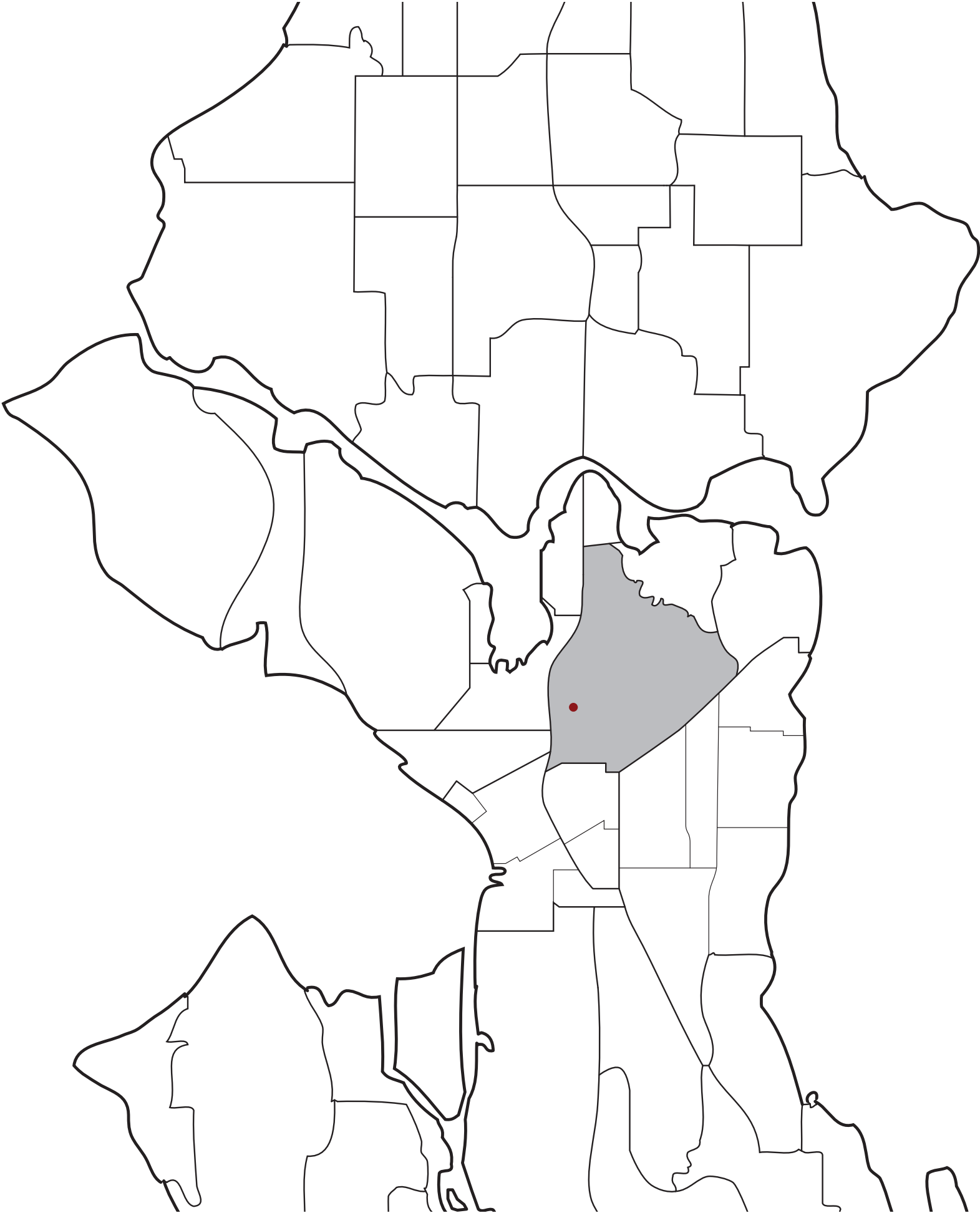


#3024635

600 E Howell St. **EARLY DESIGN GUIDANCE**

 **BLUEPRINT S+HWorks**
ARCHITECTURE & DESIGN



ADDRESS

600 E HOWELL ST
SDCI# 3024635

PROJECT TEAM

| | |
|-----------|-----------------------|
| OWNER | BLUEPRINT HOWELL, LLC |
| ARCHITECT | S+H Works, LLC |
| SURVEYOR | GeoDimensions, Inc. |

PROJECT INFO

| | |
|----------|---|
| ZONING | MR |
| OVERLAYS | Capitol Hill Station Overlay District |
| LOT SIZE | 7,676 sf Before Setback 7,636 sf After Setback |

| | |
|-----------------|-----------|
| FAR | 4.25 |
| ALLOWABLE FAR | 32,623 sf |
| PROPOSED FAR | 32,602 sf |
| PROPOSED UNITS | 77 |
| PARKING STALLS | n/a |
| BICYCLE PARKING | 57 |

PROJECT DESCRIPTION

The proposed development is a 7 story mixed-use building with 73 Small Efficiency Dwelling Units, 4 Studios, and approximately 1,500 sf of commercial. The project seeks zoning incentives based on Built Green Certification and inclusion of Affordable Housing. All existing structures to be demolished.

INDEX

| | |
|--------------------------------|--------------|
| PROJECT INFO / PROPOSAL | II |
| SITE ANALYSIS | |
| VICINITY ANALYSIS | 1 |
| ZONING MAP | 2 |
| ADJACENCIES / CIRCULATION | 3 |
| STREET ELEVATIONS | 4-6 |
| CONTEXT CHARACTER | 7-9 |
| EXISTING CONDITIONS / SURVEY | 10 |
| SITE CONDITIONS | 11 |
| ZONING STANDARDS | 12 |
| PRIORITY GUIDELINES | 13 |
| SCHEME A | 14-17 |
| SCHEME B | 18-21 |
| SCHEME C | 22-25 |
| SCHEME SUMMARY | 26 |
| DEPARTURE MATRIX | 27 |
| CONCEPT DEVELOPMENT | |
| ADJACENCIES/SITE RESPONSE | 28 |
| MASSING | 29 |
| STREET LEVEL POTENTIAL | 30 |
| ARTICULATION | 31 |
| LANDSCAPE +AMENITY | 32 |
| FORM + EXPRESSION | 33 |
| RECENT WORKS | 35 |





VICINITY ANALYSIS

Capitol Hill is one of the fastest growing neighborhoods of Seattle. It offers its community an outstanding variety of restaurants, shopping, living, and working hubs that make it an attractive destination for residents of all demographics.

The proposed project is located in the southwest portion of the Capitol Hill Urban Center Village. The site is situated just north of the Pike/Pine corridor, between the commercial areas along Broadway and East Olive Way. Transit is readily available with the Light Rail, Streetcar and buses, providing ready access to downtown & beyond.

- ▬ CAPITOL HILL URBAN CENTER VILLAGE (UCV)
- ▬ NEIGHBORHOOD BOUNDARY

ZONING

The project site is zoned Mid-Rise and is located within the Capitol Hill Urban Center Village. The adjacent and surrounding sites are also zoned Mid-Rise, eliminating any required zone transitions. The Mid-Rise area is surrounded by significant Neighborhood-Commercial zones with Pedestrian Overlays (East Olive Way, Broadway and Pike/Pine), establishing a strong mixed-use character.

Mid-Rise zones are defined as: an area that provides concentrations of housing in desirable, pedestrian-oriented urban neighborhoods having convenient access to regional transit stations, where the mix of activity provides convenient access to a full range of residential services and amenities, and opportunities for people to live within walking distance of employment.

Per SMC 23.45.504, ground floor commercial uses (including restaurants) are permitted in Mid-Rise zones.

OVERLAYS

CAPITOL HILL URBAN VILLAGE

ADDITIONAL 25' HEIGHT PERMITTED
SMC 23.47.012.A2

TRANSIT STATION 85' HEIGHT LIMIT

ZONING

LR3

MR

NCP3-65

NC3-65

MIO-105-NC3P-65

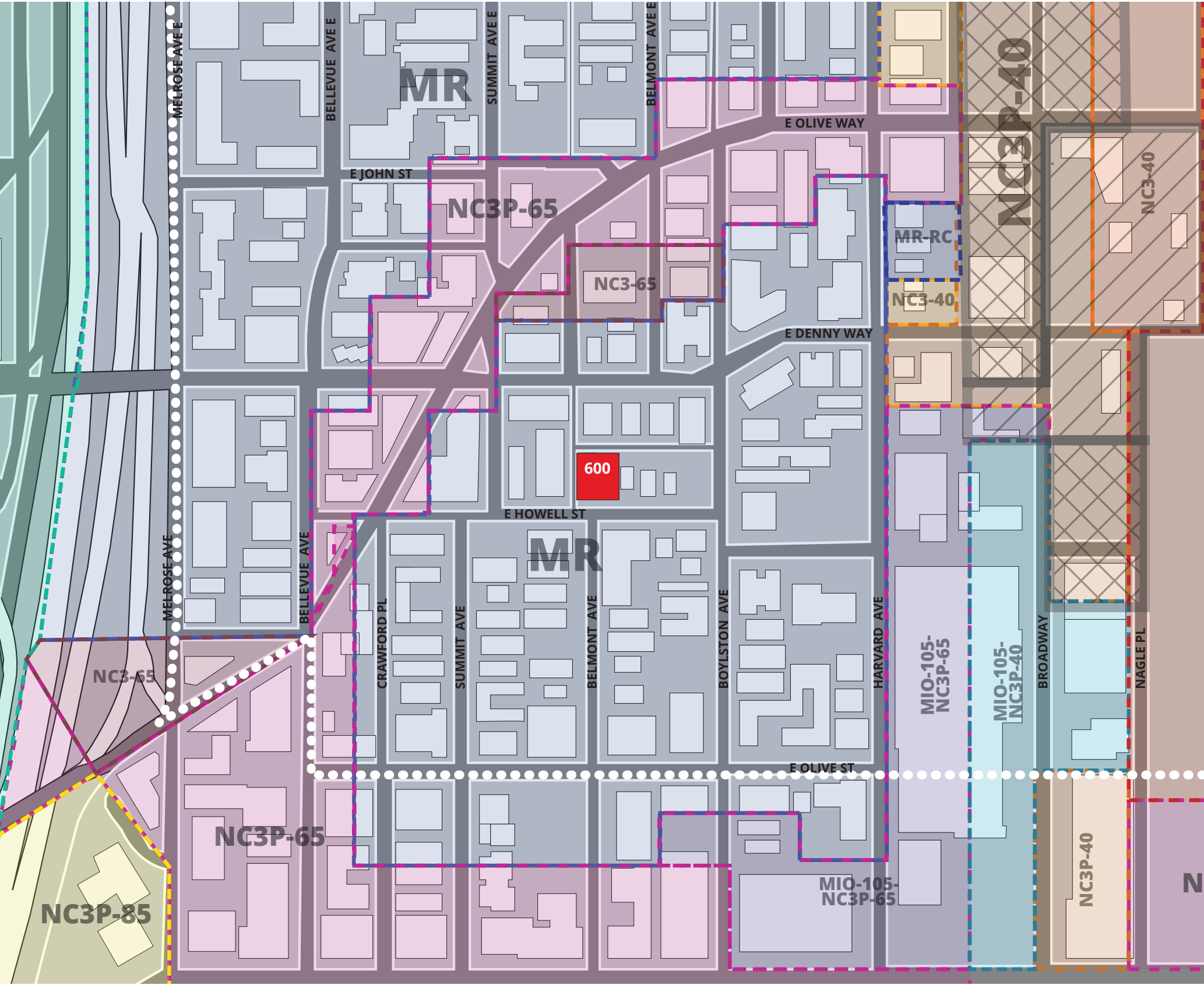
MIO-105-NC3P-40

NC3P-85

NCRP-40

NC3-40

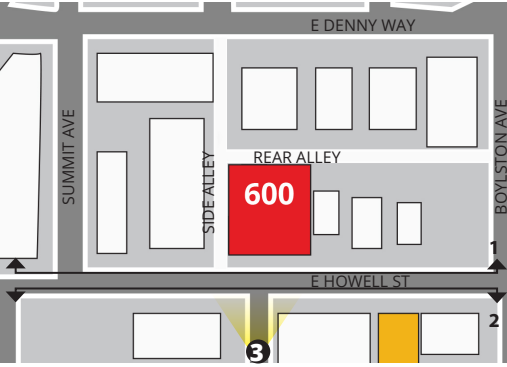
DMC



1 E HOWELL ST LOOKING NORTH



2 E HOWELL ST LOOKING SOUTH



EAST HOWELL ST

The frontage along East Howell reflects how the neighborhood has grown over time, and includes a variety of uses. The street slopes down significantly to the west. Parking is allowed on both sides of the street, and the intended R.O.W. includes a planting strip and street trees. Belmont Ave terminates at East Howell St, on axis with the proposed project.



Site from Belmont Ave

4 SIDE ALLEY LOOKING WEST



5 SIDE ALLEY LOOKING EAST



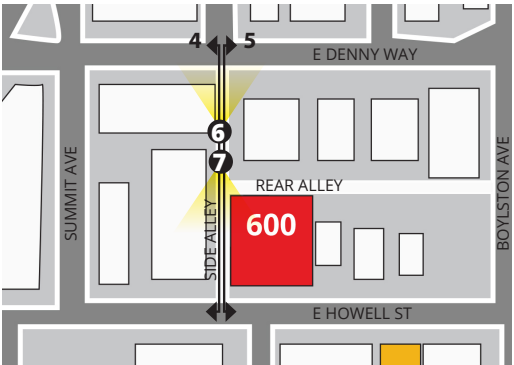
6 Side alley looking north



7 Side alley looking south

SIDE ALLEY

The alley to the west of the site acts as a continuation of Belmont Ave, and is considered a ‘side interior lot line’ for the project. Cars and pedestrians frequently use the Side Alley to continue towards East Denny Way. The alley is service oriented, including trash pickup and parking for the parcels that abut it. Despite the emphasis on service uses, the spatial character of the side alley is clearly defined.



SIDE ALLEY

600 E HOWELL ST
(across from)
MULTI-FAMILY

MULTI-FAMILY

MULTI-FAMILY

MULTI-FAMILY

BOYLSTON AVE

BOYLSTON AVE

PARKING LOT

SINGLE FAMILY

MULTI-FAMILY

MULTI-FAMILY

600 E HOWELL ST
(rear)

SIDE ALLEY

EL: 287.66'

EL: 278.18'



Rear alley looking east

ARCHITECTURAL CONTEXT

The immediate vicinity includes a variety of building typologies built over an extended period of time. The result is a neighborhood without a strong overall character, but reflective of how the city has grown. The transformative nature of the neighborhood is apparent in the following:

EARLY 20TH CENTURY HOUSES

Use: Single family, occasionally with ground-level commercial

Character: Traditional bungalow style with wood siding

Pedestrian Experience: Hard edge at sidewalk, including dense foliage and grassy planting strip



1625 Belmont Ave



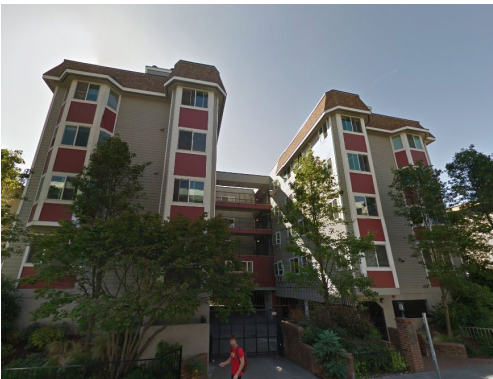
516 E Denny Way

90'S ERA APARTMENTS

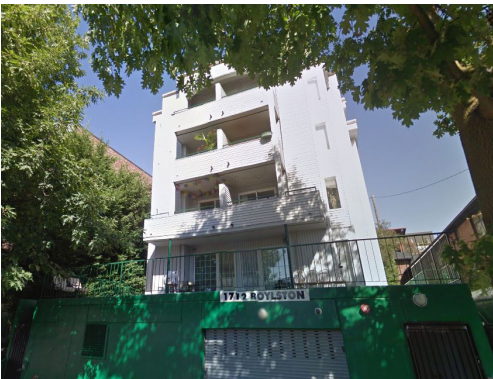
Use: Almost exclusively residential, often with enclosed ground-level parking

Character: Larger scale with prescriptive modulation including vinyl siding or EIFS

Pedestrian Experience: Little transition to the sidewalk, often abutting a blank wall with some buffer landscaping



1617 Summit Ave



1712 Boylston Ave

EARLY 20TH CENTURY APARTMENTS

Use: Almost exclusively residential, with limited ground-level commercial

Character: Large scale, typically flat brick facades with strong expression of base/middle/top

Pedestrian Experience: Little transition to the sidewalk, with formal hedges to buffer



Granada Apartments, 1736 Belmont Ave



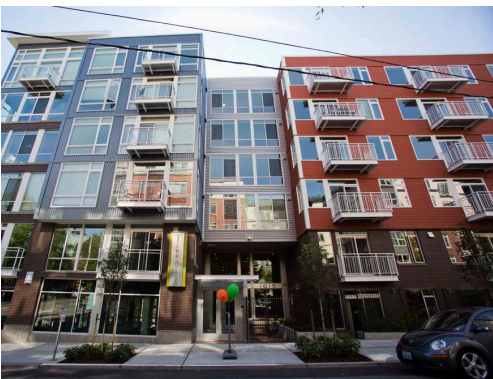
Emerald Arm Apartments, 1741 Belmont Ave.

CONTEMPORARY MIXED USE

Use: Both exclusive residential or mixed use with ground-level commercial. Parking entrances (where provided) are more discretely integrated into façade.

Character: Variety of scale and materials to break down larger sites.

Pedestrian Experience: Stronger connection to the street through ground-level uses and R.O.W. planting.



Terravita Luxury Apartments, 1615 Belmont Ave



Cue Apartments, 1525 Harvard Ave

MID-CENTURY APARTMENTS

Use: : Almost exclusively residential, often with open ground-level parking

Character: Smaller scale structures with large openings and clear expression of floor levels with contrasting materials

Pedestrian Experience: Little planting along the sidewalk, often includes a continuous curb cut or hardscape in the planting strip



Crest on Belmont, 1726 Belmont Ave



Olive Terrace, 1111 E Harrison St

COMMERCIAL IN MID-RISE

A few locations include commercial space within the multifamily structure. These are often small spaces that integrate with the overall building character.



Belroy Apartments

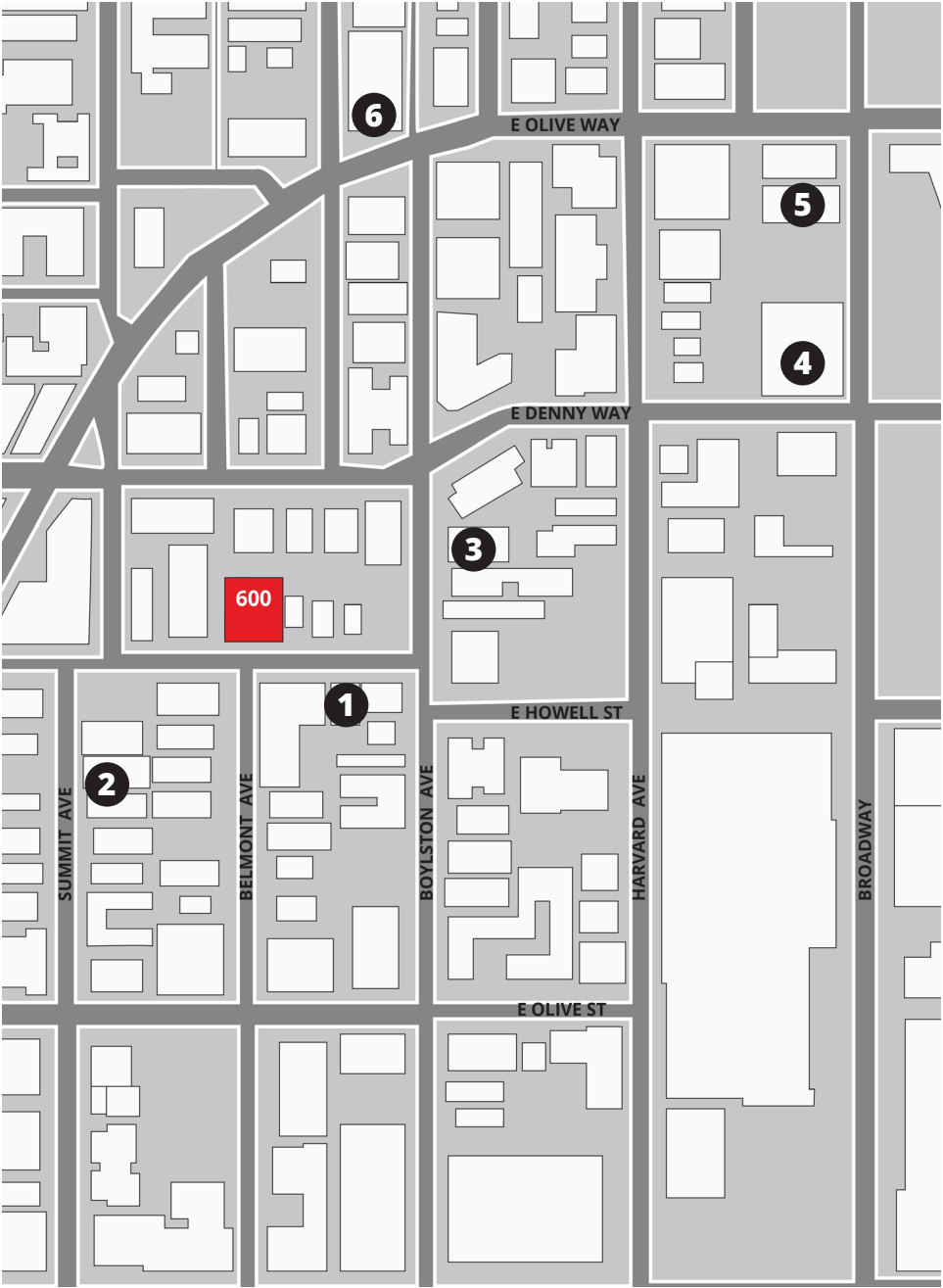


600 Summit Ave E

VICINITY CONTEXT



AERIAL SITE CONTEXT



RECENT/PROPOSED DEVELOPMENT

RECENT/ PROPOSED DEVELOPMENTS



1
3012671 - 611 E Howell St. (Integrus Architecture; under construction)



2
3013254 - 1728 Summit Ave. (Group Architect; under construction)



3
3020247 - 1820 Boylston Ave. (Nicholson Kovalchick Architect; permitting)



4
3018402 - 101 Broadway E. (Studio Meng Strazzara; under construction)



5
3013985 - 127 Broadway E



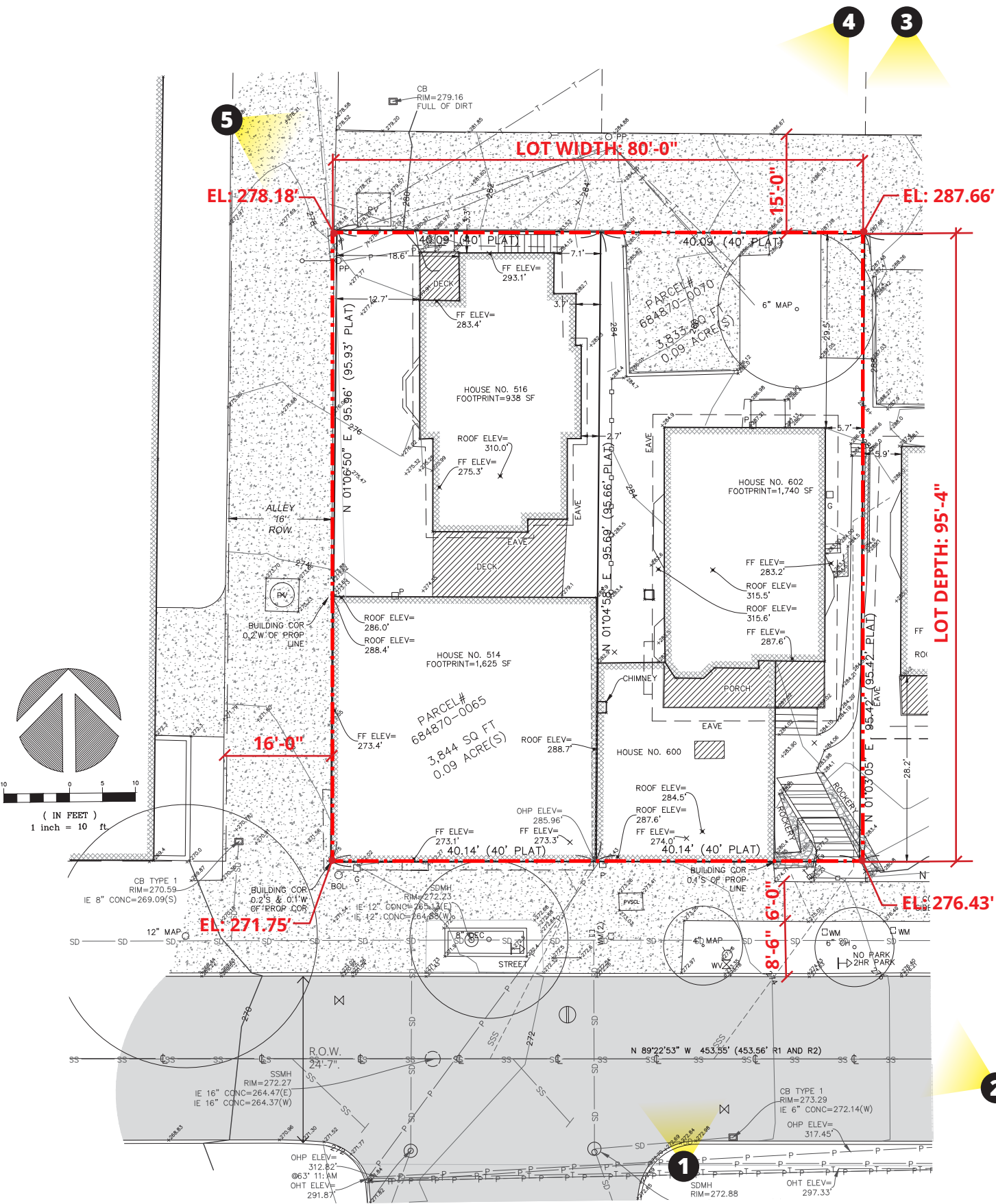
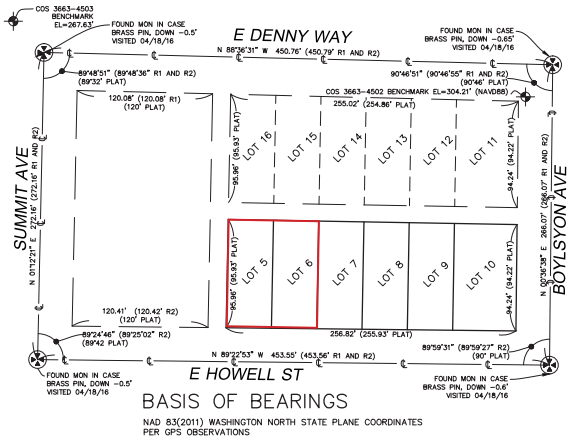
6
3002133 - 1650 E Olive Way

EXISTING CONDITIONS

The site is approximately 80'-0" x 95'-4" (7,636sf) and is located at a three way intersection with two alleys skirting the parcel. Two duplex structures and two single level commercial buildings will be removed from the site, along with any associated site features. The R.O.W. frontage includes three street trees – two of these are healthy and will remain, the third will be replaced in kind. A planting strip will be extended/provided, and a 6" sidewalk will be maintained. The Side & Rear alleys are already improved and will remain; a 6" setback is required along the Rear Alley. The site slopes up from the southwest corner to the northeast corner, creating almost 16' of cross slope on the site.

LEGAL DESCRIPTION

LOT 5 & 6, BLOCK 50, REPLAT OF BLOCKS 49, 50, AND 51, IN SUPPLEMENTARY PLAT OF PONTIUS' SECOND ADDITION TO SEATTLE, ACCORDING TO THE PLAT THEREOF RECORDED IN VOLUME 9 OF PLATS, PAGE 13, IN KING COUNTY WASHINGTON.

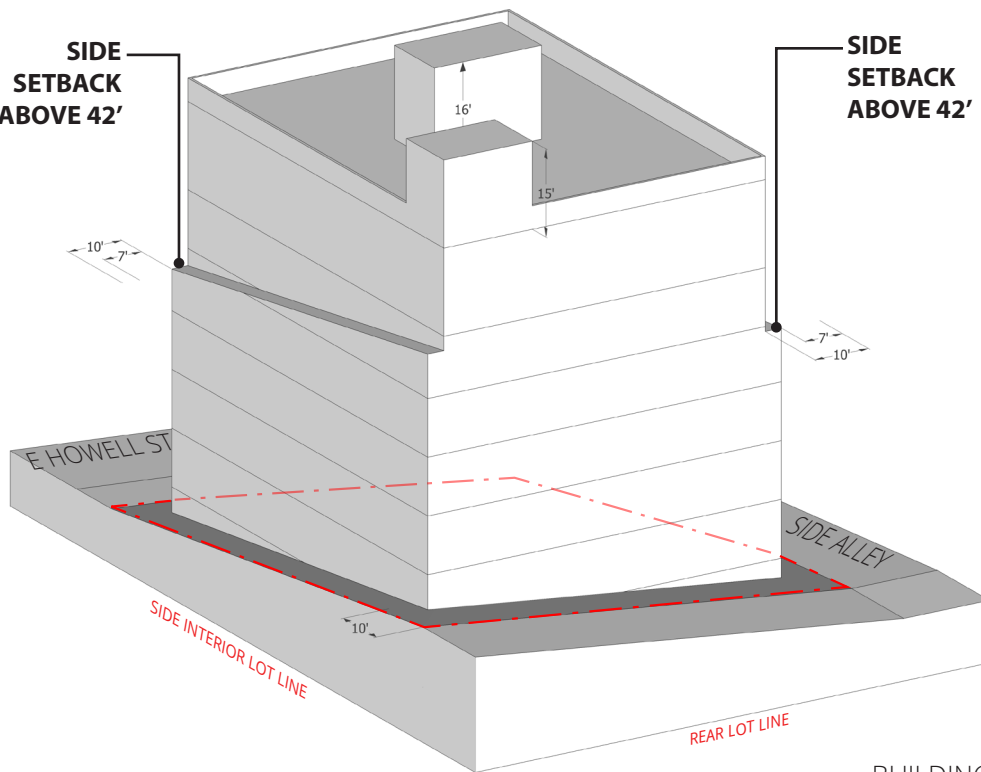
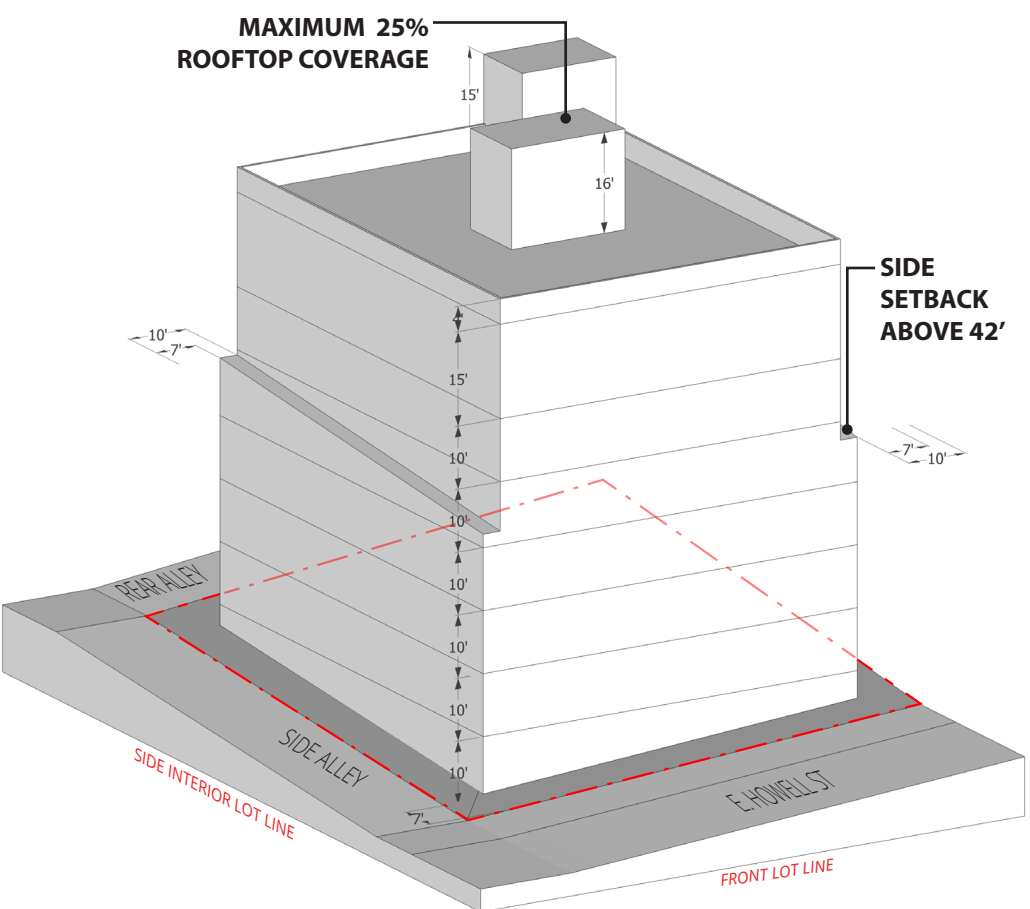


SITE CONDITIONS



ZONING: MID-RISE ZONE
OVERLAYS: CAPITOL HILL URBAN CENTER VILLAGE, FREQUENT TRANSIT

| SMC Code | Definition |
|---------------|--|
| SMC 23.41.004 | DESIGN REVIEW THRESHOLD - MR zone, >20 dwelling units or 4,000sf of non-residential gfa. |
| SMC 23.45.504 | PERMITTED USES Uses permitted outright: residential, ground floor commercial. |
| SMC 23.45.510 | FLOOR AREA RATIO - Base 3.2 FAR * 4.25 FAR allowed, with affordable housing and Built Green 4-Star certification (7,676 lot area x 4.25 allowable FAR = 32,623) - Area exempt from FAR: All underground stories and portions of a story that extend no more than 4' above existing or finished grade, whichever is lower. |
| SMC 23.45.514 | HEIGHT - Base height limit of 60' above average grade. * Additional 15' of height with affordable housing and Built Green 4-Star certification. - Parapets, railings etc. may extend 4' above the allowed height limit. - Elevator penthouses may extend 16' above the allowed height limit. - Stair penthouse may extend 15' above the allowed height limit. - Maximum of 20% rooftop coverage, maximum of 25% with screened mechanical equipment. |
| SMC 23.45.545 | ROOF SOLAR COLLECTORS - Allowed to be located up to 10' above maximum height limit. Height limit is at roof or elevator penthouse, whichever is higher. |
| SMC 23.45.518 | SETBACKS - Front lot line: 7' average, 5' minimum. - Rear: 15' from lot line that does not abut an alley and 10' minimum with an alley. - Side Interior lot line: Below 42': 7' average, 5' minimum; Above 42': 10' average, 7' minimum. |
| SMC 23.45.522 | AMENITY AREA - 5% of total floor area in residential use shall be provided as amenity area. - All units shall have access to a common or private amenity area. - No more than 50% of the amenity area may be enclosed, enclosed area shall be provided as common amenity. - Common Amenity: minimum 250 sf, 10' minimum dimension. At least 50% of common amenity at ground level shall be landscaped. - Private decks & balconies shall be 60 sf minimum, 6' minimum dimension. |
| SMC 23.45.524 | LANDSCAPING - 0.50 Green Factor required. - Street trees required. |
| SMC 23.45.526 | BUILT GREEN COMMITMENT - Built green 4-star required for new development gaining extra residential floor area. |
| SMC 23.45.532 | GROUND FLOOR COMMERCIAL - Location limited to the ground floor. On sloping lots this may be located on multiple levels where total floor area does not exceed the area of the structure's footprint. - Maximum size of one business establishment < 4,000sf or < 10,000sf for multipurpose retail establishment. - Vents shall be >10' above finished sidewalk grade and directed away from residential uses within 50' of venting. |
| SMC 23.45.534 | LIGHTING AND GLARE - Exterior lighting shall be shielded and directed away from adjacent properties. |
| SMC 23.54.015 | REQUIRED PARKING - No vehicular parking required (Urban Center Village + Frequent Transit) - Bicycle Parking - Residential, Long Term: 1 space per 4 dwelling units (1); Small Efficiency Dwelling Unit: 0.75 per unit (73 units x 0.75 = 55 bicycle spots); Commercial Eating/Drinking: 1 per 2,000sf (1 bicycle spot); Total required spots = 57 |
| SMC 23.54.040 | SOLID WASTE & RECYCLING - 375sf + 4 sf per unit above 50: 375 + (27(4)) = 483 sf; Commercial (0-5,000sf): 82sf/2 = 41sf =524sf - 12' minimum horizontal dimension. * Required area may be reduced by 15% if 20' minimum horizontal dimension is met. |



PRIORITY GUIDELINES: SEATTLE CITY-WIDE + CAPITOL HILL NEIGHBORHOOD DESIGN GUIDELINES

CS1: NATURAL SYSTEMS & SITE FEATURES

C.2: TOPOGRAPHY/ELEVATION CHANGES: THE SITE IS SLOPED ON ALL SIDES, MOST DRAMATICALLY FROM THE SW CORNER (LOW) TO THE NE CORNER (HIGH). THE PROPOSED **PROJECT ACCOMMODATES THE TOPOGRAPHY INTO THE DESIGN** FOR ENTRY POINTS, AND STEPS IN HEIGHT AT THE UPPER STORIES TO REFLECT THE CHANGE IN SLOPE.

CS2: URBAN PATTERN & FORM

A.2: ARCHITECTURAL PRESENCE: THE **PROJECT WILL BE HIGHLY VISIBLE** FROM MULTIPLE APPROACHES, INCLUDING EAST HOWELL ST (EAST & WEST APPROACHES), BELMONT AVE (SOUTH APPROACH ENDING AT A 3 WAY INTERSECTION) AND ABUTTING THE WEST ALLEY (SERVES AS SECONDARY NORTH/SOUTH CONNECTION).

C.1/2: CORNER SITE AND MID/BLOCK SITE: DUE TO THE **UNIQUE STREET CONFIGURATION**, THE SITE ACTS AS BOTH A CORNER SITE AS WELL AS A MID-BLOCK SITE. AS A CORNER SITE, THE PROPOSAL CALLS FOR SPECIAL CONSIDERATION AT THE SOUTH AND WEST LOWER LEVELS. AS A MID-BLOCK SITE, THE TREATMENT OF THE STREET FACING FACADE MUST BE INTEGRATED INTO THE STREET EDGE.

D.5 RESPECT FOR ADJACENT SITES: **LARGE MASSING SETBACKS** ARE PROVIDED AT THE EAST AND WEST SIDES OF THE SITE, WHERE ADJACENT RESIDENTIAL USES HAVE THE GREATEST PROXIMITY.

CAP. HILL CS2.I.I: STREETScape COMPATIBILITY: THE RESIDENTIAL ENTRY IS RECESSED FROM THE STREET TO BUFFER ITSELF FROM THE SIDEWALK; THE USE OF LANDSCAPING SCREENS YET MAINTAINS VIEWS INTO THE COURTYARD. SMALL NEIGHBORHOOD GROCERY AND RESTAURANT SPACES AWAY FROM PRIMARY COMMERCIAL AREAS THRIVE IN CAPITOL HILL. RE-ESTABLISHING THE COMMERCIAL FRONTAGE ON E. HOWELL ST. ACKNOWLEDGES THIS CHARACTERISTIC OF THE NEIGHBORHOOD AND MAINTAINS A **STRONG CONNECTION TO THE STREET** SEEN FROM BLOCKS AWAY.

CAP. HILL CS2.III: HEIGHT, BULK, SCALE COMPATIBILITY: THE **OVERALL BUILDING MASSING IS ARTICULATED** TO REDUCE BULK. THE BUILDING MASS AT THE COMMERCIAL FRONTAGE IS PUSHED FORWARD TO RE-ESTABLISH THE STREET EDGE AND BUILDING SCALE OF TYPICAL EXISTING COMMERCIAL SPACE IN THE NEIGHBORHOOD.

CS3: ARCHITECTURAL CONTEXT & CHARACTER

A.1: FITTING OLD AND NEW TOGETHER: SCALE AND PROPORTION, AS WELL AS DETAILING AND FENESTRATION PATTERNS, WILL **COMPLIMENT EXISTING ARCHITECTURAL CONTEXT AND ESTABLISH A NEW LANGUAGE** FOR FUTURE DEVELOPMENT ON THE BLOCK.

A.4: EVOLVING NEIGHBORHOODS: THE IMMEDIATE VICINITY ILLUSTRATES THE HISTORIC TIME-LINE OF GROWTH AND ARCHITECTURAL STYLES OF THE NEIGHBORHOOD. THE PROJECT CAN SET A **PRECEDENT FOR FUTURE DEVELOPMENT** AS A CONTEMPORARY ADDITION.

PL1: CONNECTIVITY

B.1: PEDESTRIAN INFRASTRUCTURE: SIDEWALK EXPERIENCE TO BE ENHANCED, INCLUDING **EXTENSION OF THE PLANTING STRIP AND HARDSCAPE** FOR THE 3-WAY CONNECTION FROM BELMONT AVE.

C.1: SELECTING ACTIVITY AREAS: PRIMARY ENTRIES FOR RESIDENTIAL AND COMMERCIAL USE ARE PLACED IN DIRECT LINE WITH PEDESTRIAN ROUTES, WITH A HIGH DEGREE OF VISUAL CONNECTION TO THE STREET. RESIDENTIAL COMMON AMENITY SPACE IS PROVIDED AT THE LOWER LEVEL WITH A VISIBLE CONNECTION TO THE STREET.

PL2: WALKABILITY

B.3: STREET-LEVEL TRANSPARENCY: THE PRIMARY RESIDENTIAL ENTRY WILL BE **TRANSPARENT AND READILY VISIBLE FROM THE SIDEWALK**. THE COMMERCIAL CORNER AT THE SIDE ALLEY WILL ALSO BE TRANSPARENT, FURTHER ENHANCING VISIBILITY FOR PEDESTRIANS AND VEHICLES USING THE ALLEY.

CAP. HILL PL2.I: HUMAN SCALE: THE RESIDENTIAL ENTRY IS IDENTIFIED BY A LARGE BUILDING OVERHANG BALANCED WITH HARDSCAPE AND LANDSCAPE ELEMENTS. THE COMMERCIAL ENTRY IS PROUD TO THE STREET WITH A DISTINCT, RECESSED ENTRY. A CANOPY FOR OVERHEAD PROTECTION WILL BE PROVIDED TO FURTHER FRAME THE COMMERCIAL ENTRY AND **WELCOME PEDESTRIANS**.

CAP. HILL PL2.II: PEDESTRIAN OPEN SPACE AND ENTRANCES: LANDSCAPING PATTERNS ESTABLISHED ON SITE WILL BE BROUGHT INTO THE RIGHT OF WAY TO REINFORCE THE **LINK BETWEEN BUILDING ENTRANCES AND THE STREET**.

PL3: STREET LEVEL INTERACTION

A.1: ENTRIES TO RETAIL AND RESIDENTIAL SPACES: THE PRIMARY RESIDENTIAL ENTRY WILL INCLUDE LARGE WINDOWS FOR VISIBILITY. A DEEP RECESS PROVIDES WEATHER PROTECTION, A LANDSCAPE BUFFER PROVIDES PRIVACY, AND GATES PROVIDE SECURITY AT SECONDARY ACCESS POINTS. THE RECESSED COMMERCIAL ENTRY PROVIDES OVERHEAD PROTECTION AND ADEQUATE ROOM FOR GROUPS OF PEOPLE TO ENTER OR EXIT DIRECTLY TO THE STREET.

B.1: SECURITY & PRIVACY AT RESIDENTIAL EDGES: THE RESIDENTIAL ENTRY OFF E. HOWELL ST. IS SET BACK FROM THE STREET AND THE EAST NEIGHBOR. THE WEST BUILDING EDGE PROVIDES ACCESS TO BUILDING SERVICES WITH RESIDENTIAL USES SET BACK ABOVE, AWAY FROM THE ALLEY. THE NORTH RESIDENTIAL EDGE IS BUFFERED FROM THE ALLEY WITH SCREENING AND LANDSCAPE ELEMENTS.

B.2: GROUND LEVEL RESIDENTIAL: RESIDENTIAL AT THE NORTHEAST CORNER WILL FEATURE **SCREENING & SECURITY ELEMENTS**, INCLUDING LANDSCAPE BUFFERS AND FENCING.

C.1: RETAIL/POROUS EDGES: THE COMMERCIAL FRONTAGE IS TRANSPARENT ALONG THE SIDEWALK ALONG THE WEST ALLEY TO ENGAGE PEDESTRIANS.

PL4: ACTIVE TRANSPORTATION

A.2: CONNECTION TO ALL MODES OF TRAVEL: RESIDENTIAL ENTRY IS DIRECTLY CONNECTED TO PRIMARY STREET FRONTAGE, WITH **EASY ACCESS TO TRANSIT FACILITIES & NEARBY COMMERCIAL AREAS**.

B.2: BIKE FACILITIES: SECONDARY EXTERIOR ENTRY TO INTERIOR BIKE STORAGE (ADJACENT TO PRIMARY RESIDENTIAL ENTRY) INCREASES **CONVENIENCE FOR BICYCLISTS**. A EXTERIOR GATE ENHANCES SECURITY.

DC1: PROJECT USES & ACTIVITIES

A.1: VISIBILITY / ARRANGEMENT OF INTERIOR USES: THE LEASING OFFICE, MAIL, AND BIKE STORAGE ARE LOCATED DIRECTLY OFF THE RESIDENTIAL LOBBY, ALL OF WHICH ARE VISIBLE FROM THE ENTRY. COMMERCIAL USES LOCATED DIRECTLY AT E. HOWELL STREET PROVIDE A STRONG VISIBLE CONNECTION TO THE STREET.

C.4: SERVICE USES: ACCESS TO **SERVICE AND MECHANICAL ROOMS ARE LOCATED AT OR BELOW GRADE ALONG THE SIDE ALLEY** WHERE THE WEST NEIGHBOR'S BLANK WALLS AND PARKING GARAGE ENTRANCE ARE LOCATED.

DC2: ARCHITECTURAL CONCEPT

A.2: REDUCING PERCEIVED MASS: THE BUILDING MODULATION RESPONDS TO THE SITE CONSTRAINTS AND IS **ARTICULATED TO REDUCE OVERALL MASS** FROM MULTIPLE VIEWPOINTS.

B.1: FACADE COMPOSITION: THE ARTICULATION CONCEPT ALLOWS **EACH FACADE TO EXPRESS THE BUILDING'S CIRCULATION CONFIGURATION**. OVERALL MODULATION IS BALANCED & PROPORTIONAL.

D.1: HUMAN SCALE: SPECIAL CONSIDERATION IS GIVEN TO THE **SCALE AND PROPORTION OF THE LOWER STORIES FACING THE STREET** BY BALANCING THE RECESSED RESIDENTIAL ENTRY AND PROMINENT COMMERCIAL MASS. THESE ELEMENTS PROVIDE OPPORTUNITIES FOR SPECIAL MATERIALS, LANDSCAPING AND OTHER DETAILS TO ENHANCE THE PEDESTRIAN EXPERIENCE.

DC3: OPEN SPACE CONCEPT

C.2: AMENITIES & FEATURES: **ENTRY SEQUENCING AT GRADE** WILL FEATURE SEATING, LANDSCAPE WALLS, LIGHTING & PLANTING TO ESTABLISH YEAR ROUND INTEREST. A VARIETY OF ROOF DECK SPACES WILL ENHANCE RESIDENT EXPERIENCE. LARGE GATHERING AREAS ARE PLACED AWAY FROM RESIDENTIAL NEIGHBORS FOR INCREASED PRIVACY.

CAP. HILL DC3.II: LANDSCAPE DESIGN: A ENHANCED PLANTING STRIP, COMPLIMENTED BY A COURTYARD-LIKE ENTRY WILL ESTABLISH A **NEW AND ROBUST LANDSCAPE PRECEDENT AT THE STREET**.

DC4: EXTERIOR ELEMENTS AND FINISHES

B.2: SIGNAGE / COORDINATION WITH BUILDING: RESIDENTIAL AND COMMERCIAL **SIGNAGE WILL BE BALANCED AND APPROPRIATE** FOR THE CONTEXT AND WILL INTEGRATE INTO THE DESIGN COMPOSITION.

C.2: LIGHTING / AVOIDING GLARE: LIGHTING WILL EMPHASIZE THE BUILDING ENTRIES AND ENHANCE THE LANDSCAPE AND SIDEWALK EXPERIENCE. MEASURES WILL BE TAKEN TO AVOID GLARE ON NEIGHBORING BUILDINGS.

CAP. HILL DC4.II: EXTERIOR MATERIALS: THE PROPOSED MASSING SCHEMES PROVIDE AN **OPPORTUNITY TO DEVELOP A MATERIAL PALETTE** WITH A VARIETY OF SIMPLE MATERIALS TO PROVIDE TEXTURE AND INTEREST.

SCHEME A: CENTERED (COMPLIANT)

33,704 GSF

72 UNITS

1,047 SF COMMERCIAL SPACE

3,004 SF- TOTAL AMENITY

2,166 SF - ROOF DECK

441 SF - LOBBY

397 SF - LANDSCAPED REAR YARD

4.0 FAR - 30,468 SF GFA

POSITIVE

- No departures required
- Landscape wraps around entire building

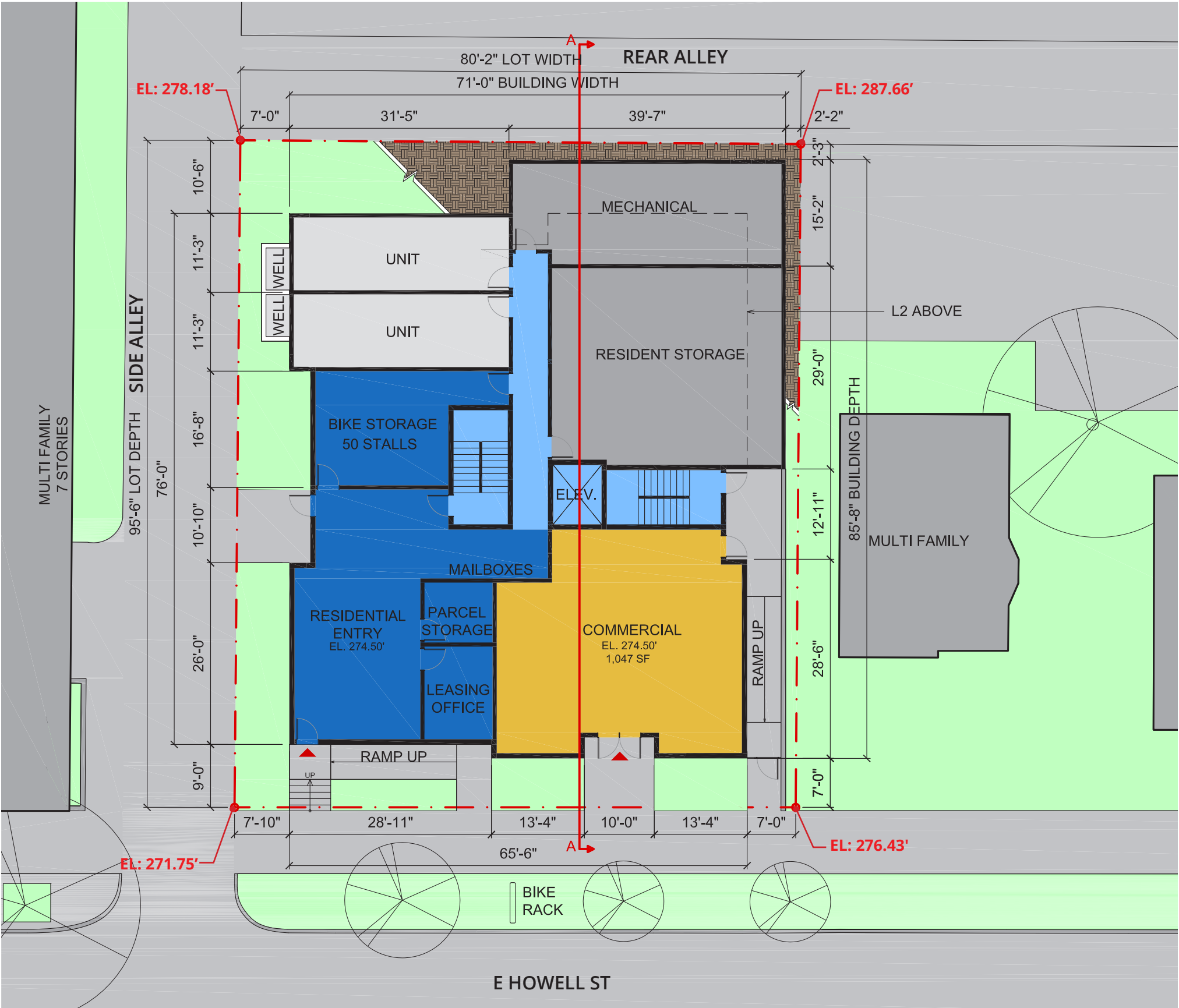
NEGATIVE

- Front setback not in character with context
- Setback at side and rear alley not desirable as amenity
- At grade units are hard to buffer
- Less opportunities for modulation of massing
- Commercial located closer to adjacent residential uses
- Trash area located closer to adjacent residential uses and does not activate corner

- RESIDENTIAL
- INTERIOR CIRCULATION
- COMMERCIAL
- COMMON
- BUILDING SERVICE
- PRIMARY BUILDING ENTRANCE

SITE / STREET LEVEL PLAN

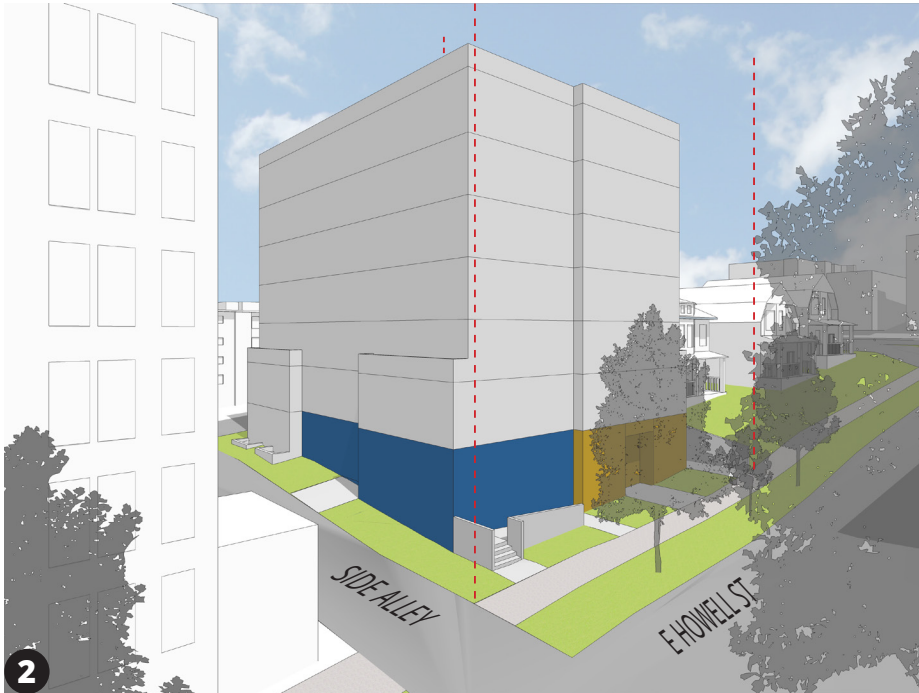
SCALE: 1/16" = 1'-0"



PERSPECTIVES: SCHEME A: CENTERED (COMPLIANT)



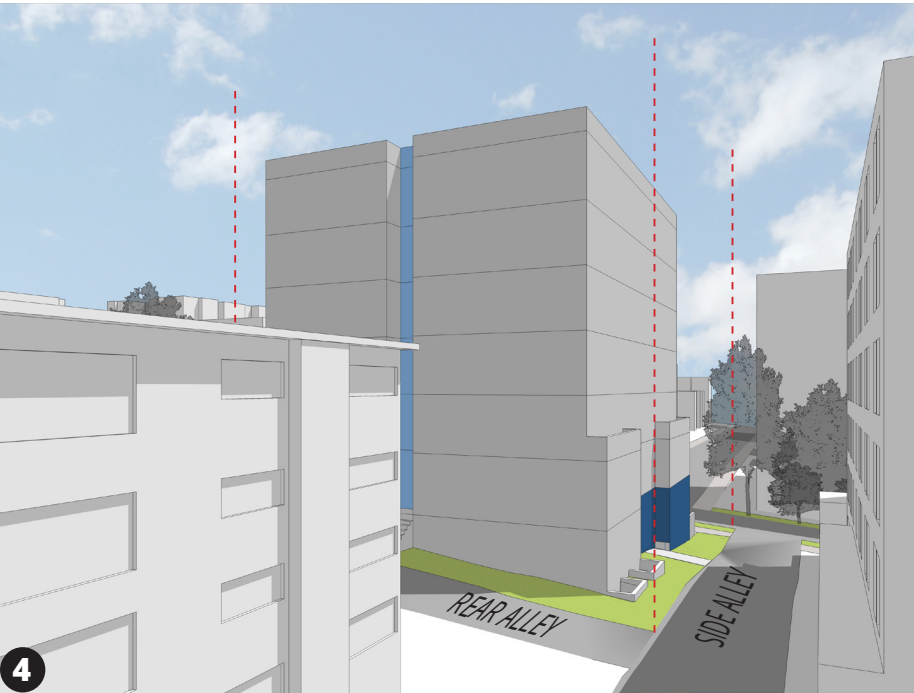
PERSPECTIVE LOOKING AT SOUTHEAST CORNER



PERSPECTIVE LOOKING AT SOUTHWEST CORNER



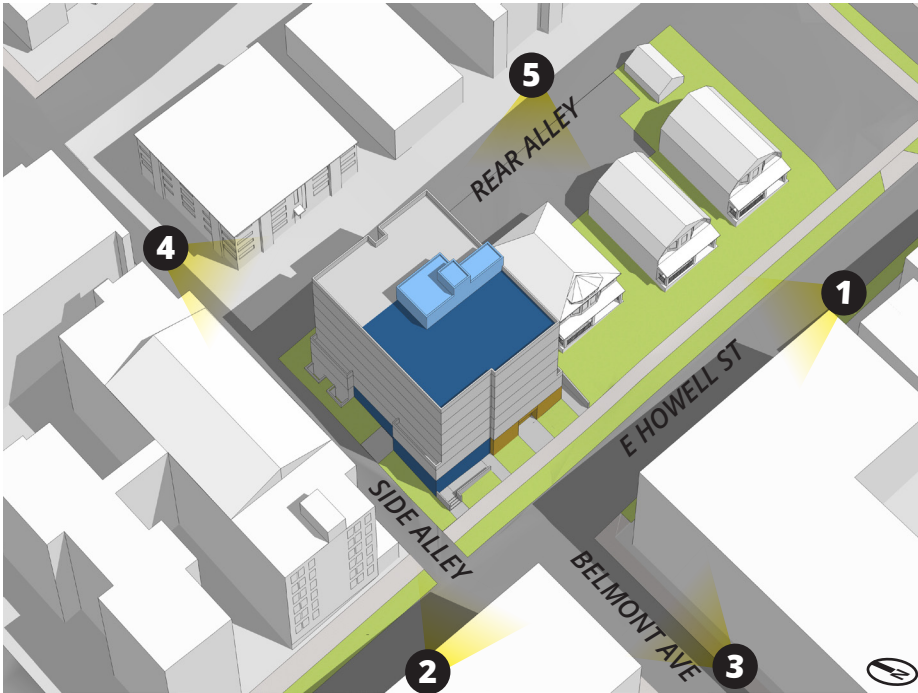
PERSPECTIVE LOOKING FROM BELMONT AVE



PERSPECTIVE LOOKING AT NORTHWEST CORNER



PERSPECTIVE LOOKING AT NORTHEAST CORNER

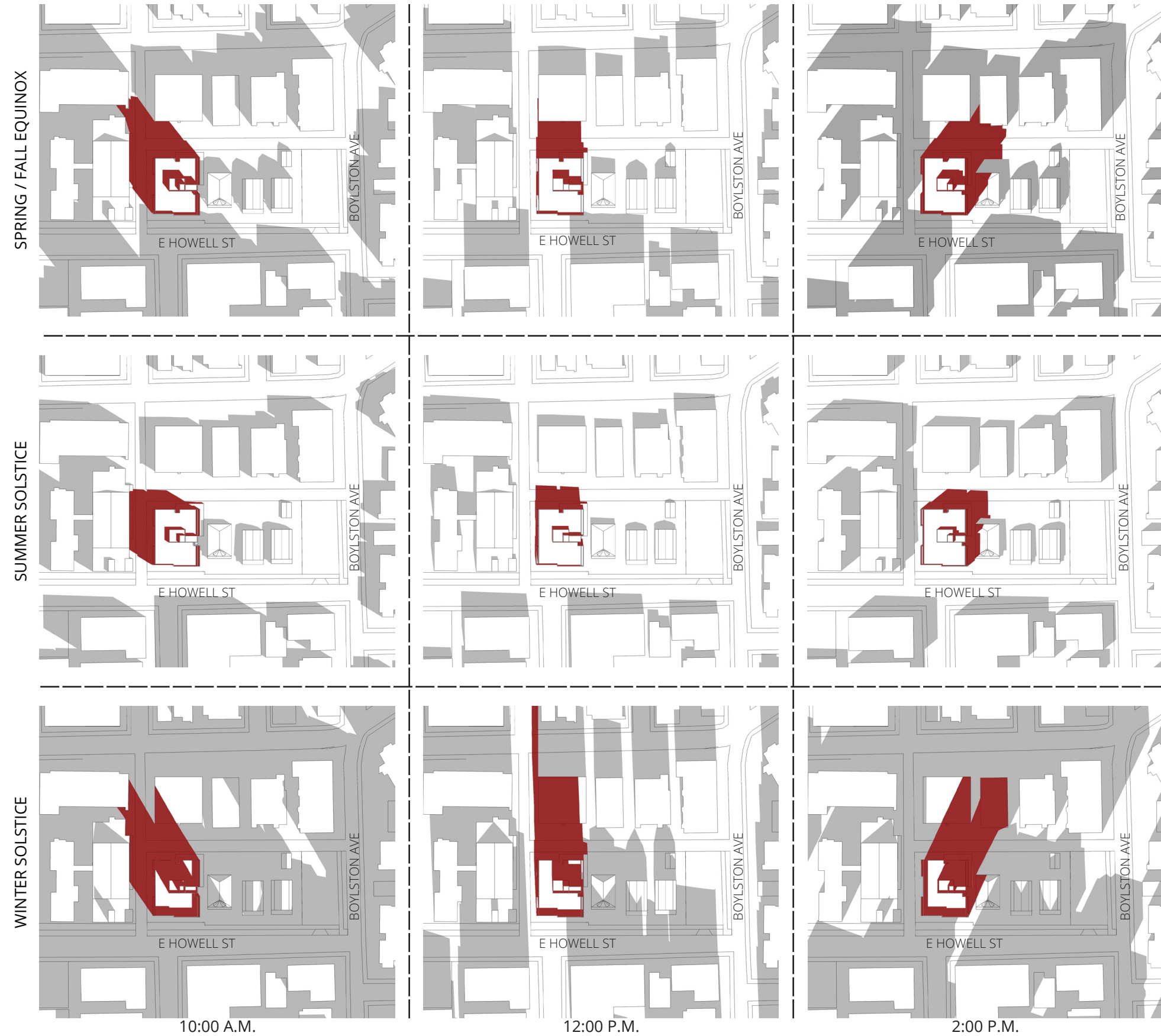


SCHEME A BIRDSEYE

DEPARTURES: SCHEME A

NO DEPARTURES REQUESTED

SUN PATH / SHADOW STUDY: SCHEME A



SCHEME B: COMPACT & OFFSET

35,784 GSF

78 UNITS

1,065 SF COMMERCIAL SPACE

3,033 SF- TOTAL AMENITY

1,694 SF - ROOF DECK

305 SF - LOBBY

196 SF - PRIVATE DECKS

838 SF - ENTRY COURT







4.25 FAR - 32,619 SF GFA

POSITIVE

- Building footprint open provides relief from the street
- Highest number of units facing street
- Variety of amenity spaces
- Building mass located near front and west property lines is more consistent with precedent, provides buffer to east neighbor
- Commercial at southwest corner is balanced buffer to residential neighbors
- Units at northeast corner activate the alley

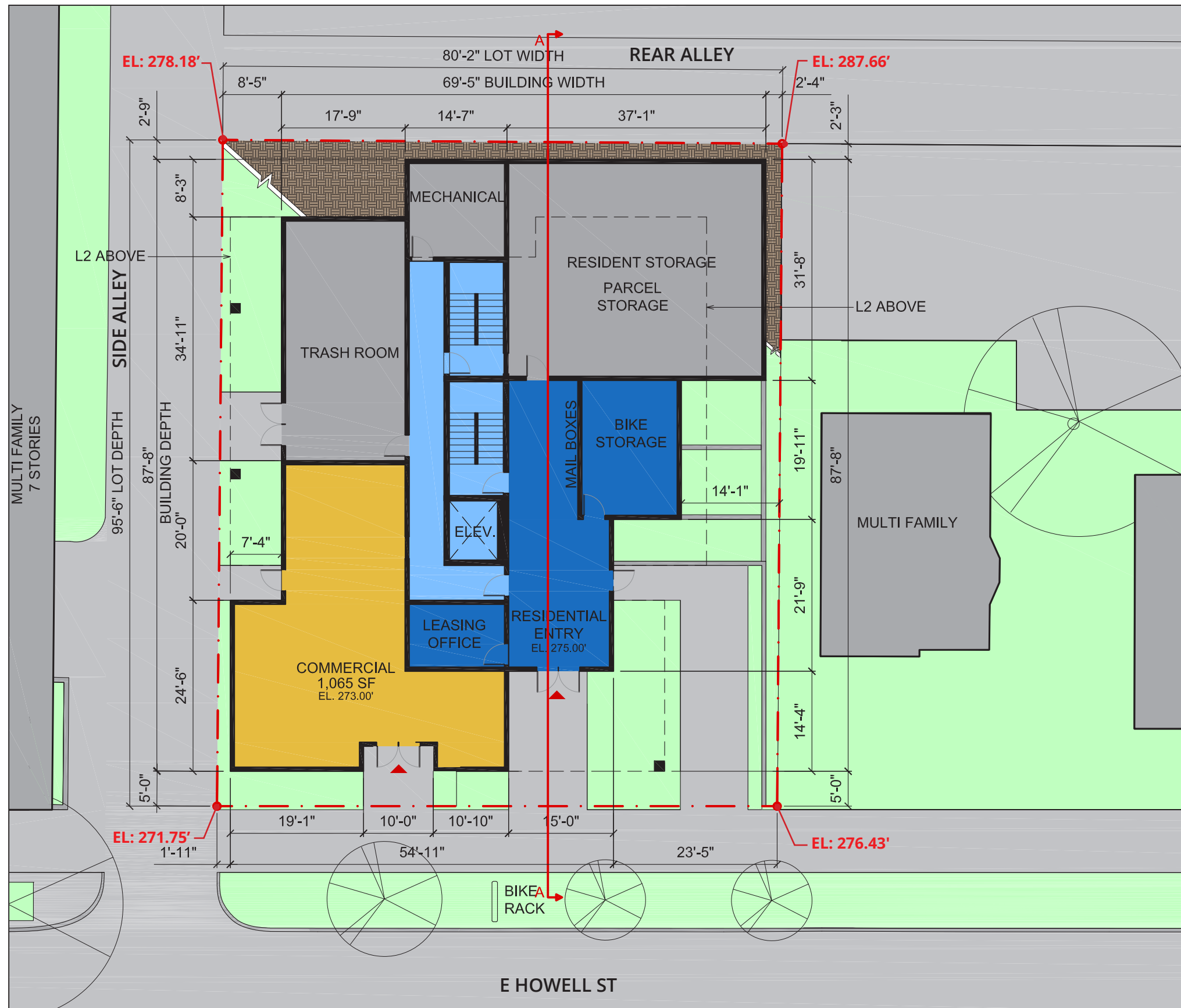
NEGATIVE

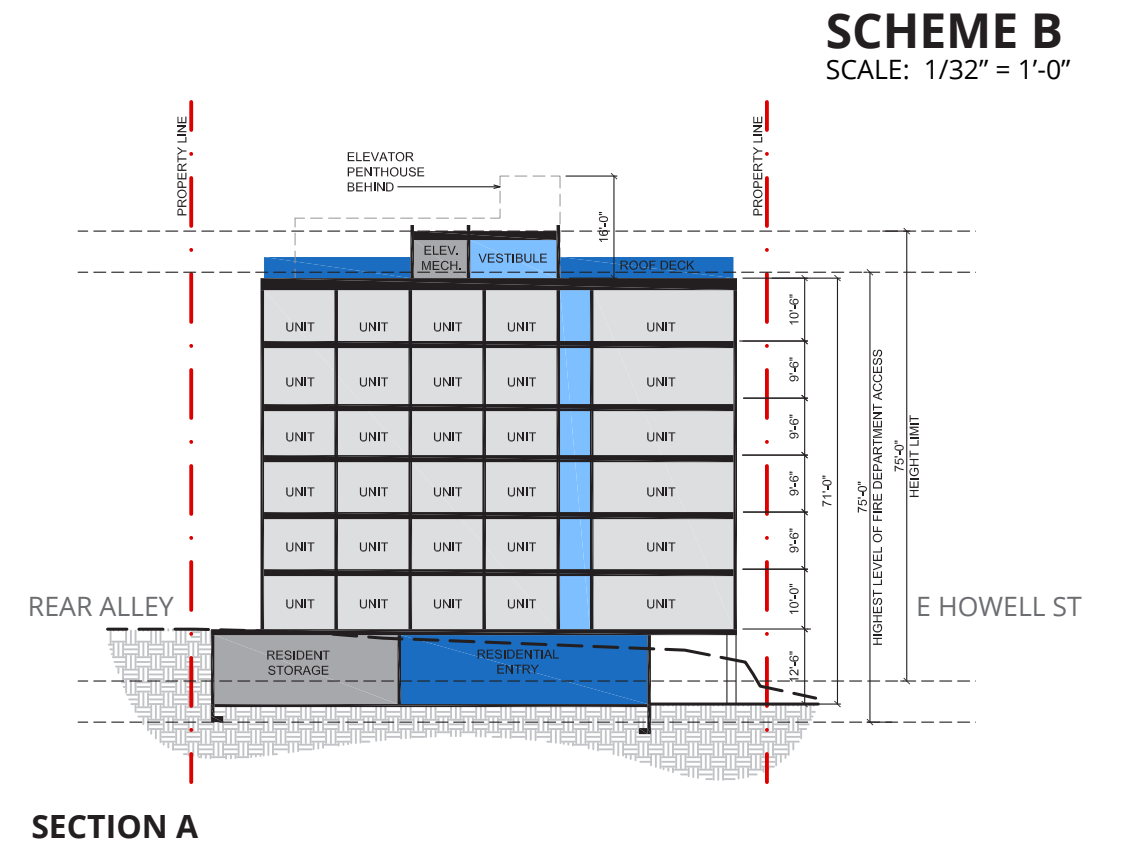
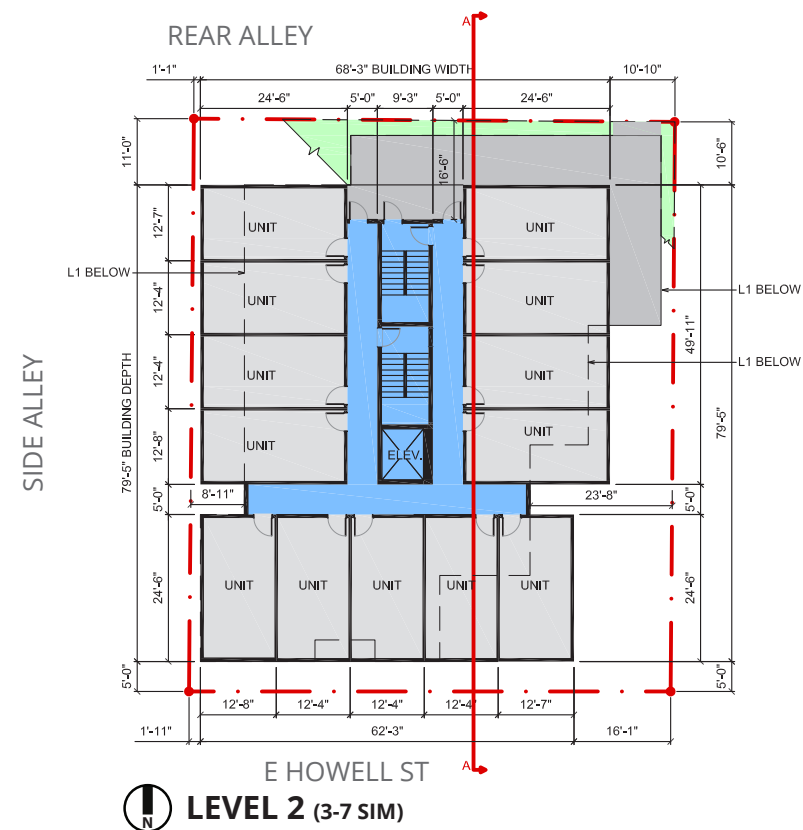
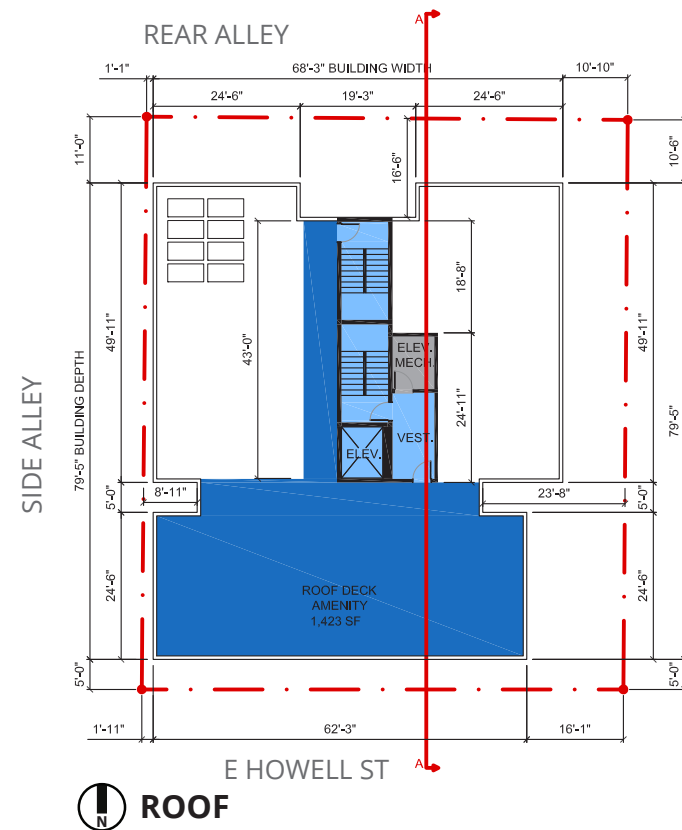
- Departures requested
- Open space @ grade could present a security issue
- Massing has more bulk at upper levels, less opportunity for modulation
- Commercial setback does not engage the sidewalk

-  RESIDENTIAL
-  INTERIOR CIRCULATION
-  COMMERCIAL
-  COMMON
-  BUILDING SERVICE
-  PRIMARY BUILDING ENTRANCE

SITE / STREET LEVEL PLAN

SCALE: 1/16" = 1'-0"





PERSPECTIVES: SCHEME B: COMPACT & OFFSET



PERSPECTIVE LOOKING AT SOUTHEAST CORNER



PERSPECTIVE LOOKING AT SOUTHWEST CORNER



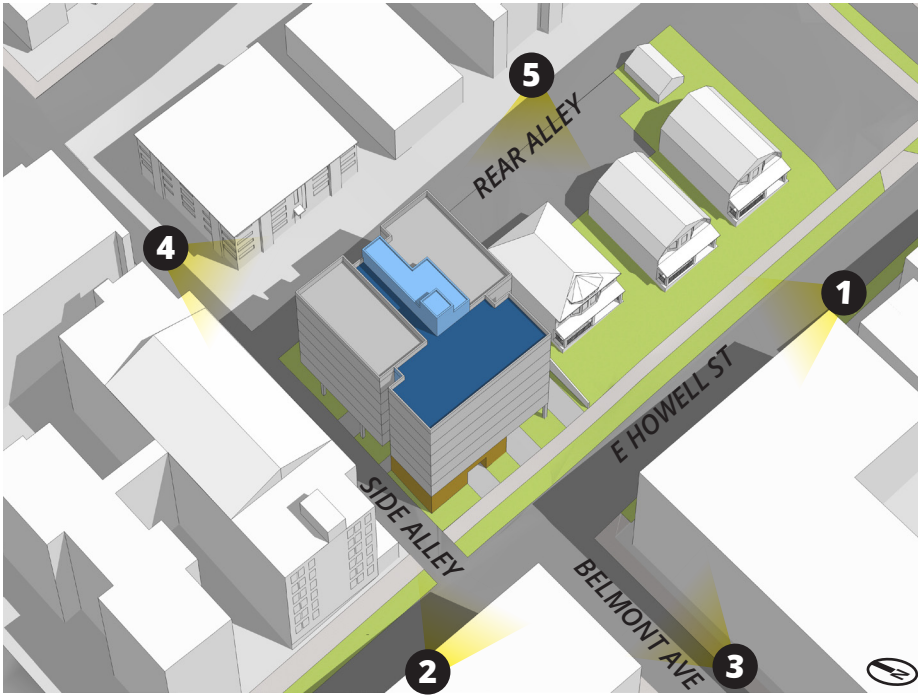
PERSPECTIVE LOOKING FROM BELMONT AVE



PERSPECTIVE LOOKING AT NORTHWEST CORNER



PERSPECTIVE LOOKING AT NORTHWEST CORNER



SCHEME B BIRDSEYE

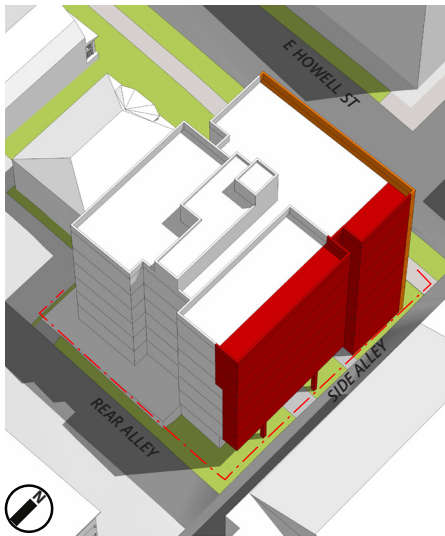
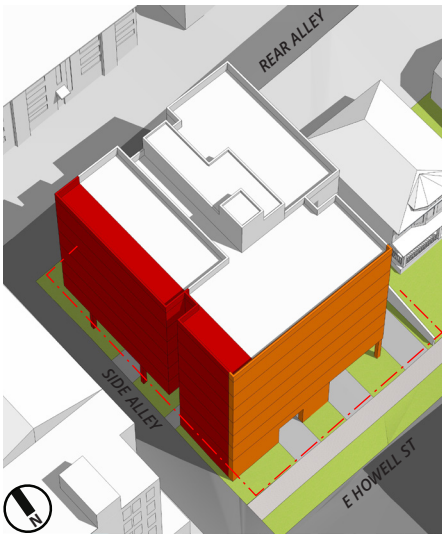
DEPARTURES: SCHEME B

*SEE GUIDELINES AND JUSTIFICATION, PAGE 27

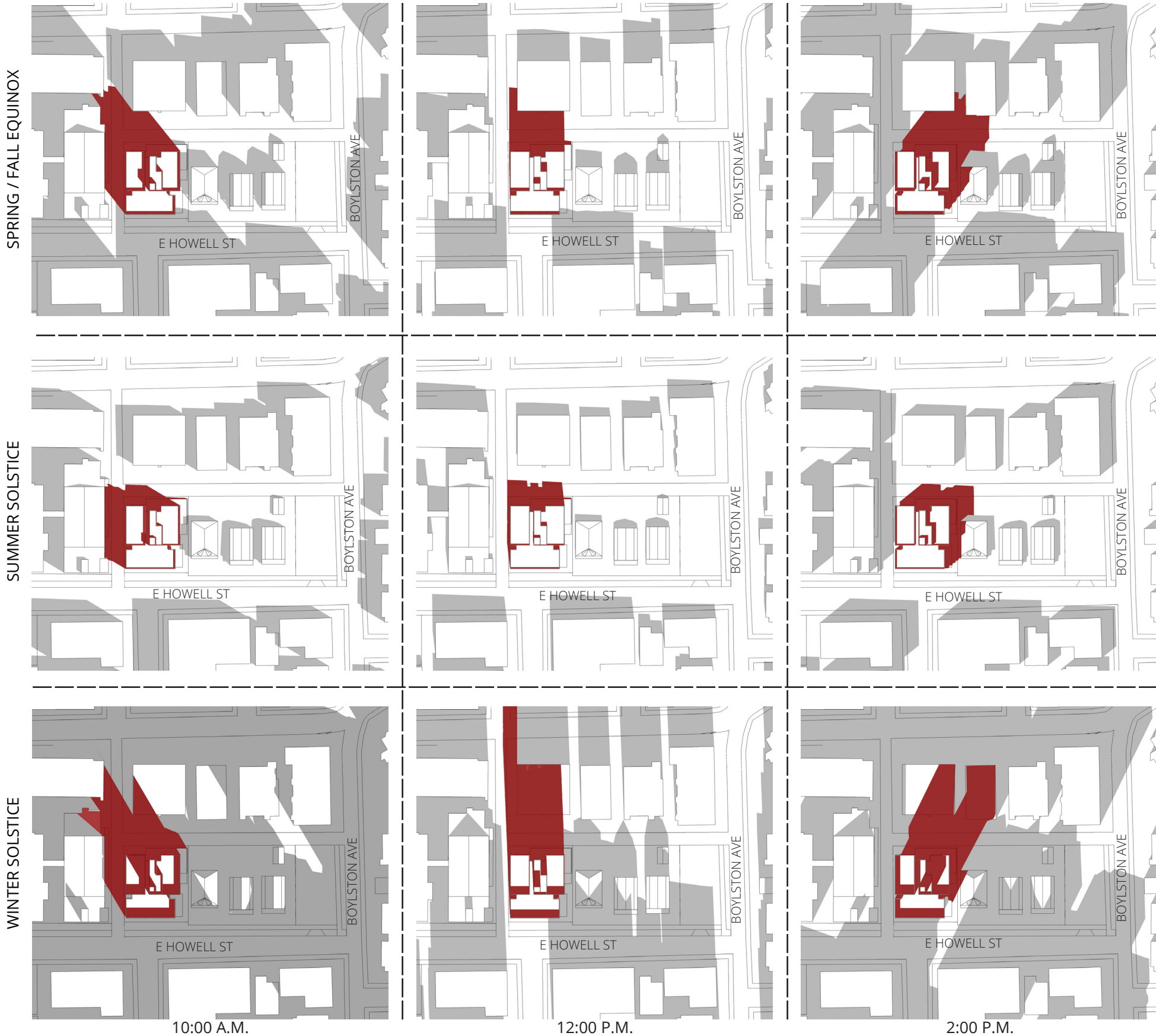
- 1

DEPARTURE: SIDE SETBACK (WEST ALLEY)
SMC 23.45.518.B - SIDE SETBACK FROM INTERIOR LOT LINE
 - **REQUIRED:** 7' average, 5' minimum < 42' above grade
10' average, 7' minimum > 42' above grade
 - **PROPOSED:** 2.95' average, 1' minimum < 42' above grade
1.44' average, 1' minimum > 42' above grade
- 2

DEPARTURE: FRONT SETBACK (E HOWELL ST)
SMC 23.45.518.B - FRONT SETBACK FROM STREET LOT LINE
 - **REQUIRED:** 7' average, 5' minimum
 - **PROPOSED:** 5.72' average, 5' minimum



SUN PATH / SHADOW STUDY: SCHEME B



SCHEME C: NARROW & ARTICULATED (PREFERRED)

35,079 GSF
77 UNITS
1,515 SF COMMERCIAL SPACE
3,368 SF- TOTAL AMENITY
397 SF - REAR YARD
1,995 SF - ROOF DECK
395 SF - PRIVATE DECKS
719 SF - ENTRY COURT
259 SF - COMMON AMENITY ROOM

4.24 FAR - 32,602 GFA

POSITIVE

- Variety of amenity spaces
- Opportunity for clear architectural concept
- Building mass located near front and west property lines is more consistent with precedent, provides buffer to neighbors
- Most articulated massing, follows topography
- Commercial at southwest corner is balanced buffer to residential neighbors
- Units at grade along rear activate the alley

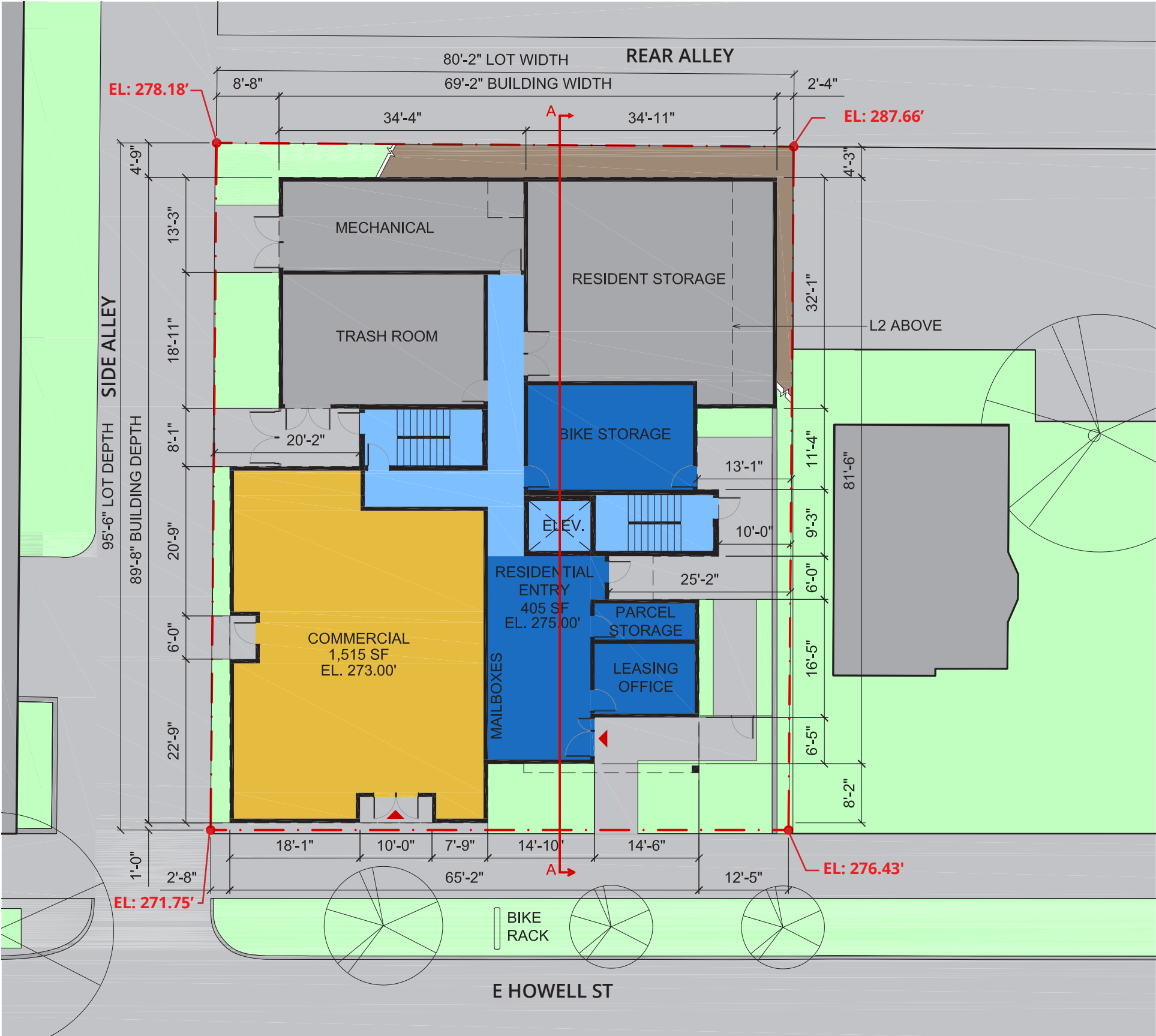
NEGATIVE

- Departures requested
- Blank wall at northwest corner to be buffered

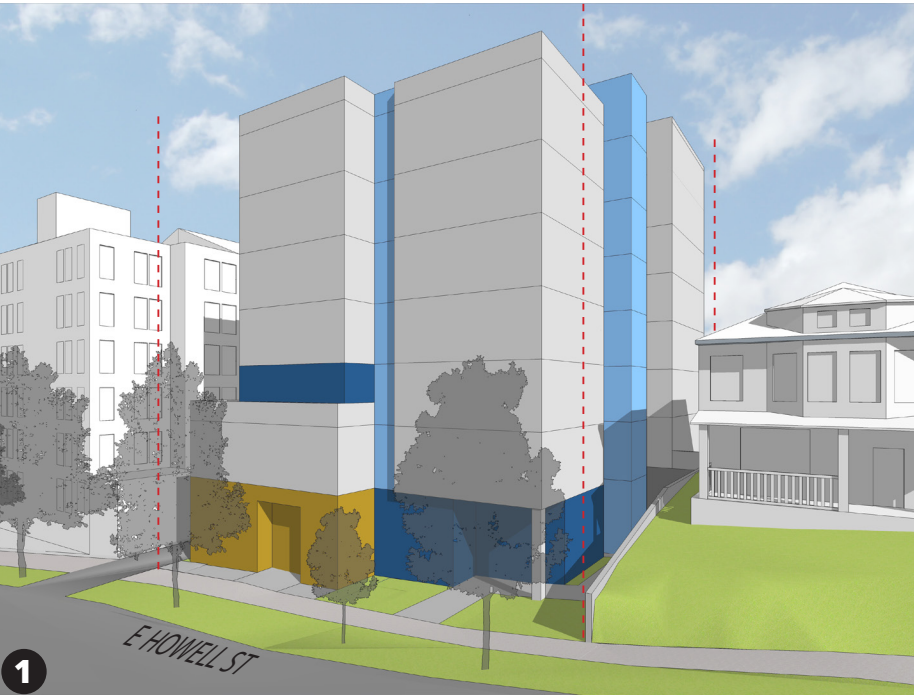
- RESIDENTIAL
- INTERIOR CIRCULATION
- COMMERCIAL
- COMMON
- BUILDING SERVICE
- PRIMARY BUILDING ENTRANCE

SITE / STREET LEVEL PLAN

SCALE: 1/16" = 1'-0"



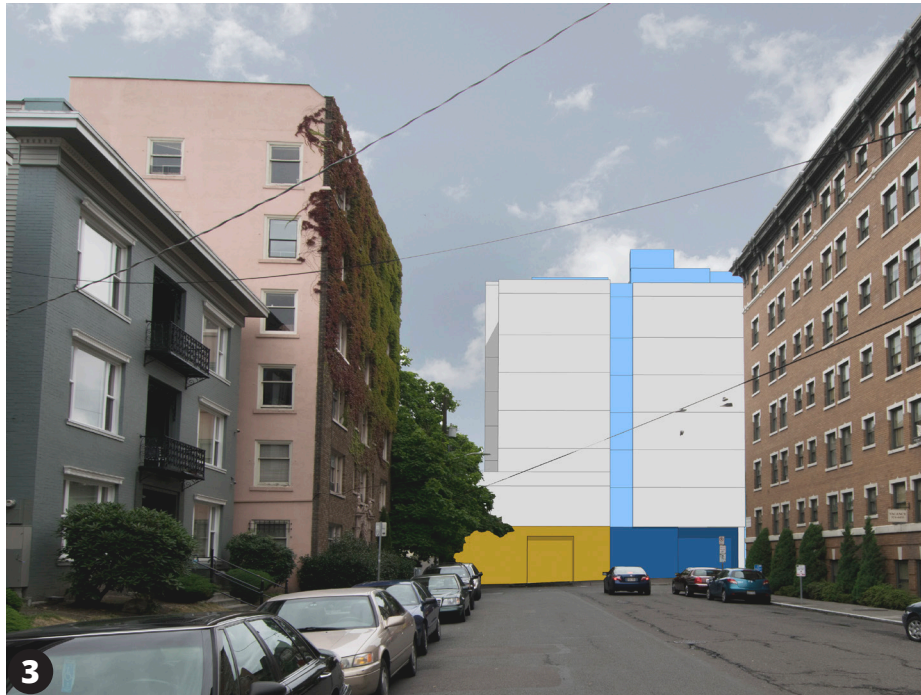
PERSPECTIVES: SCHEME C: NARROW & ARTICULATED (PREFERRED)



PERSPECTIVE LOOKING AT SOUTHEAST CORNER



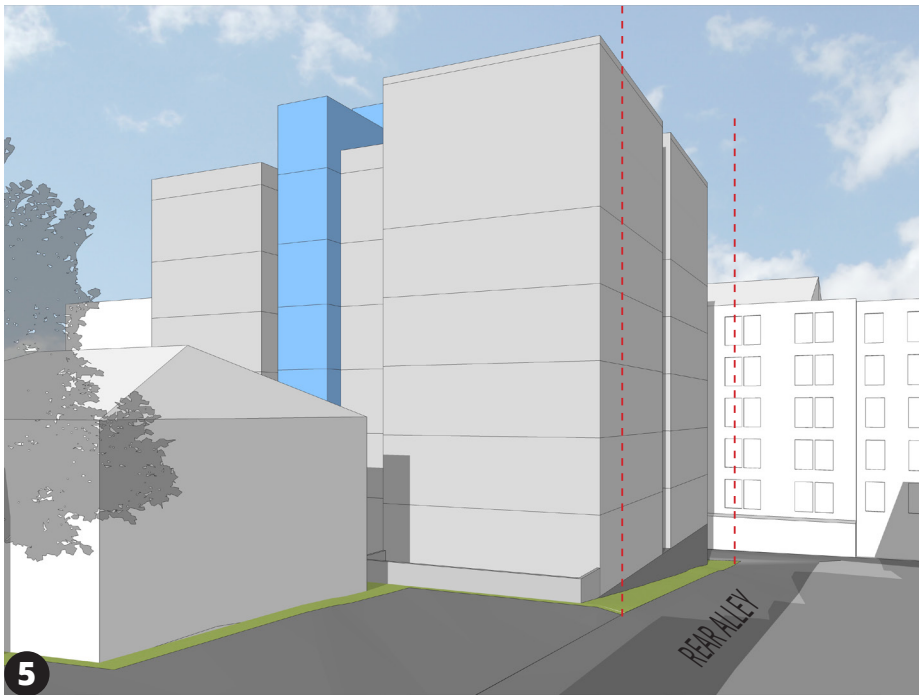
PERSPECTIVE LOOKING AT SOUTHWEST CORNER



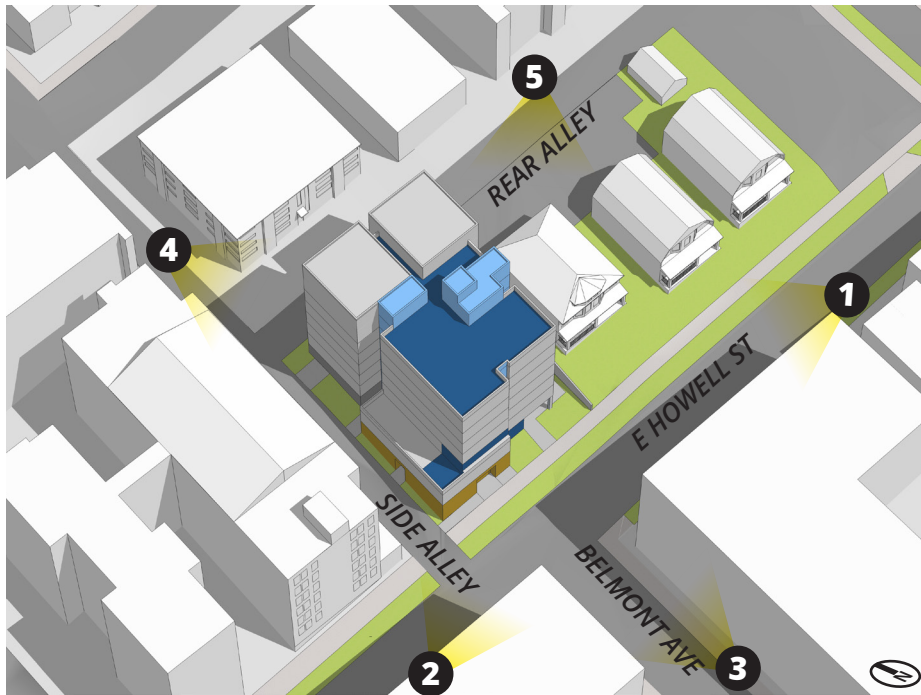
PERSPECTIVE LOOKING FROM BELMONT AVE



PERSPECTIVE LOOKING AT NORTHWEST CORNER



PERSPECTIVE LOOKING AT NORTHEAST CORNER



SCHEME C BIRDSEYE

DEPARTURES: SCHEME C
*SEE GUIDELINES AND JUSTIFICATION, PAGE 27

- 1

DEPARTURE: SIDE SETBACK (WEST ALLEY)

SMC 23.45.518.B - SIDE SETBACK FROM INTERIOR LOT LINE

 - REQUIRED:** 7' average, 5' minimum < 42' above grade
10' average, 7' minimum > 42' above grade
 - PROPOSED:** 10.83' average, 2.5' minimum < 42' above grade
12.78' average, 8.67' minimum > 42' above grade
(departure not required above 42')
- 2

DEPARTURE: FRONT SETBACK (E HOWELL ST)

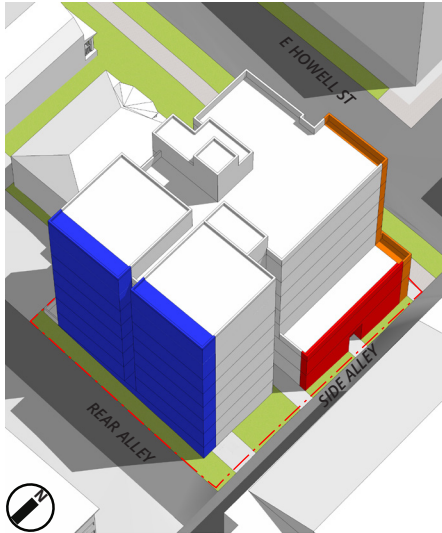
SMC 23.45.518.B - FRONT SETBACK FROM STREET LOT LINE

 - REQUIRED:** 7' average, 5' minimum
 - PROPOSED:** 5.30' average, 1' minimum
- 3

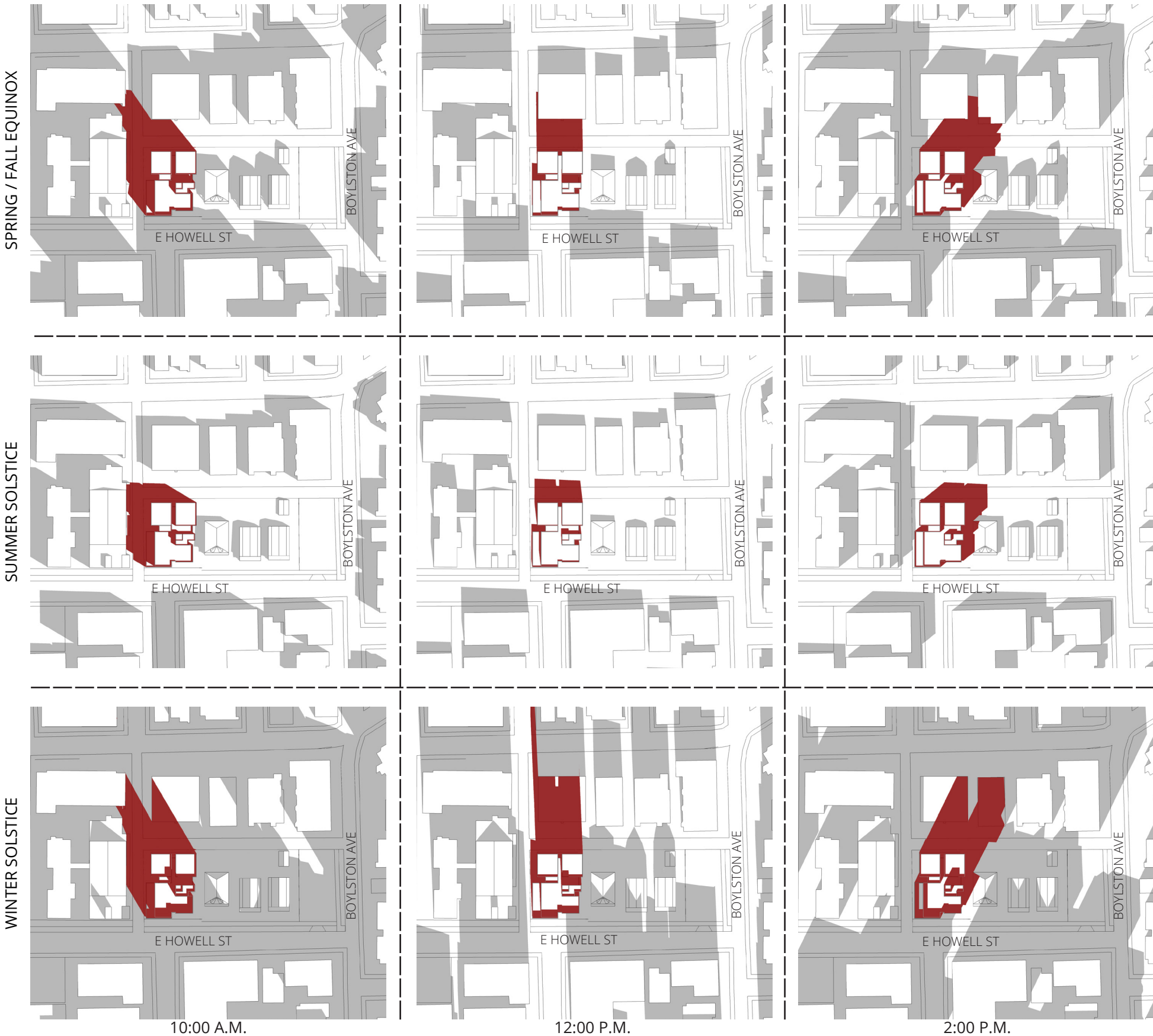
DEPARTURE: REAR SETBACK (NORTH ALLEY)

SMC 23.45.518.B - REAR SETBACK W/ ALLEY

 - REQUIRED:** 10' minimum
 - PROPOSED:** 4.25' minimum



SUN PATH / SHADOW STUDY: SCHEME C



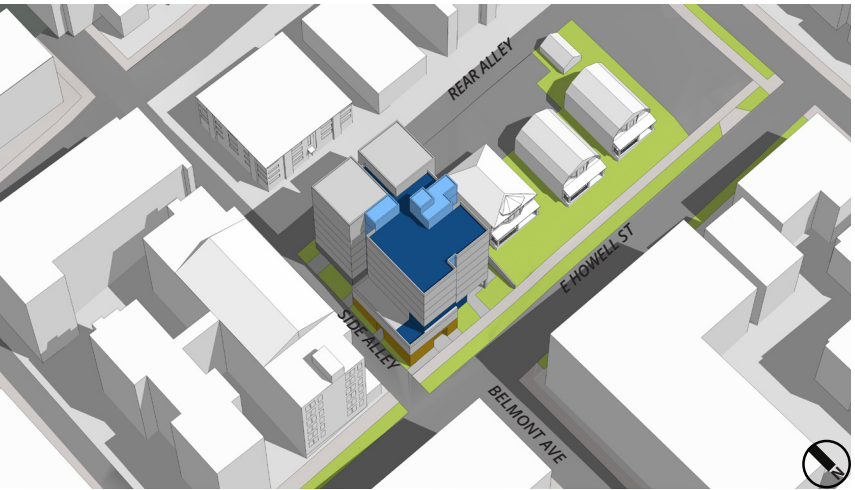
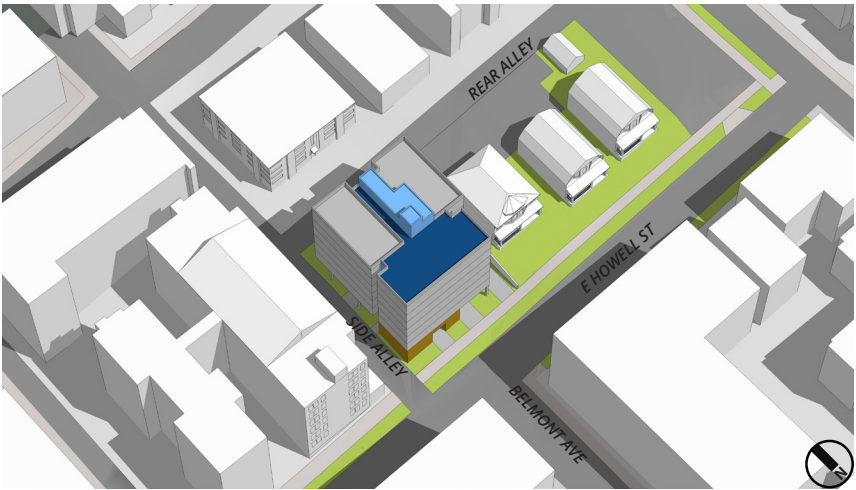
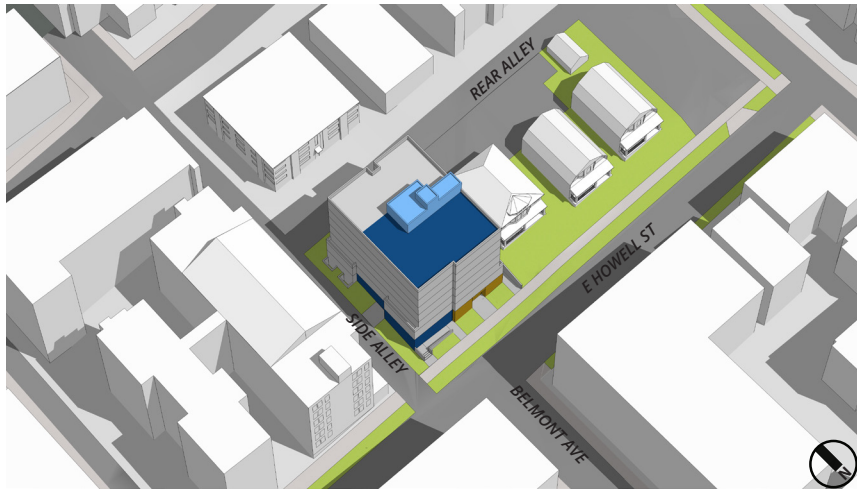
SCHEME SUMMARY

SCHEME A: CENTERED (COMPLIANT)

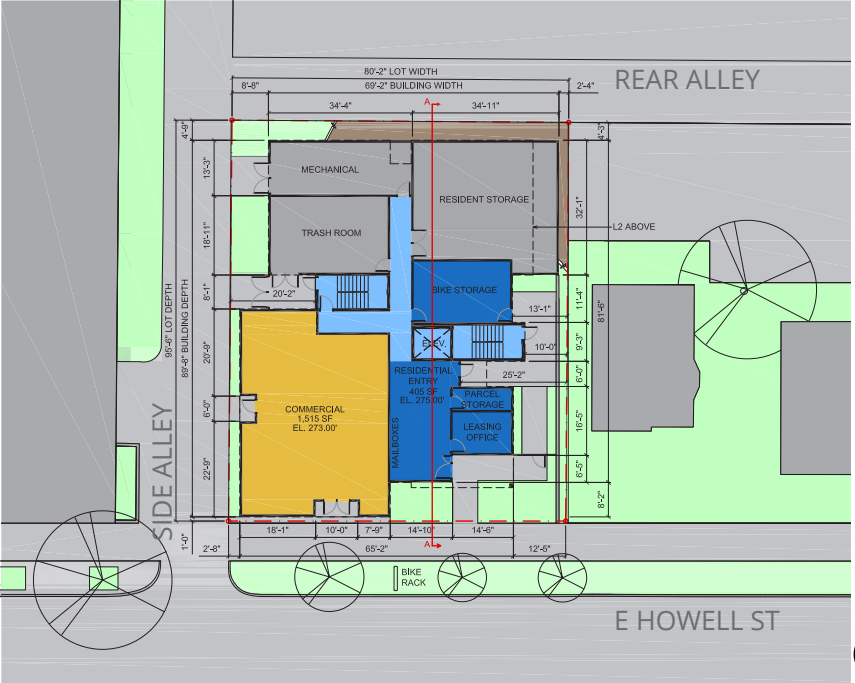
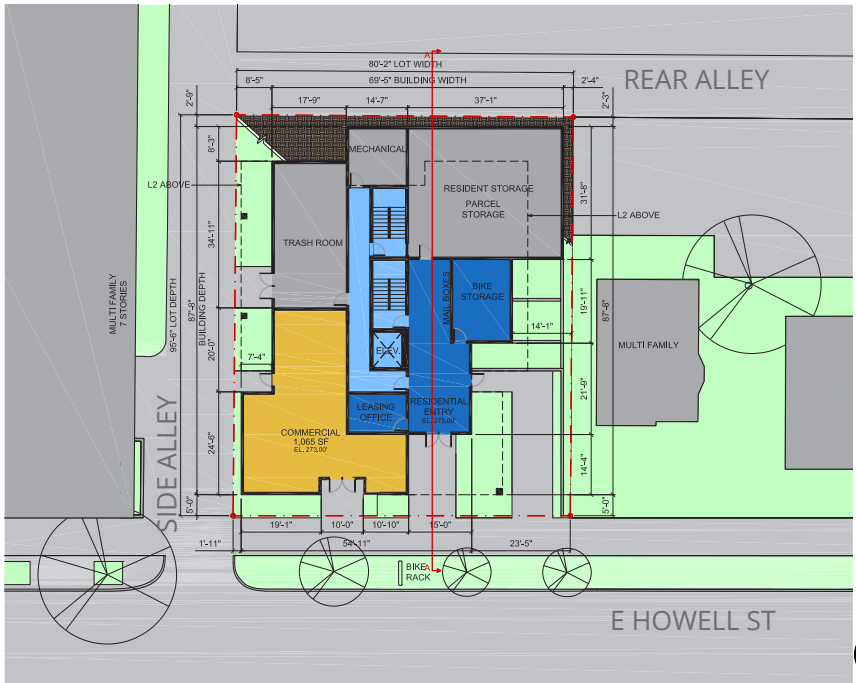
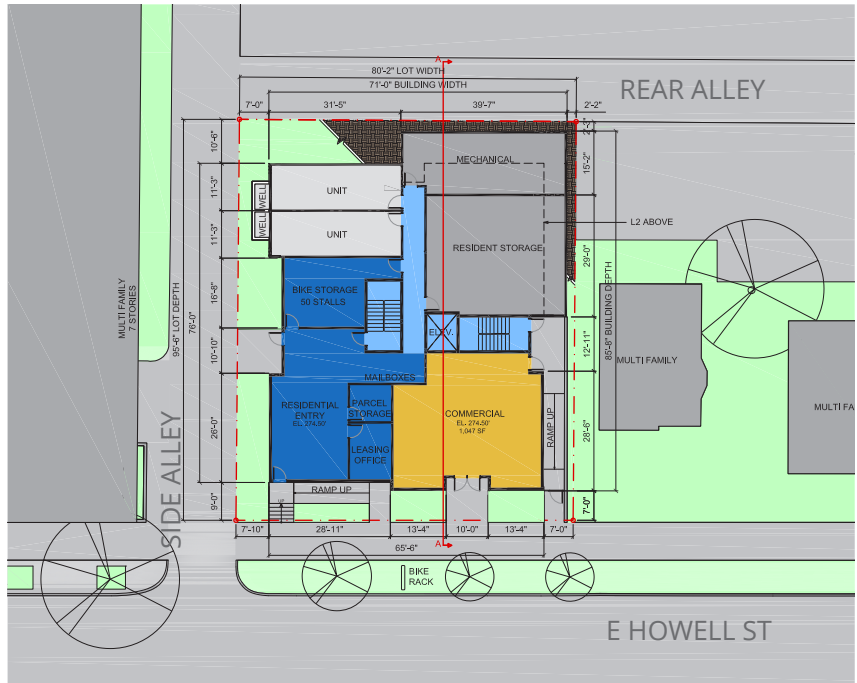
SCHEME B: COMPACT & OFFSET

SCHEME C: NARROW & ARTICULATED (PREFERRED)

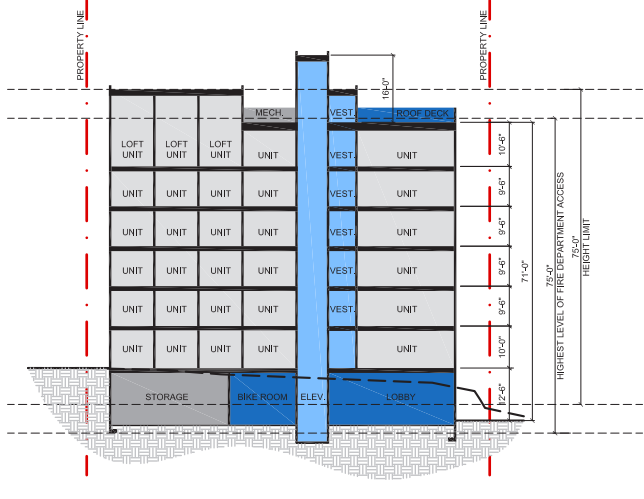
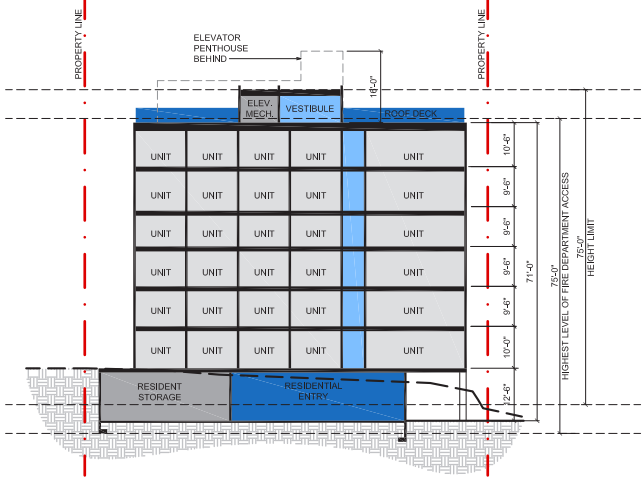
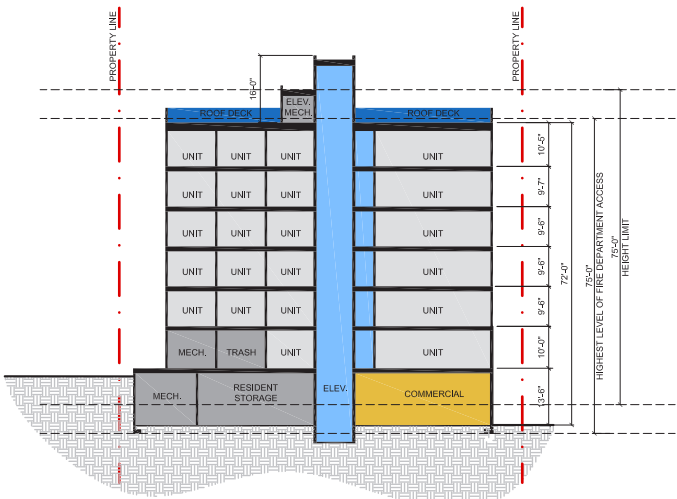
BIRD'S EYE PERSPECTIVE



SITE PLAN



SECTION



SCHEME A: CENTERED (COMPLIANT)

- NO DEPARTURES REQUESTED

SCHEME B: COMPACT & OFFSET

- 1

DEPARTURE: SIDE SETBACK (WEST ALLEY)

SMC 23.45.518.B - SIDE SETBACK FROM INTERIOR LOT LINE

 - **REQUIRED:** 7' average, 5' minimum < 42' above grade
10' average, 7' minimum > 42' above grade
 - **PROPOSED:** 2.95' average, 1' minimum < 42' above grade
1.44' average, 1' minimum > 42' above grade

GUIDELINES: CS2.C.2: Mid-block Sites; CS3.A.1: Fitting Old and New Together; CS2.D.5: Respect for Adjacent Sites

JUSTIFICATION: Proposed side setback provides additional buffer to the east neighbor, which balances the building placement with the perceived buffer at the side alley. The result is a more consistent street front rhythm.
- 2

DEPARTURE: FRONT SETBACK (E. HOWELL ST.)

SMC 23.45.518.B - FRONT SETBACK FROM STREET LOT LINE

 - **REQUIRED:** 7' average, 5' minimum
 - **PROPOSED:** 5.72' average, 5' minimum

GUIDELINES: CS2.A.2: Architectural Presence; CS3.A.1: Fitting Old and New Together

JUSTIFICATION: Reduced front setback is consistent with other 7-story buildings in immediate vicinity. Strong street front massing responds to project visibility, especially from Belmont Ave.



SCHEME C: NARROW & ARTICULATED (PREFERRED)

- 1

DEPARTURE: SIDE SETBACK (WEST ALLEY)

SMC 23.45.518.B - SIDE SETBACK FROM INTERIOR LOT LINE

 - **REQUIRED:** 7' average, 5' minimum < 42' above grade
10' average, 7' minimum > 42' above grade
 - **PROPOSED:** 10.83' average, 2.5' minimum < 42' above grade
12.78' average, 8.67' minimum > 42' above grade
(departure not required above 42')

GUIDELINES: DC1.B.1: Facade Composition; CS2.D.5: Respect for Adjacent Sites; CS2.C.1: Corner Site; DC2.D.1: Human Scale

JUSTIFICATION: Proposed side setback re-establishes commercial corner at sidewalk edge, responds to pedestrian scale, and provides separation from adjacent residential uses.
- 2

DEPARTURE: FRONT SETBACK (E. HOWELL ST.)

SMC 23.45.518.B - FRONT SETBACK FROM STREET LOT LINE

 - **REQUIRED:** 7' average, 5' minimum / **PROPOSED:** 5.30' average, 1' minimum

GUIDELINES: CS2.I.I: Streetscape Compatibility; DC1.B.1: Facade Composition; PL1.C.1: Selecting Activity Areas; CS3.A.4 Evolving Neighborhoods; CS2.A.2: Architectural Presence

JUSTIFICATION: Reduced front setback is consistent with other 7-story buildings in immediate vicinity. Proposed setback re-establishes pedestrian scaled commercial corner, and creates a balanced composition visible from multiple viewpoints including Belmont Ave.
- 3

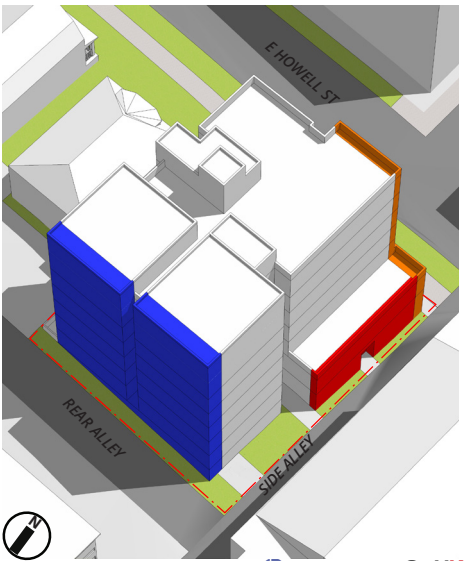
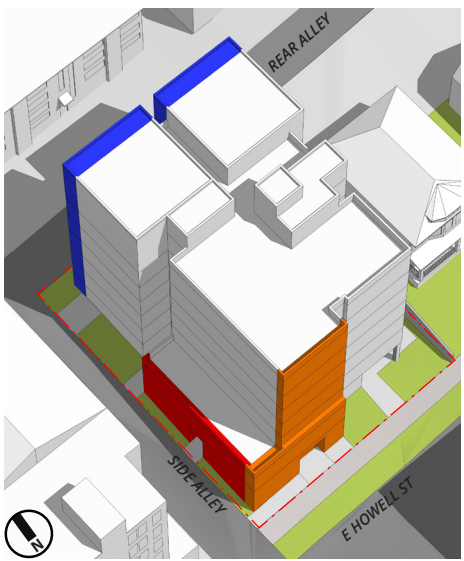
DEPARTURE: REAR SETBACK (NORTH ALLEY)

SMC 23.45.518.B - REAR SETBACK W/ ALLEY

 - **REQUIRED:** 10' minimum / **PROPOSED:** 4.25' minimum

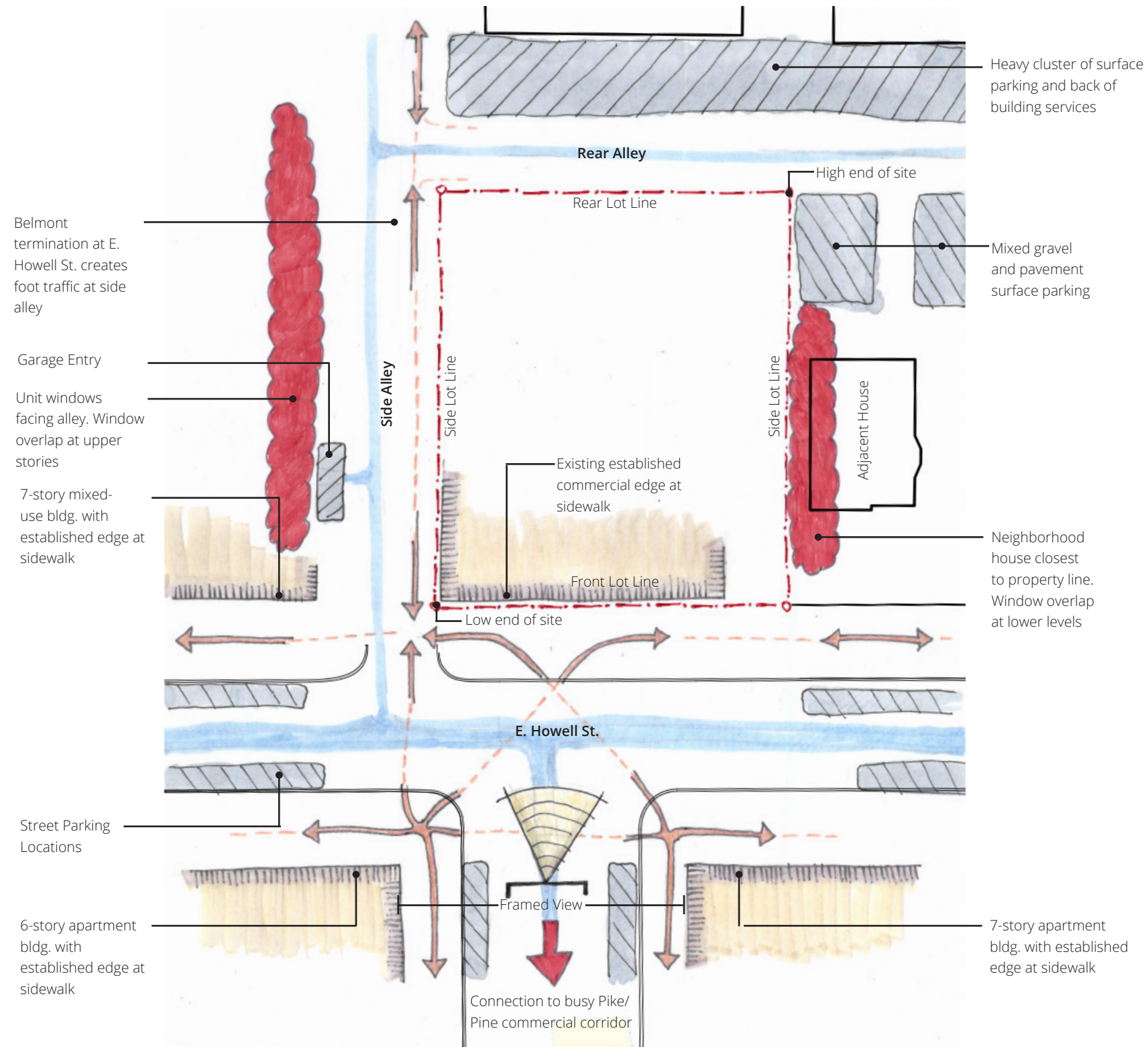
GUIDELINES: CS1.C.2: Elevation Change; CS2.D.5: Respect for Adjacent Sites; DC2.A.2: Reduced Perceived Mass

JUSTIFICATION: Proposed rear setback provides the opportunity for greater articulation along east & west facades, and a more narrow footprint towards the rear of the site. The resulting massing responds to the rising topography, and provides additional separation to east & west neighbors.



CONCEPT DEVELOPMENT: ADJACENCIES/SITE RESPONSE

SITE CONSTRAINTS



SITE RESPONSE

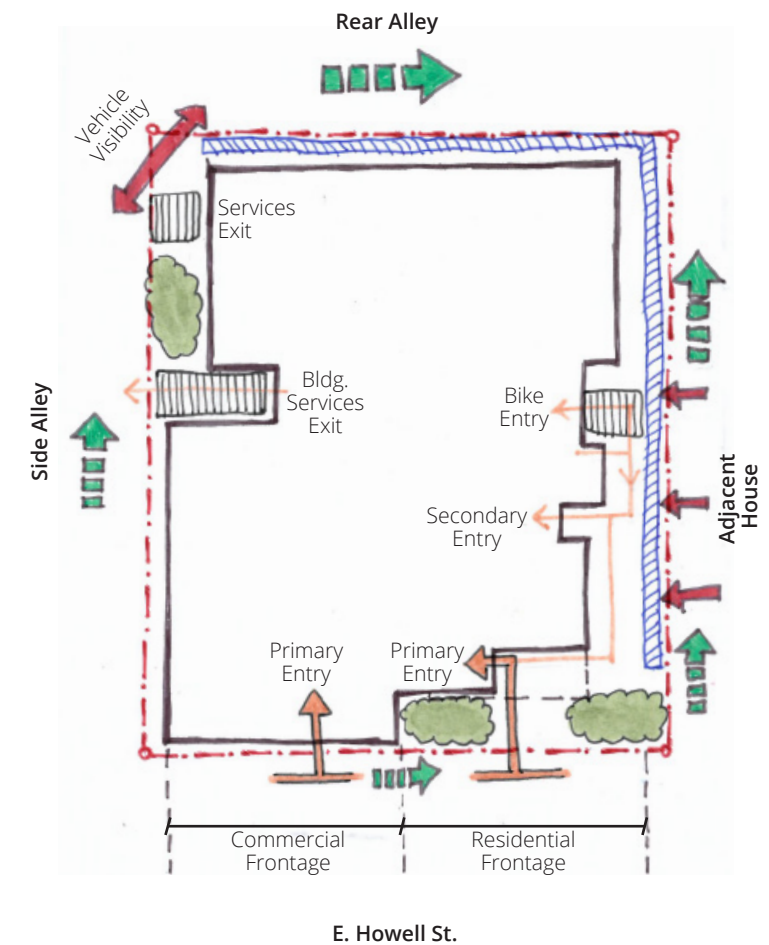
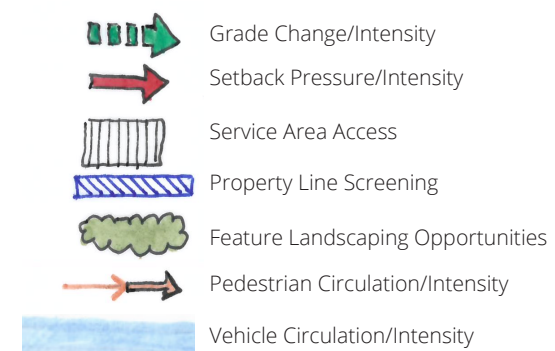
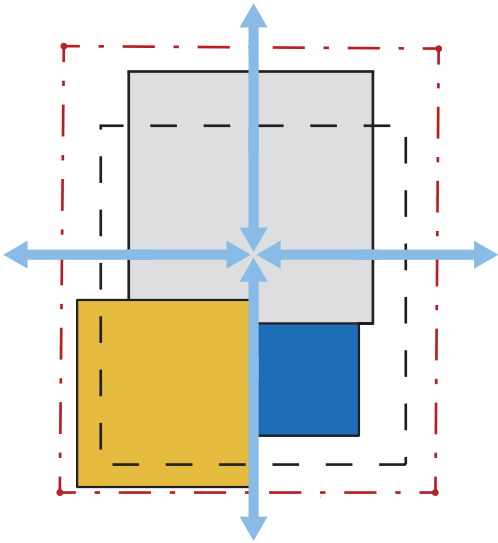
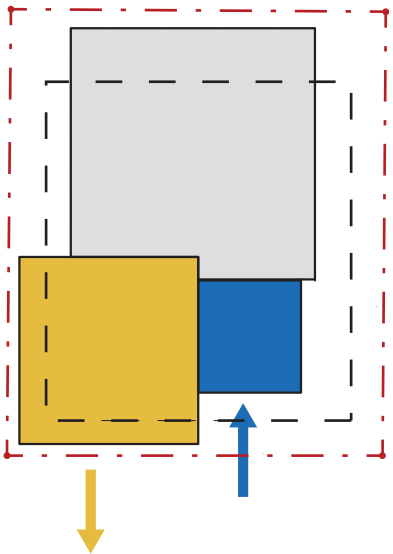
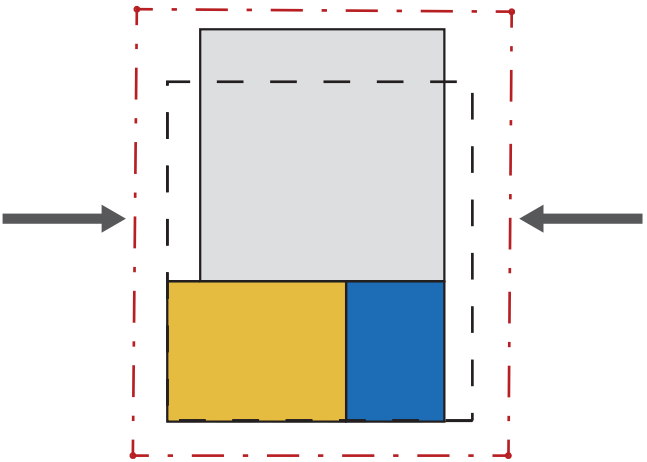
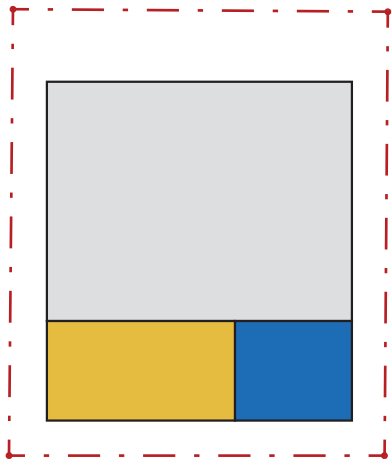


DIAGRAM LEGEND



CONCEPT DEVELOPMENT: MASSING

MASSING: PLAN



DEVELOPMENT POTENTIAL

Basic program elements for the project include residential units, residential lobby and commercial space. The first evolution reflects the starting point for setback compliance and relationship to the street & alleys.

RESPONSE TO ADJACENCIES

The residential footprint is compressed to provide additional side yard buffer, and reduce the impact on adjacent neighbors. The result is a long and narrow footprint, pushed to the rear of the alley.

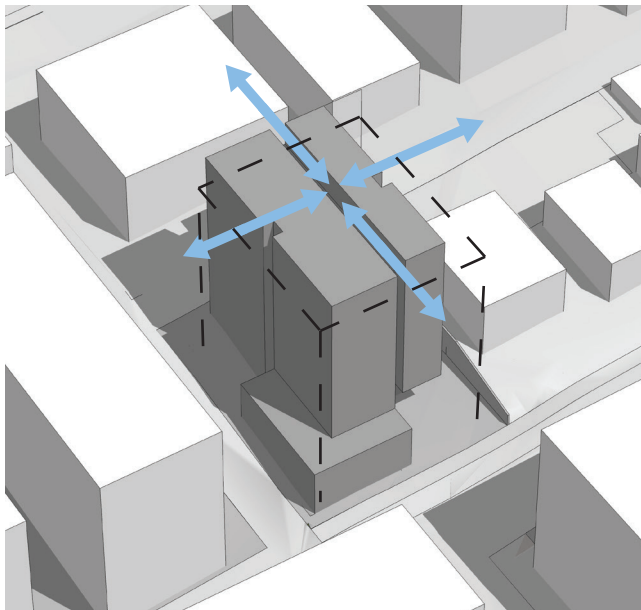
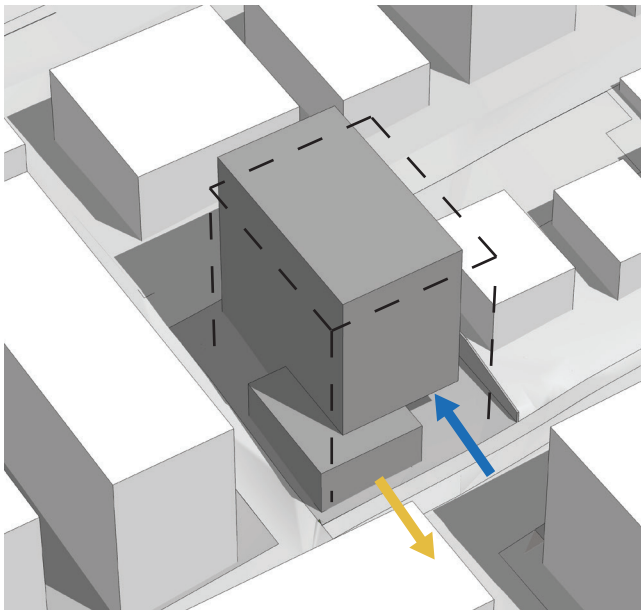
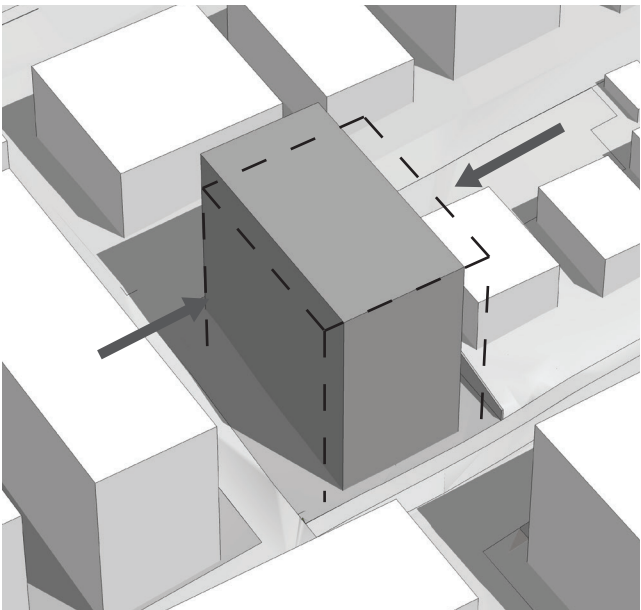
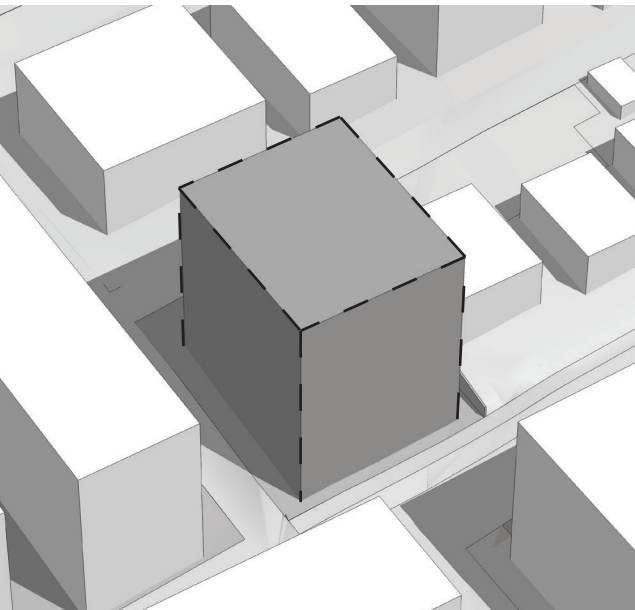
EXPRESSION OF STREET LEVEL USES

The street level uses are adjusted to better relate to the street. The commercial space is pulled forward to make a stronger corner, and to better engage the street. The residential lobby is pushed back to provide a buffer from the sidewalk and enhance the entry experience.

CIRCULATION + CORE ARTICULATION

The overall massing is articulated by expressing the circulation, establishing a clear architectural concept. The resulting modulation breaks down the perceived mass of the building, and provides the opportunity for a clear fenestration and material palette.

MASSING: AXON



CONCEPT DEVELOPMENT: STREET-LEVEL POTENTIAL



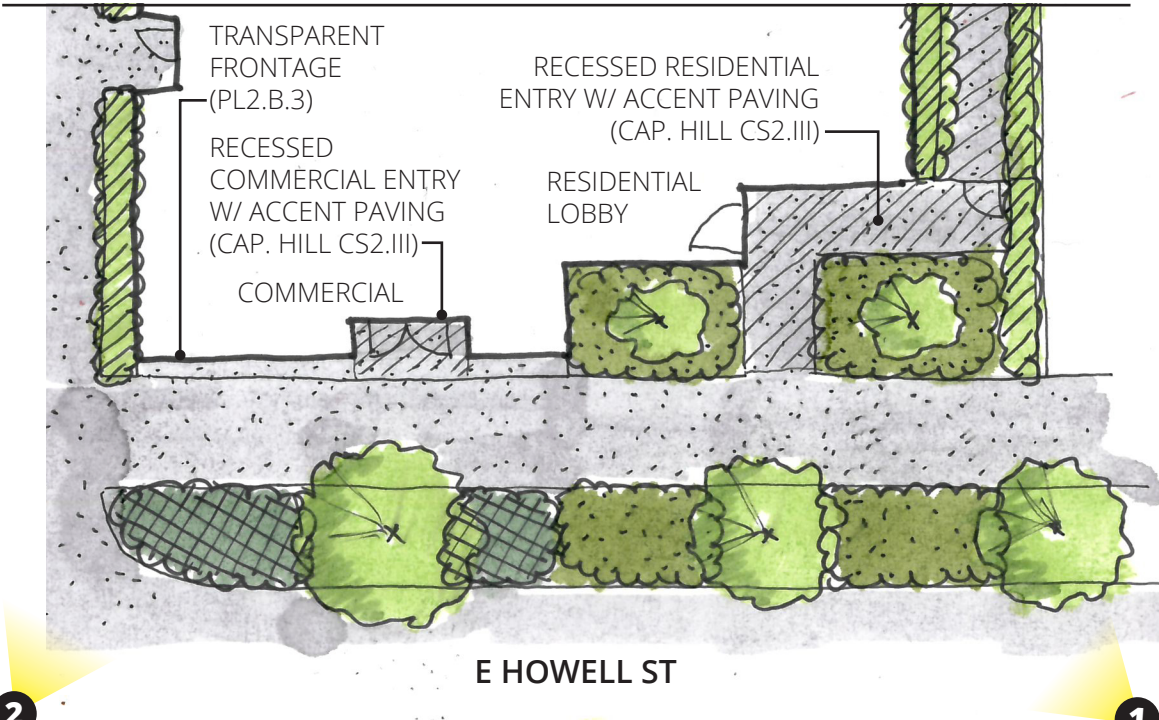
1 SOUTHEAST VIEW AT STREET-LEVEL



VIEW LOOKING NORTH FROM BELMONT



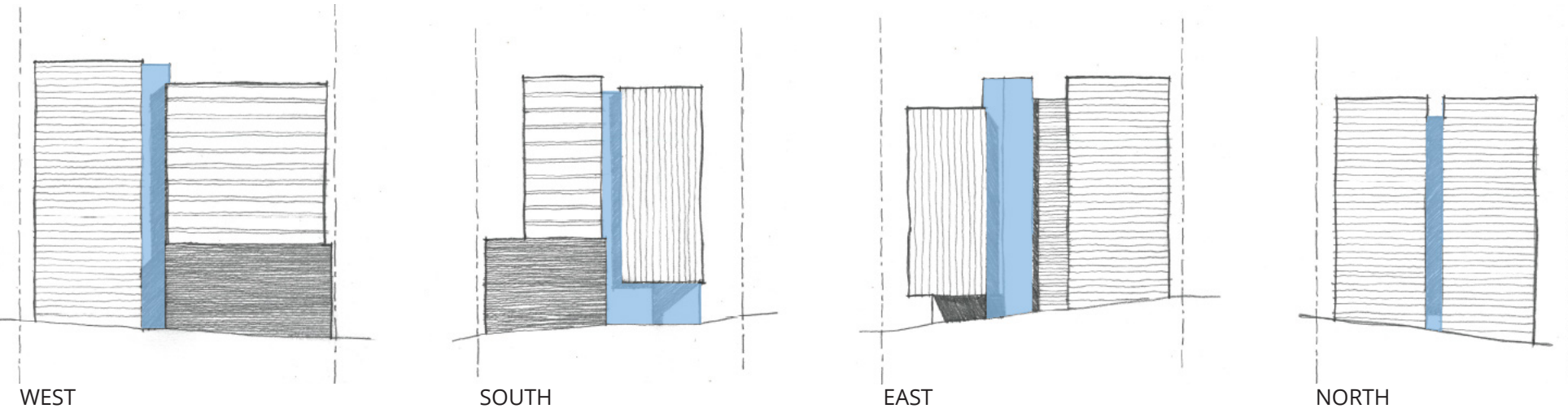
2 SOUTHWEST VIEW AT STREET-LEVEL



1 PARTIAL PLAN AT STREET EDGE

CONCEPT DEVELOPMENT: ARTICULATION

ELEVATION AND EXPRESSION STUDIES

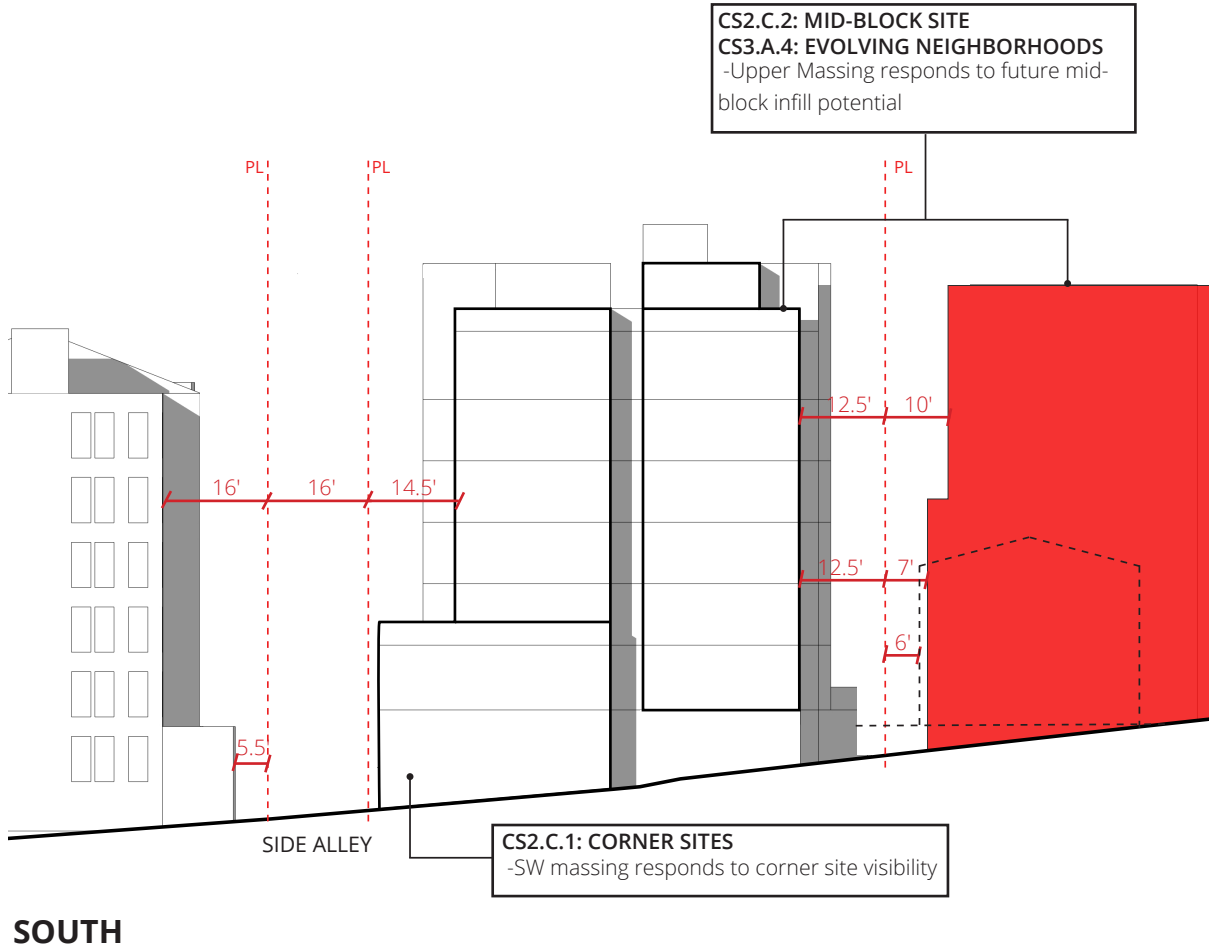
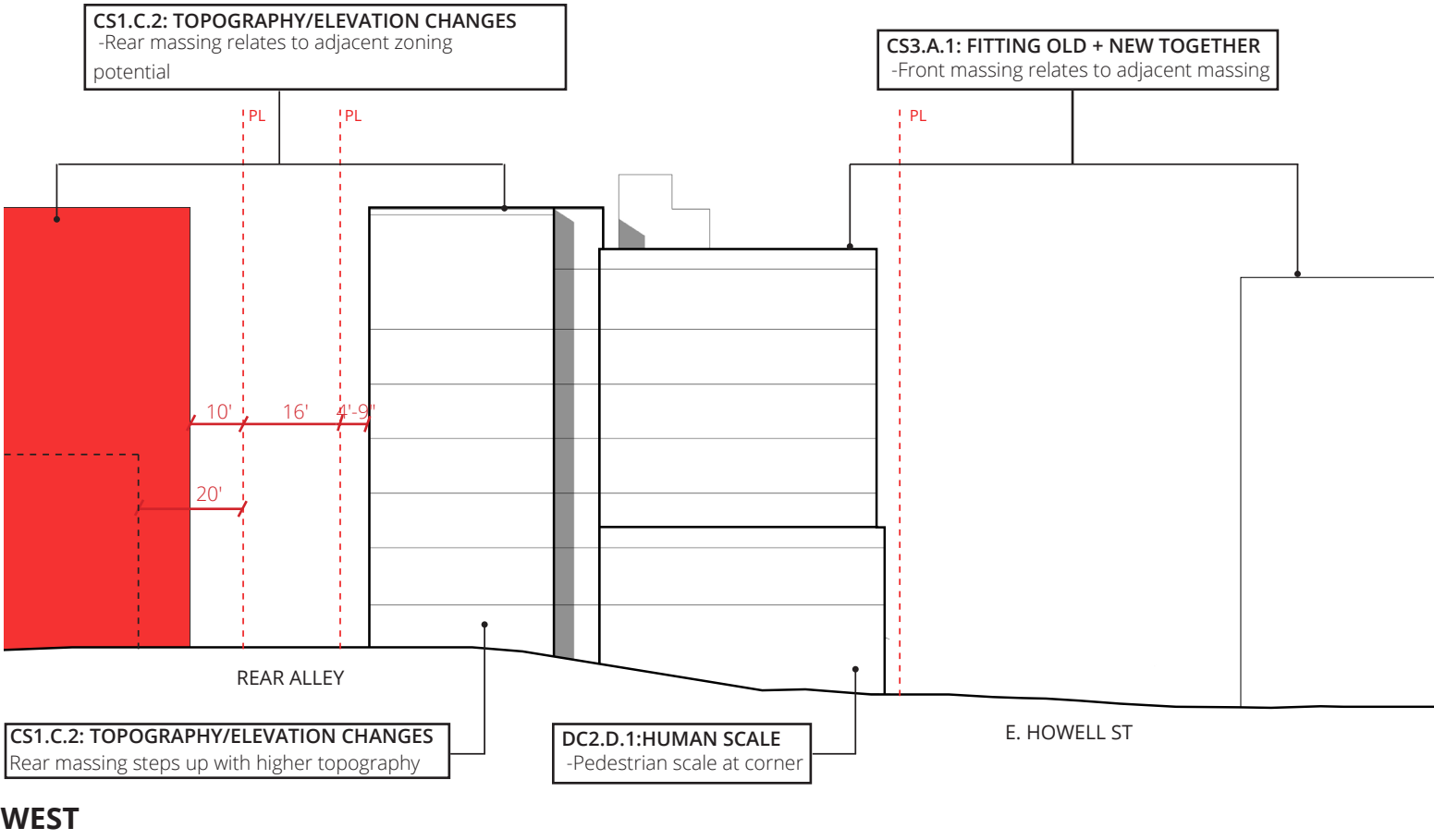


The secondary articulation of the project provides several unique opportunities for architectural concept and allows the project to fit in with surrounding adjacencies.

- Expression:**
- Modulation allows for a consistent material palette, logically applied to the massing.
 - The interior circulation can be expressed on the building exterior.
 - The scale of the modulation establishes a balanced façade composition, that unifies the entire building.

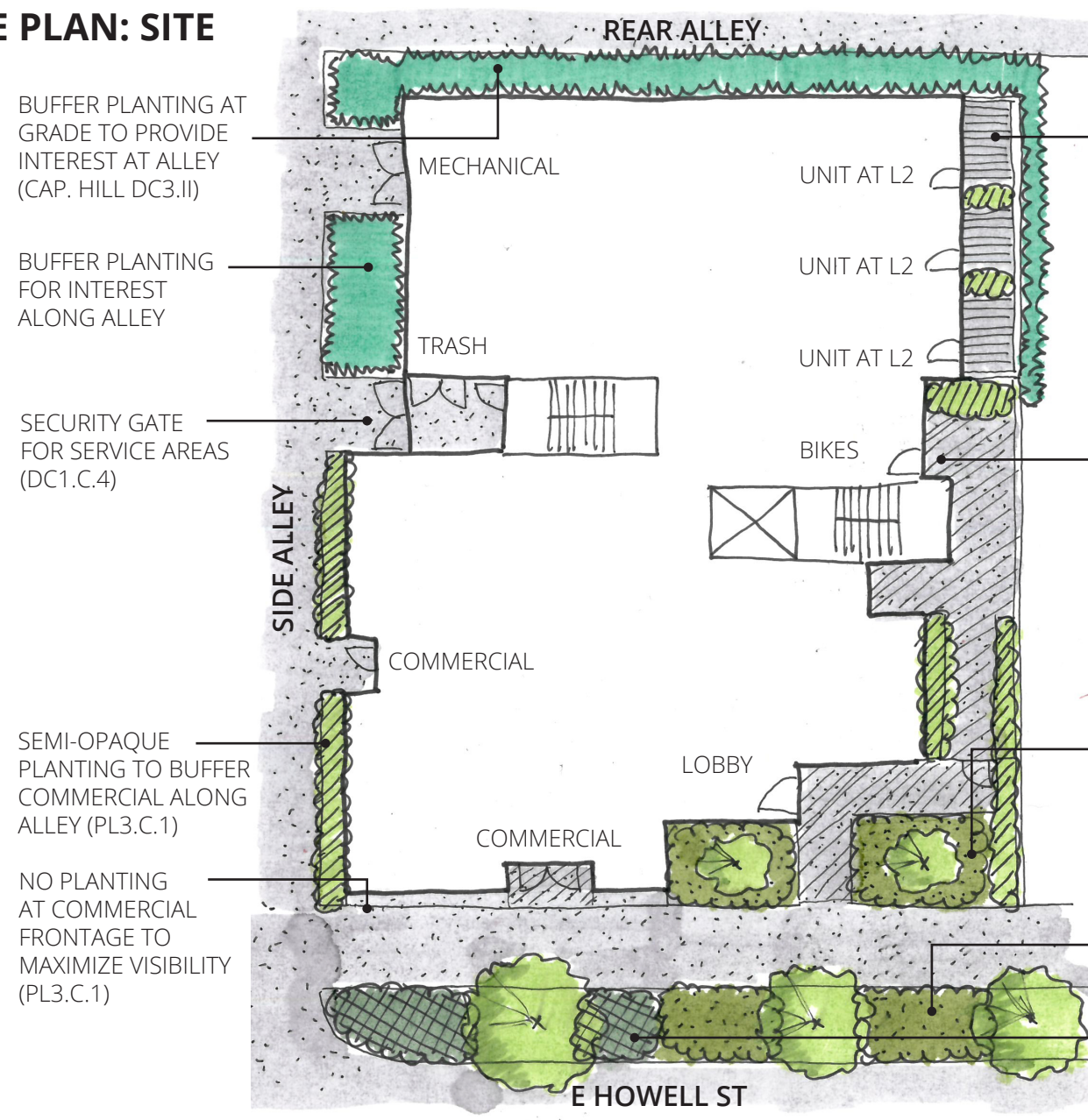
- Adjacencies:**
- The rear massing steps up with the rising topography, making a stronger relationship to the north.
 - The primary south massing fits well into the potential street edge.
 - The southwest corner massing maintains the pedestrian scale along the street.

ARTICULATION AND ADJACENCIES



CONCEPT DEVELOPMENT: LANDSCAPE + AMENITY

COMPOSITE PLAN: SITE



LANDSCAPE LEGEND

- BUFFER PLANTING
- SCREENING PLANTING
- TEXTURED PLANTING
- DURABLE PLANTING
- TREE
- OUTDOOR AMENITY

SEMI-OPAQUE PLANTING TO BUFFER COMMERCIAL ALONG ALLEY (PL3.C.1)

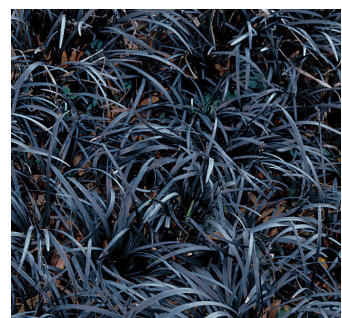
NO PLANTING AT COMMERCIAL FRONTAGE TO MAXIMIZE VISIBILITY (PL3.C.1)



AMENITY CONCEPTS

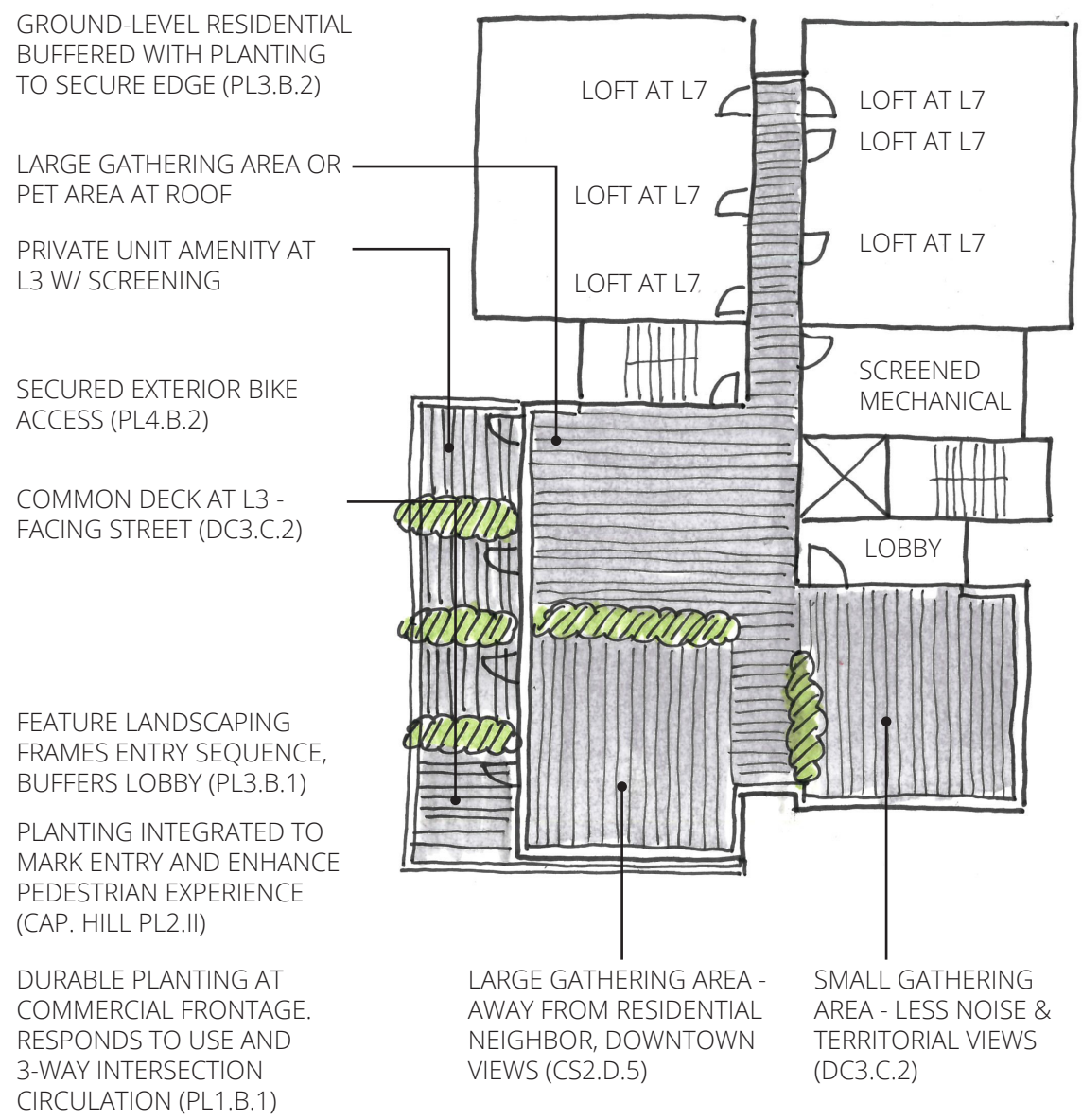


PERIMETER PLANTING



RIGHT OF WAY FRONTAGE

COMPOSITE PLAN: ROOF DECK





STREET-LEVEL PERSPECTIVE LOOKING NORTHEAST

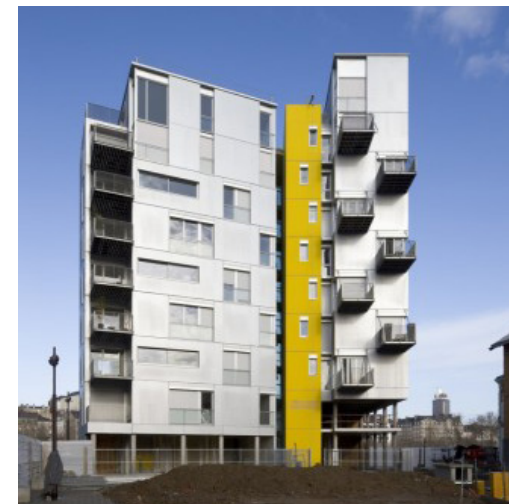
CONCEPT DEVELOPMENT: FORM + EXPRESSION



COMMERCIAL FRONTAGE



RECESSED RESIDENTIAL ENTRY



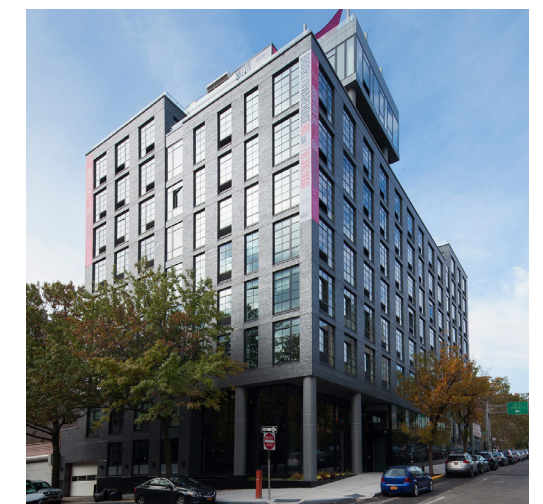
EXPRESSION OF CIRCULATION



MATERIALS AND ARTICULATION



TEXTURED MATERIALS



RIGOROUS FENESTRATION PATTERN

INTENTIONALLY BLANK



5902 22ND AVE NW



2418 NW 58TH ST



1010 E REPUBLICAN ST (UNDER CONSTRUCTION)



116 13TH AVE E



1806 23RD AVE



1404 BOYLSTON AVE (UNDER CONSTRUCTION)



120 10TH AVE E (UNDER CONSTRUCTION)