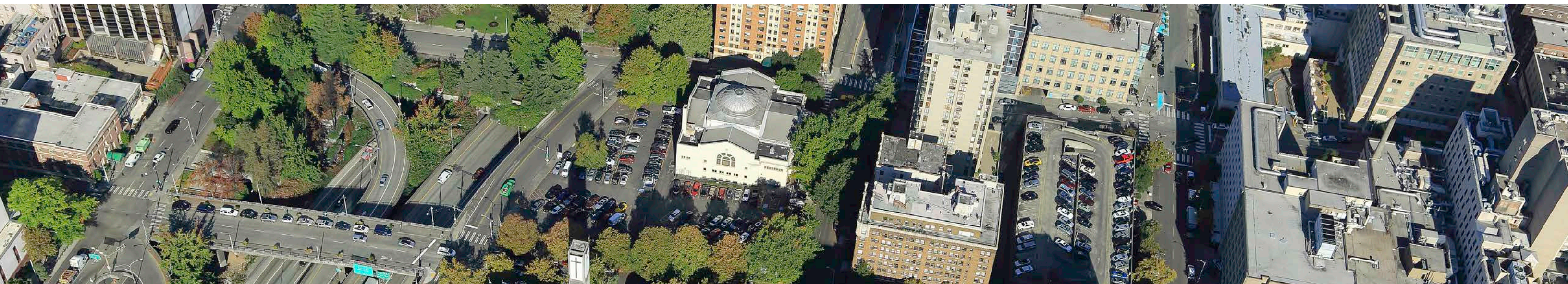


LENNAR MULTIFAMILY COMMUNITIES SPRING STREET NORTH BLOCK



SECTION 01

DEVELOPMENT

OBJECTIVES

PROPOSAL

The project is sited on the Town Hall block, located on the edge of the First Hill neighborhood. The site is bounded by Seneca Street to the north, Spring Street to the south, Hubbell Place to the west, and 8th Avenue to the east. The site encompasses about three-quarters of the block, with the existing Town Hall building occupying the remaining quarter. I-5 is located directly to the west and the First Hill access to Freeway Park is across the street to the north.

The proposed development includes **two 330’ residential towers** on the southeast and southwest corners and a plaza on the northwest corner of the site. **Approximately 550 units are planned between the two towers, including four townhomes** in the east tower – to be located along 8th Avenue. Six levels of below-grade parking provide **414 parking stalls**. A small cafe, about **1,300 SF of retail** total, is planned on the ground floor of the west tower, overlooking the northern portion of the site.

The project proposes a **notable amount of open space and landscaped area** throughout site and along the right of way to enhance the urban fabric of the surrounding context. The project seeks a full alley vacation to provide through-block connections and proposes a large plaza area on the northwest corner of the site to foster gathering and activity. The plaza will create a strong pedestrian connection to the adjacent entrance to Freeway Park. The design team and client have met and collaborated with Town Hall stakeholders to create a cohesive design that accommodates planned improvements for the historic Town Hall building.

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SITE

SECTION 04

SITE PLAN

SURVEY A

Site Address:

First Presbyterian Church Property

Hubbell Place and Spring Street, Seattle, Washington

Tax Account Number & Zoning:

197920-0046-07 HR

197920-0065-03 HR

197920-0070-06 HR

197920-0080-04 HR

Area:

Site as shown contains 32,979 square feet or 0.7571 acres, more or less

Site Description:

Lots 1, 4, 5, 6, 7, and 8, Block 52, A. A. Denny's plat of an extension to Terry's 1st addition to the City of Seattle, according to the plat thereof recorded in volume 1 of plats, page 86, in King County, Washington;

except the southwesterly 75 feet of said lots 1 and 4; and

except those portions of said lots 5 and 8, condemned for primary state highway number 1 in King County Superior Court cause number 576017.

Title Report Reference:

This survey was conducted according to the description shown, furnished by Chicago Title Company, commitment number 0012835-06, dated April 11, 2014.

SURVEY B

Site Address:

AP Urban Property

Hubbell Place & Seneca Street, Seattle, Washington

Tax Account Number & Zoning:

197920-0045-08 HR

Area:

Site as shown contains 4,265 square feet or 0.09791 acres, more or less

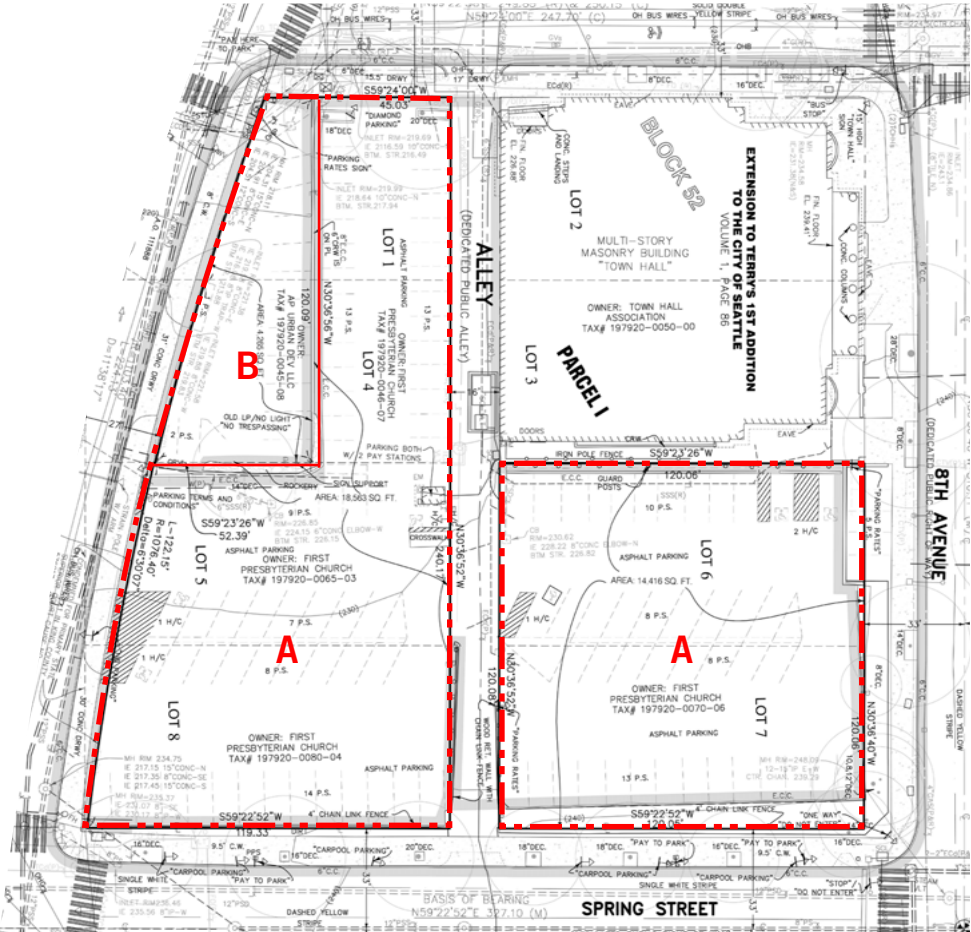
Site Description:




The southwesterly 75 feet of lots 1 and 4, Block 52, plat of an extension to Terry's 1st addition to the City of Seattle, according to the plat thereof recorded in volume 1 of plats, page 86, in King County Washington;

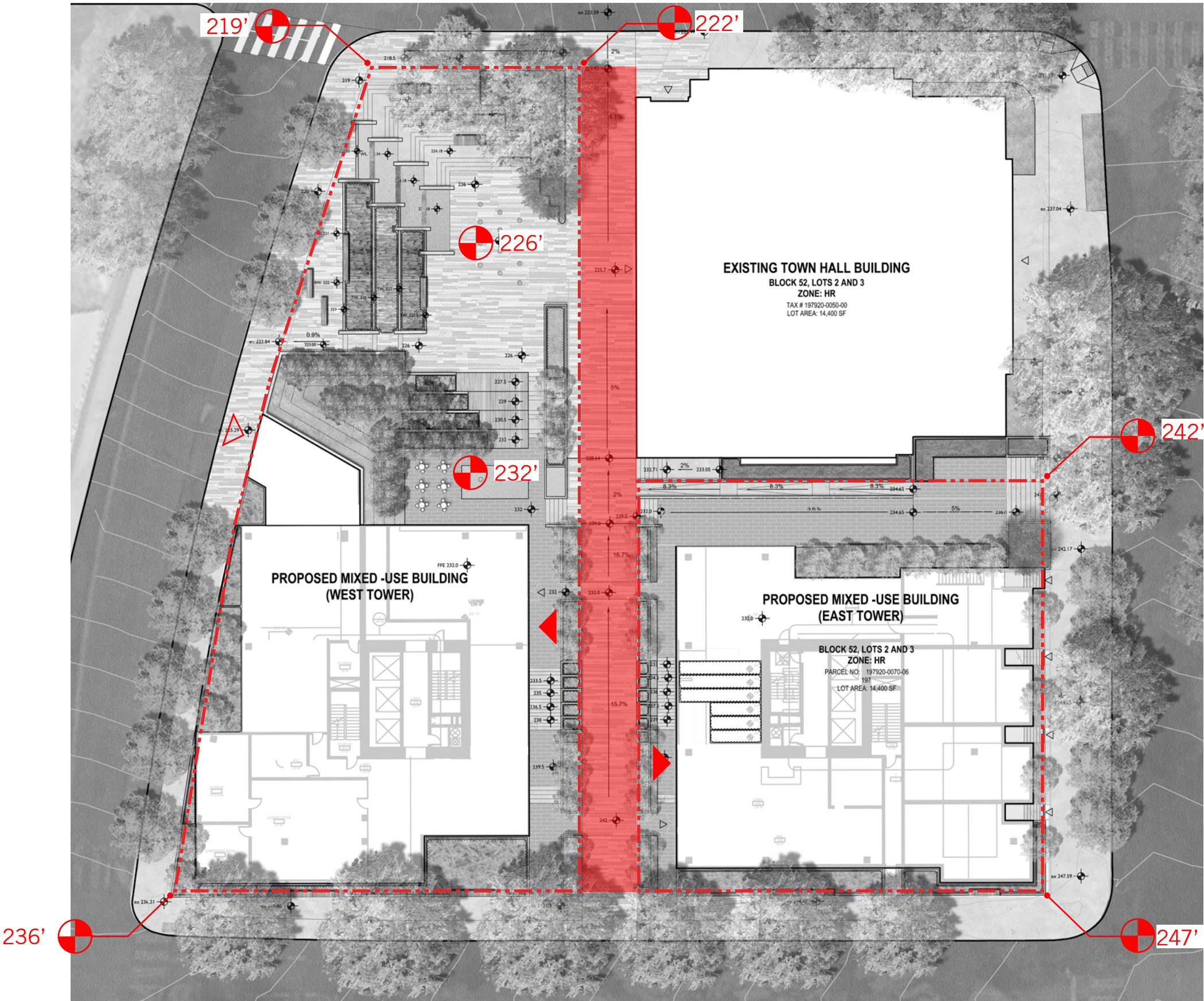
less and except that portion lying within a public right of way.

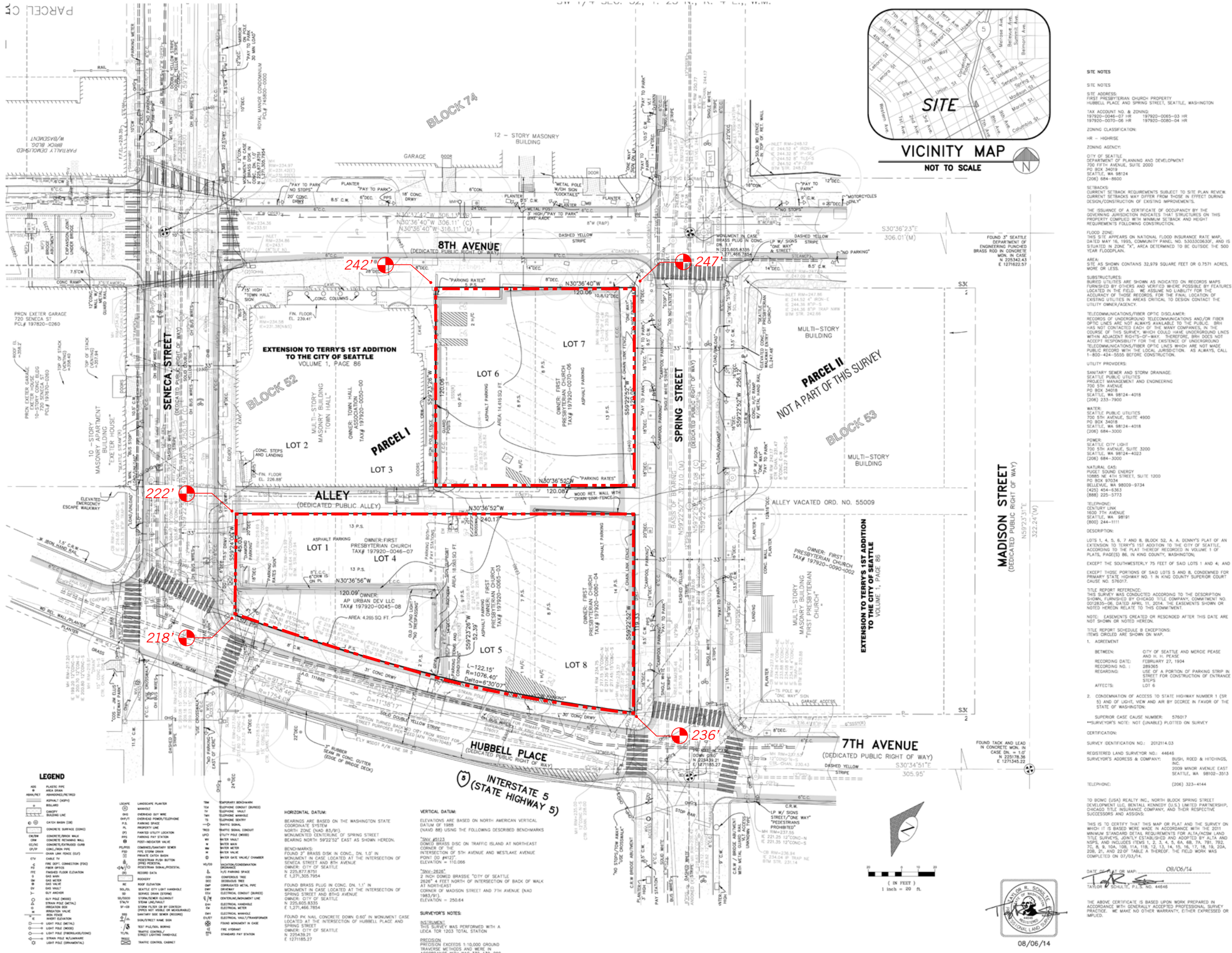
Title Report Reference:

This survey was conducted according to the description shown, furnished by Chicago Title Company, commitment number 0008613-06, dated July 21, 2014.

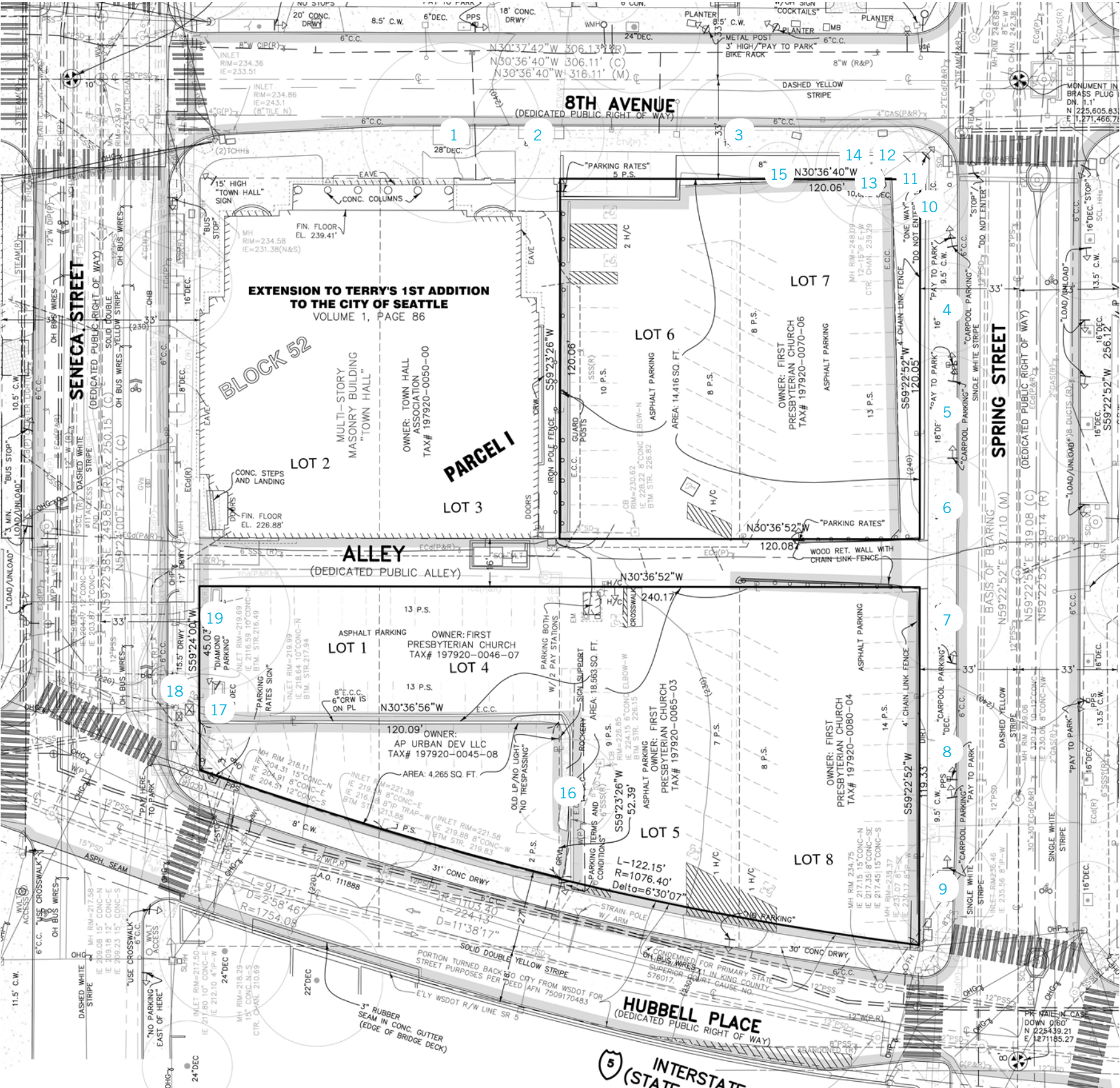


-  Pedestrian Entry
-  Vehicle Entry
-  Full Alley Vacation included with Proposal





	Name	Diameter	Radius	Condition	Exceptional?
1	Red Oak <i>Quercus rubra</i>	26"	34'	Good	N
2	Scarlett Oak <i>Quercus coccinea</i>	17"	35'	Good	N
3	Scarlett Oak <i>Quercus coccinea</i>	13"	18'	Fair	N
4	Crimean Linden <i>Tilia x euchlora</i> 'Euchlora'	18"	19'	Excellent	N
5	Crimean Linden <i>Tilia x euchlora</i> 'Euchlora'	19"	17"	Excellent	N
6	Crimean Linden <i>Tilia x euchlora</i> 'Euchlora'	17"	18'	Excellent	N
7	Littleleaf Linden <i>Tilia cordata</i>	18"	18'	Excellent	N
8	Crimean Linden <i>Tilia x euchlora</i> 'Euchlora'	18"	24'	Excellent	N
9	Crimean Linden <i>Tilia x euchlora</i> 'Euchlora'	16"	23'	Excellent	N
10	English Elm <i>Ulmus procera</i>	15"	20'	Excellent	N
11	English Elm <i>Ulmus procera</i>	15"	20'	Good	N
12	English Elm <i>Ulmus procera</i>	11"	22'	Good	N
13	English Elm <i>Ulmus procera</i>	21"	22'	Good	N
14	English Elm <i>Ulmus procera</i>	11"	22'	Good	N
15	Littleleaf Linden <i>Tilia cordata</i>	10"	21'	Excellent	N
16	European White Birch <i>Betula pendula</i>	14"	16'	Good	N
17	Sweet Gum <i>Liquidambar sturaciflua</i>	21"	35'	Excellent	N
18	California Live Oak <i>Quercus agrifolia</i>	6"	7'	Poor	N
19	Sweet Gum <i>Liquidambar styraciflua</i>	19"	28'	Excellent	N



SECTION 05

URBAN DESIGN

ANALYSIS

NINE BLOCK EXISTING USES /

The site is located on the peripheral of First Hill in an area with diverse programmatic uses, urban scales, and history. The highway corridor acts as a physical boundary, distancing the site from the primarily daytime commercial uses in the neighboring area of the downtown core. The void above the highway opens up views to the west and accentuates the immense scale and height of the downtown area, especially in comparison to the urban scale of First Hill.

The First Hill area next to the site is an eclectic mix of brownstone multifamily buildings from the early twentieth century, mid-century concrete or stucco apartments, and multi-block healthcare and medical research campuses. There are a few exceptions, including a contemporary glass highrise and a few, but still significant, places for religious and community gathering.

Freeway Park borders the northern edge of the project site and presents an opportunity for the proposed development to enrich the neighborhood fabric by connecting the park with the landscaped Spring Street and 8th Avenue arteries.

Site

Residential

Healthcare / Research

Civic / Government

Hospitality

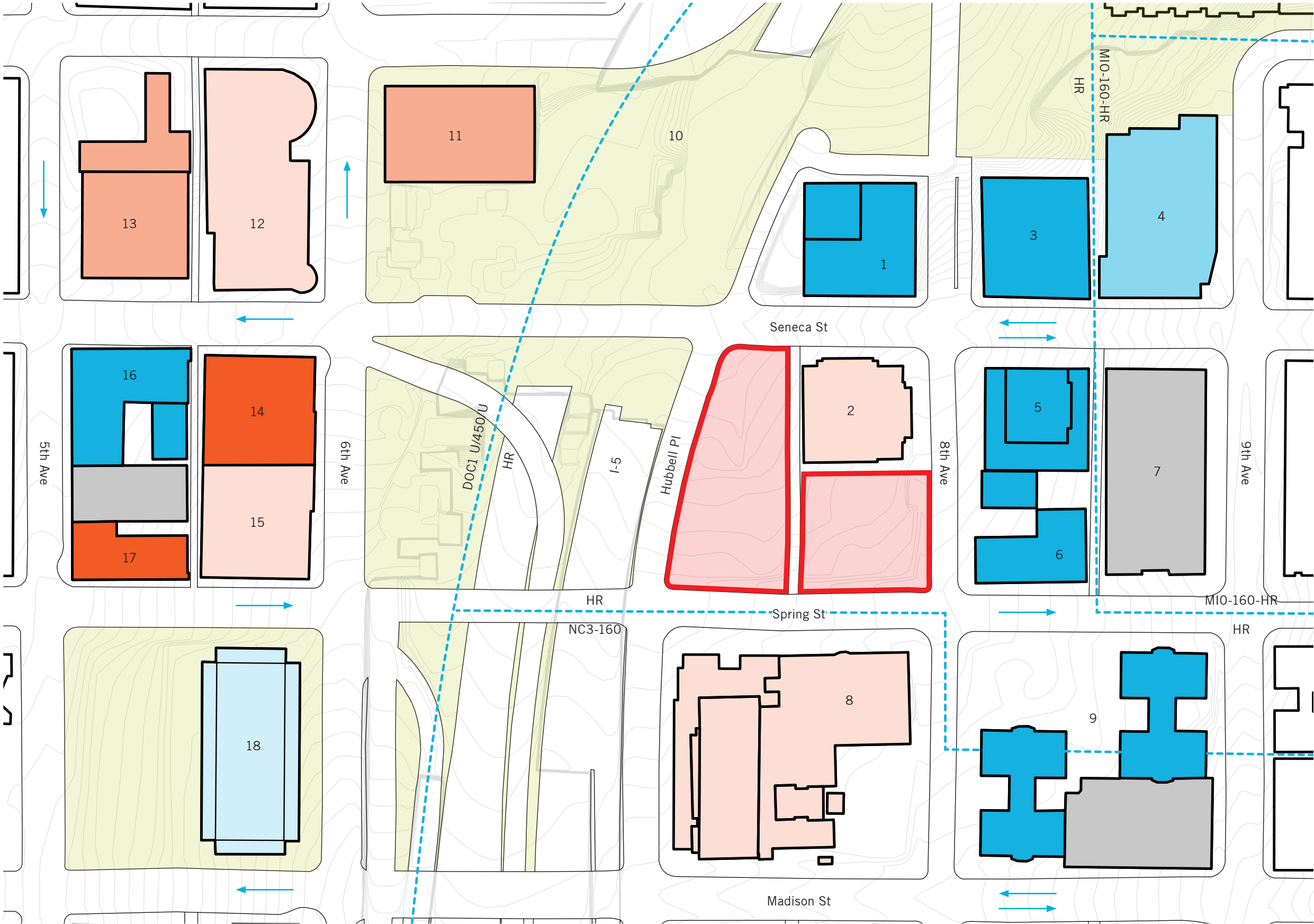
Office

Cultural / Religious

Parking

Open / Park Space

Traffic Direction





1
Exeter House, Residential



4
Benaroya Research, Healthcare



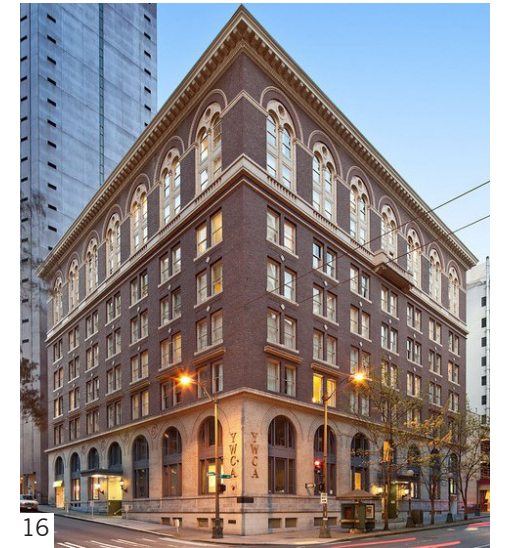
7
Virginia Mason Parking Garage, Parking



10
Freeway Park, Public Amenity



13
IBM, Office



16
YWCA Condominiums, Residential



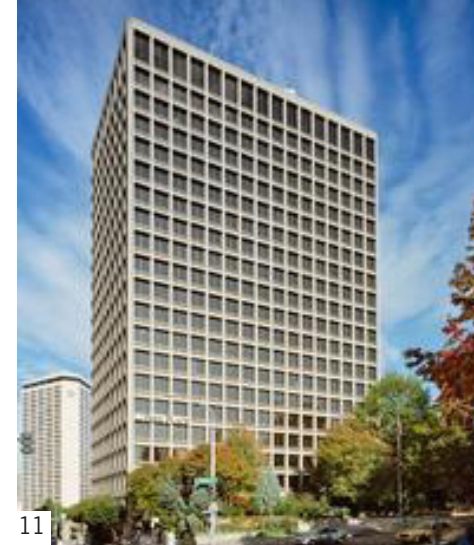
2
Town Hall, Cultural



5
Royal Manor Condominium, Residential



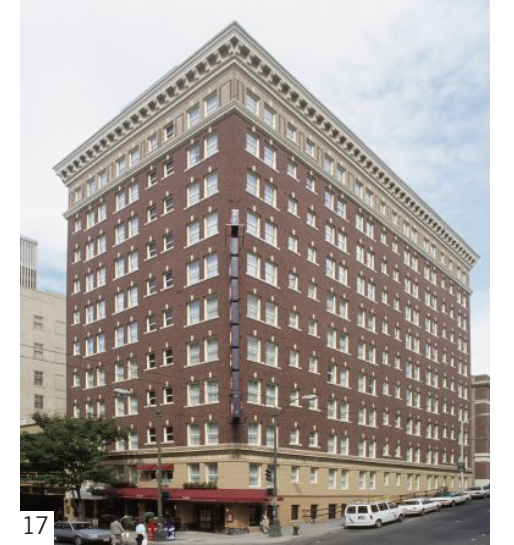
8
First Presbyterian Church, Religious Service



11
Park Place, Office



14
Crowne Plaza, Hotel



17
Hotel Vintage, Hotel



3
Cielo Apartments, Residential Mixed-Use



6
Lowell Emerson Apartments, Residential



9
The Mill at First Hill, Residential



12
Plymouth Congregational, Religious Service



15
Women's University Club, Cultural



18
Nakamura Federal Courthouse, Municipal

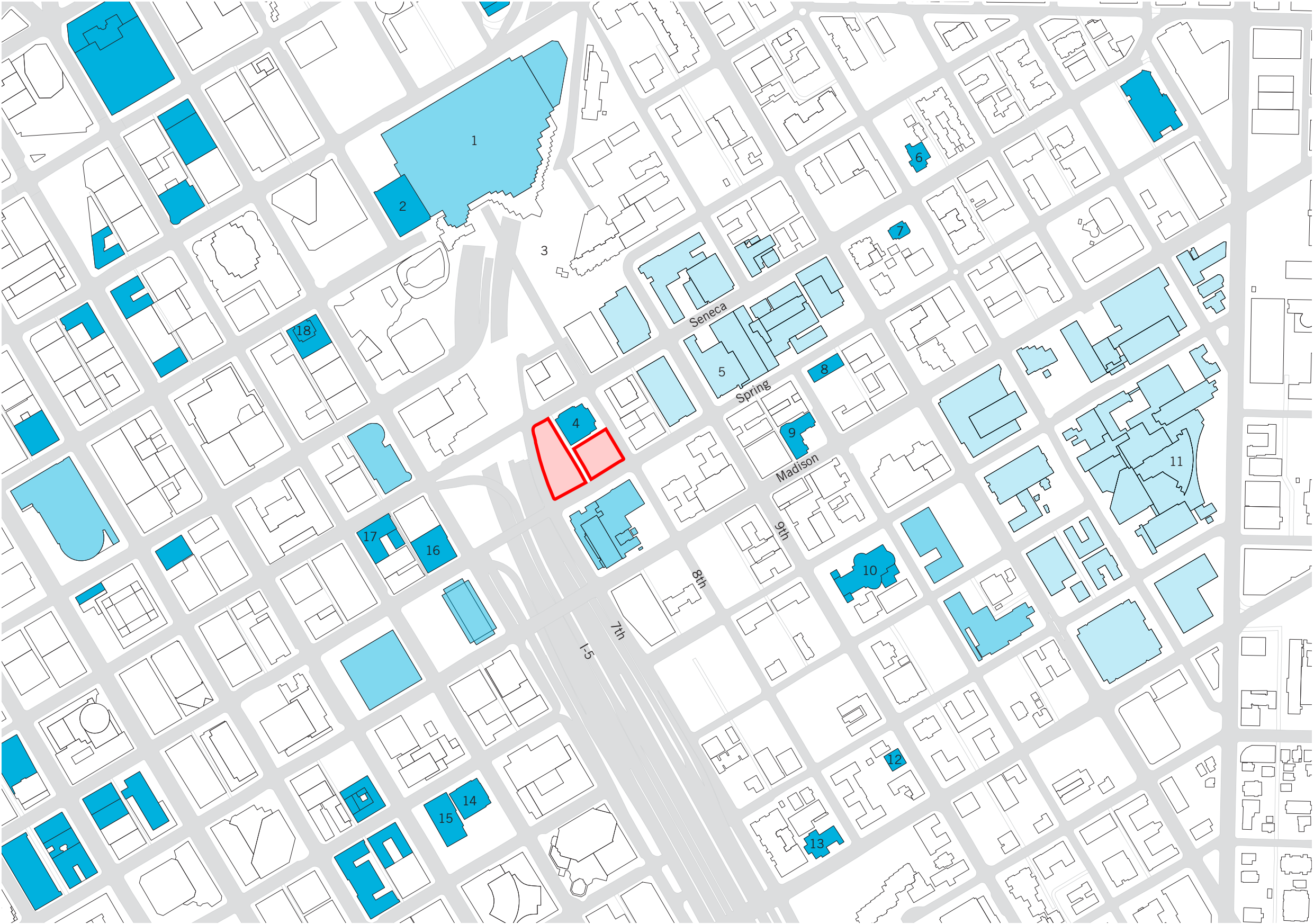
COMMUNITY NODES + LANDMARKS /

The site is located along the I-5 corridor and south of the convention center - a location and prominence that many associate with the ritual of arriving to Seattle. The other buildings along the corridor, while not always bearing architectural or historical significance in and of themselves, are iconic and identifiable as a network of place-makers for Seattle.

The nine-block area around the site is a bottleneck for traffic between First Hill and the downtown core. Pedestrian and vehicular traffic alike pass by or through the site on the way to their destination. Freeway Park, and the area neighboring it, acts as a gateway welcoming people between Downtown and First Hill, respectively.

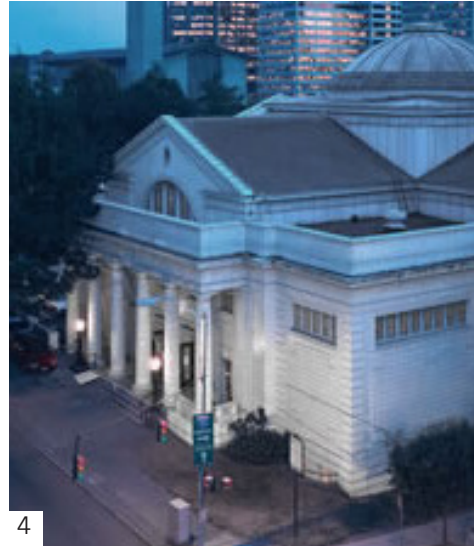
The proposed development looks to contribute to this identity with a tasteful and carefully considered addition to the Town Hall block.

- Site
- Designated Landmark Building
- Notable Building
- Notable Campus





1
Convention Center



4
Town Hall



7
Dearborn House



10
St. James Cathedral



13
Trinity Parish Episcopal Church



16
Women's University Club



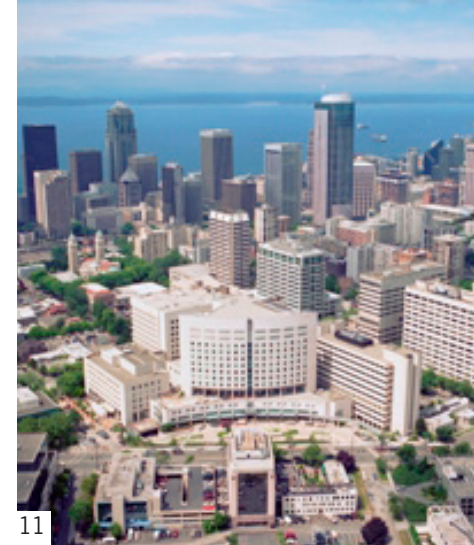
2
Eagles Temple Building



5
Virginia Mason Medical Campus



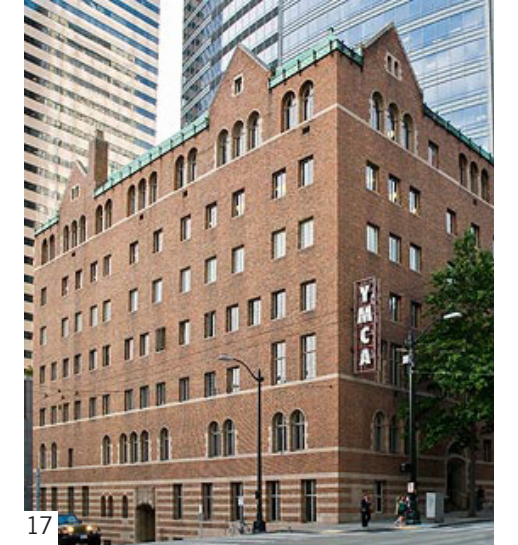
8
Baroness Apartment Hotel



11
Swedish Medical Campus



14
First United Methodist Church



17
YMCA Club



3
Freeway Park



6
Stimson / Green House



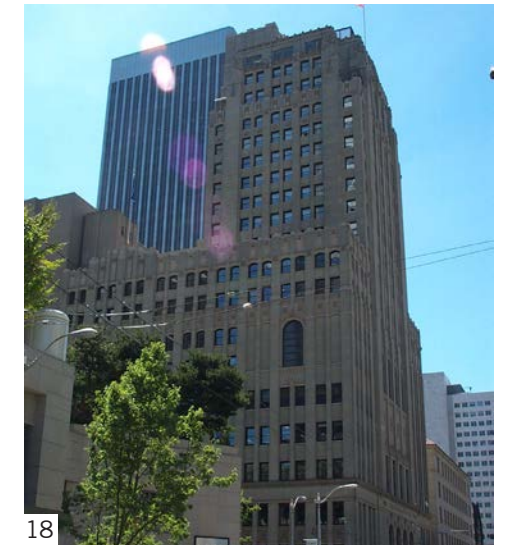
9
Sorrento Hotel



12
US Assay Office / German House



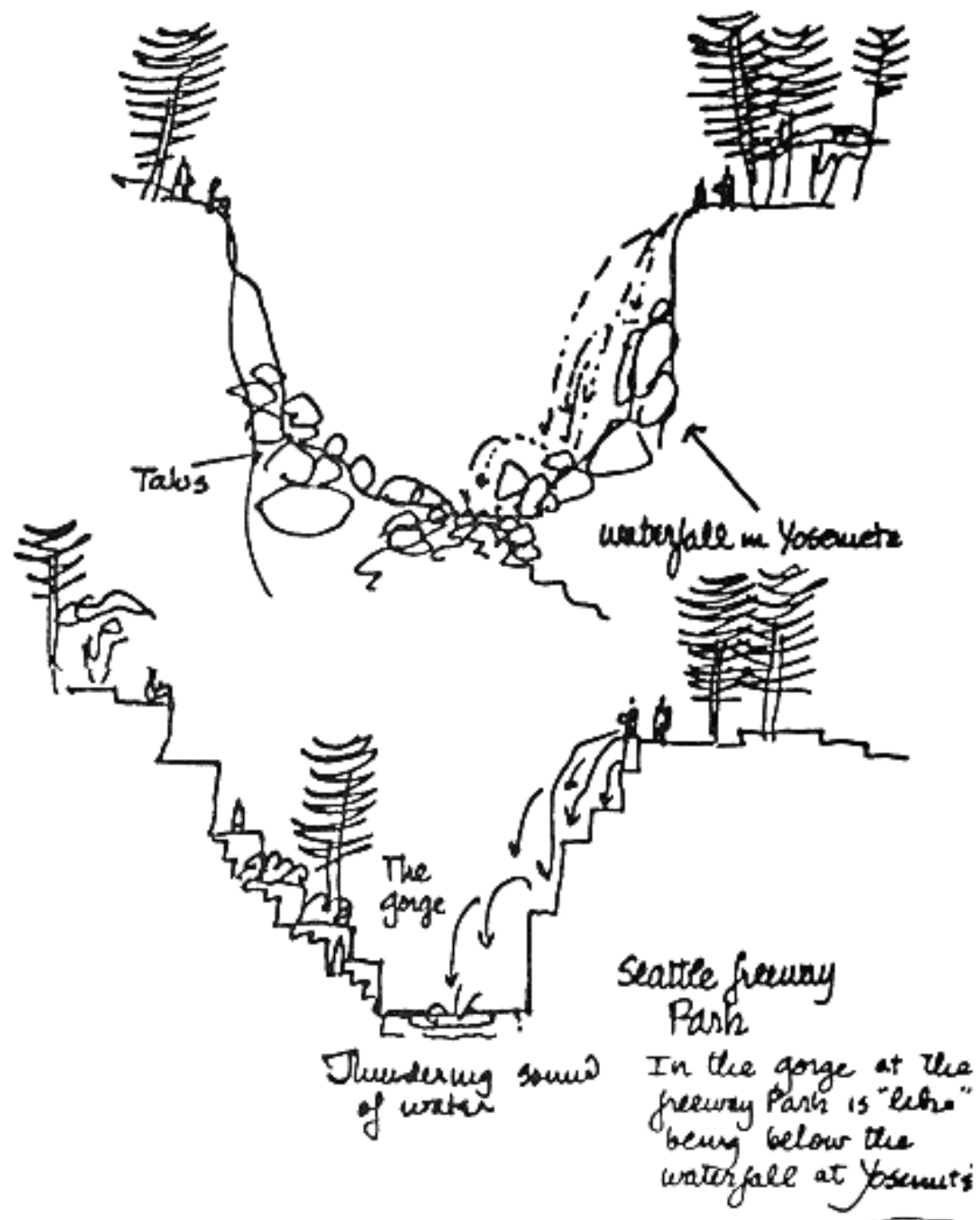
15
Rainier Club



18
Washington Athletic Club

“THE GREATEST CHALLENGE
FOR THE GARDEN DESIGNER IS
NOT TO MAKE THE GARDEN LOOK
NATURAL BUT TO MAKE THE
GARDEN SO THAT THE PEOPLE
IN IT WILL FEEL NATURAL.”

-Lawrence Halprin



HISTORY AND INFLUENCE

Freeway Park, in its most simplistic and utilitarian purpose, is a connection between the Downtown Core and the First Hill Neighborhoods. The park's elegant, playful, and poetic approach turns this connection into an experience for the passerby and parkgoer, alike. Floating above I-5, the park has added a space to the city and created an environment above an urban void. The proposed development has sought to continue and compliment the rich history and influence of the park through collaborative sessions with the Freeway Park Neighborhood Association and by creating physical connections between the development site and the park.

Freeway Park Sketch by Lawrence Halprin



Freeway Park



Freeway Park facing south



Freeway Park facing north

TOWN HALL /



© 48144
ESTHER CHOTIS

HISTORY AND INFLUENCE

Town Hall, formerly the Fourth Church of Christ Scientist, stands on the northeast corner of the block. An icon harkening back to the early twentieth century, Town Hall was constructed from 1916-1922 and was designed by Portland architect, George Foote Dunham. The glazed terra-cotta adds texture and weight while the plan of the building and its facades speak to the importance of entry, directionality and context. Town Hall was, and is today, a unique presence in the neighborhood. Approach from all sides of the structure was taken into consideration, and invites the community to approach and convene from divergent points. The proposed development has invested in a collaborative relationship with Town Hall stakeholders in a shared pursuit of a harmonious block.

**Images from Paul Dorpat, who specializes in historical photography and has published several books on early Seattle.*



Town Hall, view from corner of 8th Avenue and Seneca Street



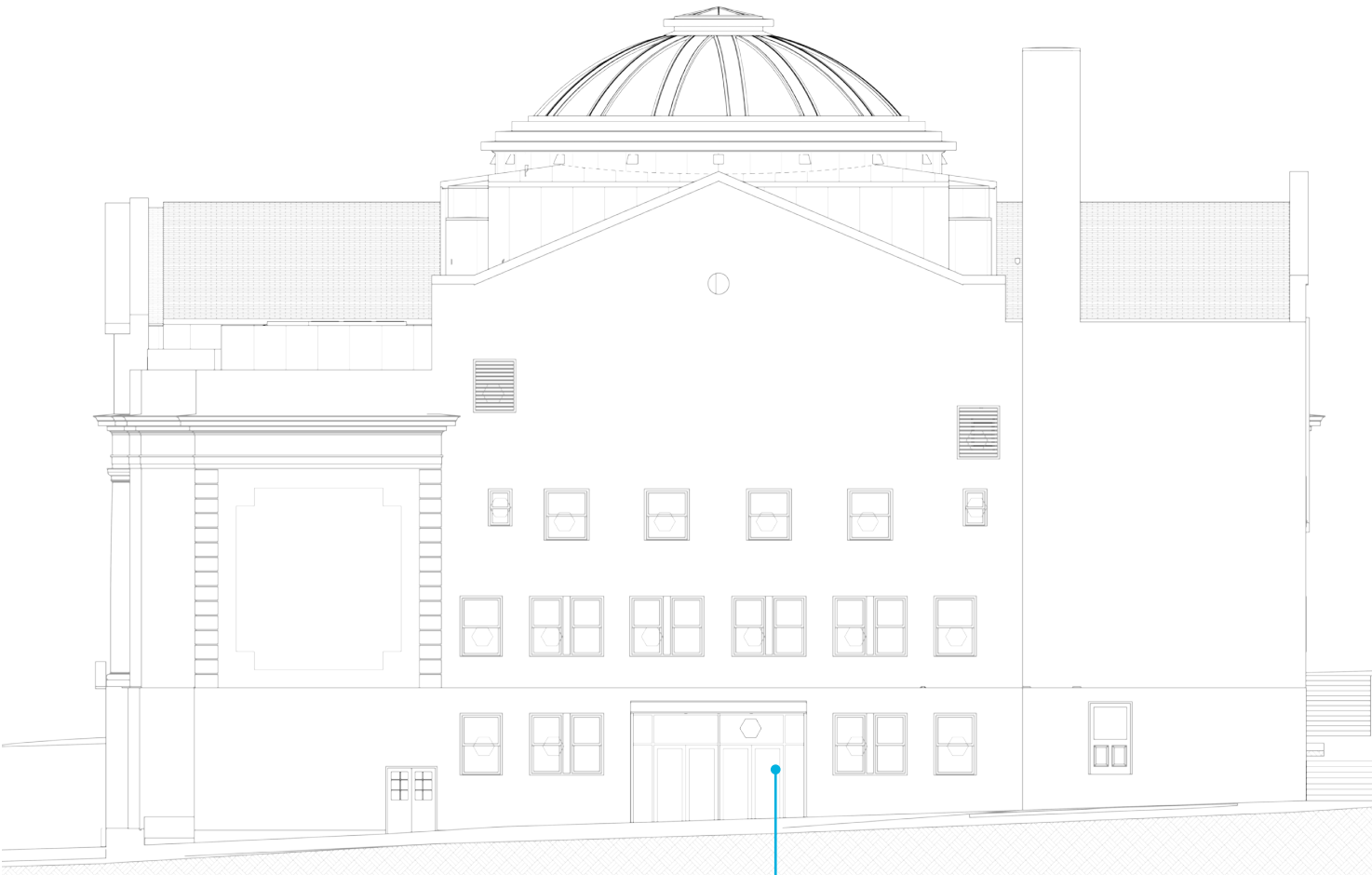
Town Hall



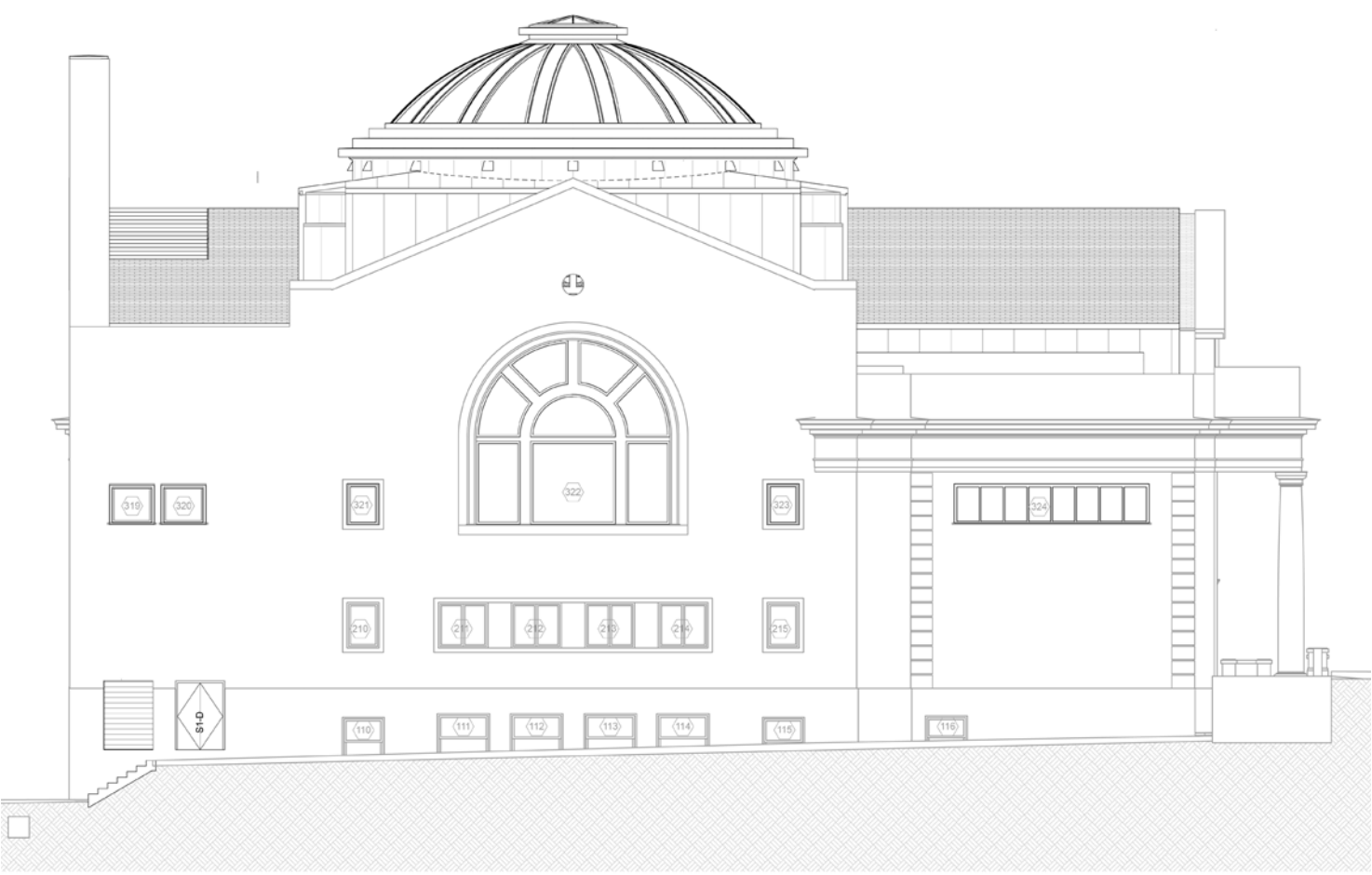
*Town Hall, view facing west towards Elliot Bay, circa early 20th century**



*Town Hall, view facing east along Seneca Street, circa early 20th century**



Town Hall West Facade - Proposed

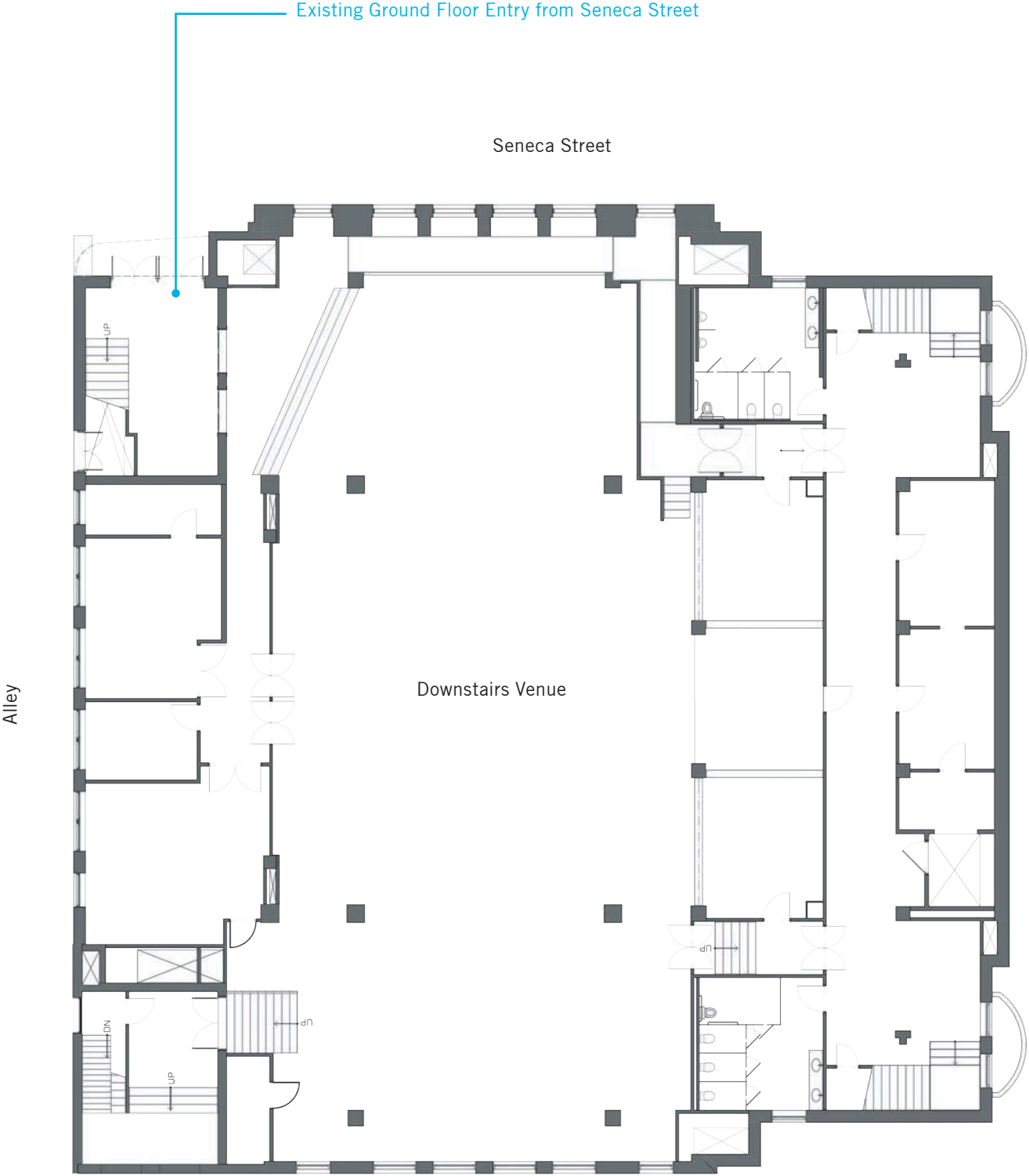


Town Hall South Facade - Proposed

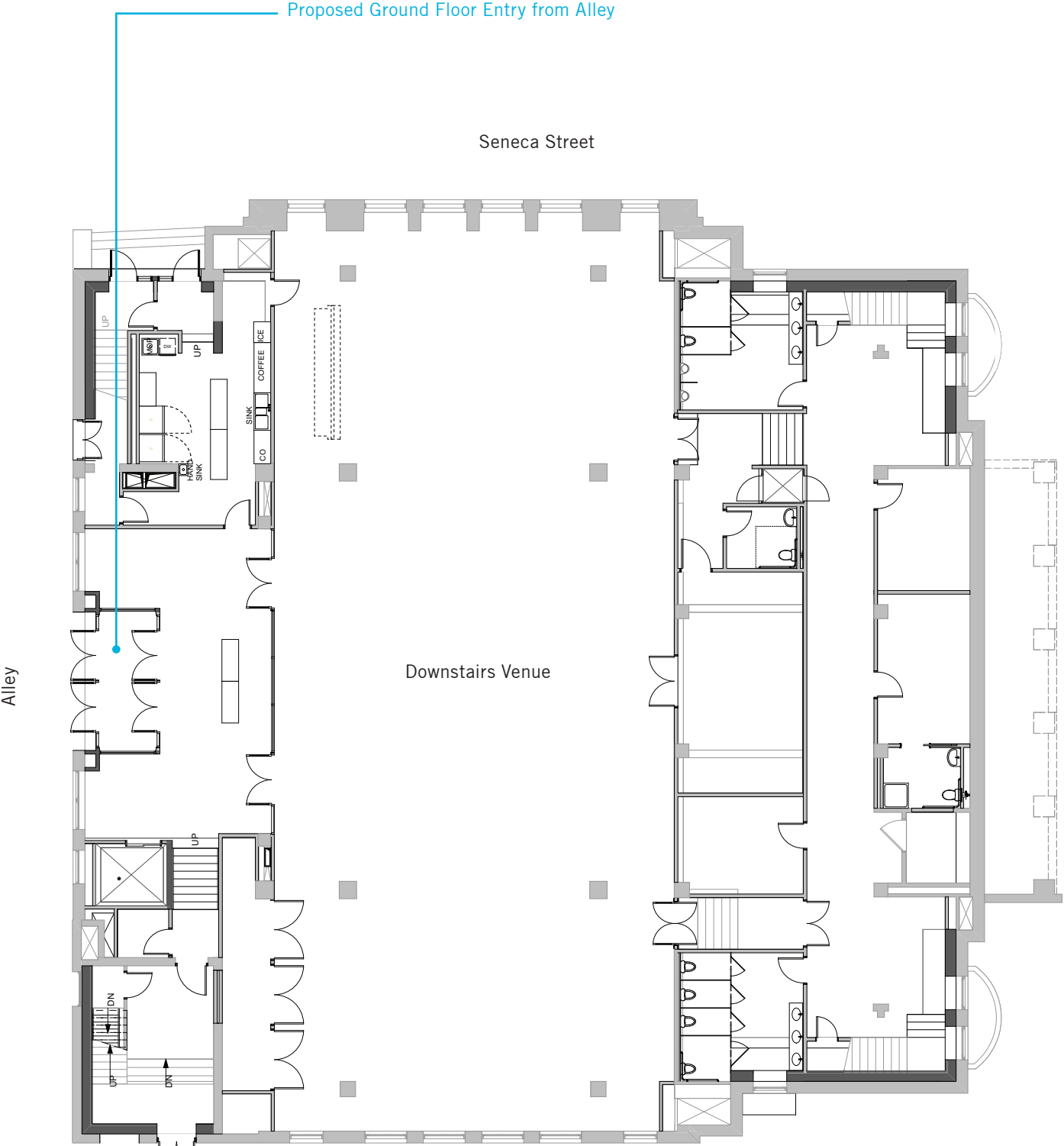
Proposed Entry in Existing Concrete Wall

SITE SYNERGY AND CONNECTION

Town Hall, in collaboration with designers Weinstein A+U, have proposed a substantial renovation of the historic Town Hall Seattle building to improve seismic performance and achieve ADA accessibility. One improvement in particular, the addition of a new entry along the alley of the west facade, creates an opportunity for site synergy between Town Hall and the proposed development.



Existing Plaza Level Plan



Proposed Plaza Level Plan



ECLECTIC PALETTE

The neighborhood surrounding the site is composed of buildings with a broad range of materials and styles. No singular element or style permeates the neighborhood enough to establish a precedent of influence.

Instead, this project looks to draw a refined set of influences from the contextual palette to create a confluence of simplicity and sophistication.

- Site
- Terracotta / Light Concrete
- Tan Brick / Light Concrete
- Red Brick
- Dark Concrete
- Glass





Red Brick - YMCA



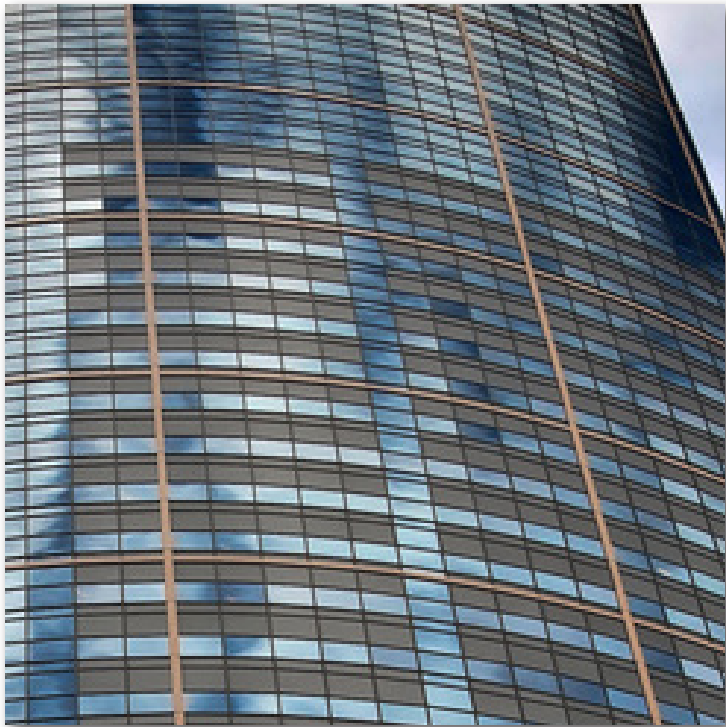
Red Brick - Sorrento Hotel



Dark Concrete - First Presbyterian



Terracotta - Town Hall



Glass - Two Union



Terracotta - Nakamura Federal Courthouse



Tan Brick - Benaryoa Research Institute



Glass - Convention Center





1
View from Hubbell Place facing east



4
View of adjacent Freeway Park space on Seneca Street



2
View from corner of Hubbell Place and Seneca Street facing southeast



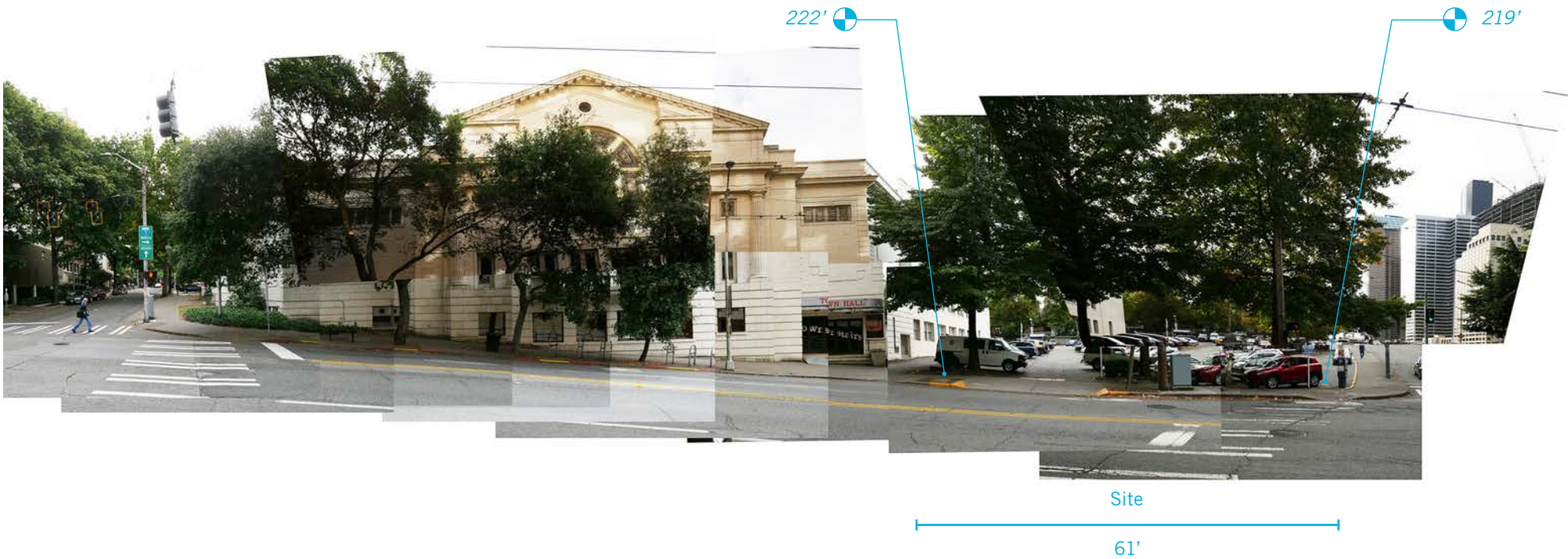
5
View from 8th Avenue facing west



3
View from corner of Hubbell Place and Spring Street facing east



6
View from 6th Avenue and Madison Street facing northeast



NORTH STREETSCAPE (SITE)



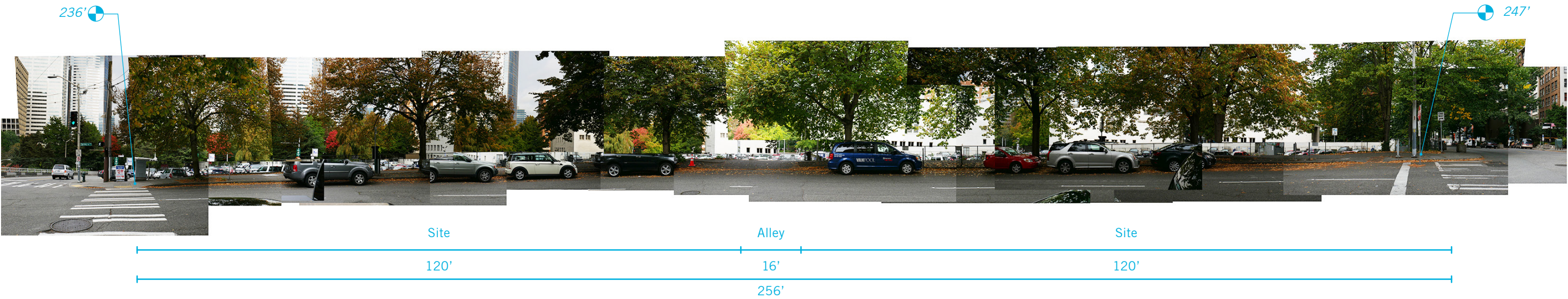
NORTH STREETSCAPE (OPPOSITE OF SITE)



WEST STREETSCAPE (SITE)



WEST STREETSCAPE (OPPOSITE OF SITE)



SOUTH STREETSCAPE (SITE)



SOUTH STREETSCAPE (OPPOSITE OF SITE)



EAST STREETSCAPE (SITE)



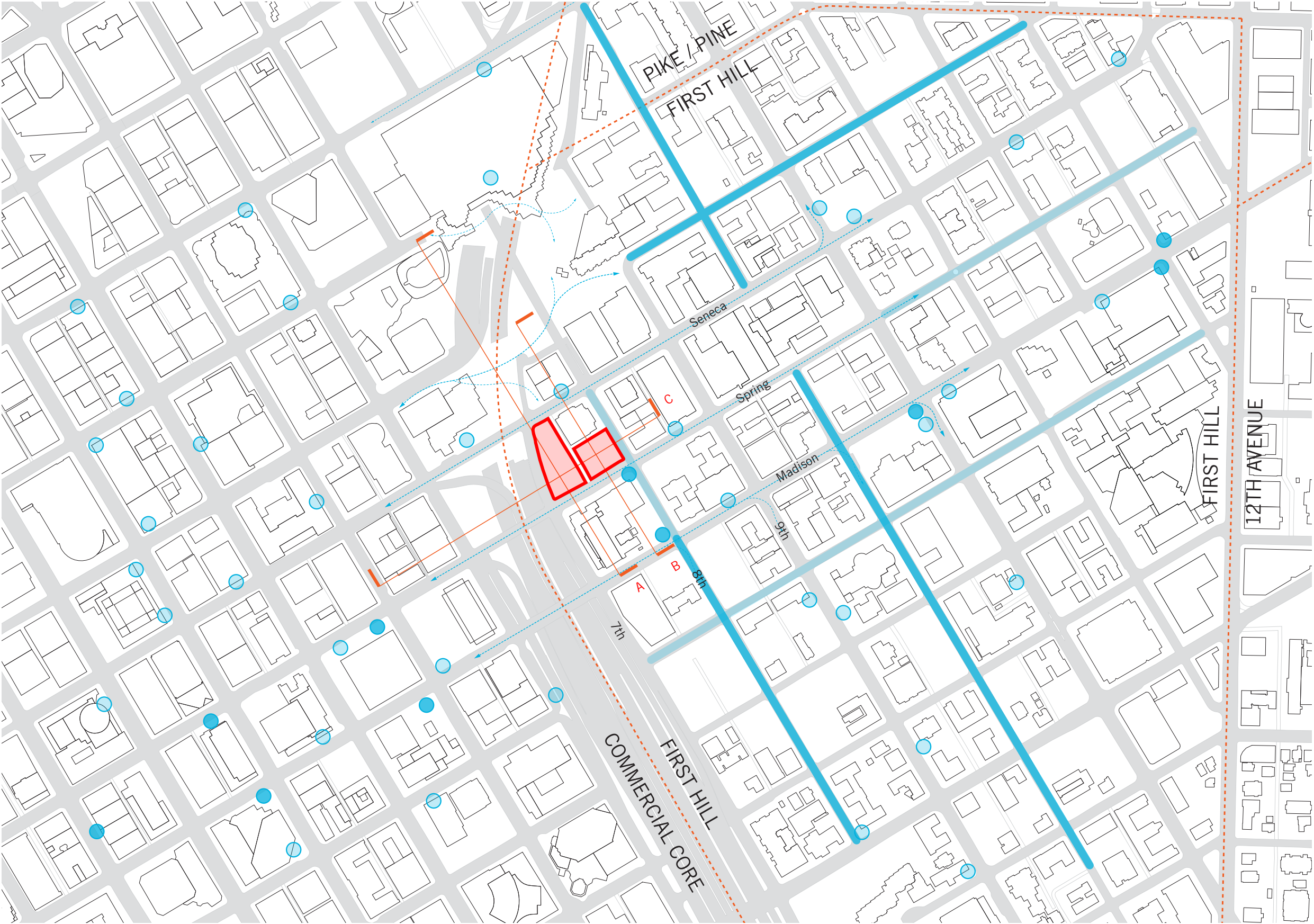
EAST STREETSCAPE (OPPOSITE OF SITE)

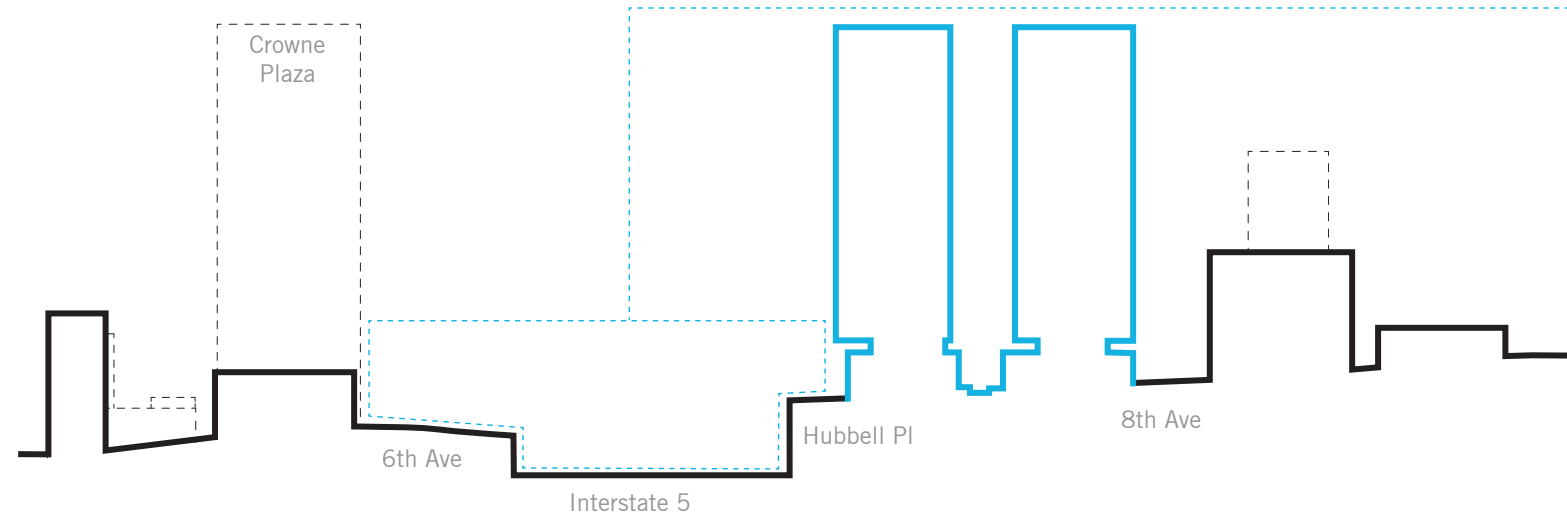
NARRATIVE

Located along the western slope of First Hill, this project sits at the convergence of major thoroughfares between the downtown business district to the west, and the residential and medical district to the east.

Major transportation routes funnel through this area including the Bus Rapid Transit line, along Spring Street to the south, as well several other major bus routes. Madison Street, to the south, acts as a major east/west arterial between Elliot Bay and Lake Washington.

- Site
- Green Street (current)
- Green Street (proposed)
- Transit Station
- BRT Station (proposed)
- Pedestrian Artery (primary path)
- Pedestrian Artery (secondary path)
- Urban Village Boundary

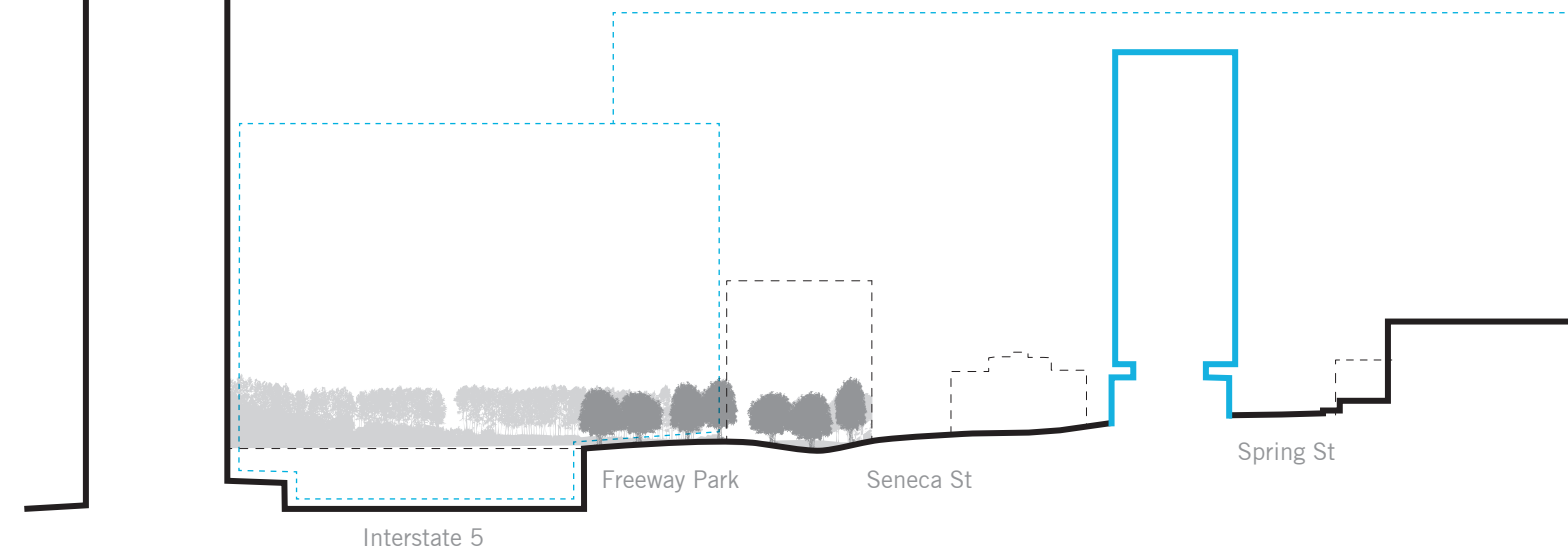
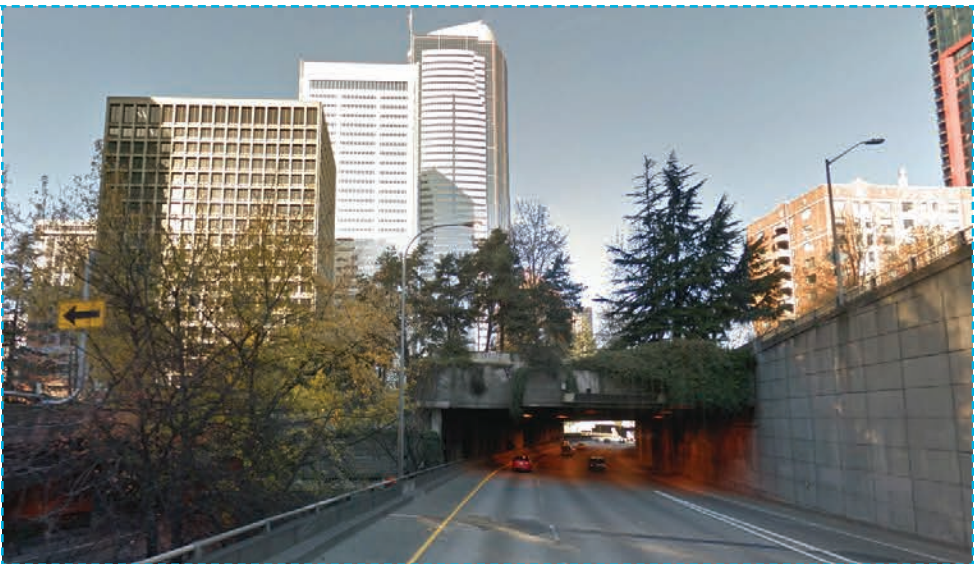




C



B



A



Context Map

PLANNED STREET IMPROVEMENTS:

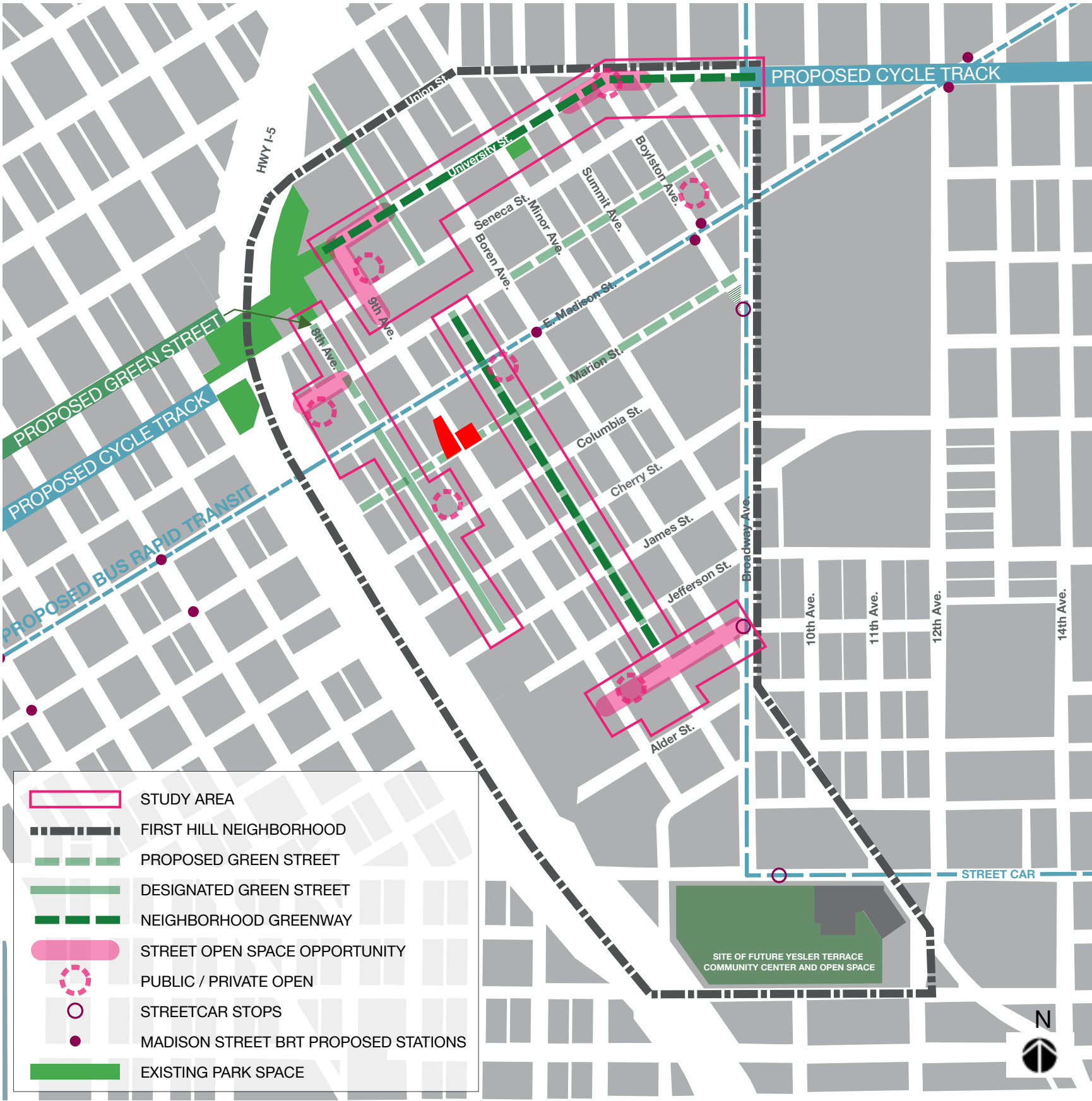
- 2015
- Broadway Streetcar
 - Madison Bus Rapid Transit Concept Design Phase
 - Construction start date TBD
- 2017
- Neighborhood Greenway - Terry Ave / Melrose Ave E / Franklin Ave E (From Broadway to Eastlake Ave E)
 - Neighborhood Greenway - Union St (From Broadway to 9th Ave)
- 2018
- Neighborhood Greenway - Marion St (From 7th Ave to Broadway)

Neighborhood greenways are a combination of small improvements that make residential streets safer and calmer for people of all ages and abilities.

Madison Street Bus Rapid Transit service will be fast, reliable and frequent. It will serve densely developed neighborhoods in First Hill, the Central Area, and downtown Seattle, connecting dozens of bus routes, the First Hill Streetcar, and ferry service at the Colman Dock Ferry Terminal.

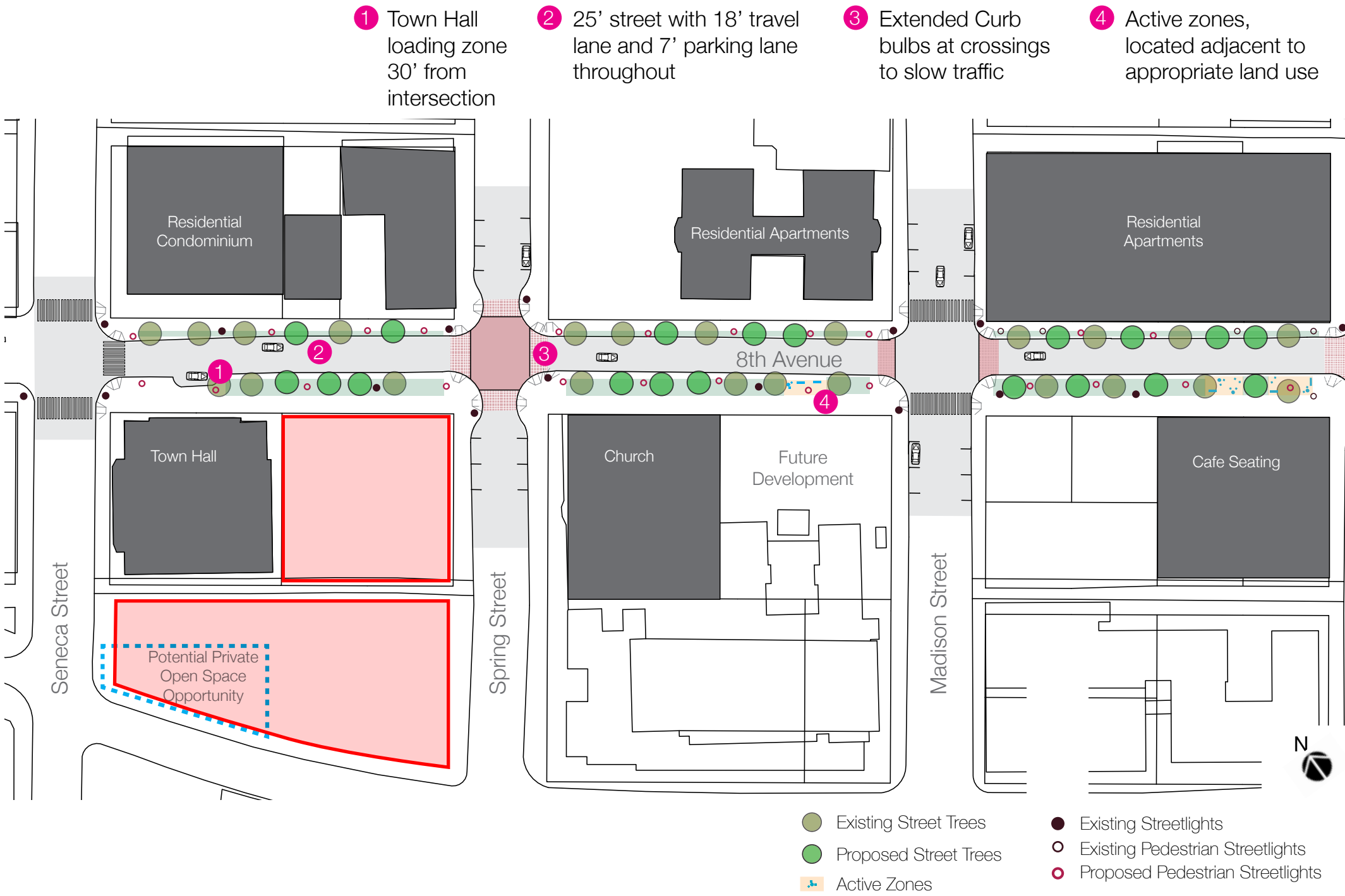
Site

Graphic content from the first Hill Public Realm Action Plan, dated Winter 2015



8th Avenue

Overall Concept Plan



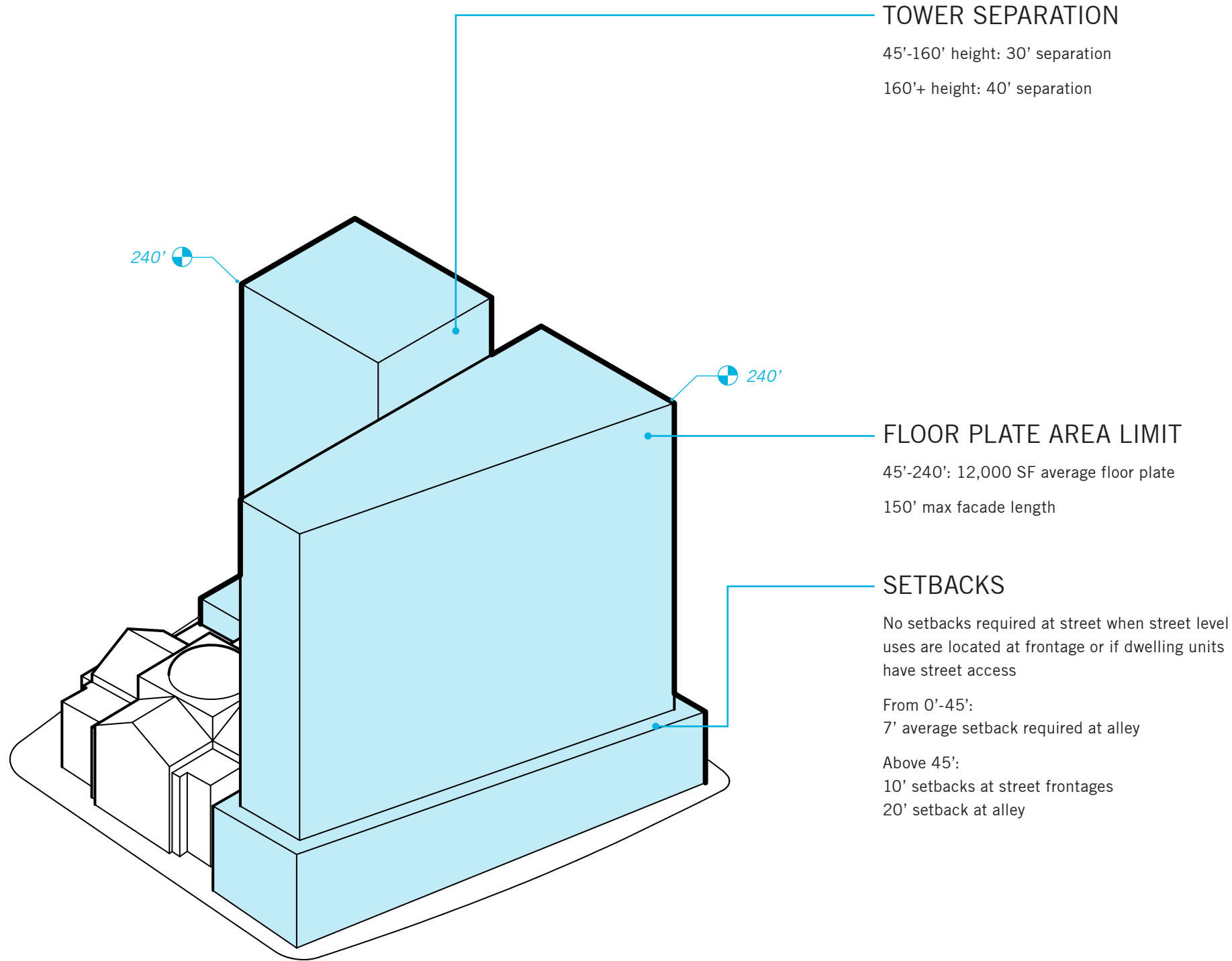
Site

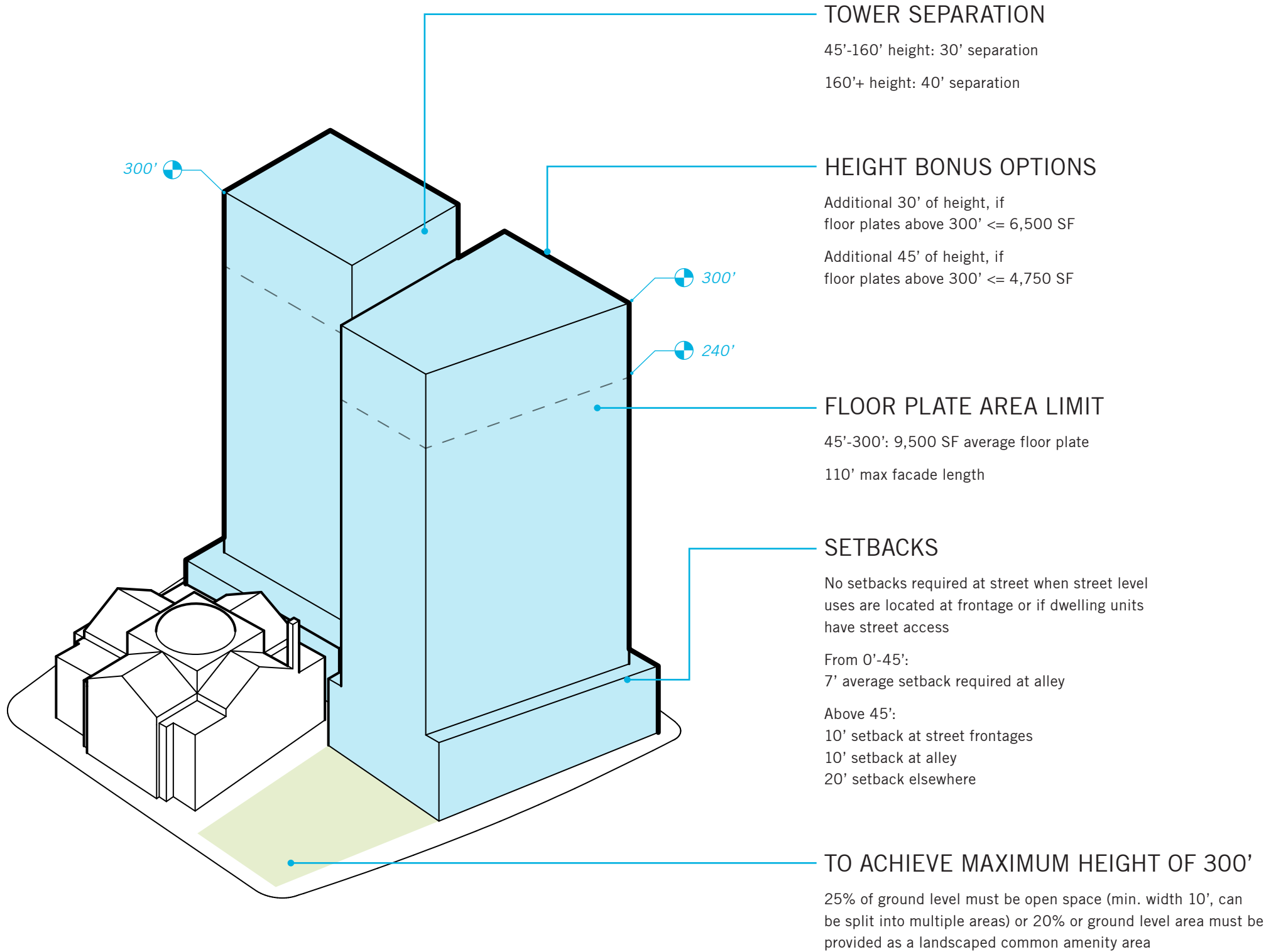
Graphic content from the first Hill Public Realm Action Plan, dated Winter 2015

FIRST HILL PUBLIC REALM ACTION PLAN
8TH AVENUE

SECTION 06

ZONING DATA





SECTION 07

DESIGN

GUIDELINES



NATURAL SYSTEMS AND SITE FEATURES:
WATER

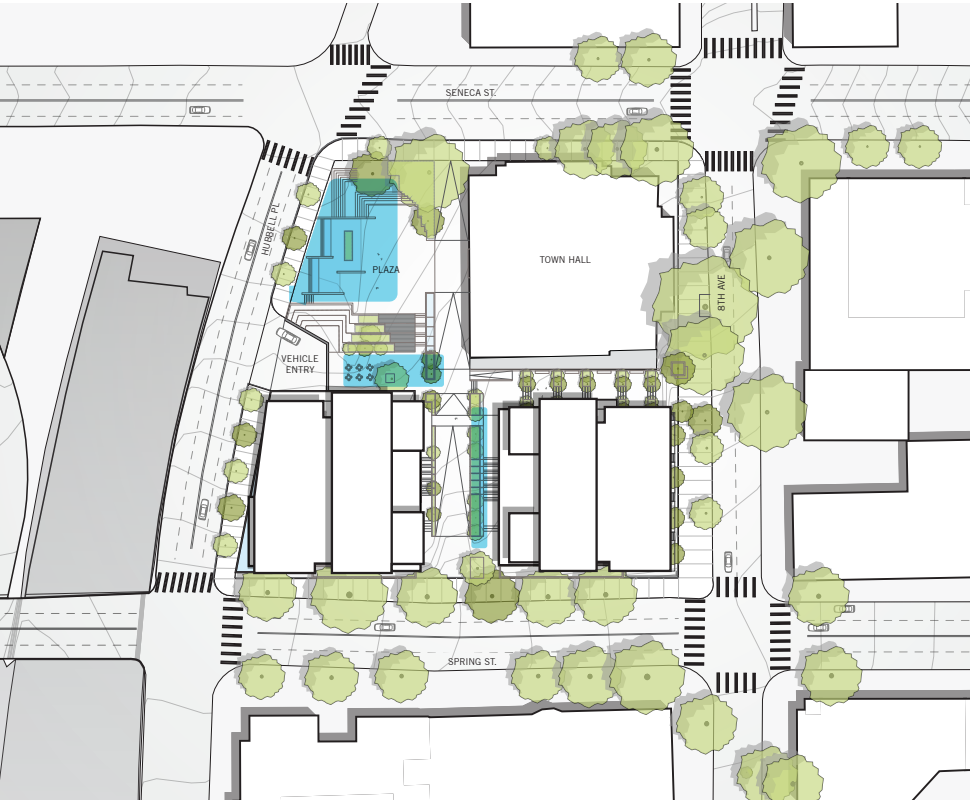
CS1-E-1 NATURAL WATER FEATURES:

If the site includes any natural water features, consider ways to incorporate them into project design, where feasible.

APPLICANT RESPONSE:

The proposed development uses water throughout the site to enhance the connection to Freeway Park and to create a unique identity for itself entirely. A cascading water feature runs along the pedestrian alley between the two towers, a fountain acts as the centerpiece for a small courtyard immediately north of the west tower, and cascading pools move water down large steps at the edge of the northwest plaza.

Right: summer in Freeway Park circa 1980



URBAN PATTERN AND FORM:
ADJACENT STREETS AND OPEN SPACES

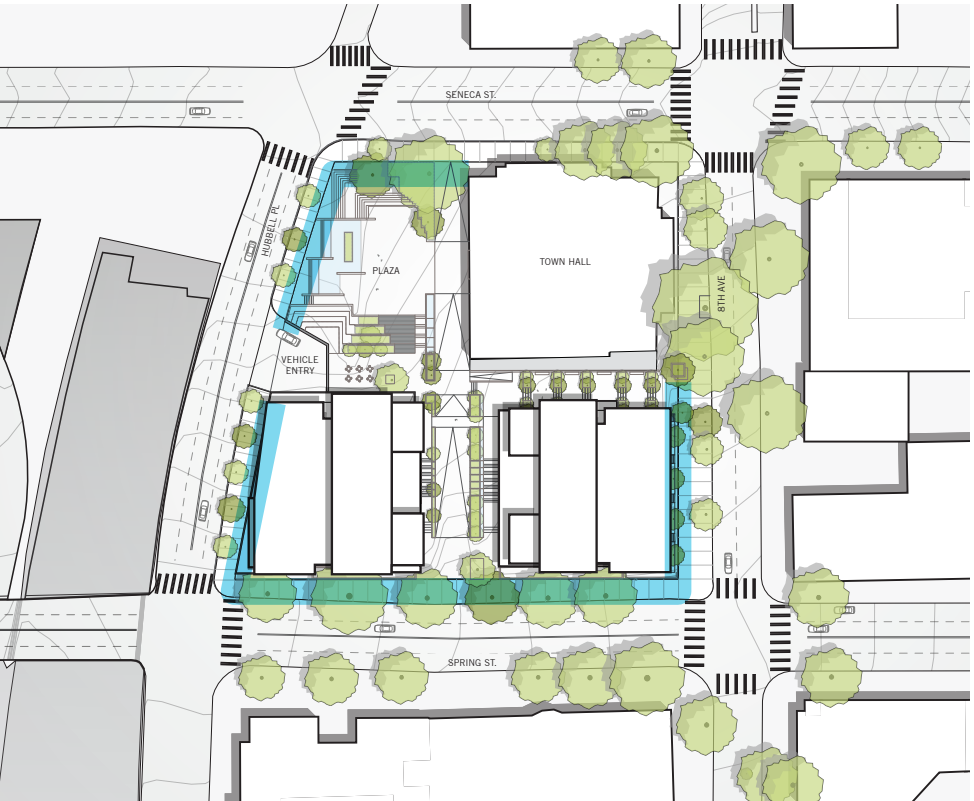
CS2-B-2 CONNECTION TO THE STREET:

Identify opportunities for the project to make a strong connection to the street and carefully consider how the building will interact with the public realm. Consider the qualities and character of the streetscape—its physical features (sidewalk, parking, landscape strip, street trees, travel lanes, and other amenities) and its function (major retail street or quieter residential street)—in siting and designing the building.

APPLICANT RESPONSE:

Open spaces and connections across the block are core principles for the proposed development. The site generously cedes a large area as a plaza in the northwest corner of the site. Pending an alley dedication, the area between the two towers, will be a landscaped terrace that steps and slopes between Seneca Street and Spring Street. Planters, trees, and seating are carefully arranged around the site to create places that invite people to rest, gather, or pass through on the way to their destination.

Right: Perkins+Will project Westbank Frye Towers in First Hill





OPEN SPACE CONCEPT: NEIGHBORHOOD FABRIC

DC3-C-1 REINFORCE EXISTING OPEN SPACE:

Where a strong open space concept exists in the neighborhood, reinforce existing character and patterns of street tree planting, buffers or treatment of topographic changes.

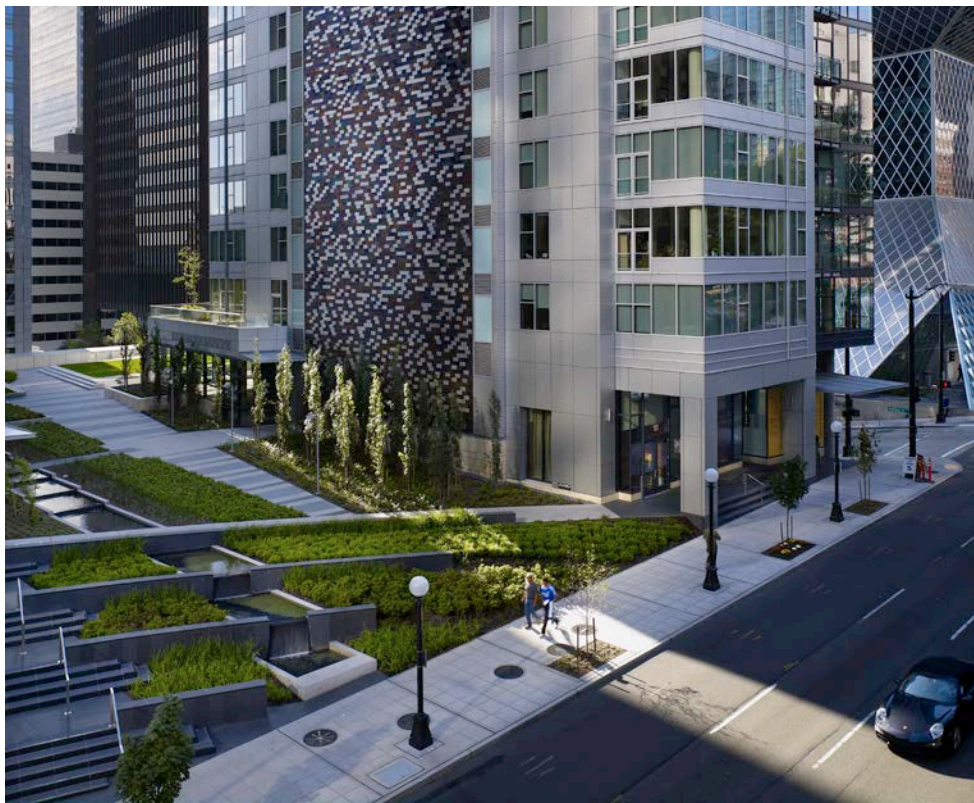
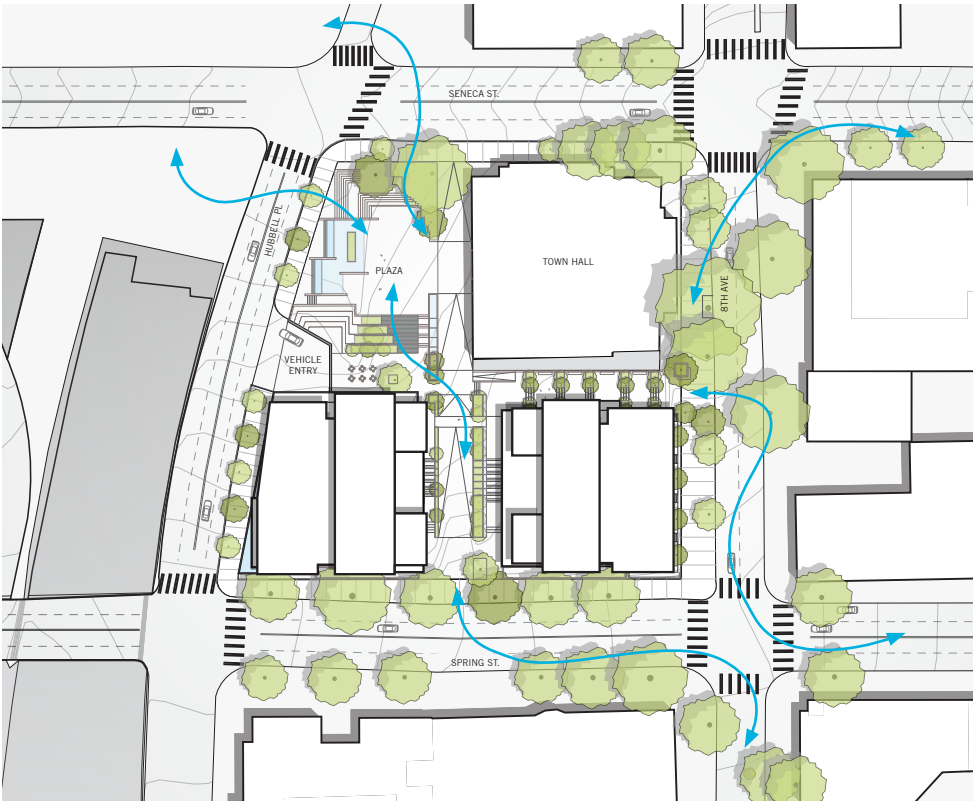
PL1-A-1 ENHANCING OPEN SPACE:

Design the building and open spaces to positively contribute to a broader network of open spaces throughout the neighborhood. Consider ways that design can enhance the features and activities of existing off-site open spaces.

APPLICANT RESPONSE:

The proposed design looks to Freeway Park and existing patterns in the First Hill neighborhood for inspiration and design direction. The combination of these neighborhood patterns and characteristics unique to itself, the proposed development seeks to create a system of open spaces that integrate, compliment, and enhance the existing open space fabric in First Hill.

Right: Harbor Steps pedestrian plaza



CONNECTIVITY: DIVERSITY OF OPEN SPACES

PL1-A-2 ADDING TO PUBLIC LIFE:

Seek opportunities to foster human interaction through an increase in the size and/or quality of project-related open space available for public life. Consider features such as widened sidewalks, recessed entries, curb bulbs, courtyards, plazas, or through-block connections, along with place-making elements such as trees, landscape, art, or other amenities, in addition to the pedestrian amenities listed in PL1.B3.

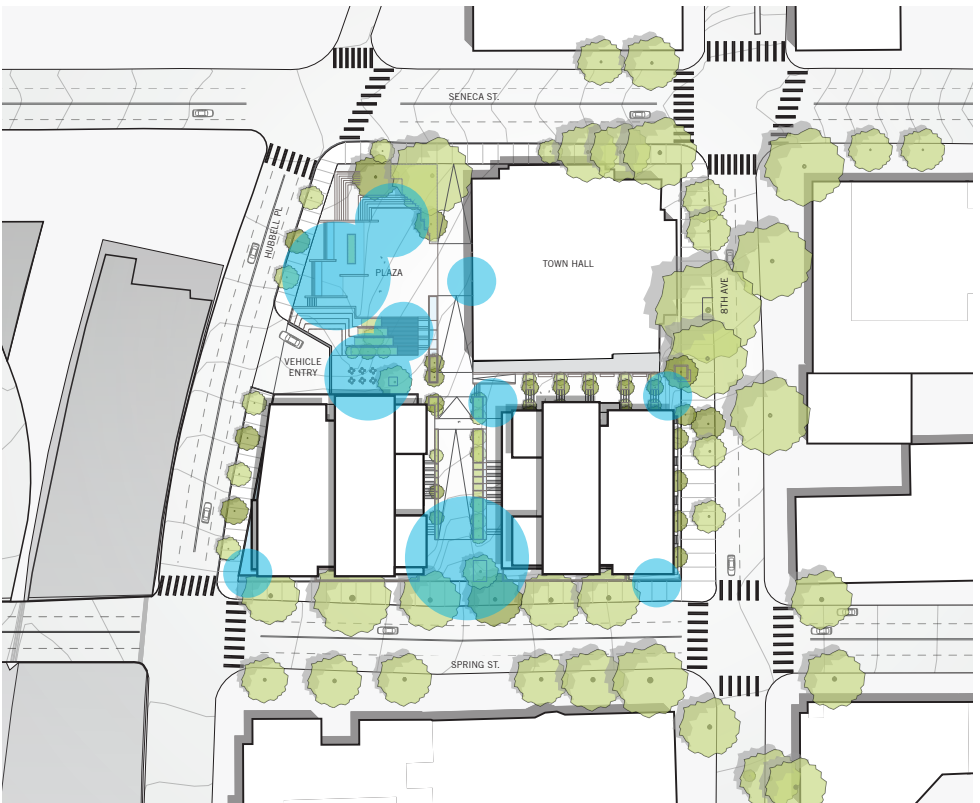
DC3-C-2 AMENITIES AND FEATURES:

Create attractive outdoor spaces well-suited to the uses envisioned for the project. Use a combination of hardscape and plantings to shape these spaces and to screen less attractive areas as needed.

APPLICANT RESPONSE:

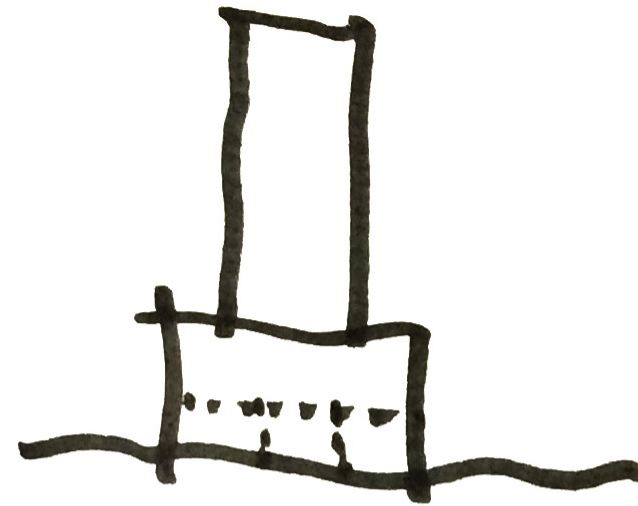
The site design uses landscaping, trees, seating, and open areas to foster human interaction and activity. Furthermore, the site is thoughtfully designed using areas of different scale, openness, and exposure to provide for varying user needs and identities.

Right: Perkins+Will and PFS Studio project at 5th and Madison in downtown

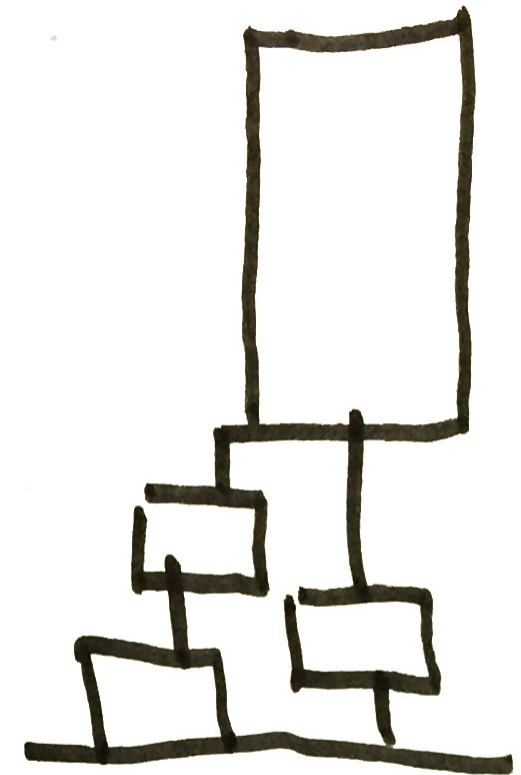


SECTION 08

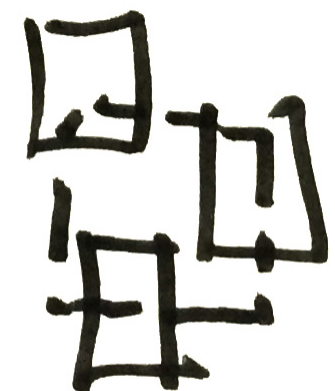
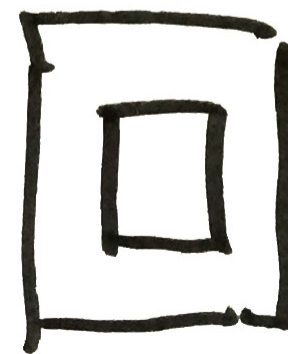
ARCHITECTURAL CONCEPTS



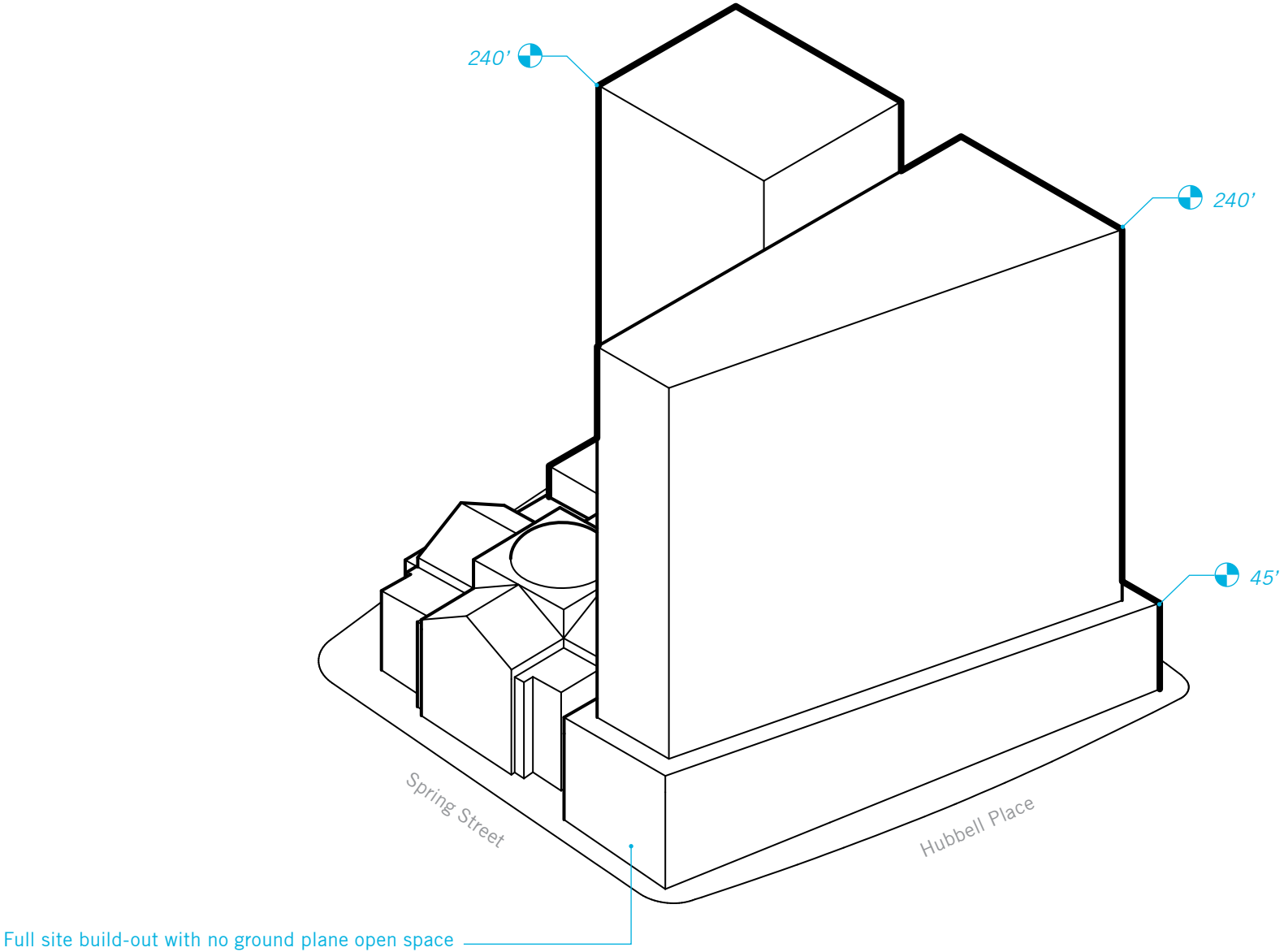
conventional



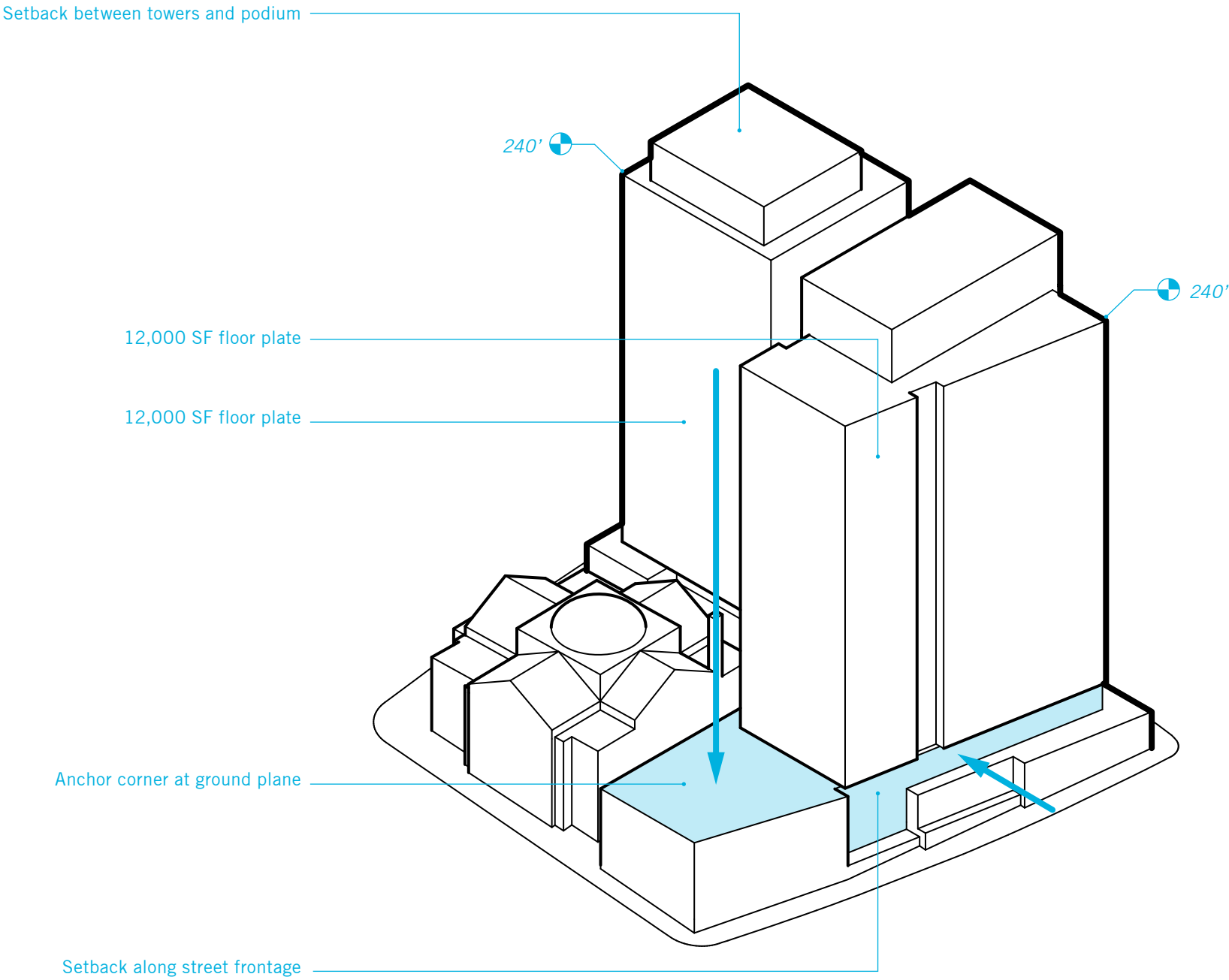
porosity



ZONING ENVELOPE







SETBACK AND SEPARATE

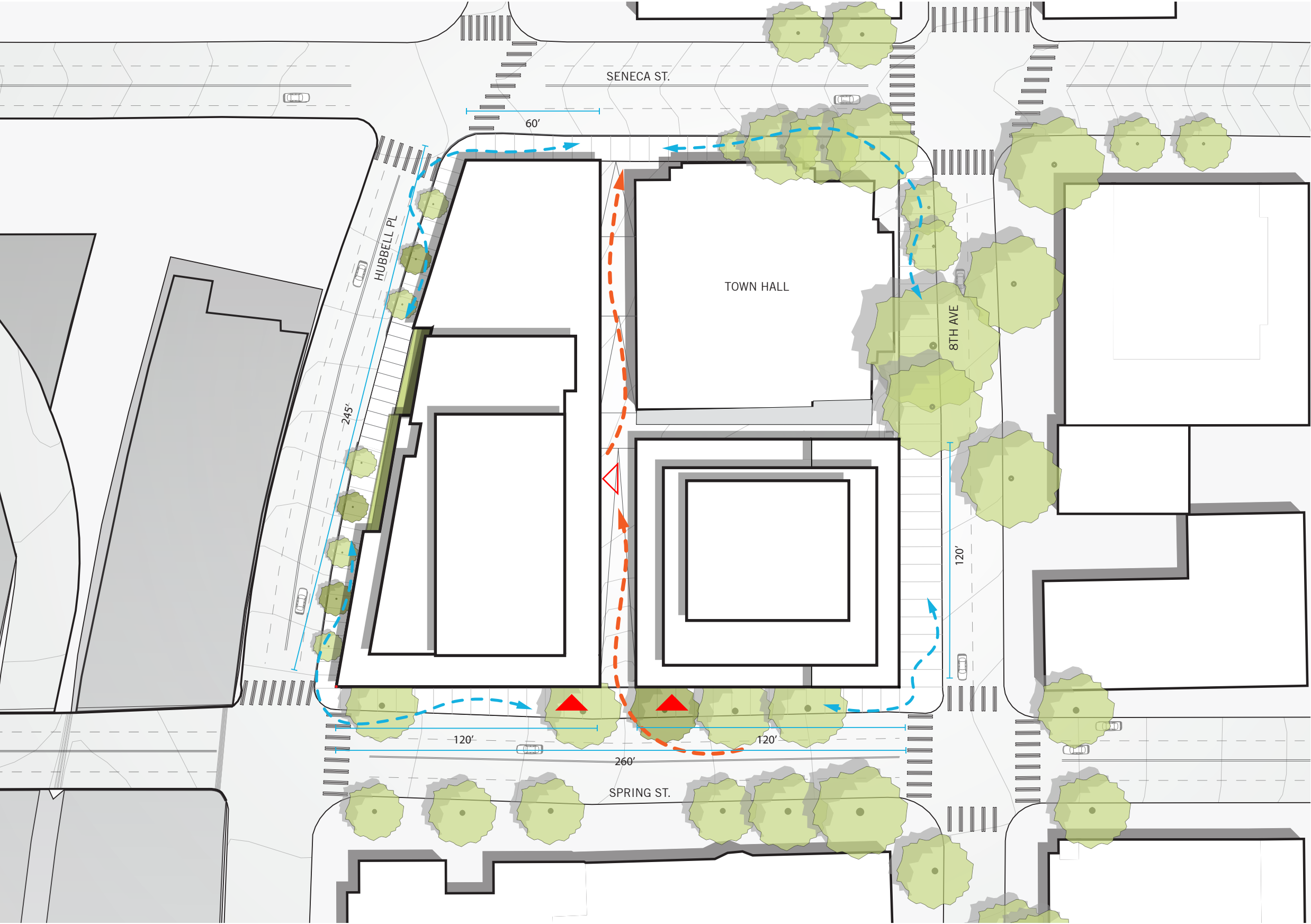




Massing model will be available at EDG Meeting

SITE PLAN

-  Pedestrian Entry
-  Vehicle Entry
-  Pedestrian Traffic
-  Vehicular Traffic





View from 8th Avenue



View from Spring Street



View from Seneca Street and Hubbell Place



View from Spring Street and 7th Avenue

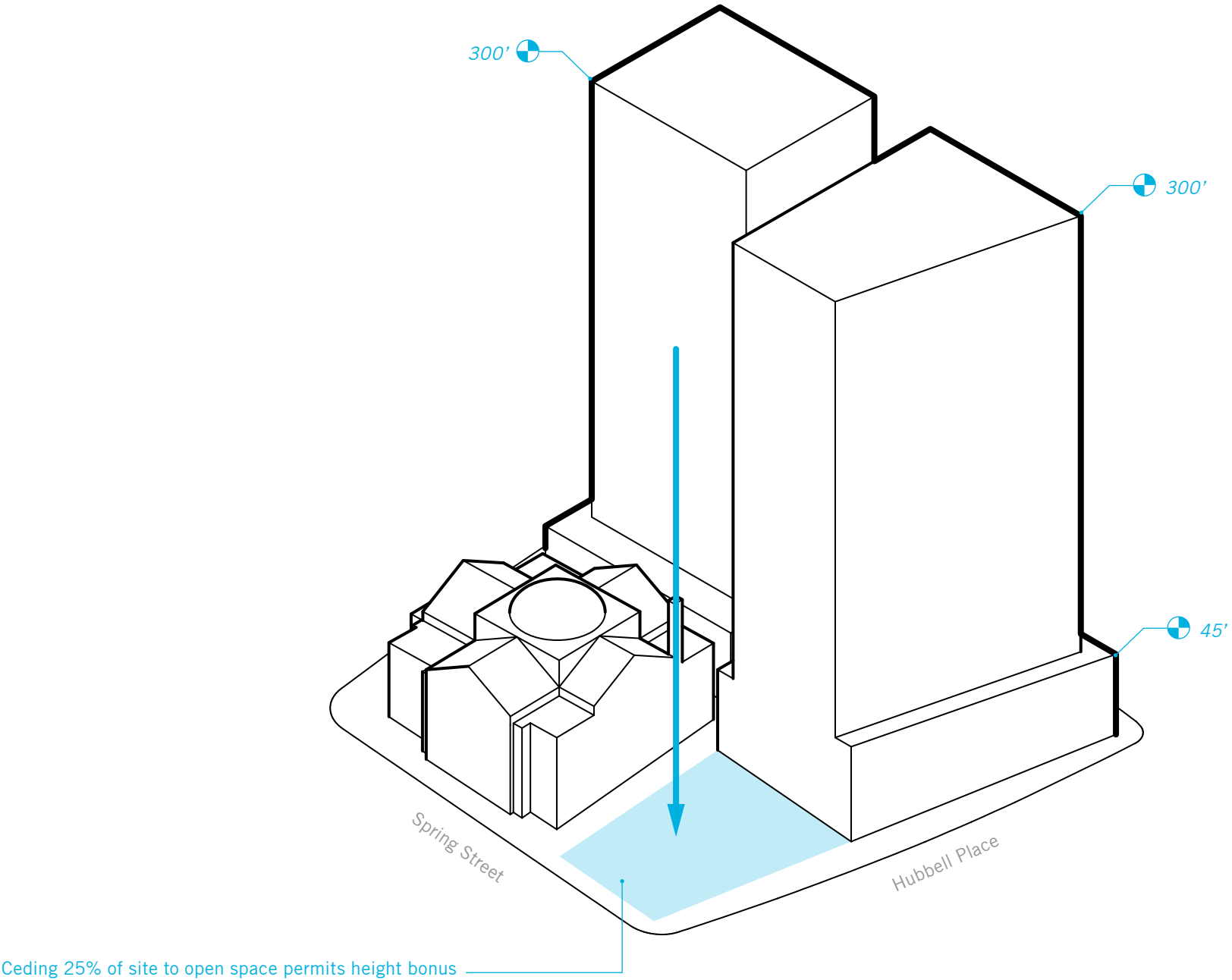


View from Seneca Street

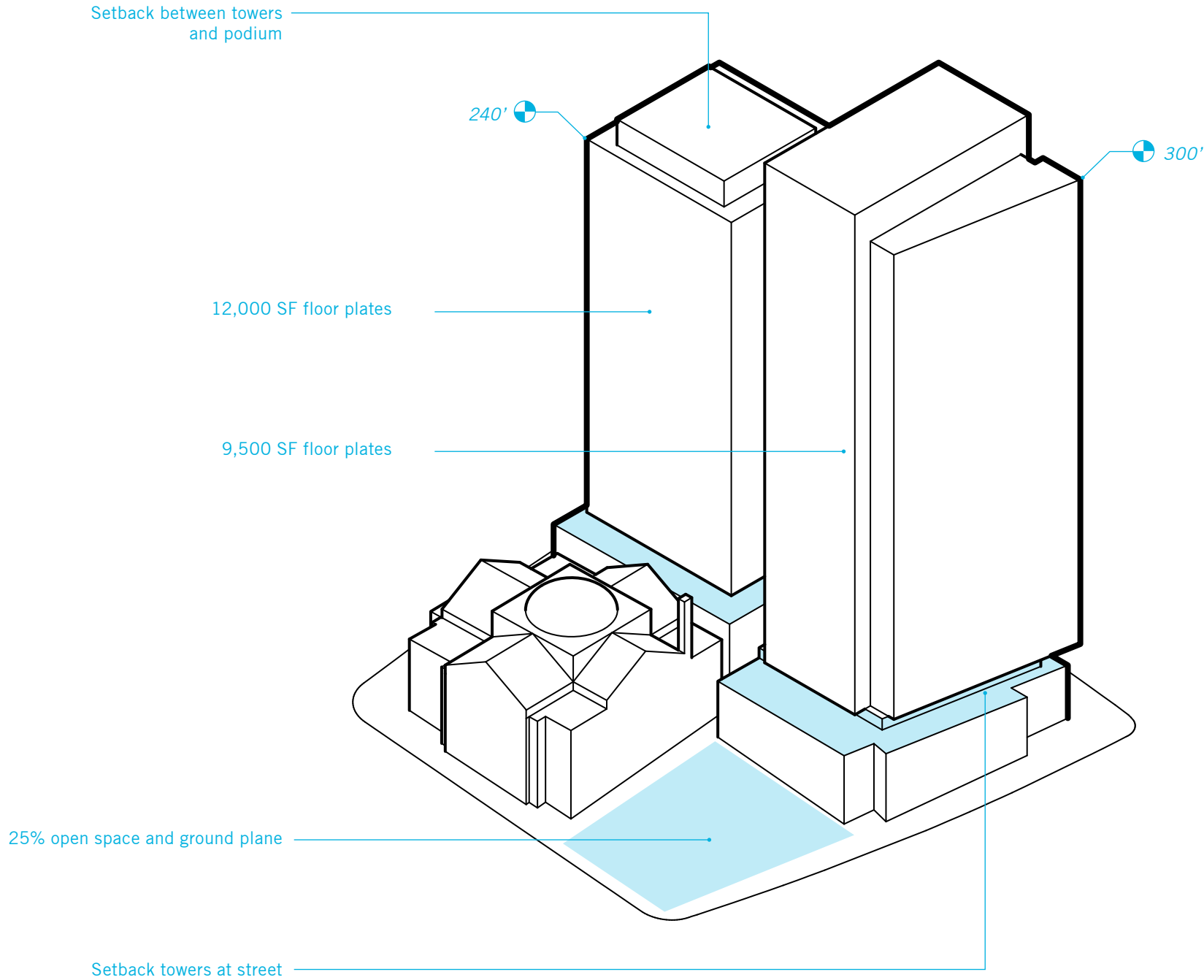


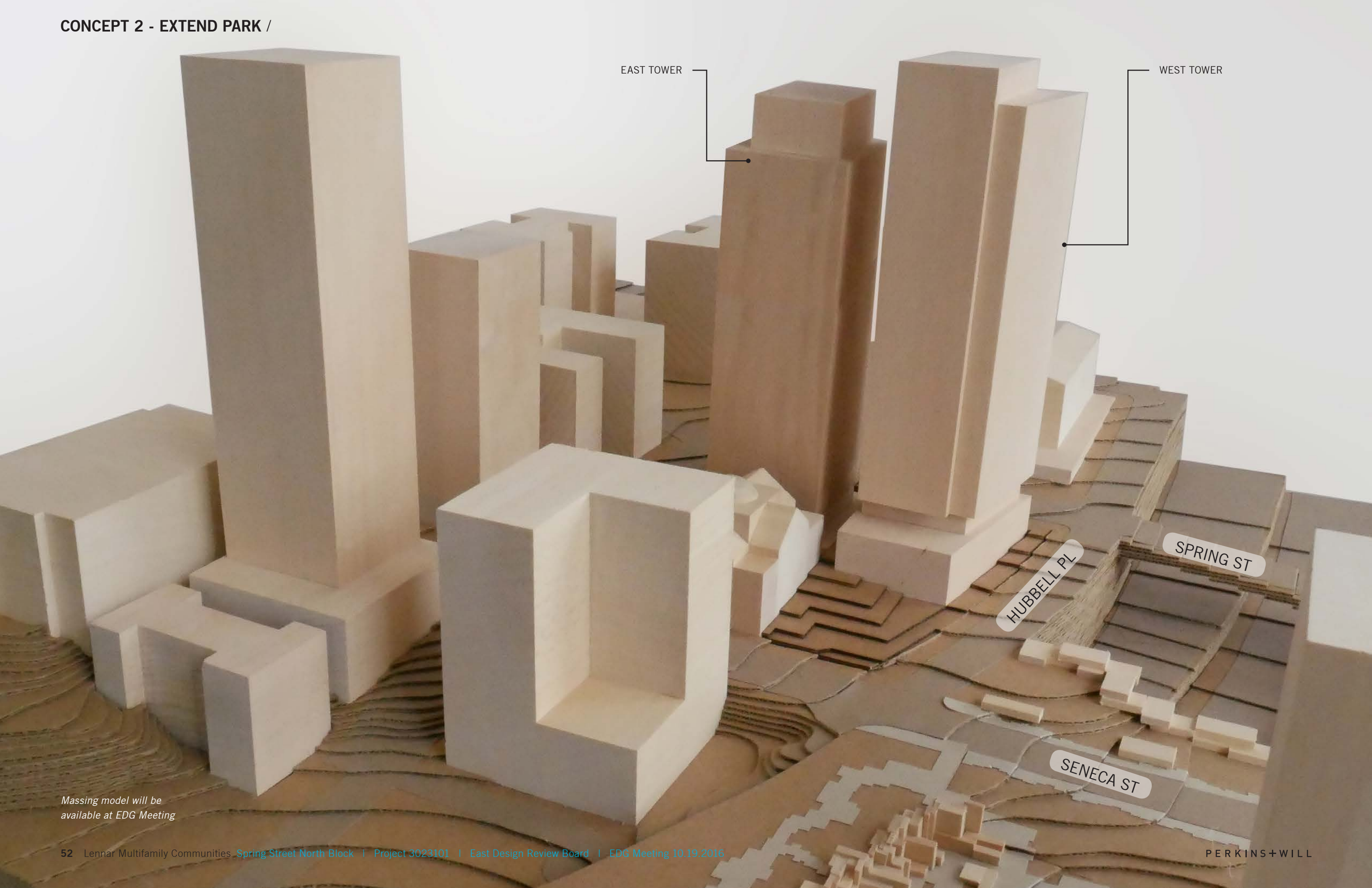
View from Seneca Street and Hubbell Place

ZONING ENVELOPE



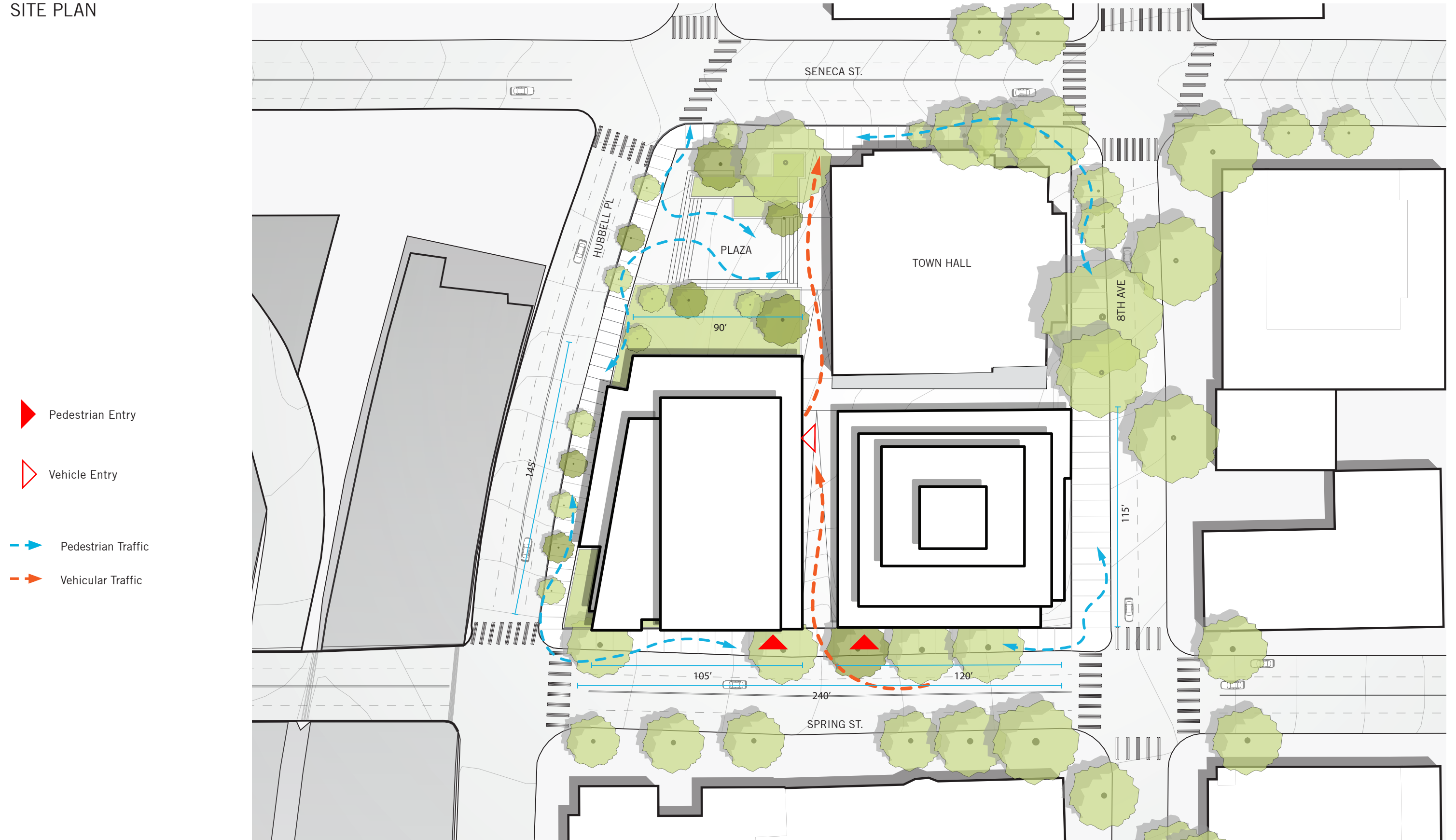
SETBACK AND SEPARATE





Massing model will be available at EDG Meeting

SITE PLAN





View from 8th Avenue



View from Spring Street



View from Seneca Street and Hubbell Place



View from Spring Street and 7th Avenue

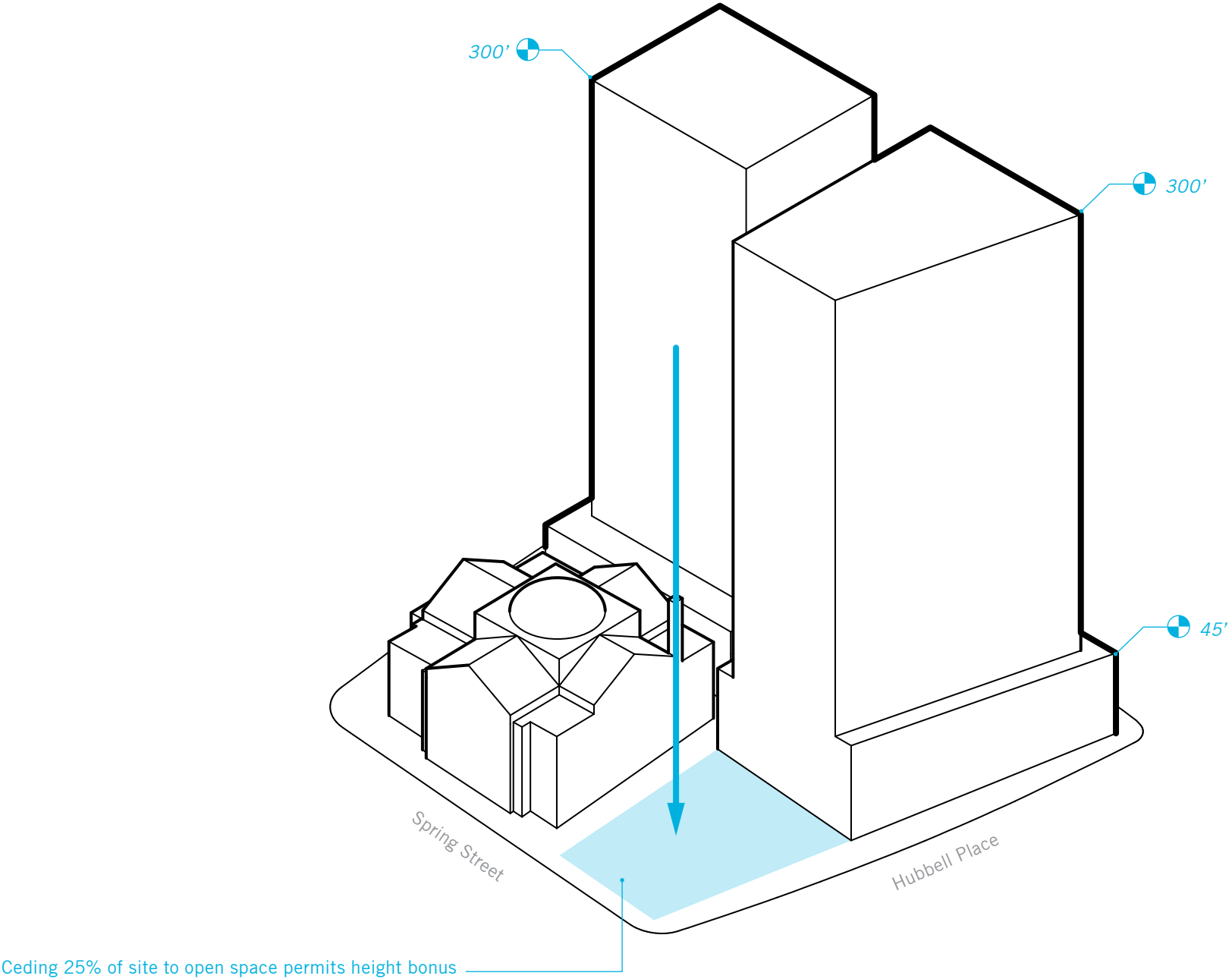


View from Seneca Street

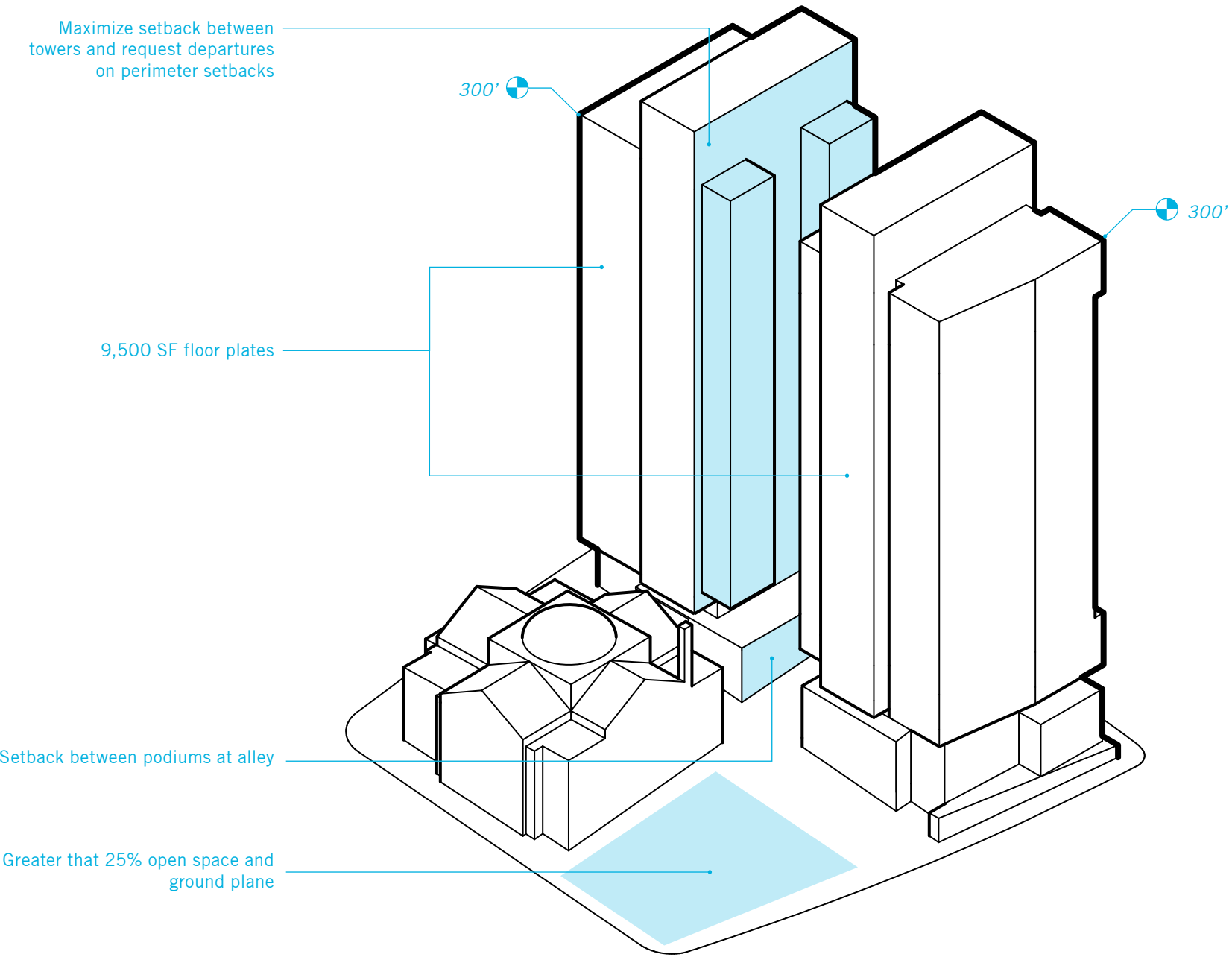


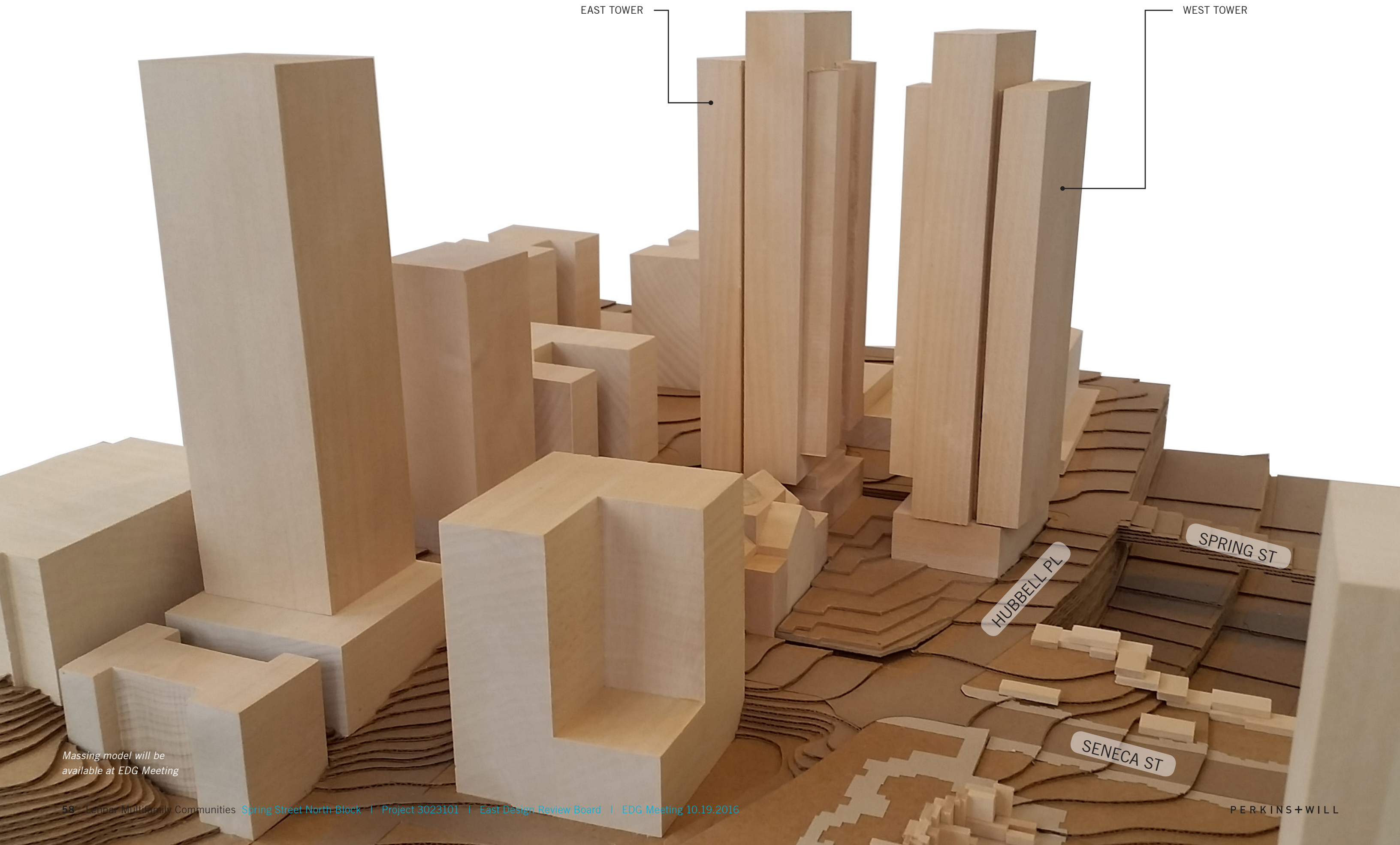
View from Seneca Street and Hubbell Place

ZONING ENVELOPE







SETBACK AND SEPARATE

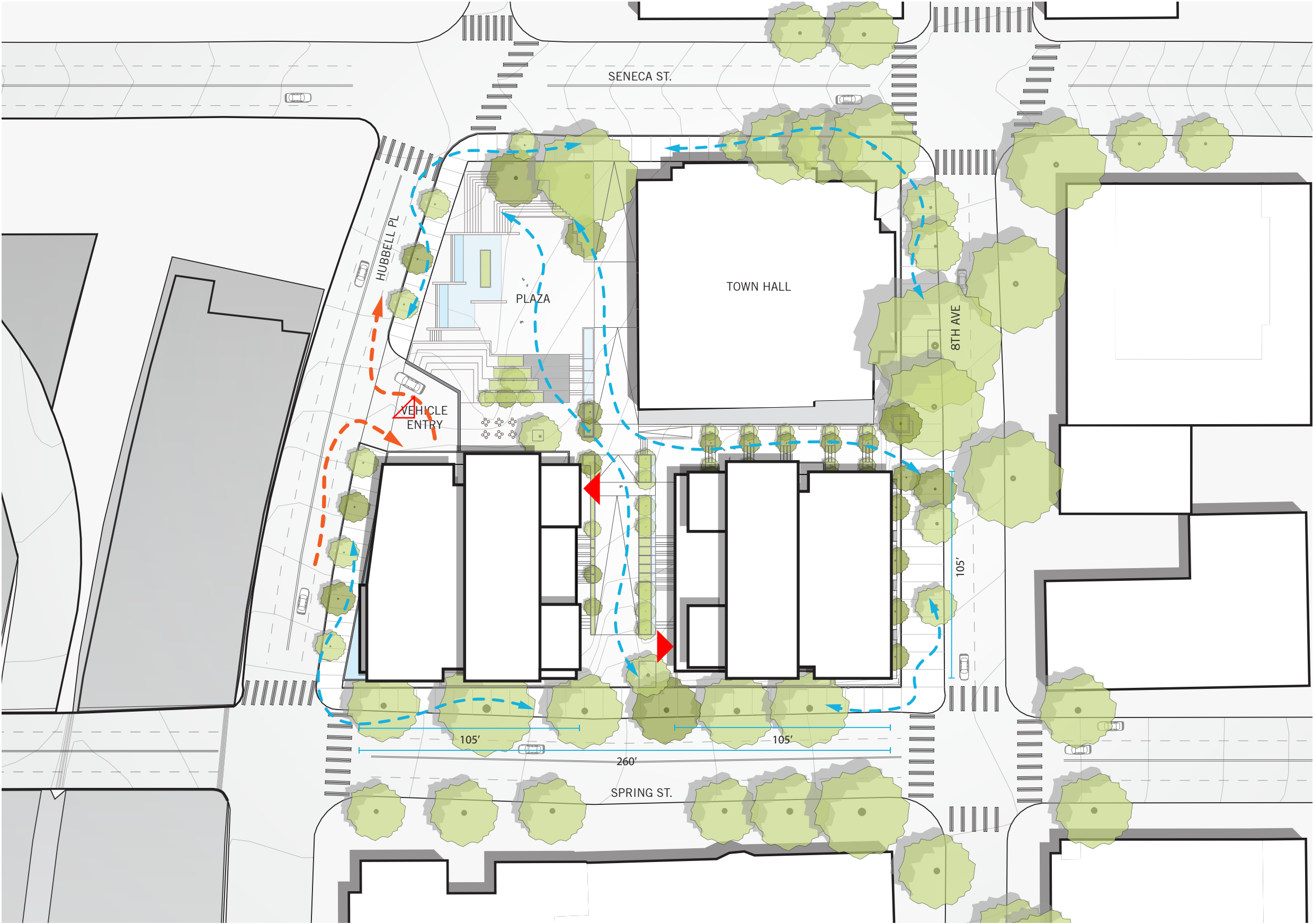




Massing model will be available at EDG Meeting

SITE PLAN

-  Pedestrian Entry
-  Vehicle Entry
-  Pedestrian Traffic
-  Vehicular Traffic





View from 8th Avenue



View from Spring Street



View from Seneca Street and Hubbell Place



View from Spring Street and 7th Avenue

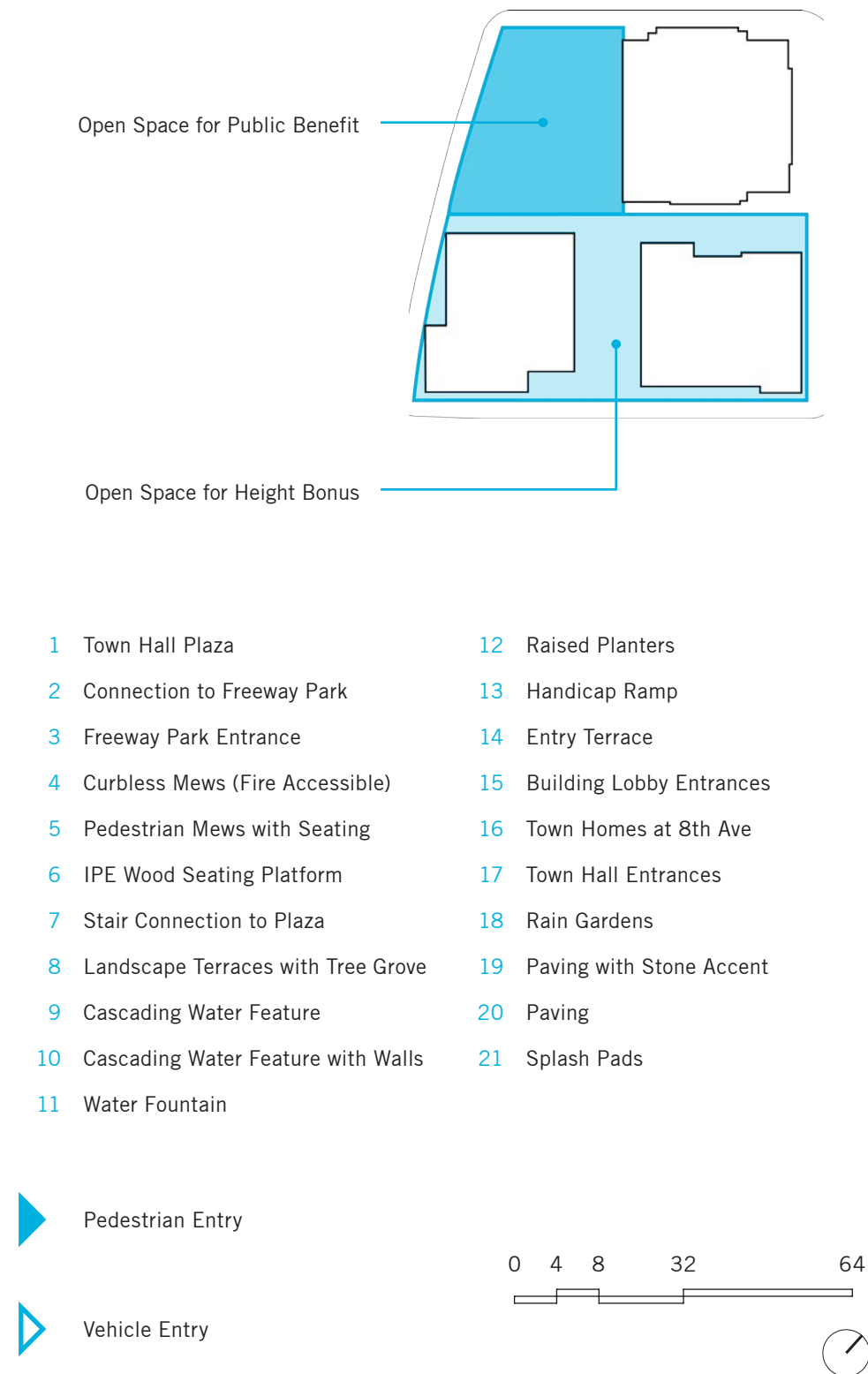


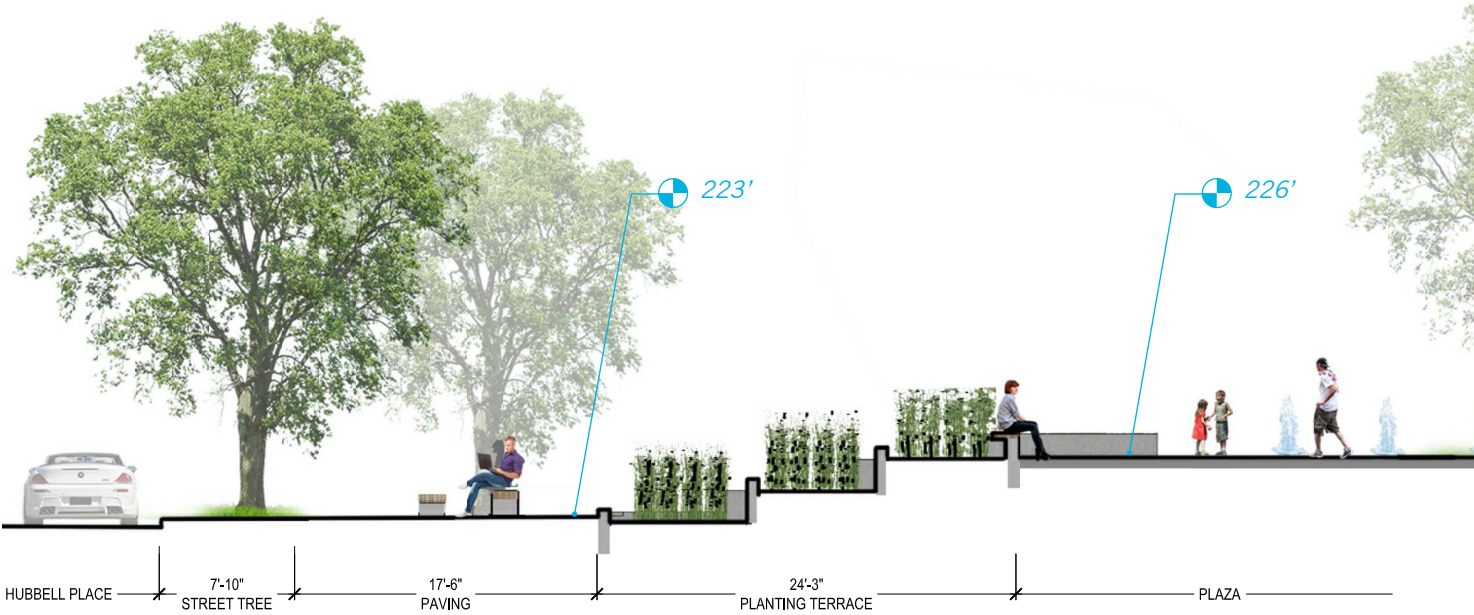
View from Seneca Street



View from Seneca Street and Hubbell Place

LANDSCAPE PLAN

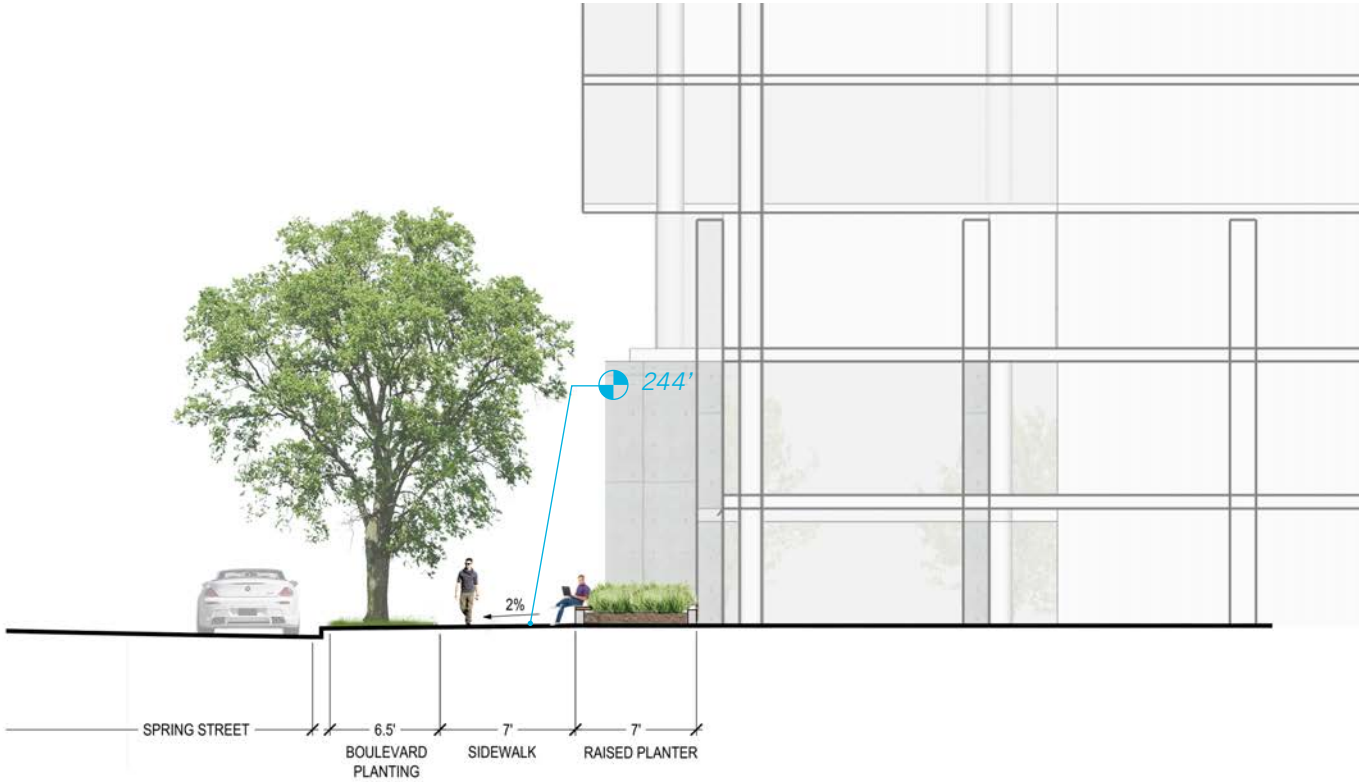




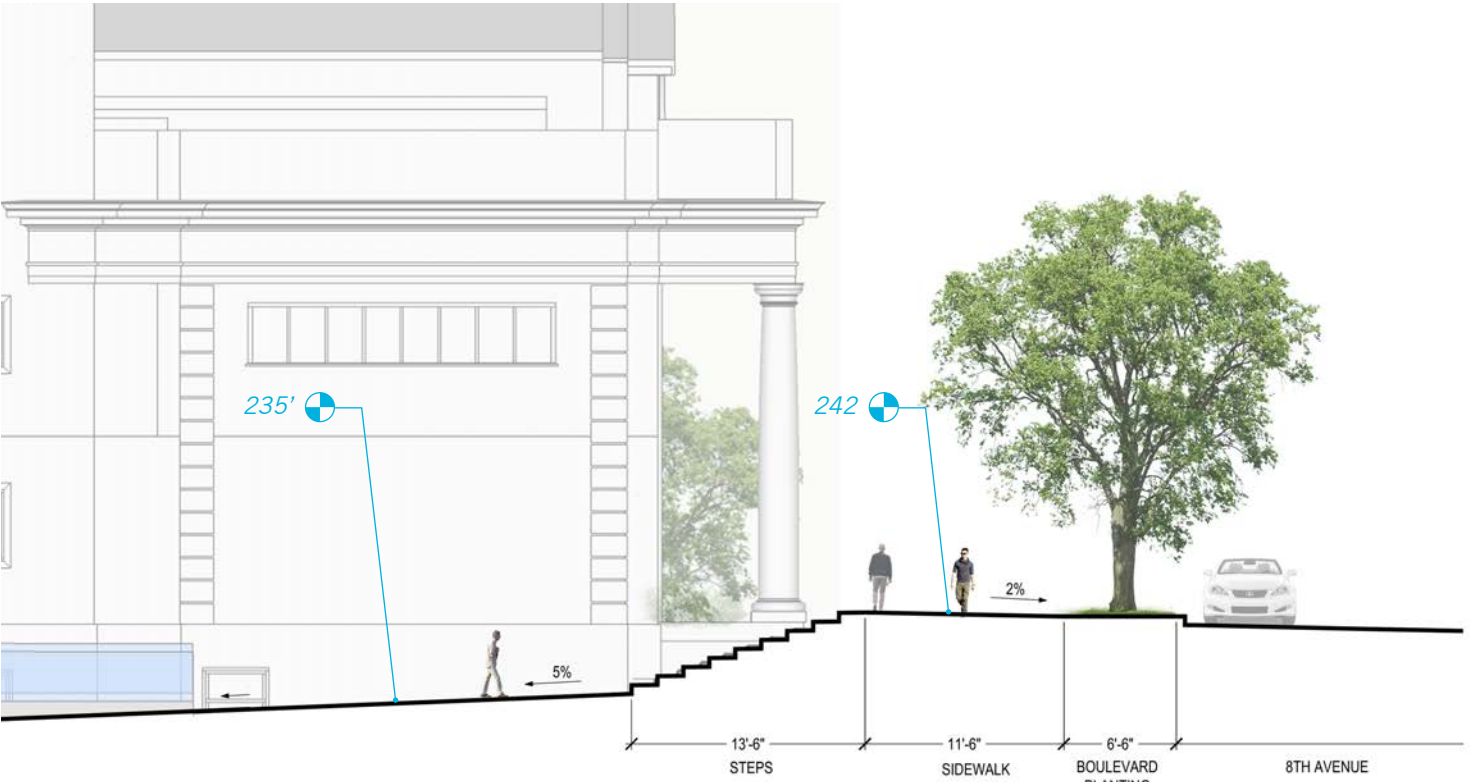
A - PLANTING TERRACE STEPS



B - 7TH AVENUE PLAZA STEPS



C - SENECA STREET PLANTERS



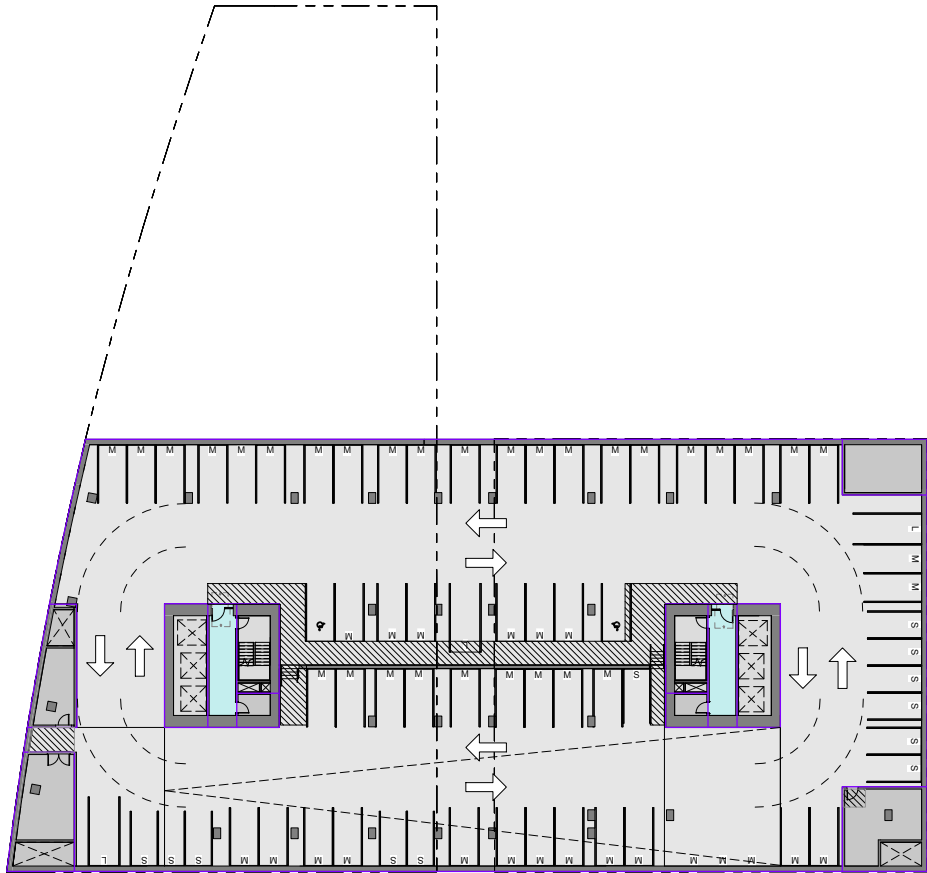
D - 8TH AVENUE BLOCK ACCESS



Concept imagery indicate aesthetic characteristics of the proposed design and are not intended to be proprietary specifications and do not preclude alternate designs with similar aesthetic and performance characteristics.



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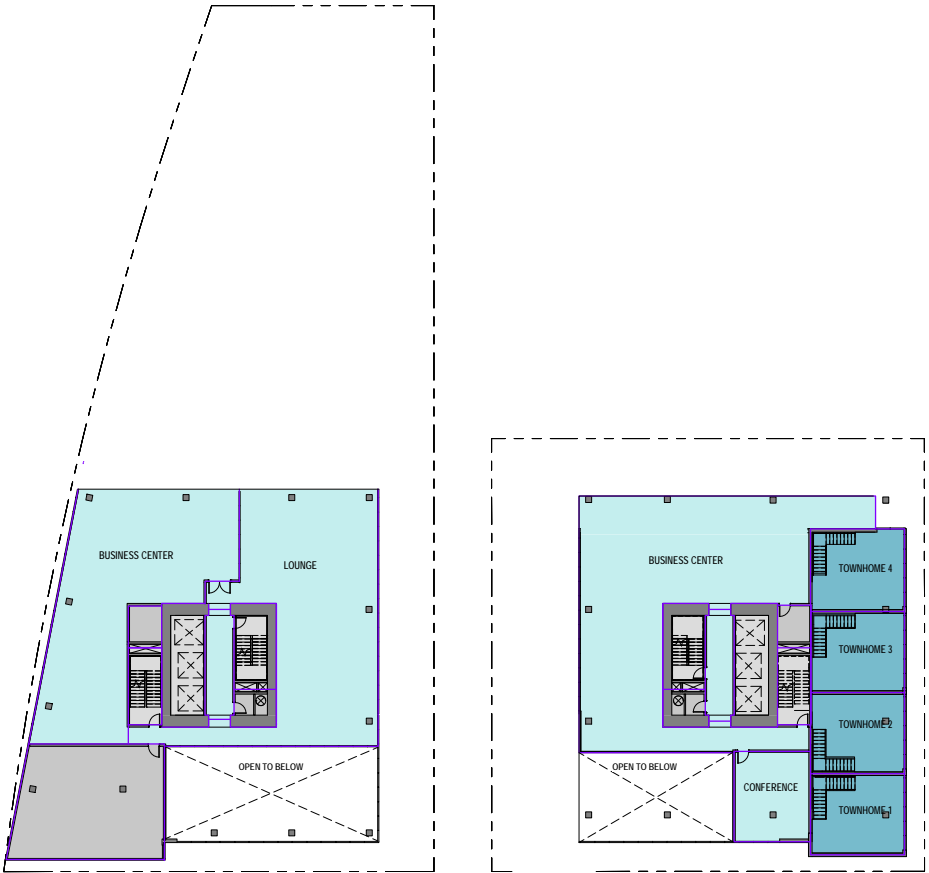
CONCEPTUAL LEVEL P4



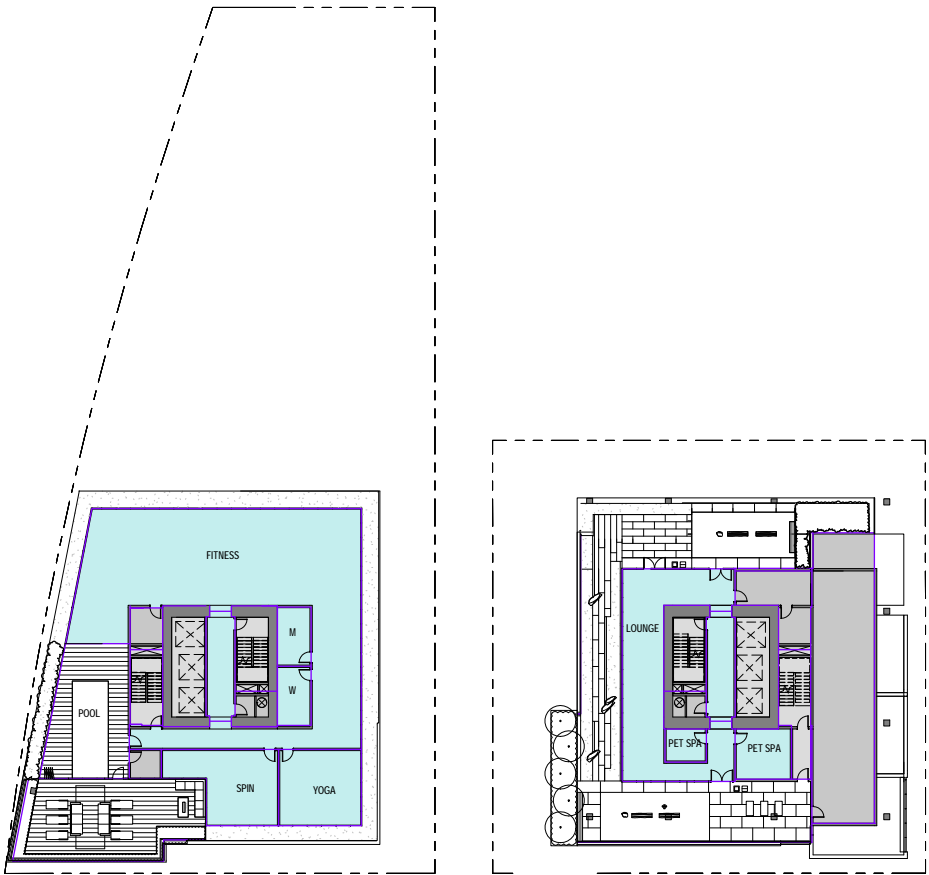
CONCEPTUAL LEVEL 01



CONCEPTUAL LEVEL 02



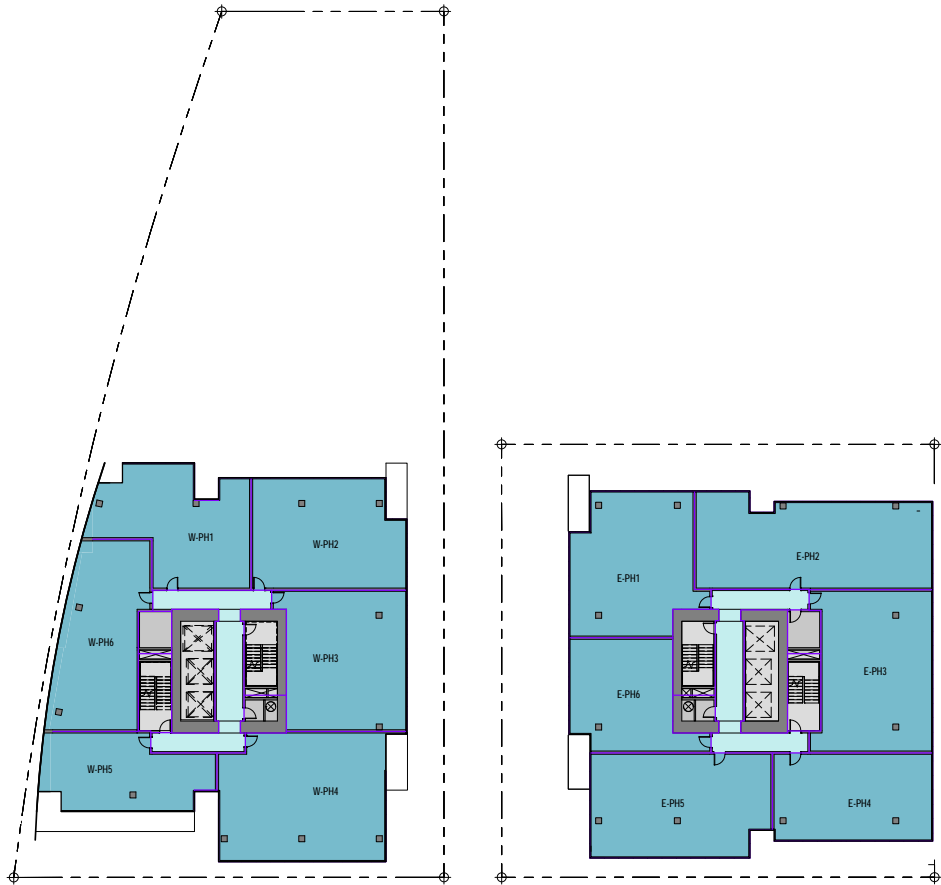
CONCEPTUAL LEVEL 03



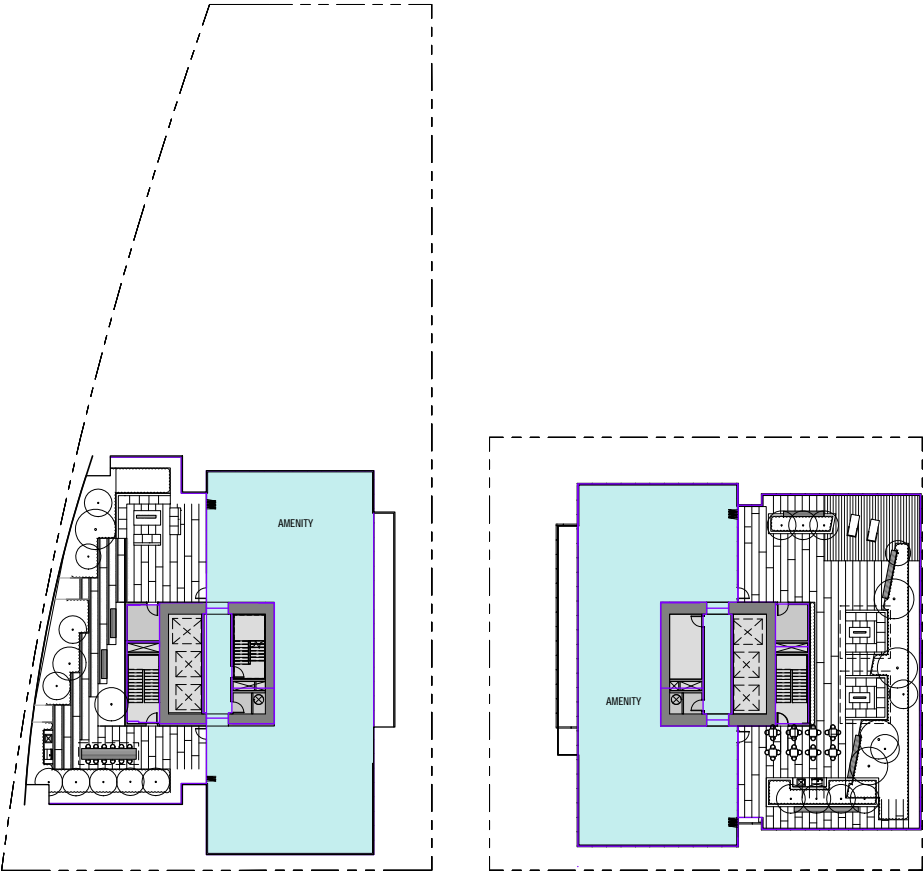
CONCEPTUAL LEVEL 04



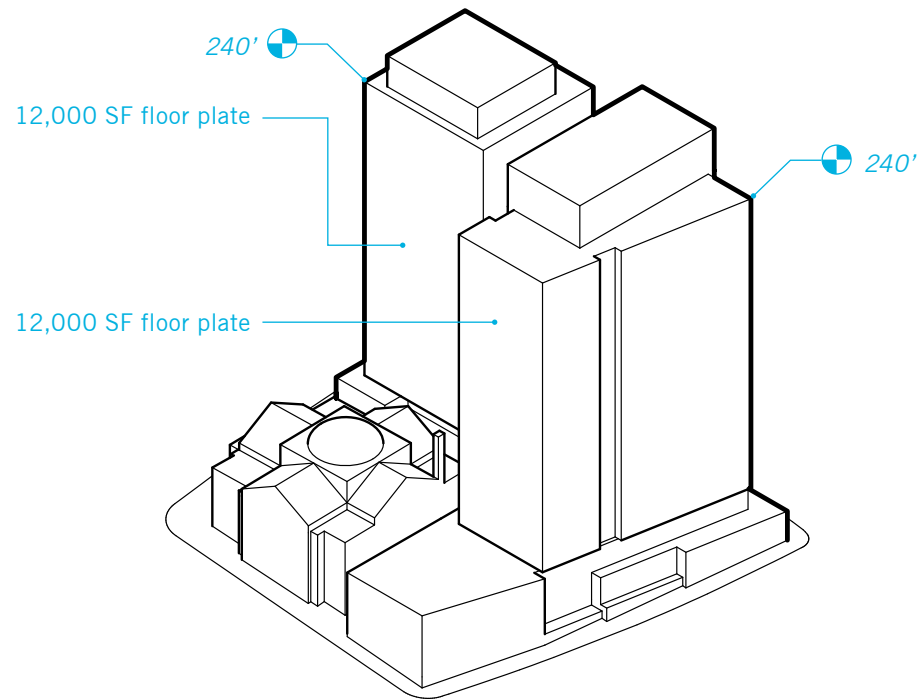
CONCEPTUAL RESIDENTIAL LEVELS



CONCEPTUAL PENTHOUSE LEVELS



CONCEPTUAL AMENITY
AND TERRACE LEVEL



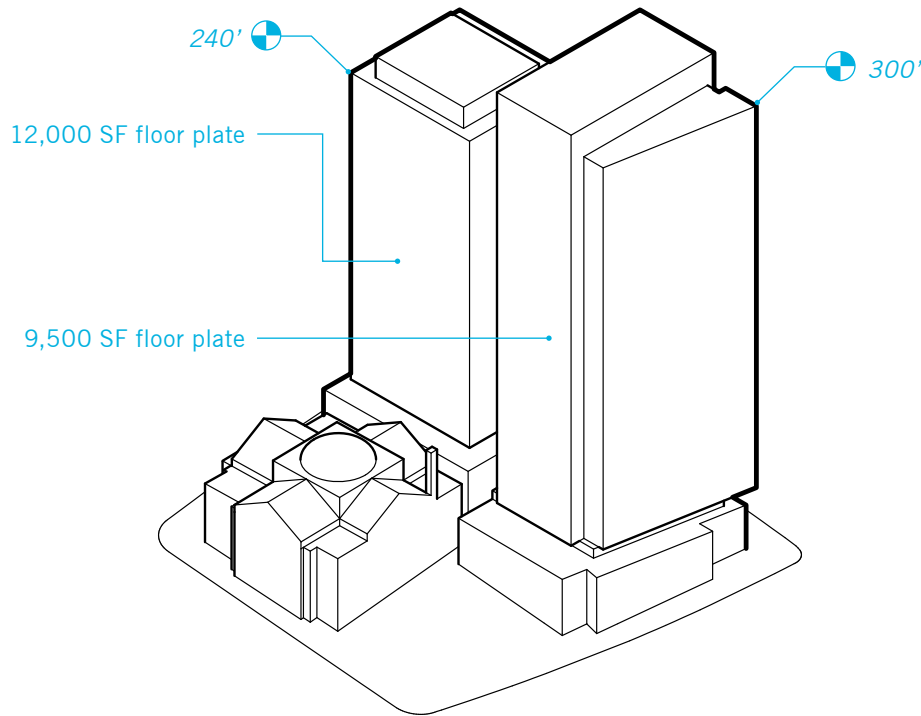
CONCEPT 1 - ANCHOR CORNER

Scheme Benefits:

- Adheres to zoning requirements
- Least Expensive to construct
- Most efficient tower plan (12,000 sf)

Scheme Disadvantages:

- Minimizes views of Town Hall
- Minimum tower separation
- Minimizes open space
- Shorter towers with difficult proportions
- Minimizes light and air to Town Hall
- Minimizes internal pedestrian site circulation



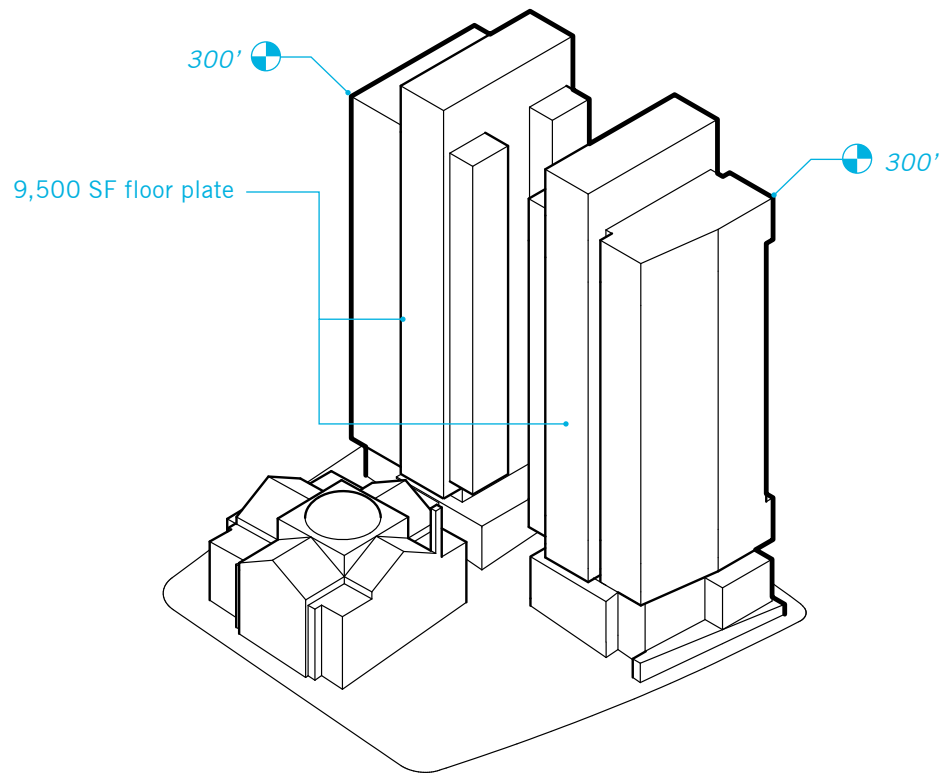
CONCEPT 2 - EXTEND PARK

Scheme Benefits:

- Adheres to zoning requirements
- Allows more open space at ground level

Scheme Disadvantages:

- Reduces views of Town Hall
- Minimum tower separation
- Densely programed podium levels
- Minimizes light and air to Town Hall
- Minimizes internal pedestrian site circulation



CONCEPT 3 - PREFERRED

Scheme Benefits:

- Maximizes views of Town Hall
- Maximum tower separation
- Maximizes open space
- Taller towers with slim proportions
- Maximizes light and air to Town Hall
- Maximizes internal pedestrian site circulation
- Maximized connection between First Hill and the CBD

Scheme Disadvantages:

- Requires zoning setback departure
- Most Expensive to construct
- Least efficient tower plan (9,500 sf)



CONCEPT 1 - ANCHOR CORNER



CONCEPT 2 - EXTEND PARK



CONCEPT 3 - PREFERRED

MARCH 21

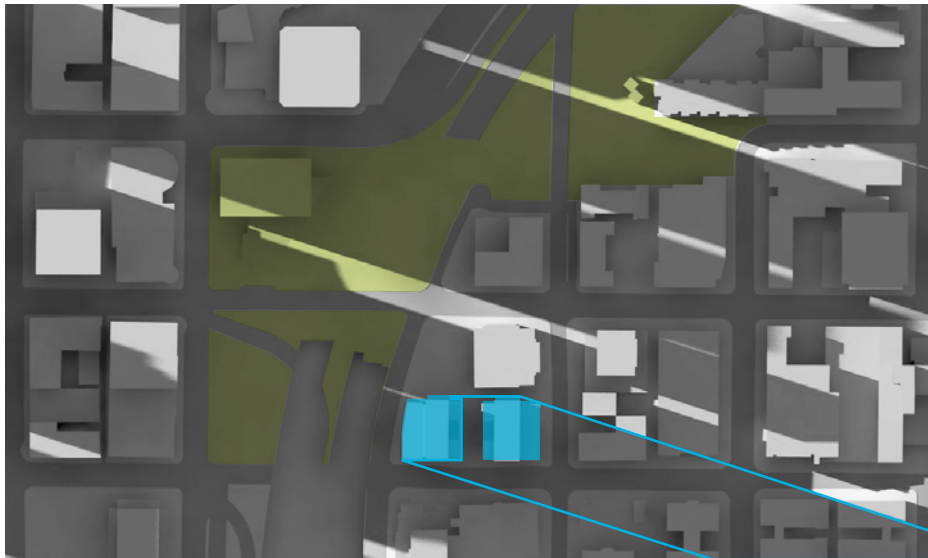
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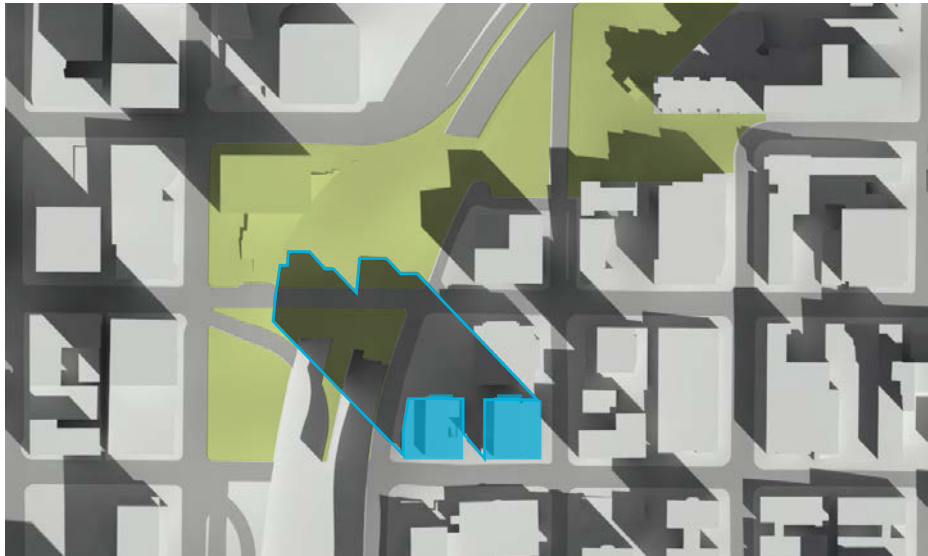


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JUNE 21

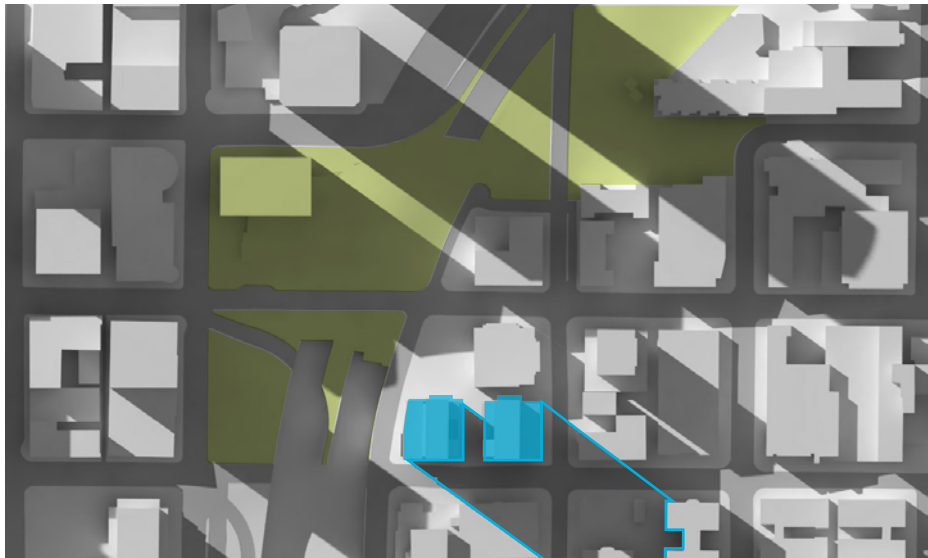
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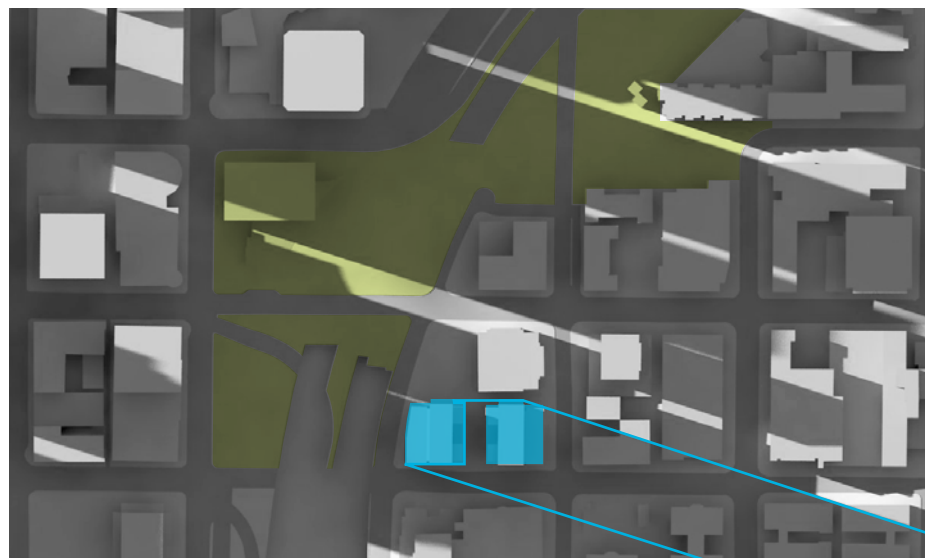
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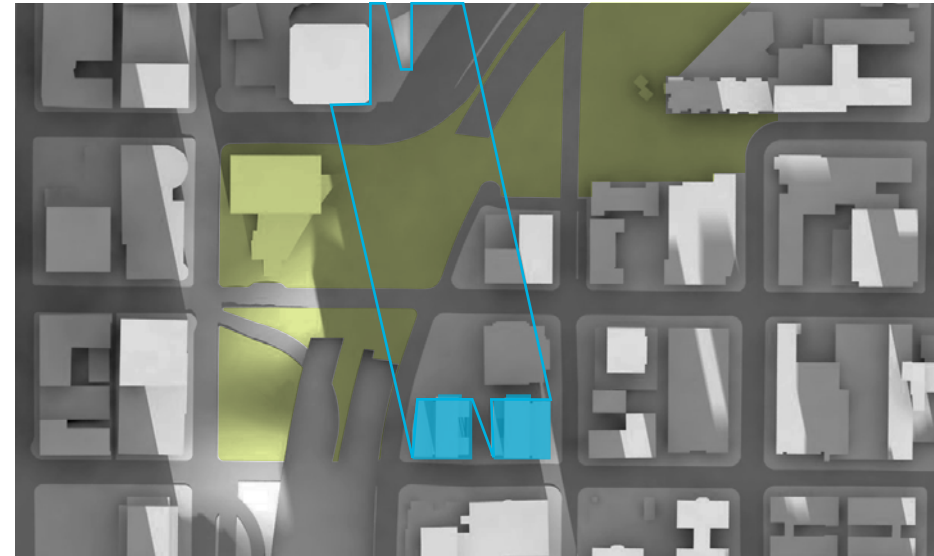


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DECEMBER 21

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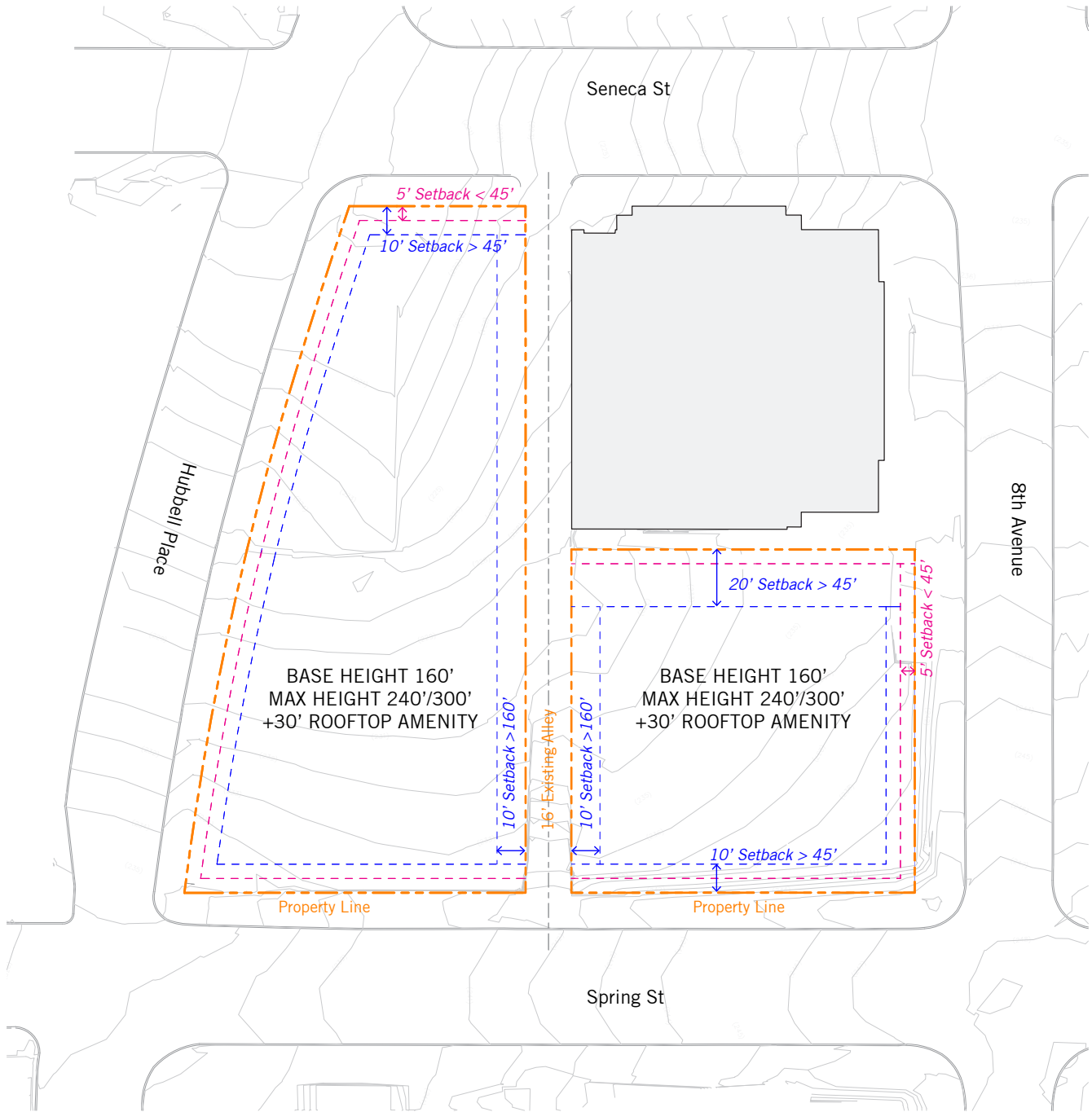
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SECTION 09

DEPARTURE

Code Citation & Requirement	Proposed Departure	Rationale
<p>SMC 23.49.056.B</p> <p>FACADE SETBACK LIMITS:</p> <p>1. Setback limits for property line facades. The following setback limits apply to all streets designated on Map 1H as requiring property line facades, except as specified in subsection 23.49.056.B.1.d.</p> <p>b. Structures greater than 15 feet in height are governed by the following criteria:</p> <p>1) No setback limits apply up to an elevation of 15 feet above sidewalk grade.</p> <p>2) Between the elevations of 15 and 35 feet above sidewalk grade, the facade shall be located within 2 feet of the street lot line, except that:</p> <p>a) Any exterior public open space that satisfies the Downtown Amenity Standards, whether it receives a bonus or not, and any outdoor common recreation area required for residential uses, is not considered part of the setback.</p> <p>b) Setbacks between the elevations of 15 and 35 feet above sidewalk grade at the street lot line are permitted according to the following standards, as depicted in Exhibit B for 23.49.056:</p> <p>i. The maximum setback is 10 feet.</p> <p>ii. The total area of a facade that is set back more than 2 feet from the street lot line shall not exceed 40 percent of the total facade area between the elevations of 15 and 35 feet.</p> <p>iii. No setback deeper than 2 feet shall be wider than 20 feet, measured parallel to the street lot line.</p> <p>iv. The facade of the structure shall return to within 2 feet of the street lot line between each setback area for a minimum of 10 feet. Balcony railings and other non-structural features or walls are not considered the facade of the structure.</p>	<p>Request departure to allow decreasing the setback set forth by code along Spring Street, Hubell Place and 8th Avenue.</p> <p>Departed setback for the Hubell Place facade is proposed to sit at the property line with facade modulation accounting for minor areas of 5' 0" setbacks. This facade amounts to 102' 0" or 45% of the length of the Hubell Place property line. This setback would create a 12' 0" wide sidewalk.</p> <p>Departed setback for the Spring Street facades is proposed as follows:</p> <p>One facade belonging to the west tower, measuring 110' 0", or 43% of the Spring Street property line would sit at the property line with facade modulation accounting for minor areas of 5' 0" setbacks. Another facade belonging to the east tower, measuring 101' 0", or 40% of the Spring Street property line would sit at the property line with facade modulation accounting for minor areas of 5' 0" setbacks. Separating these facades would be an entry corridor measuring 44' 0", or 17% of the property line, located at the vacated alleyway at the center of the block. These setbacks would create a 12'-0" wide sidewalk.</p> <p>Departed setback for the 8th Avenue facade is proposed to sit at the property line. This facade amounts to 97' 0" or 81% of the length of the 8th Avenue property line. This setback would create a 15' 0" wide sidewalk.</p>	<p>Reducing setbacks at the perimeter of the site allows increased spacing between towers and returns public space removed, as a result of the project being located at the property line, to an expanded alley corridor. In addition the separation of the towers allows for more direct sunlight to reach the plaza on the North West corner of the site.</p> <p>The departure would provide for an additional 8' 0" in tower separation to enhance opportunities for community amenities.</p>



CODE PERMITTED



CODE DEPARTURE