



4726 15TH AVE NE
DESIGN RECOMMENDATION
MEETING
MARCH 12TH, 2018
DPD Project: #3025193



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DESIGN RECOMMENDATION MEETING

MARCH 12th, 2018

PROJECT ADDRESS
4726 15th Ave NE

PROJECT TEAM

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PROJECT INFORMATION

Proposal Summary

The proposed site is located within the University District Northwest Urban Center Village, which is largely comprised of single-family homes, townhouses and mid-size to large apartment/condominium buildings. This project is located along 15th Ave NE, just opposite University Christian Church on 15th Ave NE slightly north of 47th Street. This project site is within walking distance of the major shopping and entertainment centers; University Ave to the West, University Village to the East and the University of Washington campus to the South.

Existing Uses & Structures

The project site is comprised of 3 parcels under single ownership. Each parcel is occupied by one wood-framed single family residence, each accessed by surface parking along the improved alley between 47th & 50th. The combined lot measures approximately 120 feet wide by 108 feet deep.

All existing structures and paved surfaces located on the project site are proposed to be demolished, and the new project will occupy the entirety of the site.

Physical Features

The site drops approximately 10 feet from the high NE corner to the low SW corner.

Adjacencies

University Ave, University Village, buses to Downtown Seattle, University of Washington, University of Washington IMA, and Greenlake to the Northwest.

Project Vision

Our vision for this project is to provide a more substantial mixed use building on existing site and to have a significant impact on environmental, economical and social aspects of this neighborhood or even larger contexts.

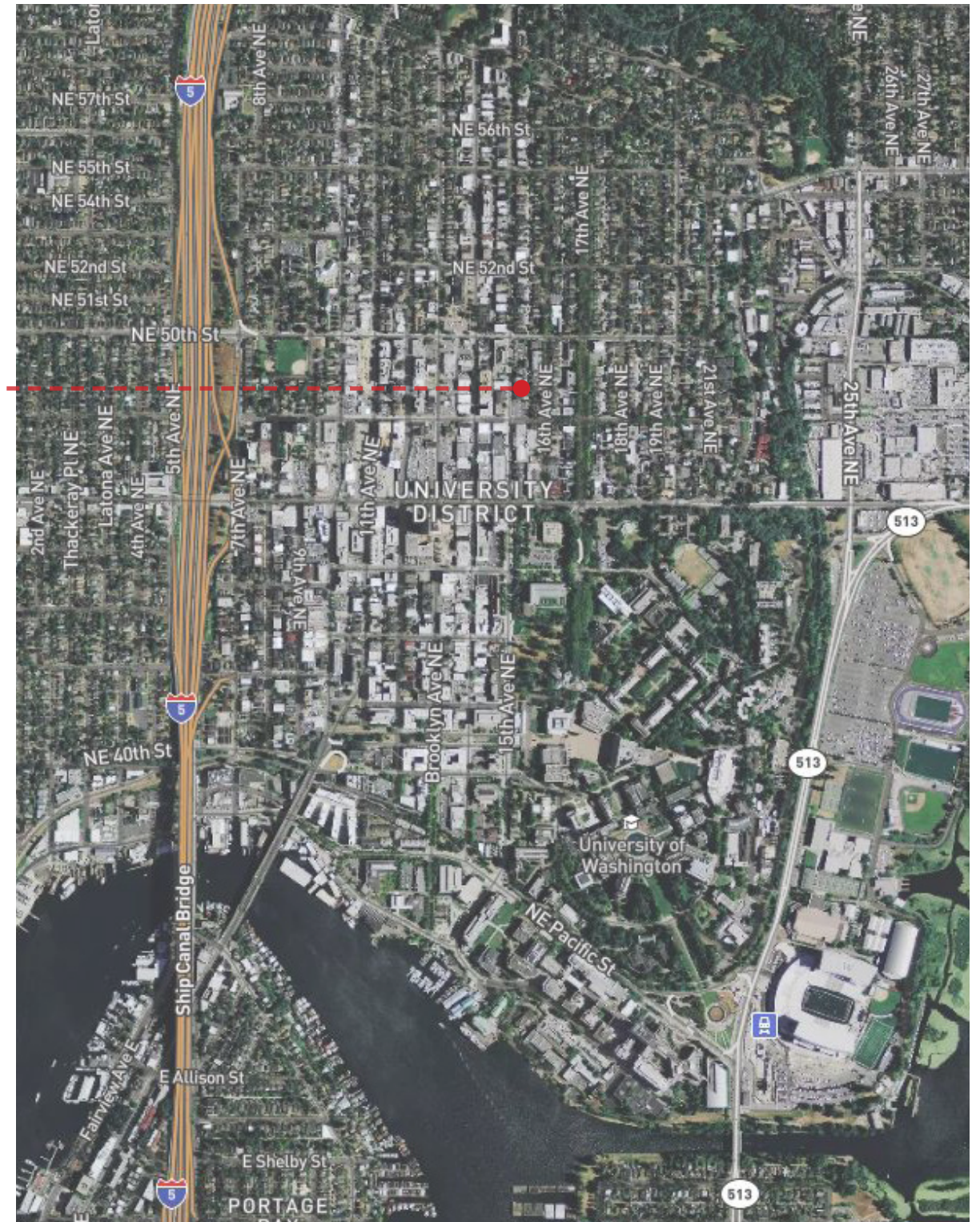
The site is currently occupied by three houses functioning as multi-family residences. The purpose of our project is to create an off-campus living environment that fosters interaction and a sense of community among its residents. It is comprised of various SEDU units as well as larger 2-bedroom/ 3-bedroom apartments, and will provide a number of semi-private amenities designed to enhance the quality of the student residents' day-to-day life. Our design goal is to provide an urban edge and enhance the urban characteristic of the site.

Program:

- Efficient market rate apartment units
- Studio units, open one bedroom units and two bedroom, three bedroom units.

Project Details:

Apartment Units:	127
Commercial Area:	1,597 gsf
Total GSF:	88,794.5 sq. ft
Parking Stalls:	41
	(24 on L-1P;17 on L-2P)





1. University Presbyterian Church



6. University Book Store



3. Deca Hotel



7. University Presbyterian Church



4. UW Tower



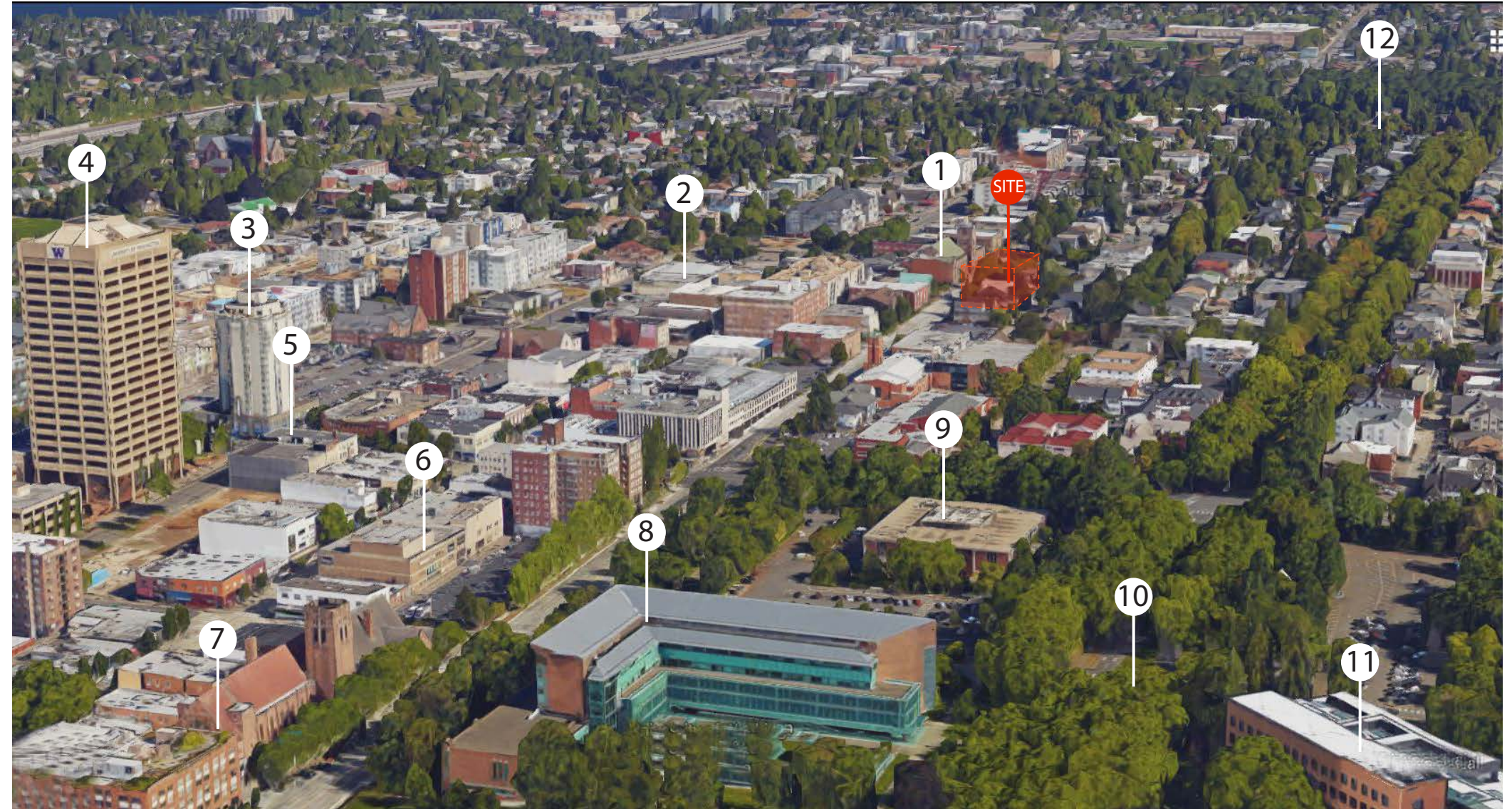
9. Burke Museum



5. Neptune Theatre



10. The "W" of Memorial Way



- 1. UNIVERSITY CHRISTIAN CHURCH
- 2. SAFEWAY GROCERY STORE
- 3. DECA HOTEL
- 4. UW TOWER
- 5. NEPTUNE THEATRE
- 6. UNIVERSITY BOOK STORE
- 7. UNIVERSITY PRESBYTERIAN CHURCH
- 8. WILLIAM H. GATES HALL

- 9. BURKE MUSEUM
- 10. THE "W" OF MEMORIAL WAY
- 11. FOSTER BUSINESS LIBRARY
- 12. RAVENNA PARK

Several nearby fraternities and sororities are of historical interest, Although, none are registered.

CONTEXT ANALYSIS: VICINITY

Vicinity

There are number of new developments to the west of the site especially on 47th St and 11th Ave , 12th Ave and Brooklyn Ave, There are a few major recently completed developments such as AVA residents , LIV Apartments, ACACIA Court and Bridges.

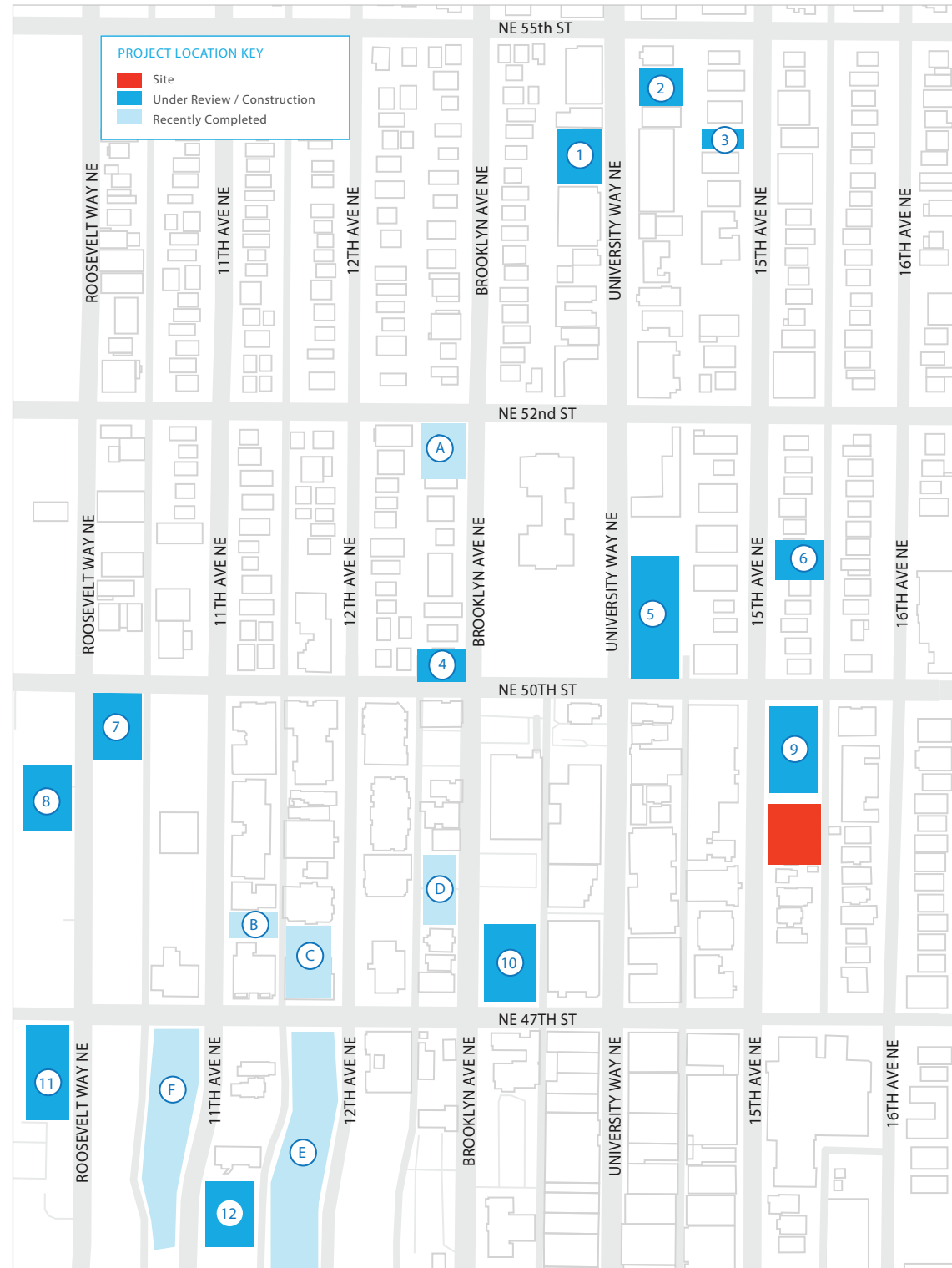
Housing developers have been building new vibrant, texturally interesting, and modern projects in the University District.

BUILT PROJECTS

- A. 5043 Brooklyn Ave NE
- B. Savanna @ 11th
- C. Academia Court
- D. LIV Brooklyn Ave
- E. BRIDGES @ 11TH
- F. AVA U-District

PROPOSED PROJECTS

- 1. 5247 UNIVERSITY WAY NE
- 2. 5260 UNIVERSITY WAY NE
- 3. 5253 15TH AVE NE
- 4. 5001 Brooklyn Ave NE
- 5. 5000 University Way NE
- 6. 5020 15th Ave NE
- 7. 4750 Roosevelt Way NE
- 8. 4737 Roosevelt Way NE
- 9. 4738 15th Ave NE
- 10. 4700 Brooklyn Ave NE
- 11. 4555 Roosevelt Way NE
- 12. 4510 11th Ave NE
- 13. 4501 12TH AVE NE



BUILT PROJECTS



5043 BROOKLYN AVE NE
4 Stories
Residential



SAVANNA
4710 11th Ave NE
7 Stories
Mixed Use
40 Residential Units



ACACIA COURT
4707 12th Ave NE
6 Stories
Mixed Use



LIV APARTMENTS
4717 Brooklyn Ave NE
7 Stories
Mixed Use
56 Residential Units



BRIDGES @ 11TH
4557 11th Ave NE
7 Stories
184 Residential Unites



AVA U-DISTRICT
4535 12th Ave NE
7 Stories
Mixed Use
167 Residential Units

PROPOSED PROJECTS



UNIVERSITY WAY APARTMENTS
 5247 UNIVERSITY WAY NE
 6 Stories
 Mixed use
 59 Residential Units



5260 UNIVERSITY WAY NE
 7 Stories
 Mixed use
 69 Residential Units



5253 15TH AVE NE
 4 Stories
 Mixed use
 28 SEDU Residential Units



THE STAX
 5001 Brooklyn Ave NE
 6 Stories
 Mixed Use
 60 Residential Units
 Rezoned from LR3 to NC3-65'



HUB ON CAMPUS SEATTLE
 5000 University Way NE
 7 Stories
 Mixed Use
 111 Residential Units



STUDIO 77
 5020 15th Ave NE
 4 Stories
 Mixed use
 31 SEDU Residential Units



4750 ROOSEVELT WAY NE
 6 Stories
 Mixed Use
 60 Residential Units



FULLER SEARS
 4737 Roosevelt Way NE
 6 Stories
 Mixed use
 66 Residential Units



BELLWETHER UCC
 4738 15th Ave NE
 7 Stories
 Mixed Use
 133 Residential Units



4700 BROOKLYN AVE NE
 6 Stories
 Mixed Use
 74 Residential Units



AMERICAN CAMPUS COMMUNITIES
 4555 Roosevelt Way NE
 8 Stories
 Mixed Use
 168 Residential Units 6 & 2 office levels



MOD STUDIOS
 4510 11th Ave NE
 7 Stories
 Mixed Use
 201 Residential Units

CONTEXT ANALYSIS

Neighborhood Zoning

Overview

Project Information

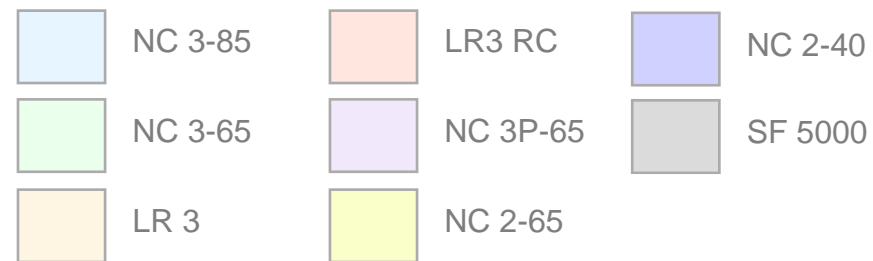
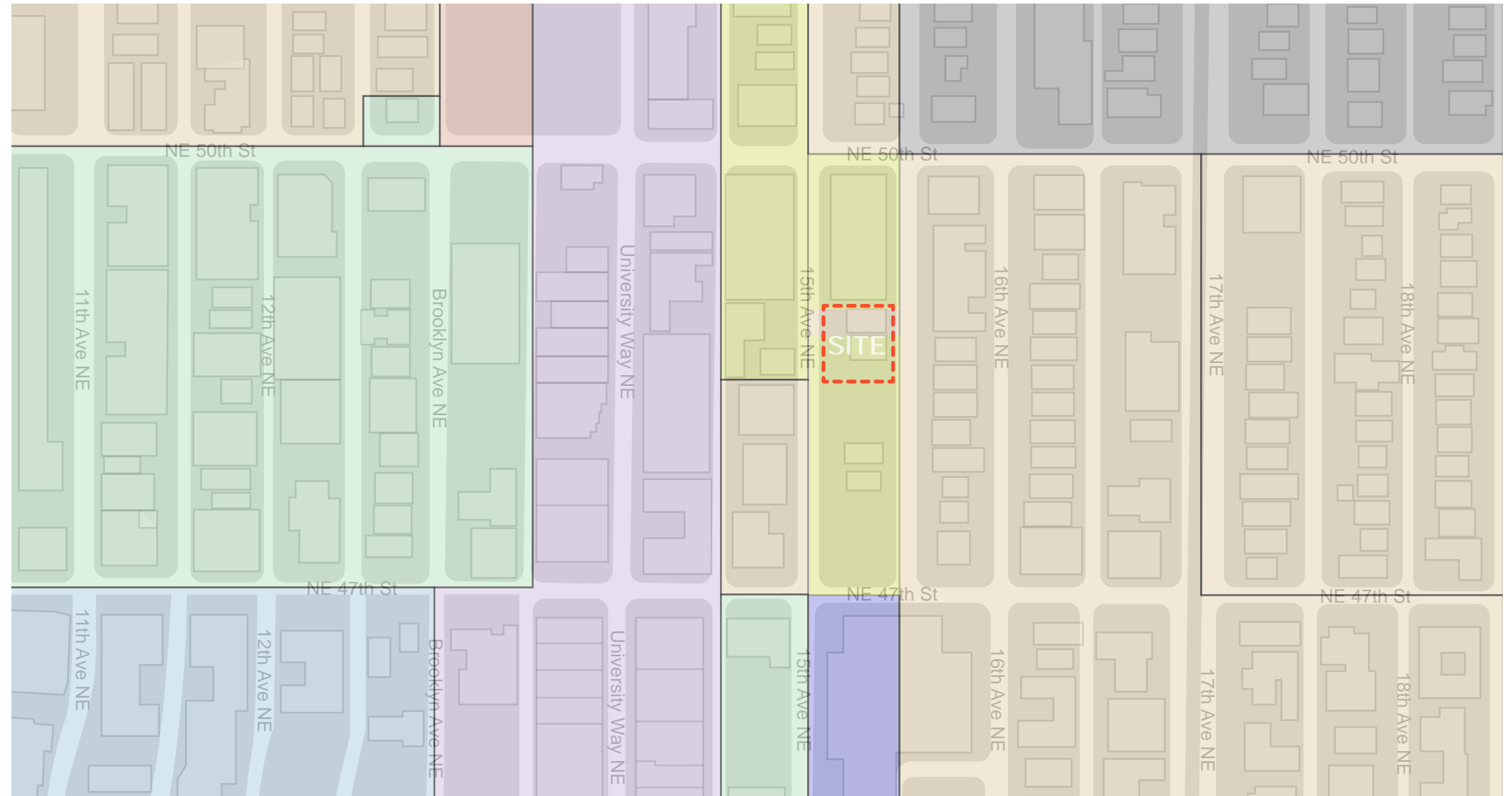
Parcel: 8823902160 , 8823902165 & 8823902170

Lot Area: 12,960 (120' x 108') sqf
 Zoning: NC 2-65/LR3 (Requires Contract Rezone)

Overlay: University District

Street Classification: 15th Ave NE (principal Arterial)

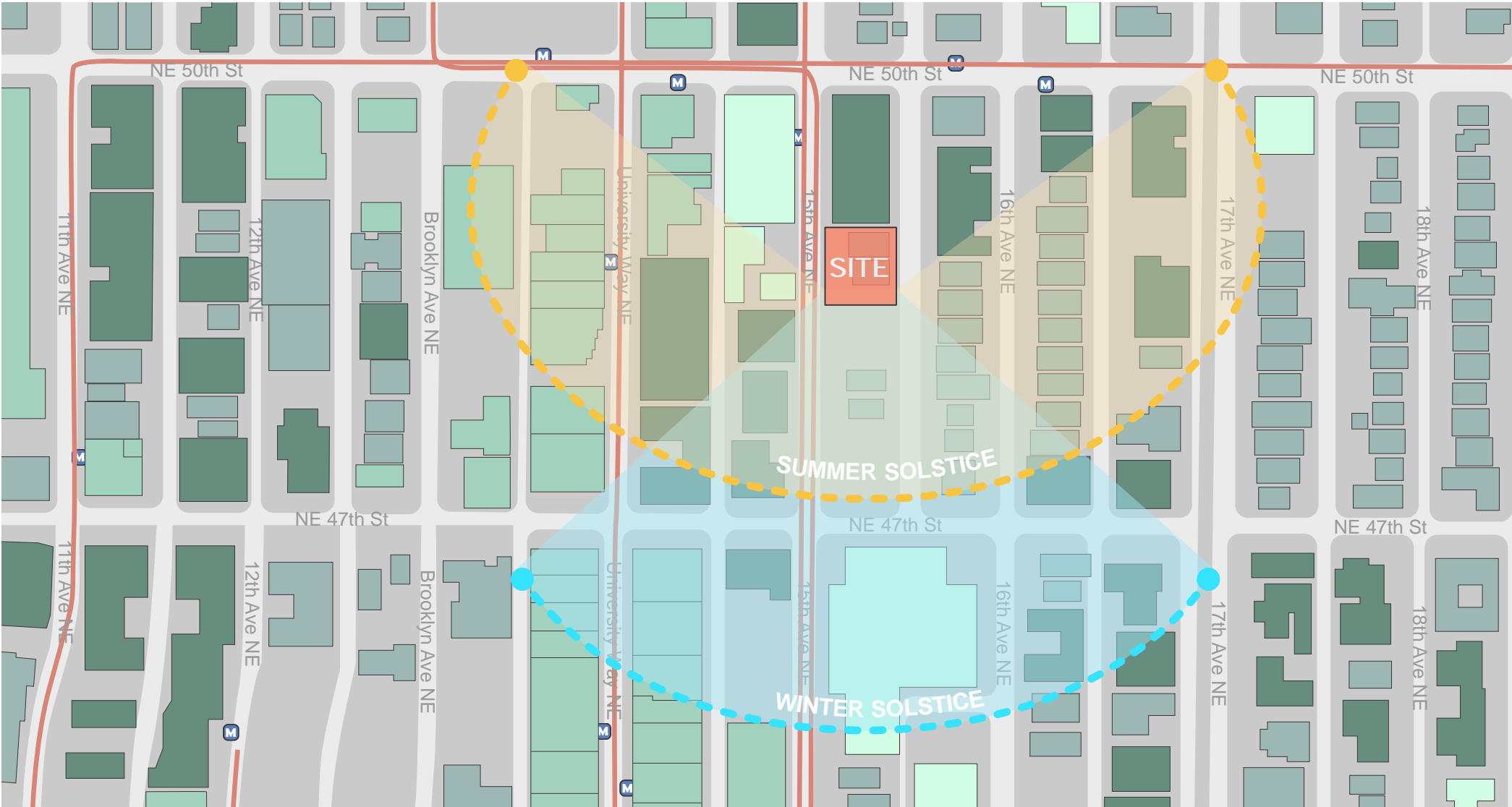
Frequent Transit: yes



⊕ ZONING MAP

CONTEXT ANALYSIS

Land Use/Transportation



- Institutional/ Others
- Commercial
- Single Family Residence
- Multi Family Residence
- Metro Bus Stops
- Metro Bus Routes

The site is located on 15th Ave NE just north off 47th Street. The surrounding area mostly includes single and multi family residential buildings. To the north of the site along 15th Ave NE are mostly mid-size to low rise multifamily buildings. Multiple commercial structures incorporating restaurants, grocery stores, and gas station are located to the west of University Way between 40th and 50th St.

15th Ave NE. is a moderately busy arterial street and allows for easy transit south towards Capital Hill, and Downtown Neighborhoods; to Ballard. Various Metro Bus routes operate along 15th Ave NE provide direct easy access to any neighborhood in the city. The nearest Metro Bus Stations is opposite less than 2 min walking distance from the site.

CONTEXT ANALYSIS: NEIGHBORHOOD

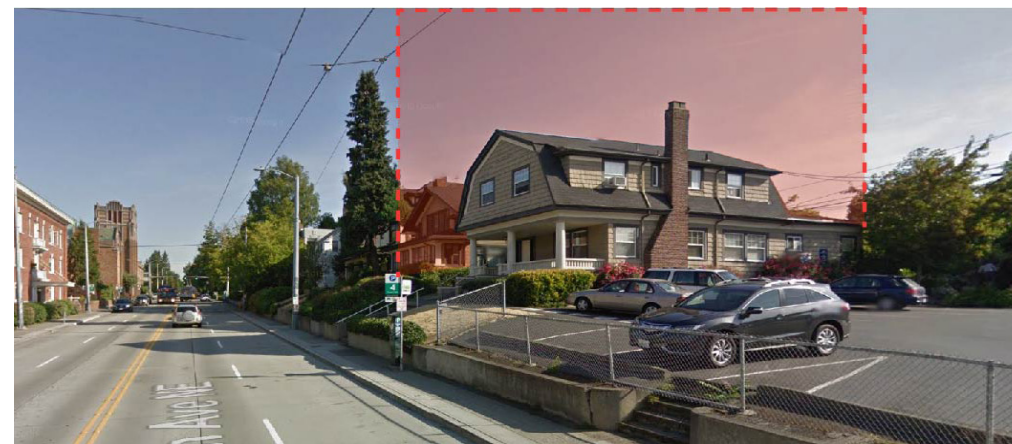
Street Elevations



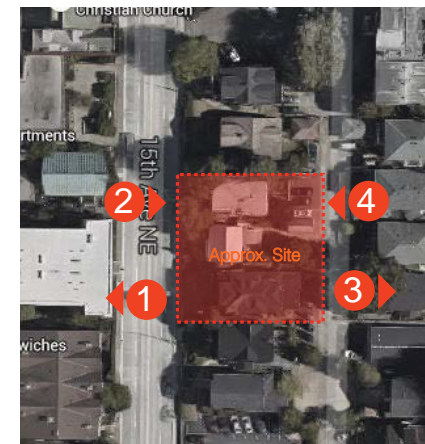
1 15th Ave NE towards West



2 15th Ave NE towards East



Key Plan 





3 Alley towards West



4 Alley towards East



View A



View B



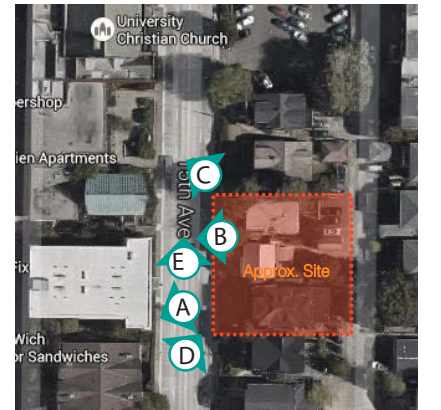
View C



View D



View E



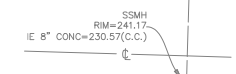
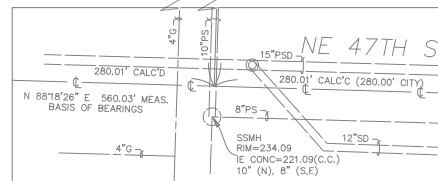
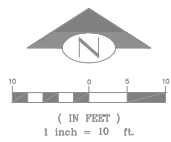
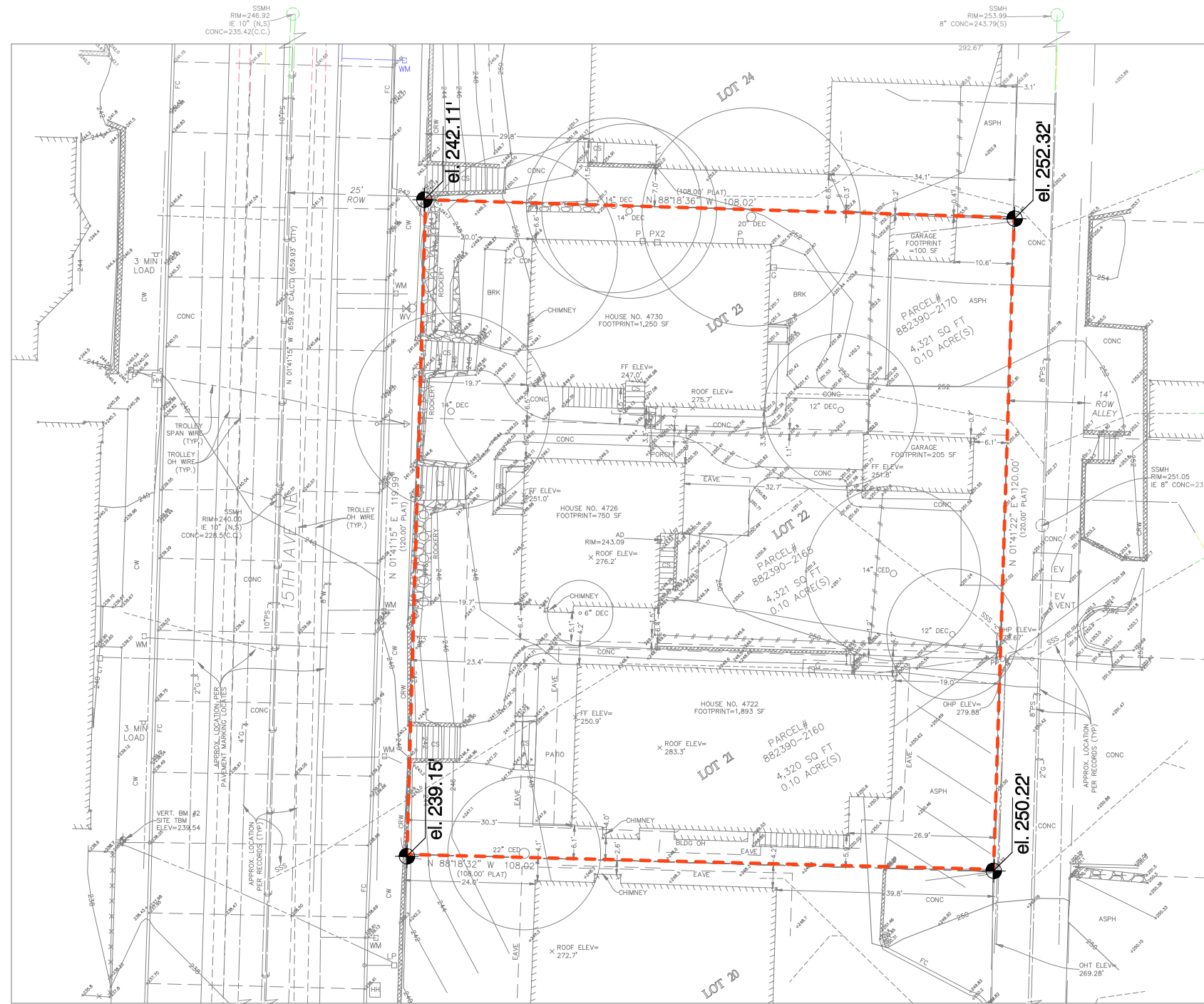
Key Plan ⊕

CONTEXT ANALYSIS

Existing (Survey)

LEGEND

	AREA DRAIN
	BUILDING
	CENTERLINE ROW
	RETAINING WALL
	FENCE LINE (CHAIN LINK)
	FENCE LINE (WOOD)
	FIRE HYDRANT
	GAS LINE
	GAS METER
	GAS VALVE
	LIGHT POLE
	NAIL AS NOTED
	MONUMENT IN CASE (FOUND)
	MONUMENT CASE (EMPTY)
	POWER HAND HOLE
	POWER (OVERHEAD)
	POWER (UNDERGROUND)
	POWER POLE
	REPAIR & CAP (AS NOTED)
	ROCKERY
	SEWER LINE
	SEWER MANHOLE
	SIGN (AS NOTED)
	TELEPHONE (OVERHEAD)
	TREE (AS NOTED)
	TREE TYPE
	WATER LINE
	WATER METER
	WATER VALVE
	CENTER CHANNEL
	CALC'D
	CB
	COS
	CONC
	COR
	DEC
	ELEV
	FF
	G
	LS#
	MEAS
	MON
	OHP
	OHT
	PP
	PROP
	(R)
	ROS
	SD
	SSMH
	SSS
	SF
	W



ZONING

Seattle Municipal Code, Land Use Regulations:

(The following are applicable to all three alternative schemes)

Chapter 23.41 Early Project Implementation

012.B Development Standard Departures

- Departures may be granted from any Land Use Code standard or requirement, except for the following: resid. density limits, Floor Area Ratios, max. size of use, structure height, storage of solid waste containers, noise and odor stds., reqs. For streets, alleys, and easements per Chapt. 23.53, definitions, and measurements.
THREE DEPARTURES REQUESTED.

Chapter 23.47A Commercial

004 Permitted and Prohibited Uses

- Residential uses in mixed use development permitted outright
UPPER FLOOR RESIDENTIAL AND GROUND FLOOR NON-RESIDENTIAL (15TH AVE. NE) PROPOSED

005 Street Level Uses

- C.2. Residential Uses may not occupy more than 20% of the street-level street facing facades when facing an arterial
NO RESIDENTIAL USES AT GROUND FLOOR

008 Street-Level Development Standards

- A.1. Applies to structures that contain residential uses in C zones.
- A.2.b. Blank segments of the street-facing façade between 2' & 8' above the sidewalk may not exceed 20' wide
NO BLANK FACADES
- A.2.c. The total blank façade segments may not exceed 40% of the width of the facade of the structure along the street
- A.3 Street-level facing facades must be located within 10' of the property line, unless as otherwise approved
STREET-LEVEL FACING FACADE IS 6' BACK FROM STREET LOT LINE
- B.2 60% of the street facing facade between 2' & 8' shall be transparent ; view into space, or in live work units into 30" deep display windows
CONTINUOUS STOREFRONT, EXCEPT AT STAIR, APPROX. 80-85% TRANSPARENT
- B.3 Nonresidential uses must extend an average of at least 30' and a minimum of 15' except if the depth requirements would result in a space greater than 50% of the structures footprint
30' AVERAGE 15' MINIMUM PROVIDED
- B.3.b Street level non residential uses shall have a floor to floor height of at least 13'
13' FLR TO FLR PROPOSED
- D.1 At least one residential use shall have a visually prominent pedestrian entry
MAIN RESIDENTIAL ENTRY IS VISUALLY PROMINENT

012 Structure Height:

- A.1 The maximum height is 65' per Land Use Map
- C.2 ...open railings, parapets, and fire walls may extend up to 4'-0" above the maximum height.
- C.4 Rooftop features including elevator and stair penthouses & mechanical equipment may not exceed 25% of the roof area.
- C.4.F Rooftop features including elevator and stair penthouses & mechanical equipment may extend up to 16' above applicable height limit

013 Floor Area Ratio

- A.3 Above grade parking within or covered by a structure must be included in FAR calculations.
- C. Maximum FAR within SAOD: Mixed-use residential and non-residential structure: 5.75
3.75 – 4.25 FAR PROPOSED
- D. Gross floor area below grade not counted against FAR

014 Setbacks:

NONE REQUIRED; 15' @ ALLEY ABOVE 1ST LEVEL

016 Landscaping and Screening Standards

- A.2 Landscaping must achieve a Green Factor of .30 for any new structure over 4 units
- B.1 Street trees are required per SDOT, existing trees count toward the requirement

020 Odor Standards:

- A. Venting of odors, vapors, smoke, etc. shall be 10'-0" above the finished sidewalk grade, and shall be directed away to the extent possible from residential uses within 50'-0".

022 Light and Glare Standards:

- A. Exterior lighting shall be shielded from adjacent uses.
- B. Interior lighting in parking garages shall be shielded.

024 Amenity Areas

- A. Residential amenity areas of 5% of the total gross residential floor area including, but not limited to, decks, balconies, terraces, roof gardens, plazas, courtyards, play areas, or sports courts
- B.1 All residents must have access to at least one amenity space
- B.2 Amenity spaces may not be enclosed
- B.4 Common amenity areas must have a minimum horizontal dimension of 10' and be a minimum of 250 sq. Ft.
- B.6 Private balconies must have a minimum horizontal dimension of 6' and be a minimum of 60 sq. ft.
ALL COMMON AMENITY AREA PROVIDED AT ROOF TERRACE; AT LEVEL 1 LOUNGE AND EXERCISE AREA 1,000 TO 1,150 SF REQUIRED; SOME PVT. PATIO AREAS PROVIDED ON L2

030 Required Parking and Loading

- A. Parking required per 23.54.015
- B. Loading berths may be required per 23.54.035

032 Parking Location and Access

- A. Access to parking
PROVIDED OFF 15TH AVE NE
- 033 Transportation Concurrency
- All uses shall meet the transportation concurrency level-of-service per chapter 23.52

Chapter 23.53 Requirements for Streets, Alleys, and Easements

015 Improvement Requirements for Existing Streets in Residential and Commercial Zones

- Street improvements required per SDOT standards

015 Alley Improvements in all zones

- Alley improvements required per SDOT standards
2' DEDICATION PROVIDED AT ALLEY. 5'-6" SET BACK IS REQUIRED ON 15TH AVE NE.

035 Structural Building Overhangs

- A.1 8' vertical from sidewalk min.
- A.2 1' horiz, 2'-6" ht., Projection for architectural , or decorative features – eaves, etc.
- A.4 window bays/balconies – 8' above sidewalk, max. 3' horiz. Projection, 50% open area, 15' max. length, 2' separation (see additional specific requirements)

Chapter 23.54 Quantity and Design Standards for Off Street Parking

015 Required Parking

- A. Min. parking per SLUC 23.54.015,, Tables A and B, except as modified in this section
- K. Bicycle parking required at 1 stall per 4 units for multi-family structures'
- Table A, PARKING FOR NONRESIDENTIAL USES:
NONE REQUIRED; 41 PARKING STALLS PROVIDED
- Table B, PARKING FOR RESIDENTIAL USES:
NONE REQUIRED.
- Table E, PARKING FOR BICYCLES:
- General Sales and Services and Eating and Drinking Establishments – 1/12,000 sf long term, 1/4,000 sf short term; Multi-family – 1/4 units

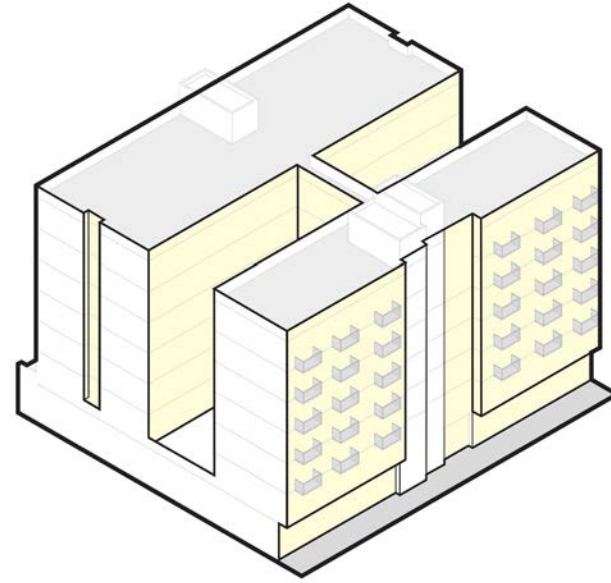
030 Parking Space Standards

- B.1A minimum medium stalls for residential use (<5 stalls provided))
- B.2 75% large stalls for nonresidential (<10 stalls provided)

040 Solid waste and recyclable materials storage and access

127 RESIDENTIAL UNITS PROPOSED – 921 SF PROVIDED

EDG SCHEME SUMMARY



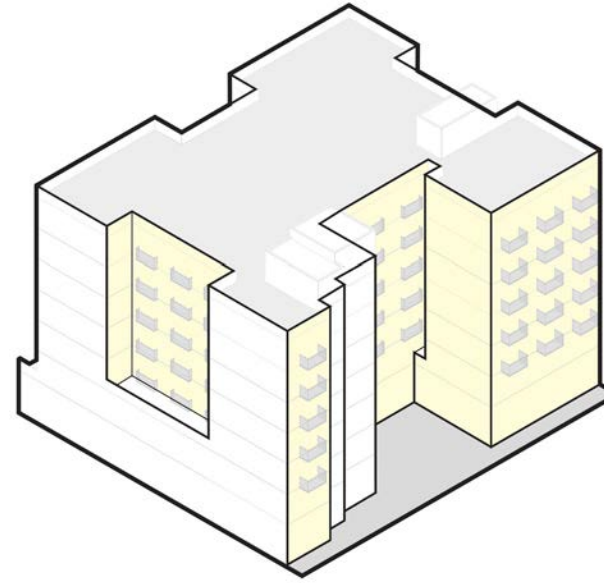
Alternative A

Pros:

- Arrangement of mass allows light penetration to interior units and corridors
- Configuration of vertical circulation provides identity to the entrance

Cons:

- Fewest Number of Units
- Interior Units don't have great views



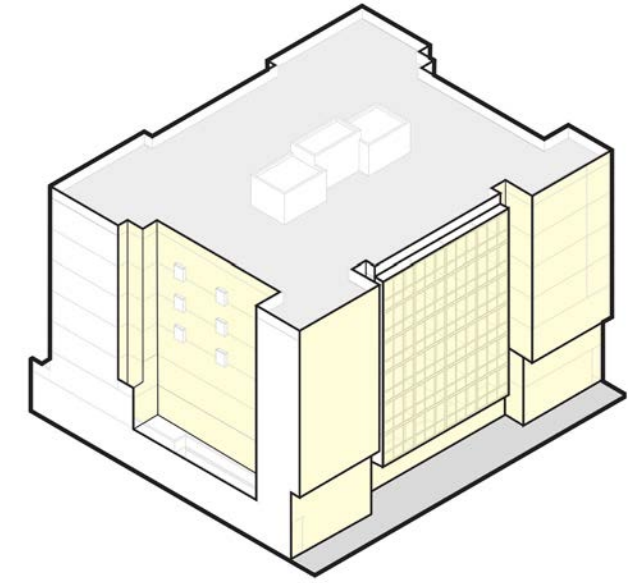
Alternative B

Pros:

- Mass is more centralized to accommodate more units
- Juliet balconies provide interior units with outdoor access without impeding light penetration
- Configuration of vertical circulation provides identity to the entrance and creates visual symmetry in the West facade

Cons:

- Interior units' views are hindered



Alternative C - preferred

Pros:

- The recessed/protruding balconies and massing give the building depth.
- Highest number of units.

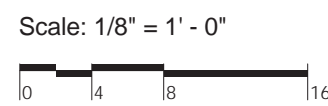
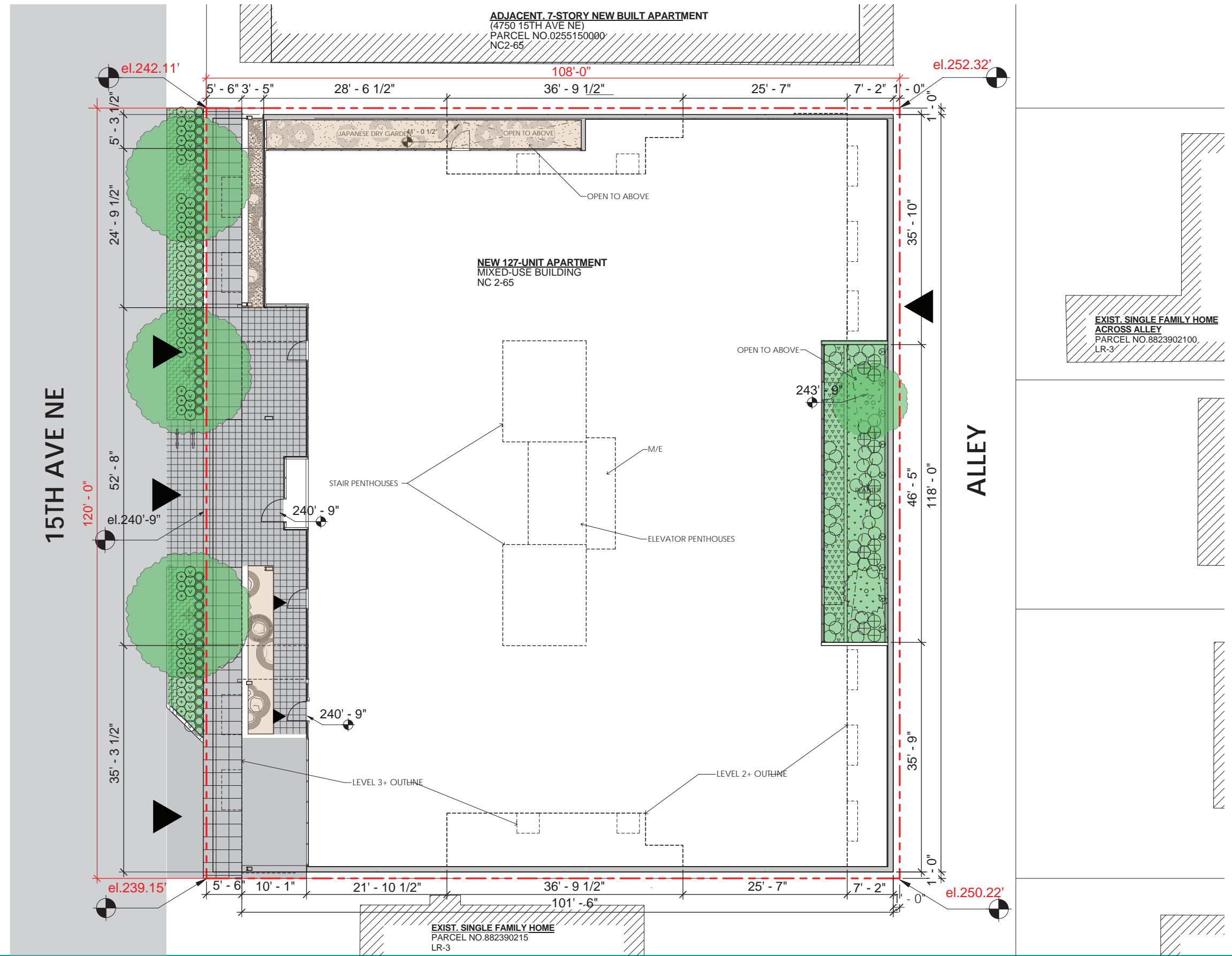
Cons:

- Least interior light penetration to corridors.

EDG BOARD RECOMMENDATIONS

PRIORITIES & BOARD RECOMMENDATIONS	APPLICANT RESPONSE
<p>1. Relationship of Building to Street</p> <p>A. The board preferred the center “bay” option. The board also appreciated the 1-2-1 rhythm along the street front and asked the applicant to retain the “bookend” language as the design develops. (CS2-1-2, CS2-C-2, DC2-B)</p> <p>B. The board felt that the commercial spaces need to relate directly to the street without steps. (CS1-C-2, CS2-B-2, PL2-A-2)</p>	<p>1. Relationship of Building to Street</p> <p>A. The center bay of the building has a strong open frame that sets it apart from the more solid appearing projecting facades to the North and South. Th building design maintains the 1-2-1, bookended, rhythm along 15th Ave. N shown in the EDG. This in-out pattern, although larger in scale, echoes the larger Bellwether Building to the North.</p> <p>B. The variation in topography along the street is relatively slight: 2’-0” from North to South across a frontage of 120’-0” (1.7%). We feel that Level 1 should all be at the same elevation to allow accessible access to building elevators and services. Level 1’s elevation was set at the midpoint of the street grade. The width of the building that is essentially at grade with the sidewalk is 58’-0” out of the building width of 117’-0”. It is also 10’ back from the sidewalk to the commercial and residential entrances. To the North and South of this entry court are concrete seating walls and Chinese Rock Gardens. This will activate the entire building frontage, especially at the central court entrance. In contrast, the Bellwether Building to the North has very little pedestrian activity until the Northernmost 1/3 of the building. Our seating wall and rock garden to the North of our entry court will also have the added benefit of providing more distance from the Bellwether’s garage entry. (SEE P37-41)</p>
<p>2. Relationship of Building to Alley</p> <p>A. The board suggested widen the light well and add interesting landscape, and suggested a tall, evergreen plant.</p> <p>B. The board concern about the safety of back units along the alley. (PL2-B)</p>	<p>2. Relationship of Building to Alley</p> <p>A. The depth of the East light well on Level 1 was increased from 6’-5” to 9’11”/ A 5’0” tall metal mesh fence/green-screen separates the Alley from the light well and the units along the Alley. The Landscape Architect has selected plants specifically for the light well. Additionally, the projecting overhead bay and the canopy is lower at the center.</p> <p>B. The light well places the four units’ balconies about 7’-0” back from the alley. The exterior patios of the three units to the South are elevated above the alley by 2’6” to 3’-0” with another 5’-0” of metal mesh fence/green-screen above that. These areas are very visible from the units above which will increase their security. (SEE P42-45)</p>
<p>3. Residential Entry</p> <p>A. The board directed to create a residential entry that is more recognizable and apparent.</p>	<p>3. Residential Entry</p> <p>The residential entry is located at the center of the court. It is flanked by two metal columns; distinguished by exterior paving treatment leading into the entry; has an entry vestibule and entry phone; and a lobby interior with furniture, unique lighting & wall finishes, and a fireplace. Additionally, the project bay above the canopy are lower at the center area with the residential entry. It will be visually recognizable and inviting. (SEE P37; P39)</p>
<p>4. Materials</p> <p>A. The board asked to specify high quality materials as presented at the early design meeting. (DC4 A)</p> <p>FROM EDG REPORT 02/08/2017</p>	<p>4. Materials</p> <p>Facade materials will be of exceptionally high quality and durability. (SEE P19; P46-47)</p>

SITE PLAN



SITE PLAN



DESIGN RESPONSE

CS2 URBAN PATTERN AND FORM.

STREETSCAPE ON 15TH AVE NE

2A. ARCHITECTURAL PRESENCE: The project is located in mid-block of the street. The commercial space and residential entry on the ground floor will activate the center of the block and create a strong street edge.

2B. OPEN SPACES: Ground floor provide 10.5' open space in front of residential entry, makes a strong connection to pedestrians.

2C. RELATIONSHIP TO THE BLOCK: The proposed structure will be similar in bulk to the building on the north and will re-sponse the datum lines and rhythm already created by that building. The building also steps back from the lower scaled zone across the alley, to the east, to provide a transition in bulk and scale.

2D. HEIGHT, BULK, AND SCALE. A contract rezone applicant has been submitted to change the north lot zoning from LR3-NC65. This would make all the lots along the 15th Ave NE block font the same.

DR. #2A & 2B. DEPARTURE REQUEST, SEE P.52-53



AERIAL VIEW

DESIGN CONCEPT



DESIGN CONCEPT

DC 2 Architectural Concept

2B. ARCHITECTURAL AND FACADE COMPOSITION: The facades are organized by massing, articulation and materials into distinct layered design elements. The largest blank area are located on the North and South facades. There are mitigated by materials and massing.

2C. SECONDARY ARCHITECTURAL FEATURES: Projecting solid and gridded bays, projecting and inset balconies, shading devices, canopies and other elements of the design combine to create visual depth and interest. This building will be similar in bulk and size to the neighboring building to the north and will respond to the datum lines and rhythm of the building.

2D. SCALE AND TEXTURE: The most visible facade of the building is the one facing the street. However, the design par-ti will be consistent on all sides of the building. Streets-cape elements, windows and balconies will provide human scale. Materials with vary in texture from smooth to rough.

2E. FORM AND FUNCTION: The building commercial, common, service, parking and residential functions of the building are legible and easy to perceive. Other than at the commercial space, flexibility is inherently difficult to accommodate in this type of building.

15TH AVE NW LOOKING EAST

MATERIAL CONCEPT



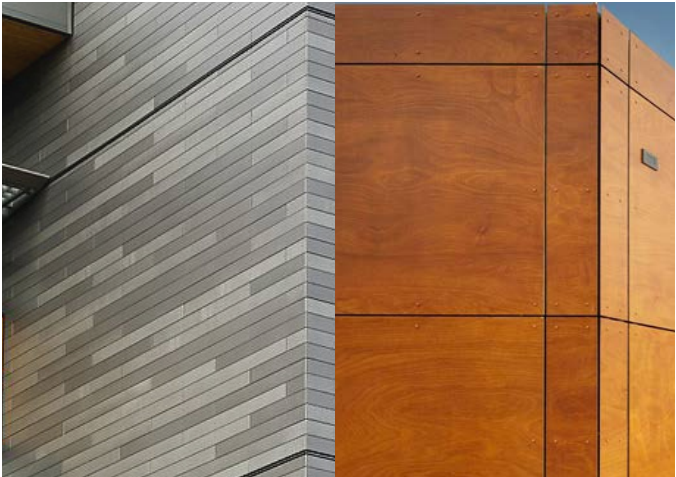
- REVEAL PANELS
- OKO SKIN CONCRETE SIDING
- METAL HORIZONTAL SUN SHADES
- METAL/GLASS BALCONY
- PRODEMA WOOD PANEL
- METAL PANEL
- EXTERIOR GLASS GUARDRAIL AT CENTRAL DECKING

DESIGN CONCEPT

DC 4 EXTERIOR ELEMENTS AND FINISHES
4A. EXTERIOR FINISH MATERIALS: Exterior finish materials are ProdeMa, Oko Skin horizontal siding, flat metal siding, reveal panels, metal and glass railings and canopies, exposed concrete and painted metal in keeping with the modern design.
4B. CLIMATE APPROPRIATENESS: Large canopies and sun shades are provided in this project. (ALSO SEE P.46-47)



REVEAL PANELS METAL PANELS



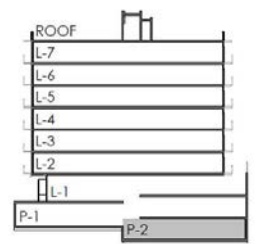
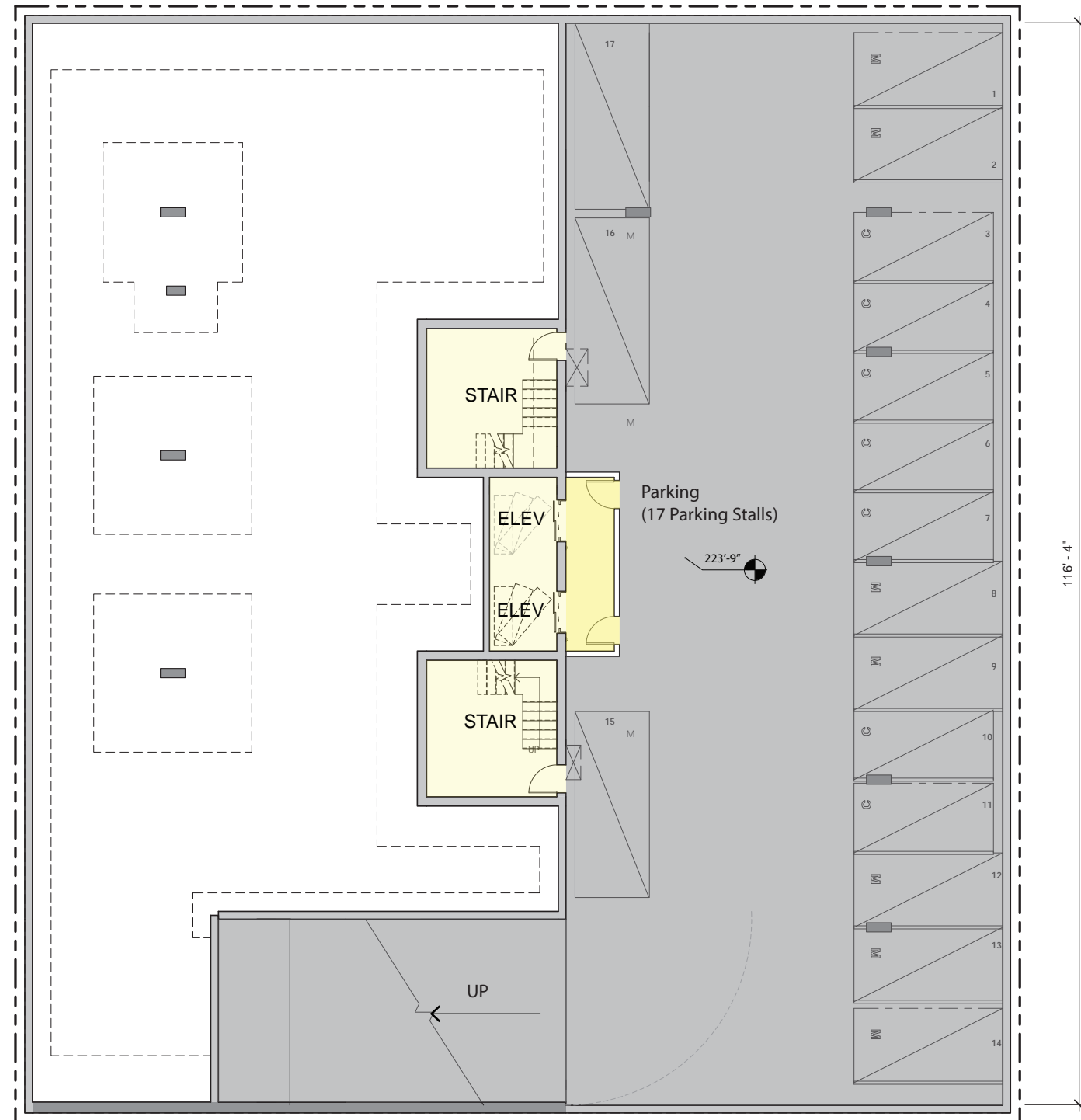
OKO SKIN PRODEMA

SOUTHWEST CORNER ON 15TH AVE NE

PLAN

PARKING LEVEL -2P

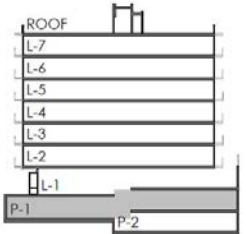
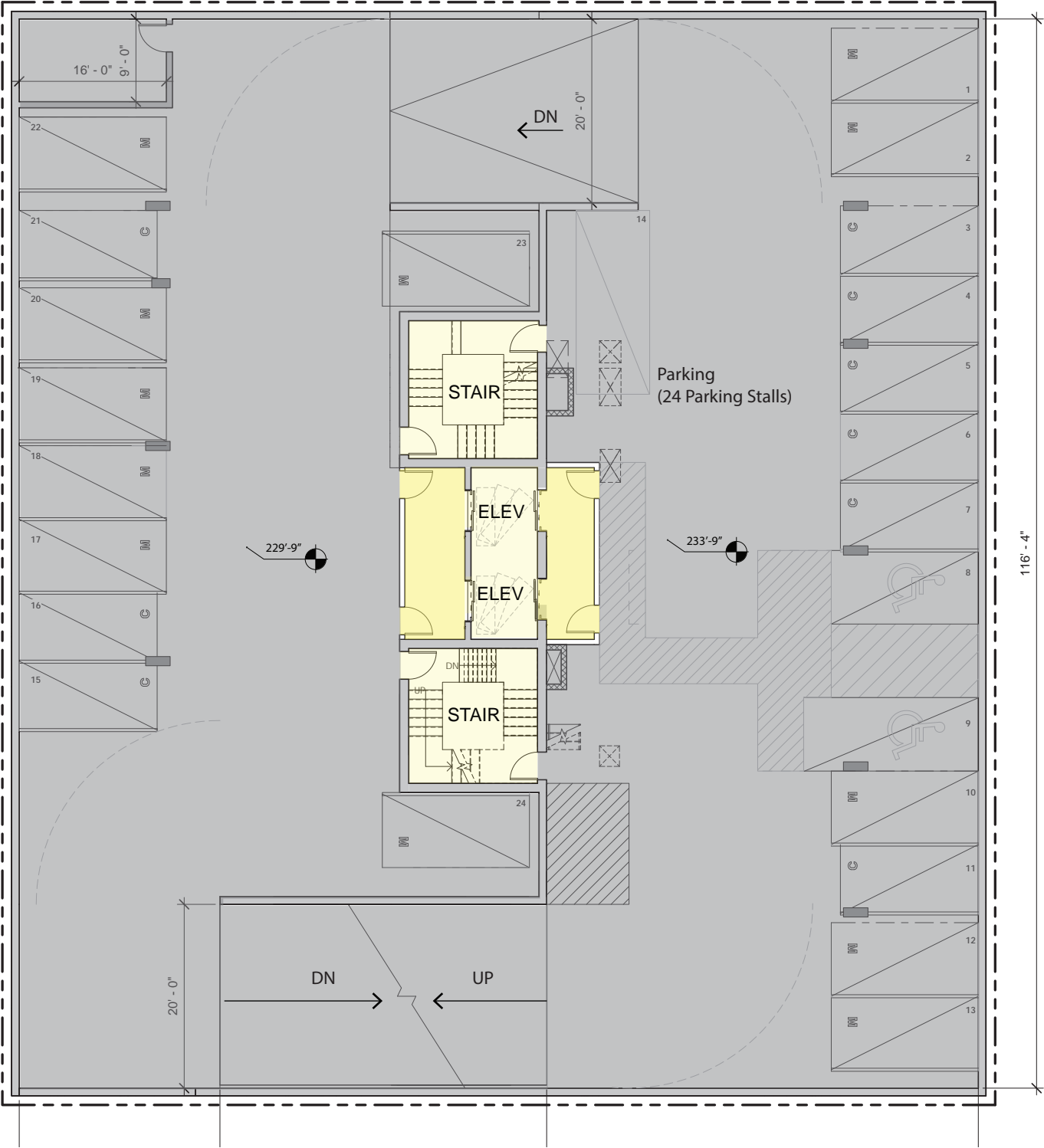
- ME / GARBAGE / PARKING
- STAIR / ELEVATOR



PLAN

PARKING LEVEL -1P

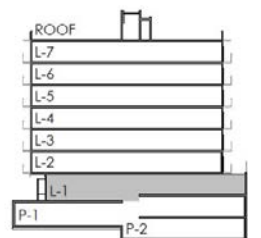
- M/E / GARBAGE / PARKING
- STAIR / ELEVATOR



PLAN

FLOOR PLAN: LEVEL 1

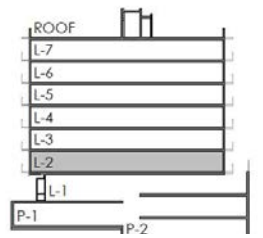
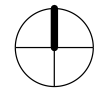
- LOBBY
- COMMERCIAL
- GYM / LOUNGE
- BICYCLE STORAGE
- SEDU STORAGE
- LEASING
- STAIR / ELEVATOR
- LANDSCAPE
- DRY GARDEN
- CORRIDOR
- M/E / GARBAGE



PLAN

FLOOR PLAN: LEVEL 2

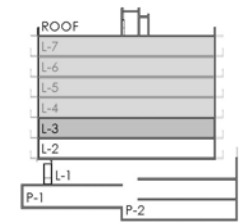
- RESIDENTIAL UNITS
- STAIR / ELEVATOR
- CORRIDOR
- M/E / GARBAGE



PLAN

FLOOR PLAN: LEVEL 3-7

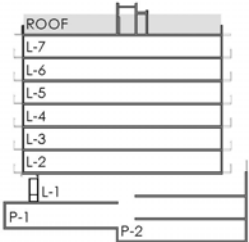
- RESIDENTIAL UNITS
- STAIR / ELEVATOR
- CORRIDOR
- M/E



PLAN

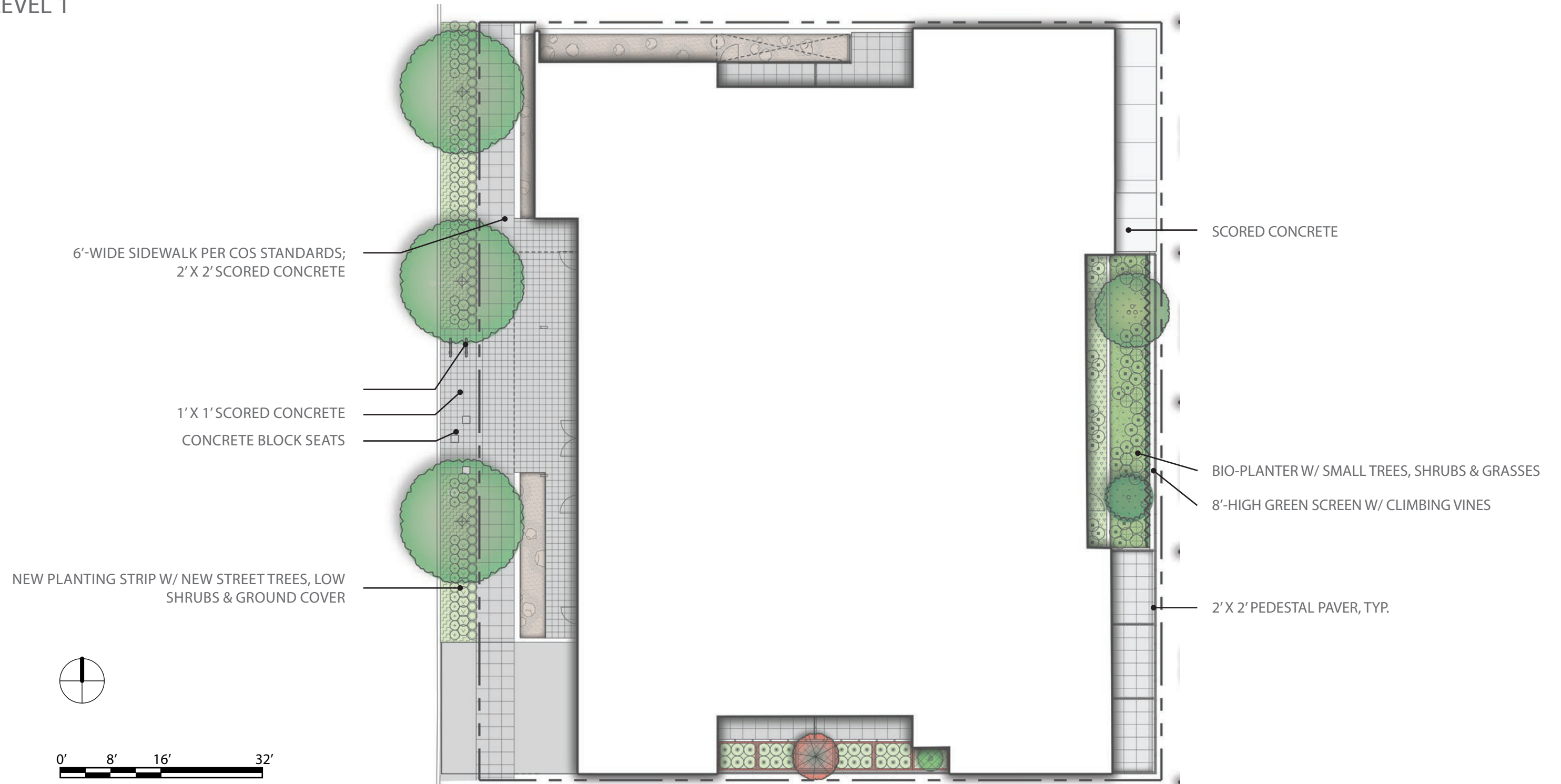
FLOOR PLAN: ROOF

- STAIR / ELEVATOR
- CORRIDOR
- M/E



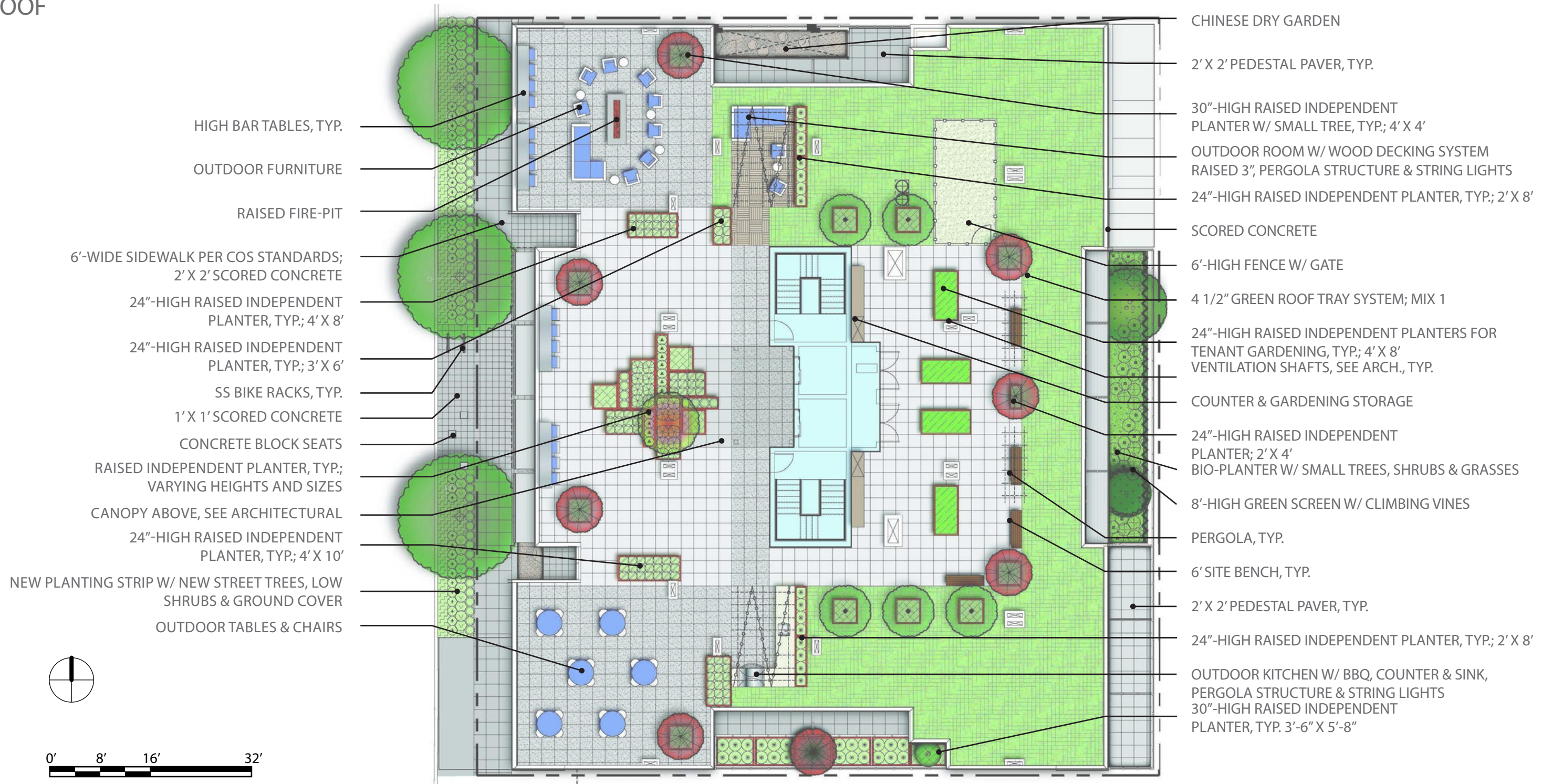
LANDSCAPE PLAN

LEVEL 1

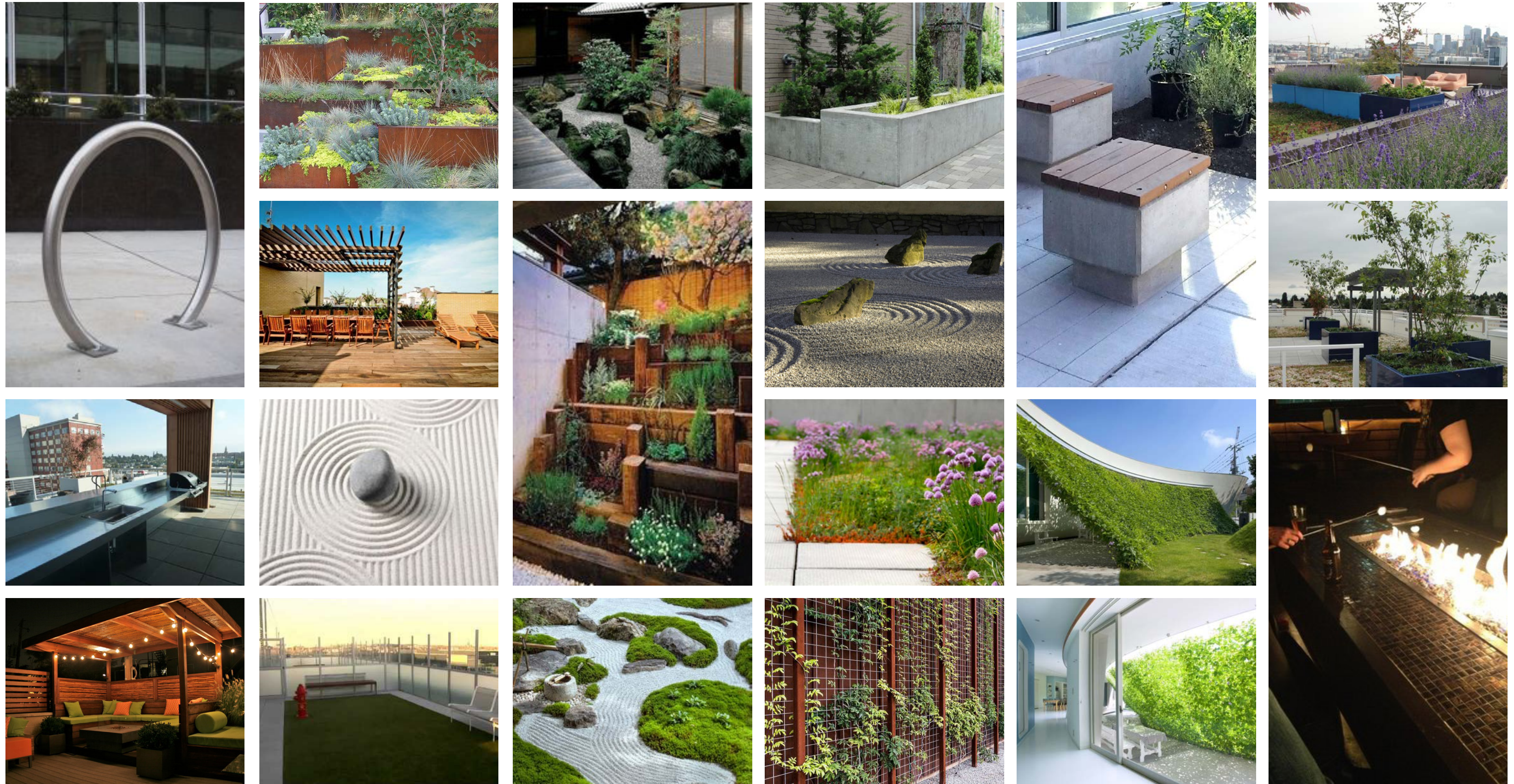


LANDSCAPE PLAN

ROOF



LANDSCAPE CONCEPT



PLANT PALETTE



ALLEE ELM (STREET TREE)



SHORE PINE



BLOODGOOD JAPANESE MAPLE



VINE MAPLE



SHARP'S PYGMY JAPANESE MAPLE



RUFA BAMBOO



DAVID'S VIBURNUM



HEAVENLY BAMBOO



GOLDFLAME SPIREA



KELSEY DOGWOOD



ENGLISH LAVENDER



DEER FERN



EVERGREEN CLEMATIS



DWARF MAIDEN GRASS



TUFTED HAIRGRASS



SLOUGH SEDGE



JAPANESE PACHYSANDRA



BEACH STRAWBERRY



KINNIKINNICK



GREEN ROOF

ELEVATION

West Elevation



West Elevation Along 15th Ave NE

ELEVATION

South Elevation



South Elevation Along Adjacent Property

ELEVATION

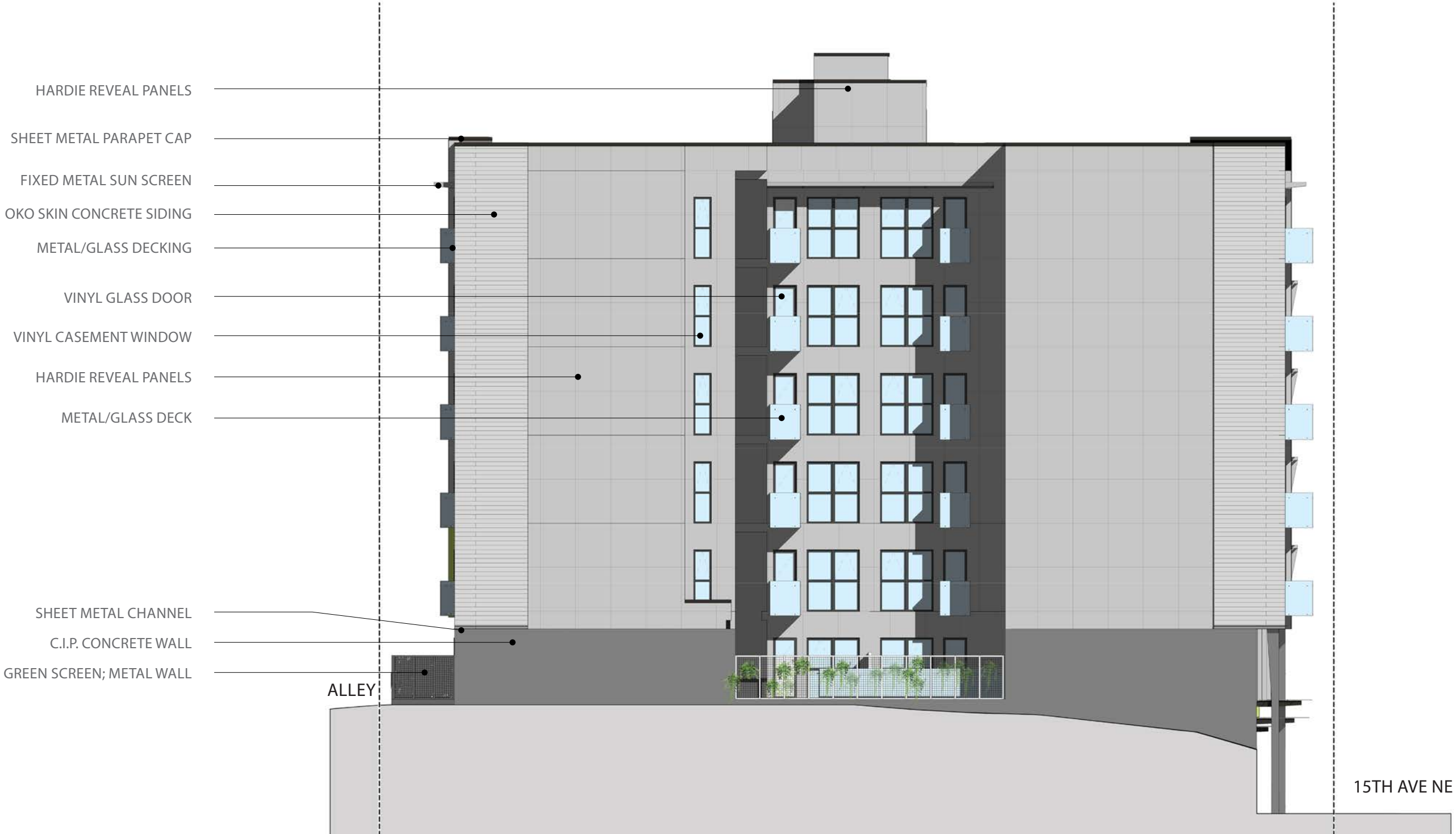
East Elevation



East Elevation Along Alley

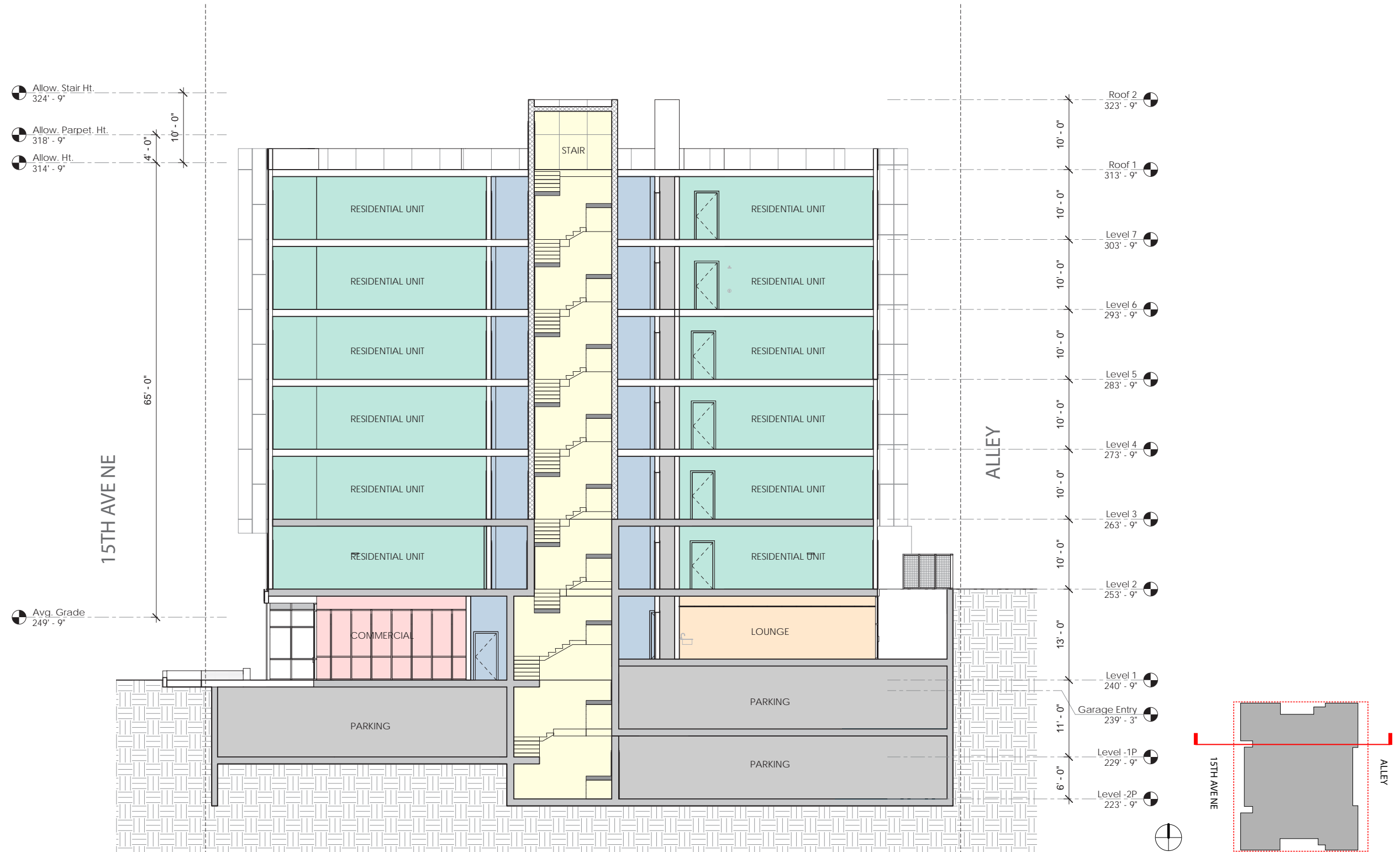
ELEVATION

North Elevation

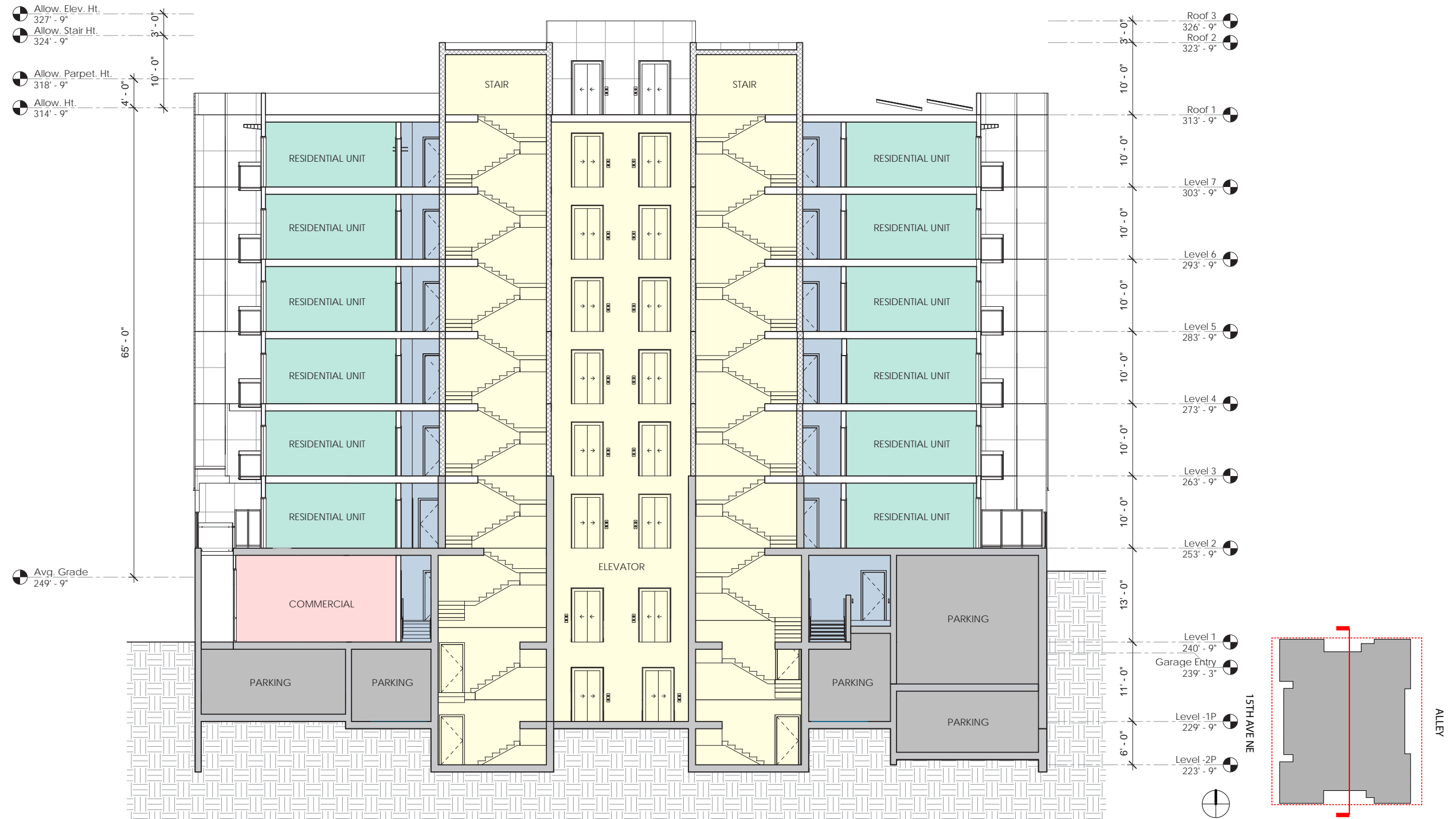


North Elevation Along Adjacent Property

SECTION

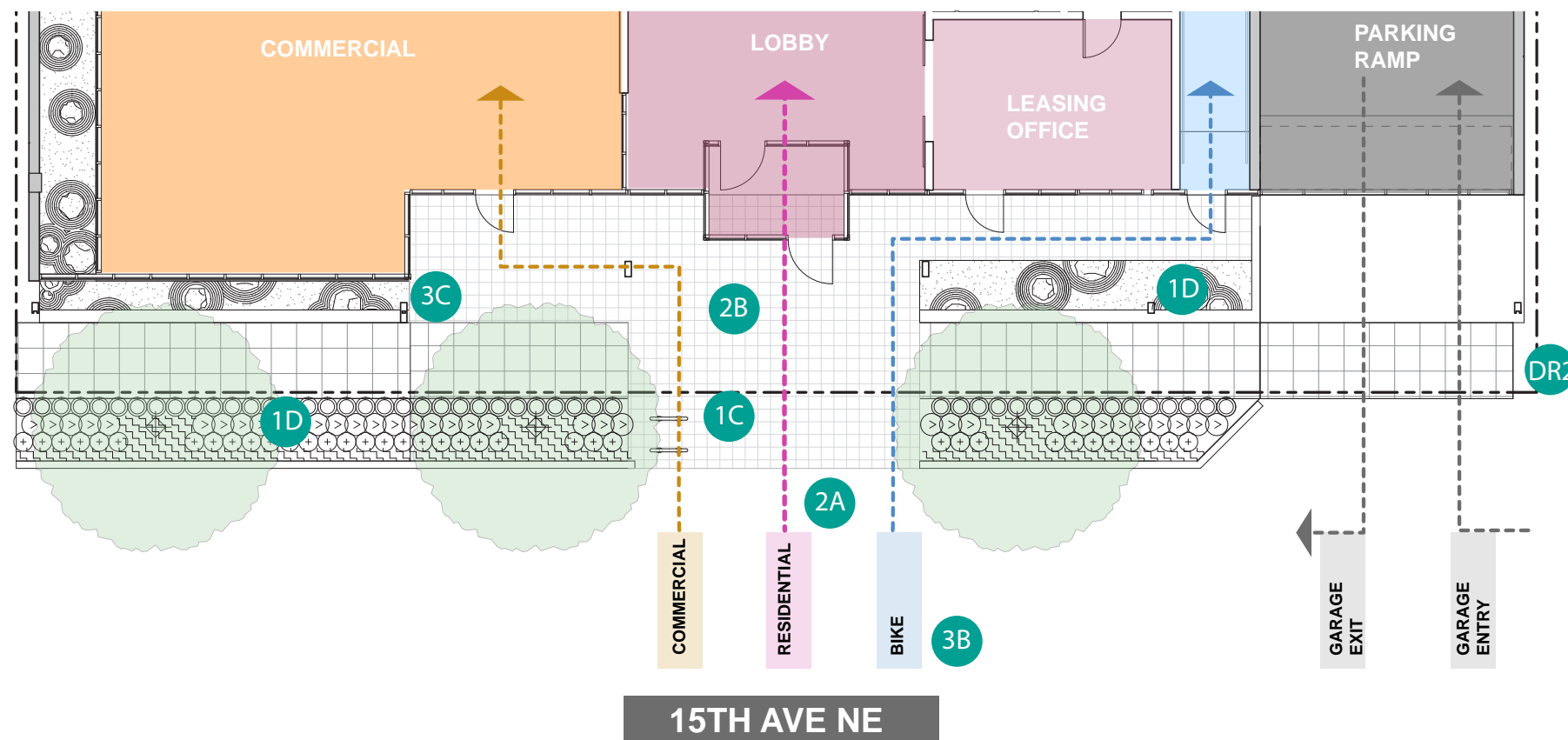


SECTION



SECTION 3-3

CONCEPT DEVELOPMENT: RELATIONSHIP TO STREET



ADDRESSING EDG | DESIGN GUIDANCE

CS2 SENSE OF PLACE

TOPOGRAPHY

1C. TOPOGRAPHY: The commercial and residential entries are located midway down the slope on 15th ave NE. With the setback, this allows the entry to flow into the sidewalk without steps.

1D. PLANTS AND HABITAT: The continuation of the streets-cape enhances the pedestrian experience along the bloc front.

Urban Pattern and Form

2B. OPEN SPACES: Ground floor provide 10.5' open space in front of residential entry, makes a strong connection to pedestrians.

STREETS-CAPE ON 15TH AVE NE

PL-2A. ACCESSIBILITY. Building entrances along 15th will be fully accessible.

PL-3A. ENTRIES: Highly transparent and porous entries to allow pedestrians interact visually with the building.

PL-3B. BIKE CONNECTIONS: Bike parking located on level 1 with direct access from 15th.

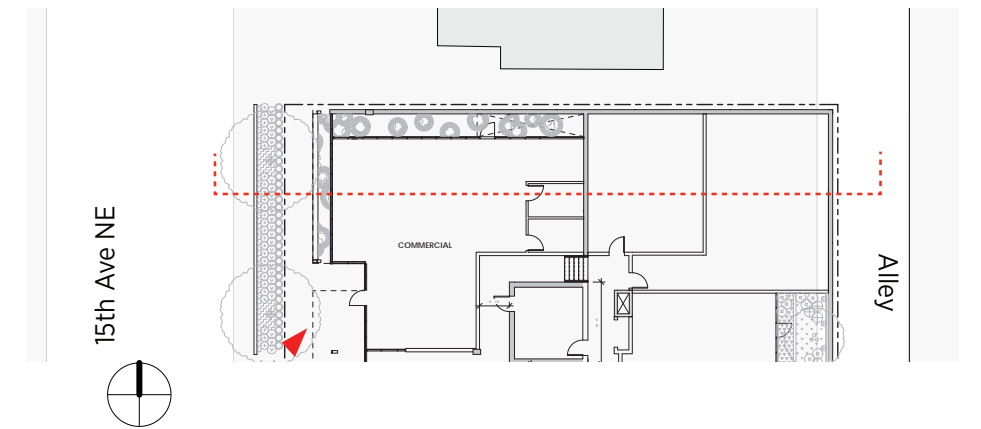
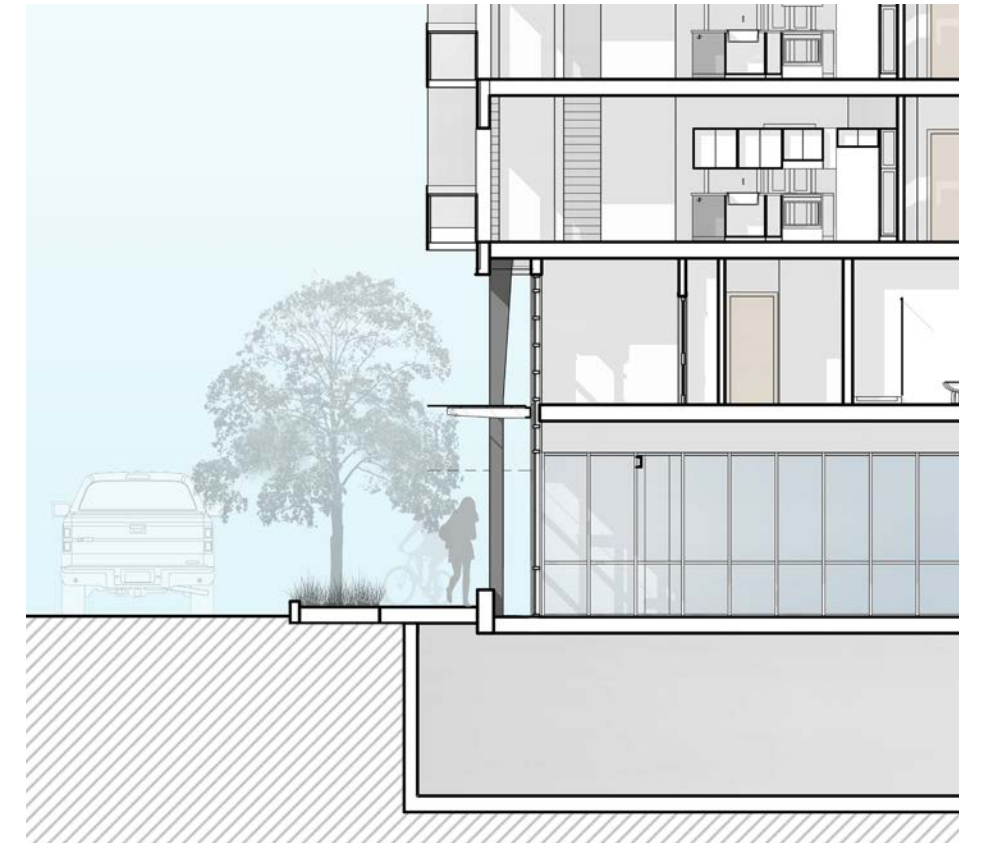
PL-3C. RETAIL EDGES: Create an active node on 15th and reactivating the sidewalks.

DR. #2A&2B. DEPARTURE REQUEST #2, SEE P.52-53

COMMERCIAL ENTRY



SECTION THROUGH COMMERCIAL



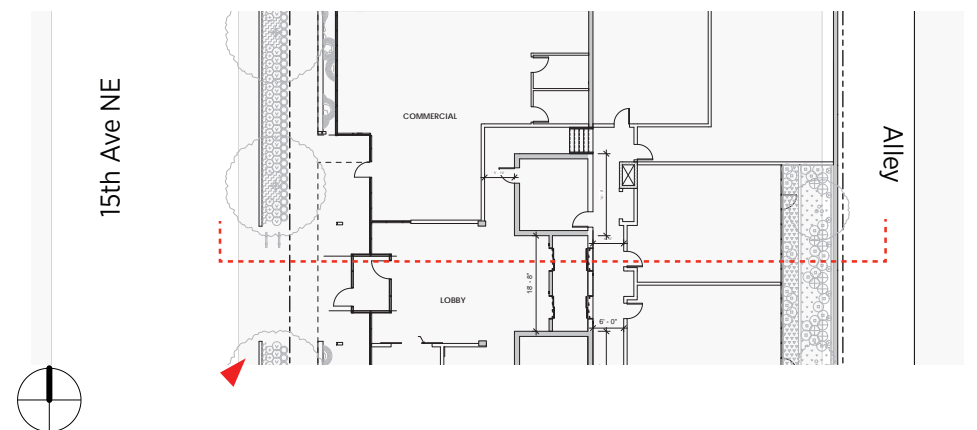
1C. Topography 1D. Plants and Habitat 2B. Open Spaces 2A. Accessibility. 3A. Entries
 3B. Bike Connections 3C. Retail Edges DR. #2. DEPARTURE REQUEST #2

RESIDENTIAL MAIN ENTRY



1C. Topography 1D. Plants and Habitat 2B. Open Spaces 2A. Accessibility. 3A. Entries
 3B. Bike Connections 3C. Retail Edges DR. #2. DEPARTURE REQUEST #2

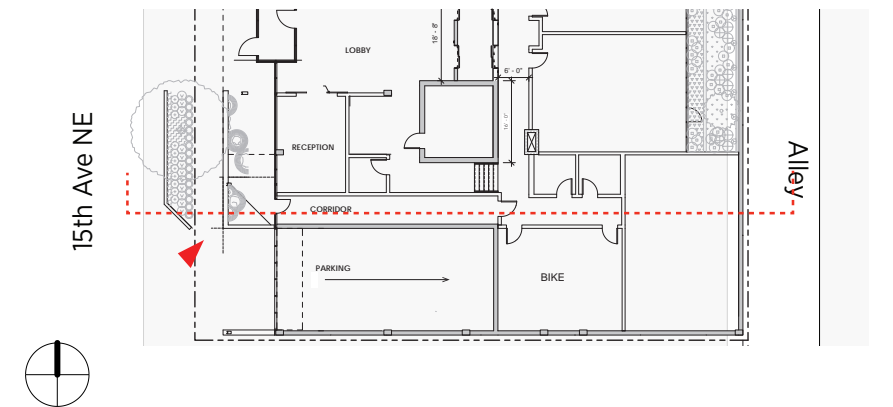
SECTION THROUGH LOBBY



BIKE ENTRY



SECTION THROUGH CORRIDOR TO BIKE ROOM



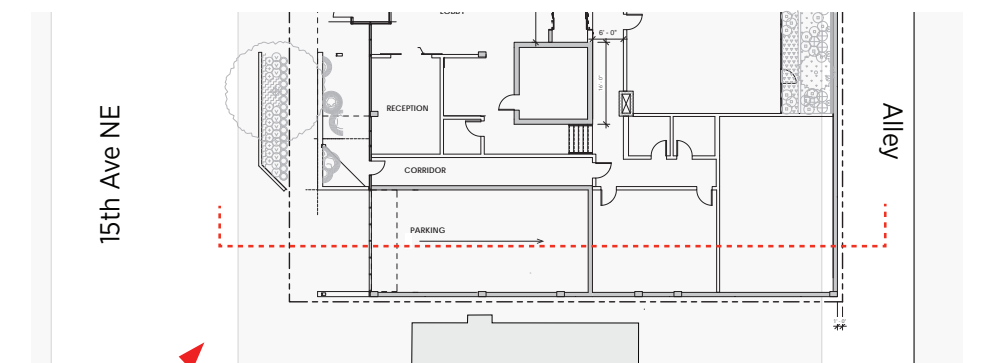
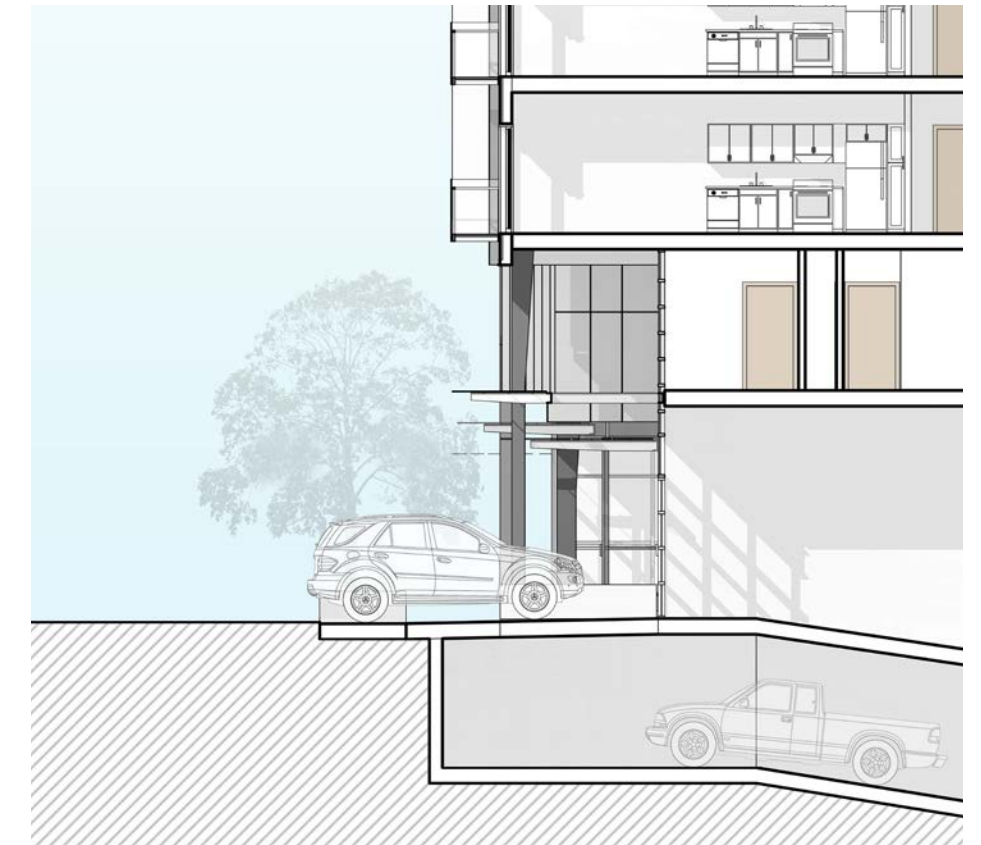
1C. Topography 1D. Plants and Habitat 2B. Open Spaces 2A. Accessibility. 3A. Entries
 3B. Bike Connections 3C. Retail Edges DR. #2. DEPARTURE REQUEST #2

PARKING GARAGE ENTRY



1C. Topography 1D. Plants and Habitat 2B. Open Spaces 2A. Accessibility. 3A. Entries
 3B. Bike Connections 3C. Retail Edges DR.#2. DEPARTURE REQUEST #2

SECTION THROUGH GARAGE



CONCEPT DEVELOPMENT: RELATIONSHIP TO ALLEY



ADDRESSING EDG | DESIGN GUIDANCE

CS2 SENSE OF PLACE

TOPOGRAPHY

1C. TOPOGRAPHY: the garbage room on L2 is directly connected to the Alley, while 3 units on L2 are raised on since located on lower part of the topography.

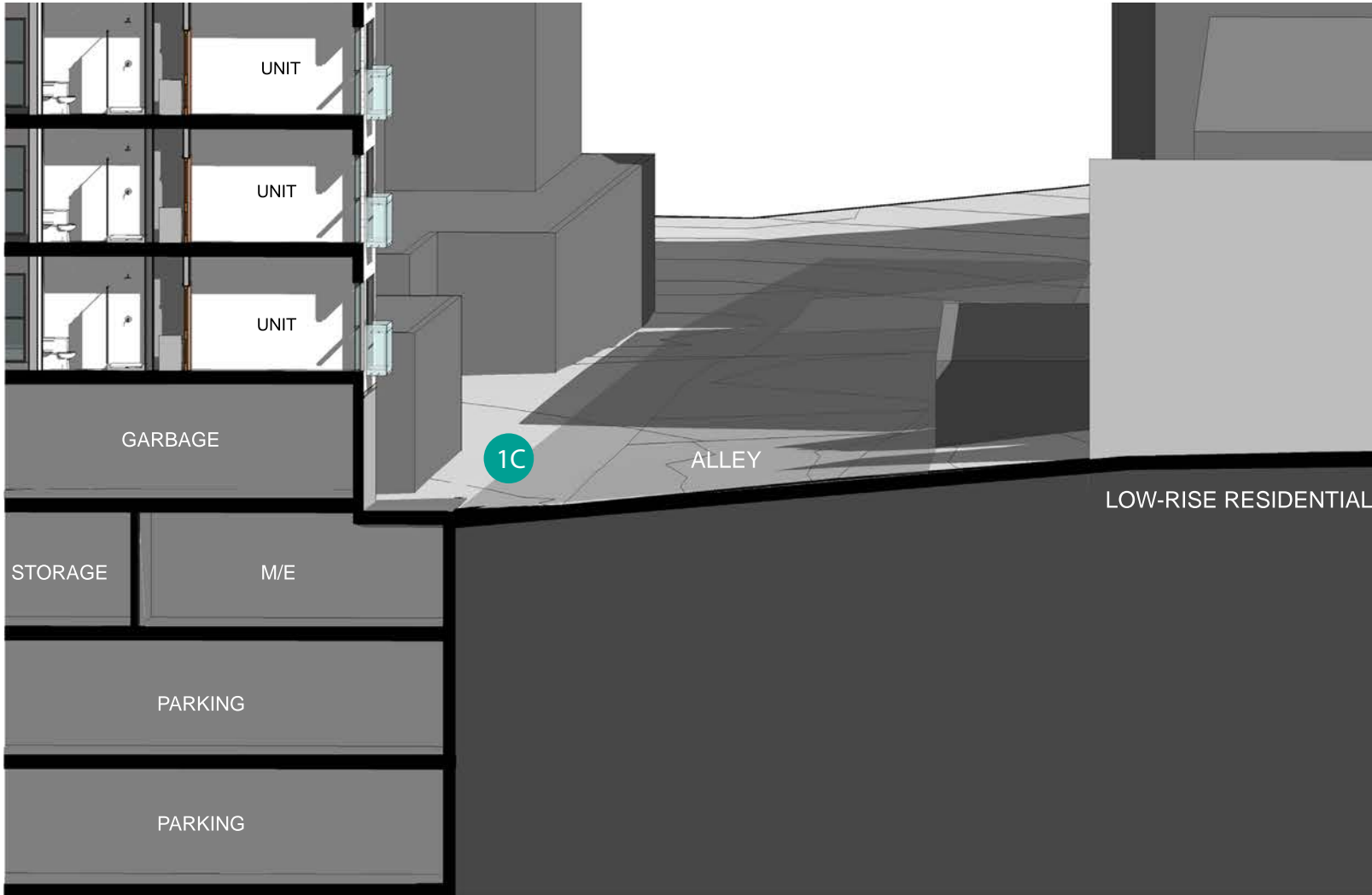
1D. PLANTS AND HABITAT: Landscape treatment is provided for the light-well located on L1.

STREETS CAPE ON ALLEY

PL-2B SAFETY AND SECURITY: The exterior patios of the units on L2 are elevated above the alley by 2'6"-3'0" with another 5' metal mesh green screen above that.

1C. Topography 1D. Plants and Habitat 2B. Safety and Security

ALLEY SECTION PERSPECTIVE

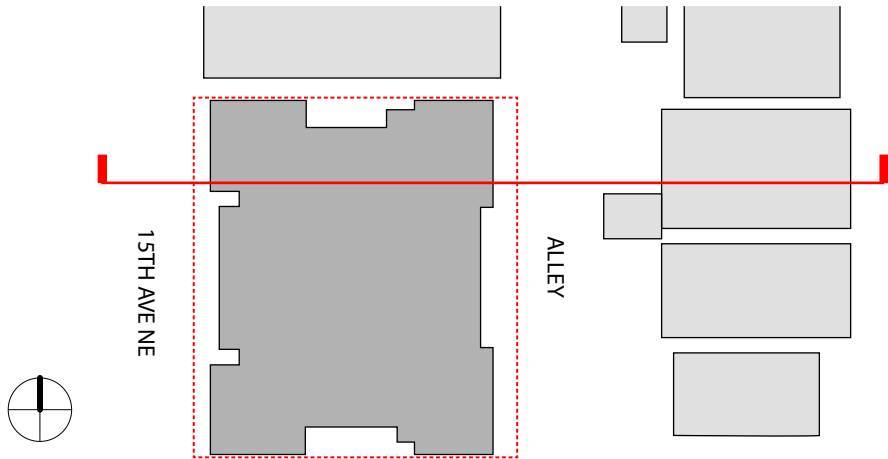


SECTION PERSPECTIVE FROM GARBAGE ROOM ALONG THE ALLEY

1C. Topography 1D. Plants and Habitat 2B. Safety and Security



ACROSS ALLEY RESIDENTIAL HOUSING PHOTO



ALLEY SECTION PERSPECTIVE

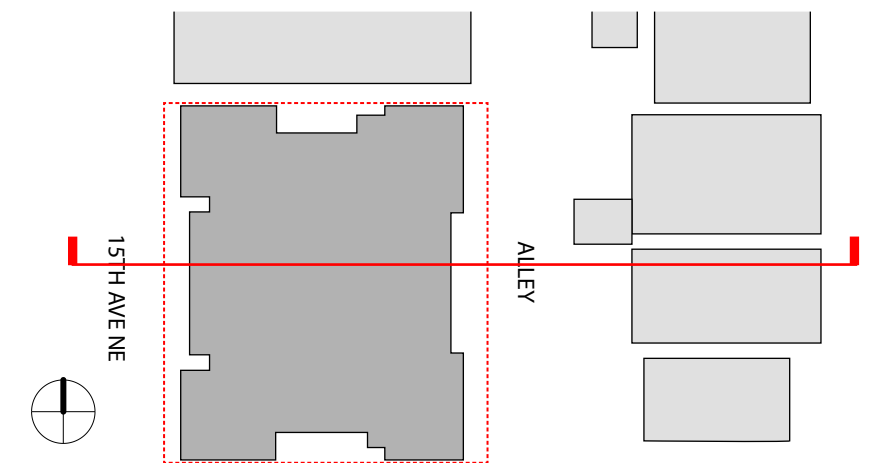


SECTION PERSPECTIVE FROM LOUNGE ALONG THE ALLEY

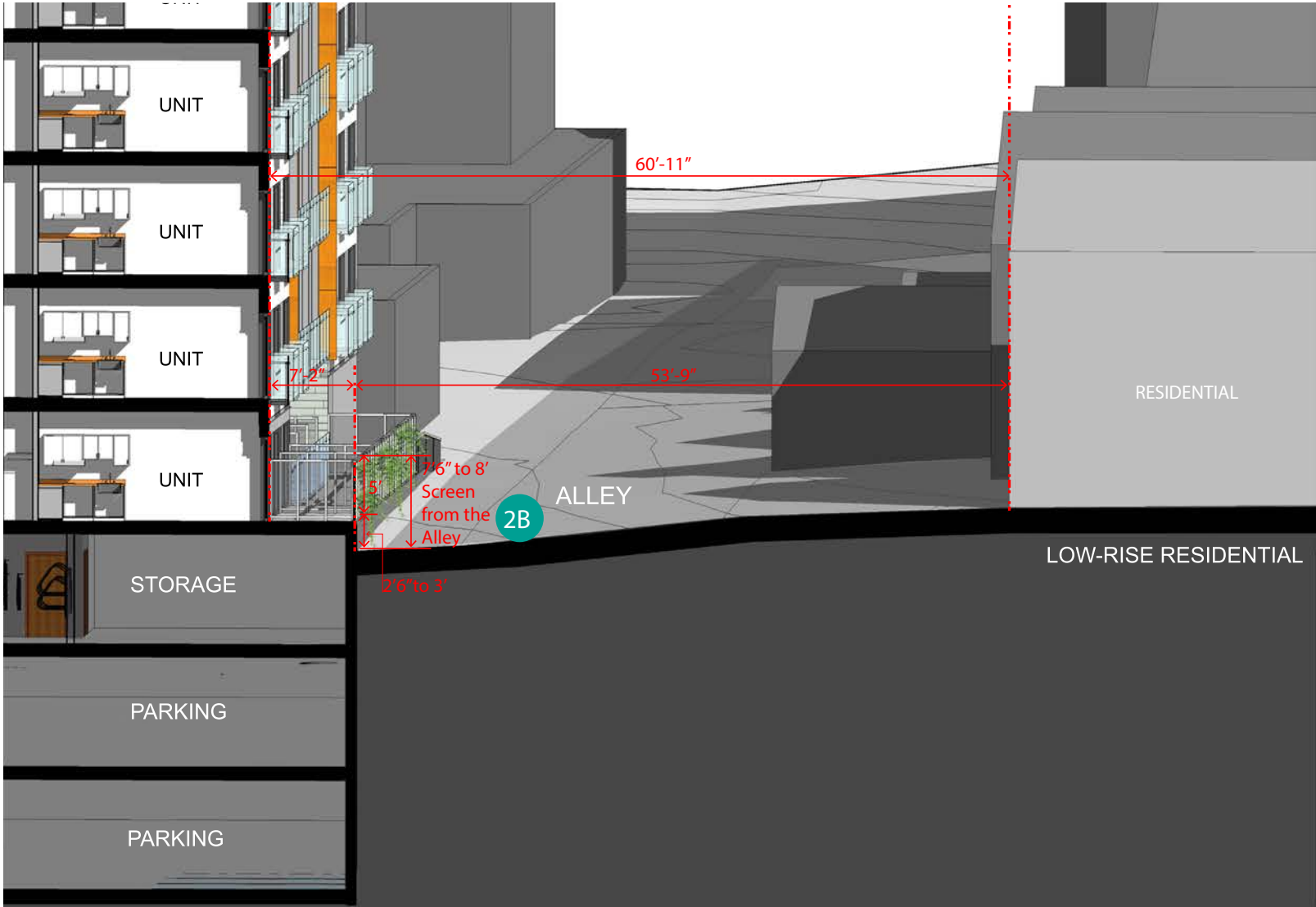
1C. Topography 1D. Plants and Habitat 2B. Safety and Security



ACROSS ALLEY RESIDENTIAL HOUSING PHOTO



ALLEY SECTION PERSPECTIVE

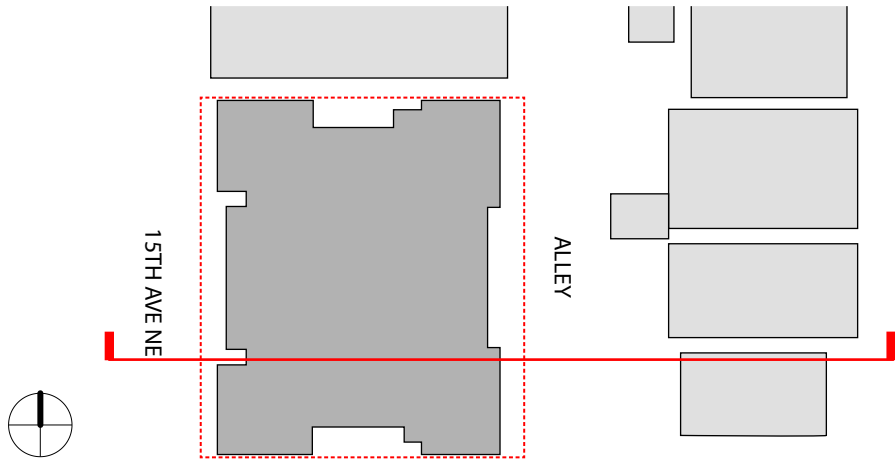


SECTION PERSPECTIVE FROM UNIT ALONG THE ALLEY

1C. Topography 1D. Plants and Habitat 2B. Safety and Security



ACROSS ALLEY RESIDENTIAL HOUSING PHOTO



MATERIALS | PALETTE

EXTERIOR MATERIAL

Exterior finish materials chosen for this project are Prodema wood panel, OKO Skin horizontal sidings, flat metal sidings, reveal panels, metal and glass railings and canopies, exposed concrete and painted metal in keeping with the modern design of this project.



Oko Skin Concrete Siding



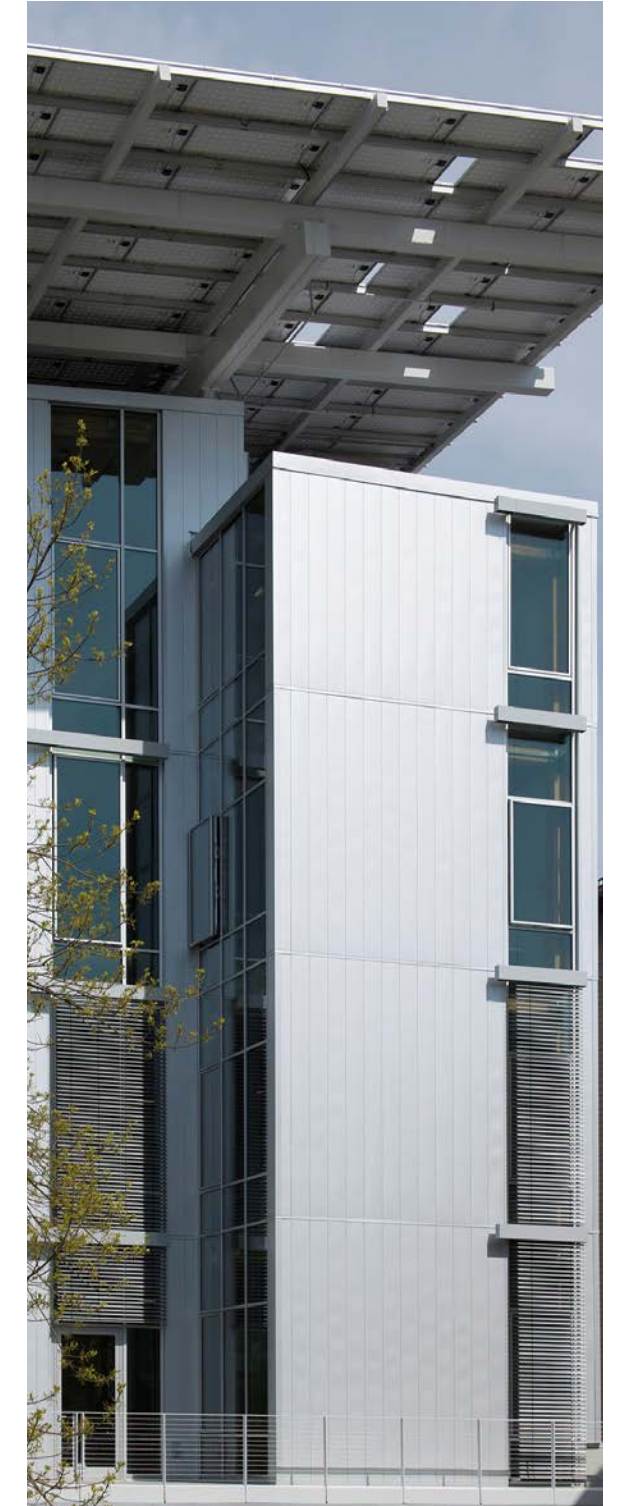
Reveal Siding Panel



Prodema Wood Panel



Storefront Curtain Wall



Metal Siding for Central Bay

MATERIALS | PALETTE



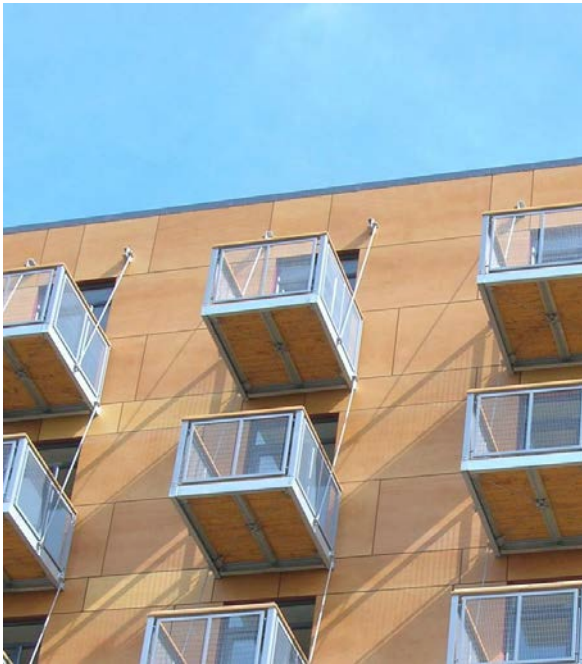
Metal Glass Canopy



Metal Glass Garage Door



Horizontal Sun Shades



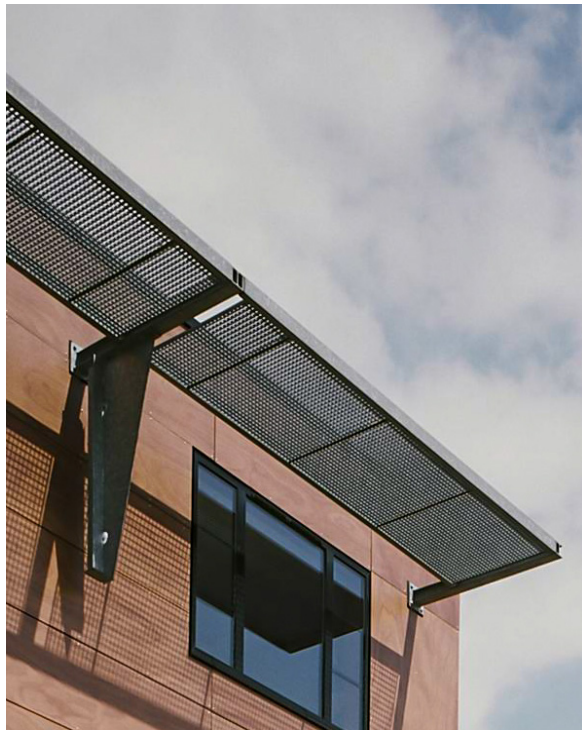
Metal/Glass Balconies



Exterior Glass Guardrail at Central Bay



Green Wall (Mesh) along Alley



Horizontal and Vertical Sun Shades



Metal/Glass Balconies

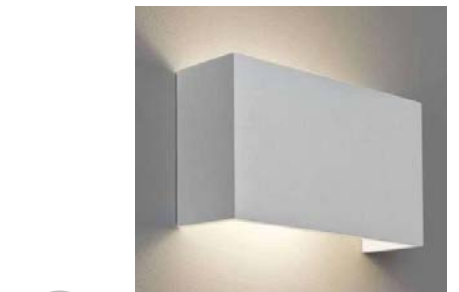
LANDSCAPE LIGHTING



 LANDSCAPE LIGHTING PLAN



1 FESTIVAL LIGHTING (LED)



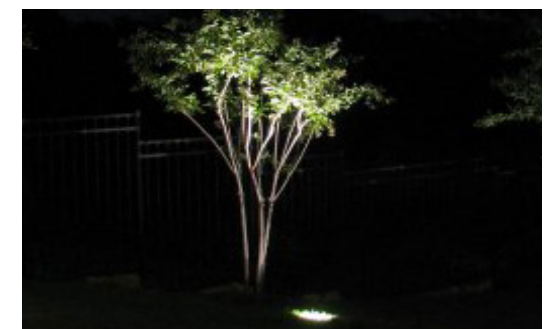
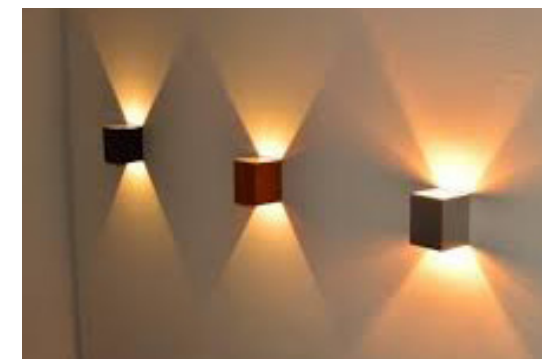
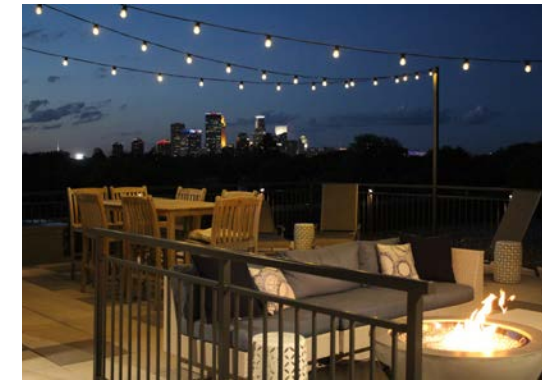
2 WALL LIGHTING (LED)



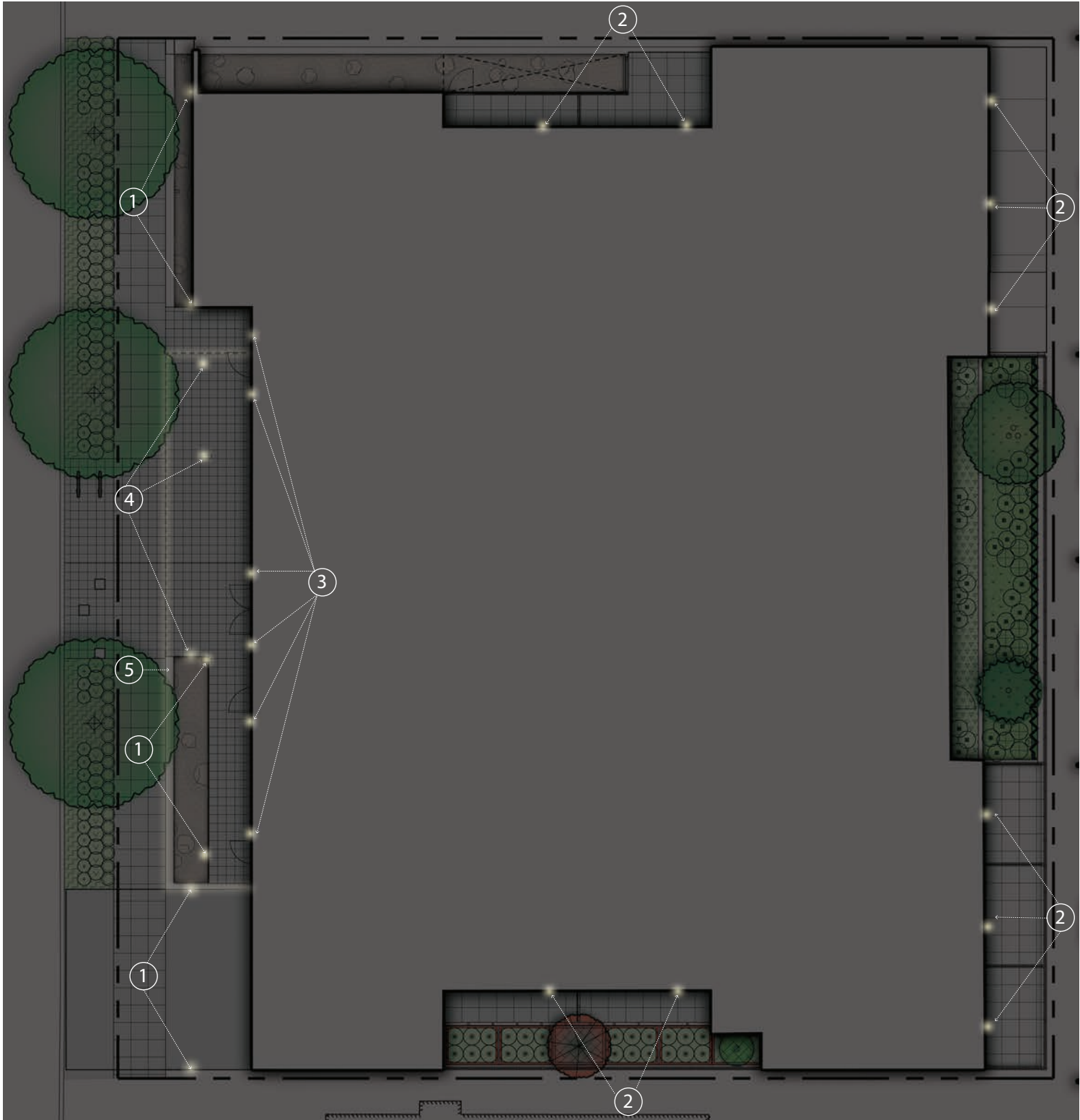
3 PATHWAY & PLANTER LIGHTING (LED)








4 LANDSCAPE LIGHTING (LED)



EXTERIOR LIGHTING



 EXTERIOR LIGHTING

- 
① RECESSED PATHWAY LIGHTING (LED)
- 
② LED WALL PACKS
- 
③ WALL WASHER (RESIDENTIAL & COMMERCIAL ENTRIES)
- 
④ LED SPOTLIGHTS (IN CANOPY)
- 
⑤ LED DOWN LIGHTING (UNDER BUILDING OVERHANG)

BUILDING SIGNAGE



1 RESIDENTIAL ENTRY SIGNAGE :
CANOPY MOUNTED PROJECT NAME AND/OR ADDRESS
SIGNAGE



2 BIKE ENTRY SIGNAGE :
WALL MOUNTED BLADE SIGNAGE



3 PARKING SIGNAGE:
WALL MOUNTED PARKING SIGNAGE ALONG 15TH AVE NE



PROPOSED DEPARTURE 1

SETBACKS FOR STRUCTURES

REQUIREMENT:

SMC 23.47A.014

Setback requirements for lots abutting or across the alley from residential zones. One-half of the width of an abutting alley may counted as part of the required setback.

PROVIDED AT THE EAST PROPERTY LINE:

In order to provide greater articulation of the facade allow two areas of encroachment into the triangle setback along the alley.

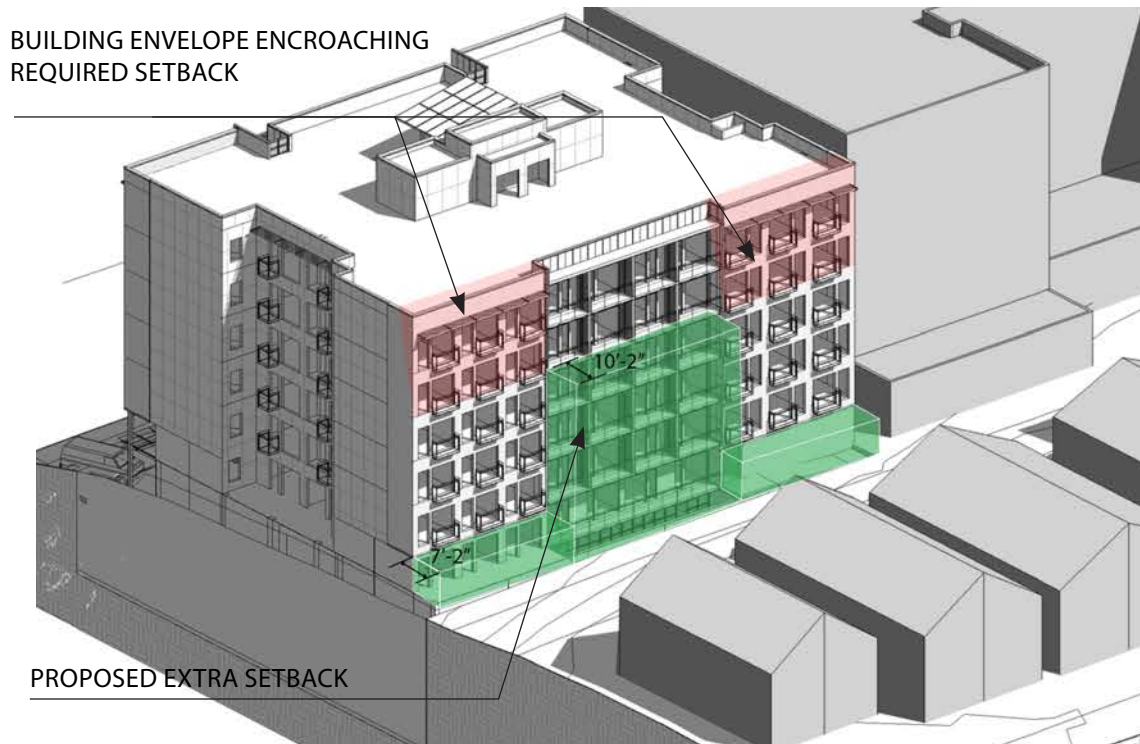
JUSTIFICATION:

The departure better meets guidelines CS2-B2 connection to the street, proposing a courtyard space while providing light to the level 1 common space, meanwhile the deeper setback breaks down the building structure into a central bay and two small bay areas, which better meets the guidance CS2-D.4.

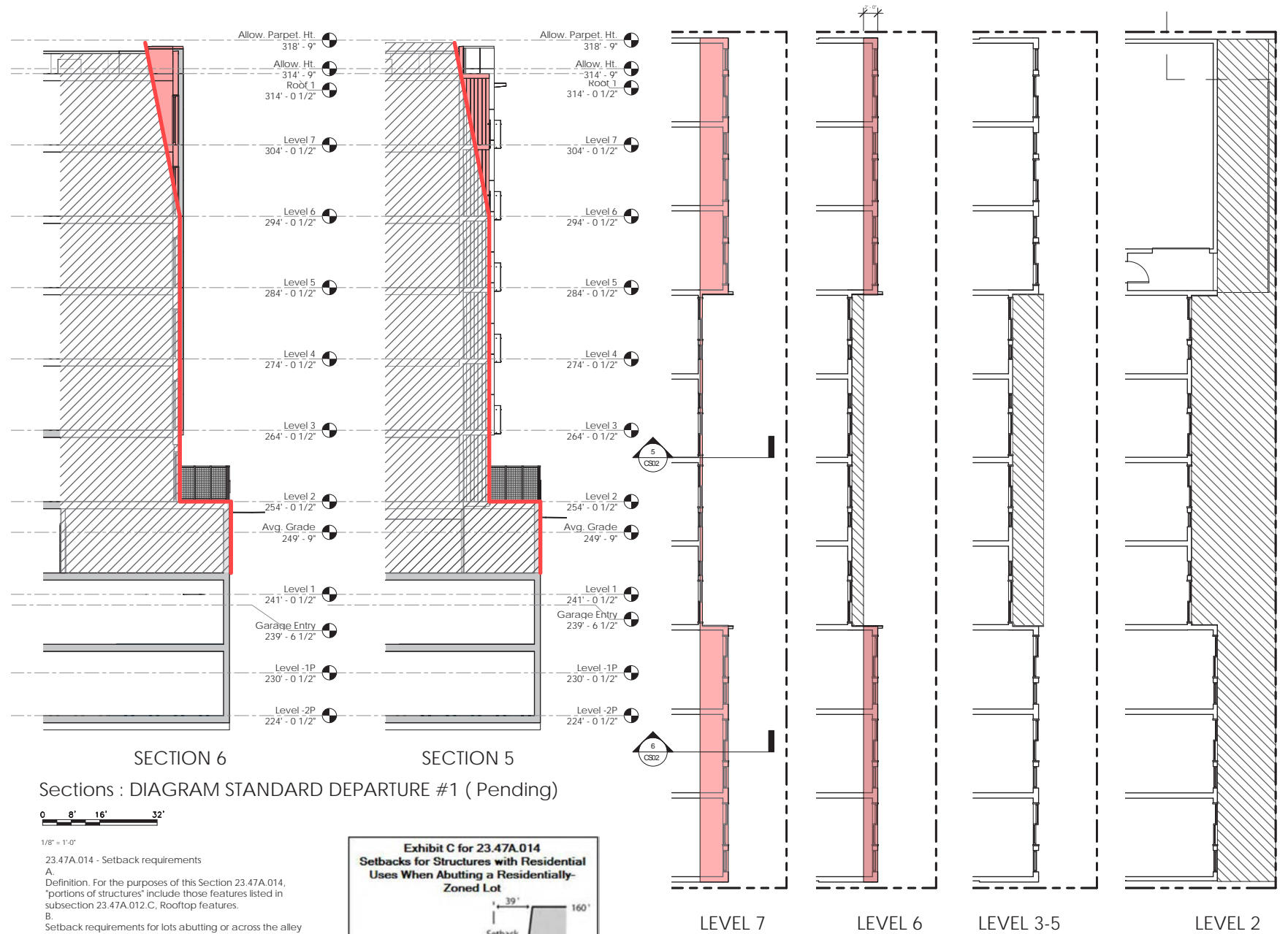
SUPPORTED DESIGN GUIDANCE:

CS2-B2. Connection to the street
CS2-D4. Height, Bulk and Scale-Massing Choices

BUILDING ENVELOPE ENCROACHING
REQUIRED SETBACK



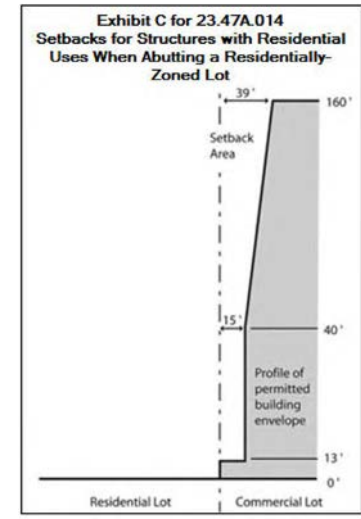
PROPOSED EXTRA SETBACK



Sections : DIAGRAM STANDARD DEPARTURE #1 (Pending)

0 8' 16' 32'

1/8" = 1'-0"
23.47A.014 - Setback requirements
A. Definition. For the purposes of this Section 23.47A.014, "portions of structures" include those features listed in subsection 23.47A.012.C, Rooftop features.
B. Setback requirements for lots abutting or across the alley from residential zones
1. A setback is required where a lot abuts the intersection of a side lot line and front lot line of a lot in a residential zone. The required setback forms a triangular area. Two sides of the triangle extend along the street lot line and side lot line 15 feet from the intersection of the residentially zoned lot's front lot line and the side lot line abutting the residentially zoned lot. The third side connects these two sides with a diagonal line across the commercially-zoned lot (Exhibit C for 23.47A.014 Setbacks for Structures with Residential Uses When Abutting a Residentially-Zoned Lot).
Porposal:
Allow the violated extension to the to required set back per S.M.C 23.47A.014
Justification:
Hatched areas shows the areas which extra set back have been provided and highlighted areas are the areas which set back has not been regulated according to code to reponse to University District Design Guidelines CS2 and DC2, DC4.



Plans : DIAGRAM STANDARD DEPARTURE #1 (Pending)

1/8" = 1'-0"
0 8' 16' 32'

EXTRA SETBACK PROVIDED
= 1406.42
EXTENSION IN REQUIRED
SETBACK = 445.92

STANDARD DEPARTURE #1 (Pending)

MUP Rev 1 Notes:
1. Revised Data
2. Revised Diagrams

PROPOSED DEPARTURE 2A

GARAGE ENTRY LOCATION

REQUIREMENT:

SMC 23.47A.032.A.1 - SIGHT TRIANGLE

1. NC zones. The following rules apply in NC zones, except as provided under subsections 23.47A.032.A.2 and 23.47A.032.D:
 - a. Access to parking shall be from the alley if the lot abuts an alley improved to the standards of subsection 23.53.030.C, or if the Director determines that alley access is feasible and desirable to mitigate parking access impacts. If alley access is infeasible, the Director may allow street access.

PROPOSED DRIVEWAY:

For the garage entry to be located off of 15th Ave NE instead of off the rear alley.

JUSTIFICATION:

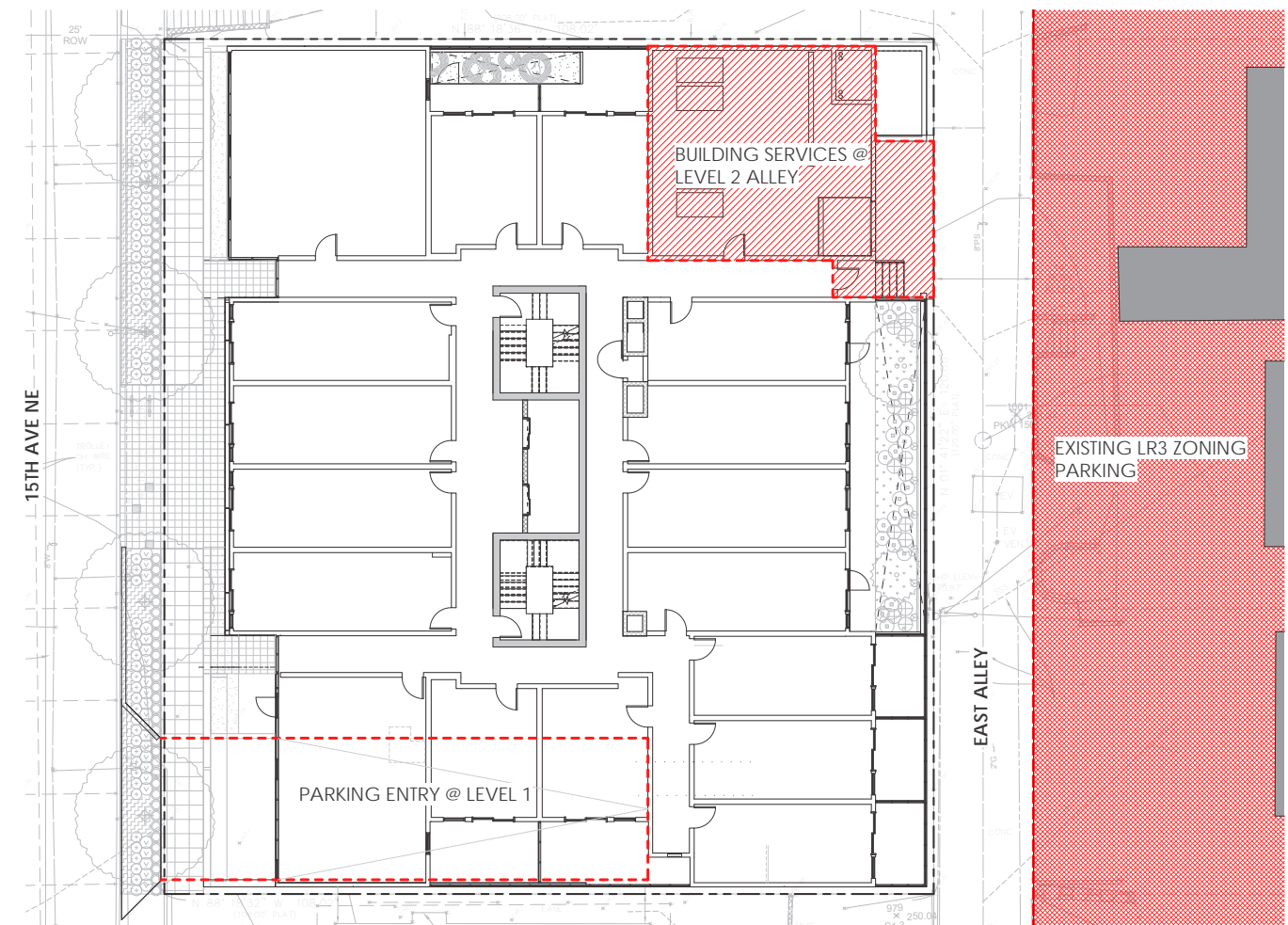
The topography of the site makes it inefficient to ramp below grade to achieve the parking. The zoning across the alley is LR3. Existing parking for those sites access the alley by backing their vehicles into the alley. The addition of additional traffic to the alley would negatively effect the access to the parking for the neighboring sites. Utilizing a parking entry off 15th Ave NE will minimize the disruption to the LR3 properties that are already utilizing the alley as access to their properties. Locating the entry at 15th, as opposed to the alley, identifies a clear connection to the project & adds to the streetscape. The deep setback at the ground level provides extensive visibility to the vehicular traffic exiting the garage. The garage will also be designed to be integrating into the visual language present at the storefront which will reduce the potential visual disruption that a garage door could cause to the storefront. The alley is also being utilized for the building services as well as the LR3 zone to the east. Having the parking entrance off the 15th Ave NE reduces the impact on these uses.

SUPPORTED DESIGN GUIDANCE:

- | | |
|---------|--------------------------------------|
| CS1. | NATURAL SYSTEMS & SITE FEATURE |
| CS2-D3. | ZONE TRANSITIONS |
| CS2-D5. | RESPECT FOR ADJACENT SITES |
| PL3. | STREET-LEVEL INTERACTION |
| DC1-B. | VEHICULAR ACCESS AND CIRCULATION |
| DC1-C. | PARKING & SERVICE USES |
| DC2-B. | ARCHITECTURAL AND FACADE COMPOSITION |



SOUTHWEST DRIVEWAY ENTRY PERSPECTIVE



PROPOSED DEPARTURE 2B

SIGHT TRIANGLE

REQUIREMENT:

SMC 23.54.030 -SIGHT TRIANGLE

For exit-only driveways and easements, and two driveways and easements less than 22 feet wide, a sight triangle on both sides of the driveway or easement shall be provided and shall be kept clear of any obstruction for distance of 10 feet from the intersection of the driveway or easement with a driveway, easement, sidewalk, or curb intersection if there is no sidewalk.

PROPOSED DRIVEWAY:

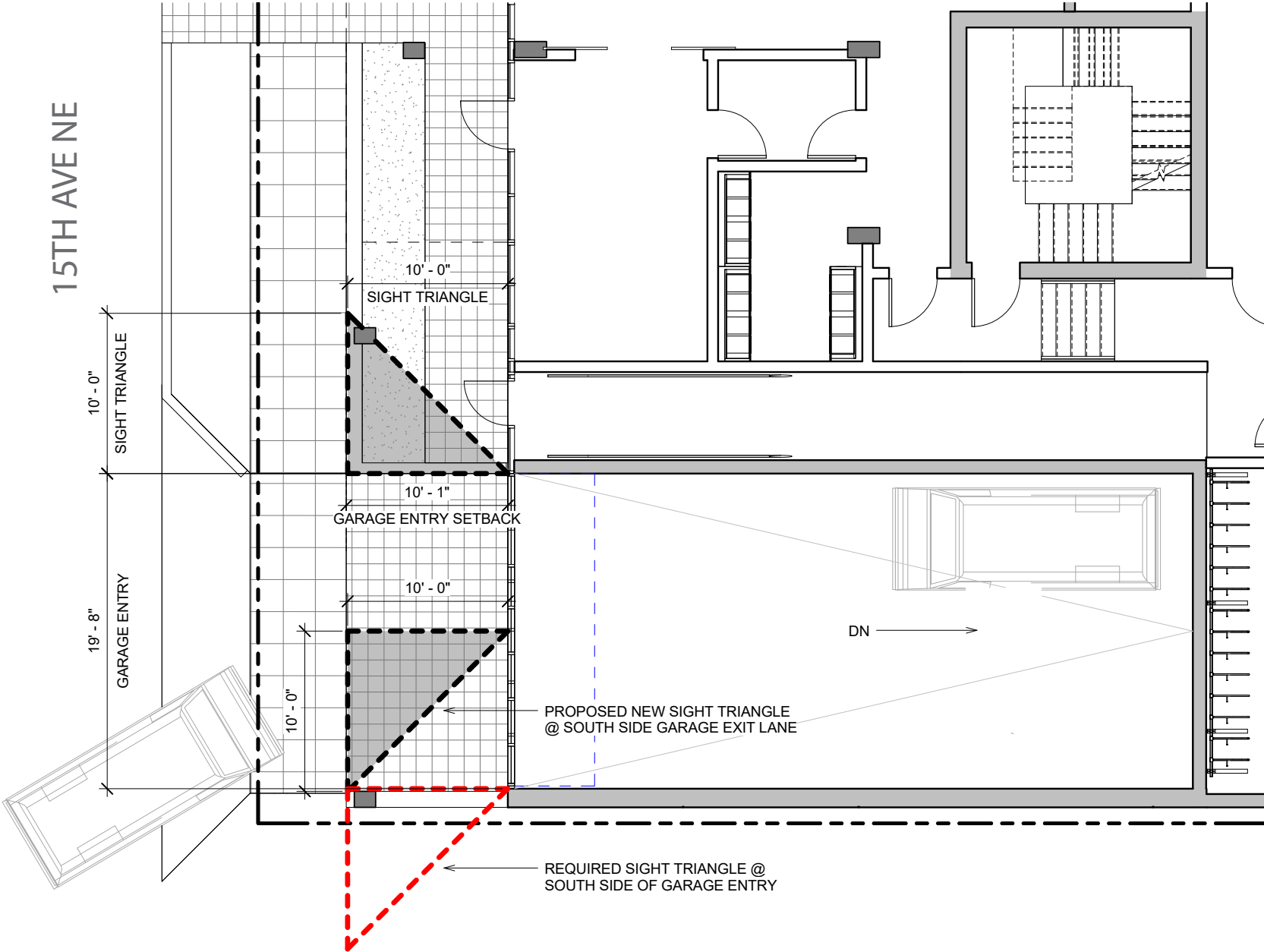
For this two-way 20 feet wide driveway, a one side sight triangle on the exit side of the driveway will be provided.

JUSTIFICATION:

The intention is to keep the size of the garage door at minimum and not a dominant element in the facade. In addition, based on its location the cars coming out need the sight triangle on both sides. However, the cars going in have a sufficient view range and will not need one. The commercial space and residential lobby are to the north of the driveway. The project to the north has no commercial on 15th ave NE (except for the church office at the corner of NE 50th St and 15th Ave NE). Our commercial space and residential lobby will create a porous edge (PL3-C3) and activate the area to the north of the driveway. The garage door is designed to seamlessly blend in with the storefront (DC2). The project location encourages pedestrians and bicyclists, which will reduce the trips to and from the garage.

SUPPORTED DESIGN GUIDANCE:

- PL3-C3. STREET LEVEL INTERACTION : SUPPORT NATURAL AREAS.
- DC2. ARCHITECTURAL CONCEPT



CS-1 NATURAL SYSTEMS AND SITE FEATURES

CS-1: NATURAL SYSTEMS AND SITE FEATURES:

Use natural systems/features of the site and its surroundings as a starting point for project design

C. Topography CS1-C-2. Elevation Changes:

Response

The topography has been integrated into design by placement of access to Services at the alley. The commercial and residential entries are located midway down the slope on 15th Ave NE. With the setback, this allows the entry to flow into the sidewalk without steps. See P.35

D-1. On-Site Features:

Response

There is a 'dry garden at the NW corner of the building. It is visible from the sidewalk and is a feature of the adjacent commercial space. At the alley side, there is a garden below the alley grade that is adjacent to the Lounge and Exercise rooms. These gardens provide amenity for the residents and tenants and function as light wells, bringing in natural light into those interior spaces. See P.35-39

D-2. Off-Site Features:

Response

The landscaping in the right of way repeats the planting and pattern of the project to the north. The continuation of this streetscape enhances the pedestrian experience along the block front. See P.35-39

CS-2 URBAN PATTERN AND FORM

CS-2: URBAN PATTERN AND FORM:

CS2-A Location in the City and Neighborhood CS2-A-2. Architectural Presence:

Response

The site has a mid-block relationship to the rest of the street. . Immediately adjacent to the north is a mixed-use building currently under construction. To the South, there is a single family house and an open parking lot owned by the church across the street. The commercial space and the residential entry will activate the center of the block and create a strong street edge. See P.17

CS2-B Adjacent Sites, Streets, and Open Spaces

Response

The additional ground floor set will provide a 10.5' open space in front of the commercial space and residential entry. This widened node along the sidewalk makes a strong connection to pedestrians. See P.35-39

CS2-C Relationship to the Block

CS2-C-2. Mid-Block Sites:

Response

The proposed structure will be similar in bulk to the neighboring building to the north and will respond to the datum lines and rhythm already created by that building. The building, also, steps back from the lower scaled zone across the alley, to the east, to provide a transition in bulk and scale. See P.17

CS2-D Height, Bulk, and Scale

CS2-D-1. Existing Development and Zoning:

Response

A Contract Rezone application has been submitted to change the North lot zoning from LR3 to NC-65. This would make all of the lots along the 15th Ave NE block front the same zone. See P.17-18

PL-1 CONNECTIVITY

PL2 Walk-ability:

PL2-A Accessibility 1115

PL2-A-1. Access for All:

Response

The building will have an entrance that will be clearly identified with landscaping, canopies, and lighting. The street level facade will be glazed, with multiple entrances that will be clearly distinguished between commercial and residential uses. There will be no residential units on the ground level. See P.35-39

PL2-A-2. Access Challenges:

Response

Building entrances along 15th Ave NE will be fully accessible. See P.35

PL2-B Safety and Security

PL2-B-1. Eyes on the Street:

PL2-B-2. Lighting for Safety:

PL2-B-3. Street-Level Transparency:

Response

Street level transparency will be 100% transparent for the residential lobby and commercial spaces. The proposed garage door and bicycle access doors will be semi-transparent. Planting and appropriate lighting along the building will be provided to create a pleasant pedestrian environment, enhance pedestrian safety and distinguish the parking garage entrance. See CS04 and Landscape design concepts.

PL3 Street-Level Interaction:

PL3-A Entries

PL3-A-1. Design Objectives:

PL3-A-2. Common Entries:

PL3-A-4. Ensemble of Elements:

Response

The commercial and residential entries will be transparent and porous to allow pedestrians to interact visually with the building. Overhead projections, canopies, and streetscape elements will emphasize place-making at the center of the building's ground floor. An objective of this project is to create a new urban edge and introduce and activate a new node. See P.35-39

L3-B Residential Edges

PL3-B-1. Security and Privacy:

PL3-B-2. Ground-level Residential:

Response

There is no ground level residential on 15th Ave NE. At the alley, residential units are set back or raised up. These private terraces will also have 5'-0" high fences for an additional safety buffer. See P.43

PL3-C Retail Edges

PL3-C-1. Porous Edge:

PL3-C-2. Visibility:

PL3-C-3. Ancillary Activities:

Response

Creating an active node on this street and reactivating the sidewalks will increase pedestrian traffic on this block. The building's commercial space and residential lobby provide opportunity for pedestrian friendly uses. See P.35-39

PL4 Active Transportation:

PL4-B Planning Ahead for Bicyclists

PL4-B-1. Early Planning:

PL4-B-2. Bike Facilities:

PL4-B-3. Bike Connections:

Response

The main entrance to the building will be located off 15 Ave. NE. This street serves as an important arterial with transit routes. The commercial, residential and parking entrances are set back from the property edge for safety and design purposes. Bicycle parking will be provided for the residents on Level 1 with direct access from 15th Ave NE. This location near the main building entry provides the safest access for bicyclists. See P.35-39

DC-2 DESIGN CONCEPT

DC2 Architectural Concept:

DC2-B Architectural and Facade Composition

DC2-B-1. Facade Composition:

DC2-B-2. Blank Walls:

Response:

The facades are organized by massing, articulation and materials into distinct layered design elements. The largest blank areas are located on the North and South facades. These are mitigated by materials and massing. See P:17

DC2-C Secondary Architectural Features

DC2-C-1. Visual Depth and Interest:

DC2-C-2. Dual Purpose Elements:

DC2-C-3. Fit With Neighboring Buildings:

Response:

Projecting solid and gridded bays, projecting and inset balconies, shading devices, canopies and other elements of the design combine to create visual depth and interest. This building will be similar in bulk and size to the neighboring building to the north and will respond to the datum lines and rhythm of that building. See P:18

DC2-D Scale and Texture

DC2-D-1. Human Scale:

DC2-D-2. Texture:

Response:

The most visible facade of the building is the one facing the street. However, the design parti will be consistent on all sides of the building. Streetscape elements, windows and balconies will provide human scale. Materials will vary in texture from smooth to rough. See P:18-19

DC2-E Form and Function

DC2-E-1. Legibility and Flexibility:

Response:

The building commercial, common, service, parking and residential functions of the building are legible and easy to perceive. Other than at the commercial space, flexibility is inherently difficult to accommodate in this type of building. See P:18

DC4 Exterior Elements and Finishes: Use appropriate and high quality elements and finishes for the building and its open spaces.

DC4-A Exterior Elements and Finishes

DC4-A-1. Exterior Finish Materials:

DC4-A-2. Climate Appropriateness:

Response:

Exterior finish materials chosen for this project are prodema, OKO Skin horizontal siding, flat metal siding, reveal panels, metal and glass railings and canopies, exposed concrete and painted metal in keeping with the modern design of this project. See P:29; P:44; P:45

DC4-B Signage

DC4-B-1. Scale and Character:

DC4-B-2. Coordination with Project Design:

Response:

Building signs along 15th will be incorporated into the canopies and on the windows. A building logo will be inset into the panel finish at the upper SW corner of the building. Signage will be understated and rely on the visible cues of use provided by the interior spaces. See P:47

DC4-C Lighting

DC4-C-1. Functions:

DC4-C-2. Avoiding Glare:

Response:

The building lighting will be carefully designed to emphasize building uses and entrances and enhance safety. See P:46

DC4-D Trees, Landscape, and Hardscape Materials

DC4-D-1. Choice of Plant Materials:

DC4-D-2. Hardscape Materials:

DC4-D-3. Long Range Planning:

DC4-D-4. Place Making:

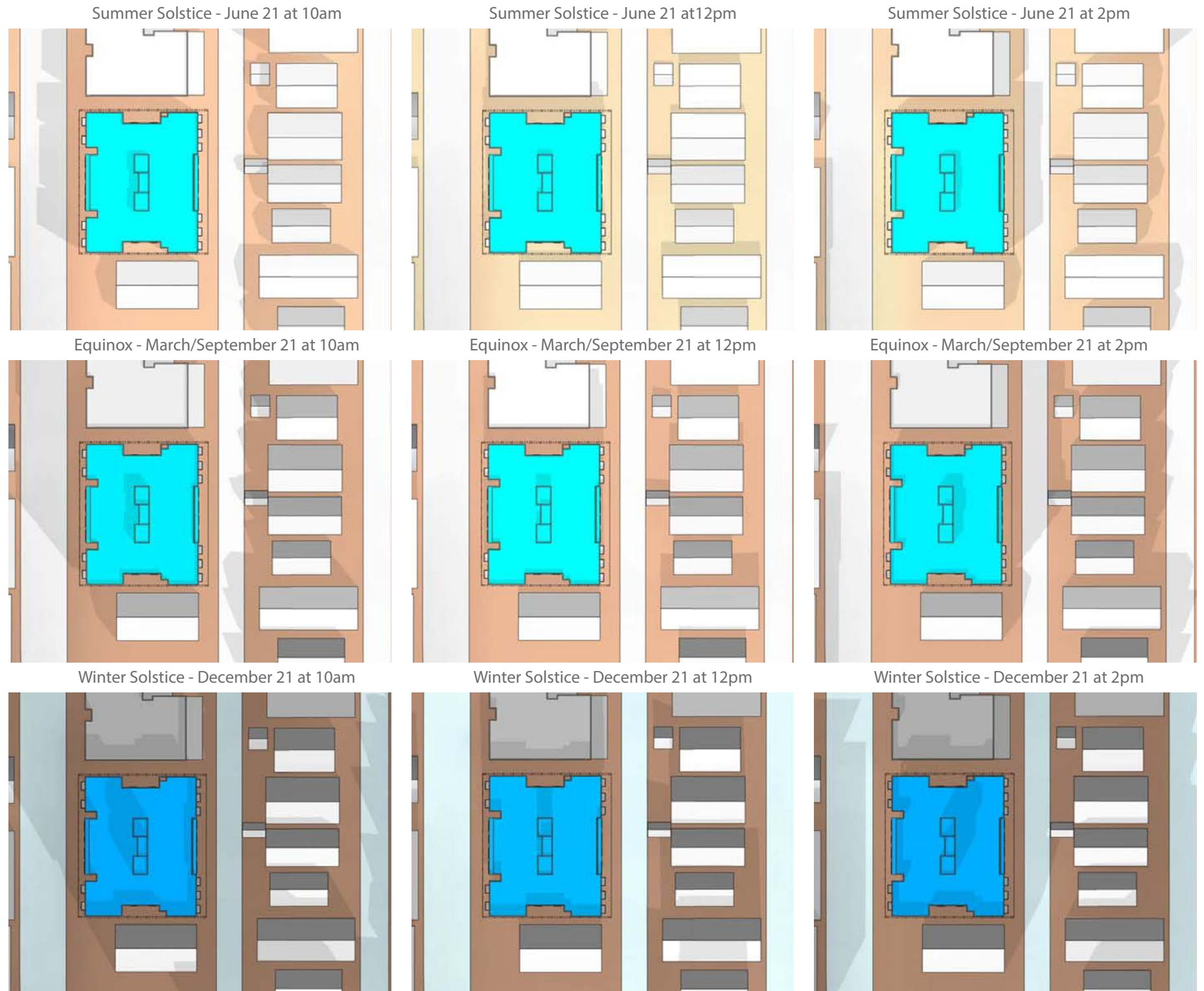
Response:

On 15th Ave NE, an extra 5 feet set back is provided to the required 5.5' to increase the opportunities for landscaping options. This creates a better sense of place for pedestrians and building residents. See P:35-39

SHADOW STUDY

Shadow Studies

The shadow studies are based on Alternative C (Preferred). The site has ample sun exposure throughout the year, there's a slight exception in the winter, primarily due to the empty adjacent lot. The shadows cast by the proposed building do not appear to impact surrounding properties significantly.



APPENDIX d/Arch Recent Projects

