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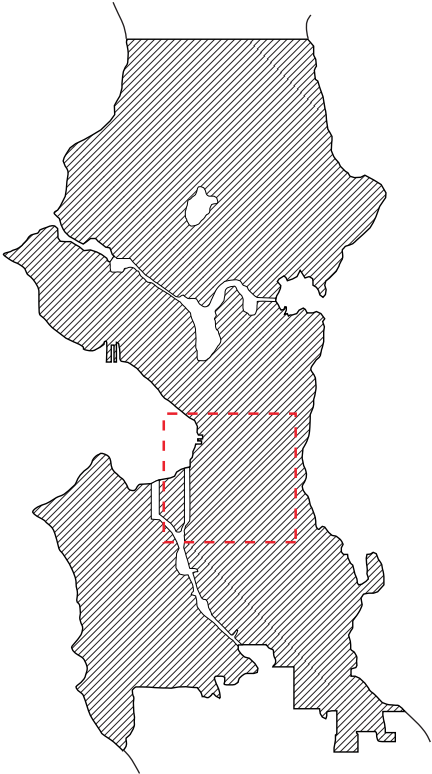
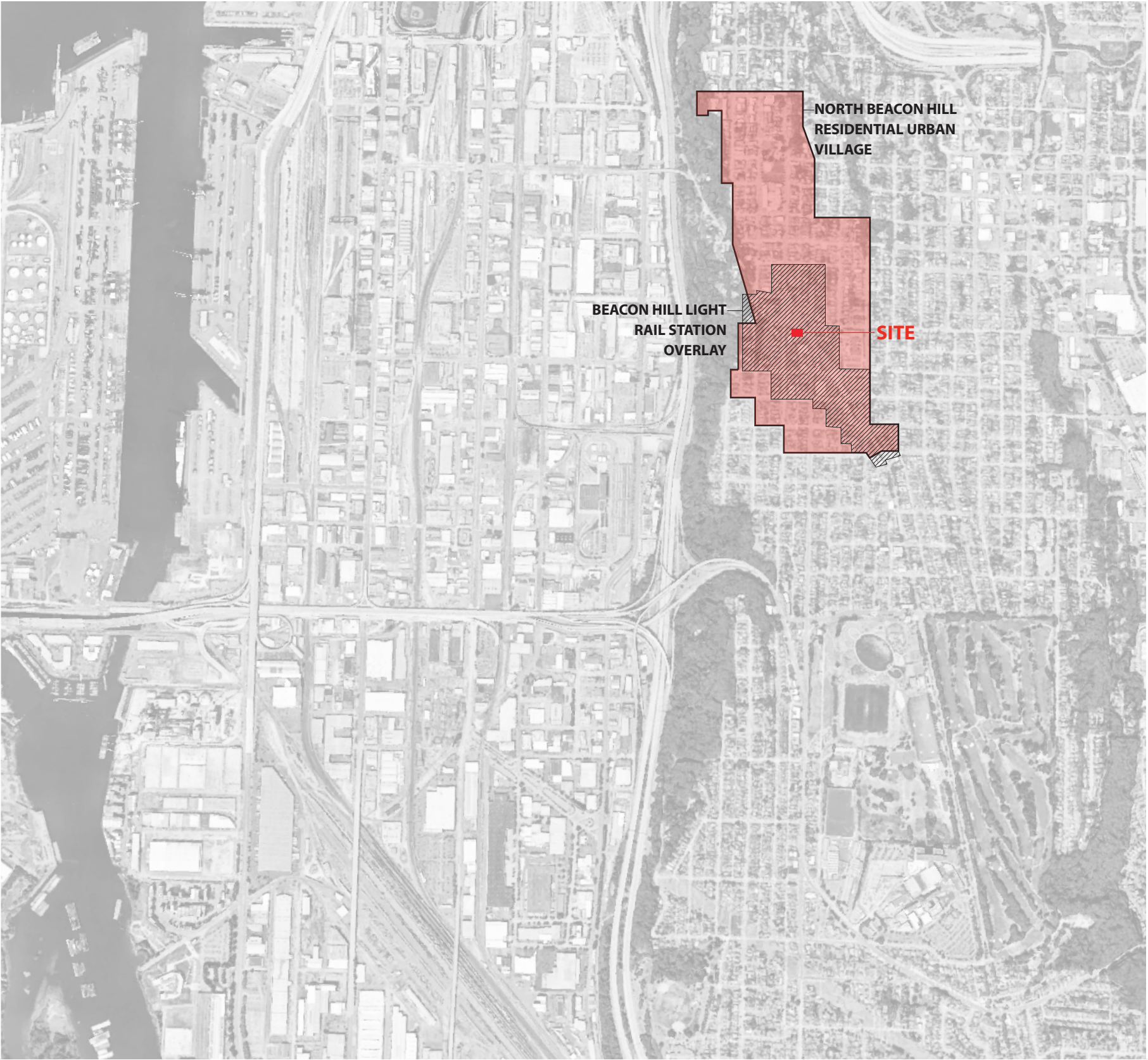
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Project Information

Property Address: 1405 S Bayview, Seattle, WA 98144	
Owner:	BIHI, LLC
Developer:	Tim Abell, Principal Pacific Housing NW LLC 229 Broadway E. Seattle. WA 981012
Architect:	Weinstein A+U LLC Matthew Zinski T (206) 443-8606
Landscape:	Karen Kiest Landscape Architects Karen Kiest

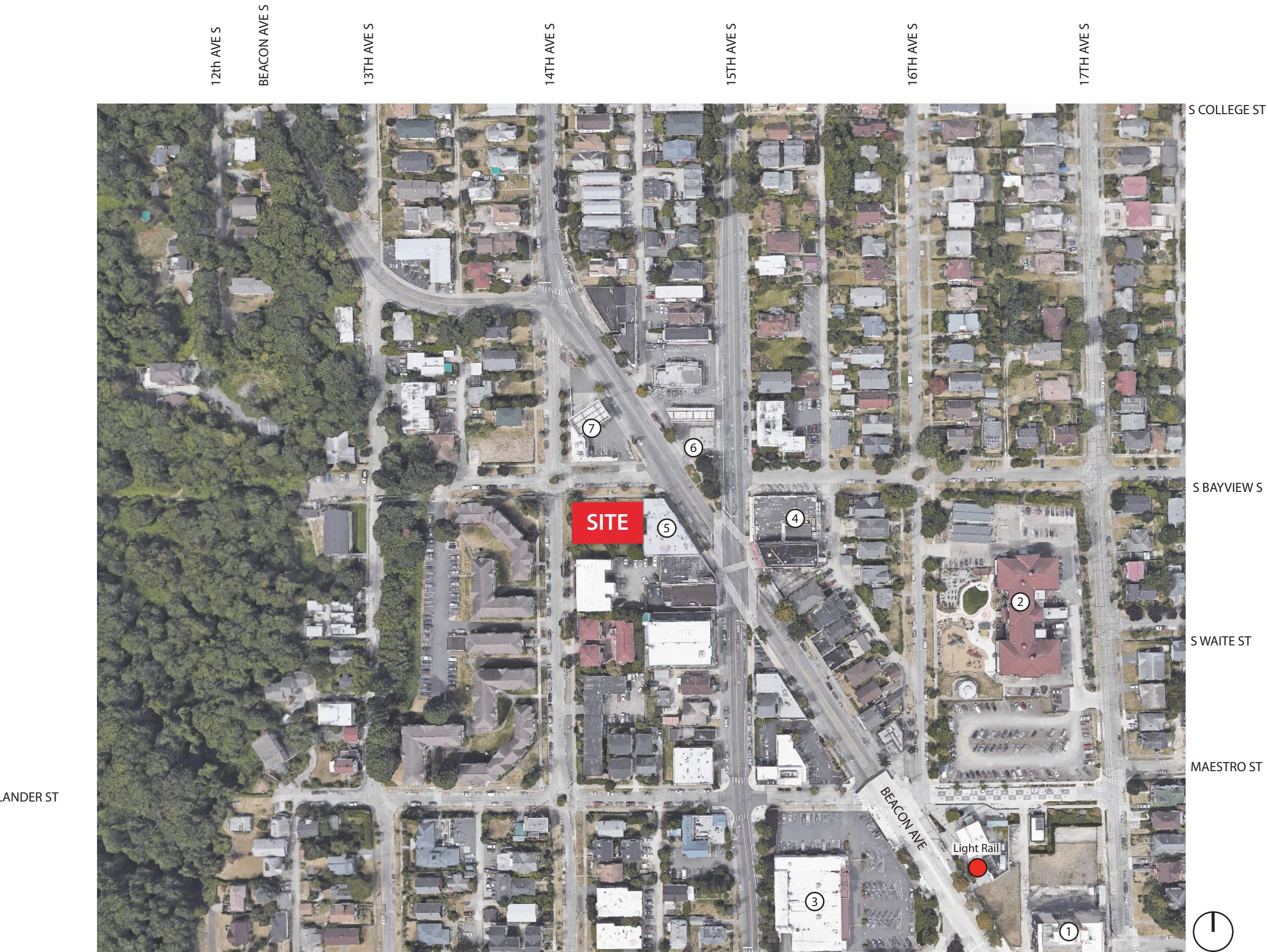




ZONING DATA

PARCEL NO	138980-0005	
LOT AREA	12,800-sf (0.29 acres)	
ZONING	NC2-65(4.0) Beacon Hill Urban Village, Beacon Hill Station Overlay	
PERMITTED USES	<ul style="list-style-type: none"><li>Eating and drinking: ≤25,000-sf as conditional use</li><li>Restuarants: ≤25,000-sf allowed</li><li>Retail sales &amp; services, general: ≤25,000-sf allowed</li><li>Live-work units: permitted outright</li><li>Residential uses: permitted outright</li></ul>	23.47A.004, Table A
STREET LEVEL STANDARDS	<ul style="list-style-type: none"><li>Blank segments of street-facing façade between 2'–8' above grade may not exceed 20' in width</li><li>Blank façade segments not to exceed 40% of façade width</li><li>Transparency (at non-residential uses)<ul style="list-style-type: none"><li>60% of façade 2'–8' above grade to be transparent</li><li>Allow unobstructed views in from outside</li></ul></li><li>Non-residential uses to have average depth of 30-feet with a minimum depth of 15-feet</li><li>Non-residential uses at street level to have minimum floor-to-floor height of 13-feet</li></ul>	23.47A.008
STRUCTURE HEIGHT	<ul style="list-style-type: none"><li>Maximum structure height as zoned: 65'</li><li>Rooftop features: open railings, planters, clerestories, parapets, firewalls permitted to extend 4-ft above applicable height limit</li><li>Mechanical equipment, stair and elevator penthouses, etc., permitted to extend 15-ft above applicable height limit—as long as total does not exceed 20% of the roof area—or 25% if the total includes stair or elevator penthouses or screen mechanical equipment.</li></ul>	23.47A.012
FAR	<ul style="list-style-type: none"><li>Applies to all structures in NC zones</li><li>Allowable FAR for in sites zoned with a 65' height limit: 4.25</li><li>Gross floor area not counted against FAR:<ul style="list-style-type: none"><li>Areas underground</li><li>Portions of a story extending no more than 4-ft above grade (lower of existing or finished grade)</li></ul></li><li>Minimum required FAR for new construction: 2.0</li></ul>	23.47A.013
SETBACKS	<ul style="list-style-type: none"><li>A setback is required where a lot abuts the intersection of a side lot line and front lot line of a lot in a residential zone.<ul style="list-style-type: none"><li>15-foot triangle</li></ul></li><li>A setback is required along any rear or side lot line that abuts a lot in a residential zone<ul style="list-style-type: none"><li>15-feet for portions of structures above 13-feet in height to a maximum of 40-feet</li><li>For each portion of a structure above 40-feet in height, additional setback at a rate of 2-foot of setback for every 10-feet in height</li></ul></li></ul>	23.47A.014
PARKING	<ul style="list-style-type: none"><li>Off-street parking per SMC 23.54.015</li><li>In pedestrian-designated zones, parking waived for first 1,500-sf of each business establishment</li><li>Bicycle parking required per Table D<ul style="list-style-type: none"><li>Eating &amp; drinking establishments, 1/12,000-sf long-term, 1/2,000-sf short-term</li><li>Sales &amp; services general, 1/12,000-sf long-term, 1/2,000-sf short-term</li><li>Multi-family structures, 1/4DUs long-term, no short-term</li></ul></li><li>Parking for Non-Residential Uses per Table A<ul style="list-style-type: none"><li>No parking required for non-residential uses in urban centers or SAOD</li></ul></li><li>Parking for Residential Uses per Table B<ul style="list-style-type: none"><li>No parking required for residential in urban centers or SAOD</li></ul></li></ul>	23.47A.030
AMENITY AREA	<ul style="list-style-type: none"><li>Residential amenity area equal to 5% of residential gfa<ul style="list-style-type: none"><li>All residents have access to at least an amenity or private area</li><li>Amenity areas to be unenclosed</li><li>Common residential amenity areas shall have a dimension of at least 10-ft and a minimum area of 250-sf</li><li>Private balconies and decks to have a dimension of at least 6-ft and an minimum area of 60-sf</li></ul></li></ul>	23.47A.024
PARKING ACCESS	<ul style="list-style-type: none"><li>If access is not from alley and site abuts two or more streets access permitted to cross one of the side street lot lines per SMC 23.47A.032.C with curb cuts per SMC 23.54.030.F.2.a.1</li><li>In pedestrian-designated zones, if access is not from alley and site abuts two or more streets, access to be from street that is not a principal pedestrian street</li><li>Director to determine which street is the front lot line when lot fronts on two or more streets</li></ul>	23.47A.032





# PROPOSAL & SITE CONTEXT

### Location

The proposed project is in Seattle’s Beacon Hill Urban Village on the northwest corner of the block bounded by 14th Ave S & Beacon Ave S and S Bayview St & S Lander St - at the intersection of S Bayview St and 14th Ave S. The site is within a frequent transit corridor and the Beacon Hill Station Overlay District.

### Development Objectives

The proposed project will be a seven-story, apartment building approximately 65-feet in height and approximately 57,650-sf, including the below grade level. The building will contain 67 residential apartment units, resident lobby and amenity spaces, an outdoor landscaped terrace and roof deck, on-site leasing office, street level commercial space, in-structure resident parking for 21 vehicles, and a below-grade level for building services and storage. The project development objectives are as follows:

- Provide a high quality living environment for residents that is convenient to work, leisure and recreation
- Provide a lively environment for urban residents, including accommodations for retail and/or restaurants
- Develop pedestrian-oriented design to benefit the neighborhood:
  - Widen sidewalks
  - Strengthen the street edge with an appropriately scaled facade
  - Create vibrant and transparent street level façades
  - Plant street trees to buffer traffic
- Be a good neighbor. This will inform the project in terms of:
  - Commercial level streetscape design
  - Landscape design & material selection
  - Exterior lighting design
  - Parking access
  - Trash & recycling storage
- Provide a place that fits with the neighborhood character and culture.

- Link Light Rail Station
- 1. The Denning Mixed-Use Development
- 2. El Centro De La Raza
- 3. Hilltop Red Apple Grocery
- 4. Beacon Hill Market Grocery
- 5. Commercial
- 6. Shell Gas Station
- 7. 76 Gas Station



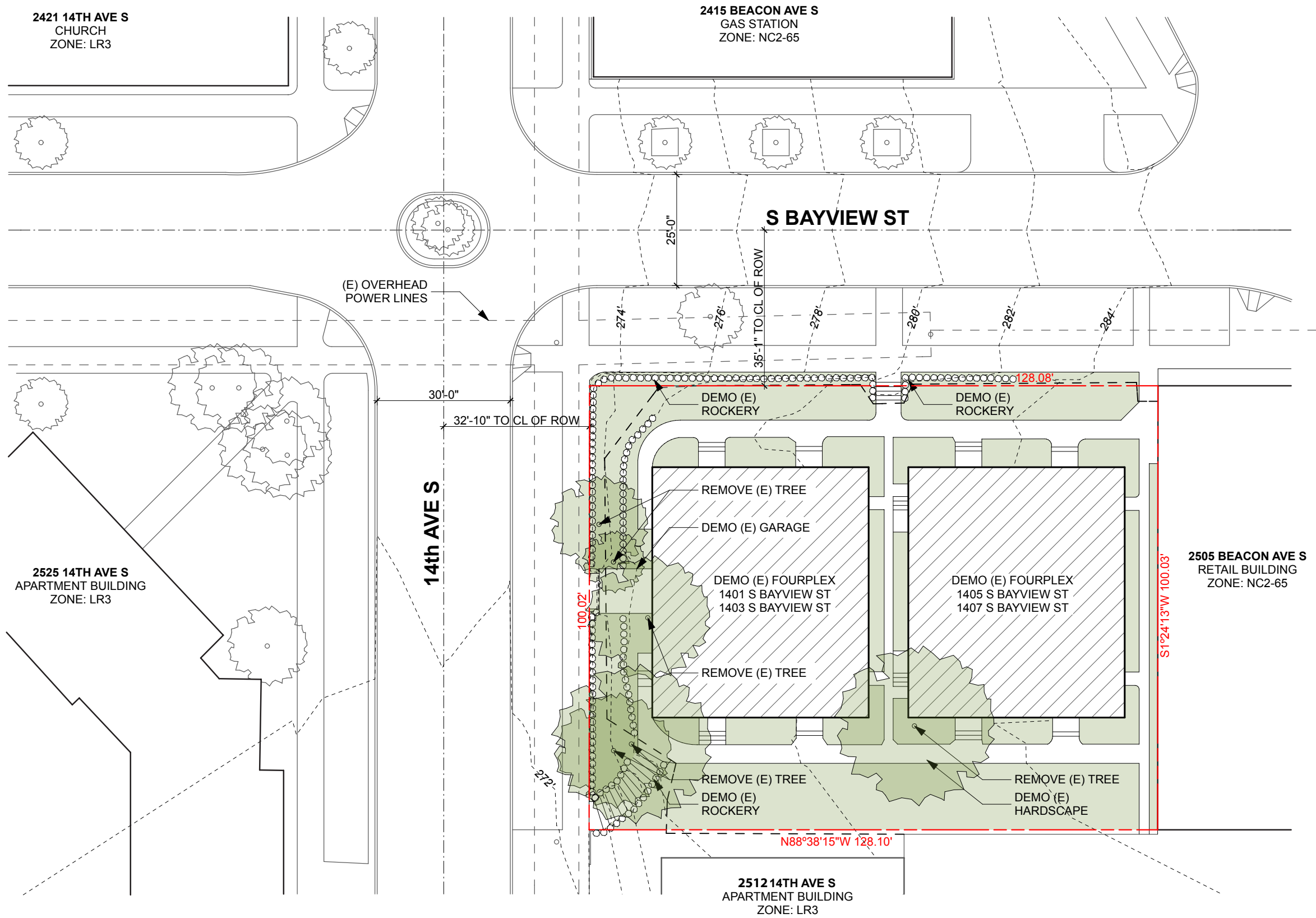
PROPOSAL & SITE CONTEXT



\* Zoning Heights Shown As Approximation / Not to Scale



# EXISTING SITE PLAN



**Legal Description**  
CARNEYS SUPL OF TR 5 BAYSIDE ADD  
PLat Block: 5  
Plat Lot: 1 & 2

**Description**  
The project site is a 12,800-sf parcel zoned NC2-65 (4.0) – 4.75 through incentive housing - and located in the middle of the North Beacon Hill Urban Village. The parcel occupies a corner site at the intersection 14th Avenue S to the west, S Bayview Street to the north, parcels located directly to the south and east. The eastern parcel is zoned NC2P-65 and the southern parcel is zoned LR3. The site is located in the Beacon Hill Station Area Overlay District and is served by Frequent Transit.

To the south of the property, abutting the property line are the Bayview Apartments at 2512 14th Ave S – zoned LR3 this is a 4-story apartment building built in 1968 containing 18 units. To the east, also abutting the property line, is the future development of 2505 Beacon Ave S – zoned NC2P-65, the property is presently occupied by a retail warehouse. This site is currently moving through the MUP process, project #3024602, with a proposed 6-story apartment building with 90 units, ground level commercial, and 3 parking spaces. To the west, across 14th Ave S, the properties are zoned LR3 and contain the Westview Apartments, a 3-story complex with 140 units. To the north, across S Bayview Street, the properties are zoned NC2P-40, occupied by a 76 Convenience Store and Gas Station.

The project site contains two, derelict fourplexes that have been unoccupied for years. Built in 1926 both structures are non-distinctive Tudor Revival style housing.

The existing topography of the site is relatively flat north to south along 14th Ave S descending from  $\pm 273.27'$  at the northwest corner to  $\pm 272.08'$  at the southwest corner (an approximate slope of 1%). The slope along S Bayview Street is more significant (approximately 9%) descending from 285.00' at the northeast corner to 273.27'.

The existing structures on the site are built up on rockery berms, 0.5-10 feet above street level. During demolition the rockeries will be removed to locate new building entries at the sidewalk grade.



# CONTEXT: S BAYVIEW ST

The northern edge of the property is bounded by S Bayview Street. The topography along the property line has a significant slope (9%) up towards the east. The sidewalk along this stretch of Bayview is generous (roughly 15-feet), inclusive of a planted strip at the curb. There are some existing street trees on either side within the neighboring blocks. The south side of the Bayview is zoned for NC with a 65-ft height limit while to the north is NC with a 40-ft height limit - though current property uses do not reflect this potential. Currently, Bayview is a mix of under developed lots. An area of higher density retail and mixed-use to the east is visible from the site.

### Design Cues

- Respond to significant slope along S Bayview St
- Locate retail in proximity to adjacent retail at Beacon Ave S
- Appropriately scale retail spaces to fit with Beacon Ave S



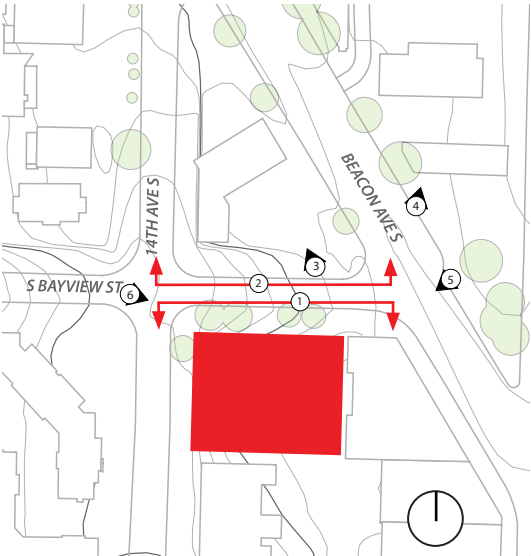
1 S Bayview - Looking South

Project Site



2 S Bayview - Looking North

Opposite of Project Site



3



4



5



6



## CONTEXT: 14TH AVE S



14th Ave S - Looking East

Project Site

The western edge of the property is bounded by 14th Avenue S. The topography along the property line has a minimal slope (1%) up towards the north. The sidewalk along this stretch of 14th is generous (roughly 15-feet), inclusive of a planted strip at the curb. There are few existing street trees on either side within the neighboring blocks. A majority of 14th is zoned for LR3 with a 30-ft height limit - the current property uses reflect this potential. Currently, 14th is a mix of mid-rise apartments.

Two, unoccupied fourplexes currently sit on the site. A single phase power line runs along the south side S Bayview St. A three phase, high voltage power line runs along 14th Ave S on the east side. There are a number of small trees on the site, scattered throughout, built-up rockeries, and some chain link fencing.

The sidewalks on S Bayview St and 14th Ave S, as mentioned previously, are generous and contains street trees.

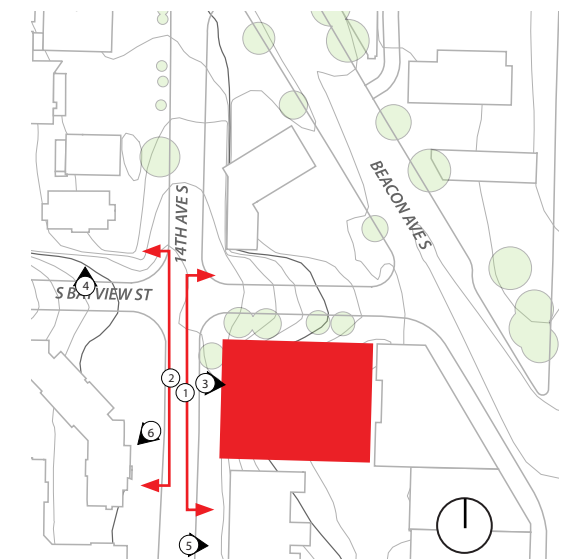
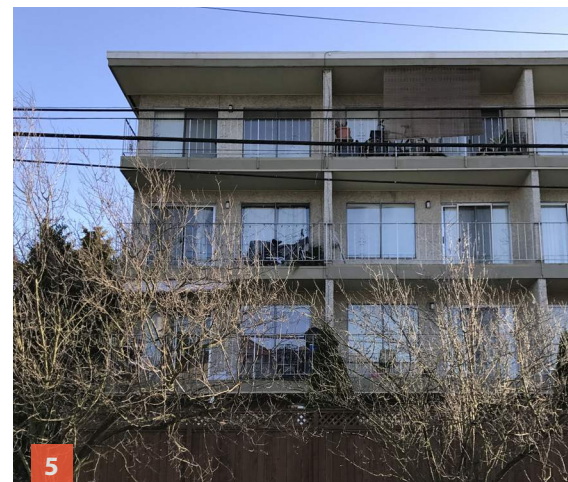
### Design Cues

- Scale transition at 14th Ave S
- Consistent setbacks along 14th Ave S on east and west side of street
- Existing building materials and facade composition



14th Ave S - Looking West

Opposite of Project Site





# CONTEXT: BEACON HILL

The neighborhood surrounding the site is in the early stages of growth with underdeveloped parcels undergoing transition to higher density, multifamily and mixed use development. The existing commercial streetscape is diverse and inconsistent with a lack of a street wall and storefront activation. Retail tenants often occupy converted residential buildings and are setback from the sidewalk. There is several projects with commercial structures separated by surface parking.

The Beacon Hill core and Beacon Hill Light Rail Station are located along Beacon Ave S just south of the project site. Retail and mixed-use markedly increase along with pedestrian amenities such as street trees, cross walks, public art, benches, and improved sidewalks. Further development between the project site and Beacon Hill Light Rail Station would enhance the pedestrian connection along Beacon Ave S.

In contrast to the evolving character and density of Beacon Ave S, the residential areas of Beacon Hill, several blocks north, east, south, and west of the proposed project site, are well-established single-family blocks. Houses and yards are well maintained. It appears houses in disrepair have been consistently torn down in favor of modern style townhouses. This has created a mix of modern style housing along side Seattle craftsmen.

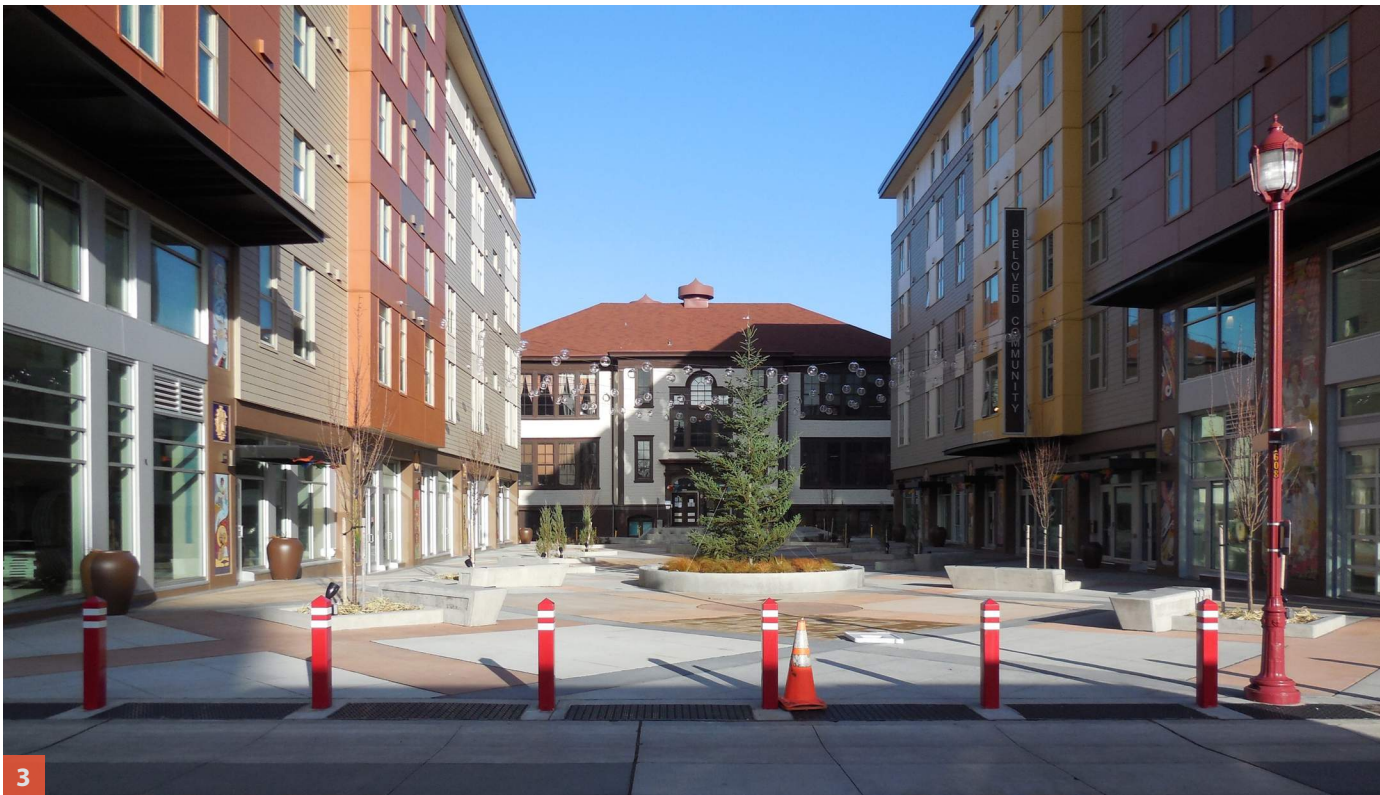
### Design Cues

- Facade composition
- Material usage
- Appropriately scaled massing and secondary elements
- Retail programming, size, and diversity



**1 Beacon Hill Public Library.**  
**2 The Beacon.** Example of historic context.  
**3 Beacon Hill Playground.** North of the site.  
**4 Single-family House.** North of the site.  
**5 The Denning.** Adjacent to Beacon Hill Light Rail Station.





**1 Perihelion Brewery.** Historic context.  
**2 Beacon Hill Light Rail Station.** South of project site.  
**3 El Centro.** Significant development in the heart of Beacon Hill.  
**4 The Station Cafe.** Neighborhood fixture.



# CULTURE: BEACON HILL

“Beacon Hill is a community where immigrants from all over the globe and their descendants settled side by side for over one hundred years. ...neighbors were Filipino, Japanese, Chinese, African-American, and Euro-American. If one ride the Number 36 bus across the Jose Rizal Bridge to Chinatown, one hears dialects of Cantonese, Vietnamese, and Spanish, as well as eastern European languages.”  
- Merrell, Frederica and Latoszek, Mira. *Images of America Seattle's Beacon Hill*. Charleston: Arcadia Publishing, 2003.

The diversity of Beacon Hill have strongly impacted three architectural elements:

- Landscape design
- Material usage and building scale
- Programmatic use and scale

It is in these three characteristics that we see the character of Beacon Hill - curated landscapes and gardens, scaled & tactile materials that subdivide a building mass, and small-scale commerical uses that support local, family-owned business.

## LANDSCAPE DESIGN



## PROGRAM AND SCALE

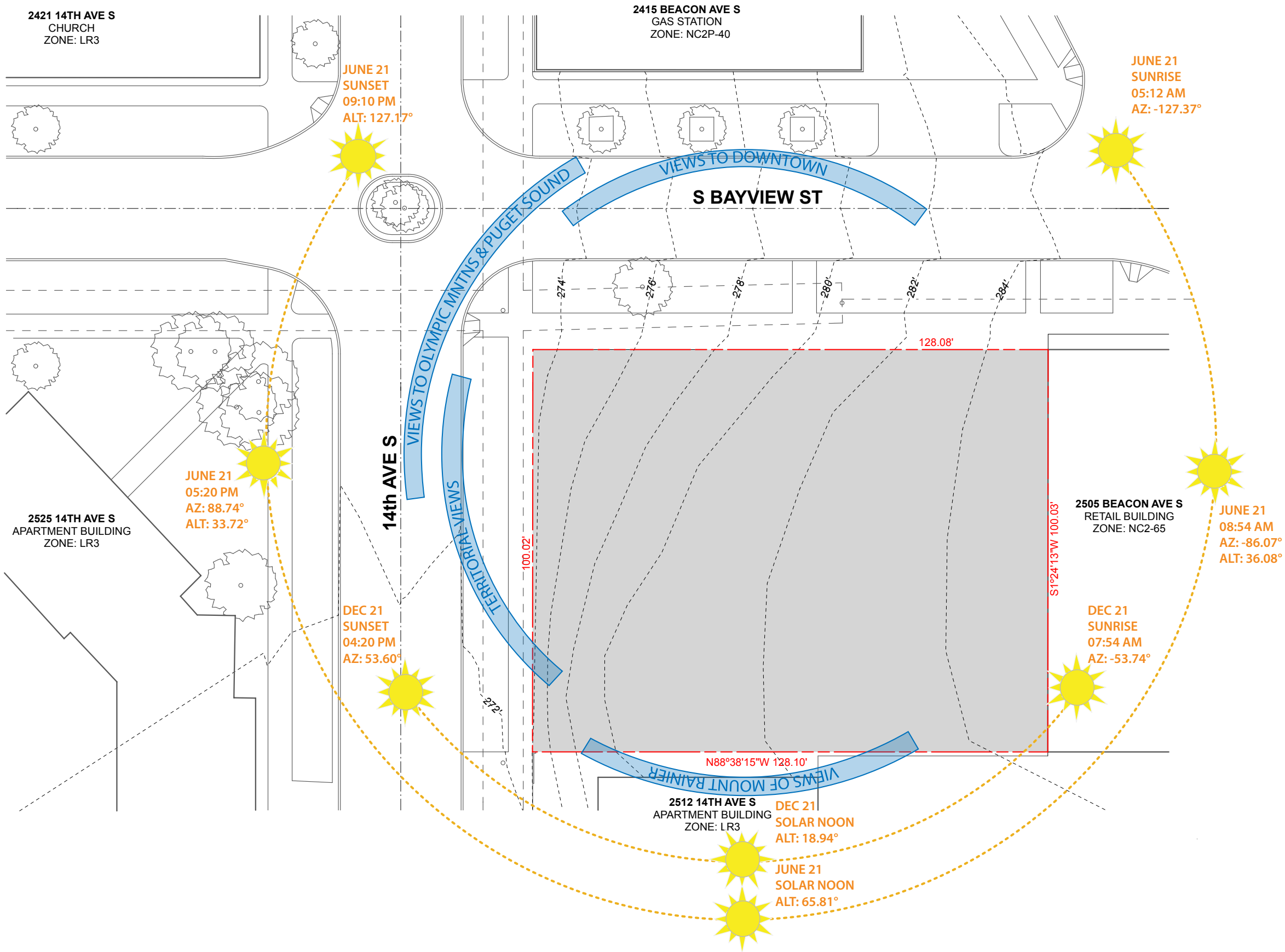


## MATERIALS AND SCALE





# SITE ANALYSIS



## Topography

- Very gentle slope along 14th Ave S
- 2' gain in elevation from south to north
- Significant slope along S Bayview St
- 11' gain in elevation from west to east

## Neighboring Buildings

- Mid-rise apartments to the west and south, LR3
- Gas station to the north, NC2P-40
- Single-story retail to the east, NC2P-65
- Planned 65' mixed-use development to the east

## Solar Access

- Excellent solar access from the north and west due to low-height LR3 development and right-of-way
- Decent solar access from the south due to 4-story development and existing grades
- Presently good solar access from the east due to low-height development. Future development will limit solar access.

## Views

- Currently, views of downtown, the Olympics, and Mt. Rainier are available to the north, west, and south above the fourth floor.

## Structure Height

- Zoned as NC2-65 with a base height limit of 65-ft
- Total maximum allowable building height: 65-ft

## Allowable Building Area

- 4.25 Base FAR (4.75 Max FAR)
- 12,800-sf lot area
- Maximum allowable floor area: 60,800-sf including incentive provisions



# APPROVED EDG CONCEPT

Alternative 3 explores the opportunities of a building that responds to the urban rhythm and sloping topography. The mass is broken down into two distinct masses, using the corridor as a “gasket”- providing an opportunity for natural light and views. The mass set back from the western property line to provide a “front yard” similar to the neighborhood along 14th Ave S. The mass is carved out at the southern property line to provide access to light and air for the residents and neighbors. Finally, the mass steps down as it moves down the slope. In effect, this massing creates a responsive urban form. The setback and building step helps to reduce the perception of mass.

**Summary**

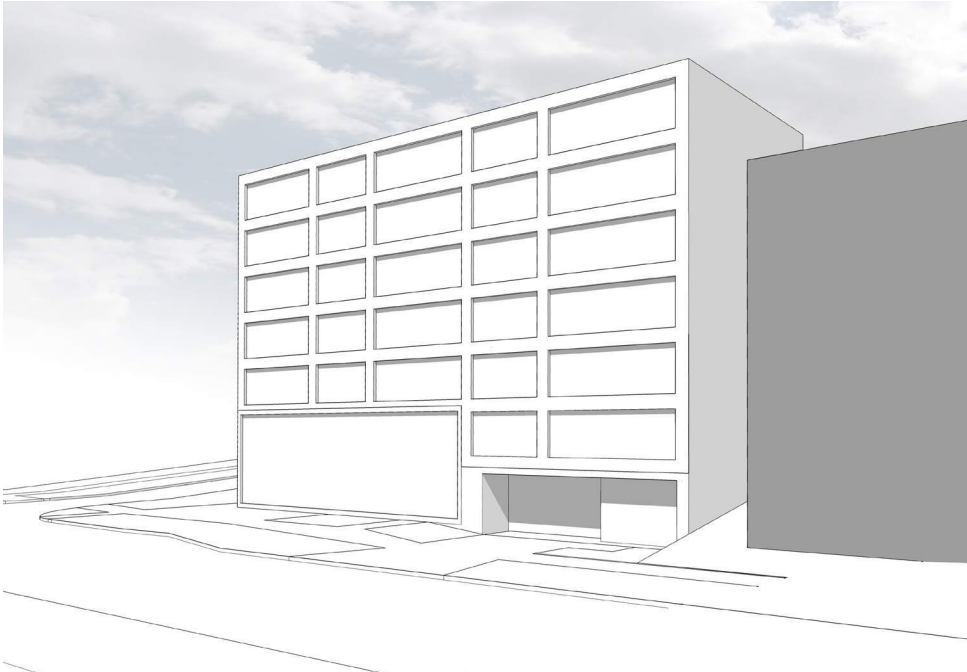
Stories: 8 (5-over-2)  
Unit Count: 79  
Floor Area: 50,100-SF Residential  
2,200-SF Commercial  
8,500-SF Parking  
60,800-SF Total  
Parking: 23 stalls

Ground Floor Uses:

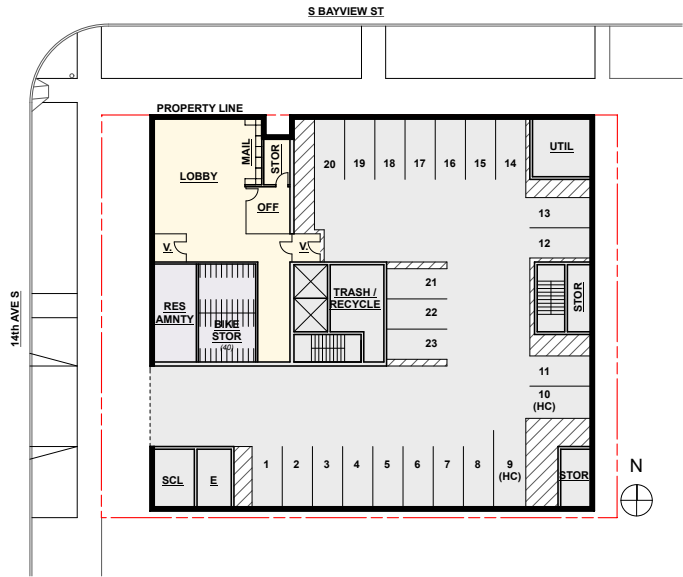
- S Bayview St: Commercial / Residential Lobby
- 14th Ave S: Residential Lobby & Amenity



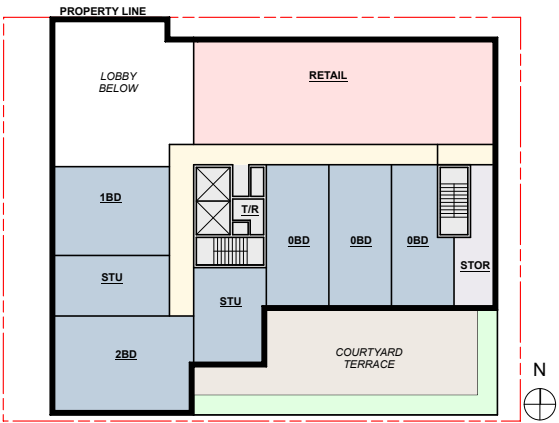
Northwest Corner (14th Ave S)



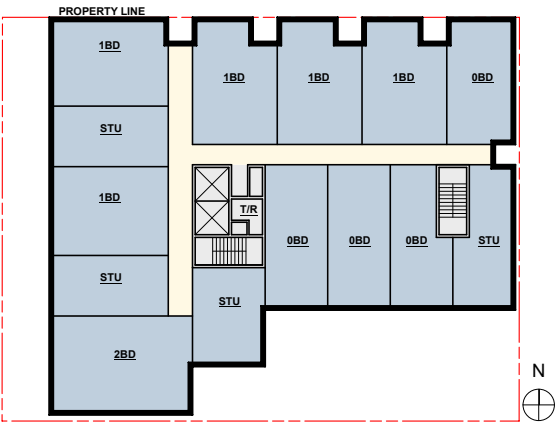
Southwest Corner (14th Ave S)



Street Level Floor Plan (Level 1)

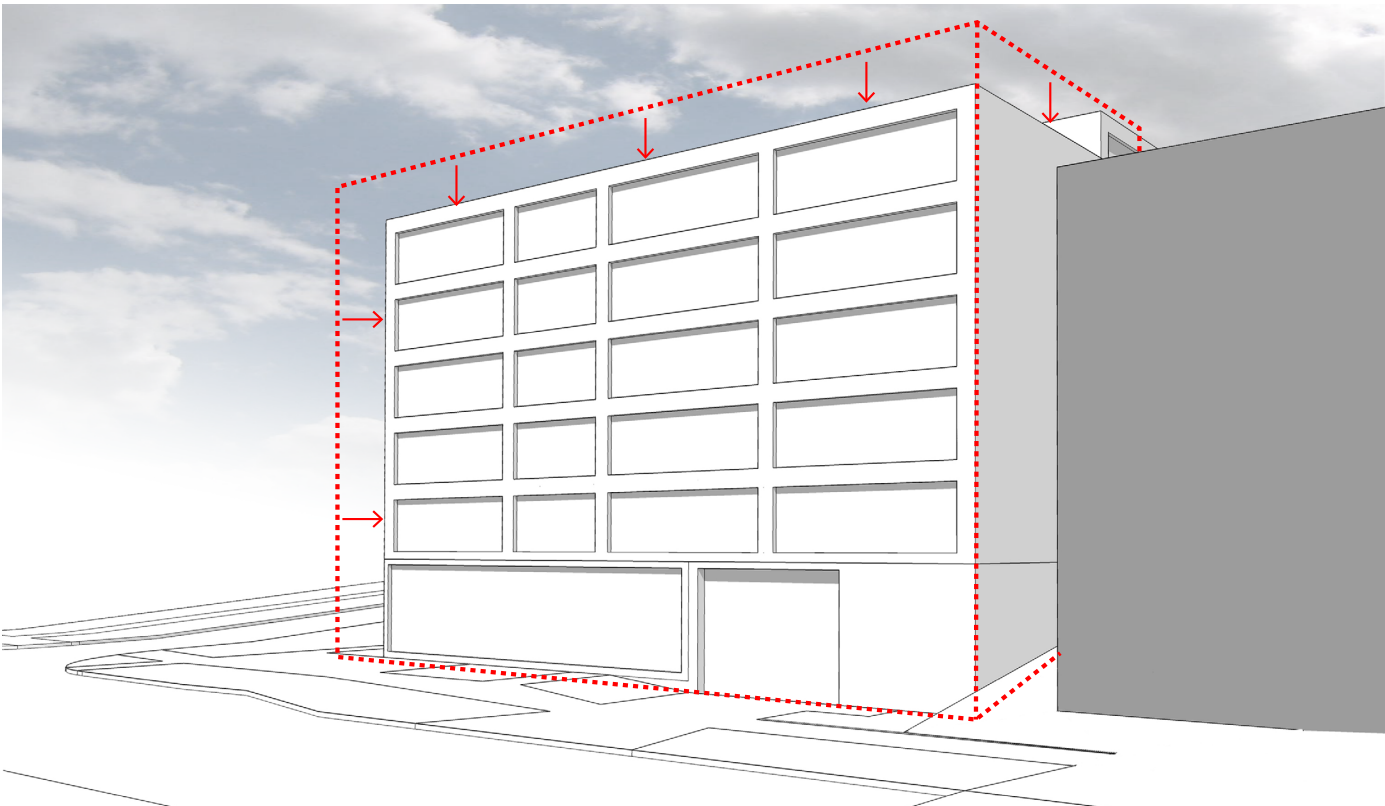
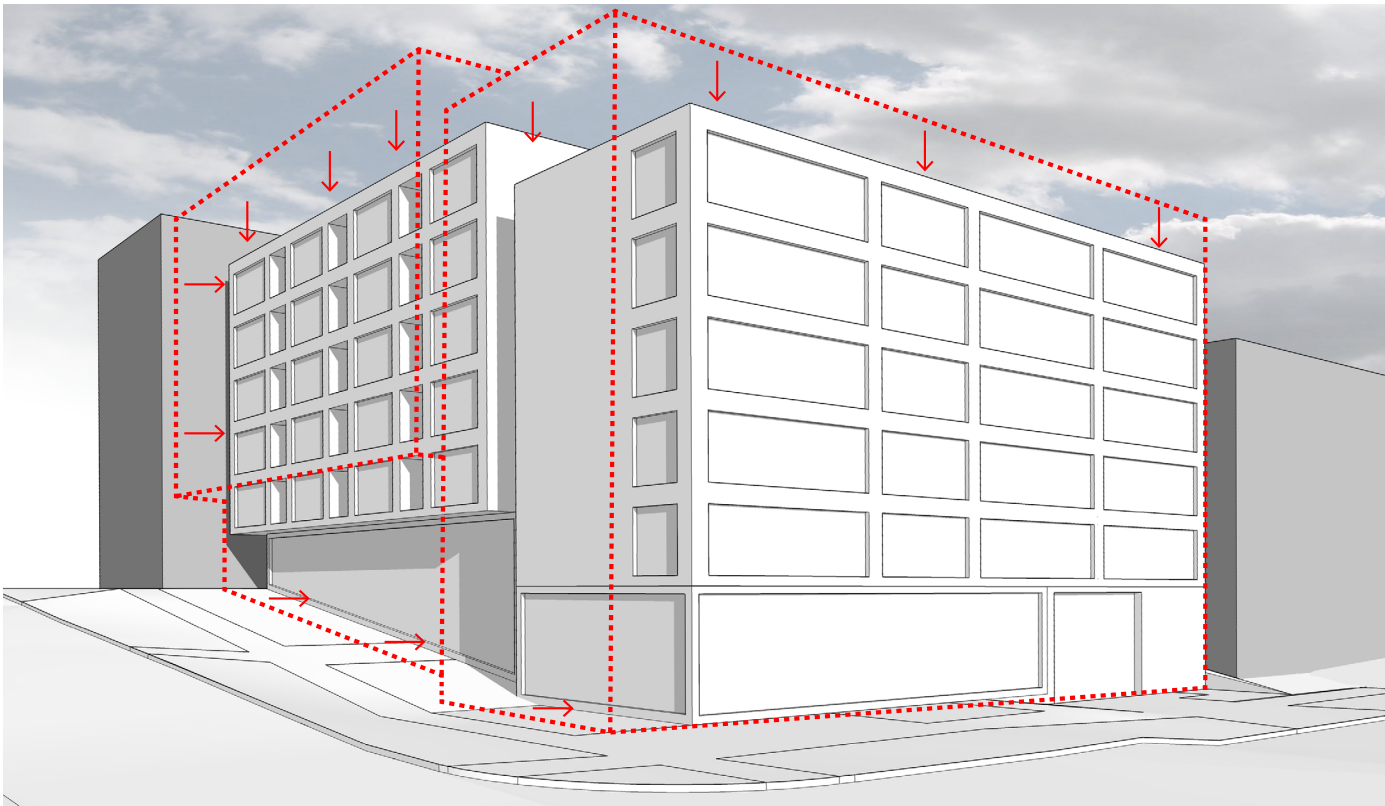


Retail Level Floor Plan (Level 2)



Typical Residential Floor Plan (Level 4)





The bullets points to the right summarize the guidance provided to the design team by the Board at the Early Design Guidance meeting on March 14, 2017. As the project design is described throughout the remainder of this packet, the guidance will be grouped as is relevant to the particular element or portion of the project being highlighted. These points will appear on the right side of each, two-page spread along with a brief description of how the design has been developed in response to the Board's guidance.

Through the design process, Alternate 3 has continued to be developed. The massing strategy and design concept have not been altered. However, the following modifications have been made:

- Reduced building size by one story
- Reduced Unit count from 79 to 67
- Provided a setback along S Bayview St

Summary

Stories:	7 (5-over-2)
Unit Count:	67
Floor Area:	49,100-SF Residential 1,850-SF Commercial 6,700-SF Parking 57,650-SF Total
Parking:	21 stalls

1

Massing and Context Response

The Board was generally supportive of the preferred option (Option 3) as they felt that it did a better job breaking up the building into large distinctive masses on a very steep building site. The Board did have concerns about the perceived scale of the proposal and lack of connection to the neighborhood.

- a. Board members asked if the applicant team had reviewed the neighboring property currently being considered for development and whether their analysis or development of the three massing options had been influenced by the adjacent proposal or if there were any significant takeaways or factors that may have contributed to the development of the three massing schemes. The applicant stated that they were interested in providing a clean and simple urban fabric building that provides a good backdrop to the neighborhood.
- b. Some board members were concerned that there might not be enough of a transition to the adjacent LR-3 zone to the east and south of the proposal site. Members felt that the options presented in the packet were massive and un-broken and not enough to a human scale and urged the applicant to take the scale of the buildings to the south and across street into consideration.
- c. Board members felt that while the preferred option showed two distinct masses, the preferred option lacked articulation, making it overly severe and austere which is not supported by the design guidelines.
- d. Echoing public comment, Board members felt that the proposal does not have enough of a design connection to Beacon Hill and the massing needed to be brought down to more of a human scale. The scale at present lacks compatibility with the Beacon Hill character which is at a more human scale.
- e. Agreeing with public comment, Board members noted that the project area is a corner site resembling a stand-alone visual gateway, while acting as a balancing element or 'end cap' to the structures on the opposing corners. Board members felt that the corner should be better emphasized while using better ground level treatments per the North Beacon Hill Guidelines.
- f. The Board asked the applicant to use more articulation, secondary architectural elements that better respond to the North Beacon Hill guidelines in order to break down the perception of mass of the building so the overall design relates better to the scale of the existing neighborhood context.

2

Architectural Character

- a. The Board asked how this proposal is unique to Beacon Hill and would like to see a design that employs materials, secondary features or other features that express how this development is suited specifically to Beacon Hill and not another location.
- b. The Board felt that the design does not pay attention to what makes a neighborhood unique culturally or architecturally and felt that the design isn't authentic to North Beacon Hill. Board members continued with assessment that the current early concept lacks consideration for the cultural heritage, community diversity, sense of place, or identity of the neighborhood.
- c. The Board requested that the applicant create more architectural character that is specific to Beacon Hill; whether it is in reference to the community that is there now, the history of the existing buildings on site, with finish materials that are on a human scale or other elements like lamp posts, signage, or other elements.

3

Commercial Zone

- a. The Board suggested that of the three options, the option that features the six-foot deep courtyard should be maximized to benefit both the residents and general public.
- b. The Board noted that while the area could accommodate the placement of tables and chairs, the area needs to be deeper than six feet as the current dimension creates a long and dark space.
- c. The Board also expressed some discomfort with the pedestrian experience along Bayview and into the commercial space which they felt needed still more of a human scale.
- d. The Board wanted the applicant to explore different ways of creating more of a human scale transition at the ground level for the commercial retail and residential lobby facades. Members suggested that this could include varying facade details, or creating differences in depth or setback.

4

Site Layout

Board members had questions about the landscape zone located on the second level and felt that the area could be geared more toward being a community amenity, to be shared by residents and wanted to know how the programming worked. During discussions, members requested further explanation as to how the children's play area interacts with the private amenity zone.



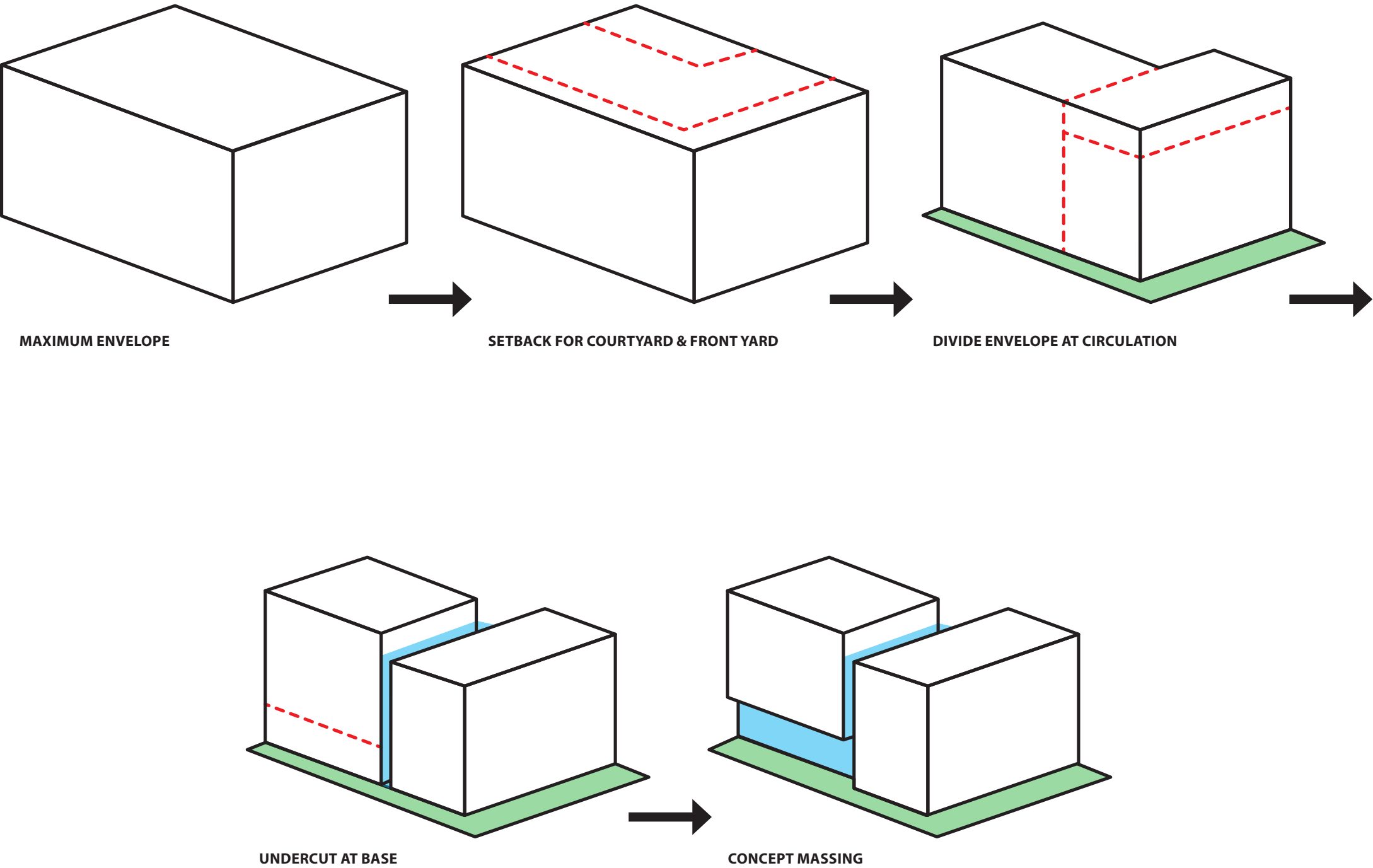
# ARCHITECTURAL CONCEPT

## Massing Design Concept

- The first move breaks the mass of the building in to three distinct pieces connected through glass “gaskets” (which provide light and views to the corridors).
- Separating the masses allows each street to be provided with an articulated facade appropriate to the zoning, context, and transitions.
- Additionally, the construction type, Type V over a Type I base, is expressed through a pedestrian level setback.
- This setback not only expresses the construction of the building, but provides an enhanced pedestrian experience and opportunity for ground level, public amenities.

## Design Approach

- The facade major elements will both protrude and inset - evocative of threading a weave
- The facade will shift - again taking cues from alternating threads weaving together
- The use of gradating color will provide a sense of movement - inspired by woven shades, future transit station, and adjacent playfield.
- Secondary rhythm and pattern will be used to further articulate the pattern and breakdown the scale of materials









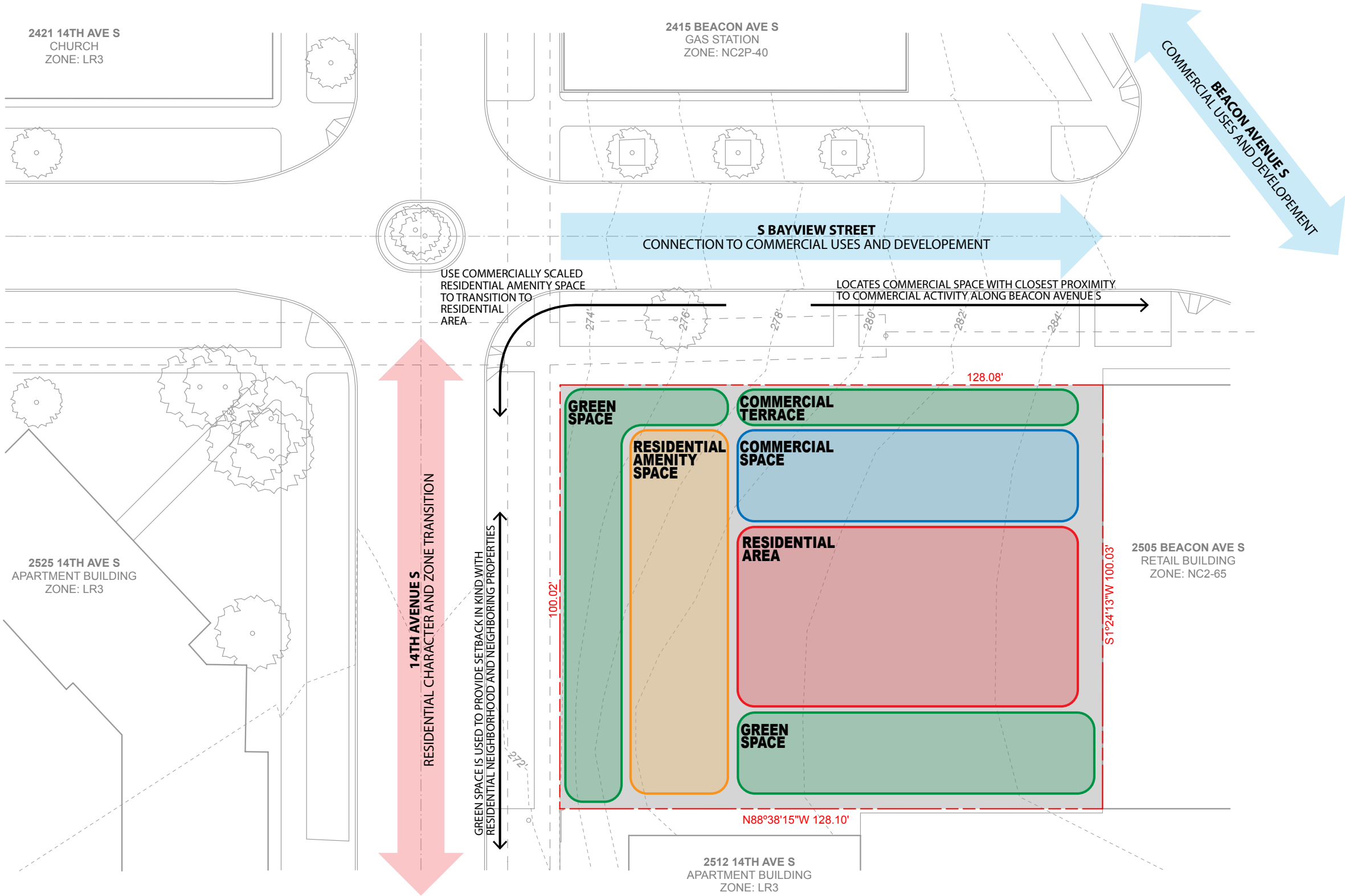
# GROUND FLOOR ANALYSIS

## Street Characteristics

- S Bayview Street  
Moderately trafficked street that provides access to commercial and transit to the east along S Beacon Avenue. Substantial grade change, sloping down to the west. Opportunities for views to the Olympic Mountains and Puget Sound.
- 14th Avenue S  
Residential character street with generous landscaping provided between buildings and right of way. Transition between neighborhood commercial zone and low rise zone. Future green way.

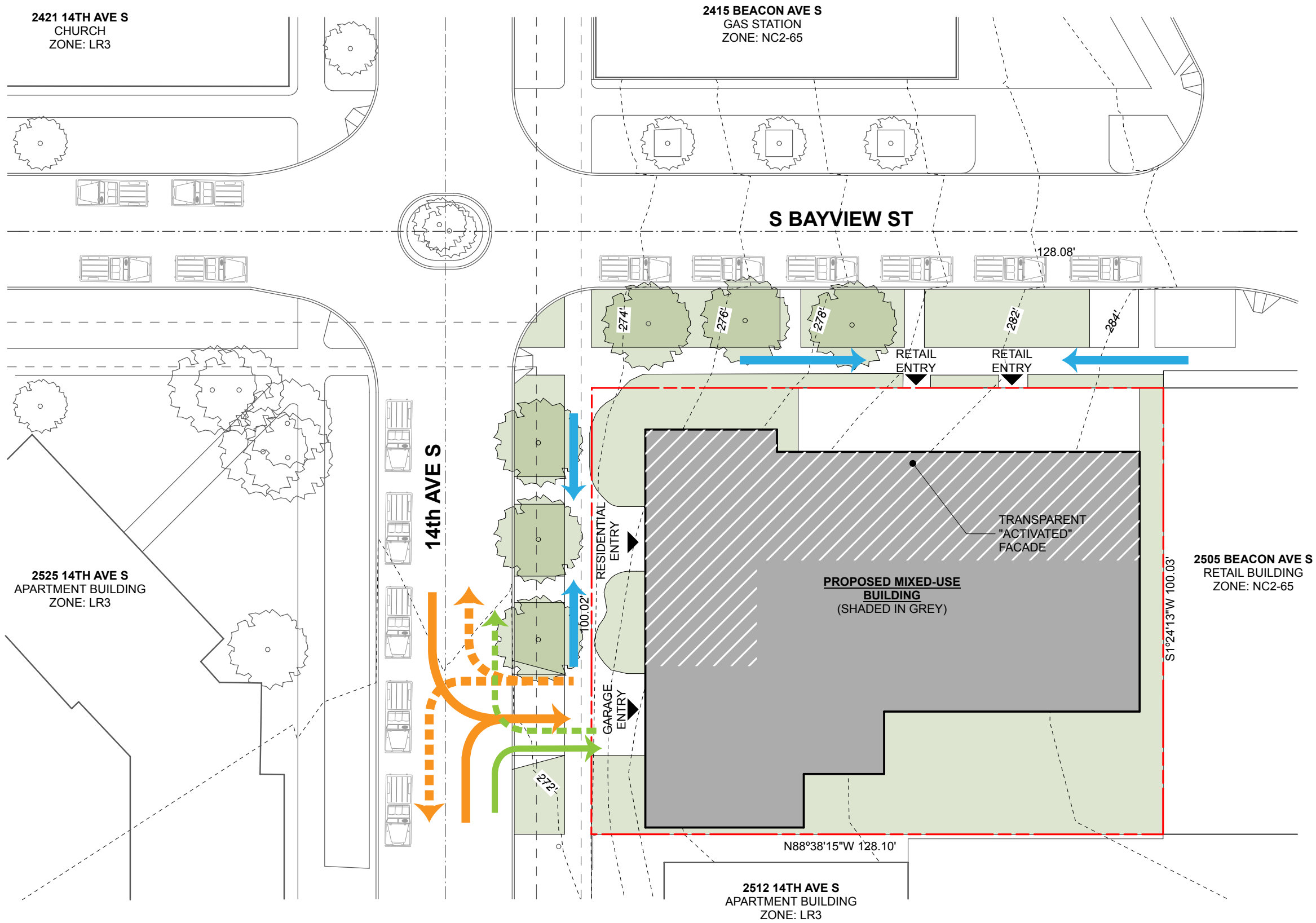
## Design Approach

- Make organizational gesture to the commercial activity along S Beacon Ave by pushing commercial shell to the northeast property line along S Bayview St.
- S Bayview St is the commercially viable and activated street but 14th Ave S is not currently viable.
- Residential amenity spaces engage the corners in an effort to transition to residential scale and character of 14th Ave S.
- Provide landscaped setback along 14th Ave S to fit the urban rhythm and pattern.





# PRELIMINARY SITE PLAN



The proposed project includes the demolition of the existing fourplexes and the associated site improvements (driveways, walkways, patios, etc.) to be replaced by a 7-story mixed-use apartment building containing approximately 67 residential units and associated amenity areas, 1,850-sf of commercial, and one below-grade parking level with parking for approximately 21 vehicles and building support spaces necessary for the proper and successful function of the building.

## Traffic and Circulation

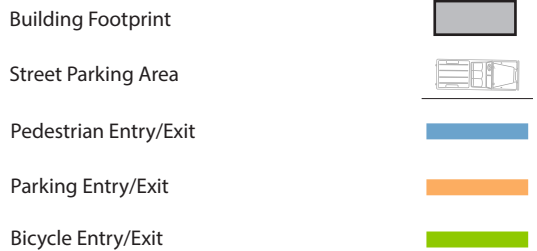
- Both S Bayview St and 14th Ave S are residential streets with parking on both sides

## Streetscape

- 14th Ave S has a residential character and scale
- Street parking on all streets surrounding project site
- S Bayview St quickly transitions to residential character and scale west of 14th Ave S

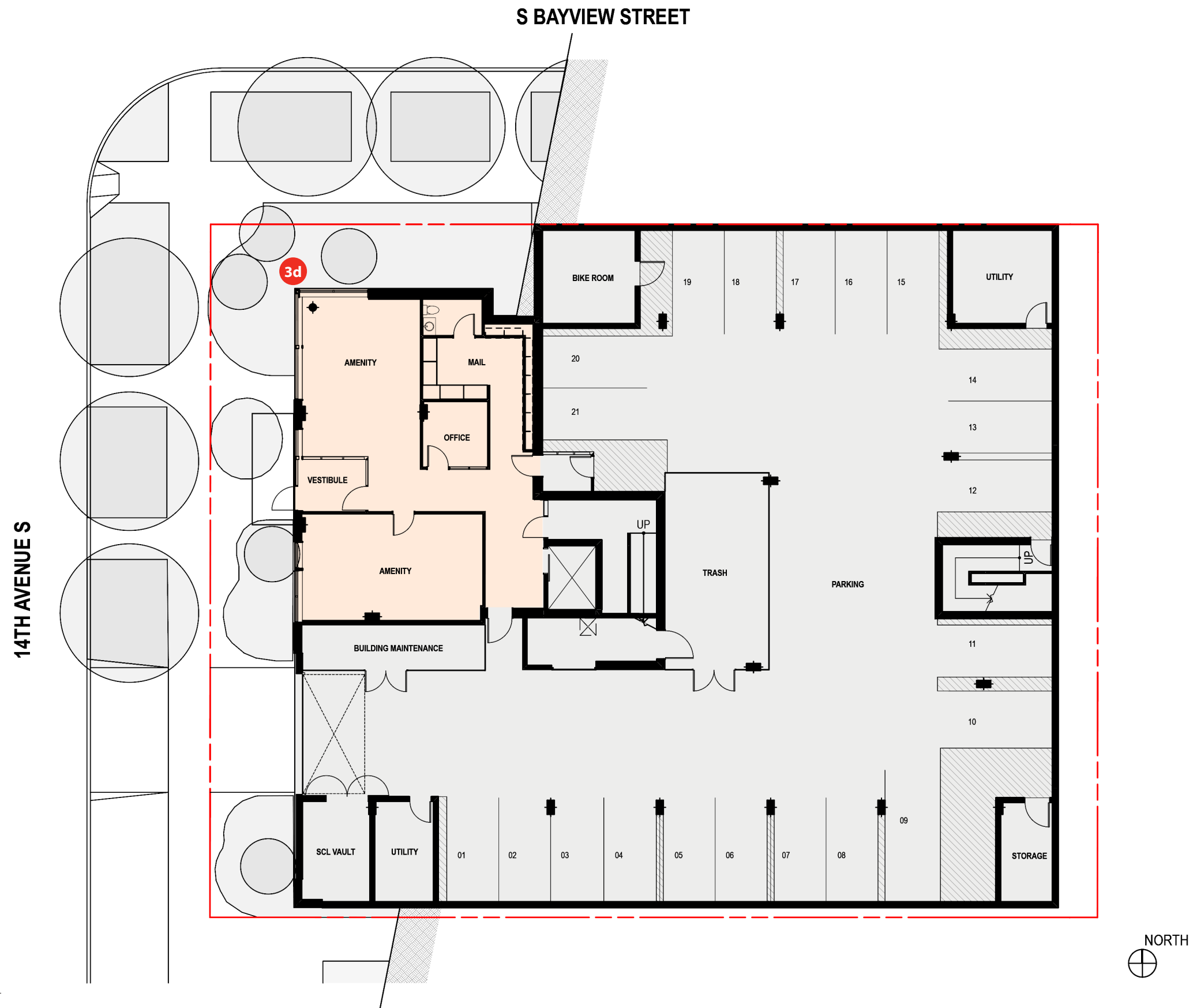
## Neighborhood Influences

- Transit oriented development to the east
- Retail to the east
- Residential character to the west and south





STREET LEVEL FLOOR PLAN

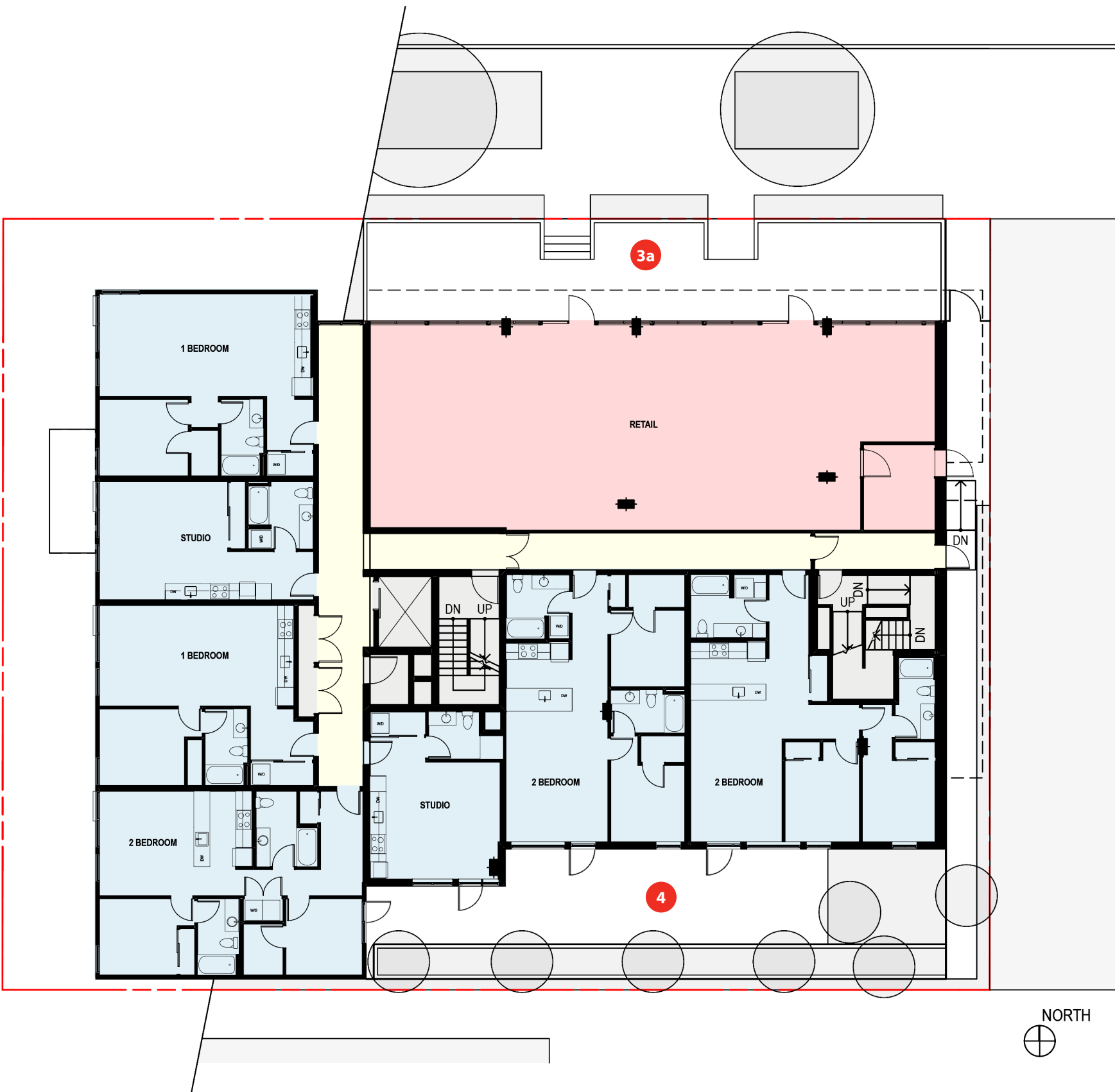




LEVEL 2 FLOOR PLAN

14TH AVENUE S

S BAYVIEW STREET



Commercial Zone

- 3a** The retail plaza has been expanded from the previous 6-foot depth to approximately 13-feet to better accommodate outdoor, retail activity.
- 3d** The mountain-to-island landscape strategy assists in the street-level transition from commercial to residential. The landscaping is taller, denser, and heavier - representing the mountain - adjacent to the retail area. The landscaping scale and texture transitions in scale and texture as it moves down the hill to the residential program on 14th Ave S.

Site Layout

- 4** The units along the south courtyard have been designed 2-bedroom units with small families in mind but as a result of the expanded retail plaza, the south courtyard has been reduced in size. Each unit is still provided access to a private patio space and plantings. The bioretention planter along the south property line mitigates stormwater but also provides privacy and security to the residential units.

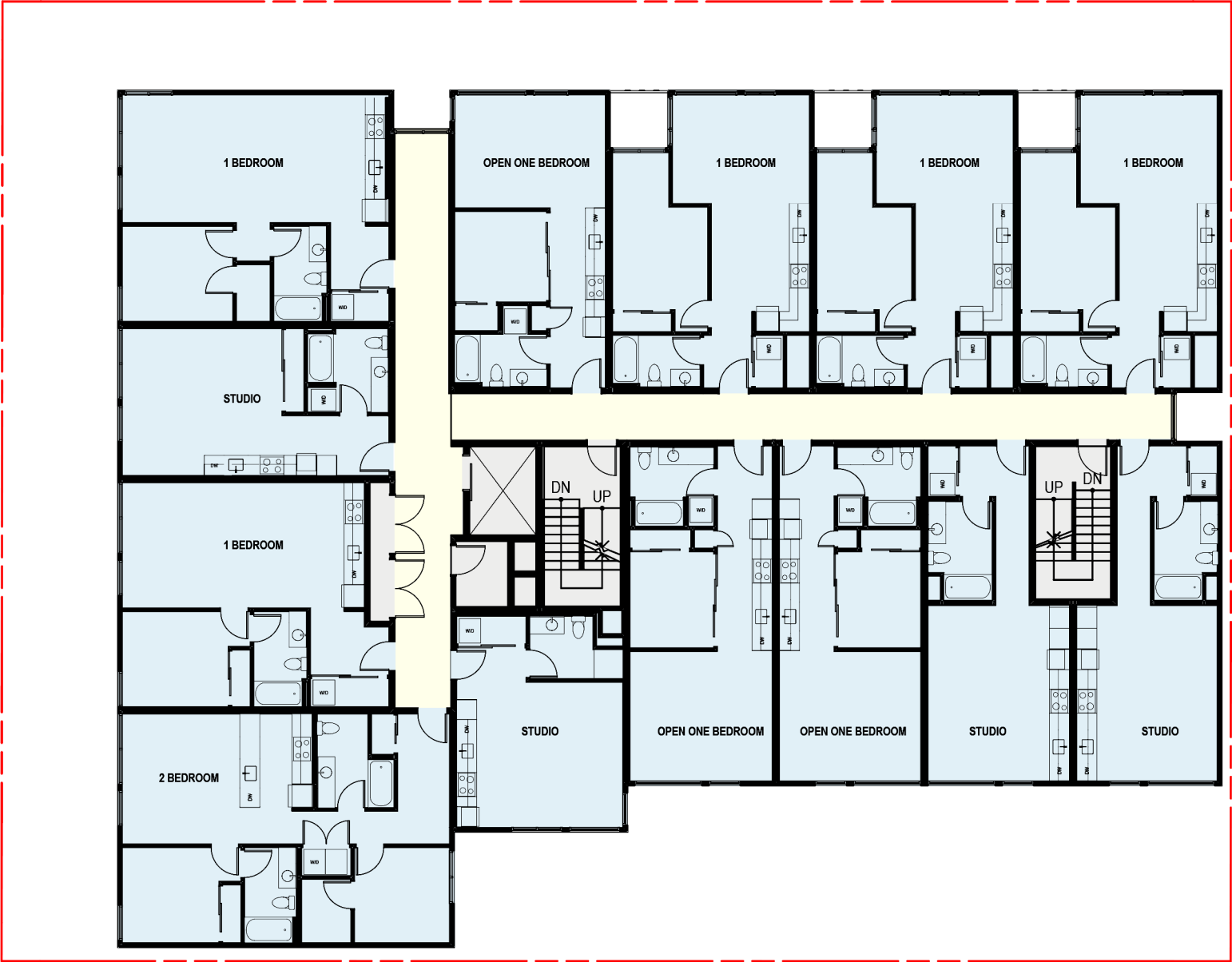
- CS2 A1** Sense of Place
- PL3 I-i** Human Activity
- PL3 I-ii** Human Activity
- PL3 I-iii** Human Activity
- PL3 I-iv** Human Activity
- PL3 I-v** Human Activity
- PL3 II-i** Streetscape Compatibility



TYPICAL RESIDENTIAL LEVEL FLOOR PLAN

S BAYVIEW STREET

14TH AVENUE S





S BAYVIEW STREET

14TH AVENUE S

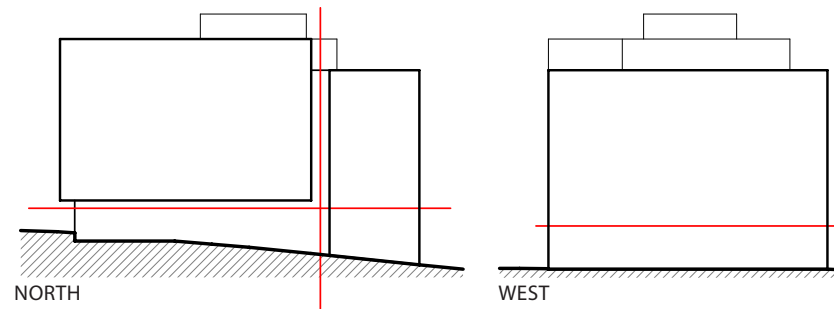




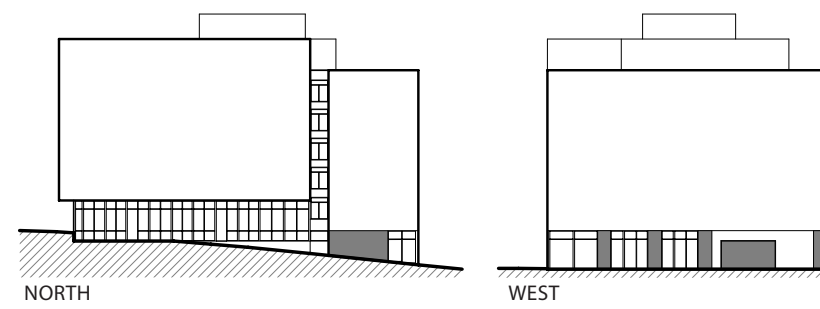
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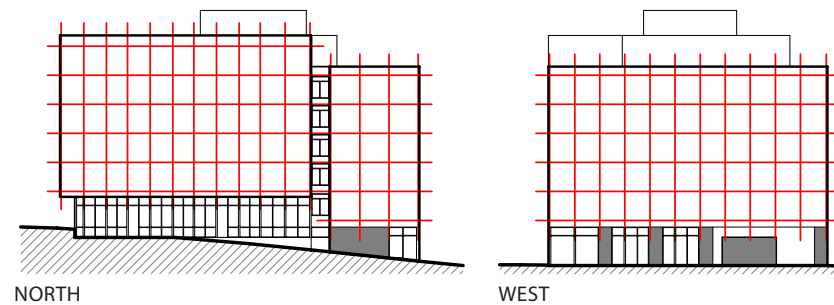
# FACADE DESIGN CONCEPT



**MASSING STRATEGY**



**GLAZED GASKET AND BASE**



**ORGANIZATIONAL GRID**



**ORGANIZATIONAL GRID**



**FENESTRATION RHYTHM/PATTERN**



**SECONDARY FACADE TEXTURE**



**FACADE RELIEF**

## Facade Design Concept

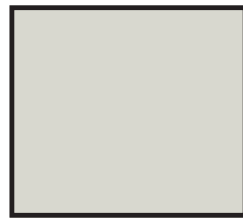
- Developing an intentionally subdued yet actively scaled facade is a project goal.
- Combining contextual influence with abstract inspiration, the facade articulation takes cues from the neighborhood context. This includes:
  - The facade is divided with a base and top that treats each with distinct materials and separates with shadow lines.
  - Utilizing materials inspired by both the historic and contemporary buildings in the neighborhood
  - Proportioning the facade into a clear grid that reduces the scale and provides rhythmic patterning
- Secondary elements are carefully located on the facade to provide additional scale, texture, and depth.
- Joints, material transitions, changes in plane, and edges are carefully aligned to maintain the elevation organization and concept.



## NORTH ELEVATION (S BAYVIEW STREET)



VINYL WINDOW  
White



FIBER CEMENT BOARD  
Paint; Light Grey



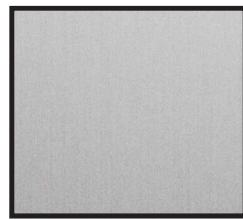
WOOD SIDING  
Stain; Charcoal



VINYL PATIO DOOR  
White



BRICK SIDING  
Smooth Face; Light Grey



ALUMINUM STOREFRONT  
Clear Anodized



MDO PANEL  
Paint; Semi-Gloss White





WEST ELEVATION (14TH AVE S)



Massing and Context Response

**1b** In consideration of the zone and scale transition to the south and east, this proposal has reduced the actual and perceived scale. The building has been reduced in height by 1-story. In addition to a reduced mass, the project thoughtfully scales the facade. Dividing the facade into a base and top reduces the perception of height and mass. Organizing the facade into a grid with material transitions reduces the perceived scale. Adding secondary elements like wood balconettes and an entry canopy provides human scaled texture and shadow lines.

**1c** he proposed design has been further articulated with a clear facade organization, material application, and secondary elements.

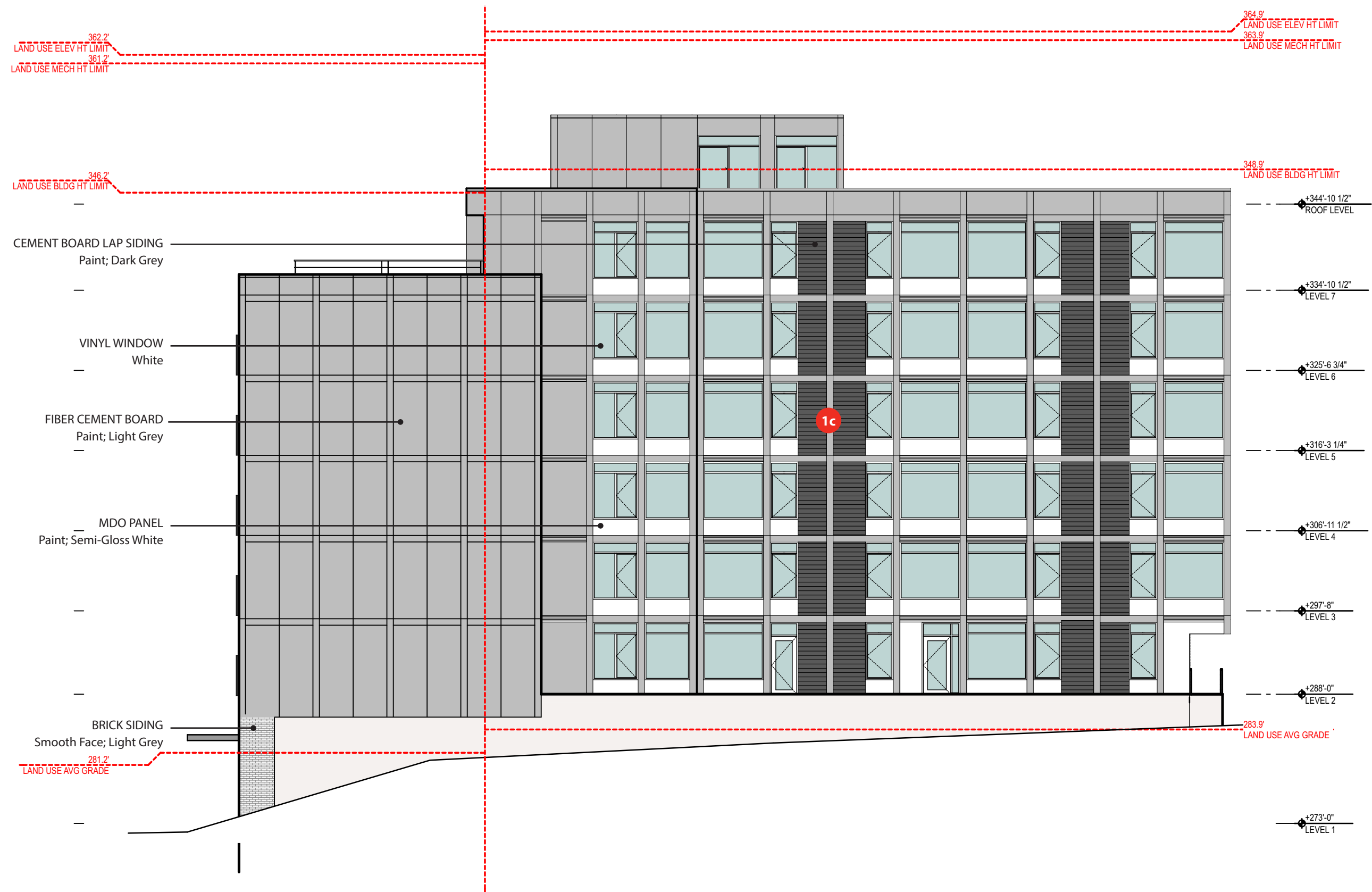
Commercial Zone

**3c** The scale of the retail plaza and storefront has been reduced. The storefront height has been reduced by several feet. The retail edge of the retail plaza interfacing with the sidewalk is buffered by landscaping and protected by a transparent railing.

- CS2 III-i Height, Bulk, and Scale Compatibility
- CS2 III-ii Height, Bulk, and Scale Compatibility
- CS2 III-iii Height, Bulk, and Scale Compatibility
- CS2 III-vi Height, Bulk, and Scale Compatibility
- CS2 III-vii Height, Bulk, and Scale Compatibility
- CS2 III-viii Height, Bulk, and Scale Compatibility
- CS2 III-ix Height, Bulk, and Scale Compatibility
- CS2 III-x Height, Bulk, and Scale Compatibility
- PL3 A2 Ensemble of Element
- DC2 D1 Human Scale
- DC2 D2 Texture
- DC4 I-i Exterior Finish Materials
- DC2 A1 Exterior Finish Materials

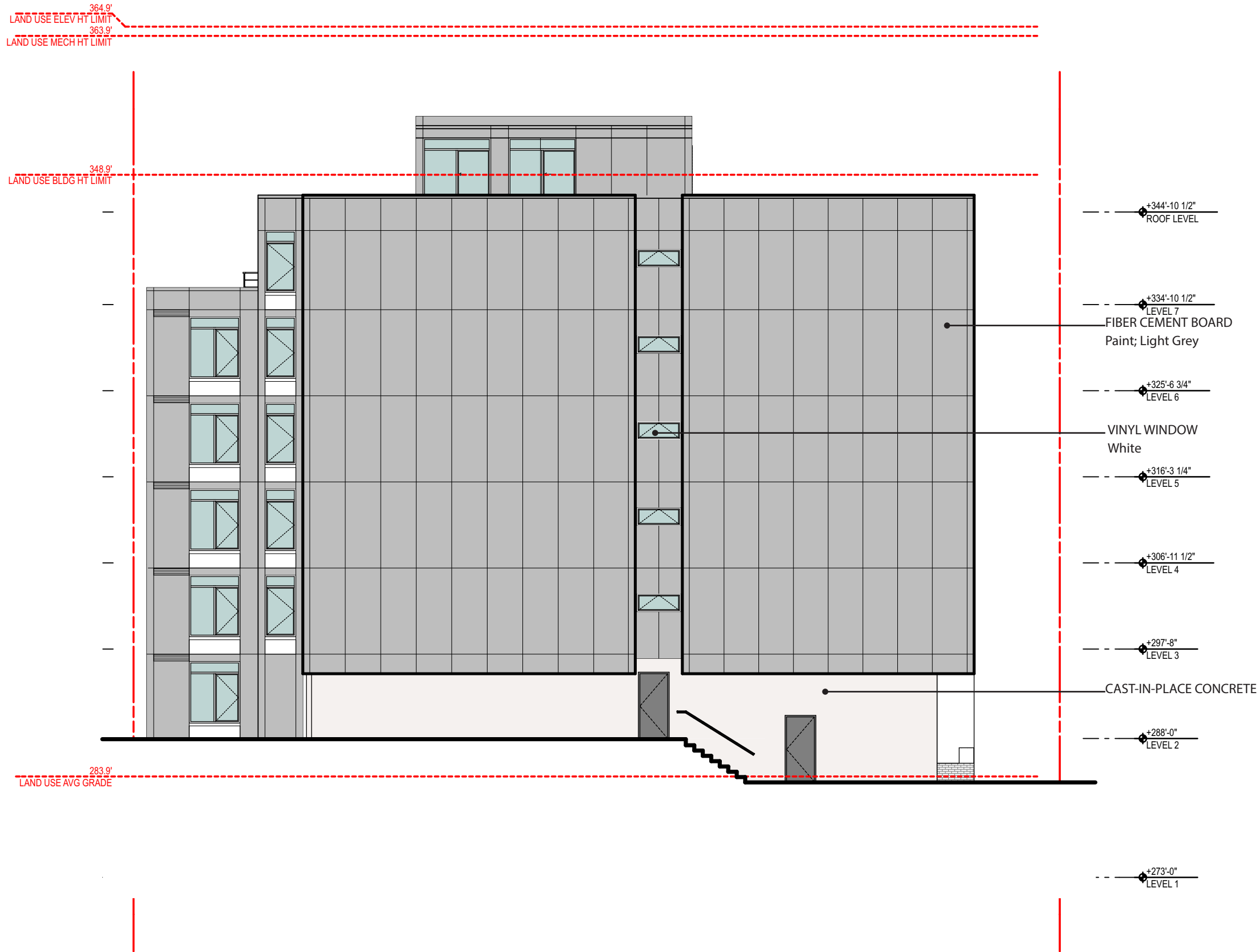


SOUTH ELEVATION





EAST ELEVATION



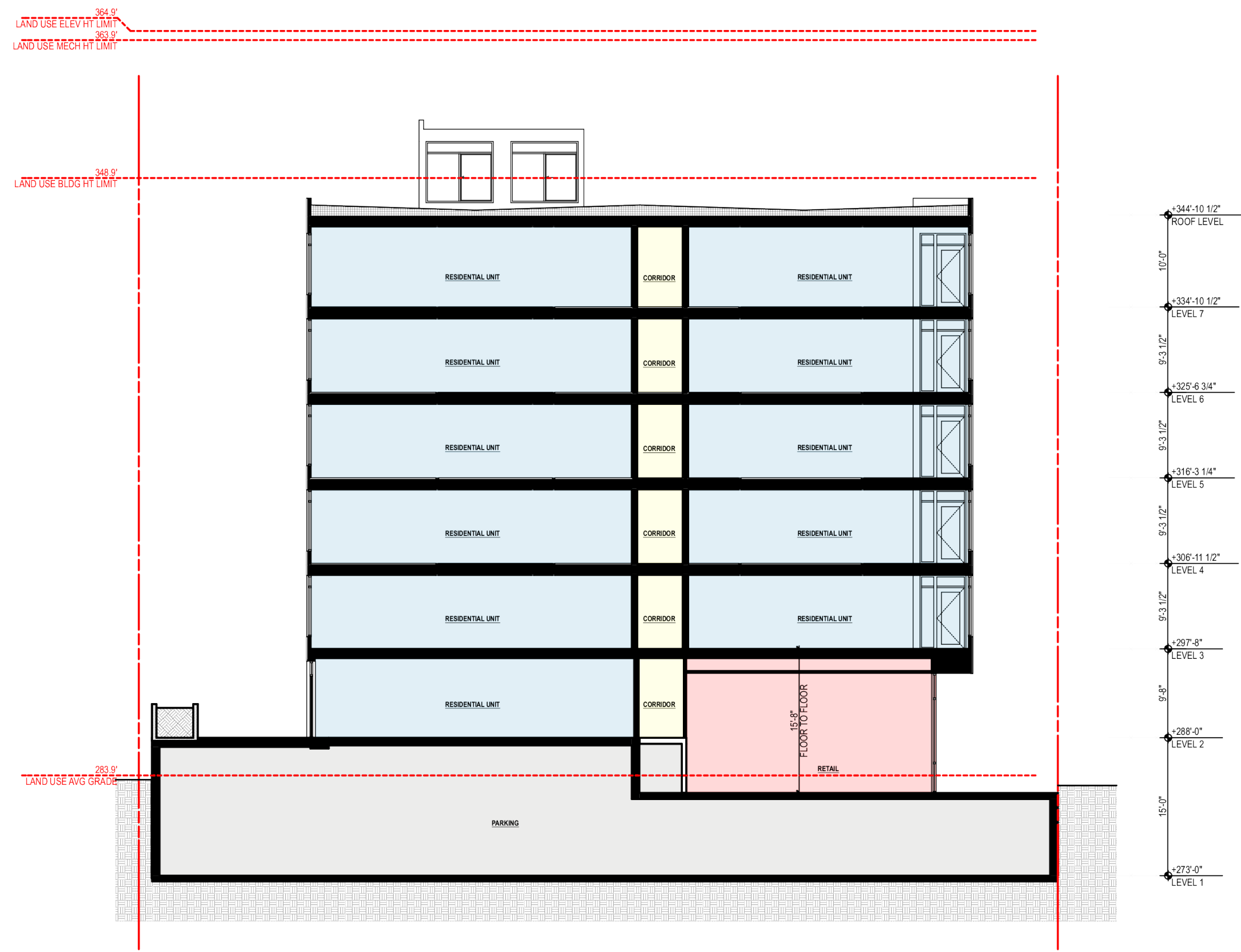
Massing and Context Response

**1c** he proposed design has been further articulated with a clear facade organization, material application, and secondary elements.

- CS2 III-i** Height, Bulk, and Scale Compatibility
- CS2 III-ii** Height, Bulk, and Scale Compatibility
- CS2 III-iii** Height, Bulk, and Scale Compatibility
- CS2 III-vi** Height, Bulk, and Scale Compatibility
- CS2 III-vii** Height, Bulk, and Scale Compatibility
- CS2 III-viii** Height, Bulk, and Scale Compatibility
- CS2 III-ix** Height, Bulk, and Scale Compatibility
- CS2 III-x** Height, Bulk, and Scale Compatibility
- PL3 A2** Ensemble of Element
- DC2 D1** Human Scale
- DC2 D2** Texture
- DC4 I-i** Exterior Finish Materials
- DC2 A1** Exterior Finish Materials

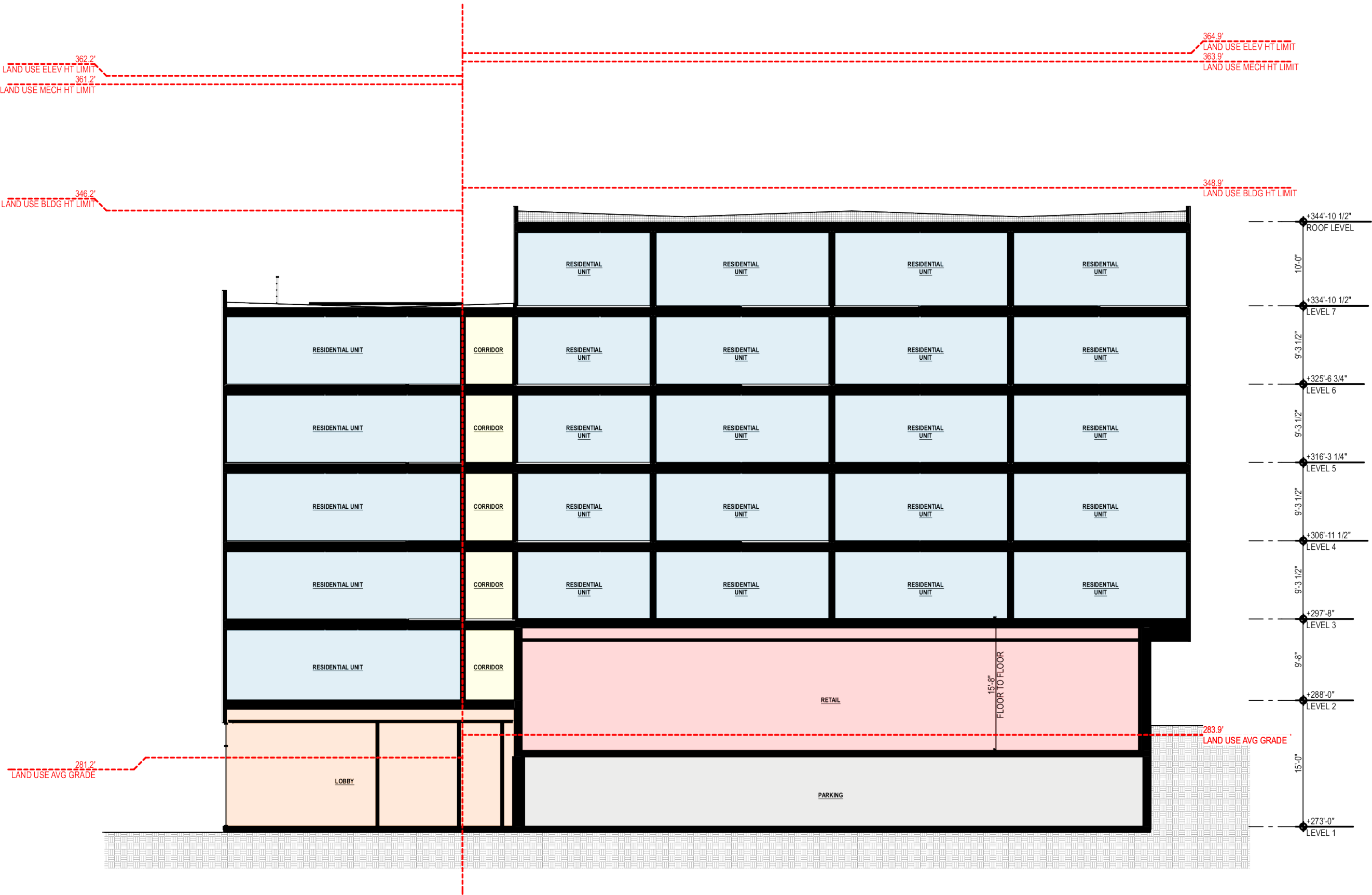


BUILDING SECTIONS NORTH/SOUTH





BUILDING SECTION EAST/WEST





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#### Massing and Context Response

**1a** The proposed design represents a contextual, urban fabric building that makes the transition from the commercial, larger-scale character of Beacon Ave S to the residential character of 14th Ave S. This project is responding to future, adjacent development by bridging between the two different zones.

#### Architectural Character

**2** Three elements distinct to Beacon Hill have influenced the design: landscape design, material usage & building scale, and programmatic use & scale. The landscape design has been developed with inspiration from the local gardens and green spaces. The design concept uses Japanese inspired garden design as a starting point to develop a concept of mountain-to-island representation. Materials have been selected based on historic and contemporary Beacon Hill buildings and includes brick, wood, glass, and some cement board. Their use is incorporated in to the facade in to reduce the scale. Dividing the facade into a base and top reduces the perception of height and mass. Organizing the facade in to a grid with material transitions reduces the perceived scale. Adding secondary elements like wood balconettes and an entry canopy provides human scaled texture and shadow lines. The retail space is located and scaled to support small tenants and activate the streetscape.

- CS2 III-i** Height, Bulk, and Scale Compatibility
- CS2 III-ii** Height, Bulk, and Scale Compatibility
- CS2 III-v** Height, Bulk, and Scale Compatibility
- CS2 III-vi** Height, Bulk, and Scale Compatibility
- CS3 A1** Fitting Old and New Together
- PL3 II-i** Streetscape Compatibility
- DC2 D1** Human Scale
- DC2 D2** Texture
- DC4 I-i** Exterior Finish Materials
- DC2 A1** Exterior Finish Materials



PEDESTRIAN VIEW LOOKING SOUTH ALONG 14TH AVE S

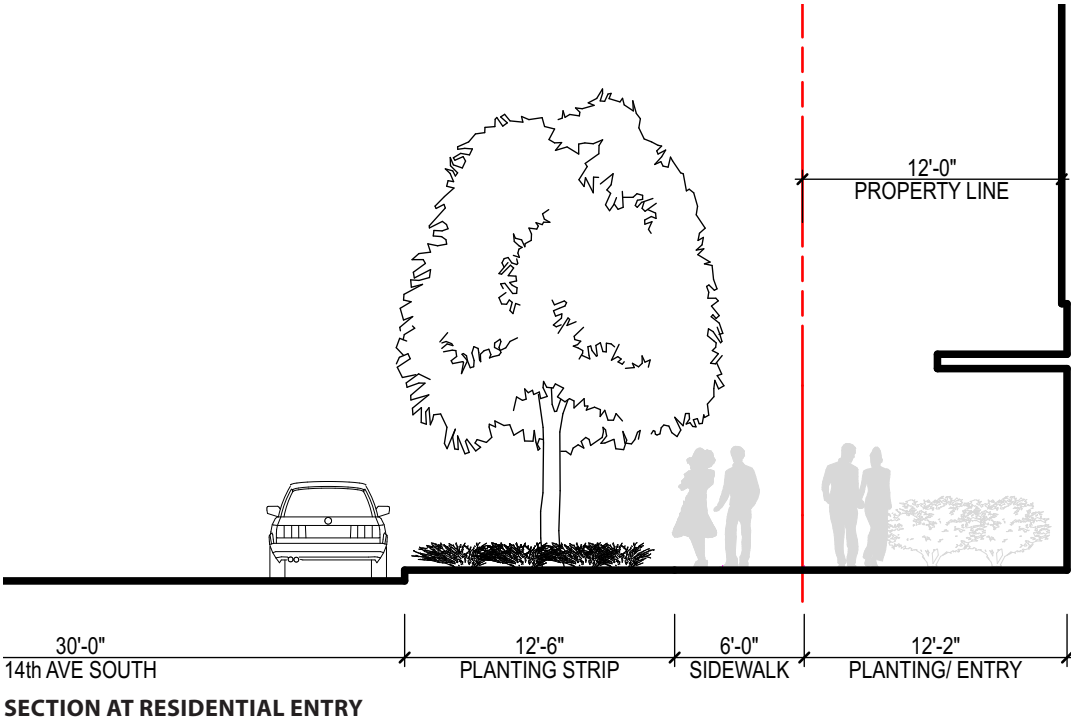
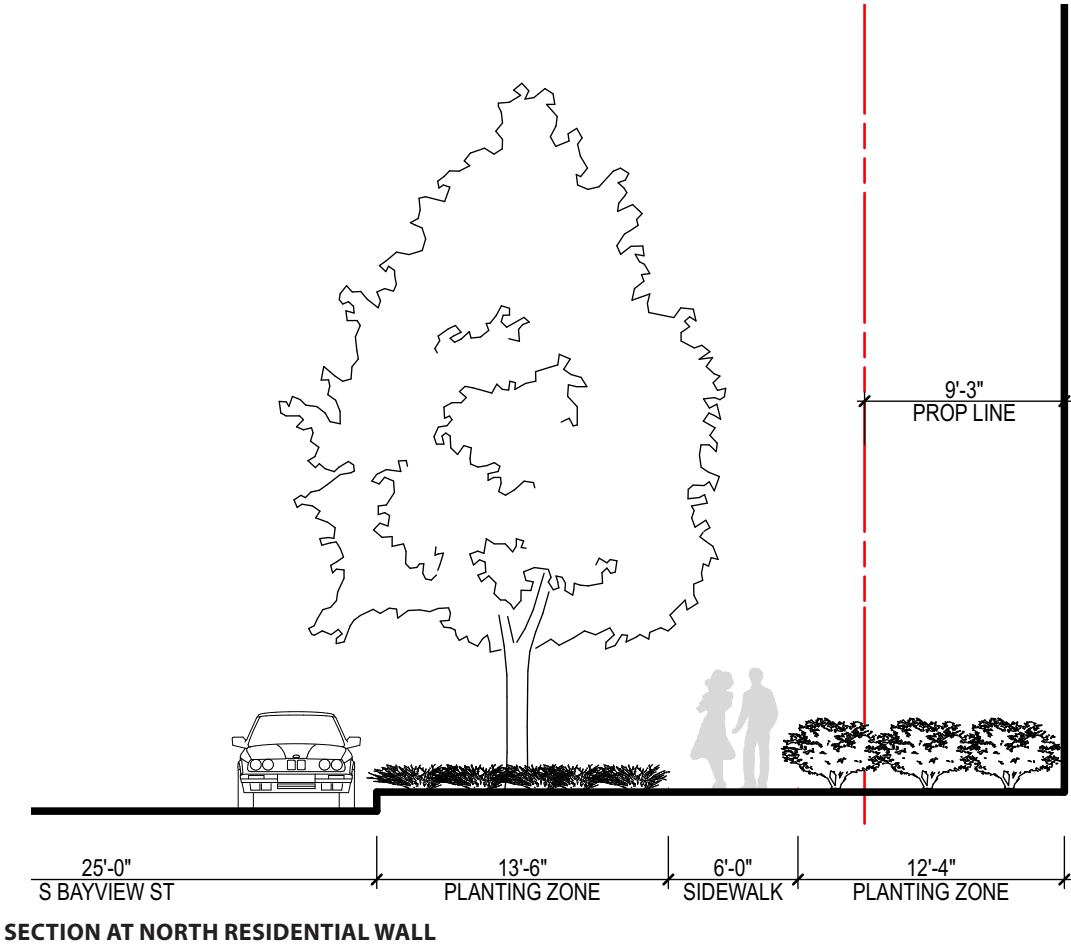




ENLARGED FLOOR PLAN AND WALL SECTIONS AT NW ENTRANCE PLAZA



ENLARGED FLOOR PLAN AT NW ENTRANCE PLAZE



- Massing and Context Response**
- 1d** The three distinctly Beacon Hill design influences, landscape design, material usage & scale, and program, have been articulated at the streetscape level. Brick paired with transparent glass occupies the base of the building for it's tactility and scale. Landscape design is used to provide a curated interface with pedestrians.
  - 1e** Emphasis on the corner site is articulated in the landscaping in the setback areas. Providing the mountain-to-island landscape design concept as the ground level provides visual interest, contextually fit with the neighborhood, and provides informal seating.
- Commercial Zone**
- 3d** The mountain-to-island landscape strategy assists in the street-level transition from commercial to residential. The landscaping is taller, denser, and heavier - representing the mountain - adjacent to the retail area. The landscaping scale and texture transitions in scale and texture as it moves down the hill to the residential program on 14th Ave S.

- CS2 III-i** Height, Bulk, and Scale Compatibility
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- CS2 III-vi** Height, Bulk, and Scale Compatibility
- CS3 A1** Fitting Old and New Together
- PL3 II-i** Streetscape Compatibility
- DC2 D1** Human Scale
- DC2 D2** Texture
- DC4 I-i** Exterior Finish Materials
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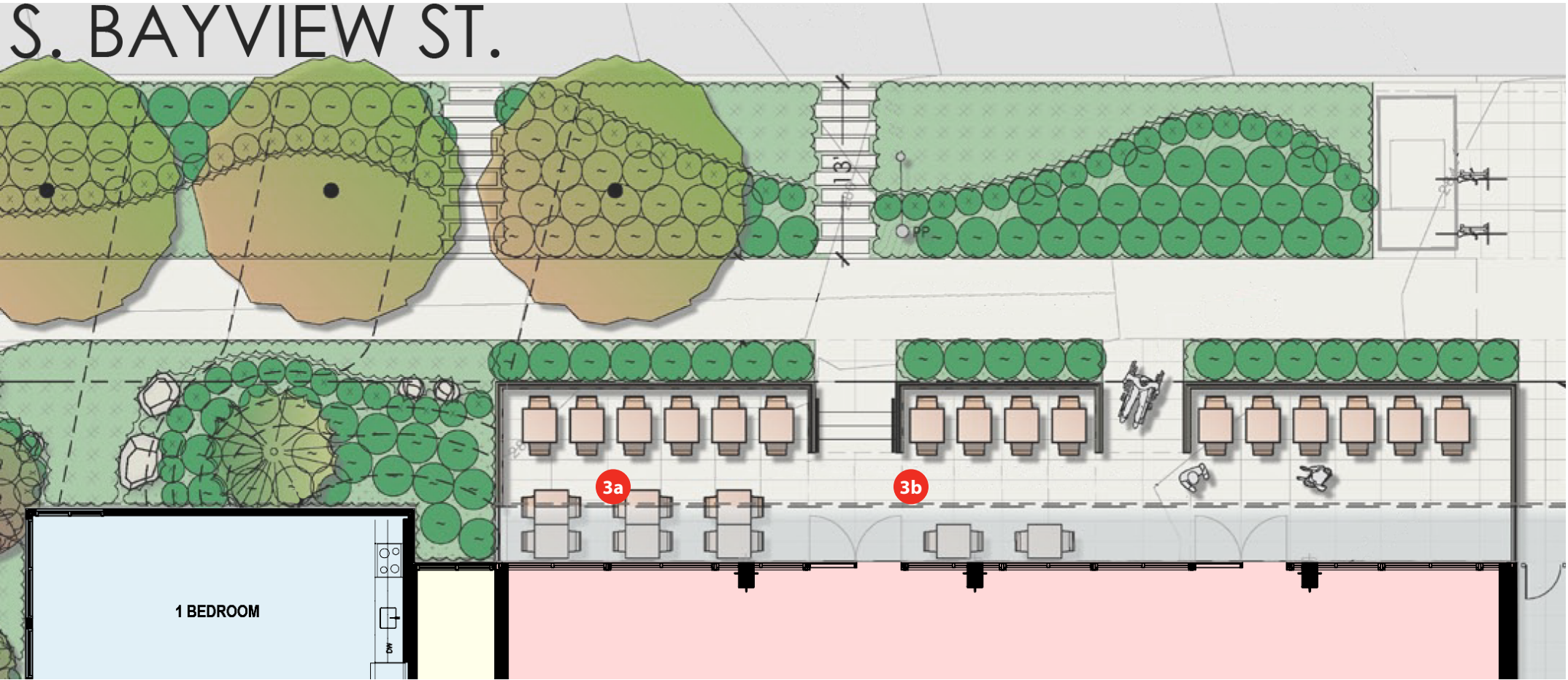


PEDESTRIAN VIEW LOOKING AT RETAIL ALONG S BAYVIEW ST

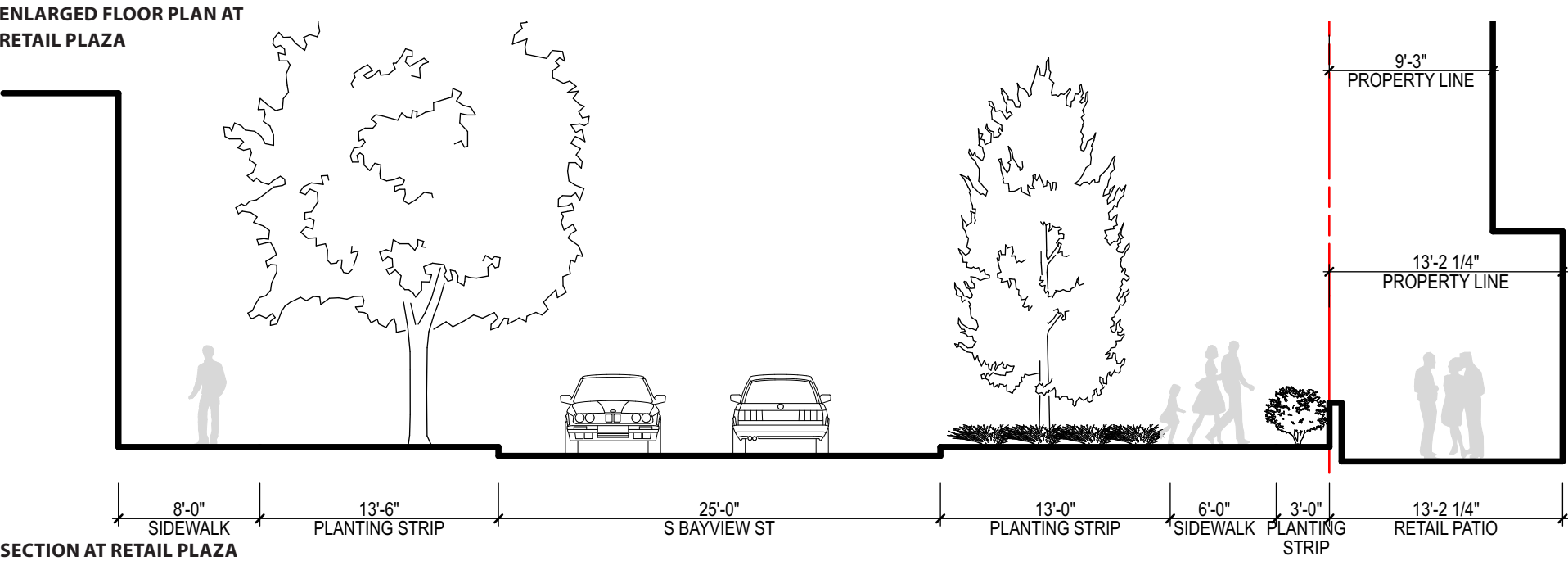




ENLARGED RETAIL FLOOR PLAN



- Commercial Zone**
- 3a** The retail plaza has been expanded from the previous 6-foot depth to approximately 13-feet to better accommodate outdoor, retail activity.
  - 3b** Providing a 13-foot retail plaza allows for two rows of cafe tables and chairs along with circulation space.
  - 3c** The scale of the retail plaza and storefront has been reduced. The storefront height has been reduced by several feet. The retail edge of the retail plaza interfacing with the sidewalk is buffered by landscaping and protected by a transparent railing.



- PL3 A2 Ensemble of Element
- PL3 I-i Human Activity
- PL3 I-ii Human Activity
- PL3 I-iii Human Activity
- PL3 I-iv Human Activity
- PL3 I-v Human Activity
- PL3 II-i Streetscape Compatibility
- DC2 D1 Human Scale
- DC2 D2 Texture

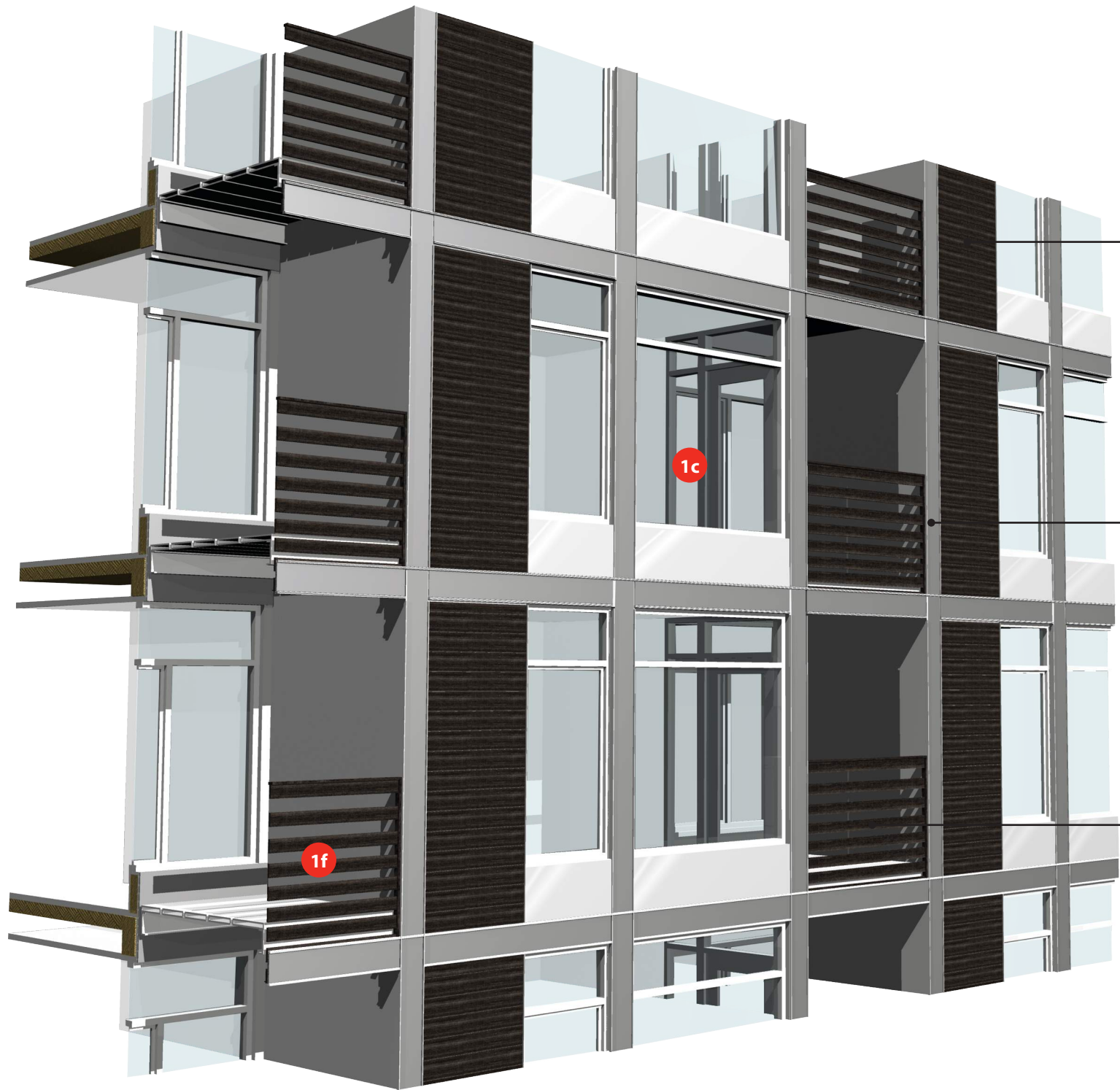


VIEW LOOKING SOUTH FROM BEACON AVE S





CONSTRUCTION VIGNETTE AT NORTH FACADE



**DC2 D1** Human Scale  
Materials with small scale texture are used to reduced the perceived scale.

**PL3 A2** Ensemble of Element  
Organizing the facade with grid unifies the materials and secondary elements together.

**DC2 D2** Texture  
Wood guardrails and inset balconies provide texture through material articulation and shadows.

Massing and Context Response

- 1b** In consideration of the zone and scale transition to the south and east, this proposal has reduced the actual and perceived scale. The building has been reduced in height by 1-story. In addition to a reduced mass, the project thoughtfully scales the facade. Dividing the facade into a base and top reduces the perception of height and mass. Organizing the facade in to a grid with material transitions reduces the perceived scale. Adding secondary elements like wood balconettes and an entry canopy provides human scaled texture and shadow lines.
- 1c** The proposed design has been further articulated with a clear facade organization, material application, and secondary elements.
- 1f** The project thoughtfully scales the facade. Dividing the facade into a base and top reduces the perception of height and mass. Organizing the facade in to a grid with material transitions reduces the perceived scale. Adding secondary elements like wood balconettes, inset balconies, and an entry canopy provides human scaled texture and shadow lines.

- CS2 III-i** Height, Bulk, and Scale Compatibility
- CS2 III-ii** Height, Bulk, and Scale Compatibility
- CS2 III-v** Height, Bulk, and Scale Compatibility
- CS2 III-vi** Height, Bulk, and Scale Compatibility
- CS3 A1** Fitting Old and New Together
- PL3 II-i** Streetscape Compatibility
- DC2 D1** Human Scale
- DC2 D2** Texture
- DC4 I-i** Exterior Finish Materials
- DC2 A1** Exterior Finish Materials



BIRD'S EYE PERSPECTIVE





CONSTRUCTION VIGNETTE AT WEST FACADE



DC2  
D1 Human Scale  
Materials with small scale  
texture are used to reduced the  
perceived scale.

PL3  
A2 Ensemble of Element  
Organizing the facade with  
grid unifies the materials and  
secondary elements together.

DC2  
D2 Texture  
Wood balconettes provide  
texture through material  
articulation and shadows.

Massing and Context Response

1a The proposed design represents a contextual, urban fabric building that makes the transition from the commercial, larger-scale character of Beacon Ave S to the residential character of 14th Ave S. This project is responding to future, adjacent development by bridging between the two different zones.

Architectural Character

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CS2  
III-i Height, Bulk, and Scale Compatibility

CS2  
III-ii Height, Bulk, and Scale Compatibility

CS2  
III-ix Height, Bulk, and Scale Compatibility

CS2  
III-x Height, Bulk, and Scale Compatibility

CS2  
III-xi Height, Bulk, and Scale Compatibility

DC2  
D1 Human Scale

DC2  
D2 Texture

DC4  
I-i Exterior Finish Materials

DC2  
A1 Exterior Finish Materials



STREET AND PODIUM LEVEL LANDSCAPE PLAN



Zen Entry - Ryoanji



Stone Seats



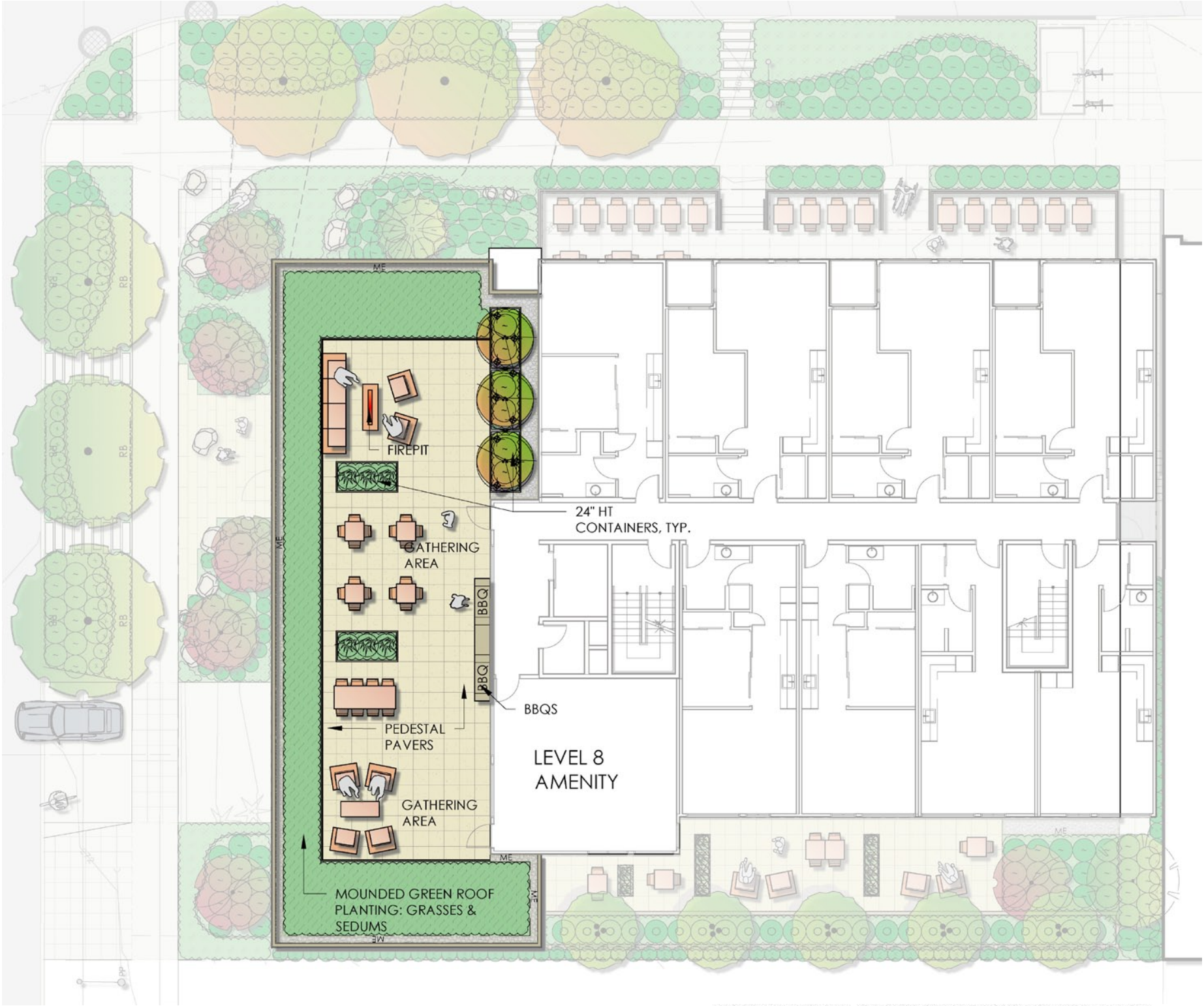
Pine Tree



Plank Pavers



ROOF PLAN



PAVERS ON PEDESTAL, 24"x24"x2" THICK PRECAST CONCRETE PAVERS, TEXADA HYDRAPRESSED SLABS, COLOR: NATURAL, AVAILABLE FROM ABBOTSFORD CONCRETE PRODUCTS, 1-800-663-4091. PEDESTALS PER ARCH.

FEATURE PAVERS  
12" X 48" TECTURA DESIGNS PLANK STYLE CONCRETE PAVERS, FINISH & COLOR TBD

0 8 16 ^  
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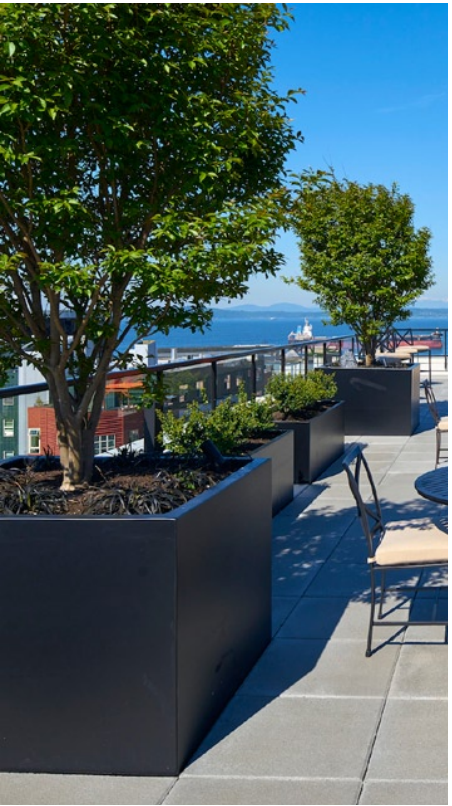
fire and view and sun



bbq



Sedums with Grasses

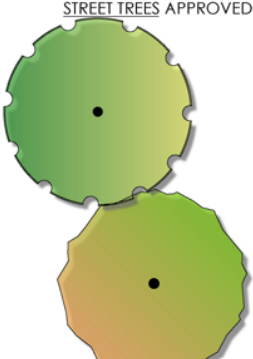


Trees in Pots



PLANTS

STREET TREES APPROVED VIA EMAIL BY BEN ROBERTYS OF SDOT URBAN FORESTRY 4/28/2017



TREES

PRUNUS X YEDOENSIS 'AKEBONO'	AKEBONO FLOWERING CHERRY
CARPINUS JAPONICUS	JAPANESE HORNBEAM
ACER CIRCINATUM	VINE MAPLE
CORNUS 'EDDIE'S WHITE WONDER'	EDDIE'S WHITE WONDER FLOWERING DOGWOOD
LAGERSTROEMIA INDICA	CRAPE MYRTLE
PINUS CONTORTA VAR. CONTORTA	SHORE PINE
SEQUOIA SEMPERVIRENS	COAST REDWOOD

SHRUBS

HYDRANGEA QUERCIFOLIA 'SNOW QUEEN'	SNOW QUEEN OAKLEAF HYDRANGEA
VACCINIUM VITIS-IDAEA	LINGONBERRY
ILEX CRENATA 'CONVEXA' *	CONVEX-LEAF JAPANESE HOLLY
VACCINIUM OVATUM*	EVERGREEN HUCKLEBERRY
CORNUS SERICEA 'KELSEYII'*	KELSEY REDTWIG DOGWOOD
RHODODENDRON FASTIGIATUM	DWARF EVERGREEN RHODODENDRON
ERICA CARNEA 'SPRINGWOOD WHITE'	WHITE WINTER HEATH
PHYLLOSTACHYS NIGRA	BLACK BAMBOO

GROUNDCOVERS

50% FRAGARIA CHILOENSIS*	BEACH STRAWBERRY
50% ARCTOSTAPHYLOS UVA-URSI*	KINNIKINNICK
50% POLYSTICHUM MUNITUM*	WESTERN SWORD FERN
50% MAHONIA REPENS*	CREeping OREGON GRAPE
50% LIRIOPE SPICATA	CREeping LILYTURF
50% DRYOPTERIS ERYTHROSORA	AUTUMN FERN
50% LIRIOPE SPICATA	CREeping LILYTURF
50% HAKONECHLOA MACRA	JAPANESE FOREST GRASS
MULCH WITH RIVER ROCK	
OPHIPOGON PLANISCAPUS 'NIGRESCENS'	BLACK MONDO GRASS

BIORETENTION PLANTS

50% CAREX OBNUTA*	SLOUGH SEDGE
50% IRIS TENAX*	TOUGH-LEAF IRIS

ROOF PLANTS

8" GREEN ROOF SYSTEM WITH SEDUM MIX	
NASSELLA TENUISSIMA (SEE REMARKS)	MEXICAN FEATHER GRASS

TREES



Prunus x yedoensis 'Akebono'  
Yoshino Cherry



Carpinus japonicus  
Japanese Hornbeam



Pinus contorta var. contorta  
Shore Pine



Sequoia sempervirens  
Coast redwood

SHRUBS



Hydrangea 'Snow Queen'  
Oakleaf Hydrangea



Vaccinium vitis idaea  
Lingonberry



Ilex crenata 'convexa'  
Convex leaf Japanese Holly



Vaccinium ovatum  
Evergreen Huckleberry

BIORETENTION



Cornus sericea 'Kelseyi'  
Kelsey Redtwig Dogwood



Erica carnea 'Springwood White'  
White Winter Heath



Phyllostachys nigra  
Black Bamboo

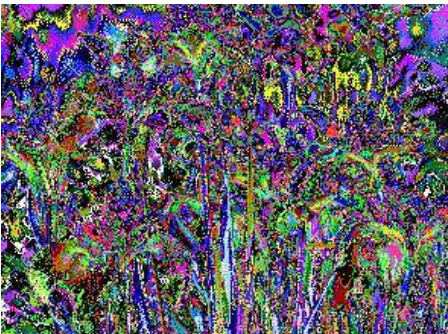


Rhododendron fastigiatum  
Dwarf Evergreen Rhododendron

ROOF



Carex obnupta  
Slough Sedge



Iris siberica  
Siberian Iris



Green Roof  
with Mexican Feather Grass



Calamagrostis acutiflora  
Karl Foerster Grass



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STREET LEVEL LIGHTING PLAN



**LED RECESSED DOWN LIGHT**  
4" down lights are recessed into the soffit at the retail terrace and residential egress to provide ambient illumination.



**LED DOWN LIGHT**  
Wall mounted down lights are located at each courtyard residential unit entry providing secure, general illumination, the parking garage entrance, and the main residential entrance.



**LED STEP LIGHT**  
Step lights are provided on planters, seat walls, and stairs at the retail terrace and residential egress to provide comfortable low-level illumination along walking surfaces.





ROOF LEVEL LIGHTING PLAN



**LED RECESSED DOWN LIGHT**  
4" down lights are recessed into the soffit at the BBQs to provide ambient illumination.



**LED GRILL LIGHT**  
This task light is mounted adjacent to the grills to provide direct illumination to the work and grilling surfaces.



**LED ACCENT LIGHT**  
Accent lights are provided at each tree in the planter pots and along the north green roof to highlight the vertical vegetation and add a soft, indirect glow of light .



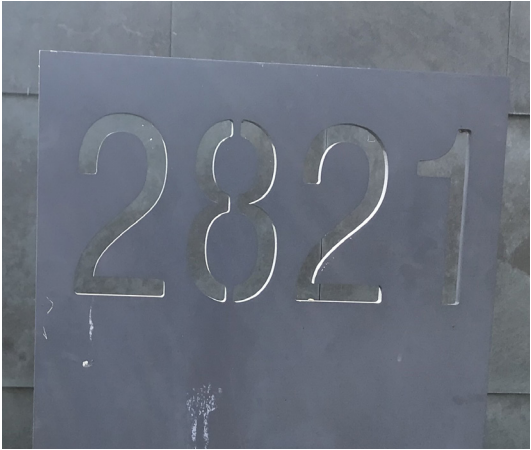


BUILDING SIGNAGE DETAIL



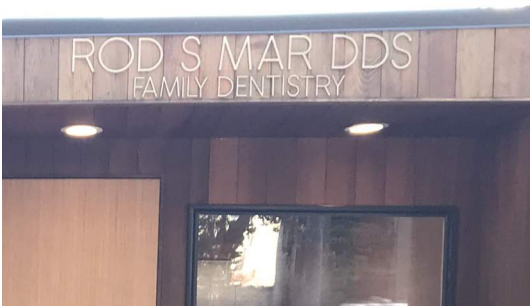
Multifamily Identification Sign  
50-SF max. per street frontage

Multifamily Identification Sign  
50-SF max. per street frontage





BUILDING SIGNAGE DETAIL





POTENTIAL DEVELOPMENT DEPARTURES

The proposed project will be a seven-story, apartment building approximately 65-feet in height and approximately 60,700-sf, including the below grade level. The building will contain 79 residential apartment units, resident lobby and amenity spaces, an outdoor landscaped terrace and roof deck, on-site leasing office, street level commercial space, in-structure resident parking for 23 vehicles, and a below-grade level for building services and storage.

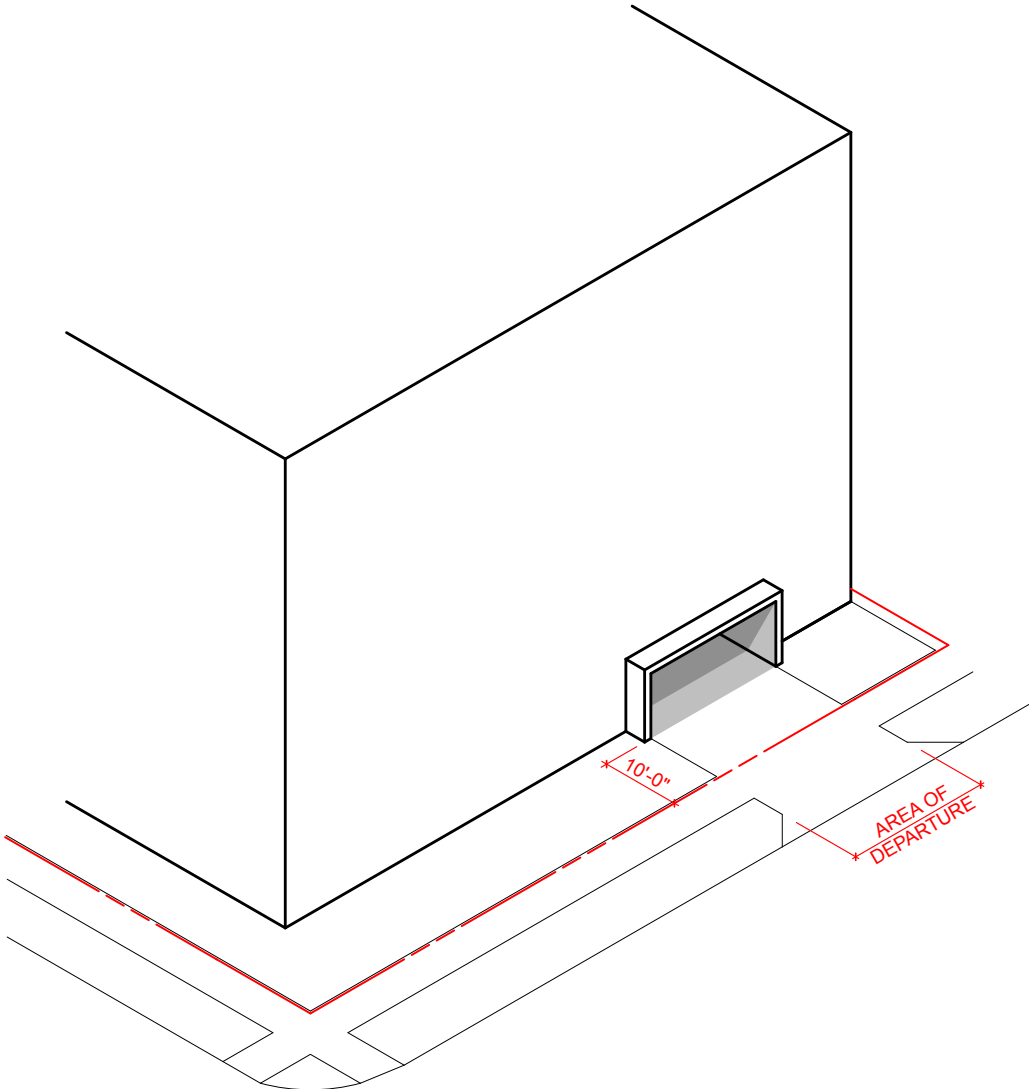
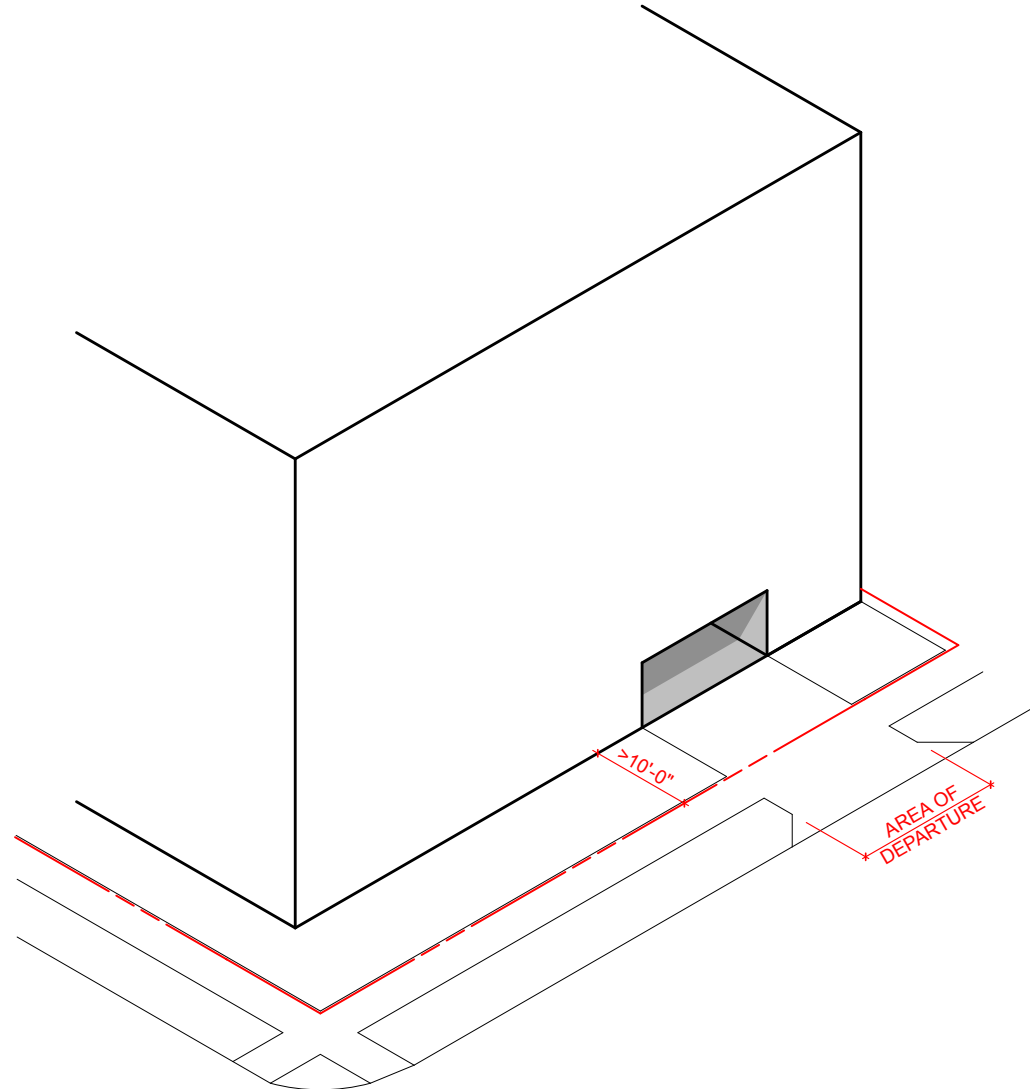
The project development objectives are as follows:

- Provide a high quality living environment for residents convenient to work, leisure and recreation
- Provide a lively environment for urban residents, including accommodations for retail and/or restaurants
- Develop pedestrian-oriented design to benefit the neighborhood:
  - Widen sidewalks
  - Strengthen the street edge with an appropriately scaled facade
  - Create vibrant and transparent street level façades
  - Plant street trees to buffer traffic
- Be a good neighbor. This will inform the project in terms of:
  - Commercial level streetscape design
  - Landscape design & material selection
  - Exterior lighting design
  - Parking access
  - Trash & recycling storage

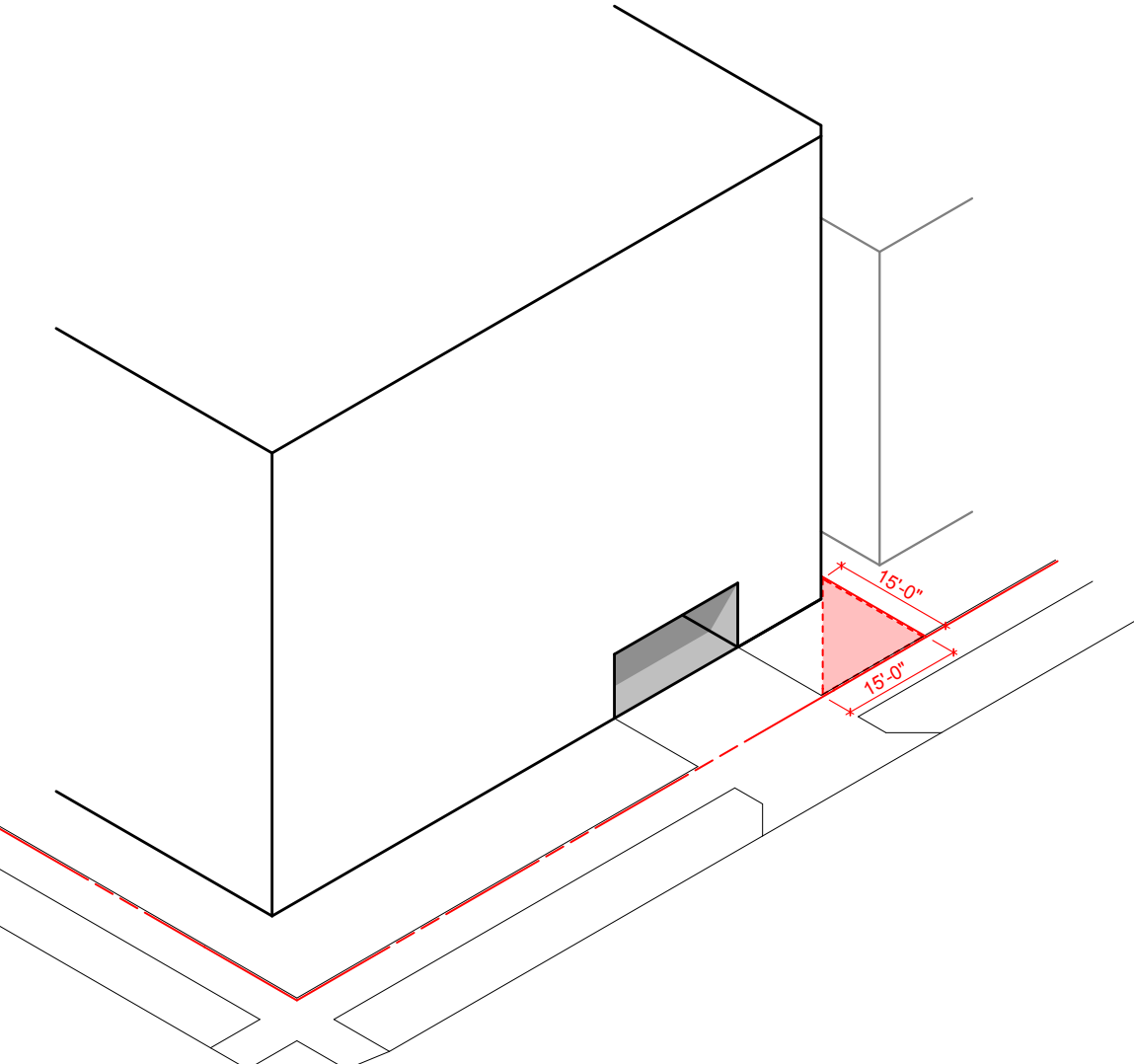
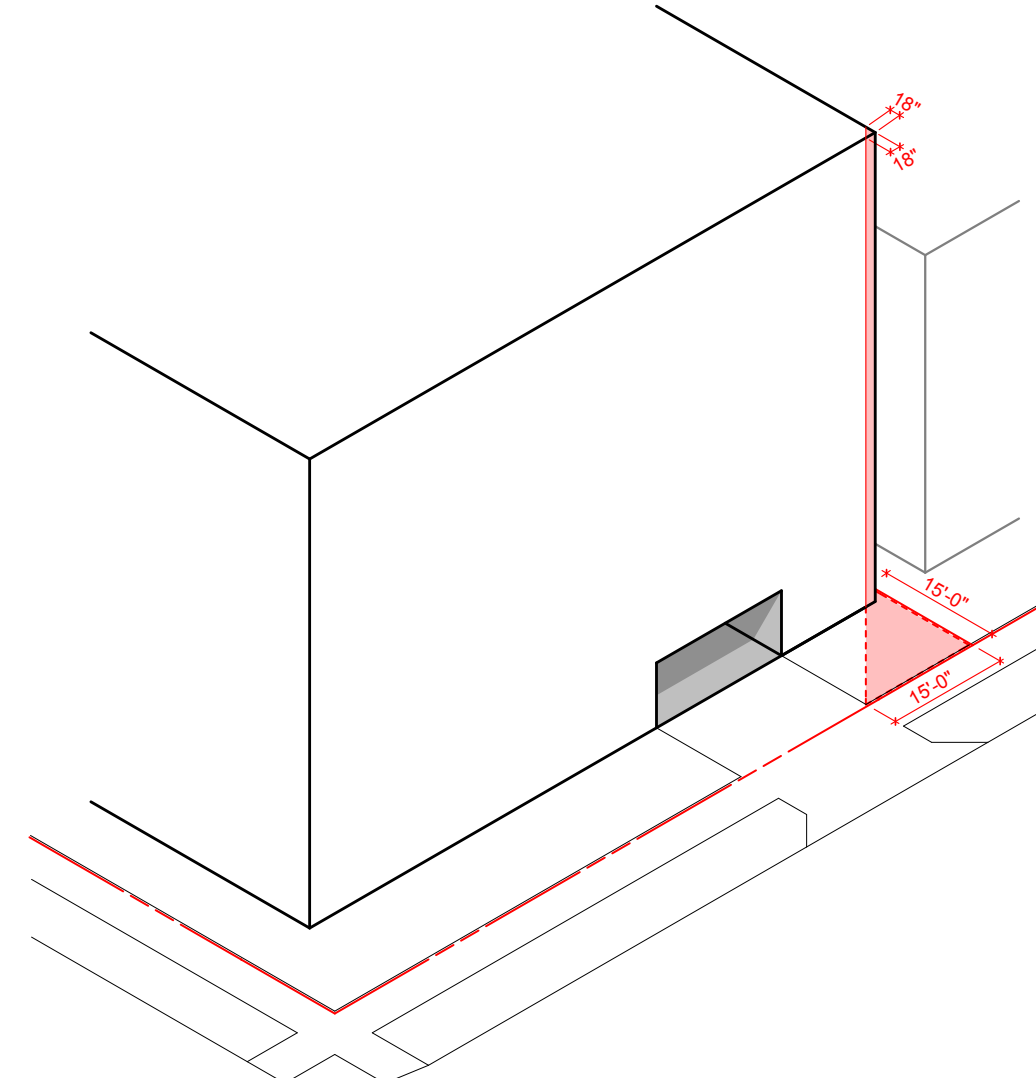
The following Development Departures have been identified as potentially necessary to achieve the preferred design alternate. If the board indicates their willingness to consider the departures, additional study and refinement of the design will be conducted in advance of the Design Recommendation meeting.

DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
<b>1    <u>23.47A.008.A.3. Street Level Development Standards</u></b> Street-level street-facing facades shall be located within 10 feet of the street lot line, unless wider sidewalks, plazas, or other approved landscaped or open spaces are provided.	Request to depart from the requirement for the street-level street-facing facade to be located within 10-feet of the street lot line where no approved landscaping or open space is provided - the parking garage entrance & driveway.	Neighborhood Conditions: The existing buildings along 14th Ave S setback 10-feet to 15-feet from the street lot line providing a urban pattern of "front yards". In addition to this, 14th Ave S has been identified as the preferred access to parking. -A project goal is to enhance the pedestrian environment and residential character along 14th Ave S. Setting back allows for a generous "front yard" space. Allowing the garage entrance to setback consistent with the rest of the building maintains design integrity.
<b>2    <u>23.47A.014.B.1 Setback Requirements</u></b> A setback is required where a lot abuts the intersection of a side lot line and front lot line of a lot in a residential zone. The required setback forms a triangular area. Two sides of the triangle extend along the street lot line and side lot line 15-feet from the intersection of the residentially zoned lot's front lot line and the side lot line abutting the residentially zoned lot.	Request to depart from the 15-foot triangular setback at street level.	Neighborhood Conditions: Setbacks at the corner between residential lots are to maintain access to light, air, and view. -A project goal is to enhance the pedestrian environment along S Bayview St and 14th Ave S. The topographic constraints combined with the preferred location of parking access limit the resulting program and use distribution. The preferred solution maximizes active and public program, space, and landscaping.
<b>3    <u>23.47A.014.B.3 Setback Requirements</u></b> For a structure containing residential use, a setback is required along any rear or side lot line that abuts a lot in a residential zone as follows: a. 15-feet for portions of structures above 13-feet in height to a maximum of 40-feet b. For each portion of a structure above 40-feet in height, additional setback at the rate of 2-foot of setback for every 10-feet in height above 40-feet	Request to depart from the setbacks required between 13-feet in height and the top of structure for approximately 36-feet of the proposed building along the south property line.	Neighborhood Conditions: The purpose of this setback is to provide access to light, air, and views for neighboring properties. The existing buildings along 14th Ave S setback 10-feet to 15-feet from the street lot line providing a urban pattern of "front yards". Additionally, the windows of the neighboring structure have been surveyed to confirm no windows conflict with the location of this departure. -A project goal is to enhance the pedestrian environment and residential character along 14th Ave S. Building within this setback allows the building to setback from 14th Ave S for a generous "front yard" space consistent with the neighboring buildings. This also provides the opportunity to step the building mass down closer to 14th Ave S.
<b>4    <u>23.54.030.E.1 Parking Space Standards</u></b> Parking aisles shall be provided according to the requirements of Exhibit C for 23.54.030 based on size of parking spaces served by the aisle. Small parking spaces require a minimum aisle width of 20-feet.	Allow parking aisles required to be a minimum 20-feet wide to be reduced to 18-feet at two locations in the parking garage - the south drive lane and the east drive lane. Request to depart from the minimum depth requirement for retail use from 30'-0" average to 26'-11".	Neighborhood Conditions: The Beacon Hill Station Overlay is a pedestrian oriented area with access to multiple modes of public transportation. - The small site along with generous setbacks along 14th Ave S and S Bayview St constrains the parking garage - this project is putting priority on providing parking and reducing parking aisle only as much as is required.
<b>5    <u>23.47A.008.B.3 Non-residential Street-level Requirements</u></b> Depth provisions for new structures or new additions to existing structures. Non-residential uses shall extend an average depth of at least 30 feet and a minimum depth of 15 feet from the street-level street-facing facade.		-A project goal is to enhance the pedestrian environment and retail relationship of S Bayview St to Beacon Ave S - The generous retail plaza provides additional, usable retail depth that activates the retail frontage and provides a vibrant streetscape.



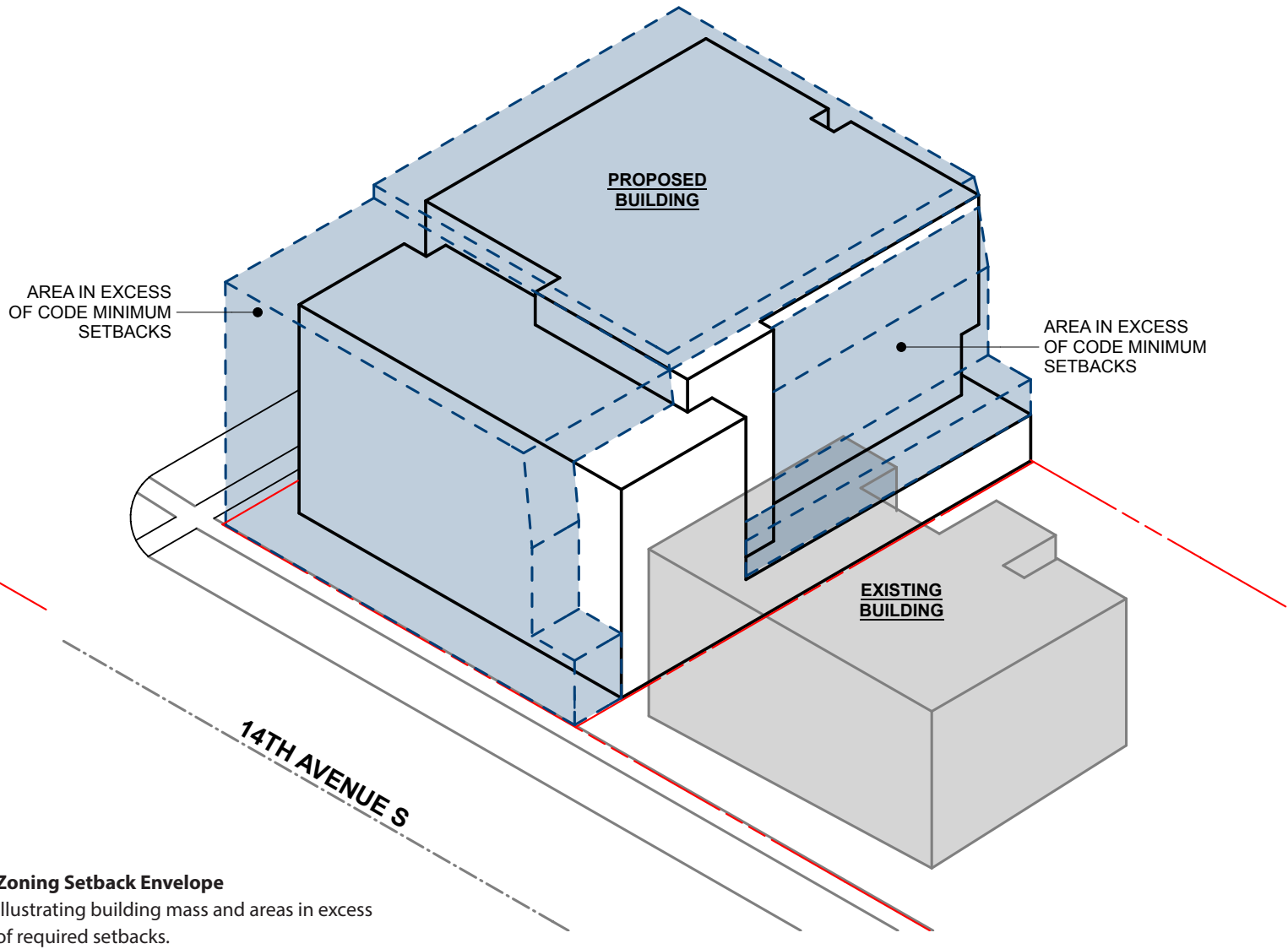
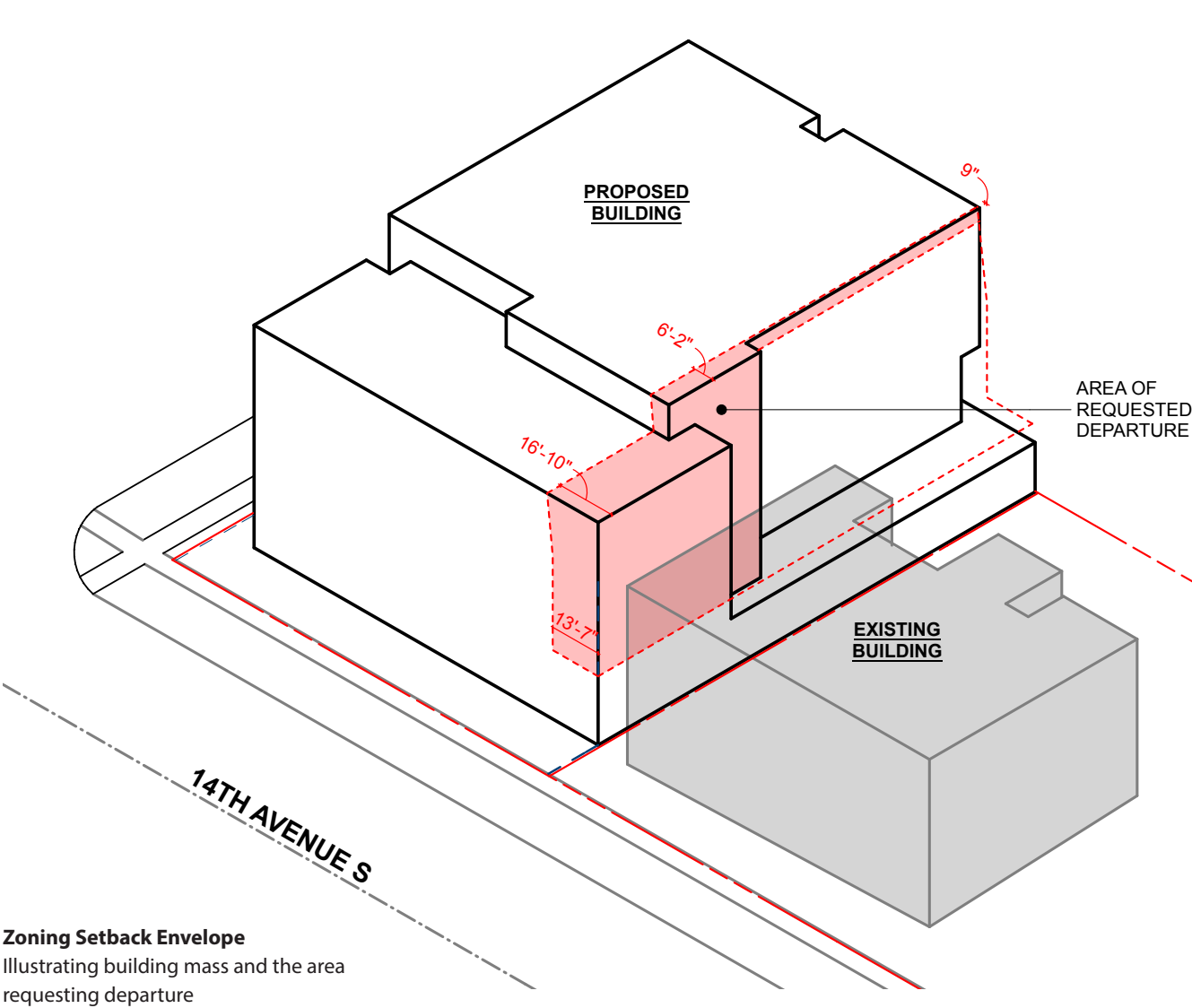
DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
<p><b>1 23.47A.008.A3. Street Level Development Standards</b>            Street-level street-facing facades shall be located within 10 feet of the street lot line, unless wider sidewalks, plazas, or other approved landscaped or open spaces are provided.</p>  <p><b>Code Compliant Condition</b>            Illustrating compliant street-level street-facing facade</p>	<p>Request to depart from the requirement for the street-level street-facing facade to be located within 10-feet of the street lot line where no approved landscaping or open space is provided - the parking garage entrance &amp; driveway.</p>  <p><b>Requested Departure</b>            Illustrating request for departure to create a safe, consistent, and well-design facade.</p>	<p>Neighborhood Conditions: The existing buildings along 14th Ave S setback 10-feet to 15-feet from the street lot line providing a urban pattern of "front yards". In addition to this, 14th Ave S has been identified as the preferred access to parking.</p> <p>-A project goal is to enhance the pedestrian environment and residential character along 14th Ave S. Setting back allows for a generous "front yard" space. Allowing the garage entrance to setback consistent with the rest of the building maintains design integrity.</p>



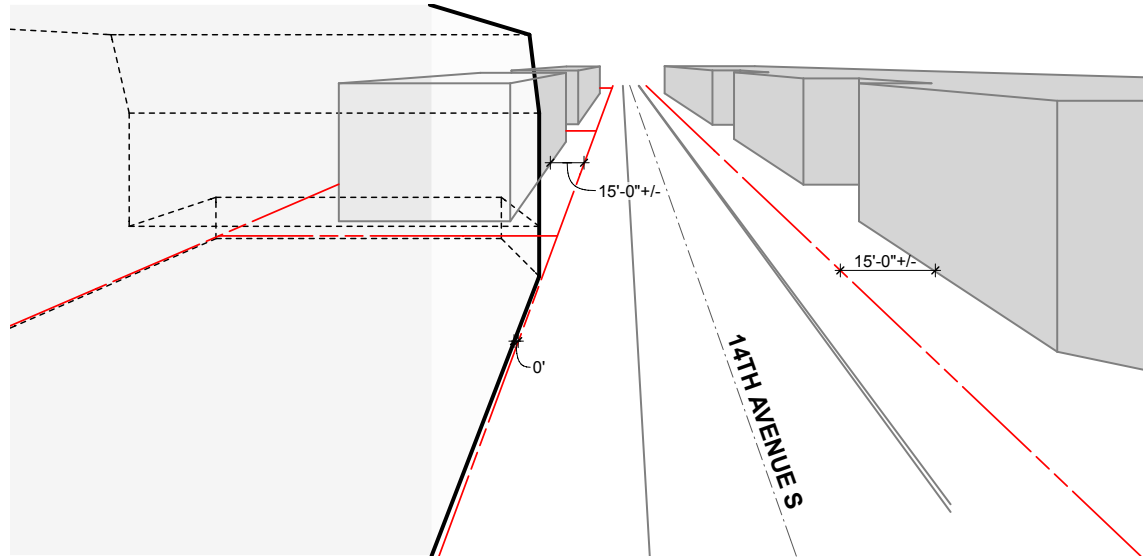
DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
<p>2 <b>23.47A.014.B.1 Setback Requirements</b></p> <p>A setback is required where a lot abuts the intersection of a side lot line and front lot line of a lot in a residential zone. The required setback forms a triangular area. Two sides of the triangle extend along the street lot line and side lot line 15-feet from the intersection of the residentially zoned lot's front lot line and the side lot line abutting the residentially zoned lot.</p>	<p>Request to depart from the 15-foot triangular setback at street level.</p>	<p>Neighborhood Conditions: Setbacks at the corner between residential lots are to maintain access to light, air, and view.</p> <p>-A project goal is to enhance the pedestrian environment along S Bayview St and 14th Ave S. The topographic constraints combined with the preferred location of parking access limit the resulting program and use distribution. The preferred solution maximizes active and public program, space, and landscaping.</p>
 <p><b>Code Compliant Condition (Option 3)</b> Illustrating compliant setback at property line corner.</p>		 <p><b>Requested Departure (Option 3)</b> Illustrating request for departure to create a consistent and well-design facade.</p>



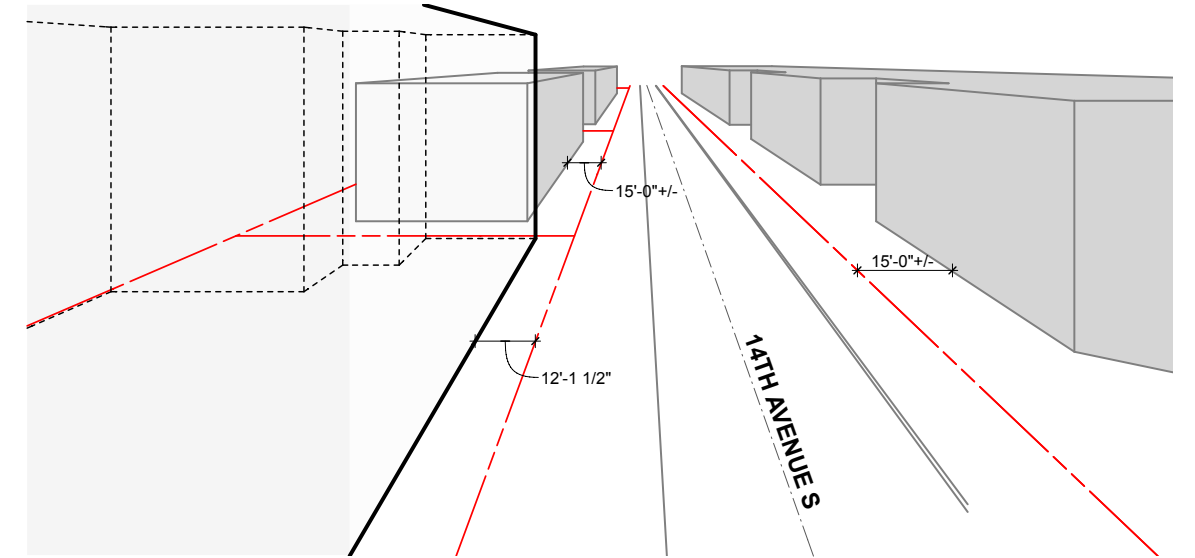
DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
<p><b>3 23.47A.014.B.3 Setback Requirements</b></p> <p>For a structure containing residential use, a setback is required along any rear or side lot line that abuts a lot in a residential zone as follows:</p> <p>a. 15-feet for portions of structures above 13-feet in height to a maximum of 40-feet</p> <p>b. For each portion of a structure above 40-feet in height, additional setback at the rate of 2-foot of setback for every 10-feet in height above 40-feet</p>	<p>Request to depart from the setbacks required between 13-feet in height and the top of structure for approximately 36-feet of the proposed building along the south property line.</p>	<p>Neighborhood Conditions: The purpose of this setback is to provide access to light, air, and views for neighboring properties. The existing buildings along 14th Ave S setback 10-feet to 15-feet from the street lot line providing a urban pattern of "front yards". Additionally, the windows of the neighboring structure have been surveyed to confirm no windows conflict with the location of this departure.</p> <p>-A project goal is to enhance the pedestrian environment and residential character along 14th Ave S. Building within this setback allows the building to setback from 14th Ave S for a generous "front yard" space consistent with the neighboring buildings. This also provides the opportunity to step the building mass down closer to 14th Ave S.</p>







**Code Compliant Condition**  
Illustrating compliant property line setback.



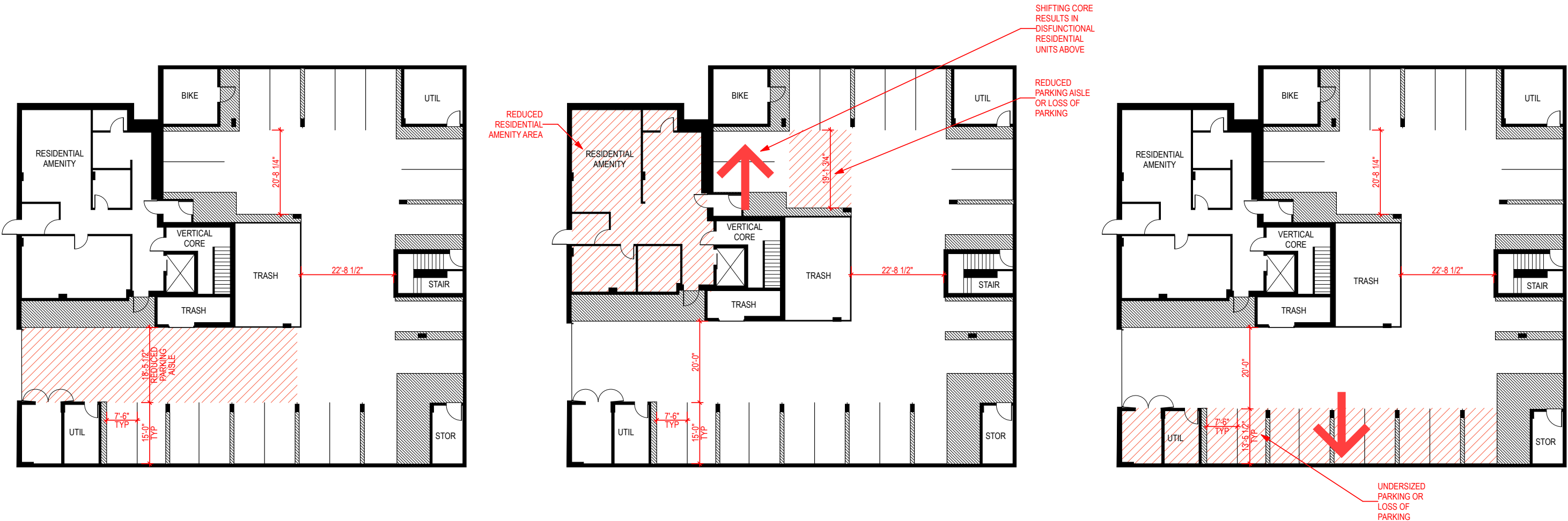
**Requested Departure**  
Illustrating request for departure to create building mass consistent with the neighborhood and urban pattern.







DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
<b>4 23.54.030.E.1 Parking Space Standards</b> Parking aisles shall be provided according to the requirements of Exhibit C for 23.54.030 based on size of parking spaces served by the aisle. Small parking spaces require a minimum aisle width of 20-feet.	Allow parking aisles required to be a minimum 20-feet wide to be reduced to 18-feet at two locations in the parking garage - the south drive lane and the east drive lane.	Neighborhood Conditions: The Beacon Hill Station Overlay is a pedestrian oriented area with access to multiple modes of public transportation. - The small site along with generous setbacks along 14th Ave S and S Bayview St constrains the parking garage - this project is putting priority on providing parking and reducing parking aisle only as much as is required.



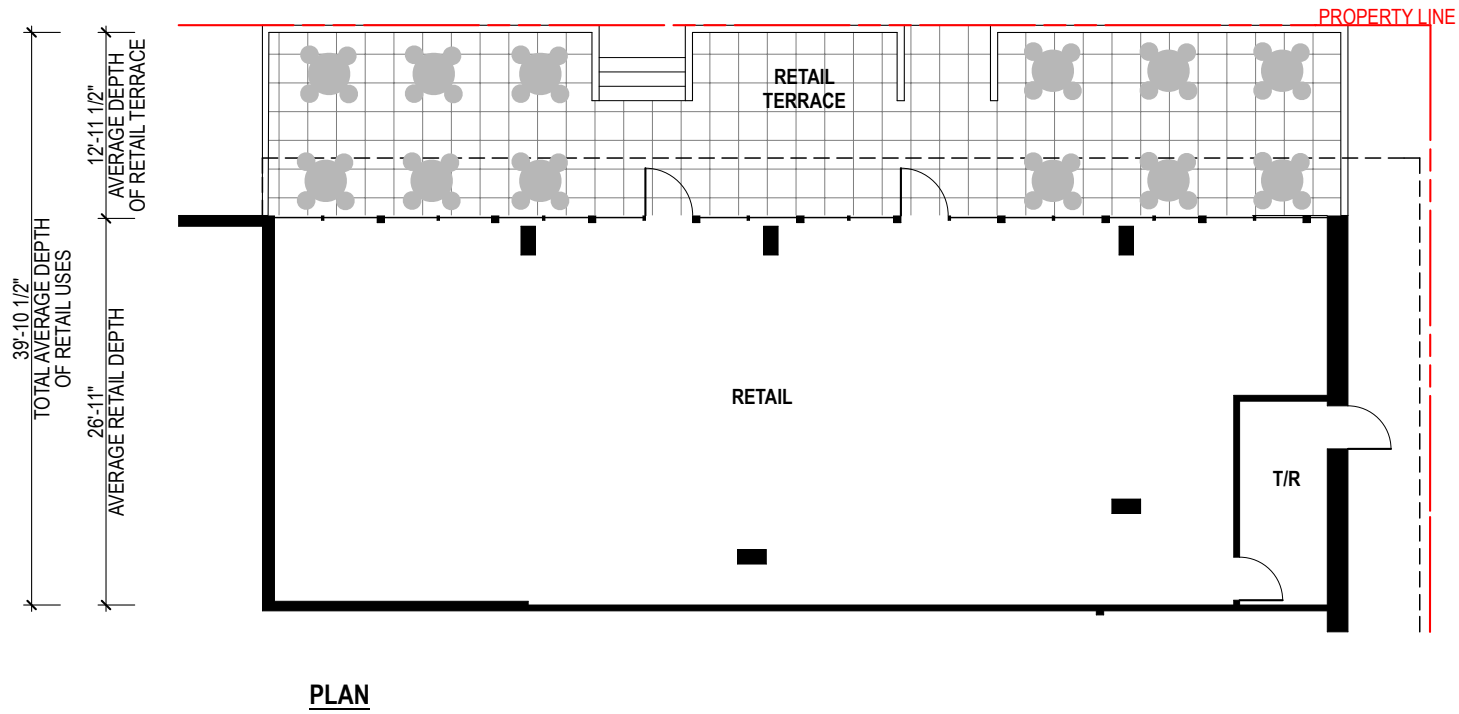
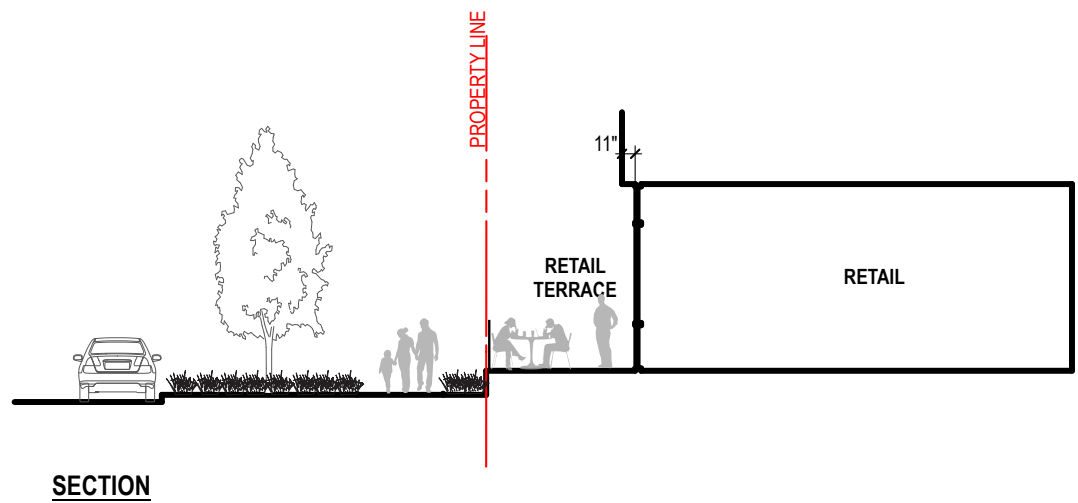
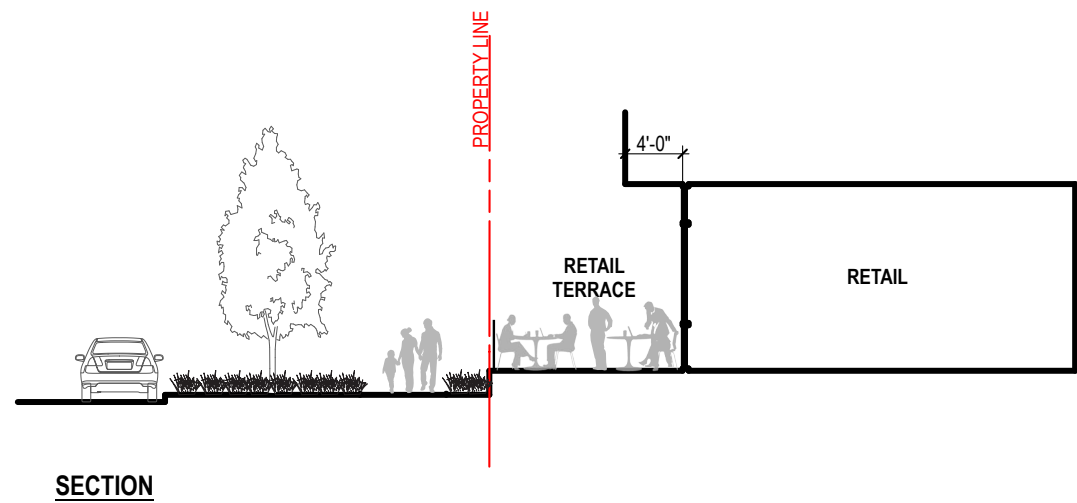
**Requested Departure**  
 Illustrating reduced parking aisle along single loaded parking aisle.

**Non-Code Compliant Option**  
 Illustrating expanding parking aisle to the north - achieved by shifting building core north.

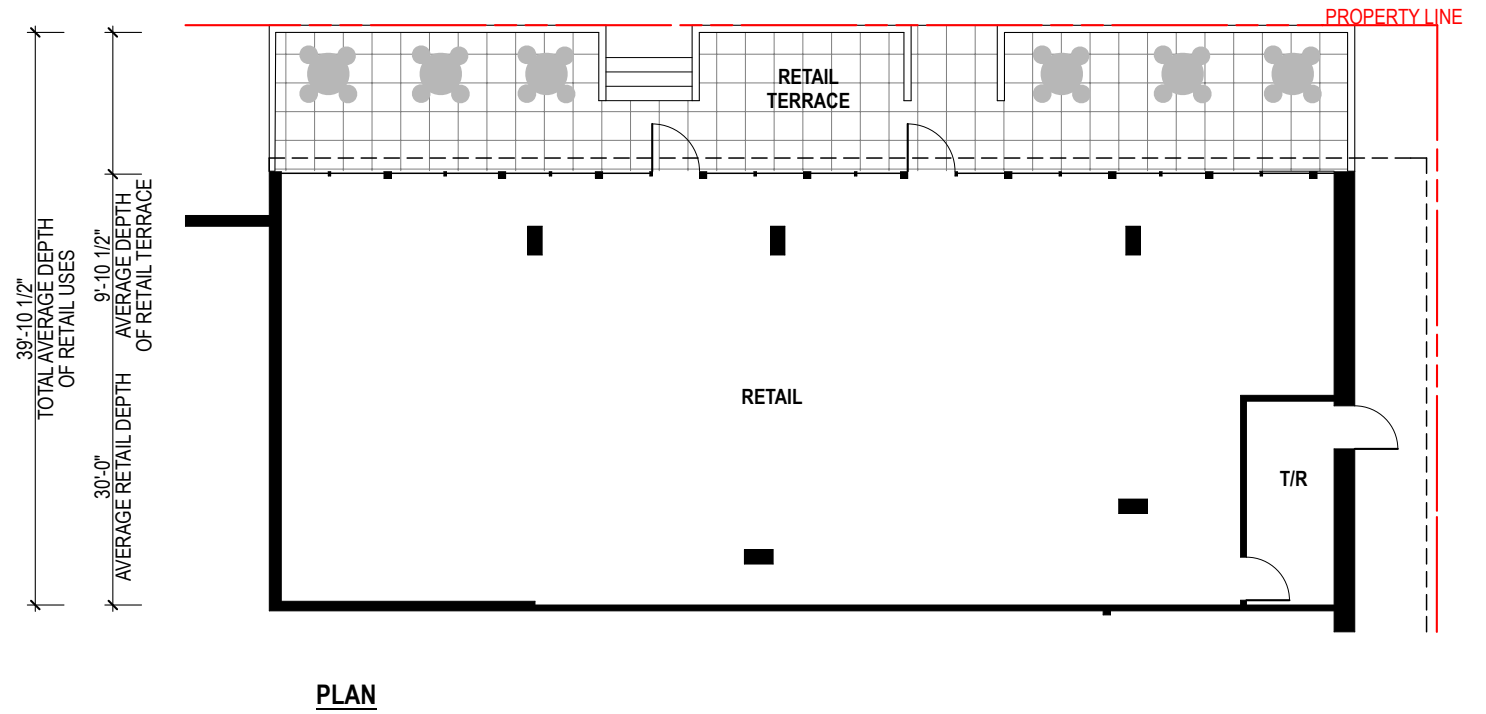
**Code Compliant Option**  
 Illustrating expanding parking aisle to the south - achieved by reducing size/quantity of parking to the south.



DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
<b>5 23.47A.008.B.3 Non-residential Street-level Requirements</b> Depth provisions for new structures or new additions to existing structures. Non-residential uses shall extend an average depth of at least 30 feet and a minimum depth of 15 feet from the street-level street-facing facade.	Request to depart from the minimum depth requirement for retail use from 30'-0" average to 26'-11".	-A project goal is to enhance the pedestrian environment and retail relationship of S Bayview St to Beacon Ave S - The generous retail plaza provides additional, usable retail depth that activates the retail frontage and provides a vibrant streetscape.



**Requested Departure**  
 Illustrating generous retail terrace.



**Code Compliant Option**  
 Illustrating constrained retail terrace and minimal soffit



PRIORITY DESIGN GUIDELINES

SEATTLE DESIGN GUIDELINES		DESIGN RESPONSE
<b>CS2 Urban Pattern and Form</b>		
<i>Strengthen the most desirable forms, characteristics, and patterns of the streets, block faces, and open spaces in the surrounding area.</i>		
	<b>A1 Location in the City and Neighborhood, Sense of Place</b> Emphasize attributes that give Seattle, the neighborhood, and/or the site its distinctive sense of place. Design the building and opens spaces to enhance areas where a strong identity already exists, and create a sense of place where the physical context is less established. Examples of neighborhood and/or site features that contributed to a sense of place include patterns of streets or blocks, slopes, sites with prominent visibility, relationships to bodies of water or significant trees, natural areas, open spaces, iconic buildings or transportation junctions, and land seen as a gateway to the community.	The ensemble of landscaping, hardscaping, programming, material selection, and building design work in concert to respond to the site context and relate to the neighborhood. This occurs from the way the massing transitions zones and uses, the material selectrion that takes cues from both historic and contemporary neighborhood architecture, the Asian-inspired landscape design, and the building programming.
<b>North Beacon Hill Supplemental Guidance</b>	<b>III Height, Bulk and Scale Compatibility</b> i. Break larger (particularly longer) buildings into separate volumes to maintain a compatible scale with smaller commercial buildings nearby. ii. Break up building mass by incorporating different façade treatments to give the impression of multiple, small-scale buildings, in keeping with the established development pattern. iii. Consider existing views to downtown Seattle, Puget Sound, Mt. Rainier, the Olympics, and the Cascade Mountains, and incorporate site and building design features that help to preserve or enhance those views from public rights of way. iv. Incorporate into the design of new buildings studies that document the shadows cast from proposed structures in order to maximize the amount of sunshine on adjacent sidewalks and residences through-out the year. v. Step back elevation at upper levels of large-scale development to take advantage of views and increase sunlight at street level. vi. Articulate the building facades vertically or existing pattern of development in the vicinity. vii. Employ architectural measures to reduce building scale such as: landscaping, trellises, complementary materials, detailing, and accent trim. viii. Soften commercial facades with dense landscaping, where appropriate. ix. Repeat domestic architectural elements of surrounding buildings (roof lines, window styles, proportions). x. Use architectural styles and details (such as roof lines or fenestration), color or materials derived from surrounding, less intensive structures. xi. Locate features, such as required open space, on the zone edge to create further separation and buffering of lower intensive structures.	The project site has a Zone Edge Transition condition along the south and west edge at the property line and public rights-of-way. The project provides a thoughtful transition thru stepped massing and "front yard" set back along 14th Ave S. The setback along 14th Ave S provides an active pedestrian landscape that increases the ground level setback and provides substantial landscaping. Additionally, the building massing, design of secondary elements, and arrangement of interior uses is meant to modulate the facade and minimize blank walls.



SEATTLE DESIGN GUIDELINES	DESIGN RESPONSE
<b>CS3 Architectural Context and Character</b> <i>Contribute to the architectural character of the neighborhood</i>	
<b>A1 Emphasizing Positive Neighborhood Attributes, Fitting Old and New Together</b> Create compatibility between new projects, and existing architectural context, including historic and modern designs, through building articulation, scale and proportion, roof forms, detailing, fenestration, and/or the use of complementary materials.	The building design aims to marry the existing architectural character of Beacon Hill with the evolving development. The use of historic and neighborhood inspired materials, the scale and proportion of the façade articulation, and fenestration pattern provide a calm, urban fabric building that compliments both the site and neighborhood.
<b>B Local History and Culture, Historical/Cultural References</b> <b>2</b> Reuse existing structures on the site where feasible as a means of incorporating historical or cultural elements into the new project.	The existing buildings and their materials are not suitable for reuse. However, the existing structures have inspired the use of masonry and wood siding.
<b>PL3 Street-Level Interaction</b> <i>Encourage human interaction and activity at the street-level with clear connections to building entries and edges</i>	
<b>A1 Entries, Design Objectives</b> Design primary entries to be obvious, identifiable, and distinctive with clear lines of sight and lobbies visually connected to the street. Scale and detail them to function well for their anticipated use and also to fit with the building of which they are a part, differentiating residential and commercial entries with design features and amenities specific to each. a. Office/commercial lobbies should be visually connected to the street through the primary entry and sized to accommodate the range and volume of foot traffic anticipated; b. Retail entries should include adequate space for several patrons to enter and exit simultaneously, preferably under cover from weather. c. Common entries to multi-story residential buildings need to provide privacy and security for residents but also be welcoming and identifiable to visitors. Design features emphasizing the entry as a semi-private space are recommended and may be accomplished through signage, low walls and/or landscaping, a recessed entry area, and other detailing that signals a break from the public sidewalk. d. Individual entries to ground-related housing should be scaled and detailed appropriately to provide for a more intimate type of entry. The design should contribute to a sense of identity, opportunity for personalization, offer privacy, and emphasize personal safety, and security for building occupants.	The entrances are located toward either end of the site where this is opportunity to increase visual connection between inside and outside through the hardscape design. The entrance to the retail space is designated to the more commercial street, S Bayview St, and will be clearly identifiable through material, details, and signage. The entrance to the residences is located on the more residential side of the building, 14th Ave S.



PRIORITY DESIGN GUIDELINES

SEATTLE DESIGN GUIDELINES		DESIGN RESPONSE
<b>CS3 Architectural Context and Character</b> <i>Contribute to the architectural character of the neighborhood</i>		
<b>North Beacon Hill Supplemental Guidance</b>	<b>A2 Entries, Ensemble of Elements</b> Design the entry as a collection of coordinated elements including the door(s), overhead features, ground surface, landscaping, lighting, and other features. Consider a range of elements such as: a. overhead shelter: canopies, porches, building extensions; b. transitional spaces: stoops, courtyards, stairways, portals, arcades, pocket gardens, decks; c. Common entries to multi-story residential buildings need to provide privacy and security for residents but also be welcoming and identifiable to visitors. Design features emphasizing the entry as a semi-private space are recommended and may be accomplished through signage, low walls and/or landscaping, a recessed entry area, and other detailing that signals a break from the public sidewalk. d. Individual entries to ground-related housing should be scaled and detailed appropriately to provide for a more intimate type of entry. The design should contribute to a sense of identity, opportunity for personalization, offer privacy, and emphasize personal safety and security for building occupants.	A combination of human-scale, durable materials, canopies, landscaping, and lighting have been designed at the entrances to the building to provide a good pedestrian experience that reduces the building scale, makes entrances intuitive, and enhances the streetscape.
	<b>I Human Activity</b> i. Provide for sidewalk retail opportunities and connections by allowing for the opening of the storefront to the street and the display of goods on the sidewalks. ii. Provide for outdoor dining opportunities on the sidewalk by allowing for the opening of restaurant or cafe windows to the sidewalk and installing outdoor seating. iii. Install clear glass windows along the sidewalk to provide visual access into the retail or dining activities that occur inside. iv. Do not block views into the interior spaces with the backs of shelving units or posters. v. Maximize window widths and heights along sidewalk face of buildings to create an inviting and interactive atmosphere between indoor and outdoor activities.	The retail space has been located and designed to maximize exposure toward the neighborhood retail on Beacon Ave S. The spaces are composed of fully glazed storefront with a generous retail plaza and landscape buffer. The design should provide an active streetscape, usable retail plaza, transparent and active retail, strong connection to Beacon Ave S, and flexible programming.
	<b>II Streetscape Compatibility</b> Provide a shallow setback and minor grade separation between the first floor and the sidewalk where residential uses occupy the ground floor; this will promote privacy and also accommodate entry porches and stoops.	Landscaping is used to provide a buffer between residential amenity spaces at street level.
<b>DC2 Architectural Concept</b> <i>Develop an architectural concept that will result in a unified &amp; functional design that fits well on the site &amp; within its surroundings</i>		



SEATTLE DESIGN GUIDELINES		DESIGN RESPONSE
<b>DC2 Architectural Concept</b> <i>Develop an architectural concept that will result in a unified &amp; functional design that fits well on the site &amp; within its surroundings</i>		
<b>D1 Scale and Texture, Human Scale</b> Incorporate architectural features, elements, and details that are of human scale into the building facades, entries, retaining walls, courtyards, and exterior spaces in a manner that is consistent with the overall architectural concept. Pay special attention to the first three floors of the building in order to maximize opportunities to engage the pedestrian and enable an active and vibrant street front.		The building scale is reduced thru the use of secondary elements (inset balconies, balconettes, and material changes) and pattern (fenestration and material). These are located throughout the façade and particular care has been taken to provide durable, human-scale materials at the street level.
	<b>D2 Scale and Texture, Texture</b> Design the character of the building, as expressed in the form, scale, and materials, to strive for a fine-grained scale, or "texture", particularly at the street level and other areas where pedestrians predominate.	The use of wood siding, brick siding, and articulated storefront provides human-scale to the materials with the fine-grained texture inherent to each material.
<b>DC4 Exterior Elements and Finishes</b> <i>Use appropriate and high-quality elements and finishes for the building and its open spaces</i>		
<b>North Beacon Hill Supplemental Guidance</b> <b>A1 Exterior Finish Materials</b> Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged.		Durable materials with inherent textures and depth have been selected including wood siding, brick, aluminum storefront, and glazing.
	<b>A1 Exterior Finish Materials</b> Brick and stone are the most common surface treatment in the commercial areas and are strongly encouraged. To the left are some examples of bricks and stone used on Beacon Hill.	The use of brick and aluminum storefront at street level is in line with the types of street level materials used throughout Beacon Hill.



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SITE CONTEXT: LAND USE

The neighborhood directly to the east of the site is zoned NC2P-65 but is currently a mix of single-story retail and low-rise retail. The site is part of the Beacon Hill Urban Village, an area seeing several future, higher-density multi-family projects developing. The block directly north of the site is zoned NC2P-40 but is underdeveloped as a gas station. The blocks directly to the west and south of the site are zoned LR3. These parcels contain mid-rise, multi-family apartments.

- Design Cues**
- Scale transition to south and west to LR3 zone
  - Commercial context and future development to the east



Legend

Beacon Hill Light Rail Station	
Future Development	
Single Family Residential	
Multi Family Residential	
Mixed Use	
Retail	
Office	
Surface Parking	
School / Institutional	
Utility	
Parks/Open Space	
North Beacon Hill Urban Village	



# SITE CONTEXT: TRANSIT MODES

The project site is well served by several bus lines located within a 5-minute walk radius. King County Metro bus routes 36 and 60 run past the site along Beacon Ave S and provide connections to Downtown retail core, International District and Capitol Hill; south connections to Georgetown, Jefferson Park, Beacon Hill, and White Center. Route 107 is located just west of Beacon Ave S on S Lander St. This route provides connection to Renton via Rainier Beach and Lakeridge. The Beacon Hill Light Rail Station is a short walk south of the site and provides connections north to University of Washington and south to SEATAC Airport.

The neighborhood is well served by automobile with on/off ramp to I-5 a short drive south of the site.

Current bicycle infrastructure includes designated bike lanes on 14th Ave S and 15th Ave S. Along with designated bike lanes, the Beacon Hill Neighborhood Greenway has opened this year, located several blocks east of the site.

The public transportation options and commercial development in the vicinity of the site also speak its pedestrian environment.

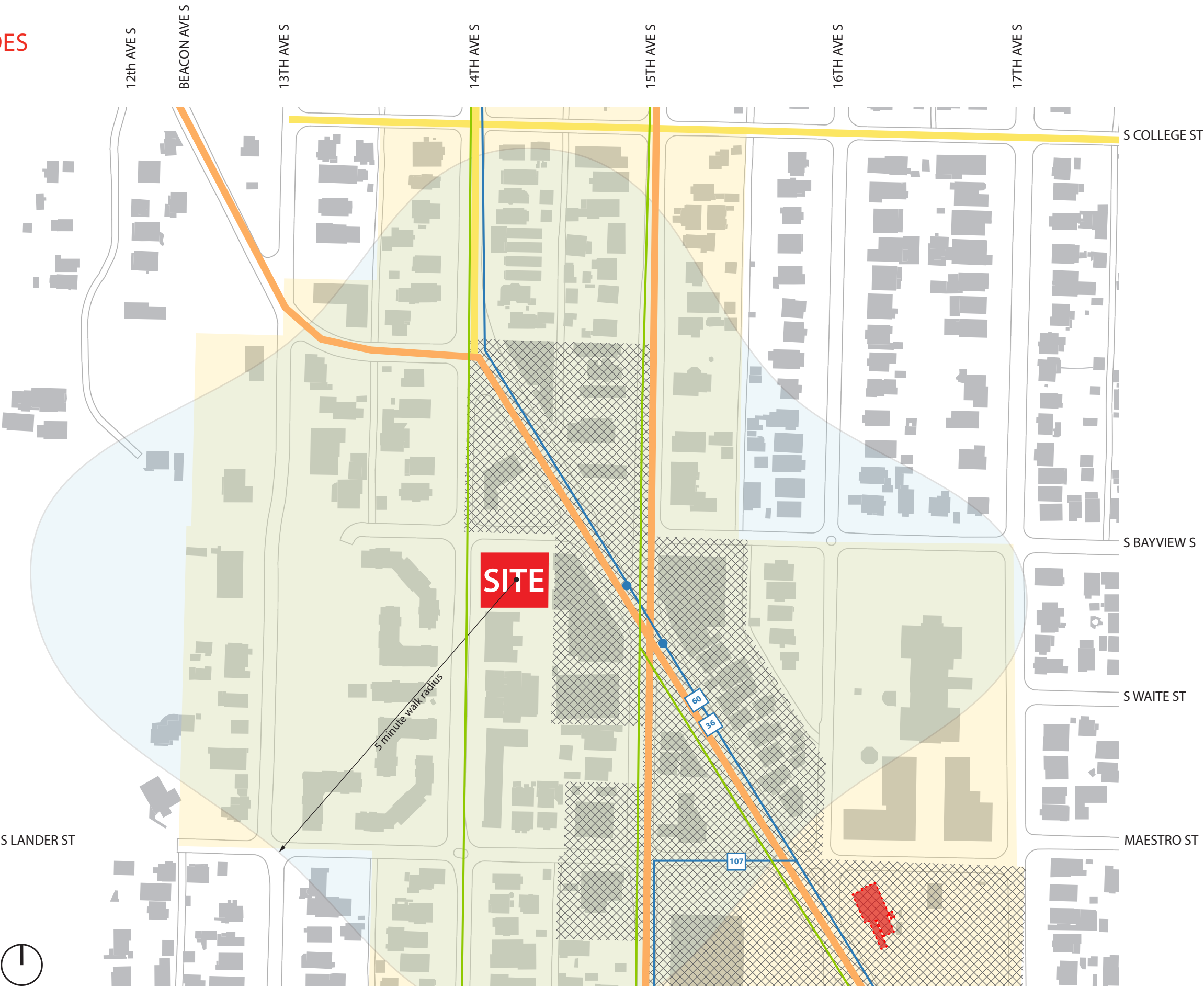
### Design Cues

- Provide bicycle amenities along 14th Ave S
- Make 14th Ave S bicycle safe and accommodating
- Consider pedestrian access up to Beacon Ave S

### Legend

#### Main Car Arterials:

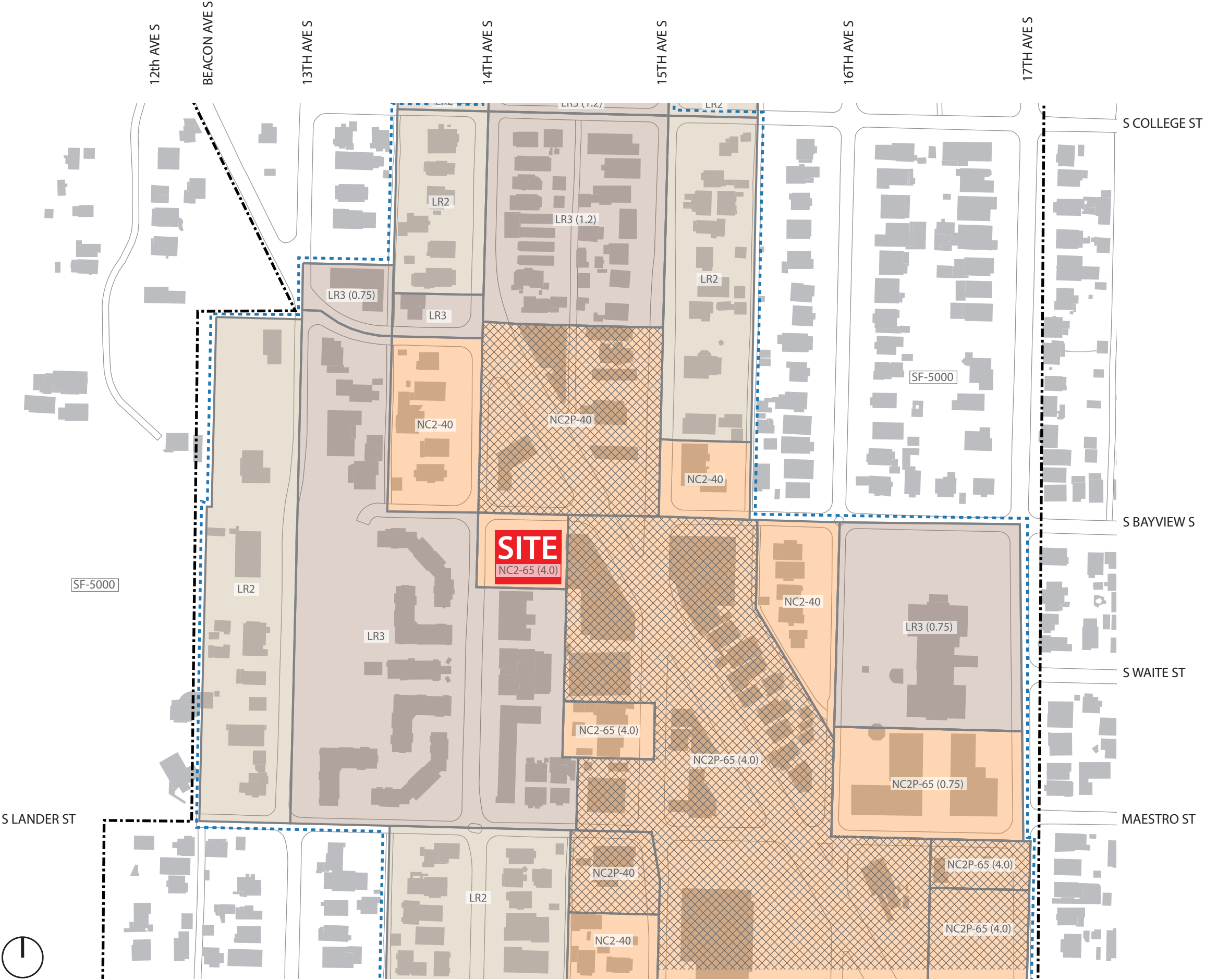
Minor Arterial	<div></div>
Collector Arterial	<div></div>
Beacon Hill Light Rail Station	<div></div>
Frequent Transit Corridors	<div></div>
Pedestrian Overlay	<div></div>
5 Minute Pedestrian Walkshed	<div></div>
Bus Routes	<div></div>
Bus Lines	<div>2</div>
Current Bicycle Routes	<div></div>





CONTEXT: EXISTING ZONING

The site is zoned NC2-65. The site is bounded by public rights-of-way to the north (S Bayview St) and west (14th Ave S). Properties beyond the rights-of-way to the west and south are zoned LR3. The property to the north beyond the rights-of-way is NC2P-40. The east property line abuts the side lot line of an NC2P-65 zone.

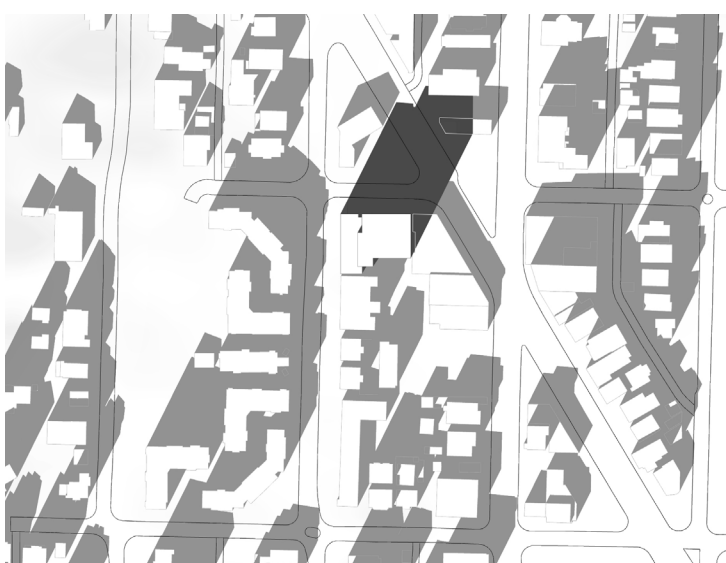
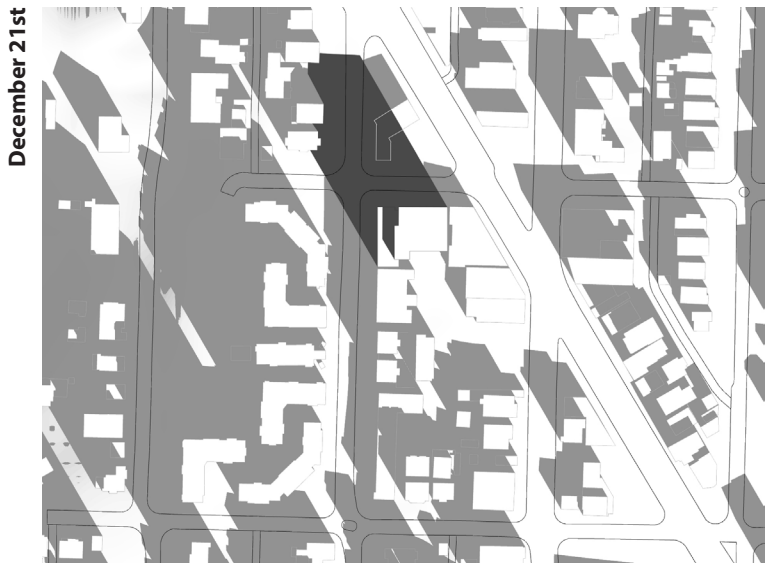
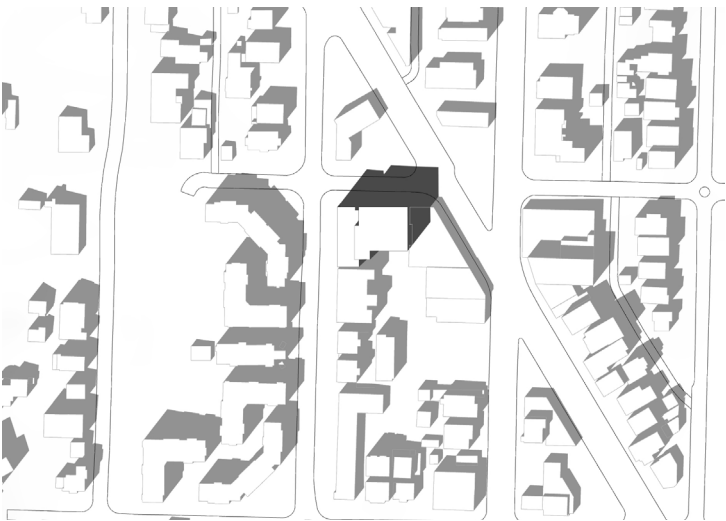
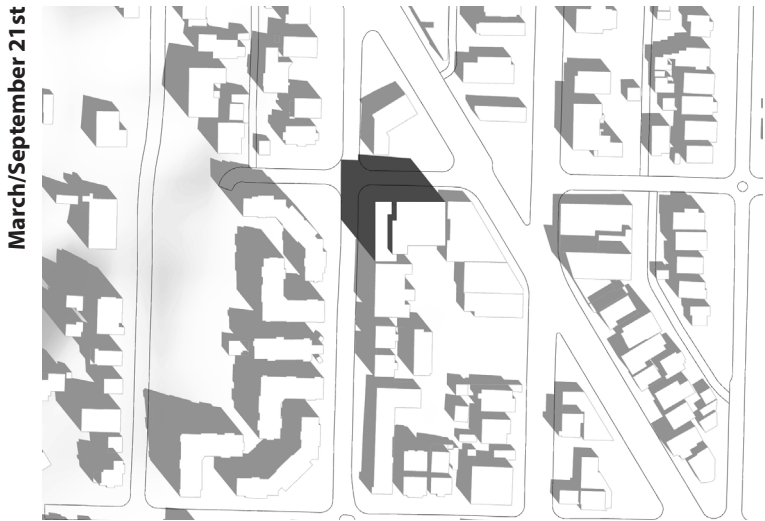
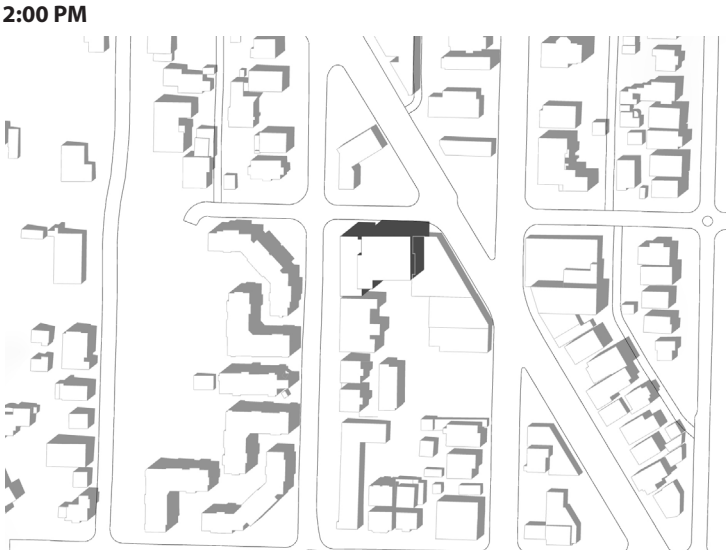
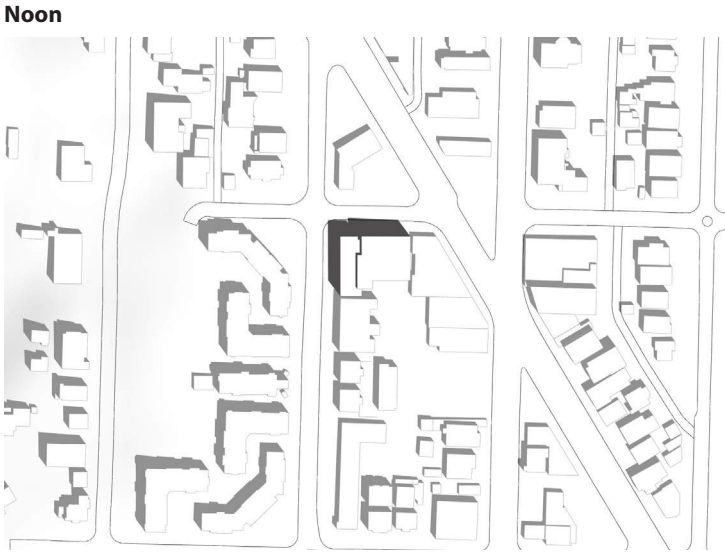


Existing Zoning Legend

NC1	
NC2	
NC3	
LR1 & LR2	
SF	
North Beacon Hill Station Area Overlay	
North Beacon Hill Urban Village	
Zone Boundaries	
Pedestrian Overlay	

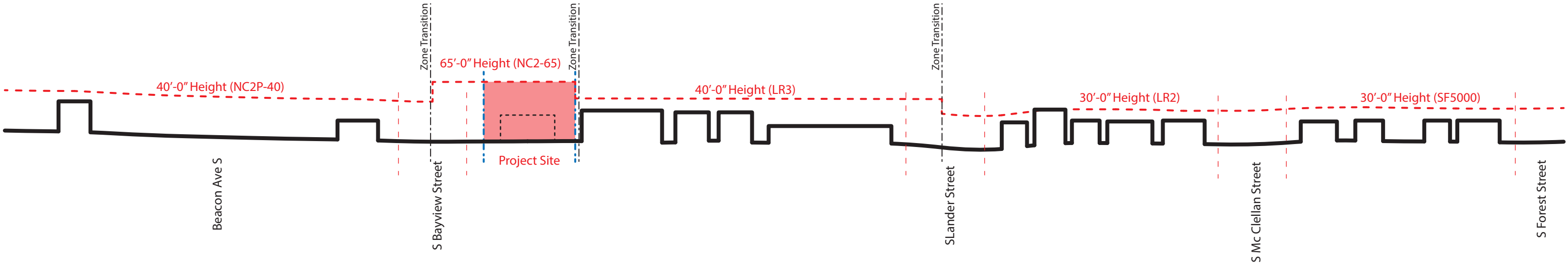


SHADOW STUDY

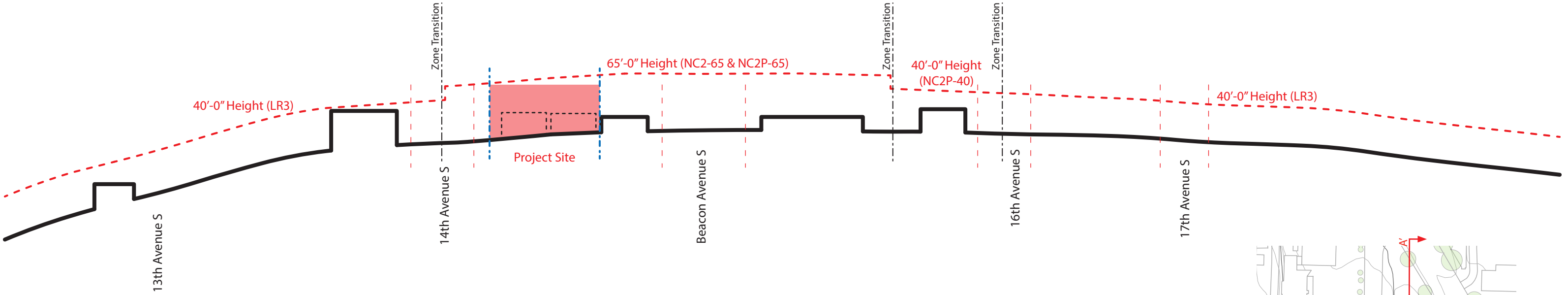




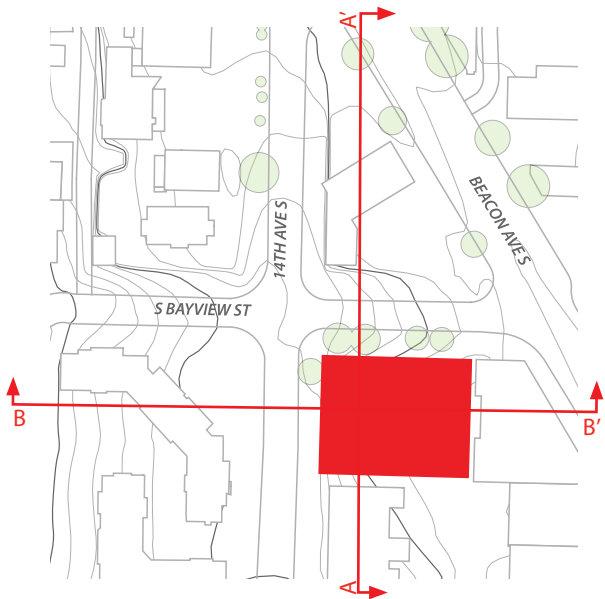
SITE CONTEXT: ZONING ENVELOPE



North-South Site Section A-A' - Looking East



East-West Site Section B-B' - Looking North



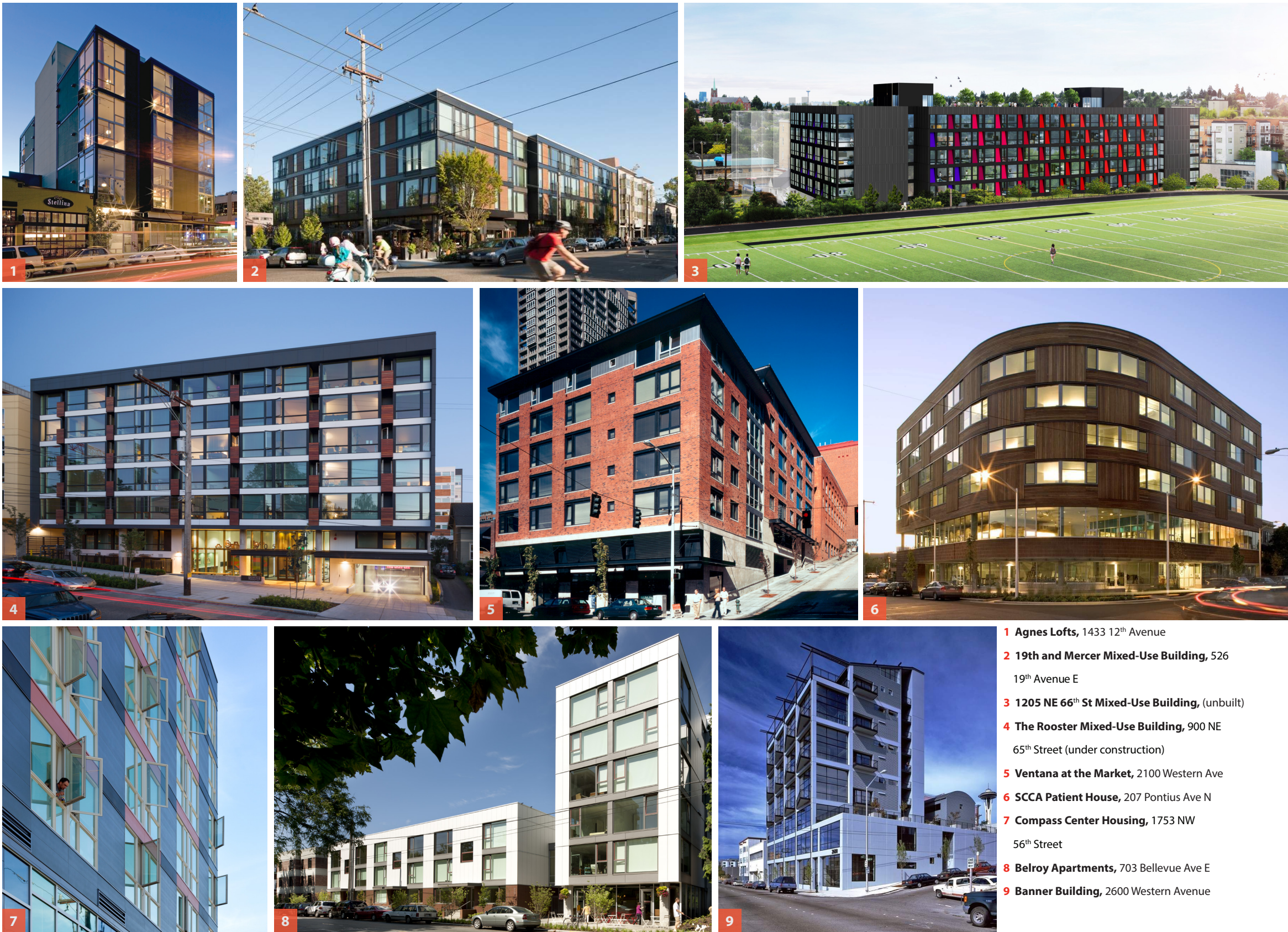


# REPRESENTATIVE PROJECTS

Weinstein A+U is recognized as one of the Northwest’s leading design firms and has continually demonstrated design excellence on a broad array of projects for State, City, Federal, private, and not-for-profit clients. We are passionate about our city and the shaping of its urban neighborhoods through the integration of architecture and urban design is central to our practice.

Well-designed and thoughtful urban housing is a special concern of ours, and we have worked aggressively to advance the expectations of mixed-use projects in Seattle, both technically and aesthetically. While each project presents very specific challenges, a number of recurring themes inform much of our work and form the basis of our approach to housing design:

- All of our buildings are situational and are inseparable from their sites. They sit comfortably amongst their established neighbors, drawing from established precedents while looking to the future
- Well-designed unit plans are essential to the success of a housing project. While the functionality of each unit type is important, the organization of units across a floor plate and their influence on building elevations is equally important
- Appropriately located and proportioned open space is a significant design determinant for most mixed-use and urban housing projects
- We avoid arbitrary façade embellishment. Instead we utilize the organization of individual units and their aggregation to establish the pattern and rhythm of multi-family facades that is furthered informed by site organization and orientation. Plans correlate to elevations and variation occurs within an established system
- The constrained budgets for typical mixed-use projects demand careful consideration of a project’s primary orientation and configuration to provide cost effective sustainable design strategies
- The scale and proportion of new mixed-use buildings must address, but need not directly reflect, those of adjacent structures. Plan, section, and elevation strategies should be integrated to provide a comprehensible “read” of the building’s composition and organization



**1 Agnes Lofts**, 1433 12<sup>th</sup> Avenue  
**2 19th and Mercer Mixed-Use Building**, 526 19<sup>th</sup> Avenue E  
**3 1205 NE 66<sup>th</sup> St Mixed-Use Building**, (unbuilt)  
**4 The Rooster Mixed-Use Building**, 900 NE 65<sup>th</sup> Street (under construction)  
**5 Ventana at the Market**, 2100 Western Ave  
**6 SCCA Patient House**, 207 Pontius Ave N  
**7 Compass Center Housing**, 1753 NW 56<sup>th</sup> Street  
**8 Belroy Apartments**, 703 Bellevue Ave E  
**9 Banner Building**, 2600 Western Avenue



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