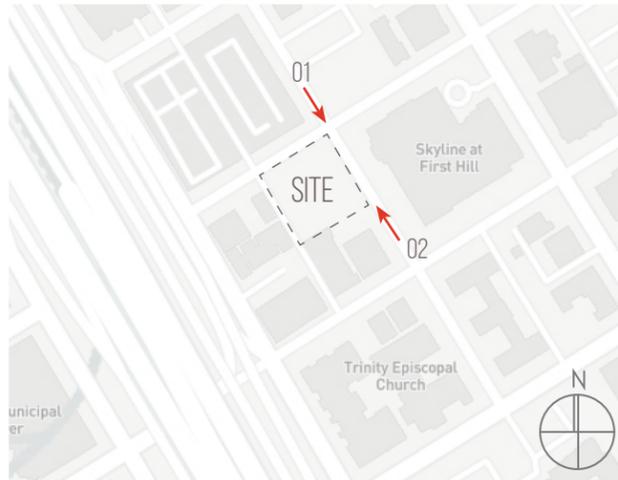
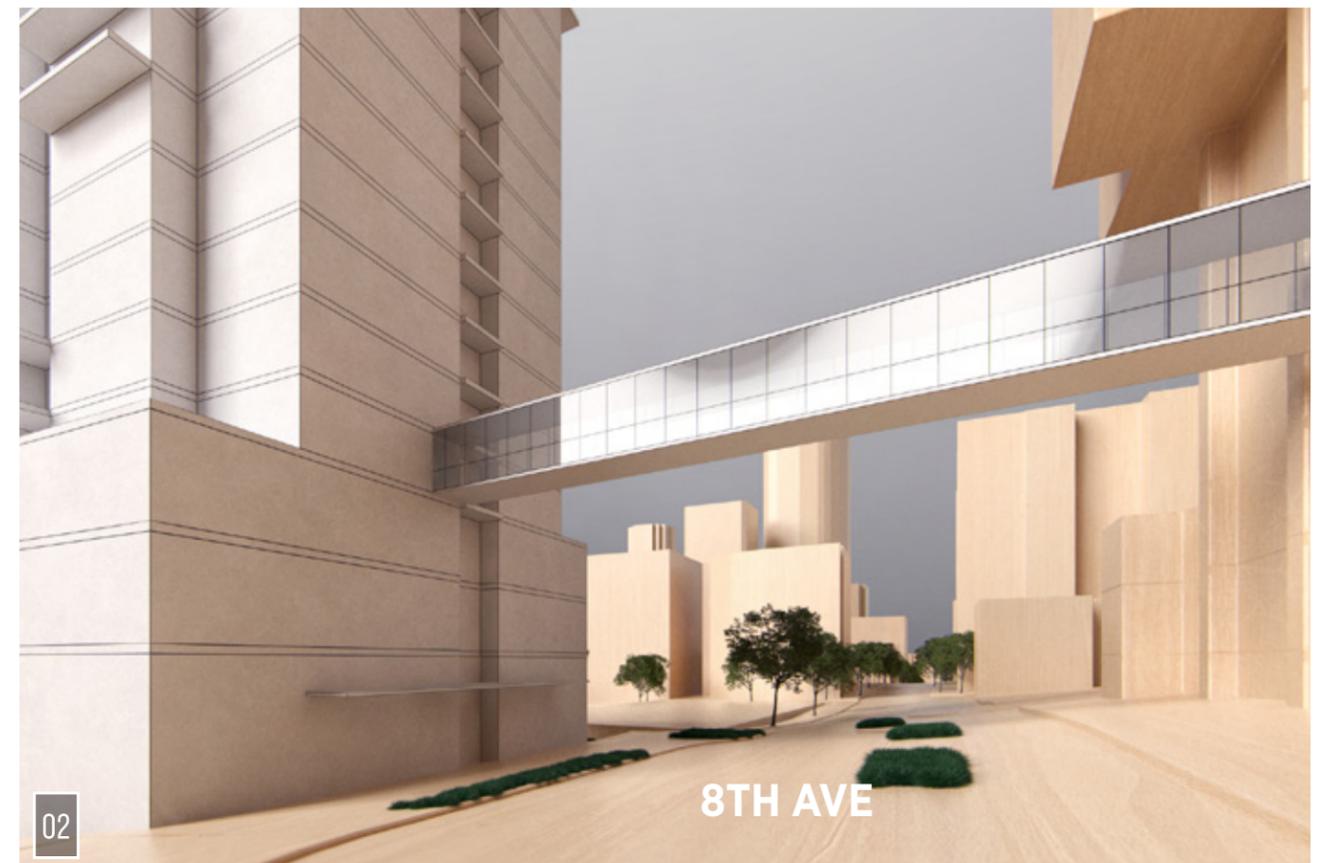


Image without skybridge

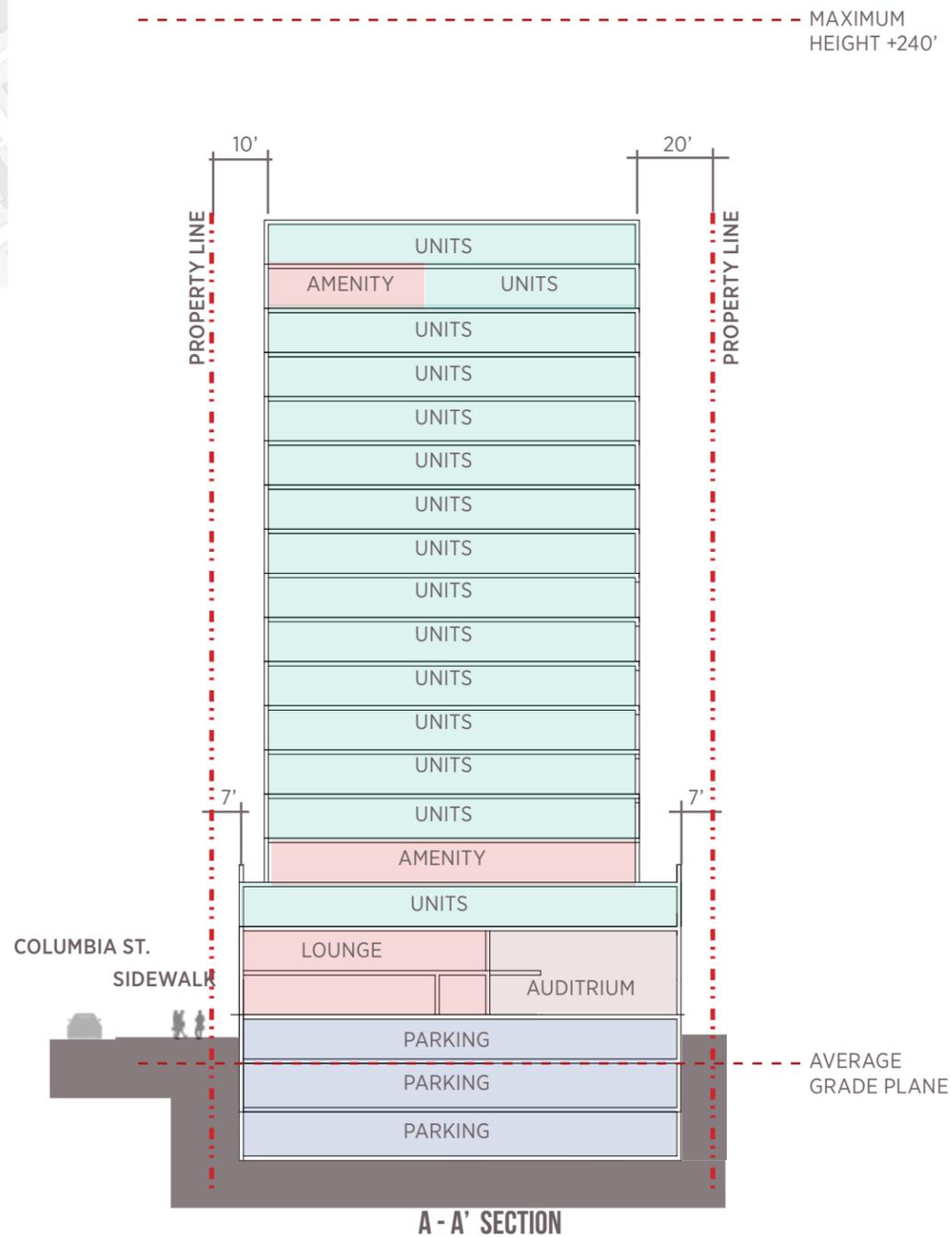


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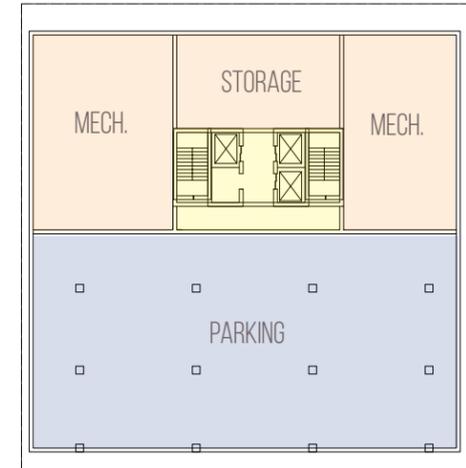


ARCHITECTURAL MASSING CONCEPTS

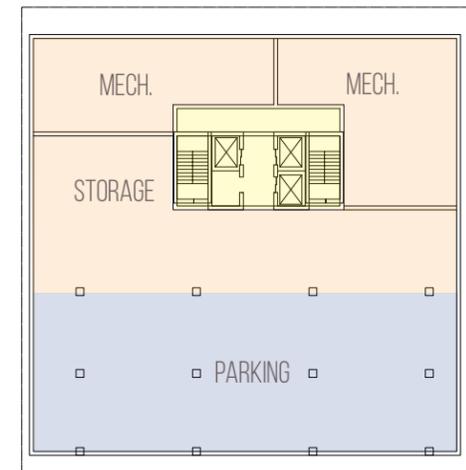
CONCEPT 01: SQUARE TOWER CODE COMPLIANT



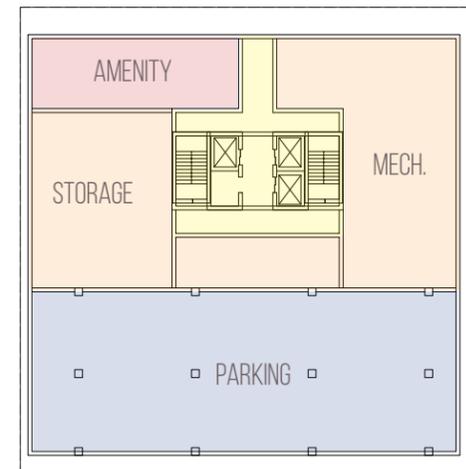
LEVEL P3 FLOOR PLAN



LEVEL P2 FLOOR PLAN

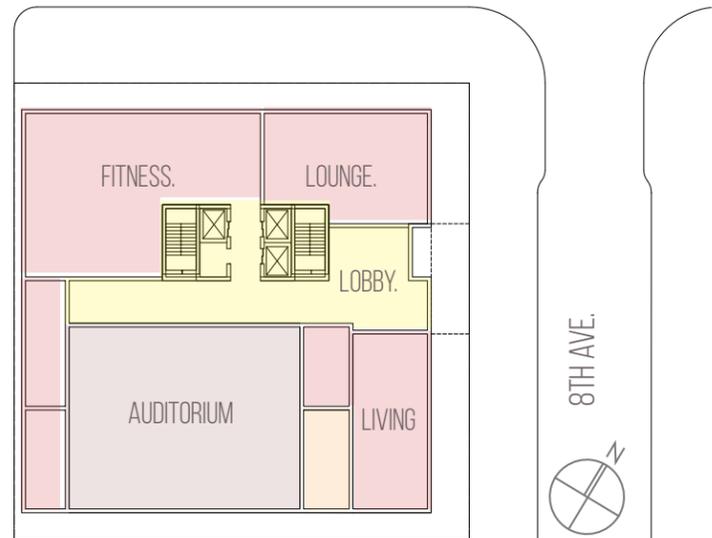


LEVEL P1 FLOOR PLAN

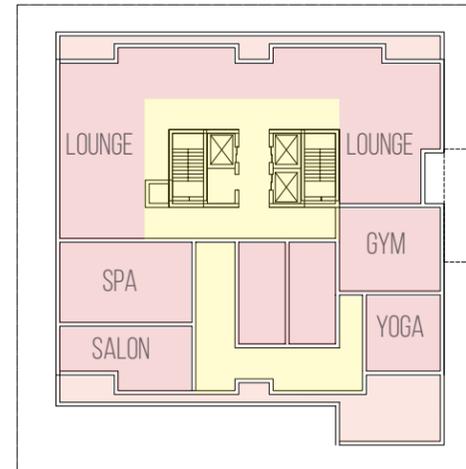


CONCEPT 01: SQUARE TOWER
CODE COMPLIANT

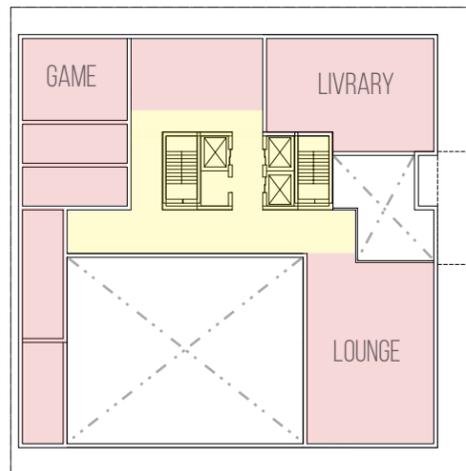
GROUND FLOOR PLAN



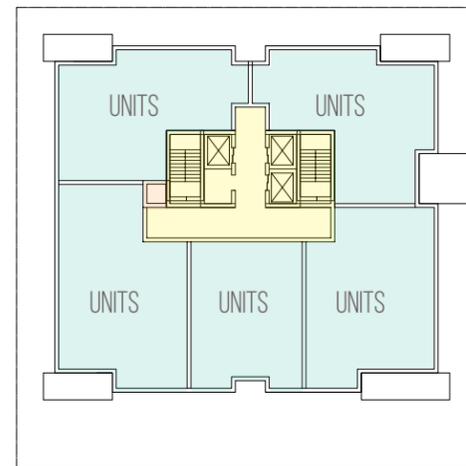
LEVEL 4 FLOOR PLAN



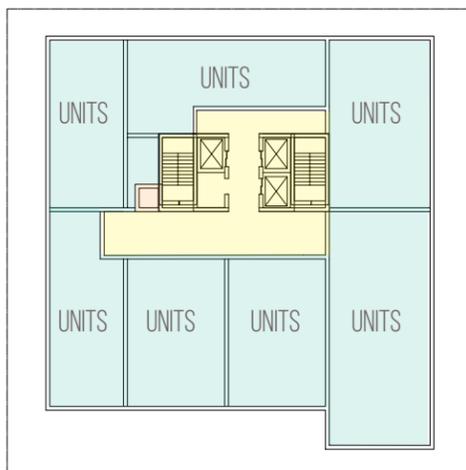
LEVEL 2 FLOOR PLAN



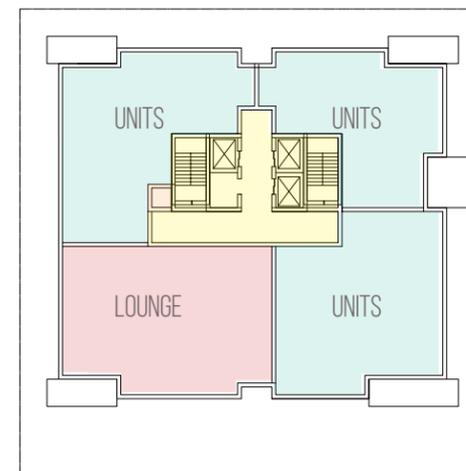
TYPICAL FLOOR PLAN



LEVEL 3 FLOOR PLAN



PENTHOUSE FLOOR PLAN



LEGEND

- AMENITY
- RESIDENTIAL
- ENTERTAINMENT
- SERVICE
- CIRCULATION
- OUTDOOR AMENITY

ARCHITECTURAL MASSING CONCEPTS

CONCEPT 01: SQUARE TOWER CODE COMPLIANT

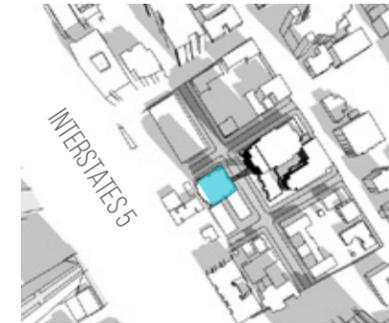


● SUMMER SOLSTICE: 21 JUNE 2017
 SUNRISE: 5:11 AM SUNSET: 9:11 PM
 SOLAR DECLINATION: 23.43°
 SOLAR ELEVATION AT NOON: 61.94°

● WINTER SOLSTICE: 21 DECEMBER 2017
 SUNRISE: 7:55 AM SUNSET: 4:21 PM
 SOLAR DECLINATION: - 23.44°
 SOLAR ELEVATION AT NOON: 18.91°

SUN / SHADOW STUDY

EQUINOX - 10AM



EQUINOX - 12PM



EQUINOX - 2PM



WITH SKYBRIDGE



WITHOUT SKYBRIDGE

SUMMER SOLSTICE - 10AM



SUMMER SOLSTICE - 12PM



SUMMER SOLSTICE - 2PM



WITH SKYBRIDGE



WITHOUT SKYBRIDGE

WINTER SOLSTICE - 10AM



WINTER SOLSTICE - 12PM



WINTER SOLSTICE - 2PM



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ARCHITECTURAL MASSING CONCEPTS

CONCEPT 02: ANGLED TOWER TOWER + BASE

CONCEPT PODIUM

OPPORTUNITIES

- Tower shape is taller and more slender and blocks less light and views.
- Tower is set back 30 feet from neighboring property line providing buffer.
- Distinct podium and tower elements

CONSTRAINTS

- Larger podium
- Tower is not visually connected to the street level.

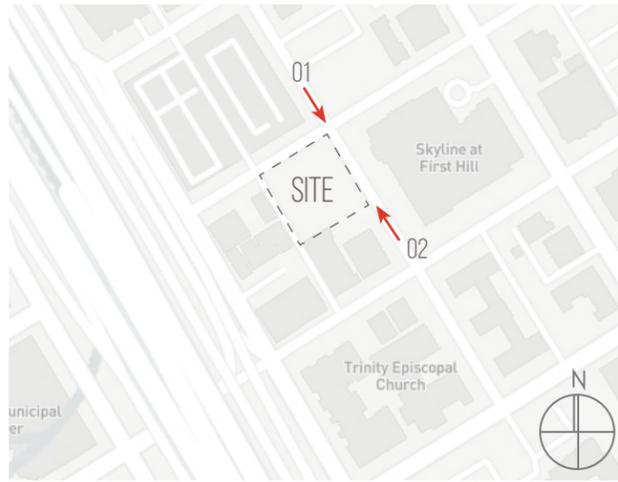
DEPARTURES

Yes

Height : 240 FT
Total Gross : 187,200 FT
Unit Count: 76
Parking Count:76



CONCEPT 02: ANGLED TOWER
TOWER + BASE



01
Image without skybridge



02
Image without skybridge



01

8TH AVE

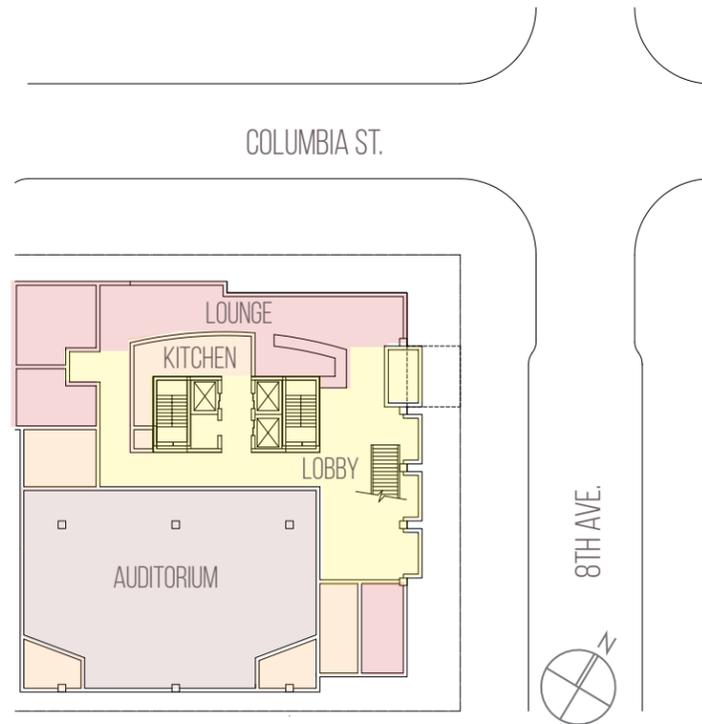


02

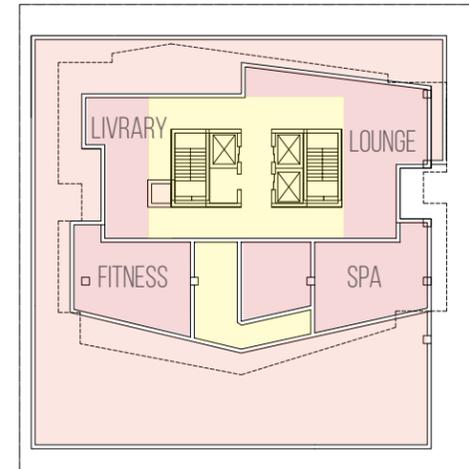
8TH AVE

**CONCEPT 02: ANGLED TOWER
TOWER + BASE**

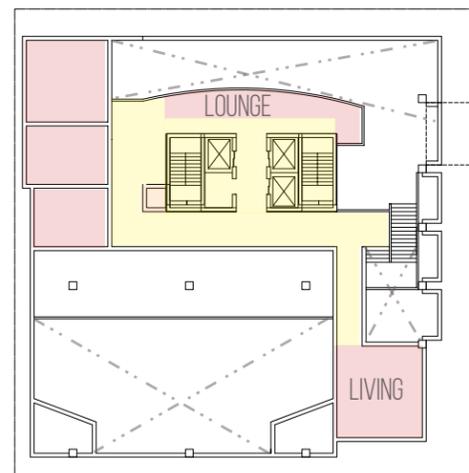
GROUND FLOOR PLAN



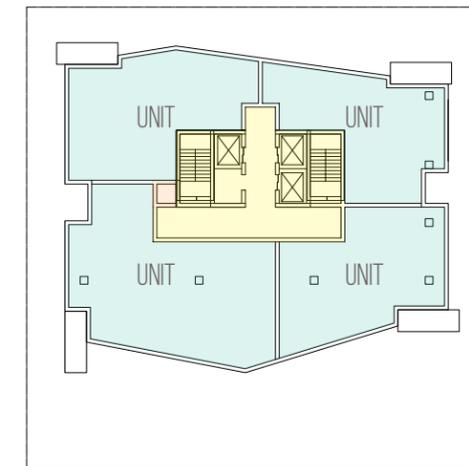
LEVEL 4 FLOOR PLAN



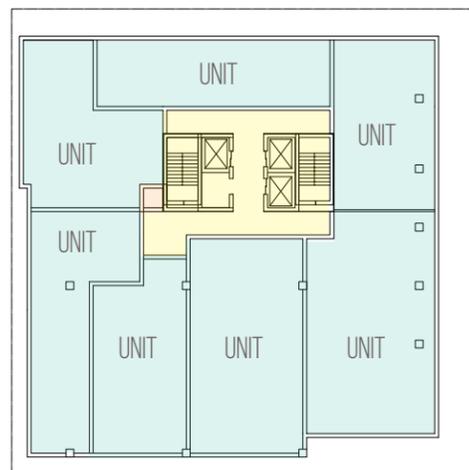
LEVEL 2 FLOOR PLAN



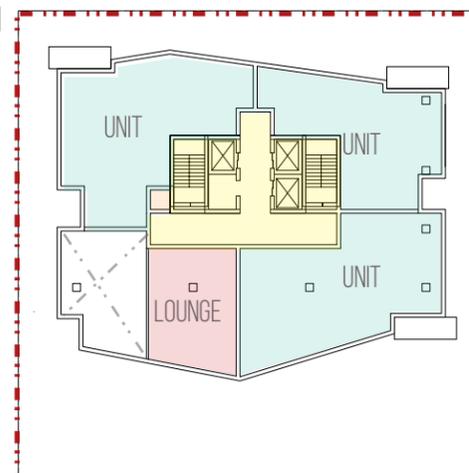
TYPICAL FLOOR PLAN



LEVEL 3 FLOOR PLAN



PENTHOUSE FLOOR PLAN



LEGEND

- AMENITY
- RESIDENTIAL
- ENTERTAINMENT
- SERVICE
- CIRCULATION
- OUTDOOR AMENITY

ARCHITECTURAL MASSING CONCEPTS

CONCEPT 02: ANGLED TOWER TOWER + BASE



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SUN / SHADOW STUDY

EQUINOX- 10AM



EQUINOX - 12PM



EQUINOX - 2PM



WITH SKYBRIDGE



WITHOUT SKYBRIDGE

SUMMER SOLSTICE- 10AM



SUMMER SOLSTICE - 12PM



SUMMER SOLSTICE - 2PM



WITH SKYBRIDGE



WITHOUT SKYBRIDGE

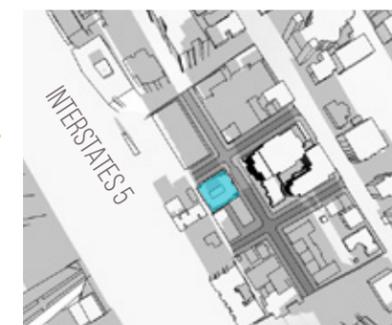
WINTER SOLSTICE- 10AM



WINTER SOLSTICE - 12PM



WINTER SOLSTICE - 2PM



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ARCHITECTURAL MASSING CONCEPTS

CONCEPT 03: CURVED TOWER (PREFERRED) TOWER MERGE INTO BASE

CONCEPT PODIUM

OPPORTUNITIES

- Curved tower shape appears the most slender and provides maximum daylight access.
- The tower has both a strong visual connection to the street as well as a distinct base.
- The base is pulled back to provide seating and gathering areas at street level to activate the street.
- Strong and dramatic corner presence.
- Tower is set back 30 feet from neighboring property line providing buffer.
- Typical floors have 4 corner units.

CONSTRAINTS

- More complex building form

DEPARTURES

YES

Height : 240 FT

Total Gross : 185,000 FT

Unit Count: 76

Parking Count: 76



CONCEPT 03: CURVED TOWER (PREFERRED)
TOWER MERGE INTO BASE

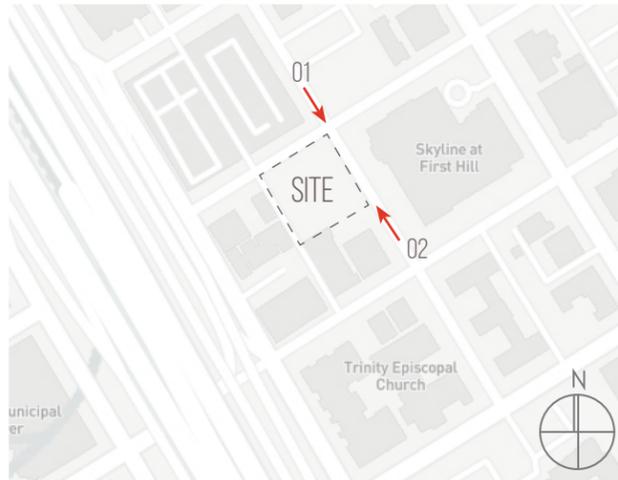


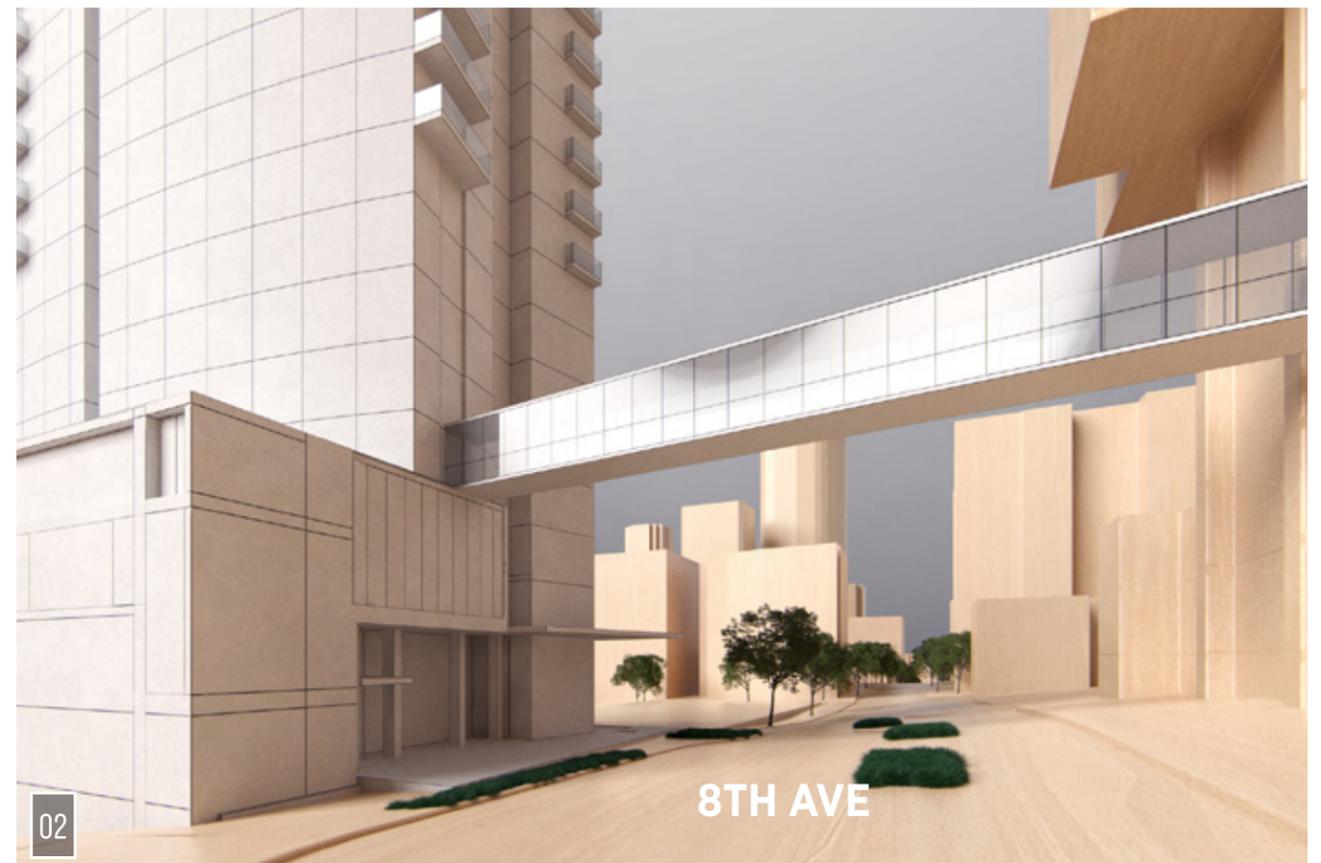
Image without skybridge



Image without skybridge



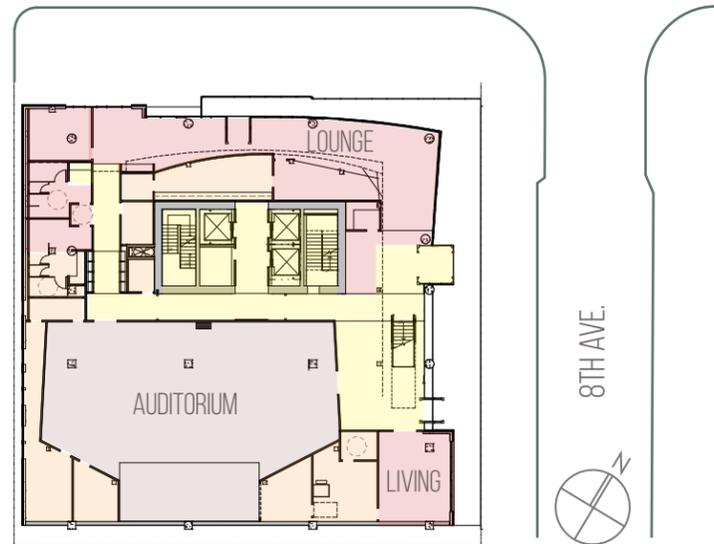
8TH AVE



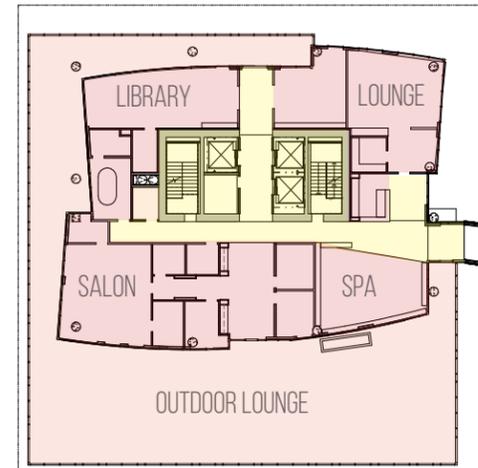
8TH AVE

CONCEPT 03: CURVED TOWER (PREFERRED)
TOWER MERGE INTO BASE

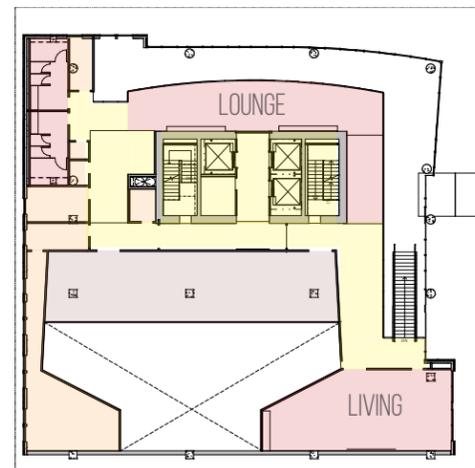
GROUND FLOOR PLAN



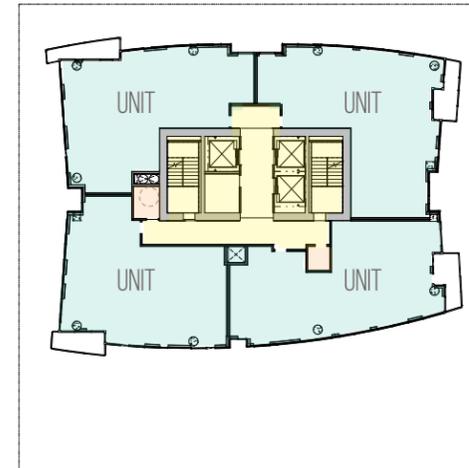
LEVEL 4 FLOOR PLAN



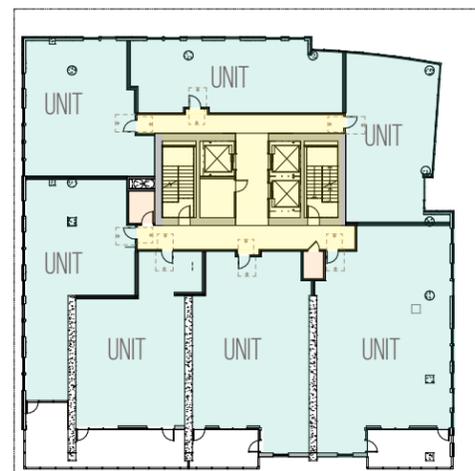
LEVEL 2 FLOOR PLAN



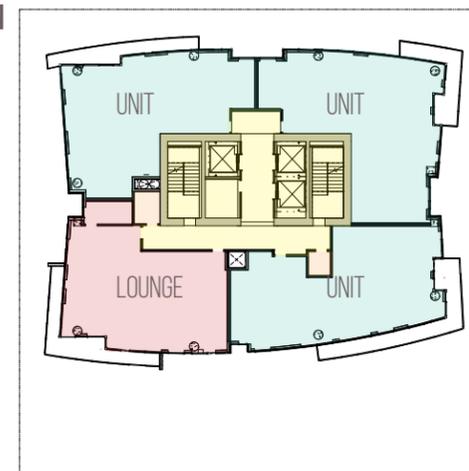
TYPICAL FLOOR PLAN



LEVEL 3 FLOOR PLAN



PENTHOUSE FLOOR PLAN



LEGEND

- AMENITY
- RESIDENTIAL
- ENTERTAINMENT
- SERVICE
- CIRCULATION
- OUTDOOR AMENITY

ARCHITECTURAL MASSING CONCEPTS

CONCEPT 03: ANGLED TOWER TOWER + BASE

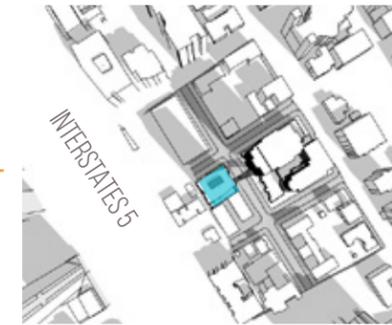


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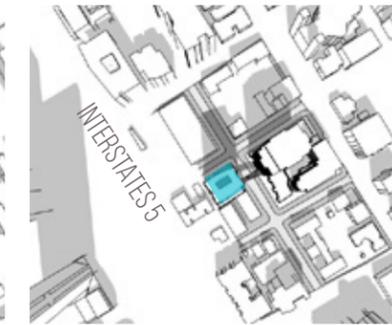
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SUN / SHADOW STUDY

EQUINOX- 10AM



EQUINOX - 12PM



EQUINOX - 2PM



WITH SKYBRIDGE



WITHOUT SKYBRIDGE

SUMMER SOLSTICE- 10AM



SUMMER SOLSTICE - 12PM



SUMMER SOLSTICE - 2PM

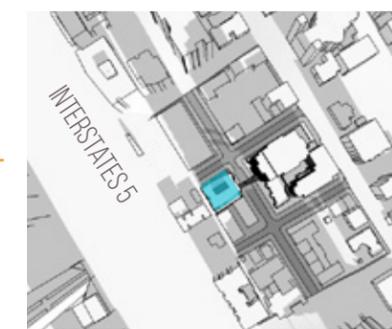


WITH SKYBRIDGE



WITHOUT SKYBRIDGE

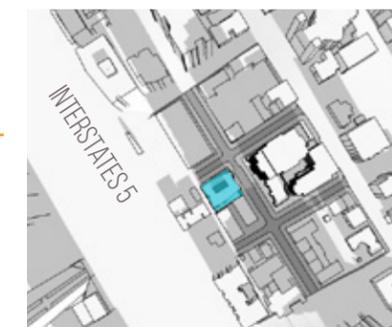
WINTER SOLSTICE- 10AM



WINTER SOLSTICE - 12PM



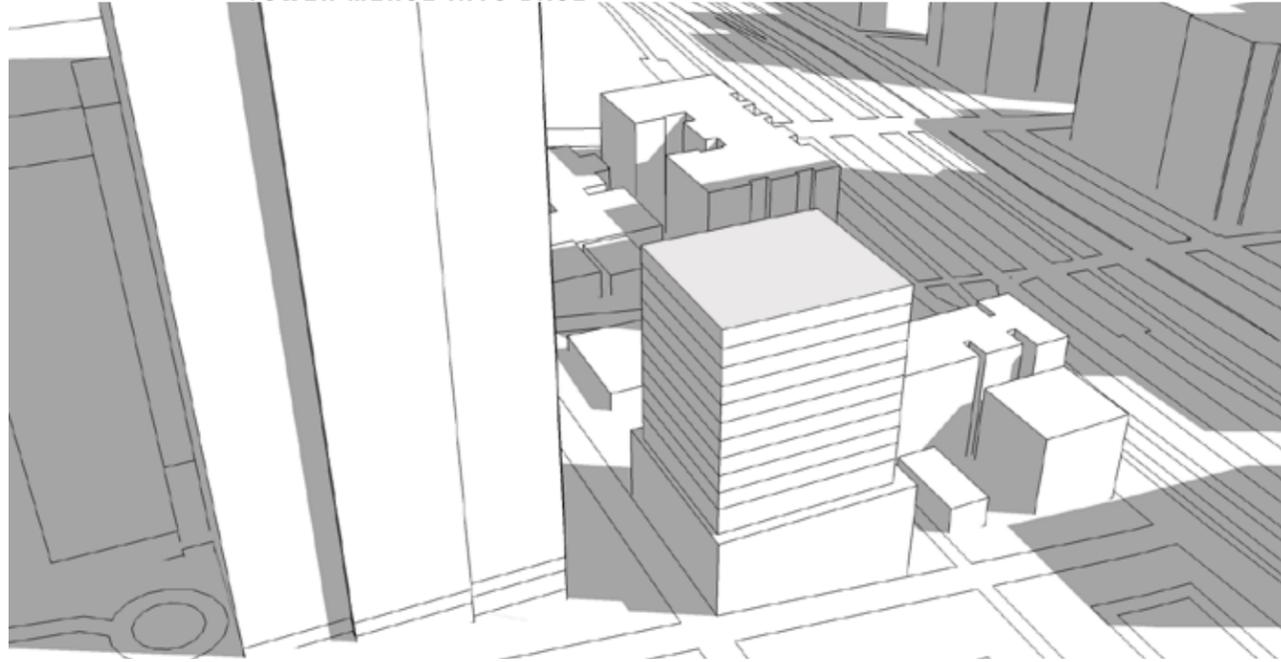
WINTER SOLSTICE - 2PM



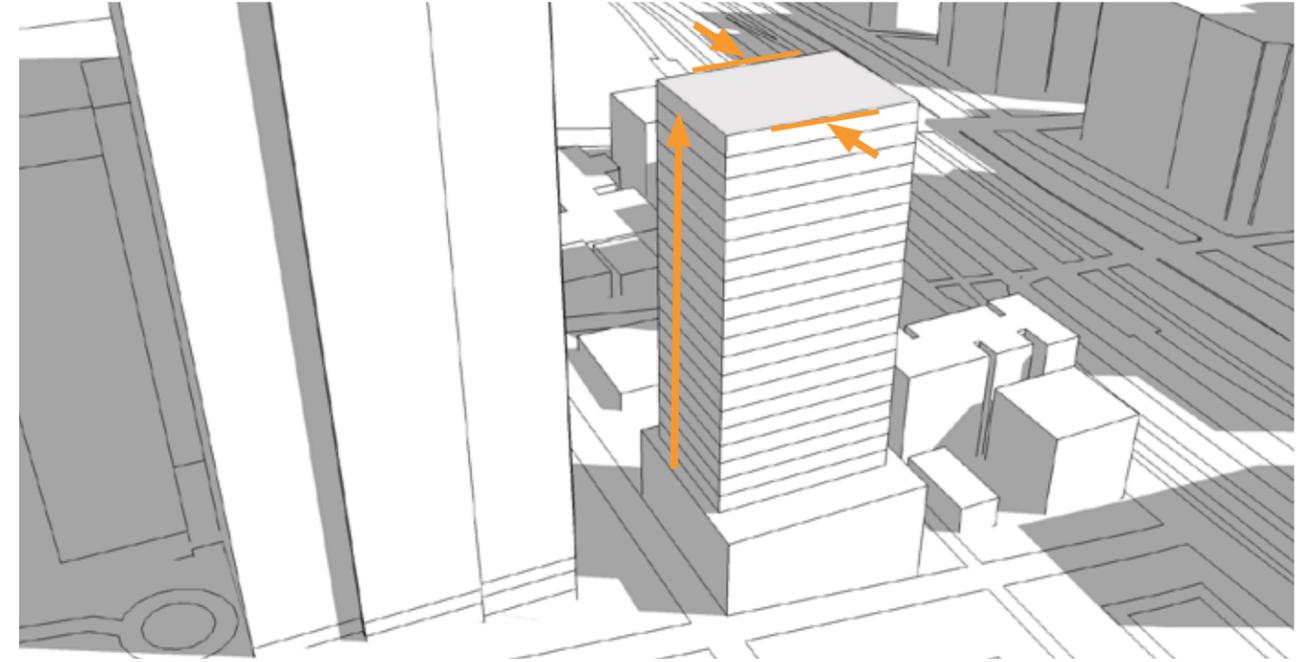
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CONCEPT 03: CURVED TOWER (PREFERRED)

TOWER MERGE INTO BASE



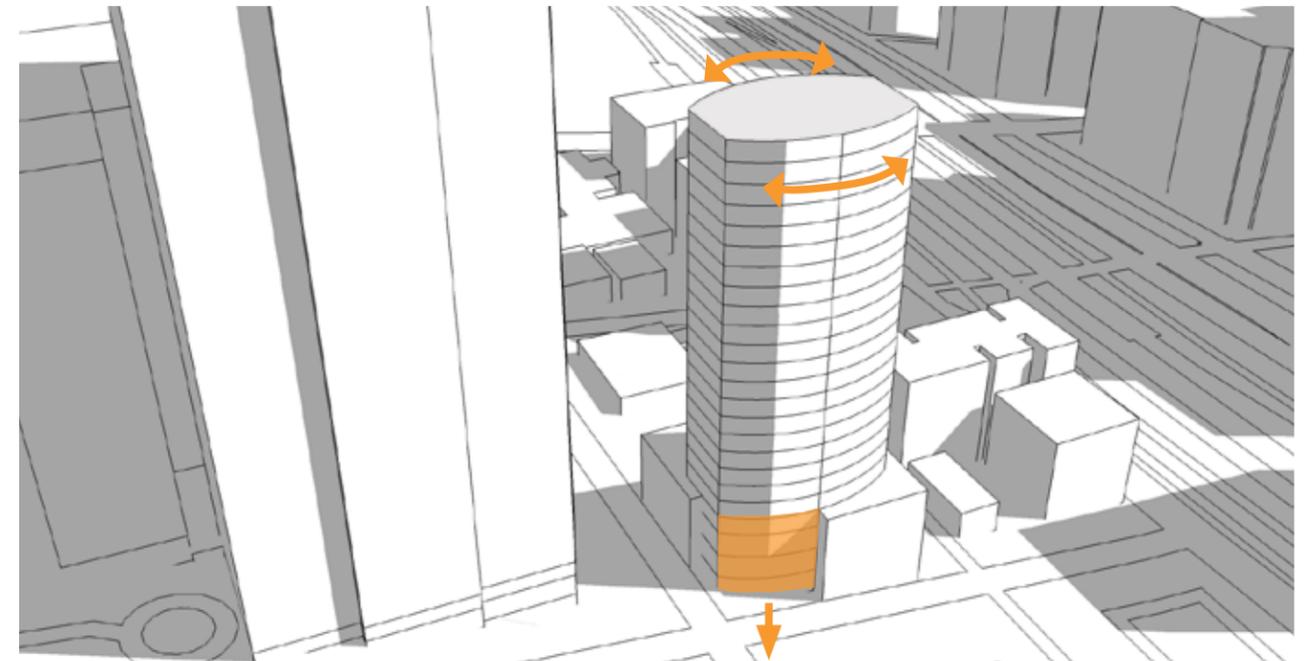
01: CODE COMPLIANT BASE ALLOWABLE MASSING



02: SMALLER FLOOR PLATES = TALLER AND MORE SLENDER TOWER



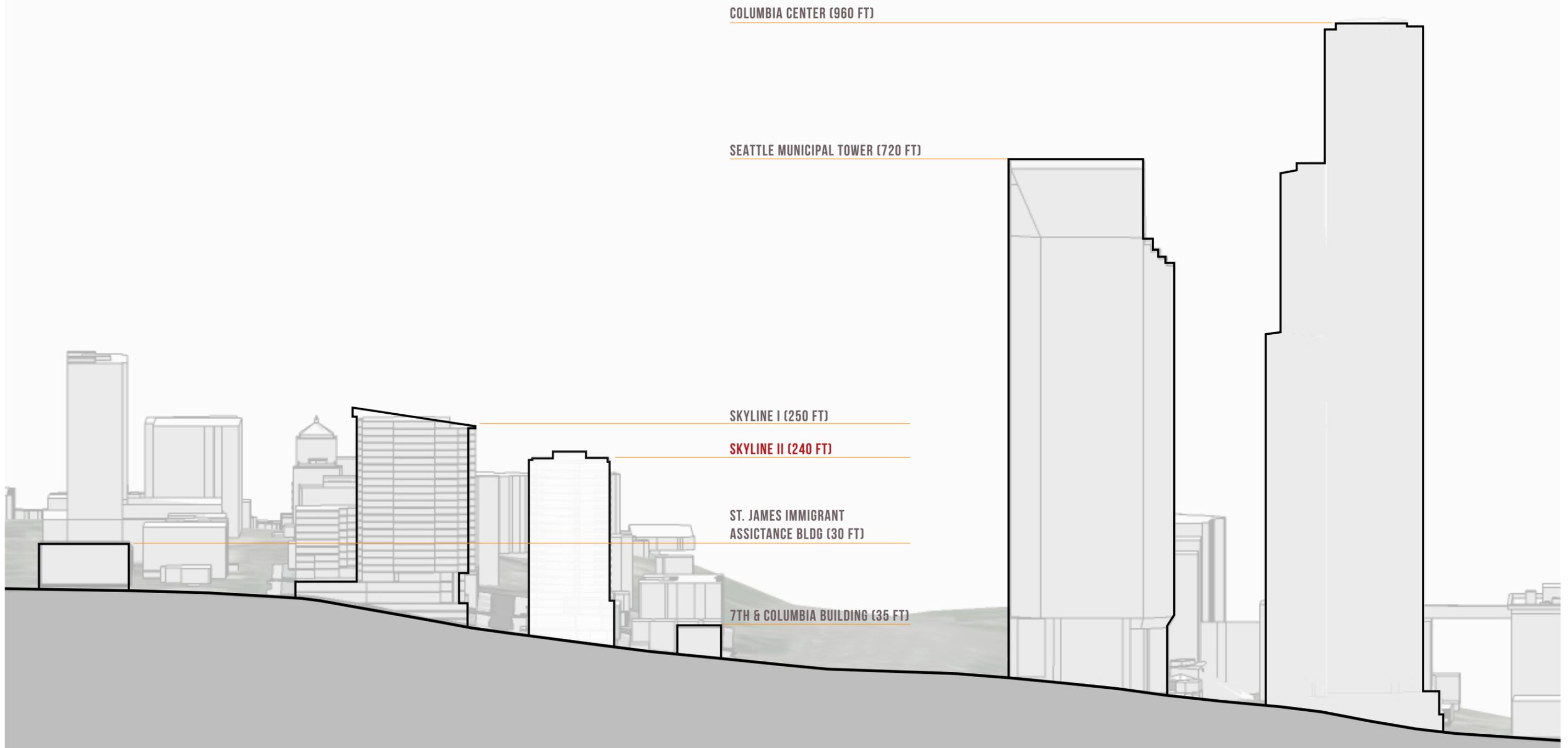
03: SHIFTING A TOWER CORE FOR PODIUM PROGRAM ELEMENTS



04: TOWER SHAPE AND ENHANCE VERTICALITY

CONCEPT 03: CURVED TOWER (PREFERRED)
TOWER MERGE INTO BASE

BUILDING ELEVATION AT COLUMBIA STREET LOOKING AT SOUTHEAST



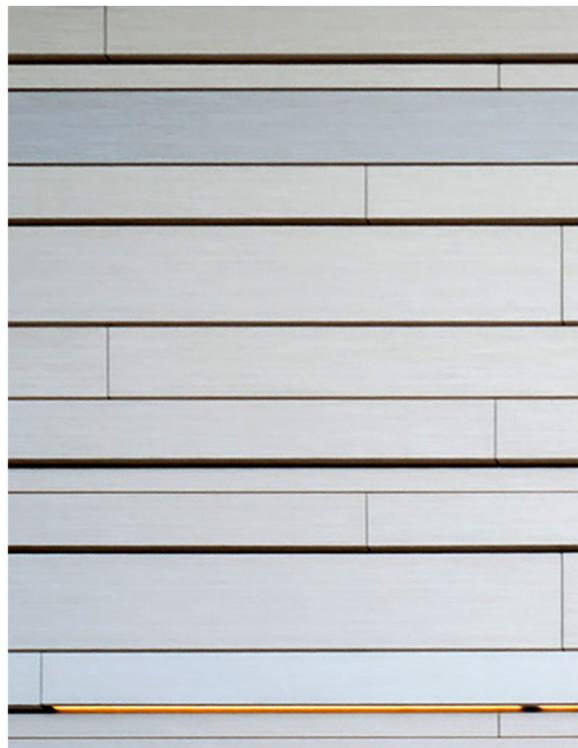
MATERIALS



01: ALUMINUM WINDOW WALL SYSTEM WITH SILVER MULLIONS



02: MECHANICAL PENTHOUSE SCREEN



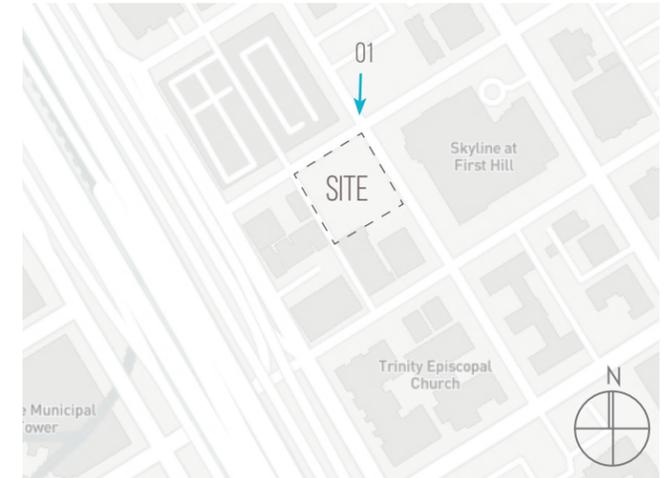
03: CLADDING AT BASE - METAL COMPOSITE PANEL



04: SOFFITS AT PODIUM - STAINED WOOD

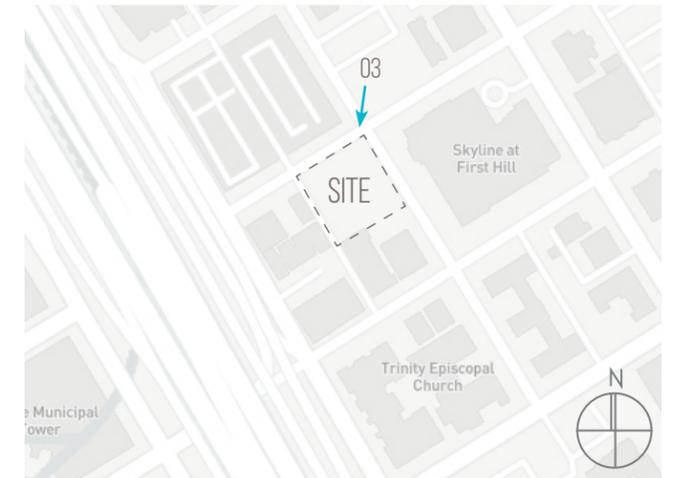


05: BOARD FROM CONCRETE



01





03

LANDSCAPE CONTEXT



Polyclinic parking landscape



800 Columbia open space

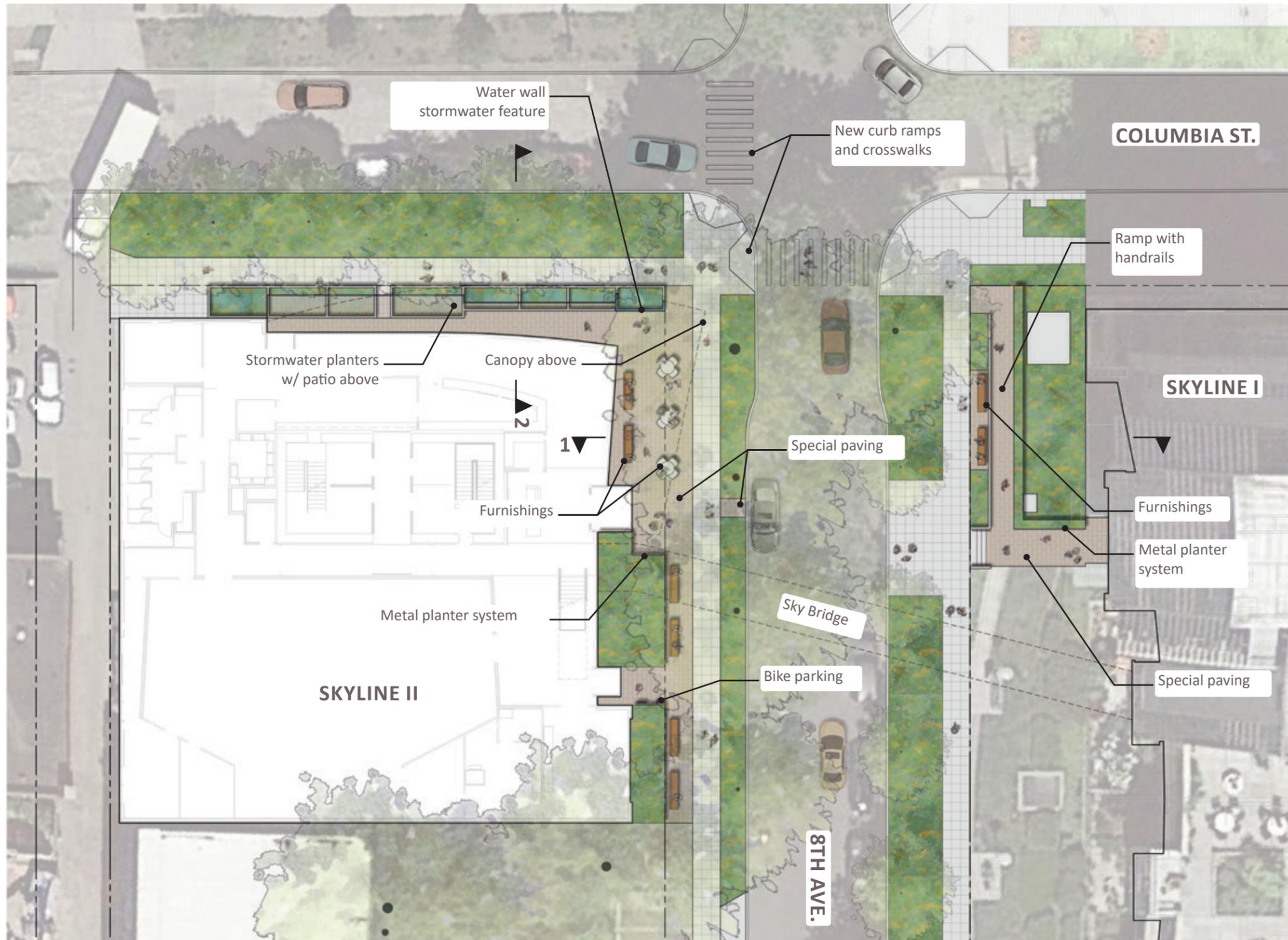


Bishop Lewis House



Skyline I

LANDSCAPE SITE PLAN

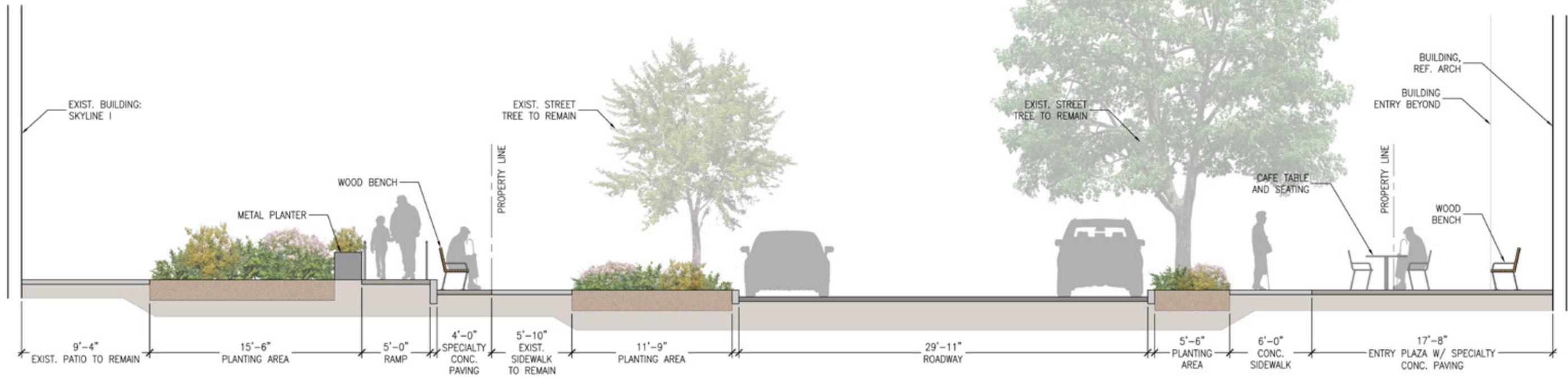


DESIGN APPROACH:

- Tie sites into Park to Park Loop and Green Street concept plan
- Reinforce walking corridors, wide sidewalks, generous landscape, rest points, and safe street crossings
- Create a “moment” in the Park to Park Loop
- Embrace design language found on adjacent streets (Columbia St., Cherry St., 8th Ave.)
- Unite Skyline buildings through common paving treatments, furnishings, and overall landscape language



LANDSCAPE SECTIONS

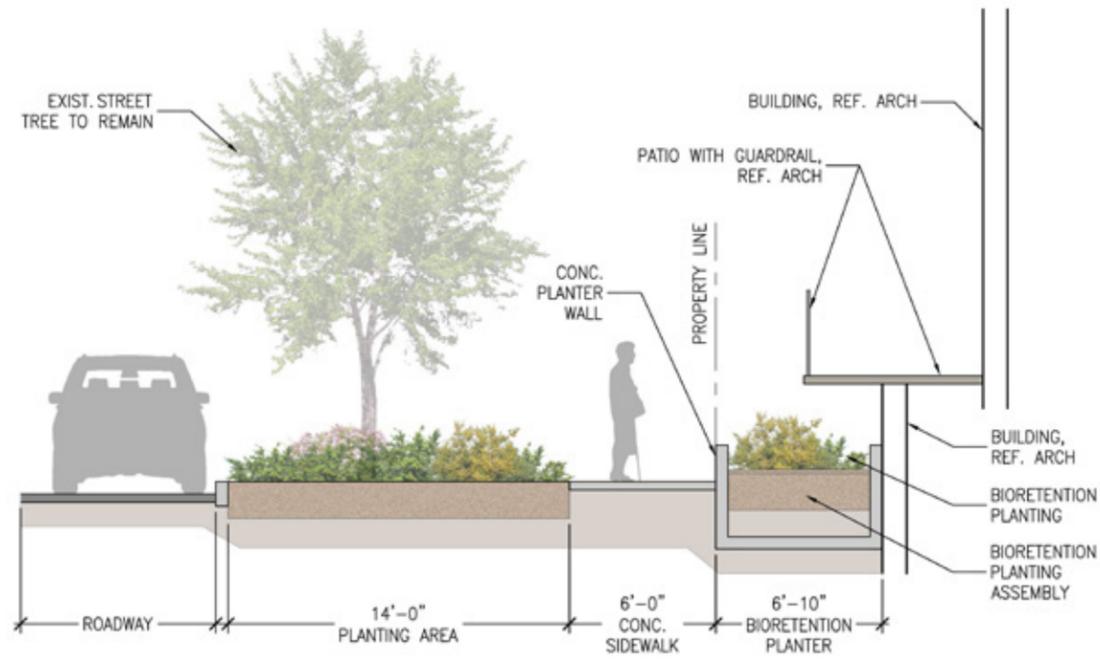


1 SECTION AT 8TH AVE.

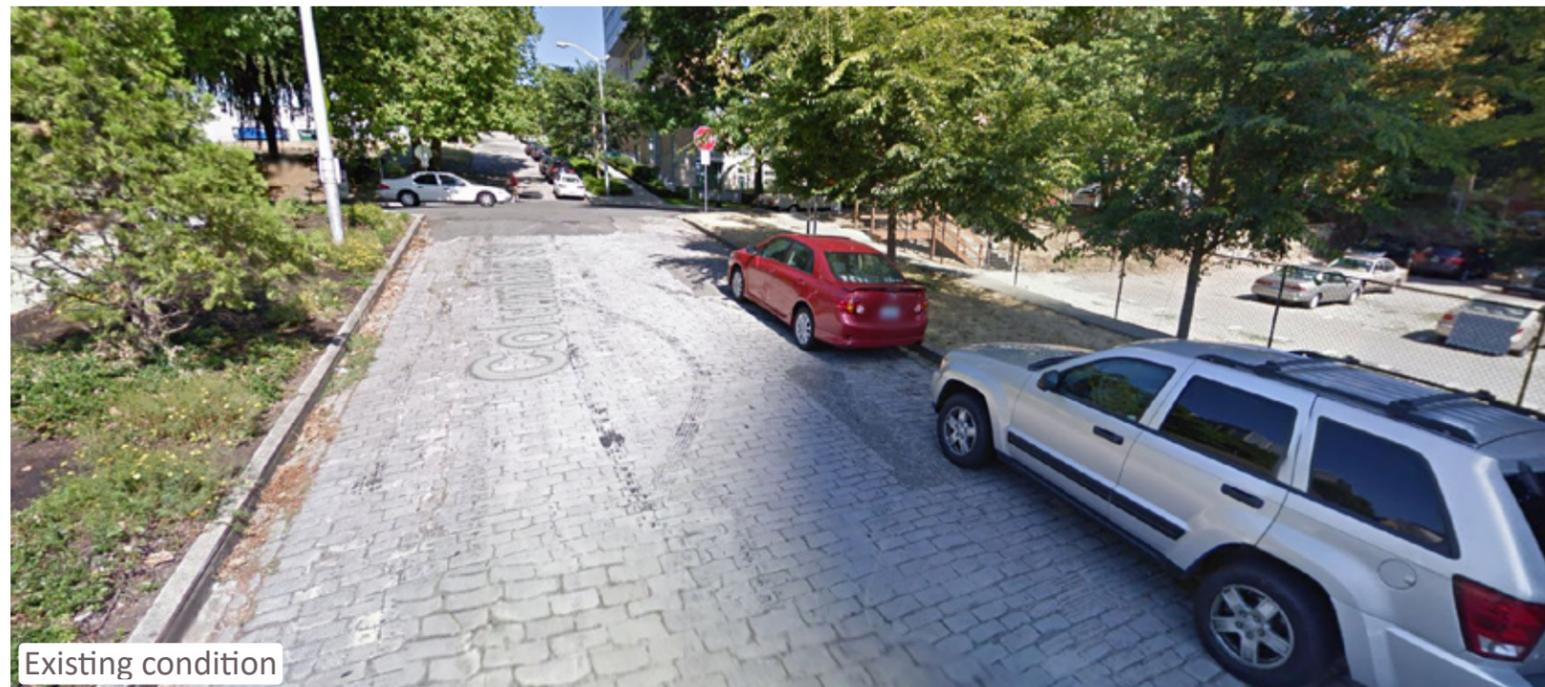


Existing condition

LANDSCAPE SECTIONS



2 SECTION AT COLUMBIA ST.
SCALE: 1/4" = 1'



Existing condition

PRECEDENT IMAGES - STREETScape



SPECIALTY CONCRETE PAVING



WOOD BENCHES



CAFE STYLE SEATING



METAL PLANTERS



CASCADING STORMWATER PLANTERS



CANOPY INTEGRATED LIGHTING

PRECEDENT IMAGES - ROOF TERRACE



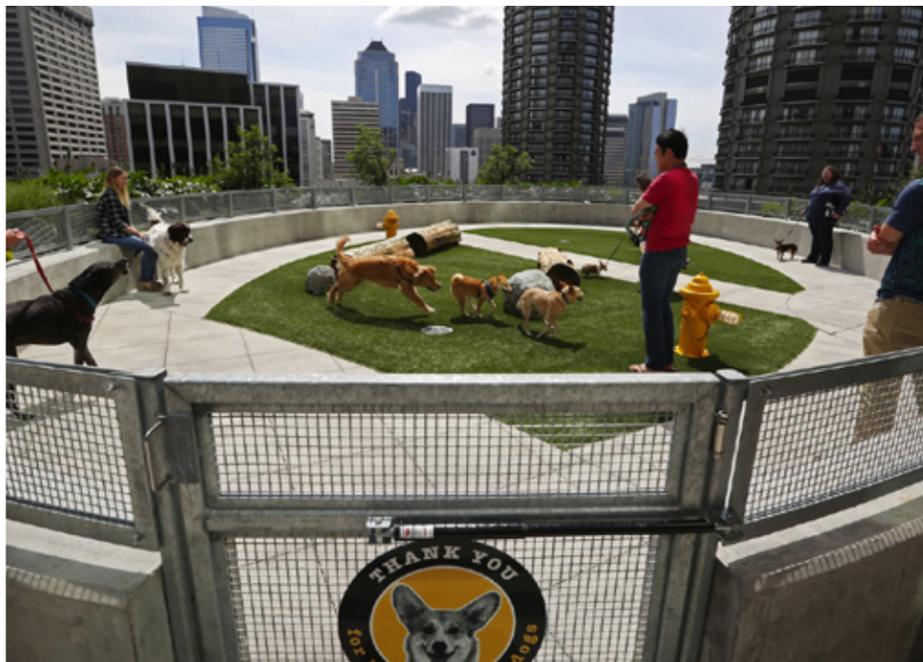
SEATING/LOUNGE AREAS



COVERED AREAS



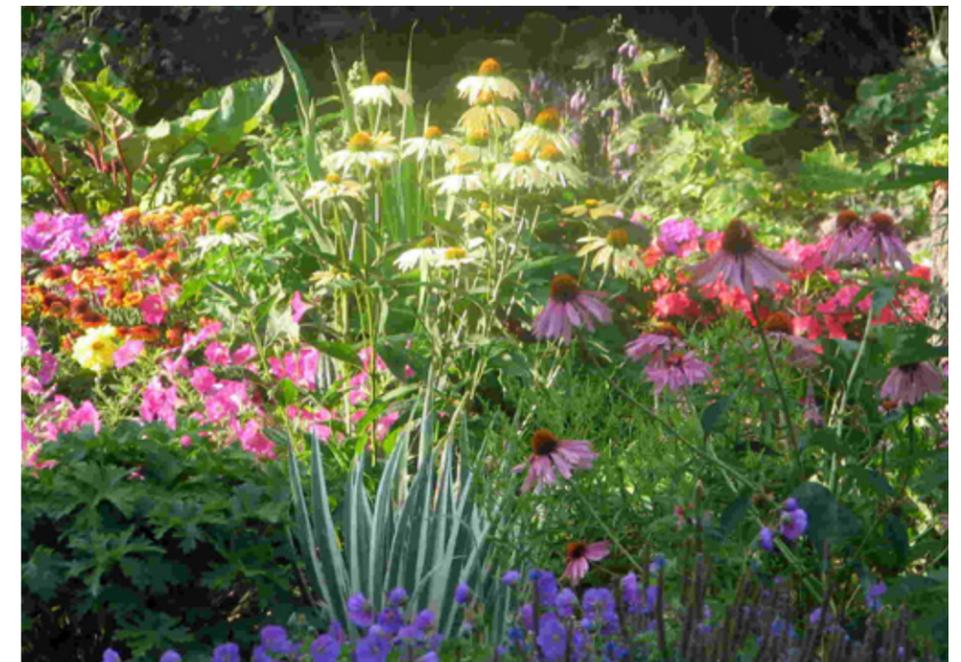
ACTIVE AREAS



DOG ZONE



FIRE PIT



LANDSCAPE/RESIDENT GARDENING

DEPARTURES

ITEM	SUMMARY	DEPARTURE REQUEST
1. Parking Aisles	<p>Parking aisle width shall be provided according to the requirements of Exhibit C for 23.54.030:</p> <ul style="list-style-type: none"> Parking angle: 90 degrees = Aisle Width: 22.0 feet 	<p>Project proposes reduced parking aisle width of 16'-10". Building footprint has been reduced to the extent possible while meeting minimum dimensions allowable in critical program spaces including providing Seattle City Light vault clearances, building core, and parking spaces.</p> <p>The proposed departure allows the project to provide underground parking that eliminates visual impacts of the parking from the streetscape while creating a generous pedestrian experience along the 8th Avenue designated green street that wraps the corner to Columbia Street with active interior spaces along these edges. Valet parking is used for all parking on site minimizing conflicts that could otherwise arise from individual resident parking.</p> <p>The following design guidelines will be better supported: CS2.B, PL1.A, DC1.A, DC1.C</p>
2. Setbacks and Separations	<p>45 feet or less in height</p> <p>Lot line that abuts neither a street nor alley: 7-foot average setback; 5-foot minimum setback, except that no setback is required for portions abutting an existing structure built to the abutting lot line</p>	<p>Project proposes reduced setback on lot line that abuts neither a street nor alley of 4-feet. Building footprint has been reduced to the extent possible while meeting minimum dimensions allowable in critical program spaces including providing Seattle City Light vault clearances, building core, and parking spaces.</p> <p>The proposed departure allows the project to provide underground parking that eliminates visual impacts of the parking from the streetscape while creating a generous pedestrian experience along the 8th Avenue designated green street that wraps the corner to Columbia Street with active interior spaces along these edges.</p> <p>The following design guidelines will be better supported: CS2.B, PL1.A, DC1.A, DC1.C</p>
3. Setbacks and Separations	<p>Greater than 45 feet in height</p> <p>Lot line abutting a street: 10-foot minimum setback Lot line abutting an alley: 10-foot minimum setback Lot line abuts neither a street nor alley: 20-foot minimum setback</p>	<p>Project proposes that podium extends into the setback limits for the portion of the building greater than 45 feet in height along a portion of the lot line abutting Columbia Street, a portion of the lot line abutting the adjacent property, and along the lot line abutting the alley a maximum of 17'-5".</p> <p>The proposed departure allows the project to provide a generous entry and pedestrian experience along the 8th Avenue designated green street. The datum line created in designing this primary building entry emphasizes the human scale of this space and carries throughout the design of all facades, creating an architectural composition which establishes a strong podium.</p> <p>The following design guidelines will be better supported: PL1.A, DC2.B.1; DC2.D.1</p>

PARKING AISLES

SMC 23.54.030..E

Parking aisle width shall be provided according to the requirements of Exhibit C for 23.54.030:

- Parking angle: 90 degrees = Aisle Width: 22.0 feet

DEPARTURE REQUEST

Project proposes reduced parking aisle width of 19'-2" typical and 16'-10" minimum. Building footprint has been reduced to the extent possible while meeting minimum dimensions allowable in critical program spaces including providing Seattle City Light vault clearances, building core, and parking spaces.

The proposed departure allows the project to provide underground parking that eliminates visual impacts of the parking from the streetscape while creating a generous pedestrian experience along the 8th Avenue designated green street that wraps the corner to Columbia Street with active interior spaces along these edges. Valet parking is used for all parking on site minimizing conflicts that could otherwise arise from individual resident parking.

The following design guidelines will be better supported: CS2.B, PL1.A, DC1.A, DC1.C

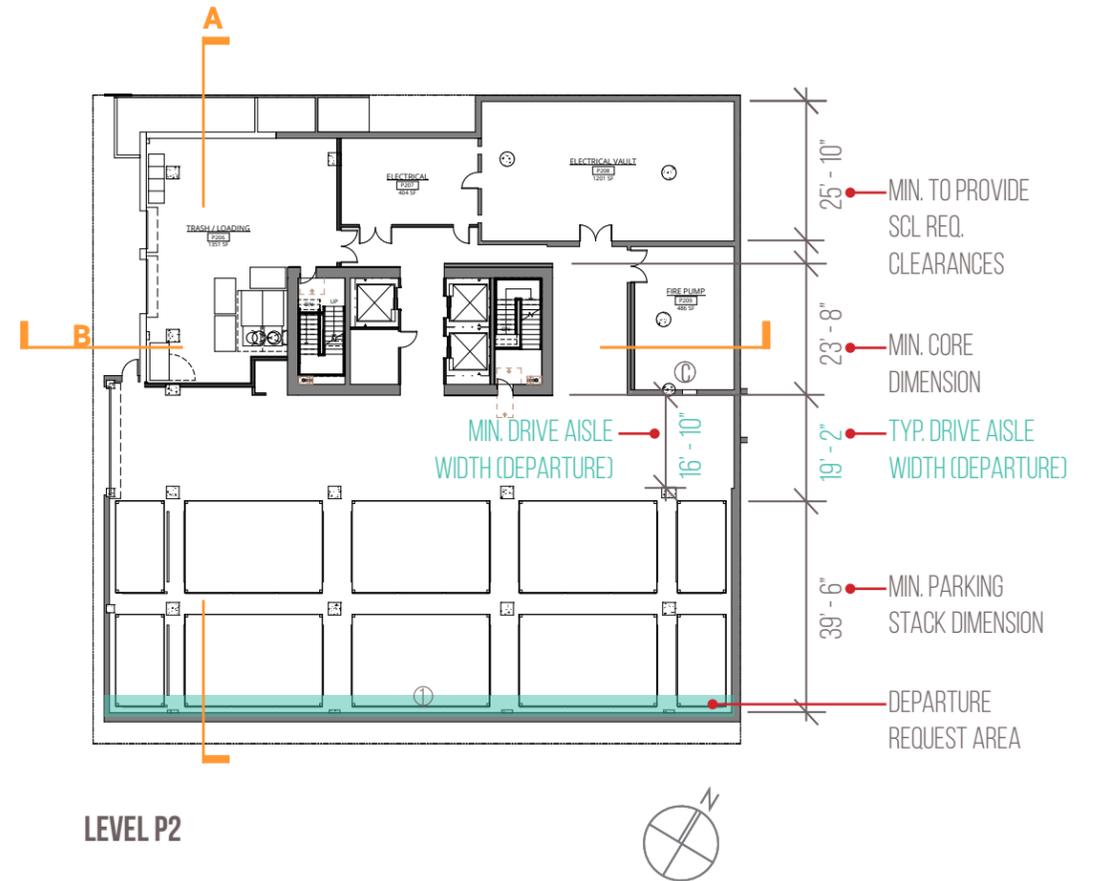
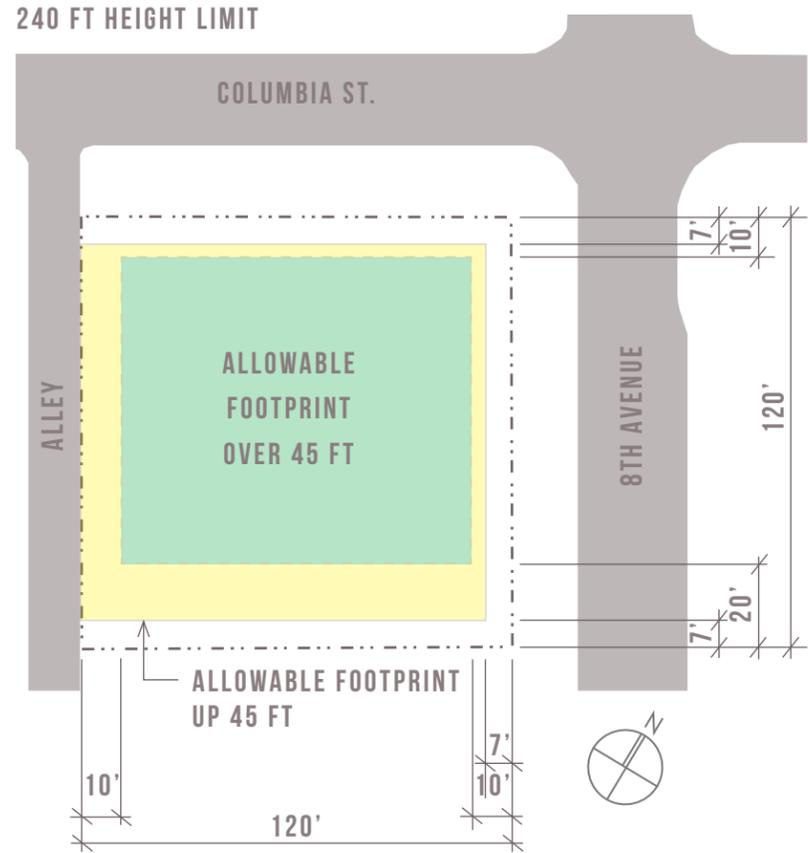
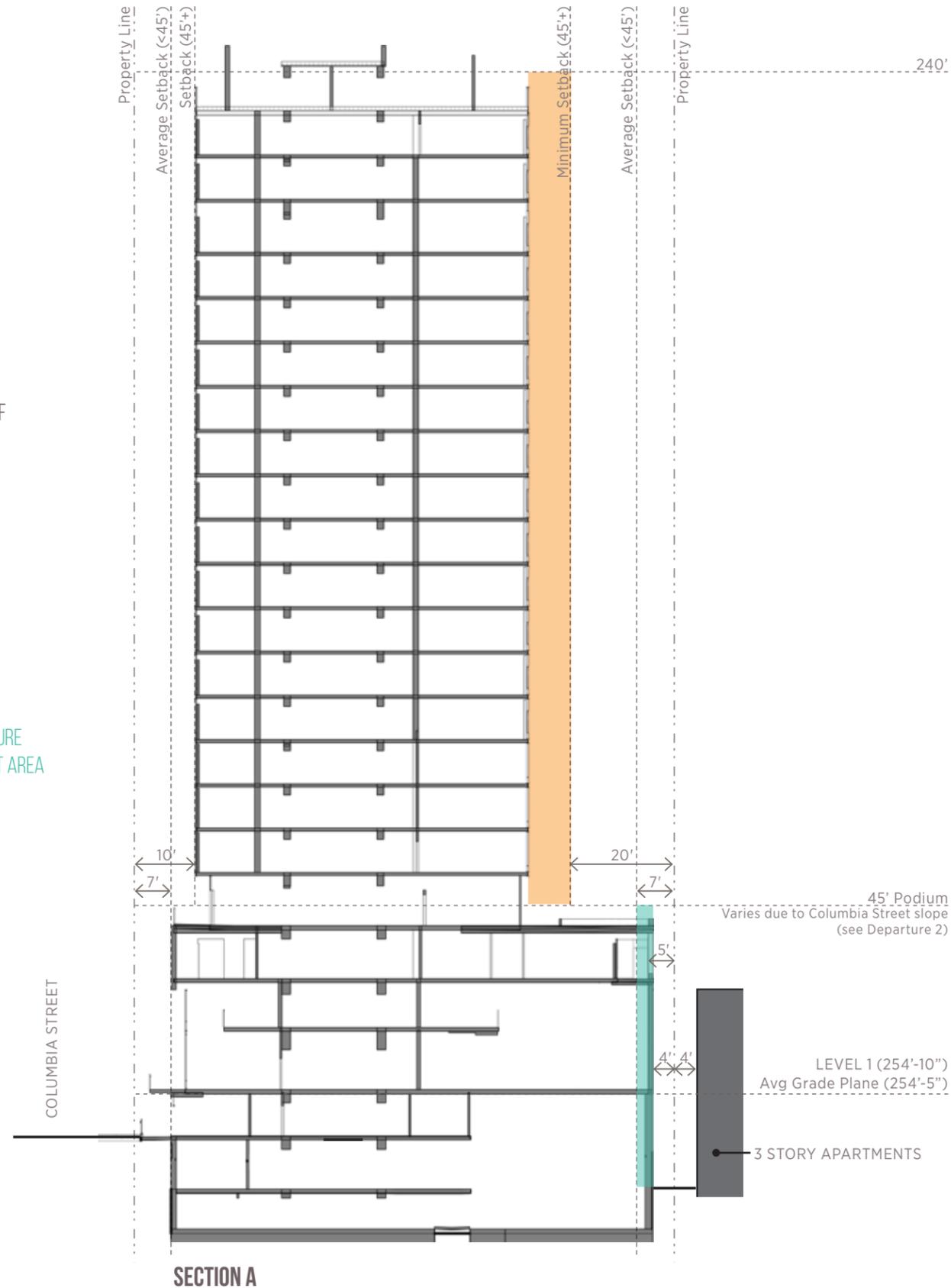
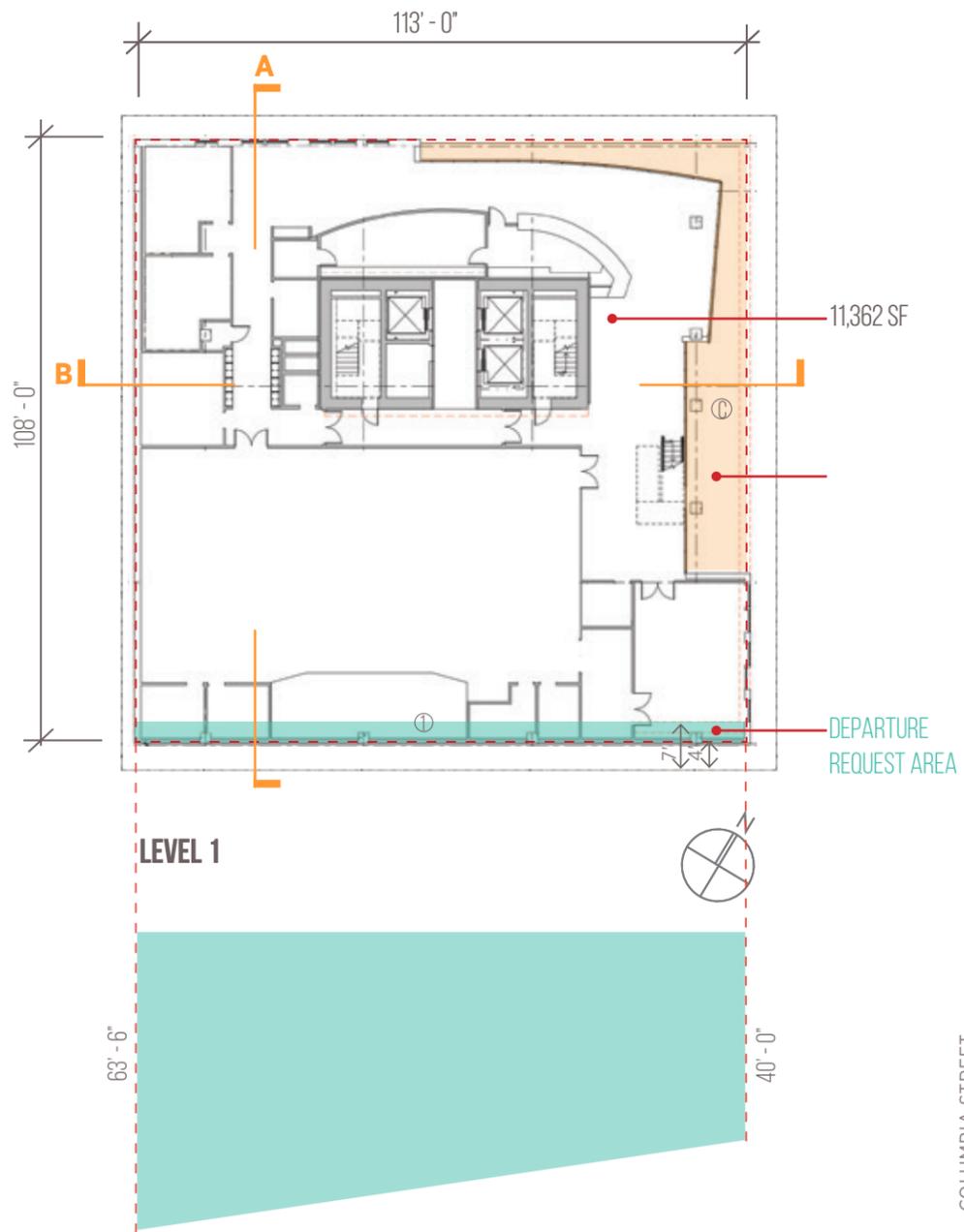


Table C - HR Setbacks	Setbacks for structures greater than 85 feet in height
Lot line that abuts neither a street nor alley	For portions of a structure: <ul style="list-style-type: none"> • 45 feet or less in height: 7-foot average setback; 5-foot minimum setback, except that no setback is required for portions abutting an existing structure built to the abutting lot line; • Greater than 45 feet in height: 20-foot minimum setback



SETBACKS AND SEPARATIONS: 45 FEET OR LESS IN HEIGHT

SMC 23.45.518

Lot line that abuts neither a street nor alley:

7-foot average setback; 5-foot minimum setback, except that no setback is required for portions abutting an existing structure built to the abutting lot line

DEPARTURE REQUEST

Project proposes reduced setback on lot line that abuts neither a street nor alley of 4-feet. Building footprint has been reduced to the extent possible while meeting minimum dimensions allowable in critical program spaces including providing Seattle City Light vault clearances, building core, and parking spaces.

The proposed departure allows the project to provide underground parking that eliminates visual impacts of the parking from the streetscape while creating a generous pedestrian experience along the 8th Avenue designated green street that wraps the corner to Columbia Street with active interior spaces along these edges.

The following design guidelines will be better supported: CS2.B, PL1.A, DC1.A, DC1.C

LEGEND

- POTENTIAL DEVELOPMENT VOLUME NOT IN USE
- Ⓐ (9,000 SF - 7,223 SF) X 189' - 0" = **335,853 CUBIC FEET**
- DEPARTURE REQUEST AREA
- Ⓚ 17,544 CUBIC FEET
- Ⓛ 16,705 CUBIC FEET
- Ⓚ + Ⓛ = **34,249 CUBIC FEET**

DEPARTURES

SETBACKS AND SEPARATIONS: GREATER THAN 45 FEET IN HEIGHT

SMC 23.45.518

Lot line abutting a street: 10-foot minimum setback

Lot line abutting an alley: 10-foot minimum setback

Lot line abuts neither a street nor alley: 20-foot minimum setback

DEPARTURE REQUEST

Project proposes that podium extends into the setback limits for the portion of the building greater than 45 feet in height along a portion of the lot line abutting Columbia Street, a portion of the lot line abutting the adjacent property, and along the lot line abutting the alley a maximum of 17'-5".

The proposed departure allows the project to provide a generous entry and pedestrian experience along the 8th Avenue designated green street. The datum line created in designing this primary building entry emphasizes the human scale of this space and carries throughout the design of all facades, creating an architectural composition which establishes a strong podium.

The following design guidelines will be better supported: PL1.A, DC2.B, DC2.D

LEGEND

POTENTIAL DEVELOPMENT VOLUME NOT IN USE
 Ⓐ (9,000 SF - 7,223 SF) X 189' - 0" = **335,853 CUBIC FEET**

DEPARTURE REQUEST AREA
 ① 17,544 CUBIC FEET
 ② 16,705 CUBIC FEET
 ① + ② = **34,249 CUBIC FEET**

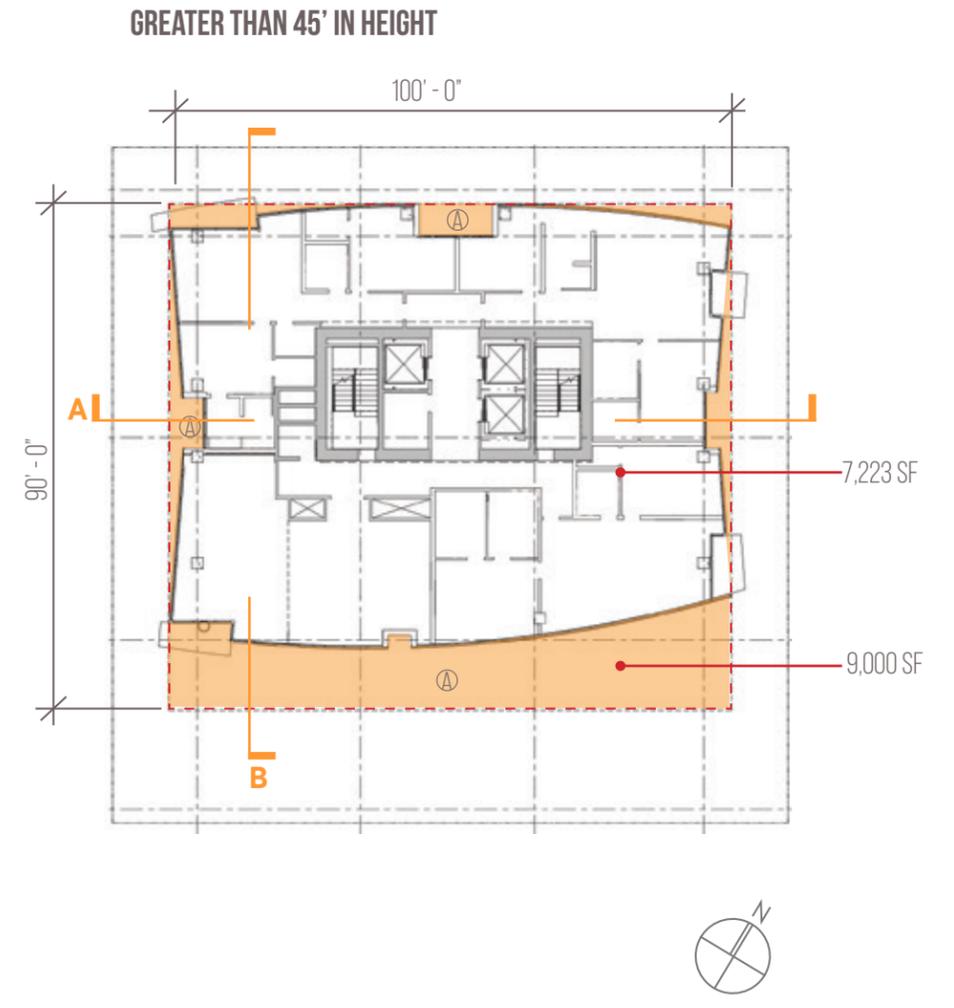
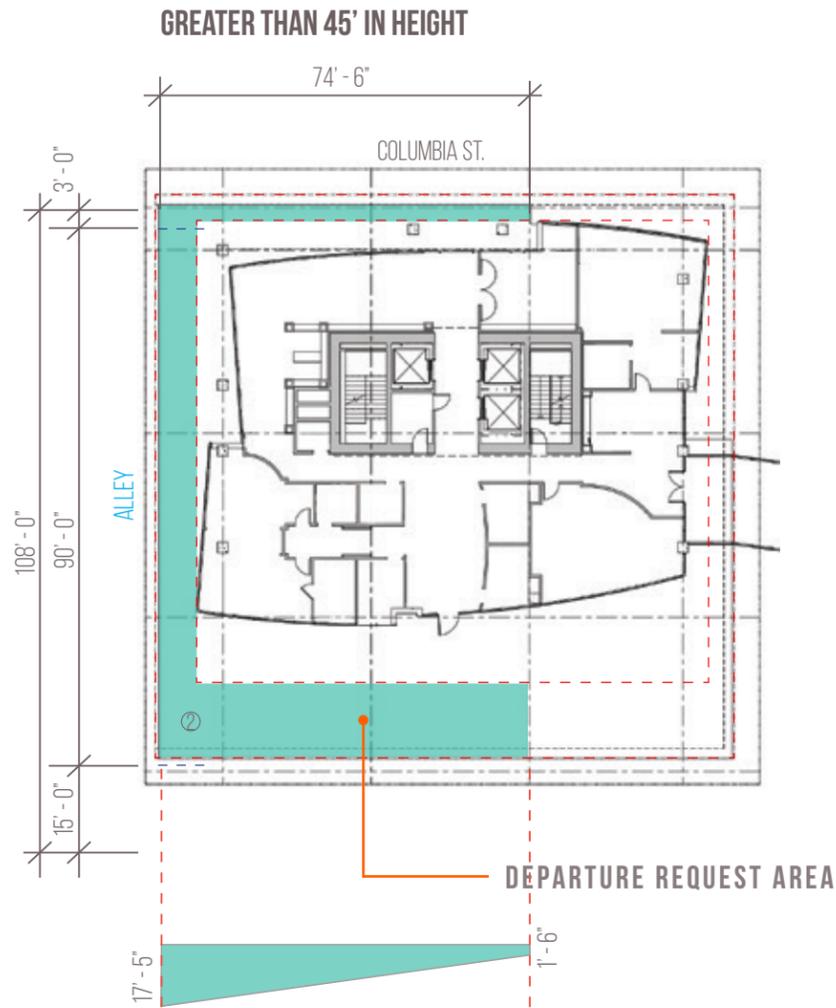
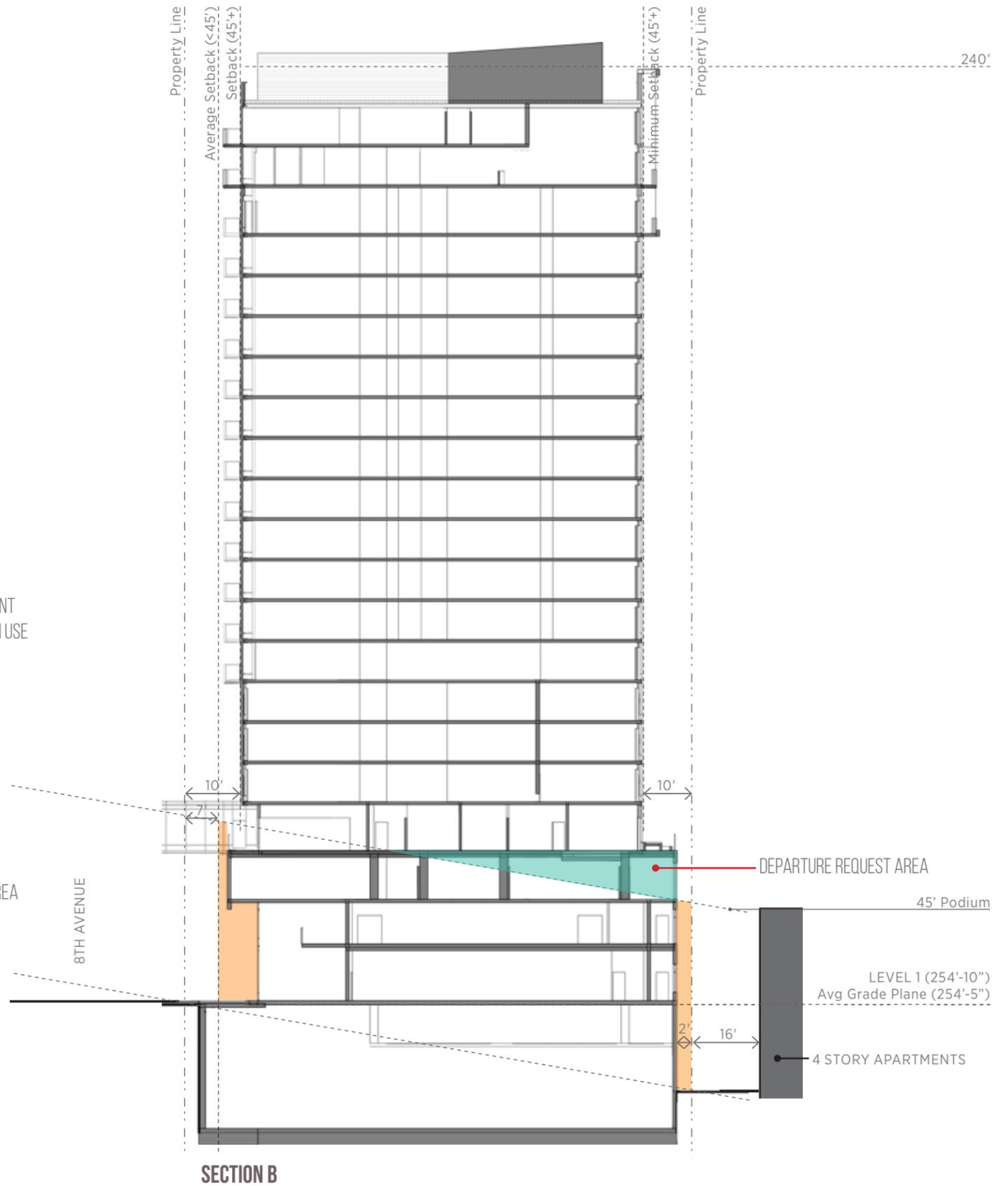
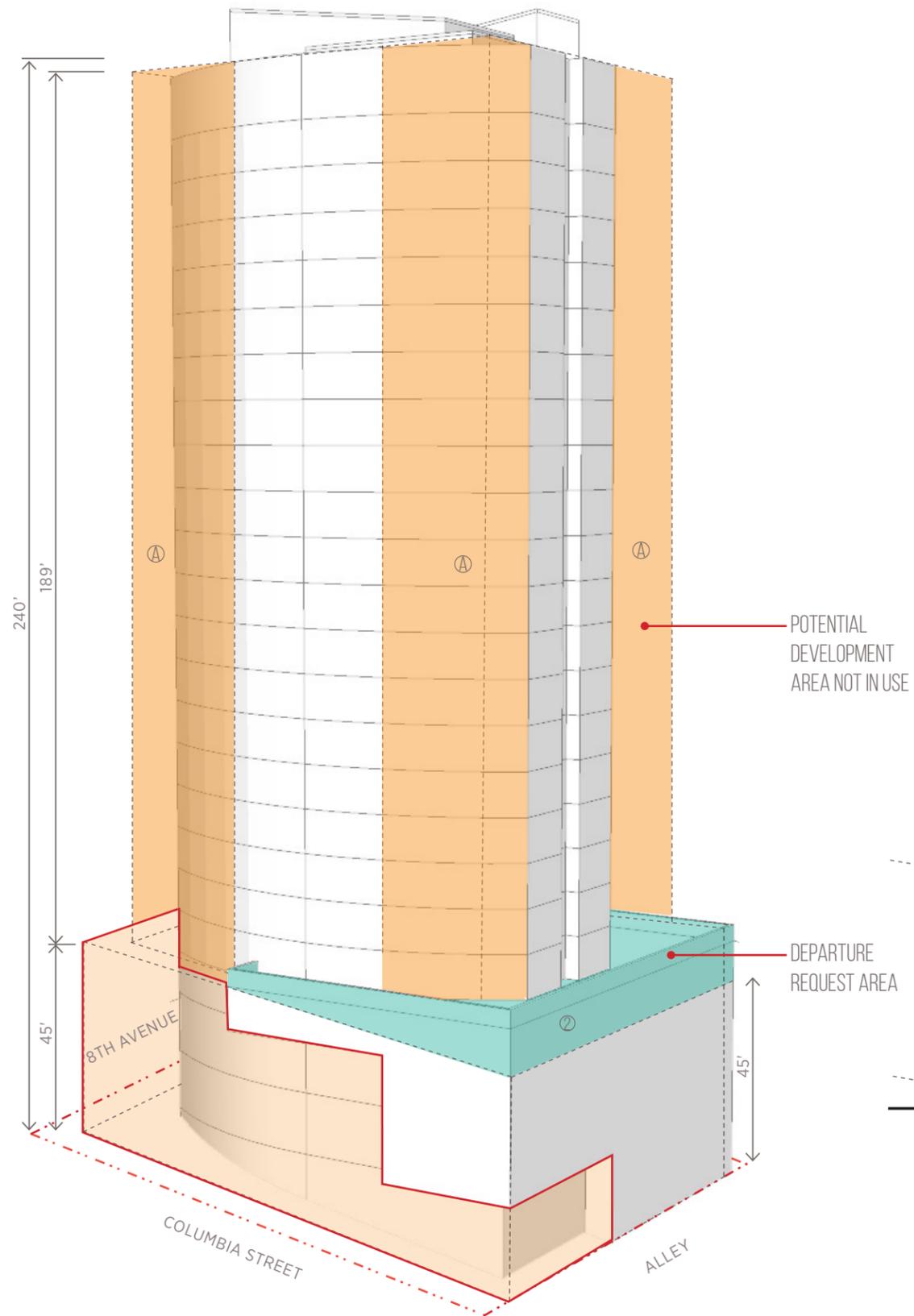


Table C - HR Setbacks	Setbacks for structures greater than 85 feet in height
Lot line abutting a street	For portions of a structure: <ul style="list-style-type: none"> 45 feet or less in height: 7-foot average setback; 5-foot minimum setback, except that no setback is required for frontages occupied by street level uses or dwelling units with a direct entry from the street; Greater than 45 feet in height: 10-foot minimum setback
Lot line abutting an alley	Rear lot line abuts an alley: For portions of a structure: <ul style="list-style-type: none"> 45 feet or less in height: no setback required Greater than 45 feet in height: 10-foot minimum setback
Lot line that abuts neither a street nor alley	For portions of a structure: <ul style="list-style-type: none"> 45 feet or less in height: 7-foot average setback; 5-foot minimum setback, except that no setback is required for portions abutting an existing structure built to the abutting lot line; Greater than 45 feet in height: 20-foot minimum setback



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