

5244 LEARY AVE NW



DESIGN TEAM | INFO

ARCHITECT | SKIDMORE JANETTE ARCHITECTURE PLANNING & DESIGN

OWNER | EAGLE PARKING LLC

MUP # | 3025228

CONTENTS

1 - 2	COVER & CONTENTS	28 - 29	BALLARD FRAMEWORK
3	AERIAL PHOTO/MAP	30 - 31	CONCEPTUAL DESIGN OPTIONS
4 - 5	DESIGN OBJECTIVES	32 - 35	OPTION A "BOOKEND HORIZONTAL"
6 - 10	NEIGHBORHOOD ANALYSIS	36 - 39	OPTION B "THE 2 OVER 3"
11 - 17	SITE ANALYSIS	40 - 43	OPTION C "BOOKEND FRAME" PREFERRED
18 - 23	CHARACTER ANALYSIS	44	DESIGN COMPARISONS
24 - 25	CODE ANALYSIS	45	SHADOW STUDY
26 - 27	DESIGN GUIDELINES - CITY OF SEATTLE	46	ARCHITECT & CLIENT WORK SAMPLES

VICINITY MAP



AERIAL MAP



DESIGN OBJECTIVES | PROJECT GOALS & PROGRAM

The enclosed project is a proposed 5-story building featuring ground floor hotel lobby, restaurant/retail space, live/work spaces and access to 3 levels of below grade parking and 4 stories of hotel, conference and rooftop pavilion and greenhouse. Located in the heart of Ballard, the project is designed for an active street level restaurant/retail/hotel lobby experience while providing upper-level hotel rooms that would serve the hospital and the greater Ballard neighborhood. The project intends to act as an introduction to the retail and historic portions of the neighborhood, both in physical location and in the aesthetic of the design.

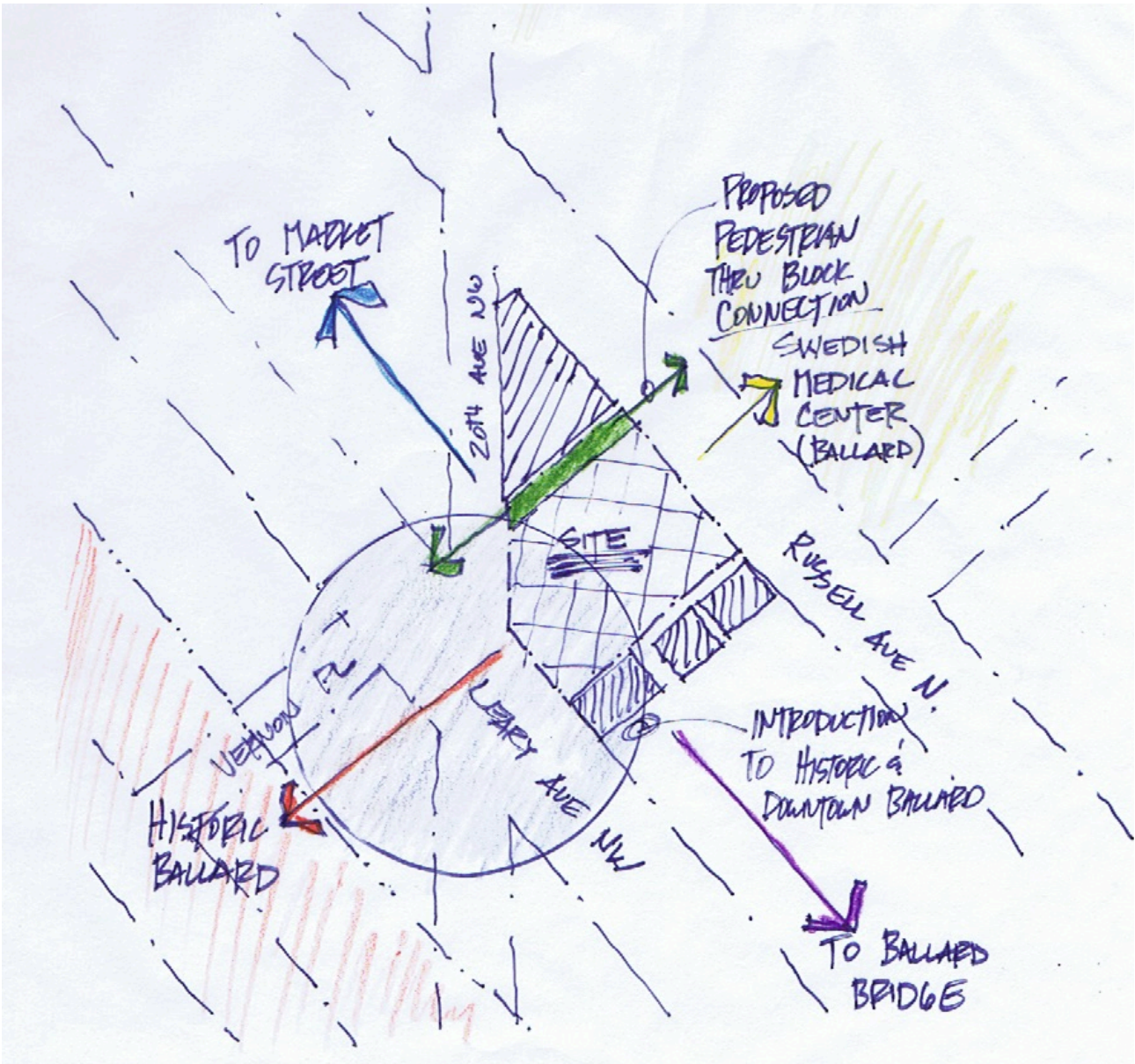
The 25,373 square foot site is located at the intersection of Leary Ave NW and 20th Ave NW and also has 150 feet of frontage on the south side of Russell Ave NW. Currently on the site and scheduled to be demolished is a mechanic shop and surface parking lot for members of the athletic club across Leary Ave NW. Directly to the north is the 2-story Salmon Bay Eagles Aerie Building as well the 2-story historic brick Louisa Building. To the south is the 3-story wood structure that contains the Senor Moose Mexican Restaurant and residential units above as well as a small home with an accessory structure. The proposed project will have close ties to the Olympic Athletic Club and Hotel Ballard across Leary Ave NW. A pedestrian through lot connection has been included on the north side of the property to reinforce the link from the hospital and other neighborhood businesses to the Historic Ballard District.

Our program consists of 3 stories of underground parking with up to 230 parking stalls, accessed from Russell Ave NW. The hotel lobby will front along 20th Ave NW or Leary Ave NW, opening out upon a proposed curb bulb at the intersection of 20th Ave NW and Leary Ave NW. The restaurant/retail space will open to the sidewalk at Leary Ave NW or 20th Ave NW, providing ample space for an outdoor seating area. Live/Work spaces will have frontage along Russell Ave NW. The upper stories will consist of up to 101 hotel rooms with an interior space allocated for meeting rooms, in-house support space and other amenities. A large meeting room/pavilion will be on the top floor, along with an outdoor area provided to take advantage of the views of the Olympic Mountains, Salmon Bay, Old Ballard and surrounding neighborhood. The upper roof will contain a greenhouse, green roof, solar area and another opportunity for views in all directions.

The neighborhood is a mix of neighboring commercial, residential and mixed-use buildings, high density residential and the Ballard Ave Landmarks district. Our proposal will act as an introduction to the historic district by blending some of the characteristics of those existing structures at the base with a more modern aesthetic on the upper levels. Since there is no clear vernacular or style for the neighborhood, our project will take cues from both the historic and modern aesthetic to inform our design. The façade treatment of the base will speak closely to the adjacent historic buildings, both in scale and material, while the upper stories will relate to the newer modern buildings that have been redefining the Ballard Neighborhood in recent years.

With the city’s proposal to redevelop the intersection of Leary Ave NW and 20th Ave NW with a stop light, cross walks and curb bulbs, the corner will become much more pedestrian friendly. By placing the hotel lobby and restaurant/retail entrances near this intersection and moving the parking/service entrance along Russell Ave NW, we are able to direct vehicular traffic away from this intersection. The proposed pedestrian through lot connection to the north offers the opportunity to provide a pleasant pedestrian link from the hospitals and neighboring residential area to the improved crossing access across Leary Ave NW and into the Ballard Historic District. In our preferred scheme we take advantage of this thoroughway to provide potential outdoor seating for the restaurant use at the north of the site.

By implementing the proposed changes in the Ballard Rezone and Development Standards; the scale and use of durable materials on the base of the building, the upper floor setbacks for light and air, the modulation of the façade and the use of punched openings, decks, planters and balconies; all of which can relate to the commercial and industrial history of the Ballard neighborhood as well as speak to the newer neighborhood development.



CONTEXT PLAN

Overview

Site Area: 25, 373 SF
Zone: MIO/NC3-65
Far: 4.25
Parking: Up to 217
Hotel Units: Up to 101

Square footages:

Restaurant/Retail: Up to 9,500 SF + 4,750 SF Mezz.
Live/Work: Up to 2,300 SF + 1,200 SF Loft
Parking: 78,000 SF-3 levels of underground parking
Hotel: 86,000 SF
Building: 106,100 SF (Contributing FAR)

CONCEPT & INSPIRATION



STONE BASE, RHYTHM OF WINDOWS



ARCHED WINDOWS, PROMINENT ENTRY



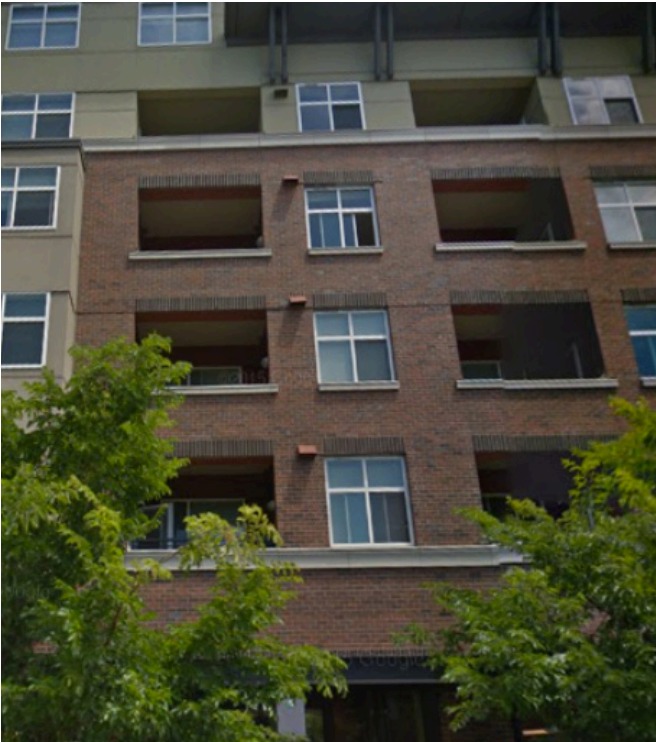
MIX OF MODERN, AND TRADITIONAL MATERIALS



DECORATIVE, PRONOUNCED AWNING



MODERN BUILDING ATOP A HISTORIC BASE



RECESSED BALCONIES, BRICK



MODERN AWNING



DETAILED AND DECORATIVE STONE, ARCHED WINDOW

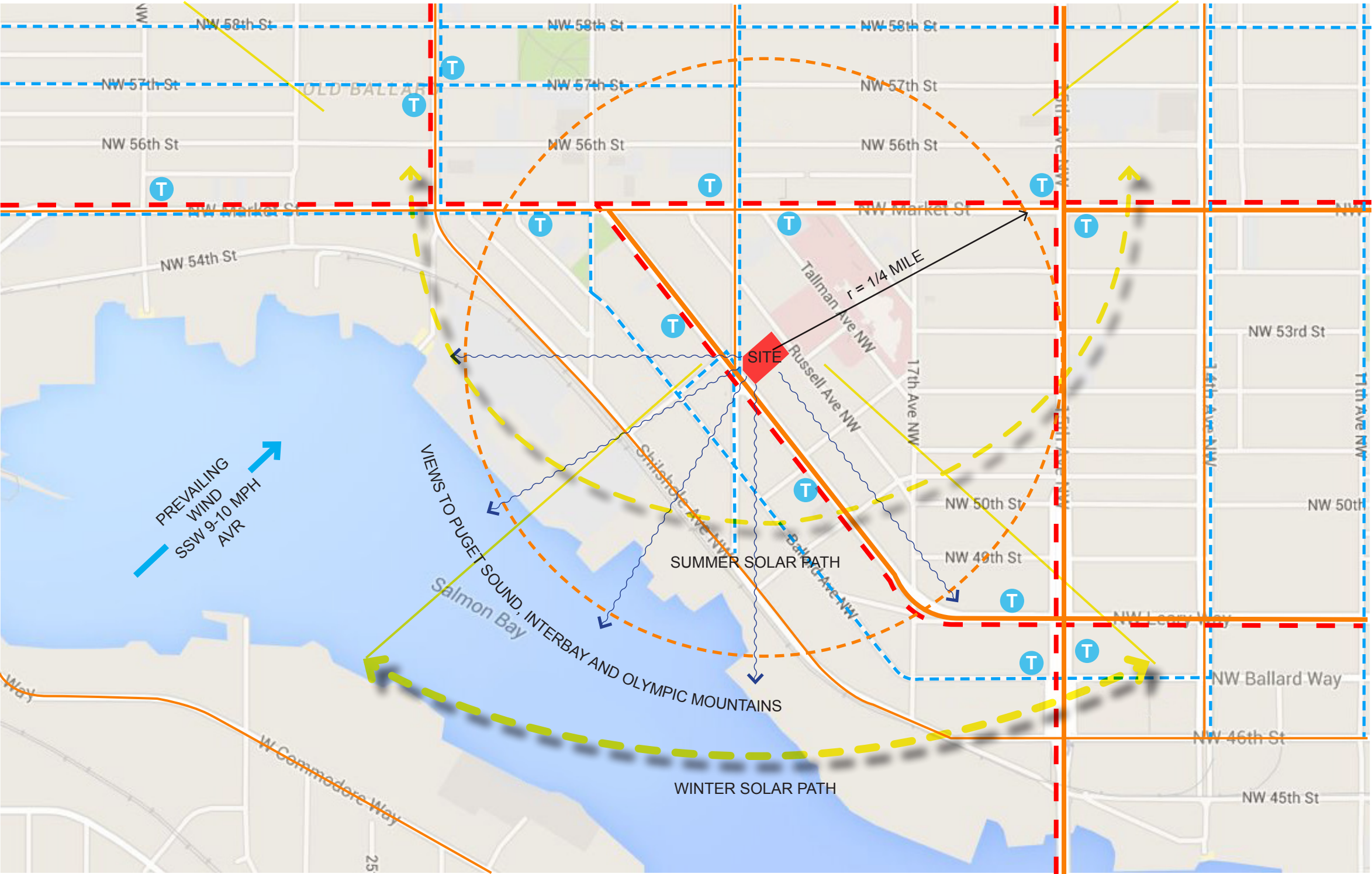
CIRCULATION, TRANSIT, & ENVIRONMENTAL ANALYSIS

KEY

- MAIN
- ARTERIAL
- BIKE ROUTE / LANES
- NEARBY TRANSIT STOP
- TRANSIT ROUTE
- VIEW OPPORTUNITIES

ANALYSIS | The site is located in a dense highly active neighborhood along a main arterial and has street frontage on three sides positioned to the Southwest. Views will be territorial to the Southwest with upper floors having clear views towards the South and West to Salmon Bay, The Locks, and the Olympic Mountains. The site is oriented for good Southern and Eastern sun exposure with upper floors having a Western exposure as well.

CONCLUSION | Providing a Hotel in the Ballard Hub Urban village, adjacent to a main arterial, and in a neighborhood with an increasing density fills a need for the neighborhood. Siting back from the adjacent building to the North allows for us to provide a link from the hospital to the historic portion of Ballard and will give the upper floors views and light from all directions.



NEIGHBORHOOD & AMENITIES

KEY

- ////

HIGH ACTIVITY CORRIDOR / PRIMARY ARTERIAL
-

NEIGHBORHOOD / SECONDARY ARTERIAL
- BALLARD HUB URBAN VILLAGE
- BALLARD / INTERBAY NORTH END
MANUFACTURING & INDUSTRIAL
- BALLARD AVENUE LANDMARK
DISTRICT
- TALL DEVELOPMENT (4+ STORIES)
- 01

BALLARD HISTORIC DISTRICT
*INCLUDES MANY DINING AND SHOPPING
OPPORTUNITIES*
- 02

HOTEL BALLARD
- 03

BALLARD FARMER'S MARKET
- 04

BERGEN PLACE
- 05

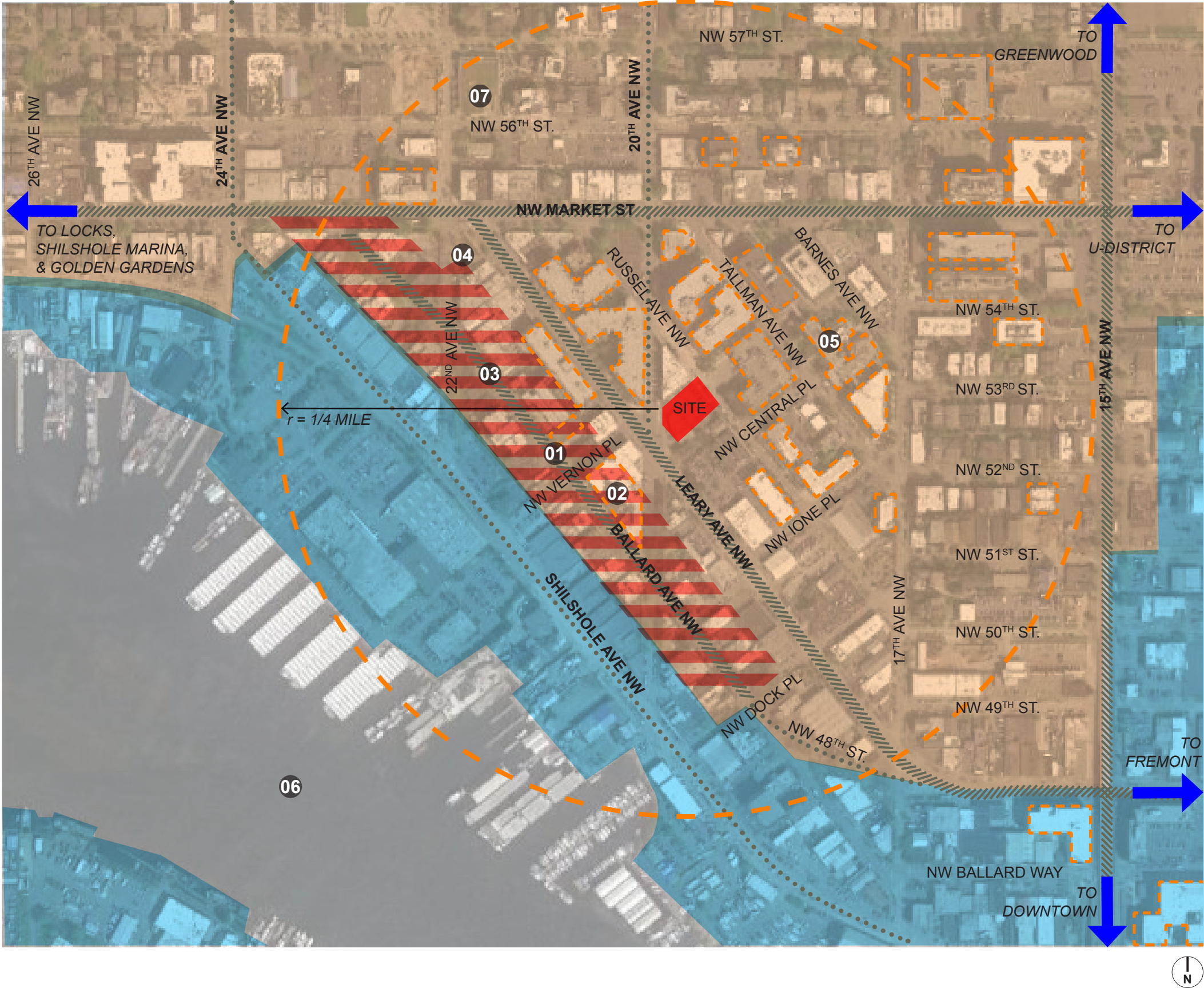
SWEDISH MEDICAL CENTER
- 06

SALMON BAY
- 07

BALLARD PUBLIC LIBRARY

ANALYSIS | Our site is located on a main arterial in Ballard, just outside of the historic district, with close proximity to many restaurants and retail shops as well as nearby medical facilities. In the immediate area adjacent to the property there are many stand-alone restaurants/retail/commercial developments mixed in with multi-family residential buildings.

CONCLUSION | The site lends itself to being developed to expand and stitch together the pedestrian and historic center of Ballard towards Leary Ave NW to activate an area that otherwise acts as a barrier, getting people from one of Ballard's many destinations to another. The addition of a new hotel in Ballard will fill a need for the area as it continues to develop into a “must-visit” destination neighborhood.



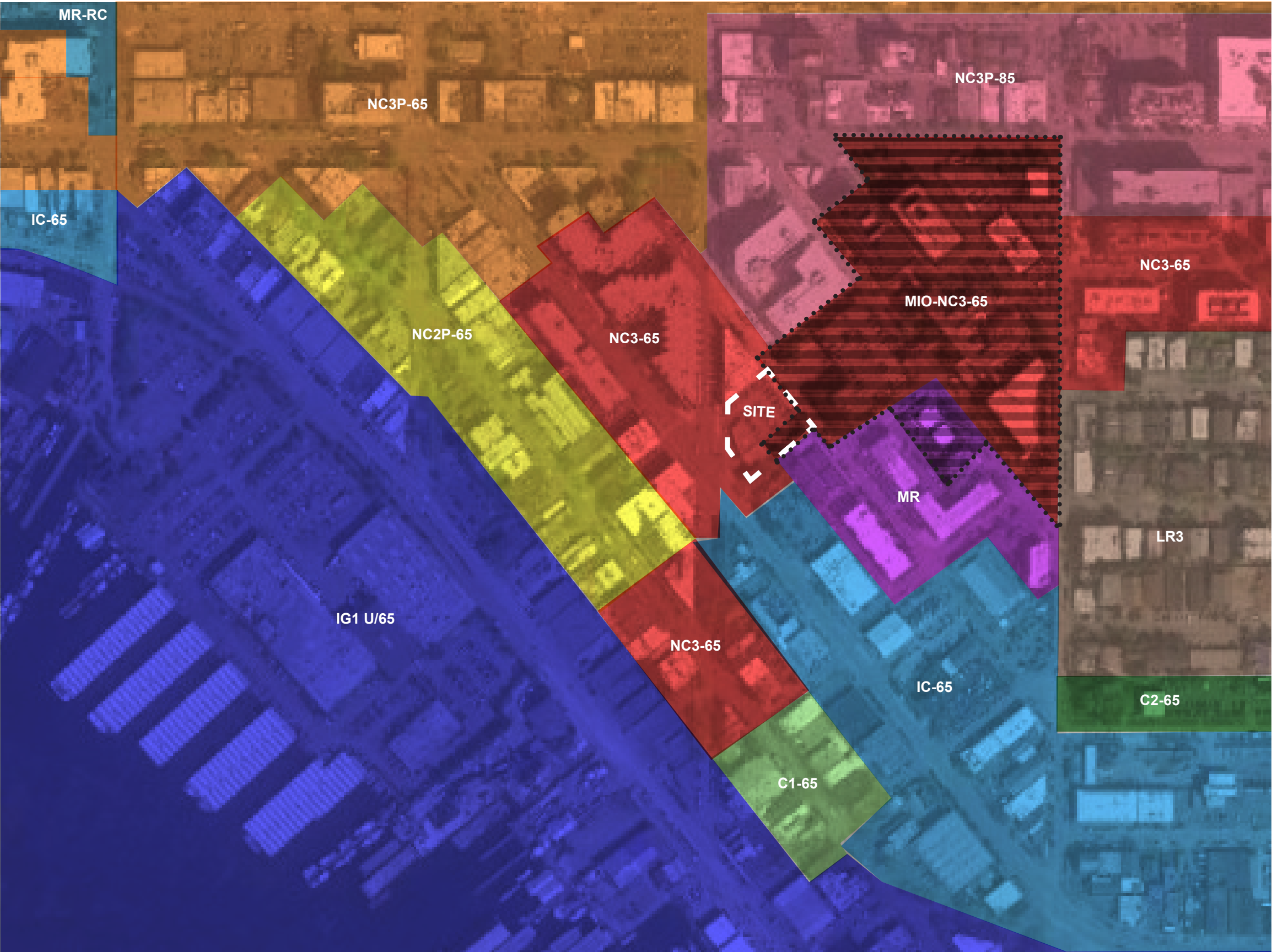
ZONING

KEY

- NEIGHBORHOOD COMMERCIAL 2 PEDESTRIAN
- NEIGHBORHOOD COMMERCIAL 3
- NEIGHBORHOOD COMMERCIAL 3 PEDESTRIAN
- NEIGHBORHOOD COMMERCIAL 3 PEDESTRIAN 85
- INDUSTRIAL COMMERCIAL 1
- GENERAL INDUSTRIAL 1
- MID RISE
- LOW RISE 3
- MID RISE - RETAIL COMMERCIAL
- COMMERCIAL 1
- COMMERCIAL 2
- MAJOR INSTITUTION OVERLAY

ANALYSIS | Our site is in an NC3-65 + MIO/NC3-65 zone adjacent to Swedish Medical Center Ballard. Close to other retail, residential, and restaurant uses the proposed site uses with complement the current and future development in Ballard.

CONCLUSION | Encompassing the site is NC3-65 to the North and West, MIO/NC3-65 to the East and MR to the South, making it an ideal place for a hotel use.



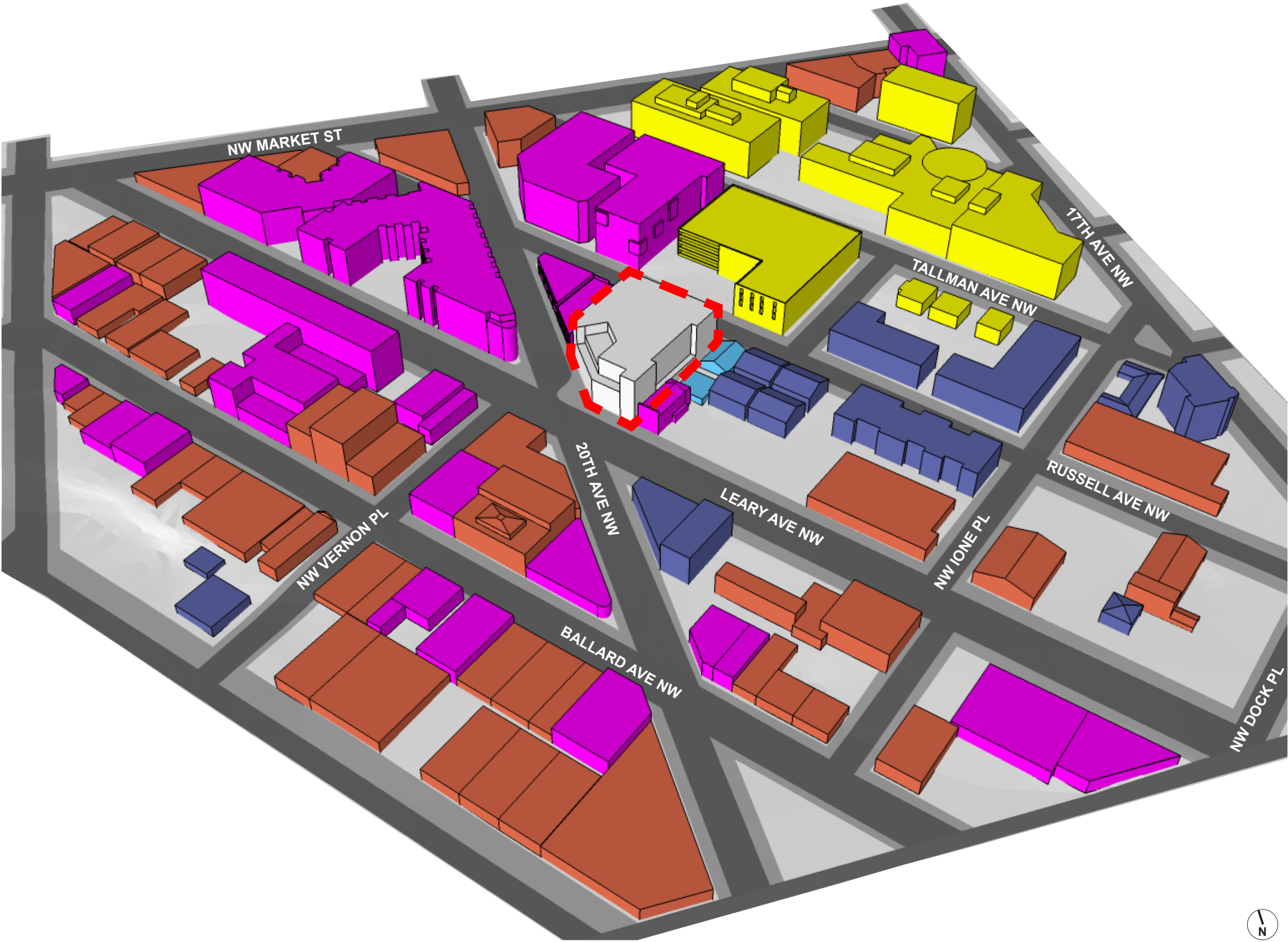
CONTEXT MASSING & ADJACENT USES

KEY

- MIXED USE
- COMMERCIAL
- MULTI-FAMILY RESIDENTIAL
- SINGLE FAMILY RESIDENTIAL
- INSTITUTIONAL/PARKING GARAGE
- PARKING
- SITE

ANALYSIS | The majority of the sites in the surrounding 9 block area are a mix of multi-family, mixed use & commercial buildings, with a large block of medical buildings to the east. To the Northwest are recently developed, larger-scale buildings, and to the East and South the massings begin to step down to smaller-scale, residential, commercial & historic buildings.

CONCLUSION | Develop a building that will act as a transition between the large-scale, more modern developments to the North and West, and the smaller scale, historic buildings to the South and East, while providing a use that will fill a need for those coming to the neighborhood as Ballard continues to develop as a destination, and to those needing close access to the nearby hospitals.



NEIGHBORHOOD CONTEXT PHOTOS



NEIGHBORHOOD VICINITY MAP



01. OLYMPIC ATHLETIC CLUB
5301 LEARY AVE NW



02. SALMON BAY EAGLES AERIE 2141
5216 20TH AVE NW



03. SENOR MOOSE
5242 LEARY AVE NW



04. CONDOS
5227 - 5235 RUSSELL AVE NW



05. CANAL STATION APARTMENTS
5440 LEARY AVE NW



06. BALLARD LANDMARK RETIREMENT
COMMUNITY
5433 LEARY AVE NW



07. ODIN APARTMENTS
5343 TALLMAN AVE NW



08. SWEDISH MEDICAL CENTER
5300 TALLMAN AVE NW



09. CARTER VOLKSWAGEN
5202 LEARY AVE NW



10. CURTIS BUILDING
5227 LEARY AVE NW



11. MASSAGE HEIGHTS
5400 LEARY AVE NW



12. CARTER SUBARU
5201 LEARY AVE NW

NEIGHBORHOOD CONTEXT | SUMMARY

The neighborhood is a mix of neighborhood commercial with mostly mixed-use buildings and the Swedish Medical Center, high density residential and the Ballard Ave Landmarks District. Our proposal will act as a transition to the historic district by blending some of the characteristics of a historic structure at the base with a more modern aesthetic on the upper levels. Without a clear vernacular for the neighborhood our project will take cues from both the historic and modern aesthetic to inform our design.

SITE CONTEXT PHOTOS



SITE VICINITY MAP

SITE CONTEXT | SUMMARY

- Existing site has a mechanic shop and a surface parking lot, slated to be demolished.
- Directly to the north is the 2-story Salmon Bay Eagles Aerie Building as well the 2-story historic brick Louisa Building.
- To the south is the 3-story wood structure that contains the Senor Moose Mexican Restaurant and residential units above as well as a small home with an accessory structure.
- East of the site is Swedish Medical Center Ballard Campus.
- Directly across Leary Ave NW is the Olympic Athletic Club and Hotel Ballard
- North and West of our block are high-density condominiums and apartments. South and East of our lot is mostly mixed use properties – multi-family housing, commercial, industrial, etc.



01. LOOKING NE FROM LEARY AVE NW



02. LOOKING N FROM LEARY AVE NW



03. LOOKING SOUTHWEST FROM RUSSEL AVE NW



04. LOOKING SW FROM TOP OF PARKING GARAGE



05. LOOKING EAST AT PARKING LOT FROM 20TH



06. LOOKING SOUTHEAST TOWARDS SITE

SITE ANALYSIS

SIZE

25, 373 SF

TOPOGRAPHY

8.5' of elevation change from North to South

RIGHT OF WAYS / STREETS

At the corner of Leary Ave NW & 20th Ave NW and has 150' of frontage on the south side of Russel Ave NW

SETBACKS

AVG 10'-0" Upper floor setback at 45'-0"
Max 100'-0" Facade length at Russel Ave NW

LEGAL DESCRIPTIONS:

PARCEL A: 5208 20TH AVE NW

Tax lot #276770-3410
Lots 3 and 4, and that portion of lot 23 in block 78, Gilman Park, according to the plat Thereof recorded in volume 3 of plats, page(s) 40, in King County, Washington, lying between The northwesterly and southeasterly lines of said lot 3, extended southwesterly to 20th Avenue Northwest

PARCEL B: 5307 RUSSELL AVE NW

Tax lot #276770-3420
Lot 5, block 78, Gilman Park, according to the plat thereof recorded in volume 3 of plats, Page(s) 40 in King County, Washington

PARCEL C: 5244 LEARY AVE NW

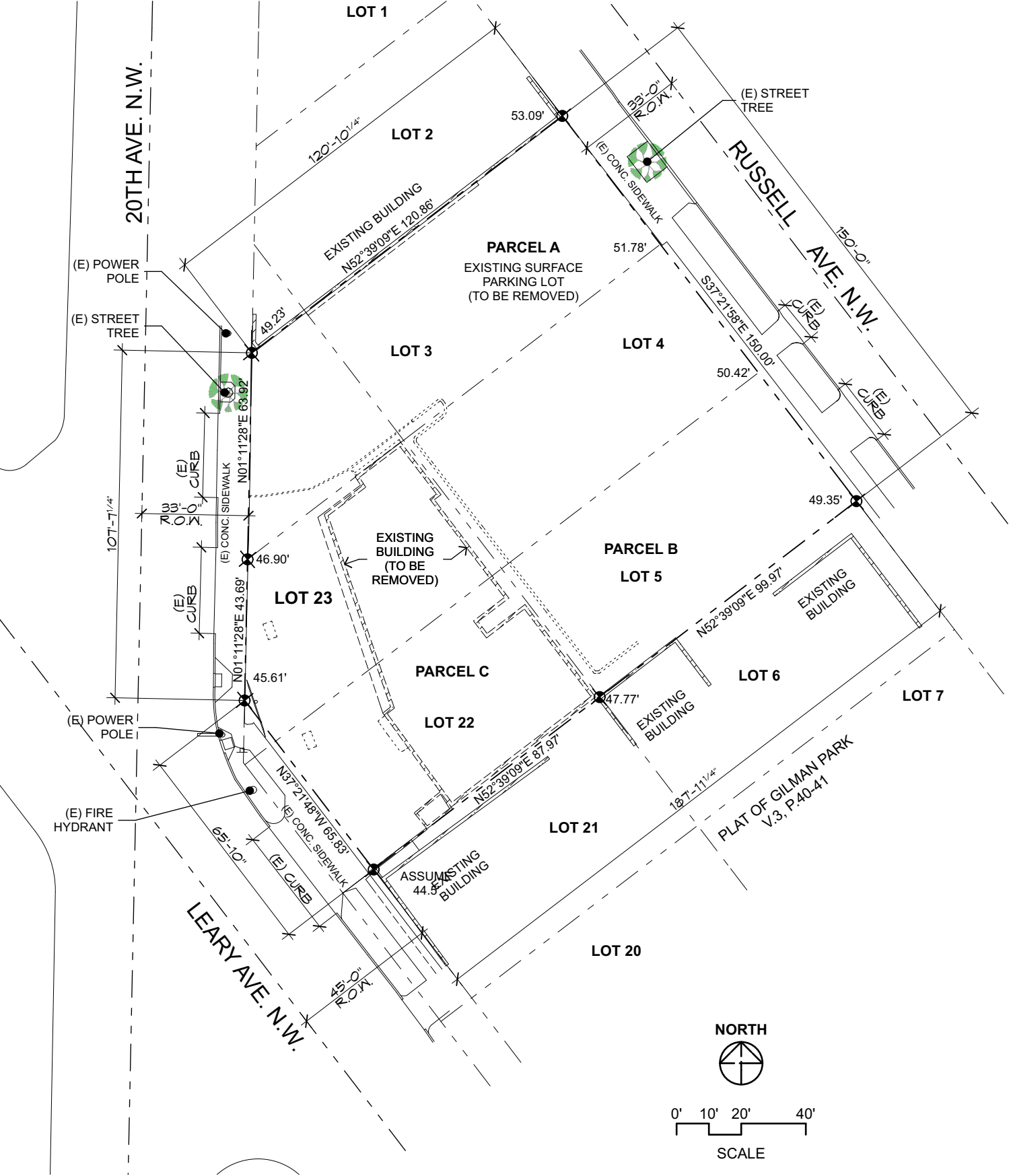
Tax lot #276770-3505
Lot 22 and that portion of lot 23, block 78, map of Gilman Park, according to the plat Thereof recorded in volume 3 of plats, page 40, records of King County auditor, lying Southeasterly of the line between lots 3 and 4 of said block 78 produced southwesterly; Except the southwesterly 12 feet thereof heretofore condemned in King County Superior Court Case no. 69865 For widening of Leary Avenue, as provided by ordinance no. 21303 Of the City of Seattle; situate in the City of Seattle, County of King, State of Washington

KEY

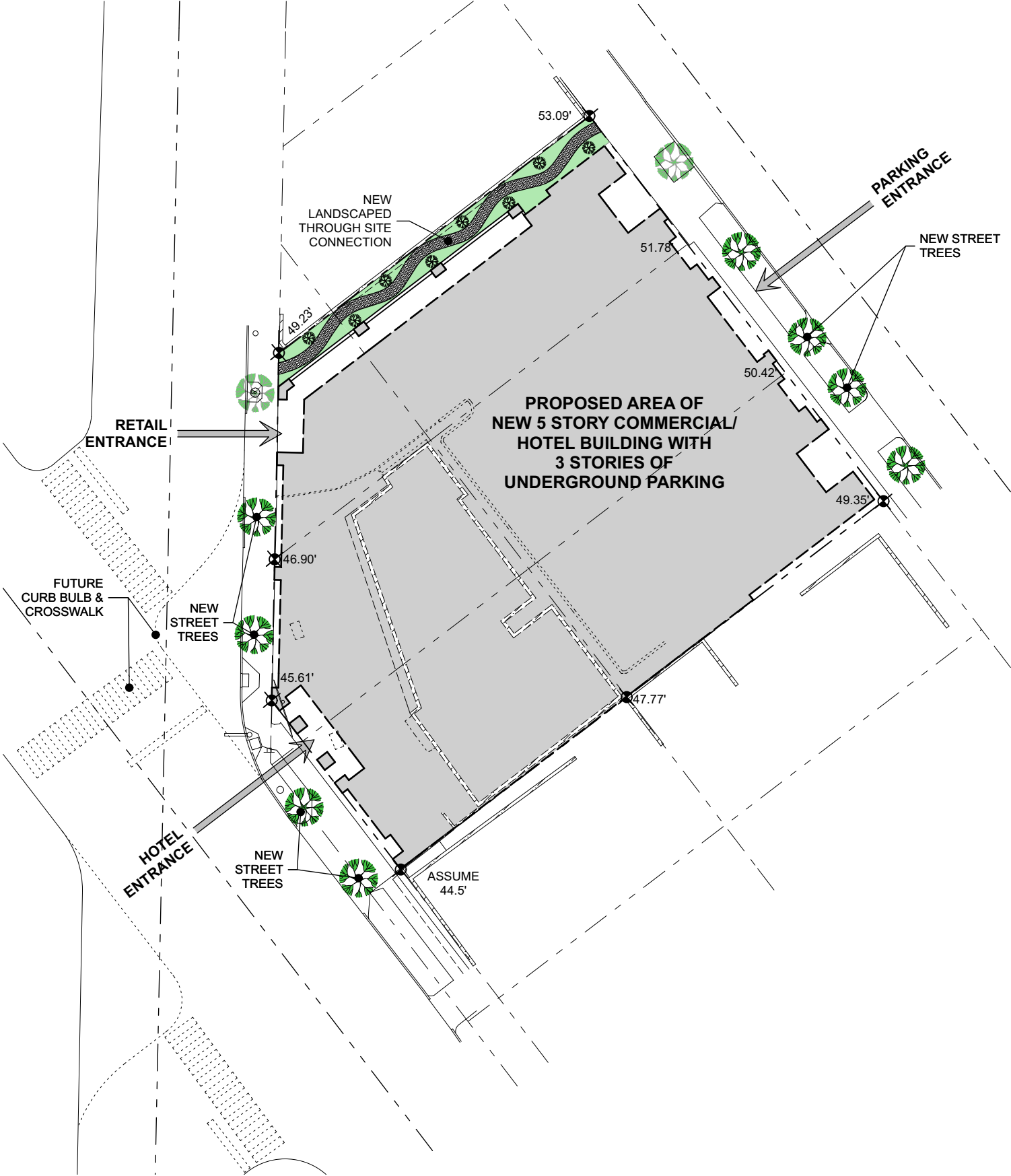
- PROPERTY LINE
- OUTLINE OF EXISTING BUILDING TO BE DEMOLISHED
- REQUIRED SETBACKS
- EXISTING TREES & LANDSCAPING
- EXISTING HIGH VOLTAGE POWER LINES
- EXISTING LOW VOLTAGE POWER LINES
- FUTURE PEDESTRIAN THROUGH-BLOCK CONNECTION



SITE PLAN



EXISTING GROUND PLANE LANDSCAPING PLAN



PROPOSED GROUND PLANE LANDSCAPING PLAN





STREETSCAPE OF SITE ALONG LEARY AVE NW

SITE



STREETSCAPE ALONG LEARY AVE NW



STREETSCAPE OF SITE ALONG 20TH AVE NW

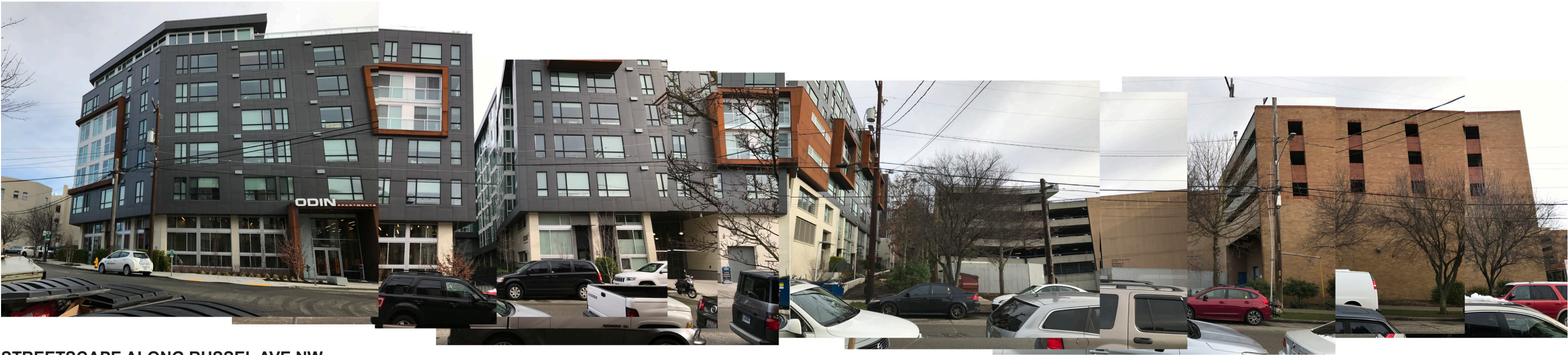


STREETSCAPE ALONG 20TH AVE NW



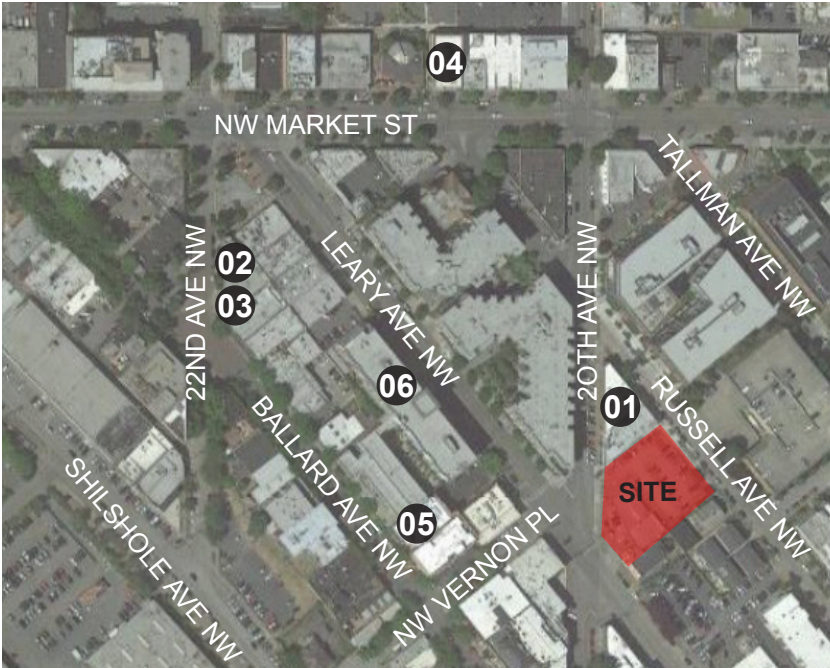
STREETSCAPE OF SITE ALONG RUSSELL AVE NW

SITE



STREETSCAPE ALONG RUSSEL AVE NW

NEIGHBORHOOD DESIGN CUES



NEIGHBORHOOD VICINITY MAP



01. LOUISA BUILDING
DISTINCT BASE & TOP, RHYTHMIC WINDOW PATTERNING, LARGE STOREFRONT



02. 5408 22ND AVE NW
PILASTERS PROUD OF MAIN BUILDING PLANE, EQUALLY SPACED
FRAME EXPRESSION AROUND STOREFRONT



03. 5408 22ND AVE NW
STRONG CORNER ELEMENT, TOWER FEATURE,
DIFFERENT WINDOW EXPRESSION AT THE TOWER



04. 2020 NW MARKET ST.
STRONG CORNER ELEMENT, TOWER FEATURE,
WINDOW & ENTRY COLLECTED IN TOWER

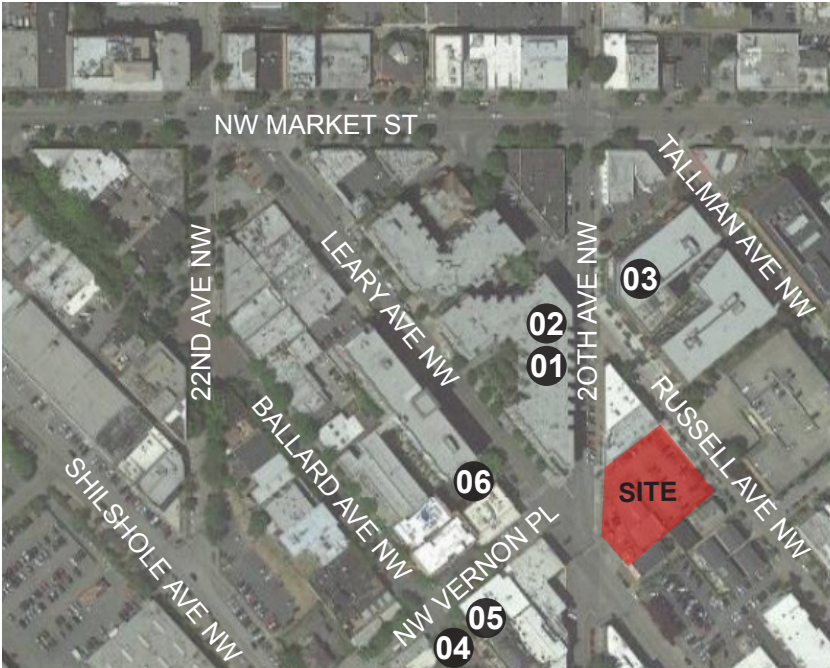


05. VALHALLA BUILDING
DISTINCT TOP & BASE EXPRESSIONS,
FRAME ELEMENTS EXPRESSED THROUGH
BRICK DETAILING & MATERIAL COLOR
CHANGE



06. 5433 LEARY AVE NW
BRICK FRAME ELEMENT AT BASE,
SIMPLIFIED MATERIAL AT UPPER LEVELS

NEIGHBORHOOD DESIGN CUES



NEIGHBORHOOD VICINITY MAP



01. 5450 LEARY AVE NW
STRONG BASE, MIDDLE & TOP, DURABLE & TRADITIONAL MATERIALS AT THE BASE & MIDDLE, PUNCHED OPENINGS THROUGH BRICK AT BALCONIES



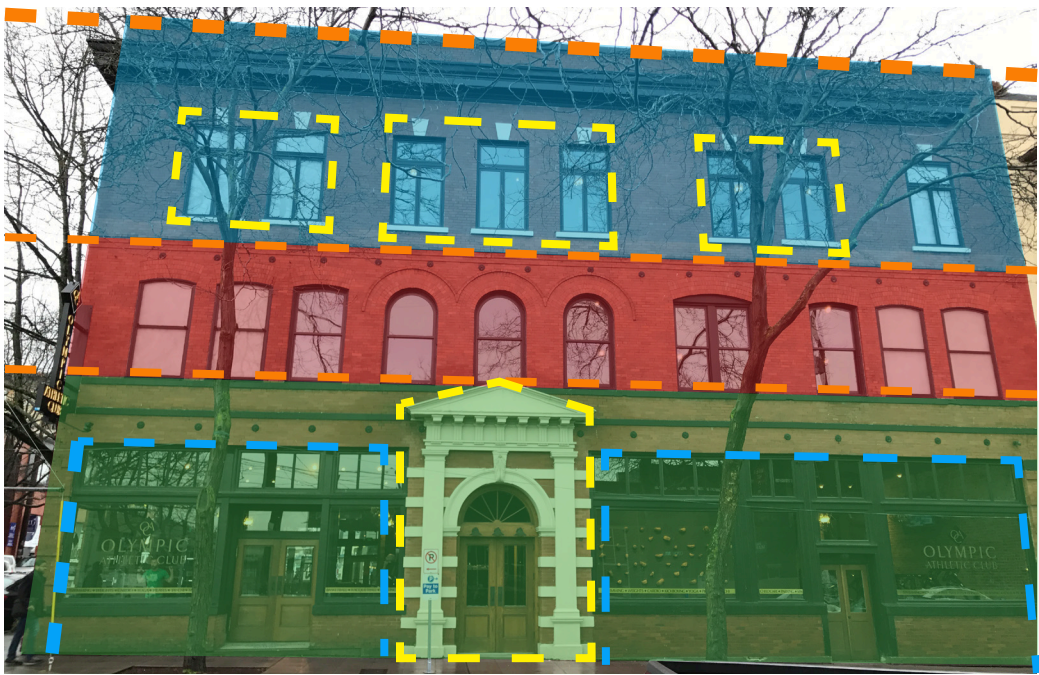
02. 5450 LEARY AVE NW
UPPER LEVELS STEPPED BACK, SIMPLE, NEUTRAL MATERIAL AT UPPER LEVELS



03. ODIN APARTMENTS
UPPER LEVELS STEPPED BACK, SIMPLE, NEUTRAL MATERIAL AT UPPER LEVELS



04. OLYMPIC ATHLETIC CLUB
STRONG BASE, MIDDLE & TOP, RHYTHMIC WINDOW PATTERNING, ELEMENTS WRAP AROUND EACH SIDE OF THE BUILDING



05. OLYMPIC ATHLETIC CLUB
DISTINCT ENTRY EXPRESSION W/ MIXED MATERIALS, BRICK FRAME CREATED AROUND STOREFRONT, RHYTHMIC WINDOW PATTERNING



06. 5405 LEARY AVE NW
GROUPED WINDOWS, FRAME ELEMENT

NEIGHBORHOOD DESIGN CUES



NEIGHBORHOOD VICINITY MAP



01. 5450 LEARY AVE NW
EMPHASIZED ENTRY,
DIFFERENTIATION OF MATERIAL



02. LOUISA BUILDING
EMPHASIZED ENTRY, PILASTER
FRAMED



03. OLYMPIC ATHLETIC CLUB
MIX OF MATERIALS &
PATTERNING AT ENTRY, DOUBLE
DOORS



04. 5308 BALLARD AVE NW
PATTERNING AT ENTRY, ADDED
TEXTURE & INTEREST



05. LOUISA BUILDING
PAIRING OF WINDOWS, ARCHED



06. 5450 LEARY AVE NW
PUNCHED VOIDS IN BRICK VOLUME
AT BALCONIES



07. 2020 MARKET ST. NW
REPETITION OF WINDOW SIZE & SHAPE

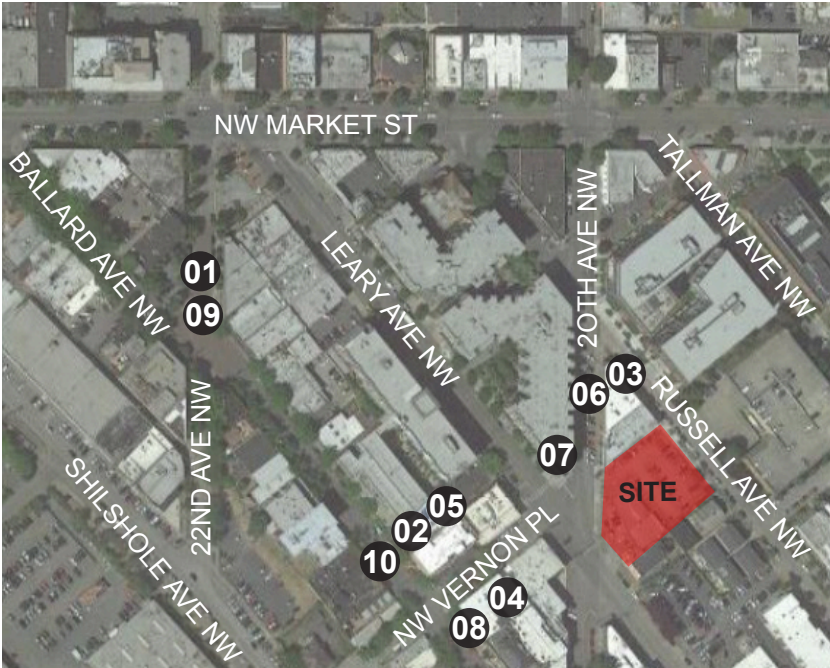


08. VALHALLA BUILDING
MODERN LINTEL & SILL



09. OLYMPIC ATHLETIC CLUB
COMBINATION OF ARCHED &
RECTANGULAR WINDOWS

NEIGHBORHOOD DESIGN CUES



NEIGHBORHOOD VICINITY MAP



01. 5408 22ND AVE NW
SIMPLIFIED CORNICE DETAIL,
MIX OF BRICK COLORS



02. VALHALLA BUILDING
STACK BOND BRICK, FRAME



03. LOUISA BUILDING
BRICK BANDING DETAIL AT FLOOR
LEVELS



04. OLYMPIC ATHLETIC CLUB
CONTRASTING BANDING DETAIL



05. VALHALLA BUILDING
ARCHED WINDOWS, SIMPLIFIED
CORNICE



06. LOUISA BUILDING
SIMPLIFIED BASE W/ BRICK
ABOVE



07. 5450 LEARY AVE NW
CONCRETE BASE W/ BRICK
ACCENT PATTERN



08. OLYMPIC ATHLETIC CLUB
CORNER DETAIL ADDING DEPTH

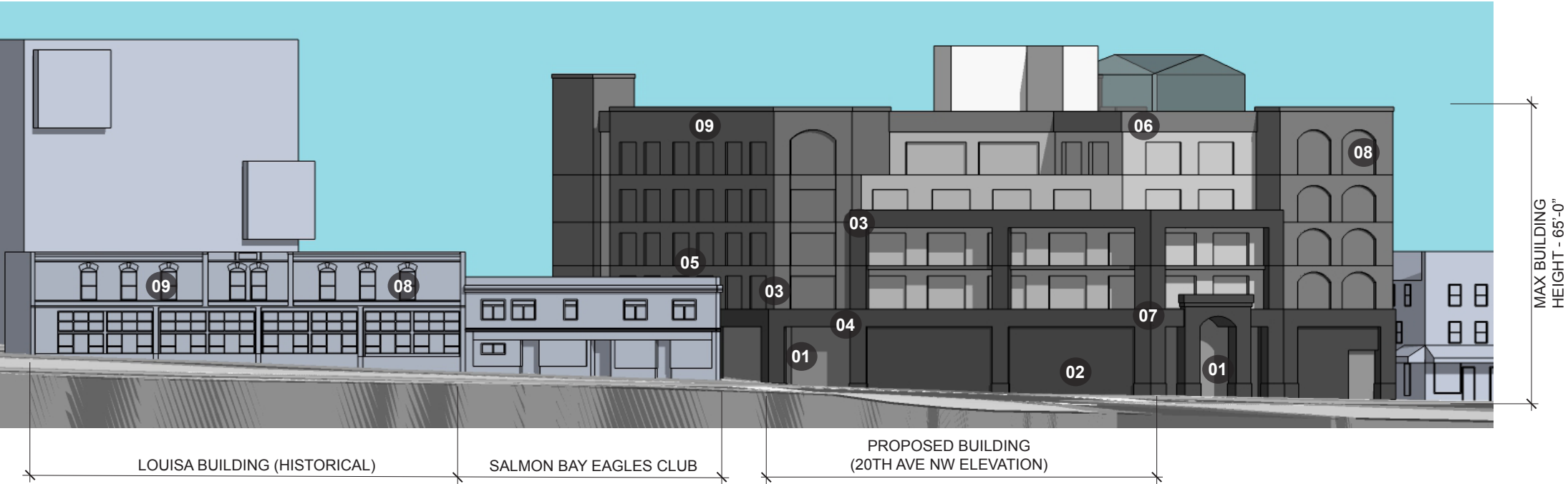


09. 5408 22ND AVE NW
DETAILED BASE AT RETAIL W/
MOLDING



10. VALHALLA BUILDING
MODERN BUILDING BASE, DETAILED BASE AT RETAIL GIVING
DEPTH & INTEREST

NEIGHBORHOOD SECTIONS



STREETSCAPE ALONG 20TH AVE NW

- KEY**
- 01 RECESSED HOTEL & RETAIL ENTRIES (TRANSPARENT)
 - 02 OPERABLE TRANSPARENT DOORS TO ALLOW INSIDE-OUTSIDE CONNECTION
 - 03 VARIED BASE HEIGHT OF BUILDING TO VISUALLY RELATE TO THE BUILDING TO THE NORTH
 - 04 ALTERNATING AWNING HEIGHT TO HIGHLIGHT ENTRIES & LOWER SCALE AT TRANSPARENT STOREFRONT
 - 05 RELATE TO DATUM OF BUILDING TO THE NORTH
 - 06 10'-0" AVERAGE SETBACK AT 45'-0"
 - 07 DURABLE MATERIALS TO CONNECT THE BUILDING TO THE HISTORIC NATURE OF THE NEIGHBORHOOD
 - 08 ARCHED WINDOWS
 - 09 STRUCTURED WINDOW RHYTHM



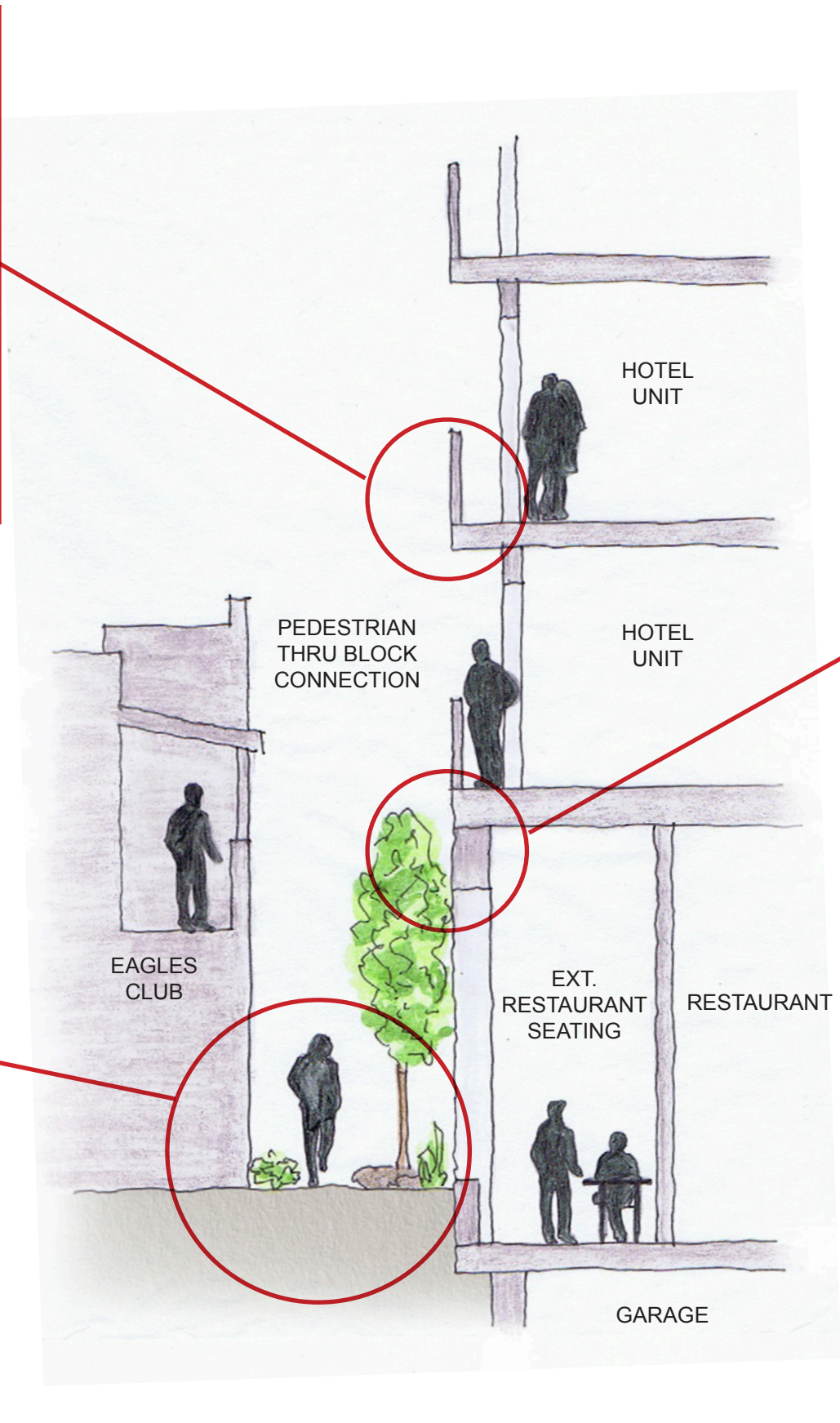
STREETSCAPE ALONG LEARY AVE NW



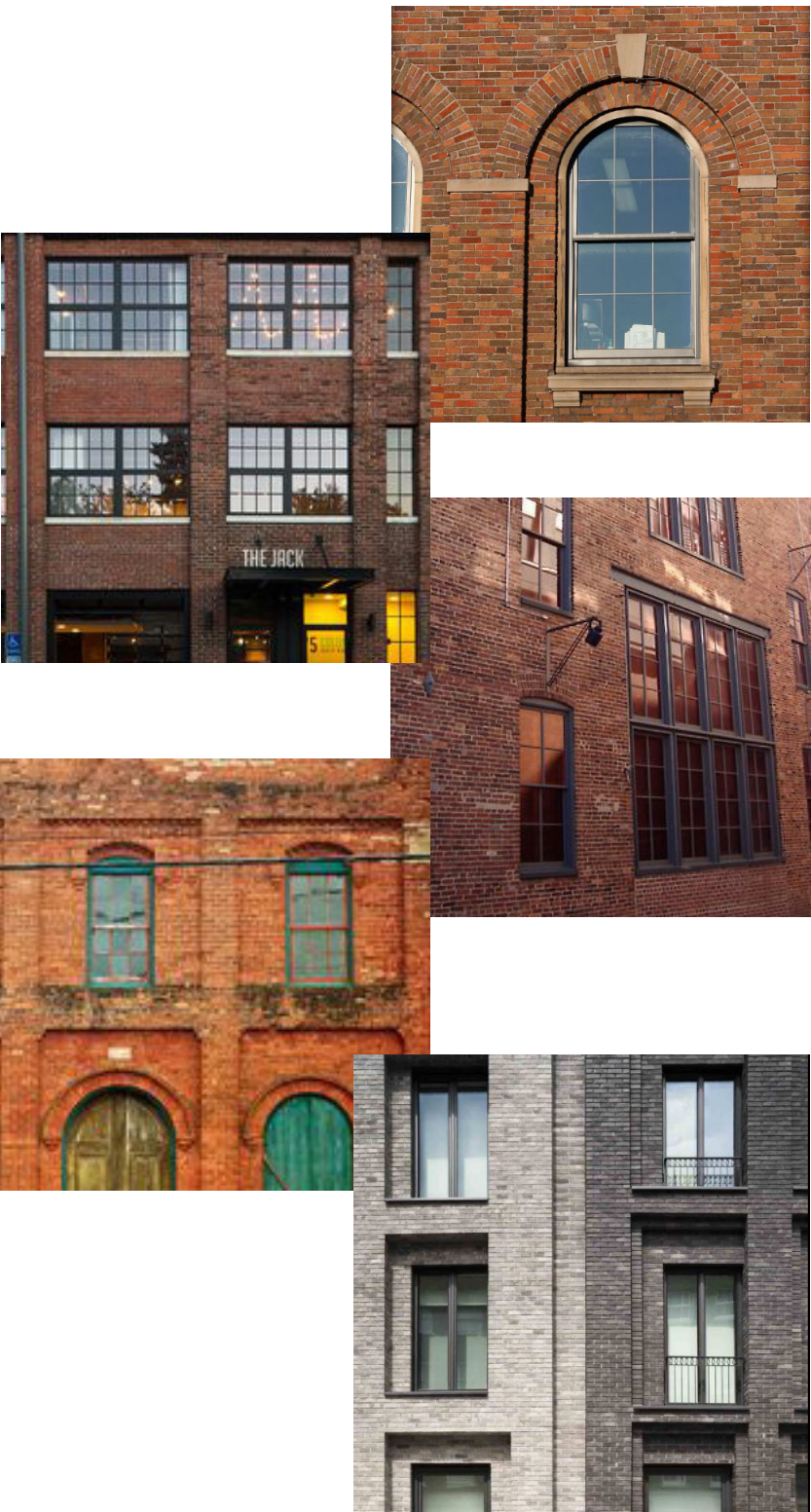
BALCONY INSPIRATION



PEDESTRIAN THRU BLOCK CONNECTION



SECTION AT MID-BLOCK CONNECTION



FACADE DETAILING INSPIRATION

ZONING SUMMARY | SEATTLE MUNICIPAL CODE TITLE 23

Seattle Municipal Code Title 23
Zoning C1-65 & MIO-65/NC3-65

Requirements for Commercial-1 65 & Neighborhood Commercial-3 65 Zones:
SMC 23.47A
Major Institution Overlay District – 65
SMC 23.69

SMC 23.47A.002 – Scope of Provisions:
C. Some land in C & NC zones may be regulated by subtitle III, Division 3, Overlay Districts.

SMC 23.47A.004 - Permitted Uses:
B. Table A Permitted Uses:
For C1-65 & NC3-65:
C.2.b. Restaurants - Permitted
C.6. Lodging Uses - Permitted
C.10.1. Retail sales and services, general – Permitted
F. Live Work Units – Permitted (Subject to subsection 23.47A.004.G.
G.1 – Live work units permitted outright
G.4 – Live work units shall be deemed a nonresidential use.

SMC 23.47A.005 – Street Level Uses
No restriction on uses other than residential. No residential uses proposed

SMC 23.47A.008 – Street-level Development Standards
A.1.b – Provisions of this section apply to Structures in NC Zones.
A.2. Blank Facades
b. Blank segments of the street-facing façade between 2 and 8 feet above the sidewalk may not exceed 20’ in width.
c. Total of all blank façade segments may not exceed 40 percent of the width of the façade of the structure along the street.
A.3. Street-level street-facing facades shall be located within 10’ of the street lot line, unless wider sidewalks, plazas, or other approved landscaped or open spaces are provided.
B. Non-residential street-level requirements:
B.1.a. Structures with street-level non-residential uses in NC zones.
B.2. Transparency
a. 60% of street facing façade between 2 and 8’ above sidewalk shall be transparent. Width of a drive way at street level, not to exceed 22 feet, may be subtracted from the width of the street-facing façade if the access cannot be provided from an alley or from a street that is not a designated principal pedestrian street.
b. Transparent areas of facades shall be designed and maintained to provide views into and out of the structure.
B.3. Depth provisions, Non-residential use shall extend an average depth of 30 feet and a minimum depth of 15’ from the street-facing façade.
B.4. Height provisions, Non-residential uses at street level shall have a floor-to-floor height of at least 13’.
C. Does not currently apply. Not a pedestrian designated zone.
D. Does not apply. No residential uses.
E.1 Portion of each live-work unit in which a business is conducted must be located between the principal street and the residential portion of the live-work unit. The non-residential portion of the unit shall extend the width of the street-level street-facing façade, shall extend a minimum depth of 15 feet from the street-level street-facing façade and shall not contain primary features such as kitchen, bathroom, sleeping, or laundry facilities.
E.2. Each live work unit must include an exterior sign with the name of the business associated with the unit. Sign to be visible to pedestrians outside the building
E.3. Owner of each live work unit must keep a copy of the current business license associated with the business located in that unit on file.

SMC 23.47A.009.F – Standards applicable to specific areas
2. Facade modulation
a. Facade modulation requirements apply to all portions of a street-facing facade of a structure up to a height of 45 feet located within 10 feet of a street lot line, according to provisions of subsection 23.47A.009.F.2.c
b. The maximum width of any unmodulated street-facing facade is 100 feet. Facades longer than 100 feet shall be modulated at no greater than 100-foot intervals by stepping back the facade from the street lot line for a minimum depth of 10 feet and a minimum width of 15 feet.
c. Facade modulation requirements do not apply to portions of a structure that are below grade or that do not extend more than 2 feet above the existing or finished grade at the street lot line, whichever is lower.
4. Setback requirements
a. Street-level setbacks
1. In the area shown on Map D for 23.47A.009, portions of a structure up to 10 feet above the abutting sidewalk grade facing 15th Ave NW shall be setback from the street lot line by a minimum depth of 6 feet up to a maximum depth of 10 feet.
2. The provisions of subsection 23.47A.009F.2 to not apply to the area described in subsection 23.47A.F.4.a.1.
b. Upper-level setbacks
1. A setback with an average depth of 10 feet from all abutting street lot lines is required for portions of a structure above a height of 45 feet. The maximum depth of a setback that can be used for calculating the average setback is 20 feet.
2. A setback with an average depth of 15 feet from all street lot lines is required for portions of a structure above a height of 65 feet. The maximum depth of a setback that can be used for calculating the average setback is 25 feet.

SMC 23.47A.010 – Maximum Size of nonresidential use.
No limit restrictions for proposed uses per Table A for 23.47A.004

SMC 23.47A.011 – Outdoor Activities.
No outdoor activities proposed.

SMC 23.47A.012 – Structure Height - Base Height: 65’

C.2. Open railings, planters skylights, clerestories, greenhouses, solariums, parapets, and fire walls are permitted 4’ above applicable height limit. Insulation material, rooftop decks and other similar features, or soil for landscaping located above the structural roof surface may exceed height limit by 2’ if enclosed by parapets that comply with 23.47A.012.c.2.

C.3. Solar Collectors may extend up to 7 feet above the otherwise applicable height limit for entire area of roof.

C.4. The following rooftop features may extend up to 15’ above applicable height limit as long as the combined total coverage of all features gaining additional height does not exceed 20% of roof area, or 25% if the total includes stair, elevator, or screened mechanical:

- C.4.a. Solar collectors
- C.4.b. Mechanical equipment
- C.4.f. Stair and Elevators up to 16’

C.6. Greenhouses that are dedicated to food production are permitted to extend 15 feet above the applicable height limit if the combined total coverage of all features gaining additional height listed in 23.47A.012C does not exceed 50 percent of the roof area, and the greenhouse adheres to the setback requirements in subsection 23.47A.012.C.7.

- C.7. The following rooftop features shall be located at least 10’ from the north edge of the roof.
- a. Solar collectors
 - b. Planters
 - c. Clerestories
 - d. Greenhouses and solariums
 - f. Non-firewall parapets

ZONING SUMMARY | SEATTLE MUNICIPAL CODE TITLE 23

SMC 23.47A.013 – Floor Area Ratio

B.1. Maximum FAR per table A for 23.47A.013 = 4.25 FAR

D. Gross floor area not counted toward maximum FAR:

- 1. All underground stories
- 8. Rooftop greenhouse areas meeting the standards of 23.47A.012.C.6 and C.7.

H.1.a. Minimum FAR in pedestrian-designated zone in an Urban Center, Urban Village per Table C for 23.47A.013 = 2.0

SMC 23.47A.014 – Setback Requirements

None required. Adjacent to other commercial zones on all sides.

SMC 23.47A.016 – Landscaping requirements

- A.2. Green Factor score of .3 or greater
- B.1. Street Trees are required

SMC 23.47A.022 – Light and Glare standards

- A. Exterior lighting must be shielded and directed away from adjacent uses.
- B. Interior lighting in parking garages must be shielded
- D. Exterior lighting on poles permitted to 40’ above finished grade

SMC 23.47A.030 – Required parking and loading.

- A. Parking is not required per SMC 23.54.015
 - 23.54.12 Table A ii. K. Ballard Hub Urban Village Urban Village within 1320’ of frequent transit service – No Minimum required.
 - Bicycle Parking required per 23.54.015 Table D. Retail/Restaurant 1/12,000SF LT, 1/4000SF ST. Lodging 1/20 Rooms LT, 2 ST
- B. Loading Berths are required per SMC 23.54.035
 - 23.54.035 1. Table A;
 - Low Demand 60,001 to 160,000 = 2 Berths
 - Medium Demand- Not required under 10,000SF

SMC 23.47A.032 – Parking location and access

- A.1.c. In NC zones Access permitted across one of the side street lot lines per 23.47A.032.C
- A.3. In C1, access allowed from street, alley or both.
- C. When lot fronts on two or more streets, the director will determine which of the streets will be considered the front lot line.

SMC 23.47A.033 – Transportation concurrency level-of-service- standards.

Shall meet the transportation concurrency level-of-service standard prescribed in 23.52.

SMC 23.53.035 – Structural Building Overhangs and Minor Architectural Encroachments

SMC 23.53.035 – Building overhangs and architectural encroachments shall meet the following

- A. – Minor architectural encroachments that does not increase volume or floor area – cornices, eaves, sills, belts, etc.
 - 2. Maximum vertical dimension = 2’-6”
 - 3. Maximum horizontal dimension
 - a. 1’-0” below roof level
 - b. 3’-0” at roof level
 - 4. Minimum vertical clearance at sidewalk – 8’-0”, alley – 16’-0”

B. – Structural building overhangs that increases volume or floor area – Bay windows, balconies and other projections, as defined under Title 15

- 1. An annual permit from SDOT
- 2. Overhangs must be removable
- 3. Overhangs must not be part of the building structure or systems
- 4. Minimum vertical clearance at sidewalk – 8’-0”, alley – 26’-0”
- 5. Maximum depth = 3’-0” and no closer then 8’-0” of centerline of alley
- 6. 50% of bay window shall be glass. 60% of that glass must be parallel to the property line
- 7. Maximum length of overhang shall be 15’-0”
- 10. Minimum separation between bays shall be 8’-0”
- 11. Minimum separation from interior lot lines shall be 1’-0”, unless there is a building on the adjoining lot that is at the lot line, then the bay may be flush with the lot line.

C. – Total building overhang shall not exceed 30% of the overall façade area. Solid balcony railings are included in that calculation. Open railings are not.

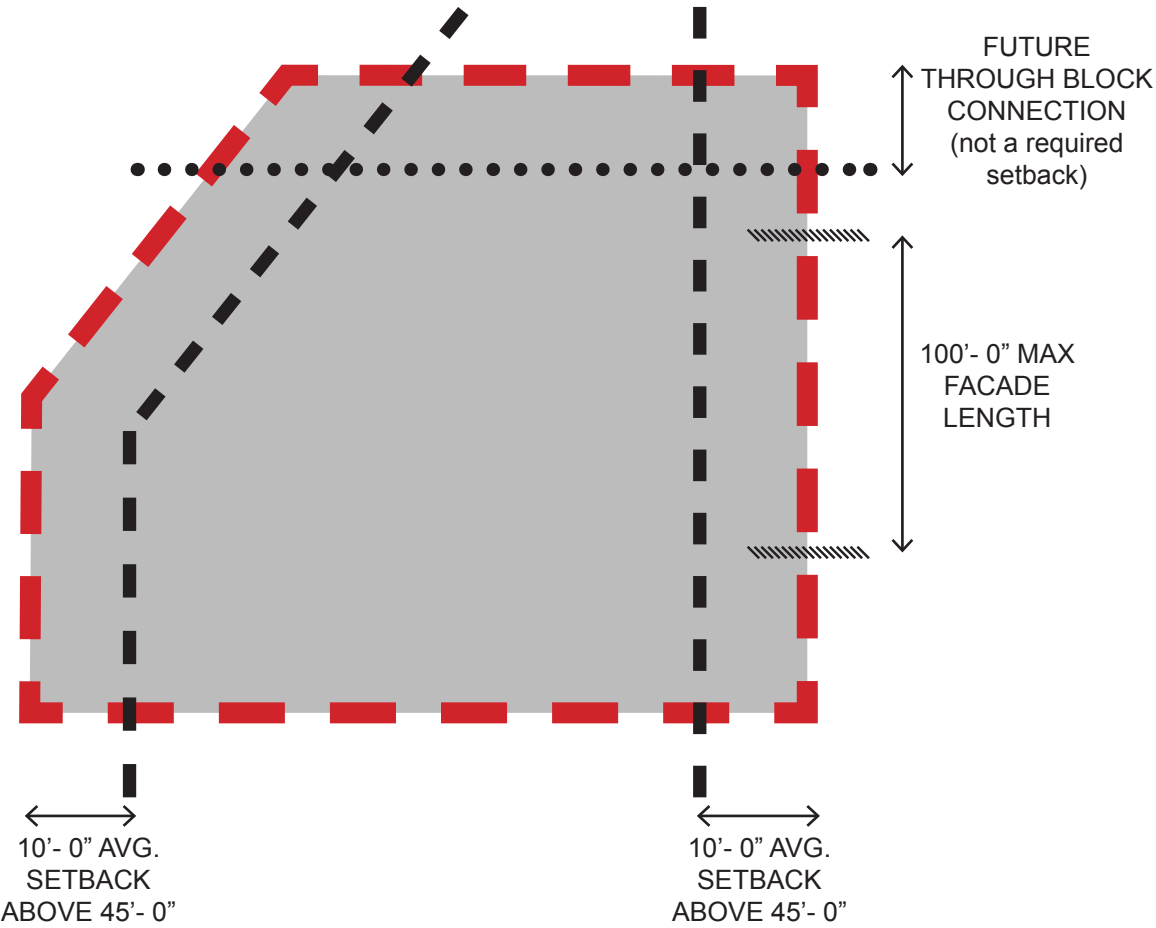
SMC 23.54. - Other Requirements

23.54.040 – Solid Waste and recyclable materials storage and access

A – Provide storage space for solid waste and recyclable materials per Table A

Non-Residential development

10,001-200,000 SF = 275 SF Storage Space



REQUIRED SETBACKS



CS2 A.2 | ARCHITECTURAL PRESENCE:
Evaluate the degree of visibility or architectural presence that is appropriate or desired given the context, and design accordingly. A site may lend itself to a “high-profile” design with significant presence and individual identity, or may be better suited to a simpler but quality design that contributes to the block as a whole. Buildings that contribute to a strong street edge, especially at the first three floors, are particularly important to the creation of a quality public realm that invites social interaction and economic activity. Encourage all building facades to incorporate design detail, articulation and quality materials.

CS2 B.2 | CONNECTION TO THE STREET:
Identify opportunities for the project to make a strong connection to the street and carefully consider how the building will interact with the public realm. Consider the qualities and character of the streetscape— its physical features (sidewalk, parking, landscape strip, street trees, travel lanes, and other amenities) and its function (major retail street or quieter residential street)—in siting and designing the building.

CS2 C.1 | CORNER SITES:
Corner sites can serve as gateways or focal points; both require careful detailing at the first three floors due to their high visibility from two or more streets and long distances. Consider using a corner to provide extra space for pedestrians and a generous entry, or build out to the corner to provide a strong urban edge to the block.

CS3 A.1 | FITTING OLD AND NEW TOGETHER:
Create compatibility between new projects, and existing architectural context, including historic and modern designs, through building articulation, scale and proportion, roof forms, detailing, fenestration, and/or the use of complementary materials.

CS3 A.4 | EVOLVING NEIGHBORHOODS:
In neighborhoods where architectural character is evolving or otherwise in transition, explore ways for new development to establish a positive and desirable context for others to build upon in the future.

PL2 B.1 | EYES ON THE STREET:
Create a safe environment by providing lines of sight and encouraging natural surveillance through strategic placement of doors, windows, balconies and street-level uses.

PL2 B.2 | LIGHTING FOR SAFETY:
Provide lighting at sufficient lumen intensities and scales, including pathway illumination, pedestrian and entry lighting, and/or security lights.

PL2 B.3 | STREET-LEVEL TRANSPARENCY:
Ensure transparency of street-level uses (for uses such as nonresidential uses or residential lobbies), where appropriate, by keeping views open into spaces behind walls or plantings, at corners, or along narrow passageways. Choose semi-transparent rather than opaque screening.

PL3 A.1 | DESIGN OBJECTIVES:
Design primary entries to be obvious, identifiable, and distinctive with clear lines of sight and lobbies visually connected to the street. Scale and detail them to function well for their anticipated use and also to fit with the building of which they are a part, differentiating residential and commercial entries with design features and amenities specific to each.
b. Retail entries should include adequate space for several patrons to enter and exit simultaneously, preferably under cover from weather.

RESPONSE |

- Focus on the detailing and materials of the first three floors
- Connection of the building to the neighboring historic buildings and new construction/ Bringing the vocabulary of old Ballard down Leary and setting the expectation for the neighborhood
- Providing a retail presence to bring pedestrian activity to Leary

RESPONSE |

- Generous entry, substantial materials, highly detailed entry, curb bulb, large sliding windows to allow for spill out and connection to the sidewalk
- Inviting thru-block connection
- Framing & treatment of the first 3 floors in a solid/substantial material with detailing
- Major intersection providing opportunities for a gateway

RESPONSE |

- Stitching old and new Ballard together through use of quality materials, detailing, patterning, window openings
- Use of complimentary colors & materials for a timeless look/feel

RESPONSE |

- Balconies on each facade allows for visibility to the street at all times of day
- Large, operable sliding/folding windows and doors at retail spaces
- Hotel lobby visible from street/sidewalks/24 hour surveillance
- Active retail space/increased pedestrian activity

RESPONSE |

- Emphasized entry at hotel with awning at different height
- Well-detailed and prominent entry at hotel lobby, lobby with visibility to the street
- Retail entries with awnings to compliment main entry through use of materials, color, style



PL3 C.1 | POROUS EDGE:
Engage passersby with opportunities to interact visually with the building interior using glazing and transparency. Create multiple entries where possible and make a physical and visual connection between people on the sidewalk and retail activities in the building.

PL3 C.2 | VISIBILITY:
Maximize visibility into the building interior and merchandise displays. Consider fully operational glazed wall-sized doors that can be completely opened to the street, increased height in lobbies, and/or special lighting for displays.



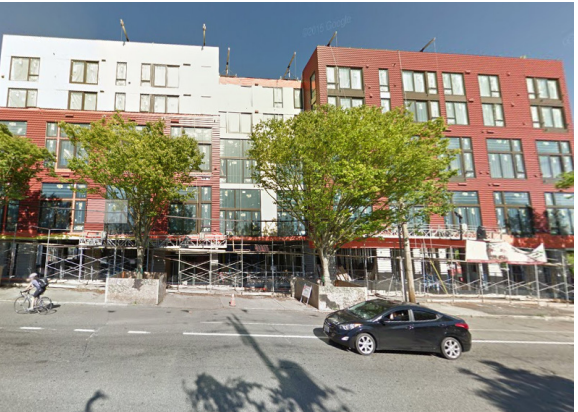
DC4 A.1 | EXTERIOR FINISH MATERIALS:
Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged.



DC1 B.1 | ACCESS LOCATION AND DESIGN:
Choose locations for vehicular access, service uses, and delivery areas that minimize conflict between vehicles and non-motorists wherever possible. Emphasize use of the sidewalk for pedestrians, and create safe and attractive conditions for pedestrians, bicyclists, and drivers by:

- a. using existing alleys for access or, where alley access is not feasible, choosing a location for street access that is the least visually dominant and/or which offers opportunity for shared driveway use;

DC1 C.1 | BELOW-GRADE PARKING:
Locate parking below grade wherever possible. Where a surface parking lot is the only alternative, locate the parking in rear or side yards, or on lower or less visible portions of the site.



DC2 A.2 | REDUCING PERCEIVED MASS:
Use secondary architectural elements to reduce the perceived mass of larger projects. Consider creating recesses or indentations in the building envelope; adding balconies, bay windows, porches, canopies or other elements; and/or highlighting building entries.



DC2 B.1 | FACADE COMPOSITION:
Design all building facades—including alleys and visible roofs—considering the composition and architectural expression of the building as a whole. Ensure that all facades are attractive and well-proportioned through the placement and detailing of all elements, including bays, fenestration, and materials, and any patterns created by their arrangement. On sites that abut an alley, design the alley facade and its connection to the street carefully. At a minimum, consider wrapping the treatment of the street-facing facade around the alley corner of the building.

DC2 C.1 | VISUAL DEPTH AND INTEREST:
Add depth to facades where appropriate by incorporating balconies, canopies, awnings, decks, or other secondary elements into the facade design. Add detailing at the street level in order to create interest for the pedestrian and encourage active street life and window shopping (in retail areas). Detailing may include features such as distinctive door and window hardware, projecting window sills, ornamental tile or metal, and other high-quality surface materials and finishes.

RESPONSE |

- Large blocks of glazing on lower/retail level
- Main spaces of lobby & retail oriented to be visible from the street to draw interest from pedestrians and those driving by
- Operable windows at the retail space

RESPONSE |

- Stone/brick facade at base/first few levels
- Durable/quality material at upper levels
- Materials that compliment the aesthetic of old Ballard, & that tie together with new Ballard
- Stone/Brick detailing and patterning

RESPONSE |

- Parking entry located at back of the building on Russell, where there is less pedestrian activity & least visible from the busiest areas
- Reserving pedestrian entrances for the most prominent areas of the building/site
- Parking located below-grade
- Through-block connection

RESPONSE |

- Use of setbacks at the upper levels
- Break-up of materials
- Break up of massing through height changes, punched window patterning, balconies

RESPONSE |

- Creating a cohesive building by carrying details, materials, window patterning, etc around the whole building
- Creating depth with recessed & protruding balconies at the upper levels
- Varied awnings per each type of entrance (lobby, retail, garage, etc)
- Careful detailing of the retail level with grand doors, hardware, material patterning, etc.

BALLARD URBAN DESIGN & TRANSPORTATION FRAMEWORK

OVERALL RECOMMENDATIONS

3.3 Land Use & Zoning

Mixed use vs. Single Purpose Development

...Along Ballard’s key commercial streets, active ground floor uses are desired - or in the case of pedestrian-designated areas - required.

3.4 Development Standards

Building Massing & Scale

The proposed changes to development standards generally seek to create continuity and an intimate scale at the street level and first thirty feet of building facades. Above this level, proposed standards will help provide a sense of openness and reduce the perceived bulk of large development (canyon feel). In doing this, building forms should reflect the nature of their uses, rather than copy elements found in smaller (e.g. residential) development.

Building width

Along commercial streets, the dominant parcel width of areas platted up to the 1930’s was 50’. Building facades created continuous frontages along the block lengths, but the individual buildings were between 50 and 100 feet. Reflect this character in new buildings by either limiting lengths or by creating distinct designs or material changes, or vertical modulation that break up facades into this scale.

Upper level

Above 30 - 40 feet (three to four stories) the priority shifts from reinforcing the desired street-level activity to incorporating elements that reduce bulk and increase light to the street.

Relationship to the street

Meeting the sidewalk

Throughout downtown Ballard, place buildings and active uses close to the sidewalk to support a pleasant walking environment...

Street level

At the street level, create a sense of lively enclosure. In shopping areas, at transit hubs, and in the Civic Core, where there are lots of pedestrians and gathering places, place buildings abutting the sidewalk or provide small, carefully programmed spaces such as outdoor cafe seating areas. Design entrances, highly transparent shop windows, sidewalks and open spaces to work together to support the highest level of social interaction.

Existing Context

In the character core, Ballard has a great collection of old buildings, new development should contribute to this legacy.

Location on a Block

Provide continuity of vertical and horizontal elements such as window size and spacing and location of entrances along a block. Take advantage of the visibility of corner locations by placing entrances and strong design features on corners. On the north side of the intersection, make use of the light available throughout the winter, fall, and spring.

Facade Design

Treat the design of facades as a single integrated element. Older buildings in Ballard, especially larger ones, were often more ornately designed or included interesting architectural features. While such visual interest is desirable and can reduce the perceived bulk of larger buildings, a well-defined design concept with restrained materials and color palette is desired. Horizontal divisions that create distinctive base and cap levels can provide a scale and order to the mass of a building. Facades may also be divided vertically by piers or may be modulated to form repetitive window bays.

Street level design of shop-fronts

Ballard Avenue and the older parts of Market Street demonstrate successful street-level design. New development on shopping streets should provide street-level interest that enhances the pedestrian environment, with highly transparent windows to encourage pedestrians to focus on the product or service offered. New development should provide variety and distinctiveness in materials and smaller details such as hardware, fixtures, and colors.

Building Materials

- Traditional materials like brick and stone are recommended for the Character Core... Select materials to convey permanence.
- Development should reinforce the historic character with use of high quality materials and a selective color palette.
 - Ground floor windows of businesses should not be overly recessed.
 - Masonry is preferred at the street-level and for the first 40 feet.
 - Window openings should incorporate lintels and sills on a scale that is appropriate to the size of the building.

3.6 Streetscape and Open Space

Open Space

Open spaces in downtown Ballard provide for respite, socializing and recreation. Open spaces range from balconies where one can enjoy a sunset to rooftop decks that host private gatherings, as well as parks and streets. Creating a healthy public realm requires the thoughtful attention to the building elements described above and streetscape elements described below.



Photos and Graphics by Ballard Urban Design & Transportation Framework Document

BALLARD URBAN DESIGN & TRANSPORTATION FRAMEWORK

Character core

Overall massing

The community is strongly in favor of human-scaled buildings, appropriate upper level setbacks, and massing breaks that can contribute to mitigating the canyon feeling...

Street level design

Draw attention to entrances. Avoid deep window reveals at street level. Encourage variety in awnings, signage. Street level facades should have greater proportion of voids than solids.

Adjoining structures

Design buildings so that the facade and massing take cues from adjoining buildings

Strong horizontal elements

Design new buildings to have horizontal divisions that create distinctive base and cap levels.

Establish human-scale

Pay special attention to details on the first 30’ of the facade to establish scale, engage pedestrians, and respond to a walking speed.

Materials

Scale and context

Reinforce the historic character with restrained use of high quality materials. Brick and stone masonry are preferred for the first 30-40 feet. Metal and other industrial finishes can be used to compliment traditional materials or create interesting contrast.

Sense of permanence

Convey a sense of structural permanence through the choice of materials

Clear design approach

Pursue one clear design approach while responding to existing structures. Example approaches include restoration, replication, complementary additions, differential contrast, and assertive contrast.

CHARACTER CORE RECOMMENDATIONS

4.1 Character Core (details p.26-29)

Role

Ballard’s Downtown is centered on its “main streets” – Market Street and Ballard Avenue. Its inviting mix of historic and heritage buildings, and a welcoming business district with shops, restaurants and bars, and services define Ballard’s character. Sustain this historic role and character of Ballard’s main streets as they evolve and grow. This includes strengthening the Landmark District’s streetscape and writing new development standards to ensure new buildings integrate Ballard’s historic design precedents for well-detailed, quality construction.

Overall Strategies

Expand and strengthen the “main street” character of Market Street.

1. Selectively extend this character along:
 - Leary Way NW from NW Market Street to 20th Avenue NW
2. Establish new requirements and guidelines so that new development is more compatible with existing character and reinforces these commercial streets
3. Explore developing criteria to identify and incentives to retain certain older buildings in the Character Core (but outside the Ballard Ave Landmark District).
4. Pursue strategies to attract more jobs such as encouraging office uses over street level retail, and allowing single-purpose commercial buildings to achieve the highest allowable FAR.
5. Expand the Pedestrian designation to extend the walkable “main street” character of Market Street.
6. Improve the ease of walking through intersection improvements, through-block connections, and way-finding strategies.
7. Encourage active uses around open spaces, and orienting buildings so that entrances and private open spaces create a physical or visual connection with public open spaces.

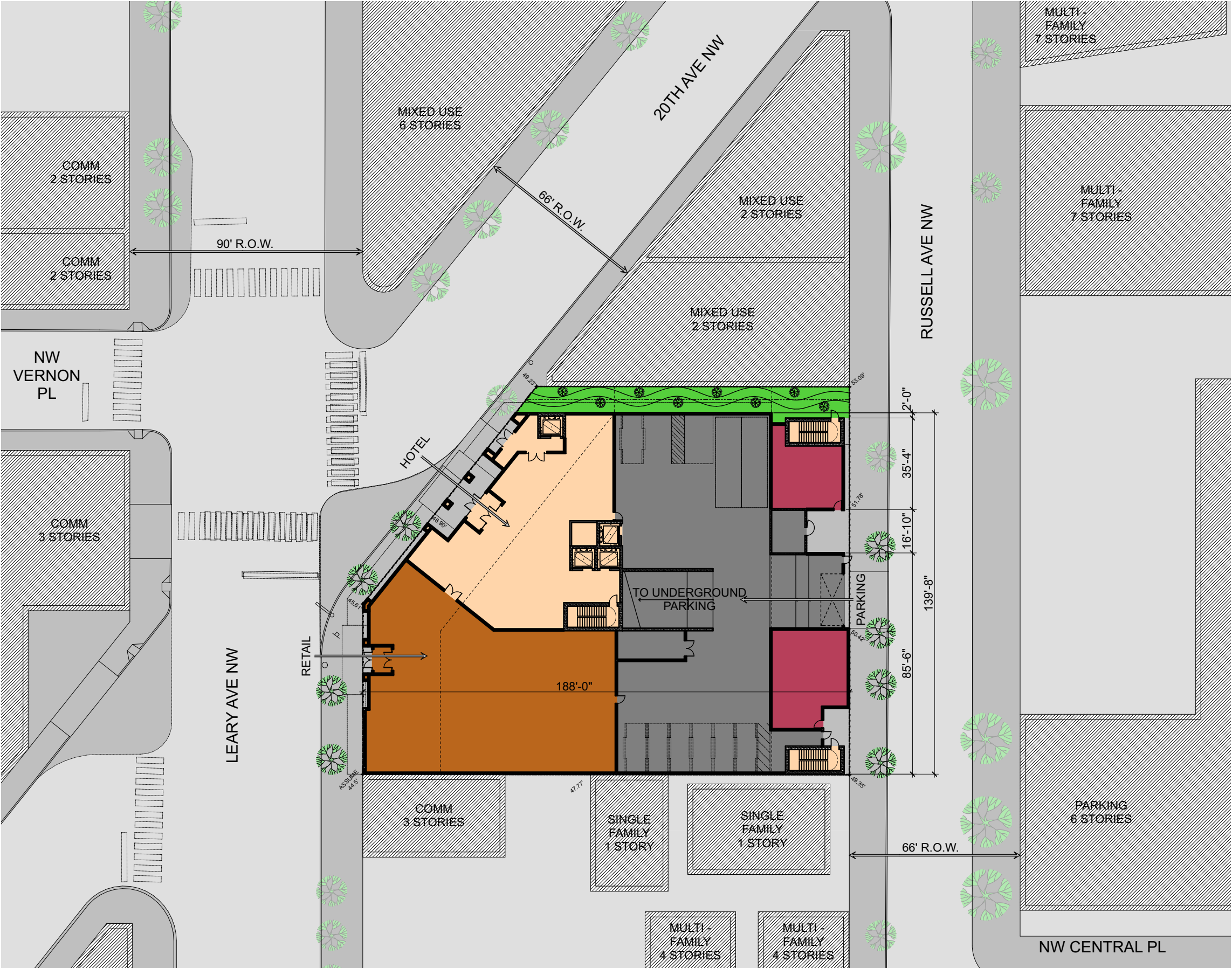


Photos and Graphics by Ballard Urban Design & Transportation Framework Document



CONCEPTUAL DESIGN OPTIONS

OPTION A - BOOKEND HORIZONTAL



OPTION A - BOOKEND HORIZONTAL











Hotel Rooms:	94 units
Live Work:	2 units
Parking:	230 stalls-3 levels of under-ground parking
Retail space:	6,200 sf + 3,100 sf mezz.
Overall FAR:	103,663 sf
Remaining FAR:	4,172 sf

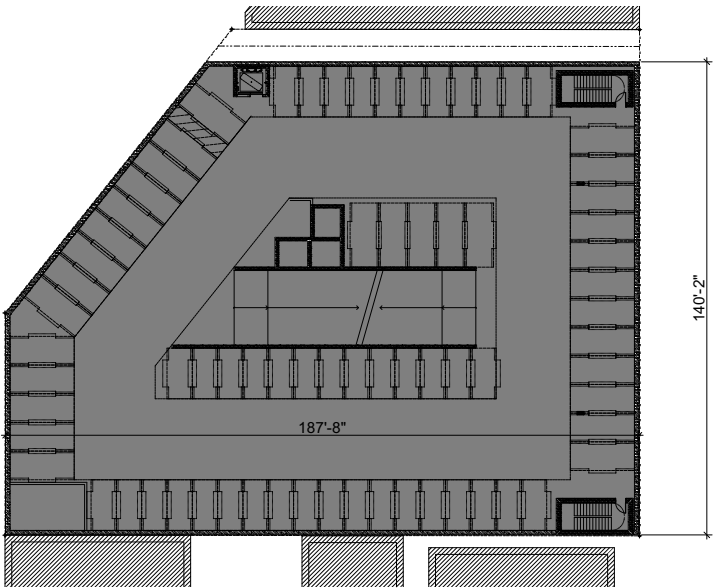
This option's genesis is a horizontal expression above a single story masonry base. With a strong single story masonry base, the horizontality of the upper portion lightens the overall mass of the building. The linear nature of the decks and other outdoor space take precedence over the punched openings of the individual rooms. The single story base reduces the street level mass for the lobby and retail spaces, further engaging the street and public at large. Upper level setbacks create a 2nd recessed plane to further emphasize the horizontal nature of the facades.

- Pros**
- Single Story base – scale and material relates to the pedestrian scale.
 - Upper level is set back enhances horizontal nature of composition.
 - Horizontal deck elements shorten the perceived height of facade
 - Through block pedestrian connection between Russell Ave NW & 20th Ave NW
 - Parking access from Russell Ave NW

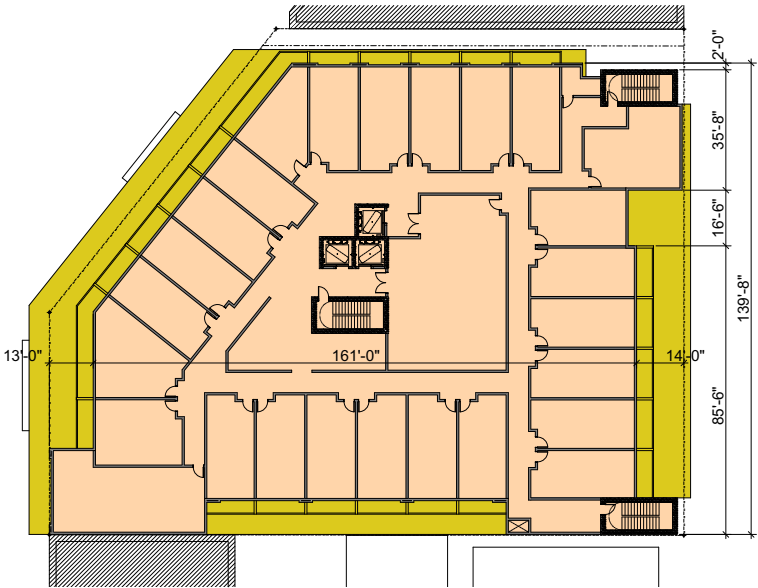
- Cons**
- Strong Horizontal elements do not relate well with neighborhood.
 - Lack of entry definition along street level
 - Monotonous upper level composition.
 - No opportunities to tie through block connection to retail/ restaurant uses.

KEY

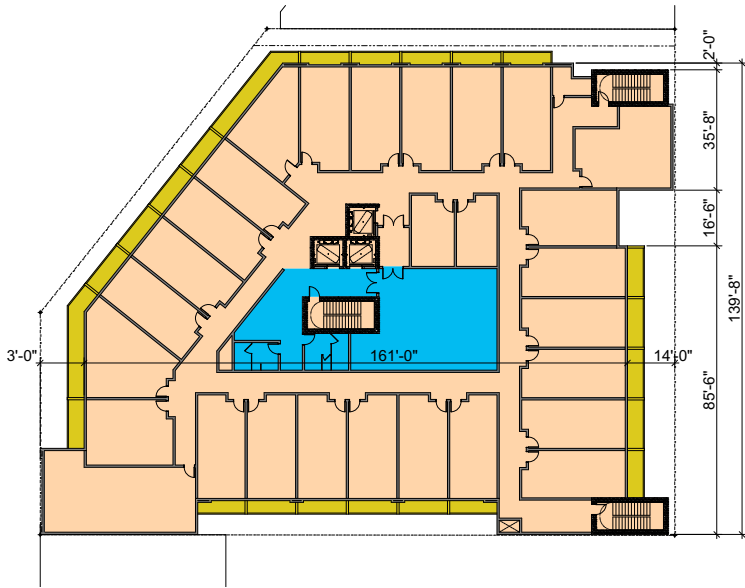
	EXISTING BUILDINGS		RETAIL
	SIDEWALK/DRIVE/ALLEY		HOTEL
	PARKING/UTILITIES		ROOF/DECKS
	LIVE WORK		EVENT SPACE
	ROOF TOP GREEN HOUSE		THROUGH SITE CONNECTION



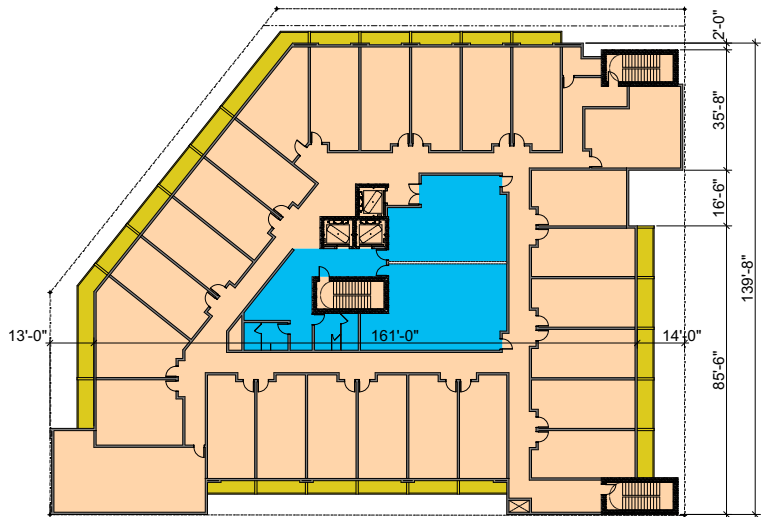
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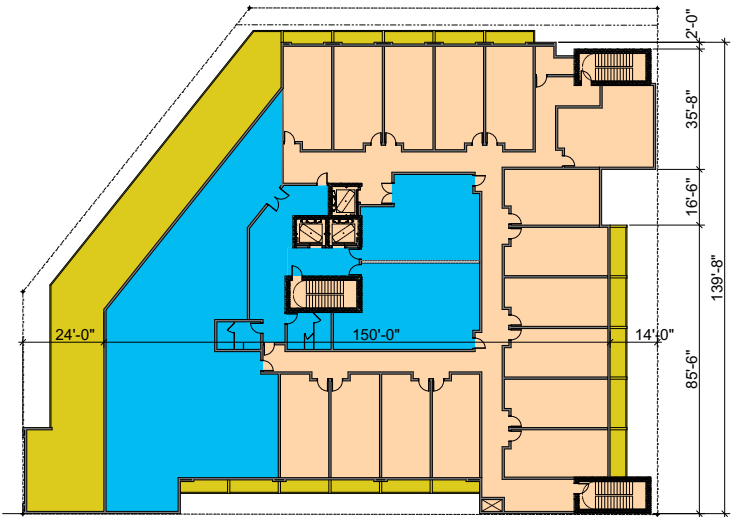
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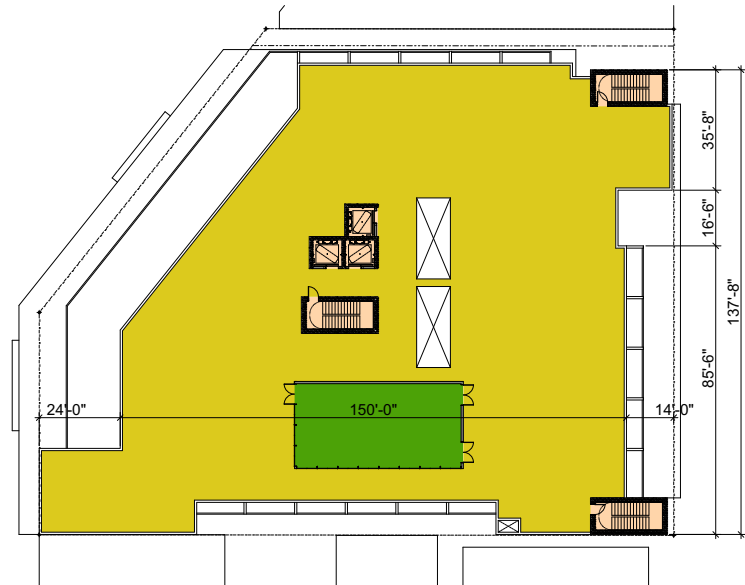
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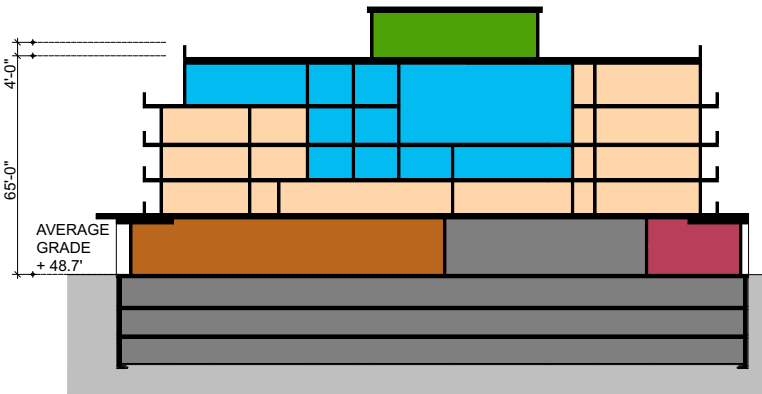
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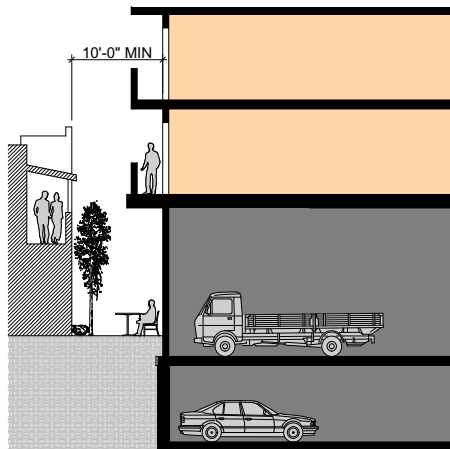
LEVEL 5



ROOF LEVEL



SECTION 1



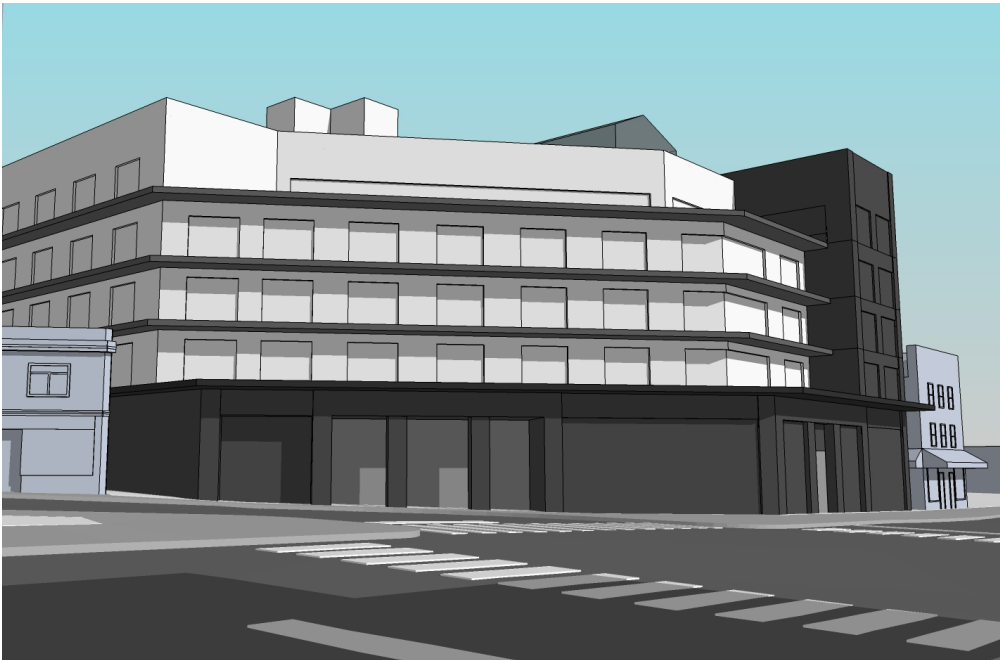
SECTION 2 - PEDESTRIAN THRU CONNECTION



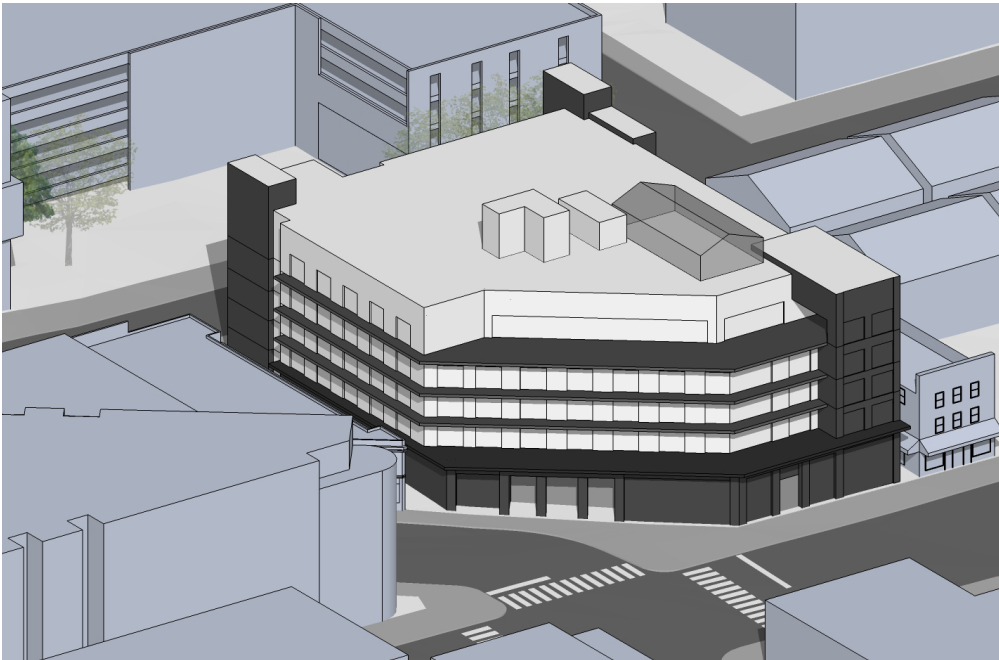
PEDESTRIAN THRU CONNECTION

KEY

	EXISTING BUILDINGS
	SIDEWALK/DRIVE/ALLEY
	PARKING/UTILITIES
	LIVE WORK
	ROOF TOP GREEN HOUSE
	RETAIL
	HOTEL
	ROOF/DECKS
	EVENT SPACE
	THROUGH SITE CONNECTION



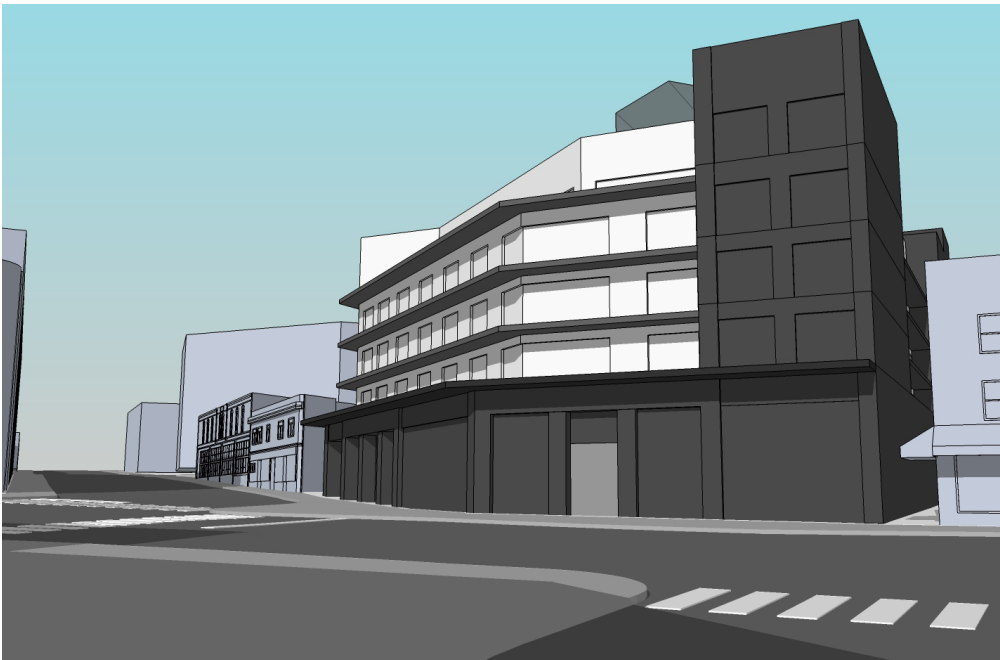
FROM LEARY AVE NW



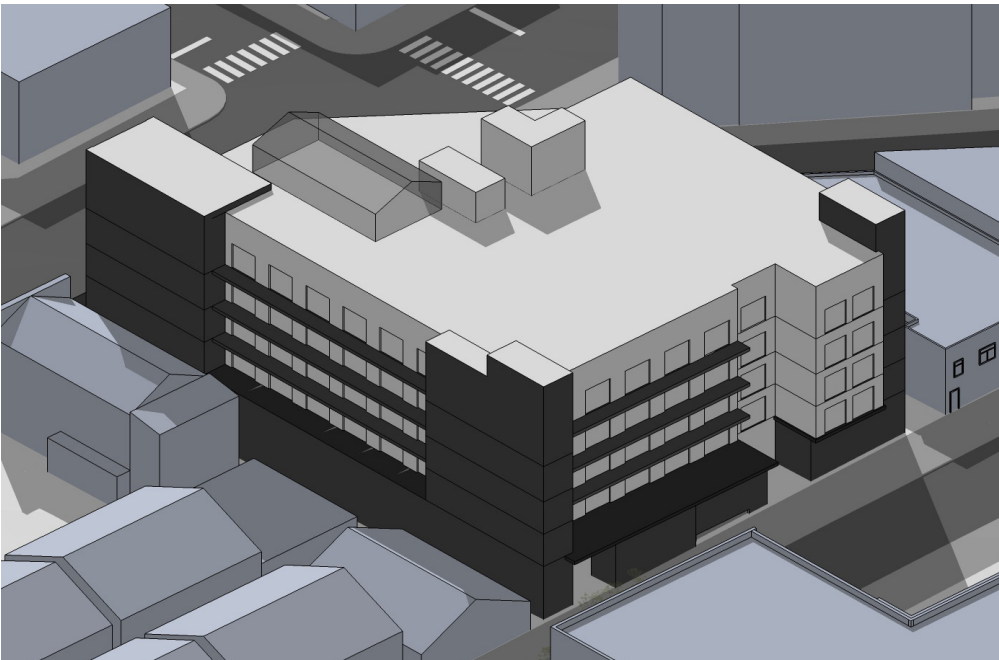
AXON FROM THE SW



FROM RUSSEL AVE NW



FROM 20TH AVE NW



AXON FROM THE NE



AXON FROM THE SE

OPTION B - THE 2 OVER 3



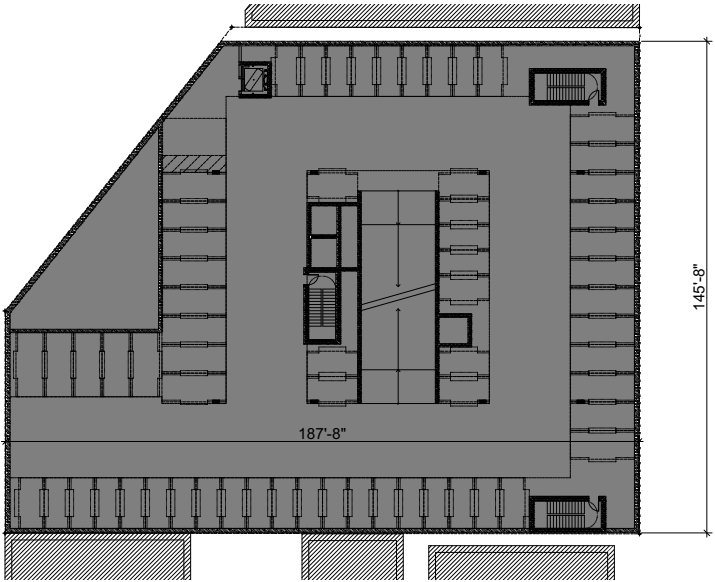
OPTION B - THE 2 OVER 3

Hotel Rooms:	101 units
Live Work:	2 units
Parking:	201 stalls-3 levels of under-ground parking
Retail space:	9,500 sf + 4,750 sf mezz.
Overall FAR:	107,116 sf
Remaining FAR:	719 sf

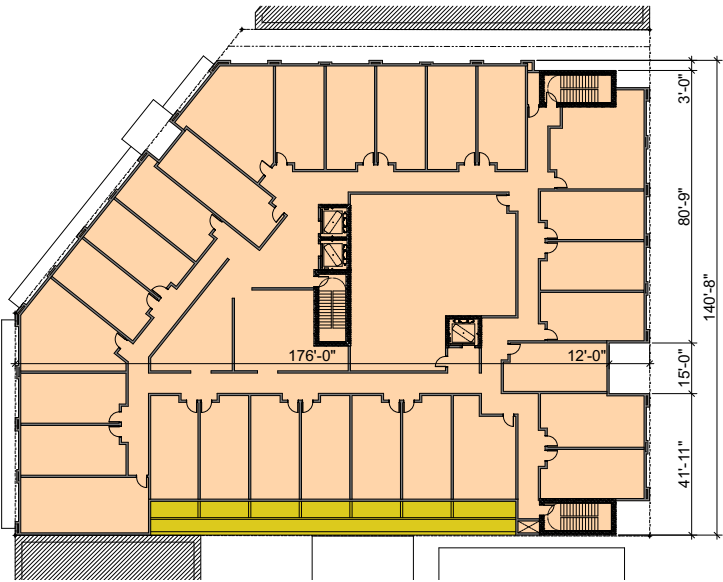
This option's genesis is a full edge-to-edge base, middle, and top along the entire street frontage maximizing building square footage and number of guest rooms. The three-story base of durable materials, center relief of green, and setback top of modern contextual materials completes this simple building expression.

- Pros**
- Continuous upper level setback to decrease scale.
 - Maximizes site potential for square footage and number of guest rooms.
 - Through block pedestrian connection between Russell Ave NW & 20th Ave NW
 - Parking access from Russell Ave NW
- Cons**
- Least façade articulation at upper and lower levels.
 - No opportunities to tie through block connection to retail/restaurant uses.
 - Lack of interest in facade - to regular.

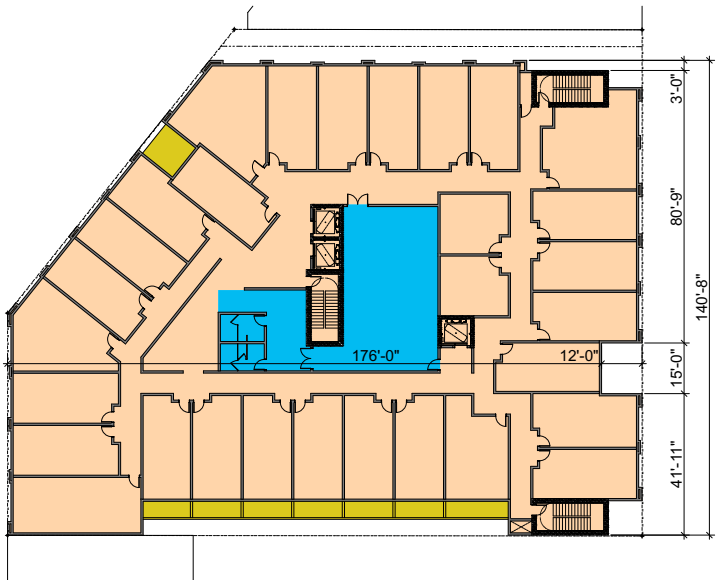
EXISTING BUILDINGS	RETAIL
SIDEWALK/DRIVE/ALLEY	HOTEL
PARKING/UTILITIES	ROOF/DECKS
LIVE WORK	EVENT SPACE
ROOF TOP GREEN HOUSE	THROUGH SITE CONNECTION



PARKING LEVEL 1 (LEVEL 2-3 SIMILAR)

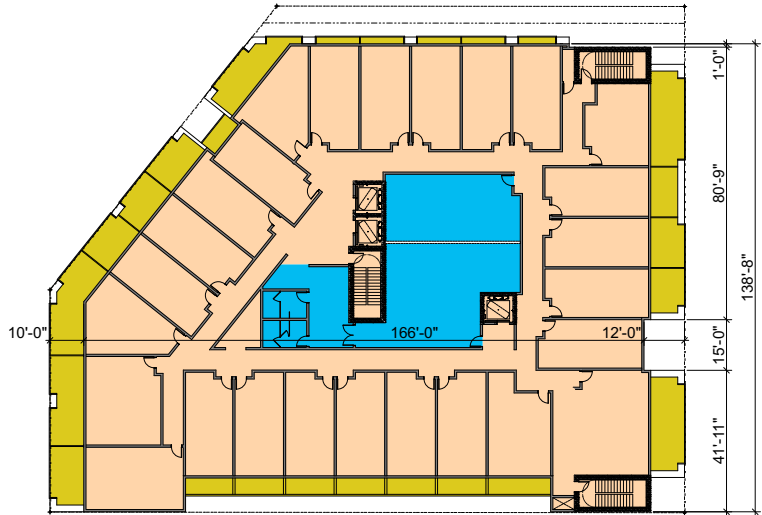


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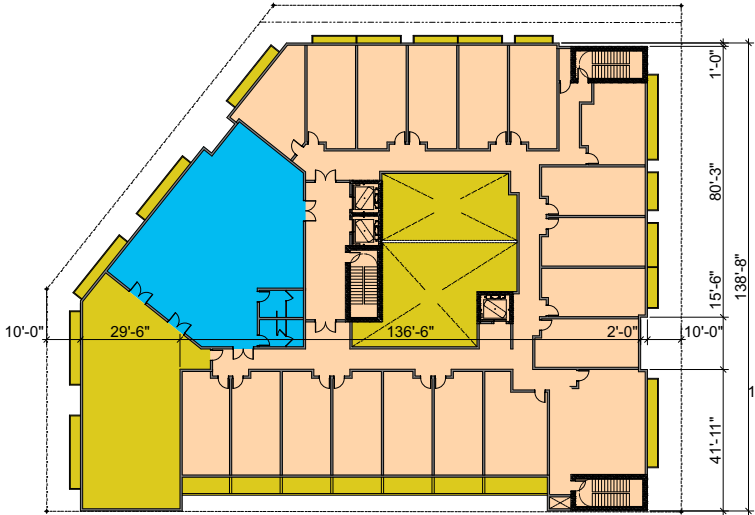


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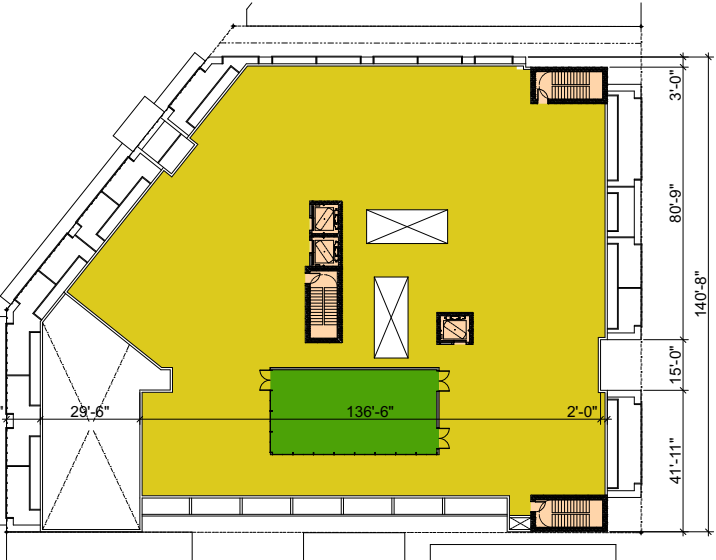
- KEY**
- EXISTING BUILDINGS
 - SIDEWALK/DRIVE/ALLEY
 - PARKING/UTILITIES
 - LIVE WORK
 - ROOF TOP GREEN HOUSE
 - RETAIL
 - HOTEL
 - ROOF/DECKS
 - EVENT SPACE
 - THROUGH SITE CONNECTION



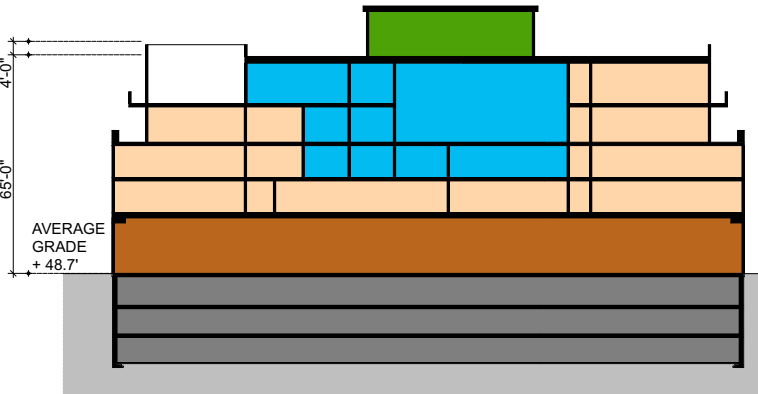
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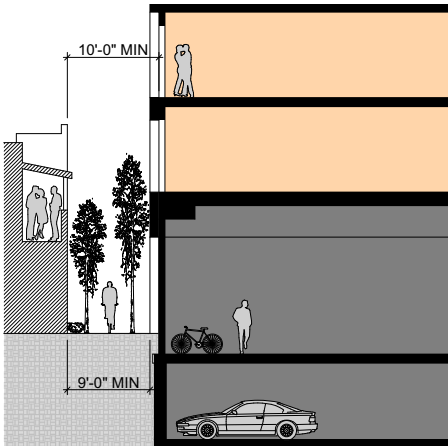
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ROOF LEVEL



SECTION 1



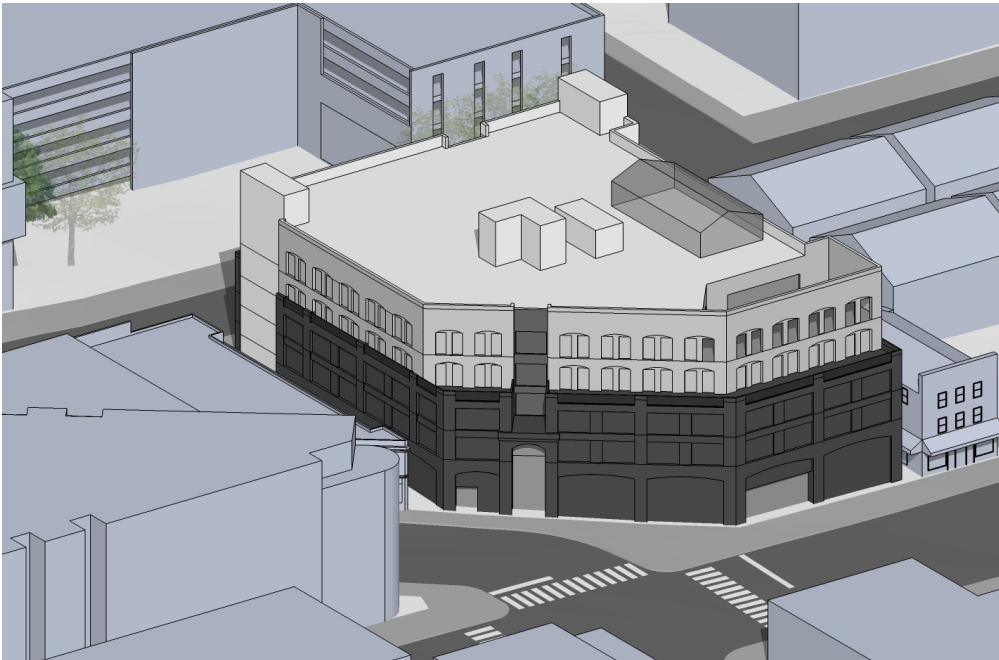
SECTION 2 - PEDESTRIAN THRU CONNECTION



PEDESTRIAN THRU CONNECTION



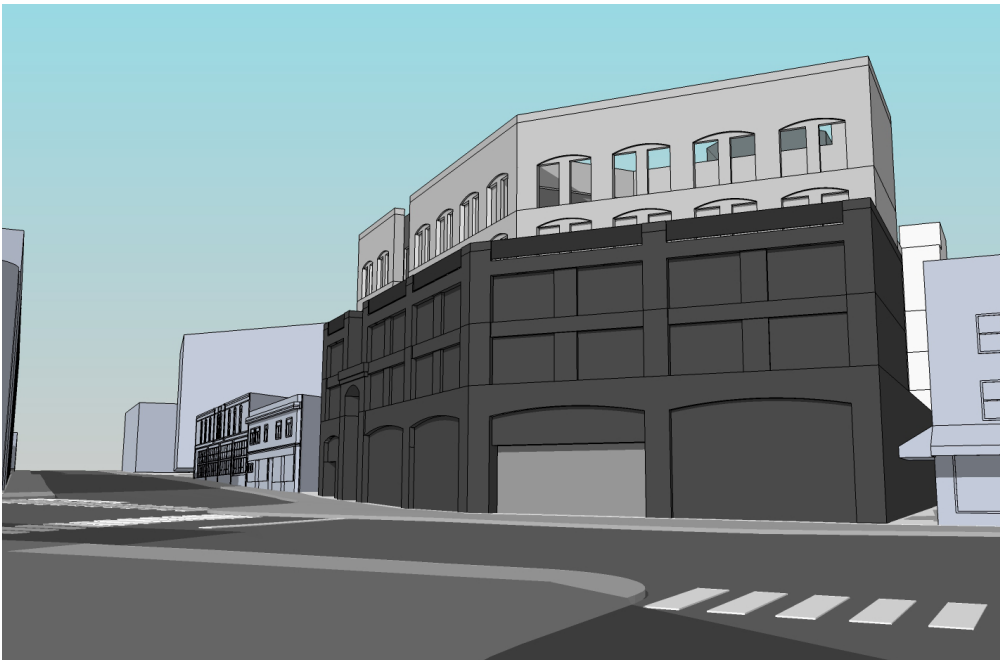
FROM LEARY AVE NW



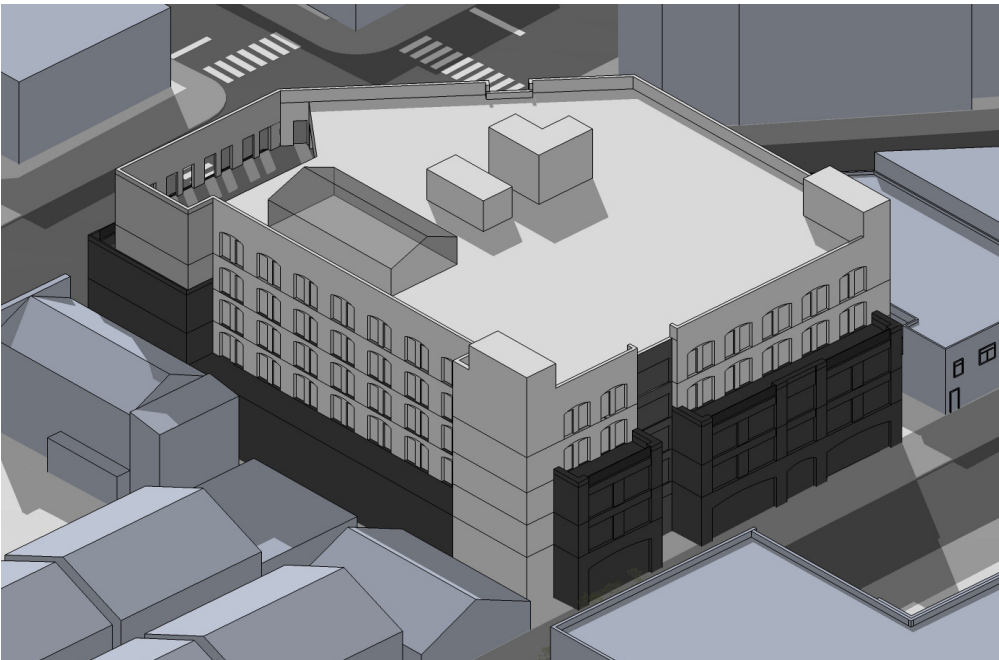
AXON FROM THE SW



FROM RUSSEL AVE NW



FROM 20TH AVE NW

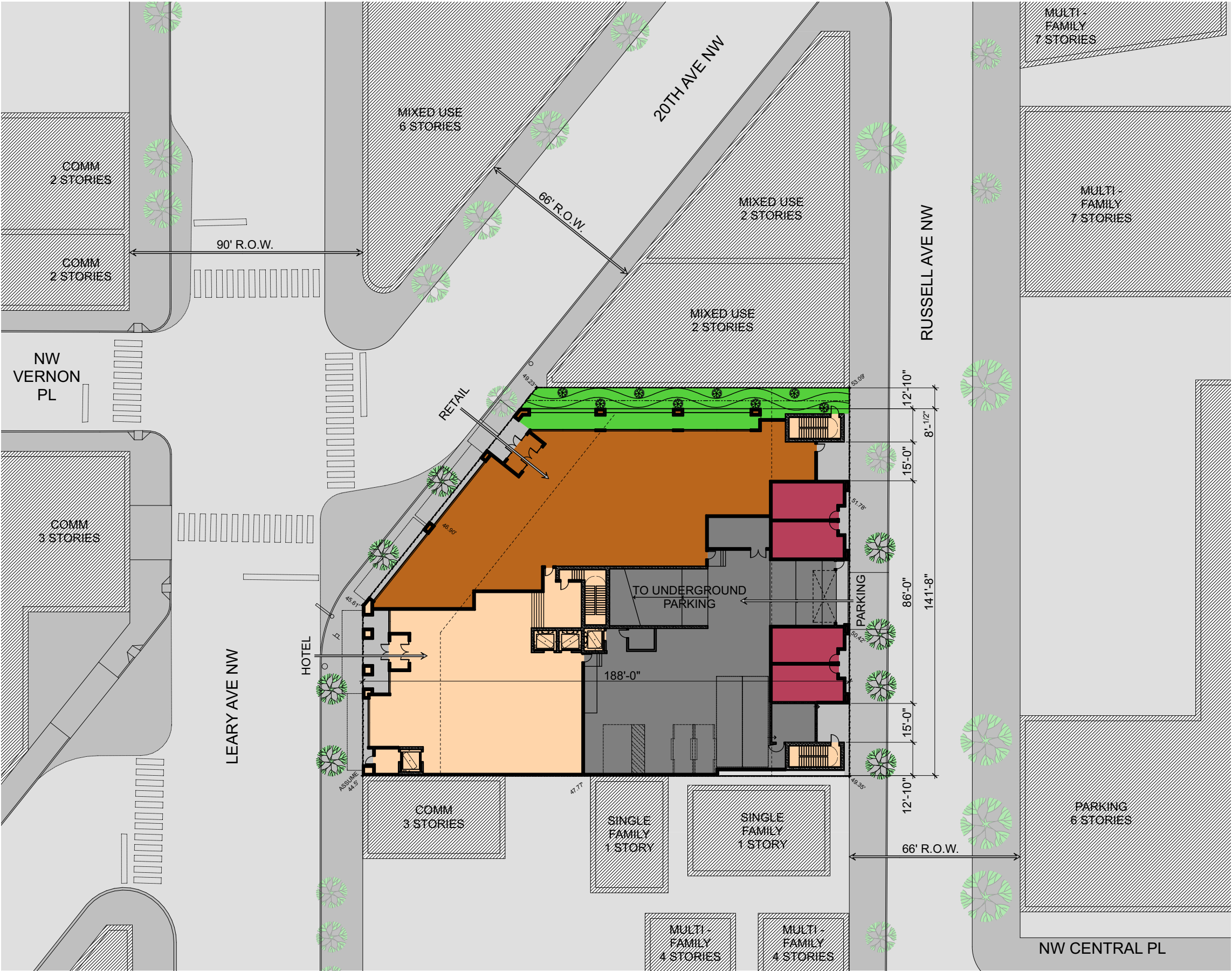


AXON FROM THE NE



AXON FROM THE SE

OPTION C - THE BOOKEND FRAME (PREFERRED)



OPTION C - BOOKEND FRAME

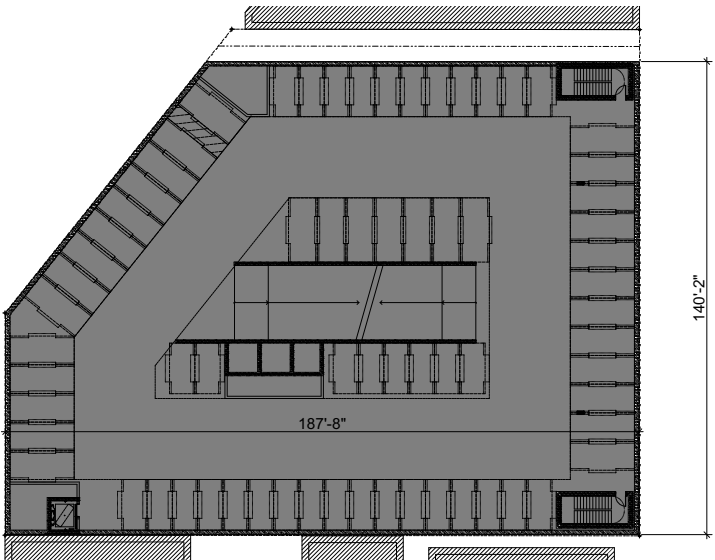
Hotel Rooms:	99 units
Live Work:	4 units (or 2 large units)
Parking:	213 stalls-3 levels of under-ground parking
Retail space:	7,000 sf + 3,500 sf mezz.
Overall FAR:	105,468 sf
Remaining FAR:	2,367 sf

This option's genesis is a strong vertical bookend frame that collects a three story central base. By honoring traditional building materials for the bookends and central frame we can then setback the upper levels with a modern vernacular and finish. The modulated upper level setbacks in an asymmetrical rhythm lends interest to the overall composition. The large, punched openings allow for ample air and light into each hotel room while provide a rhythmic cadence along the exterior. Large transparent openings between the frames, engages the street and interacts with the public realm.

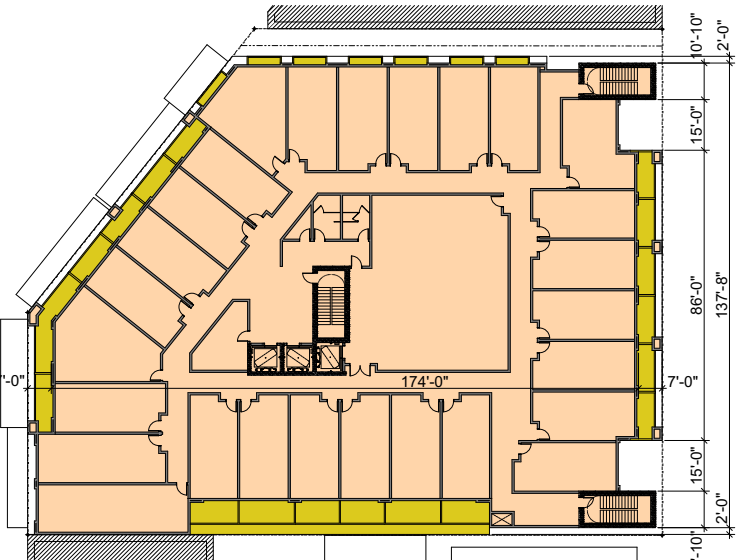
- Pros**
- Strong base – scale and material relates to the historic character of the Ballard Neighborhood
 - Upper level is set back to decrease scale in asymmetrical nature.
 - Rhythmic pattern of punched openings framed in durable materials
 - Overall setback creates interest in façade forms.
 - Strong connection at street level to define pedestrian entries
 - Enhanced through block pedestrian connection between Russell Ave NW & 20th Ave NW connects to Restaurant/ Retail opportunities.
 - Parking access from Russell Ave NW

- Cons**
- Least efficient use of FAR
 - Least rooftop area for roof deck

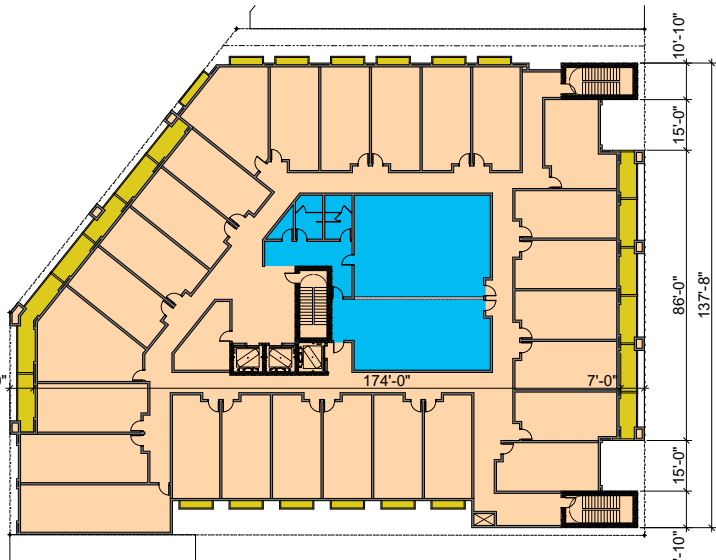
EXISTING BUILDINGS	RETAIL
SIDEWALK/DRIVE/ALLEY	HOTEL
PARKING/UTILITIES	ROOF/DECKS
LIVE WORK	EVENT SPACE
ROOF TOP GREEN HOUSE	THROUGH SITE CONNECTION



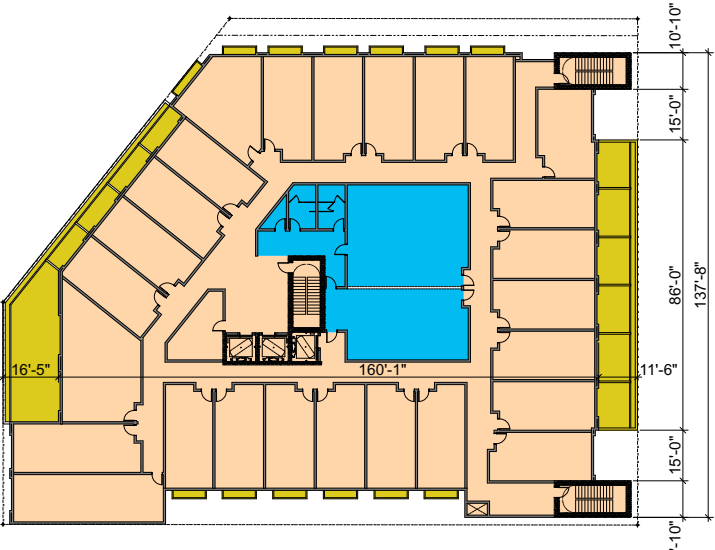
PARKING LEVEL 1 (LEVEL 2-3 SIMILAR)



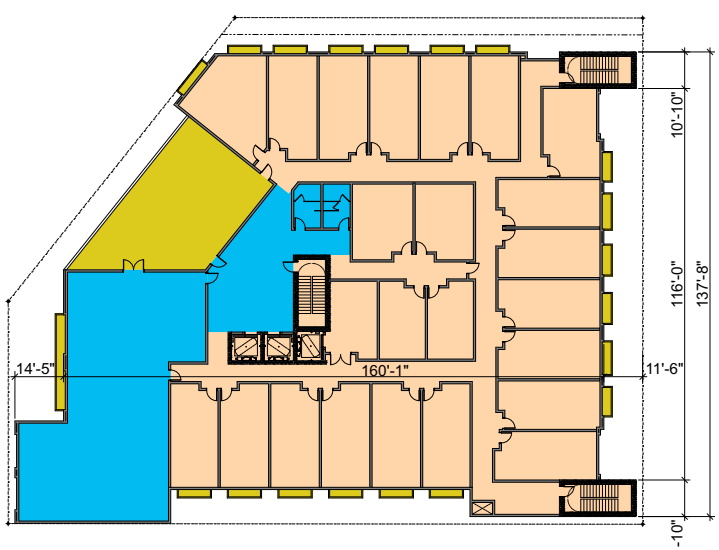
LEVEL 2



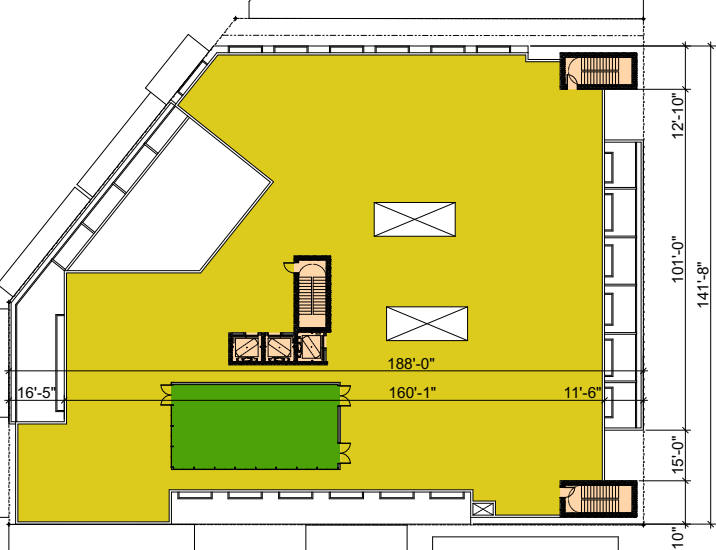
LEVEL 3



LEVEL 4

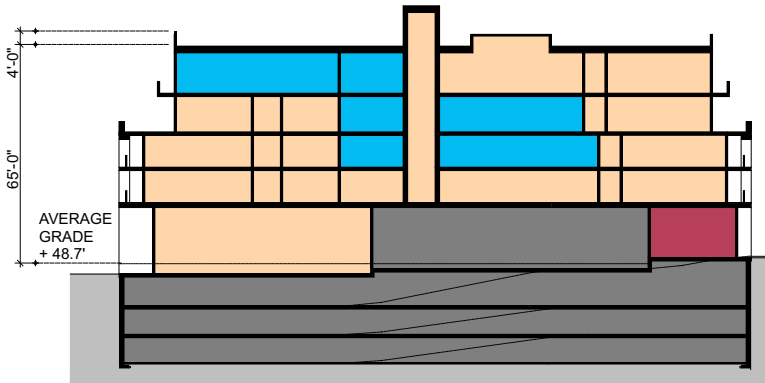


LEVEL 5

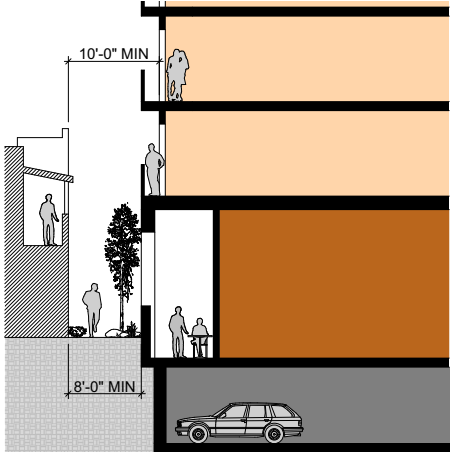


ROOF LEVEL

- KEY**
- EXISTING BUILDINGS
 - SIDEWALK/DRIVE/ALLEY
 - PARKING/UTILITIES
 - LIVE WORK
 - ROOF TOP GREEN HOUSE
 - RETAIL
 - HOTEL
 - ROOF/DECKS
 - EVENT SPACE
 - THROUGH SITE CONNECTION



SECTION 1



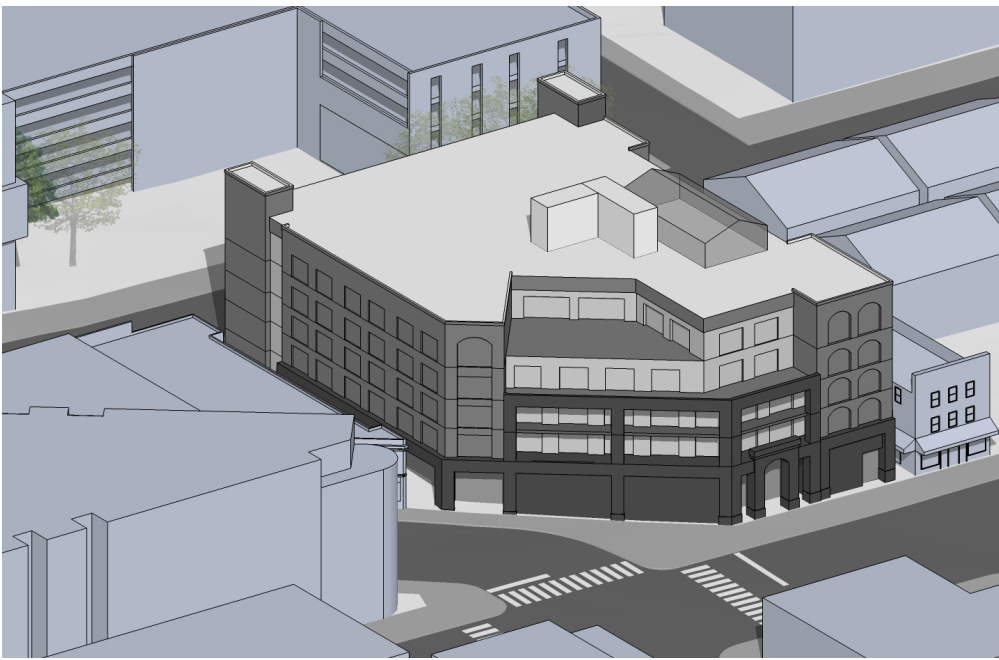
SECTION 2 - PEDESTRIAN THRU CONNECTION



PEDESTRIAN THRU CONNECTION



FROM LEARY AVE NW



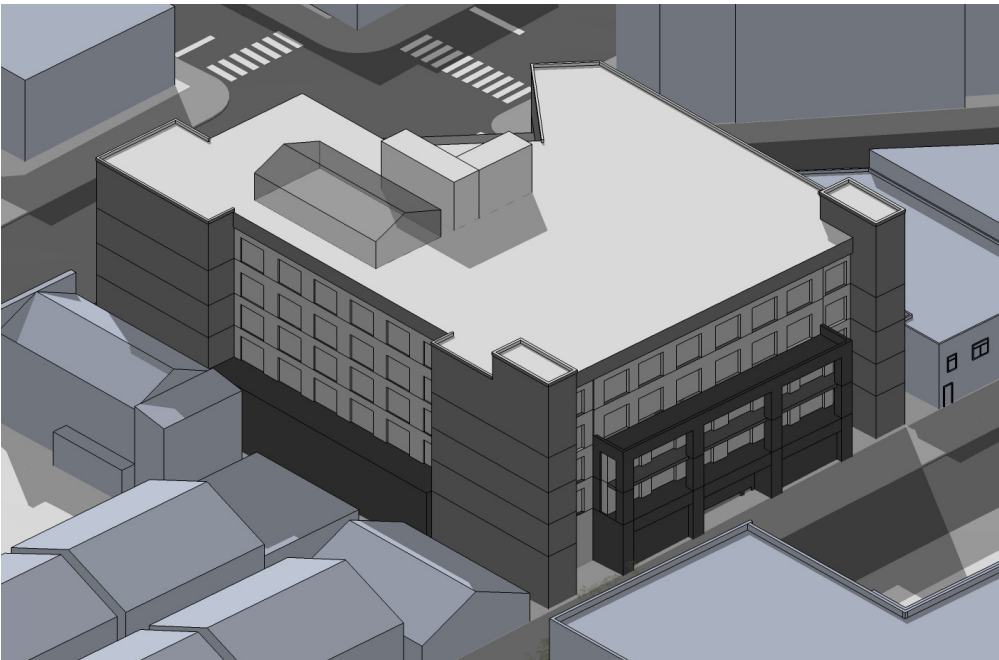
AXON FROM THE SW



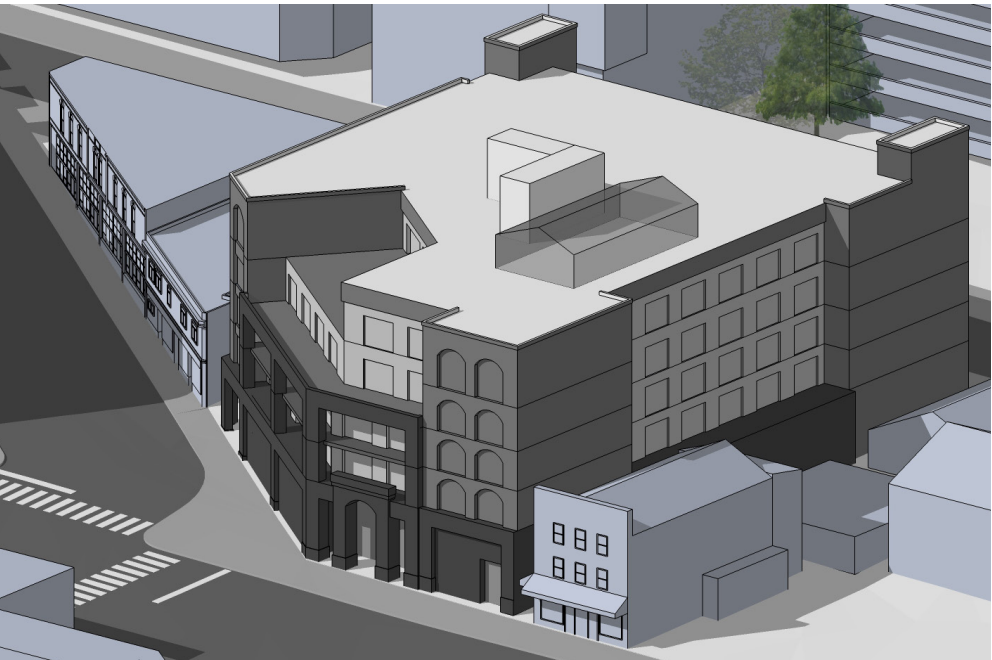
FROM RUSSEL AVE NW



FROM 20TH AVE NW



AXON FROM THE NE



AXON FROM THE SE

DESIGN COMPARISONS



OPTION A

Pros

- Single Story base – scale and material relates to the pedestrian scale.
- Upper level is set back enhances horizontal nature of composition.
- Horizontal deck elements shorten the perceived height of facade
- Through block pedestrian connection between Russell Ave NW & 20th Ave NW
- Parking access from Russell Ave NW

Cons

- Strong Horizontal elements do not relate well with neighborhood.
- Lack of entry definition along street level
- Monotonous upper level composition.
- No opportunities to tie through block connection to retail/restaurant uses.



OPTION B

Pros

- Continuous upper level setback to decrease scale.
- Maximizes site potential for square footage and number of guest rooms.
- Through block pedestrian connection between Russell Ave NW & 20th Ave NW
- Parking access from Russell Ave NW

Cons

- Least façade articulation at upper and lower levels.
- No opportunities to tie through block connection to retail/restaurant uses.
- Lack of interest in facade - to regular.



OPTION C (PREFERRED)

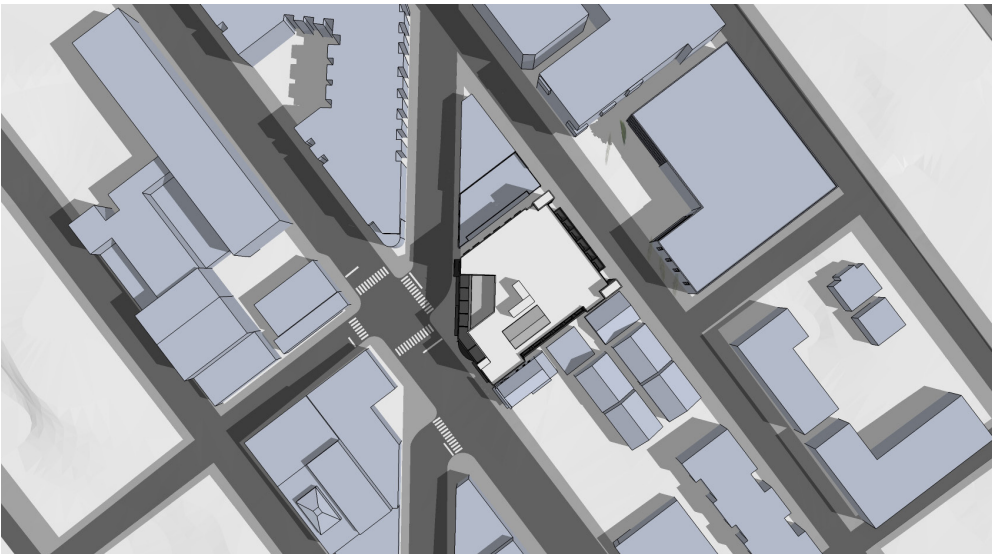
Pros

- Strong base – scale and material relates to the historic character of the Ballard Neighborhood
- Upper level is set back to decrease scale in asymmetrical nature.
- Rhythmic pattern of punched openings framed in durable materials
- Overall setback creates interest in façade forms.
- Strong connection at street level to define pedestrian entries
- Through block pedestrian connection between Russell Ave NW & 20th Ave NW also connects to Restaurant/Retail opportunities.
- Parking access from Russell Ave NW

Cons

- Least efficient use of FAR
- Least rooftop area for roof deck

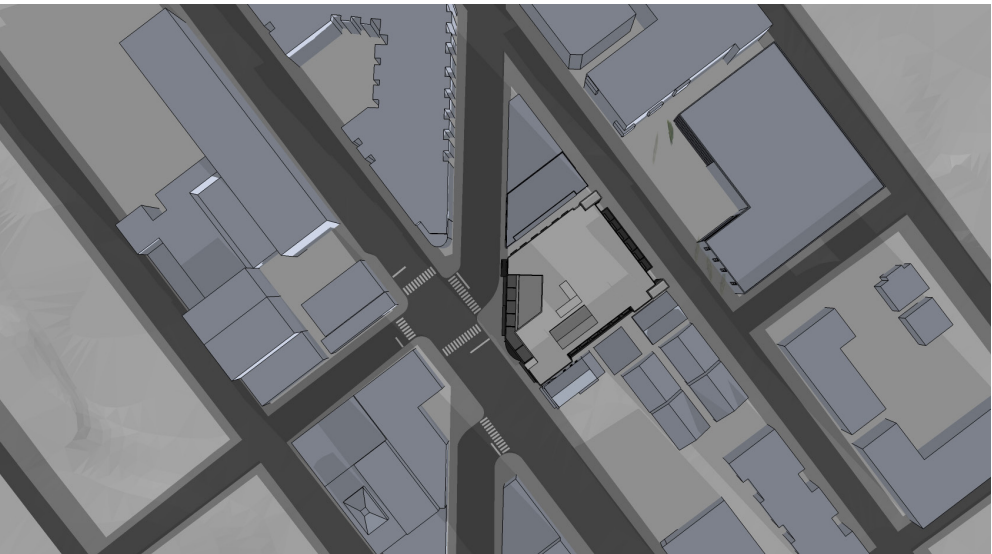
CONCEPTS



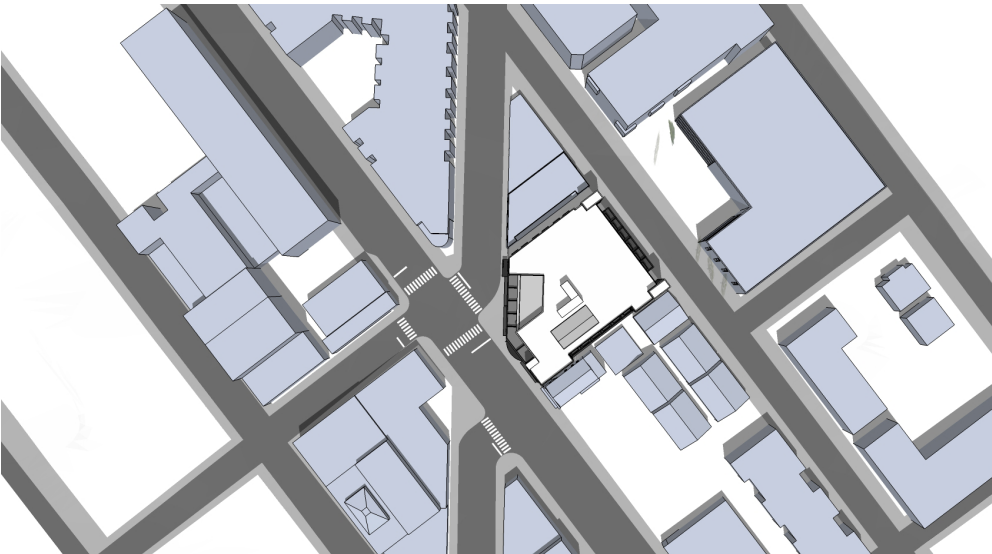
JUNE 21 | 9:00 AM



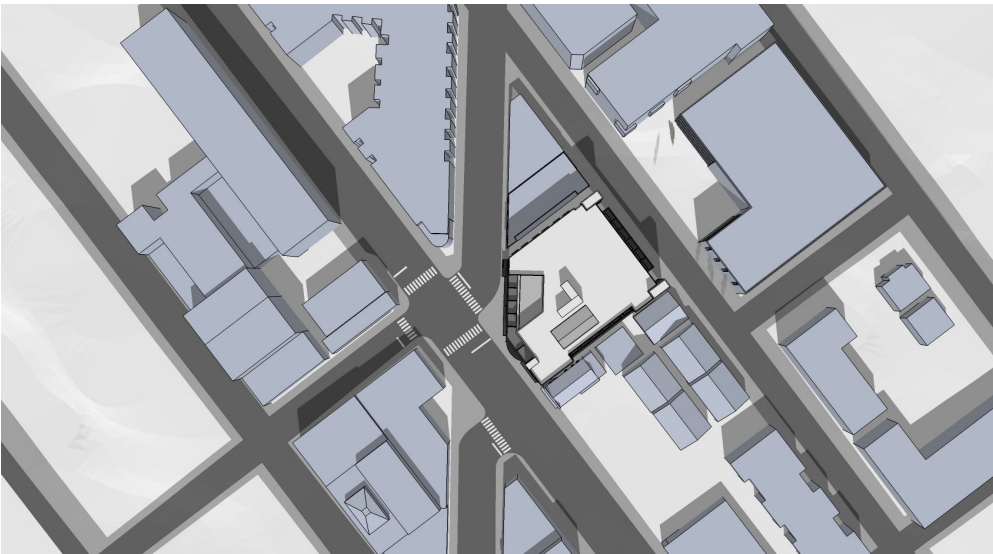
MARCH/SEPTEMBER 21 | 9:00 AM



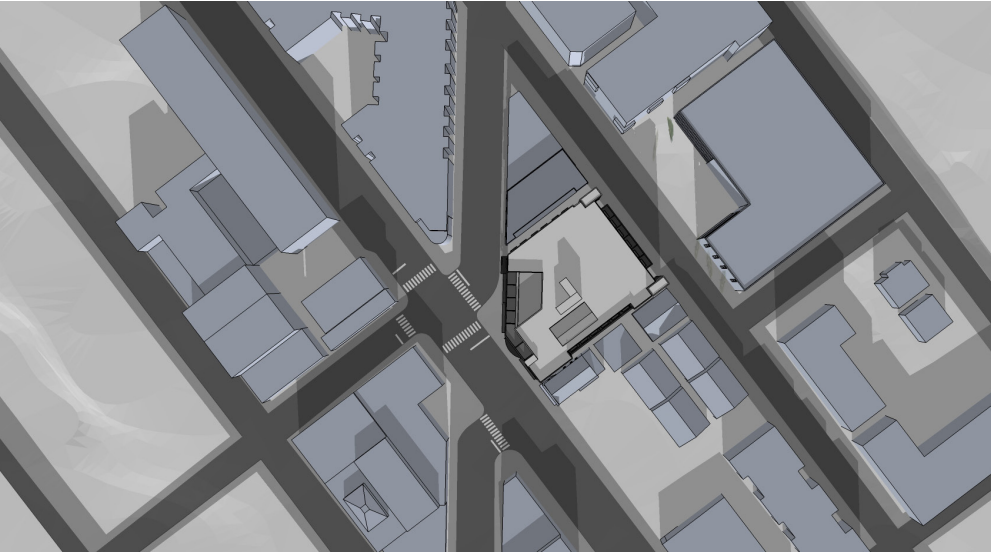
DECEMBER 21 | 9:00 AM



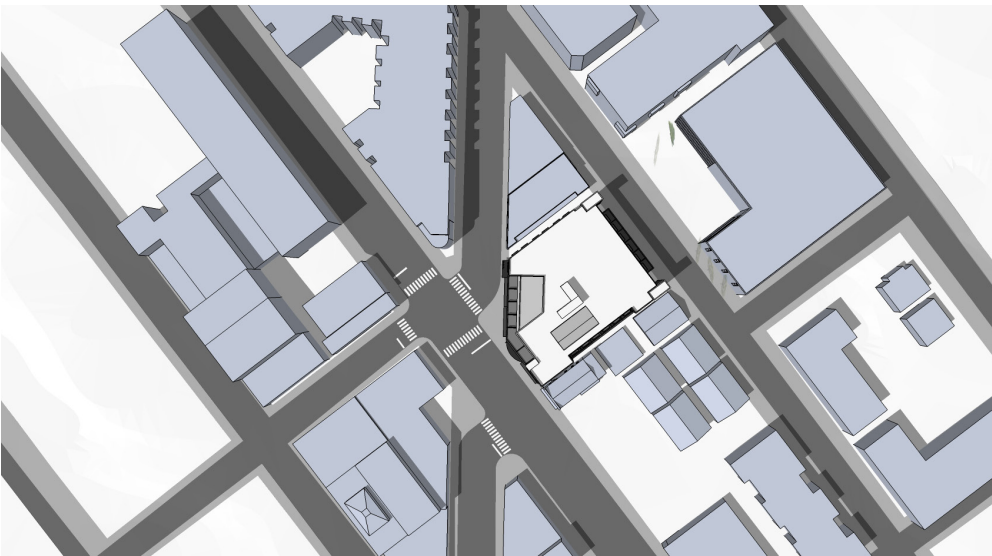
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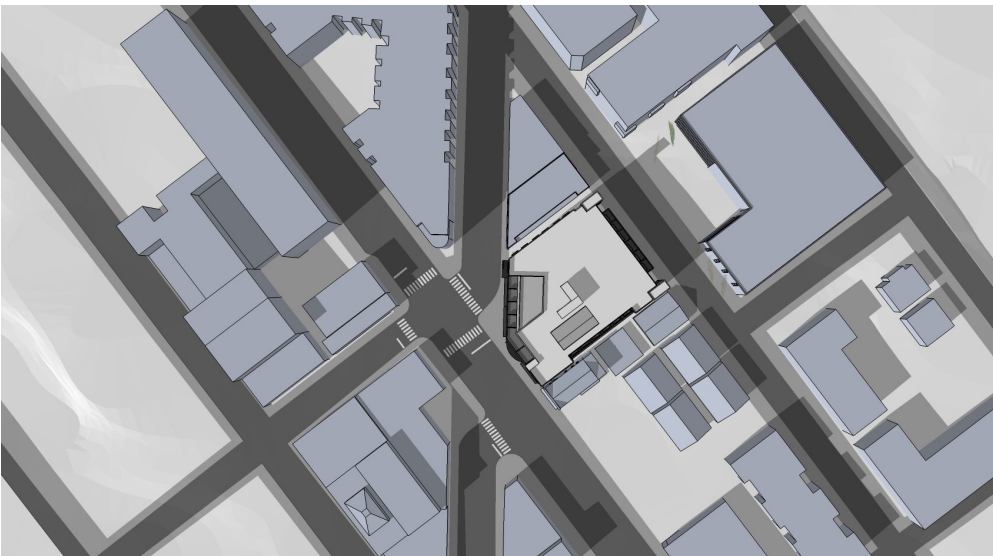
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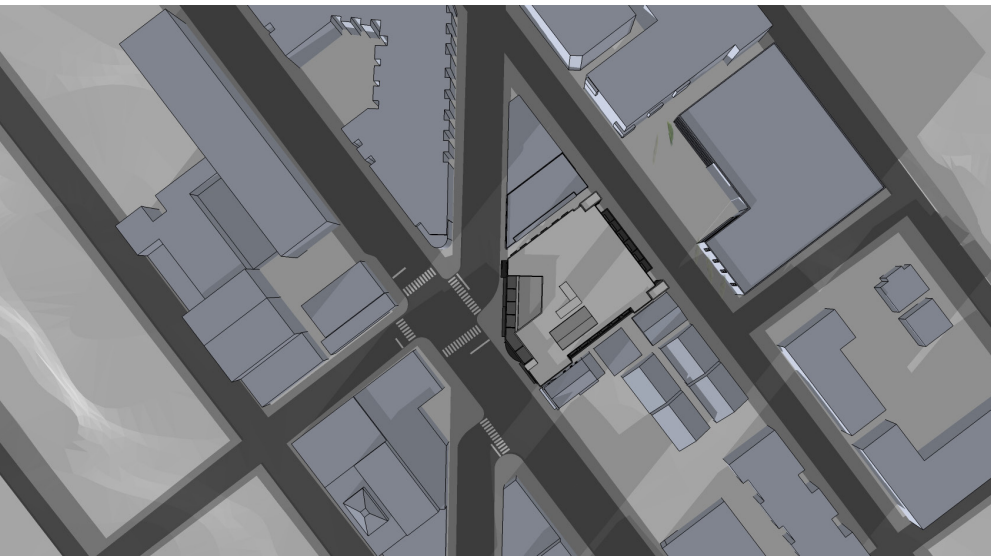
DECEMBER | 12:00 PM



JUNE 21 | 3:00 PM



MARCH/SEPTEMBER 21 | 3:00 PM



DECEMBER | 3:00 PM

WORK EXAMPLES
SKIDMORE JANETTE APD



EAGLE PARKING LLC | CLIENT

