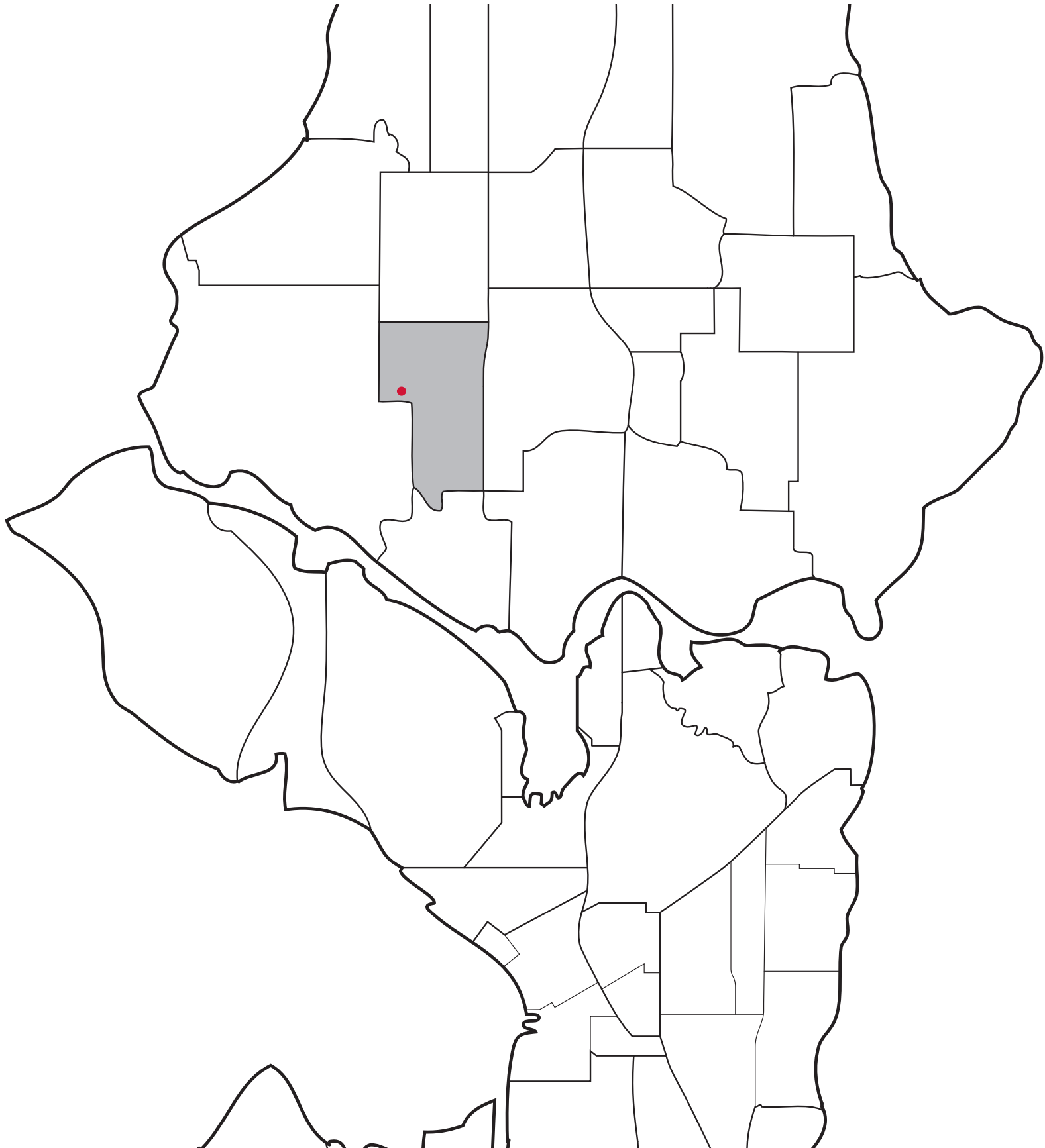


#3024585

412 NW 65th St. **EARLY DESIGN GUIDANCE**

47 | **SHW**



ADDRESS

412 NW 65TH ST.
SDCI# 3024585

PROJECT TEAM

OWNER47 NORTH, LLC

ARCHITECTSHW, LLC

SURVEYORGeoDimensions, inc.

PROJECT INFO

ZONINGNC2P-30

LOT SIZE10,000 sf

FAR2.5

ALLOWABLE FAR25,000 sf

PROPOSED FAR15,108 sf

PROPOSED UNITS9

LIVE/WORK UNITS3

COMMERCIAL GFA2,740 sf

PARKING STALLS9

BICYCLE PARKING4

PROJECT DESCRIPTION

The project involves demolishing an existing commercial structure and constructing a new commercial, live-work, and apartment building.

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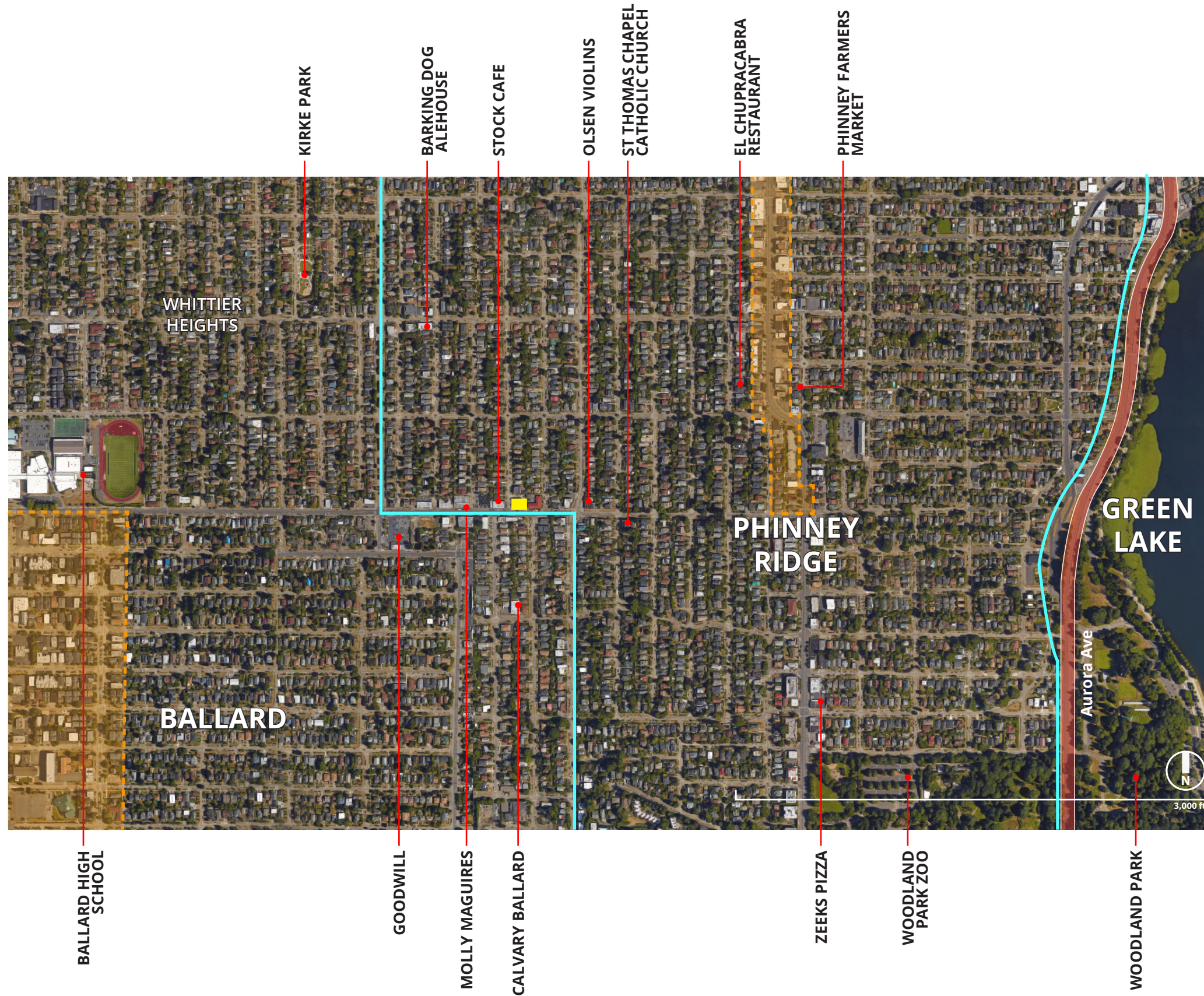
34

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VICINITY ANALYSIS

The NW 65th St. neighborhood offers a variety of commercial, restaurant, and shopping opportunities for residents. The project site is located between the Ballard and Greenwood/Phinney Ridge Urban Villages. It is situated on an active retail and business strip, with nearby public transportation options available (buses and car sharing). This portion of NW 65th St is a popular and dynamic area that will continue to grow in the future.

LEGEND

- SITE
- PHINNEY RIDGE/ BALLARD URBAN CENTER VILLAGE
- NEIGHBORHOOD BOUNDARY

ZONING

This site is zoned NC2P-30, and is adjacent to SF 5000 and LR2-RC at the north and east of the property.

NC2 zoning: "A moderately sized pedestrian oriented shopping area that provides a full range of retail sales and services to the surrounding neighborhood." (SDCI Handout)

P zoning: "P designations are applied to NC and zones along pedestrian-oriented commercial streets." (SDCI Handout)

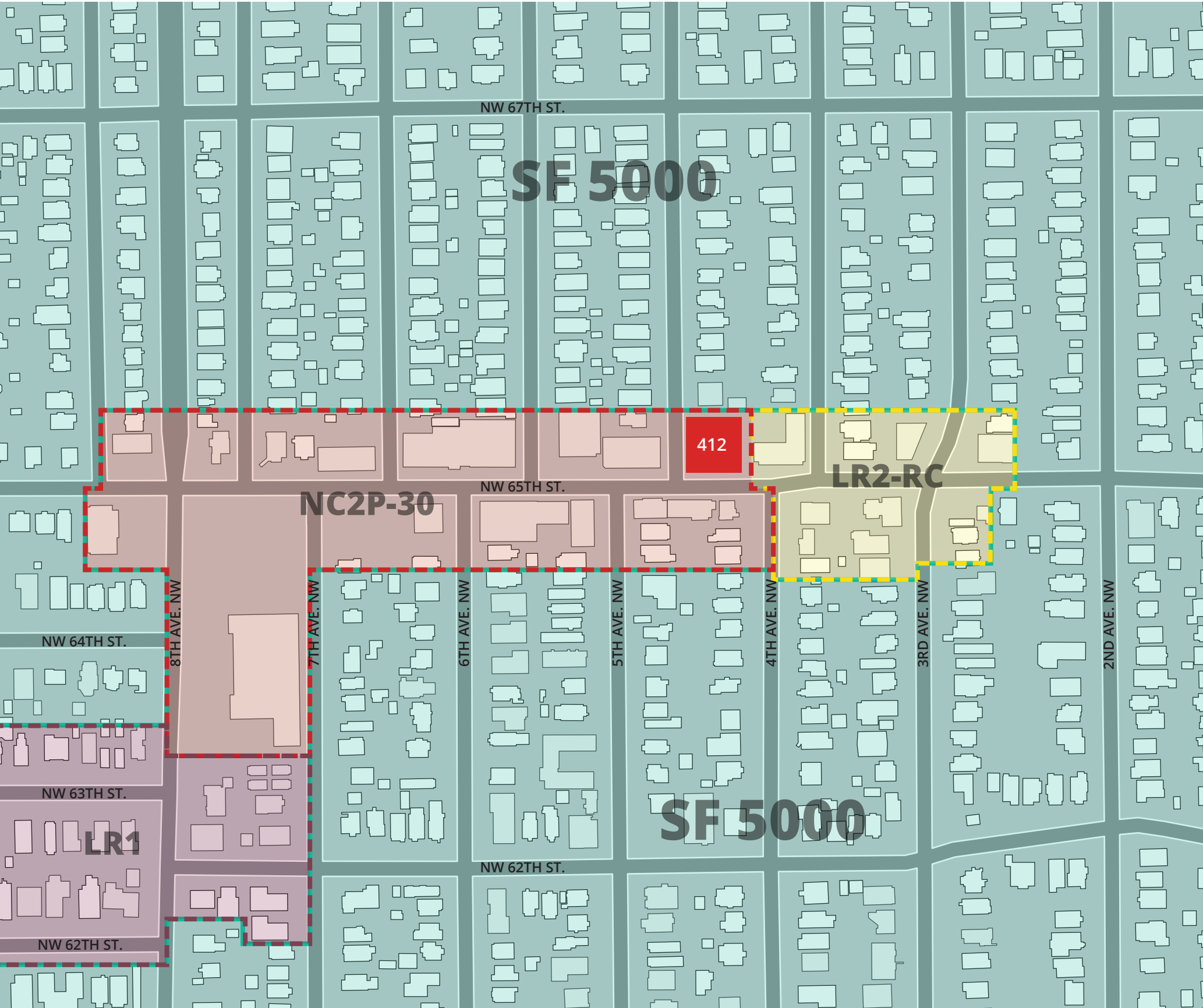
ZONING

NC2P-30

LR2-RC

LR1

SF5000





ADJACENCIES / CIRCULATION

The immediate vicinity of the site is dominated by commercial usages such as restaurants, bars, shops, and light industrial and service businesses. Surrounding this area are single family structures with a small amount of multifamily buildings. This portion of NW 65th St. is a moderately busy arterial street connecting Phinney Ridge to Ballard, and sits in a valley just west of Phinney Ridge. The street grid shifts between 3rd and 8th Ave. NW, creating a sawtooth condition with 3 sided intersections along NW 65th St. Bus route #28 and #994 are readily available for public transportation options, with the #5 bus nearby on Phinney Ave. N. Car sharing system is available from Zipcar.

TPOLOGY

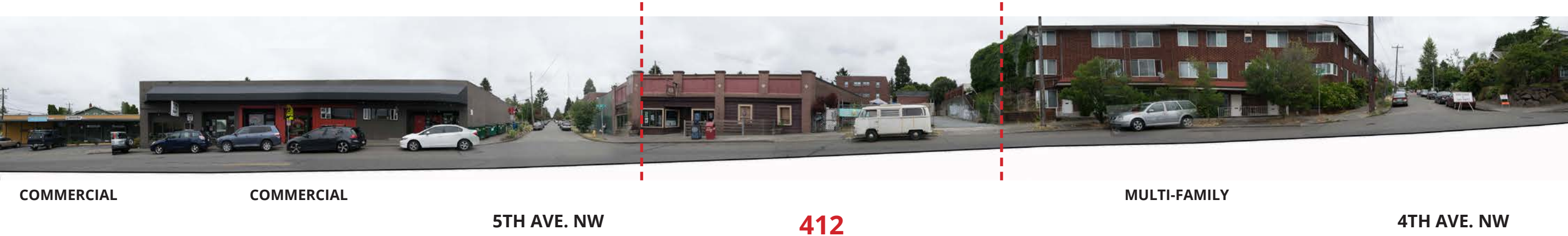
- SINGLE FAMILY
- DUPLEX/TRIPLEX
- MULTI-FAMILY
- MIXED USE
- COMMERCIAL
- RELIGIOUS
- PARKING
- VACANT LOT

CIRCULATION

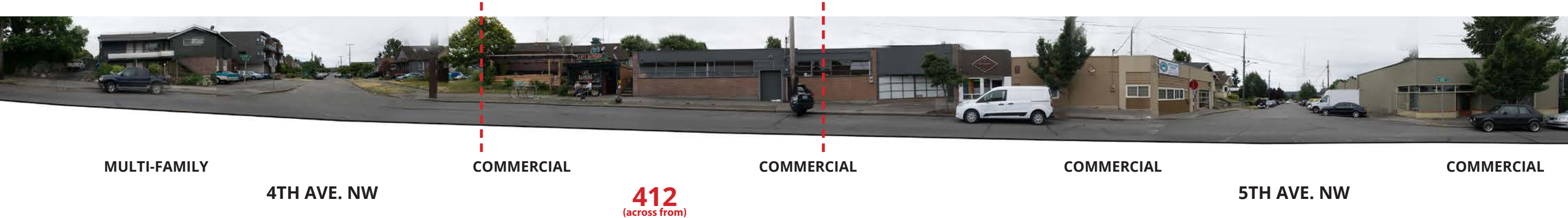
- MAJOR ARTERIAL
- MINOR ARTERIAL
- DEDICATED BIKE LANE
- TRANSIT ROUTE
- DESIGNATED BUS STOP
- ZIPCAR LOCATIONS
- CITY OWNED BIKE RACKS



1 NW 65TH ST. LOOKING NORTH



2 NW 65TH ST. LOOKING SOUTH



NW 65TH ST.

NW 65th St. is an engaging commercial and retail area with a variety of restaurants and shops. The scale is generally low, with buildings of various styles and with unique signage. This portion of the neighborhood is in a transitional state and the streetscape needs to be carefully considered in relation to setting a future precedent.



NW 65TH ST. FACING WEST



NW 65TH ST. FACING EAST

3 5TH AVE. NW LOOKING EAST



4 5TH AVE. NW LOOKING WEST



5TH AVE NW - FACING NORTH



5TH AVE NW - FACING SOUTH

5TH AVE. NW

5th Ave. NW transitions the commercial frontage on NW 65th St. to a single family neighborhood. The north end of the site is a 3 story apartment building which transitions the scale and intensity from NW 65th St.



ARCHITECTURAL CONTEXT

SINGLE FAMILY HOUSES

- Built up from sidewalk
- Many houses have garages facing the street
- Strong sense of rhythm of single family houses
- Generous front landscaping
- Traditional forms and styles



6542 5TH AVE. NW



6542 5TH AVE. NW

INFILL COMMERCIAL

- Varied facade styles
- Uniform street presence
- Transparent storefronts
- Generous signage and window dressing



BABALOUISE, THE SNEAKERY, MOLLY MAGUIRES
610-616 NW 65TH ST.

MULTI-FAMILY

- Varied styles and time periods
- Good 'eyes on the street' urban form
- Entries face main street at front of building



404 NW 65TH ST.



6512 5TH AVE. NW

COMMERCIAL IN HOUSES

- Eclectic business types and styles
- Elevated from street
- Unique signage and street presence



HERBS HOUSE
716 NW 65TH ST.



CAFE BAMBINO
405 NW 65TH ST.

MIXED-USE

- Clear delineation of housing and commercial
- Strong window rhythm
- Transparent street-facing facades
- Entry recesses, signage, and canopies create friendly pedestrian experience



OLSEN VIOLINS
214 NW 65TH ST.



EYES ON THE CITY
603 NW 65TH ST.

COMMERCIAL WITH PARKING

- Low and horizontal facades
- Wide exposure on street/ parking lot
- Recessed entry with parking in front is less desirable urban form

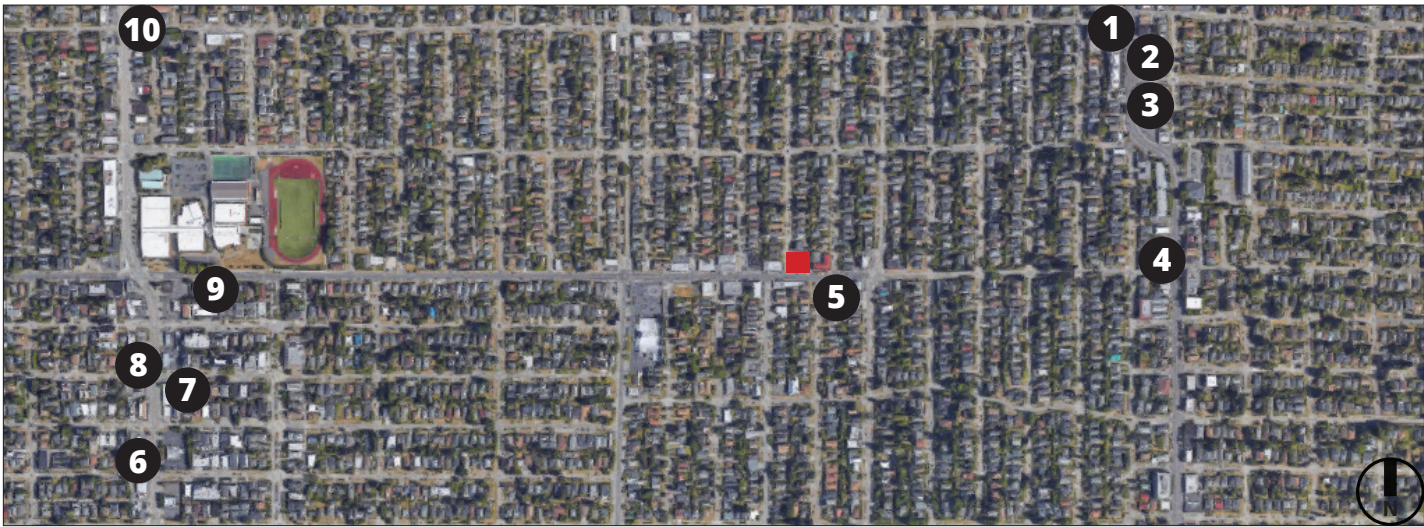


SURF CLEANERS, REMNANT KING CARPET
AND FLOORING
800 NW 65TH ST.



EQUINOX HEALING ARTS, SALMON BAY
PHYSICAL THERAPY
6500 6TH AVE. NW

RECENT/PROPOSED DEVELOPMENT



7009 GREENWOOD AVE. N (Johnston Architects)



6800 GREENWOOD AVE. N (NK Architects)



6726 GREENWOOD AVE. N (SJArchitect)



6503 PHINNEY AVE. NW (Cone Architecture)



6254 4TH AVE. NW (unknown)



1506 NW 61ST ST. (Skidmore Janette)



1443 NW 63RD ST. (Cleave Architecture + Design)



6301 15TH AVE. NW (SHW)



1427 NW 65TH ST. (b9 Architects)



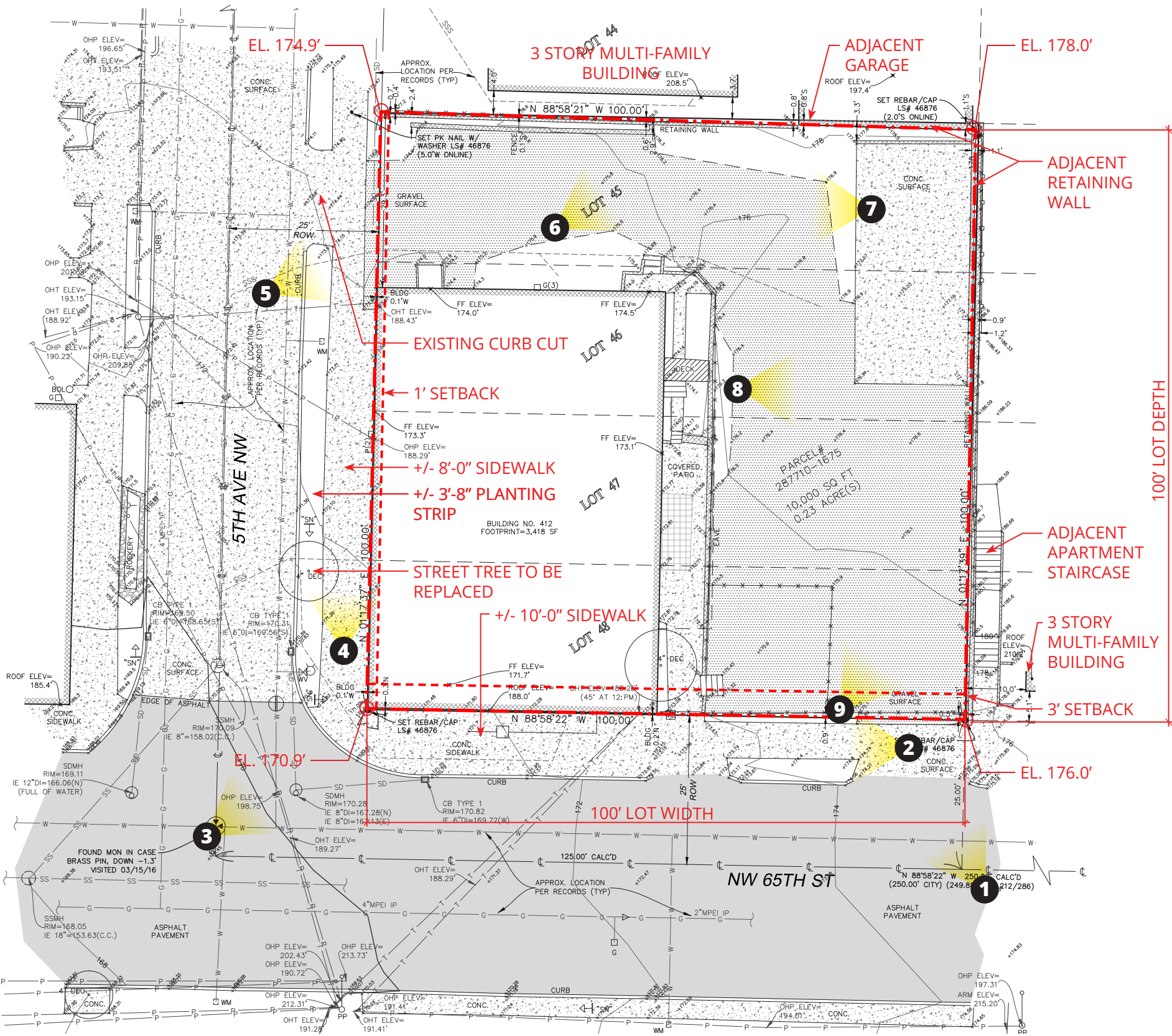
7016 15TH AVE. NW (Caron Architecture)

SURVEY

The site is located on the corner of NW 65th St. and 5th Ave. NW. It includes a 1 story commercial building on the corner and empty space used mostly as a parking lot. A retaining wall approximately 10 feet high is located on the north of property line, with a smaller wall and out-building to the north. The topography of the site slopes up from the southwest corner to the northeast corner. A 3 foot setback along NW 65th St. and a 1 foot setback on 5th Ave. NW are required. One curb cut off of 5th Ave. NW and two curb cuts off NW 65th St. access the site. A small planting strip approximately 3 to 4 feet wide runs along portions of NW 65th St. and the entirety of 5th Ave. NW.

LEGAL DESCRIPTION

TAX PARCEL NO. 2877101675
LOTS 45 TO 48, INCLUSIVE, BLOCK 7, GREEN LAKE CIRCLE RAILROAD ADDITION TO THE CITY OF SEATTLE, AS PER PLAT. RECORDED IN VOLUME 2 OF PLATS, PAGE 170, RECORDS OF KING COUNTY: SITUATED IN THE COUNTY OF KING, STATE OF WASHINGTON. COMMONLY KNOWN AS 412 NW 65TH STREET, SEATTLE, WASHINGTON, 98117.



Surveyor: GeoDimensions Date: 03/22/16

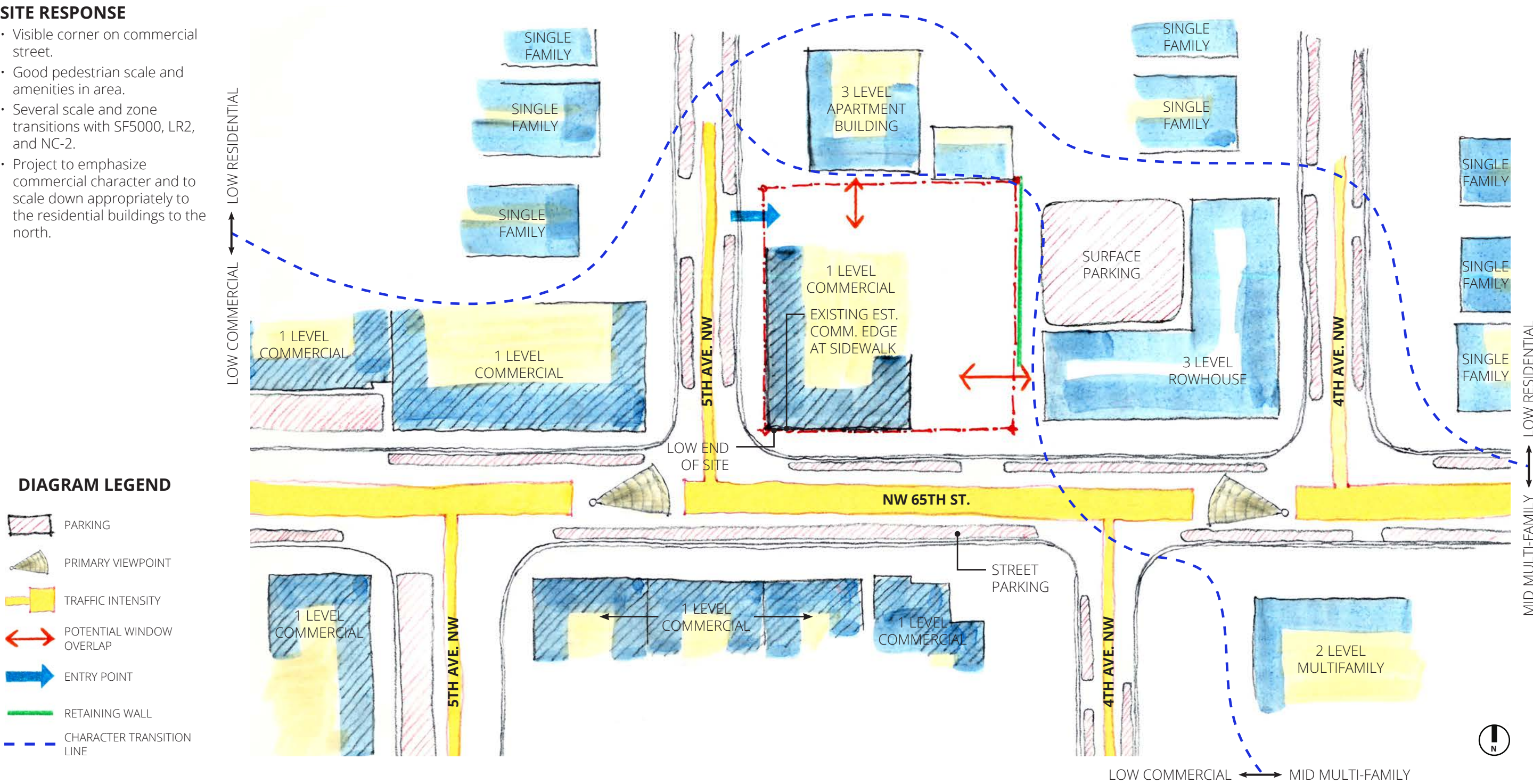
EXISTING SITE: IMAGES



EXISTING SITE: CONSTRAINTS

SITE RESPONSE

- Visible corner on commercial street.
- Good pedestrian scale and amenities in area.
- Several scale and zone transitions with SF5000, LR2, and NC-2.
- Project to emphasize commercial character and to scale down appropriately to the residential buildings to the north.



ZONING: NC2P-30 (NW 65TH ST. IS PRINCIPAL PEDESTRIAN STREET)
OVERLAYS: N/A

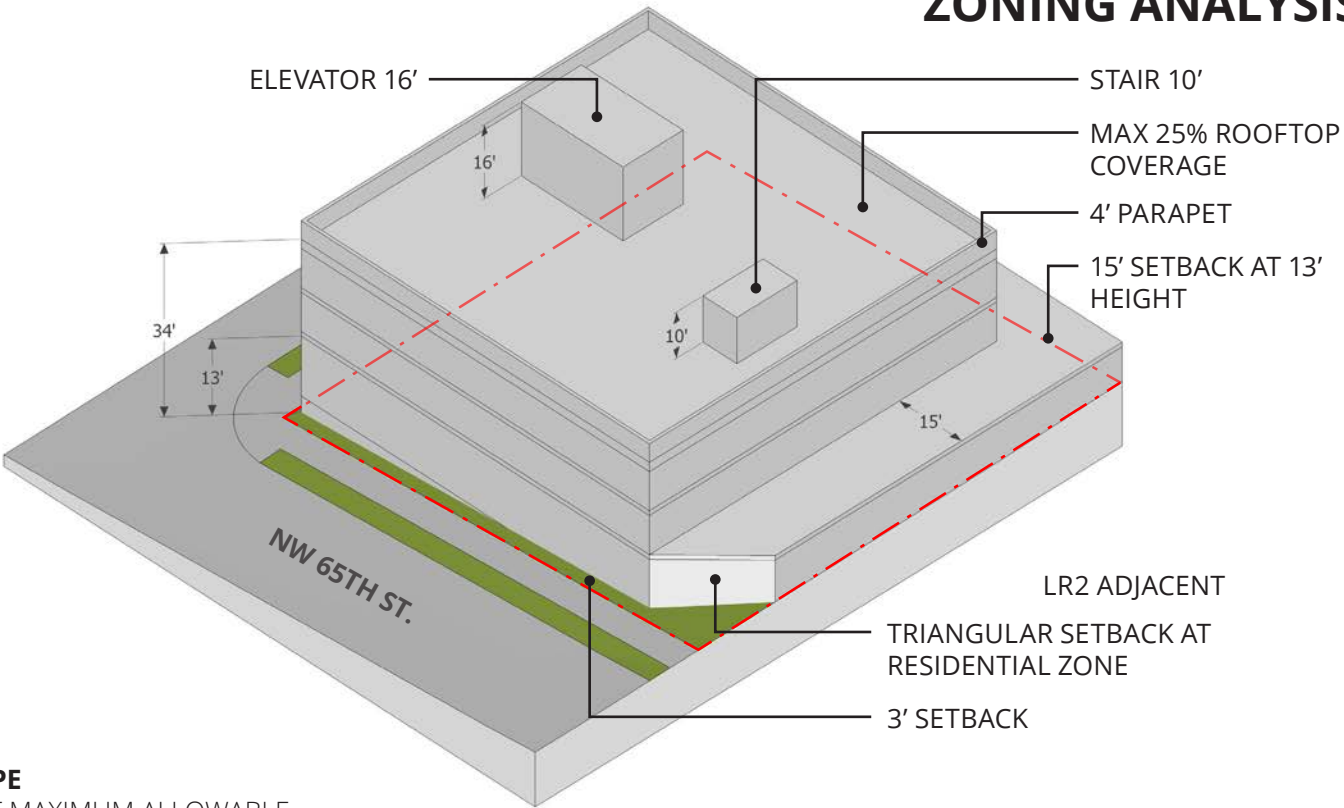
SMC CODE	DEFINITION
SMC 23.47A.004	PERMITTED USES <ul style="list-style-type: none">- Residential, commercial, and live-work use permitted.
SMC 23.47A.013.B	FLOOR AREA RATIO <ul style="list-style-type: none">- 2.5 FAR allowed for mixed-use provided the limit for either use does not exceed 2.25 FAR.
SMC 23.47A.012.A	HEIGHT <ul style="list-style-type: none">- Base height limit of 30' above average grade.- Additional 4' of height if non-residential uses at street-level provides 13' floor-to-floor.- Parapets, railings etc. may extend 4' above the allowed height limit.- Elevator penthouses: 16', stair penthouses: 10' above allowed height limit.
SMC 23.47A.014.B.3	SETBACKS <ul style="list-style-type: none">- Front: No setback- Side: 15'-0" setback at side and rear lot line for structures above 13'-0" to a height of 40'-0", with a 2'-0" additional setback for every 10'-0" of height.- Side: 15'x15' triangular setback to adjacent residential zones.
SMC 23.47A.024.A	AMENITY AREA <ul style="list-style-type: none">- 5% of total residential gross floor area.
SMC 23.47A.016.A.2	LANDSCAPING <ul style="list-style-type: none">- Green factor of 0.30 minimum for 4 or more dwelling units.
SMC 23.47A.008.A.2	BLANK FACADES <ul style="list-style-type: none">- Between 2'-8" above the sidewalk, blank facades may not exceed 20' in width, or 40% of the total width of the facade along the street.
SMC 23.47A.008.A.3	STREET LEVEL FACADES <ul style="list-style-type: none">- Facades shall be located within 10'-0" of the street lot line.
SMC 23.47A.008.B.3	NON-RESIDENTIAL DEPTH <ul style="list-style-type: none">- For non-residential provide 30' average depth and minimum 15' from street level street facing facade.
SMC 23.47A.008.B.4	NON-RESIDENTIAL HEIGHT <ul style="list-style-type: none">- Non-residential uses at street level shall have minimum floor-to-floor height of 13'-0".
SMC 23.47A.008.C.1	STREET LEVEL USES <ul style="list-style-type: none">- A minimum of 80 percent of the width of a structure's street-level street-facing facade that faces a principal pedestrian street shall be occupied by uses listed in subsection 23.47A.005.D.1. The remaining 20 percent of the street frontage may contain other permitted uses and/or pedestrian entrances.
SMC 23.47A.008.C.4	OVERHEAD WEATHER PROTECTION <ul style="list-style-type: none">- Continuous overhead weather protection (i.e., canopies, awnings, marquees, and arcades) is required along at least 60 percent of the street frontage of a structure on a principal pedestrian street.
SMC 23.47A.022.A	LIGHTING AND GLARE <ul style="list-style-type: none">- Exterior lighting shall be shielded and directed away from adjacent properties.
SMC 23.54.015.D.1	REQUIRED PARKING <ul style="list-style-type: none">- 1 space for each dwelling unit, 0 spaces for L/W unit under 1,500 SF, 0 spaces for first 1,500 SF of each business establishment in pedestrian designated zones.
SMC 23.54.040.B	SOLID WASTE & RECYCLING <ul style="list-style-type: none">- Residential (9 units): 150 SF- Commercial (0-5,000 SF): 82 SF/2 for mixed used developments = 41 SF- Total trash area required: 191 SF

ZONING ANALYSIS



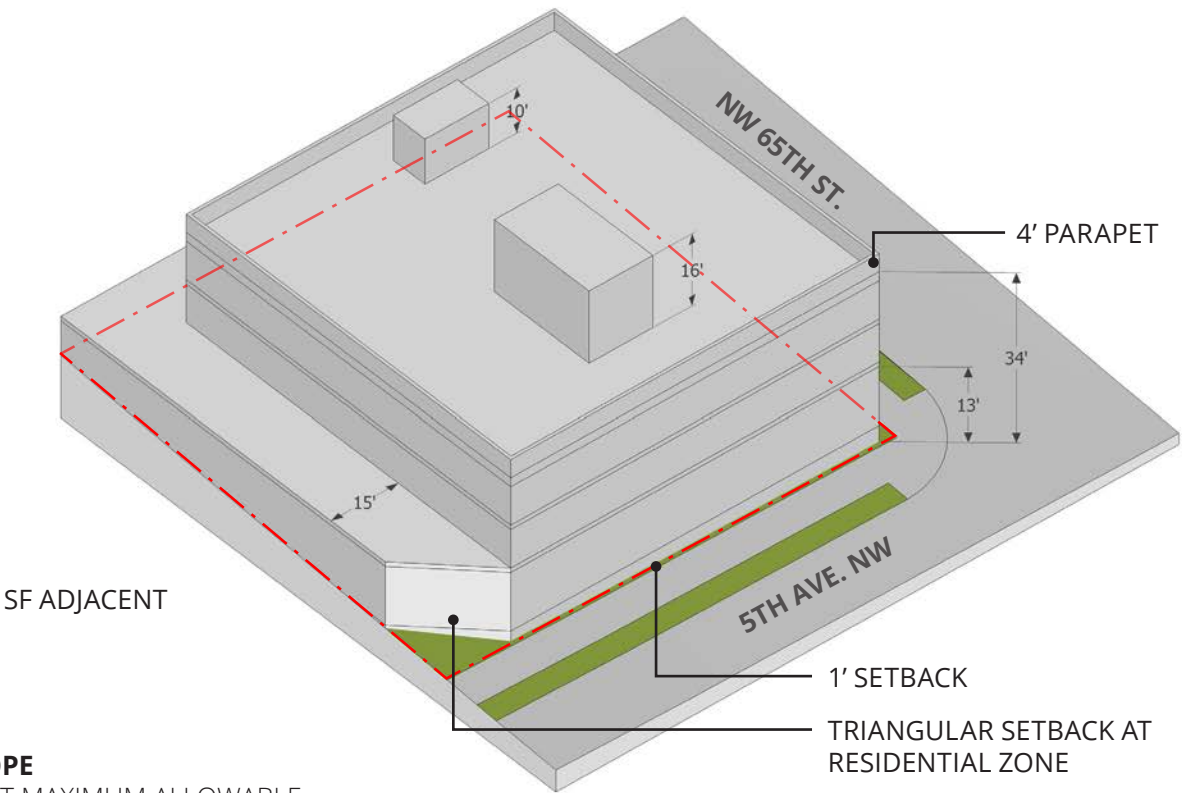
SE CORNER

ZONING ENVELOPE
BUILDING MASS AT MAXIMUM ALLOWABLE
ZONING ENVELOPE



NW CORNER

ZONING ENVELOPE
BUILDING MASS AT MAXIMUM ALLOWABLE
ZONING ENVELOPE



PRIORITY GUIDELINES: CITY WIDE GUIDELINES

CONTEXT AND SITE

CS1: URBAN PATTERN & FORM

B.1: SUN AND WIND

The upper level massing is broken down and articulated to allow sun exposure and ventilation to reach the two story units.

C.1: TOPOGRAPHY

A ten foot high retaining wall along the northeast property line allows the parking to be essentially buried in the back of the site, reducing view into the garage. An out building on the property to the north further blocks views into the garage.

CS2: URBAN PATTERN & FORM

C.1: SENSE OF PLACE

NW 65th St is a developing commercial district and presents an opportunity to strengthen and reinforce the existing character while setting a precedent for future development. The area has a unique light industrial quality mixed with pedestrian scaled commercial.

C.1: CONNECTION TO THE STREET

Project to establish a strong street presence with a commercial edge and a distinct residential entry on NW 65th St. Two commercial spaces will front NW 65th St., with transparent facades and cafe seating or landscaping. Canopies, signage, and lighting will add texture and help orient visitors to building uses. The live-work units are set back farther that the commercial spaces along NW 65th St. in order to help transition zoning and building scale.

C.1: CORNER SITES

Visible corner and zone transition(s) provides opportunity for strong massing presence and articulation. The three-sided corner will allow a variety of views into and from the site, while providing a bookend to the neighborhood's main commercial strip.

C.1: EXISTING DEVELOPMENT AND ZONING

The neighborhood has an eclectic mix of scales , styles, and uses. An appealing pedestrian oriented commercial district is reinforced through a transparent facade and prominent corner at NW 65th St. This project will set a future precedent for a growing neighborhood.

D.3: ZONE TRANSITIONS

The live-work units along 5th Ave. NW help transition scale and building function to the multifamily and single-family structures to the north. They are set back further from the commercial frontage by over four feet, mitigating the requirements between the two zones. The LR2 adjacency to the east is elevated from the project site, allowing parking to be located below grade on the east property line, and to reduce the perceived bulk and height of the proposed structure.

CS3: ARCHITECTURAL CONTEXT & CHARACTER

A.4: EVOLVING NEIGHBORHOODS

The immediate area has had little recent development. This project sets a new precedent, and will establish and strengthen a strong pedestrian oriented area. The building materials, scale, and massing will balance with the existing conditions and anticipate future building integration.

PUBLIC LIFE

PL1: CONNECTIVITY

B.3: PEDESTRIAN AMENITIES

The commercial space at the corner will be transparent and incorporate cafe seating where possible. The project will establish a strong street edge, with clear entries, and R.O.W. features including plantings, canopies, and bike racks. Along 5th Ave. NW, low planting in front of the live-work entries will buffer the transition to residential scale.

PL2: WALKABILITY

B.1: EYES ON THE STREET

Street level uses and units along NW 65th St. are oriented toward the street. Two commercial spaces, the primary residential entry, and six two-story apartment units all face NW 65th St. Along 5th Ave. NW the live-work entries open directly onto the sidewalk.

B.3: STREET LEVEL TRANSPARENCY

Organization of street level uses allows for a transparent ground floor to face both NW 65th St, and 5th Ave. NW.

D.1: DESIGN AS WAYFINDING

The entries into the residential portion of the building will be open and clearly marked by signage, architectural features, and material changes. The live-work entries on 5th Ave. NW have clearly marked, transparent entries and feature landscaping at each front door.

PL3: STREET LEVEL INTERACTION

A.1: ENTRIES

The entries, including the primary residential entry, commercial entries, and live-work entries along 5th Ave. NW will be clearly defined and articulated. The materials and detailing at these conditions will reflect the uses the entries serve, and signage will help direct visitors.

A.1: ENSEMBLE OF ELEMENTS

The primary residential entry will feature special paving, landscaping, lighting, and signage to create interest and visual appeal. The commercial entires and residential entry will be clearly defined and separated visually and by program.

B.1: LIVE-WORK EDGES

The non-residential portions of the live-work units are oriented towards the street, and landscaping at each entry creates visual interest and a buffer from 5th Ave. NW. The mix of residential and commercial use presents an opportunity to transition between the NC-2 and SF zones.

PL4: ACTIVE TRANSPORTATION

A.1: CONNECTIONS TO ALL MODES

The primary commercial entries as well as the primary residential entry are located on NW 65th St. because this street is the most logical to locate building access.

B.1: BIKE FACILITIES

Residential parking to be conveniently located for tenants. Commercial parking to be integrated into the R.O.W.

DESIGN CONCEPT

DC1: PROJECT USES & ACTIVITIES

A.4: VIEWS AND CONNECTIONS

The commercial spaces are organized to maximize street connections and views into and out of the building. An open corner and possible cafe seating will activate the corner of NW 65th St. and 5th Ave. NW. The second floor apartments are oriented to allow air, light, and views to penetrate the site.

B.1: ACCESS LOCATION AND DESIGN

The driveway is located in order to minimize its presence on 5th Ave. NW. Buffer landscaping along the north property line and green wall along the existing retaining wall to the east help mitigate the impact of parking and vehicle circulation.

C.1: BELOW GRADE PARKING

Taking advantage of the existing topography allows the parking for the project to be located below grade at the northeast portion of the site. Service uses and accessory program elements will be located in this area as well, hiding it from the street-facing facades.

DC2: ARCHITECTURAL CONCEPT

A.1: MASSING/SITE CHARACTERISTICS

The building massing is modulated into smaller parts to minimize the visual weight of the project. The design creates a strong visual connection to the street, with a prominent corner open to the street at the pedestrian level.

B.1: FACADE COMPOSITION

All facades are carefully composed, articulated, and well-proportioned to minimize blank walls and create an attractive design from all angles. Window arrangements will vary according to building use and materials to create further visual interest.

C.1: VISUAL DEPTH AND INTEREST

The lower portions of the building will be set back slightly from the massing above, creating a shadow line throughout the project. The commercial fenestration is recessed, and landscaping and seating elements will create interest and activate the building edges.

E.1: LEGIBILITY AND FLEXIBILITY

The project will have a classic quality, with residential over a tall commercial space that is clearly defined by massing and material choices.

DC3: OPEN SPACE CONCEPT

B.4: MULTIFAMILY OPEN SPACE

The open space at the level 2 plaza is well landscaped and furnished, with integrated planting, flexible seating, and privacy screening to neighbors. Roof deck design provides opportunities for a variety of uses, and is also separated from adjacent properties.

DC4: MATERIALS

A.1: EXTERIOR FINISH MATERIALS

The building materials will be of high quality and be designed to emphasize massing articulation and scale. The material choices and configurations will help transition scales from commercial on NW 65th st. to residential on 5th Ave. NW.

B.1: SIGNAGE

Because the project has a variety of uses and access relationships, high quality signage will be used throughout to direct visitors and maintain privacy for residents.

INTENTIONALLY BLANK

SCHEME A: CODE COMPLIANT

SCHEME INFO

GSF: 18,745
UNITS: 8
LIVE/WORK UNITS: 3
COMMERCIAL: 2,249 SF (2 SPACES)
TOTAL AMENITY: 2,616 SF
2,616 SF - ROOF DECKS
1.44 FAR - 14,473 GFA

POSITIVE

- No departures requested
- Efficient layout

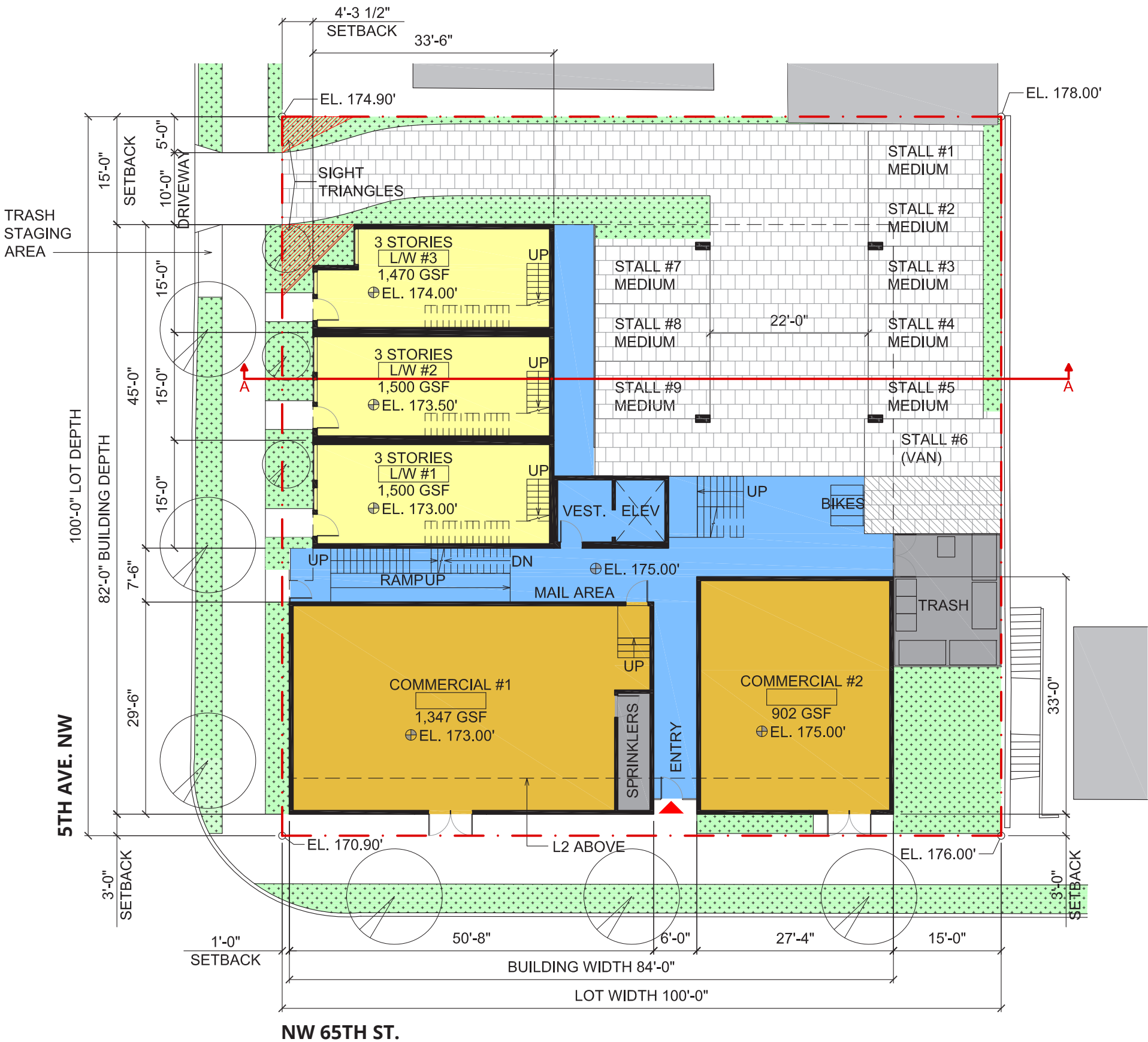
NEGATIVE

- Most bulk, interior space
- Less opportunities for modulation of massing
- Most exposure to east and north SF/LR adjacencies
- Least useful shared amenity spaces

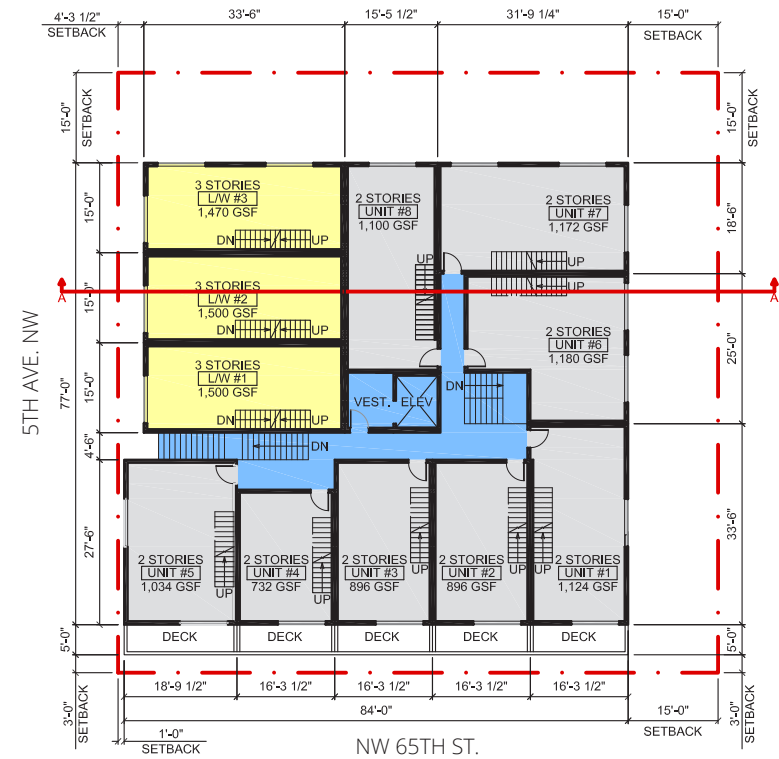
STREET LEVEL PLAN

SCALE: 1/16" = 1'-0"

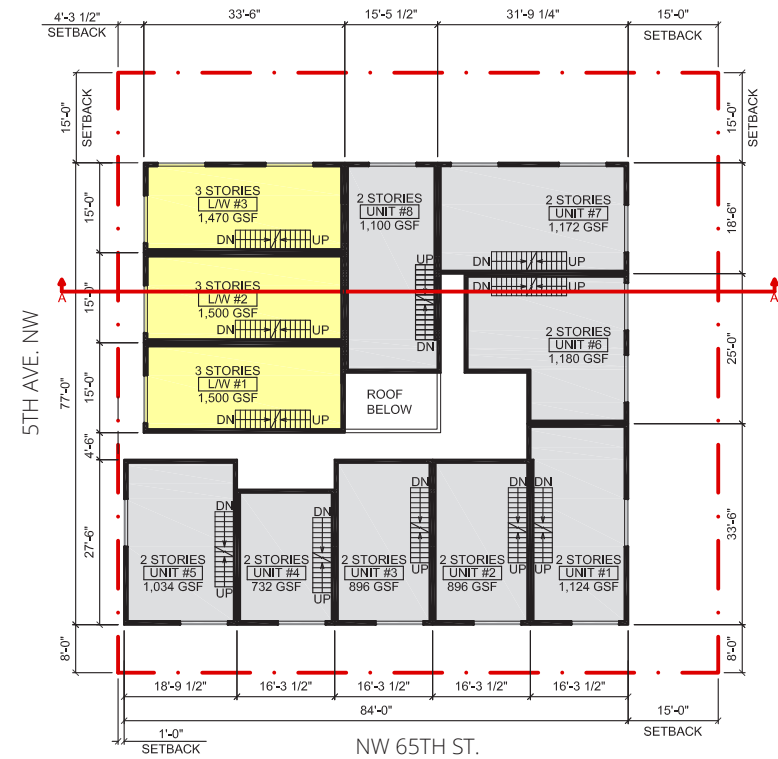
- LIVE/WORK
- COMMERCIAL
- BUILDING SERVICE
- RESIDENTIAL
- CIRCULATION
- PRIVATE AMENITY
- PRIMARY BUILDING ENTRANCE



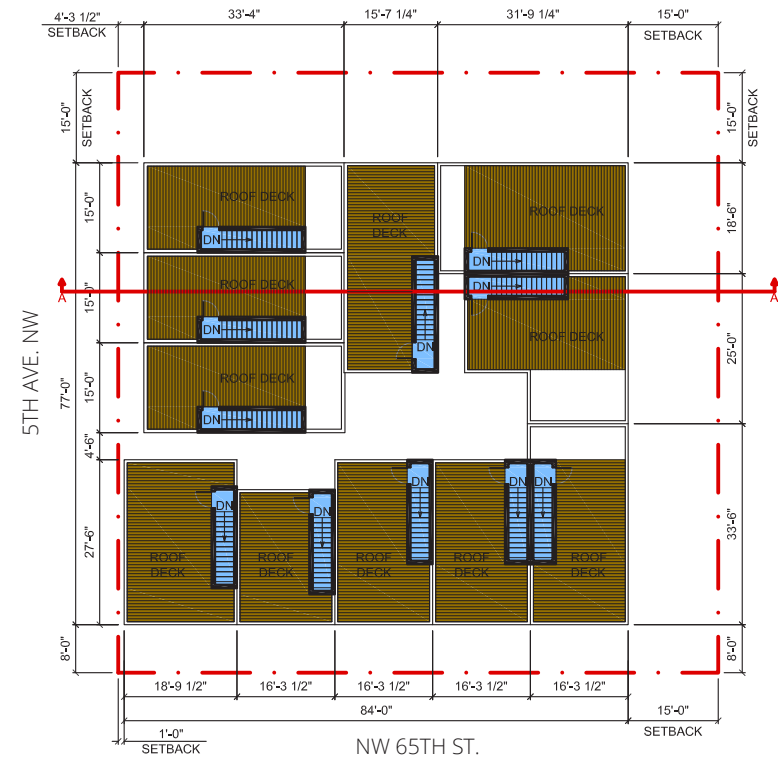
SCHEME A: BUILDING PLANS/SECTIONS SCALE: 1/32" = 1'-0"



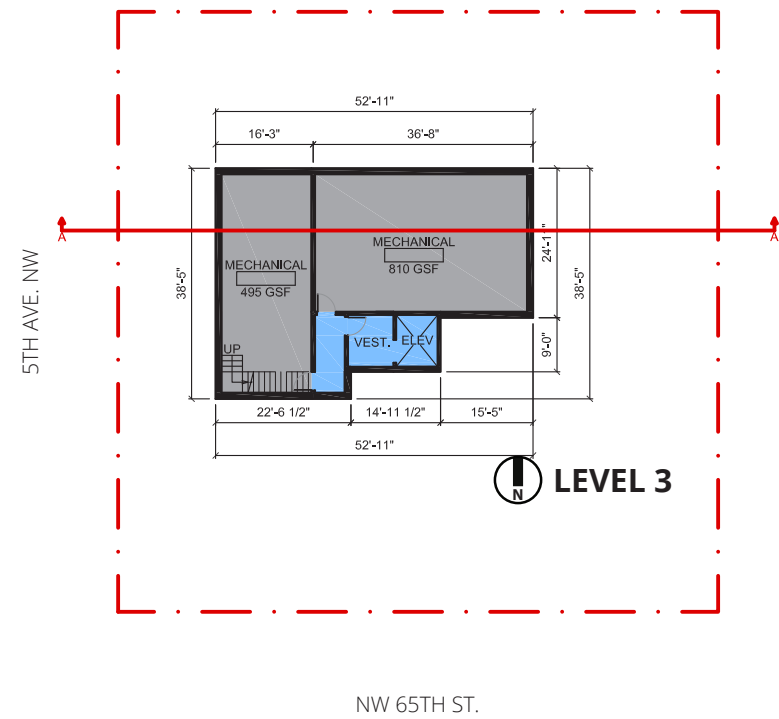
LEVEL 2



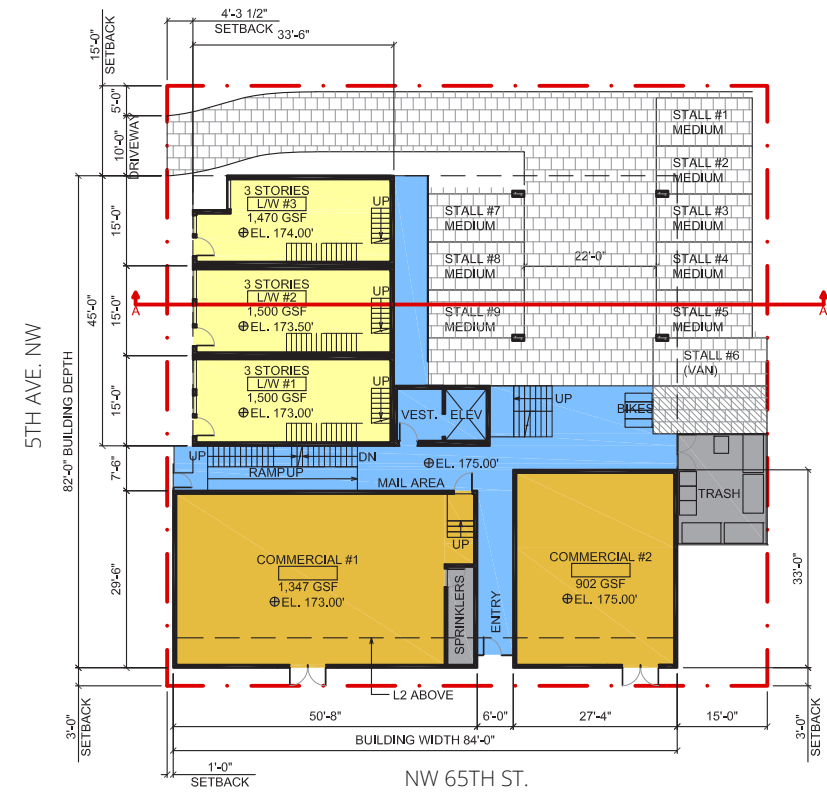
LEVEL 3



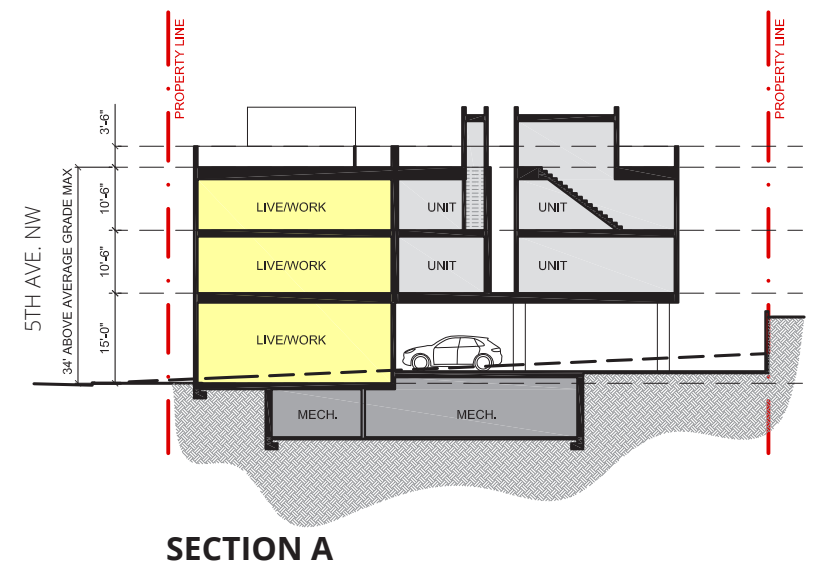
ROOF



BASEMENT



LEVEL 1



SECTION A

SCHEME A: CODE COMPLIANT

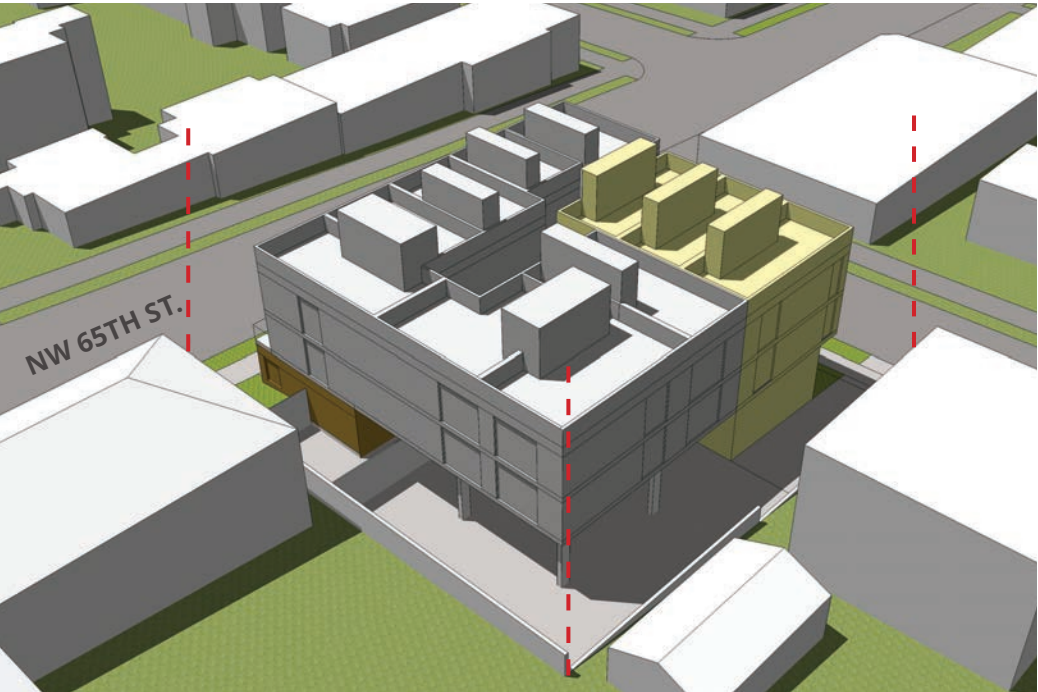
*Street trees and canopies not shown for clarity



PERSPECTIVE LOOKING AT SOUTHWEST CORNER



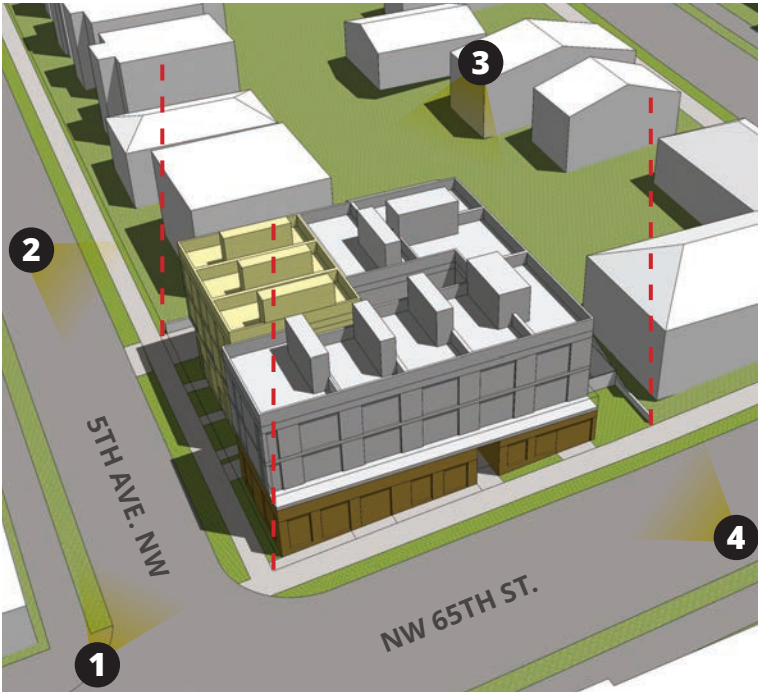
PERSPECTIVE LOOKING AT NORTHWEST CORNER



AERIAL PERSPECTIVE LOOKING AT NORTHEAST CORNER



PERSPECTIVE LOOKING AT SOUTHEAST CORNER



AERIAL PERSPECTIVE LOOKING AT SOUTHWEST CORNER

SCHEME A: DEPARTURES

NO DEPARTURES REQUESTED

SCHEME A: SUN PATH / SHADOW STUDY



SCHEME B: COURTYARD

SCHEME INFO

GSF: 19,756
UNITS: 9
LIVE/WORK UNITS: 3
COMMERCIAL: 2,752 SF (2 SPACES)
TOTAL AMENITY: 3,528 SF
2,968 SF - ROOF DECKS
300 SF - PLAZA AT L2
260 SF - AT GRADE
1.61 FAR - 16,077 GFA

POSITIVE

- Creates ‘courtyard’ type entry on NW 65th St.
- Axial layout allows open exterior at level 2

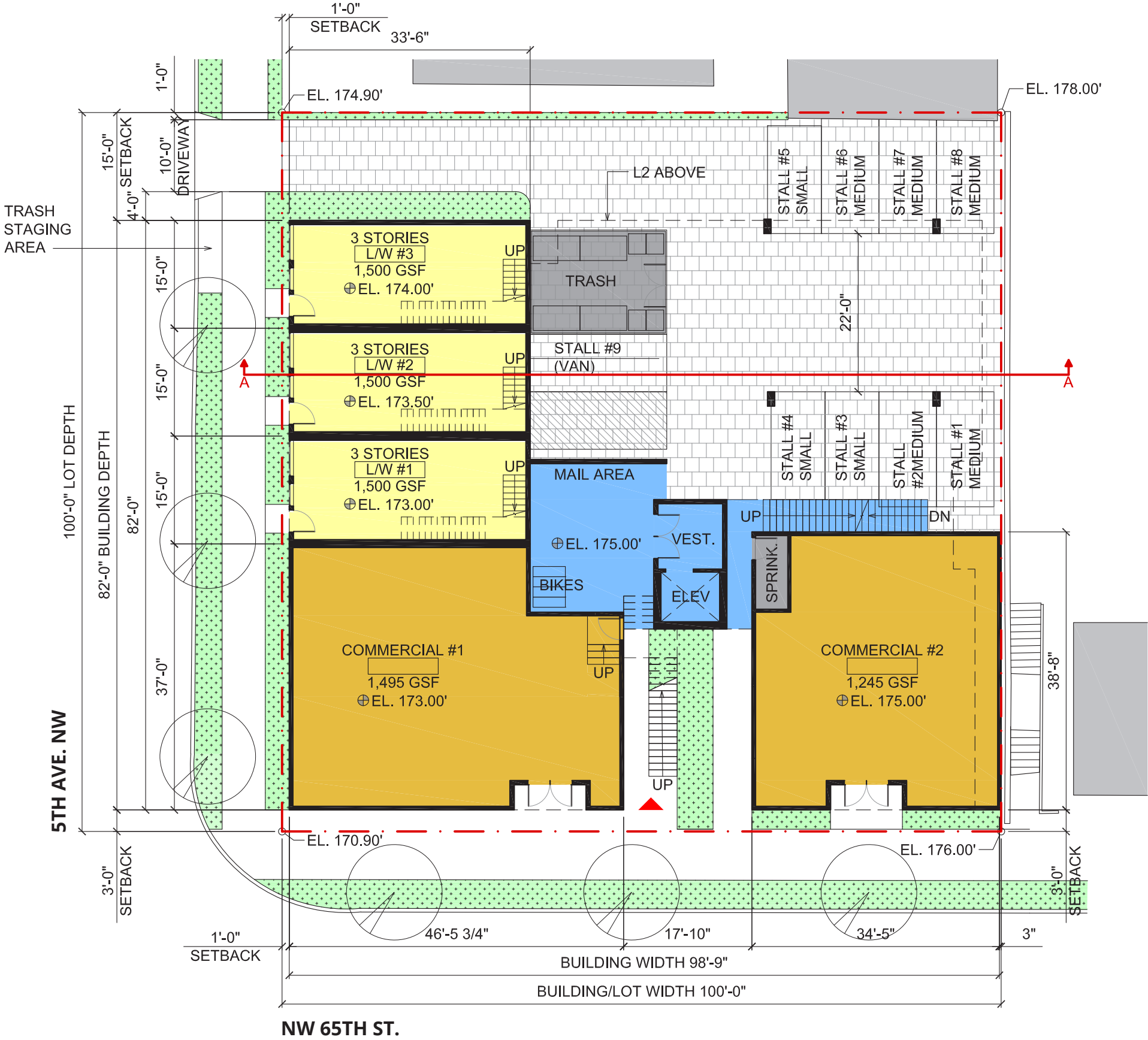
NEGATIVE

- Setback departures
- Gap in commercial street facade may not be desirable
- Least amount of massing articulation
- Most north (rear) facing units

STREET LEVEL PLAN

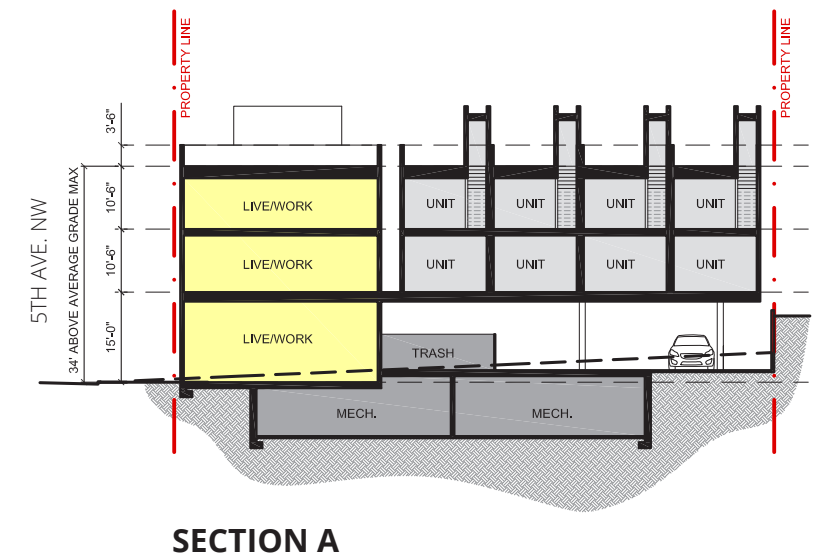
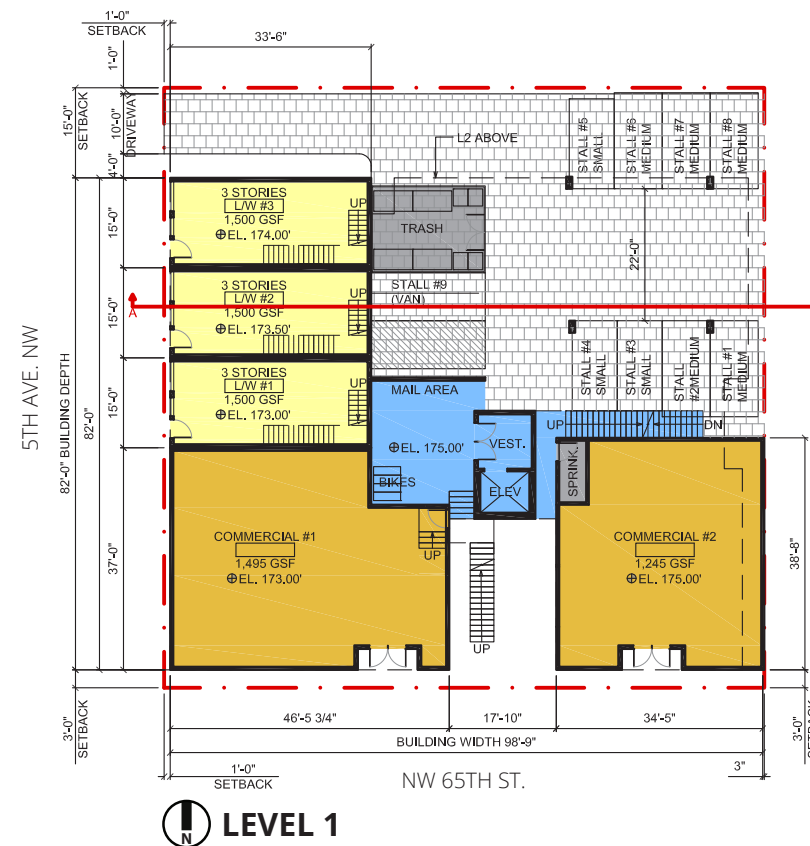
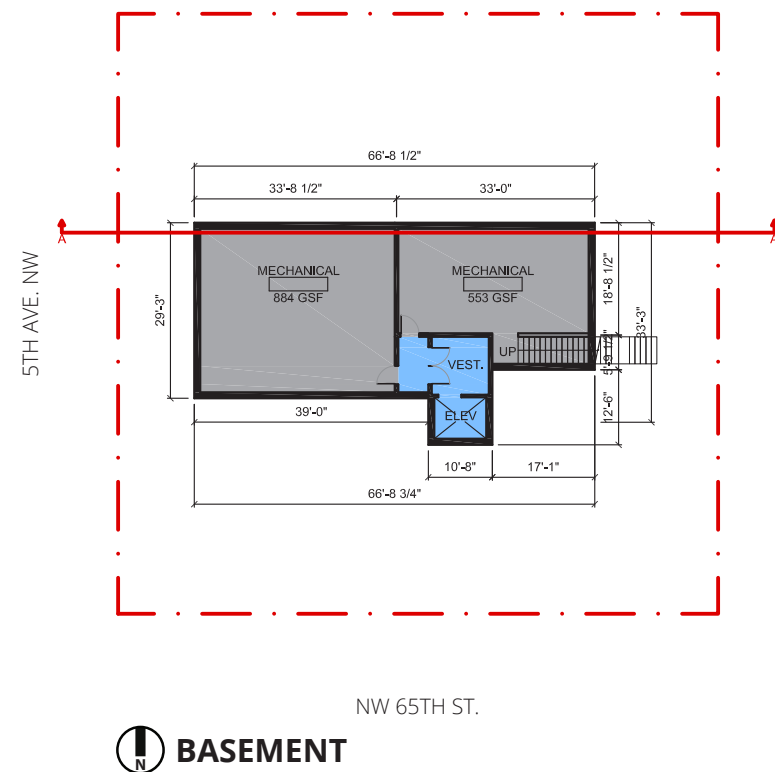
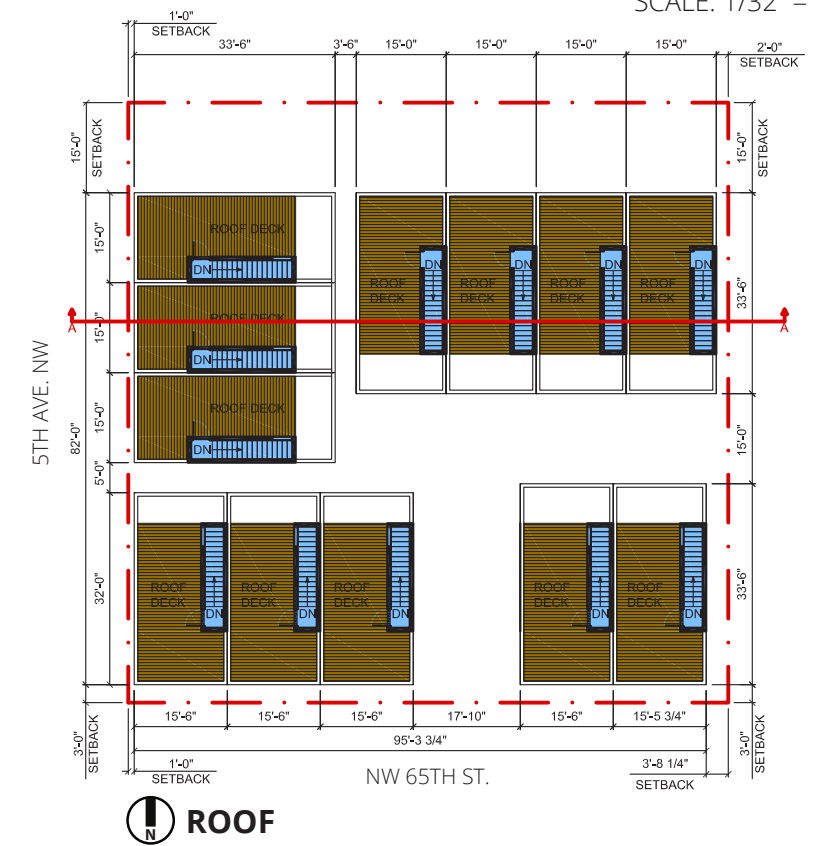
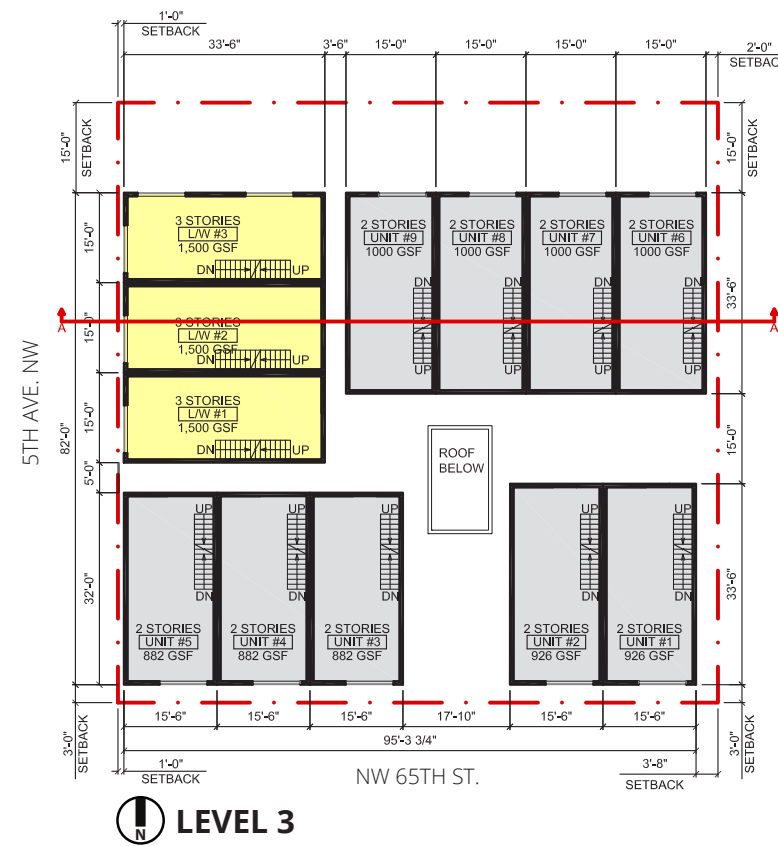
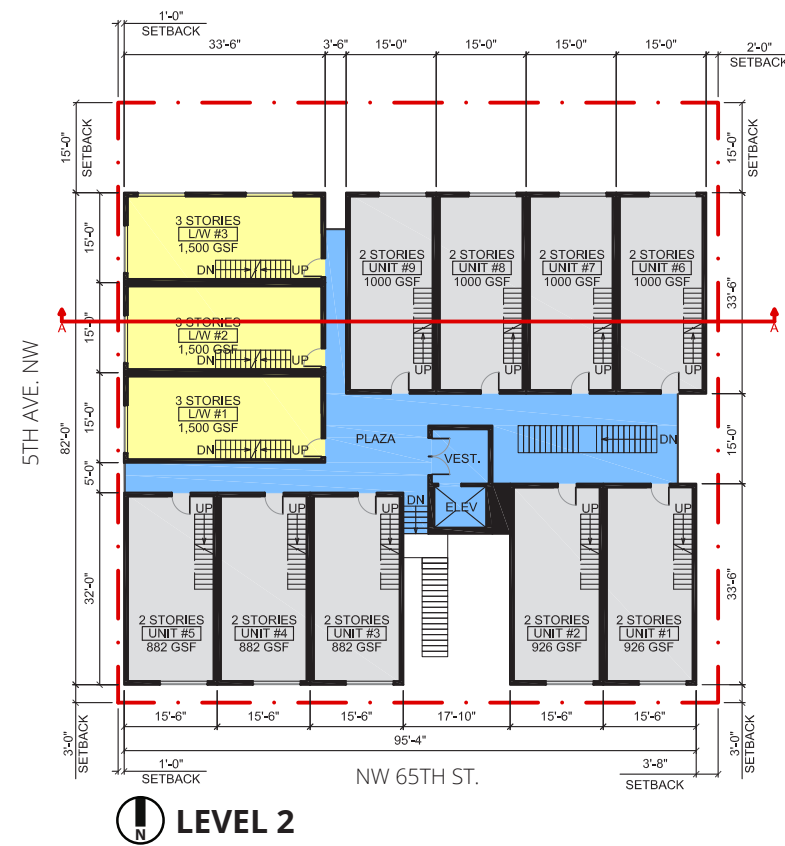
SCALE: 1/16" = 1'-0"

- LIVE/WORK
- COMMERCIAL
- BUILDING SERVICE
- RESIDENTIAL
- CIRCULATION
- PRIVATE AMENITY
- PRIMARY BUILDING ENTRANCE



SCHEME B: BUILDING PLANS/SECTIONS

SCALE: 1/32" = 1'-0"



SCHEME B: COURTYARD

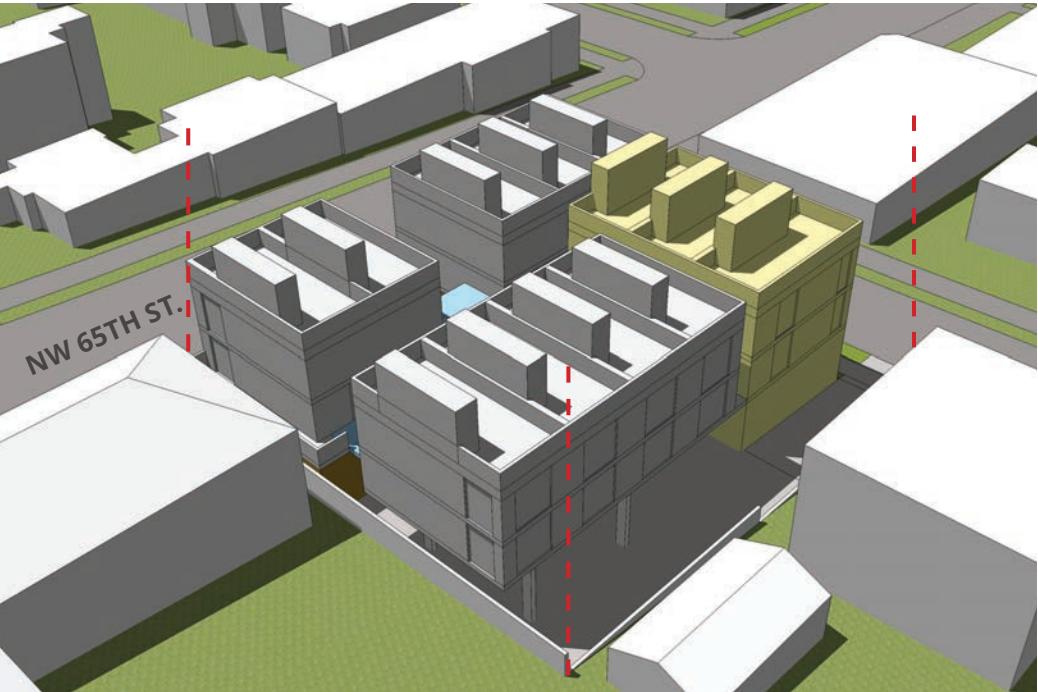
*Street trees and canopies not shown for clarity



PERSPECTIVE LOOKING AT SOUTHWEST CORNER



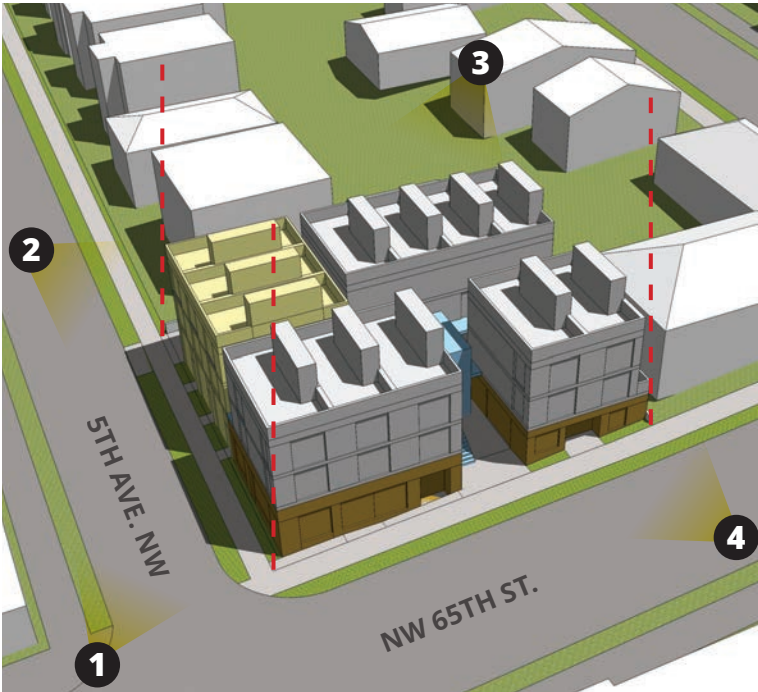
PERSPECTIVE LOOKING AT NORTHWEST CORNER



AERIAL PERSPECTIVE LOOKING AT NORTHEAST CORNER



PERSPECTIVE LOOKING AT SOUTHEAST CORNER



AERIAL PERSPECTIVE LOOKING AT SOUTHWEST CORNER

SCHEME B: SUN PATH / SHADOW STUDY



SCHEME B: DEPARTURES

1 RESIDENTIAL ZONE ABUTMENT
SMC 23.47A.014.B.1: RESIDENTIAL ZONE ABUTMENT

REQUIRED: 15'-0" triangular setback at side/front lot line of residential lot.

PROVIDED: 0'-0" triangle at east property line, 14'-4" at north property line. *14'-9" triangle departure requested at east property line, 8" triangle at north property line.*

JUSTIFICATION: The triangular setback along NW 65th St. breaks up the commercial character and continuity on NW 65th St., and creates a non-desirable empty space along the street. The requested departure allows a more coherent street experience. Additionally, the retaining wall and elevated side yard condition to the east minimize the departure impact. (PL2.B.3, DC2.E.1)

2 SETBACK AT RESIDENTIAL ZONE
SMC 23.47A.014.B.3: SETBACK AT RESIDENTIAL ZONE

REQUIRED: 15'-0" between 13'-0" to 40'-0" height, plus 2'-0" for every 10'-0" above 40'-0" height.

PROVIDED: 2'-4" – 3'-8" above 13'-0", 0'-3" below 13'-0" at east. *11'-4" – 12'-8" departure requested.*

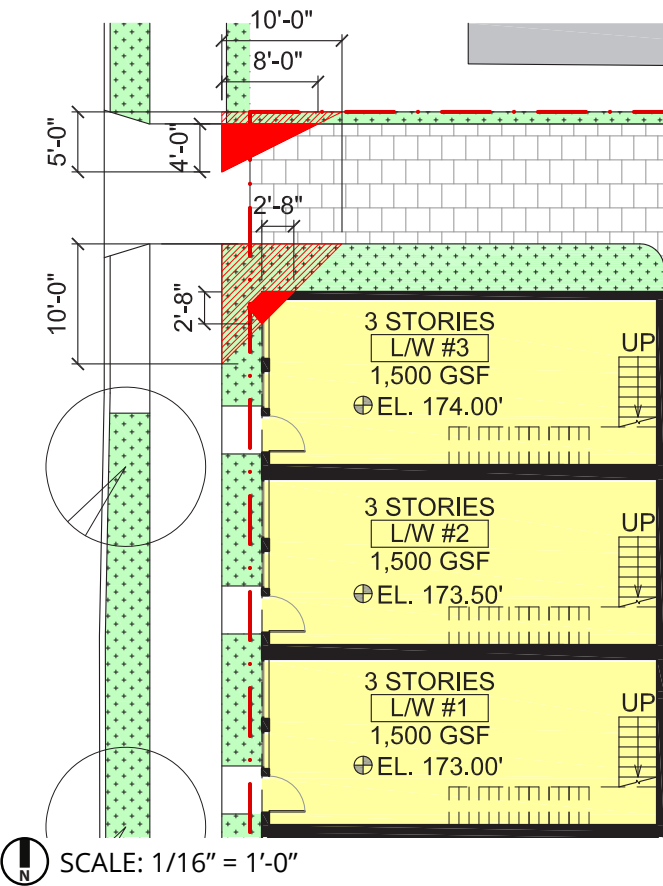
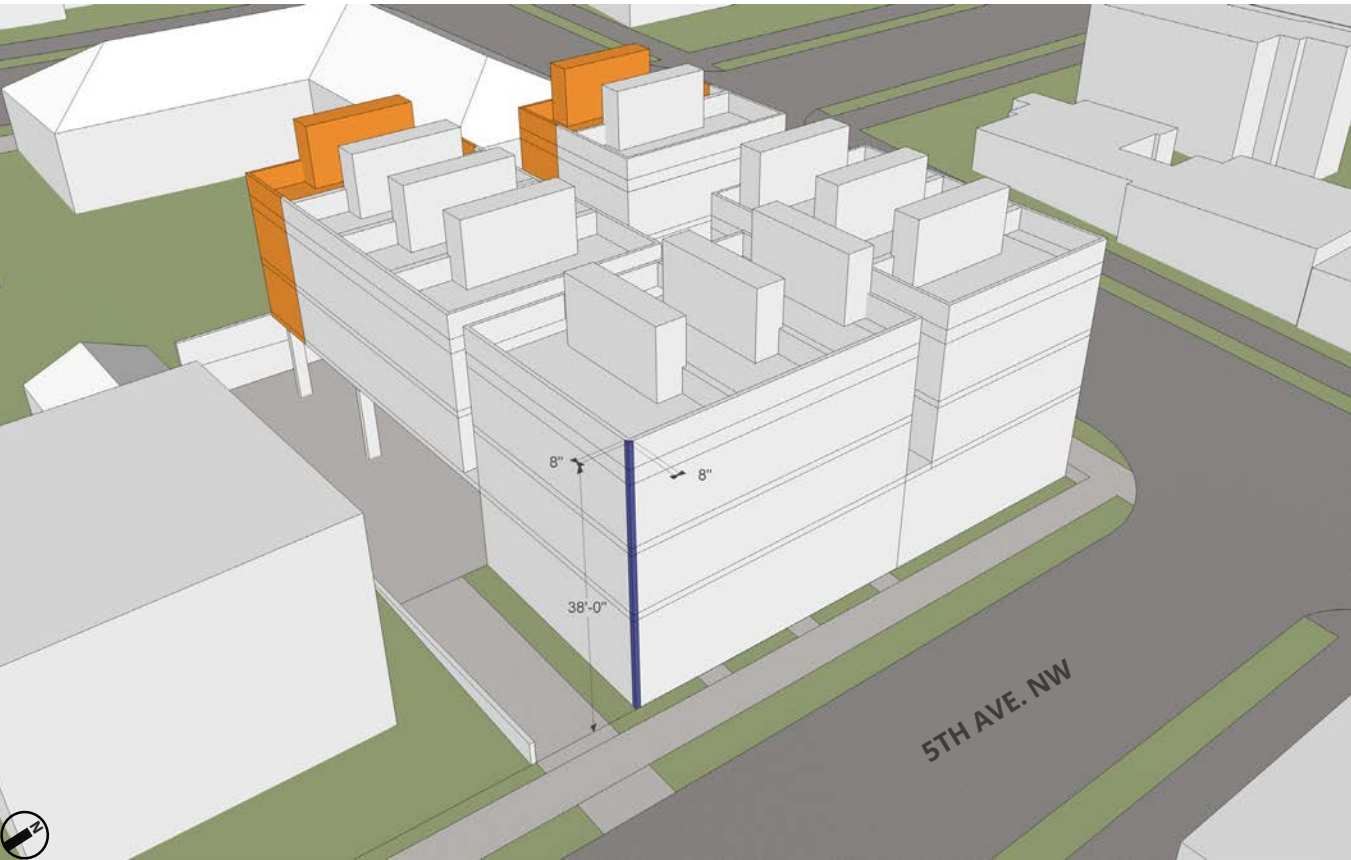
JUSTIFICATION: The requested side setback departure allows a courtyard to face NW 65th St., articulating the facade and providing a dynamic street experience. To provide an activated and usable L2 plaza, the primary massing must shift east. For the south massing, the apartment building to the east has little exposure on that adjacency, and the overall character on NW 65th St. is commercial. At the north massing, the east property is occupied by surface parking, minimizing privacy conflicts and adjacency pressures. (DC1.A.4, DC2.B.1)

3 SIGHT TRIANGLE
SMC 23.54.030.G.4: SIGHT TRIANGLE AT LOT LINE

REQUIRED: 10'-0" triangle and 5' x 10' triangle at lot line.

PROVIDED: 7'-4" triangle at building, +/-1'-0" at lot line. *2'-8" triangle, 4'x8' triangle at lot line departure requested.*

JUSTIFICATION: The sight triangle creates an overly large driveway opening. This departure allows for a more coherent and continuous streetscape on 5th Ave. NW, and an improved pedestrian experience. Traffic mirrors and rumble paving will help ensure safety. (DC1.A.4)



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SCHEME C: PREFERRED

SCHEME INFO
GSF: 18,891
UNITS: 9
LIVE/WORK UNITS: 3
COMMERCIAL: 2,755 SF (2 SPACES)
TOTAL AMENITY: 3,385 SF
2,671 SF - ROOF DECKS
714 SF - PLAZA AT L2
1.51 FAR - 15,108 GFA

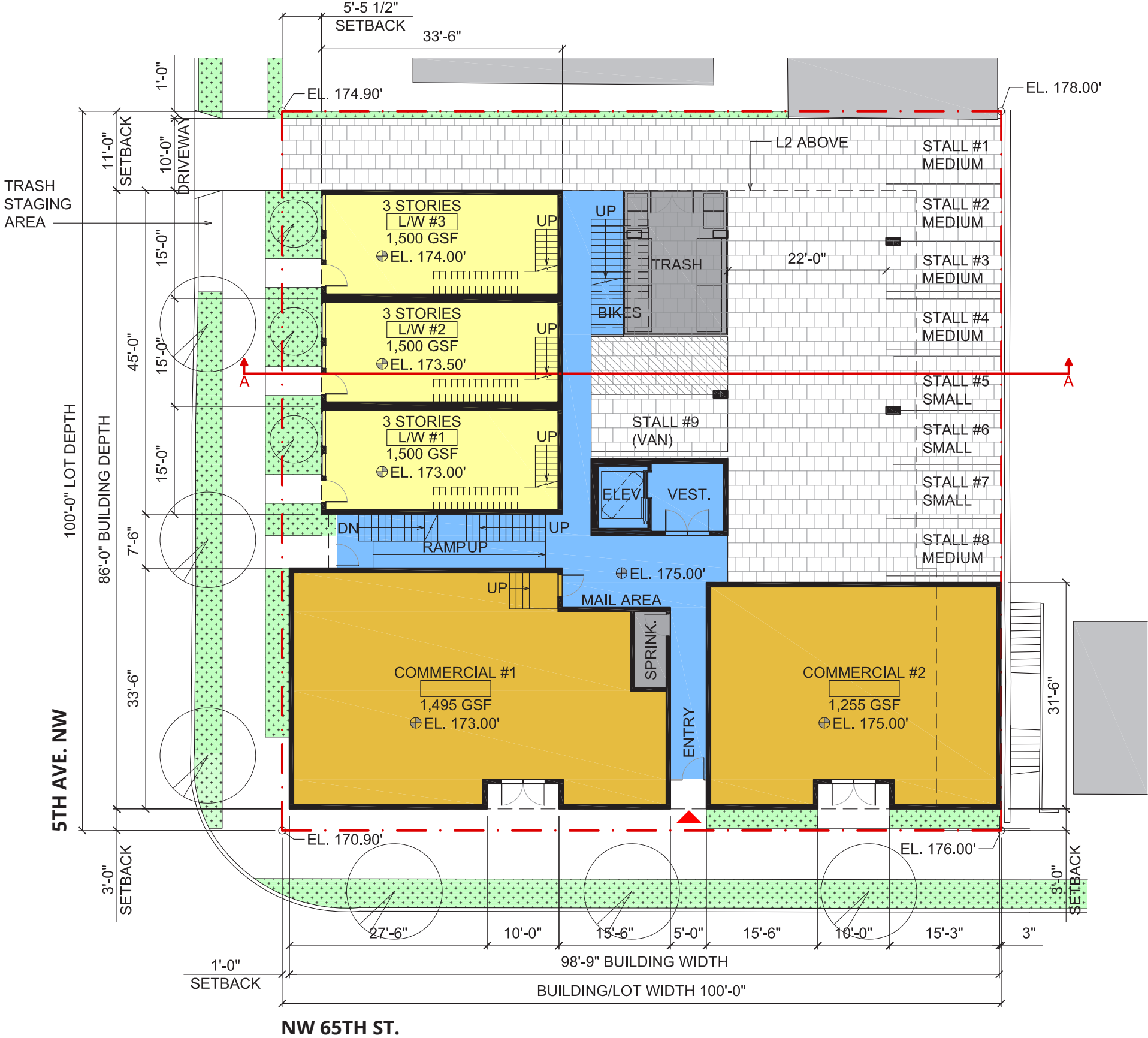
- POSITIVE
- Best balance of massing and articulation
 - Well articulated along 5th Ave. NW
 - Most units oriented to R.O.W's
 - Minimizes driveway presence

- NEGATIVE
- Departures requested

STREET LEVEL PLAN

SCALE: 1/16" = 1'-0"

- LIVE/WORK
- COMMERCIAL
- BUILDING SERVICE
- RESIDENTIAL
- CIRCULATION
- PRIVATE AMENITY
- PRIMARY BUILDING ENTRANCE



SCHEME C: PREFERRED

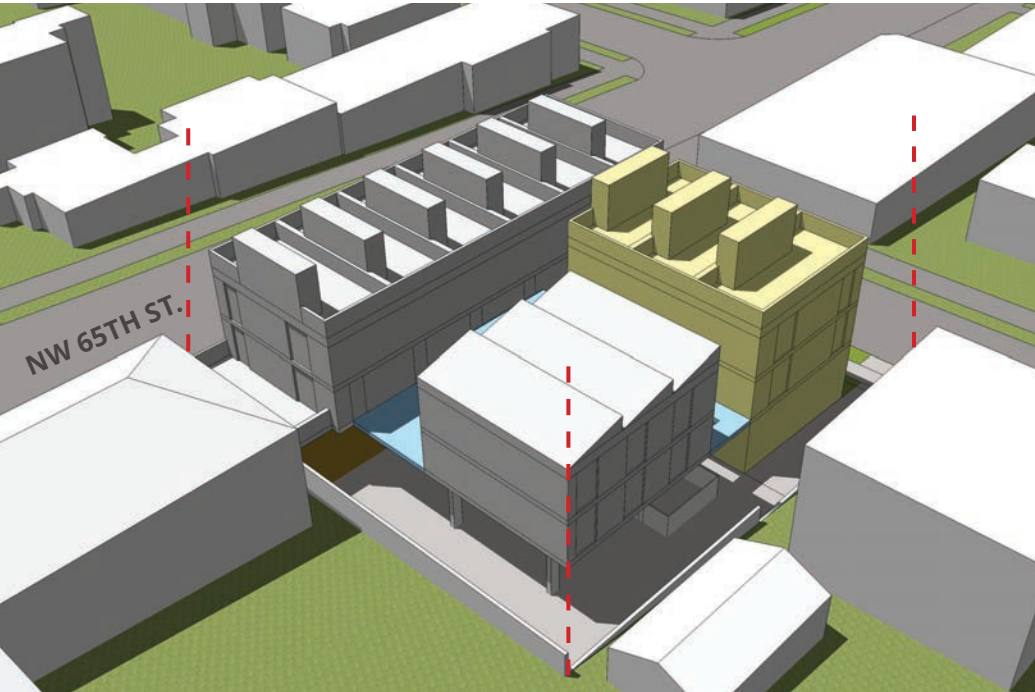
*Street trees and canopies not shown for clarity



PERSPECTIVE LOOKING AT SOUTHWEST CORNER



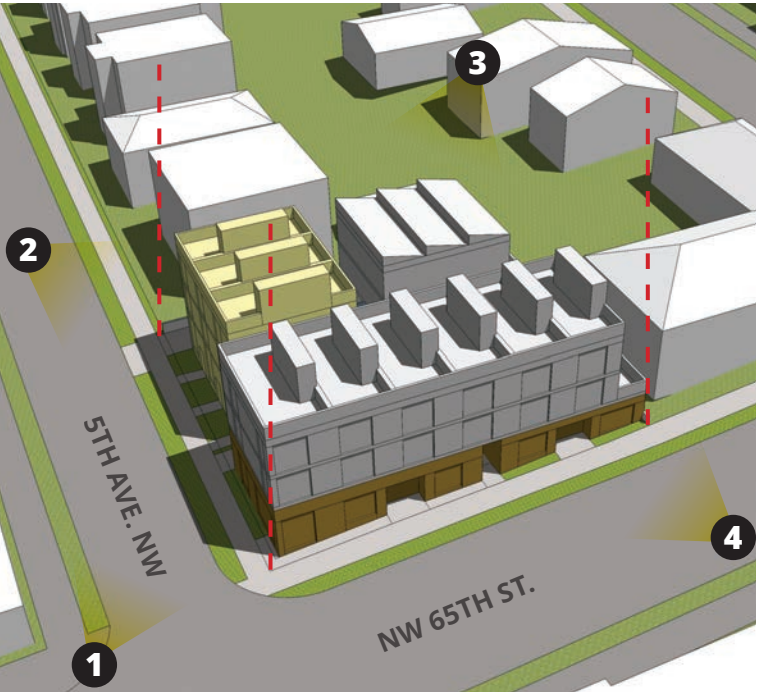
PERSPECTIVE LOOKING AT NORTHWEST CORNER



AERIAL PERSPECTIVE LOOKING AT NORTHEAST CORNER

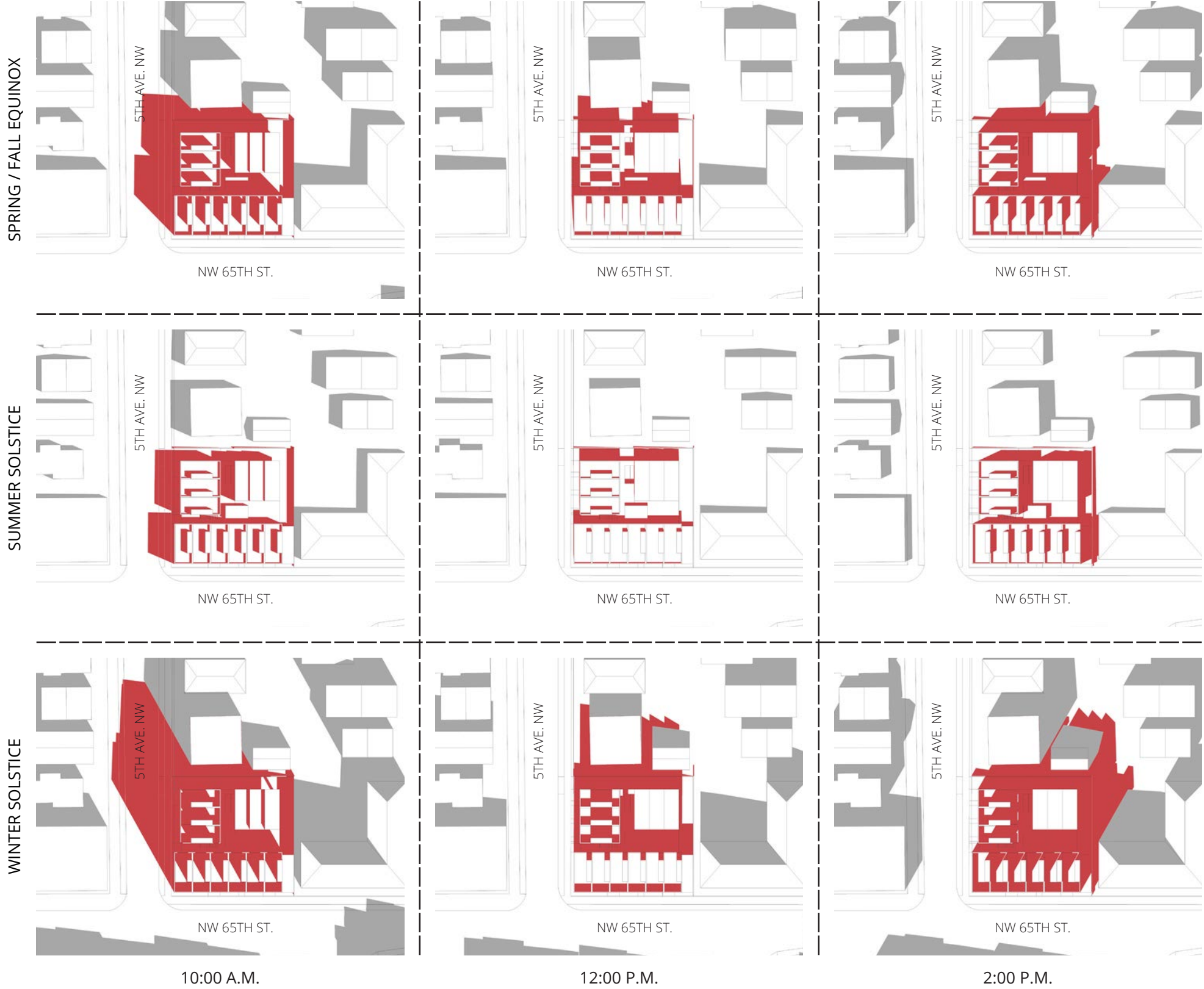


PERSPECTIVE LOOKING AT SOUTHEAST CORNER



AERIAL PERSPECTIVE LOOKING AT SOUTHWEST CORNER

SCHEME C: SUN PATH / SHADOW STUDY



SCHEME C: DEPARTURES

1 RESIDENTIAL ZONE ABUTMENT
SMC 23.47A.014.B.1: RESIDENTIAL ZONE
ABUTMENT

REQUIRED: 15'-0" triangular setback at side/front lot line of residential lot.

PROVIDED: 0'-0" triangle at east property line, compliant at north property line. 14'-9" triangle departure requested.

JUSTIFICATION: The triangular setback along NW 65th St. breaks up the commercial character and continuity on NW 65th St., and creates a non-desirable empty space along the street. The requested departure allows a more coherent street experience. Additionally, the retaining wall and elevated side yard condition to the east minimize the departure impact. (DC2.A.1, DC2.E.1)

2 SETBACK AT RESIDENTIAL ZONE
SMC 23.47A.014.B.3: SETBACK AT
RESIDENTIAL ZONE

REQUIRED: 15'-0" between 13'-0" to 40'-0" height, plus 2'-0" for every 10'-0" above 40'-0" height.

PROVIDED: 9'-0" above 13'-0" at east, 11'-0" at north, 0'-3" below 13'-0" at east. 3'-2" to 6'-0" departure requested.

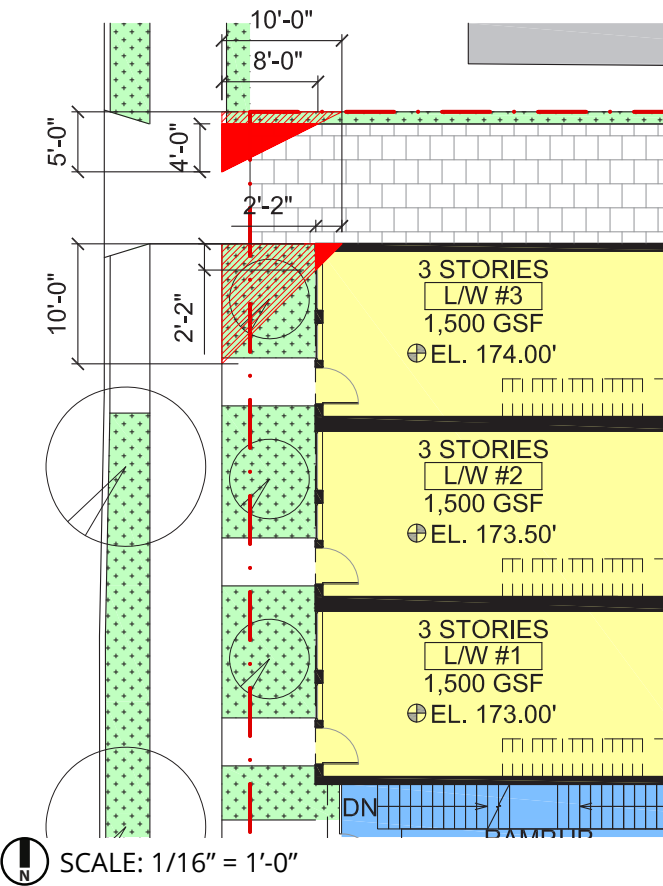
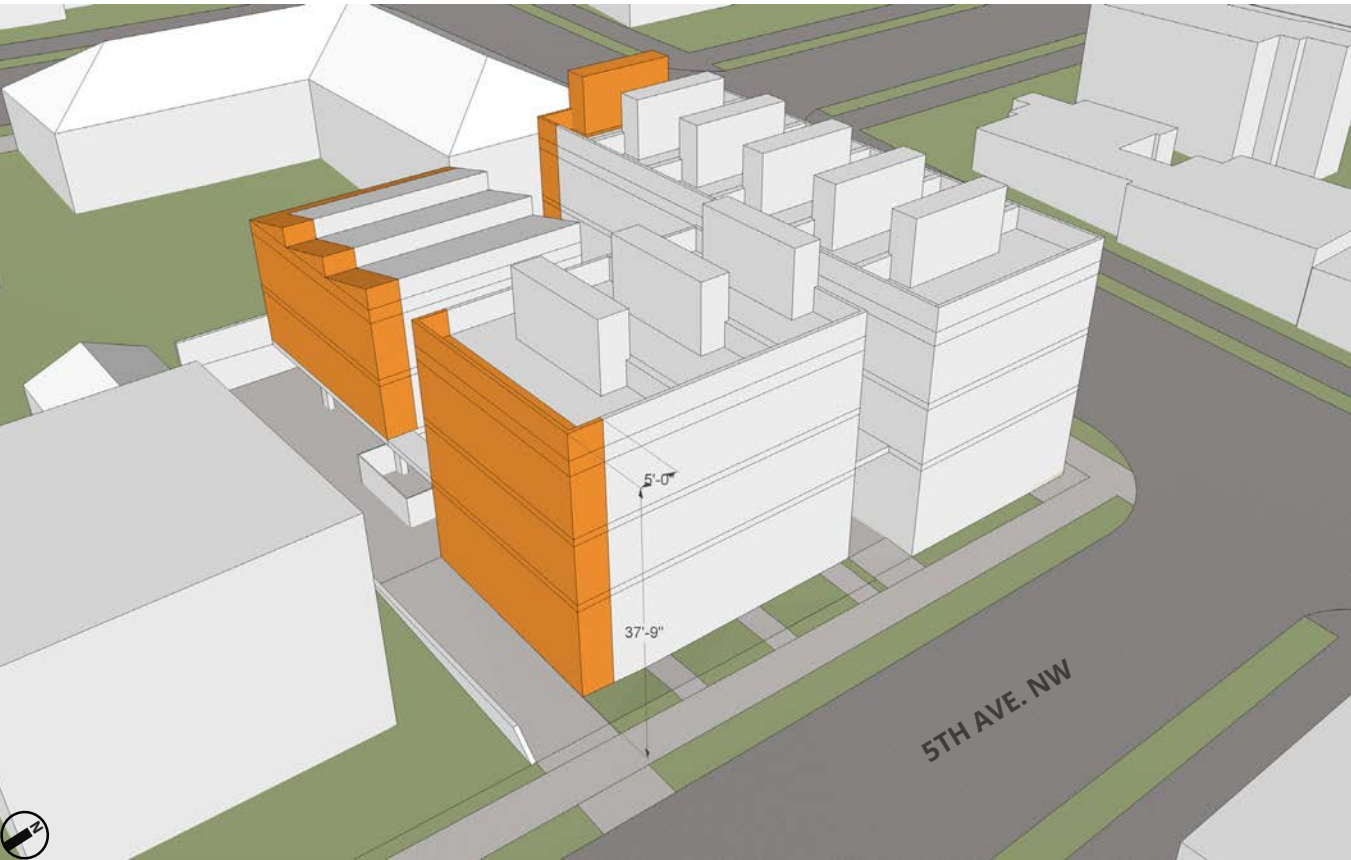
JUSTIFICATION: The departure allows more continuity on the commercial street frontage in character with the surrounding similar building types. Additionally, to provide an activated and usable L2 plaza, the primary massing must shift east. The east property is occupied by surface parking at the north massing, minimizing privacy conflicts and adjacency pressures. The apartment building to the north helps transition zones, and is more in character with a side setback than a rear setback in this case at the adjacency. (DC1.A.4, DC2.A.1, DC3.B.4)

3 SIGHT TRIANGLE
SMC 23.54.030.G.4: SIGHT TRIANGLE AT LOT
LINE

REQUIRED: 10'-0" triangle and 5' x 10' triangle at lot line.

PROVIDED: 7'-10" triangle at building, +/-1'-0" at lot line. 2'-2" triangle, 4x8' triangle at lot line departure requested.

JUSTIFICATION: The sight triangle creates an overly large driveway opening. This departure allows for a more coherent and continuous streetscape on 5th Ave. NW, and an improved pedestrian experience. Traffic mirrors and rumble paving will help ensure safety. (DC1.A.4)



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SCHEME SUMMARY

SCHEME A

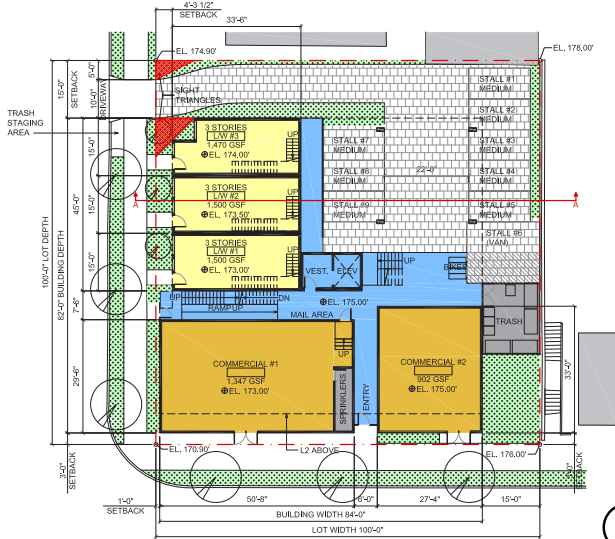
SCHEME B

SCHEME C (PREFERRED)

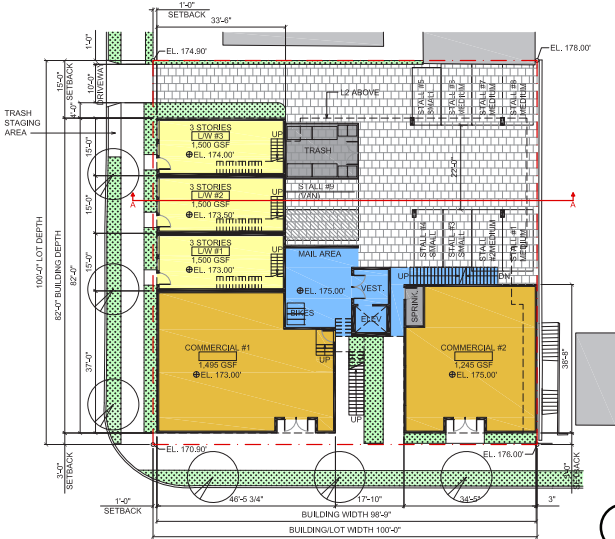
AERIAL PERSPECTIVE



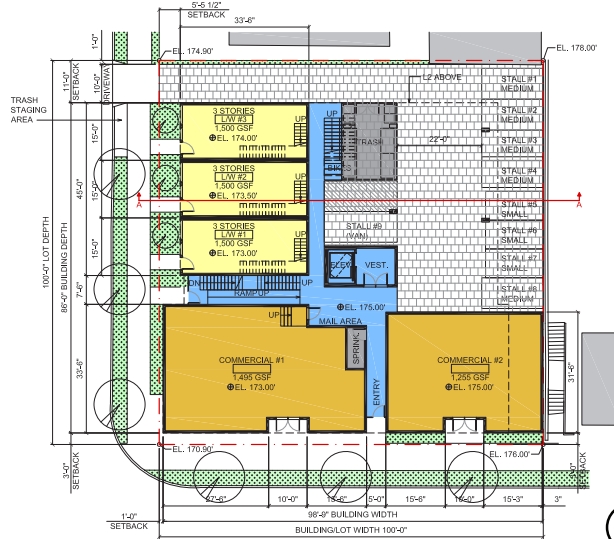
SITE PLAN



SCALE: N.T.S.

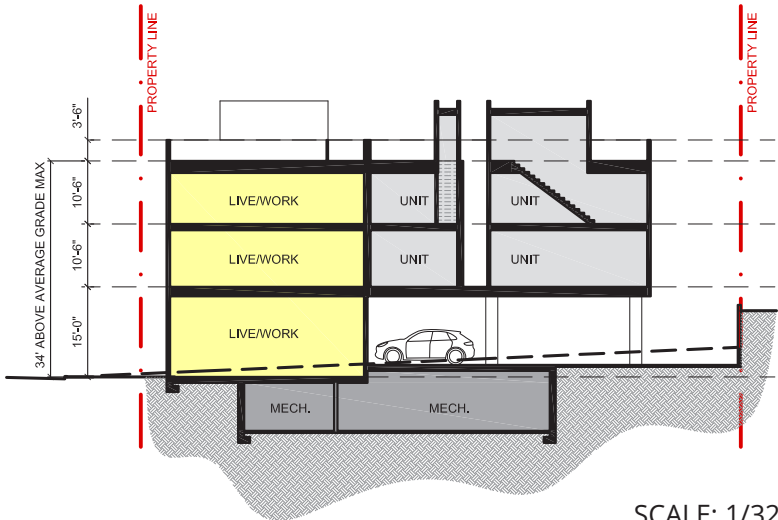


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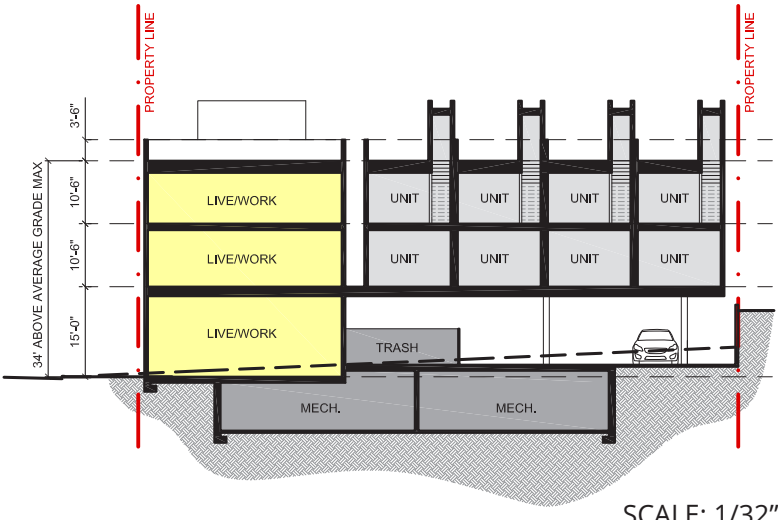


SCALE: N.T.S.

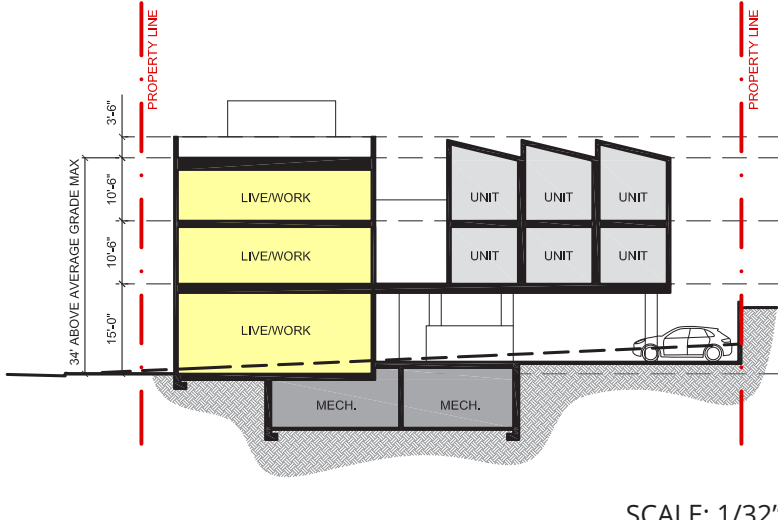
SECTION



SCALE: 1/32"=1'-0"



SCALE: 1/32"=1'-0"



SCALE: 1/32"=1'-0"

NOTE: SCHEME A HAS NO DEPARTURES REQUESTED

SCHEME B

1 RESIDENTIAL ZONE ABUTMENT
SMC 23.47A.014.B.1: RESIDENTIAL ZONE ABUTMENT

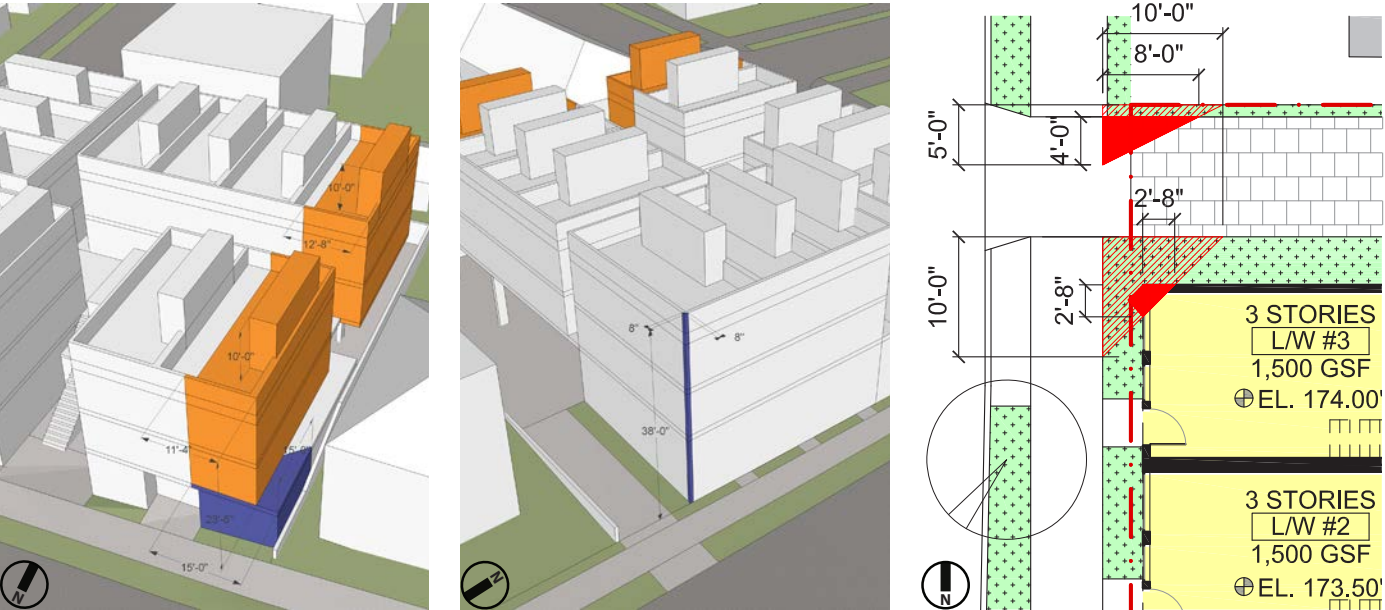
REQUIRED: 15'-0" triangular setback at side/front lot line of residential lot.
PROVIDED: 0'-0" triangle at east property line, 14'-4" at north property line. *14'-9" triangle departure requested at east property line, 8" triangle at north property line.*
JUSTIFICATION: The triangular setback along NW 65th St. breaks up the commercial character and continuity on NW 65th St., and creates a non-desirable empty space along the street. The requested departure allows a more coherent street experience. Additionally, the retaining wall and elevated side yard condition to the east minimize the departure impact. (PL2.B.3, DC2.E.1)

2 SETBACK AT RESIDENTIAL ZONE
SMC 23.47A.014.B.3: SETBACK AT RESIDENTIAL ZONE

REQUIRED: 15'-0" between 13'-0" to 40'-0" height, plus 2'-0" for every 10'-0" above 40'-0" height.
PROVIDED: 2'-4" – 3'-8" above 13'-0", 0'-3" below 13'-0" at east. *11'-4" – 12'-8" departure requested.*
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SMC 23.54.030.G.4: SIGHT TRIANGLE AT LOT LINE

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DEPARTURE MATRIX

SCHEME C

1 RESIDENTIAL ZONE ABUTMENT
SMC 23.47A.014.B.1: RESIDENTIAL ZONE ABUTMENT

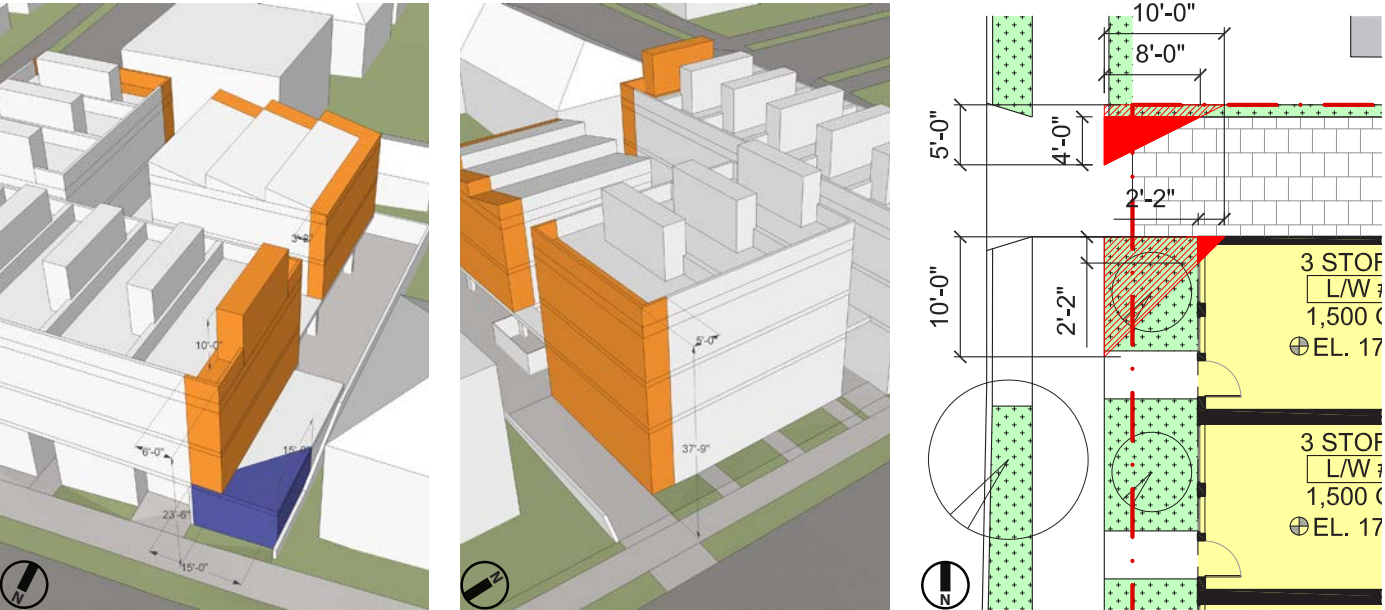
REQUIRED: 15'-0" triangular setback at side/front lot line of residential lot.
PROVIDED: 0'-0" triangle at east property line, compliant at north property line. *14'-9" triangle departure requested.*
JUSTIFICATION: The triangular setback along NW 65th St. breaks up the commercial character and continuity on NW 65th St., and creates a non-desirable empty space along the street. The requested departure allows a more coherent street experience. Additionally, the retaining wall and elevated side yard condition to the east minimize the departure impact. (DC2.A.1, DC2.E.1)

2 SETBACK AT RESIDENTIAL ZONE
SMC 23.47A.014.B.3: SETBACK AT RESIDENTIAL ZONE

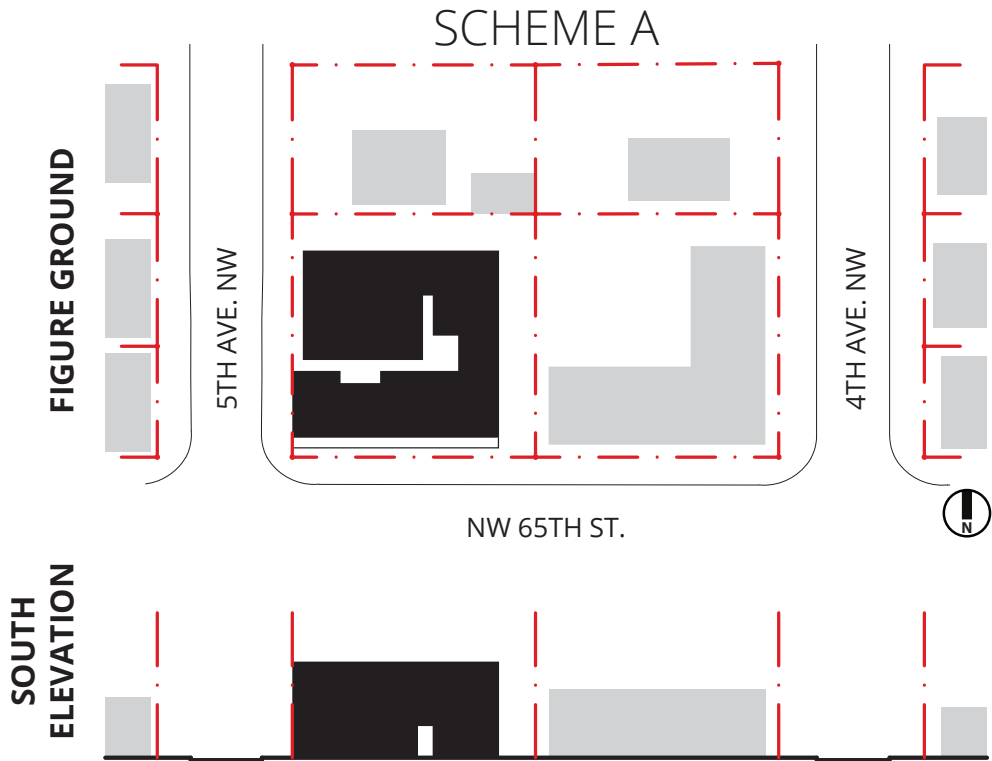
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JUSTIFICATION: The departure allows more continuity on the commercial street frontage in character with the surrounding similar building types. Additionally, to provide an activated and usable L2 plaza, the primary massing must shift east. The east property is occupied by surface parking at the north massing, minimizing privacy conflicts and adjacency pressures. The apartment building to the north helps transition zones, and is more in character with a side setback than a rear setback in this case at the adjacency. (DC1.A.4, DC2.A.1, DC3.B.4)

3 SIGHT TRIANGLE
SMC 23.54.030.G.4: SIGHT TRIANGLE AT LOT LINE

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JUSTIFICATION: The sight triangle creates an overly large driveway opening. This departure allows for a more coherent and continuous streetscape on 5th Ave. NW, and an improved pedestrian experience. Traffic mirrors and rumble paving will help ensure safety. (DC1.A.4)



CONCEPT DEVELOPMENT: MASSING CONFIGURATION

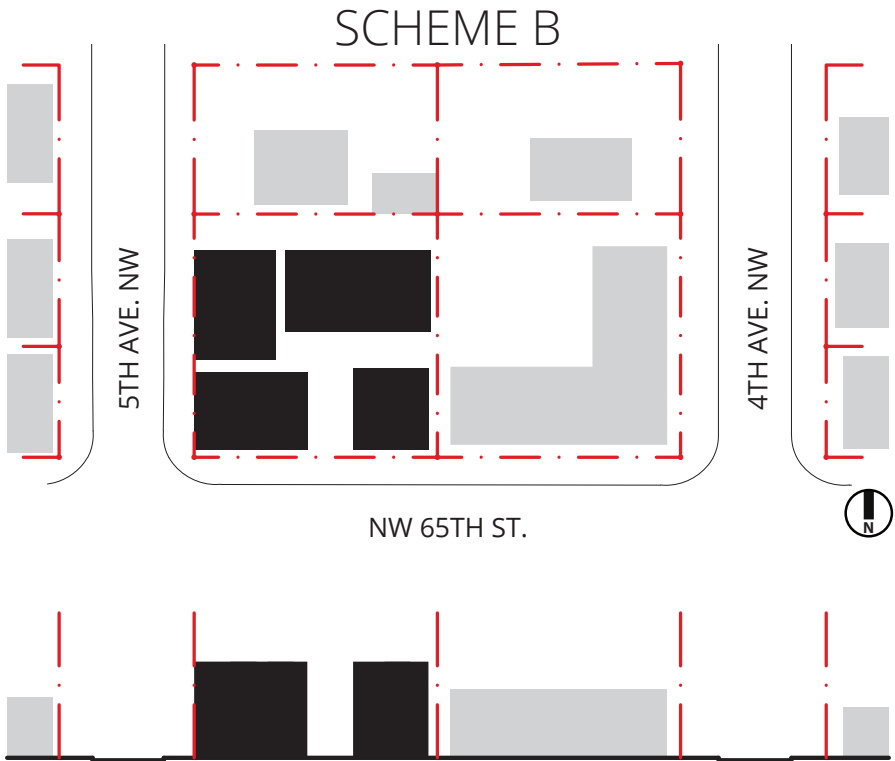


POSITIVE

- No departures requested
- Efficient layout for units

NEGATIVE

- Most bulk and interior space circulation
- Less opportunities for modulation of massing
- Most exposure to east and north SF/LR adjacencies
- Least useful shared amenity spaces

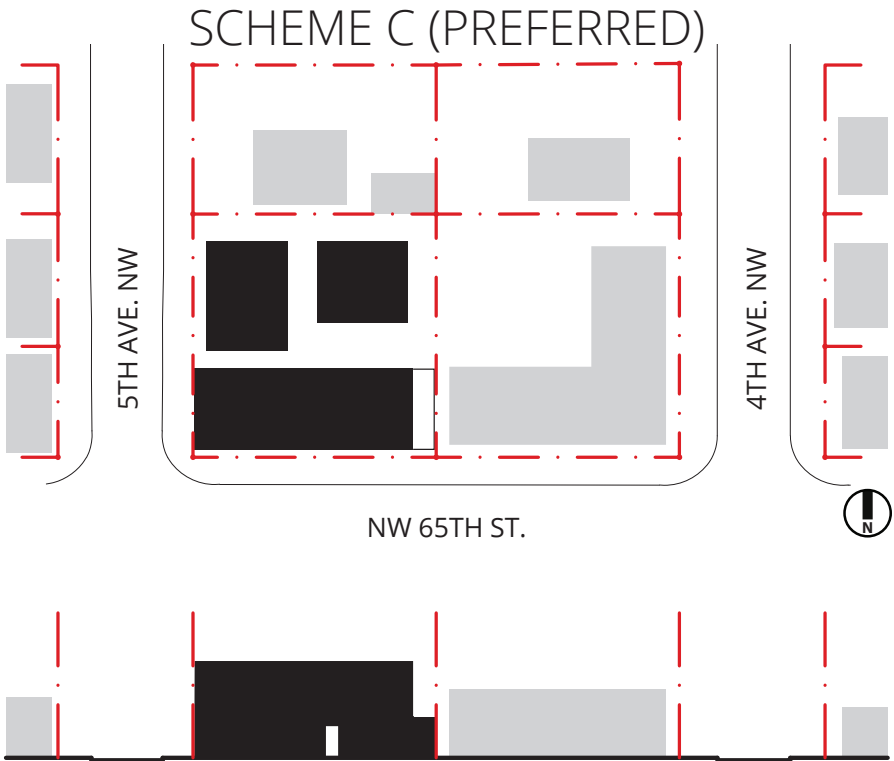


POSITIVE

- Creates 'courtyard' type entry on NW 65th St.
- Axial layout allows open exterior on four sides
- Plaza at level two faces NW 65th St.

NEGATIVE

- Setback departures requested
- Broken street facade may not be desirable in commercial area
- Most north (rear) facing units of all schemes
- Most aggressive east adjacency



POSITIVE

- Good balance of massing and articulation
- Generous setback along 5th Ave. NW at live/work units
- Most units oriented toward R.O.W.'S of all schemes
- Articulated and broken up massing at north and east property lines
- Maintains commercial frontage and character along NW 65th St.

NEGATIVE

- Setback departures requested



CONCEPT DEVELOPMENT: ARCHITECTURAL CHARACTER



NW 65TH ST. CORNER PERSPECTIVE



MASSING VARIETY



INTERIOR RESIDENTIAL COURT



BALANCED COMPOSITION



ARTICULATED VOLUMES



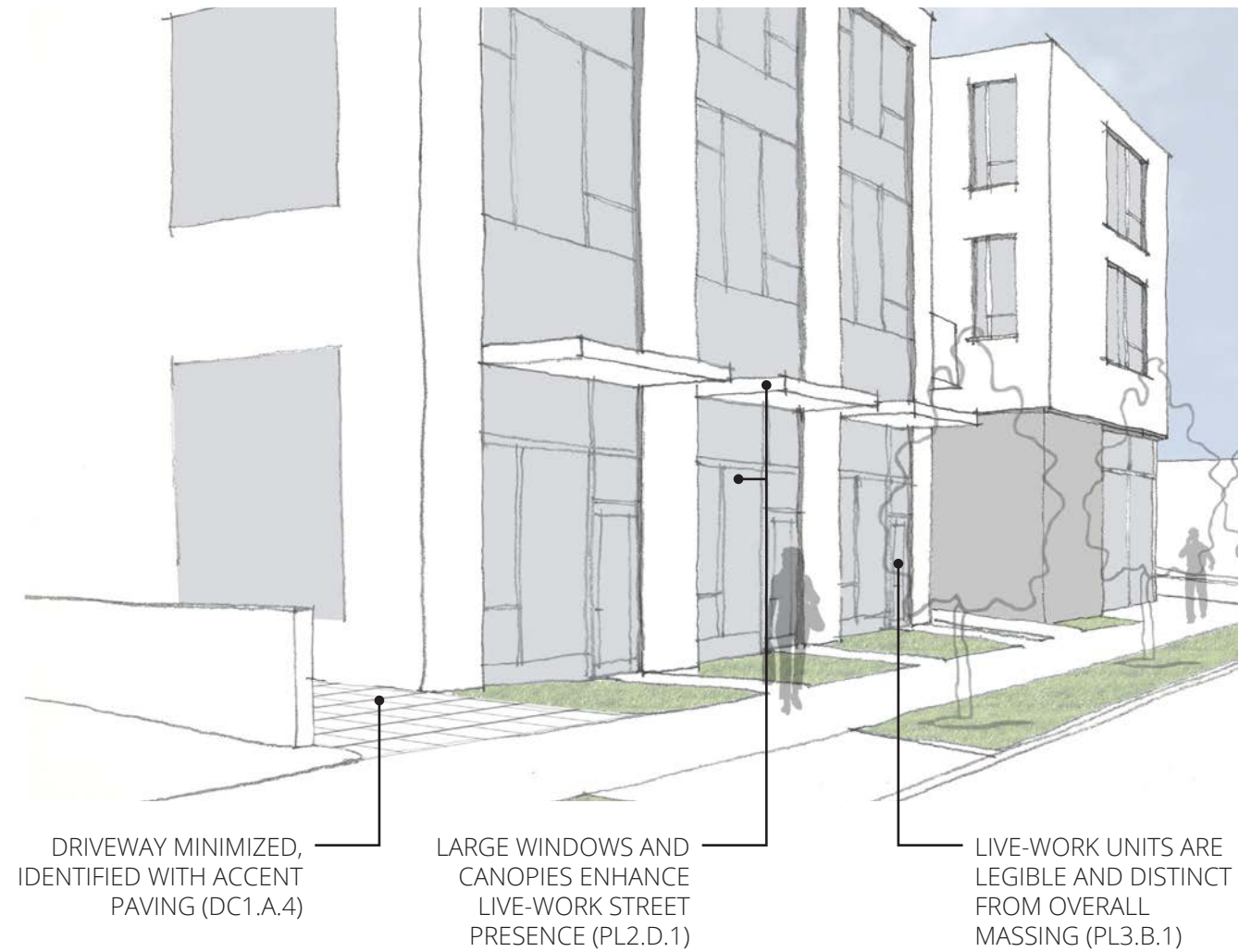
SCALED MASSING



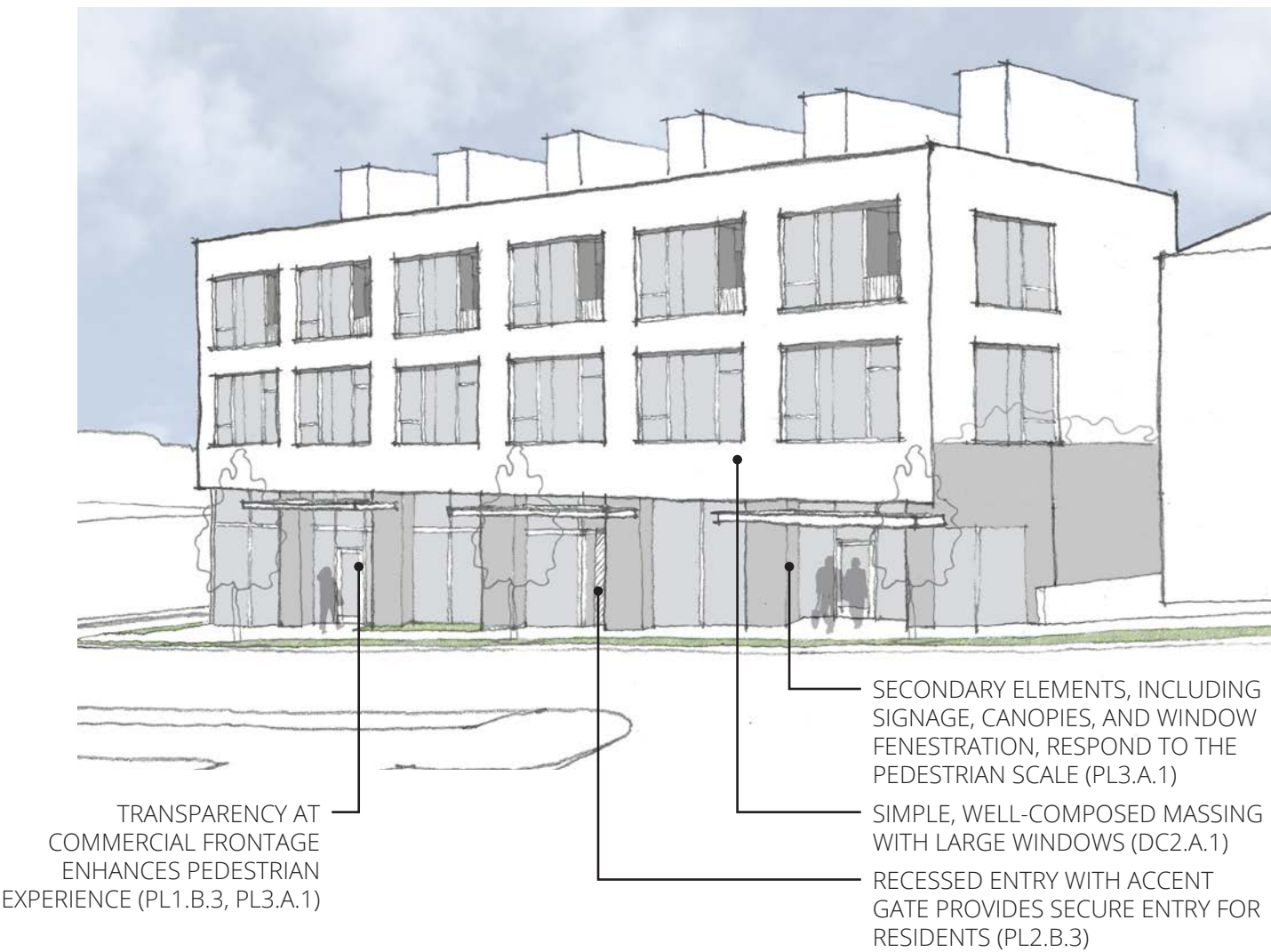
OPEN STREETScape

CONCEPT DEVELOPMENT: STREET FRONTAGE

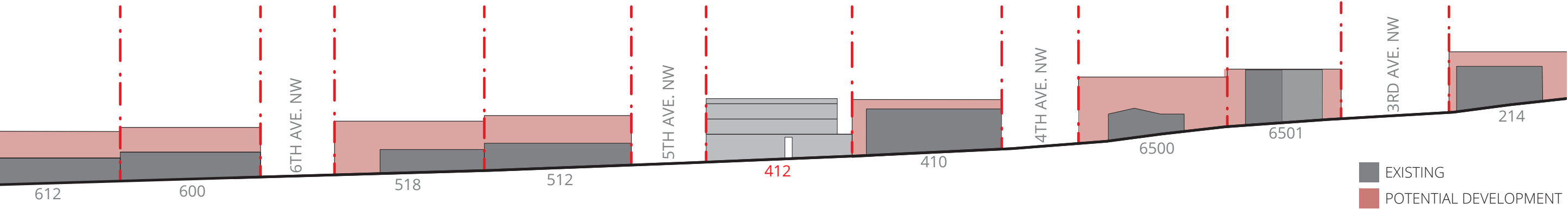
5TH AVE. NW PERSPECTIVE



NW 65TH ST. PERSPECTIVE



STREET WALL ANALYSIS - NW 65TH ST.



CONCEPT DEVELOPMENT: LANDSCAPE



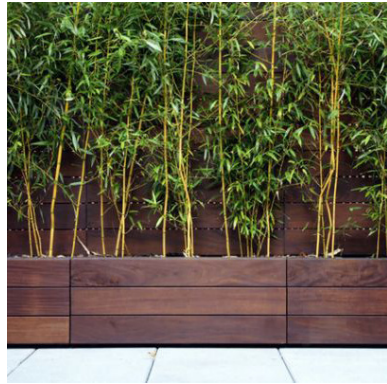
FLEXIBLE SEATING



GREEN ROOF



NARROW PLANTING



VERTICAL PLANTING



GREEN WALL



STREET TREES



TEXTURED PLANTING



INTEGRATED PLANTING



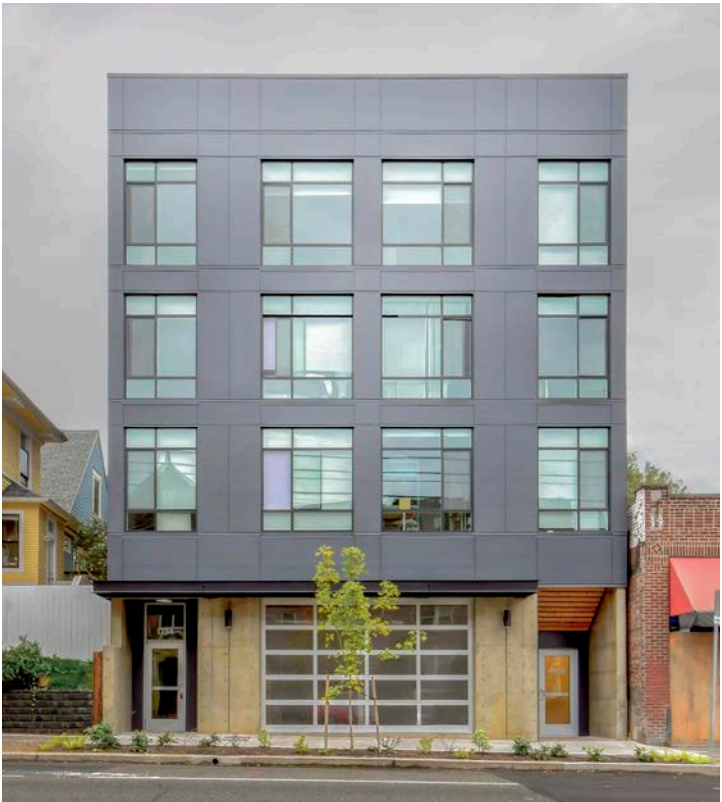
RECENT WORK **SHW**



5902 22ND AVE NW



1114 16TH AVE (SHW + 47 NORTH)



1728 12TH AVE



120 10TH AVE E (UNDER CONSTRUCTION)



111 E HAMLIN (SHW + 47 NORTH, UNDER CONSTRUCTION)



6301 15TH AVE NW (UNDER CONSTRUCTION)