

1804 E SPRUCE ST

SECOND EARLY DESIGN GUIDANCE MEETING

EARLY DESIGN GUIDANCE 212 18TH AVE - # 3023583
EARLY DESIGN GUIDANCE 208 18TH AVE - # 3023584
EARLY DESIGN GUIDANCE 1804 E SPRUCE ST - # 3023032



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- DESIGN OBJECTIVES:** 21 Units [Rowhouses & Townhouses]
21 Open parking stalls
0 SF Commercial
- Promote density in this central neighborhood within Seattle while providing parking for each household.
 - Contributes to vibrant community already established along 18th Ave and E Spruce St.

KEY METRICS

ADDRESS: 212 18th Ave, Seattle, Washington 98122
208 18th Ave, Seattle, Washington 98122
1804 E Spruce St, Seattle, Washington 98122

PROJECT NUMBERS: 212 18th Ave (Lot A) - #3023583
208 18th Ave (Lot B) - #3023584
1804 E Spruce St (Lot C) - #3023032

PARCEL NUMBER: 1934800090

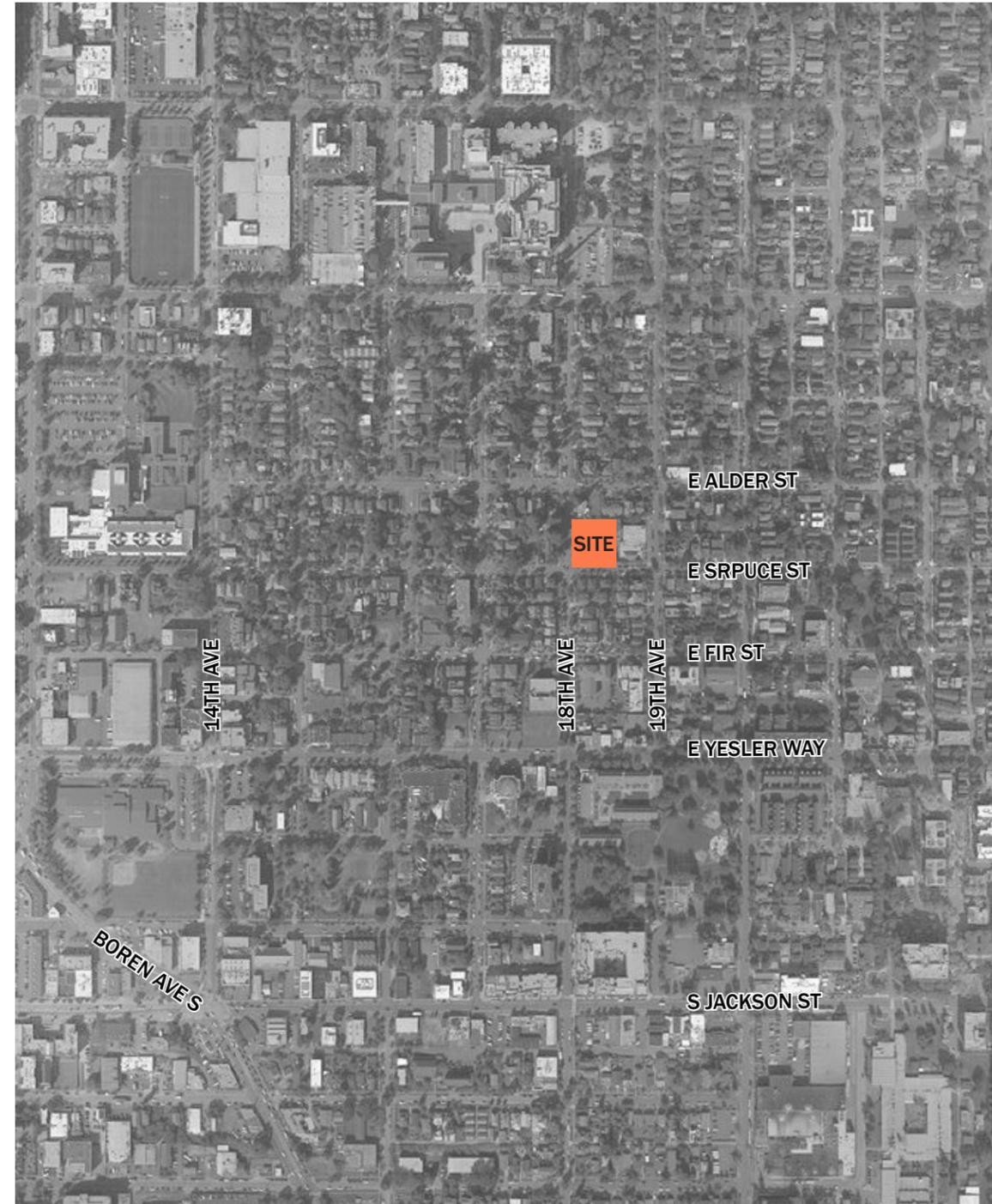
ZONE: LR 2

LOT SIZE (SCHEME 4): 212 18th Ave (Lot A) - 5,283 SF
208 18th Ave (Lot B) - 7,120 SF
1804 E Spruce (Lot C) - 6,797 SF

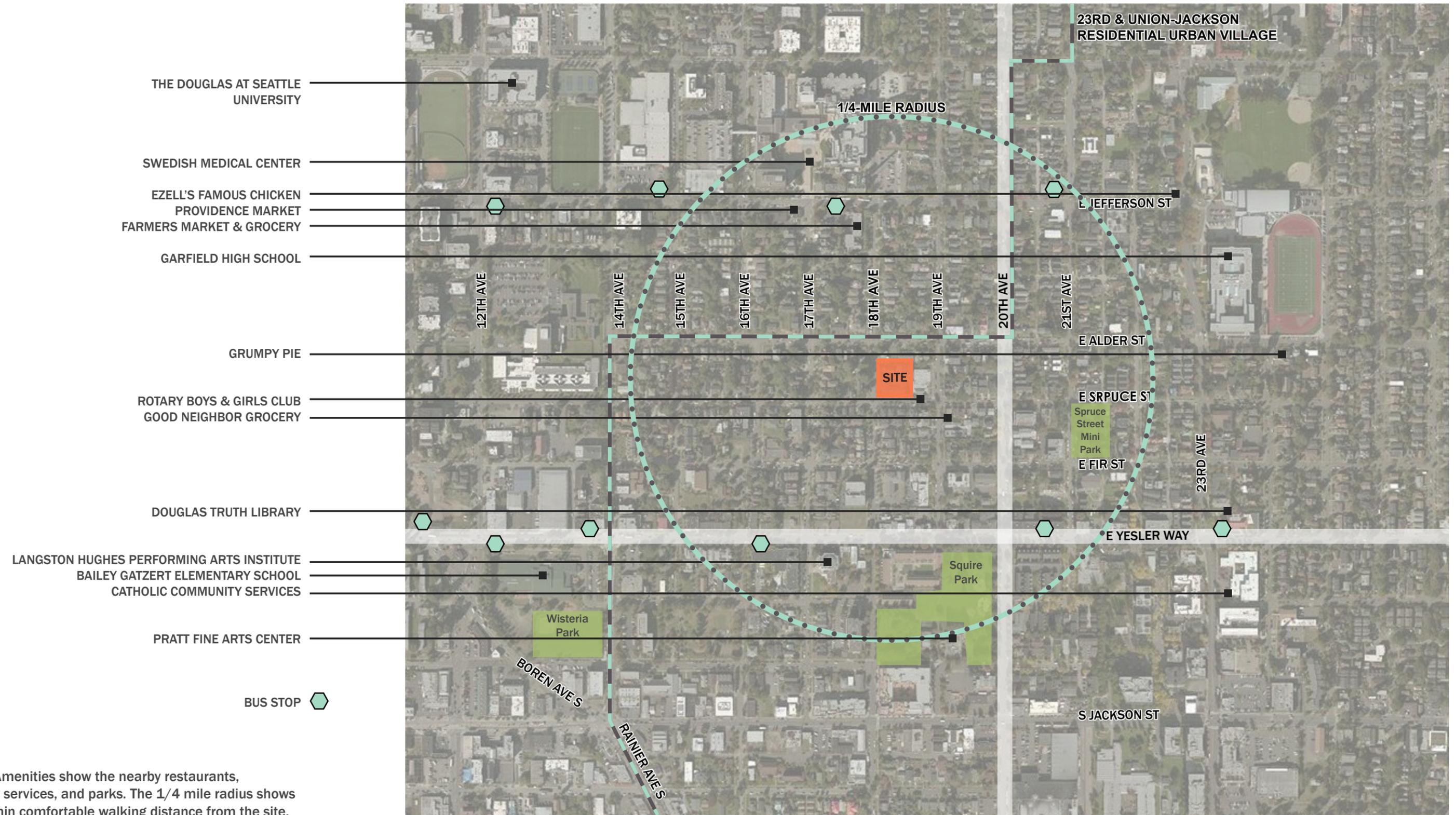
OVERLAYS: 23rd & Union-Jackson (Residential Urban Village)

ANALYSIS OF CONTEXT: This project is located on the corner of 18th Ave and E Spruce St in the 23rd & Union-Jackson Residential Urban Village in the Central District. The site has good access to transit and major arterials and sits in a residential neighborhood that is growing with more housing and businesses.

The project site is zoned LR2. The neighboring zones are SF-5000 and LR2. Just (2) blocks away is NC1-40 with businesses along E Yesler Way.



SITE ANALYSIS



The Local Amenities show the nearby restaurants, community services, and parks. The 1/4 mile radius shows what is within comfortable walking distance from the site.

LOCAL AMENITIES

- SF 5000
- LR 2
- LR 3
- NC1-40



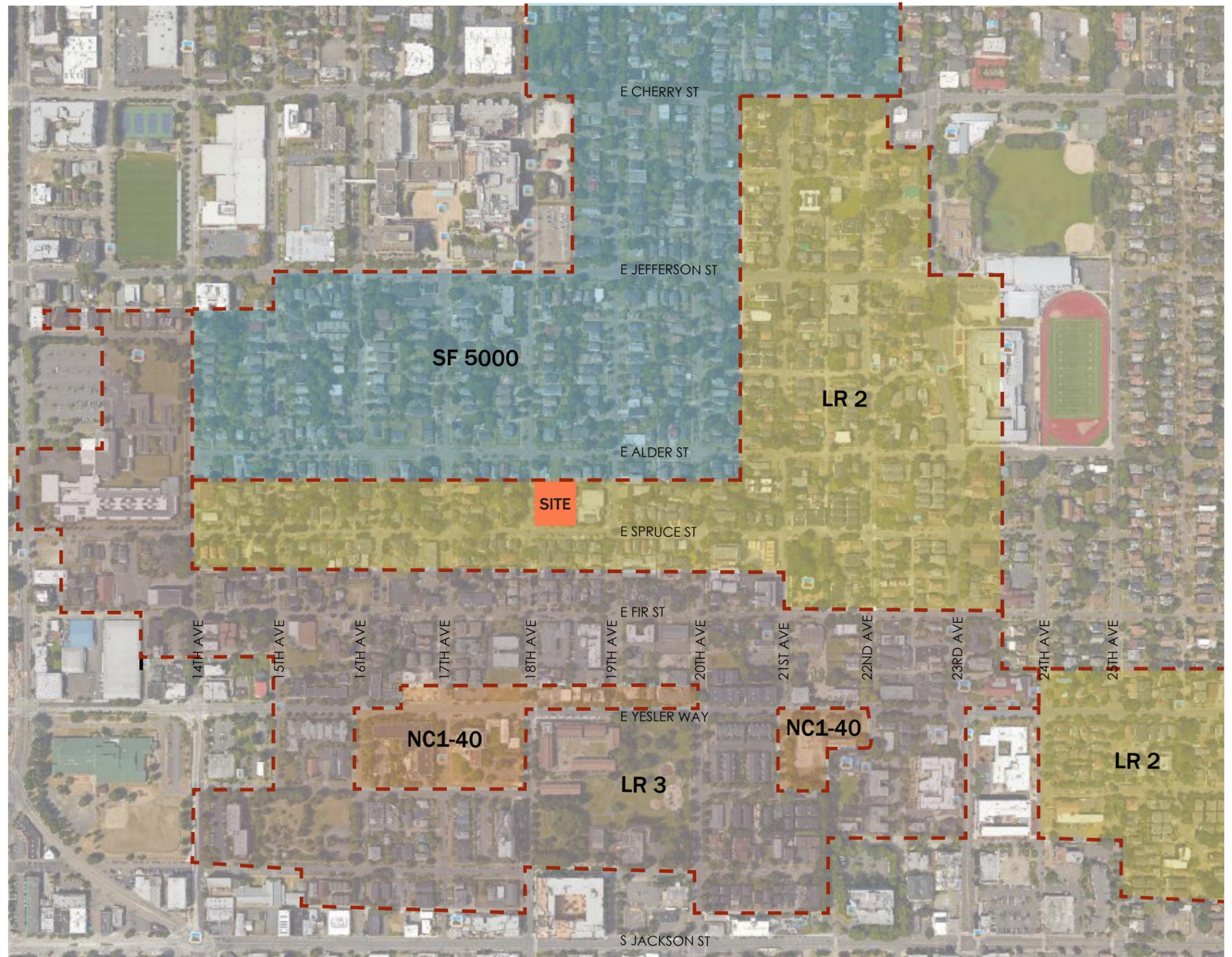
AT SITE LOOKING NORTHWEST



AT SITE LOOKING NORTHEAST



AT SITE LOOKING SOUTHEAST



ZONING AERIAL DIAGRAM

- SF 5000
- LR 2
- LR 3
- NC1-40

SITE

E ALDER ST

E SPRUCE ST

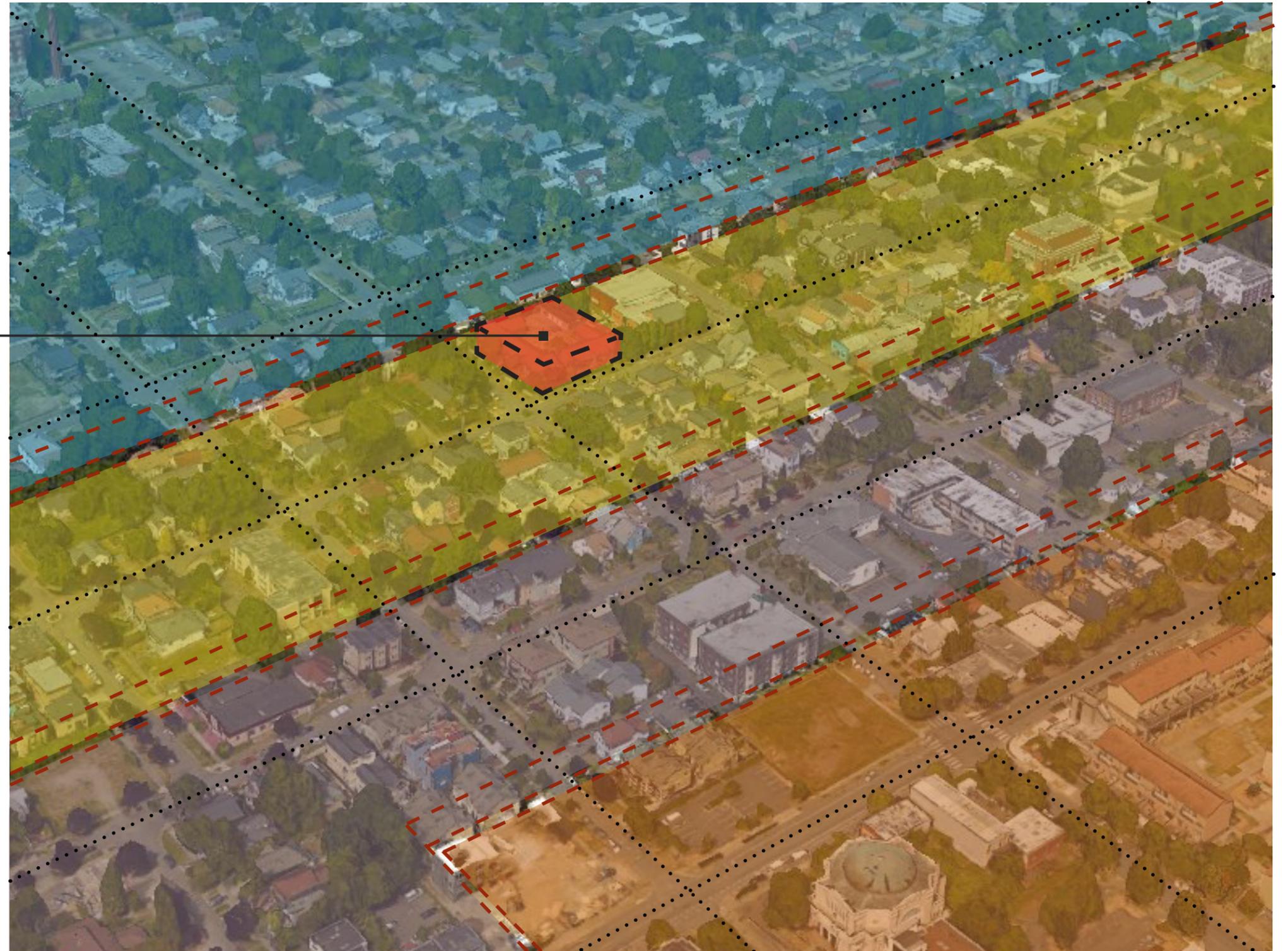
E FIR ST

E YESLER WAY

17TH AVE

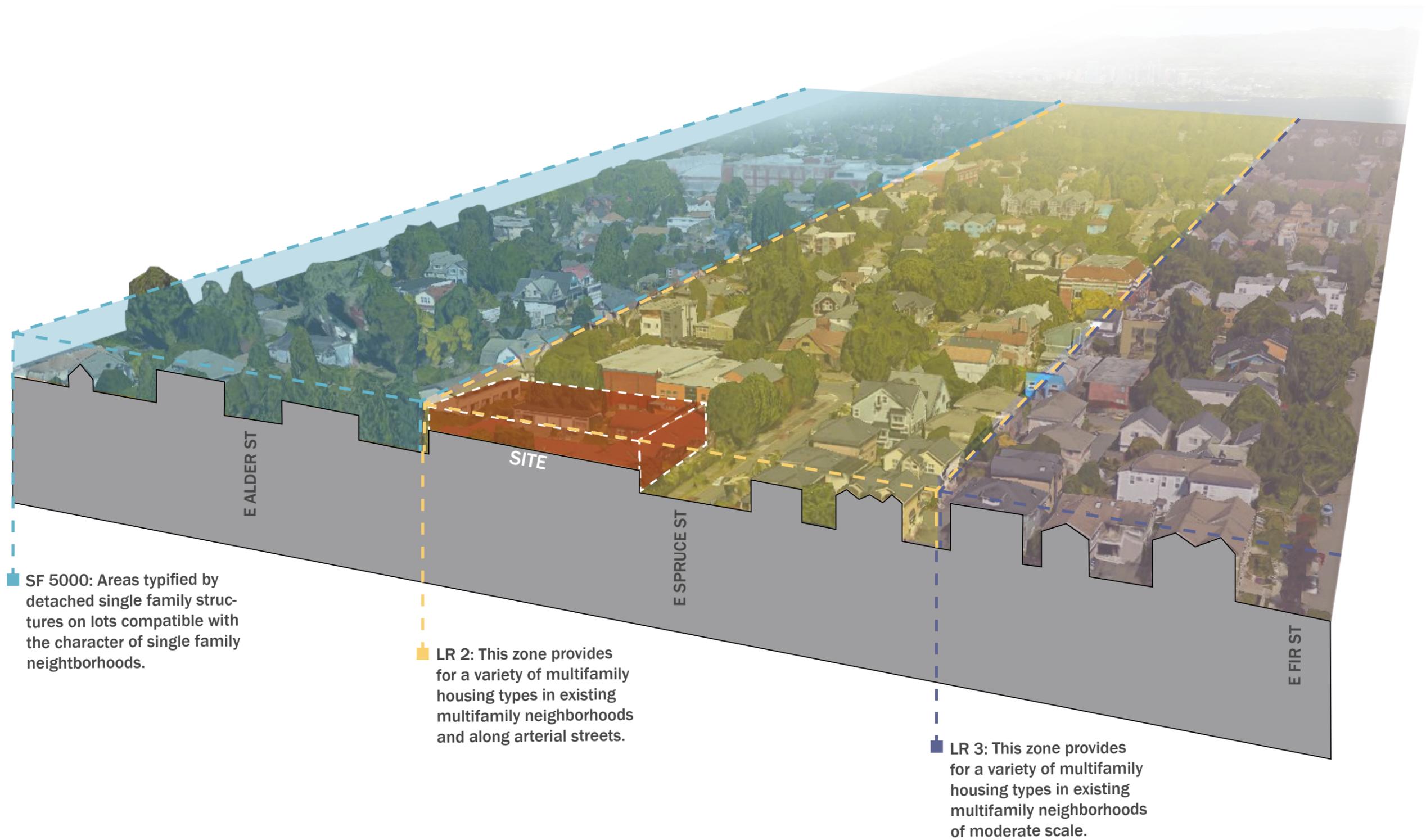
18TH AVE

The site is zoned LR2 and is surrounded by both LR3 and SF5000 with NC1-40 further to the south. Our project is responsive to its zone and surrounding zones with characteristics, such as entries, that respond to the SF homes to the North while promoting density that responds to the LR3 zone to the South.



ZONING MAP DIAGRAM

- SF 5000
- LR 2
- LR 3



ZONING SECTION DIAGRAM

A

1803 E ALDER ST
SF5000
1-STORY SFR
1,020 SF



E

1805 E SPRUCE ST
LR2
2-STORY SFR
2,000 SF



F

1801 E SPRUCE ST
LR2
2-STORY SFR
1,510 SF



G

207 18TH AVE
LR2
25-UNIT
APARTMENT
#3019262



H

213 18TH AVE
LR2
3-story (3) unit
townhouses under
SDR (#3022629)
behind 3-story
(2) unit
rowhouses under
separate permit
(#6502116)



B

1815 E ALDER ST
SF5000
2-STORY TRIPLEX
3,570 SF



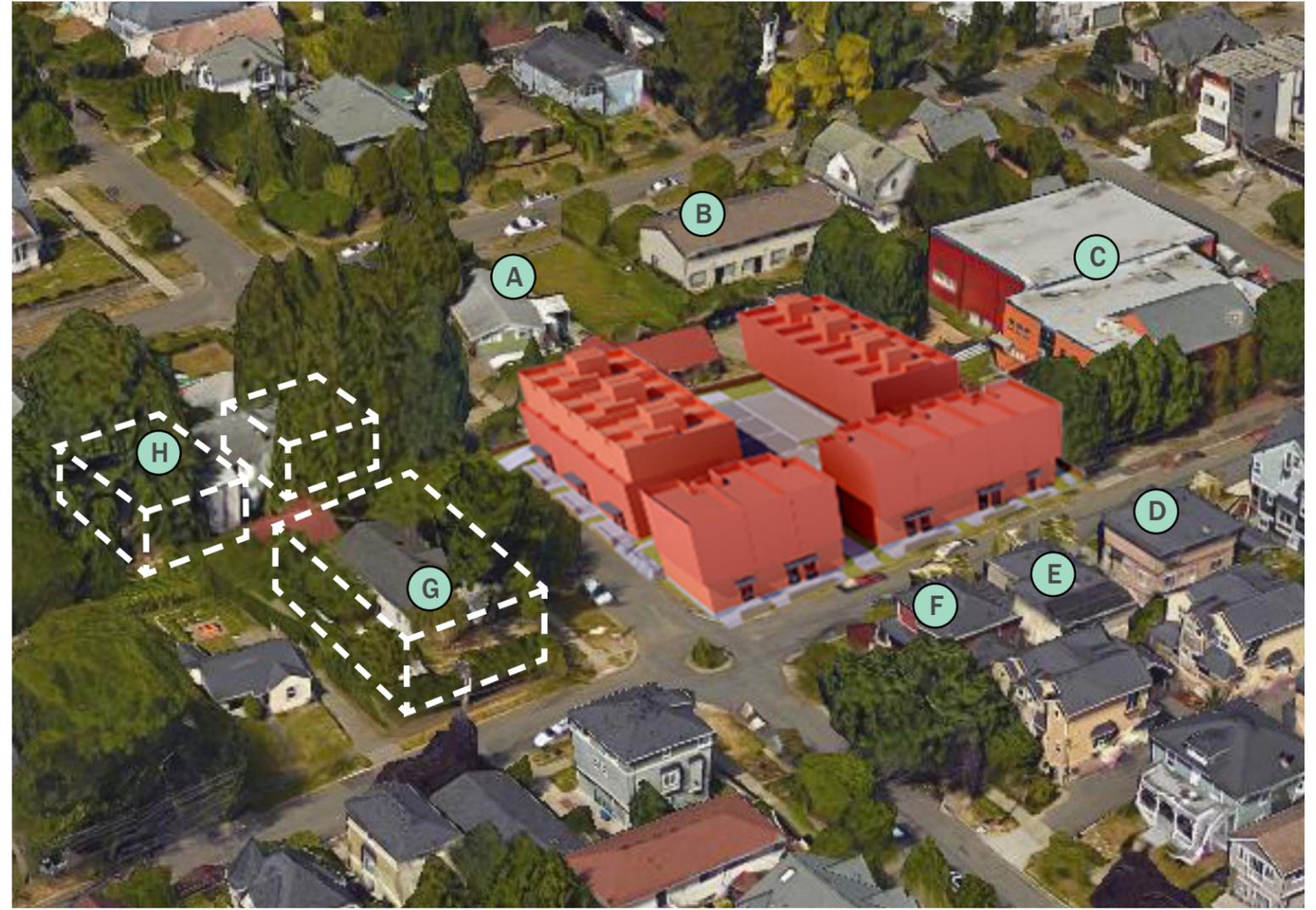
C

201 19TH AVE
LR2
2-STORY BOYS
AND GIRLS CLUB
15,058 SF



D

1811 E SPRUCE ST
LR2
3-STORY SFR
1,860 SF



NEIGHBORING CONTEXT

1

The Douglas
at Seattle
University

1223 E Cherry St



5

DEP City 15

16th and Fir



2

Swedish
Medical Center

500 17th Ave



6

Douglas Truth
Library

2300 E Yesler Wy



3

Rotary Boys and
Girls Club

201 19th Ave



7

Avant Live-Work
Townhomes

17th and Yesler



4

Townhomes

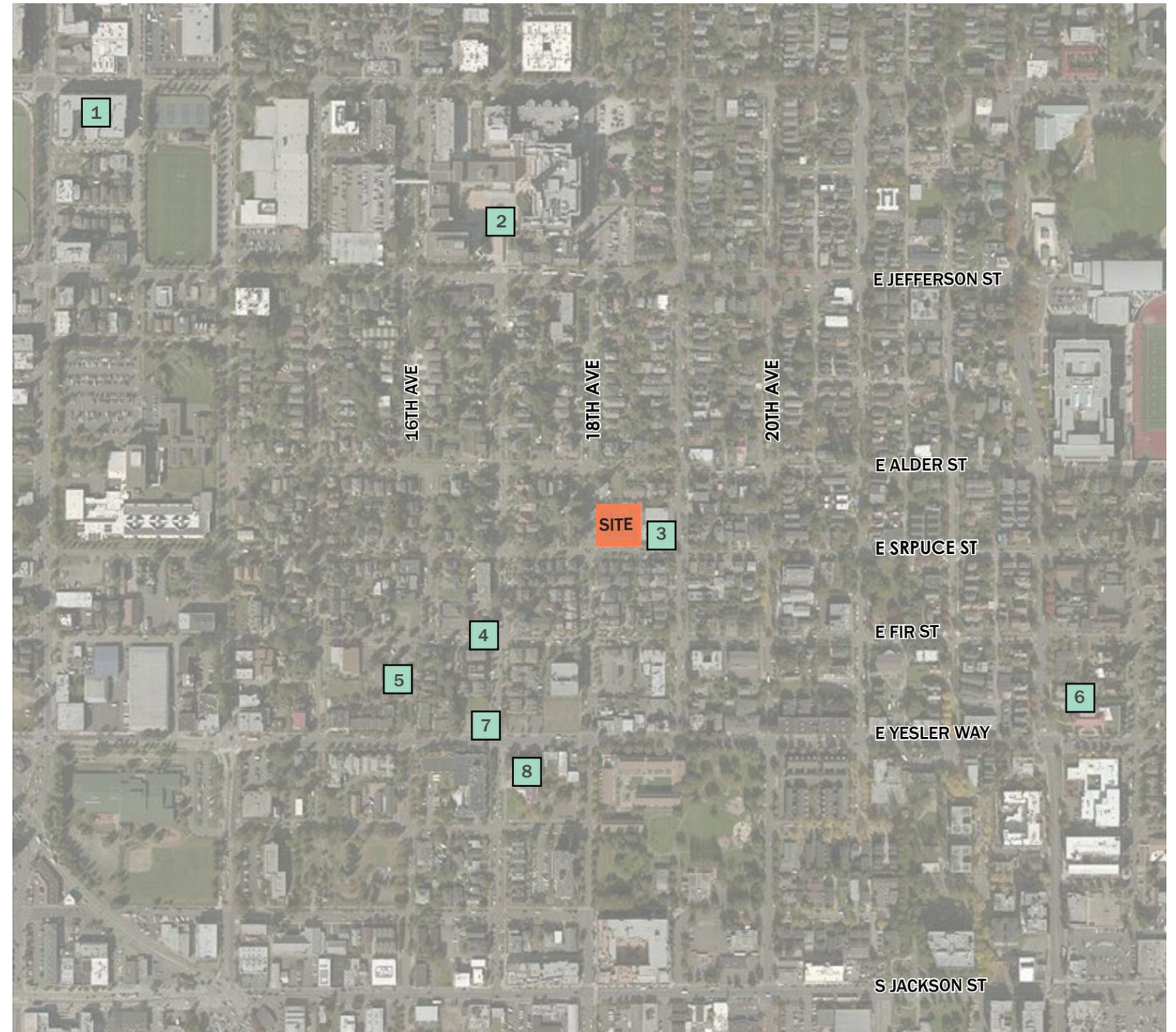
17th and Fir



8

Langston
Hughes
Performing Arts
Center

104 17th Ave S



This map indicates buildings in the neighborhood that have architectural influence on our project.

NEIGHBORHOOD CONTEXT

A

207 18th Ave

4-story (25) unit apartment.



E

114 16TH AVE

SDR - (1) 3-unit rowhouse structure and (1) 4-unit townhouse structure.



F

1622 E Yesler Way

7 4-story live-work units.



G

115 20TH AVE

SDR - (2) 3-unit townhouse structures with (6) units total.



H

120 20TH AVE

SDR - (2) 4-unit townhouse structures.



B

213 18TH AVE

SDR - 3-story (3) unit townhouses.



C

206 22ND AVE

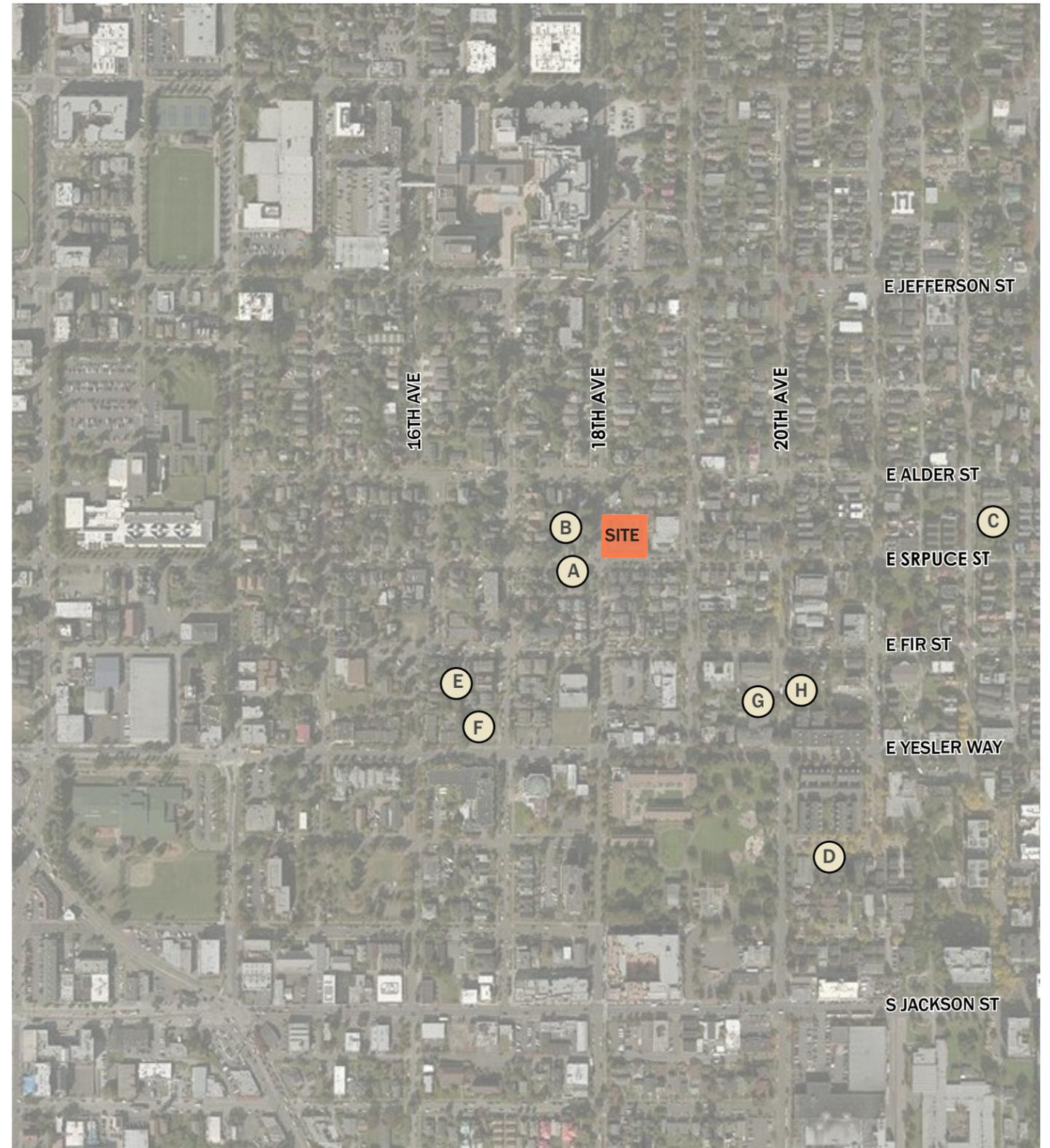
SDR - (2) 3-story townhouses structure with (3) units each, totaling (6) units.



D

2027 S Washington St

SDR - (2) 3-story 2-unit townhouse structures.



This map indicates projects in the neighborhood undergoing SDR or EDG.

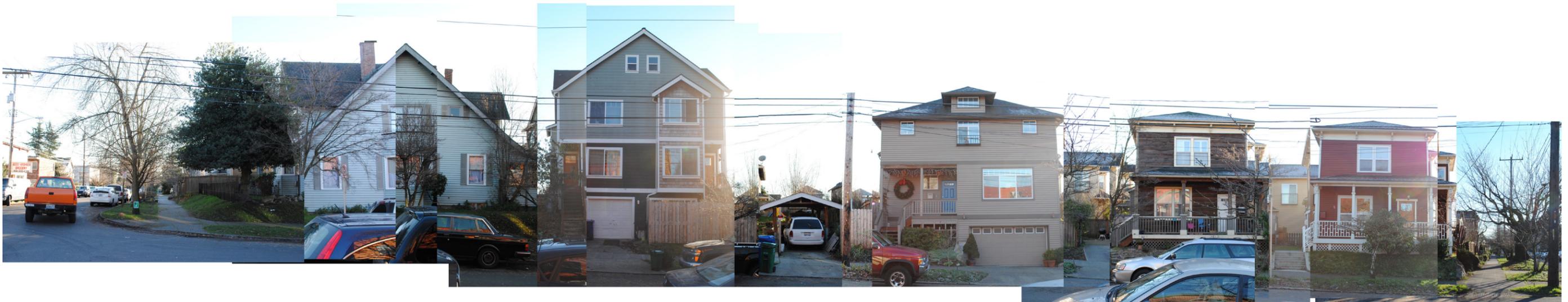
NEAR-BY DESIGN REVIEW PROJECTS



LR 2

THE SITE

E SPRUCE ST - NORTH FACING

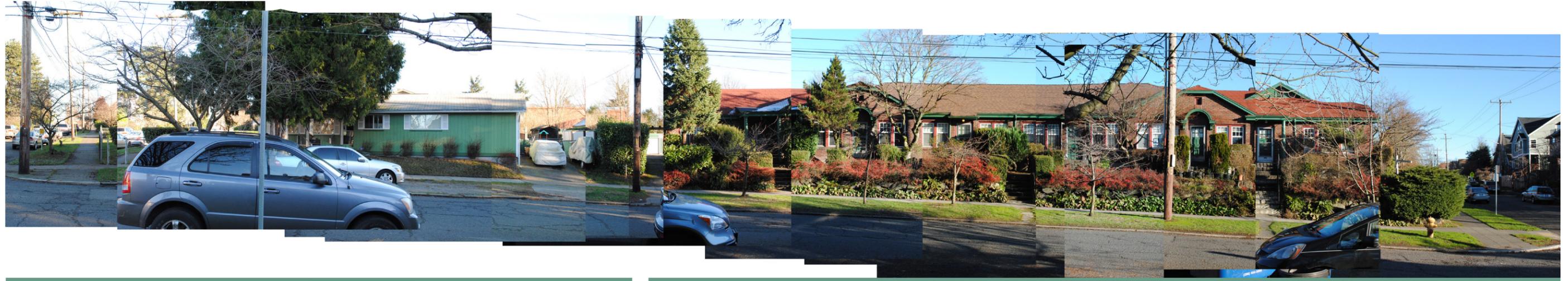


LR 2

ACROSS THE STREET

E SPRUCE ST - SOUTH FACING

E SPRUCE ST FACADES



SF 5000

LR 2

THE SITE

18TH AVE - EAST FACING

LINE OF FUTURE DEVELOPMENT
@ 207 18TH AVE - #3019262

LINE OF FUTURE DEVELOPMENT
@ 215 18TH AVE - #6502116



LR 2

SF 5000

18TH AVE - WEST FACING

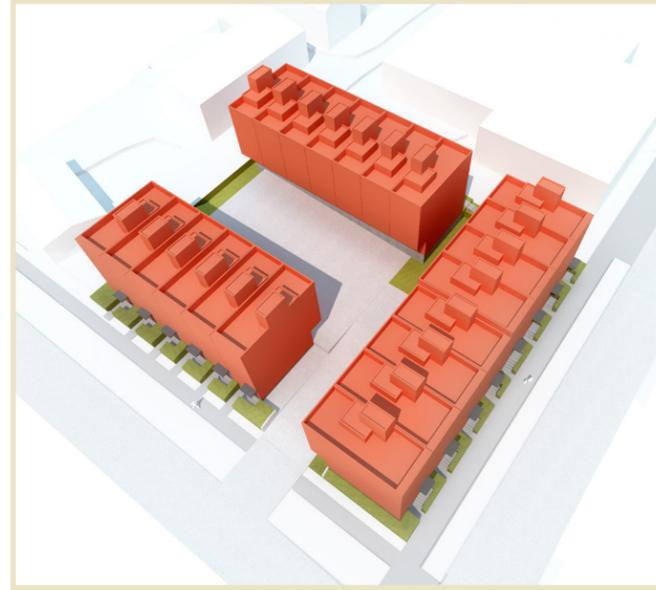
ACROSS THE STREET

18TH AVE FACADES



FIRST SCHEME - code compliant

The first scheme utilizes the existing lot lines, organizing the buildings into clusters on each site. Townhouses are placed on the north and middle sites with rowhouses running along the south lot giving a strong street presence along E Spruce St. There is one main driveway for the project servicing all the units. The project provides (21) open parking stalls at the rear of the site adjacent to the east neighboring site. The townhouses on the north and middle lots are paired together with a common walkway between them. The rowhouses along the south lot are sited so the entries are 30" or less above the sidewalk grade with stoops at each entry for private access.



SECOND SCHEME - code compliant

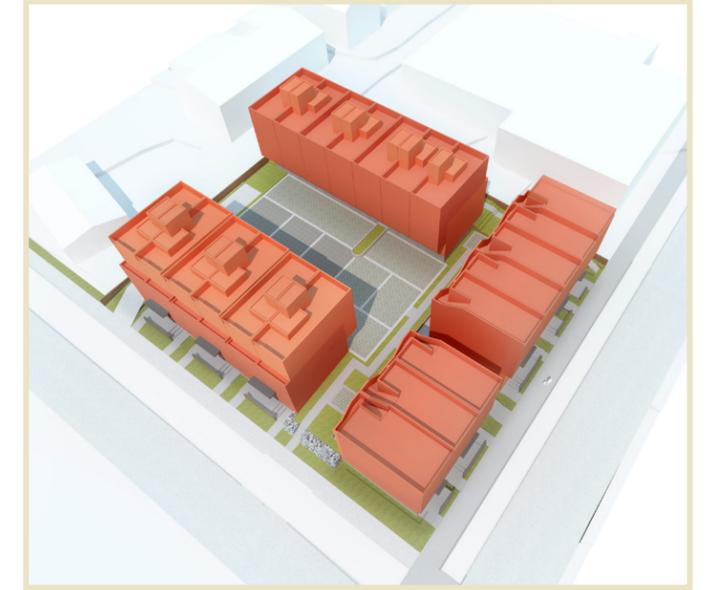
The second scheme for this site intends to create a street friendly community with easy access into the site. The plan utilizes the lot boundary adjustment, adjusting the lot lines. Townhouses and rowhouses are then organized in the parcels to front the street and have parking centered in the site. The building is pushed down into the site creating a recessed entry into each unit. The entries repeat along the facade and create personality for the street. Although this scheme meets required setbacks, access, driveway widths, and vertical raise to the entry within the setback, it requires an extensive amount of excavation to get the entry within 30" above the sidewalk. The access point for the driveway along 18th Ave is very wide and prominent, giving more attention to the car traffic than to the street facing units. The buildings are maximized for property development with no modulation, however, flat roofs allow access to roof decks.



THIRD SCHEME

The third scheme also utilizes the lot boundary adjustment and provides a strong street facing presence on both 18th Ave and E Spruce St. The driveway width is reduced to create more common amenity space and a more friendly pedestrian environment. This gives more attention to the unit entries than the cars entering and exiting the site. The internal courtyard provides a common amenity space that is screened from the street. The building along E Spruce St is split in two to create a pedestrian path that immediately opens to the common amenity space providing additional modulation along the street frontage. The buildings are situated at the same elevation as the existing apartment buildings which reduces site disturbance significantly.

Departures are requested to further enhance the residential experience and create a community that speaks to the rest of the neighborhood. They include a reduced driveway width, reduced sight triangle, and increased raise from the sidewalk within the setback.



FOURTH SCHEME - preferred

The new fourth scheme includes moves to reduce the massing of the street-facing units as well as create better amenity space and entry stoops. The driveway has been shifted to the north part of the site, utilizing the existing curb cut. The driveway will be buffered for screening along the North property line. The parking area is separated into two zones using landscaped islands and paving changes to reduce the apparent depth and create a stronger link to the shared green space. It also terminates at a large green space in the middle of the site. This green space acts as common amenity, transition through the site, and an open space to make the site feel less dense. The units along E Spruce St have been redesigned to put the living-dining-kitchen on the first floor and have an open exterior stair for roof deck access. The entries along 18th Ave and E Spruce St have stoops that are 5'-0" wide and feature brick detailing to provide a relationship to the existing building and to create a welcoming area that transitions from public to private. Landscape buffers are integrated along the entire east property line to create privacy from the neighboring Boys and Girls Club.

SCHEME OVERVIEW

FIRST SCHEME



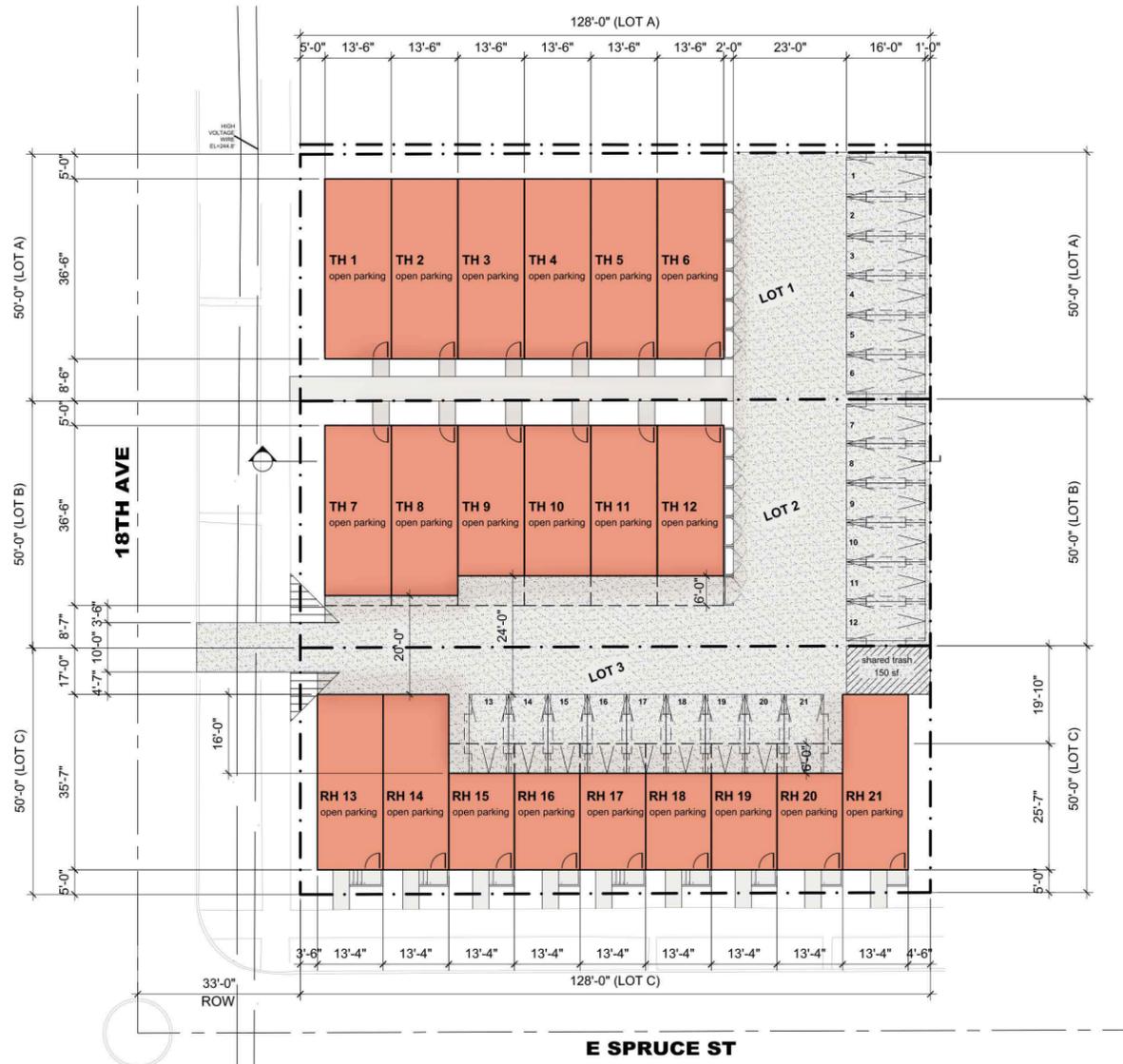
DISTINGUISHING FEATURES:

- 3 STORIES PLUS PENTHOUSES
- FLAT ROOFS ALLOWING FOR ROOF TOP DECKS
- 21 UNITS (12 TH'S, 9 RH'S)
- (21) OPEN PARKING STALLS
- 10' DRIVE OPENING TO 24' AT THE PROPERTY LINE

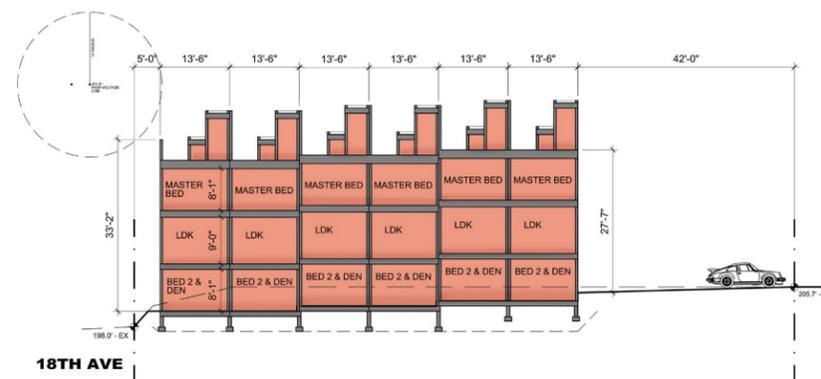
REQUESTED DEPARTURES:

- None

The first scheme utilizes the existing lot lines, organizing the buildings into clusters on each site. Townhouses are placed on the north and middle sites with rowhouses running along the south lot giving a strong street presence along E Spruce St. There is one main driveway for the project servicing all the units. The project provides (21) open parking stalls at the rear of the site adjacent to the east neighboring site. The townhouses on the north and middle lots are paired together with a common walkway between them. The rowhouses along the south lot are sited so the entries are 30" or less above the sidewalk grade with stoops at each entry for private access.



SITE PLAN



SECTION

LOT 1

The first scheme for the original Lot 1 places (6) townhouses on the site with pedestrian access to units on the south side along a shared walkway. There are (6) open parking stalls located at the rear of the site which are accessed from a common driveway for the whole site. Each townhouse has a roof deck but there is little modulation to increase the floor area.

LOT 2

The first scheme for the original Lot 2 places (6) townhouses on the site with pedestrian access to units on the south side along a shared walkway. There are (6) open parking stalls located at the rear of the site which are accessed from a common driveway for the whole site. Each townhouse has a roof deck but there is little modulation to increase the floor area.

LOT 3

The first scheme for Lot 3 places (9) rowhouses along E Spruce St. There are (9) open parking stalls with a common shared trash area at the east part of the site. The units along E Spruce St have stoops at the entries to create a division between the public and private realms. Each townhouse has a roof deck but there is little modulation to increase the floor area.

HOUSE
1-STORY (SF5000)

BOYS & GIRLS CLUB
2-STORIES (LR2)



BIRDSEYE



VIEW FROM CORNER OF E SPRUCE AND 18TH AVE



APPROACH FROM SOUTHEAST ON E SPRUCE ST

SCHEME 1

PROPOSED LBA: SCHEME 2 & 3

LOT A - ROWHOUSES
 LOT SIZE: 6,048 sf
 ZONE: LR2
 ADDRESS: 212 18TH AVE
 EDG PROJECT: #3023583
 PROPOSED USE: 7 ROWHOUSES

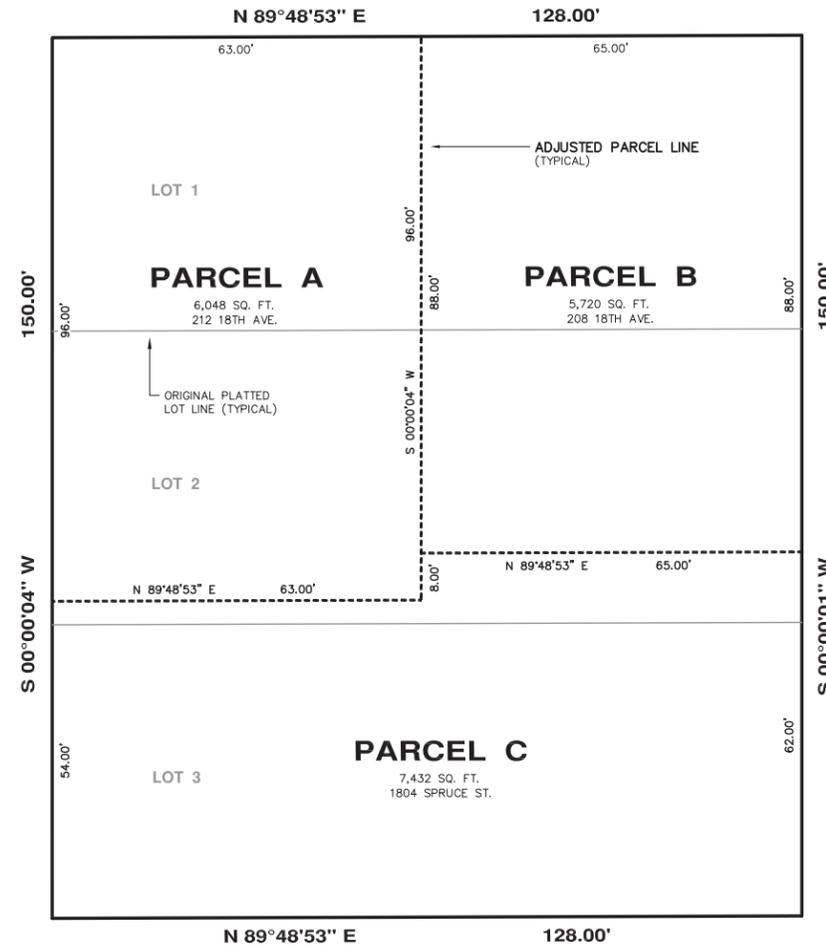
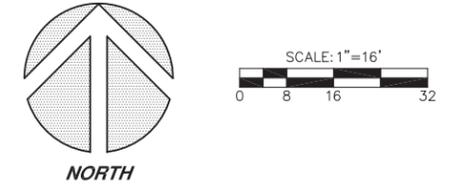
LOT B - TOWNHOUSES
 LOT SIZE: 5,720 sf
 ZONE: LR2
 ADDRESS: 208 18TH AVE
 EDG PROJECT: #3023584
 PROPOSED USE: 6 TOWNHOUSES

LOT C - ROWHOUSES
 LOT SIZE: 7,432 sf
 ZONE: LR2
 ADDRESS: 1804 E SPRUCE ST
 EDG PROJECT: #3023032
 PROPOSED USE: 8 ROWHOUSES

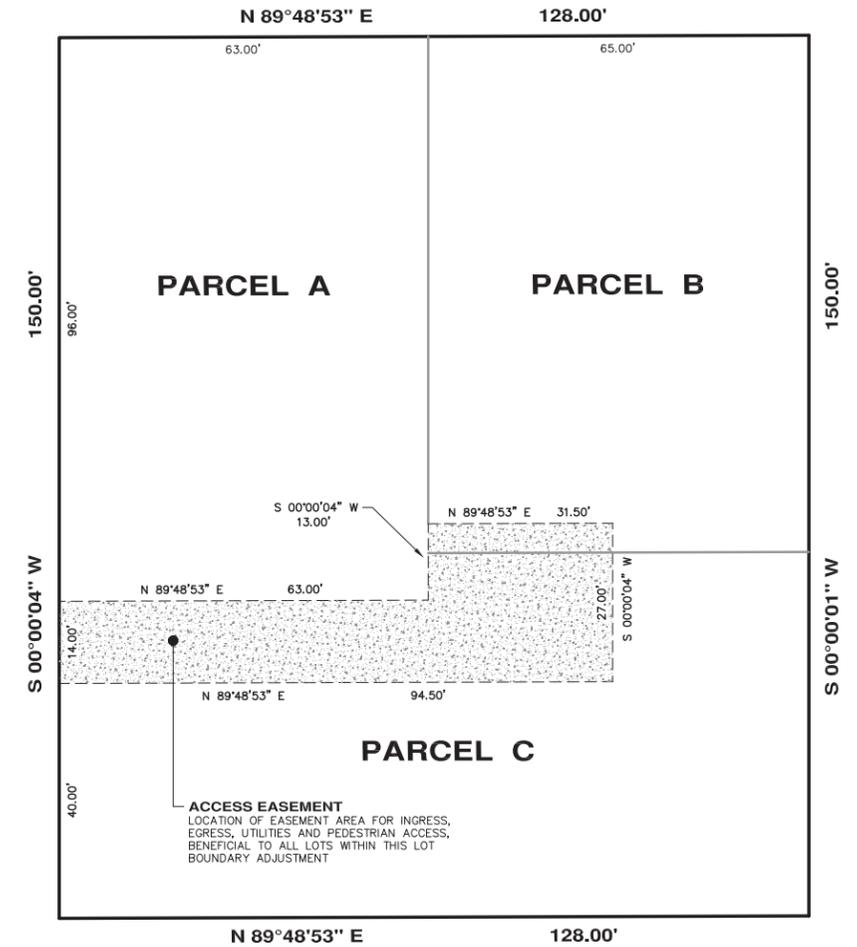
	ALLOWED	PROPOSED
FAR:	Lot A: 1.3 x 6,048 SF = 7,862 SF	7,860 SF
	Lot B: 1.2 x 5,720 SF = 6,864 SF	6,860 SF
	Lot C: 1.3 x 7,432 SF = 9,662 SF	9,032 SF
HEIGHT:	30' in LR2; 4' parapet; 10' penthouse	29.3'
BLDG LENGTH:	Lot A: 63' x 65% = 40.9'	34'-6"
	Lot B: 62' x 65% = 40.3'	35'-0"
	Lot C: 65' x 65% = 42.3'	34'-6"



LOT BOUNDARY ADJUSTMENT NO. 3022616 FOR ISOLA REAL ESTATE, LLC



ADJUSTED PARCEL DETAIL



EASEMENT DETAIL



CHADWICK WINTERS
 LAND SURVEYING AND MAPPING
 1422 N.W. 85TH ST., SEATTLE, WA 98117
 PHONE: 206.297.0996
 FAX: 206.297.0997
 WEB: WWW.CHADWICKWINTERS.COM

SURVEY IN: SW 1/4, SW 1/4, SEC. 33, T. 25 N., R. 4 E., W.M. KING COUNTY, WASHINGTON		
FOR: ISOLA REAL ESTATE, LLC		
DRAWN BY: SAL	DATE: 11-24-15	PROJECT #: 15-5330
CHK. BY: RHW	SCALE: 1" = 16'	SHEET: 4 OF 5

LOT BOUNDARY ADJUSTMENT

LOT A #3023583

LOT SIZE: 6,048 sf

ZONE: LR2

ALLOWED FAR: 6,048 sf x 1.3 = 7,862 sf
(RHs + garages + green building)

SETBACKS: 5' front setback
0' side setback (RH next to RH)
3.5' side setback (RH next to residential)
5' rear setback (7' average)

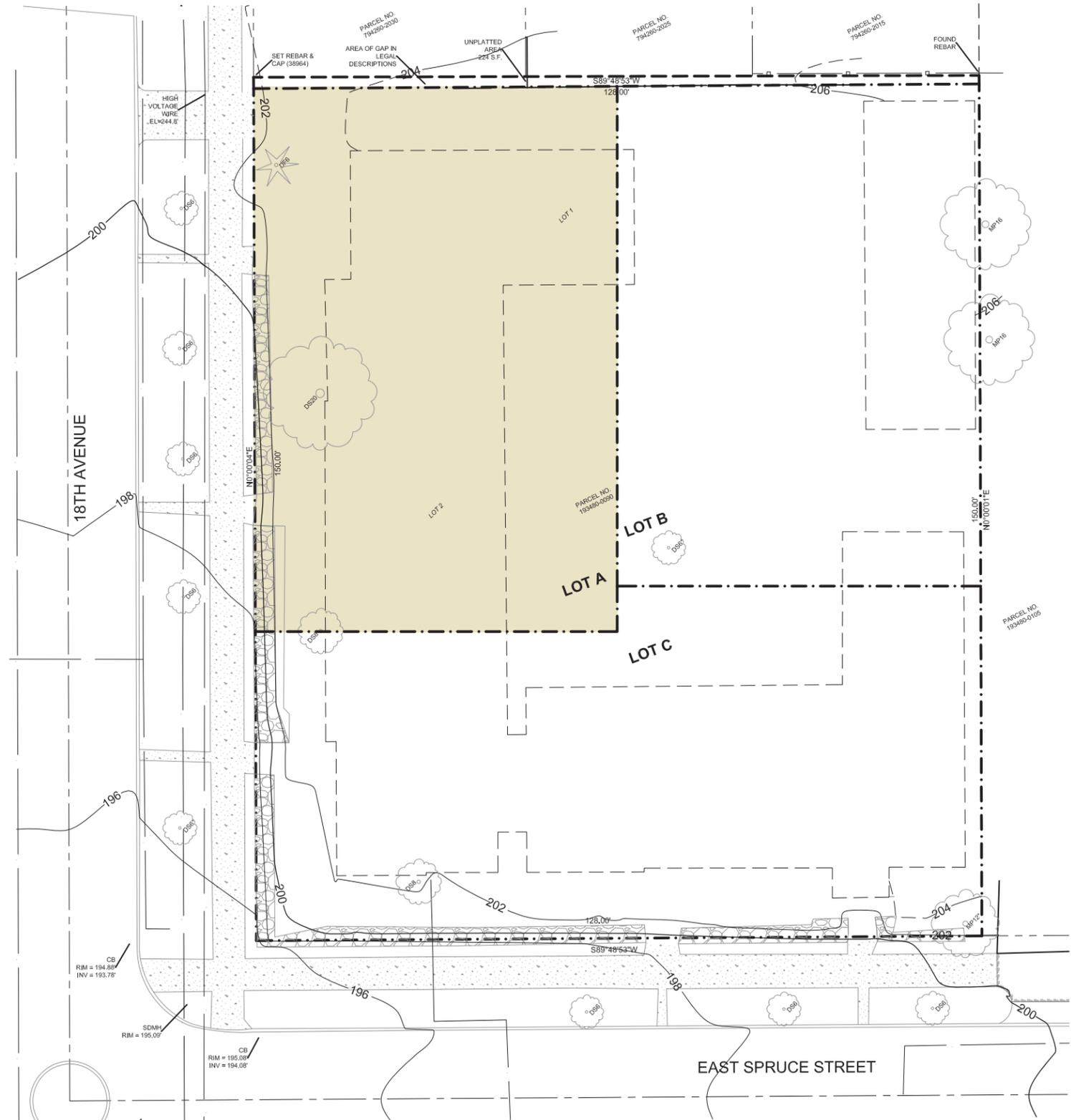
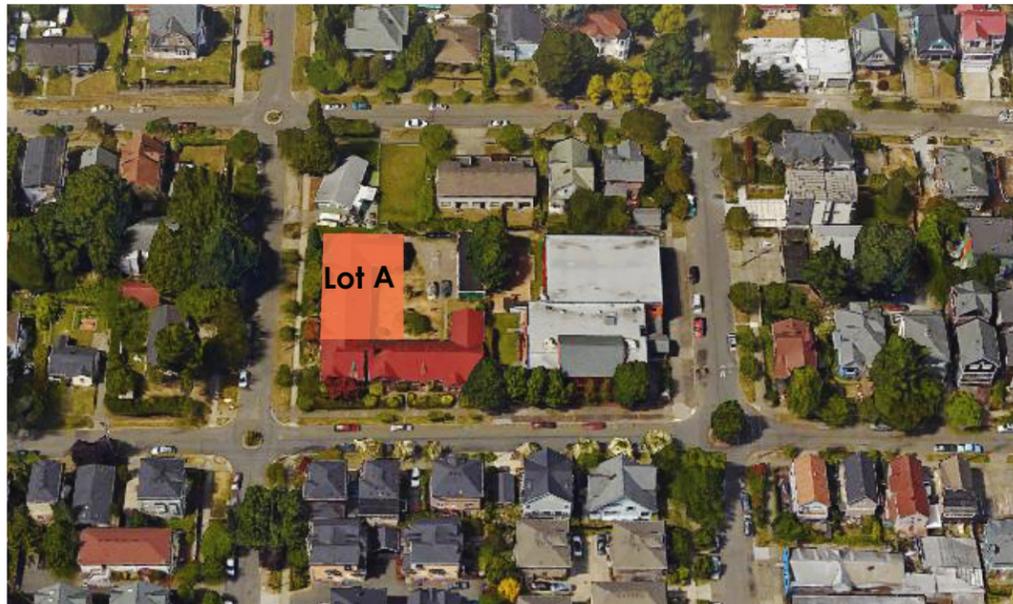
ALLOWED HEIGHT: 30' + 4' Parapet Allowance & 10' Penthouse

FACADE LENGTH: 36' (allowed 40' per 23.45.527.B.2)

LEGAL DESCRIPTION: THAT PORTION OF LOTS 1 AND 2, BLOCK 3, DEANS'S ADDITION TO THE CITY OF SEATTLE, ACCORDING TO THE PLAT THEREOF RECORDED UNDER VOLUME 1 OF PLATS, PAGE 206, RECORDS OF KING COUNTY, WA. BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE N.W. CORNER OF SAID LOT 1; THENCE N 89°48'53" E ALONG THE NORTH LINE OF SAID LOT FOR A DISTANCE OF 63.00 FT.; THENCE S 00°00'04" W, 96.00 FT.; THENCE S 89°48'53" W, 63.00 FT.; THENCE N 00°00'04" E, 96.00 FT. TO THE POINT OF BEGINNING.

TOGETHER WITH AN EASEMENT FOR INGRESS, EGRESS, UTILITIES AND PEDESTRIAN ACCESS AS SHOWN AND DESCRIBED AS "ACCESS EASEMENT" ON CITY OF SEATTLE LOT BOUNDARY ADJUSTMENT NO. 3022616.



LOT A - SIZE & FAR

LOT B #3023584

LOT SIZE: 5,720 sf

ZONE: LR2

ALLOWED FAR: 5,720 sf x 1.2 = 6,864 sf
(THs + garages + green building)

SETBACKS: 5' front setback (7' average)
5' side setback
5' rear setback (7' average)

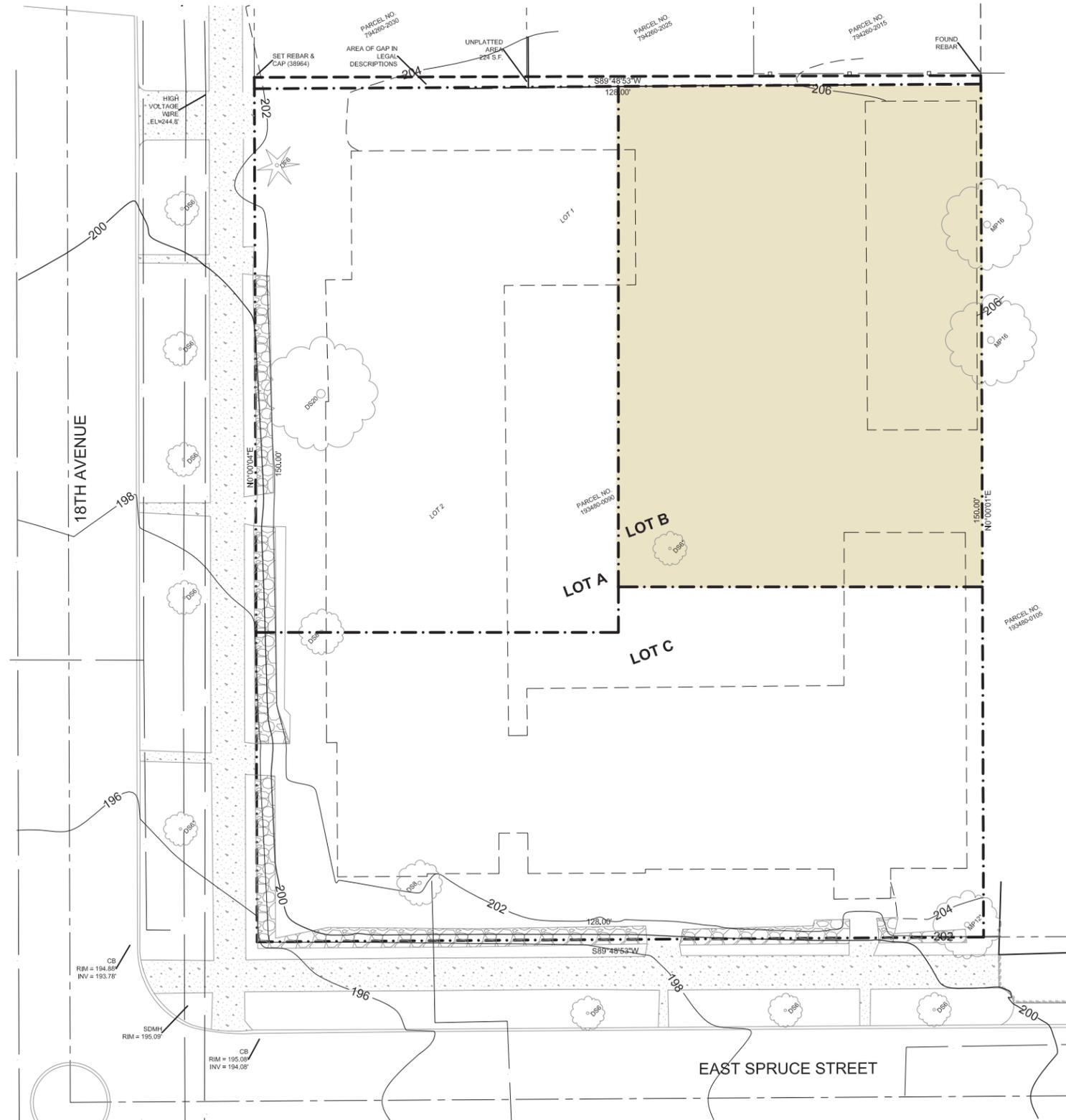
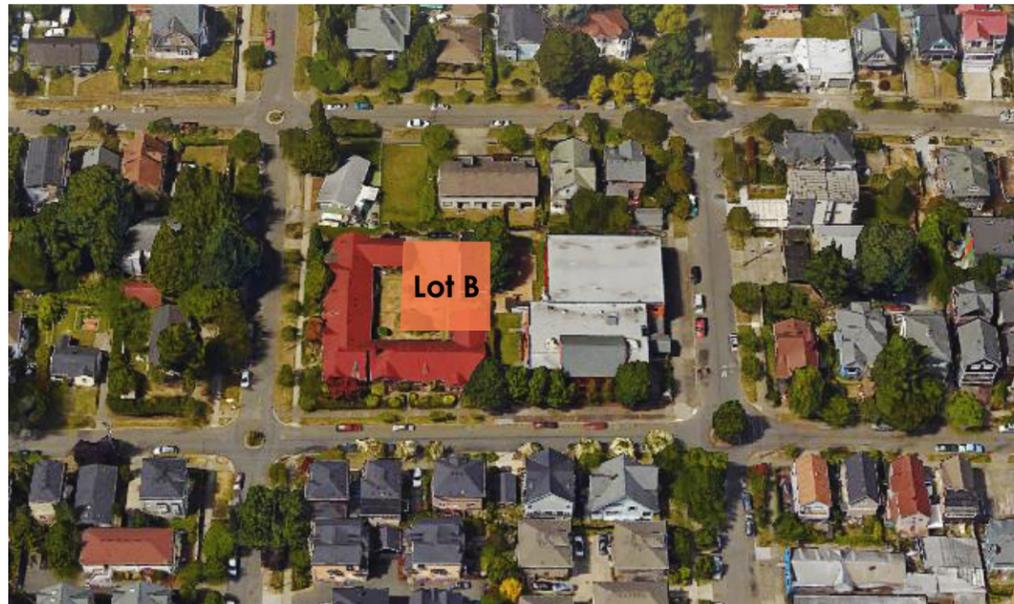
ALLOWED HEIGHT: 30' + 4' Parapet Allowance & 10' Penthouse

FACADE LENGTH: 65' x 65% = 42.3' allowed facade length
88' x 65% = 57.2' allowed facade length

LEGAL DESCRIPTION: THAT PORTION OF LOTS 1 AND 2, BLOCK 3, DEANS'S ADDITION TO THE CITY OF SEATTLE, ACCORDING TO THE PLAT THEREOF RECORDED UNDER VOLUME 1 OF PLATS, PAGE 206, RECORDS OF KING COUNTY, WA. BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE N.E. CORNER OF SAID LOT 1; THENCE S 00°00'01" W ALONG THE EAST LINE OF SAID LOT FOR A DISTANCE OF 88.00 FT.; THENCE S 89°48'53" W, 65.00 FT.; THENCE N 00°00'04" E, 88.00 FT.; THENCE N 89°48'53" E, 65.00 FT. TO THE POINT OF THE BEGINNING.

SUBJECT TO AND TOGETHER WITH AN EASEMENT FOR INGRESS, EGRESS, UTILITIES AND PEDESTRIAN ACCESS AS SHOWN AND DESCRIBED AS "ACCESS EASEMENT" ON CITY OF SEATTLE LOT BOUNDARY ADJUSTMENT NO. 3022616.



LOT B - SIZE & FAR

LOT C #3023032

LOT SIZE: 7,432 sf

ZONE: LR2

ALLOWED FAR: 7,432 sf x 1.3 = 9,662 sf
(RHs + garages + green building)

SETBACKS: 5' front setback
0' side setback (RH next to RH)
3.5' side setback (RH next to residential)
5' rear setback (7' average)

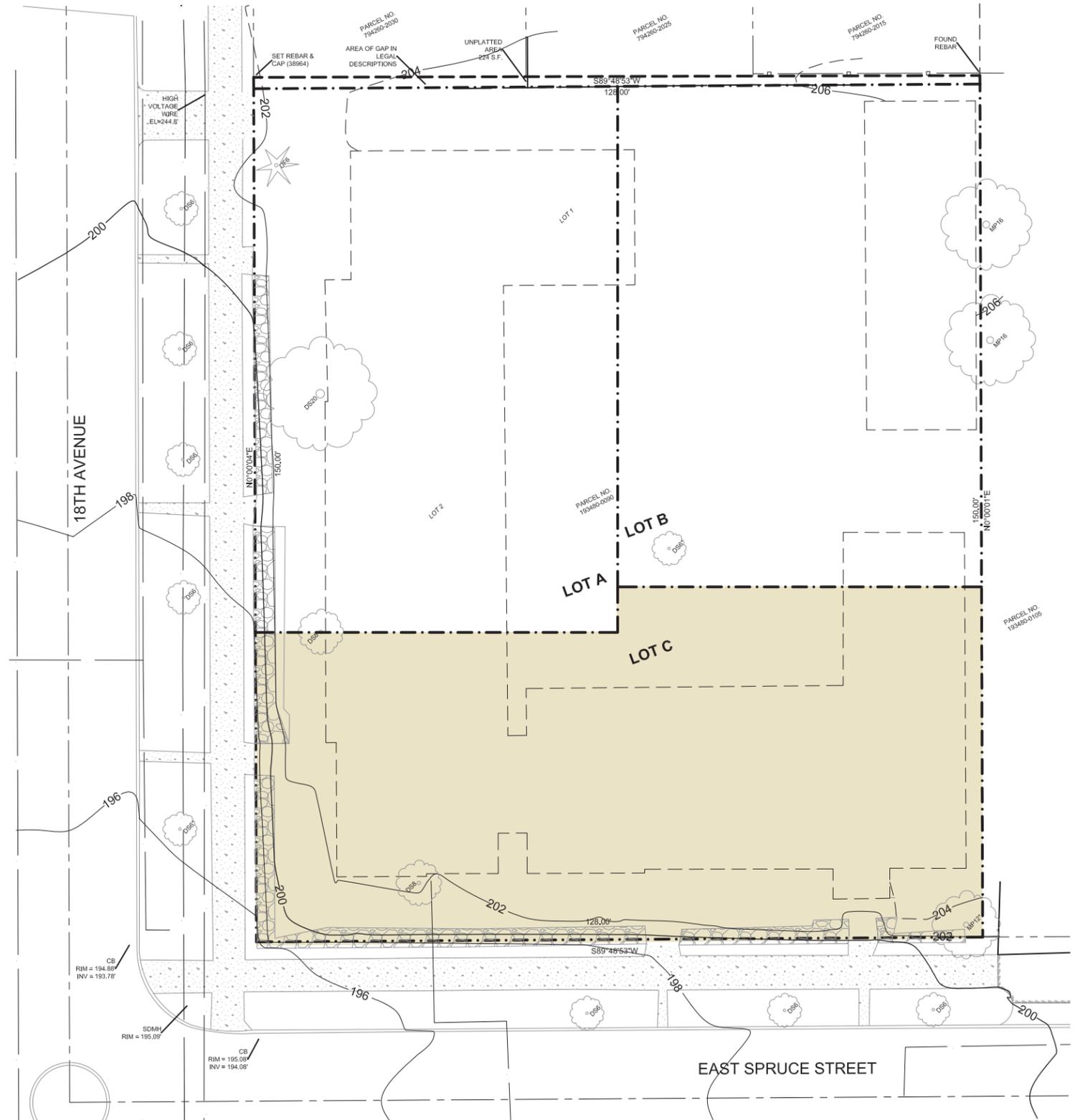
ALLOWED HEIGHT: 30' + 4' Parapet Allowance & 10' Penthouse

FACADE LENGTH: 36' (allowed 40' per 23.45.527.B.2)

LEGAL DESCRIPTION: THAT PORTION OF LOTS 1 AND 2, BLOCK 3, DEANS'S ADDITION TO THE CITY OF SEATTLE, ACCORDING TO THE PLAT THEREOF RECORDED UNDER VOLUME 1 OF PLATS, PAGE 206, RECORDS OF KING COUNTY, WA. BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE S.E. CORNER OF SAID LOT 3; THENCE S 89°48'53" W ALONG THE SOUTH LINE OF SAID LOT FOR A DISTANCE OF 128.00 FT.; THENCE N 00°00'04" E, 54.00 FT.; THENCE N 89°48'53" E, 63.00 FT.; THENCE N 00°00'04" E, 8.00 FT.; THENCE N 89°48'53" E, 65.00 FT.; THENCE S 00°00'01" W, 62.00 FT. TO THE POINT OF BEGINNING.

TOGETHER WITH AN EASEMENT FOR INGRESS, EGRESS, UTILITIES AND PEDESTRIAN ACCESS AS SHOWN AND DESCRIBED AS "ACCESS EASEMENT" ON CITY OF SEATTLE LOT BOUNDARY ADJUSTMENT NO. 3022616.



LOT C - SIZE & FAR

SECOND SCHEME: CODE COMPLIANT



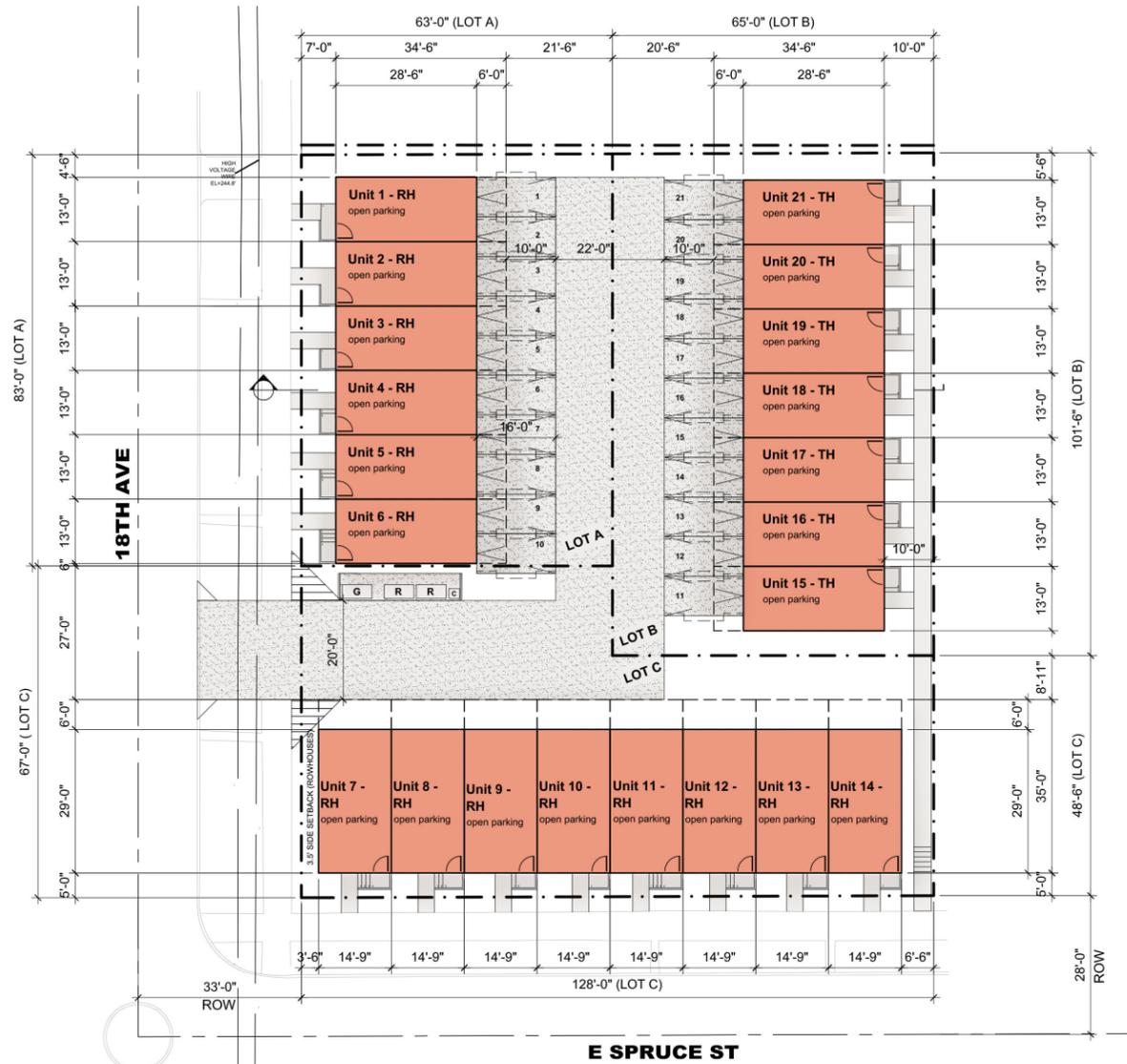
DISTINGUISHING FEATURES:

- 3 STORIES PLUS PENTHOUSES
- FLAT ROOFS ALLOWING FOR ROOF DECKS
- 21 UNITS (14 RHs, 7 THs)
- 21 PARKING SPACES
- 20' DRIVEWAY WIDTH
- SIDEWALK LEVEL ENTRIES

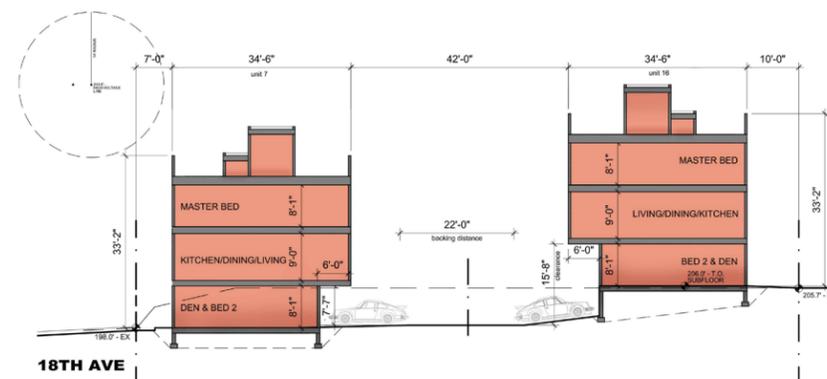
REQUESTED DEPARTURES:

- No departures requested

The second scheme for this site intends to create a street friendly community with easy access into the site. The plan utilizes the lot boundary adjustment, adjusting the lot lines. Townhouses and rowhouses are then organized in the parcels to front the street and have parking centered in the site. The building is pushed down into the site creating a recessed entry into each unit. The entries repeat along the facade and create personality for the street. Although this scheme meets required setbacks, access, driveway widths, and vertical raise to the entry within the setback, it requires an extensive amount of excavation to get the entry within 30" above the sidewalk. The access point for the driveway along 18th Ave is very wide and prominent, giving more attention to the car traffic than to the street facing units. The buildings are maximized for property development with no modulation, however, flat roofs allow access to roof decks.



SITE PLAN



SECTION

LOT A - Project #3023583

The second scheme for Lot A on this site includes six rowhouses along the street front. The rowhouses are placed along the street and provide flat roofs for roof top decks. Each rowhouse has an open parking space behind within the parking court. The building is cantilevered 6'-0" over the parking spaces to maximize the interior square footage.

LOT B - Project #3023584

The second scheme for Lot B on this site places seven townhouses towards the back of the site. Pedestrian access is located along the east providing private entrances. The setback is increased to create a more open entry and the units have flat roofs for roof top decks. Each townhouse has an open parking space behind within the parking court. The building is cantilevered 6'-0" over the parking spaces to maximize the interior square footage.

LOT C - Project #3023032

The second scheme for Lot C on this site lines up eight rowhouses along the south portion of the site. The rowhouses have great street frontage and contain private entries. They help to optimize the development of the property by maximizing the building footprint and providing flat roofs for roof top decks. Parking is provided for each unit within the parking court. A portion of the site is dedicated to the driveway into the site.

HOUSE
1-STORY (SF5000)

BOYS & GIRLS CLUB
2-STORIES (LR2)



BIRDSEYE



VIEW FROM CORNER OF E SPRUCE AND 18TH AVE



APPROACH FROM SOUTHEAST ON E SPRUCE ST

THIRD SCHEME



DISTINGUISHING FEATURES:

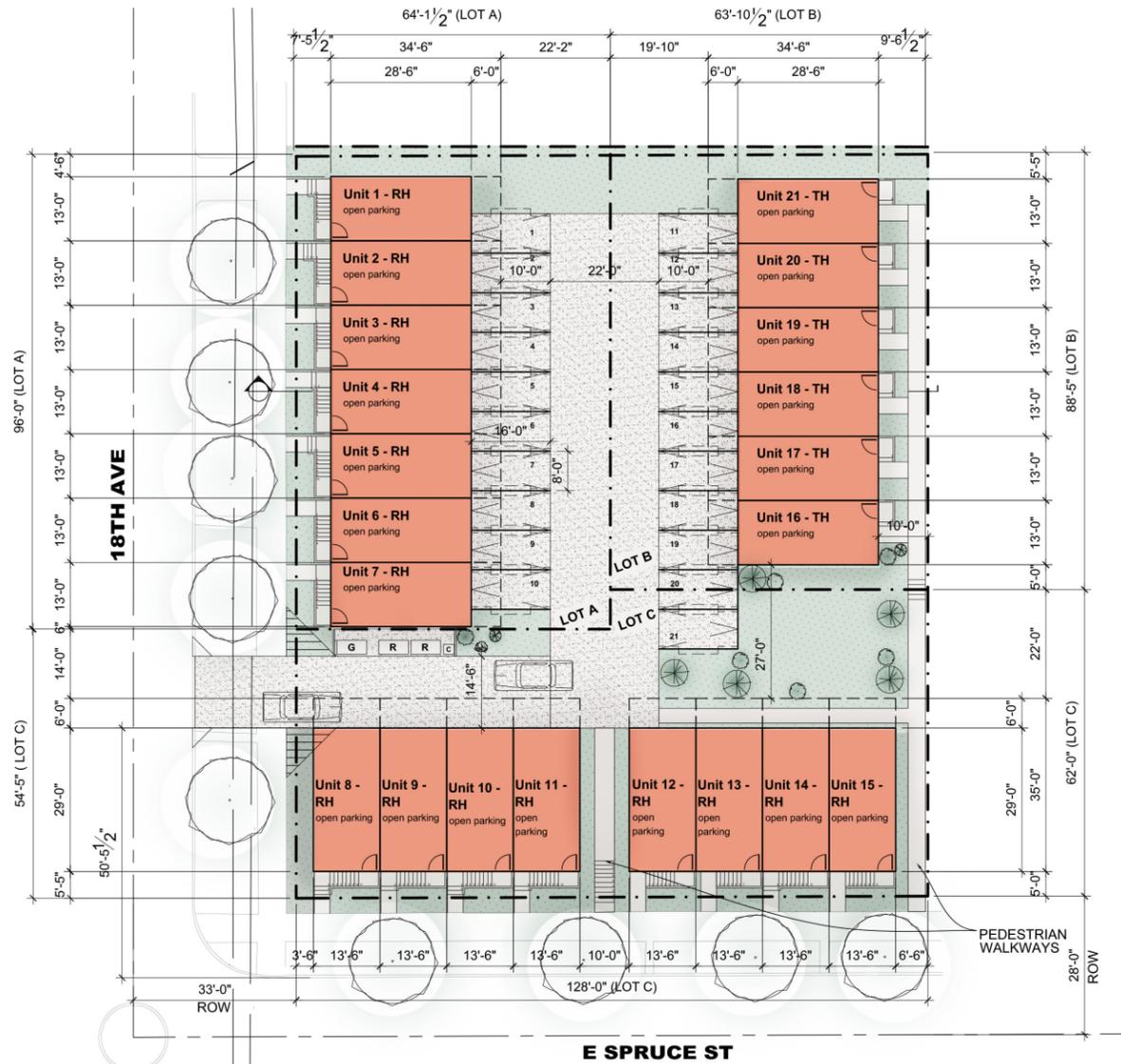
- 3 STORIES PLUS PENTHOUSES
- FLAT ROOFS ALLOWING FOR ROOF TOP DECKS
- 21 UNITS (14 TH'S, 7 RH'S)
- 21 PARKING SPACES
- 14'-6" DRIVE
- BUILDINGS AT EXISTING GRADE

REQUESTED DEPARTURES:

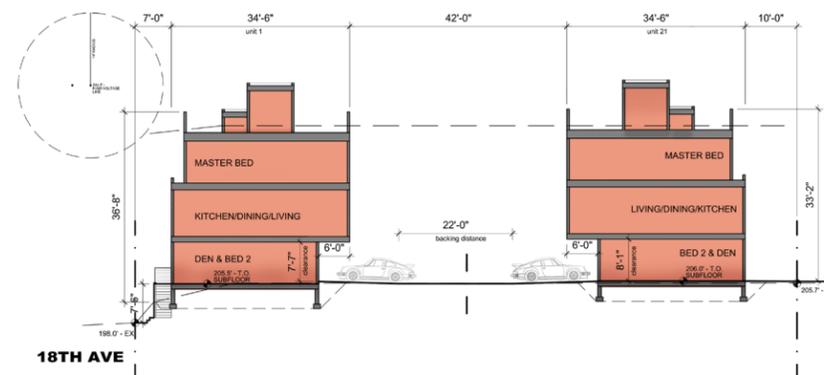
- Access driveway reduced from the required 20' to 14'-6" (Lot A, B, & C)
- Reduced sight triangle (Lot A, B, & C)
- Increased raise from sidewalk to unit entry within setback (Lot A & C)

The third scheme also utilizes the lot boundary adjustment and provides a strong street facing presence on both 18th Ave and E Spruce St. The driveway width is reduced to create more common amenity space and a more friendly pedestrian environment. This gives more attention to the unit entries than the cars entering and exiting the site. The internal courtyard provides a common amenity space that is screened from the street. The building along E Spruce St is split in two to create a pedestrian path that immediately opens to the common amenity space providing additional modulation along the street frontage. The buildings are situated at the same elevation as the existing apartment buildings which reduces site disturbance significantly.

Departures are requested to further enhance the residential experience and create a community that speaks to the rest of the neighborhood. They include a reduced driveway width from 20' to 14'-6", a reduced sight triangle, and an increased raise from the sidewalk to the entry within the setback. By creating new entrances at existing elevations, we are mimicking the current apartment entry arrangement and maintaining neighborhood character. In addition, the reduced site disturbance will be a benefit to the neighbors during construction (fewer large truck trips) and generally better environmentally.



SITE PLAN



SECTION

LOT A - Project #3023583

The third scheme for Lot A on this site includes seven rowhouses along the street front. The rowhouses are placed along the street and provide flat roofs for roof top decks. Each rowhouse has an open parking space behind within the parking court. The building is cantilevered 6'-0" over the parking spaces to maximize the interior square footage. More space along the street is dedicated to the entry stairs providing a unique experience for each resident.

LOT B - Project #3023584

The third scheme for Lot B on this site places six townhouses towards the back of the site. Pedestrian access is located along the East providing private entrances. The setback is increased to create a more open entry and the units have flat roofs for roof top decks. Each townhouse has an open parking space behind within the parking court. The building is cantilevered 6'-0" over the parking spaces to maximize the interior square footage.

LOT C - Project #3023032

The third scheme for Lot C on this site provide two buildings of four rowhouses along the south portion of the site. The rowhouses have great street frontage and contain private entries along with flat roofs for roof top decks. The split buildings provide another pedestrian access. Parking is provide for each unit within the parking court. Portions of the site are dedicated to the driveway and common amenity space.

HOUSE
1-STORY (SF5000)

BOYS & GIRLS CLUB
2-STORIES (LR2)



BIRDSEYE



VIEW FROM CORNER OF E SPRUCE AND 18TH AVE



APPROACH FROM SOUTHEAST ON E SPRUCE ST

SCHEME 3

FIRST EARLY DESIGN GUIDANCE OF THE EAST DESIGN REVIEW BOARD

MASSING

Board Comment

Response

- | Board Comment | Response |
|--|---|
| 1. The mass of the structures is overbearing and should be eroded to reduce the visual impact of the upper floors. | 1. The building has been pushed down into the site to reduce the height, bulk and scale. The unit entries are now within 30" above the sidewalk grade. |
| 2. The bulk should be further reduced by using a more transparent parapet. | 2. The parapets are broken up to have areas with transparent railings. The corner unit on 18th and E Spruce has a full transparent railing to reduce the visual scale of the building. |
| 3. The penthouse stairs should be more internal to each structure to reduce the height of the buildings. | 3. The penthouses on units 1-6 and 15-21 are located at the center of the building so as not to be visible on the exterior facade. Units 7-14 have open stairs to the roof deck which reduces the visual apparent height. |
| 4. Further renderings should clearly show the upper two floors. | 4. We've shown all floors in the renderings showing the full building. |

SITE DESIGN

- | | |
|---|---|
| 1. Lack of connection between townhomes and the amenity area. Units along eastern property line should be connected to amenity areas and have less isolated pedestrian circulation. | 1. The central amenity space has expanded by moving the driveway to the north part of the site. This opens up the center of the site for amenity space and pedestrian paths. The units along the east are accessed from the central amenity area or the path along the east property line. |
| 2. Stoops should clearly mark residential entries while provided large enough landing areas for a gathering space. | 2. The stoops have increased in size from 3' to 5' wide. They are paired so two units share one stoop. A brick clad wall has been integrated to create a threshold from the sidewalk as well as create a place to pause to interact with neighbors or to find your keys. |
| 3. Residences should be grouped together and separated from the parking to create more cohesive residential spaces. | 3. The parking is internal to the site keeping it visually separated from the street. The units front the street to create a strong urban edge while forming a common amenity space at the center of the site. The setbacks are gracious along all edges to incorporate the stoops - this enhances the residential experience along with creating a positive transition from public to private. |

EDG 1 MEETING - COMMENTS & RESPONSE

FIRST EARLY DESIGN GUIDANCE OF THE EAST DESIGN REVIEW BOARD

Board Comment

Response

4. The design should clearly commit to either large stoops or larger amenity space, not a half measure of each.

4. Although recommended to commit to either a larger stoop or larger amenity space, we've opted to enhance both the stoops and the amenity space. We feel both are assets to the project and site and both should be committed to. Therefore, the stoops have been enlarged and are a shared feature along the street edge. The amenity space was made larger by shifting the driveway to the north part of the site. This opened up the space between the buildings and allowed us to activate the site with common shared spaces and private back patios. The south portion of the parking court has pavers to blend into the common area, creating a larger gathering space for the residents.

CONTEXT

1. The existing building's character should be acknowledged in the project's design through the use of similar material or proportions.

1. The existing building's brick will be reused to clad the entry stoops. The entry sequence mimics the character of the existing building by having narrow pathways leading up to the front door. The existing building made a strong corner presence and our proposal is in kind with holding the urban edge. By pushing the building down, the height of the building is closer to the existing building height.

2. Landscape buffers should be included to the east for the Boys and Girls Club and the north bordering single-family residences

2. Landscape buffers are integrated along all edges of the site especially along the east for the Boys and Girls Club and the north for the single-family residences.

3. The Board wants to see perspectives from the street at the second EDG Meeting to better understand the project's relationship to its surroundings

3. Multiple renderings have been included to show the project's relationship to its surroundings along with an added aerial image of the site.

4. The applicant should use high-quality materials. One option is to use brick, a material present on the existing building

4. Brick has been incorporated into the entry stoops. Cementitious lap siding will be used along with cementitious panel for window accents. Other high-quality materials will be used to accent the entry and create modulation.

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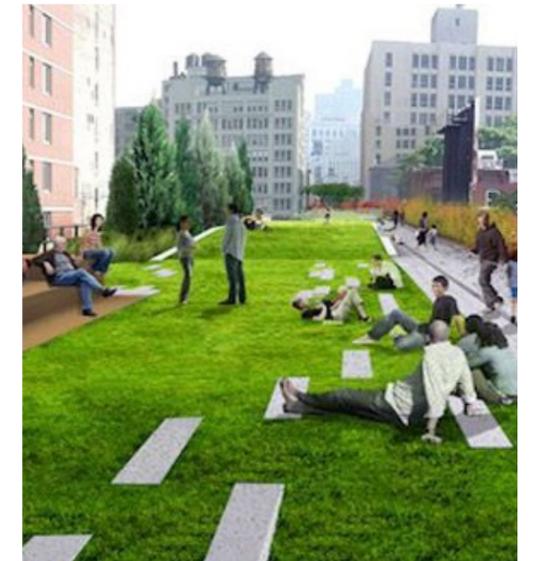
RE-USE

By integrating brick as a connected theme throughout the project, the essence of the existing building remains and is highlighted as a clear, unifying element connecting the project back to the neighborhood.



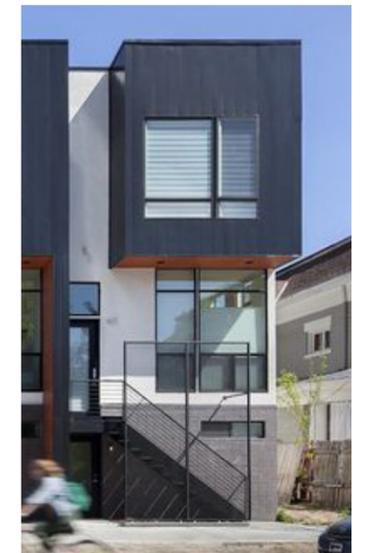
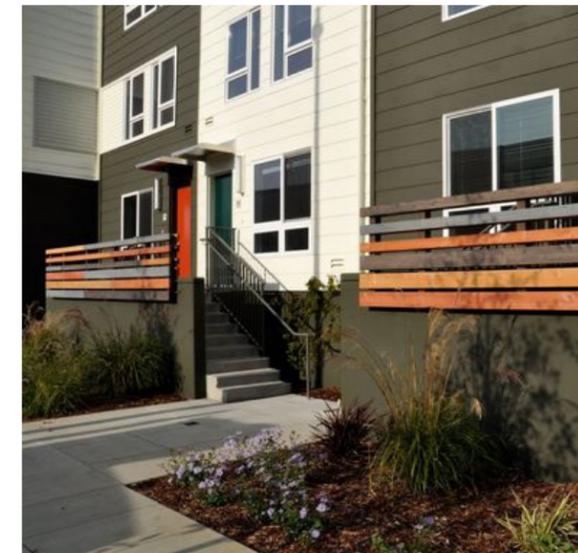
ENHANCED CIRCULATION

Circulation can be under-exploited as a physical and social network. As transitional space, it has the potential to become much more significant. By incorporating designed layers of hardscape and landscape, this unique form of shared spaces has the potential to become enriched.



ENTRY

The concept of the front porch, or “the stoop”, is often used as a built transition between public and private thresholds. By opening up the foreground of the face of a building, visual and physical links can be designed to enhance and enrich this threshold.



PROJECT INSPIRATION

FOURTH SCHEME: PREFERRED



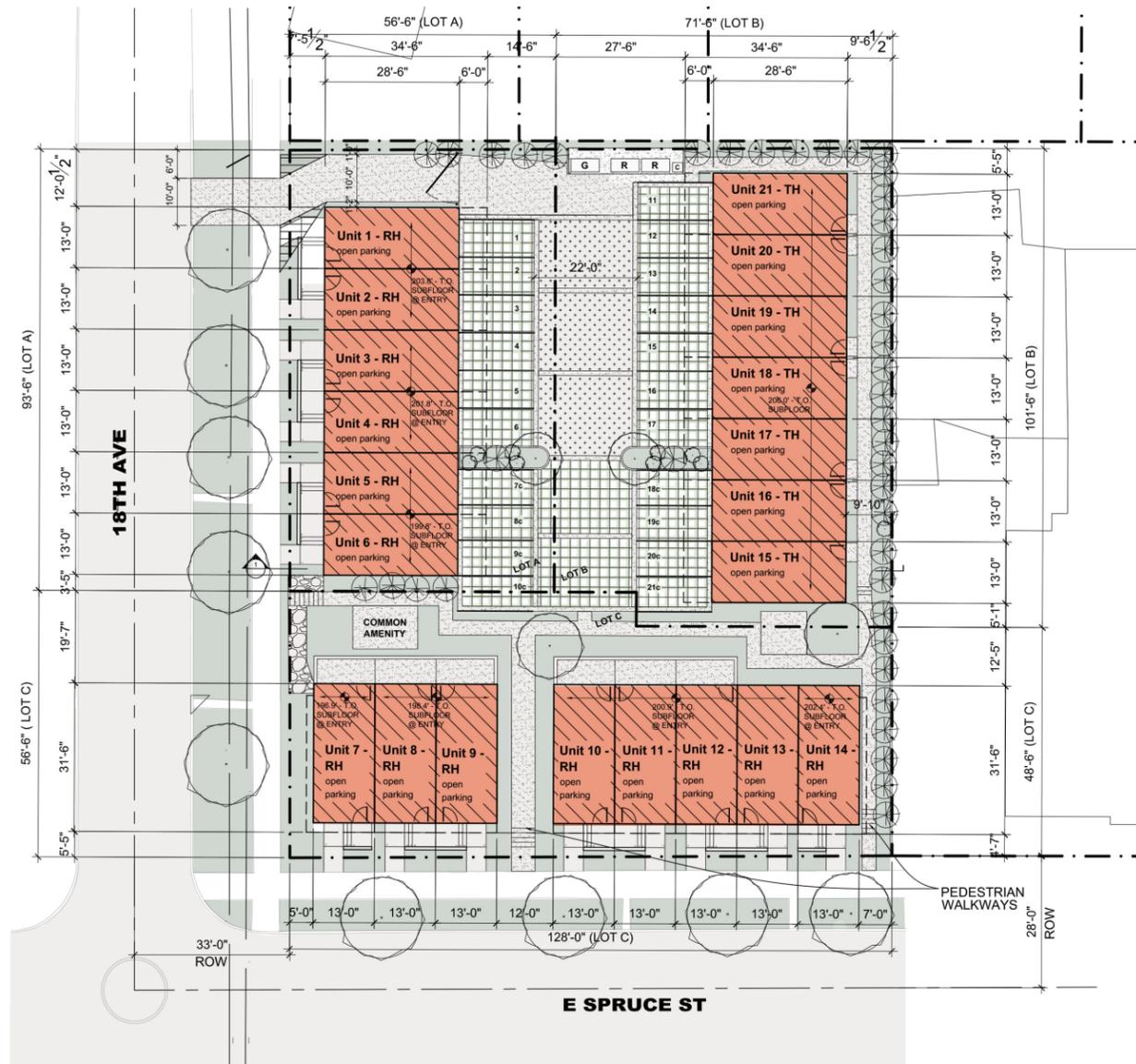
DISTINGUISHING FEATURES:

- 3 STORIES PLUS PENTHOUSES & OPEN STAIRS
- FLAT ROOFS ALLOWING FOR ROOF TOP DECKS
- 21 UNITS (14 TH'S, 7 RH'S)
- 21 PARKING SPACES
- 10'-0" DRIVE

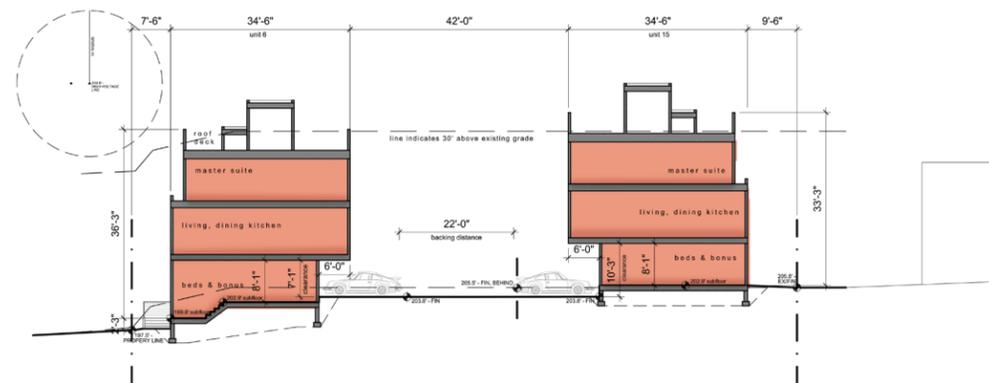
REQUESTED DEPARTURES:

- None

The new fourth scheme includes moves to reduce the massing of the street-facing units as well as create better amenity space and entry stoops. The driveway has been shifted to the north part of the site, utilizing the existing curb cut. The driveway will be buffered for screening along the North property line. The parking area is separated into two zones using landscaped islands and paving changes to reduce the apparent depth and create a stonger link to the shared green space. The parking court acts as a flexible space giving the residents options for how they want to use the space. The court terminates at a large green space in the middle of the site. This green space acts as common amenity, transition through the site, and an open space to make the site feel less dense. The units along E Spruce St have been redesigned to put the living-dining-kitchen on the first floor and have an open exterior stair for roof deck access. The entries along 18th Ave and E Spruce St have stoops that are 5'-0" wide and feature brick detailing to proved a relationship to the existing building and to create a welcoming area that transitions from public to private. Landscape buffers are integrated along the entire east property line to create privacy from the neighboring Boys and Girls Club.



SITE PLAN



SECTION

LOT A - Project #3023583

The fourth scheme for Lot A on this site includes six rowhouses along the street front. The rowhouse entries are within 30" of the sidewalk grade and have paired entry stoops that are 5'-0" wide. The buildings step down with the natural slope of the site helping to reduce the scale of the buildings. The entry driveway to the site is along the North, pushing the massing away from the neighboring SF5000 zone and integrating a landscape buffer for privacy.

LOT B - Project #3023584

The fourth scheme for Lot B on this site places seven townhouses towards the back of the site. Pedestrian access is located along the East providing private entrances. The setback is 9'-7" to create a more open space and integrates a landscaped buffer to the neighboring Boys and Girls Club to the East. There also is a landscape buffer to the North. A pedestrian path leads from the street or the parking area around to encourage interaction with neighbors.

LOT C - Project #3023032

The fourth scheme for Lot C on this site provides two distinct buildings, one of 3 rowhouses at the corner of E Spruce and 18th Ave, and the second of 5 rowhouses, separated by a landscaped connecting pathway to the integral green space. This path provides access to the units and the amenity space. For the most part, the units have shared stoops that are clad with brick and are 5'-0" wide to create a welcoming space for residents as they transition from public to private.

HOUSE
1-STORY (SF5000)

BOYS & GIRLS CLUB
2-STORIES (LR2)



BIRDSEYE



APPROACH FROM NORTHWEST ON 18TH AVE



VIEW FROM CORNER OF E SPRUCE AND 18TH AVE



APPROACH FROM SOUTHEAST ON E SPRUCE ST

SCHEME 4: PREFERRED

PROPOSED LBA: SCHEME 4

LOT A - ROWHOUSES
 LOT SIZE: 5,283 sf
 ZONE: LR2
 ADDRESS: 212 18TH AVE
 EDG PROJECT: #3023583
 PROPOSED USE: 6 ROWHOUSES

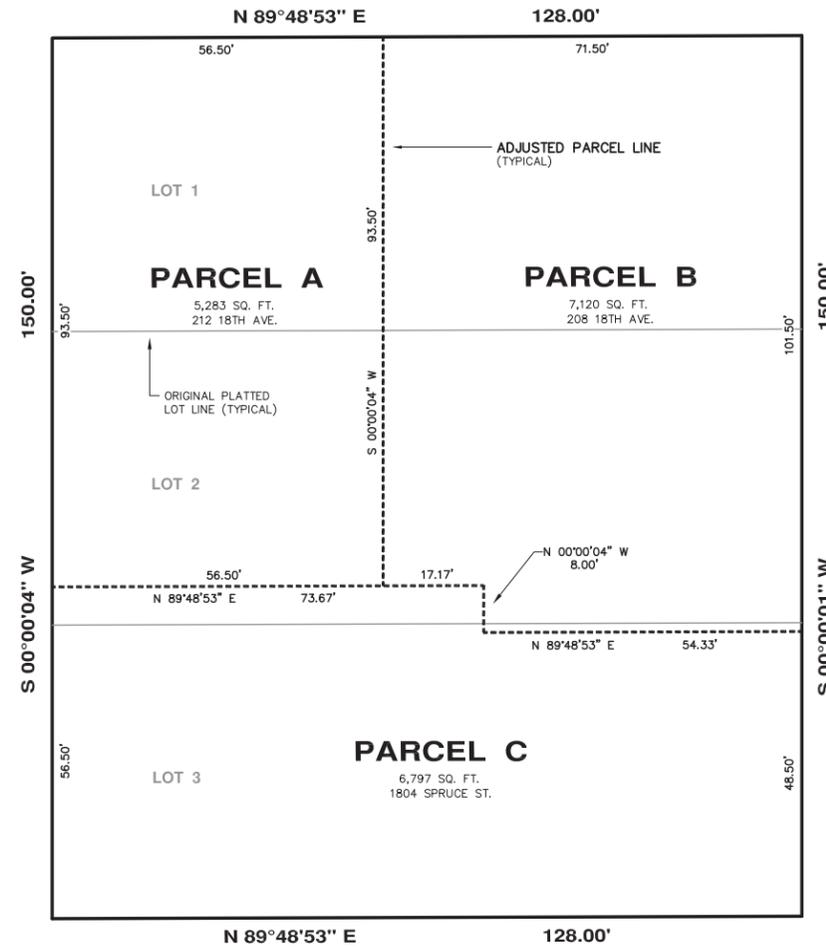
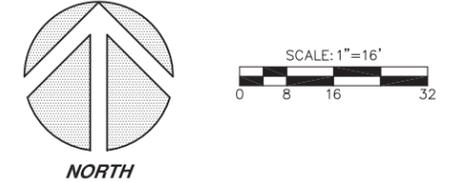
LOT B - TOWNHOUSES
 LOT SIZE: 8,544 sf
 ZONE: LR2
 ADDRESS: 208 18TH AVE
 EDG PROJECT: #3023584
 PROPOSED USE: 7 TOWNHOUSES

LOT C - ROWHOUSES
 LOT SIZE: 8,836 sf
 ZONE: LR2
 ADDRESS: 1804 E SPRUCE ST
 EDG PROJECT: #3023032
 PROPOSED USE: 8 ROWHOUSES

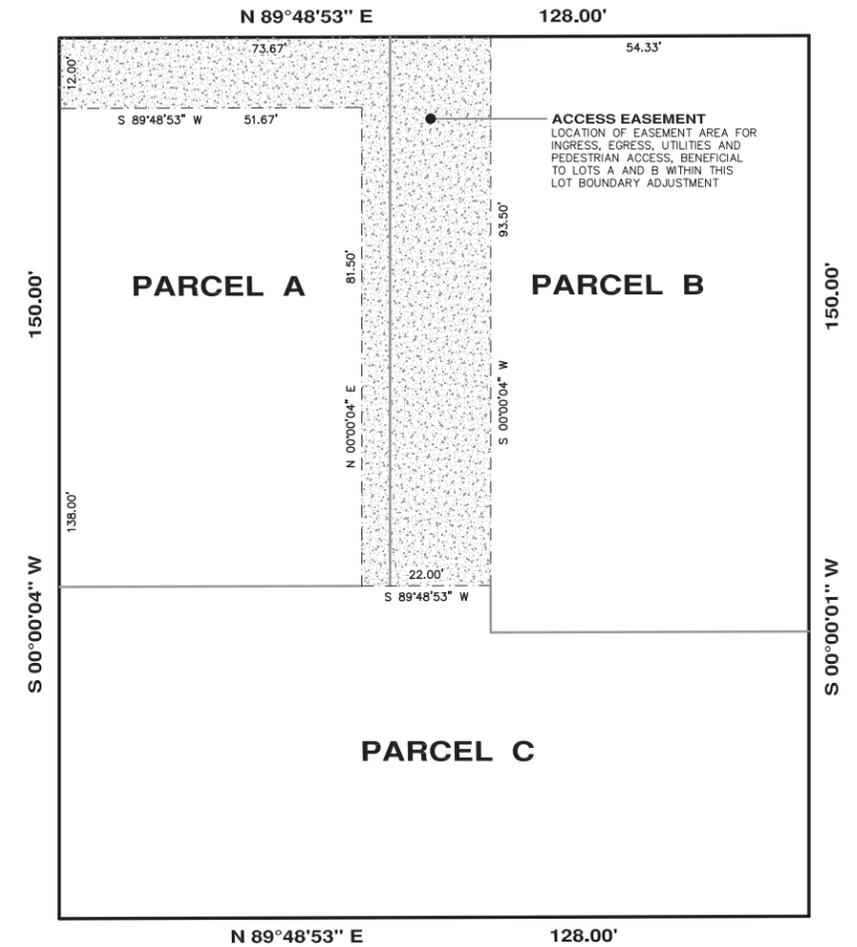
	ALLOWED	PROPOSED
FAR:	Lot A: 1.3 x 5,283 SF = 6,868 SF	6,868 SF
	Lot B: 1.2 x 7,120 SF = 8,544 SF	8,544 SF
	Lot C: 1.3 x 6,797 SF = 8,836 SF	8,836 SF
HEIGHT:	30' in LR2; 4' parapet; 10' penthouse	29.3'
BLDG LENGTH:	Lot A: 56.5' x 65% = 36.7'	34'-6"
	Lot B: 71.5' x 65% = 46.5'	34'-6"
	Lot C: 48.5' x 65% = 31.5'	31'-6"



LOT BOUNDARY ADJUSTMENT NO. 3022616 FOR ISOLA REAL ESTATE, LLC



ADJUSTED PARCEL DETAIL



EASEMENT DETAIL



CHADWICK WINTERS
 LAND SURVEYING AND MAPPING
 1422 N.W. 85TH ST., SEATTLE, WA 98117
 PHONE: 206.297.0996
 FAX: 206.297.0997
 WEB: WWW.CHADWICKWINTERS.COM

SURVEY IN:
 SW 1/4, SW 1/4, SEC. 33, T. 25 N., R. 4 E., W.M.
 KING COUNTY, WASHINGTON
 FOR: ISOLA REAL ESTATE, LLC

DRAWN BY: SAL	DATE: 11-24-15	PROJECT #: 15-5330
CHK. BY: RHW	SCALE: 1" = 16'	SHEET: 4 OF 5

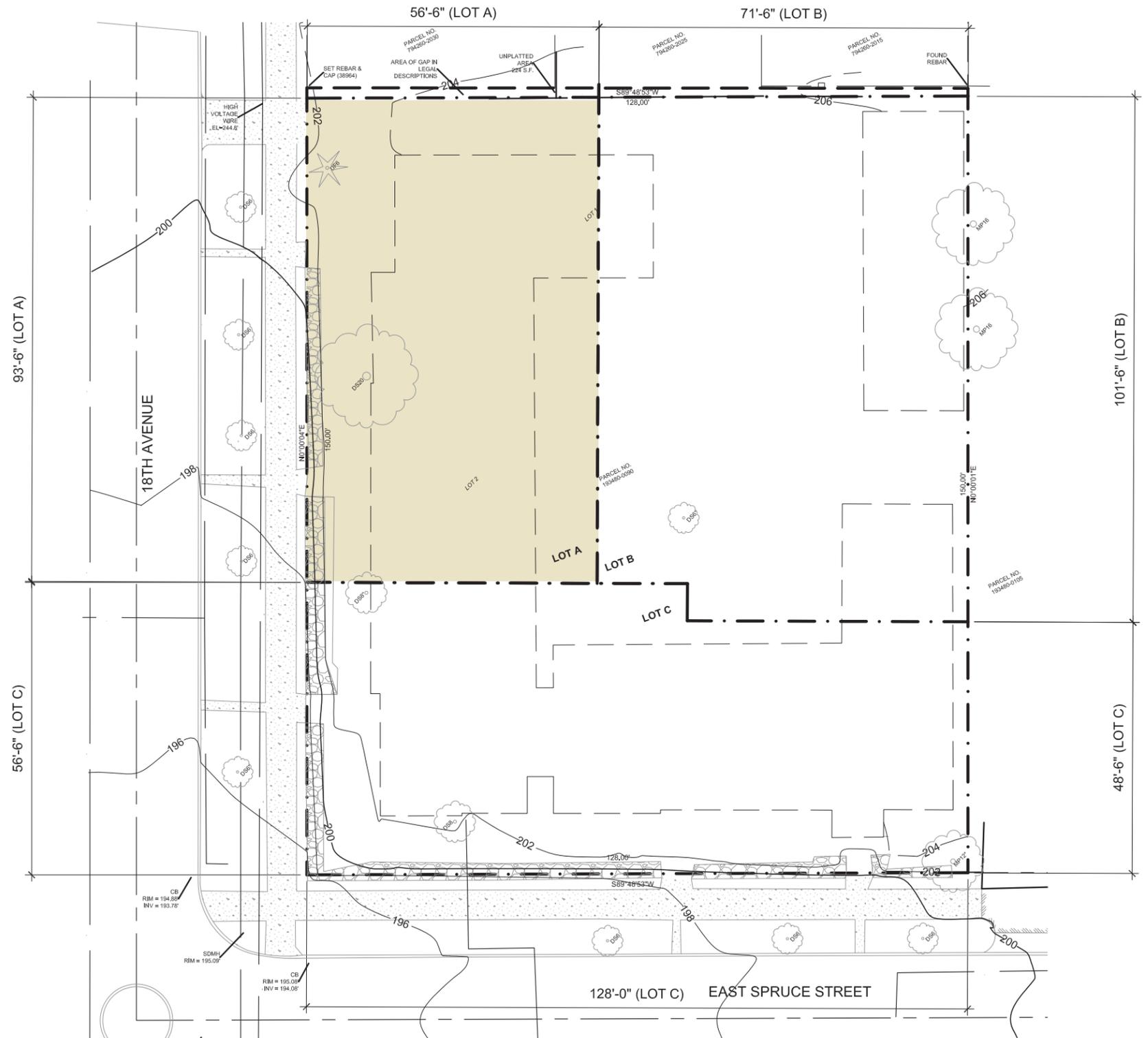
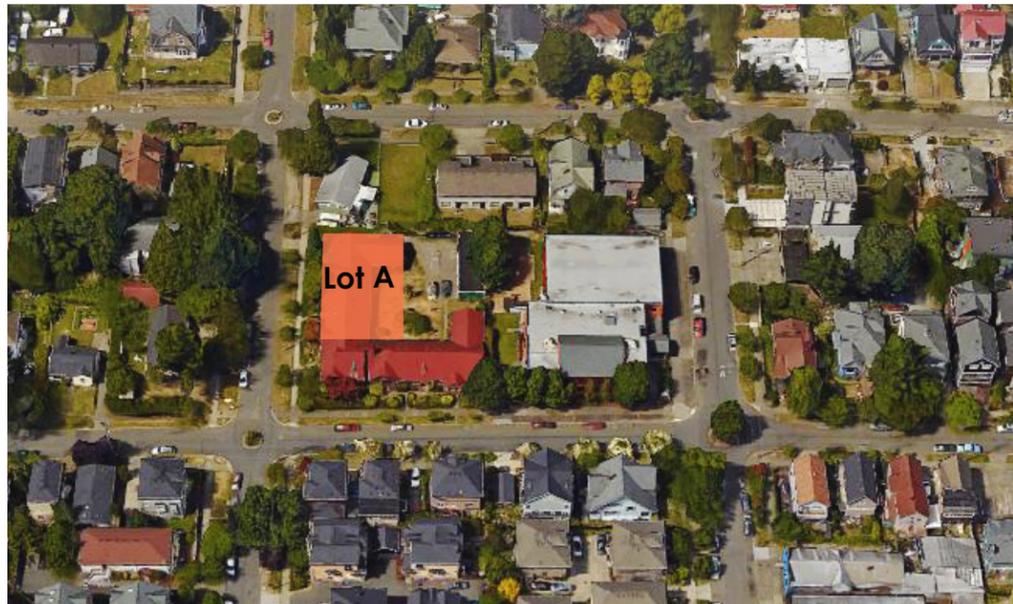
LOT BOUNDARY ADJUSTMENT

LOT A #3023583
LOT SIZE: 5,283 sf
ZONE: LR2
ALLOWED FAR: 5,283 sf x 1.3 = 6,868 sf
 (RHs + garages + green building)
SETBACKS: 5' front setback
 0' side setback (RH next to RH)
 3.5' side setback (RH next to residential)
 5' rear setback (7' average)
ALLOWED HEIGHT: 30' + 4' Parapet Allowance & 10' Penthouse
FACADE LENGTH: 34.5' (allowed 36.7')

LEGAL DESCRIPTION: THAT PORTION OF LOTS 1 AND 2, BLOCK 3, DEANS'S ADDITION TO THE CITY OF SEATTLE, ACCORDING TO THE PLAT THEREOF RECORDED UNDER VOLUME 1 OF PLATS, PAGE 206, RECORDS OF KING COUNTY, WA. BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE N.W. CORNER OF SAID LOT 1; THENCE N 89°48'53" E ALONG THE NORTH LINE OF SAID LOT FOR A DISTANCE OF 56.50 FT.; THENCE S 00°00'04" W, 93.50 FT.; THENCE S 89°48'53" W, 56.50 FT.; THENCE N 00°00'04" E, 93.50 FT. TO THE POINT OF BEGINNING.

SUBJECT TO AND TOGETHER WITH AN EASEMENT FOR INGRESS, EGRESS, UTILITIES AND PEDESTRIAN ACCESS AS SHOWN AND DESCRIBED AS "ACCESS EASEMENT" ON CITY OF SEATTLE LOT BOUNDARY ADJUSTMENT NO. 3022616.



LOT A - SIZE & FAR

LOT B #3023584

LOT SIZE: 7,120 sf

ZONE: LR2

ALLOWED FAR: 7,120 sf x 1.2 = 8,544 sf
(THs + garages + green building)

SETBACKS: 5' front setback (7' average)
5' side setback
5' rear setback (7' average)

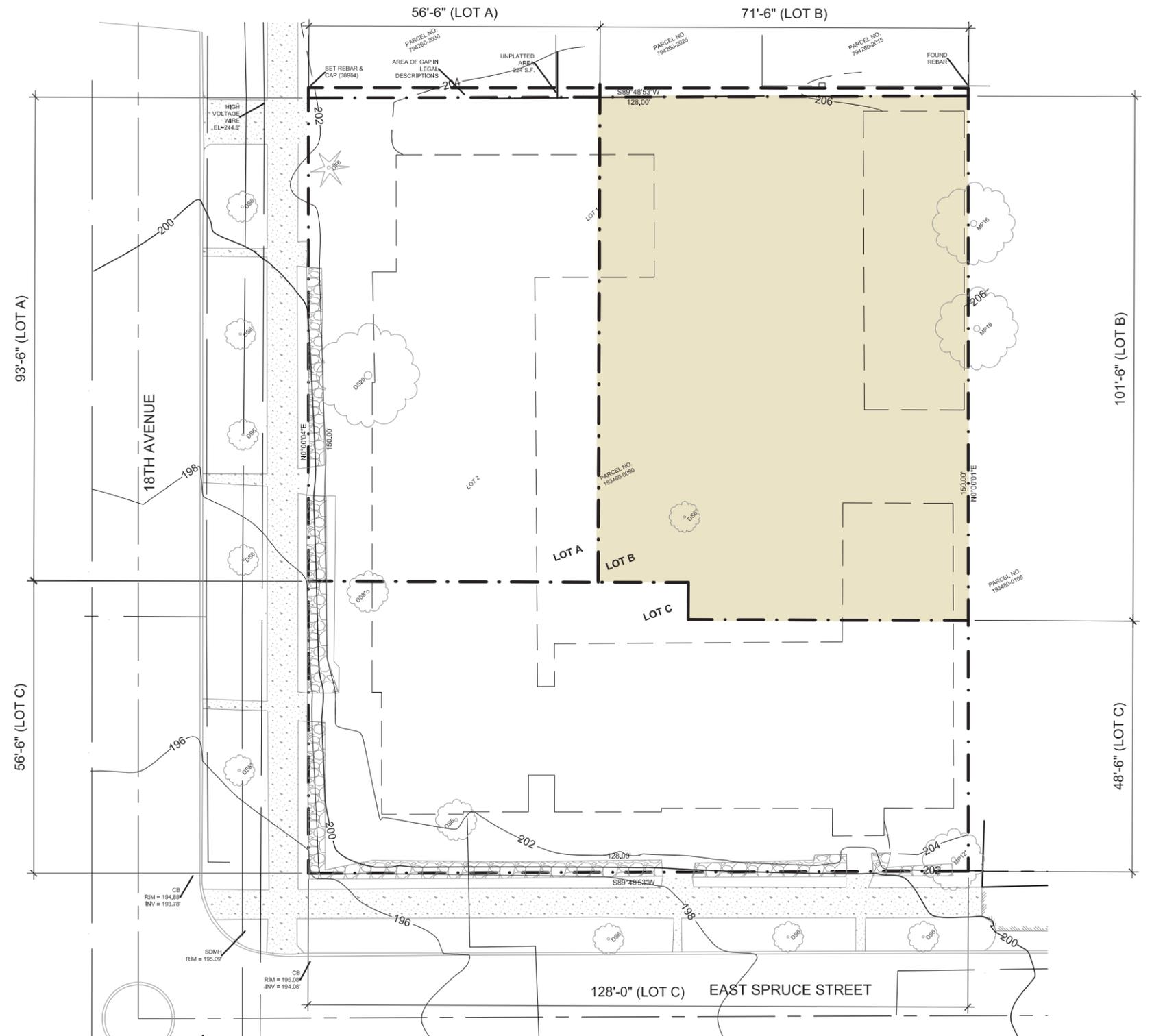
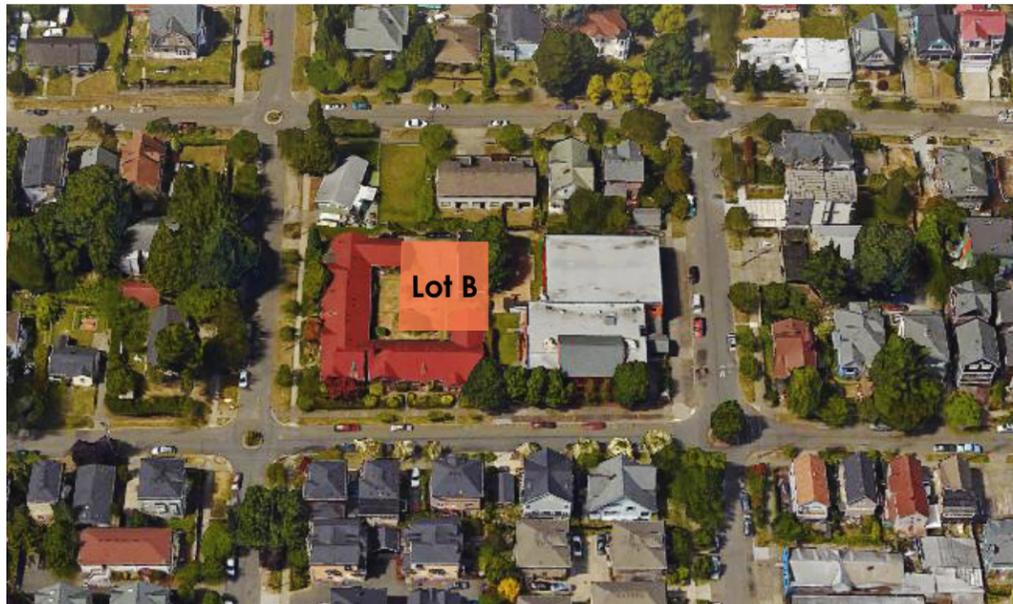
ALLOWED HEIGHT: 30' + 4' Parapet Allowance & 10' Penthouse

FACADE LENGTH: 71.5' x 65% = 46.5' allowed facade length
54.3' x 65% = 35.3' allowed facade length

LEGAL DESCRIPTION: THAT PORTION OF LOTS 1 AND 2, BLOCK 3, DEANS'S ADDITION TO THE CITY OF SEATTLE, ACCORDING TO THE PLAT THEREOF RECORDED UNDER VOLUME 1 OF PLATS, PAGE 206, RECORDS OF KING COUNTY, WA. BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE N.E. CORNER OF SAID LOT 1; THENCE S 00°00'01" W ALONG THE EAST LINE OF SAID LOT FOR A DISTANCE OF 101.50 FT.; THENCE S 89°48'53" W, 54.33 FT.; THENCE N 00°00'04" E, 8.00 FT.; THENCE S 89°48'53" W, 17.17 FT.; THENCE N 00°00'04" E, 93.50 FT.; THENCE N 89°48'53" E, 71.50 FT. TO THE POINT OF THE BEGINNING.

SUBJECT TO AND TOGETHER WITH AN EASEMENT FOR INGRESS, EGRESS, UTILITIES AND PEDESTRIAN ACCESS AS SHOWN AND DESCRIBED AS "ACCESS EASEMENT" ON CITY OF SEATTLE LOT BOUNDARY ADJUSTMENT NO. 3022616.



LOT B - SIZE & FAR

LOT C #3023032

LOT SIZE: 6,797 sf

ZONE: LR2

ALLOWED FAR: 6,797 sf x 1.3 = 8,836 sf
(RHs + garages + green building)

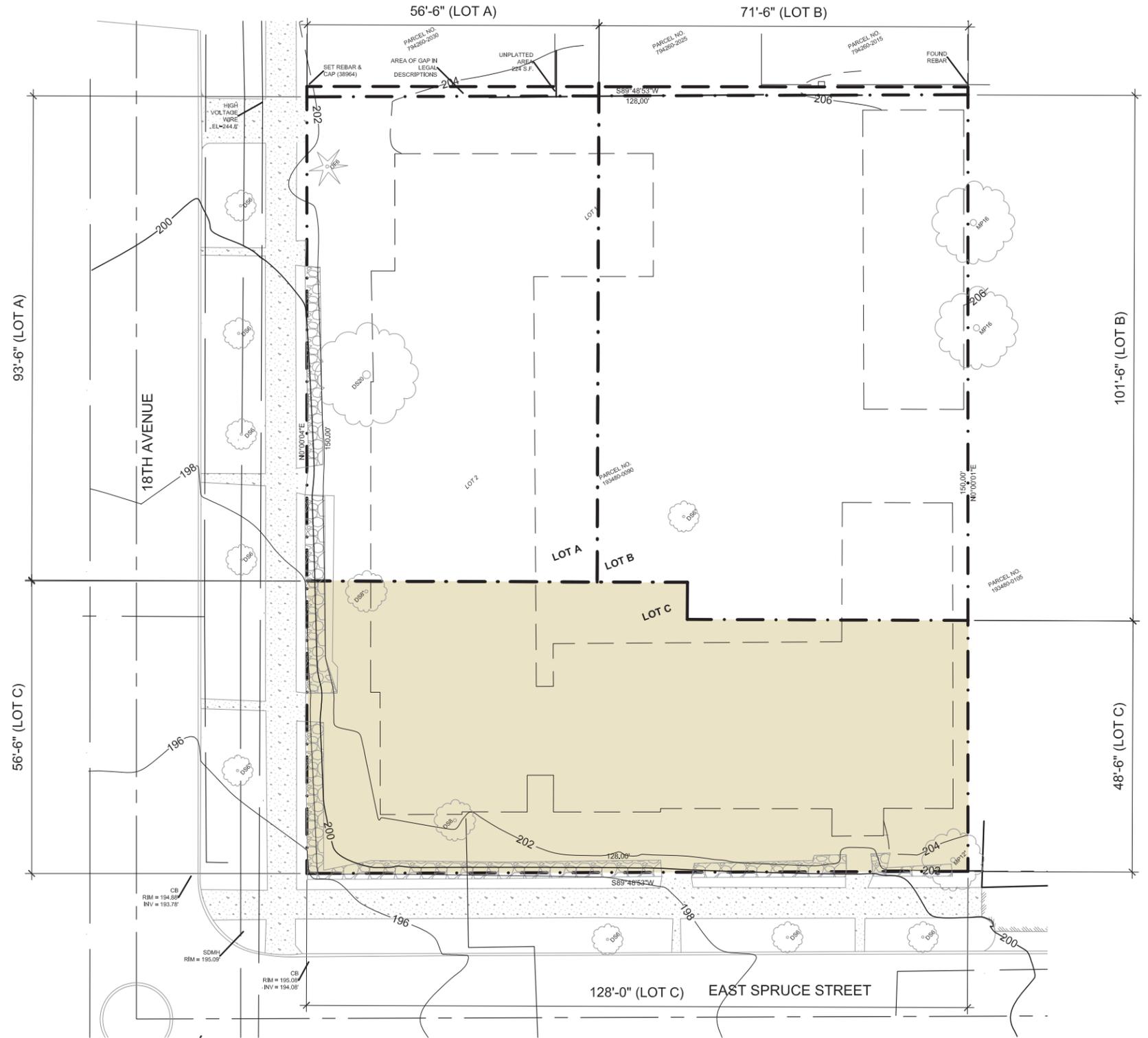
SETBACKS: 5' front setback
0' side setback (RH next to RH)
3.5' side setback (RH next to residential)
5' rear setback (7' average)

ALLOWED HEIGHT: 30' + 4' Parapet Allowance & 10' Penthouse

FACADE LENGTH: 31.5' (allowed 31.5')

LEGAL DESCRIPTION: THAT PORTION OF LOTS 1 AND 2, BLOCK 3, DEANS'S ADDITION TO THE CITY OF SEATTLE, ACCORDING TO THE PLAT THEREOF RECORDED UNDER VOLUME 1 OF PLATS, PAGE 206, RECORDS OF KING COUNTY, WA. BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE S.E. CORNER OF SAID LOT 3; THENCE S 89°48'53" W ALONG THE SOUTH LINE OF SAID LOT FOR A DISTANCE OF 128.00 FT.; THENCE N 00°00'04" E, 56.50 FT.; THENCE N 89°48'53" E, 73.67 FT.; THENCE S 00°00'04" E, 8.00 FT.; THENCE N 89°48'53" E, 54.33 FT.; THENCE S 00°00'01" W, 48.50 FT. TO THE POINT OF BEGINNING.



LOT C - SIZE & FAR

CONCEPT- A PLACE TO PAUSE

The building entries, nodes, and common amenity area are spaces that provide residents with a place to pause. This allows people to interact by creating physical spaces for activities. The foundation for a community is realized by developing a unifying aesthetic that creates both visual and physical links.

1. BUILDING ENTRIES



Jane Jacobs described stoops and porches as places that create social overlook. Oscar Newman referred to these features as 'symbolic boundaries' - a way for the resident to feel privacy while maintaining visual connection to the street. Our intention is to create a modern stoop that reaches back out to the community by:

- A. integrating natural surveillance,
- B. reinforcing the private/public realm,
- C. refocusing externally to the community as a whole.

2. NODES



Circulation drives the way people move through and interact with a site. It also has the power to activate a multitude of exterior spaces while transitioning site users from the public to the private realm of their own home. By excentuating the circulation cores with hardscape and landscape, we aim to:

- A. enforce clear circulation through and around the site,
- B. activate planned and designed shared space,
- C. buffer private space and site from neighbors.

3. COMMON AMENITY AREA



Common space is created to foster proximity, interaction, and accessibility. Different combinations of program will foster different natures of common space. For this project, the common amenity area will provide a strong link between residents by:

- A. energizing the site,
- B. acting as a unique design feature,
- C. focusing residents internally to maintain strong residential connections.

PROJECT CONCEPT

CS2

Adjacent Sites, Streets, and Open Spaces:

The project makes a strong connection to the street by integrating stoops with landscape buffers at each entry. The sidewalks are buffered from cars with street trees and the units are buffered from the sidewalk with landscaping and raised stoops.

PL1

Walkways and Connections:

The circulation around and within the site directs the way people move through and interact with the site. Multiple areas are connected transitioning the users from the public to the private realm.

PL3

Entries

The entries are paired with wide stoops to provide an intimate entry that also encourages interaction with neighbors. The stoops are clad in brick to hint back to the existing building on the site.

DC1

Parking and Service Uses:

The parking court is accessed at the north edge of the site, utilizing the existing curb cut. This provides a secure access point that is separate from the main circulation zone. The pathways in and around the site are secure from vehicular traffic.

DC3

Open Space Uses and Activities:

Open space is created to foster interaction and accessibility. The common space energizes the site and focuses the residents internally to maintain strong residential connections. Although focused internally, it also opens out generously to the public realm.

- Open Space/ Stoops ■
- Primary Circulation ■
- Buffers ■

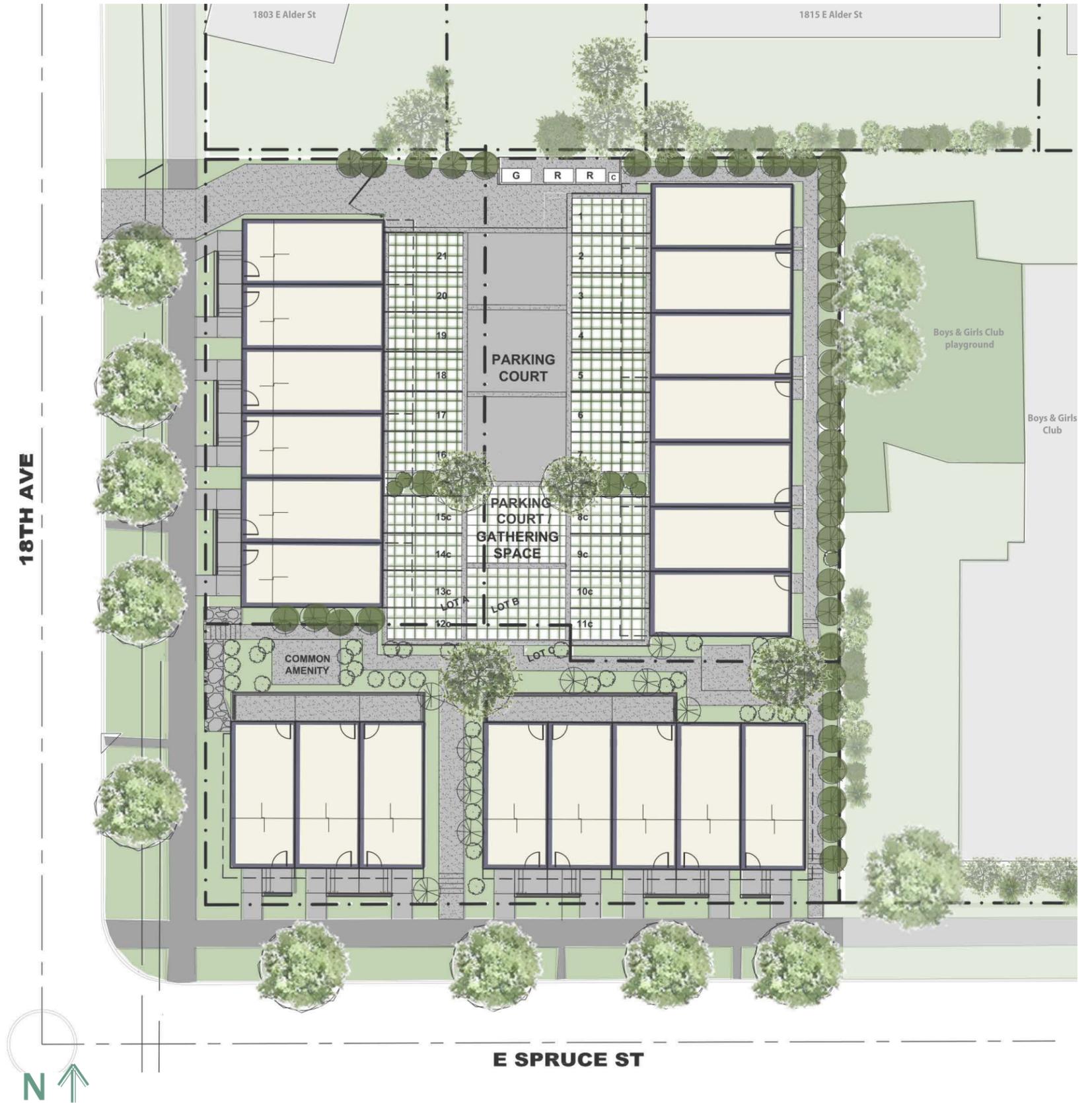


DESIGN GUIDELINE HIGHLIGHTS



The landscape for the project plays a significant role in activating the outdoor spaces. The driveway into the project needs to be functional yet appealing to drivers entering the site. The pedestrian paths also need to feel inviting. The common driveway is flanked with parking spaces that are intended to be permeable. The paving provides an attractive look to the inner parking court. The theme is then carried to the common amenity spaces. This helps with the infiltration on site and provides a flexible space for the community members to gather.

LANDSCAPE PLAN



LANDSCAPE CONCEPT

HOUSE
1-STORY (SF5000)

BOYS & GIRLS CLUB
2-STORIES (LR2)

- Vignette 3:**
Common Amenity
- Vignette 2:**
Entry Stoops
- Vignette 1:**
Building Programming



EXPERIENTIAL RENDERINGS



BOYS & GIRLS CLUB
2-STORIES (LR2)

Vignette 1: Building Programming
The south rowhouses along 18th Ave have been reorganized to have the living, dining, kitchen on the ground floor. The units have stoops with the featured brick cladding. The ground floor is able to be more active with eyes on the street and the glazing allows the building to reach back out to the neighborhood. Landscape buffers are integrated to alleviate the transition from public to private.

E SPRUCE ST STOOPS

VIGNETTES



18TH AVE STOOPS

Vignette 2: Entry Stoops

The entry stoops are now expressive of the existing building on the site. The paired entries are clad in brick and are accentuated with brick pathways. Planting buffers are integrated between the stoops and the sidewalk. The 5'-0" wide stoops encourage interaction between neighbors, act as an outdoor room, and enhance the transition from public to private.

VIGNETTES



COMMON AMENITY SPACE LOOKING TOWARDS 18TH AVE

Vignette 3: Common Amenity
The common amenity space is now a part on the intermediate zone which encompasses the stoops, the pathways and the common amenity space. All 3 enhance the user's experience in the site, promoting safe pathways to the unit entries, encouraging residents to engage in conversations and create a space for community interaction.

VIGNETTES

DRIVEWAY LOCATION:

The new driveway utilizes the existing curb cut - keeping the natural flow in and out of the site.



ENTRY STAIRS:

The new stoops mimic the existing apartment's entry sequence with stairs immediately ascending and then turning 90 degrees to the front door pathway.



BRICK:

The brick will be salvaged and reused at the entry stoops to create the entry sequence.



ROCKERY:

Rockery is sustained on the site to keep the common amenity space raised and inclusive to the residents.



The board made a comment regarding acknowledgement of the existing building's character through the use of material or proportions. We've taken aspects of the existing building's character and applied them to the proposed design. In particular, we looked at the existing flow in and out of the site, the entry sequence to the units, the existing rockery along the street edges, and the use of brick as a material.

ELEMENTS OF EXISTING BUILDING

SEATTLE DESIGN GUIDELINES

CS2. Urban Pattern and Form

Strengthen the most desirable forms, characteristics, and patterns of the streets, block faces, and open spaces in the surrounding area.

CS3. Architectural Context and Character

Contribute to the architectural character of the neighborhood.

PL1.Connectivity

Complement and contribute to the network of open spaces around the site and the connections among them.

PL3.Street-Level Interaction

Encourage human interaction and activity at the street-level with clear connections to building entries and edges.

DC1.Project Uses and Activities

Optimize the arrangement of uses and activities on site.

DC2.Architectural Concept

Develop an architectural concept that will result in a unified and functional design that fits well on the site and within its surroundings.

Adjacent Sites, Streets, and Open Spaces

B2 Connection to the Street: Identify opportunities for the project to make a strong connection to the street and carefully consider how the building will interact with the public realm. Consider the qualities and character of the streetscape -- its physical features (sidewalk, parking, landscape strip, street trees, travel lanes, and other amenities) and its function (major retail street or quieter residential street) -- in siting and designing the building.

Emphasizing Positive Neighborhood Attributes

A1 Fitting Old and New Together: Create compatibility between new projects, and existing architectural context, including historic and modern designs, through building articulation, scale and proportion, roof forms, detailing, fenestration, and/or the use of complementary materials.

A2 Contemporary Design: Explore how contemporary designs can contribute to the development of attractive new forms and architectural styles; as expressed through use of new materials or other means.

Walkways and Connections

B3 Pedestrian Amenities: Opportunities for creating lively, pedestrian oriented open spaces to enliven the area and attract interest and interaction with the site and building should be considered. Visible access to the building's entry should be provided. Examples of pedestrian amenities include seating, other street furniture, lighting, year-round landscaping, seasonal plantings, pedestrian scale signage, site furniture, art work, awnings, large storefront windows, and engaging retail displays and/or kiosks.

Entries

A2 Ensemble of Elements: Design the entry as a collection of coordinated elements including the door(s), overhead features, ground surface, landscaping, lighting, and other features.

Residential Edges

B2 Ground-level Residential: Privacy and security issues are particularly important in buildings with ground-level housing, both at entries and where windows are located overlooking the street and sidewalk. Consider providing a greater number of transition elements and spaces, and choose materials carefully to clearly identify the transition from public sidewalk to private residence.

Vehicular Access and Circulation

B1 Access Location and Design: Choose locations for vehicular access, service uses, and delivery areas that minimize conflict between vehicles and non-motorists wherever possible. Emphasize use of the sidewalk for pedestrians, and create safe and attractive conditions for pedestrians, bicyclists, and drivers...

Parking and Service Uses

C2 Visual Impacts: Reduce the visual impacts of parking lots, parking structures, entrances, and related signs and equipment as much as possible. Consider breaking large parking lots into smaller lots, and/or provide trees, landscaping or fencing as a screen. Design at-grade parking structures so that they are architecturally compatible with the rest of the building and streetscape.

Massing

A2 Reducing Perceived Mass: Use secondary architectural elements to reduce the perceived mass of larger projects. Consider creating recesses or indentations in the building envelope; adding balconies, bay windows, porches, canopies or other elements; and/or highlighting building entries.

DESIGN RESPONSE

By creating new entrances at existing elevations, we are mimicking the current apartment entry arrangement and maintaining neighborhood character. Street trees will buffer the resident from the street and other plantings will create an area in front of the entry for personalization and attractive street fronts.

The design proposes to reflect the scale, regulating lines, and texture of the existing residences on the streets. By setting the third floor back, a visual line is created that speaks to the neighboring buildings. The buildings step down with the slope to keep the visual height of the building as low as possible. The design will use awnings, material changes, and fenestrations to define the unit entries as well as create proportions and articulation that respond to the context of the neighborhood.

The east and west buildings will have flat roofs for roof decks with reduced penthouses to keep the scale of the buildings down. The south building will have open stairs and eliminates the penthouse. The contemporary design reinforces the rhythm along the street front which are articulated more with the entry stairs.

The proposed design features multiple pedestrian paths into the site from the sidewalk. The paths lead to a common amenity space that is inviting and flexible for the residents. The paths are lined with plantings to soften the edges. Each unit is expressed along the street with a stoop that leads from the sidewalk up to the units. The stairs create a space between the entry and the sidewalk to ease the transition from public to private. Plantings can be added to personalize each entry and soften the hardscapes.

Street-level residential entries will be clearly articulated with modulation, exterior finishes, and pedestrian scaled entry elements. Entries will feature stoops with landscape buffers to reinforce the public/private threshold and distinguish each individually entry as its own.

Ground-level residential entries are raised above the existing grade with stoops for each unit. The stairs provide a transition from public to private while also giving an architectural feature to the building. The area between the stairs and the sidewalk will be planted to provide a nice landscape as well as privacy and noise control.

The parking for all residences is located in the middle of the site. It is not visible from the street and provides safe access for the residents to park their vehicles. Pedestrian access is located away from the driveway to limit the interaction between pedestrians and vehicles. The hardscape will consist of varying materials including grasscrete which delineates the parking area while providing a flexible space that blends into the common area and is aesthetically pleasing.

The parking for all residences is located in the middle of the site. It is not visible from the street and provides safe access for the residents to park their vehicles. The shared trash area is also located to the side of the driveway for easy pick-up. This area will be fenced to screen it from neighbors, pedestrians, and the street. The driveway entrance is reduced in width to limit the visual impact of cars entering and exiting the site. It is pushed to the north portion of the site to keep vehicle and pedestrian interaction to a minimum.

The preferred design scheme steps the massing of the structures back at the third story for the west and east buildings. The south building is reversed to allow for larger stoops. This staggered massing allows for a diminished visual presence while still reaching the intended density of the site's zoning. The penthouses for the roof deck are pushed back to limit the visual appearance from the street. Awnings are also provided at each entry.

DESIGN GUIDELINES



MARCH/
SEPTEMBER 21

9am



JUNE 21

9am



DECEMBER 21

9am



MARCH/
SEPTEMBER 21

12pm



JUNE 21

12pm



DECEMBER 21

12pm



MARCH/
SEPTEMBER 21

3pm



JUNE 21

3pm



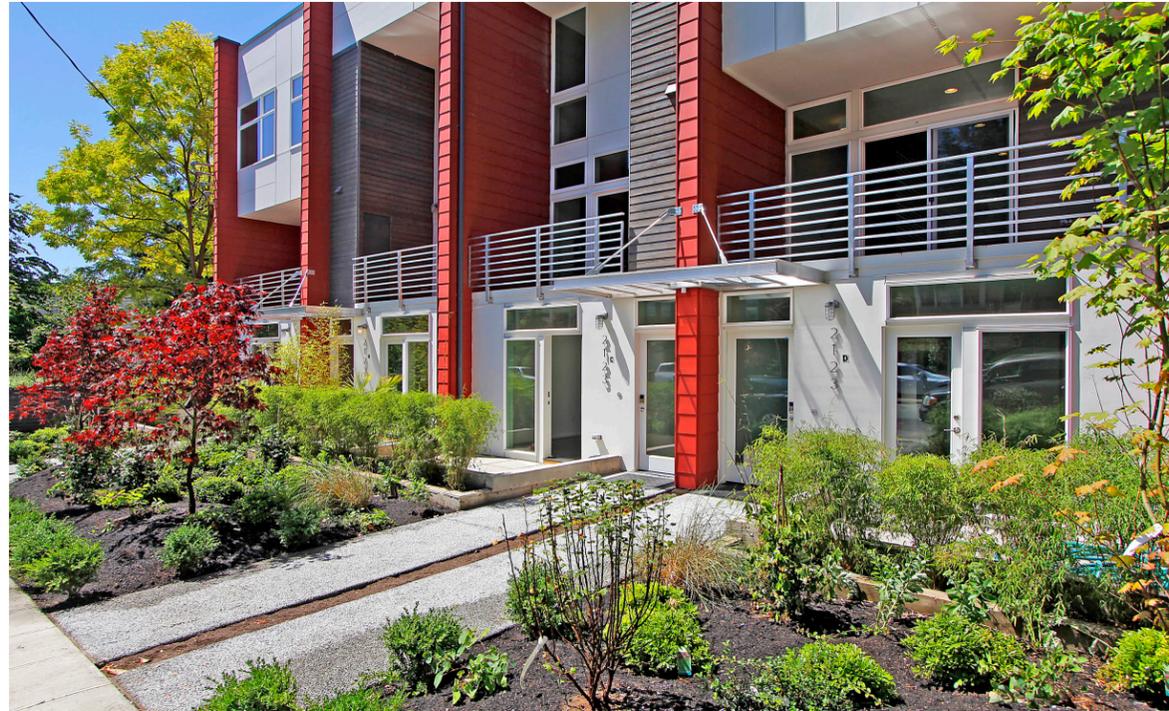
DECEMBER 21

3pm

SHADOW DIAGRAMS - SCHEME 4: PREFERRED



NATURAL WOOD ACCENTS



ENTRY ELEMENTS



LARGE GLAZING NEXT TO FRONT DOORS



PAVERS FOR PATHWAYS



VERTICAL MATERIALS TO CREATE RHYTHM



PANEL SIDING TO ACCENTUATE MODULATION

RECENT PROJECTS AND MATERIAL EXAMPLES

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ADDITIONAL RENDERINGS



ADDITIONAL RENDERINGS