

**LUXE II: MIXED-USE BUILDING**

DESIGN RECOMMENDATION MEETING • AUGUST 14, 2017

1403 NE 65TH STREET • PROJECT NO. 3025139







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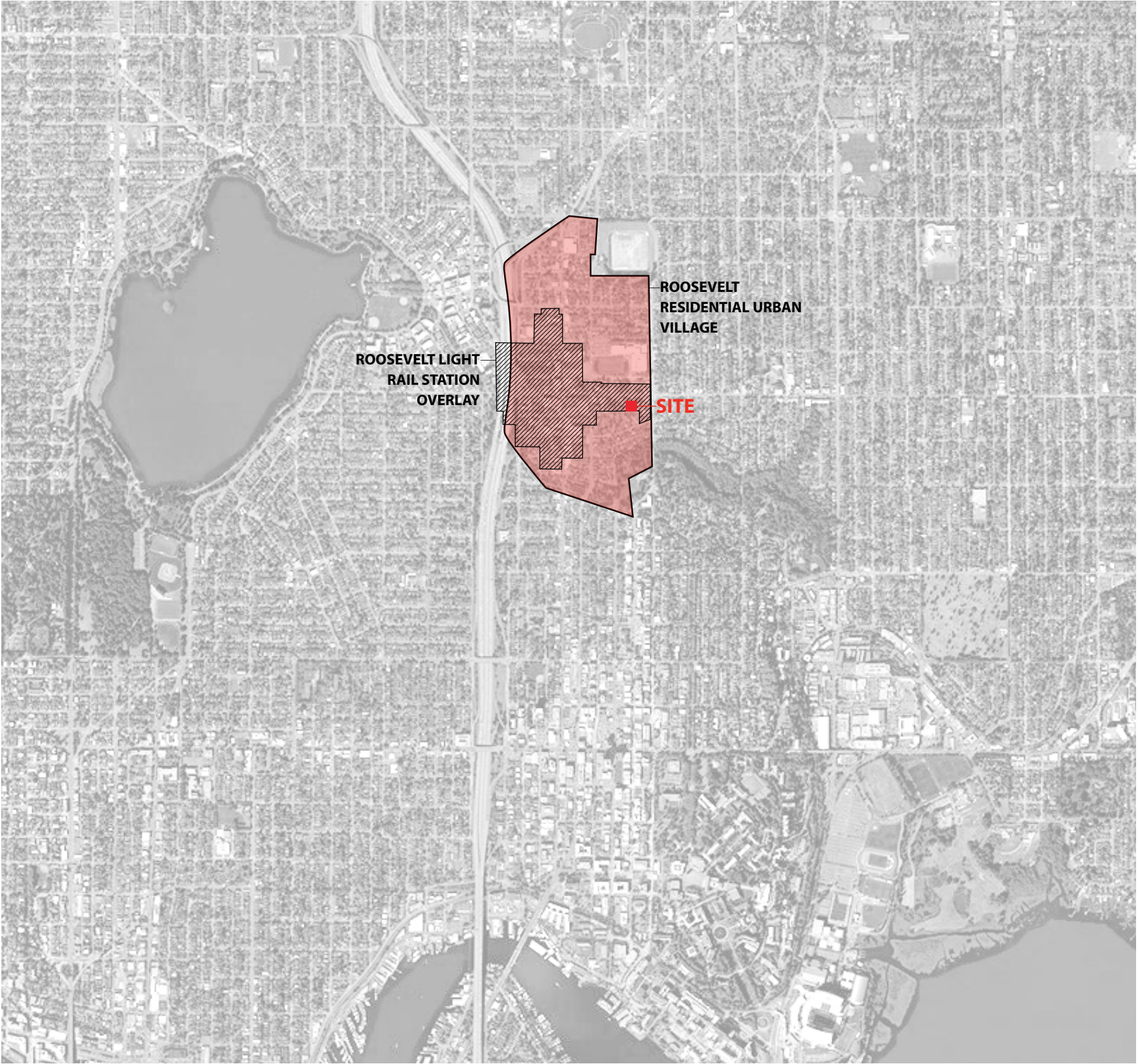
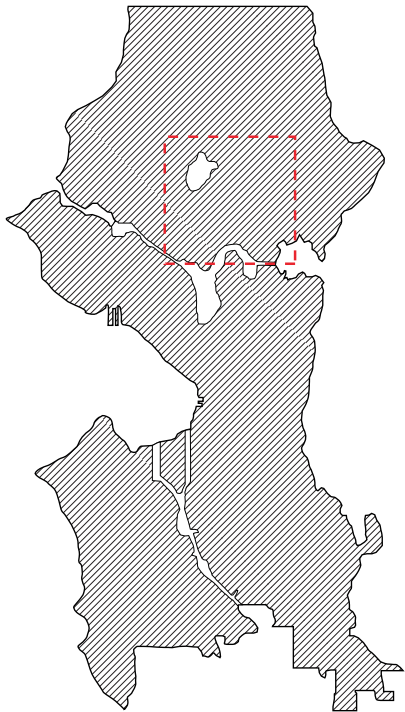
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Project Information

|                   |  |
|-------------------|--|
| Property Address: | 1403 NE 65th St.   |
| Owner:            | Roosevelt Development Group LLC  |
| Developer:        | Roosevelt Development Group LLC<br>Nick Miller (Contact)<br>T (206) 812-8126 |
| Architect:        | Weinstein A+U LLC<br>Heather Hargesheimer (Contact)<br>T (206) 443-8606      |
| Landscape:        | Karen Kiest   Landscape Architects   |





# DEVELOPMENT OBJECTIVES & SITE CONTEXT

## Development Objectives

The proposed project is a four-story, apartment building approximately 44-feet in height and approximately 29,000-sf. The building will contain 48 residential units, resident lobby and amenity spaces, an outdoor landscaped terrace and roof deck, 1,400-sf street level commercial, 7 structured parking stalls, and a storage and service basement.

The project development objectives are as follows:

- Provide a high quality living environment for residents convenient to work, leisure and play
- Provide a pedestrian-oriented streetfront on NE 65th St with features to benefit the neighborhood:
  - Widen the sidewalk and plant street trees to buffer the busy street
  - Strengthen the street edge with an appropriately scaled and transparent facade
  - Offer continuous weather protection
- Provide a resident-oriented streetfront on 14th Ave NE:
  - Locate entries or program to provide eyes on the street but minimize noise nuisances
  - Provide landscape to blend with neighbors
- Be sensitive to the site's location in a transition zone from neighborhood commercial to single-family. This will inform building massing, scale, street level program and building access.
- Be a good neighbor. This will inform the project in terms of:
  - Streetscape design including lighting
  - Landscape design & material selection
  - Parking access
  - Trash & recycling storage

- Future Link Light Rail Station
- 1. Roosevelt High School
- 2. Rising Sun Produce
- 3. Pies and Pints
- 4. Whole Foods
- 5. Pizza Hut
- 6. Silhouette Antiques and Gifts
- 7. Transit Oriented Development Sites
- 8. Cowen Park
- 9. Ravenna Park
- 10. Dwell Condominiums
- 11. Bartell Drugs, Wells Fargo, UPS Store
- 12. Toronado Beer Hall
- 13. Health Mutt
- 14. All-Star Cleaners
- 15. Qwest Communications
- 16. Future Development parcels
- 17. Future 7-Story Mixed-Use Project (MUP approval)
- 18. Future 4-story Mixed-Use Project (MUP approval)

NE 67TH ST

NE 66TH ST

NE 65TH ST

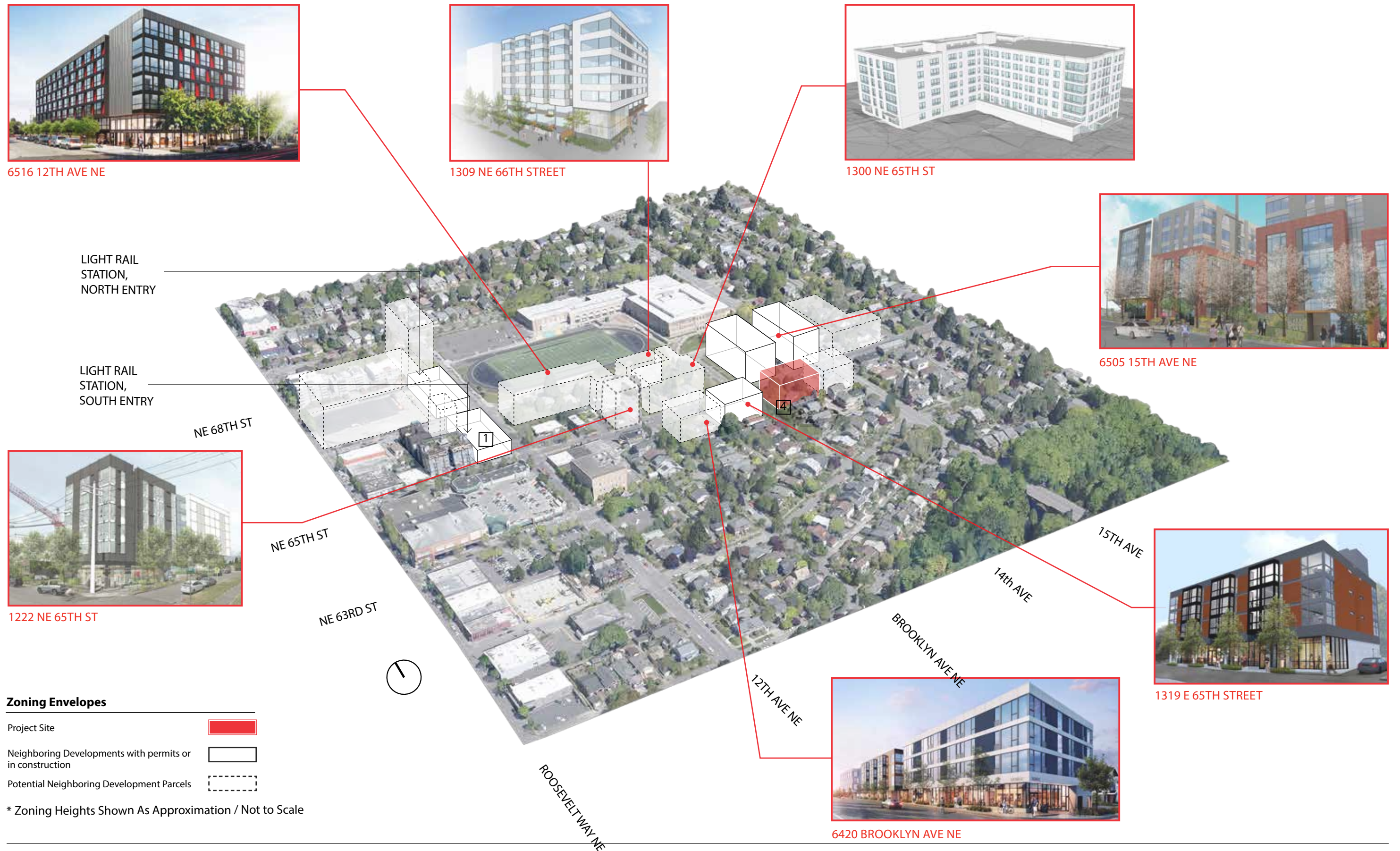
NE 64TH ST

NE 63RD ST





## CONTEXT: FUTURE DEVELOPMENT ZONING ENVELOPES









# SITE PLAN

## Setback Requirements

- A 15-ft setback for portions of structures above 13-ft in height to a max of 40-ft, and for each portion above 40-ft, an additional setback at the rate of 2-ft of setback for every 10-ft, is required where a lot abuts a side lot in a residential zone. This setback is from the south property line.
- A setback forming a triangular area 15-ft on a side is required where a lot abuts the intersection of a side lot line and front lot line of a lot in a residential zone. This setback is from the southwest corner of the property.
- Single-phase overhead power lines that cross NE 65th at the northeast corner of the property and extend down the alley along the eastern property line require a minimal setback (6" or less).

## Traffic and Circulation

- NE 65th is a minor arterial/comm. connector street with time-limited parking to the south, and unrestricted parking to the north.
- NE 65th serves a number of bus lines
- NE 65th is a principal pedestrian street.
- 14th Ave NE is a residential with minimal traffic and resident parking on both sides of the street.
- A mid-block alley off NE 65th can serve vehicular access needs.

## Streetscape

- NE 65th St has a narrow sidewalk (+ 7'-6") and lacks street trees on either side of the street. There are infrequent pedestrian crosswalks (and closest crosswalks are at 12th Ave NE and 15th Ave NE).
- 14th Ave NE has wide tree-lined sidewalks (+ 17'-6") with generous planting strips at the curb edge. Street trees are well established.

## Neighborhood Influences

- NE 65th St is zoned to become commercial. Locate major building entries, and commercial programs on this street.
- 14th Ave NE is residential and quiet. Avoid adding significant activity to this street to preserve privacy.

Building Footprint

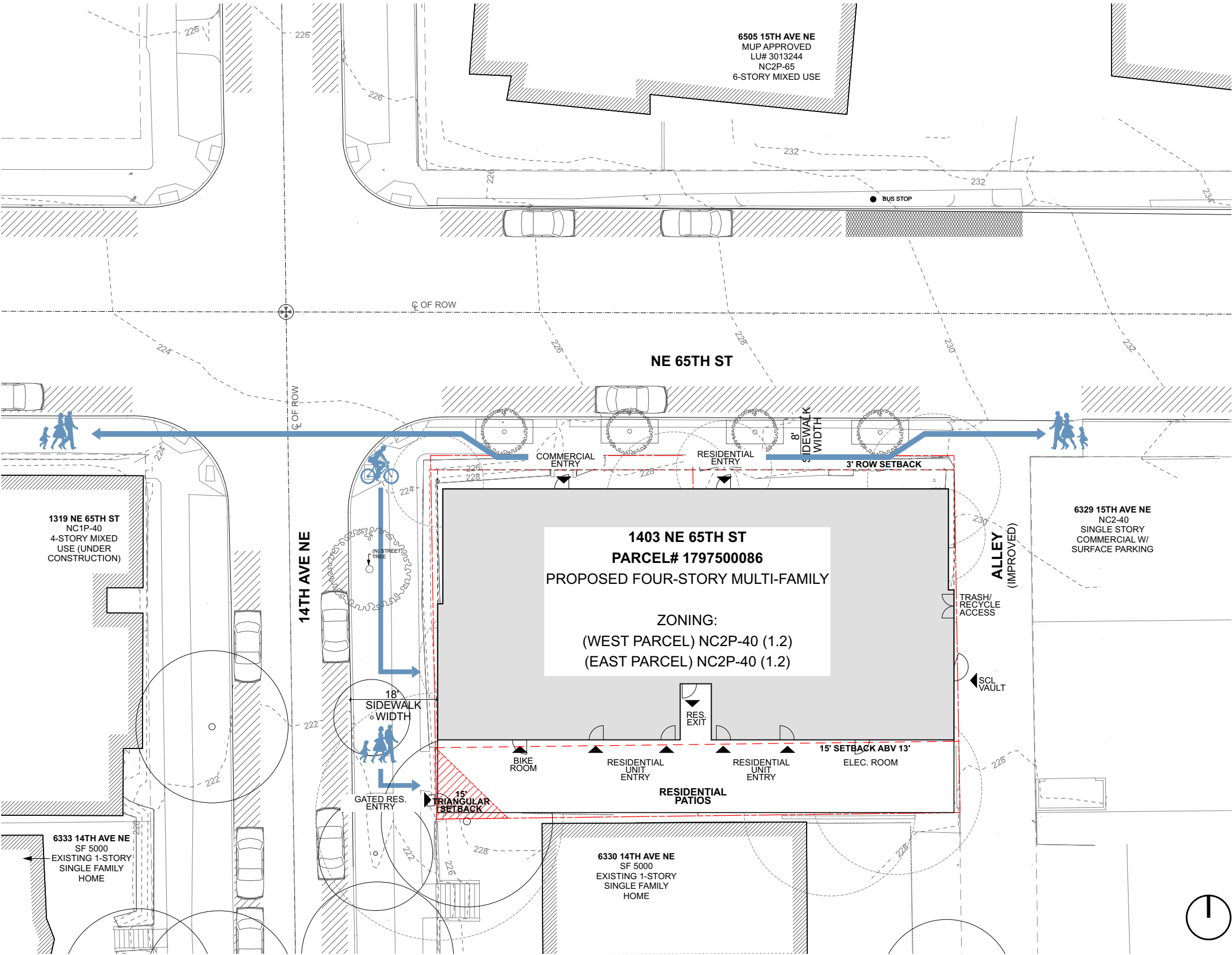
Street Parking Area

Pedestrian Entry/Exit











DESIGN ALTERNATE SUMMARY



ALTERNATE 1 (L-SHAPE)

**Summary**  
Stories: 4 (3-over-1 + 1 below grade)  
Unit Count: 45 (5,524 unit SF/ Floor)  
Floor Area: 16,572-SF Residential  
2,520-SF Commercial  
2,205-SF Parking  
**27,365-SF FAR (33,304 Max FAR)**  
Parking: 6 stalls (residential)  
Ground Floor Uses:  
• NE 65th St: Commercial / Residential Amenity  
• 14th Ave NE: Residential Lobby & Amenity  
• Alley: Parking Access

**Primary Disadvantages/Concerns**  
• Transition to south neighbor is abrupt and unfriendly  
• No overhead weather protection

**Potential Departures**  
• Reduced width of overhead weather protection on principal pedestrian streets (< 6-ft)  
• Reduced non-residential use depth at street level  
• Structure in triangular setback abutting a residential lot  
• Structure in south PL setback above 13-ft



ALTERNATE 2 (NO DEPARTURES)

**Summary**  
Stories: 4 (3-over-1 + 1 below grade)  
Unit Count: 45 (5,198 unit SF/ Floor)  
Floor Area: 15,594-SF Residential  
2,965-SF Commercial  
2,034-SF Parking  
**26,739-SF FAR (33,304 Max FAR)**  
Parking: 4 stalls (residential)  
Ground Floor Uses:  
• NE 65th St: Commercial / Residential Amenity  
• 14th Ave NE: Residential Lobby & Amenity  
• Alley: Parking Access

**Primary Disadvantages/Concerns**  
• 15’ setback has potential to be an unsafe space with limited visibility from residents or passerby  
• Limited parking on site due to the size of retail, 15-ft triangular setback, and bike parking requirement  
• Minimal lobby is narrow and cannot accommodate an amenity room, which would be desired to offset the small units

**Potential Departures**  
• None



ALTERNATE 3 (PREFERRED)

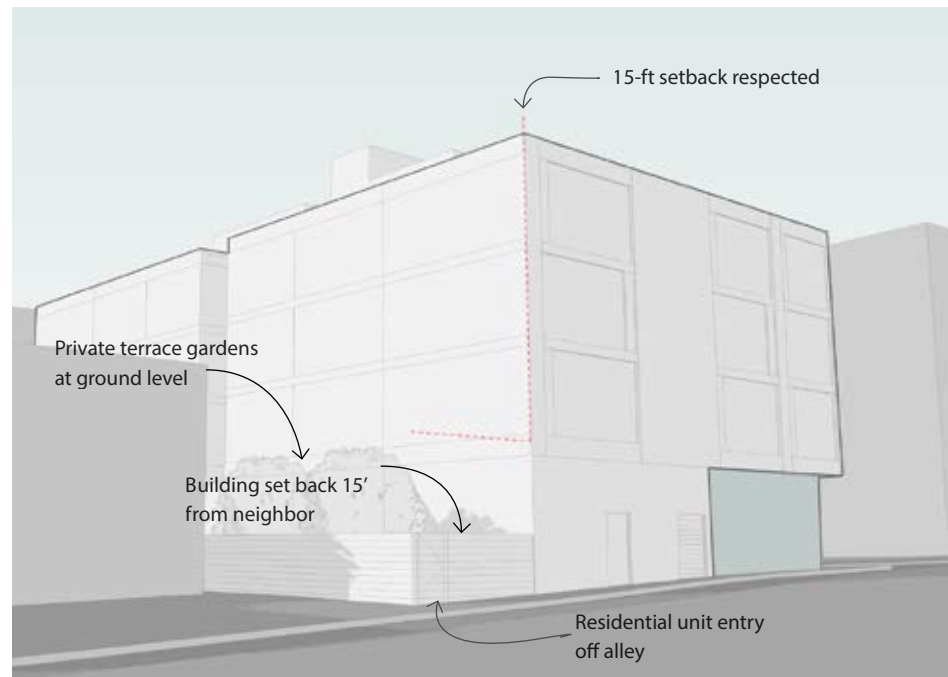
**Summary**  
Stories: 4 (3-over-1 + 1 below grade)  
Unit Count: 52 (4,906 unit SF/ Floor)  
Floor Area: 15,966-SF Residential  
1,400-SF Commercial  
0-SF Parking  
**24,221-SF FAR (33,304 Max FAR)**  
Parking: 0 stalls (residential)  
Ground Floor Uses:  
• NE 65th St: Commercial / Residential Amenity  
• 14th Ave NE: Commercial and Bike Room

**Primary Advantages/Benefits**  
• 4-ft setbacks at street level on NE 65th St and 14th Ave NE enhance the pedestrian experience: providing continuous weather protection, a wider sidewalk, and street trees in curbside planters  
• The garden spaces on the south provides screening and a privacy buffer to the neighbor  
• Provides continuous weather protection with a first floor setback on 65th St and 14th Ave  
• Central location of stair and elevator cores minimizes their visibility from the street, creating a mass that appears smaller and lighter

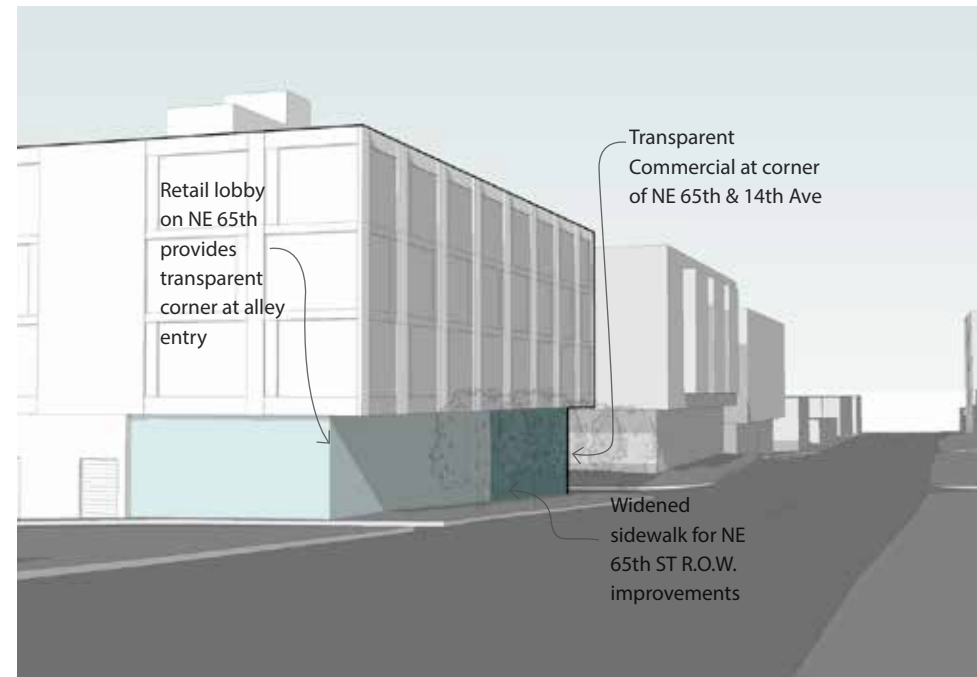
**Potential Departures**  
• Increased percentage of residential use at street level on primary pedestrian street (NE 65th)  
• Reduced width of overhead weather protection on principal pedestrian streets (< 6-ft)  
• Reduced non-residential use depth at street level



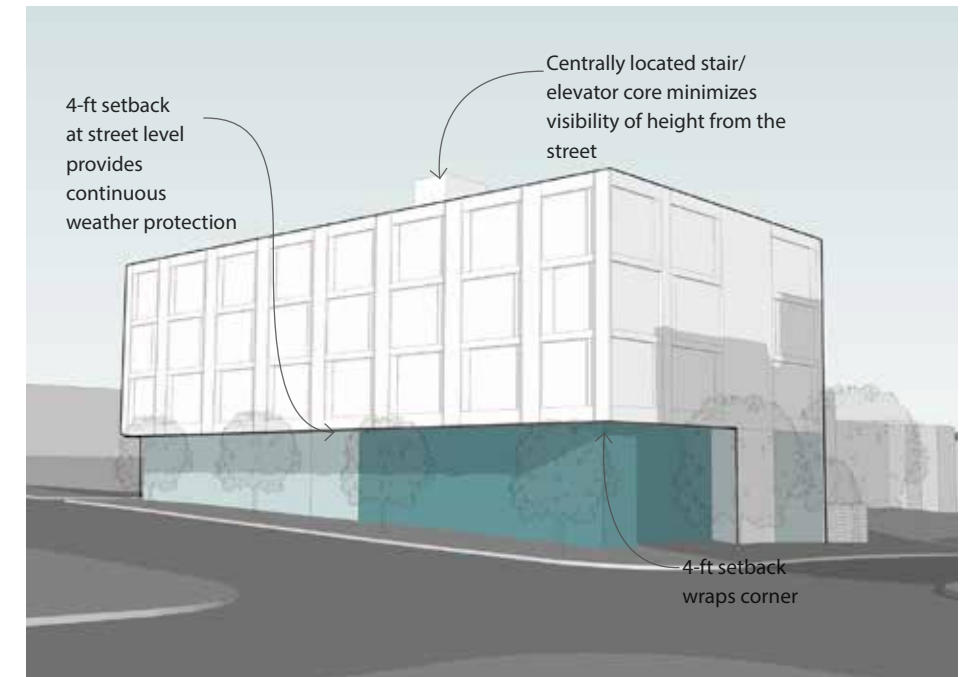
## DESIGN ALTERNATE SUMMARY



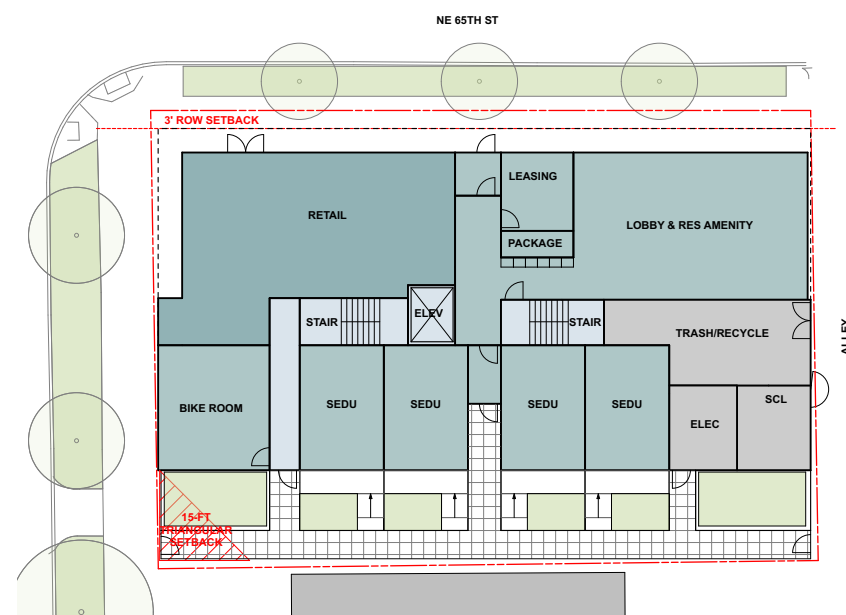
**Southeast Corner (Alley)**



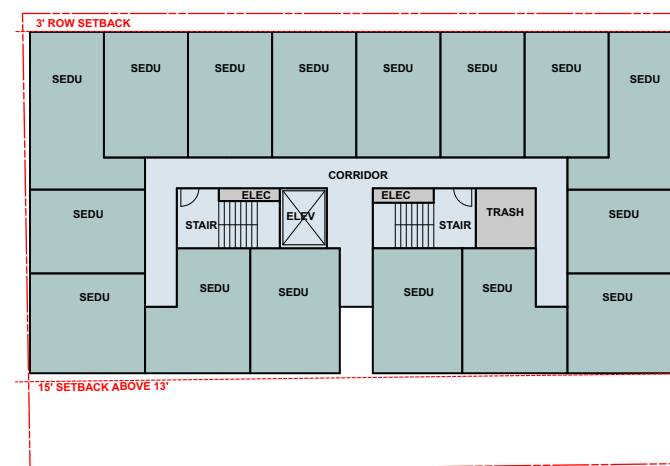
**Northeast Corner (Corner of NE 65th St and Alley)**



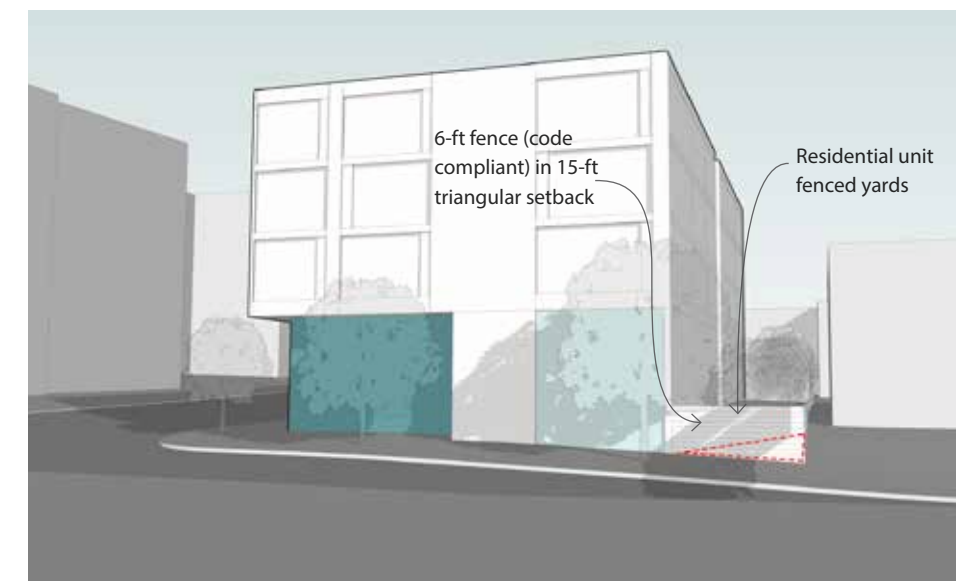
**Northwest Corner (Corner of NE 65th St and 14th Ave NE)**



**Street Level Plan (Level 1)**



**Typical Residential Plan (Level 2)**



**Southwest Corner (14th Avenue NE)**



CURRENT MASSING & SUMMARY OF EDG GUIDANCE



The points to the right summarize the guidance provided to the design team by the Board at the Early Design Guidance meeting on January 9, 2017. The guidance has been grouped into categories that appear on the right side of the following two-page spreads with a brief description of how the design has been developed in response to the Board's guidance.

1 Massing, Facade, and Context

- a. Board supported Option 3, the preferred massing option. It promotes a strong connection to the street, allows for mature growth of street trees, and sets back from single family zone.
- b. Board supported the creation of a united, continuous streetscape along NE 65th Street with street trees and high visibility, while providing variation from neighboring masses at the upper levels.
- c. Board generally supported the complimentary forms between the three adjacent sites, but also encouraged the exploration of variation between the upper levels and facade composition The board supported the “woven” composition of the preferred massing option as it creates visual interests.
- d. Board identified interior flexibility of of the ground level spaces as a priority, and directed the applicant to build in flexibility so that the ground level residential amenity space may change to a commercial space over time.
- e. Board would like to see the project visualized in the greater context of redevelopment.

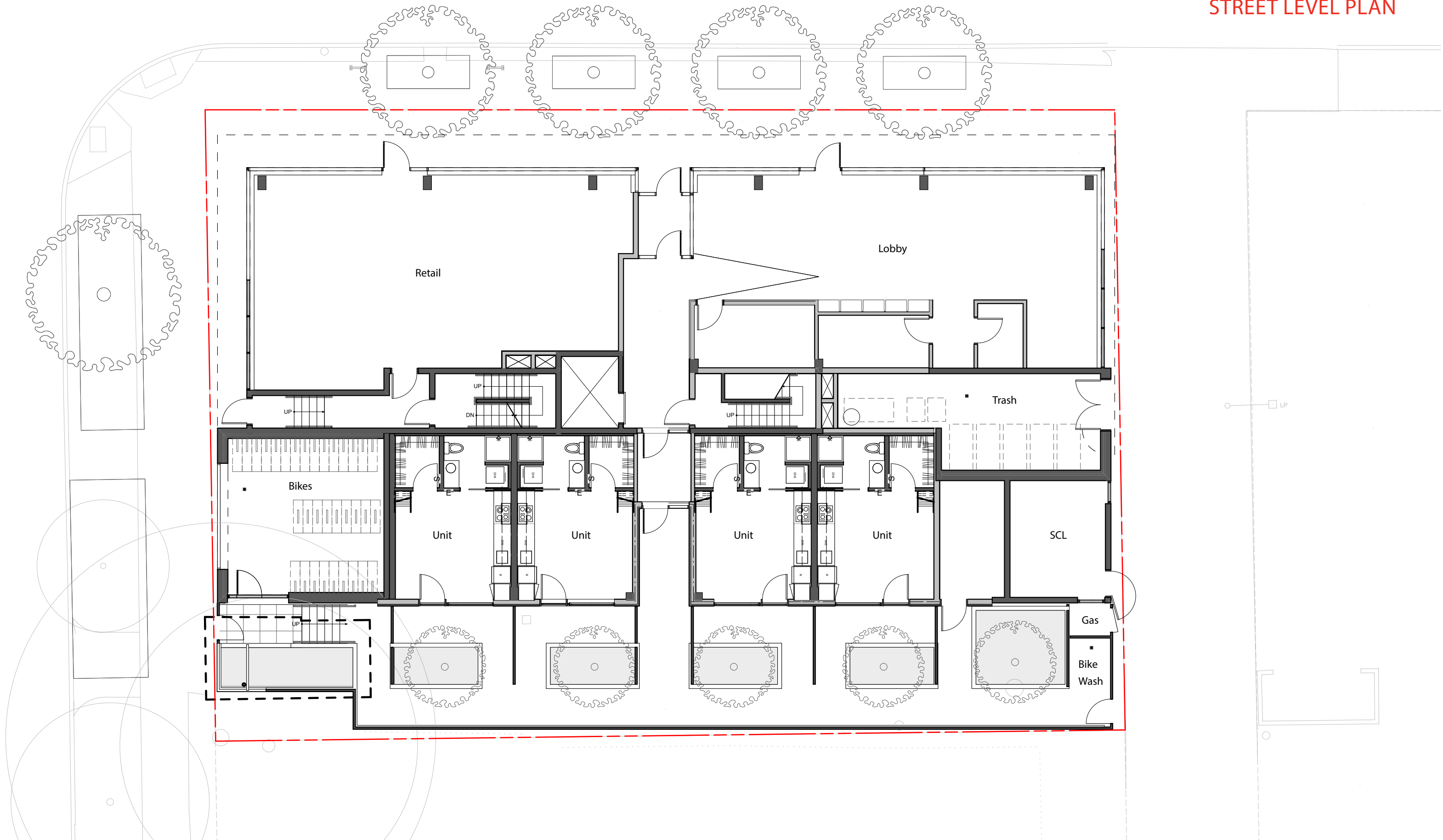


2 Zone Transition & Landscaping

- a. Board supported the proposed 15-foot rear setback as it provides an appropriate zone transition.
- b. Board prioritized preservation of the Black Locust Tree located in the southwest corner on the property line.
- c. Board discussed the high level of glazing on the south facade and directed further study of secondary architectural elements that could be employed to obscure downward views and minimize disruption of the privacy of residents on adjacent sites.
- d. Board supported the location of the rooftop terrace along the northern building edge as it is sensitive to the adjacent single family zone. The Board encouraged further study of the the impacts of noise, light and glare, and service uses on adjacents sites. The design of the fence along the south property line should be sound absorptive.
- e. Board supported the rear-facing ground-floor units with exterior access and private patios within the setback as it promotes livability, landscaping and material screening should be employed as a buffer to dampen noise.

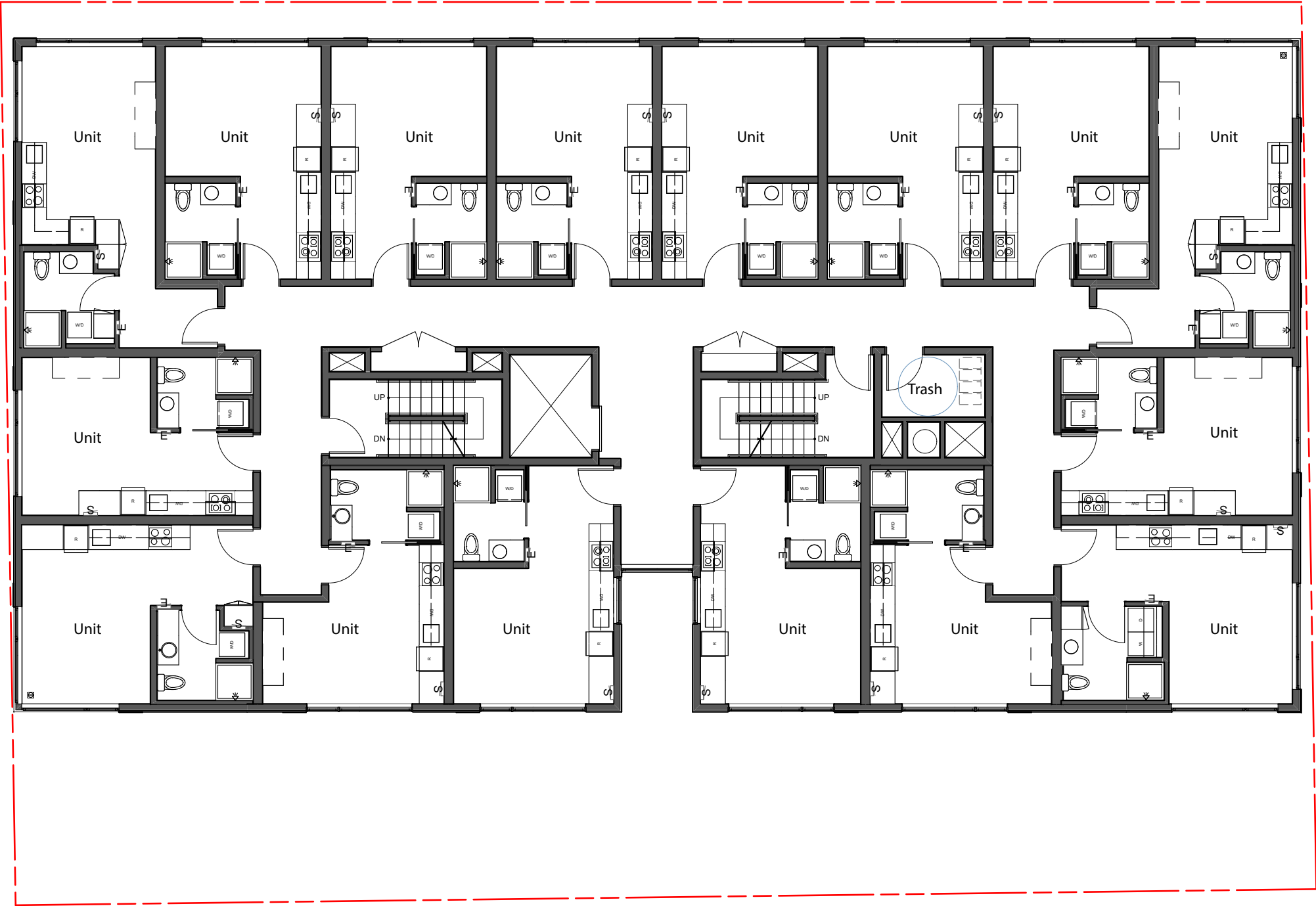


## STREET LEVEL PLAN

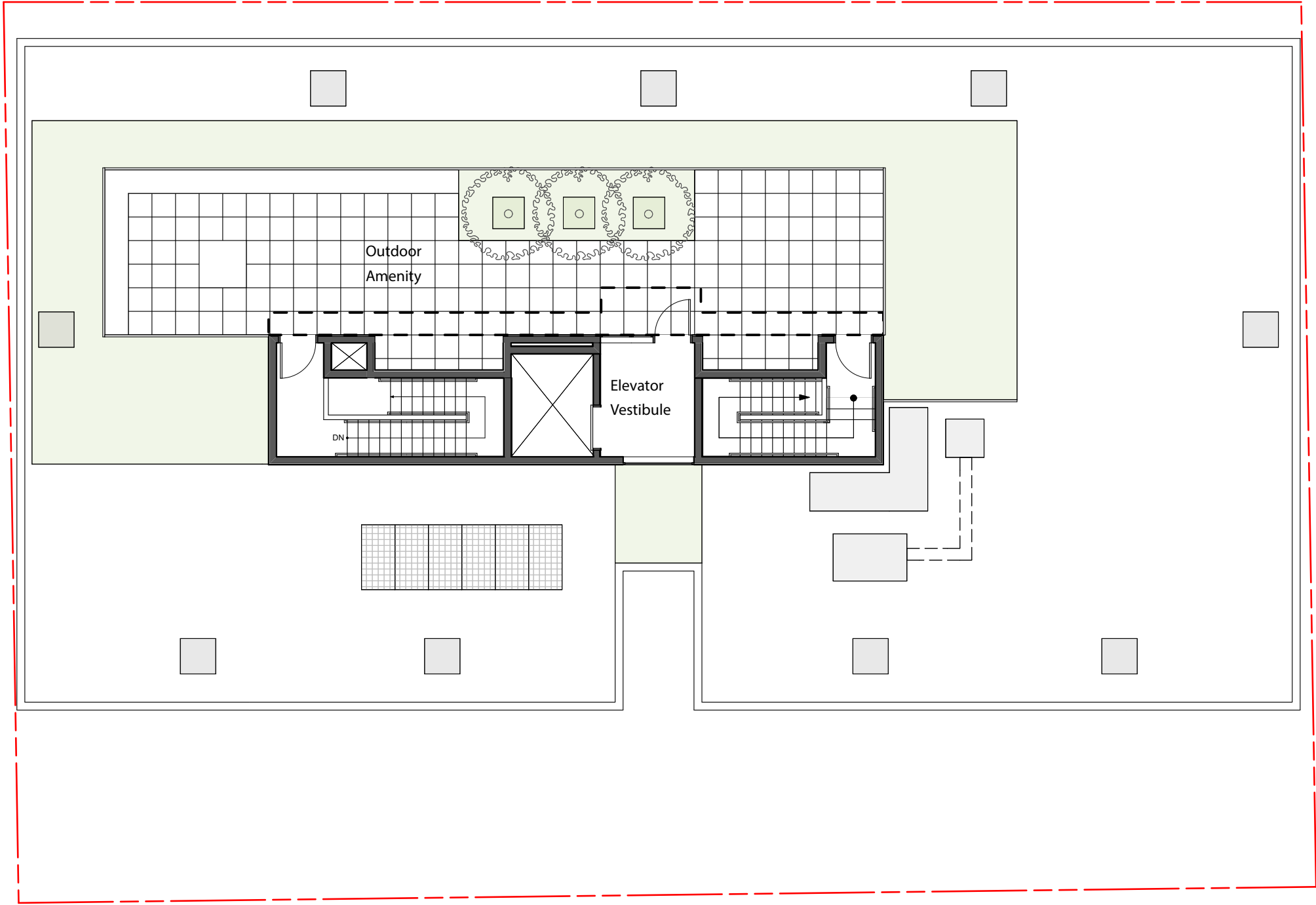




TYP RESIDENTIAL PLAN

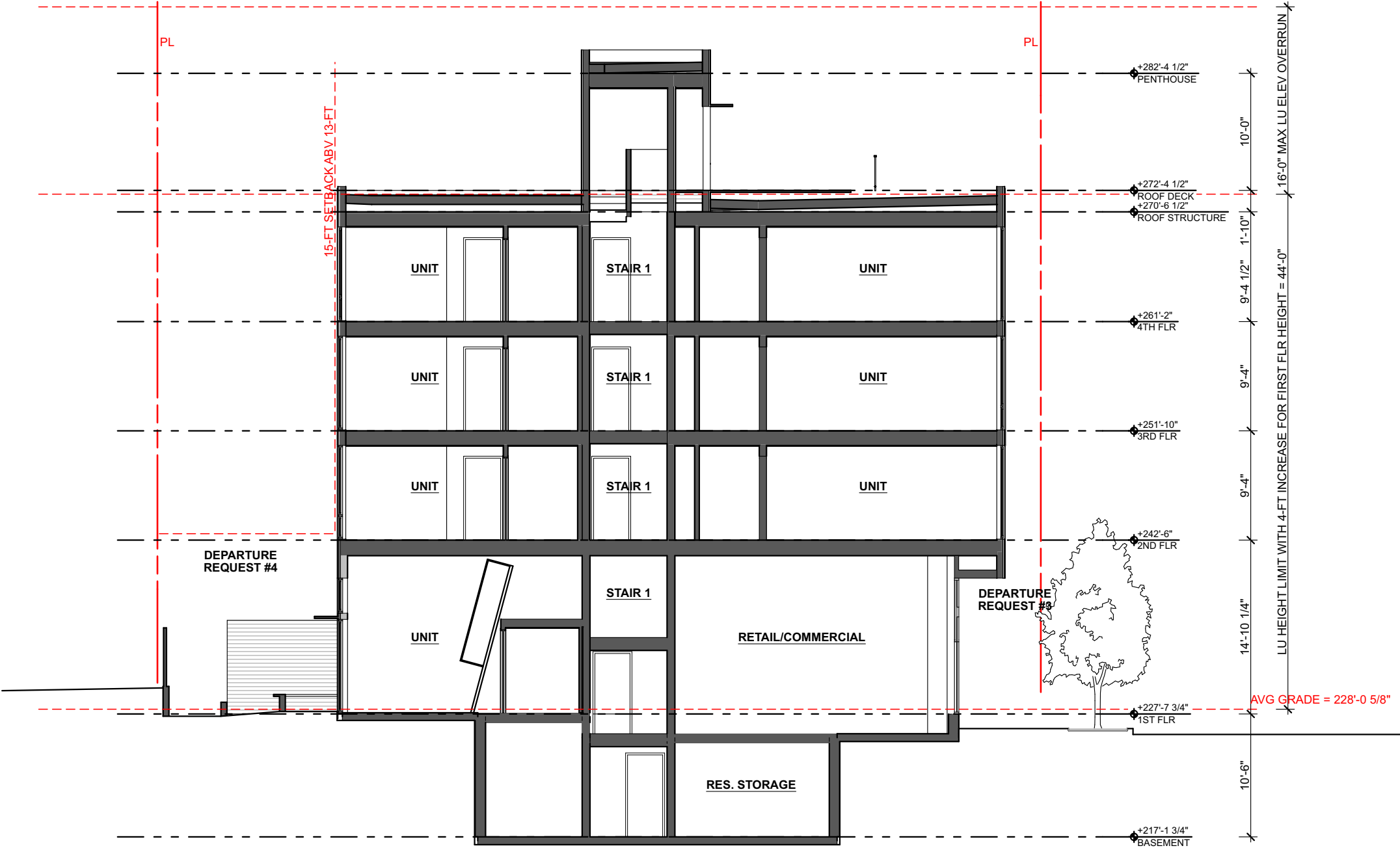








SECTION





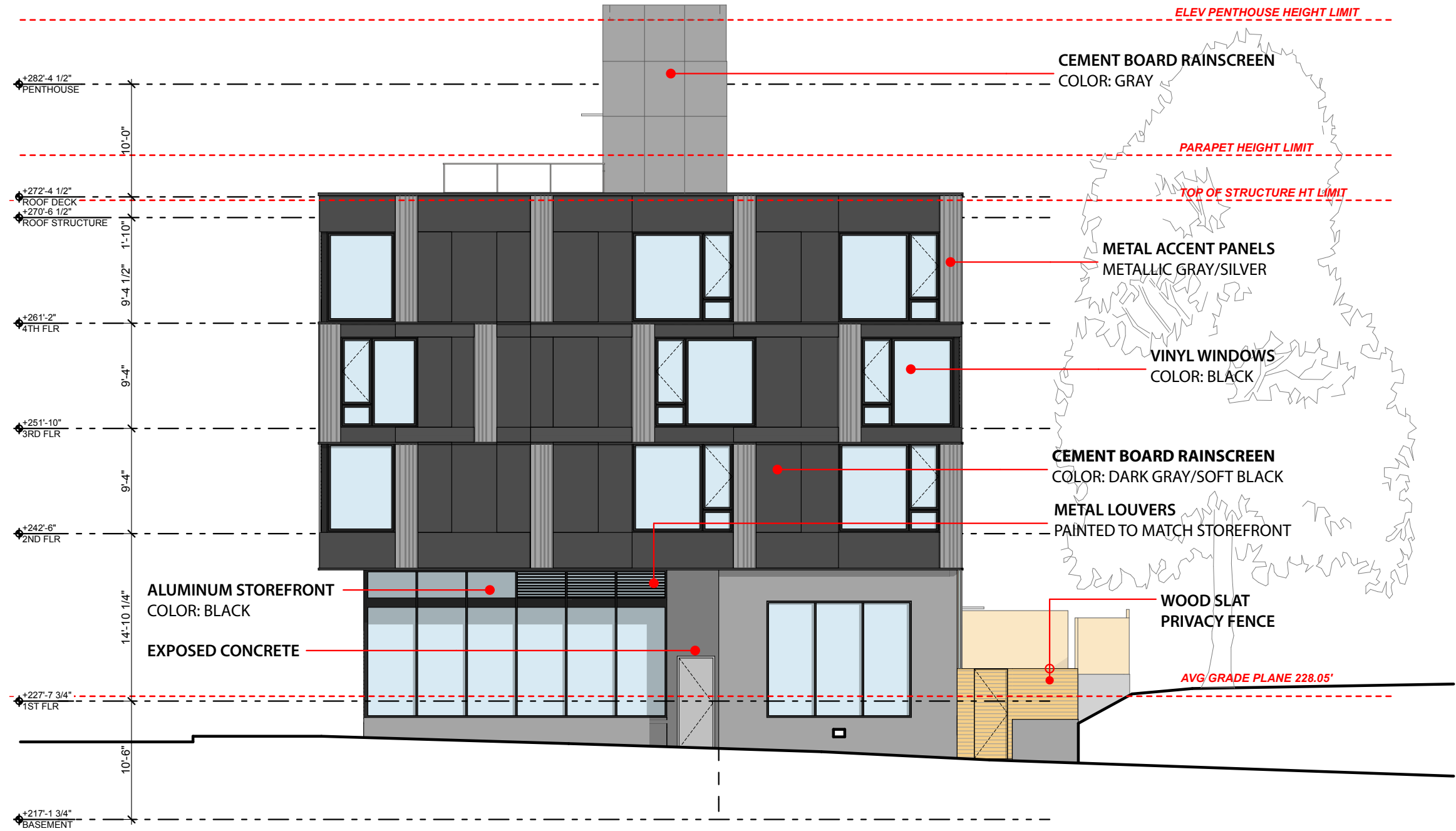


NORTH ELEVATION





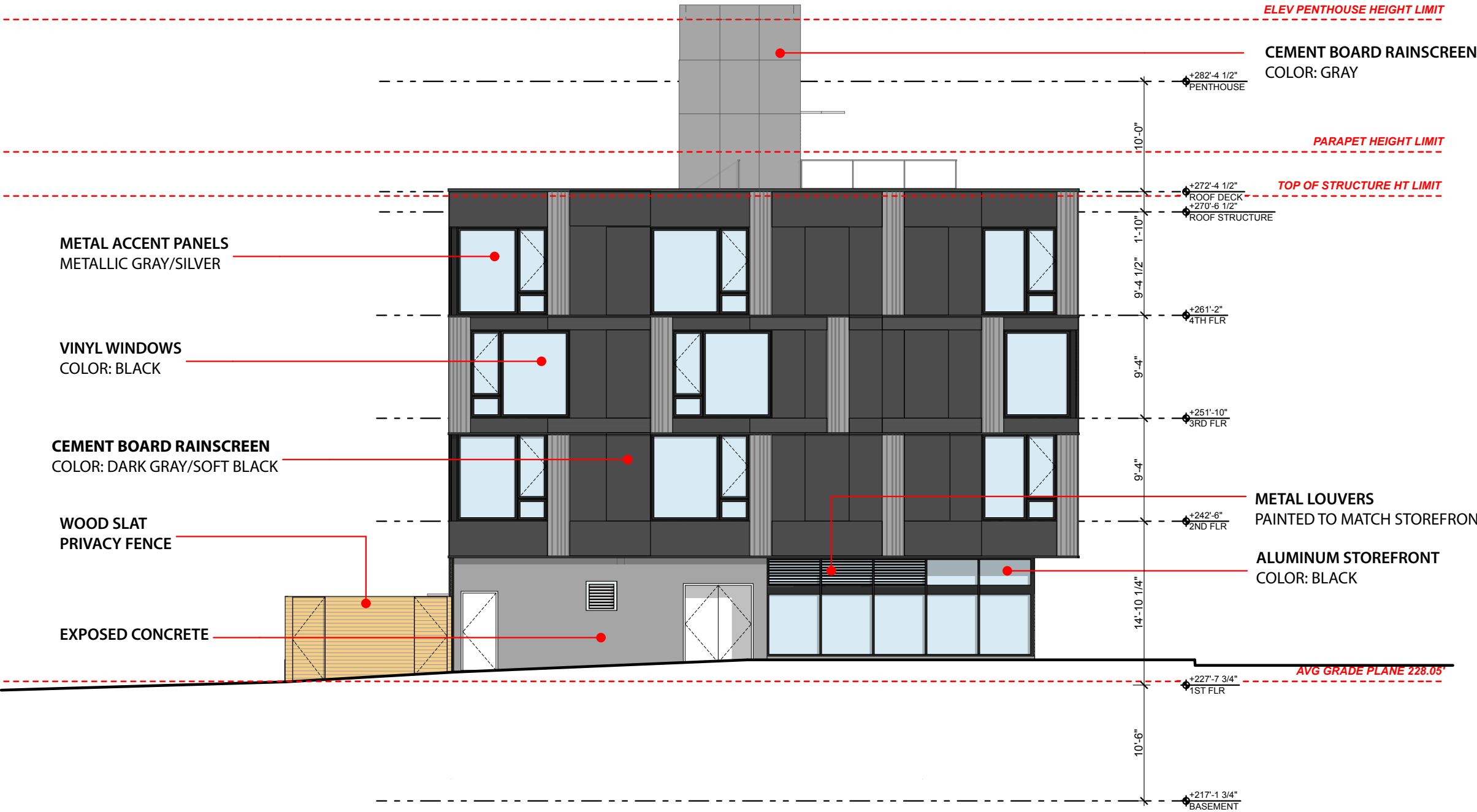
WEST ELEVATION



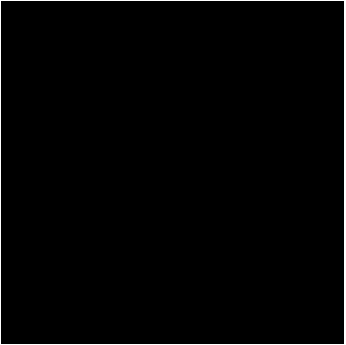
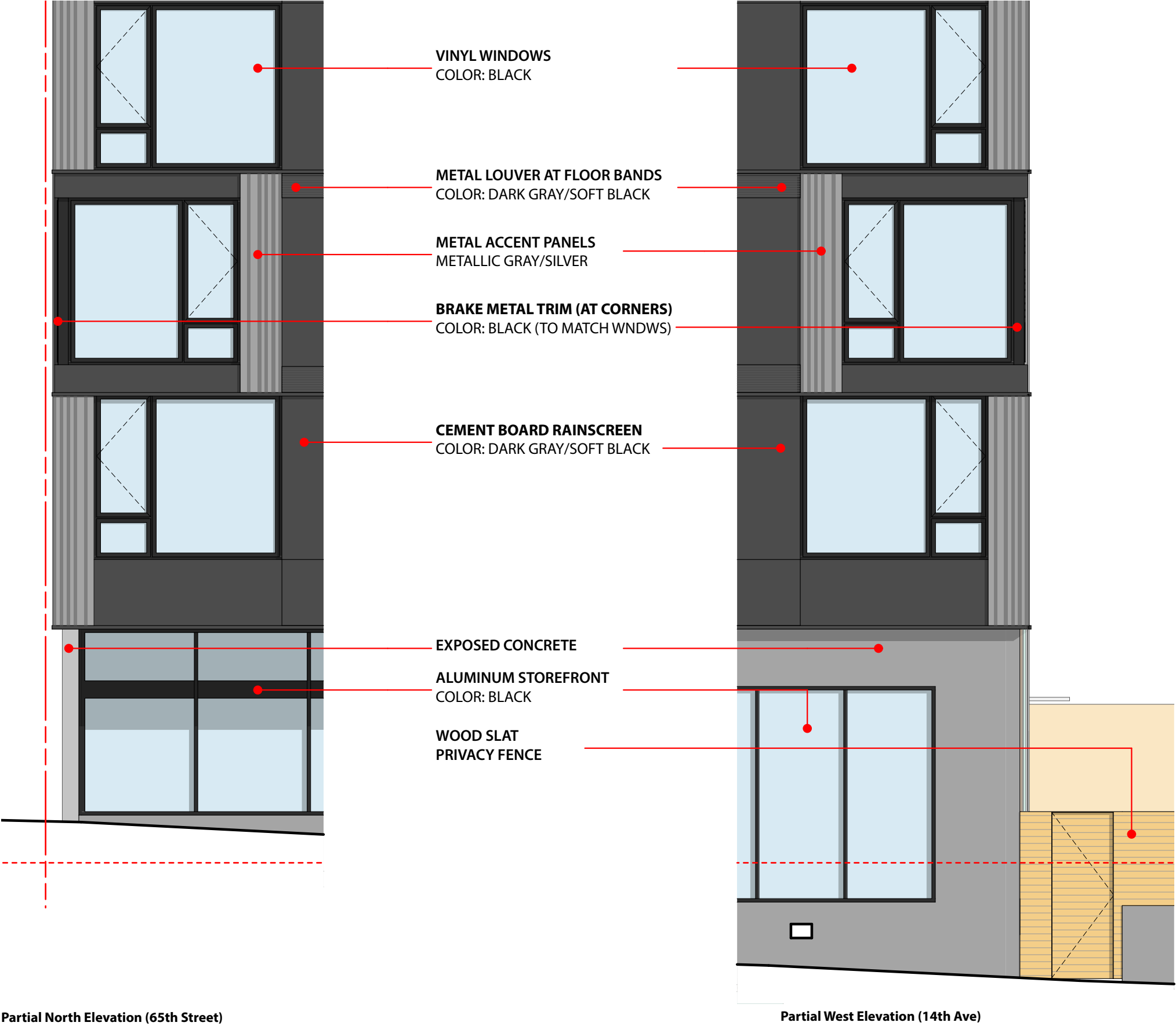
EAST ELEVATION







MATERIAL PALETTE



**VINYL WINDOW FRAME**  
COLOR: BLACK



**BRAKE METAL TRIM (AT CORNERS)**  
COLOR: BLACK (TO MATCH VINYL WDW)



**CEMENT BOARD RAINSCREEN**  
COLOR: CHARCOAL GREY



**METAL ACCENT PANELS**  
COLOR: SILVER



**WOOD OPEN SLAT TERRACE SCREEN**



**ALUMINUM STOREFRONT**  
COLOR: BLACK



**CAST CONCRETE**







LIGHTING AND SIGNAGE







Residential Entry Signage



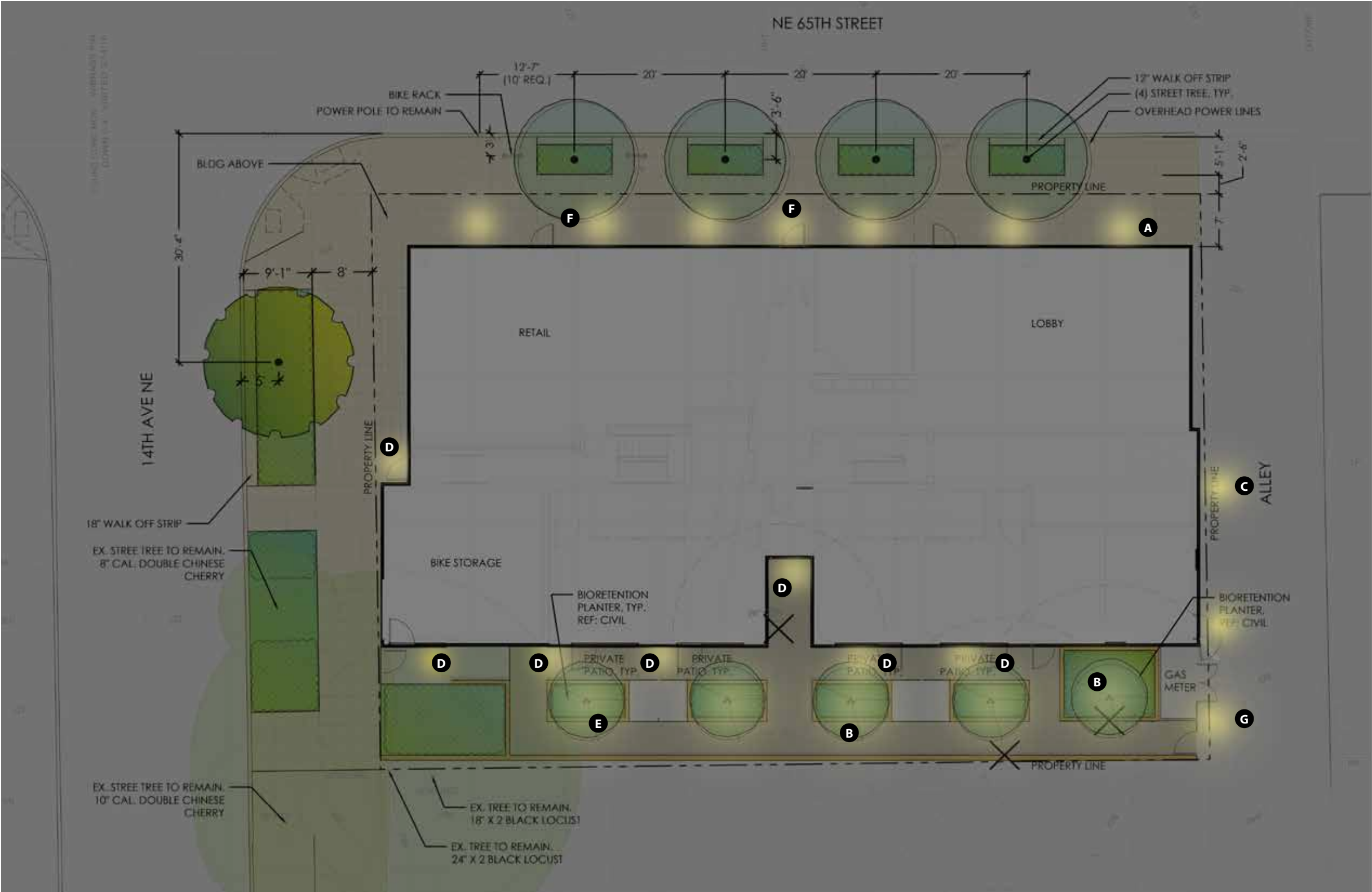
Commercial Signage - NE 65th St



Commercial Signage - 14th Ave NE



LIGHTING STREET LEVEL PLAN



A Canopy downlight



B Landscape accent light



C Trash entry light



LIGHTING ROOF PLAN



D Residential entry light



E Landscape Downlight

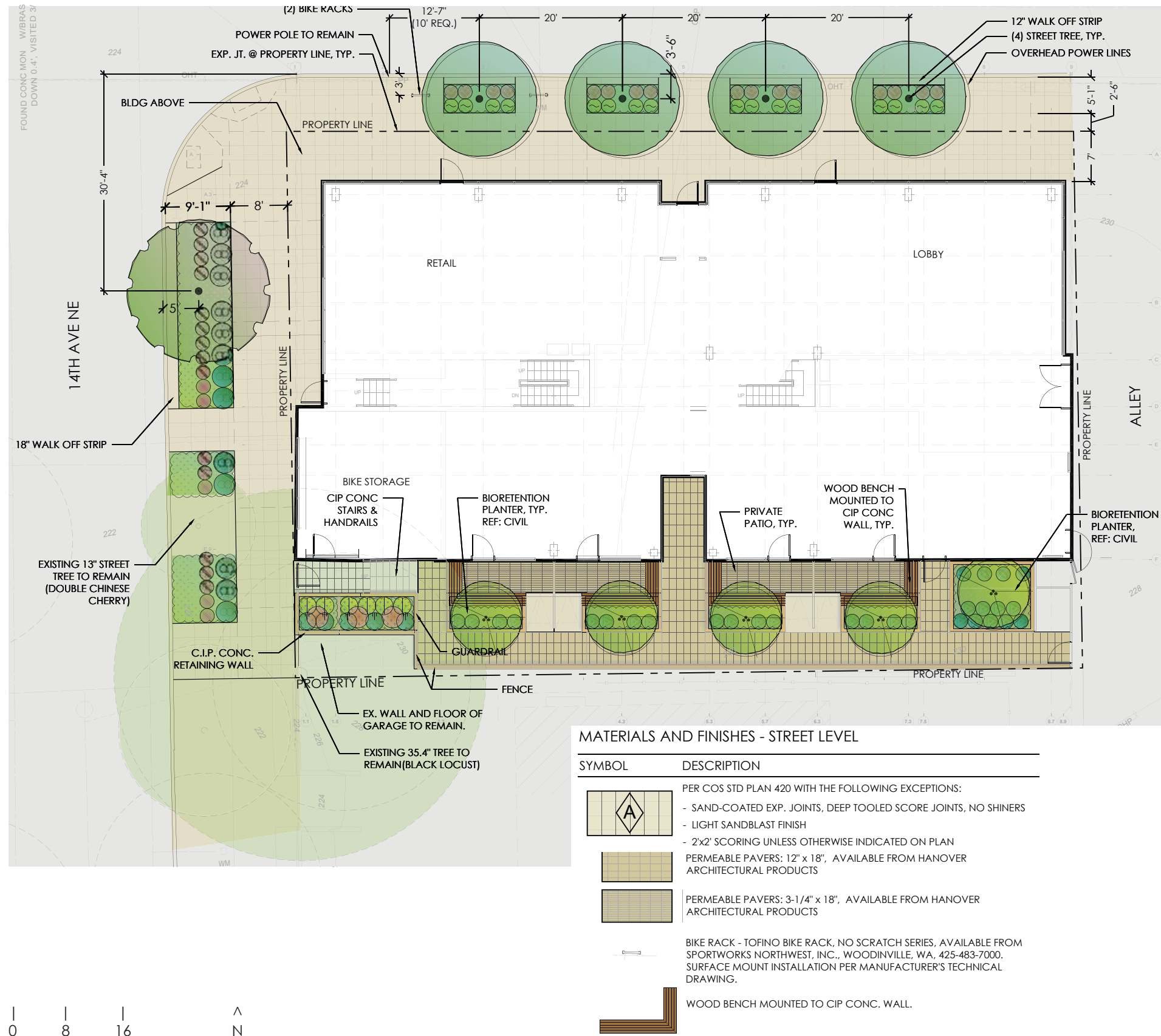


F Directional signage light

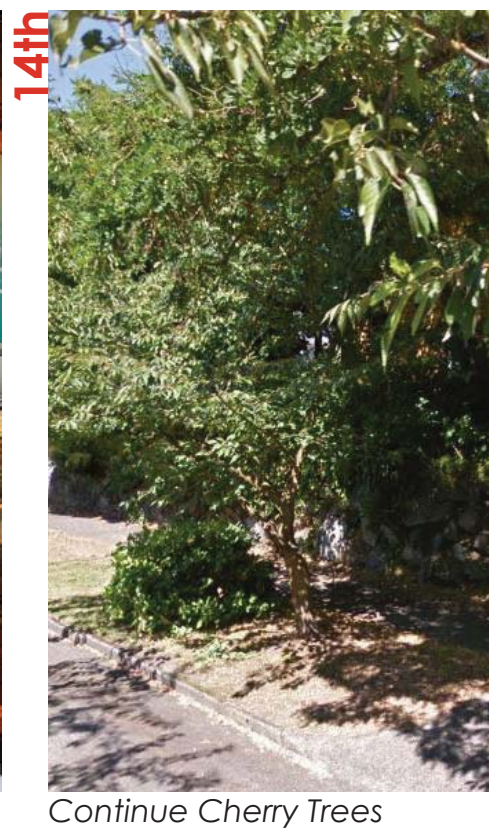


G Bollard

LANDSCAPE STREET LEVEL PLAN



Commercial Spillover



Continue Cherry Trees



Through Block Walk


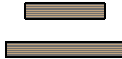




At Grade Bioretention Planter





MATERIALS AND FINISHES

| SYMBOL  | DESCRIPTION  |
|---|--|
|  | PAVERS ON PEDESTAL, 24"x24X2" THICK PRECAST CONCRETE PAVERS, TEXADA HYDRAPRESSED SLABS, COLOR: NATURAL, AVAILABLE FROM ABBOTSFORD CONCRETE PRODUCTS, 1-800-663-4091. PEDESTALS PER ARCH. REF: 1/L212 |
|  | BENCH: 15.75" W X 17" HT. BENCHES WITH BACKREST, 2 LENGTHS: 80" AND 119" MODEL: LANDSCAPE AVAILABLE FROM MMCITE. REP: SALES@MDSFCO.COM   |
|  | SMALL DECIDUOUS TREE<br>3X3X36"HT FIBERGLASS PLANTER<br>30" DEPTH LIGHTWEIGHT SOIL   |
|  | BEACH PEBBLES, 2-3" Ø, COLOR - DARK, AVAILABLE FROM MARENAKOS ROCK CENTER OR COVERALL STONE. 4" DEPTH, MIN. 2 LAYERS.  |



Narrow but Generous Terrace



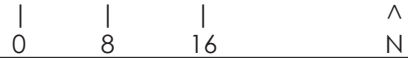
Bench



Sedums with Grasses



Trees in Pots

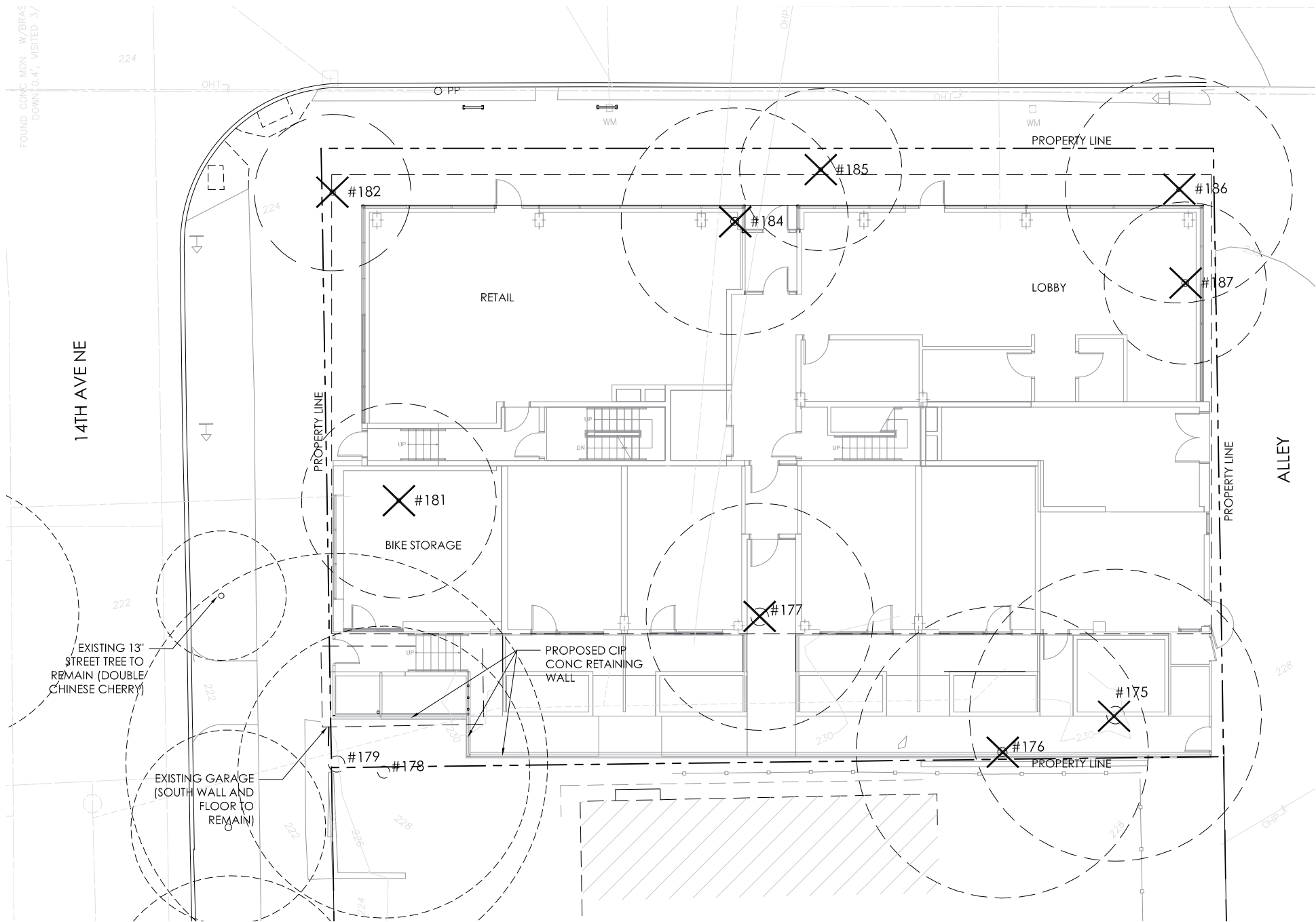




PLANTS

| SYMBOL  | BOTANICAL NAME/ COMMON NAME  | TREES   |
|---|--|---|
| PROPOSED STREET TREES:  |  |   |
|    | <b>NE 65TH STREET:</b><br>ULMUS PROPINQUA 'JFS-BIEBERICH' / EMERALD SUNSHINE ELM |    |
|   | <b>14TH AVE NE:</b><br>PRUNUS SERRULATA /DOUBLE CHINESE CHERRY*                  |    |
|   | AMELANCHIER X GRANDIFLORA 'AUTUMN BRILLIANCE' **/ SERVICEBERRY                   |    |
| ON-SITE TREES   |  |   |
| SHRUBS & PERENNIALS   |  |   |
|    | BUXUS MICROPHYLLA JAPONICA 'WINTER GEM' / WINTER GEM JAPANESE BOXWOOD *          |   |
|    | LONICERA PILEATA / BOXLEAF HONEYSUCKLE *   |   |
|    | ROSA 'AMBER'/ FLOWER CARPET AMBER GROUNDCOVER ROSE                               |   |
|    | SPIRAEA X BUMALDA 'DENISTAR' / SUPERSTAR SPIREA *                                |   |
|   | VIBURNUM DAVIDII / DAVID'S VIBURNUM *  |   |
|  | VIBURNUM X BODNANTENSE 'DAWN'/ PINK DAWN VIBURNUM                                |   |
|  | SALIX PURPUREA 'NANA'/ DWARF ARCTIC WILLOW                                       |   |
|  | CAMELLIA SASANQUA 'JEAN MAY'/ JEAN MAY CAMELLIA                                  |   |
|  | FRAGARIA CHIOLENSIS/ COASTAL STRAWBERRY *  |   |
|  | LIRIOPE MUSCARI / LILYTURF *   |   |
| BIORETENTION PLANTS   |  |   |
|  | JUNCUS EFFUSUS/ COMMON RUSH  |  |
|  | CAREX OBNUPTA/ SLOUGH SEDGE  |   |
|  | IRIS SIBERICA/SIBERIAN IRIS  |   |
|  | CORNUS STOLONIFERA 'FARROW'/ ARCTIC FIRE RED TWIG DOGWOOD                        |   |
|  | LAGERSTROEMIA INDICA X FAURIEI 'MUSKOGEE CRAPE MYRTLE                            |   |
| SHRUBS & GROUNDCOVER  |  |   |
|  | SEDUM TILE BY ETERA 'COLOR MAX' PRE-PLANTED WITH THE FOLLOWING:                  |  |
|  | SEDUM 'AUTUMN JOY'/AUTUMN JOY STONECROP  |   |
|  | STIPA TENUISSIMA / MEXICAN FEATHER GRASS   |   |
|  | ACHILLEA MILLEFOLIUM 'SUMMER PASTELS'/YARROW                                     |   |
|  | ACHILLEA LEWISII 'KING EDWARD' /YARROW   |   |
|  | LIRIOPE MUSCARI/ LILYTURF (50%)  |  |
|  | LIATRIS SPICATA 'KOBOLD' / KOBOLD GAYFEATHER (50%)                               |   |
|  | CAREX TESTACEA/ NEW ZEALAND SEDGE  |   |
| BIORETENTION PLANTS   |  |   |
|  | JUNCUS EFFUSUS 'CARMEN'S GRAY' COMMON RUSH                                       |  |
|  | CAREX OBNUPTA SLOUGH SEDGE   |  |
|  | IRIS SIBERICA Siberian Iris  |  |
|  | Cornus stolonifera 'Farrow' Arctic Fire Redtwig Dogwood                          |  |
|  | Carex testacea Orange Sedge  |  |



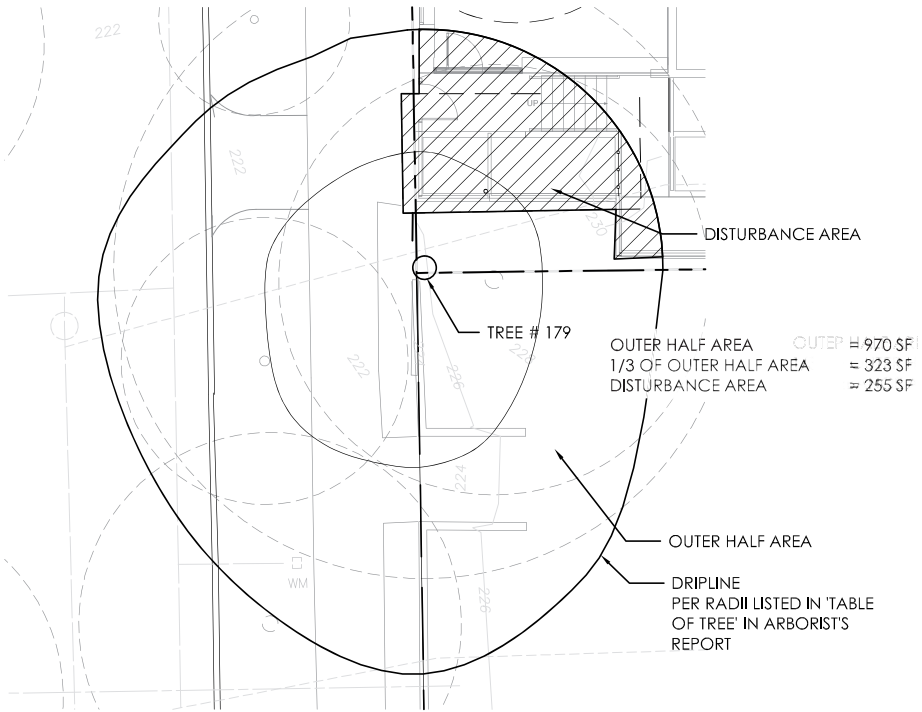


EXISTING TREES

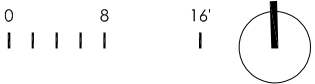
| SYMBOL                                   | DESCRIPTION            |   |              |
|--|------------------------|---|--------------|
|  | #175                   | EXISTING TREE TO REMAIN - PROTECT PER COS STANDARDS |              |
|  | #175                   | EXISTING TREE TO BE REMOVED                         |              |
| TREE I.D.#                               | SCIENTIFIC NAME        | COMMON NAME   | DSH (INCHES) |
| 175                                      | THUJA PLICATA          | WESTERN RED CEDAR                                   | 24.2         |
| 176                                      | TSUGA HETEROPHYLLA     | WESTERN HEMLOCK                                     | 22.2         |
| 177                                      | CHAMAECYPARIS PISIFERA | SAWARA CYPRESS                                      | 26.0         |
| 178                                      | ROBINIA PSEUDOACADIA   | BLACK LOCUST  | 29.4         |
| 179 *                                    | ROBINIA PSEUDOACADIA   | BLACK LOCUST  | 35.4         |
| 181                                      | ROBINIA PSEUDOACADIA   | BLACK LOCUST  | 9.7          |
| 182                                      | PRUNUS LAUROCERASUS    | CHERRY LAUREL                                       | 19.1         |
| 184                                      | ACER MACROPHYLLUM      | BIGLEAF MAPLE                                       | 19.7         |
| 185                                      | ILEX AQUIFOLIUM        | ENGLISH HOLLY                                       | 18.4         |
| 186                                      | PRUNUS LAUROCERASUS    | CHERRY LAUREL                                       | 18.6         |
| 187                                      | MALUS DOMESTICA        | APPLE   | 10.0         |
| * EXCEPTIONAL TREE PER ARBORIST'S REPORT |                        |   |              |

EXISTING TREE NOTES

1. PER SMC 25.11.050, BASIC TREE PROTECTION AREA SHALL BE THE AREA WITHIN THE DRIP LINE OF THE TREE.
2. EXISTING SIGNIFICANT TREES HAVE BEEN IDENTIFIED BY ISA CERTIFIED ARBORIST KATHERINE TAYLOR OF TREE SOLUTIONS INC. IN A REPORT ISSUED MAY 12, 2017. SEE REPORT FOR ADDITIONAL INFORMATION.
2. REFERENCE ARBORIST REPORT FOR EXISTING TREES ON ADJACENT PROPERTY REQUIRING TREE PROTECTION.
3. REFER TO CIVIL DRAWINGS FOR ADDITIONAL SCOPE OF SITE DEMOLITION



1 EXISTING TREE PLAN  
scale 1/8" = 1' - 0"



2 EXCEPTIONAL TREE DISTURBANCE DIAGRAM  
scale 1/8" = 1' - 0"

DEVELOPMENT DEPARTURES

| STANDARD REQUIREMENT |                 | REQUEST  | RATIONALE   |
|----------------------|-----------------|--|---|
| 1A                   | 23.47A.005.C.1  | Residential uses may occupy no more than 20% of the street-level street-facing façade on a designated principal pedestrian street  | To allow a larger percentage of residential uses (and consequently smaller percentage of the required uses) at the NE 65th St façade. While the proposed design does not meet the required percentages, the uses are intended to compliment the streetscape. Approximately 50% of the façade will be a commercial space, with large windows and high transparency, while the remaining 50% will be the residential lobby, leasing center and resident amenity. With small residential units, the public spaces like the lobby and business center are expected to be highly-used gathering spaces. From the exterior, these residential amenity spaces will have large windows consistent with the commercial frontage. |
| 1B                   | 23.47A.005.D.1  | Along designated principal pedestrian streets eating and drinking establishments, offices, or sales and services is required along 80% of the street-facing façade.  |   |
| 1C                   | 23.47A.008.C.1  | In pedestrian-designated zones, a minimum of 80% of the width of a structure's street-level façade facing a principal pedestrian street shall be occupied by uses listed in subsection 23.47a.005.D.1.   |   |
| 2                    | 23.47A.008.B.3  | Non-residential uses shall extend an average depth of at least 30-ft and a minimum depth of 15-ft from the street-level street-facing façade.  | Allow a reduced depth of an average of 26-ft for the commercial space with a 32-ft space for 18-ft of the façade and 22-ft space for the remainder. The projects aims to enhance the pedestrian environment on NE 65th St, with widened sidewalks, plantings, and weather protection. To match the adjacent project's street frontage, the facade of the first floor needs to be setback. Due to the narrow site, the setback depth is taken out of the commercial depth.   |
| 3                    | 23.47A.008.C.4B | Overhead weather protection: continuous overhead weather protection is required along at least 60% of the street frontage on a principal pedestrian street. The covered area shall have a min. Width of 6-ft. The lower edge of the overhead weather protection shall be min 8-ft and max 12-ft above the sidewalk | To allow 4' deep weather protection between 9' and 14'-10" above grade along 100% of the NE 65th façade. This proposal provides continuous weather protection across the building, instead of just at 60% coverage, albeit at a slightly narrower dimension, and slightly higher elevation than code dictates. This weather protection depth matches the streetscape of the Ecoluxe project to the west across 14th Ave.  |
| 4                    | 23.47A.014.E.5  | Structures in setbacks: fences, freestanding walls and other similar structures 6-ft or less in height above existing or finished grade, whichever is lower, are permitted in required setbacks  | To allow an 8' high fence along the south property line and south portion of 14th Ave ne. The 8' high fence provides better privacy and separation between the single family home to south and the units and walkway at grade on the south side of the project.   |

ADMINISTRATIVE DEPARTURES

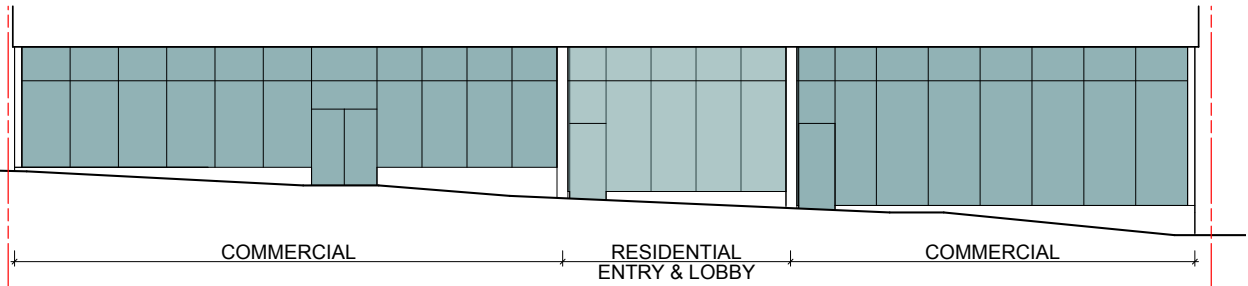
|   |             |   |   |
|---|-------------|---|---|
| 1 | 23.54.040.B | Mixed use developments shall meet the storage space requirements shown in Table A for 23.54.040 for residential development, plus 50% of the requirement for non-residential development. | To allow a 350-sf storage room, smaller than the code-minimum size. This is a Type I decision that is handled administratively. |
|---|-------------|---|---|



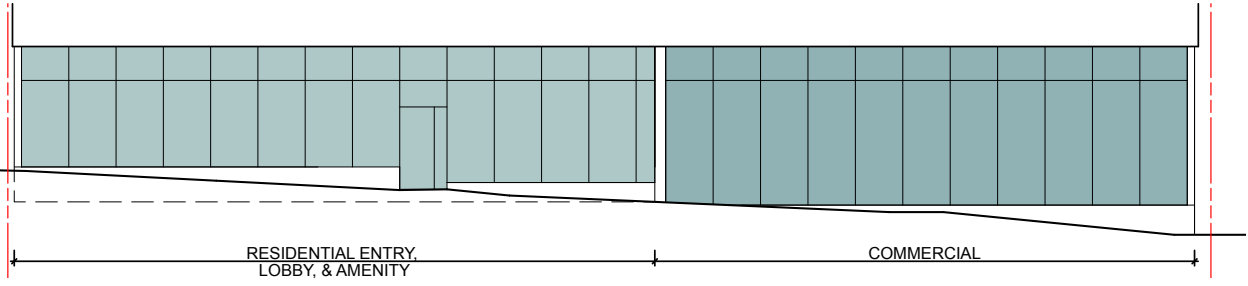


DEPARTURE REQUEST DIAGRAMS

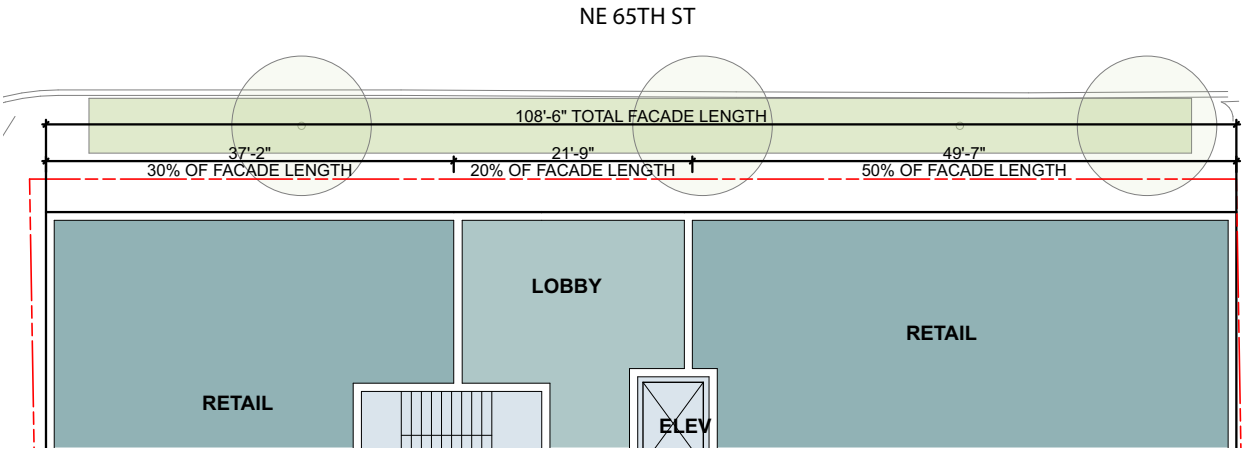
| STANDARD REQUIREMENT |                | REQUEST  | RATIONALE   | DESIGN GUIDELINES   |
|----------------------|----------------|--|---|---|
| 1A                   | 23.47A.005.C.1 | Residential uses may occupy no more than 20% of the street-level street-facing façade on a designated principal pedestrian street  | To allow a larger percentage of residential uses (and consequently smaller percentage of the required uses) at the NE 65th St façade. While the proposed design does not meet the required percentages, the uses are intended to compliment the streetscape. Approximately 50% of the façade will be a commercial space, with large windows and high transparency, while the remaining 50% will be the residential lobby, leasing center and resident amenity. With small residential units, the public spaces like the lobby and business center are expected to be highly-used gathering spaces. From the exterior, these residential amenity spaces will have large windows consistent with the commercial frontage. | <b>PL2 WALKABILITY</b><br>(Safety and Security: Eyes on the Street, Street Level Transparency)<br><br><b>PL3 STREET-LEVEL INTERACTION</b><br>(Retail Edges: Visibility) |
| 1B                   | 23.47A.005.D.1 | Along designated principal pedestrian streets eating and drinking establishments, offices, or sales and services is required along 80% of the street-facing façade.                                    |   |   |
| 1C                   | 23.47A.008.C.1 | In pedestrian-designated zones, a minimum of 80% of the width of a structure's street-level façade facing a principal pedestrian street shall be occupied by uses listed in subsection 23.47a.005.D.1. |   |   |



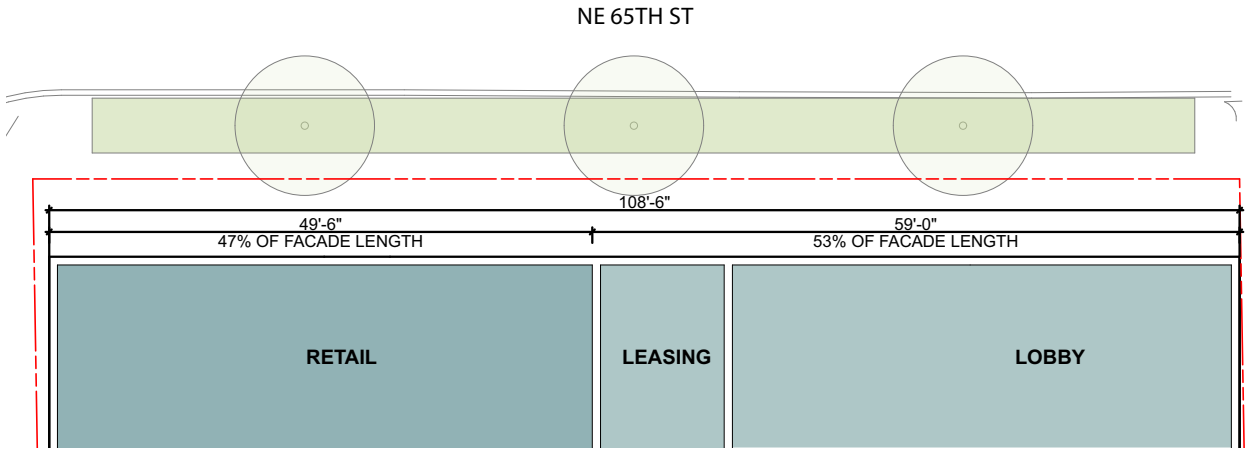
COMPLIANT FIRST FLOOR NE 65TH ST ELEVATION



PREFERRED FIRST FLOOR NE 65TH ST ELEVATION



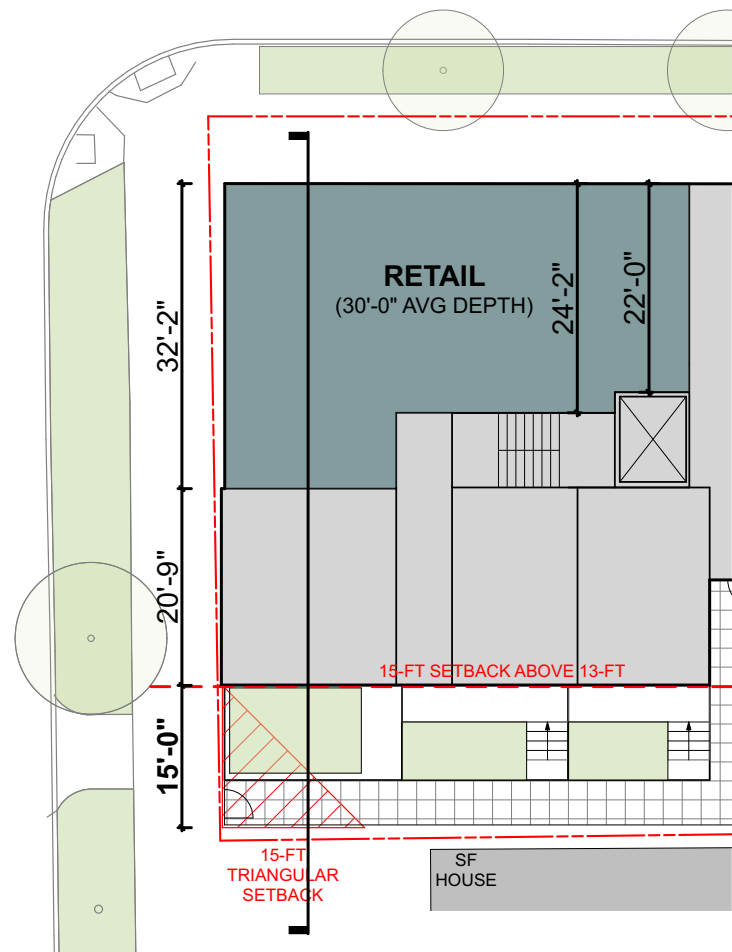
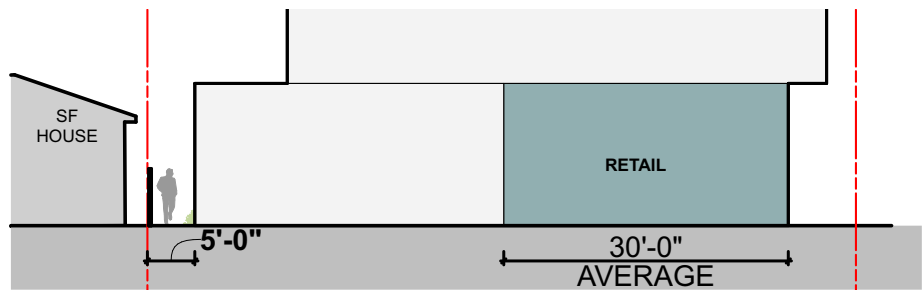
COMPLIANT FIRST FLOOR PLAN



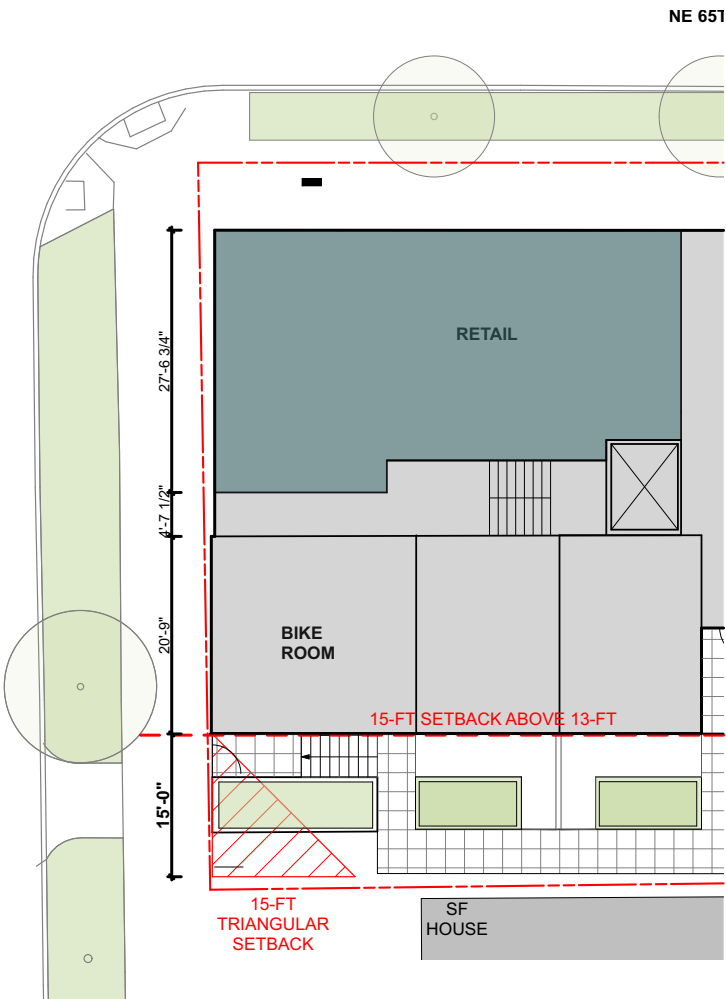
PROPOSED FIRST FLOOR PLAN



| STANDARD REQUIREMENT |                |   | REQUEST   | RATIONALE   | DESIGN GUIDELINES   |
|----------------------|----------------|---|---|---|---|
| 2                    | 23.47A.008.B.3 | Non-residential uses shall extend an average depth of at least 30-ft and a minimum depth of 15-ft from the street-level street-facing façade. | Allow a reduced depth of an average of 26-ft for the commercial space with a 32-ft space for 18-ft of the façade and 22-ft space for the remainder. | The projects aims to enhance the pedestrian environment on NE 65th St, with widened sidewalks, plantings, and weather protection. To match the adjacent project's street frontage, the facade of the first floor needs to be setback. Due to the narrow site, the setback depth is taken out of the commercial depth. | <p><b>PL2 WALKABILITY</b><br/>(Safety and Security: Eyes on the Street, Street Level Transparency)</p> <p><b>PL3 STREET-LEVEL INTERACTION</b><br/>(Retail Edges: Visibility) ...“maximize visibility into building interiors”</p> |



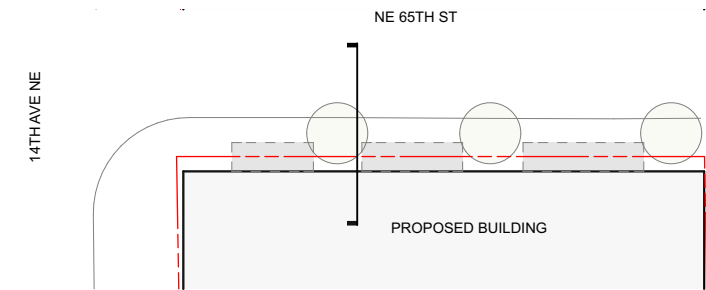
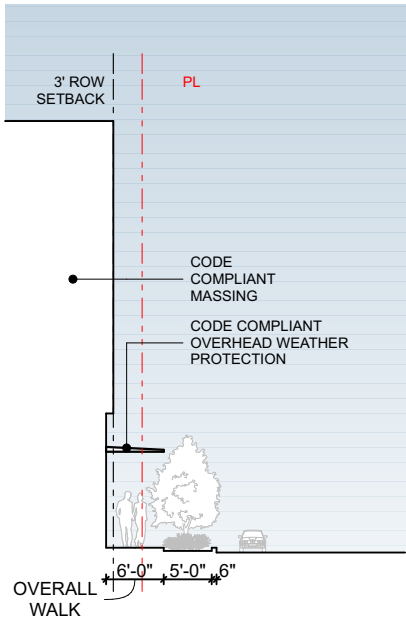
COMPLIANT FIRST FLOOR PLAN & SECTION



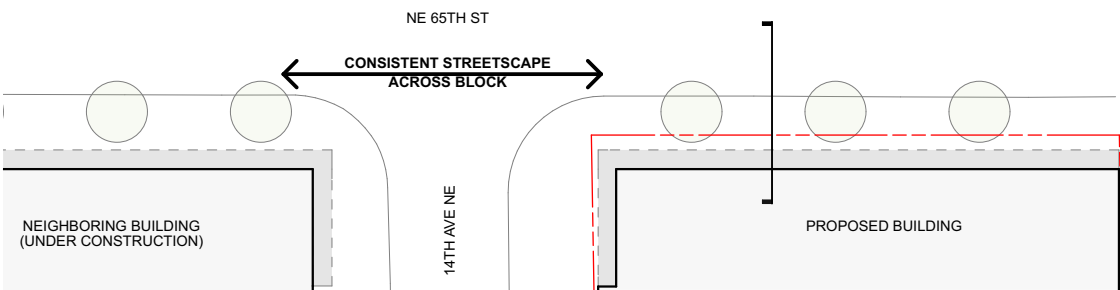
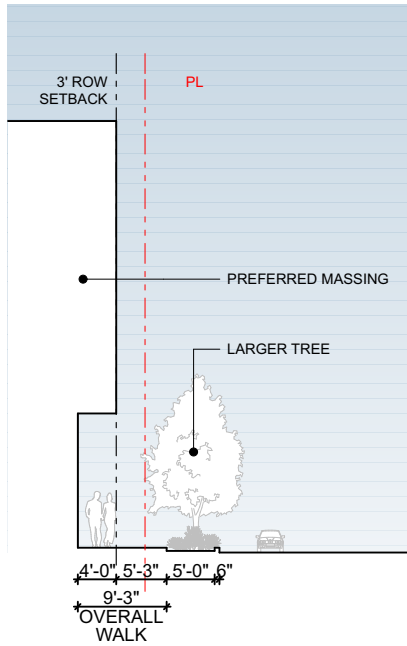
PROPOSED FIRST FLOOR PLAN

DEPARTURE REQUEST DIAGRAMS

| STANDARD REQUIREMENT  | REQUEST  | RATIONALE   | DESIGN GUIDELINES   |
|---|--|---|---|
| <b>3</b> 23.47A.008.C.4B Overhead weather protection: continuous overhead weather protection is required along at least 60% of the street frontage on a principal pedestrian street. The covered area shall have a min. Width of 6-ft. The lower edge of the overhead weather protection shall be min 8-ft and max 12-ft above the sidewalk | To allow 4' deep weather protection between 9' and 14'-10" above grade along 100% of the NE 65th façade. | This proposal provides continuous weather protection across the building, instead of just at 60% coverage, albeit at a slightly narrower dimension, and slightly higher elevation than code dictates. This weather protection depth matches the streetscape of the Ecoluxe project to the west across 14th Ave. | <b>PL1 CONNECTIVITY</b><br>(Walkways and Connections: Pedestrian Volumes)<br><br><b>PL2 WALKABILITY</b><br>(Weather Protection: Locations and Coverage)<br><br><b>Roosevelt I Supplemental Guidance</b><br>(Human Activity along sidewalks) |



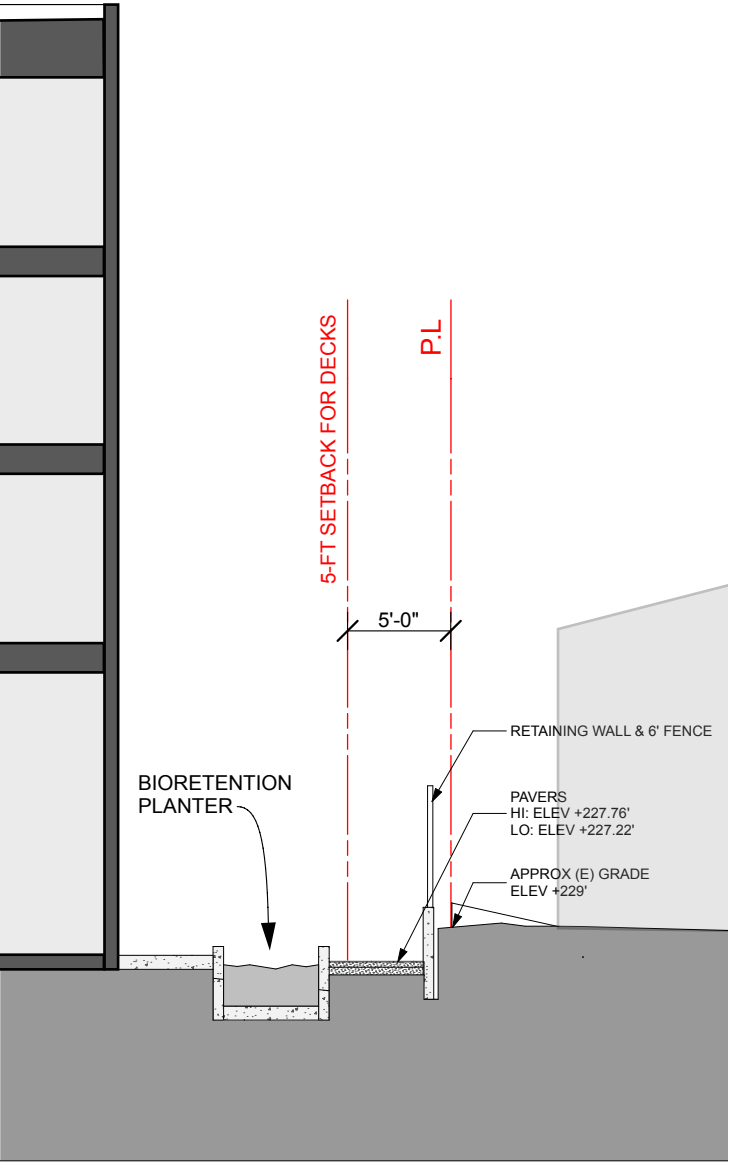
COMPLIANT FIRST FLOOR PLAN & STREET SECTION



PROPOSED FIRST FLOOR PLAN & STREET SECTION



| STANDARD REQUIREMENT   | REQUEST   | RATIONALE   | DESIGN GUIDELINES   |
|--|---|---|---|
| 4 23.47A.014.E.5 Structures in setbacks: fences, freestanding walls and other similar structures 6-ft or less in height above existing or finished grade, whichever is lower, are permitted in required setbacks | To allow an 8' high fence along the south property line and south portion of 14th Ave ne. | The 8' high fence provides better privacy and separation between the single family home to south and the units and walkway at grade on the south side of the project. | <p><b>PL2 WALKABILITY</b><br/>(Safety and Security: Eyes on the Street, Street Level Transparency)</p> <p><b>PL3 STREET-LEVEL INTERACTION</b><br/>(Retail Edges: Visibility) ...“maximize visibility into building interiors”</p> |





## DESIGN GUIDELINES

## DESIGN RESPONSE

### CS2-A Location in the City and Neighborhood

#### CS2-A-1. Sense of Place

Emphasize attributes that give a distinctive sense of place. Design the building and open spaces to enhance areas where a strong identity already exists, and create a sense of place where the physical context is less established.

The proposed project is located on NE 65th St in the Roosevelt neighborhood. While the project is near the Roosevelt commercial core, the surrounding blocks on NE 65th are currently in transition and lack a sense of place due to the changing surroundings, prevalence of undeveloped sites, and narrow pedestrian ROWs with no street trees. The property to the west of the site is currently in construction and will be the first new development to set the tone for a new neighborhood. That project develops a widened sidewalk with street trees in curbside planting beds, as well as continuous weather protection. This proposed project intends to continue that new neighborhood character across the NE 65th St frontage. See response to CS2-I-ii and CS3-A-4 for more detail on the relationship of the proposed project to the neighboring building.

### CS2-B Adjacent Sites, Streets, and Open Spaces

#### CS2-B-2. Connection to the Street

Identify opportunities for the project to make a strong connection to the street and public realm.

The commercial and residential lobby/amenity spaces at level 1 will have a high degree of transparency at the street level with a continuous storefront allowing for a strong connection between the level 1 uses and the streetscape.

### CS2-D Height, Bulk, and Scale

#### CS2-D-3. Zone Transitions

For projects located at the edge of different zones, provide an appropriate transition or complement to the adjacent zone(s). Projects should create a step in perceived height, bulk and scale between the anticipated development potential of the adjacent zone and the proposed development.

The south property line of the site is shared with the side lot of a single-family zone, so a transition in scale is necessary. To respond to the lower height neighbor, the highest mass of the building is located to the north, and the building sets back 15-ft from the southern property line. A 8' high fence and retaining wall is located at this rear yard to create physical separation between unit terraces and the property line, while maintaining a scale that the appropriate to the residential character of the neighboring property.

#### CS2-D-4. Massing Choices

Strive for a successful transition between zones where a project abuts a less intense zone.

The location of program in the building is a response to the zone edge transition. The high-intensity, high-activity program elements are located on the north side of the building, fronting NE 65th St, a commercial active street. A lower-intensity bike room for building residents is located closer to the southern property line on 14th Ave NE. This provides some eyes on the street and activity on 14th Ave NE for safety and security, but will not create loud noise or disruption for neighboring single-family.

#### CS2-D-5. Respect for Adjacent Sites

Respect adjacent properties with design and site planning to minimize disrupting the privacy of residents in adjacent buildings.

The massing of the building is setback 15-ft from the southern property line to respond to the neighboring zone. Additional design measures are proposed to increase the privacy between the new building and the neighboring lot: A 8' high fence and retaining wall is located along the southern edge of the ground-floor thru way to create physical separation between unit terraces and the property line, while maintaining a scale that the appropriate to the residential character of the neighboring property.

### CS2-I Sense of Place

#### CS2-I-ii

Develop a fabric of connected buildings through streetscapes rather than a series of isolated structures.

The building to the west of the proposed project is currently under construction and sets a new streetscape profile and landscape scheme for NE 65th St. The proposed project intends to continue this new standard across its frontage: a widened sidewalk created by a 4-ft street-level setback, new street trees, and continuous overhead weather protection created by the street-level setback. There is a second project at a similar point in design to the west of the project under construction that will also follow this street-level formula for consistency. These three projects are being considered holistically to create a cohesive NE 65th St that have similarities at street level and elevations that complement one another.



| DESIGN GUIDELINES   | DESIGN RESPONSE  |
|---|--|
| <b>CS2-III Height, Bulk and Scale</b>   |  |
| <b>CS2-III-iii. Multi-family/Residential Zone Edges</b><br>Careful siting, building design and building massing should be used to achieve an integrated neighborhood character in multi-family zones. Some of the techniques preferred in Roosevelt include:<br>Increasing building setbacks from the zone edge at ground level;<br>Reducing the bulk of the building's upper floors;<br>Reducing the height of the structure;<br>Use of landscaping or other screening (such as a 5-foot landscape buffer);<br>Modulation of bays;<br>Stepping down the height of structures to 40' – 45' at the zone edge to provide transition to the height of traditional single-family areas; and<br>Minimizing use of blank walls. | The south property line of the site is shared with the side lot of a single-family zone, so a transition in scale is necessary. To respond to the lower height neighbor, the highest mass of the building is located to the north, and the building sets back 15-ft from the southern property line. A 8' high fence and retaining wall is located at this rear yard to create physical separation two projects. A landscaped zone is provided in the rear yard, located between the unit terraces and the neighboring property. |
| <b>CS3-A Emphasizing Positive Neighborhood Attributes</b>   |  |
| <b>CS3-A-4. Evolving Neighborhoods</b><br>In neighborhoods where architectural character is evolving or otherwise in transition, explore ways for new development to establish a positive and desirable context for others to build upon in the future.   | As noted in previous responses to CS2-A-1 and CS2-I-ii, the stretch of NE 65th St surrounding the proposed project is currently in transition, moving from single-family residential to mixed-use multi-family. One building, just to the west of the proposed project, is currently under construction and will set a new streetscape character for NE 65th St. This character will be continued in this proposed project.  |
| <b>PL2-C Weather Protection</b>   |  |
| <b>PL2-C-1. Locations and Coverage</b><br>Overhead weather protection is encouraged and should be located at or near uses that generate pedestrian activity such as entries, retail uses, and transit stops.  | Continuous weather protection is proposed across NE 65th St via a stepped-back street-level facade. The related increase in sidewalk width allows for greater pedestrian activity due to the new light rail station and multiple transit stops on the street. The added trees at NE 65th St also act to soften the transition between the sidewalk and the street, creating a more people-friendly environment.  |
| <b>PL2-C-2. Design Integration</b><br>Integrate weather protection, gutters and downspouts into the design of the structure as a whole, and ensure that it also relates well to neighboring buildings in design, coverage, or other features.   |  |
| <b>PL2-C-3. People-Friendly Spaces</b><br>Create an artful and people-friendly space beneath building.  |  |
| <b>PL3-C Retail Edges</b>   |  |
| <b>PL3-C-1. Porous Edge</b><br>Engage passersby with opportunities to interact visually with the building interior using glazing and transparency. Create multiple entries where possible and make a physical and visual connection between people on the sidewalk and retail activities in the building.   | The commercial and residential lobby/amenity spaces have generous floor-to-floor heights of 15-ft or more and will have a high degree of transparency at the street level.   |
| <b>PL4-A Entry Locations and Relationships</b>  |  |
| <b>PL4-A-1. Serving all Modes of Travel</b><br>Provide safe and convenient access points for all modes of travel.   | The location of the main residential entry on NE 65th St is idea for transit located a few block from the new light rail entry, and steps from bus routes. Bike use is also considered, with bike racks located on NE 65th St for short-term visitors to the commercial space that fronts NE 65th St, and a dedicated bike room for residents directly off 14th Ave NE.  |
| <b>PL4-A-2. Connections to All Modes</b><br>Site the primary entry in a location that logically relates to building uses and clearly connects all major points of access.   |  |

| DESIGN GUIDELINES   | DESIGN RESPONSE   |
|---|---|
| <b>PL4-B Planning Ahead for Bicyclists</b>  |   |
| <b>PL4-B-2. Bike Facilities</b><br>Facilities such as bike racks and storage, bike share stations, shower facilities and lockers for bicyclists should be located to maximize convenience, security, and safety.  | A secure bike room is proposed for the building, with direct access of 14th Ave NE for ease of use by cyclists. The bike room will also have a bike workspace.  |
| <b>DC1-A Arrangement of Interior Uses</b>   |   |
| <b>DC1-A-1. Visibility</b><br>Locate uses and services frequently used by the public in visible or prominent areas, such as at entries or along the street front.   | Along NE 65th St, the commercial space and residential lobby/amenity space have full views to the street and provide added activation to the streetscape for pedestrians. The leasing office is also located on NE 65th St to provide security and eyes on the main residential entry. On 14th Ave NE, a bike room with a bike workspace is located on the facade, with windows for natural light as well as a visual connection to the street.   |
| <b>DC1-A-3. Flexibility</b><br>Build in flexibility so the building can adapt over time to evolving needs, such as the ability to change residential space to commercial space as needed.   | Along NE 65th St, the retail space and the residential lobby/amenity space share a transparent language at the street level. If additional commercial space was needed over time, it would be possible to transform a portion of the lobby/amenity space into additional retail.  |
| <b>DC1-C Parking and Service Uses</b>   |   |
| <b>DC1-C-4. Service Uses</b><br>Locate and design service entries, loading docks, and trash receptacles away from pedestrian areas or to a less visible portion of the site to reduce possible impacts of these facilities on building aesthetics and pedestrian circulation. | Trash pickup will be located in the Alley. Additional service spaces will either be accessed from the alley or a rear yard located at the southern edge of the property.  |
| <b>DC2-B Architectural and Facade Composition</b>   |   |
| <b>DC2-B-1. Façade Composition</b><br>Design all building facades—including alleys and visible roofs— considering the composition and architectural expression of the building as a whole. Ensure that all facades are attractive and well-proportioned.                      | All four facades are designed to a common level of material and detail due to their high visibility from the street and the fact that the layout of the residential floors locates units fronting all four facades. This translates to a similar level of transparency around the building to reflect the uses within. However, there are some specialties to certain facades to tailor them to their surroundings, such as the use of added landscape and trees on the south elevation to create a planted buffer for the zone transition. The roof is considered the fifth facade and has a residential amenity deck with surrounding planting beds and treelets. |



| DESIGN GUIDELINES   | DESIGN RESPONSE  |
|---|--|
| <b>DC2-C Secondary Architectural Features</b>   |  |
| <b>DC2-C-1. Visual Depth and Interest</b><br>Add depth to facades where appropriate by incorporating balconies, canopies, awnings, decks, or other secondary elements into the façade design. Add detailing at the street level in order to create interest for the pedestrian and encourage active street life and window shopping (in retail areas).  | The proposed project mass is comprised of three levels of residential cantilevered over a recessed street-level of glazed storefront. The street-level recess creates built-in weather protection for pedestrians and the high-level of glazing encourages activity. The upper levels floating above the glazed street-level have a woven pattern to create visual interest. Accent panels are emphasized as the primary pattern as they are pulled slightly proud of the rest of the facade. The large windows create a secondary pattern, adding to the woven feel of the overall project. |
| <b>DC2-C-3. Fit With Neighboring Buildings</b><br>Use design elements to achieve a successful fit between a building and its neighbors.   | The proposed project’s street front on NE 65th St shares commonalities with its neighbor to the west across the 14th Ave NE. They both share the same street-level setback with a cantilevered upper three levels. Within these similarities, the two projects differ in patterning, material and changes in plane, but share a similar overall rhythmic scale to create complementary instead of competing facades.   |
| <b>DC2-D Scale and Texture</b>  |  |
| <b>DC2-D-1. Human Scale</b><br>Incorporate architectural features, elements, and details that are of human scale into the building facades, entries, retaining walls, courtyards, and exterior spaces in a manner that is consistent with the overall architectural concept.  | The human scale is considered at the NE 65th St elevation, from the rhythm of new trees to the profile of the street-level setback that carves out a pedestrian realm. Particular focus to incorporating architectural features with a fine grain or human scale will be paid to building and retail entries, the rear yard landscaping. See response to DC2-C for additional information regarding accent materials and overall composition.  |
| <b>DC2-D-2. Texture</b><br>Design the character of the building, as expressed in the form, scale, and materials, to strive for a fine-grained scale, or “texture,” particularly at the street level and other areas where pedestrians predominate.  |  |
| <b>DC2-E Form and Function</b>  |  |
| <b>DC2-E-1. Legibility and Flexibility</b><br>Strive for a balance between building use legibility and flexibility. Design buildings such that their primary functions and uses can be readily determined from the exterior, making the building easy to access and understand. At the same time, design flexibility into the building so that it may remain useful over time even as specific programmatic needs evolve. | The proposed project mass is comprised of three levels of residential cantilevered over a recessed street-level of glazed storefront. This allows for a clear separation between the more private residential nature of the upper floors and the more public retail and residential lobby/amenity spaces at grade. The retail and residential lobby/amenity spaces maintain a consistent language to provide flexibility.  |

| DESIGN GUIDELINES  | DESIGN RESPONSE   |
|--|---|
| <b>DC2-II Architectural and Façade Composition</b>   |   |
| <b>DC2-II-i. Along Major Arterials</b><br>Maximize the retail and street-level transparency (commercial zones);<br>Maximize the quality of exterior finish, especially at the base;<br>Incorporate a series of storefronts along the commercial street frontages.                              | The street level transparency along NE 65th St is maximized because appropriate functions (retail space, residential lobby and amenity) have been located at street level. This creates a strong connection between the interior uses and the streetscape.  |
| <b>DC4-A Exterior Elements and Finishes</b>  |   |
| <b>DC4-A-1. Exterior Finish Materials</b><br>Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged.             | The building exterior will be constructed of durable and maintainable materials. Special attention will be focused on the level one storefront, which will be perceived by the greatest number of passersby and pedestrians, and the accent panels at the upper levels, which are emphasized as the primary architectural pattern. High quality details will be used throughout the project.  |
| <b>DC4-A-2. Climate Appropriateness</b><br>Select durable and attractive materials that will age well in Seattle’s climate, taking special care to detail corners, edges, and transitions.   |   |
| <b>DC4-C Lighting</b>  |   |
| <b>DC4-C-2. Avoiding Glare</b><br>Design project lighting based upon the uses on and off site, taking care to provide illumination to serve building needs while avoiding off-site night glare and light pollution.  | The proposed design will provide overhead lighting under the overhead weather protection along NE 65th St and NE 14th Ave where this type of lighting is most appropriate. Path lighting will be provided at the rear yard for security and safety. Care will be taken to minimize light pollution impacts at the single-family residential property to the south.  |
| <b>DC4-D Trees, Landscape, and Hardscape Materials</b>   |   |
| <b>DC4-D-1. Choice of Plant Materials</b><br>Reinforce the overall architectural and open space design concepts through the selection of landscape materials.  | The landscape materials include street trees, extended planting strips, and a more lushly landscaped rear yard to provide an inviting neighborhood setting.   |
| <b>DC4-D-2. Hardscape Materials</b><br>Use exterior courtyards, plazas, and other hard surfaced areas as an opportunity to add color, texture, and/or pattern and enliven public areas through the use of distinctive and durable paving materials. Use permeable materials wherever possible. | The rear yard of the project will incorporate permeable pavers as feasible to create the exterior terraces at the back units. Additional landscaping will be provided between the building and the adjacent neighbor to the south.  |
| <b>DC4-D-3. Long Range Planning</b><br>Select plants that upon maturity will be of appropriate size, scale, and shape to contribute to the site as intended.   | All plants have been reviewed to provide an immediate impact, but not overwhelm the site over time. In particular, smaller street trees were selected for the north, while larger trees have been chosen for the west, where the sidewalk space is more significant.  |
| <b>DC4-D-4. Place Making</b><br>Create a landscape design that helps define spaces with significant elements such as trees.  | Along NE 65th St, this project seeks to develop a widened sidewalk with street trees in curbside planting beds, as well as continuous weather protection. This is consistent with the building to the west of the proposed project that is currently under construction and sets a new streetscape profile and landscape scheme for NE 65th St. There is a second project at a similar point in design to the west of the project under construction that will also follow this street-level formula for consistency. These three projects are being considered holistically to create a cohesive NE 65th St that have similarities at street level and elevations that complement one another. |







# REPRESENTATIVE PROJECTS

Weinstein A+U is recognized as one of the Northwest’s leading design firms and has continually demonstrated design excellence on a broad array of projects for State, City, Federal, private, and not-for-profit clients. We are passionate about our city and the shaping of its urban neighborhoods through the integration of architecture and urban design is central to our practice.

Well-designed and thoughtful urban housing is a special concern of ours, and we have worked aggressively to advance the expectations of mixed-use projects in Seattle, both technically and aesthetically. While each project presents very specific challenges, a number of recurring themes inform much of our work and form the basis of our approach to housing design. The projects shown here illustrate successfully executed strategies to be pursued on the proposed building.

- At 19th and Mercer in Capitol Hill, recessing the street level facade back from the main mass of the building above created a generous streetscape scaled to pedestrians.
- At the Rooster in Roosevelt, full-height windows maximized natural light in the units and created a lighter, delicate quality to the larger building.
- At Compass Housing in Ballard, the facade patterning considered and integrated the unit venting strategy to minimize the visual impact.
- At Agnes Lofts in Capitol Hill, measured variation to the window patterning created visual interest within a simple material palette.



**1 The Rooster Mixed-Use Building,**  
900 NE 65<sup>th</sup> Street

**2 Compass Center Housing,**  
1753 NW 56<sup>th</sup> Street

**3 19th and Mercer Mixed-Use Building,**  
526 19<sup>th</sup> Avenue E

**4 Agnes Lofts,** 1433 12<sup>th</sup> Avenue





|                        |  |  |
|------------------------|--|--|
| PARCEL NO              | 1797500086 (west parcel) ; 1797500085 (east parcel)  |  |
| LOT AREA               | 4,182 SF (west parcel) ; 4,144 SF (east parcel) = 8,326 SF Total   |  |
| ZONING                 | NC2P-40 (1.2) (west parcel) ; NC2P-40 (1.2) (east parcel)<br>Roosevelt Residential Urban Village, Frequent Transit Corridor, Roosevelt Station Overlay District  |  |
| PERMITTED USES         | <ul style="list-style-type: none"><li>• Most commercial uses permitted up to 25,000-SF</li><li>• Multi-purpose retail sales facility permitted up to 50,000-SF</li><li>• Residential uses permitted outright</li></ul>   | 23.47A.004, Table A  |
| STREET LEVEL STANDARDS | <ul style="list-style-type: none"><li>• Non-residential street level frontage to comply with transparency &amp; blank façade provisions.</li><li>• Required average depth of street level nonresidential spaces 30-ft (15-ft min)</li><li>• Street level nonresidential required to have 13-ft floor to floor height.</li><li>• Along designated principal pedestrian streets, one or more of the following is required along 80% of street-facing façade: general sales, retail, eating and drinking establishments.</li></ul>  | 23.47A.008<br><br>23.47A.005.C<br>23.47A.005.D1 (NE 65th is a principal pedestrian street) |
| STRUCTURE HEIGHT       | <ul style="list-style-type: none"><li>• 40-ft as zoned.</li><li>• Height of a structure may exceed the otherwise applicable limit by up to 4' provided a floor-to-floor height of 13' or more is provided for nonresidential uses at street level.</li><li>• Open railings, planters, parapets, etc permitted up to 4-ft above height limit.</li><li>• Solar collectors, mechanical equipment, stair &amp; elevator penthouses allowed to extend up to 15-ft above height limit, provided the combined total coverage of all features gaining additional height does not exceed 20% of the roof area, or 25% of the roof area if the total includes stair and elevator penthouses or screened mechanical equipment.</li><li>• Stair and elevator penthouses may extend above the applicable height limit up to 16-ft</li></ul> | 23.47A.012   |
| FAR                    | <ul style="list-style-type: none"><li>• 4 for lots with 40-ft height limit with a mixed-use building. Both lots have incentive suffixes:<ul style="list-style-type: none"><li>• west parcel: (1.2 incentive), 4 Max = 4 x 4,182-SF lot area</li><li>• east parcel: (1.2 incentive), 4 Max = 4 x 4,144-SF lot area</li><li>• Combined maximum allowable area: 33,304-SF, including all incentive provisions</li></ul></li></ul>   | 23.47A.013   |
| SETBACKS               | <ul style="list-style-type: none"><li>• A setback is required where a lot abuts the intersection of a side lot line and a front lot line of a lot in a residential zone. The required setback forms a triangular area 15-ft on a side</li><li>• For a structure containing a residential use, a setback is required along any side or rear lot line that abuts a lot in a residential zone, as follows: 15-ft for portions of structures above 13' in height to a maximum of 40-ft - for each portion above 40-ft in height additional setback at the rate of 2' of setback for every 10' by which the height of such portion exceeds 40-ft</li><li>• Structures in setbacks: Decks with open railings may extend into the required setback, but are not permitted within 5-ft of a lot line in a residential zone.</li></ul>  | 23.47A.014   |
| PARKING QUANTITY       | <ul style="list-style-type: none"><li>• Vehicular: No vehicular parking required within urban village / station area overlay district</li><li>• Bicycle:<ul style="list-style-type: none"><li>• Eating and Drinking Establishments/Sales and Services:<ul style="list-style-type: none"><li>• 1 per 12,000 SF long-term</li><li>• 1 per 2,000 SF short-term</li></ul></li><li>• Multifamily Structures:<ul style="list-style-type: none"><li>• None required short-term</li><li>• 0.75 per dwelling small efficiency dwelling unit (SEDU) long-term</li></ul></li></ul></li></ul>  | 23.54.015  |
| AMENITY AREA           | <ul style="list-style-type: none"><li>• Amenity spaces equivalent to 5% of residential gross floor area required for residential uses<ul style="list-style-type: none"><li>• Amenity Spaces shall not be enclosed.</li><li>• Common amenity area: min area 250-sf; 10-ft min horizontal dimension</li><li>• Private balconies: min area 60-sf; 6-ft min horizontal dimension</li></ul></li></ul>   | 23.47A.024   |
| PARKING ACCESS         | <ul style="list-style-type: none"><li>• Access to parking shall be from the alley if the lot abuts an alley improved to the standards (paved and min. 12' width)</li></ul>   | 23.47A.032 (Lot abuts a paved alley)   |
| PARKING STANDARDS      | <ul style="list-style-type: none"><li>• Driveway Slope: No portion of a driveway... shall exceed a slope of 15%</li><li>• Backing Distances: Ingress to and egress from all parking spaces shall be provided without requiring backing more than 50-ft.</li></ul>  | 23.54.030  |



# EXISTING SITE PLAN

## Location

The proposed project is in Seattle's Roosevelt Residential Urban Village on the SE corner of the intersection of NE 65th St and 14th Ave NE.

## Existing Uses

Two single-family houses are located on the site.

## Legal Description:

Lot 17 and 18, Block 1, Cowen's Univeristy Park, According to the plat thereof recorded in plot 13 of plats, page 53. In King County Washington.

## Existing Site Conditions

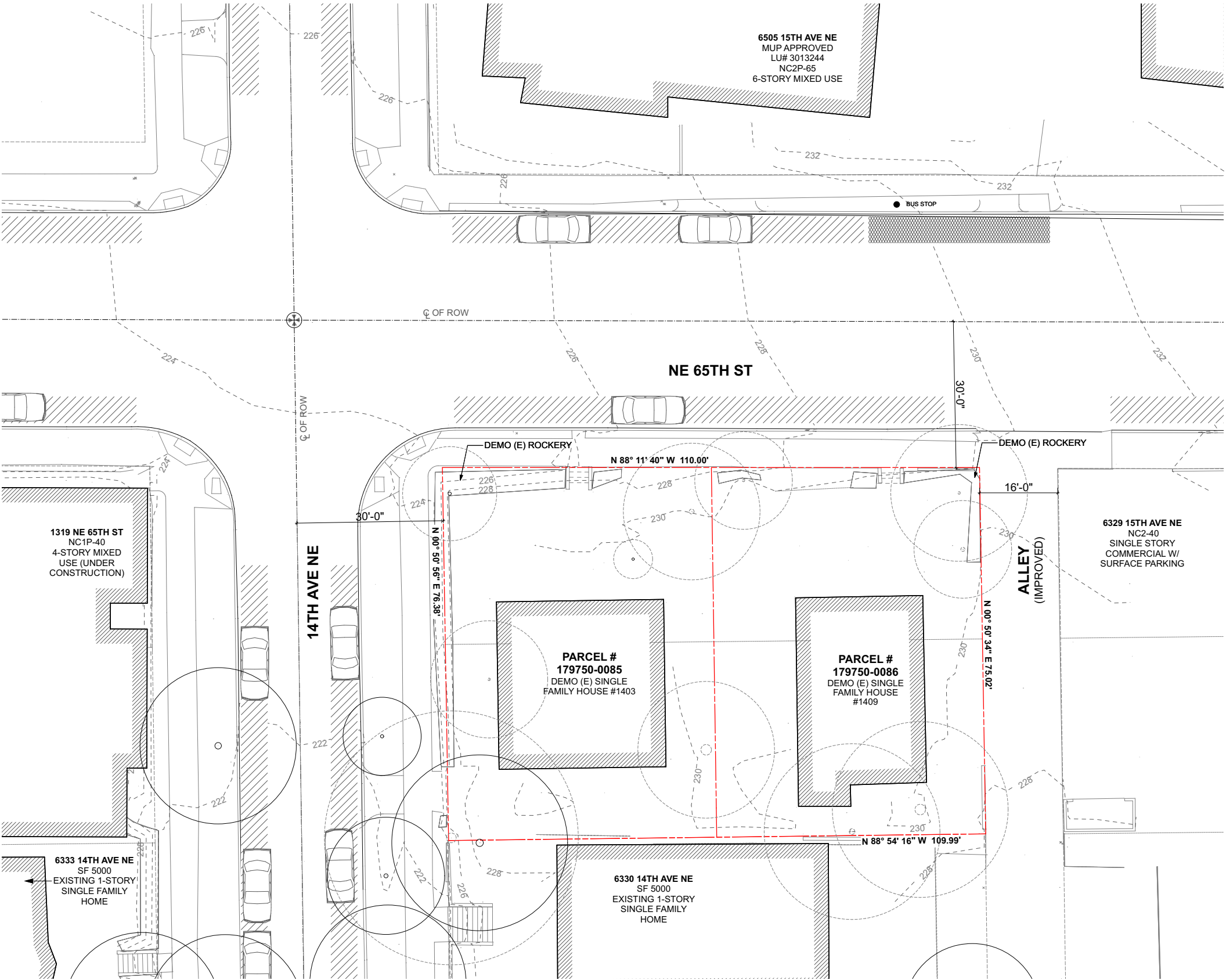
The proposed project site's two parcels total 8,326 SF. The site fronts NE 65th St to the north, 14th Ave NE to the west, a paved alley at the east, and the side lot line of a single-family property to the south.

The NE 65th St frontage is 108-8' in length. The pedestrian rights-of-way width is +7'-6" from back of curb, inclusive of a narrow 1'-6" planter. There no street trees along this portion of NE 65th, and overhead high-voltage power lines are located on the opposite side of the street. Sidewalk curb ramps are present at the corner, but no painted crosswalks are provided across 14th Ave NE or NE 65th St.

The 14th Ave NE frontage is 76' in length. The pedestrian right-of-way width is +17'-6" from back of curb, and characterized by a wide planter and street trees on the street side, and a narrow planter on the property side. Parking access to properties is provided from the alley, limiting curb cuts on 14th.

Rockeries at the property lines on both street fronts raise the grade of the site within the property boundaries to 2-3 feet above the right-of-way. The property generally slopes to the north-east at the right of way. The lowest point is at 65th and 14th Ave NE the NW corner of the site. Along 65th the grade changes from +230.3' at 14 Ave NE to 224.2' at the alley (2.7 % slope). Along 14th Ave NE the grade drops to + 223.0' (0.5%) at the south property line and along the ally it drops to +227.7' (0.9%).

The two existing structures on the site located on the rockery berms are accessed by stairs from street level, a condition that continues at the single-family homes south of the property on 14th Ave NE. There are 11 existing trees on the parcels, 9 of which are of greater than 6" caliper. None are exceptional trees.



SITE CONTEXT: TRANSIT MODES

The project site is well served by several bus lines located within a 5-minute walk radius. King County Metro bus routes run past the site along NE 65th Street:

- 48 - Ballard, Capitol Hill, the Central Area, Mount Baker
- 62 - Hawthorne Hills, Queen Anne, Downtown
- 64 - Jackson Park, UW
- 73 - Jackson Park, UW, Downtown
- 76 - Wedgwood, Hawthorne Hills

Travel times to the University, Capitol Hill and Downtown will greatly improve with the opening of Sound Transit light rail service to Roosevelt Station located just over two blocks to the west. The relative flatness of NE 65th Street provides an easy walk to the future light rail station as well as bike routes throughout the region. Roosevelt’s commercial core is within a 5-minute walk to the west as is Cowen Park. Roosevelt High School is a block north.

- Design Cues:
- Make NE 65th pedestrian-friendly
  - Consider pedestrians access the Light Rail
  - Provide amenities for pedestrians accessing transit modes (bus, rail)

Legend

**Main Car Arterials:**

Principal Arterial / Regional Connector

Minor Arterial / Commercial Connector

Future Light Rail Station (2021)

Gateway Intersection per Roosevelt Neighborhood Guideline

Frequent Transit Corridors

Pedestrian Overlay

5 Minute Pedestrian Walkshed

Bus Routes

Bus Lines

Current Bicycle Routes

Proposed Bicycle Routes per Seattle bicycle Master Plan

Proposed Green Street per Roosevelt Streetscape Concept Plan

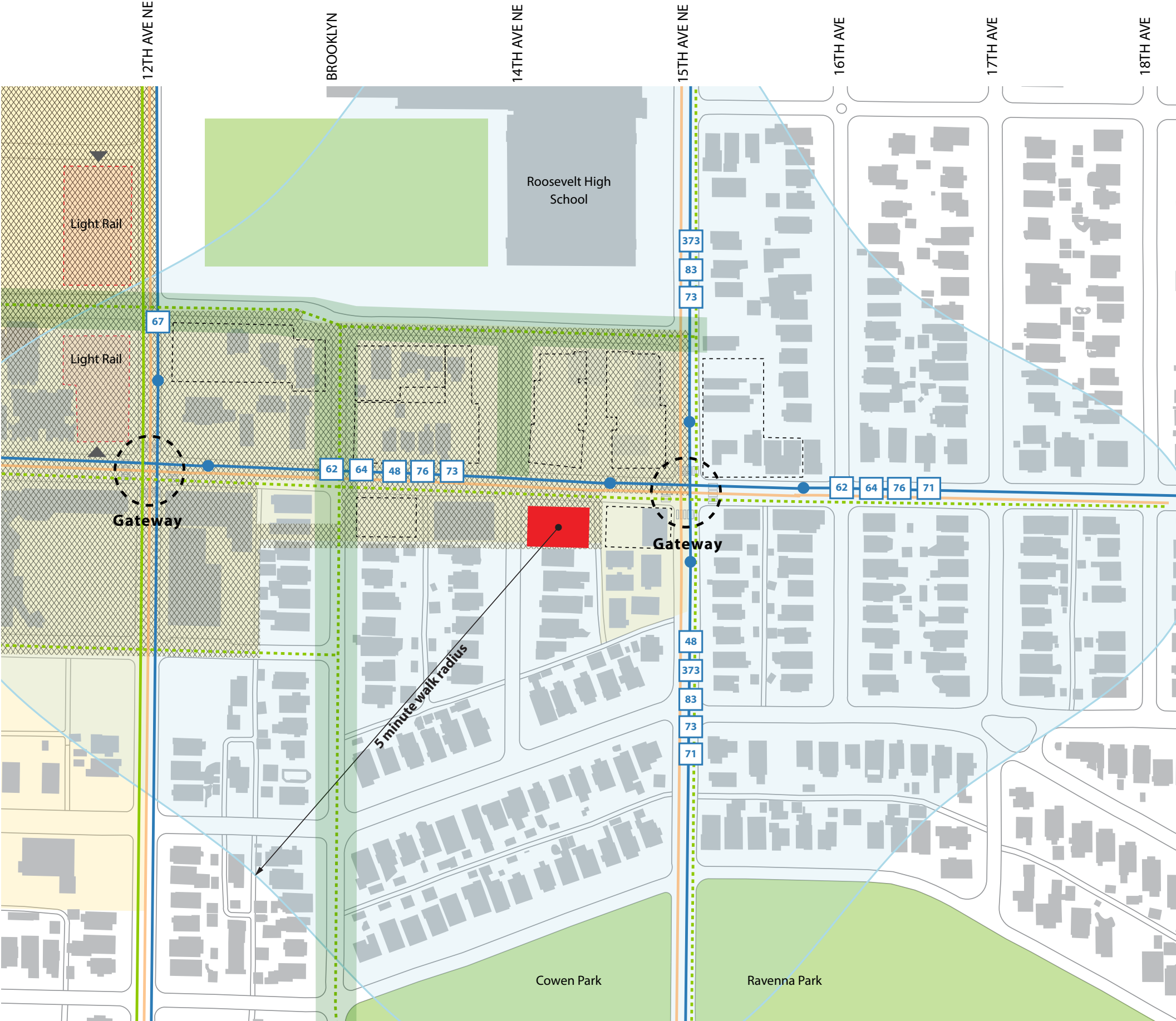
NE 67TH ST

NE 66TH ST

NE 65TH ST

NE 64TH ST

NE 63RD ST







## SITE CONTEXT: LAND USE

NE 68TH ST

The site is bounded by public rights-of-way to the north (NE 65th St), west (14th Ave NE), and east (alley). The south property line abuts a single-family home's side-lot line.

The neighborhood directly south is single-family residential. The surrounding neighborhood to the north, east, and west of the site is a mix of single-family homes, low-rise density retail with surface parking, and undeveloped lots. The Roosevelt Neighborhood's Core Commercial Area begins 2 blocks west of the site, and is characterized by higher density multi-family housing and retail. The new Roosevelt Light Rail Station will be located just over two blocks west of the site on NE 65th St.

Two blocks north of the site is Roosevelt High School, and two blocks south of the Site are Cowen and Ravenna Parks.

### Design Cues:

- Neighborhood is in transition due to recent upzoning and incoming Light Rail
- Scale transition to south single-family neighborhood

NE 65TH ST

NE 63RD ST

### Legend

Future Light Rail Station (2021)

Future Development

Single Family Residential

Multi Family Residential

Mixed Use

Retail

Office

Surface Parking

School / Institutional

Utility

Parks/Open Space

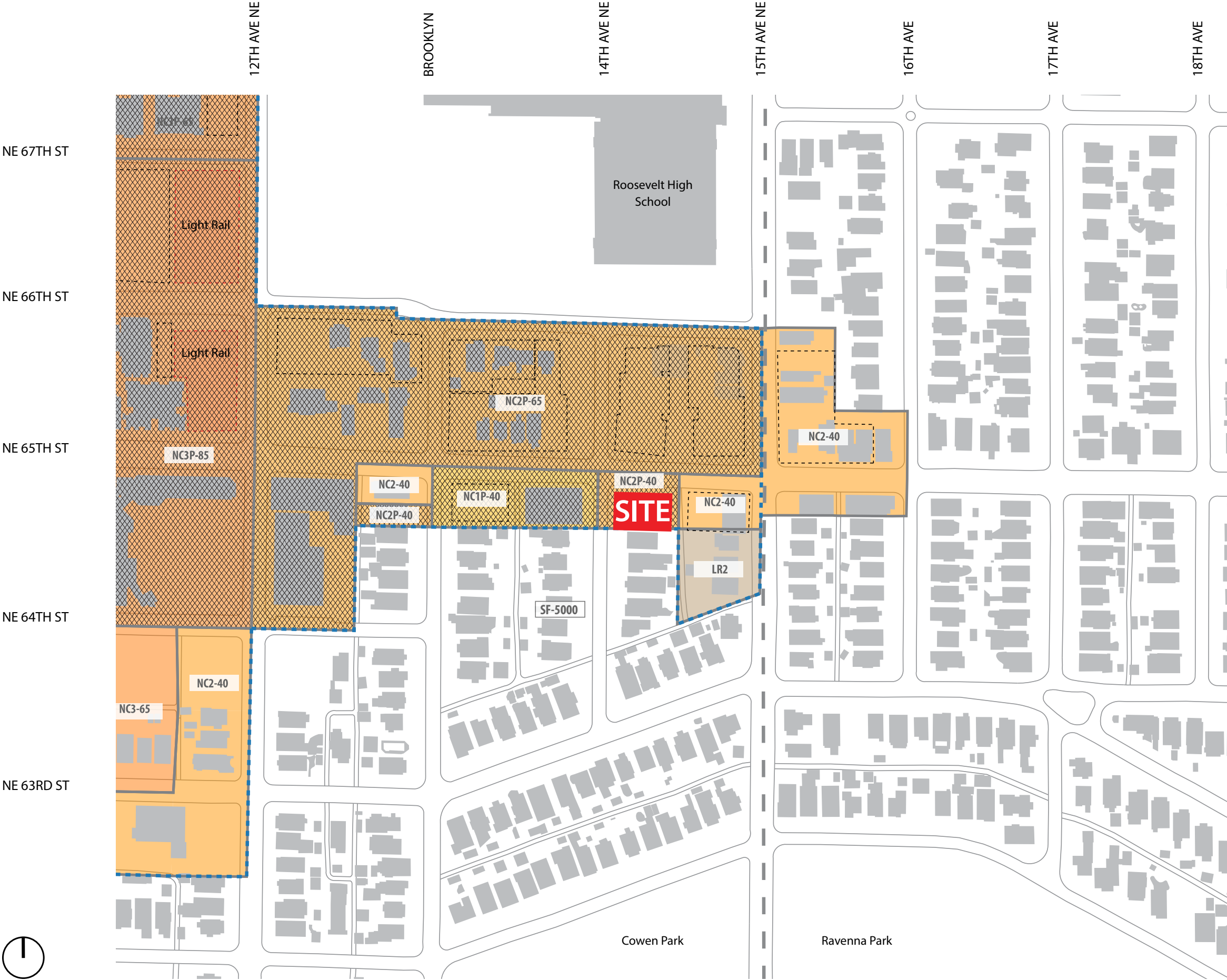
Roosevelt Commercial Core

CONTEXT: EXISTING ZONING

The site is zoned NC2P-40. The site is bounded by public rights-of-way to the north (NE 65th St), west (14th Ave NE), and east (alley). Properties beyond the rights-of-way in these three directions are NC1P and NC2P zones. The south property line abuts the side lot line of a SF 5000 zone.

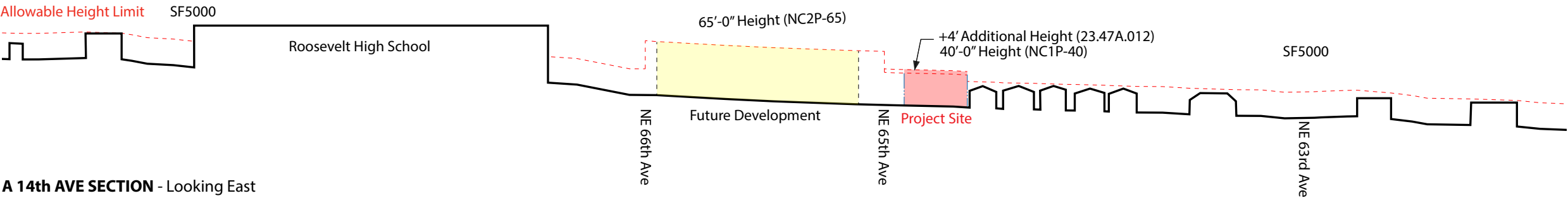
Existing Zoning Legend

- NC1
- NC2
- NC3
- LR1 & LR2
- SF
- Roosevelt Station Area Overlay
- Roosevelt Residential Urban Village
- Zone Boundaries
- Pedestrian Overlay

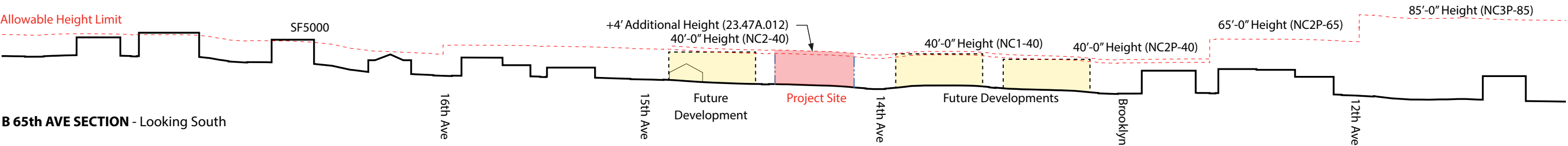




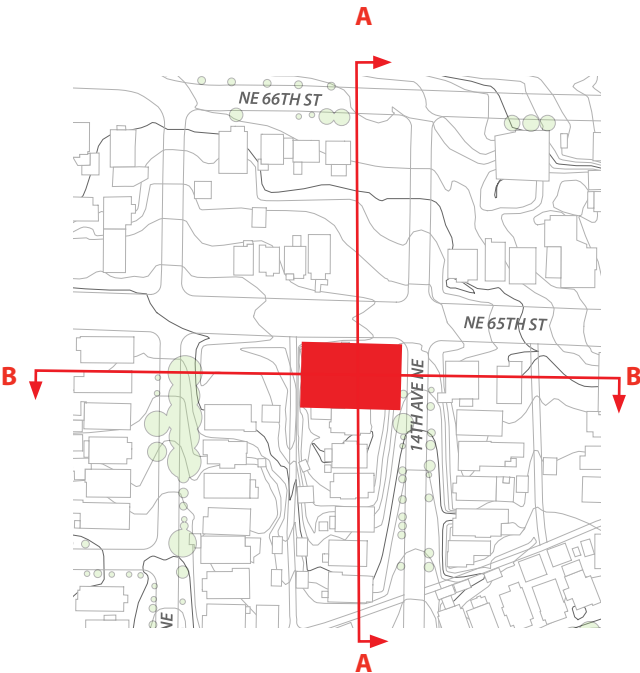
CONTEXT: ZONING ENVELOPE



A 14th AVE SECTION - Looking East



B 65th AVE SECTION - Looking South



CONTEXT: FUTURE DEVELOPMENT ZONING ENVELOPES

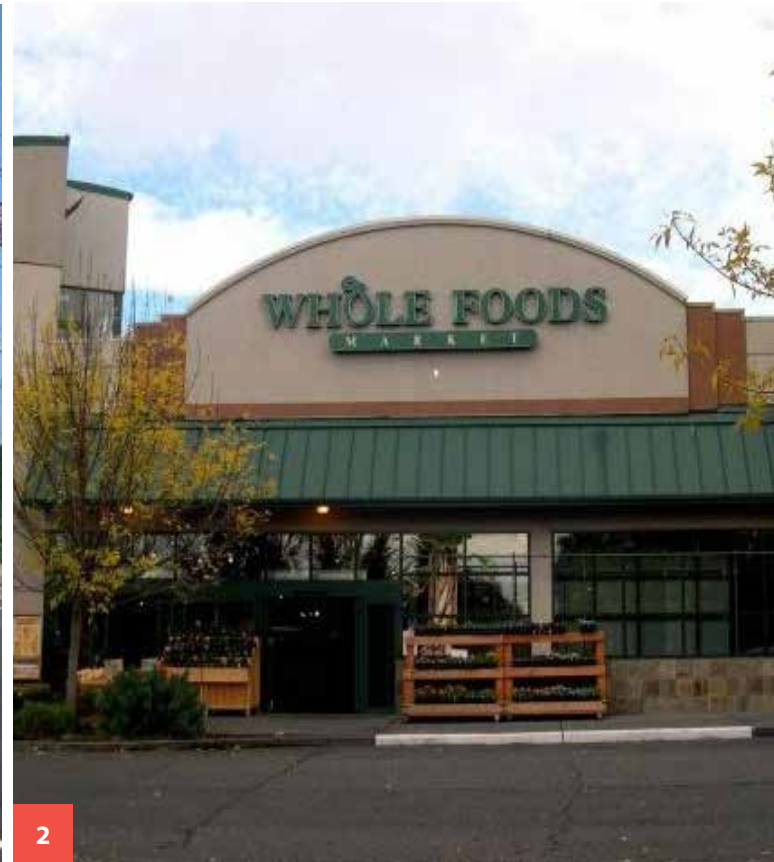




## CONTEXT: SURROUNDING NEIGHBORHOOD



1



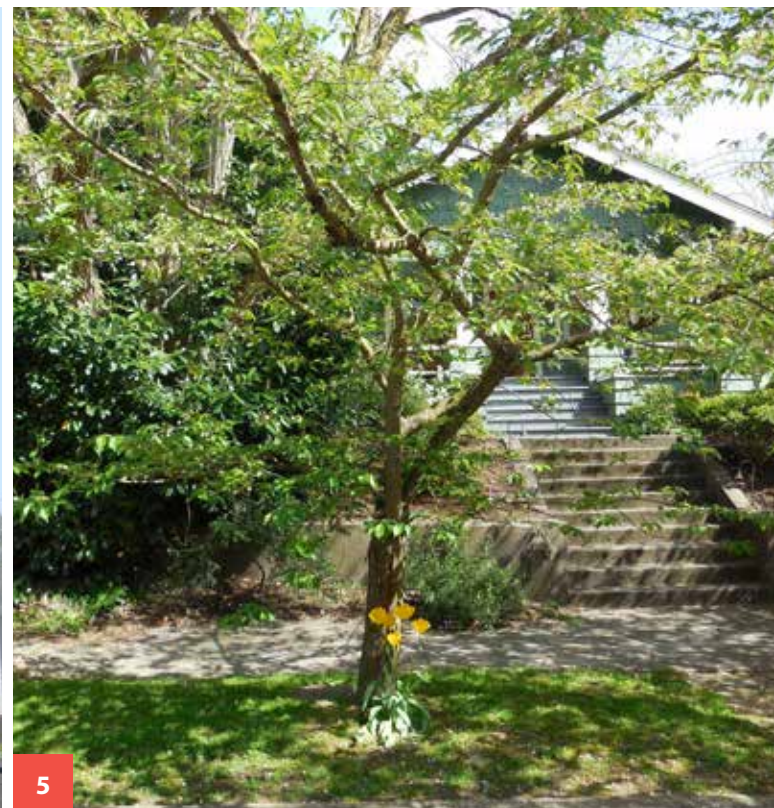
2



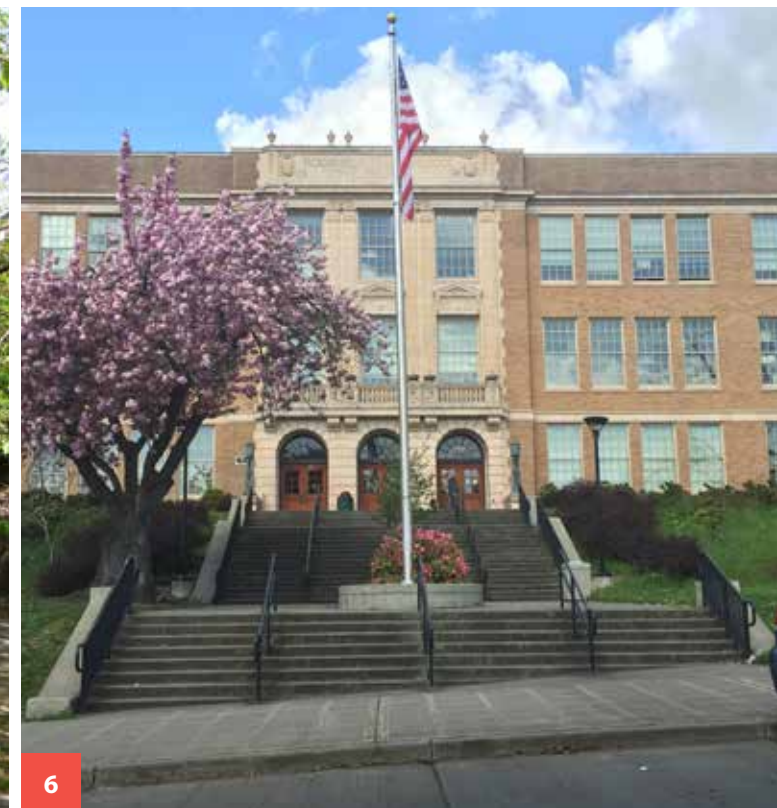
3



4



5



6

The neighborhood surrounding the site is in transition, due in large part to the Link light rail station that will soon open at the corner of NE 65th and 12th Ave NE. Currently, multiple lots are boarded up or empty, awaiting redevelopment. Some change has already begun - two neighboring projects, one directly west of the project site, and one north across NE 65th St have issued MUPs for new mixed-use mid-rise and high-rise projects (see zoning envelope diagram on facing page). The existing commercial streetscape lacks street wall continuity, as building setbacks and building siting on the lots vary, with some commercial structures separated from the street by surface parking.

The eastern edge of the Roosevelt Neighborhood Commercial Core is a block west of the site, where there are more retail and mixed-use properties, pedestrian amenities, and taller buildings. With the incoming light rail and current zoning allowances, the blocks surrounding the site could develop as an extension of the commercial core. The Roosevelt Design Guidelines acknowledge this extension, with the identification of two neighborhood "gateway" intersections on 65th: one at the 12th Ave NE intersection and one at 15th Ave. Further development on the blocks surrounding the site would enhance the pedestrian connection between these two gateways.

In contrast, the residential areas of the Roosevelt neighborhood, such as south of the proposed project site, are well-established single-family blocks. Houses and yards are maintained, and the building style is cohesive with pitched roofs with eaves, and raised porches.

Roosevelt High School, the largest high school in the city, is located a block north of the site. Two parks, Cowen and Ravenna, are located a few blocks south.

- 1 Link Light Rail Roosevelt Station.** Expected opening in 2021, located 1-1/2 blocks from the site.
- 2 Whole Foods Market at SE corner of 64th and 12th.** In commercial core.
- 3 Rising Sun Produce at SW corner of 65th and 15th.**
- 4 Properties on NE 65th St Awaiting Redevelopment.**
- 5 Typical Residential.** South of the site is a well-established single-family residential zone.
- 6 Roosevelt High School.** The school is located a block north of the site



CONTEXT: NE 65TH AVE

The northern edge of the property is bounded by NE 65th Street. The topography along the property line has a minimal slope (under 2%) up towards the east. The sidewalk along this stretch of 65th is narrow (roughly 7 ½-feet), inclusive of a thin, planted strip at the curb. There are no street trees on either side within the neighboring blocks. Both sides of the 65th are zoned for NC, with a 65-ft height limit at the north side, and a 40-ft height limit on the south, though current property uses do not reflect this potential. Currently, 65th is a mix of single-family houses, undeveloped lots, and low-intensity commercial, often with surface parking. Across the alley on 65th is a produce stand and adjacent parking lot (see image 1 to right).

The Roosevelt Commercial Core boundary edge is two blocks west, and this area of higher density retail and mixed-use is a short walk from the site.

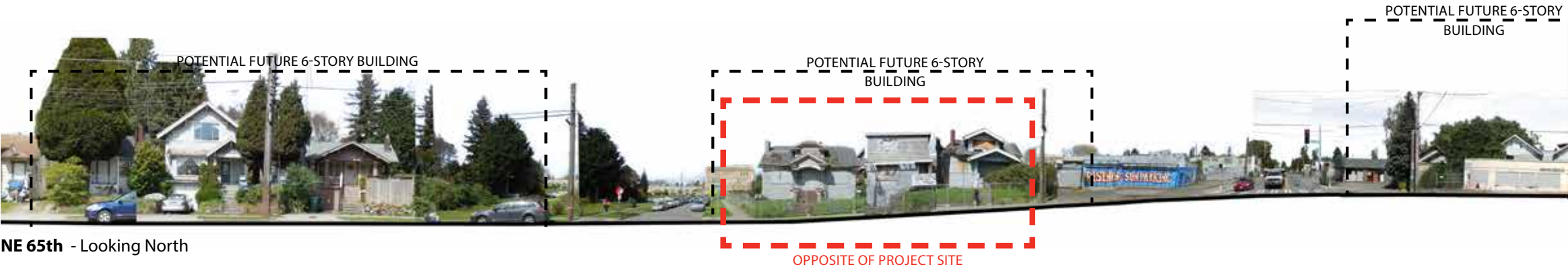
The future Roosevelt Link Light Rail Station, set to open in 2020, is two and a half blocks west of the site on the opposite side of the street.

Design Cues:

- Extend new streetscape profile (widened sidewalk, weather protection, trees) from neighbor under construction for consistent streetwall
- Locate major entries and active programs on NE 65th St



NE 65th - Looking South



NE 65th - Looking North





CONTEXT: 14TH AVE NE



14th AVE NE - Looking East

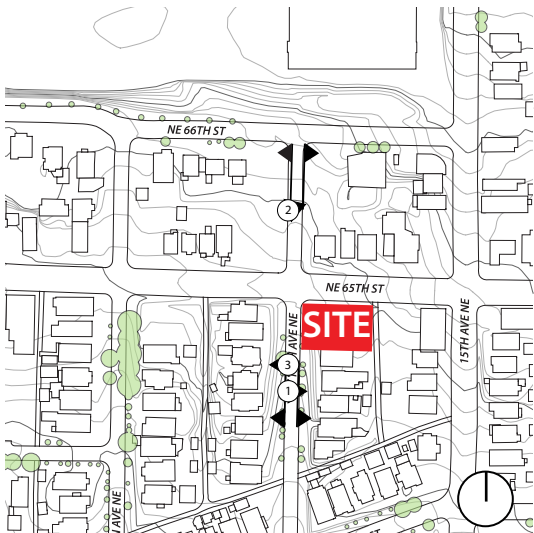


14th AVE NE - Looking West

The eastern edge of the property is bounded by 14th Ave NE. The topography along the property line has a gentle slope (under 3%) towards the north. 14th is a quiet street characterized by single-family residential houses, typically set back from the property line and raised above the street level by landscaped rockeries or retaining walls. Street trees are well established and set within wide planting beds. Resident-only parking is provided on both sides of the street. One block north of the site, across NE 65th, both sides of the street are awaiting new development (empty lots and boarded houses), and at the end of that block 14th dead-ends into the Roosevelt High School campus.

Design Cues:

- Provide massing transition to single-family zone
- Use landscaping to blend with street character
- Respect privacy of neighbors with first-floor program layout and access doors





# CONTEXT: EXISTING SITE

The site is currently occupied by two small single-family structures, raised approximately 4-7' above the street level grades by a continuous rockery at the property lines facing NE 65th St and 14th Ave NE. No curb cuts exist on 65th, 14th has one cut at the existing detached garage adjacent to the south property line. Power lines for all the single-family houses on the block run overhead in the adjacent alley. The power lines are currently 2 phase and will need to upgraded to 3 phase to accommodate the project. There are eleven existing trees on the site nine of which are greater than 6" caliper. None are identified as exceptional trees by an arborist.

The sidewalk on NE 65th St, as mentioned previously, is narrow and lacks street trees. This width, coupled with the heavy vehicular use on NE 65th leads to an unsafe pedestrian condition.

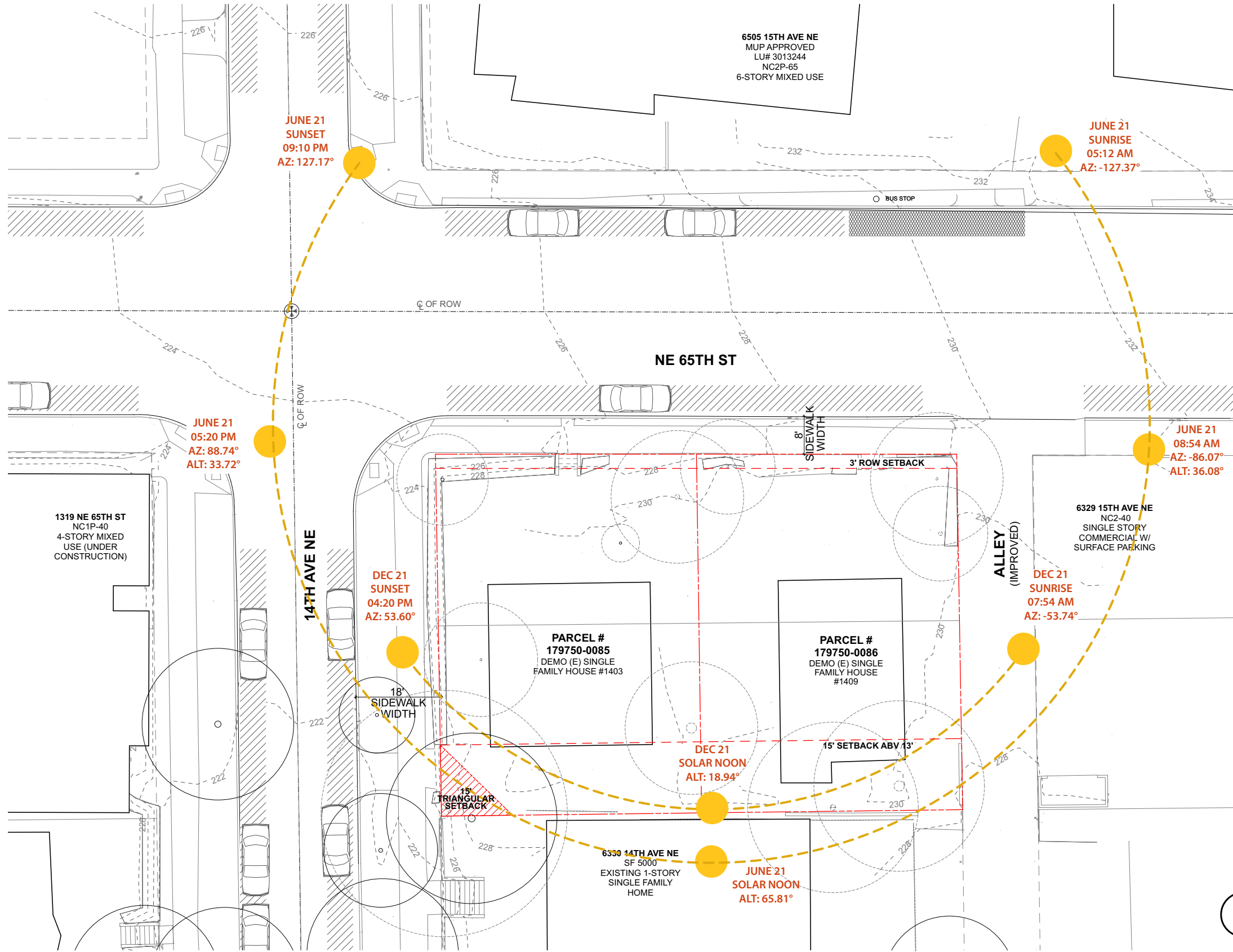
By contrast, the sidewalk on 14th Ave NE is spacious and inclusive of a wide planting strip at the curb. Street trees line both sides of 14th.



- 1 View of Site from North.** Existing single-family structures sit above the street level set back from 65th.
- 2 View of Site from Northwest Corner.** The site has many large trees and a rockery that extends down 14th Ave.
- 3 Sidewalk at 14th Ave NE.** The western structure has an adjacent garage and curb cut that break the rockery and the generous planting strip.
- 4 Sidewalk at NE 65th St.** A narrow sidewalk and minimal planting strip without street trees fronts the north side of the property.
- 5 View of alley.** The eastern structure abuts the alley which is across from a parking lot. Down the alley are new townhomes and the backs of existing single family houses.



# SITE ANALYSIS



## Topography

- Slope along NE 65th St: 6' gain from west to east
- Gentle slope along 14th Ave NE: 1' gain from south to north

## Neighboring Buildings

- NC2P-65 zoned lot to north, MUP approved (across NE 65th St)
- Commercial to east (across alley)
- Single-family residence to south
- Multi-family under construction, NC1P-40 zoned lot to west (across 14th Ave NE)

## Solar Access

- Excellent solar access due to low-height single-family zone to the south.
- Some overshadowing or shading possible on lower levels in the afternoon due to abundance of street trees on 14th

## Views

- Currently, territorial views are available in all directions above the first floor. Views to the south are protected due to adjacent single-family zoning.

## Structure Height

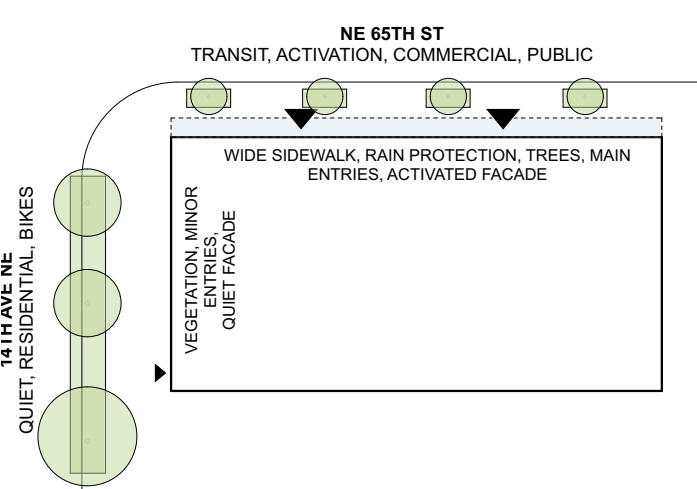
- Zoned as NC2P-40 with a base height limit of 40-ft.
- Located within 40-ft mapped height limit zone, which allows a 4-ft height increase when a floor-to-floor height of 13' or more is provided for nonresidential uses at street level.
- Total maximum allowable building height: 44-ft

## Allowable Building Area

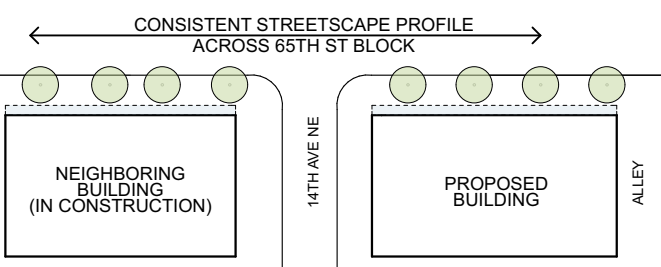
- The maximum FAR and site areas for the building site's two parcels are:
  - West Parcel:
    - 1.2 Base FAR (4 Max FAR\*)
    - 4,182-sf lot area
  - East parcel:
    - 1.2 Base FAR (4 Max FAR\*)
    - 4,144-sf lot area
  - Maximum allowable area: 33,304-sf (including incentive provisions)

POTENTIAL APPLICABLE GUIDELINES

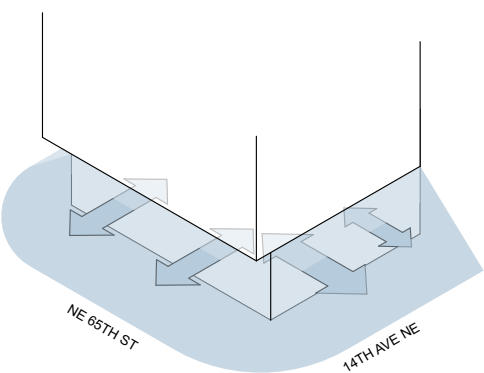
| CS2 Urban Pattern and Form   |  |  |
|--|--|--|
| Strengthen the most desirable forms, characteristics, and patterns of the streets, block faces, and open spaces in the surrounding area.   |  |  |
| B2 Location in the City and Neighborhood, Connection to the Street   | Identify opportunities for the project to make a strong connection to the street and carefully consider how the building will interact with the public realm. Consider the qualities and character of the streetscape— its physical features (sidewalk, parking, landscape strip, street trees, travel lanes, and other amenities) and its function (major retail street or quieter residential street)—in siting and designing the building | <p>The site is located at the intersection of a transit-oriented commercial street (NE 65th St) and a quiet residential street (14th Ave NE).</p> <p>The facade at NE 65th St reflects the street's development towards a pedestrian-friendly, transit-oriented commercial strip with:</p> <ul style="list-style-type: none"><li>- A widened sidewalk (by setting back the street-level facade)</li><li>- Continuous overhead weather protection</li><li>- New street trees to provide a safety/sound buffer between pedestrians and cars</li><li>- Siting of main entries, commercial, and residential amenity spaces for street activation</li></ul> <p>The facade on 14th Ave responds to the quieter residential character with:</p> <ul style="list-style-type: none"><li>- New street trees and increased vegetation to match the existing landscape character</li><li>- Siting of a residential bike room</li><li>- No major building entries for privacy and quiet</li></ul> |
|  | D1 Height Bulk and Scale, Existing Development & Zoning  | Review the height, bulk, and scale of neighboring buildings as well as the scale of development anticipated by zoning for the area to determine an appropriate complement and/or transition. Note that existing buildings may or may not reflect the density allowed by zoning or anticipated by applicable policies   |
| Roosevelt I Supplemental Guidance  | <p><b>Streetscape Compatibility, Commercial and Mixed-Use Developments: Continuity of the Street Wall Along Sidewalks</b></p> <p>Where building setbacks vary along the street due to required street dedications, new developments are encouraged to introduce elements that can help preserve the continuity of adjacent street-facing building walls, especially within the Core Commercial Area.</p>                                     | <p>The current streetscape of NE 65th St on the neighboring blocks is haphazard. However, the property just west of the site (across 14th Ave) is currently under construction and will provide a strong streetwall. This project proposes to continue the new streetscape profile created by that building to provide continuity across the block. This includes:</p> <ul style="list-style-type: none"><li>- A 4-ft street-level setback for sidewalk widening and overhead weather protection</li><li>- New street trees with species to match the northern neighbor's plantings</li><li>- Lighting in the building soffit for pedestrians</li></ul>  |
| PL1 Connectivity   |  |  |
| Complement and contribute to the network of open spaces around the site and the connections among them                                     |  |  |
| B2 Walkways and Connections, Pedestrian Volumes  | Provide ample space for pedestrian flow and circulation, particularly in areas where there is already heavy pedestrian traffic or where the project is expected to add or attract pedestrians to the area.   | The Preferred Alternative widens the street section on NE 65th in anticipation of a higher volume of pedestrian activity resulting from neighborhood redevelopment and the incoming Light Rail Station. The existing sidewalk on NE 65th is a narrow 7'-6", with a minimal planting separation between pedestrians and the street, and no street trees. The wider sidewalk will accommodate more pedestrians and provide planted separation between pedestrians and vehicles.  |
| PL2 Walkability  |  |  |
| Create a safe and comfortable walking environment that is easy to navigate and well-connected to existing pedestrian walkways and features |  |  |
| B1 Safety and Security, Eyes on the Street   | Create a safe environment by providing lines of sight and encouraging natural surveillance through strategic placement of doors, windows, balconies and street-level uses  | <p>The first floor is organized to provide:</p> <ul style="list-style-type: none"><li>- Transparency and activation on NE 65th St with commercial and residential communal spaces</li><li>- Transparency and eyes on the street on 14th w/res. amenity lobby at the corner and bike room</li><li>- Transparency and eyes on the street at the Alley entry w/ commercial spaces</li></ul>   |



CS2-B2 DIAGRAM



ROOSEVELT SUPPLEMENTAL GUIDANCE DIAGRAM



PL2-B2-B2 DIAGRAM



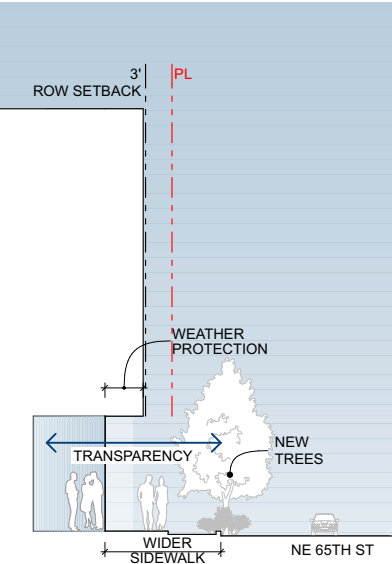
|   |   |   |
|---|---|---|
| <b>C1 Weather Protection, Locations and Coverage</b><br>Overhead weather protection is encouraged and should be located at or near uses that generate pedestrian activity such as entries, retail uses, and transit stops. Address changes in topography as needed to provide continuous coverage the full length of the building, where possible |   | Weather protection is built into the massing on NE 65th due to a street-level setback. The setback will cover entries to the multifamily residential lobby and commercial, and provide protection to pedestrians crossing in front of the site to reach transit stops.  |
| <b>PL3 Street-Level Interaction</b><br><i>Encourage human interaction and activity at the street-level with clear connections to building entries and edges</i>   |   |   |
| <b>C2 Retail Edges, Visibility</b><br>Maximize visibility into the building interior and merchandise displays. Consider fully operational glazed wall-sized doors that can be completely opened to the street, increased height in lobbies, and/ or special lighting for displays   |   | In all Design Alternatives the NE 65th St street level façade is highly transparent to optimize opportunities for retail visibility and connections.  |
| <b>Roosevelt   Supplemental Guidance</b>  | <b>Human Activity</b><br>Roosevelt is looking for opportunities to encourage pedestrian activity along sidewalks within the Commercial Core. This is especially important because sidewalks along Roosevelt and 65th are considered too narrow. If not required with new development, applicants are encouraged to increase the ground level setback in order to accommodate pedestrian traffic and amenity features. | While the project site is just outside the defined Roosevelt Commercial Core, the project still strives to increase pedestrian activity along NE 65th. It is expected that with the addition of the new Light Rail station, pedestrian activity along NE 65th further east of the Commercial Core is likely, thus the project designs for this potential. To this end, pedestrian realm improvements are proposed, such as: <ul style="list-style-type: none"><li>-Continuous Weather Protection</li><li>- Glazed storefronts at Commercial and Residential Lobbies</li><li>-Widened sidewalks (building setbacks)</li><li>- New street trees</li></ul> |
|   | <b>A4 Arrangement of Interior Uses, View and Connections</b><br>Locate interior uses and activities to take advantage of views and physical connections to exterior spaces and uses, particularly activities along sidewalks, parks or other public spaces  | The Commercial space at street level is located at the corner of NE 65th St and the alley, closer to the highly trafficked intersection of NE 65th St and 15th Ave, and across the alley from the current Sunrise Produce Stand.<br><br>The Residential lobby/amenity space is located on the corner of NE 65th St and 14th Ave NE. This location is most proximate to the future light rail station.   |

**DC1 Project Uses and Activities**  
*Optimize the arrangement of uses and activities on site*

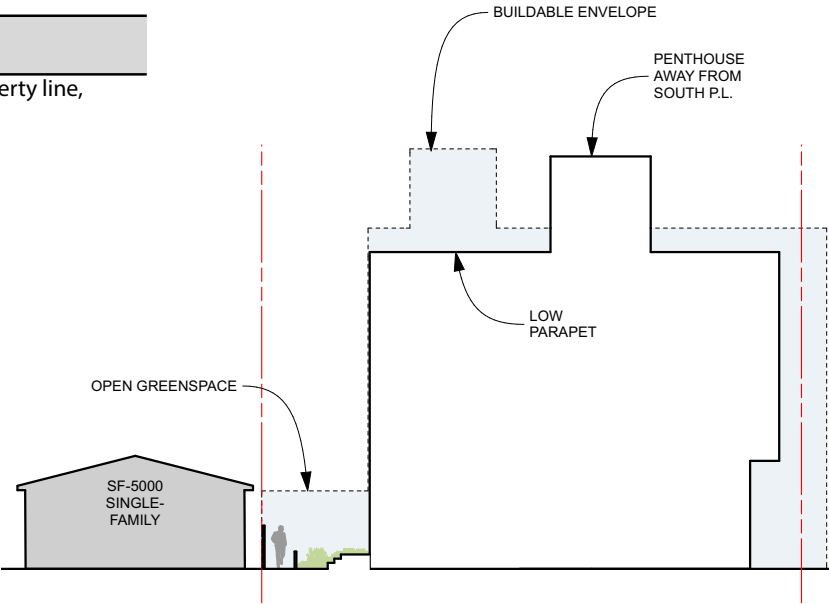
**DC2 Architectural Concept**  
*Develop an architectural concept that will result in a unified & functional design that fits well on the site & within its surroundings*

**A1 Massing, Site Characteristics and Uses**  
Arrange the mass of the building taking into consideration the characteristics of the site and the proposed uses of the building and its open space. In addition, special situations such as very large sites, unusually shaped sites, or sites with varied topography may require particular attention to where and how building massing is arranged as they can accentuate mass and height

The Preferred Alternative steps back 15' for the full height of building from the south property line, providing a buffer of open space from the neighboring single family zone to the south.



PL2, PL3 DIAGRAM



DC2-A1 DIAGRAM

# DESIGN PROPOSAL: PARKING GARAGE - RESIDENTIAL LOBBY ON 14TH (ALTERNATE 1)

### Summary

Stories: 4 (3-over-1 + 1 below grade)  
Unit Count: 45 (5,524 unit SF/ Floor)  
Floor Area: 16,572-SF Residential  
2,520-SF Commercial  
2,205-SF Parking  
**27,365-SF FAR (33,304 Max FAR)**  
Parking: 6 stalls (residential)

### Ground Floor Uses:

- NE 65th St: Commercial / Residential Amenity
- 14th Ave NE: Residential Lobby & Amenity
- Alley: Parking Access

### Ground Floor

- Commercial space along NE 65th St
- 5 parking spaces + van stall
- Lobby and Bike Room along 14th Ave
- Basement houses laundry, storage, and building services

### Upper Floors

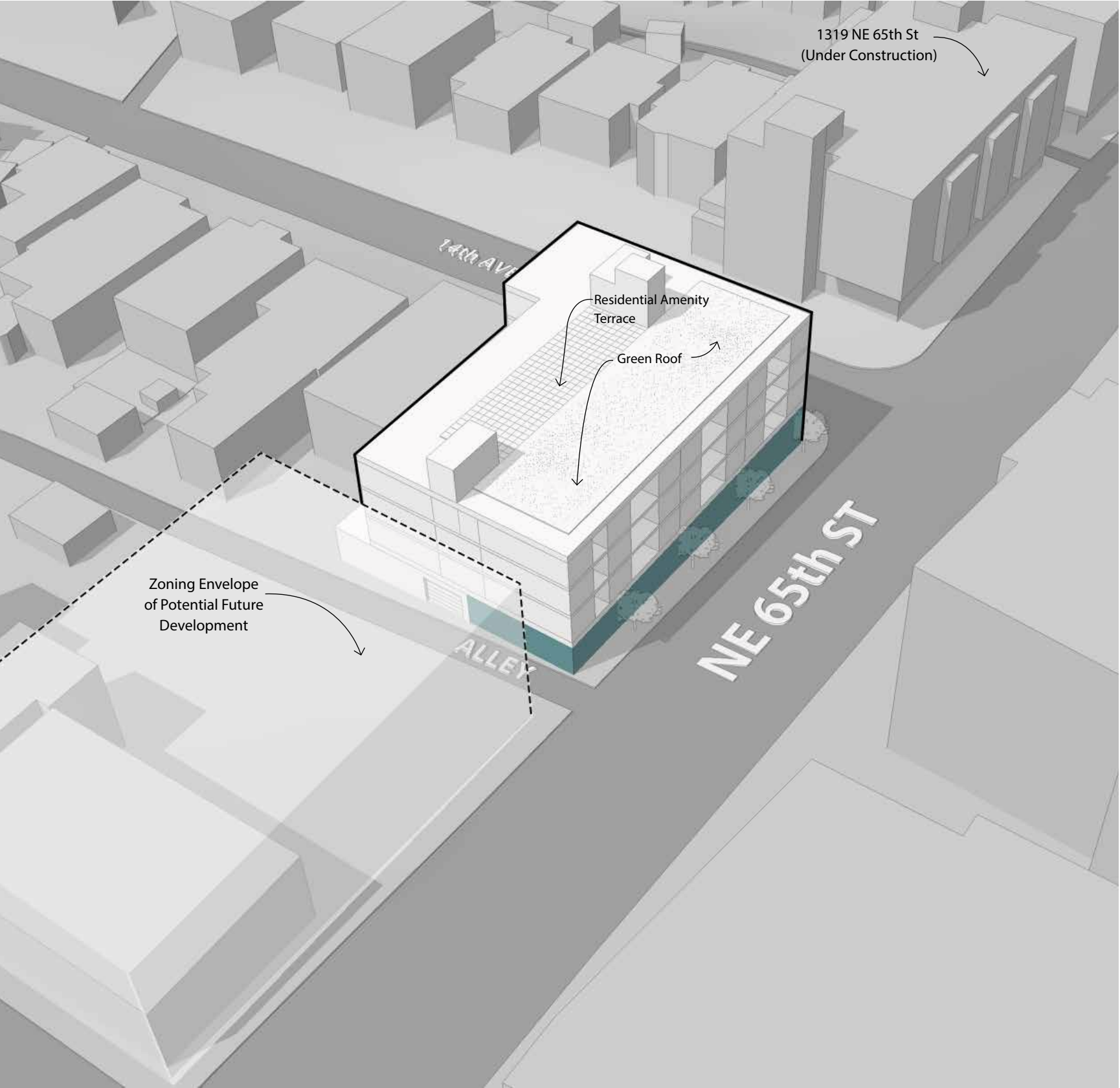
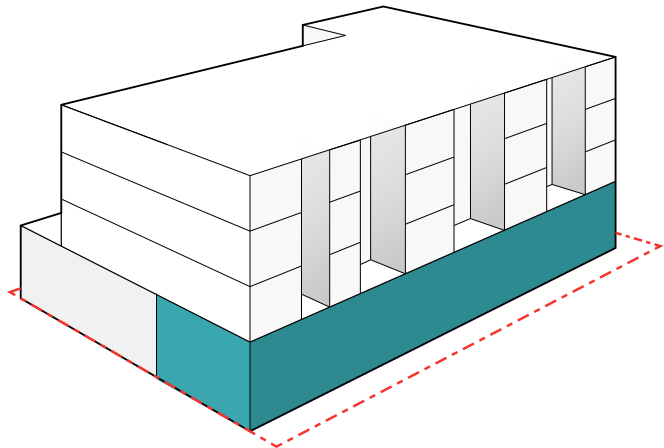
- 14 units per floor (all SEDU's)
- Private terraces at Level 2
- Roof terrace and green roof provide residents access to the outdoors and views
- Decks along NE 65th and 14th provide residents outdoor access

### Primary Disadvantages/Concerns

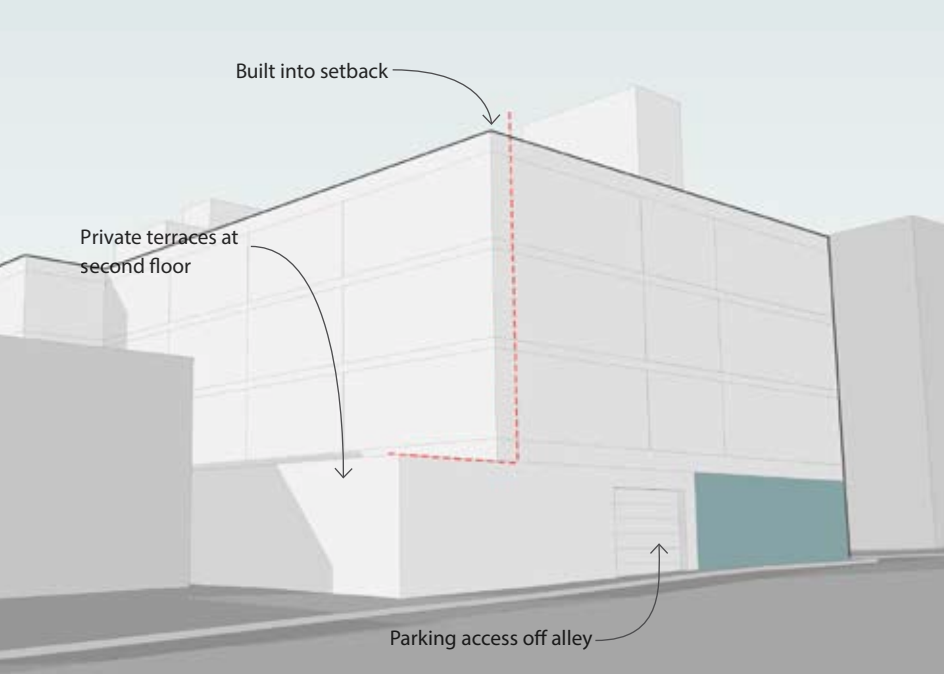
- Transition to south neighbor is abrupt and unfriendly
- No overhead weather protection

### Potential Departures

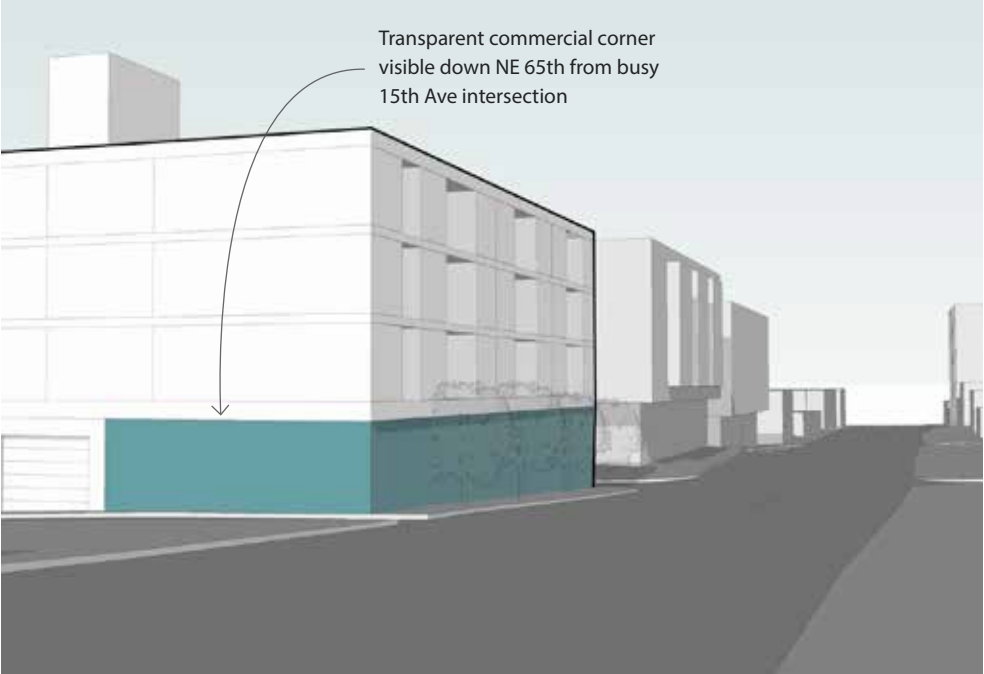
- Reduced width of overhead weather protection on principal pedestrian streets (< 6-ft)
- Reduced non-residential use depth at street level
- Structure in triangular setback abutting a residential lot
- Structure in south PL setback above 13-ft



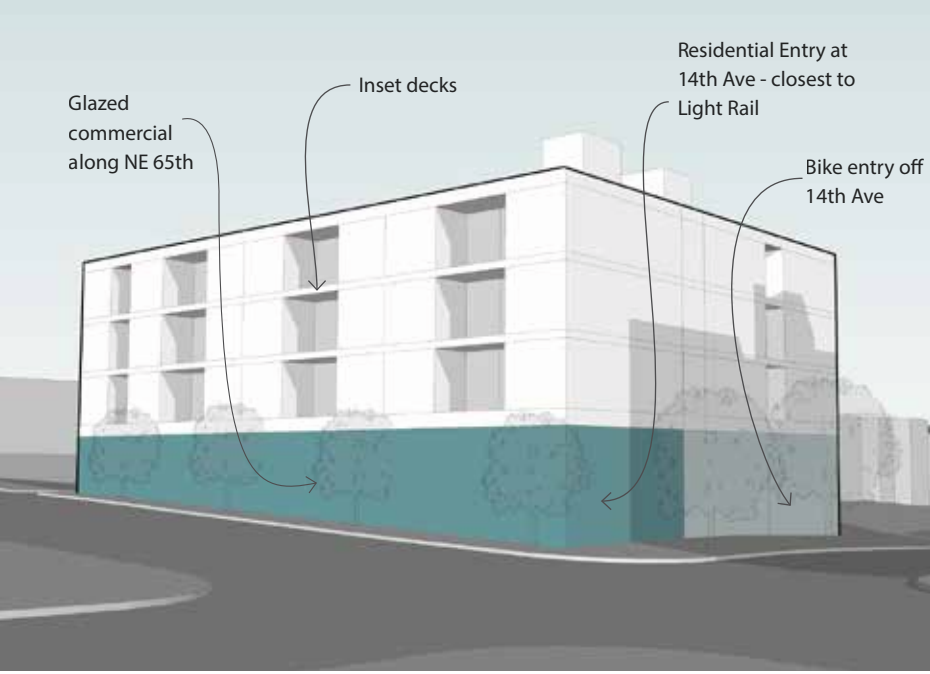




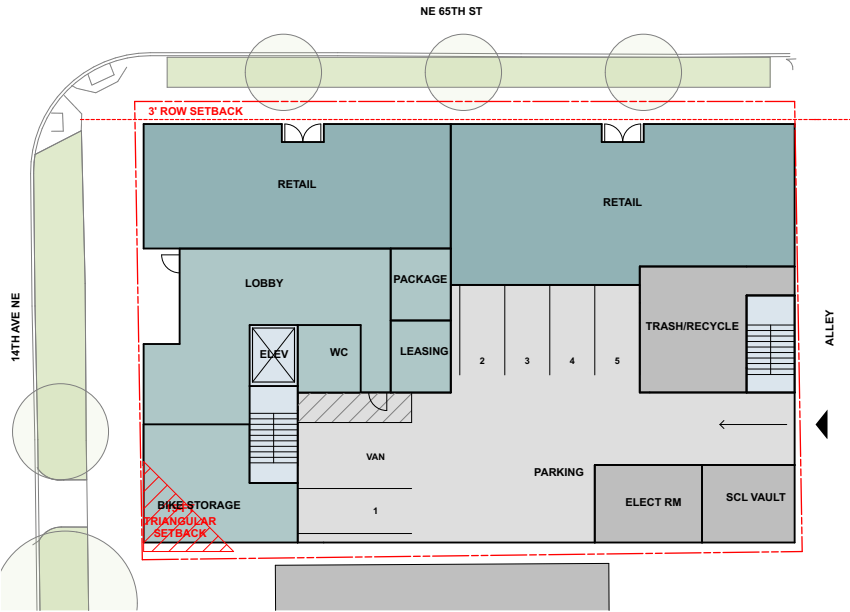
Southeast Corner (Alley)



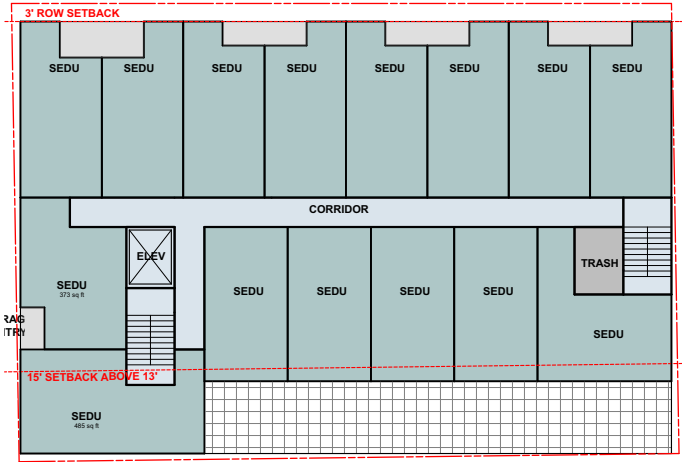
Northeast Corner (Corner of NE 65th St and Alley)



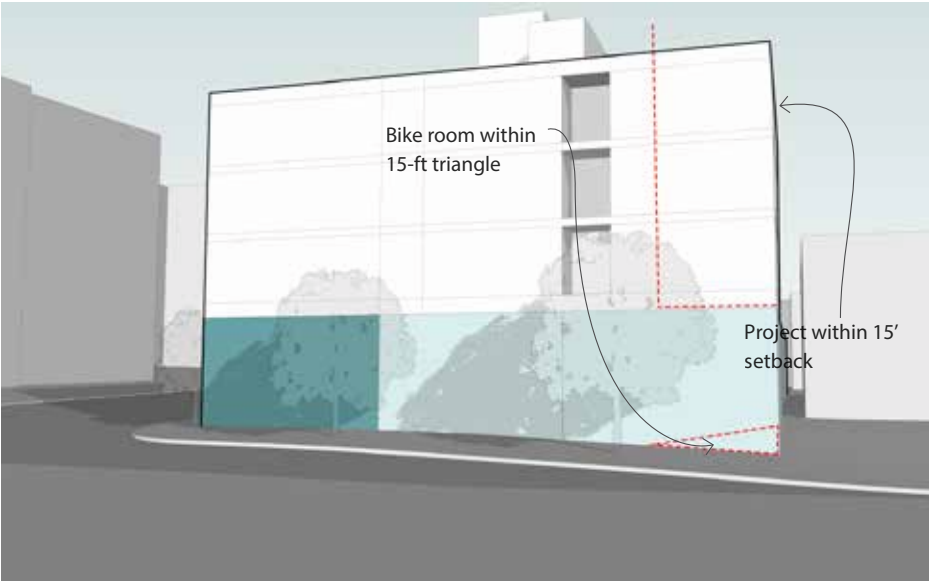
Northwest Corner (Corner of NE 65th St and 14th Ave NE)



Street Level Plan (Level 1)



Typical Residential Plan (Level 2)



Southwest Corner (14th Avenue NE)

DESIGN PROPOSAL: PARKING GARAGE - RESIDENTIAL LOBBY ON 65TH - CODE COMPLIANT (ALTERNATE 2)

Summary

Stories: 4 (3-over-1 + 1 below grade)  
Unit Count: 45 (5,198 unit SF/ Floor)  
Floor Area: 15,594-SF Residential  
2,965-SF Commercial  
2,034-SF Parking  
**26,739-SF FAR (33,304 Max FAR)**  
Parking: 4 stalls (residential)

Ground Floor Uses:

- NE 65th St: Commercial / Residential Amenity
- 14th Ave NE: Residential Lobby & Amenity
- Alley: Parking Access

Ground Floor

- Two separate retail spaces (each 30' deep)
- 3 parking spaces + van stall
- Minimal residential lobby midway along 65th (<20% of street frontage)
- Overhead weather protection provided by canopies
- Basement houses laundry, storage, and building services

Upper Floors

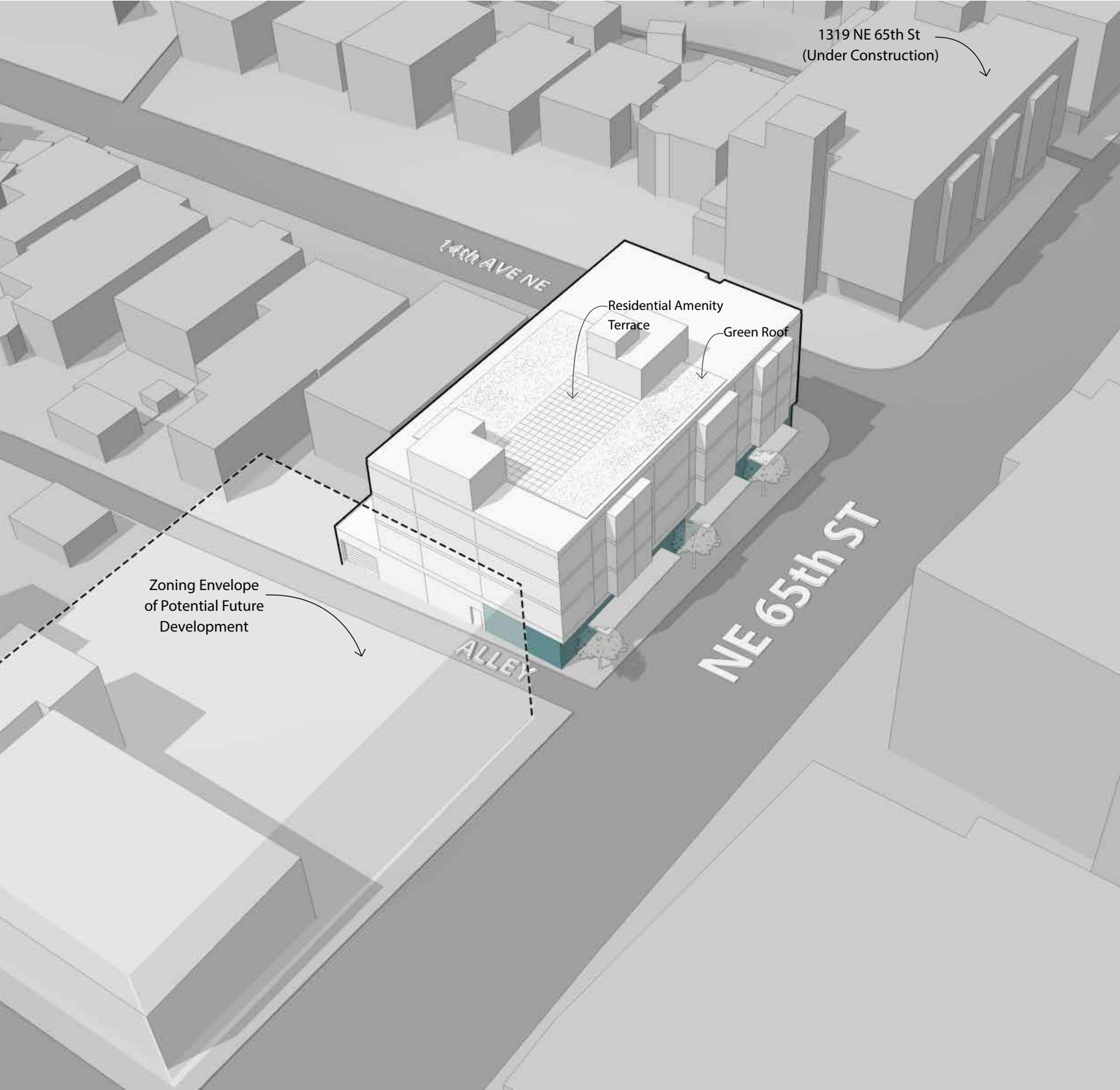
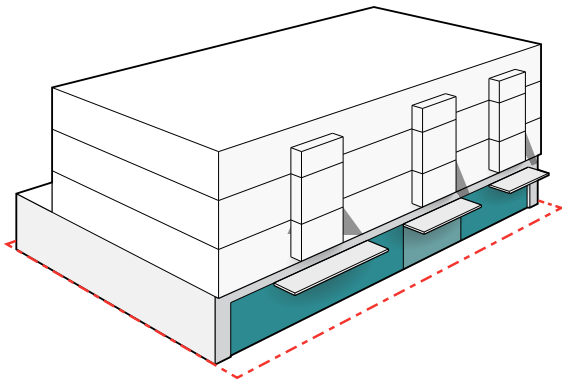
- 15 units per floor (all SEDU's)
- Double loaded corridor
- Garden terrace at Level 2, provides scale transition and privacy buffer to south neighbor
- Roof terrace and green roof provide residents access to the outdoors and views

Primary Disadvantages/Concerns

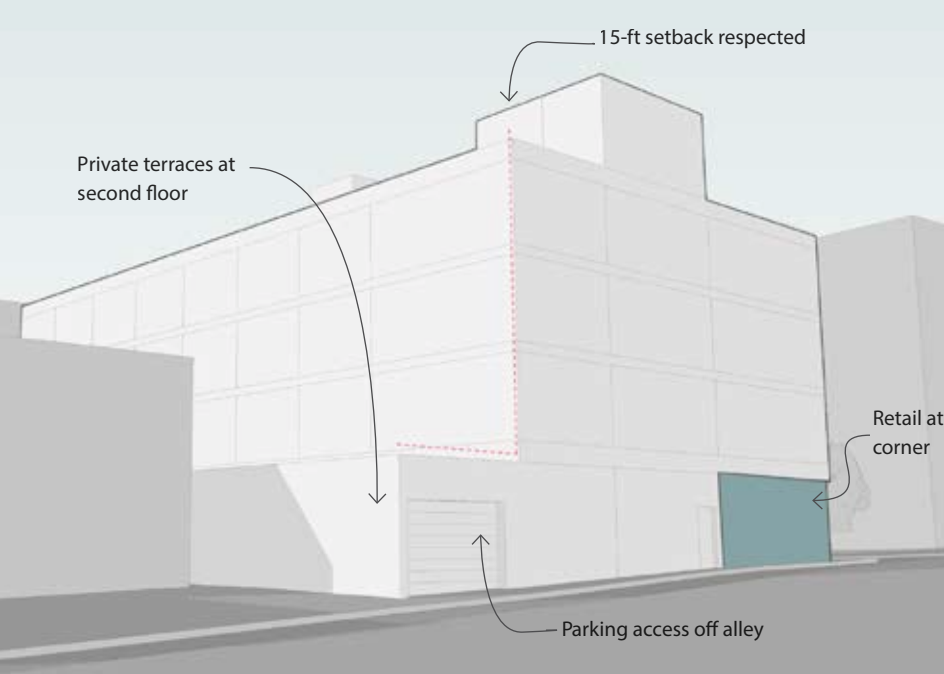
- 15' setback has potential to be an unsafe space with limited visibility from residents or passerby
- Limited parking on site due to the size of retail, 15-ft triangular setback, and bike parking requirement
- Minimal lobby is narrow and cannot accommodate an amenity room, which would be desired to offset the small units

Potential Departures

- None



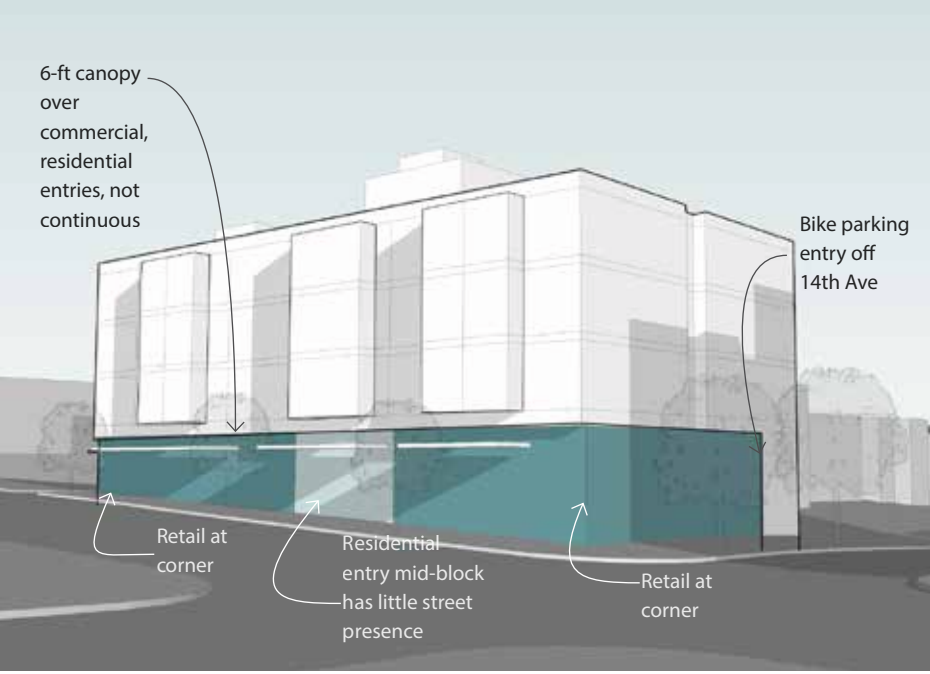




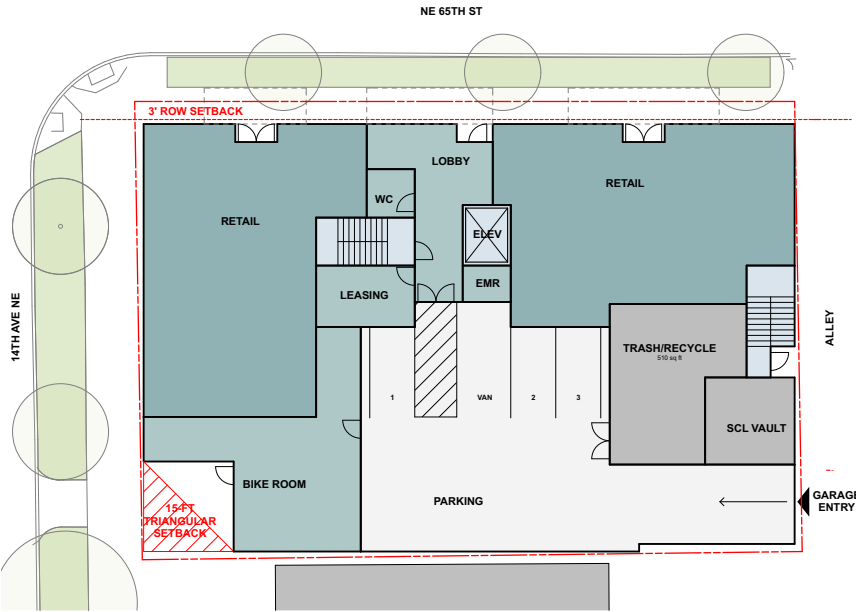
Southeast Corner (Alley)



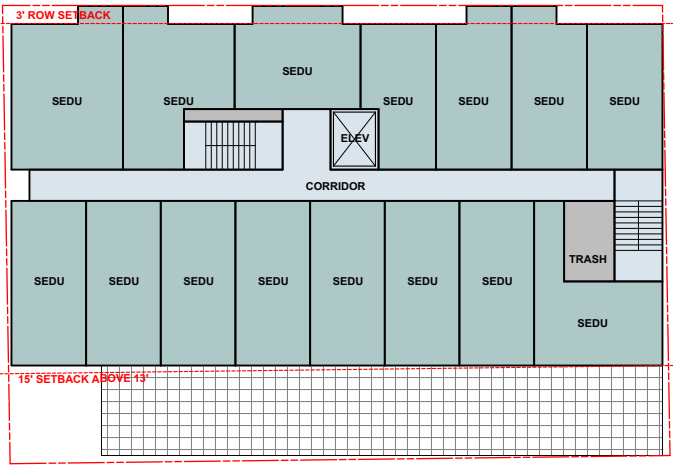
Northeast Corner (Corner of NE 65th St and Alley)



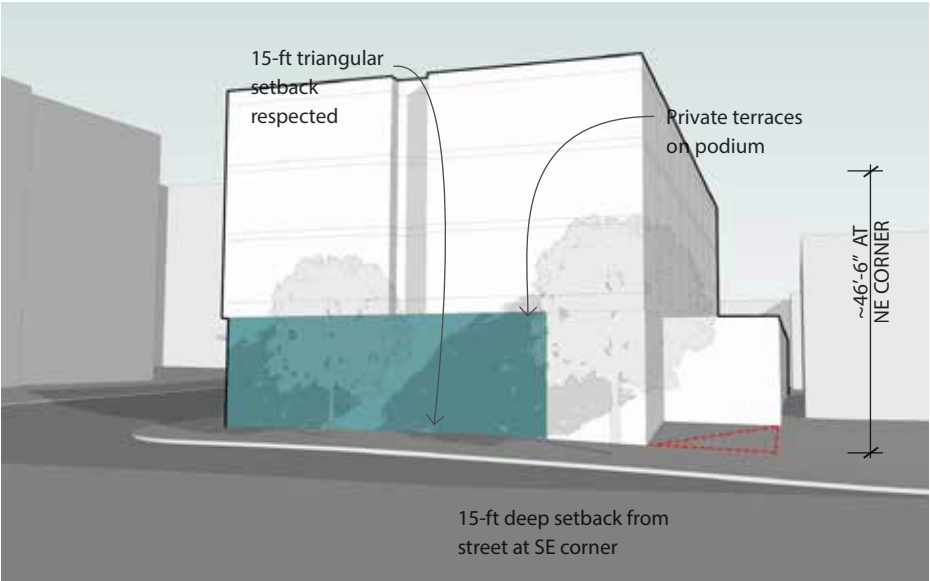
Northwest Corner (Corner of NE 65th St and 14th Ave NE)



Street Level Plan (Level 1)



Typical Residential Plan (Level 2)



Southwest Corner (14th Avenue NE)

DESIGN PROPOSAL: GROUND FLOOR UNITS (ALTERNATE 3)

Summary

- Stories: 4 (3-over-1 + 1 below grade)
- Unit Count: 52 (4,906 unit SF/ Floor)
- Floor Area: 15,966-SF Residential  
1,400-SF Commercial  
0-SF Parking  
**24,221-SF FAR (33,304 Max FAR)**
- Parking: 0 stalls (residential)
- Ground Floor Uses:
- NE 65th St: Commercial / Residential Amenity
  - 14th Ave NE: Commercial and Bike Room

Ground Floor

- Lobby/Amenity Space on northwest corner (glazed similar to commercial space) activates corner
- Commercial space at northeast corner
- Units along southside open out to private garden, provide transition to residential zone to the south and maximize rentable area in building
- Basement houses storage and building services
- Garden terraces along the southside provide a transition to the residential neighborhood

Upper Floors

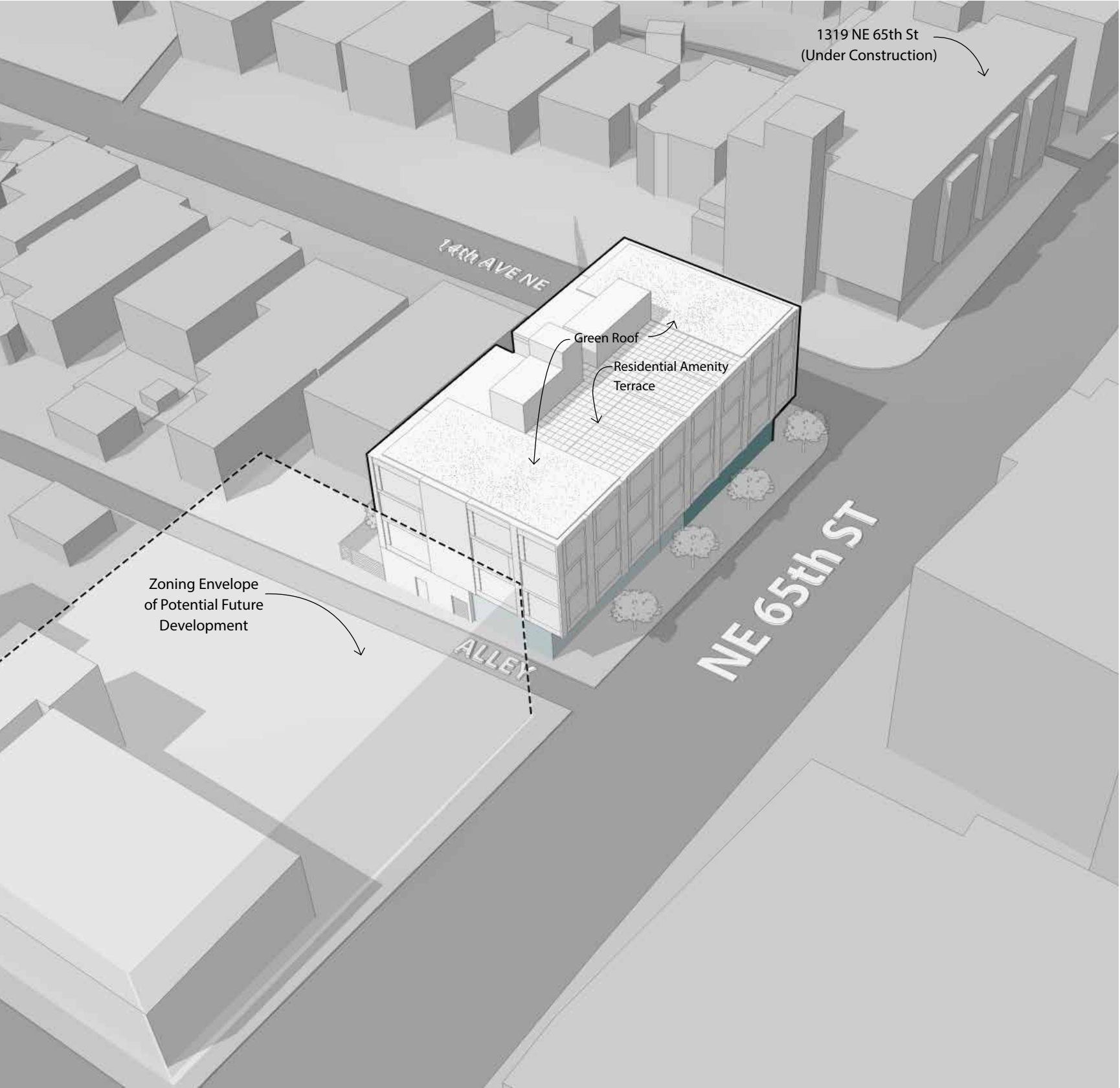
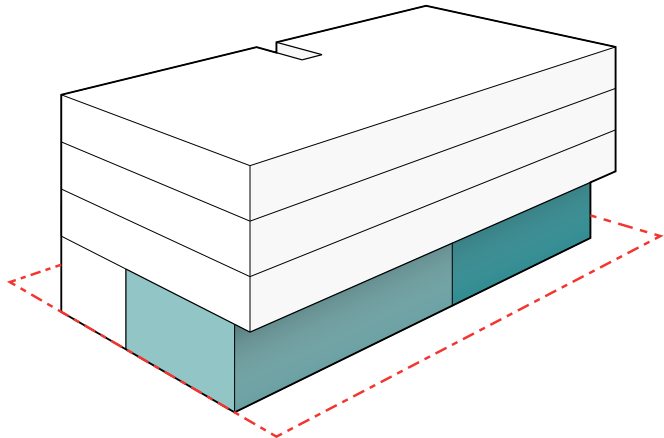
- 16 units per floor (all SEDU's)
- Notch on south facade provides light to elevator vestibule and corridors
- Roof terrace and green roof provide residents access to the outdoors and views

Primary Advantages/Benefits

- 4-ft setbacks at street level on NE 65th St and 14th Ave NE enhance the pedestrian experience: providing continuous weather protection, a wider sidewalk, and street trees in curbside planters
- The garden spaces on the south provides screening and a privacy buffer to the neighbor
- Provides continuous weather protection with a first floor setback on 65th St and 14th Ave
- Central location of stair and elevator cores minimizes their visibility from the street, creating a mass that appears smaller and lighter

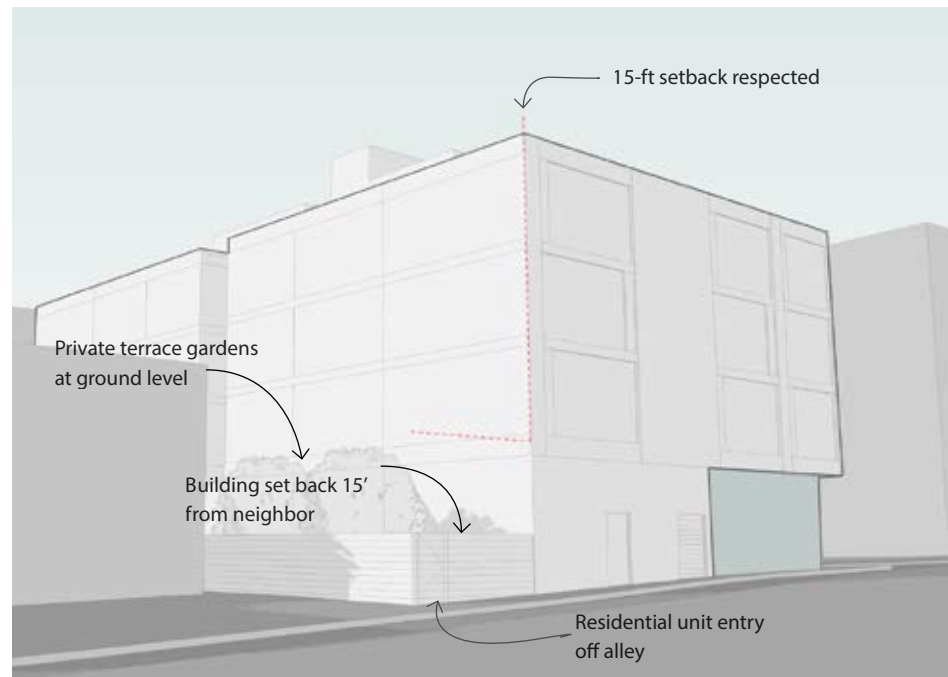
Potential Departures

- Increased percentage of residential use at street level on primary pedestrian street (NE 65th)
- Reduced width of overhead weather protection on principal pedestrian streets (< 6-ft)
- Reduced non-residential use depth at street level

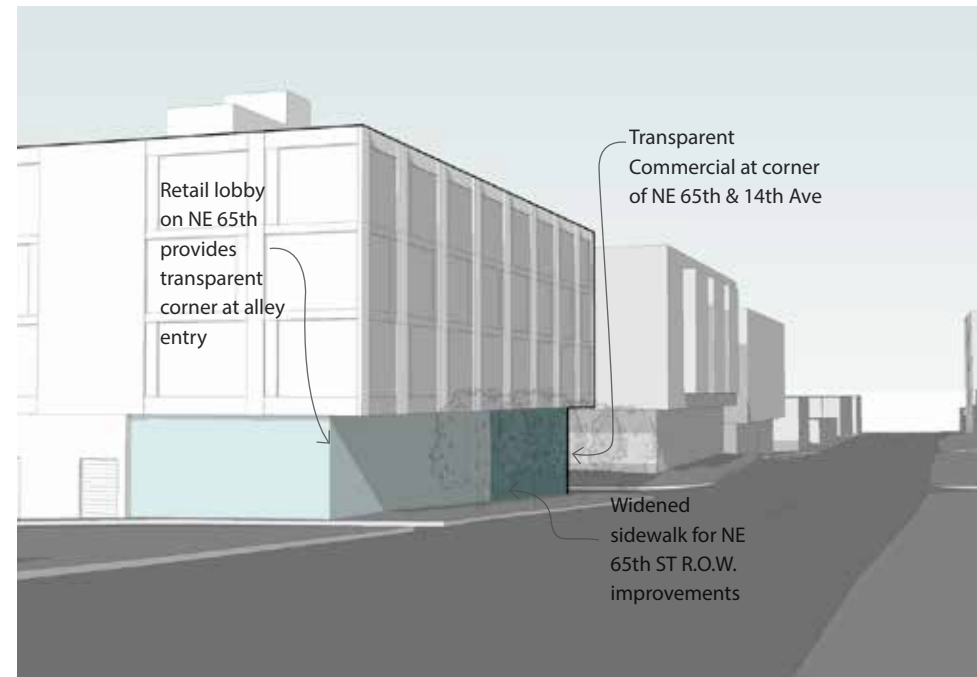




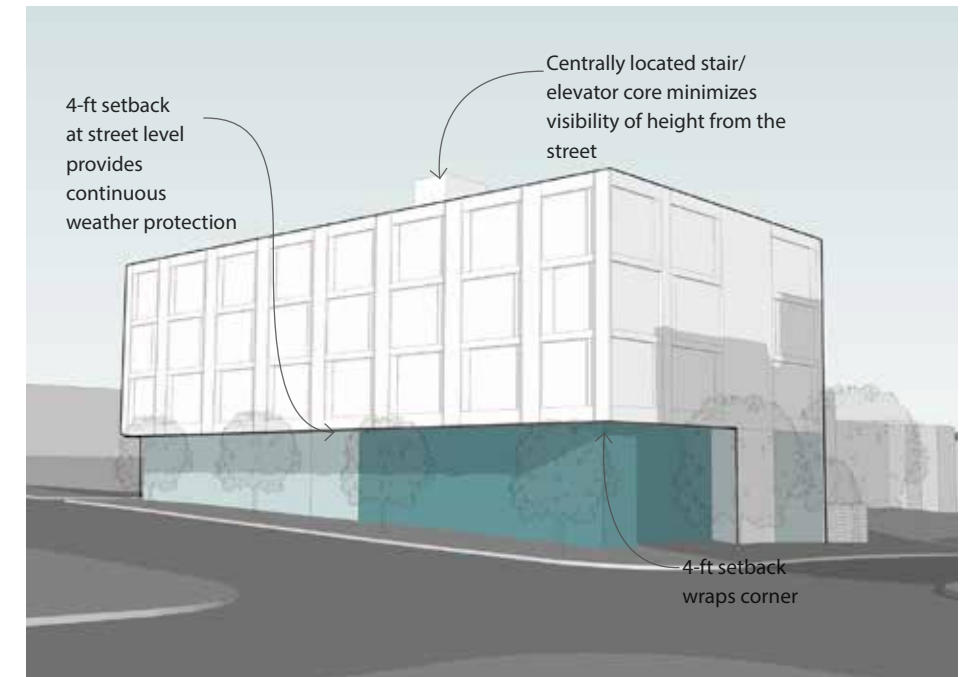
## DESIGN ALTERNATE SUMMARY



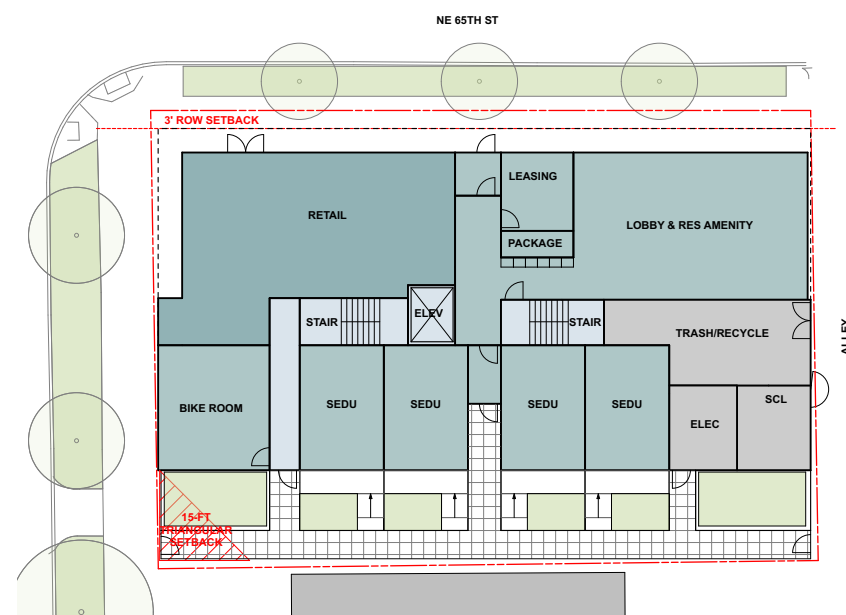
**Southeast Corner (Alley)**



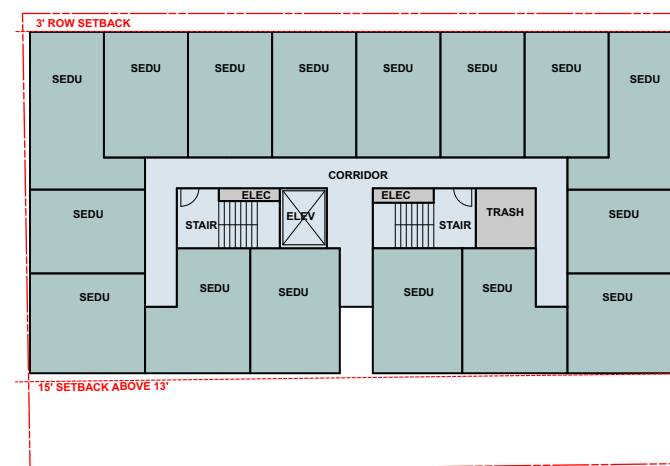
**Northeast Corner (Corner of NE 65th St and Alley)**



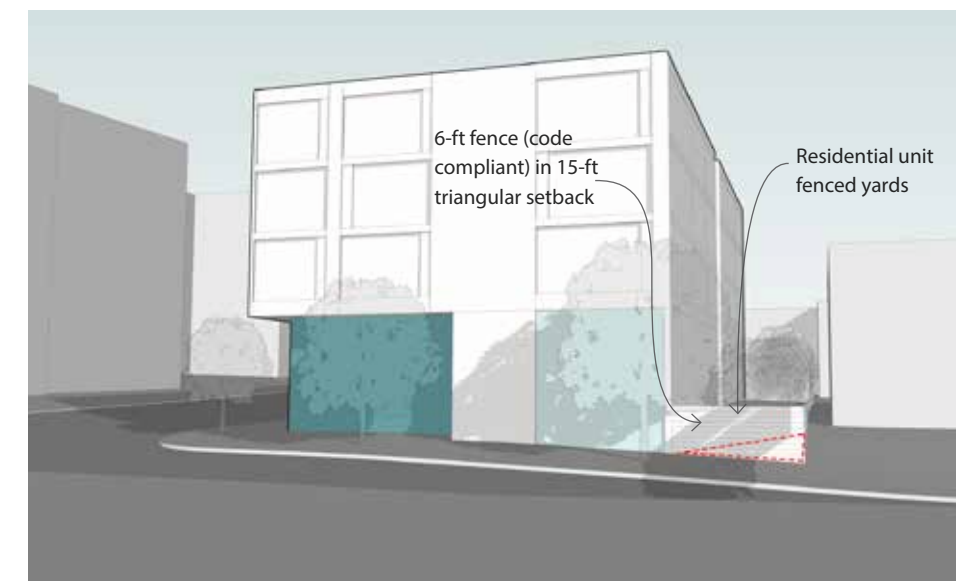
**Northwest Corner (Corner of NE 65th St and 14th Ave NE)**



**Street Level Plan (Level 1)**

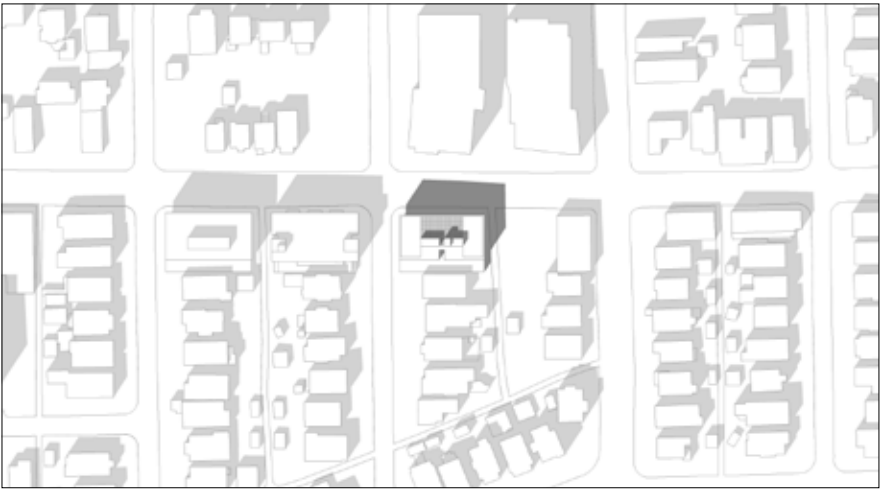
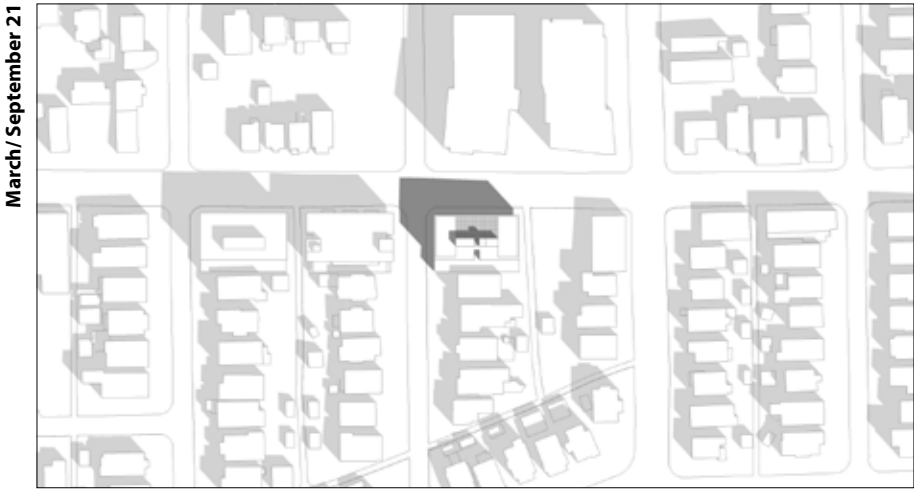
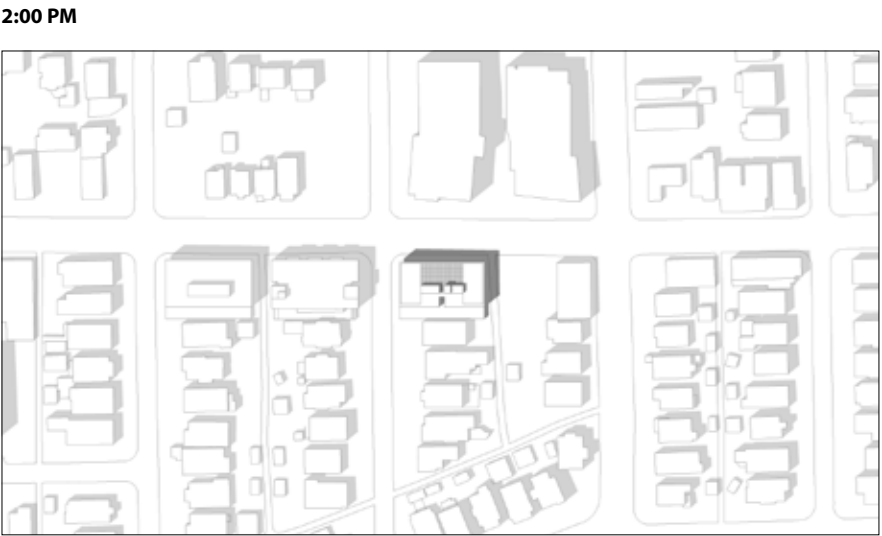
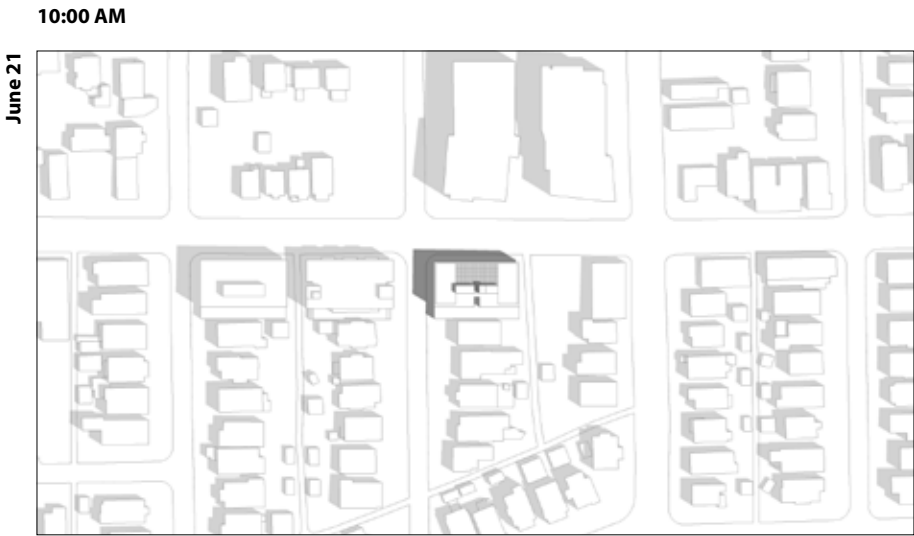


**Typical Residential Plan (Level 2)**



**Southwest Corner (14th Avenue NE)**

PREFERRED ALTERNATIVE SHADOW STUDY





DESIGN ALTERNATE SUMMARY



ALTERNATE 1 (L-SHAPE)

**Summary**  
Stories: 4 (3-over-1 + 1 below grade)  
Unit Count: 45 (5,524 unit SF/ Floor)  
Floor Area: 16,572-SF Residential  
2,520-SF Commercial  
2,205-SF Parking  
**27,365-SF FAR (33,304 Max FAR)**  
Parking: 6 stalls (residential)  
  
Ground Floor Uses:  
• NE 65th St: Commercial / Residential Amenity  
• 14th Ave NE: Residential Lobby & Amenity  
• Alley: Parking Access

**Primary Disadvantages/Concerns**  
• Transition to south neighbor is abrupt and unfriendly  
• No overhead weather protection

**Potential Departures**  
• Reduced width of overhead weather protection on principal pedestrian streets (< 6-ft)  
• Reduced non-residential use depth at street level  
• Structure in triangular setback abutting a residential lot  
• Structure in south PL setback above 13-ft



ALTERNATE 2 (NO DEPARTURES)

**Summary**  
Stories: 4 (3-over-1 + 1 below grade)  
Unit Count: 45 (5,198 unit SF/ Floor)  
Floor Area: 15,594-SF Residential  
2,965-SF Commercial  
2,034-SF Parking  
**26,739-SF FAR (33,304 Max FAR)**  
Parking: 4 stalls (residential)  
  
Ground Floor Uses:  
• NE 65th St: Commercial / Residential Amenity  
• 14th Ave NE: Residential Lobby & Amenity  
• Alley: Parking Access

**Primary Disadvantages/Concerns**  
• 15’ setback has potential to be an unsafe space with limited visibility from residents or passerby  
• Limited parking on site due to the size of retail, 15-ft triangular setback, and bike parking requirement  
• Minimal lobby is narrow and cannot accommodate an amenity room, which would be desired to offset the small units

**Potential Departures**  
• None



ALTERNATE 3 (PREFERRED)

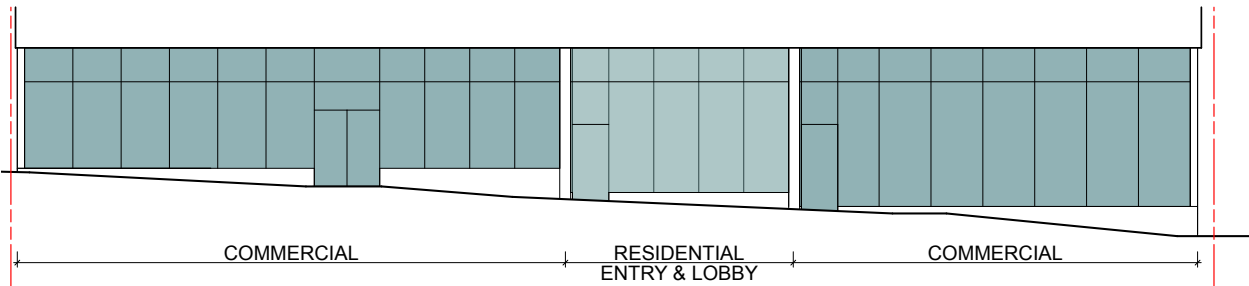
**Summary**  
Stories: 4 (3-over-1 + 1 below grade)  
Unit Count: 52 (4,906 unit SF/ Floor)  
Floor Area: 15,966-SF Residential  
1,400-SF Commercial  
0-SF Parking  
**24,221-SF FAR (33,304 Max FAR)**  
Parking: 0 stalls (residential)  
  
Ground Floor Uses:  
• NE 65th St: Commercial / Residential Amenity  
• 14th Ave NE: Commercial and Bike Room

**Primary Advantages/Benefits**  
• 4-ft setbacks at street level on NE 65th St and 14th Ave NE enhance the pedestrian experience: providing continuous weather protection, a wider sidewalk, and street trees in curbside planters  
• The garden spaces on the south provides screening and a privacy buffer to the neighbor  
• Provides continous weather protection with a first floor setback on 65th St and 14th Ave  
• Central location of stair and elevator cores minimizes their visibility from the street, creating a mass that appears smaller and lighter

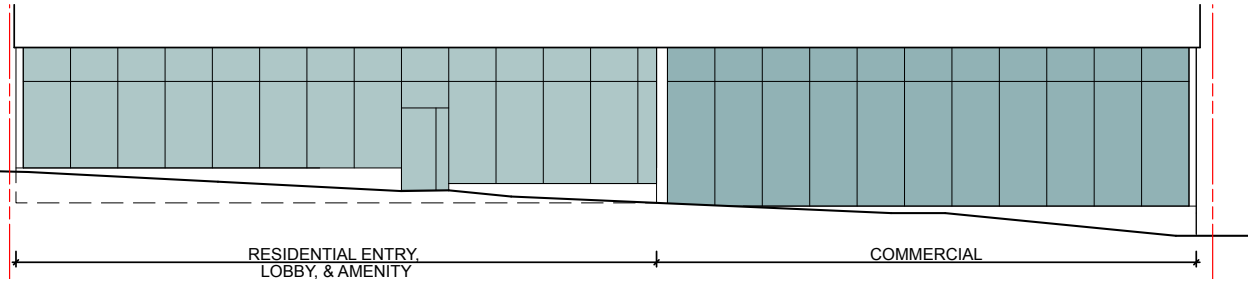
**Potential Departures**  
• Increased percentage of residential use at street level on primary pedestrian street (NE 65th)  
• Reduced width of overhead weather protection on principal pedestrian streets (< 6-ft)  
• Reduced non-residential use depth at street level

DEPARTURE REQUEST DIAGRAMS

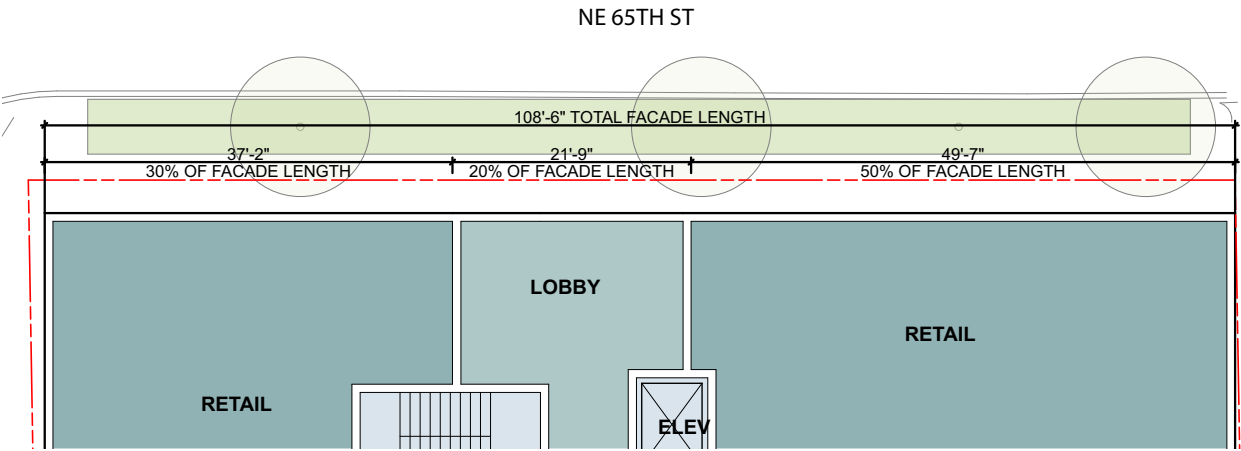
| STANDARD REQUIREMENT |                | REQUEST  | RATIONALE   | DESIGN GUIDELINES   |
|----------------------|----------------|--|---|---|
| 1A                   | 23.47A.005.D.1 | Residential uses may occupy no more than 20% of the street-level street-facing façade on a designated principal pedestrian street  | To allow a larger percentage of residential uses (and consequently smaller percentage of the required uses) at the NE 65th St façade. While the proposed design does not meet the required percentages, the uses are intended to compliment the streetscape. Approximately 50% of the façade will be a commercial space, with large windows and high transparency, while the remaining 50% will be the residential lobby, leasing center and resident amenity. With small residential units, the public spaces like the lobby and business center are expected to be highly-used gathering spaces. From the exterior, these residential amenity spaces will have large windows consistent with the commercial frontage. | <b>PL2 WALKABILITY</b><br>(Safety and Security: Eyes on the Street, Street Level Transparency)<br><br><b>PL3 STREET-LEVEL INTERACTION</b><br>(Retail Edges: Visibility) |
| 1B                   | 23.47A.005.D.1 | Along designated principal pedestrian streets eating and drinking establishments, offices, or sales and services is required along 80% of the street-facing façade.                                    |   |   |
| 1C                   | 23.47A.008.C.1 | In pedestrian-designated zones, a minimum of 80% of the width of a structure's street-level façade facing a principal pedestrian street shall be occupied by uses listed in subsection 23.47A.005.D.1. |   |   |



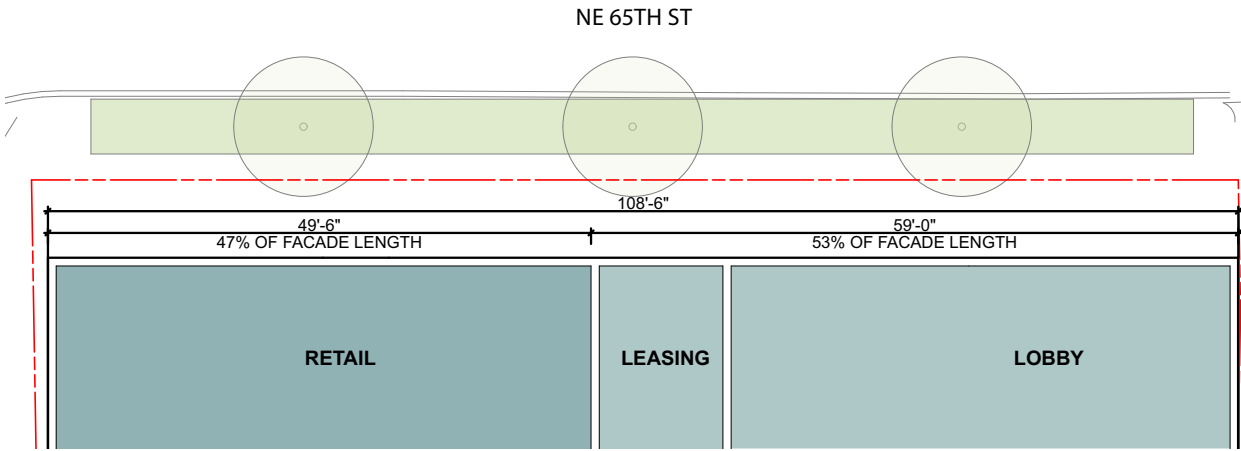
COMPLIANT FIRST FLOOR NE 65TH ST ELEVATION



PREFERRED FIRST FLOOR NE 65TH ST ELEVATION



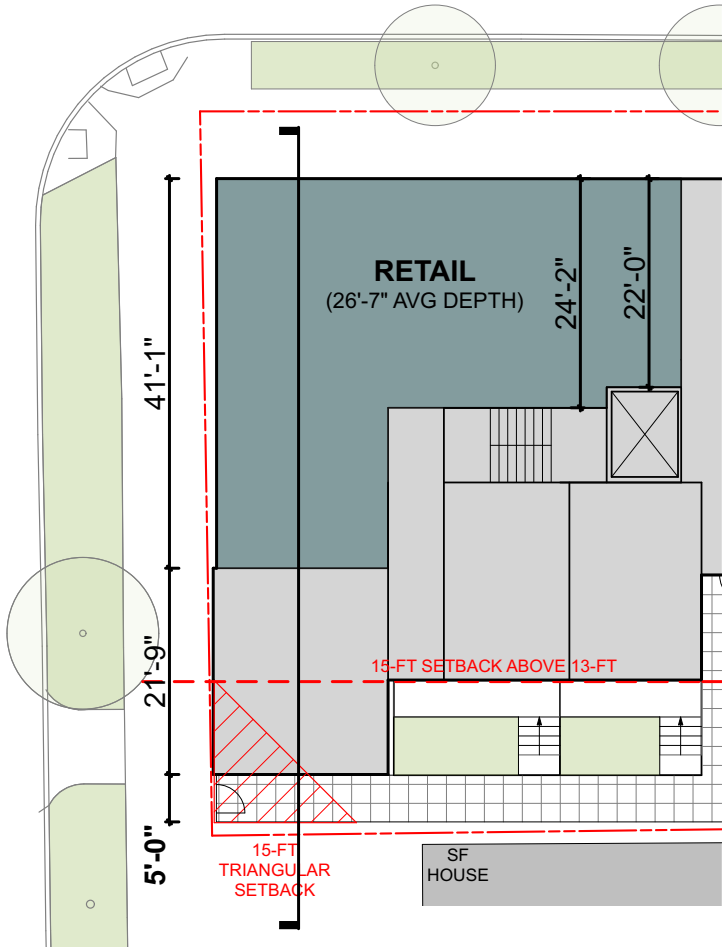
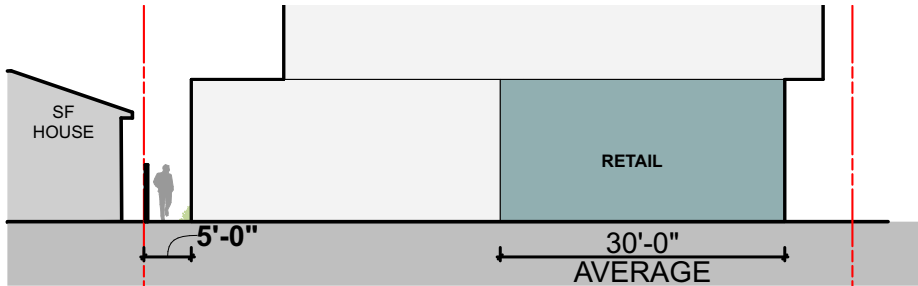
COMPLIANT FIRST FLOOR PLAN



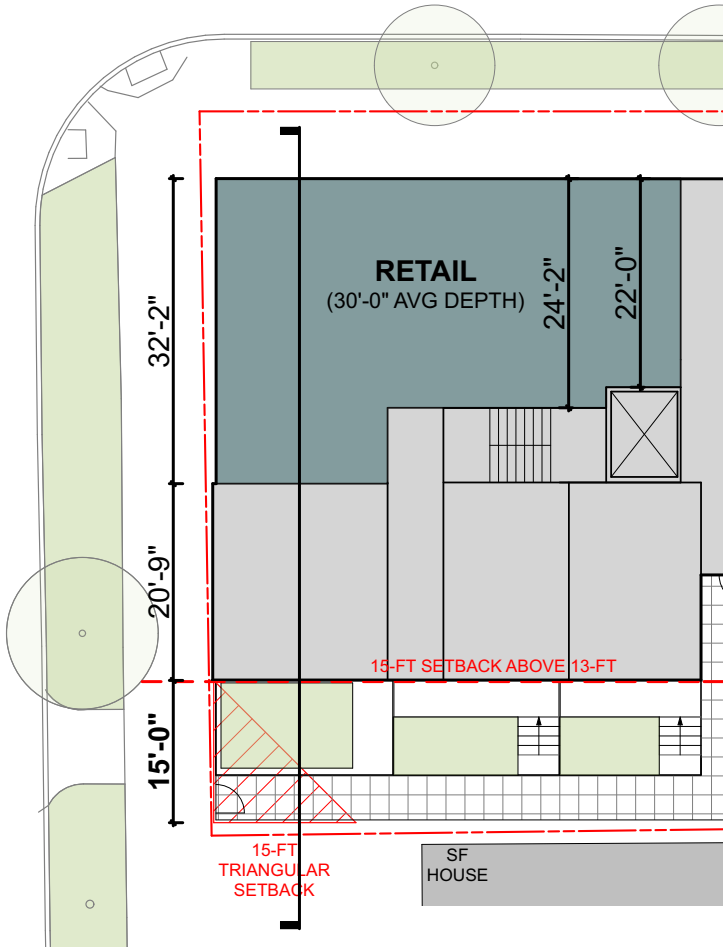
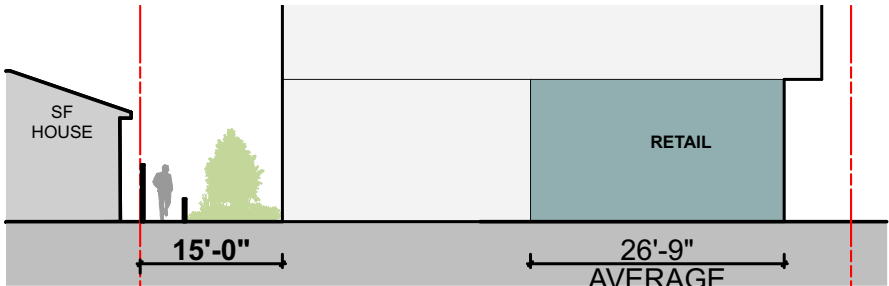
PROPOSED FIRST FLOOR PLAN



| STANDARD REQUIREMENT   | REQUEST  | RATIONALE   | DESIGN GUIDELINES   |
|--|--|---|---|
| 2 23.47A.008.B.3 Non-residential uses shall extend an average depth of at least 30-ft and a minimum depth of 15-ft from the street-level street-facing façade. | Allow a reduced depth of an average of 26-ft for the commercial space with a 32-ft space for 18-ft of the façade | The projects aims to enhance the pedestrian environment on NE 65 <sup>th</sup> St, with widened sidewalks, plantings, and weather protection. To match the adjacent project's street frontage, the facade of the first floor needs to be setback. Due to the narrow site, the setback depth is taken out of the commercial depth. | <p><b>PL2 WALKABILITY</b><br/>(Safety and Security: Eyes on the Street, Street Level Transparency)</p> <p><b>PL3 STREET-LEVEL INTERACTION</b><br/>(Retail Edges: Visibility) ...“maximize visibility into building interiors”</p> |



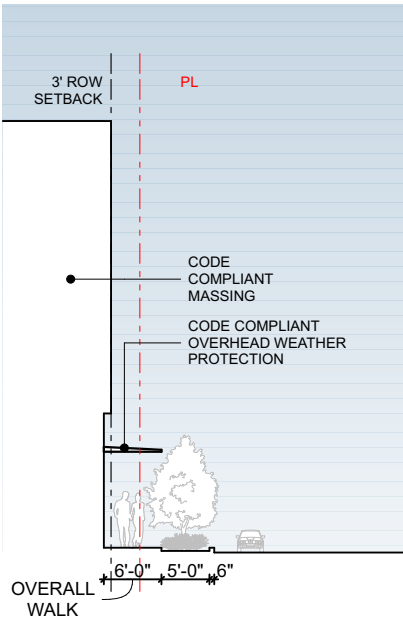
COMPLIANT FIRST FLOOR PLAN & SECTION



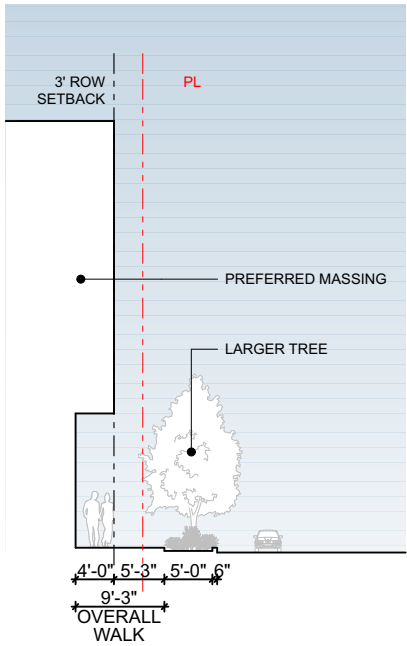
PROPOSED FIRST FLOOR PLAN & SECTION

DEPARTURE REQUEST DIAGRAMS

| STANDARD REQUIREMENT |                |   | REQUEST   | RATIONALE   | DESIGN GUIDELINES  |
|----------------------|----------------|---|---|---|--|
| 3                    | 23.47A.008.C.1 | Overhead Weather Protection: Continuous overhead weather protection is required along at least 60% of the street frontage on a principal pedestrian street. The covered area shall have a | and 22-ft space for the remainder.  |   | <b>PL1 CONNECTIVITY</b><br>(Walkways and Connections: Pedestrian Volumes)    |
|                      |                |   | To allow 4' deep weather protection between 10'-13' above grade along 100% of the | This proposal provides continuous weather protection across the building, instead of just at 60% coverage, albeit at a slightly narrower dimension than code dictates. This matches the streetscape of the Ecoluxe project to the west across 14th Ave. | <b>PL2 WALKABILITY</b><br>(Weather Protection: Locations and Coverage)       |
|                      |                |   |   |   | <b>Roosevelt I Supplemental Guidance</b><br>(Human Activity along sidewalks) |



COMPLIANT FIRST FLOOR PLAN & STREET SECTION



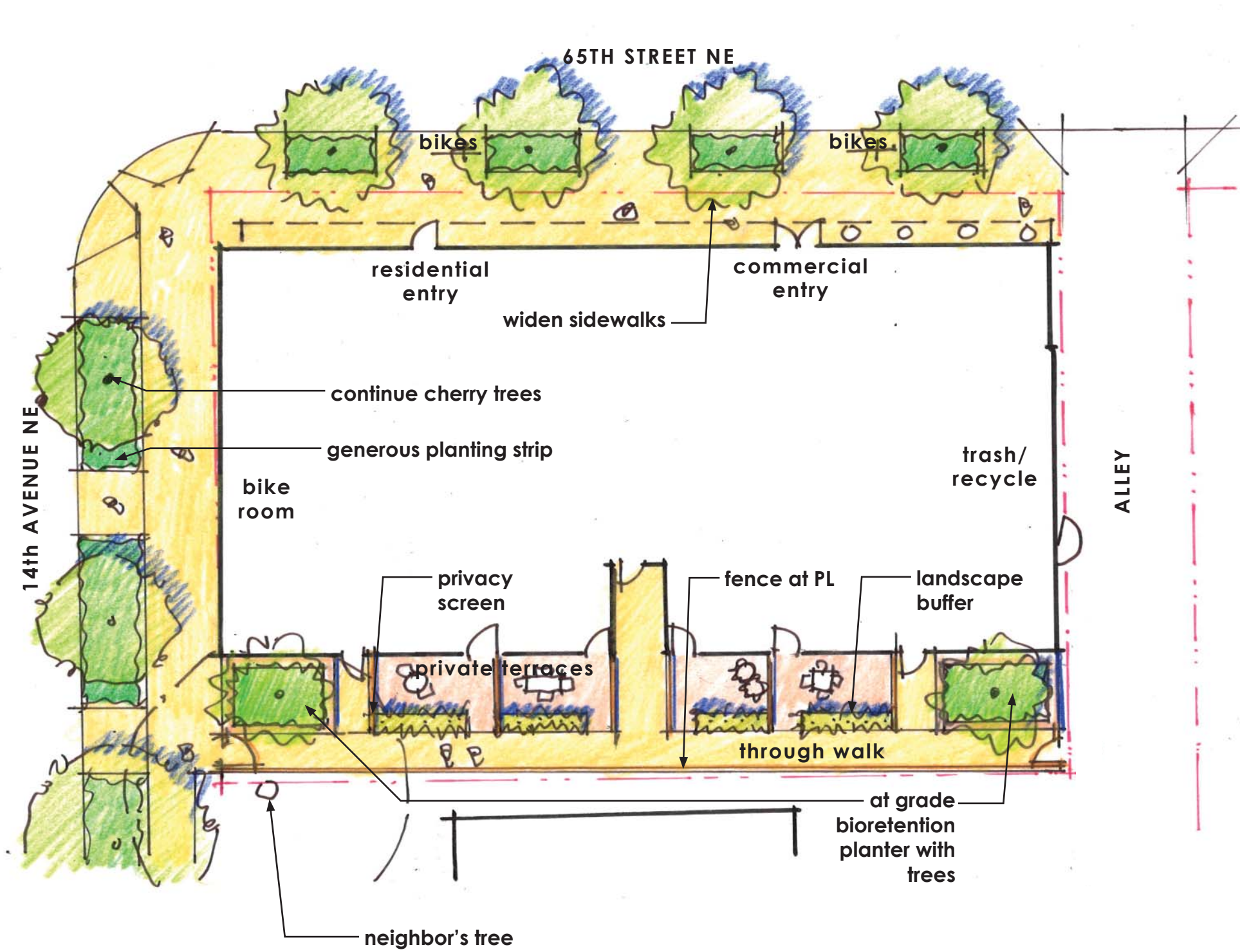
PROPOSED FIRST FLOOR PLAN & STREET SECTION



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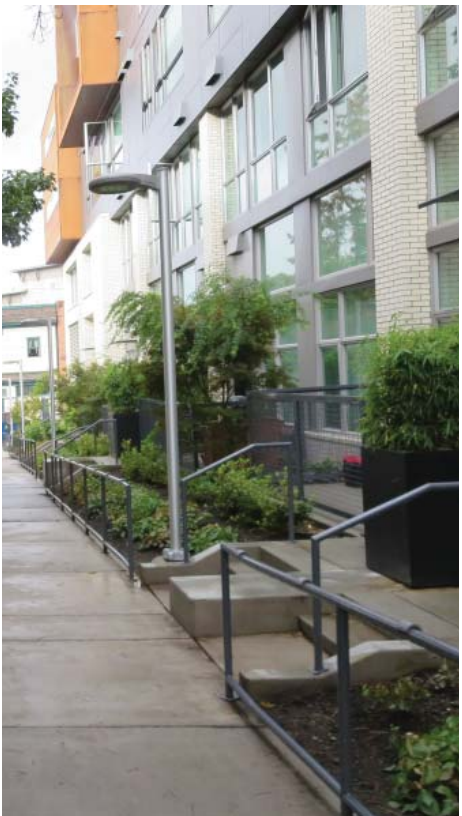
CONCEPTUAL LANDSCAPE PLAN (STREETSCAPE AND PODIUM)



continue cherry trees



generous planter strip



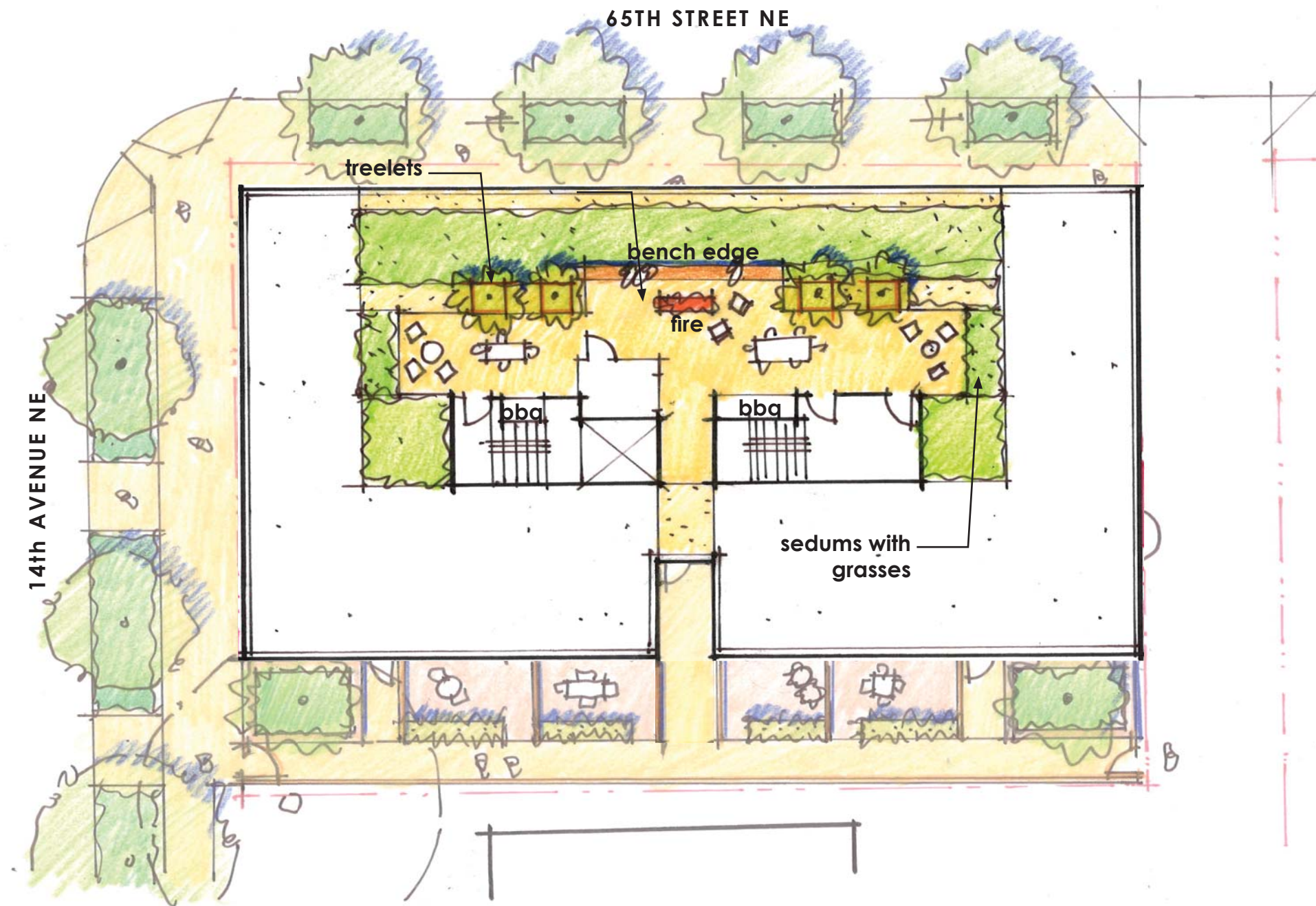
through walk



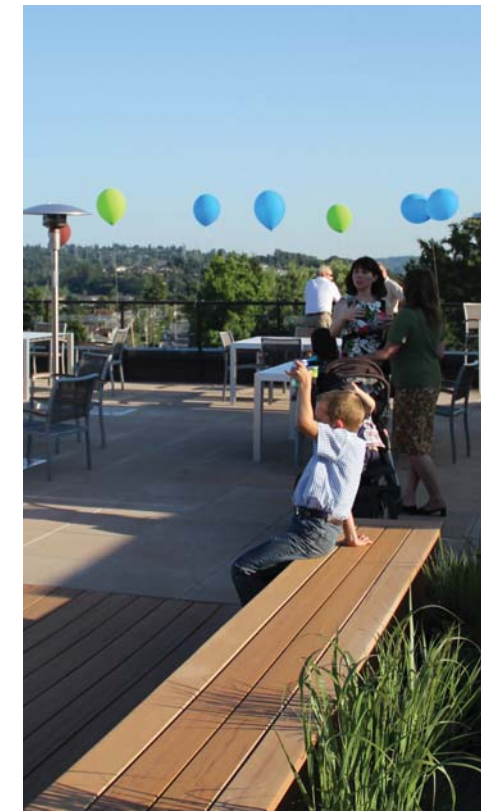
at grade bioretention planter



## CONCEPTUAL LANDSCAPE PLAN (ROOF)



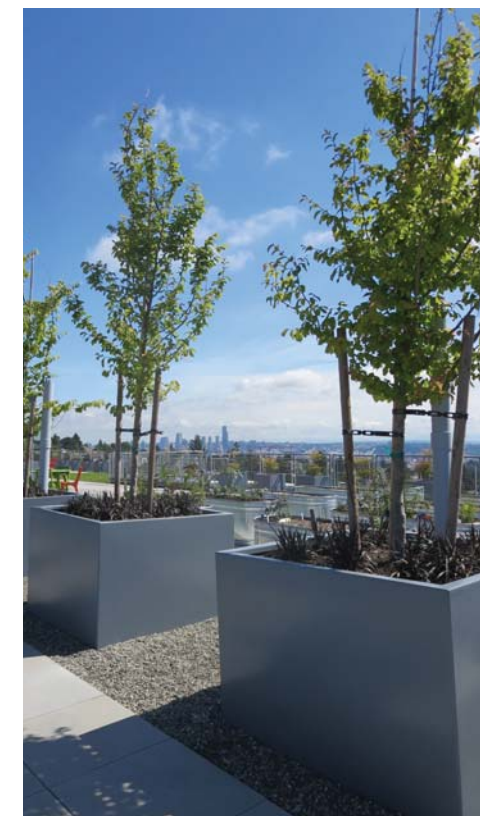
Narrow but Generous Terrace



Bench Edge



Sedums with Grasses



Treelets







