

1.0 | CHECKLIST PACKET STANDARDS

CLIENT:
NORTHWEST KIDNEY CENTERS

PROJECT NAME:
RAINIER BEACH CLINIC

PROJECT ADDRESS [2.1]:
4401 SOUTH TRENTON STREET
SEATTLE, WA 98118

SDCI PROJECT NUMBERS [2.2]:
6590658 (SDCI BUILDING PERMIT)
3027973 (SDCI LAND USE)

DESIGN REVIEW MEETING DATE [2.3]:
13 FEBRUARY, 2018

EDG MEETING DATE:
22 AUGUST, 2017

PROJECT TEAM:
 :: SPECTRUM DEVELOPMENT SOLUTIONS
 :: MAHLUM ARCHITECTS
 :: LPD ENGINEERING, PLLC
 :: BRUMBAUGH & ASSOCIATES
 :: ALDRICH + ASSOCIATES, INC.
 :: PCS STRUCTURE SOLUTIONS
 :: EMERALD AIRE, INC.
 :: STIRRETT JOHNSEN, INC.
 :: NORTHSTAR ELECTRICAL
 :: PANGEO, INC.

Section	Information to be included in each Section
1.0 Packet Standards	<ul style="list-style-type: none"> ✓ 1.1: 11x17 sized design packets. Stapled or side bound. ✓ 1.2: Printed double sided and in color. (No plastic covers). ✓ 1.3: All pages numbered. ✓ 1.4: All type fonts legible and 1/8" minimum ✓ 1.5: All graphics should be oriented consistently with street names and north arrows. ✓ 1.6: All drawings should fill the 11"x17" page; 1" white margins are adequate.
2.0 Cover	<ul style="list-style-type: none"> ✓ 2.1: Project address. ✓ 2.2: Project number. ✓ 2.3: Meeting type and meeting date.
3.0 Proposal	<ul style="list-style-type: none"> ✓ 3.1: Number of residential units (approx). ✓ 3.2: Amount of total commercial square footage, number of live work units (approx). ✓ 3.3: Number and location of parking stalls (approx). ✓ 3.4: Aerial photograph with streets and site labeled (9 block area).
4.0 Summary Context Analysis	<ul style="list-style-type: none"> ✓ 4.1: Aerial photograph with streets and site labeled. ✓ 4.2: Vicinity map, indicating surrounding uses, structures, zoning, and overlay designations. ✓ 4.3: Axonometric or other three-dimensional drawing, photos or models of the nine block area (3 block x 3 block area) surrounding the project site.
5.0 Existing Site Conditions	<ul style="list-style-type: none"> ✓ 5.1: 9-block (3 block x 3 block area) map of zoning, existing uses and structures.
6.0 Zoning Data	<ul style="list-style-type: none"> ✓ 6.1: Brief summary of applicable development standards and how proposed development will meet these standards. (1 page)
7.0 Composite Site Plan	<ul style="list-style-type: none"> ✓ 7.1: Building ground floor and use labels. ✓ 7.2: Basic site landscape elements and features. Identify any Exceptional Trees and tree protection areas. ✓ 7.3: Vehicular and pedestrian access. ✓ 7.4: All property lines (in red) with dimensions. ✓ 7.5: Spot elevations at property corners, setbacks and dimensions. ✓ 7.6: Street names, curb lines and trees including dimensions. ✓ 7.7: Adjacent buildings and features (within 30' min) including basic landscaping/trees, building footprints and uses if known. ✓ 7.8: Other useful site/context information. ✓ 7.9: Dimensions.
8.0 Itemized Response to EDG	<ul style="list-style-type: none"> ✓ 8.1: Brief description of how the proposal meets the intent of the applicable citywide and neighborhood design review guidelines. ✓ 8.2: May include pictures or diagram of project specifics to contextualize or visually demonstrate. ✓ 8.3: Item by item response (2-3 lines) to specific guidance from the EDG report.
9.0 Floor Plans	<ul style="list-style-type: none"> ✓ 9.1: Include property lines. ✓ 9.2: Identify major uses and elements for all floors. ✓ 9.3: Clearly indicate residential entries/primary entry/commercial entries and spot elevations. ✓ 9.4: Clearly indicate vehicular access. ✓ 9.5: Building modulation (including solid and glazing at walls and structural columns). ✓ 9.6: Major grade changes, retaining walls and relevant spot elevations. ✓ 9.7: Setbacks, including dimensions. ✓ 9.8: Adjacent building locations within 20' of property line, features (courtyard, setbacks) and ground floor uses. ✓ 9.9: Indication of any sections provided.

Section	Information to be included in each Section
10.0 Composite Landscape/Hardscape Plan	<ul style="list-style-type: none"> ✓ 10.1: Ground plane and all roof gardens, terraces, etc. ✓ 10.2: Include basic material notes. (Species and size). ✓ 10.3: Provide photo/images of specified plants. ✓ 10.4: Design of seating types, and other special features. ✓ 10.5: Design of soft and hardscape materials.
11.0 Elevations	<ul style="list-style-type: none"> ✓ 11.1: Include property lines; corner spot elevations; height dimensions (overall from grade and floor-to-floors). ✓ 11.2: Basic material textures and notes, especially along entire ground levels. ✓ 11.3: Include scale figures and "transparent" street trees/landscape foliage. ✓ 11.4: Shadows to show modulation optional, but should use light tone to not obscure architectural finishes. Shadow lines should accurately reflect depth of modulation. ✓ 11.5: When privacy impacts to neighbors are an issue, include fenestration plans of the facing elevations to demonstrate minimizing direct views between the buildings. ✓ 11.6: Exterior venting locations and designs. ✓ 11.7: Materials call outs – see Material and Color Palette below.
12.0 Material and Color Palette	<ul style="list-style-type: none"> ✓ 12.1: Material and descriptive color details. ✓ 12.2: Material callouts on elevations that correspond to and are keyed to the palette. ✓ 12.3: Photograph of material and color board with accurate colors and materials call-outs. ✓ 12.4: (Physical color and material board should be provided at meeting itself; where colors/materials are critical issues, Planner may request physical samples or reduced size palette board for the file).
13.0 Renderings	<ul style="list-style-type: none"> ✓ 13.1: Photo-realistic, eye-level perspective renderings from at least two opposite street locations, showing all corners if larger project; include at least one full lot of adjacent context/buildings with basic massing and character, or accurate photo-montage preferred; street trees must be highly transparent; accurate but not obscuring black shadows; minimize cars and foreground features to not conceal ground level architecture. ✓ 13.2: Highlight special components, such as courtyard, street level experience or views of site.
14.0 Exterior Lighting Plan	<ul style="list-style-type: none"> ✓ 14.1: Approximate location of exterior light fixtures and light spillage at night. ✓ 14.2: Design of fixtures.
15.0 Signage Concept Plan	<ul style="list-style-type: none"> ✓ 15.1: Signage plan indicating areas on the building/site where signage might be mounted, any specific building identification signage and any building management signage plan outlining requirements for signage design.
16.0 Building Sections	<ul style="list-style-type: none"> ✓ 16.1: Include property lines. ✓ 16.2: Include corner spot elevations. ✓ 16.3: Include height dimensions (overall from grade and floor-to-floor). ✓ 16.4: Include grade lines and approximate profile of adjacent buildings.
17.0 Departures	<ul style="list-style-type: none"> ✓ 17.1: Departure summary table for <u>all development standard departures</u>, including the following: <ul style="list-style-type: none"> ✓ Code citation; ✓ Code requirement; ✓ Proposed design departure; ✓ Rationale explaining how the departure results in a project which better meets the intent of the design guidelines; ✓ 17.2: Graphics as needed to specifically identify the area of the departure(s) showing code compliant and requested departure with dimensions.
18.0 Other	<ul style="list-style-type: none"> ✓ 18.1: Other graphics as specified during the EDG phase.

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2.0 | COVER

PROJECT INFORMATION

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3.0 | PROPOSAL

PROJECT DESCRIPTION & OBJECTIVES

SITE ADDRESS:

4401 S TRENTON ST 98118

PARCEL NUMBER:

680410-0005

LEGAL DESCRIPTION:

PIONEER ADD PCL A SEATTLE LBA #9905264 REC #20000424900010
SD LBA DAF - LOTS 1 THRU 8 SD BLK 1 LESS TRANS R/W

PLAT BLOCK: 1

PLAT LOT: 1 THRU 8

LOT SIZE:

29961 SF

RESIDENTIAL UNITS [3.1]:

N/A

COMMERCIAL AREA [3.2]:

12,073 GSF

LIVE/WORK UNITS [3.3]:

N/A

PARKING SPACES:

20 SPACES IN SIDE LOT (SEE SECTION 7.0 | SITE PLAN)

GREEN SPACES:

8679 SF

SIZE:

-1-STORY, 12,073 GSF DIALYSIS CLINIC FOR NORTHWEST KIDNEY CENTERS

LOCATION:

- LOCATED OFF OF S. TRENTON STREET AND NEAR RENTON AVENUE S
- WITHIN 1/4 MILE OF LIGHT RAIL AND BUS STOPS
- ADJACENT TO CHIEF SEALTH TRAIL

FUNCTION:

- NEW 12-STATION KIDNEY TREATMENT CENTER
- TRAINING CENTER FOR HOME DIALYSIS
- PATIENT EDUCATION SPACES
- STAFF OFFICES AND SUPPORT SPACES
- COMMUNITY OUTREACH AND EDUCATION AROUND NUTRITION & KIDNEY DISEASE

FEATURES:

- PUBLIC FUNCTIONS SITUATED ALONG STREET FRONTAGE
- PARKING LOT WITH 20 PARKING STALLS (4 ADA STALLS WITHIN CLOSE PROXIMITY TO ENTRANCE FOR PATIENTS)
- CLEAR, WELCOMING SPACE AT THE BUILDING ENTRANCE AND DROP-OFF ZONE WITH CURB CUTS
- COURTYARDS FOR DAYLIGHTING PRIVATE CLINIC SPACES

COMMUNITY OUTREACH:

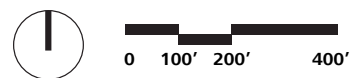
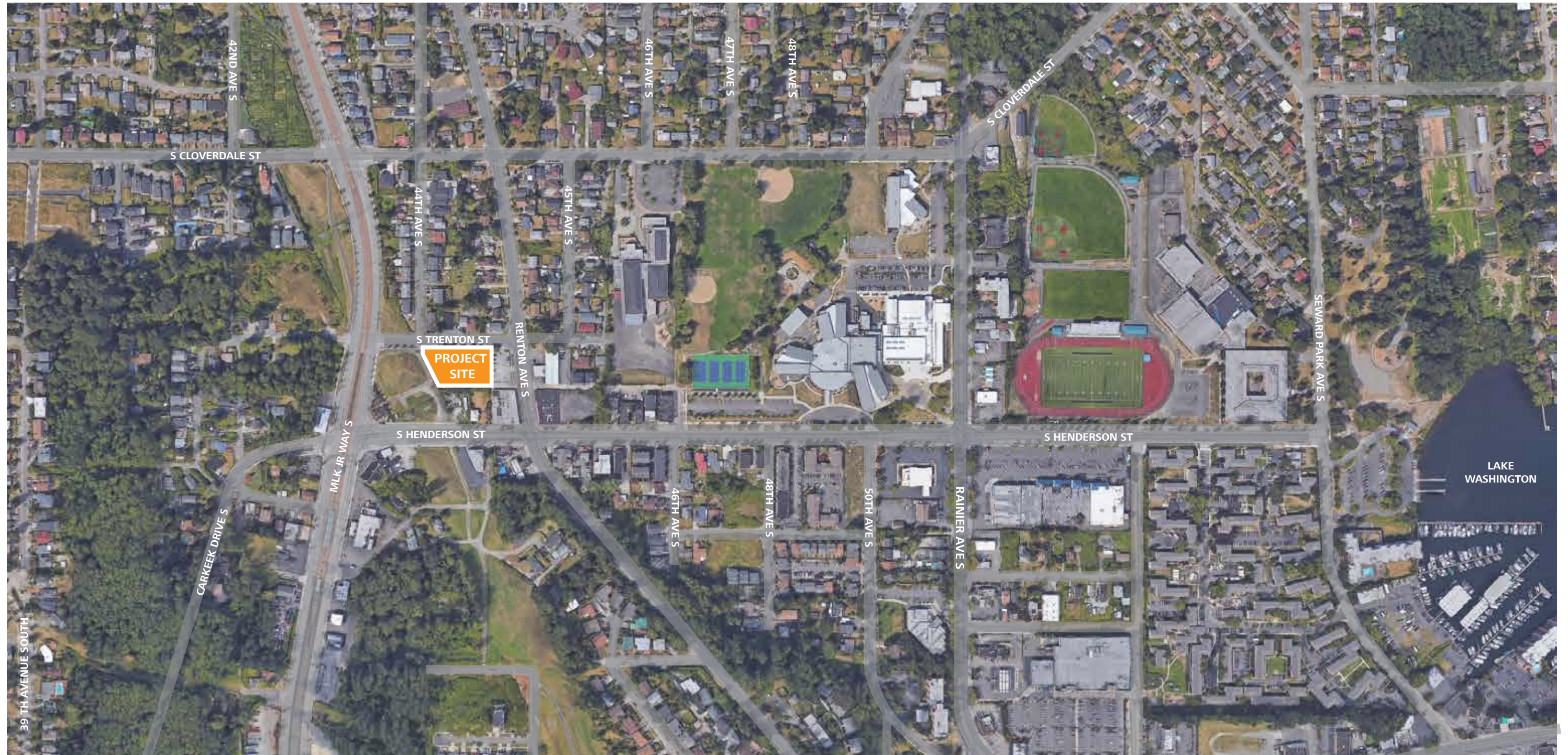
- A COMMUNITY OUTREACH MEETING WAS HELD AUGUST 8, 2017 TO DISCUSS THE PROPOSED PROJECT WITH THE COMMUNITY AND TO ANSWER QUESTIONS

PROJECT OBJECTIVES:

- ACTIVATE TRENTON STREET.
- PROVIDING ESSENTIAL COMMUNITY SERVICE THROUGH DIALYSIS TREATMENT, HOME DIALYSIS TRAINING, GENERAL EDUCATION, AND NUTRITIONAL EDUCATION.
- CREATE A WELCOMING, PLEASANT BUILDING FOR PATIENTS AND VISITORS SEEKING KIDNEY TREATMENT OR EDUCATION.
- MAINTAIN A CLEAR AND LEGIBLE PRESENCE FROM S TRENTON STREET AND VIEW CORRIDOR FROM MARTIN LUTHER KING JR. WAY S.
- RESPECT THE CHARACTER AND SCALE OF THE CURRENT NEIGHBORHOOD WHILE ANTICIPATING FUTURE GROWTH AND DEVELOPMENT.
- PROVIDE CLEAR SIGNAGE, WAYFINDING, AND ACCESSIBLE PATHS FOR PATIENTS WITH LIMITED MOBILITY.
- CONTRIBUTE TO THE SAFETY AND VITALITY OF THE NEIGHBORHOOD THROUGH AN ACTIVATED STREET FRONT AND BY ENCOURAGING NATURAL SURVEILLANCE OF THE STREET FROM STAFF AREAS.
- ENCOURAGE ALTERNATE MODES OF TRANSIT FOR STAFF AND VISITORS THROUGH STRATEGIC PLACEMENT ALONG THE CHIEF SEALTH TRAIL AND ACROSS FROM TRANSIT STOPS.












3.0 | PROPOSAL

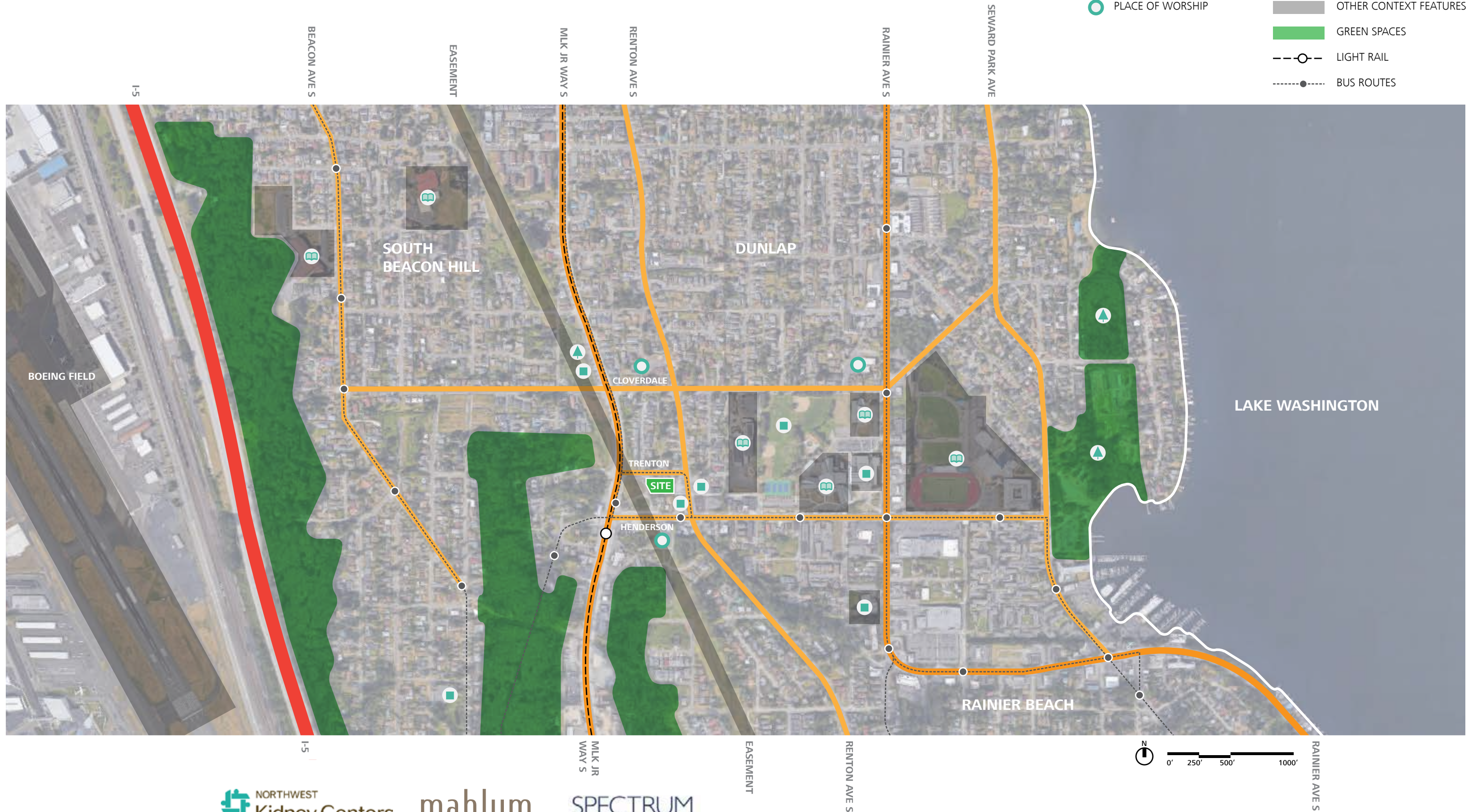
AERIAL PHOTOGRAPH [3.4] [4.1]



4.0 | SUMMARY CONTEXT ANALYSIS

VICINITY MAP [4.2]

-  SCHOOL
-  PARK
-  COMMUNITY/ RECREATIONAL
-  PLACE OF WORSHIP
-  MAJOR HIGHWAY
-  MAJOR ARTERIALS
-  MINOR ARTERIALS
-  OTHER CONTEXT FEATURES
-  GREEN SPACES
-  LIGHT RAIL
-  BUS ROUTES



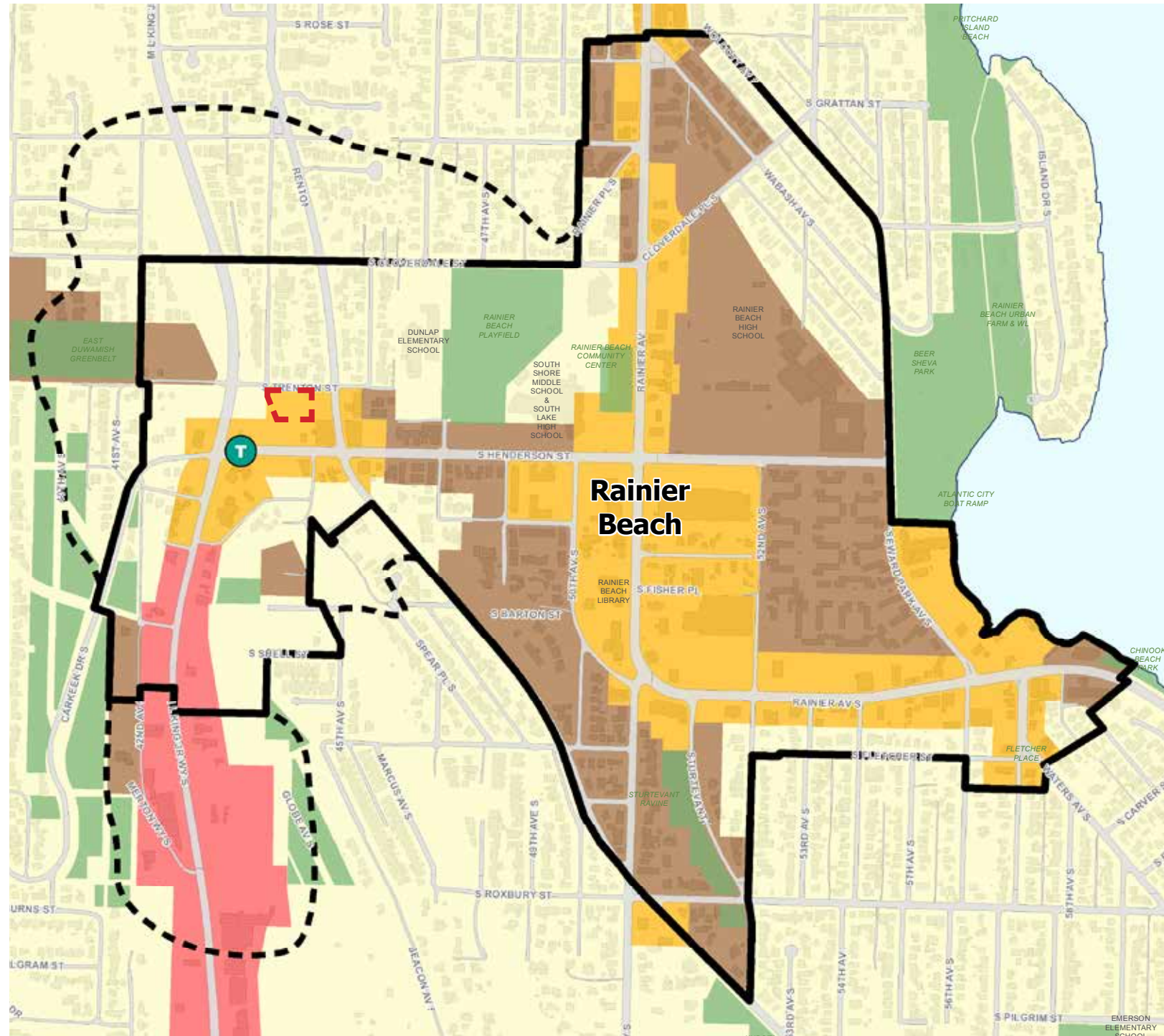
4.0 | SUMMARY CONTEXT ANALYSIS

AXONOMETRIC OF 9-BLOCK AREA [4.3]

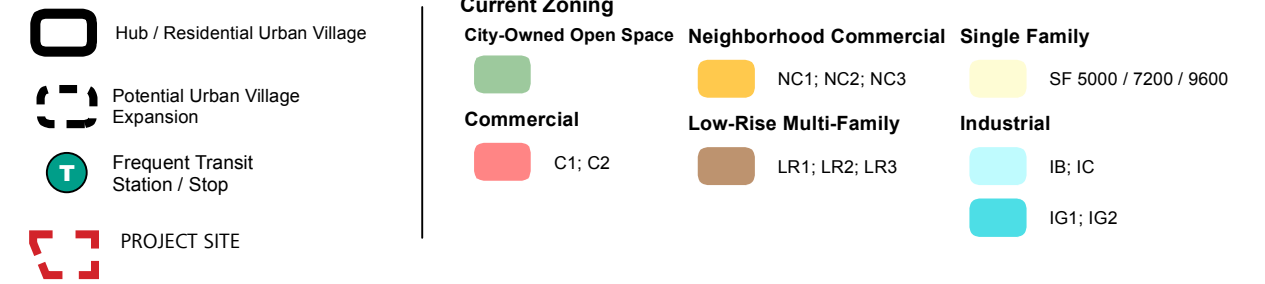


4.0 | SUMMARY CONTEXT ANALYSIS

ZONING VICINITY MAP [4.2]



Rainier Beach Residential Urban Village



SITE ADDRESS:

4401 S TRENTON ST 98118

PARCEL NUMBER:

680410-0005

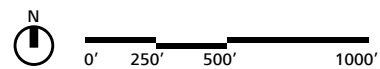
ZONING:

NC2P-40 (NEIGHBORHOOD COMMERCIAL 2)

APPLICABLE ZONING OVERLAYS:

- PEDESTRIAN OVERLAY*
- RAINIER BEACH URBAN VILLAGE
- RAINIER BEACH STATION OVERLAY DISTRICT

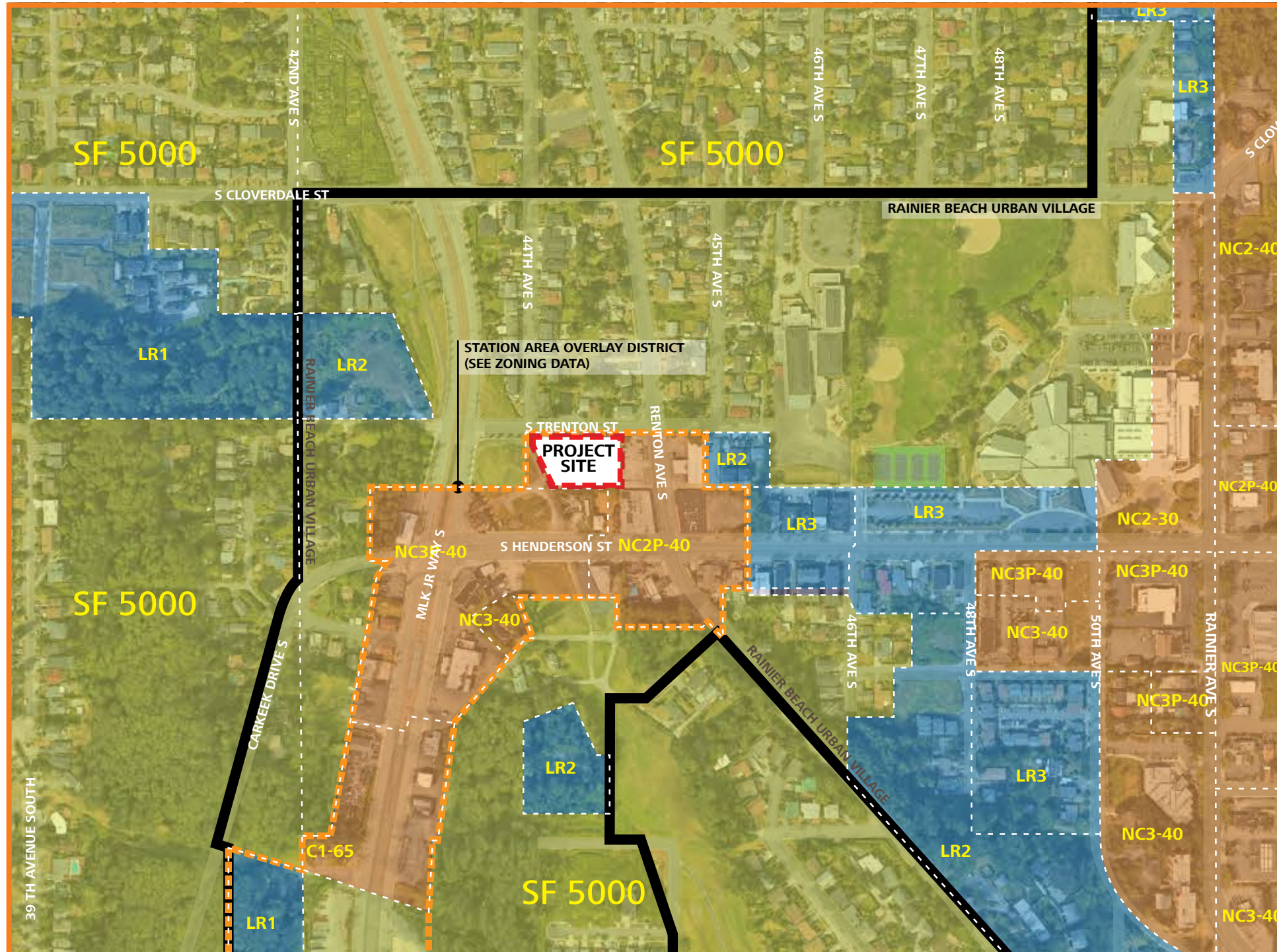
* S. TRENTON ST IS NOT A PRINCIPAL PEDESTRIAN STREET PER SMC 23.47A.005.D.2



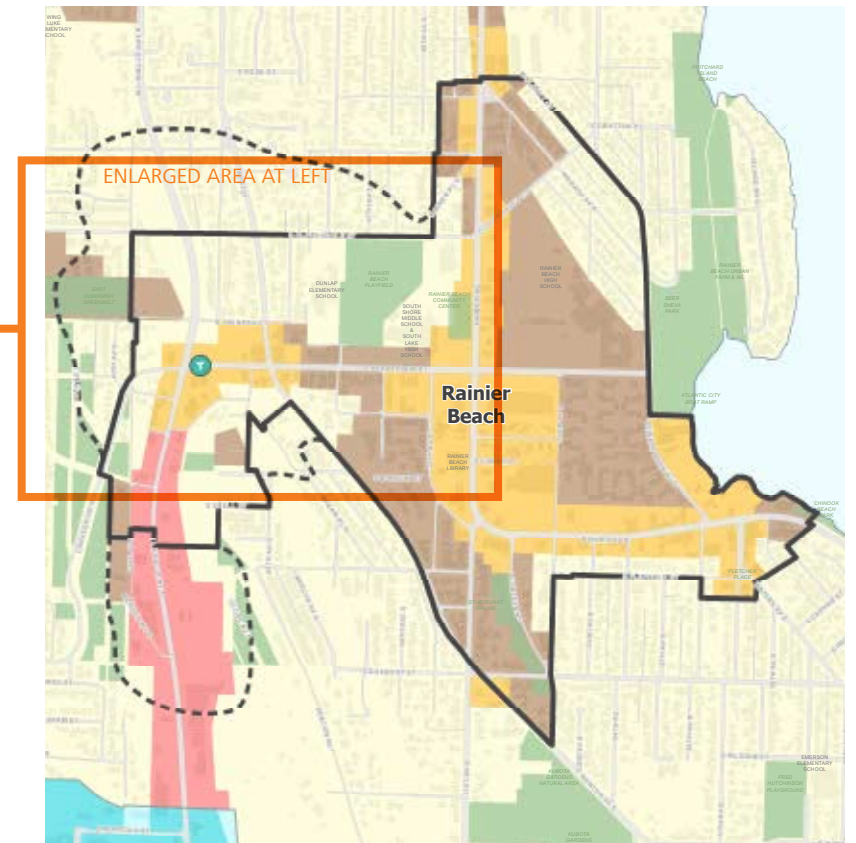
ZONING MAP FROM CITY OF SEATTLE, AUGUST 10, 2015

5.0 | EXISTING SITE CONDITIONS

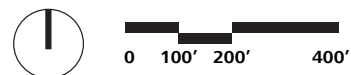
9-BLOCK ZONING MAP [5.1]



- NEIGHBORHOOD COMMERCIAL
- LOW RISE
- SINGLE FAMILY
- PROJECT SITE
- STATION OVERLAY
- URBAN VILLAGE OVERLAY

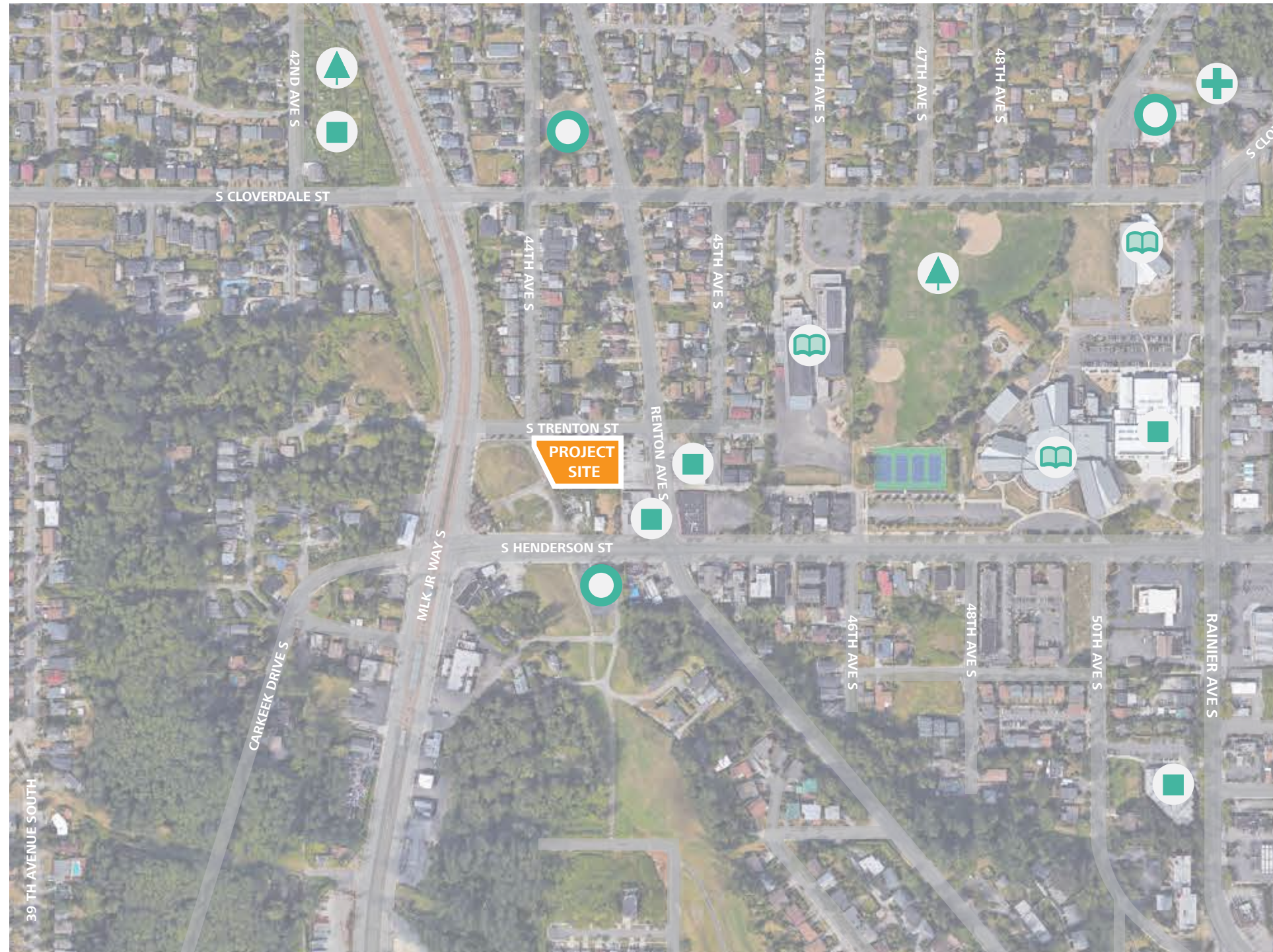







ZONING MAP FROM CITY OF SEATTLE, AUGUST 10, 2015

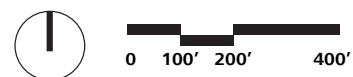


5.0 | EXISTING SITE CONDITIONS

9-BLOCK EXISTING USES MAP [5.1]

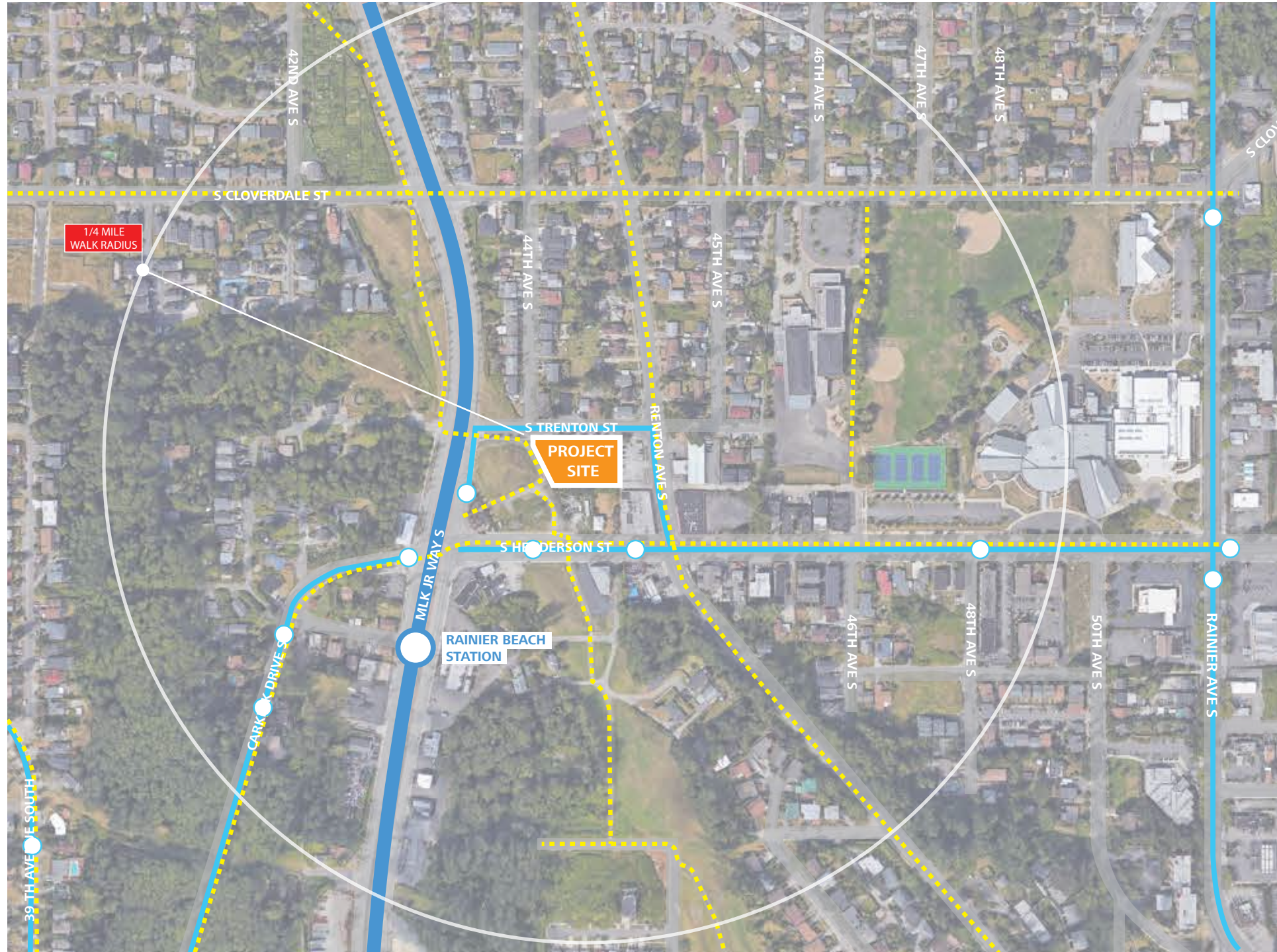





-  Park
-  School
-  Place of Worship
-  Medical
-  Community/Recreational

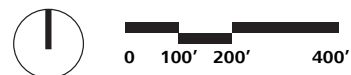


5.0 | EXISTING SITE CONDITIONS

9-BLOCK TRANSIT & WALK/BIKE ACCESSIBILITY MAP [5.1]



-  Bus Route
-  Bike Route
-  Light Rail



5.0 | EXISTING SITE CONDITIONS

9-BLOCK ARCHITECTURAL CONTEXT MAP [5.1]



A - SOUTH SHORE K-8 SCHOOL



B - RAINIER BEACH POOL / COMMUNITY CENTER



C - SOUTH LAKE HIGH SCHOOL



D - RAINIER BEACH PUBLIC LIBRARY



E - FUTURE S. HENDERSON ST. DEVELOPMENT



F - DUNLAP ELEMENTARY SCHOOL

DESIGN CUES:

The design aims to respect the single-family residences across the street on Trenton through a modest, 1-story scale and a welcoming entry plaza. It also anticipates the evolution of the area and specifically, to the planned future development at the adjacent S. Henderson Street lot, and is positioned as a transition between these two residential scales.

The design also takes cues from existing neighborhood community amenities to establish a positive and desirable context for future development to build on.

Many of these precedent community buildings utilize brick and 1-story elements to relate to the surrounding residential context, while also articulating their massing and form to better relate to their context and

avoid long, blank facades. Entry canopies and entry plazas are also utilized to create more welcoming pedestrian environments and clear entrances for way-finding. There is also community precedent for articulated eaves and roof edges (See A,B,C) as well as parapets and flat roofs (A, D, F), which the design massing options explore.

5.0 | EXISTING SITE CONDITIONS

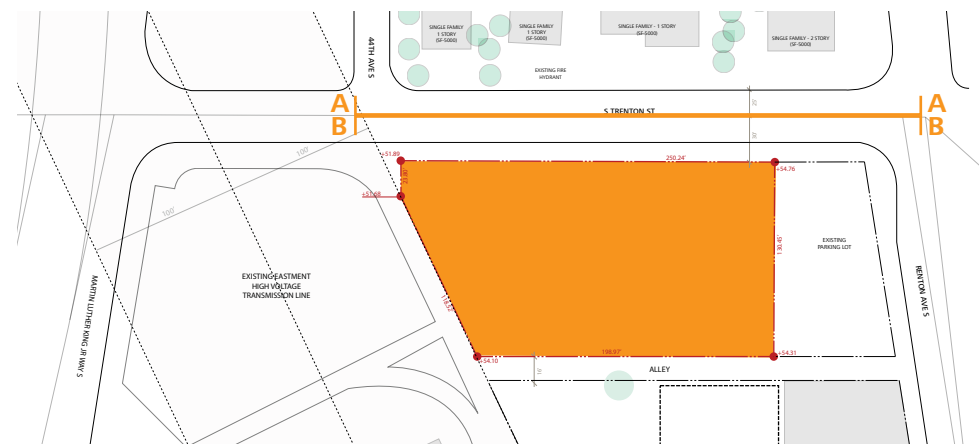
STREET ELEVATIONS



AA - VIEW OF SOUTH TRENTON STREET - LOOKING NORTH

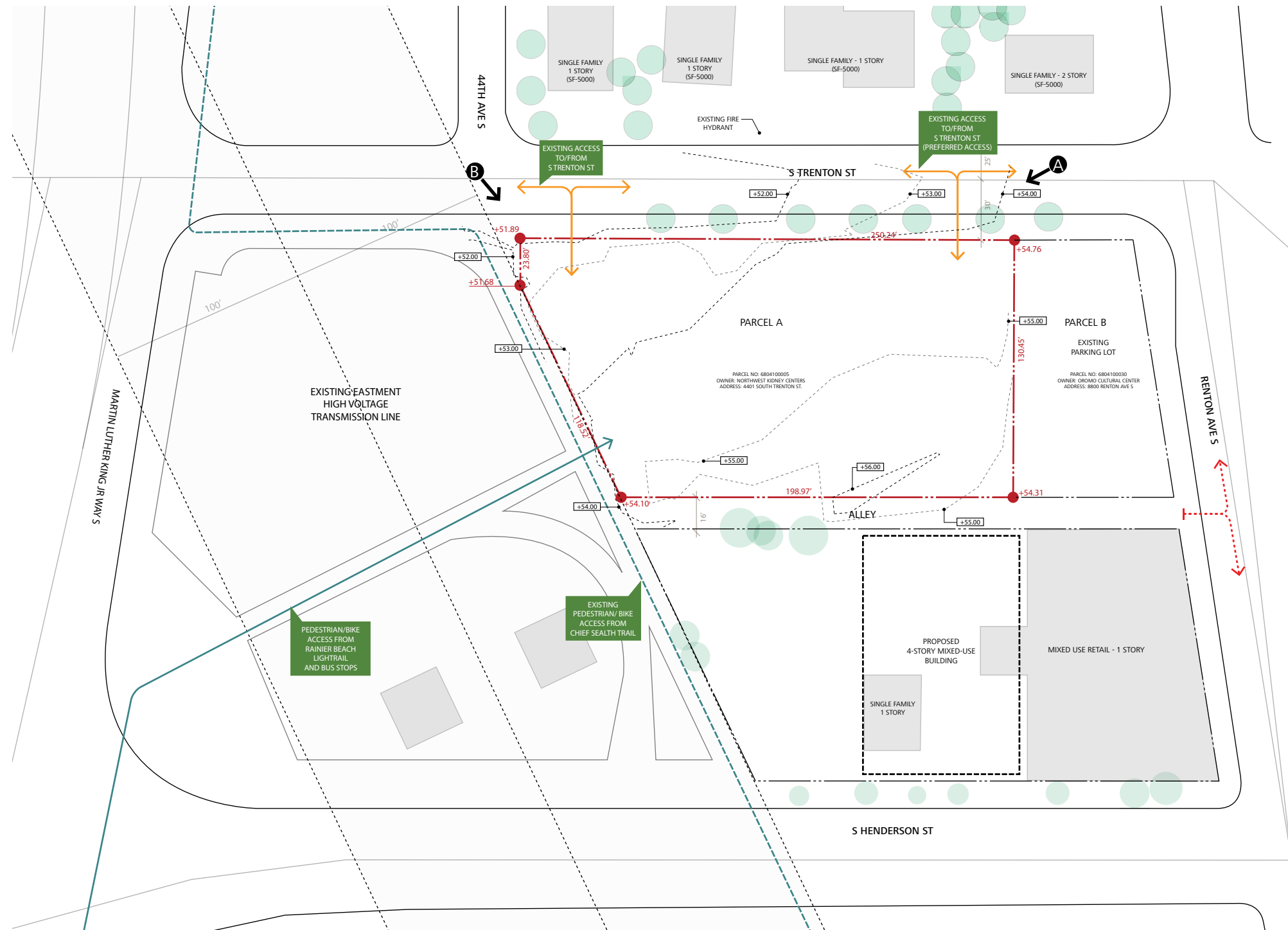


BB - VIEW OF SOUTH TRENTON STREET - LOOKING SOUTH



5.0 | EXISTING SITE CONDITIONS

SITE SURVEY, ACCESS OPPORTUNITIES



■ SITE OPPORTUNITY

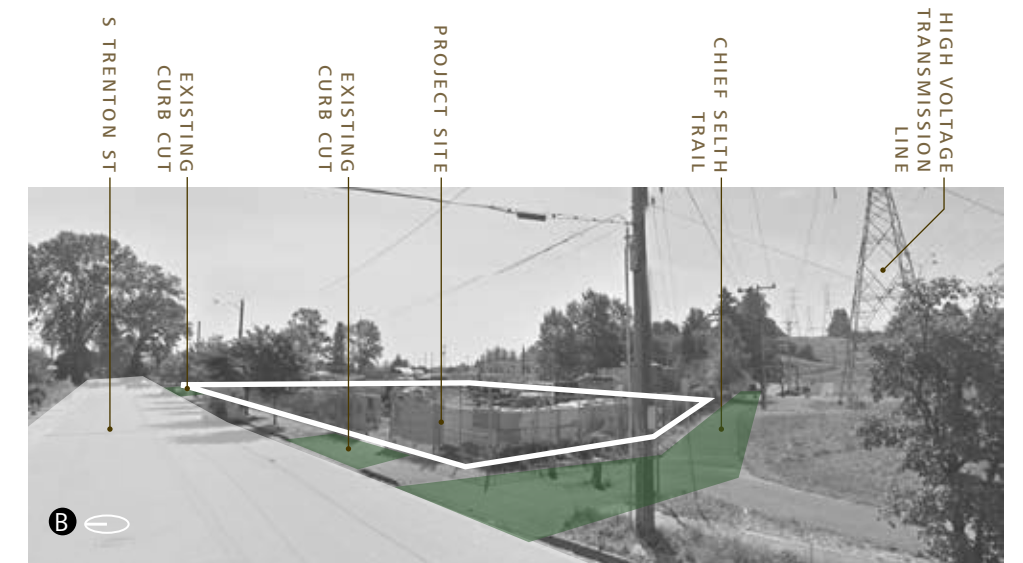


SCALE: 1/64" = 1'-0"
(Printed on 11"x17")
16' 32' 64'

OPPORTUNITIES:



VIEW OF SITE FROM S. TRENTON ST. AND RENTON AVE. S.



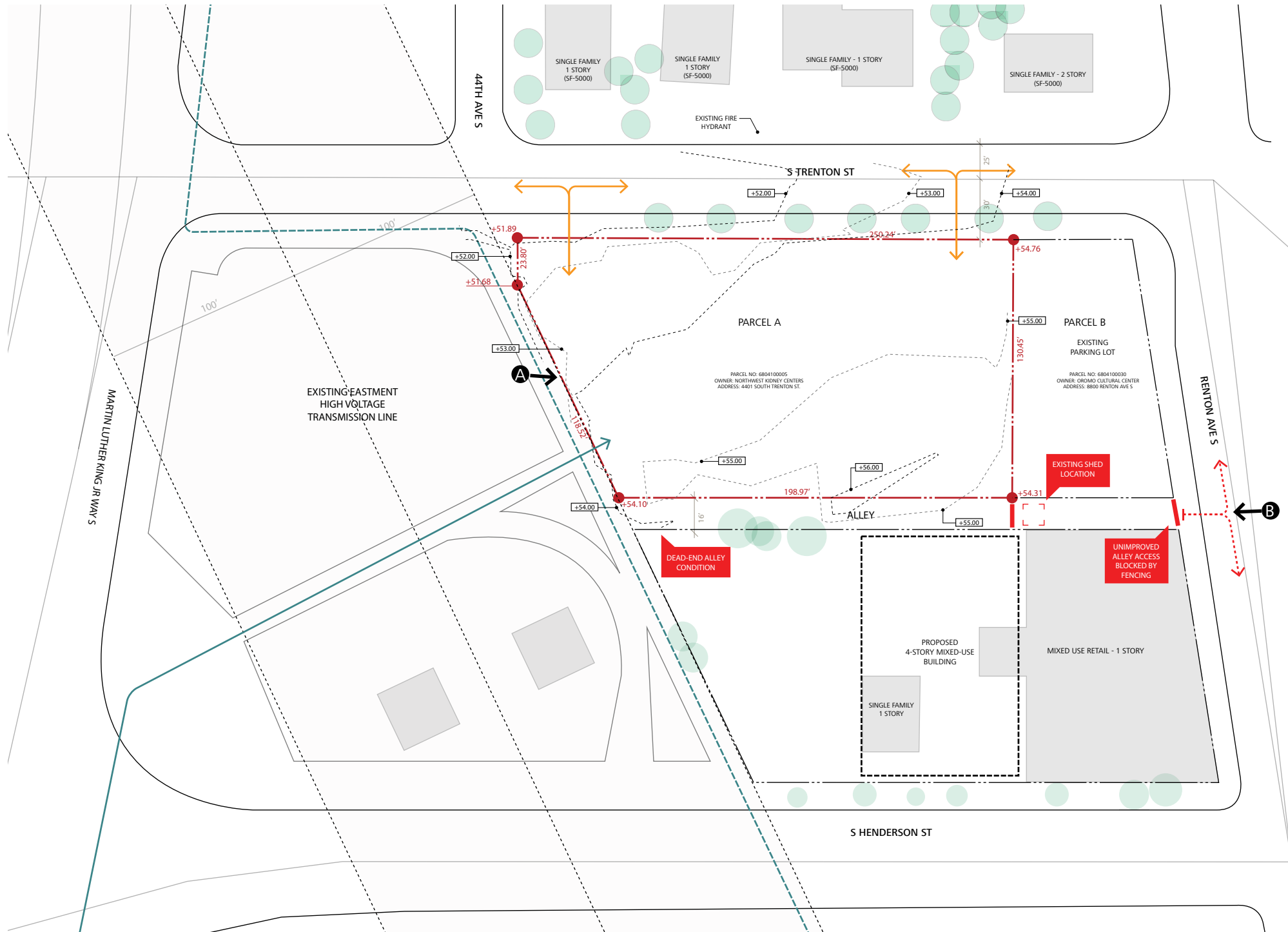
VIEW OF SITE FROM S. TRENTON ST. AND 44TH AVE. S.

Vehicular access from S. Trenton Street near the existing easternmost curb cut is necessary to allow guaranteed access to and from the facility for patients, staff and medical deliveries. Accessing the parking area only from the alley is problematic since the 16' wide dead end alley may be blocked by service vehicles at times and it is not a clearly discernible path for patient way-finding when arriving by car.

The adjacent, existing high-voltage transmission line easement guarantees that the building will have clear exposure from the Chief Sealth Trail and the intersection of S. Henderson St. and Martin Luther King Jr. Way S, providing opportunities for encouraging alternative transit options for staff, patients, and visitors.

5.0 | EXISTING SITE CONDITIONS

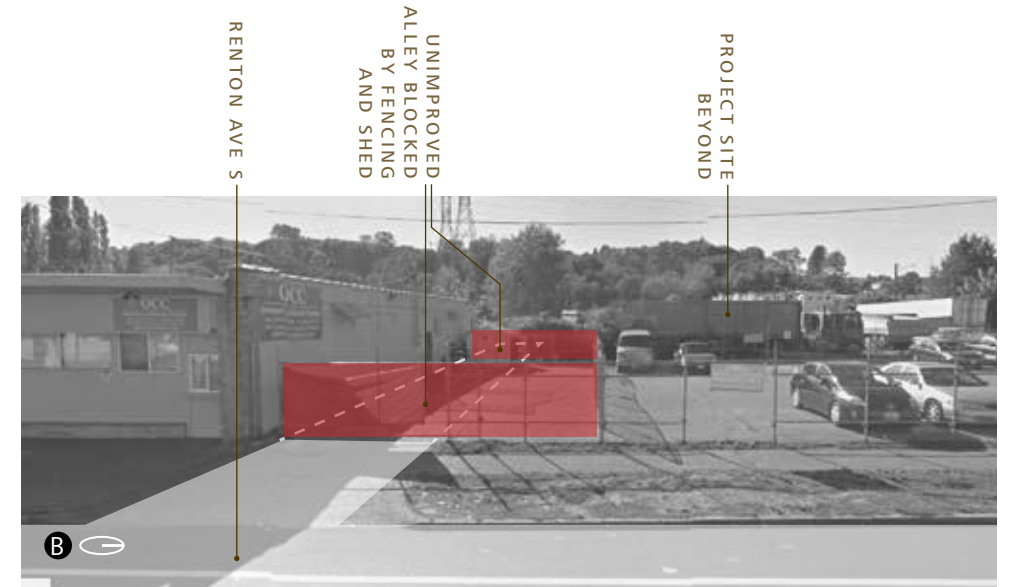
SITE SURVEY, ACCESS CONSTRAINTS



CONSTRAINTS:



VIEW OF SITE FROM UTILITY EASEMENT



VIEW OF SITE FROM RENTON AVE S.

█ SITE CONSTRAINT



SCALE: 1/64" = 1'-0"
(Printed on 11"x17")

16' 32' 64'

6.0 | ZONING DATA

GUIDELINES & CODE SUMMARY

SITE ADDRESS:

4401 S TRENTON ST 98118

PARCEL NUMBER:

680410-0005

LEGAL DESCRIPTION:

PIONEER ADD PCL A SEATTLE LBA #9905264 REC #20000424900010
SD LBA DAF - LOTS 1 THRU 8 SD BLK 1 LESS TRANS R/W

PLAT BLOCK: 1

PLAT LOT: 1 THRU 8

LOT SIZE:

29961 SF

ZONING:

NC2P-40 (NEIGHBORHOOD COMMERCIAL 2)

APPLICABLE ZONING OVERLAYS:

PEDESTRIAN OVERLAY*

RAINIER BEACH URBAN VILLAGE

RAINIER BEACH STATION OVERLAY DISTRICT

* S. TRENTON ST IS NOT A PRINCIPAL PEDESTRIAN STREET
PER SMC 23.47A.005.D.2

23.41.004 DESIGN REVIEW

Design Review **REQUIRED**

23.47A.004 PERMITTED AND PROHIBITED USES

COMPLIES, BUILDING GROSS AREA UNDER 12000 SF - Permitted for Medical services and limited to 25,000 SF.

COMPLIES - site is not within 2,500 feet of a medical Major Institution Overlay

23.47A.008 STREET-LEVEL DEVELOPMENT STANDARDS

A - BASIC STREET-LEVEL REQUIREMENTS

COMPLIES - Blank segments of the street-facing facade between 2 feet and 8 feet above the sidewalk may not exceed 20 feet in width. (SEE SECTION 11.0 FOR ELEVATIONS AND CALCULATIONS)

COMPLIES - The total of all blank facade segments may not exceed 40% of the width of the facade of the structure along the street. (SEE SECTION 11.0)

COMPLIES, SIDEWALK/PLAZA PROVIDED - Street-level street-facing facades shall be located within 10 feet of the street lot line, unless wider sidewalks, plazas, or other approved landscaped or open spaces are provided. (SEE SECTION 7.0 FOR SITE PLAN)

B - NON-RESIDENTIAL STREET-LEVEL REQUIREMENTS

COMPLIES - 60% of the street-facing facade between 2 feet and 8 feet above the sidewalk shall be transparent. (SEE SECTION 11.0 FOR CALCULATIONS)

COMPLIES, ENTIRE PROJECT IS A NON-RESIDENTIAL USE - Non-residential uses shall extend an average depth of at least 30 feet and a minimum depth of 15 feet from the street-level street-facing facade.

COMPLIES, BUILDING IS ONLY 1-STORY - Non-residential uses at street level shall have a floor-to-floor height of at least 13 feet.

C - ADDITIONAL REQUIREMENTS

COMPLIES, SITE IS NOT ON A PRINCIPAL PEDESTRIAN STREET - Continuous overhead weather protection is required along at least 60% of the street frontage of a structure on a principal pedestrian street. The covered area shall have a minimum width of 6 feet.

23.47A.010 MAXIMUM SIZE OF NONRESIDENTIAL USE

COMPLIES - Specified in 23.47A.004: limited to 25,000 SF.

23.47A.012 STRUCTURE HEIGHT

COMPLIES, HEIGHT AT HIGHEST POINT: 16 FEET - Project in zones with a 40 foot mapped height limit.

SOLAR READY - solar collectors may extend up to 4 feet above the otherwise applicable height limit, with unlimited rooftop coverage.

SOLAR READY - Solar collectors and/ or Mechanical equipment may extend up to 15 feet above the applicable height limit, as long as the combined total coverage of all features gaining additional height does not exceed 20 percent of the roof area, or 25 percent of the roof area if the total includes stair or elevator penthouses or screened mechanical equipment.

23.47A.013 FLOOR AREA RATIO

COMPLIES - Maximum FAR: 3.

VARIANCE REQUIRED - Minimum FAR: 1.5 (required for a pedestrian-designated zone in an urban center, or Station Area Overlay District per Table C)

23.47A.014 SETBACK

NO setback **REQUIRED**.

23.47A.016 LANDSCAPING & SCREENING STANDARDS

COMPLIES, EXISTING STREET TREES TO REMAIN, ADDING NEW TREES - Street trees are required.

COMPLIES - Surface parking area with 20 to 50 parking spaces required landscaped area 18 SF per parking space.

COMPLIES - Each landscaped area shall be no smaller than 100 SF, and shall be no less than 4 feet in width or length.

COMPLIES - One tree is required for every 10 parking spaces.

COMPLIES - Three-foot-high screening is required along street lot lines.

COMPLIES - Three-foot-high screening is required around trash storage area.

6.0 | ZONING DATA

GUIDELINES & CODE SUMMARY (CONTINUED)

23.47A.022.C LIGHT & GLARE STANDARDS

COMPLIES - Exterior lighting is shielded and directed away from adjacent uses (SEE SECTION 14.0 EXTERIOR LIGHTING PLAN)

COMPLIES - A evergreen hedge is provided to prevent vehicle lights from affecting adjacent properties

COMPLIES - Lighting on poles is permitted up to a maximum height of 30 ft

23.47A.032 PARKING LOCATION AND ACCESS

DEPARTURE REQUIRED - Access to parking shall be from the alley if the lot abuts an alley improved to the standards of subsection 23.53.030.C. (SEE SECTION 17.0 | DEPARTURES FOR ADDITIONAL INFORMATION).

COMPLIES - Parking shall not be located between a structure and a street lot line.

COMPLIES - Parking to the side of a structure not exceed 60 feet of street frontage.

SITE IS NOT ON A PRINCIPAL PEDESTRIAN STREET - In pedestrian designated zones, surface parking is prohibited abutting the street lot line along a principal pedestrian street.

23.54.015 REQUIRED PARKING

SITE LOCATION INSIDE STATION AREA OVERLAY DISTRICT, NO PARKING SPACES REQUIRED; HOWEVER, 20 PARKING SPACES ARE PROVIDED

23.54.030.B.2.C PARKING SPACE STANDARDS

DEPARTURE REQUIRED - When 20 or more parking spaces are provided, a minimum of 35% of the parking spaces shall be striped for small vehicles. (SEE SECTION 17.0 | DEPARTURES FOR ADDITIONAL INFORMATION).

COMPLIES - For two-way traffic, the minimum width of curb cuts is 22 feet, and the maximum width is 25 feet.

COMPLIES - For two-way driveways, a sight triangle on the side of the driveway used as an exit shall be provided, and shall be kept clear of any obstruction for a distance of 10 feet from the intersection of the driveway and the sidewalk, and in the vertical spaces between 32" and 82" from the ground in this triangle

23.54.035 LOADING BERTH REQUIREMENTS AND SPACE STANDARDS

1 LOADING BERTH PROVIDED ON PROPERTY - For uses with less than 16,000 square feet of gross floor area that provide a loading space on a street or alley, the loading berth requirements may be waived.

COMPLIES - Each loading berth shall be not less than ten (10) feet in width and shall provide not less than fourteen (14) feet vertical clearance.

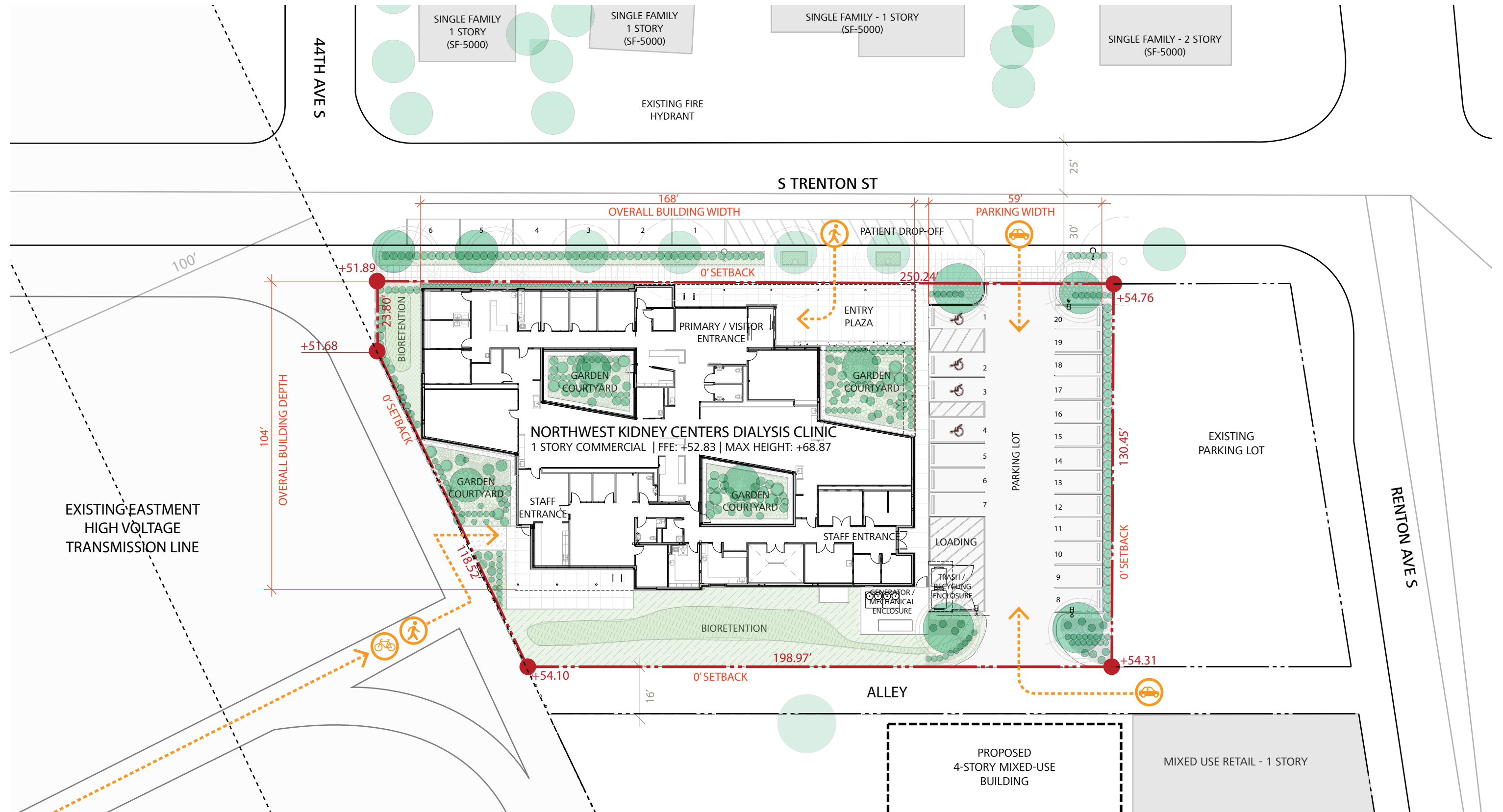
COMPLIES - Each loading berth for low- and medium-demand uses shall be a minimum of 35 feet in length.

23.54.040 TRASH & RECYCLING STORAGE / ACCESS

COMPLIES - 125 SF minimum area for shared trash/recycling storage required

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7.0 | COMPOSITE SITE PLAN



SCALE: 1/32" = 1'-0"
(Printed on 11"x17")

8' 16' 32'

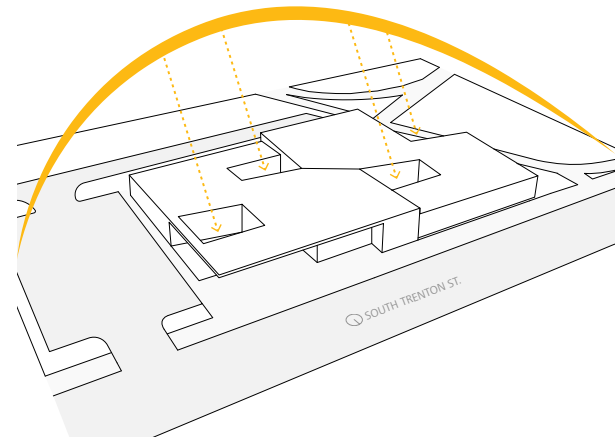
8.0 | EDG RESPONSE

PRIORITY GUIDELINES [8.1] [8.2]

PRIORITY GUIDELINES

In addition to the many patients the proposed Kidney Treatment Center will be serving in the Rainier Beach community, the building has been designed to benefit the staff, visitors, and surrounding community. The design responds to the current state of the neighborhood and unique adjacency to mass transit while looking ahead towards the future of the area. The following design guidelines have been priorities throughout the design process for the new clinic:

CONTEXT & SITE



CS1.B SUNLIGHT

CS1.B.2. Daylight & Shading: Maximize daylight for interior and exterior spaces and minimize shading on adjacent sites through the placement and/or design of structures on the site.

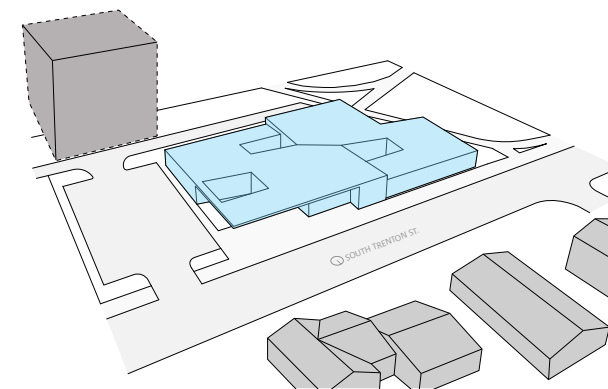
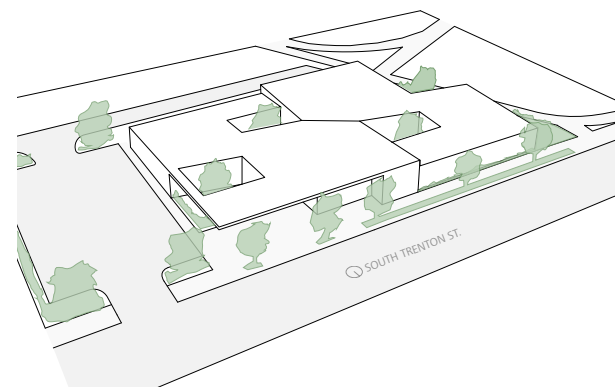
CS1.B.3. Managing Solar Gain: Manage direct sunlight falling on south and west facing facades through shading devices and existing or newly planted trees

The building has been oriented to maximize daylighting in public, patient, and staff spaces. The design of the roof, trees and landscaping, and perforated sun shades allow for control of glare and solar heat gain while allowing in ample daylight. Additionally, the low, 1-story nature of the building reduces shading on adjacent sites.

CS1.D PLANTS AND HABITAT

CS1-D-1. On-Site Features: Incorporate on-site natural habitats and landscape elements into project design and connect those features to existing networks of open spaces and natural habitats wherever possible. Consider relocating significant trees and vegetation if retention is not feasible.

The project is designed around four landscaped courtyards that create pockets of natural habitat for birds and other wildlife. These courtyards are designed to provide beauty and visual interest throughout the year, creating a vibrant and peaceful environment for patients, visitors, natural wildlife and the community.



CS2.D HEIGHT, BULK, AND SCALE

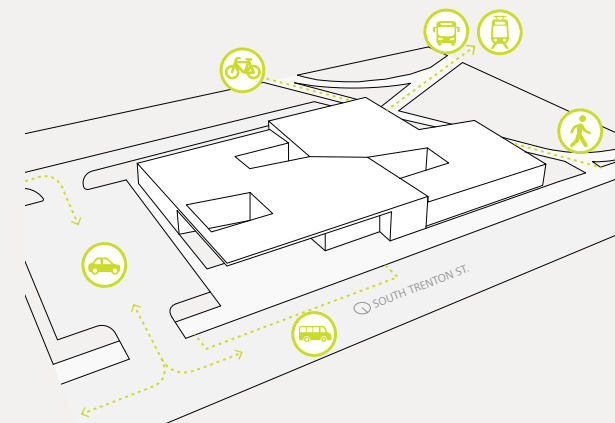
CS2.D.3 Zone Transitions: For projects located at the edge of different zones, provide an appropriate transition or complement to the adjacent zone(s).

CS2.D.4 Massing Choices: Strive for a successful transition between zones where a project abuts a less intense zone.

CS2.D.5 Respect for Adjacent Sites: Respect adjacent properties with design and site planning to minimize disrupting the privacy of residents in adjacent buildings.

The project anticipates the change that the surrounding neighborhood is seeing, looking ahead to higher density developments being planned for the area. The proposed roof articulations and massing options for the 1-story structure anticipate these potentials while still acknowledging the smaller scale, single-family residences to the north, serving as an effective transition between the two scales.

PUBLIC LIFE



PL4.A ENTRY LOCATIONS AND RELATIONSHIPS

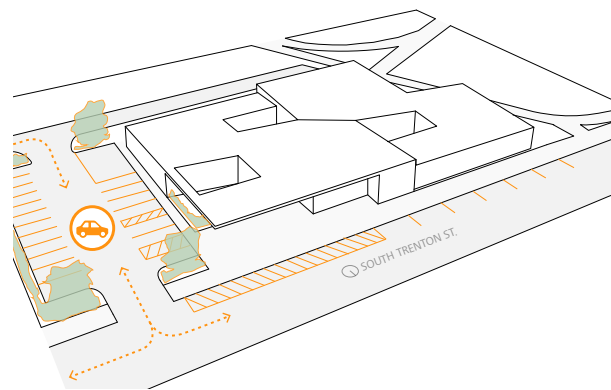
PL4.A.1 Serving All Modes of Travel: Provide safe and convenient access points for all modes of travel.

As many of the patients who utilize the kidney treatment center may rely on wheelchairs, scooters, or walkers for mobility, accessibility design is a high priority for this clinic. The building entrance features a covered entry plaza that is positioned for clear wayfinding and ease of access, directly adjacent to the patient drop-off zone and ADA parking. The building's location near the light rail station and Cheif Sealth Trail is also facilitated with pedestrian connections and bike storage at visitor and employee entrances.

8.0 | EDG RESPONSE

PRIORITY GUIDELINES [8.1] [8.2]

DESIGN CONCEPT



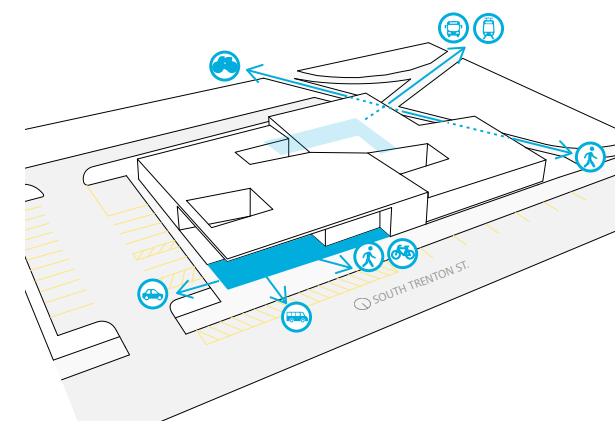
DC1-B VEHICULAR ACCESS AND CIRCULATION

DC1-B-1. Access Location and Design: Choose locations for vehicular access, service uses, and delivery areas that minimize conflict between vehicles and non-motorists wherever possible. Emphasize use of the sidewalk for pedestrians, and create safe and attractive conditions for pedestrians, bicyclists, and drivers.

DC1-C PARKING AND SERVICE USES

DC1-C-2. Visual Impacts: Reduce the visual impacts of parking lots, parking structures, entrances, and related signs and equipment as much as possible.

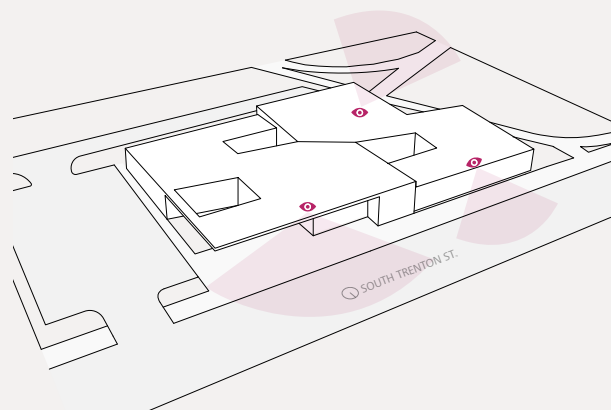
Parking access is provided off of S. Trenton Street for clear wayfinding with a secondary access point off the alley for service use and to alleviate congestion. The parking lot is screened with landscaping to reduce visual impact and help emphasize pedestrian zones on site.



DC3.B OPEN SPACE USES AND ACTIVITIES

DC3.B.3. Connections to Other Open Space: Site and design project-related open spaces to connect with, or enhance, the uses and activities of other nearby public open space where appropriate.

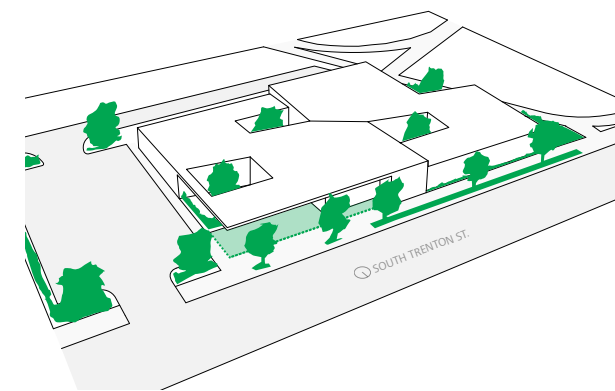
The entry plaza along S. Trenton Street contains a generous covered canopy bordered by one of the project's landscaped courtyards, creating an amenity not only for patients and visitors but for the community as well. At the opposite end of the building, the staff break room opens up to another covered area with an adjacent courtyard garden and connections to bicycle and transit amenities, encouraging alternate modes of transit for employees.



PL2.B SAFETY AND SECURITY

PL2.B.1. Eyes on the Street: Create a safe environment by providing lines of sight and encouraging natural surveillance through strategic placement of doors, windows, balconies and street-level uses.

The primary reception counter is positioned to have full visibility to the entrance plaza and patient-drop off area to help maintain safe conditions for patients and community members alike. Other staff and community-use spaces are positioned along S. Trenton Street and the Chief Sealth Trail to encourage natural surveillance and safe conditions for the building and surrounding community.



DC4.D LANDSCAPE & HARDSCAPE MATERIALS

DC4-D-1. Choice of Plant Materials: Reinforce the overall architectural and open space design concepts through the selection of landscape materials.

DC4-D-2. Hardscape Materials: Use exterior courtyards, plazas, and other hard surfaced areas as an opportunity to add color, texture, and/or pattern and enliven public areas through the use of distinctive and durable paving materials. Use permeable materials wherever possible.

Visitors, patients, and the community are welcomed to the building with a covered entry plaza with landscaping and materials chosen for their resilience and beauty throughout the year.

8.0 | EDG RESPONSE

ITEM BY ITEM RESPONSE [8.3]

COMMENT 1.A (MASSING):

The Board unanimously favored the proposed massing of Concept C, the applicant's preferred option. The Board found Concept C was the best integrated into the urban form of this neighborhood; the building's articulation and street frontage were also preferred over Concepts A and B. (CS2-D)

Response: The team has proceeded with the development of Concept C, working to maintain and enhance the qualities of the concept that benefit the neighborhood. The articulation of the facade and roof, the inclusion of pedestrian plaza at the entry, and landscaped courtyards have all been developed in response to the Priority Guidelines outlined on p.20-21. See Section 18.1 Appendix 1 for original EDG options.

COMMENT 2.A (ACCESS):

The Board acknowledged SDOT's recommendation to eliminate the existing vehicle curb cut along S. Trenton Street and require all off-street parking via the alley on the project's south boundary. However, the Board agreed that a curb cut on S. Trenton Street should be permitted and supported the proposed curb cut location and associated parking lot shown in Concept C. The Board supported the curb cut noting that:

- A curb cut will provide alternative entering and exiting from the site instead of funneling all vehicle and delivery traffic down the alley;
- The alley width is currently undersized for commercial uses;
- The mobility of many clients of the future clinic is limited and an easily identifiable and accessible off-street parking lot from S. Trenton Street would better accommodate the clinic's clients. (DC1-B, PL4-A)

Response: The team has proceeded the inclusion of the curb cut at the northeast end of the site along S. Trenton Street in accordance with the Design Board's recommendations, allowing for clear wayfinding for patients and visitors. Access to the parking lot is also provided to the alley which will allow an alternative entrance/exit for service vehicles and delivery traffic. See Section 7.0 Composite Site Plan for layout.

COMMENT 3.A (LANDSCAPING):

The Board requested more details with the proposed landscaping at the Recommendation meeting. The Board noted the landscape design details need to include:

- General perimeter landscaping with special attention to the landscaping on the site's west side adjacent to the Chief Sealth Trail.
- Interior landscape areas for patients.
- Provide a visually pleasing and interesting space and be designed to complement the building. (CS1-D, DC3-B)

Response: The team has developed a landscape layout that provides visual interest from S. Trenton Street, the Chief Sealth Trail, and entry Plaza while also protecting patient privacy. The landscaped courtyards have been developed to provide dynamic, visual interest throughout the year for patients, staff, and the surrounding community. See Section 10.0 Composite Landscape / Hardscape Plan for additional details and plant selections.

COMMENT 3.B (LANDSCAPING):

The Board requested two renderings of the building and proposed landscaping from S Trenton Street. The first would show the building and landscaping at the time it is installed. The second would show the building with the landscaping at maturity. The landscaping depiction in the renderings will need to include the proposed landscaping in the internal landscape courtyards. (DC4-D)

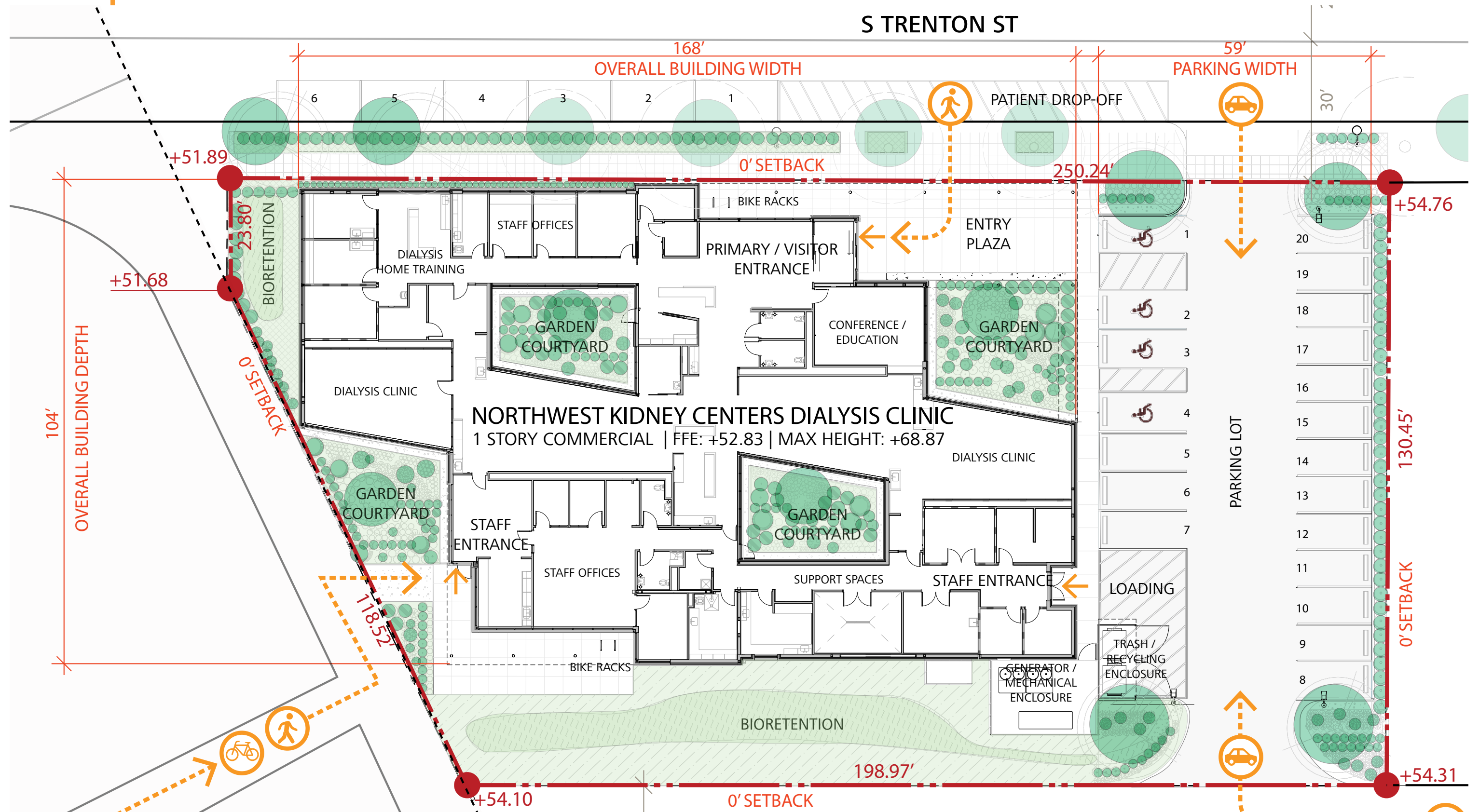
Response: See Section 13.0 (p.33) for the two renderings that compare the size of the landscape upon project completion and full maturity. The landscaping for the courtyard adjacent to the entry plaza is shown, as well as the tops of the trees in the interior building courtyards that can be seen through the roof.

COMMENT 3.C (LANDSCAPING):

The Board was concerned with the appearance of the parking lot as it relates to S Trenton Street and neighboring properties. The Board recommended the landscaping plan include adequate screening of the parking lot from S Trenton Street. The Board also recommended that the internal landscape island be installed within the parking lot to break up the large amount of asphalt and the alley side of the parking lot should be well landscaped. (DC1-C)

Response: The parking lot is softened by four planting areas at each corner of the lot. They flank the access points and provide screening from Trenton and the alley. The east edge of the parking has been shifted to allow for a continuous hedge planting between the parking and the adjacent property to provide both visual and physical separation. The planting areas include canopy trees, evergreen and deciduous shrubs, and perennial/ornamental grasses for accent, offering seasonal interest throughout the year. While the plantings will soften the parking entry and provide screening, the height is limited to 3' to prevent hiding places and to allow for visibility at sight triangles for vehicles entering and exiting and for the safety of pedestrians on the sidewalk. This layout responds to the Priority Guidelines detailed in DC1 and diagrammed in Section 8.0 (p.21), and also provides visual interest for the community. See Section 10.0 Composite Landscape / Hardscape Plan for the layout and details.

9.0 | FLOOR PLAN



SCALE: 1/20" = 1'-0"
 (Printed on 11"x17")
 5' 10' 20'

10.0 | COMPOSITE LANDSCAPE / HARDSCAPE PLAN

SITE PLAN [10.1] [10.2] [10.5]



SCALE 1"=20'

10.0 | COMPOSITE LANDSCAPE / HARDSCAPE PLAN

PLANT SPECIES AND DESIGN ELEMENTS [10.3] [10.4]

STREET & PARKING LOT TREE



ACER TRUNCATUM
X. A.PLATANOIDES
'WARRENSRED' / PACIFIC
SUNSET MAPLE
3" CAL



ACER CIRCINATUM
10" HT.



CHAMAECYPARIS OBTUSA
'GRACILIS'
6" HT.

FRONTAGE PLANTINGS AT PARKING LOT AND RIGHT-OF-WAY



THUJA OCCIDENTALIS 'DEGROOT'S
SPIRE'
24" SPR. 5' HT.



CEPHALOTAXUS HARRINGTONIA
'FASTIGIATA'
24"-36" HT.



LONICERA NITIDA
'LEMON BEAUTY'
18" HT.



LIRIOPE MUSCARI
'BIG BLUE'
1 GAL.



CALAMAGROSTIS
ACUTIFLORA 'KARL
FOERSTER'
2 GAL.



PENNISETUM
ALOPECUROIDES
'HAMELN' / FOUNTAIN
GRASS
1 GAL.

COURTYARD PLANTINGS



HYDRANGEA PANICULATA
'JANE'
21"-34" HT



CORNUS SERICEA 'KELSEY'
18"-21" HT.



MAHONIA X MEDIA 'CHARITY'
24"-30" SPR.



ACANTHUS MOLLIS
1 GAL.



HEUCHERA OBSIDIAN
1 GAL.



ASTILBE
1 GAL.



DRYOPTERIS ERYTHROSORA
'BRILLIANCE'
1 GAL.



HELLEBORUS X HYBRIDUS
'WALHELIVOR'
1 GAL.



HAKONECHLOAE MACRA
1 GAL.



BESIA DELTOPHYLLA
1 GAL.



POLYGONATUM FALCATUM
'VARIEGATUM'
1 GAL.



STACHYS BYZANTINA 'BIG
EARS'
1 GAL.

SITE FURNISHING



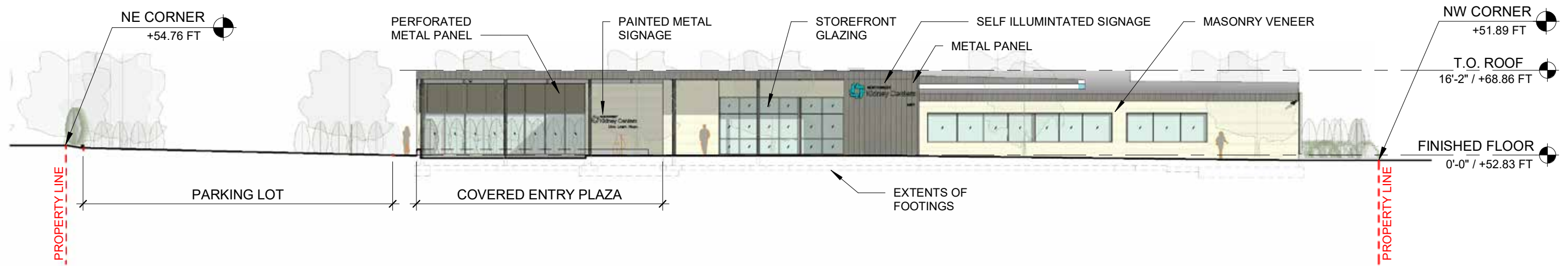
BIKE RACK



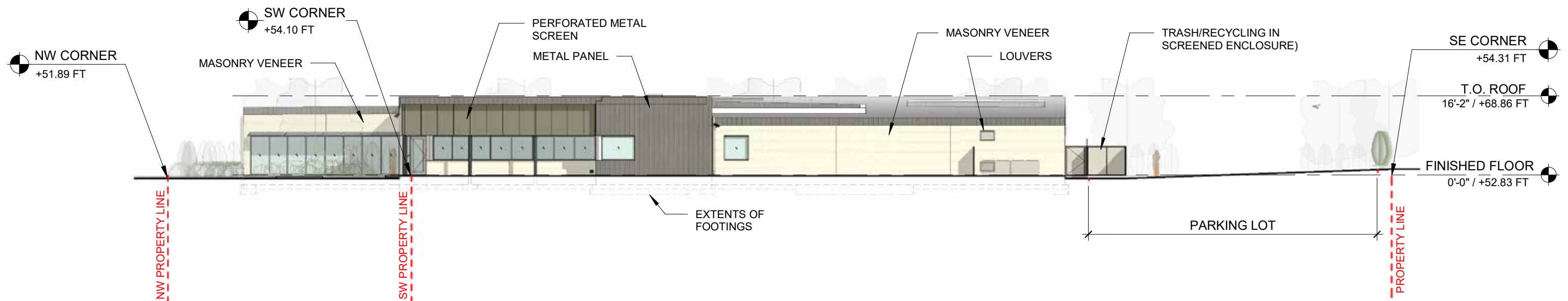
BACKED BENCH WITH ARMS

11.0 | ELEVATIONS

ELEVATIONS (NORTH AND SOUTH)



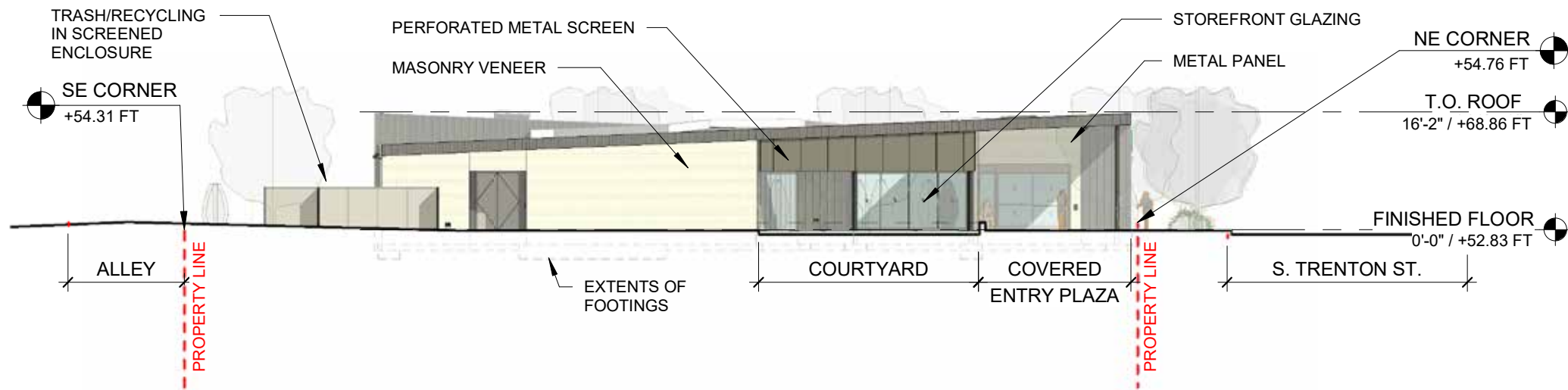
NORTH ELEVATION
SCALE: 1" = 20'-0"



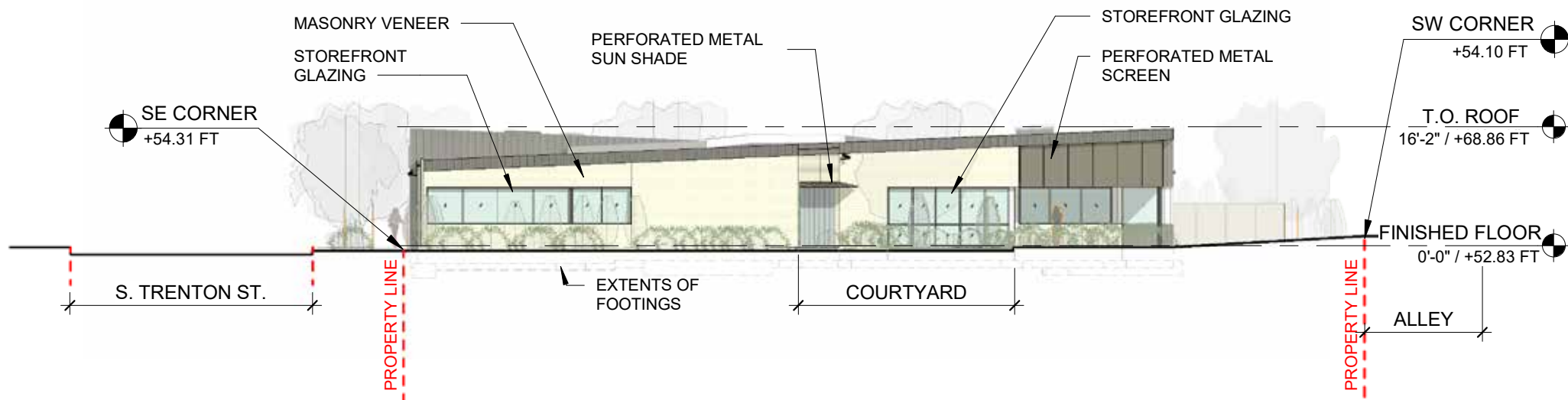
SOUTH ELEVATION
SCALE: 1" = 20'-0"

11.0 | ELEVATIONS

ELEVATIONS (EAST AND WEST)



EAST ELEVATION
SCALE: 1" = 20'-0"

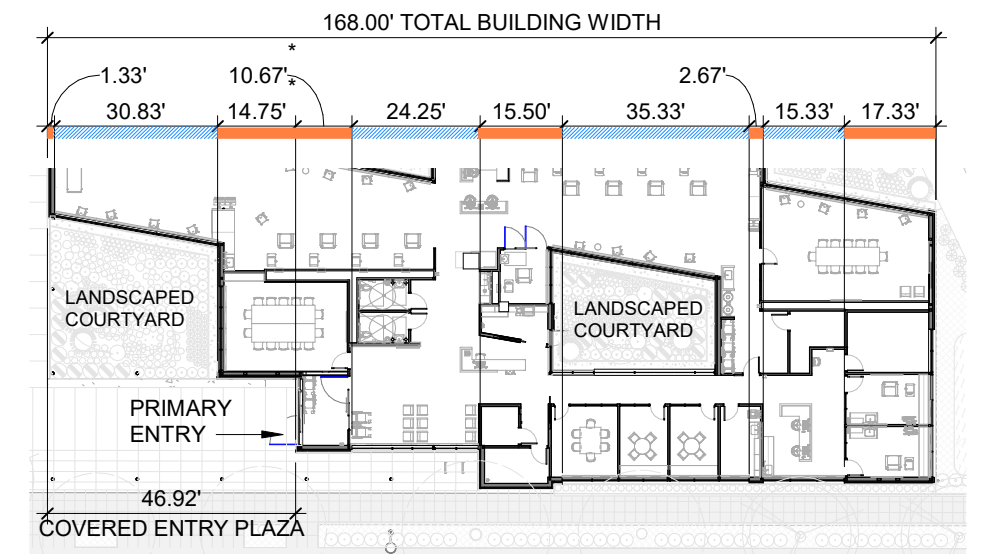
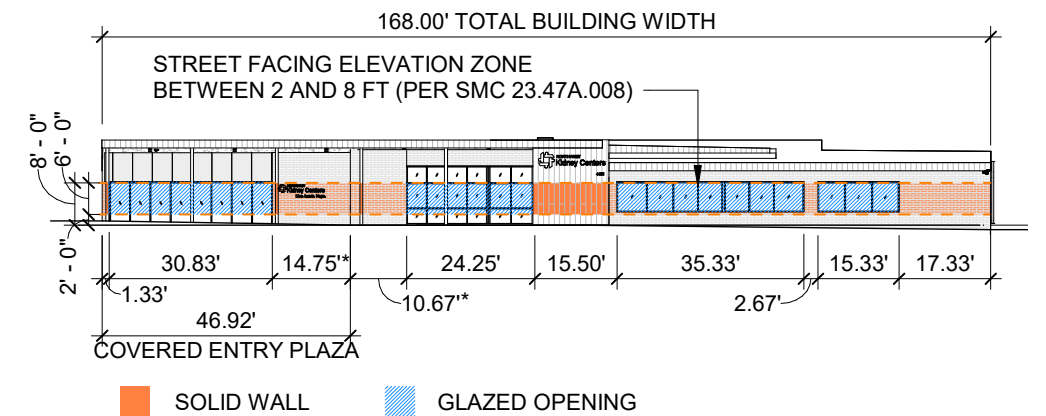


WEST ELEVATION
SCALE: 1" = 20'-0"

TRANSPARENCY CALCULATIONS (SMC 23.47A.008)

$$= [1.33'(0) + 30.83'(6') + 14.75'(0) + 10.67'(0) + 24.25'(6') + 15.50'(0) + 35.55'(5.5') + 2.67'(0) + 15.33'(5.5') + 17.33'(0)] / [168.00'(6')] = 609 \text{ SF (GLAZED AREA)} / 1008 \text{ SF (TOTAL AREA)} = 60.4 \% \text{ GLAZED OPENINGS BETWEEN 2 AND 8 FEET}$$

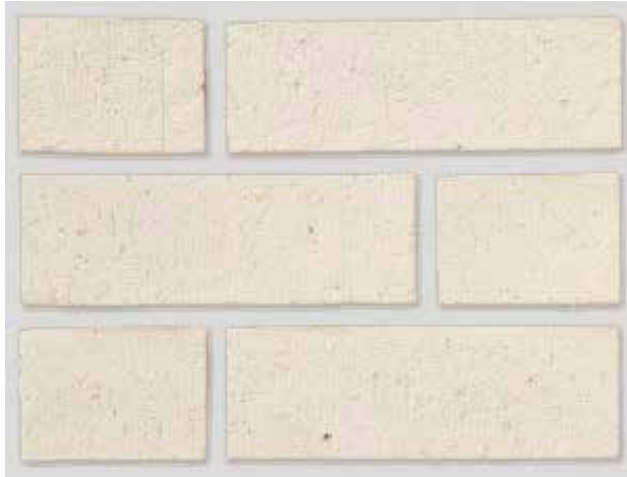
609 SF (GLAZED AREA) / 1008 SF (TOTAL AREA) = 60.4 % GLAZED OPENINGS BETWEEN 2 AND 8 FEET



* NOTE THAT THE FACADE MARKED ABOVE IS BROKEN BY ENTRY PLAZA AND IS NOT CONTINUOUS, PER 23.47A.008.2.A.2. SEE G-001 RENDERING FOR REFERENCE.

12.0 | MATERIAL AND COLOR PALETTE

BUILDING MATERIAL DESCRIPTIONS [12.1]



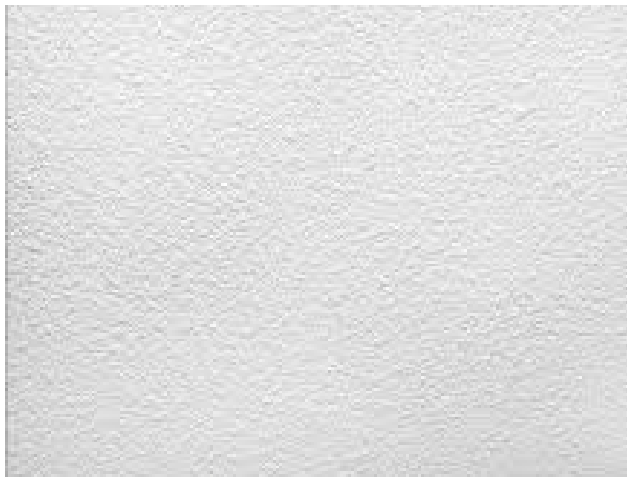
MASONRY VENEER

COLOR: LIMESTONE W/GREY MORTAR
SIZE: 4X4X16" RUNNING BOND



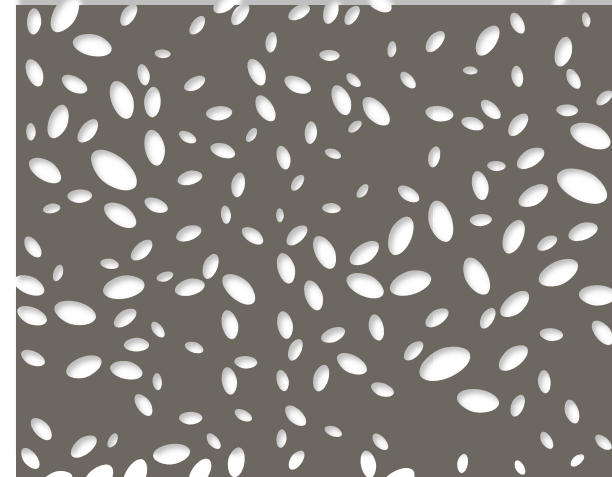
METAL WALL PANEL / FASCIA

COLOR: COOL ZINC GRAY
SIZE: 12"X72" VERTICAL RUNNING BOND



PAINTED GYPSUM SOFFIT

COLOR: WARM WHITE



PERFORATED METAL SCREEN

COLOR: COOL ZINC GRAY
SIZE: 48" WIDE PANELS



STOREFRONT GLAZING

MULLIONS: LIGHT BROWN (BROWN/GREY)
SIZE: 2" MULLIONS
GLAZING: CLEAR TINTED

13.0 | RENDERINGS

VIEW FROM S. TRENTON STREET AT ENTRY PLAZA



13.0 | RENDERINGS

VIEW FROM THE CHIEF SEALTH TRAIL



13.0 | RENDERINGS

LANDSCAPE COMPARISON FROM S. TRENTON ST.

PROJECT COMPLETION



FULL MATURITY



13.0 | RENDERINGS

ENTRY PLAZA AND INTERIOR VIEWS

ENTRY PLAZA



LOBBY / WAITING



INTERIOR COURTYARDS

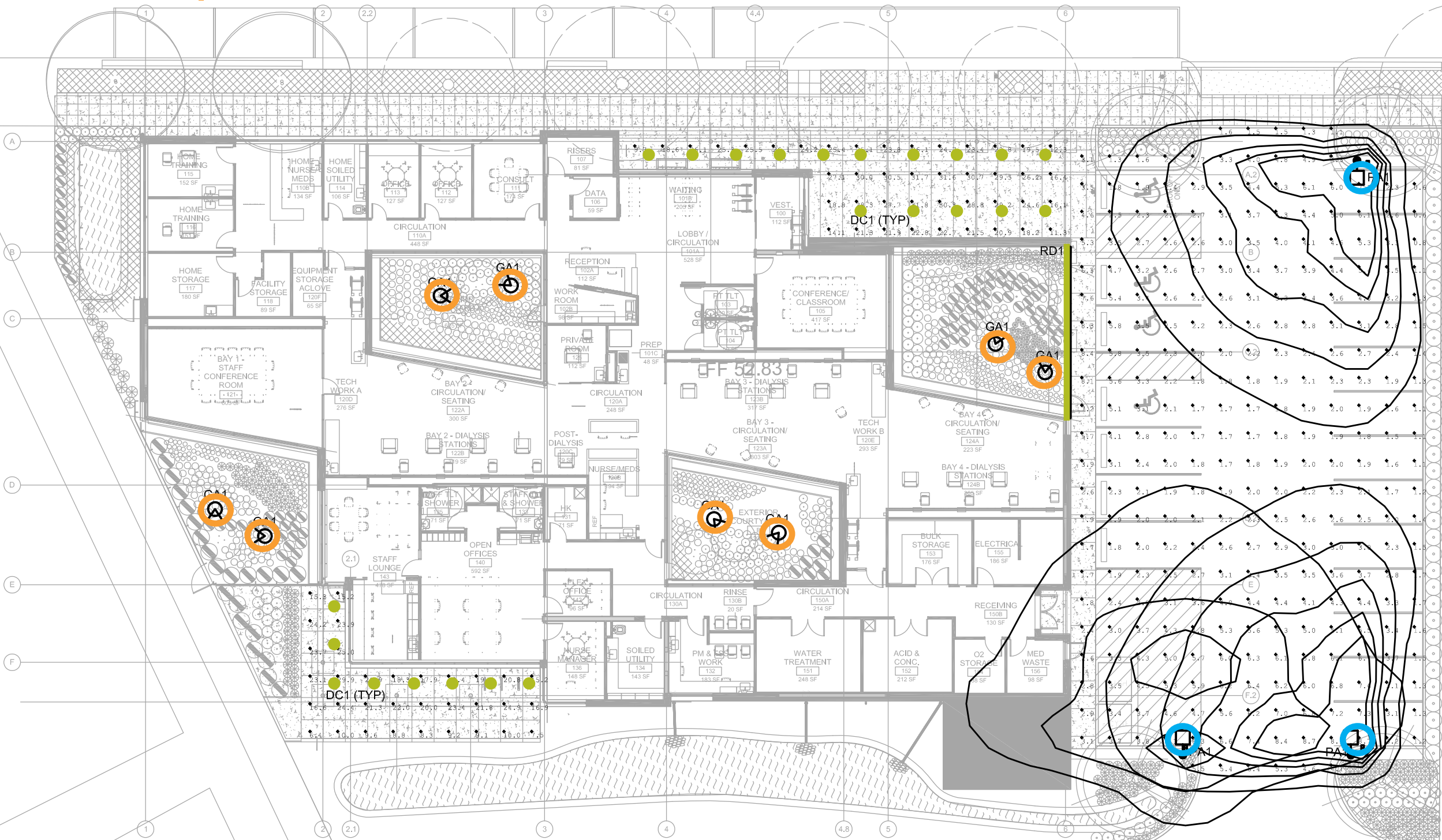
DIALYSIS CLINIC

13.0 | RENDERINGS

PRIMARY SIGNAGE VIEW



14.0 | EXTERIOR LIGHTING PLAN




POLE-MOUNTED LED LIGHTING
 BOD: MCGRAW-EDISON
 GLEON LED

LED SOFFIT LIGHTING
 CAN BOD: LITON DL360
 LINEAR BOD: PINNACLE
 EV3WET SERIES

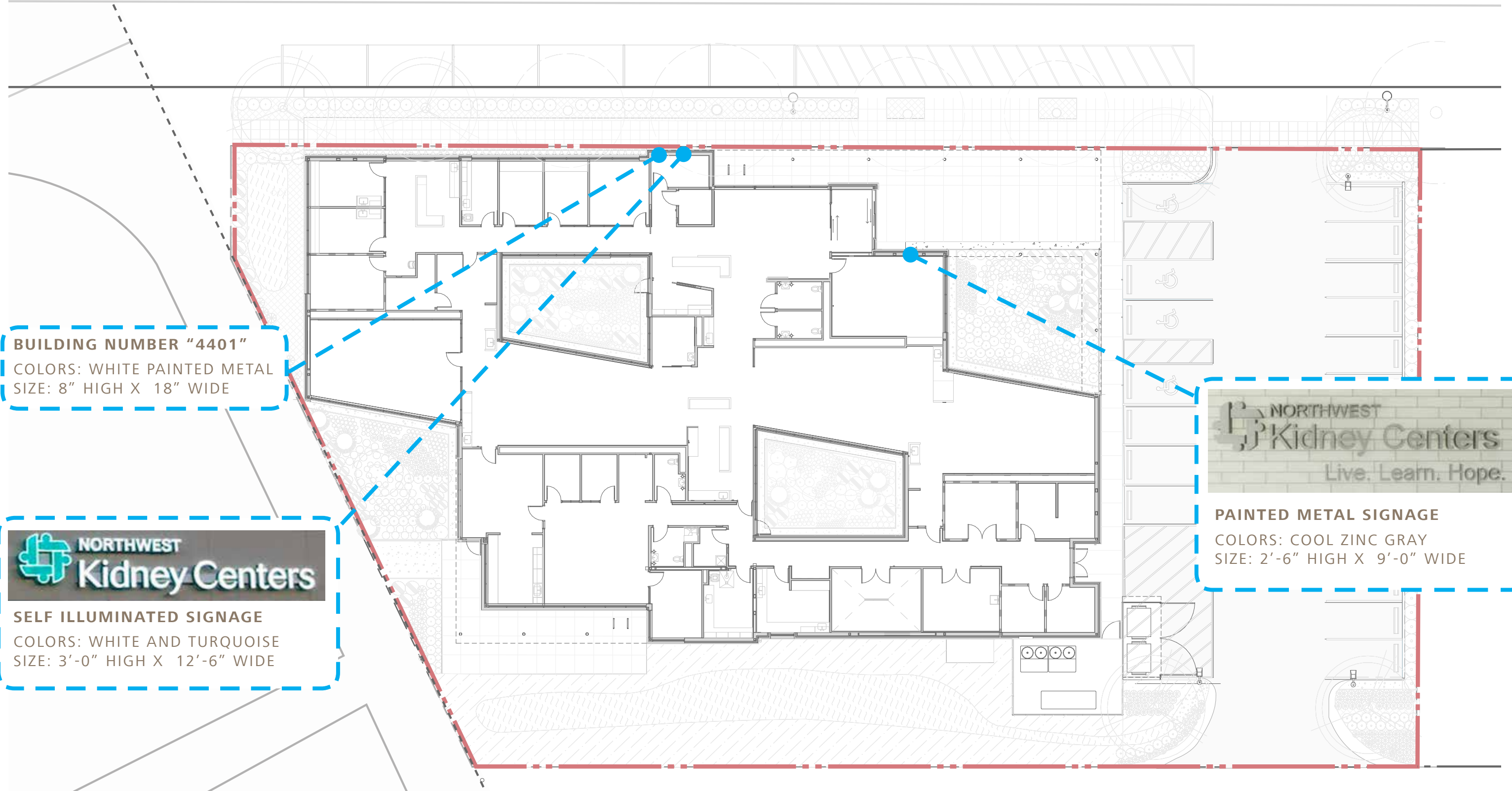


LANDSCAPE ACCENT LIGHTING
 BOD: VISTA 1043 LED


LIGHTING STATISTICS Parking lot overall		LIGHTING STATISTICS Parking lot, ADA stalls		LIGHTING STATISTICS Main entry		LIGHTING STATISTICS Staff entry	
Foot Candle Average	3.4 FC	Foot Candle Average	3 FC	Foot Candle Average	23.8 FC	Foot Candle Average	17.7 FC
Foot Candle Maximum	8.7 FC	Foot Candle Maximum	5.8 FC	Foot Candle Maximum	31.8 FC	Foot Candle Maximum	30 FC
Foot Candle Minimum	0.6 FC	Foot Candle Minimum	1.7 FC	Foot Candle Minimum	9.7 FC	Foot Candle Minimum	6.4 FC
Maximum /Minimum Ratio	14:1	Maximum /Minimum Ratio	4:1	Maximum /Minimum Ratio	3:1	Maximum /Minimum Ratio	5:1
Average/Minimum Ratio	6:1	Average/Minimum Ratio	2:1	Average/Minimum Ratio	2:1	Average/Minimum Ratio	3:1

15.0 | SIGNAGE CONCEPT PLAN

S TRENTON ST



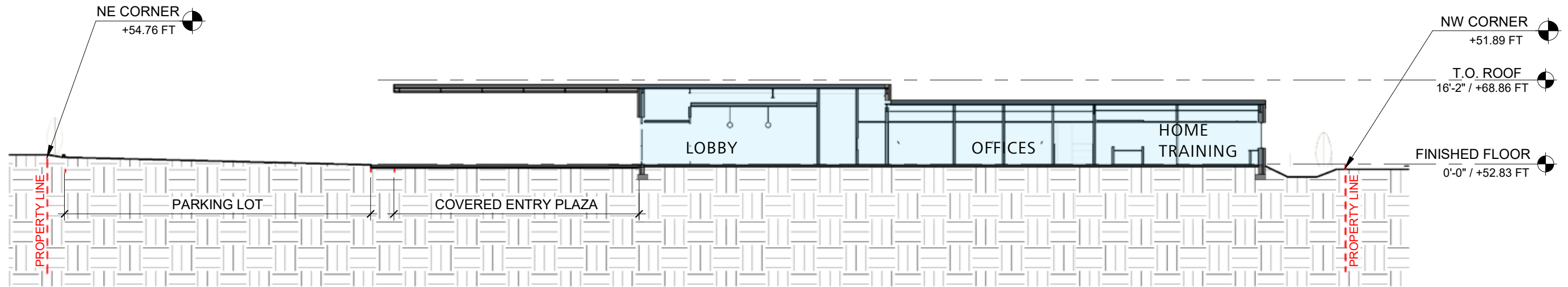
BUILDING NUMBER "4401"
COLORS: WHITE PAINTED METAL
SIZE: 8" HIGH X 18" WIDE

 **NORTHWEST
Kidney Centers**
SELF ILLUMINATED SIGNAGE
COLORS: WHITE AND TURQUOISE
SIZE: 3'-0" HIGH X 12'-6" WIDE


PAINTED METAL SIGNAGE
COLORS: COOL ZINC GRAY
SIZE: 2'-6" HIGH X 9'-0" WIDE

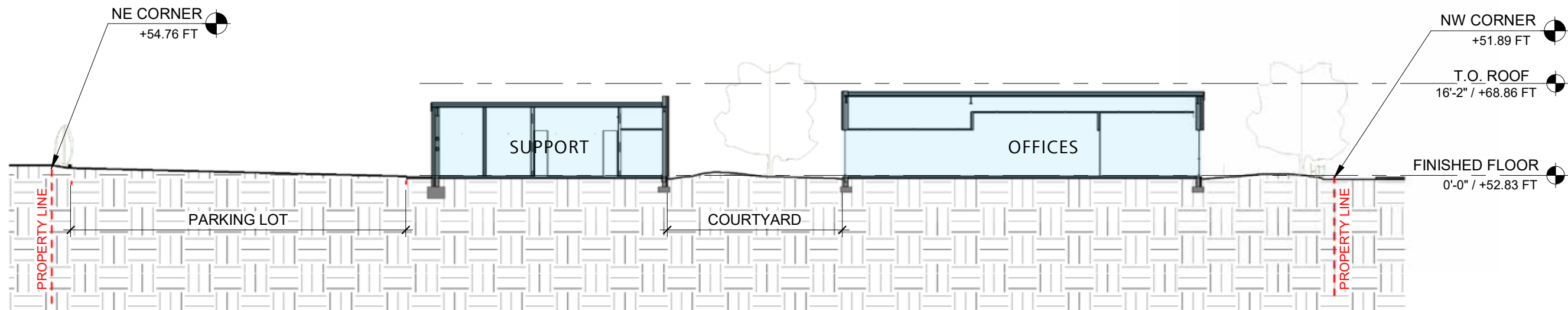
16.0 | BUILDING SECTIONS

EAST-WEST SECTIONS



E-W SECTION 01

SCALE: 1" = 20'-0"

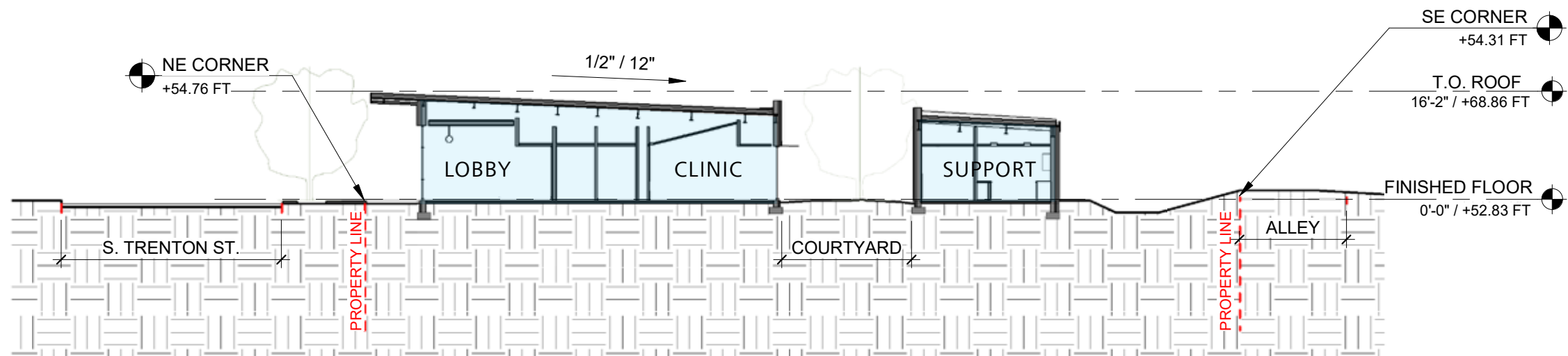
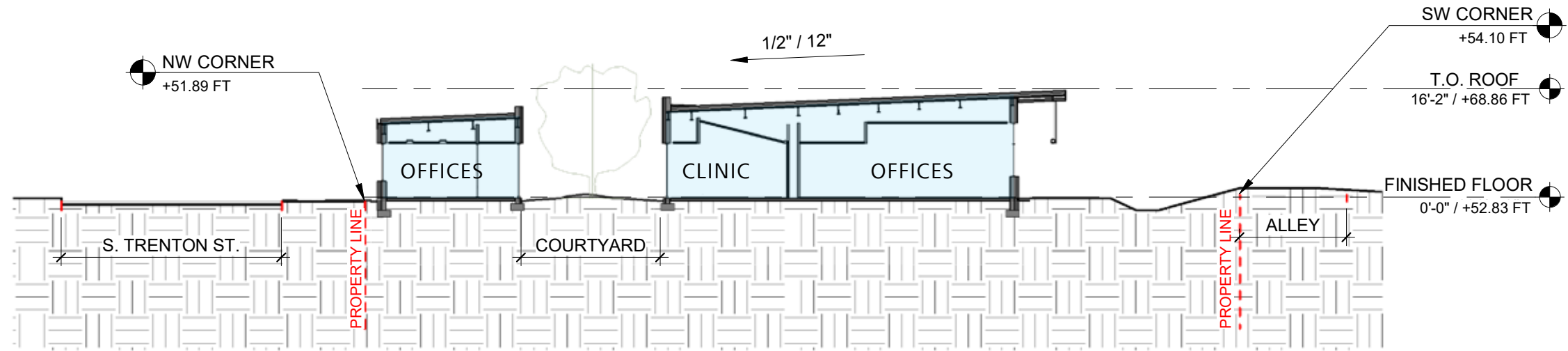


E-W SECTION 02

SCALE: 1" = 20'-0"

16.0 | BUILDING SECTIONS

NORTH-SOUTH SECTIONS



17.0 | DEPARTURES

DEPARTURE REQUEST 1 [17.1]

DEPARTURE REQUEST 1: CURB CUT OFF OF S. TRENTON STREET

The project requests a curb cut off of S. Trenton Street as the primary access point to the site parking. This access would be in addition to required alley improvements to allow access to parking from the alley.

PERTINENT CODE SECTIONS:

23.47A.032 PARKING LOCATION AND ACCESS

A. ACCESS TO PARKING

1. **NC zones. The following rules apply in NC zones**, except as provided under subsections 23.47A.032.A.2 and 23.47A.032.D:

a. **Access to parking shall be from the alley if the lot abuts an alley improved to the standards of subsection 23.53.030.C, or if the Director determines that alley access is feasible and desirable to mitigate parking access impacts. If alley access is infeasible, the Director may allow street access.**

b. **If access is not provided from an alley and the lot abuts only one street, access is permitted from the street, and limited to one two-way curb cut.**

2. **In addition to the provisions governing NC zones in subsection 23.47A.032.A.1, the following rules apply in pedestrian-designated zones, except as may be permitted under subsection 23.47A.032.D:**

a. If access is not provided from an alley and the lot abuts two or more streets, access to parking shall be from a street that is not a principal pedestrian street.

b. **If access is not provided from an alley and the lot abuts only a principal pedestrian street or streets, access is permitted from the principal pedestrian street, and limited to one two-way curb cut. (NOTE: South Trenton Street is not a principal pedestrian street)**

D. EXCEPTIONS TO PARKING LOCATION AND ACCESS REQUIREMENTS.

1. **Access to off-street parking may be from a street if, due to the relationship of an alley to the street system, use of the alley for parking access would create a significant safety hazard as determined by the Director.**

RATIONALE:

23.47A.032D of the Land Use Code notes that "Access to off-street parking may be from a street if, due to the relationship of an alley to the street system, **use of the alley for parking access would create a significant safety hazard as determined by the Director.**"

Additionally, the Design Review Board provided feedback during the Early Design Guidance meeting, stating in comment 2.A:

The Board acknowledged SDOT's recommendation to eliminate the existing vehicle curb cut along S. Trenton Street and require all off-street parking via the alley on the project's south boundary. However, the Board agreed that a curb cut on S. Trenton Street should be permitted and supported the proposed curb cut location and associated parking lot shown in Concept C. The Board supported the curb cut noting that:

- *A curb cut will provide alternative entering and exiting from the site instead of funneling all vehicle and delivery traffic down the alley;*
- *The alley width is currently undersized for commercial uses;*
- *The mobility of many clients of the future clinic is limited and an easily identifiable and accessible off-street parking lot from S. Trenton Street would better accommodate the clinic's clients. (DC1-B, PL4-A)*

Seattle Design Guidelines PL4 (Active Transportation) A.1 notes that "Serving all modes of travel: **provide safe and convenient access points for all modes of travel.**"

Seattle Design Guidelines DC1 (Project Uses and Activities) B.1 notes that "**where driveways and curbcuts are unavoidable, minimize the number and width as much as possible,**" and also recommends "**employing a multi-sensory approach to areas of potential conflict between pedestrian and vehicles.**"

(SEE FIGURE 1.A)

The project proposes one two-way curbcut with a width of 24'-0", as well as the use of contrasting or textured pavement at the curb cut as a multi-sensory device to alert pedestrians to potential vehicular traffic.

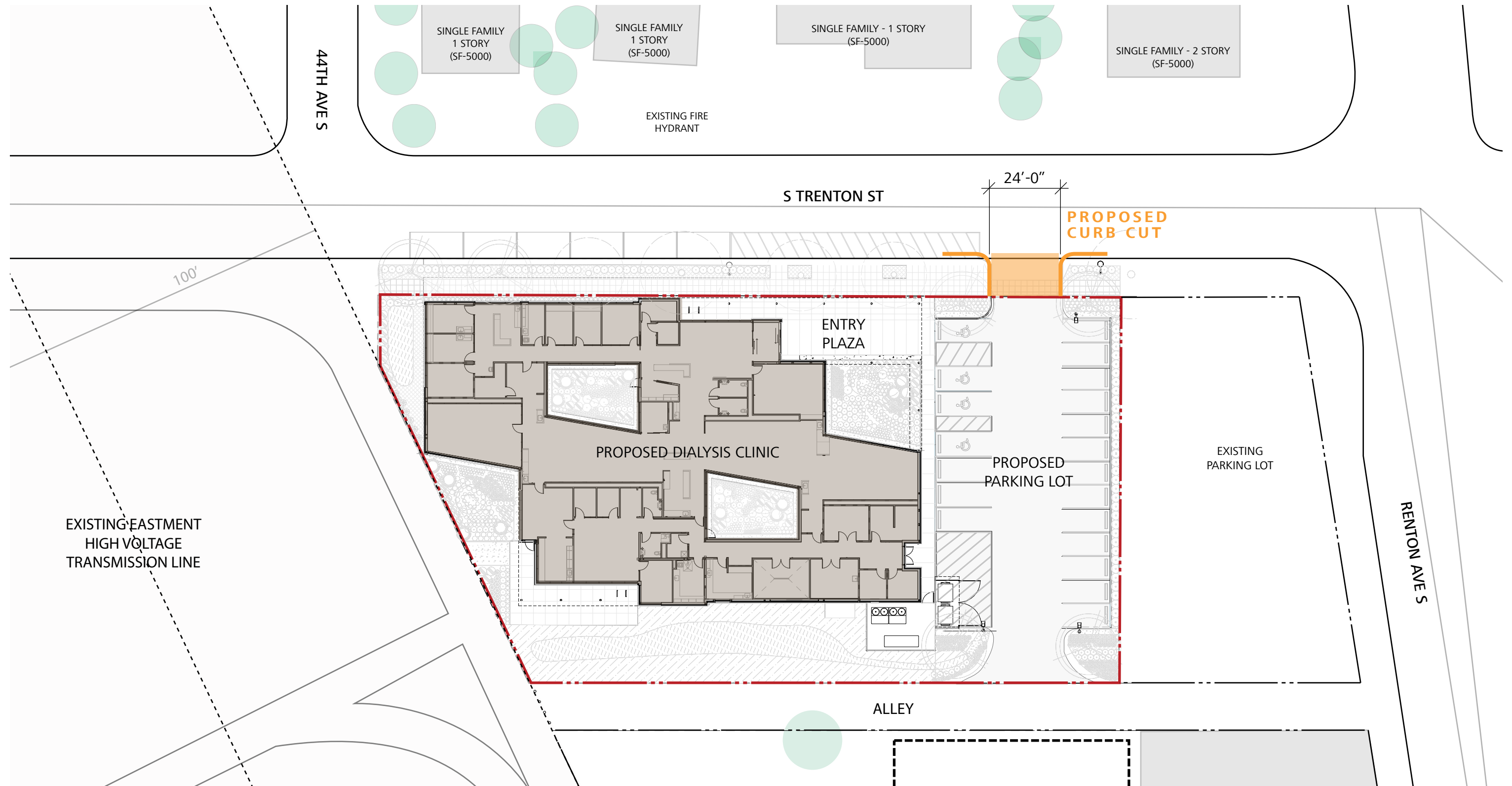
Northwest Kidney Centers performs an essential community service through dialysis treatment. It is critical that patients and staff be able to easily locate and access parking and the facility. If access to parking is only allowed through the existing 16'-0" wide alley, the team has the following concerns:

- Providing parking access through a curb cut adjacent to the access van drop area and main building entry allows for a natural ingress/egress point for patients. Considering patients may not be familiar with alley access, traffic congestion on South Trenton Street would result from patients searching for the parking entry located around the block. Requiring all access to the parking area from the alley off of Renton Avenue South will be problematic for patient ease of access and wayfinding to parking.
- Two-way traffic will be problematic with the 16' width and the existing alley is a dead-end condition, compounding the issue by not providing multiple routes for drivers utilizing the alley.
- If service vehicles are stopped in the alley for allowed loading purposes, all parking ingress and egress will be blocked.
- To Share alley with adjacent residential building as the only access to Northwest Kidney Centers could be problematic. It could cause the potential traffic impact on Renton Avenue South due to the higher traffic volume, as well as the mounting conflict between vehicles and pedestrian, and higher risk to their safety and walkability.
- Northwest Kidney Centers receives supplies by truck that will be easier to maneuver off of a curb cut on South Trenton Street than via the alley.

The concerns noted above show the safety hazard of only having parking access off of the alley. Due to the unique aspect of the existing alley being a dead end, allowing the curb cut off of S. Trenton Street will better serve the community.

17.0 | DEPARTURES

FIGURE 1.A [17.2]



SCALE: 1/32" = 1'-0"
(Printed on 11"x17")

8' 16' 32'

17.0 | DEPARTURES

DEPARTURE REQUEST 2 [17.1]

DEPARTURE REQUEST 2: PARKING STALL SIZES

The project requests that the parking

PERTINENT CODE SECTIONS:

23.54.030 PARKING SPACE STANDARDS

B. PARKING SPACE REQUIREMENTS. The required size of parking spaces shall be determined by whether the parking is for a residential, live-work, or non-residential use. In structures containing residential uses and also containing either non-residential uses or live-work units, parking that is clearly set aside and reserved for residential or live-work use shall meet the standards of subsection 23.54.030.B.1; parking for all other uses within the structure shall meet the standards of subsection 23.54.030.B.2. All uses shall provide barrier-free accessible parking if required by the Building Code, Subtitle I of Title 22, or the Residential Code, Subtitle IA of Title 22.

2. NON-RESIDENTIAL USES

- c. **When 20 or more parking spaces are provided, a minimum of 35 percent of the parking spaces shall be striped for small vehicles.** The minimum required size for small parking spaces shall also be the maximum size. A maximum of 65 percent of the parking spaces may be striped for small vehicles. A minimum of 35 percent of the spaces shall be striped for large vehicles.

RATIONALE:

Per SMC 23.54.015, no parking is required for the site. However, 7 on-site, "large vehicle" and 13 "regular vehicle" parking spaces are desired by the Owner, Northwest Kidney Centers, to accommodate the patient, visitor, and employee parking needs typical for a dialysis clinic of this size. Due to the unique needs of the dialysis patient population, this quantity of large and regular parking spaces are desired to accommodate the typical range of vehicle sizes for patients and provide extra room for getting in and out of the vehicles.

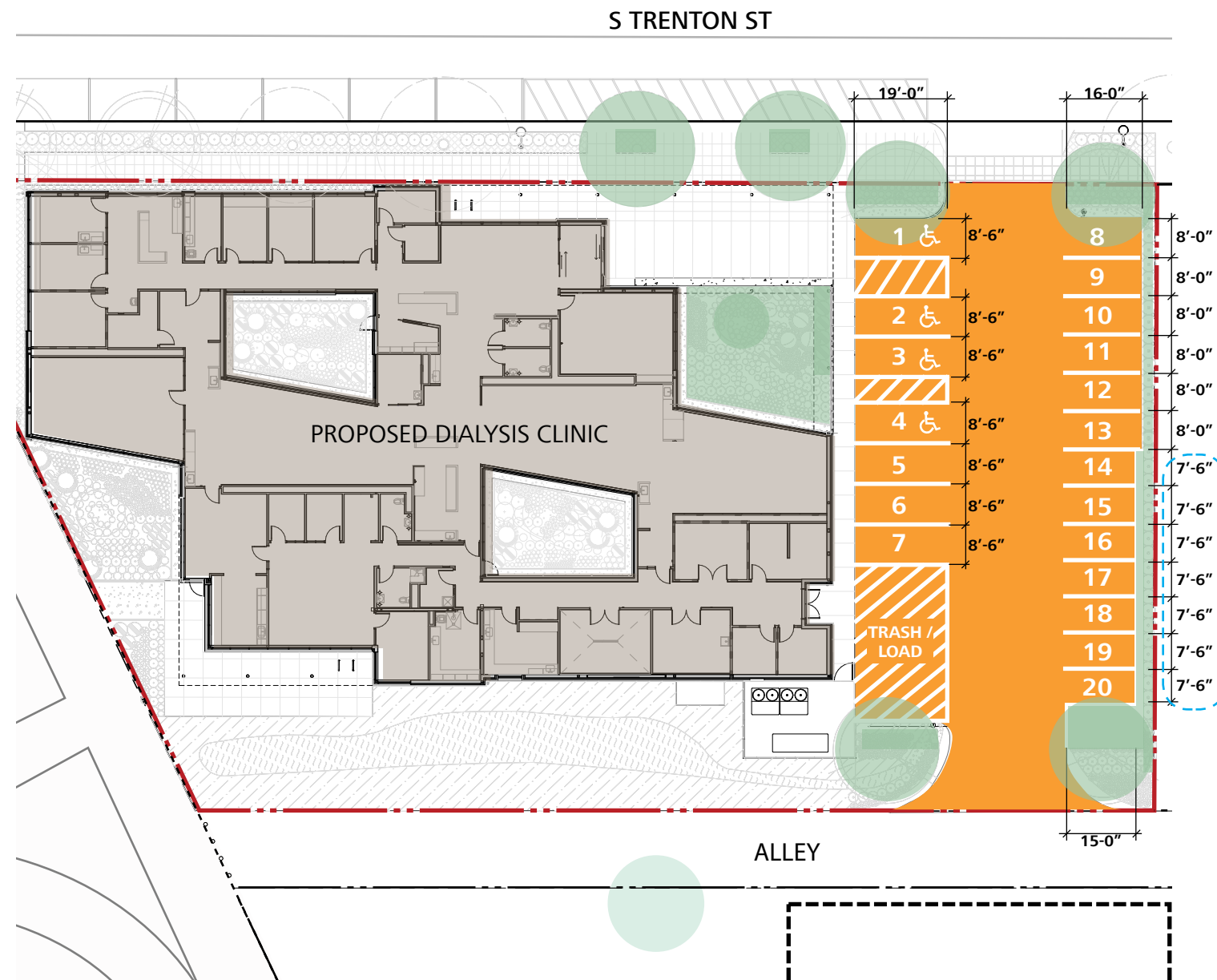
Per SMC 23.54.030.B.2.C, the project would be required 7 "small vehicle" parking stalls (35%) of the 20 desired spaces, which would impose a hardship on this user group and would not advance the goals of the City as stated in the Seattle Design Guidelines.

Seattle Design Guidelines PL4 (Active Transportation) A.1 notes that "Serving all modes of travel: **provide safe and convenient access points for all modes of travel.**" The reduction in size of 7 standard parking spaces would restrict the total available patient parking for those with non-compact vehicles and would prove to be less safe and convenient for the patient and visitor population using the parking lot.

Additionally, the project has already provided landscaped screening in accordance with Seattle Design Guidelines DC1 (Project Uses and Activities) C.1, which encourages projects to "**reduce the visual impacts of parking lots, parking structures, entrances, and related signs and equipment as much as possible.**" Per (FIGURE 2.A), the impact of the parking lot on the site and the screening strategy is consistent for both the requested departure and the code compliant layout.

17.0 | DEPARTURES

FIGURE 2.A [17.2]

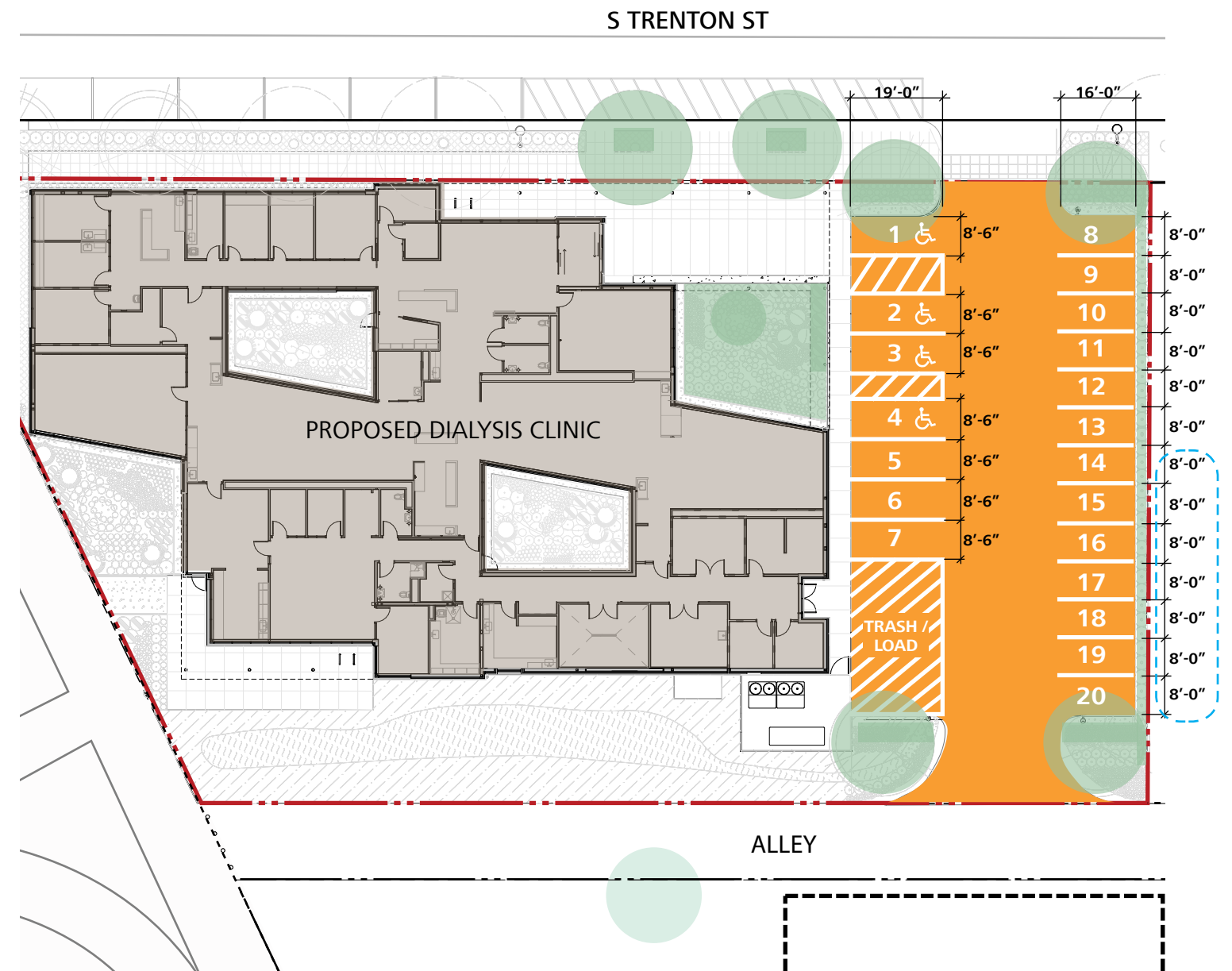


SCALE: 1/32" = 1'-0"
(Printed on 11"x17")

8' 16' 32'

CODE COMPLIANT PARKING SPACE SIZES

LARGE (8'-6" x 19'-0"):	7 (4 ADA)	35%
REGULAR (8'-0" x 16'-0"):	6	30%
SMALL (7'-6" x 15'-0"):	7	35%



SCALE: 1/32" = 1'-0"
(Printed on 11"x17")

8' 16' 32'

DEPARTURE REQUEST PARKING SPACE SIZES

LARGE (8'-6" x 19'-0"):	7 (4 ADA)	35%
REGULAR (8'-0" x 16'-0"):	13	65%

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18.0 | OTHER

APPENDIX 1: FEATURED PROJECTS



NORTH PORTLAND CLINIC

OWNER: Providence Health and Services
LOCATION: Portland, OR
TEAM: Mahlum
SCOPE: 19,500 SF



MERIDIAN CENTER

OWNER: Neighborcare
LOCATION: Seattle, WA
TEAM: Spectrum Development Solutions, NBBJ
SCOPE: 44,800 SF



WEST CAMPUS HOUSING

OWNER: University of Washington
LOCATION: Seattle, WA
TEAM: Mahlum, Spectrum Development Solutions
SCOPE: 675,000 SF (4 Buildings)



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