



The Maritime Building

911 Western Avenue, Seattle, Washington

Design Recommendation Submittal

Project # 3022652 June 21, 2016



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Beacon Capital Partners proposes to transform one of Seattle's most significant and recently landmarked waterfront buildings into an iconic class A office building. Located along Alaskan Way, the New Maritime Building is positioned at the heart of Seattle's new re-imagined waterfront and will provide its tenants with spectacular views of the waterfront and the Olympic Mountains.

The mixed-use development in this proposal consists of:

Complete and substantial renovation of the existing Maritime Building – approximately 185,200 GSF existing office space.

Addition of two new office floors – approximately 48,000 GSF.

Addition of a rooftop amenity pavilion.

Landscaped terraces on level 7 and the rooftop Street level retail of approximately 23,000 GSF.

Total development is approximately 240,000 GSF.

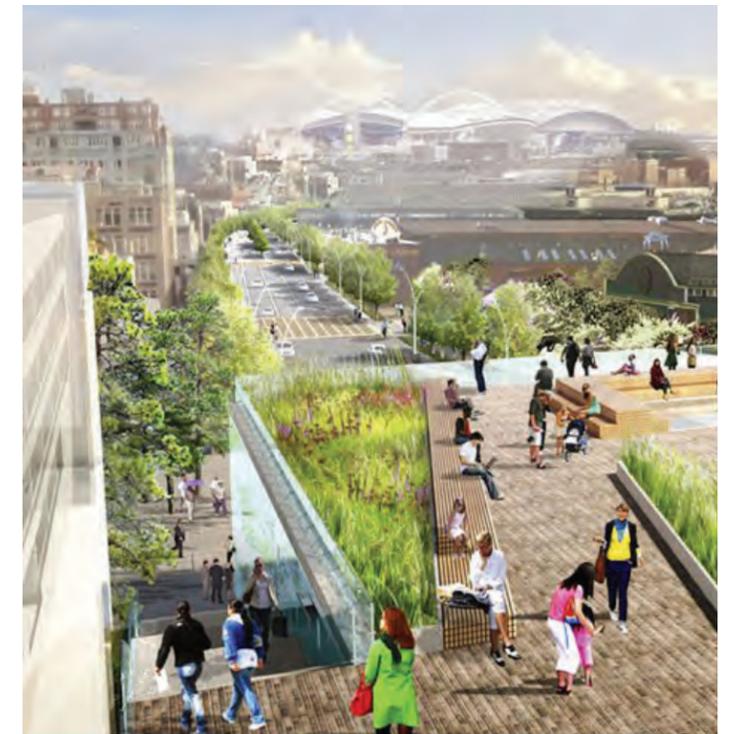
Approximately 75 parking stalls located on the existing basement level.

2.0

DESIGN VISION

RE-IMAGINED WATERFRONT

A new era begins for the Seattle waterfront. It is being transformed into a vibrant public space from Pioneer Square to Belltown and the newly re-imagined Maritime Building is at its heart. The ultimate urban convergence, the Maritime Building's central address along Seattle's waterfront is walking distance to the city's most popular urban neighborhoods and landmarks.



2.0

DESIGN VISION

DESIGN VISION

Since 1910, the Maritime Building has played a central role in the commercial district along Seattle's waterfront. In early 2016, the Maritime Building was designated a Landmark by the Seattle Preservation Landmark Board.

Our vision for the future of this simple heavy timber warehouse is to recreate the building's function along the waterfront and support the West Edge Neighborhood and Seattle's vision for its newly re-imagined public open space. To accomplish this goal, a complete and substantial renovation of the existing building is planned.

Along with the addition of 2 floors of office space, a rooftop amenity pavilion and ground level retail, the Maritime Building will be transformed into an active and vibrant structure. Responding to the new waterfront, the modernized building will realign itself and become an outward facing development by relocating its original address from Western Avenue to the new Alaskan Way. A new lobby for the commercial office space will be placed on Alaskan Way along with active food and beverage retail environments and outdoor dining overlooking the new waterfront. All sides of the building will be carefully designed to assure active uses along all street frontages.



2.0

DESIGN VISION

WESTERN AVENUE - RETAIL



2.0

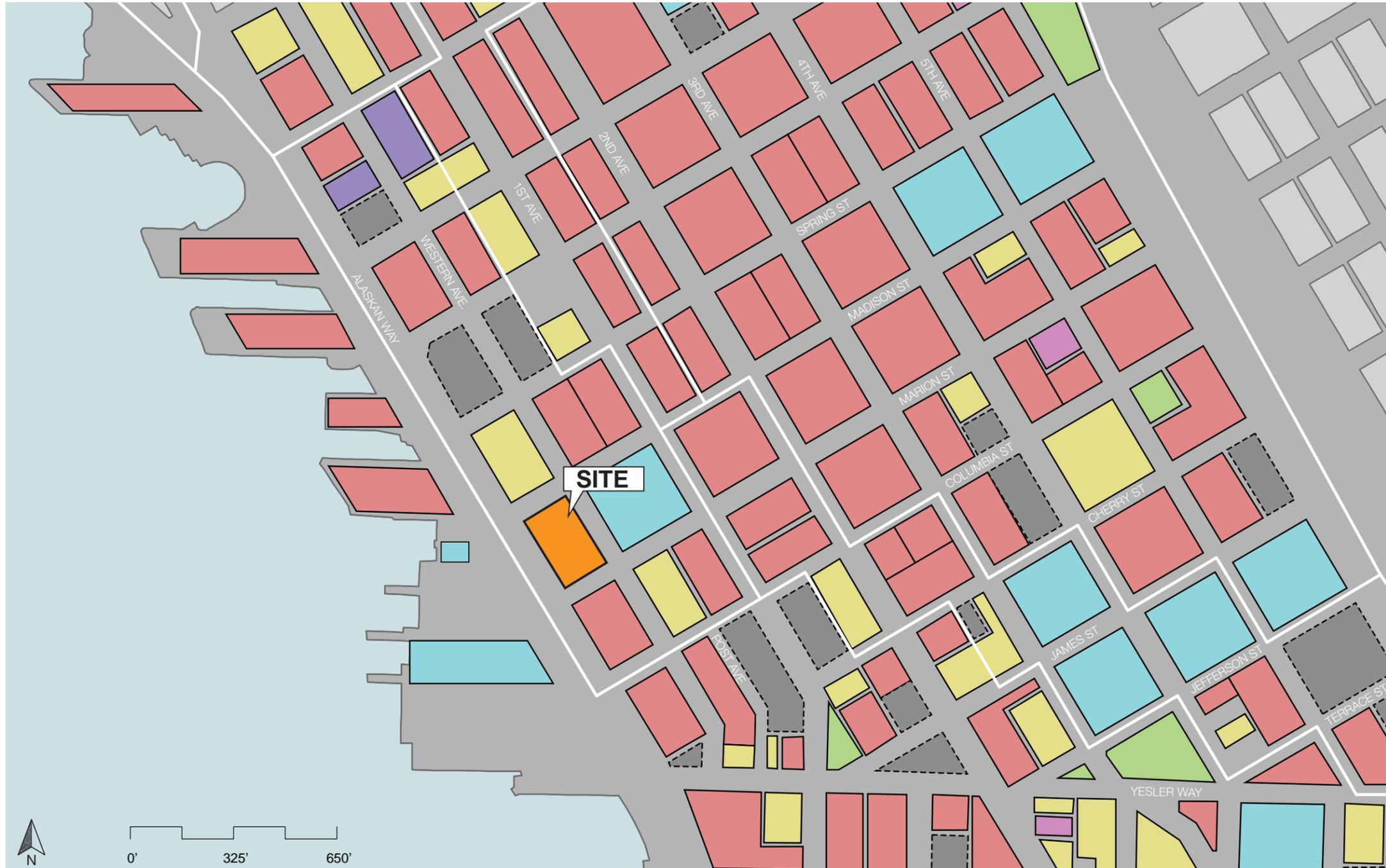
DESIGN VISION

ALASKAN WAY - DINING TERRACE



3.0

SUMMARY CONTEXT ANALYSIS



SURROUNDING USES

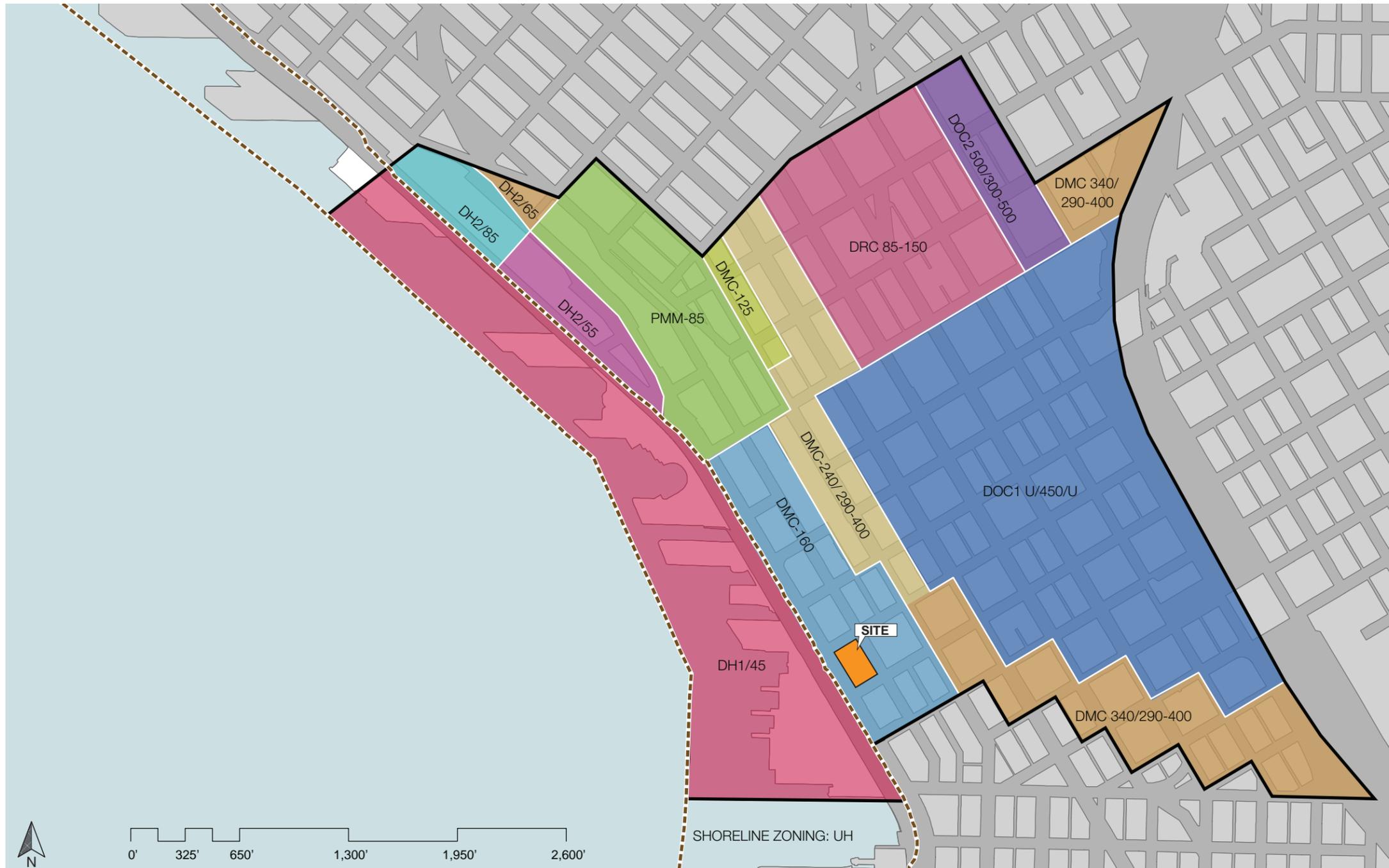
The project is located in the heart of Seattle's rapidly changing waterfront neighborhood and across the street from Elliott Bay. The Maritime Building is surrounded by various building uses, predominantly mixed-use structures with retail on the ground level and either residential or commercial space above. The Maritime Building is across the street from the Ferry Terminal at Colman Dock. There are no adjacent religious or utility buildings.

MAP KEY

Residential	
Commercial	
Government	
Utility	
Parking	
Religious	
Park	

SUMMARY CONTEXT ANALYSIS

The project is located in downtown Seattle's DMC-160 zone, along the new Waterfront Development currently under construction and across Alaskan Way from the Downtown Harborfront and Shoreline zoning.



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SUMMARY CONTEXT ANALYSIS

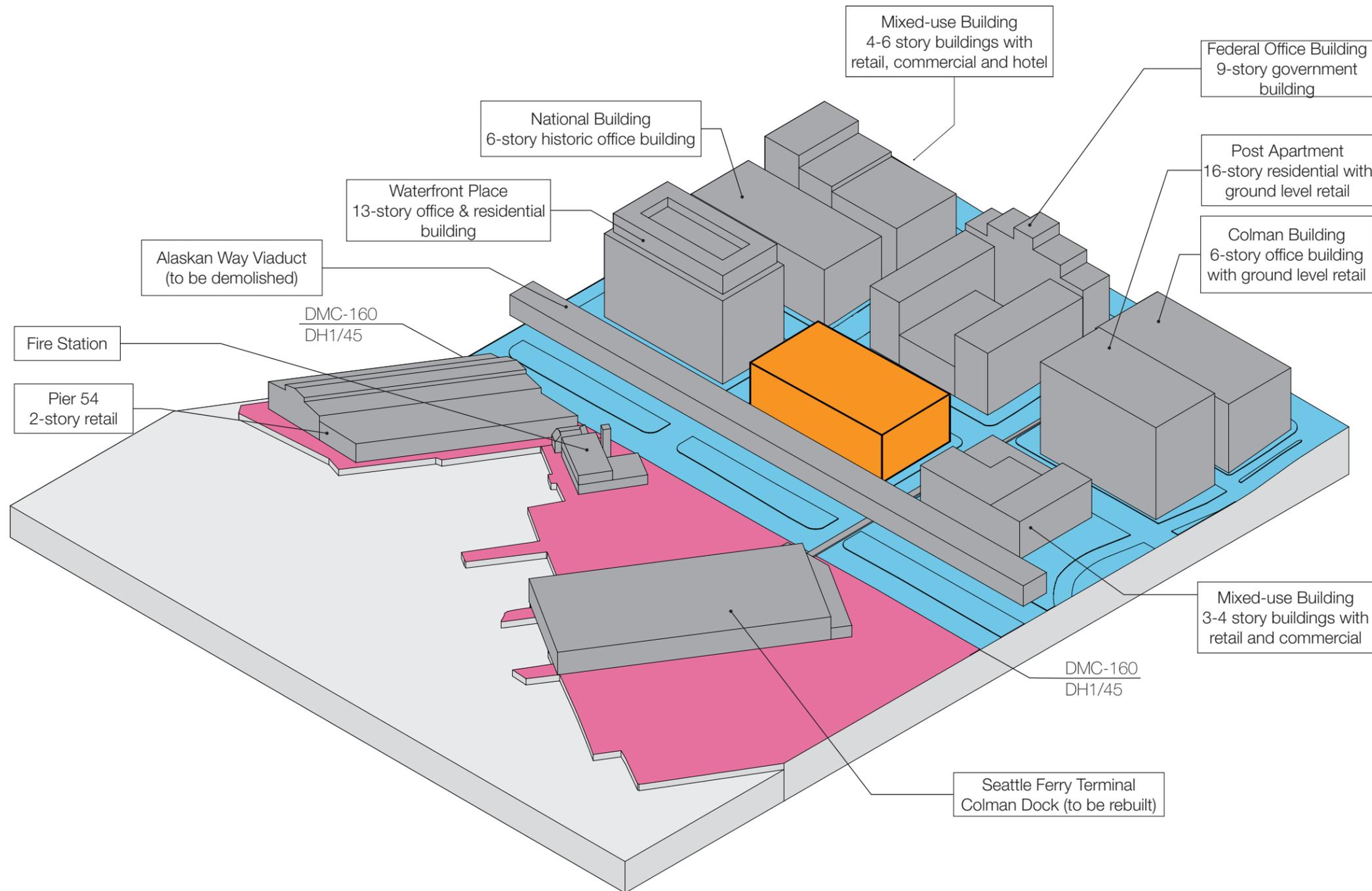
SITE AERIAL VIEW



SUMMARY CONTEXT ANALYSIS

9 BLOCK AXONOMETRIC: ZONING AND USES

This 9 block axonometric illustrates a combination of site zoning and detailed descriptions of adjacent building uses and heights.



4.0

LAND-USE CODE ANALYSIS

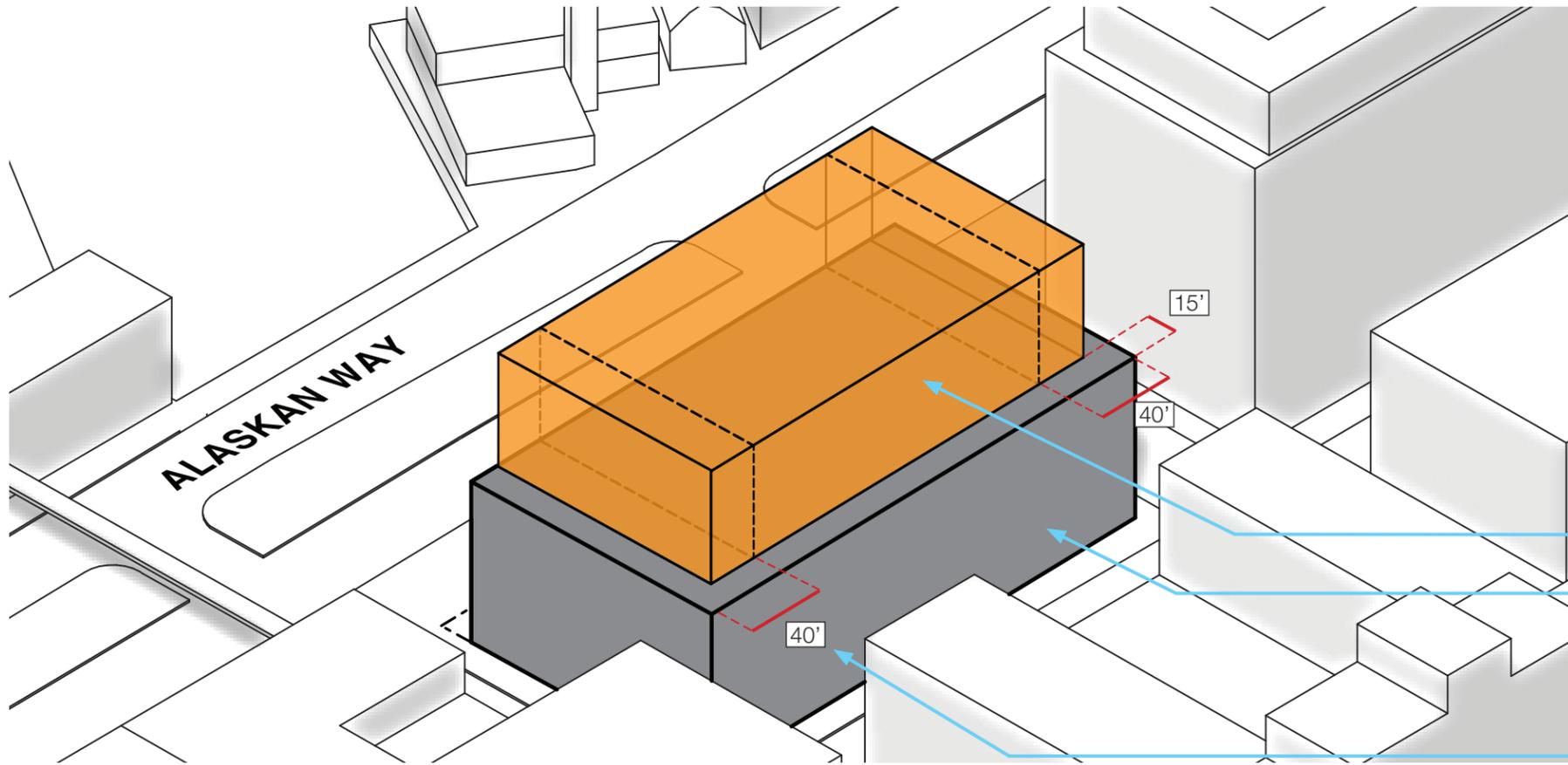
Zoning Data Compliance Table
Downtown Mixed Commercial (DMC 160)

Code Section	Subject	Requirements Summary	Proposal – MUP	Proposal – HALA option
SUBCHAPTER I – GENERAL PROVISIONS				
23.49.008	Structure height.	<ul style="list-style-type: none"> 160' Additional 5' height is permitted subject to street-level use requirements per 23.49.008.e. 	Structure height proposed is within the 160' height limit.	Structure height proposed is within the 160' height limit.
23.49.009	Street-level use requirements.	<ul style="list-style-type: none"> Per map 1G: Street Level Uses required on: <ul style="list-style-type: none"> Alaskan Way Along Alaskan Way, a minimum of seventy-five (75) percent of street frontage at street-level requires street-level uses. A 16' setback from Alaskan Way is provided per 23.49.056.B.1.d 	<ul style="list-style-type: none"> 75% of Alaskan Way frontage will contain Street Level Uses The existing building abuts the setback along Alaskan Way. 	<ul style="list-style-type: none"> 75% of Alaskan Way frontage will contain Street Level Uses The existing building abuts the setback along Alaskan Way.
23.49.011	Floor area ratio.	<ul style="list-style-type: none"> Base FAR = 5 Maximum available FAR under current zoning = 5 	Site Area = 35,988 SF FAR chargeable SF = 179,696 SF Proposed FAR = 5	Site Area = 35,988 SF FAR chargeable SF = 215,928 SF Proposed FAR = 6
23.49.016	Open space.	Office use over 85,000 SF must provide open space for workers at 20 SF per 1000 SF of office space.	Gross Office Floor Area Proposed = 185,986 SF Open Space required = 185,696 / 1,000 * 20 = 3,714 SF.	Gross Office Floor Area Proposed = 223,485 SF Open Space required = 223,485 / 1,000 * 20 = 4,470 SF.
23.49.018	Overhead weather protection and lighting.	A. Continuous overhead weather protection shall be required for new development.	The development is an addition to an existing building. Overhead weather protection will be provided at building entrances and along Alaskan Way frontage.	The development is an addition to an existing building. Overhead weather protection will be provided at building entrances and along Alaskan Way frontage.
23.49.019	Parking quantity, location and access requirements, and screening and landscaping of surface parking areas.	No parking is required in this DMC zone. Maximum parking for nonresidential uses = 1 space per 1000 sf.	<p>Maximum parking:</p> <ul style="list-style-type: none"> Office square footage (nonresidential use) = 185,986sf Retail square footage (nonresidential use) = 23,135sf Total area in nonresidential use = 210,433sf Maximum parking for nonresidential use = 210,433sf/1000 = 211 spaces. Proposed parking = approx. 85 spaces total. 	<p>Maximum parking:</p> <ul style="list-style-type: none"> Office square footage (nonresidential use) = 223,485 sf Retail square footage (nonresidential use) = 23,135sf Total area in nonresidential use = 246,620sf Maximum parking for nonresidential use = 246,620sf/1000 = 246 spaces. Proposed parking = approx. 85 spaces total.
		<p>Bicycle parking:</p> <ul style="list-style-type: none"> 1 space per 5,000sf of office use. 0.5 spaces per hotel room. 1 space per each 5,000sf of retail use over 10,000sf. 1 space for every 2 dwelling units. After first 50 spaces are provided for a use, additional spaces are required at one half (½) the ratio shown in Table 23.49.019 A as. <p>Bicycle commuter shower facilities:</p> <ul style="list-style-type: none"> 1 shower per gender for every 250,000sf of office use. <p>Off-street loading per requirements of 23.54.035.</p> <p>Access to parking: Location per Director's decision</p> <p>Curb cut width and number per requirements of 23.54.030.</p>	<p>Bicycle Parking:</p> <ul style="list-style-type: none"> Office use = 185,986 / 5,000 = 38 spaces required. Retail use = (23,135-10,000) / 5,000 = 3 spaces required. Total spaces required = 38 + 3 = 41 spaces. <p>Bicycle commuter shower facilities not required since office area is less than 250,000 sf.</p>	<p>Bicycle Parking:</p> <ul style="list-style-type: none"> Office use = 223,485 / 5,000 = 45 spaces required. Retail use = (23,135-10,000) / 5,000 = 3 spaces required. Total spaces required = 45 + 3 = 48 spaces. <p>Bicycle commuter shower facilities not required since office area is less than 250,000 sf.</p>
23.49.022	Minimum sidewalk width.	Minimum sidewalk widths per map 1C	Proposal complies with minimum sidewalk widths.	Proposal complies with minimum sidewalk widths.
23.49.024	View corridor requirements	40' setback from street property lines required above 60' elevation along Marion and Madison Streets.	Per 23.42.118 Landmarks Preservation Board allowed non-conformity since the proposed massing preserves the integrity of the landmark structure.	Per 23.42.118 Landmarks Preservation Board allowed non-conformity since the proposed massing preserves the integrity of the landmark structure.
23.49.025	Odor, noise, light/glare, and solid waste recyclable materials storage space standards	See code section for specific requirements. Solid waste and recyclable materials storage space per requirements of 23.54.040.	<ul style="list-style-type: none"> Exterior and interior lighting will be shielded to minimize glare to adjacent uses. Venting of odors will be at least 10 feet above finished grade and directed away from residential uses within 50 feet of the vent. No major noise generator uses are proposed. 	<ul style="list-style-type: none"> Exterior and interior lighting will be shielded to minimize glare to adjacent uses. Venting of odors will be at least 10 feet above finished grade and directed away from residential uses within 50 feet of the vent. No major noise generator uses are proposed.

SUBCHAPTER II – Downtown Mixed Commercial				
23.49.056	Downtown Mixed Commercial: Street facade, landscaping, and street setback requirements.	<p>Street Classification per map 1F:</p> <ul style="list-style-type: none"> Marion – Green Street Madison – Class II Western – Class I Alaskan – Class I <p>Per map 1H, Property Line Facades are required on:</p> <ul style="list-style-type: none"> Western Avenue Alaskan Way <p>Minimum façade height:</p> <ul style="list-style-type: none"> 35 feet along Western Avenue and Alaskan Way (property line facades) 25 feet along Marion (Green street) 15 feet along Madison (Class II Pedestrian Street) Façade setback limits apply per 23.49.056.B.1 <p>General setback limits apply on streets not requiring property line facades per 23.49.056.B.2 (i.e. University Street).</p> <p>Façade Transparency:</p> <ul style="list-style-type: none"> Minimum 60 percent transparency required on Class I pedestrian streets and designated green streets. Minimum 30 percent transparency required on Class II pedestrian streets. <p>Blank façade limits:</p> <ul style="list-style-type: none"> Green streets and Class I Pedestrian streets: 15 feet wide maximum; total width less than 40% of street frontage. Class II Pedestrian Streets: 30 feet wide maximum; total not to exceed 70% of street frontage. <p>Street trees are required on all streets that have a pedestrian classification and abut a lot.</p>	<p>Façade Height and Setbacks:</p> <ul style="list-style-type: none"> Existing building is in compliance. <p>Façade Transparency:</p> <ul style="list-style-type: none"> Proposal will meet façade transparency requirements. <p>Blank Facades:</p> <ul style="list-style-type: none"> Proposal will meet blank façade requirements. <p>Street trees have been provided; see Landscape plans.</p>	<p>Façade Height and Setbacks:</p> <ul style="list-style-type: none"> Existing building is in compliance. <p>Façade Transparency:</p> <ul style="list-style-type: none"> Proposal will meet façade transparency requirements. <p>Blank Facades:</p> <ul style="list-style-type: none"> Proposal will meet blank façade requirements. <p>Street trees have been provided; see Landscape plans.</p>
23.49.058	Downtown Mixed Commercial: Upper-level development standards	<p>Facade modulation per Table 23.49.058A is required above a height of sixty (60) feet above the sidewalk for any portion of a structure located within fifteen (15) feet of a street property line. No modulation is required for portions of a facade set back fifteen (15) feet or more from a street property line.</p> <p>Any portion of a facade exceeding the maximum length of facade prescribed on Table 23.49.058A shall be set back a minimum of fifteen (15) feet from the street property line for a minimum distance of sixty (60) feet before any other portion may be within fifteen (15) feet of the street property line.</p>	<p>Façade Modulation:</p> <ul style="list-style-type: none"> No departures requested. 	<p>Façade Modulation:</p> <ul style="list-style-type: none"> Western Avenue: departure requested for façade length exceeding 125' above 60'.
Chapter 23.54 - QUANTITY AND DESIGN STANDARDS FOR ACCESS, OFF-STREET PARKING, AND SOLID WASTE STORAGE				
23.54.035	Loading berth requirements and space standards.	See calculations in column on right.	<p>Office Use 185,986 sf LOW 3 spaces</p> <p>Retail Use 23,135 sf MEDIUM 1 space</p> <p>Due to site constraints; loading is proposed from Madison Street.</p>	

4.0

BUILDING ENVELOPE



General Site Information

Overall lot: 35,940 SF
 Site dimensions: 240' x 150'
 Development Potential: FAR=5
 Zone: DMC-160
 Max Building Height: 160'

Proposed Building Envelope

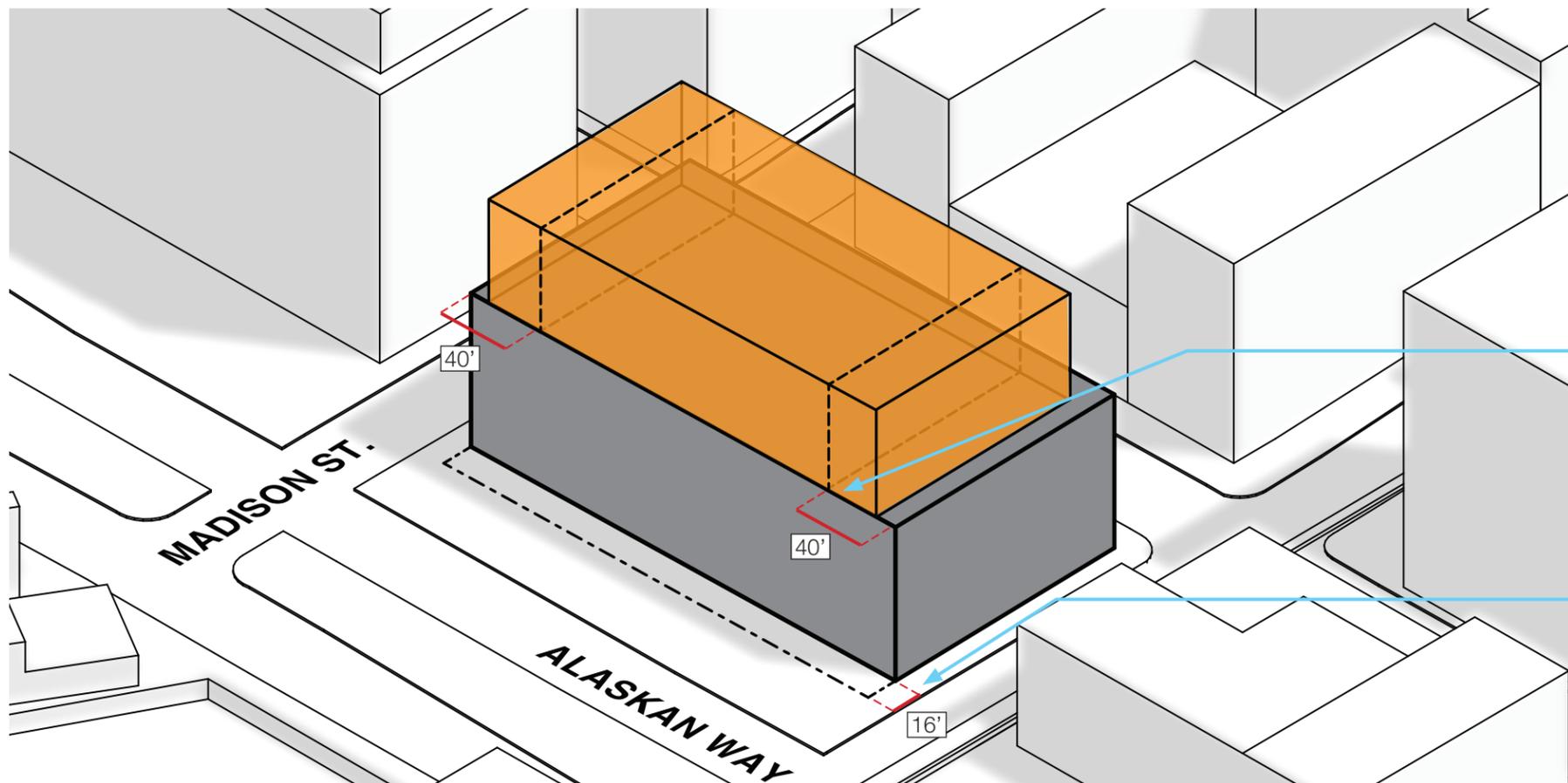
Existing Building Information

Total SF: 173,000 GSF (5 levels)
 Current use: Office, Retail
 Structure: Heavy timber

View Corridors

40' set back is required for elevation above 60' for lots from Post Alley to Alaskan Way

Per 23.42.118 Landmarks Preservation Board allowed non-conformity since the proposed massing preserves the integrity of the landmark structure.



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Street Level Façade

Setback abut Alaskan Way in DMC-160
 A continuous setback of up to 15' from the lot line abutting Alaskan Way is allowed (the existing building is setback 16' from the lot line.)

4.0

SITE ANALYSIS

PROJECT SURVEY

Site area:

35,940 SF with approximately 240 feet of frontage on Western Avenue and Railroad Avenue S., and approximately 150 feet of frontage along Madison Street and Marion Street.

Topography:

The site is relatively flat along the Madison Street, Western Avenue, and Marion Street frontages. The grade drops towards the west to allow for the existing loading dock on the Alaska Way frontage.

Tree Survey:

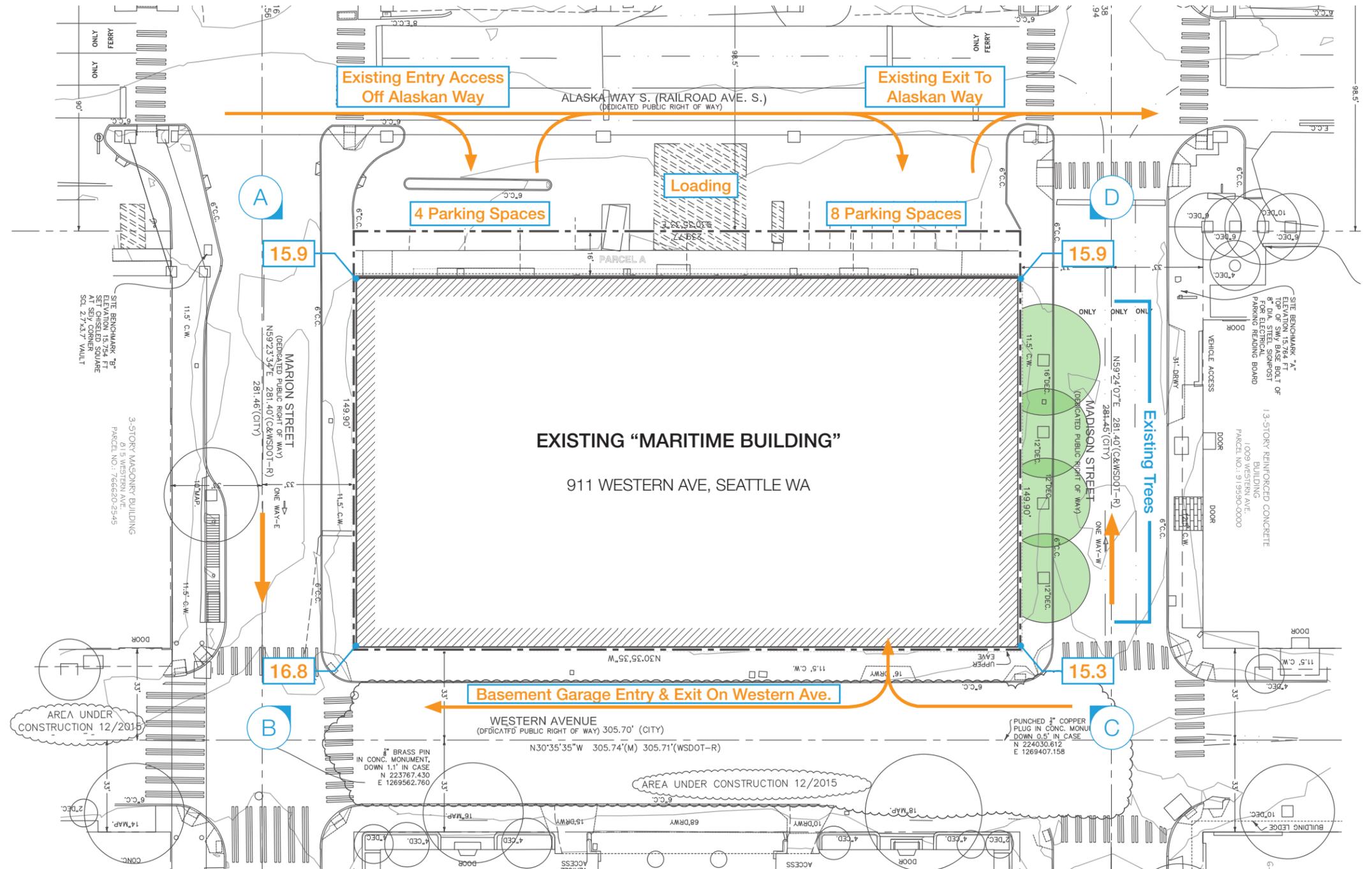
A tree survey was conducted by Greenforest Incorporated on November 9, 2015. Their assessment shows there are four existing Norway Maple trees within the sidewalk ROW along Madison Street. All four trees are healthy with no visible structural defects or health problems. The canopies of all the trees extend over Madison Street: 20 feet from the trunks to the north, and 8 feet to the south, nearly touching the building. Trunks range from 11" to 15". Tree heights range from 45' to 55'.

Existing Buildings:

The Maritime Building is a 5-story reinforced concrete exterior and heavy timber structure with a 1-story basement, constructed in 1910. The building extends to the property line on Madison Street, Western Avenue, and Marion Street sides. The building is set back 16 feet from the Railroad Avenue S. side. A building-length loading dock projects approximately 9 feet from the building face.

Existing Easements:

No easements noted on survey.



SITE /EXISTING BUILDING PHOTOS



A. Corner of Marion & Alaskan



B. Corner of Marion & Western



C. Corner of Madison & Western

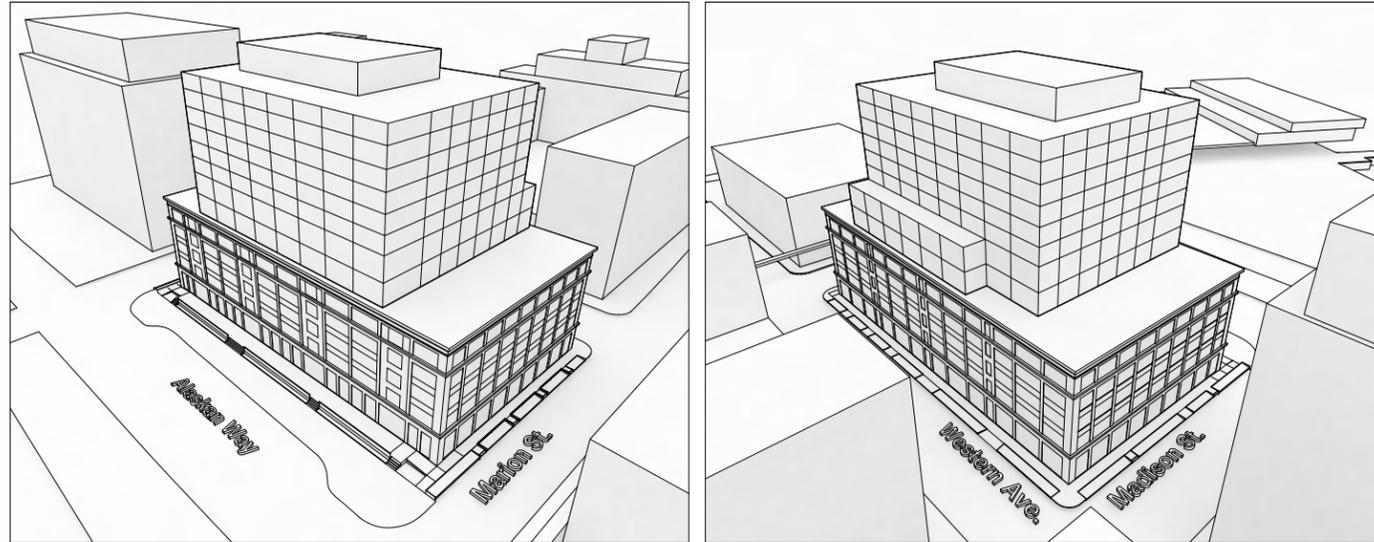


D. Corner of Madison & Alaskan

5.0

3 CONCEPTS AT EDG & GENERAL BOARD RESPONSE

CONCEPT 1

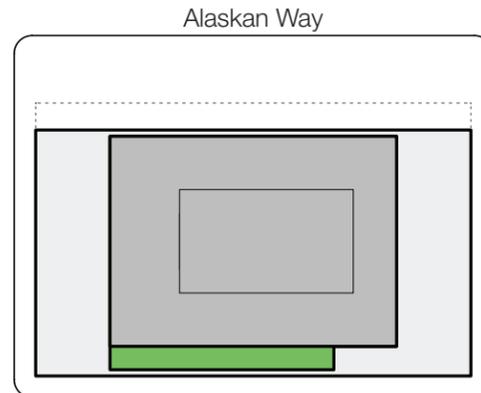


Summary

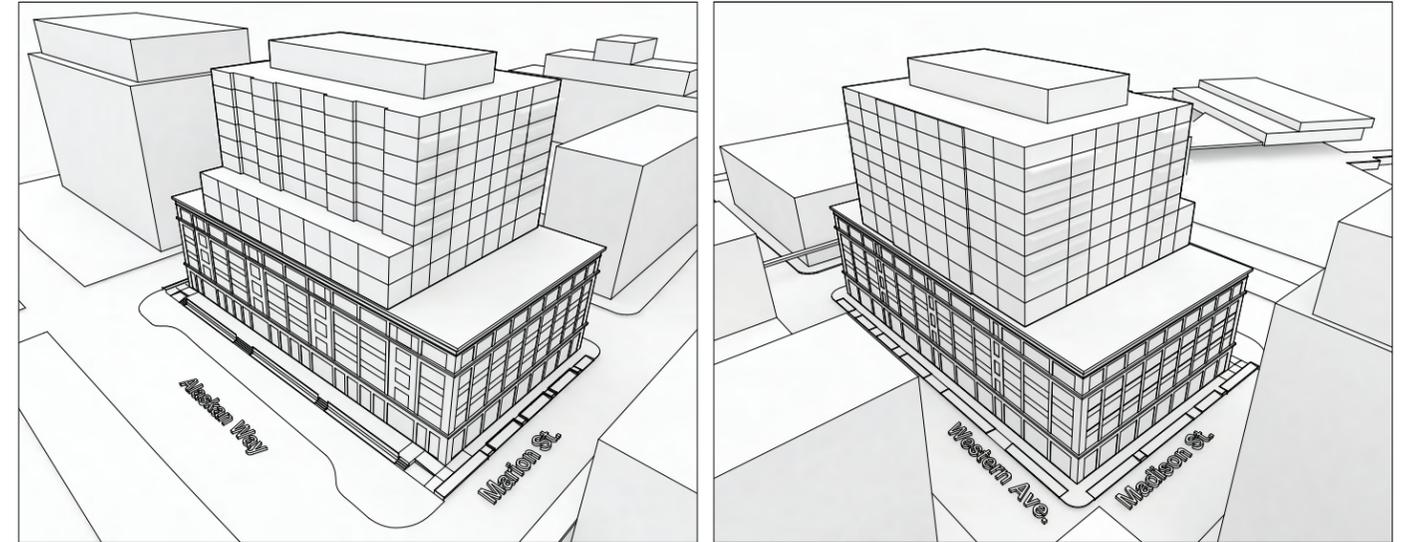
Concept 1 is a simple and elegant manifestation of the enclosed program. It positions the addition's mass to maximize its presence and height along Seattle's new waterfront while honoring its historic neighbor across Western Avenue. The architectural concept seeks to distinguish the modern new addition from the existing warehouse expression.

Board Response at EDG

- Least preferable due to the lack of setback at Alaskan Way from existing structure.



CONCEPT 2

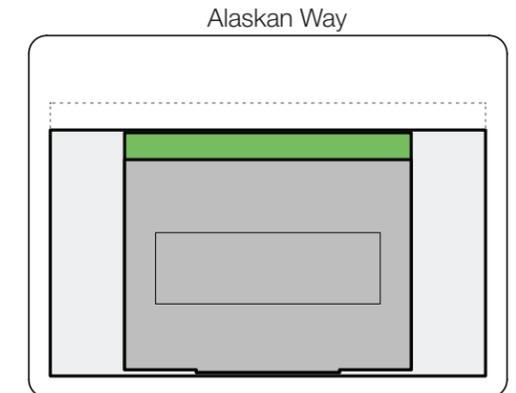


Summary

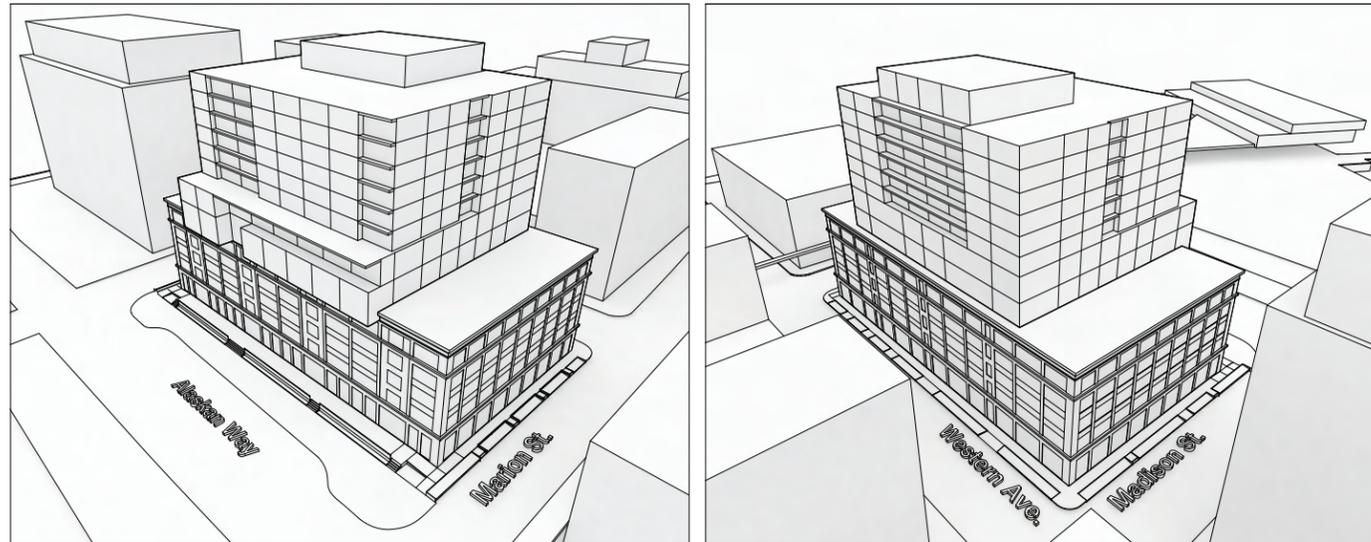
Concept 2 positions the addition's mass to acknowledge the city's natural topography by terracing the building back from the waterfront and toward the city. By shifting the mass of the residential tower to the east and back from the new office floors and existing Maritime Building below, this concept further distinguishes each programmatic function and creates outdoor terraces on the water side of the building.

Board Response at EDG

- The Board responded favorably to this scheme due to itv avvs response to the site's natural topography



CONCEPT 3 | PREFERRED

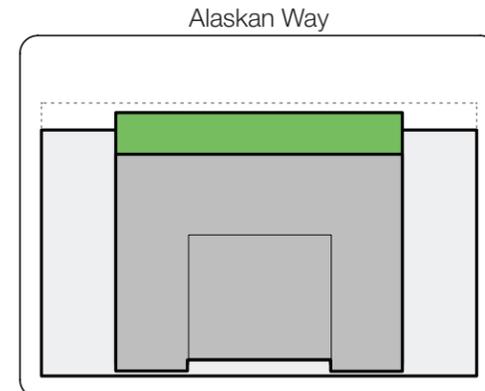


Summary

The preferred Concept 3 seeks to maximize the building's presence on Seattle's new waterfront while respecting the city's natural topography by stepping the building mass back from the waterfront and toward the city. Cantilevering the new office floors beyond the existing west façade of the Maritime Building and shifting the new residential tower back from the existing building wall places all three masses in different planes, thereby providing a clear distinction for each component and establishing a dynamic building form along Alaskan Way. This creates significant outdoor urban lawns on the water side of the building.

Board Response at EDG

- The Board responded favorably to this scheme, but showed concern around the office floors overhanging the cornice of the existing structure.



General Board Comments

“Continue to develop a landscape plan for all four sides of the structure that would take into account the volumes of people (and dogs) utilizing the areas for a variety of purposes”

“**Maintain the existing scale** along Western Avenue”

“Relocating the loading area for the structure to Madison Street was a good move”

“There was support for the location and configurations of the two proposed lobbies”

“The applicants preferred scheme showed promise, although the **stepping back up the topographic slope** conveyed in the second conceptual scheme was not without its appeal”

“The raised dining terrace and circulation space along the ground floor of the waterfront facade was conceptually strong”

“The north and south facades of the additional structure **need not be symmetrical**; support was shown for exploring more extensive opportunities for balconies as part of the composition of the residential floors”

“The Board members would like to see **explorations of asymmetrical upper facades** and optional alternatives to the overhanging cantilevered office space and a clear demonstration of the superiority of specific ways the upper and lower structures might otherwise interlock at the Maritime Building's cornice line.”

5.0

LANDMARKS PRESERVATION BOARD

ARC #1, February 11th

Summary

- Concern about the proportions of the proposed design
- Is it preferable to decrease the height and build into the view corridor? Maybe
- Alaskan Way elevated pedestrian plaza should reflect old loading dock feel

ARC #2, March 11th

Summary

- Questioned size of proposed addition
- Appreciated horizontal expression of existing building
- Support of precedents exhibiting horizontality

ARC #3, April 29th

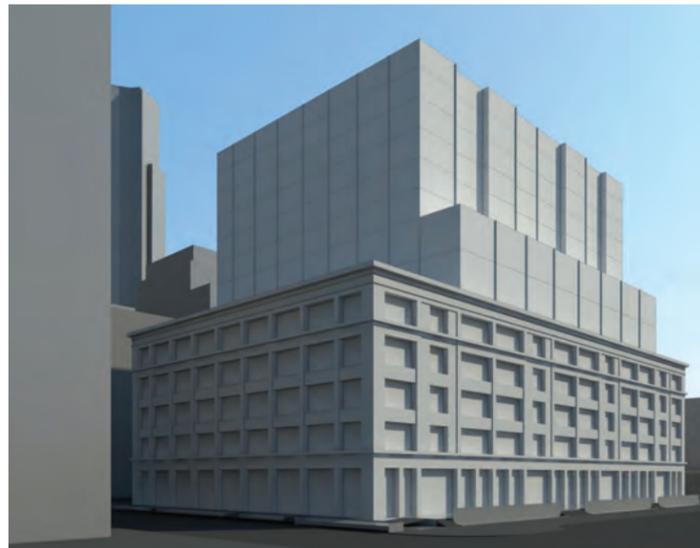
Summary

- Positive feedback of new massing direction and support for building into view corridor
- More development on east facade and materiality of solid walls
- More development of the ground level on Alaskan Way

Board Briefing, May 18th

Summary

- General support for the HALA massing alternative.
- The board reiterated support for the multi-frame window expression in the existing building.
- Supported the current approach to the loading dock design.
- Requested review of alternative massing articulation of the west facade.



SITE PLANNING AND MASSING

A-1 Respond to the Physical Environment

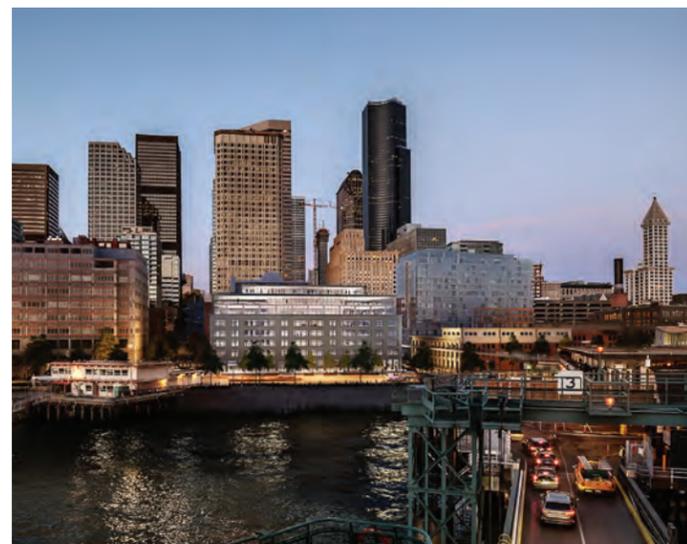
Develop an architectural concept and compose the building's massing in response to geographic conditions and patterns of urban form found nearby or beyond the immediate context of the building site.

RESPONSE: The Maritime Building has been recently designated a Historic Landmark. Our vision for the future of this simple warehouse is to recreate the building's function along the waterfront in support of Seattle's newly re-imagined public open space. A complete and substantial renovation of the existing building and an addition of two floors of office space and a rooftop amenity pavilion are planned. Respecting the stature of the Landmarked structure, the mass of the new addition is set back from the edges of the existing building on all sides and articulated to establish a horizontal expression to harmonize with the strong horizontal lines of the existing building. Also, the mass of the addition sets back along Western Avenue to respect the historic Federal Building to the east. Finally, the addition steps back from the waterfront and up to the city in response to the natural topography of the city.

A-2 Enhance the Skyline

Design the upper portion of the building to promote visual interest and variety in the downtown skyline. Respect existing landmarks while responding to the skyline's present and planned profile. A sculptured top can lend a distinctive identity to the building while helping to orient people as they approach and go places downtown. Reducing the area of the top floors reduces the appearance of the overall bulk.

RESPONSE: The addition is designed as a modern light structure to visually contrast with the stout and heavy expression of the Landmark building. The mass of the addition steps back as it rises to the smaller amenity pavilion on the roof and the west wall of the top office floor is angled back to the existing building below creating a sculpted appearance. Further, a deep overhang on the west side of the top floor creates a strong visual presence along the waterfront.



ARCHITECTURAL EXPRESSION

B-1 Respond to Neighborhood Context

Develop an architectural concept and compose the major building elements to reinforce desirable urban features existing in the surrounding neighborhood.

RESPONSE: To contrast to the heavy and solid expression of the existing Landmark Maritime Building and other adjacent historic warehouse structures in the neighborhood, the architectural expression of the addition is as a modern transparent lightweight mass appearing as a lantern when viewed from afar and as a gateway to the city as one arrives by ferry. At the street level, the existing loading dock will be expanded and reimagined to provide an active retail environment to support the pedestrian activity exiting the bridge from Colman Dock.

B-2 Create a Transition in Bulk and Scale

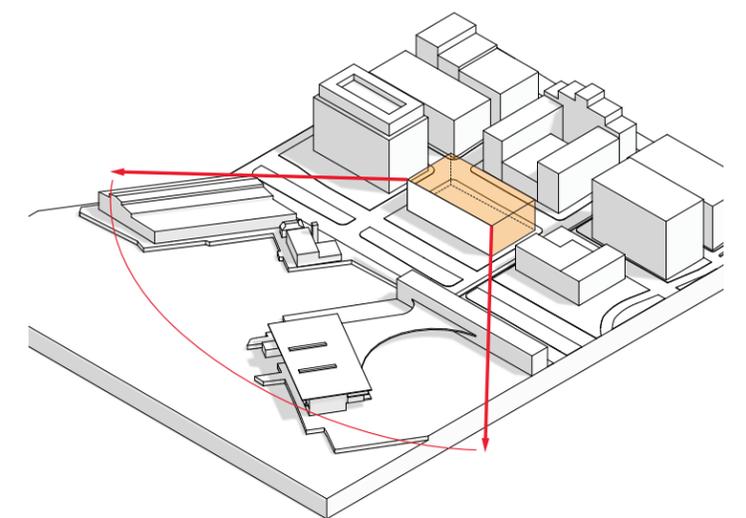
Compose the massing of the building to create a transition to the height, bulk, and scale of development in neighboring or nearby less intensive zones.

RESPONSE: At seven floors plus an amenity pavilion, the new Maritime Building is designed to mediate between the lower historic buildings to the north such as the Federal Building, the National Building, the Olympic Cold Storage Building and 51 University and the taller Waterfront Place and the newly constructed Post Apartments to the south.

B-3 Reinforce the Positive Urban Form & Architectural Attributes of the Immediate Area

Consider the predominant attributes of the immediate neighborhood and reinforce desirable siting patterns, massing arrangements, and streetscape characteristics of nearby development.

RESPONSE: Responding to the new waterfront, the modernized Maritime Building will realign itself by relocating its original address from Western Avenue to the new Alaskan Way. A new lobby for the commercial office space will be placed on Alaskan Way. To support the new active waterfront, the existing loading dock will be expanded and developed to provide for an active food and beverage retail environments and outdoor dining. All sides of the building will be carefully designed to assure active uses along all street frontages. New street trees and landscaping are planned on all sides of the building. Additionally, the proposed addition to the building is setback 15' from the east façade of the existing building in response to the historic Federal Building across Western Avenue.



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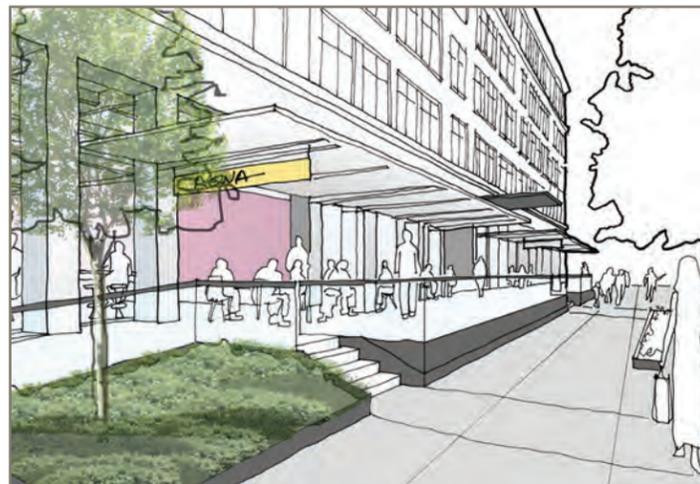
PRIORITY CITY DESIGN GUIDELINES

ARCHITECTURAL EXPRESSION (CONT.)

B-4 Design a Well-Proportioned & Unified Building

Compose the massing and organize the interior and exterior spaces to create a well-proportioned building that exhibits a coherent architectural concept. Design the architectural elements and finish details to create a unified building, so that all components appear integral to the whole.

RESPONSE: Responding to the new waterfront, the modernized building will realign itself and become an outward facing development by relocating its original address from Western Avenue to the new Alaskan Way. The existing loading dock along the west side of the building will be expanded to provide adequate space for a gracious new commercial office lobby and an active food and beverage retail environments and “spill out” outdoor dining overlooking the new waterfront. The same color is proposed for the window wall of the new addition and the replacement windows of the existing building to create a unified building expression. New architectural details at the ground level, lobby entry and loading dock retail are being designed to retain the look and feel of a working warehouse.



THE STREETScape

C-1 Promote Pedestrian Interaction

Spaces for street level use should be designed to engage pedestrians with the activities occurring within them. Sidewalk-related spaces should appear safe, welcoming, and open to the general public.

RESPONSE: In support of the new waterfront, the new maritime building will realign itself by relocating its original address from Western Avenue to the new Alaskan Way. A new lobby for the commercial office space will be placed on Alaskan Way. The historic raised loading dock will be transformed into an active food and beverage retail environments and outdoor dining terrace overlooking the new waterfront. All sides of the building will be carefully designed to assure active retail uses along all street frontages.

C-4 Reinforce Building Entries

To promote pedestrian comfort, safety, and orientation, reinforce the building's entry.

RESPONSE: A continuous new canopy is planned for the west side of the building providing overhead weather protection for the dining terrace. The height of the canopy will increase signaling the entrances to the main building lobby.

PUBLIC AMENITIES

D-1 Provide Inviting & Usable Open Space

Design public open spaces to promote a visually pleasing, safe, and active environment for workers, residents and visitors. Views and solar access from the principal area of the open space should be especially emphasized.

RESPONSE: The historic raised loading dock on the west side of the building will provide significant public open space and support the increased pedestrian activity along the new waterfront. The dock will be expanded in width and transformed into an active food and beverage retail environments featuring comfortable “spill out space” for outdoor dining overlooking the new waterfront and Alaskan Way. With a western exposure, the terrace will have significant solar exposure and views of the active waterfront. A large landscaped roof terrace and an amenity pavilion for the building tenants is also planned on the seventh floor.



D-2 Enhance the Building with Landscaping

Enhance the building and site with substantial landscaping—which includes special pavements, trellises, screen walls, planters, and site furniture, as well as living plant material.

RESPONSE: Street trees and extensive grade level planting is planned for all sides of the building.

D-3 Provide Elements that Define the Place

Provide special elements on the facades, within public open spaces, or on the sidewalk to create a distinct, attractive, and memorable “sense of place” associated with the building.

RESPONSE: The Maritime Building has been designated as a Landmark by the Historic Landmark Preservation Board. The Building itself along with the enhanced loading dock will create a memorable location along Alaskan Way. Architectural features such as handrails and overhead canopies will be designed and materials selected to maintain the historic qualities of a working loading dock.

PUBLIC AMENITIES (CONT.)

D-4 Provide Appropriate Signage

Design signage appropriate for the scale and character of the project and immediate neighborhood. All signs should be oriented to pedestrians and/or persons in vehicles on streets within the immediate neighborhood.

RESPONSE: The Maritime Building has been designated as a historic landmark in the City of Seattle. Building signage features will be designed and materials selected to maintain the historic working nature qualities of the building.

D-5 Provide Adequate Lighting

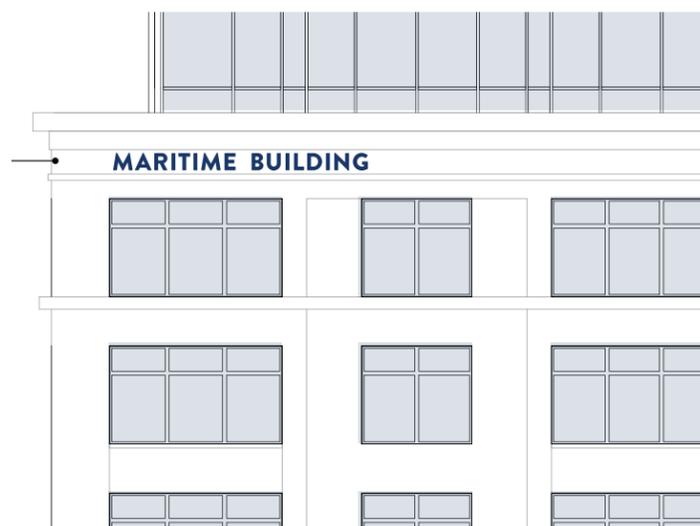
To promote a sense of security for people downtown during nighttime hours, provide appropriate levels of lighting on the building facade, on the underside of overhead weather protection, on and around street furniture, in merchandising display windows, and on signage.

RESPONSE: Appropriate pedestrian level lighting will be provide on all sides of the building. The west side loading dock and dining terrace will be illuminated from the overhead canopy with additional lighting at the building lobby and stairs up to the dock. The remaining three sides of the building will be illuminated from the retail spaces around the building as well as existing street lights.

D-6 Design for Personal Safety & Security

Design the building and site to enhance the real and perceived feeling of personal safety and security in the immediate area.

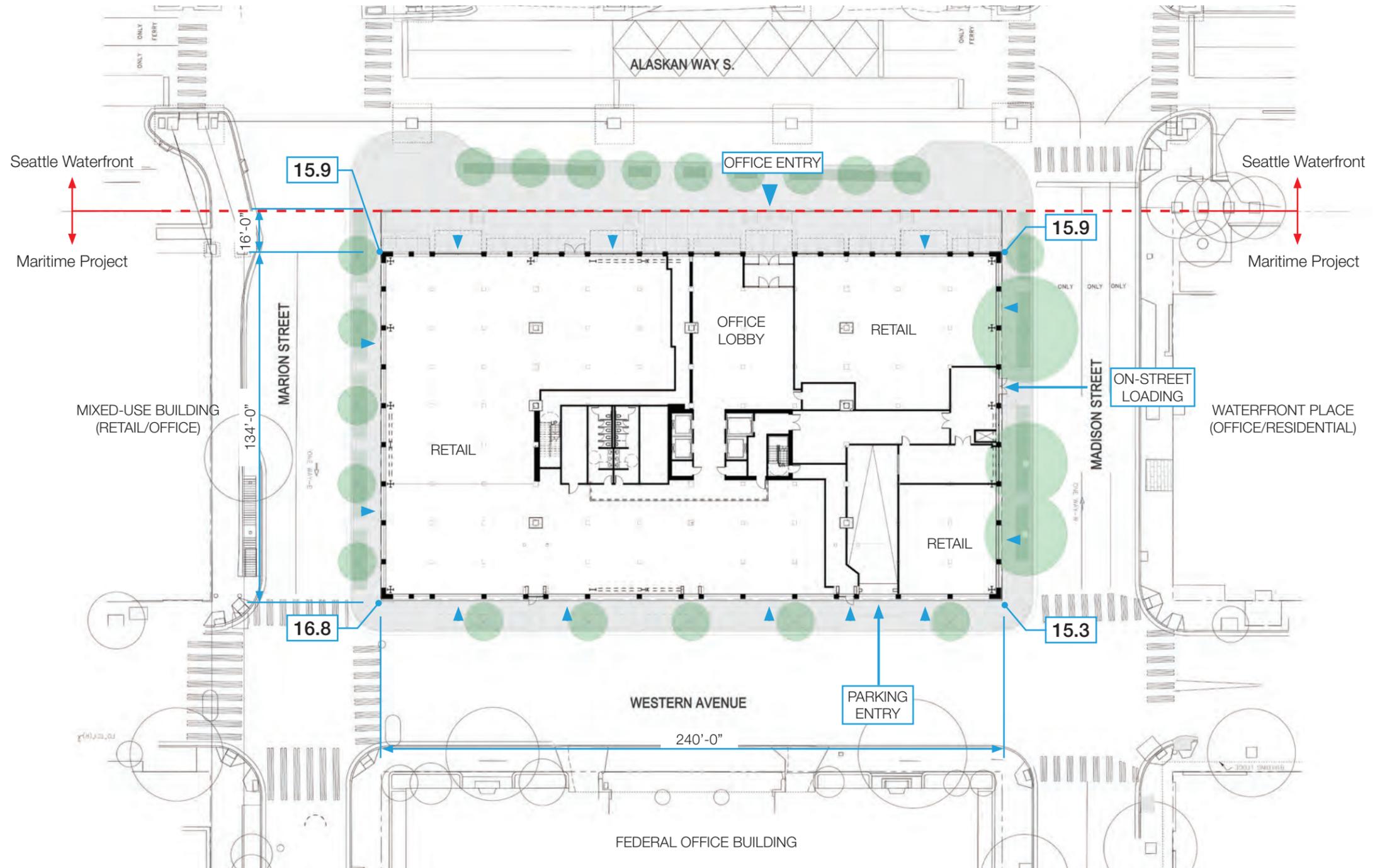
RESPONSE: Retail Shops with large extents of glazing are planned around all sides of the building creating an active “eyes on the street” environment. Appropriate light levels will be provided to provide a safe pedestrian experience during the nighttime hours.



COMPOSITE FLOOR PLAN

Retail entrances are proposed for all four street frontages: Alaskan Way, Marion Street, Madison Street, and Western Avenue, to promote life and activity on the edges of the building.

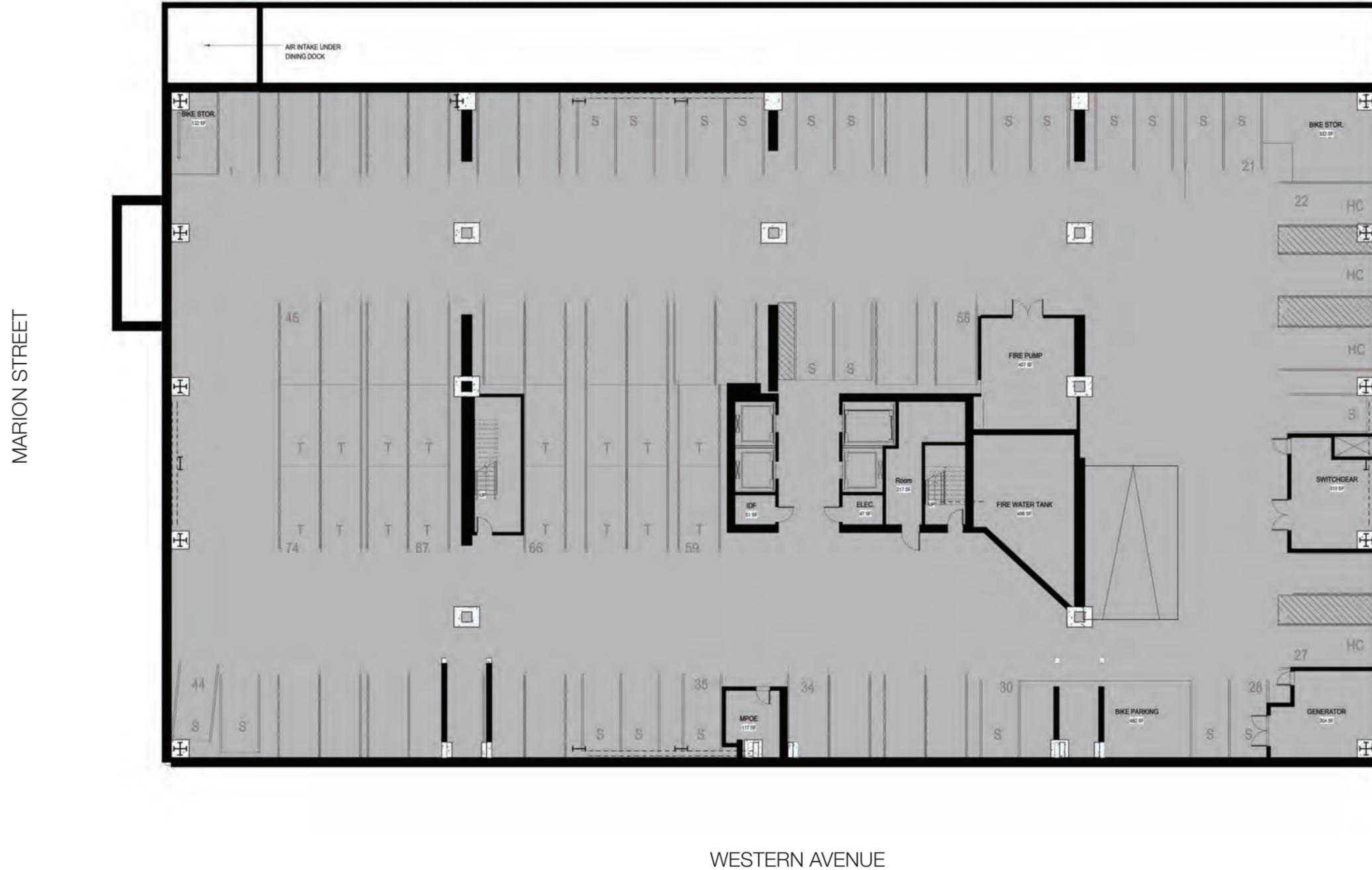
The commercial office building lobby / entrance is located on the automobile-oriented and newly transformed Alaskan Way.



7.0

MUP FLOOR PLANS

ALASKAN WAY



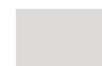
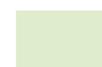
MADISON STREET

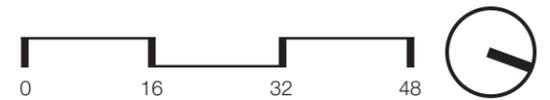
MARION STREET

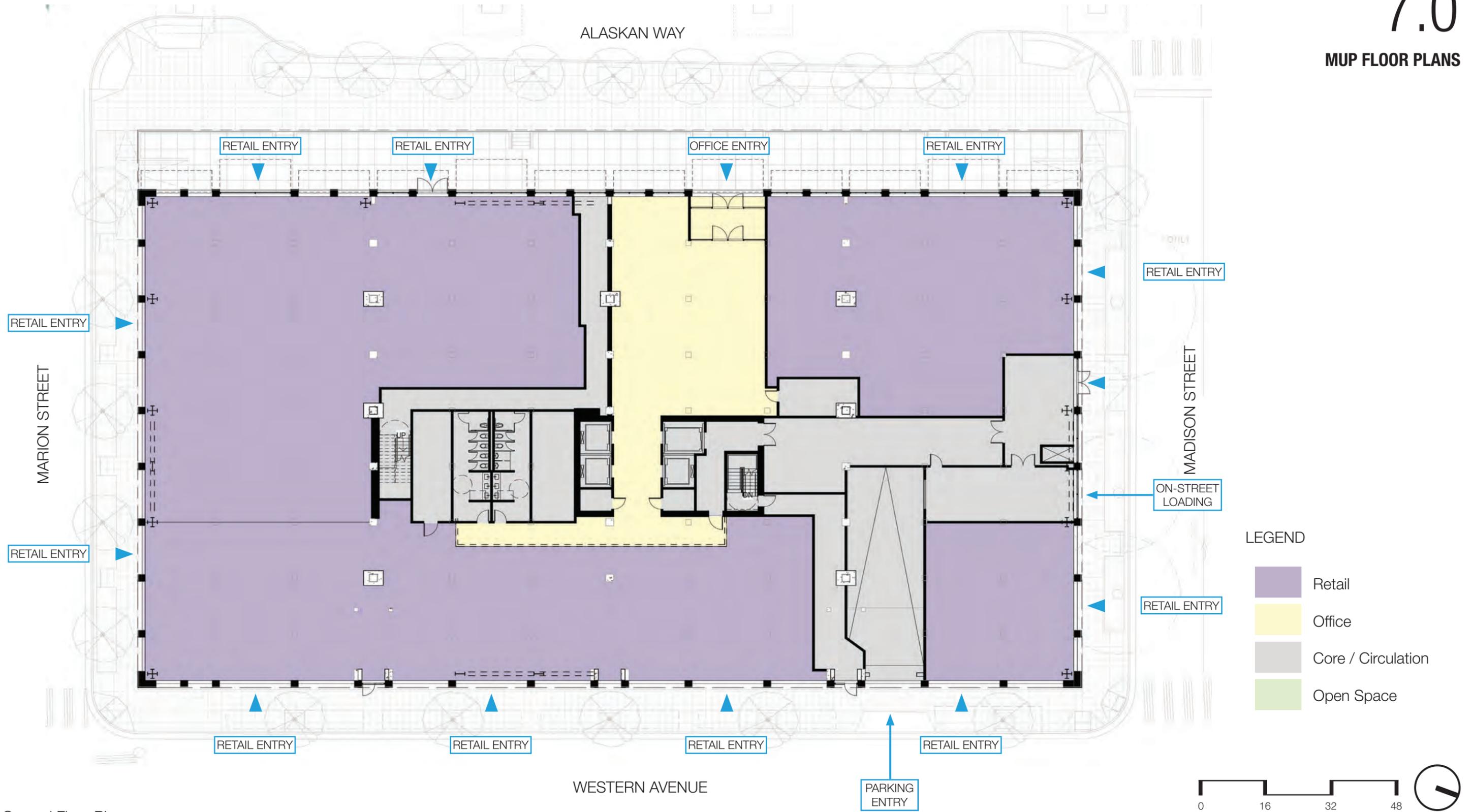
WESTERN AVENUE

Basement Floor Plan

LEGEND

-  Retail
-  Office
-  Core / Circulation
-  Open Space





Ground Floor Plan

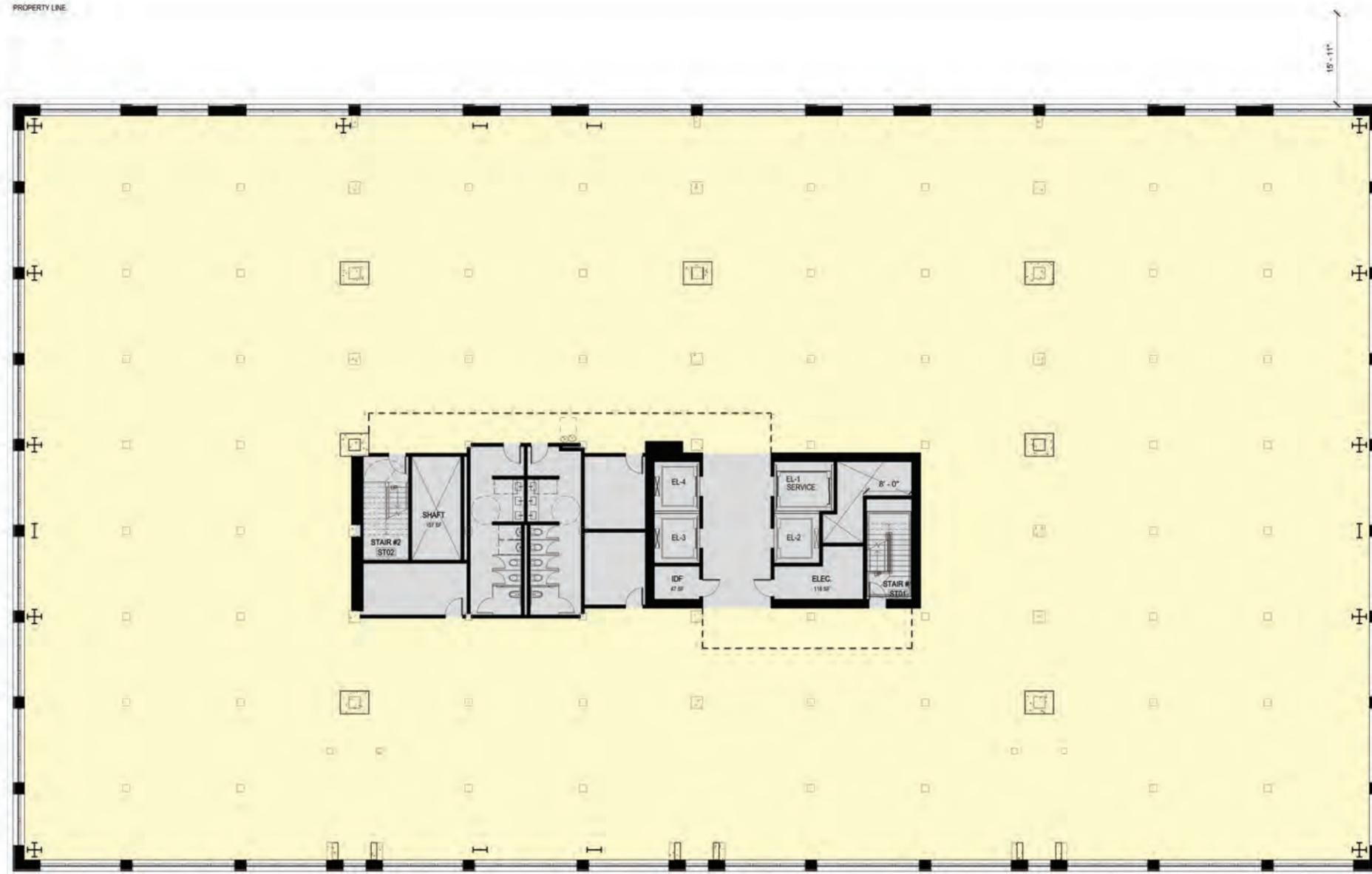
7.0

MUP FLOOR PLANS

ALASKAN WAY

MARION STREET

MADISON STREET

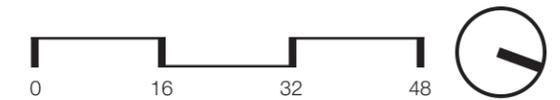


LEGEND

-  Retail
-  Office
-  Core / Circulation
-  Open Space

WESTERN AVENUE

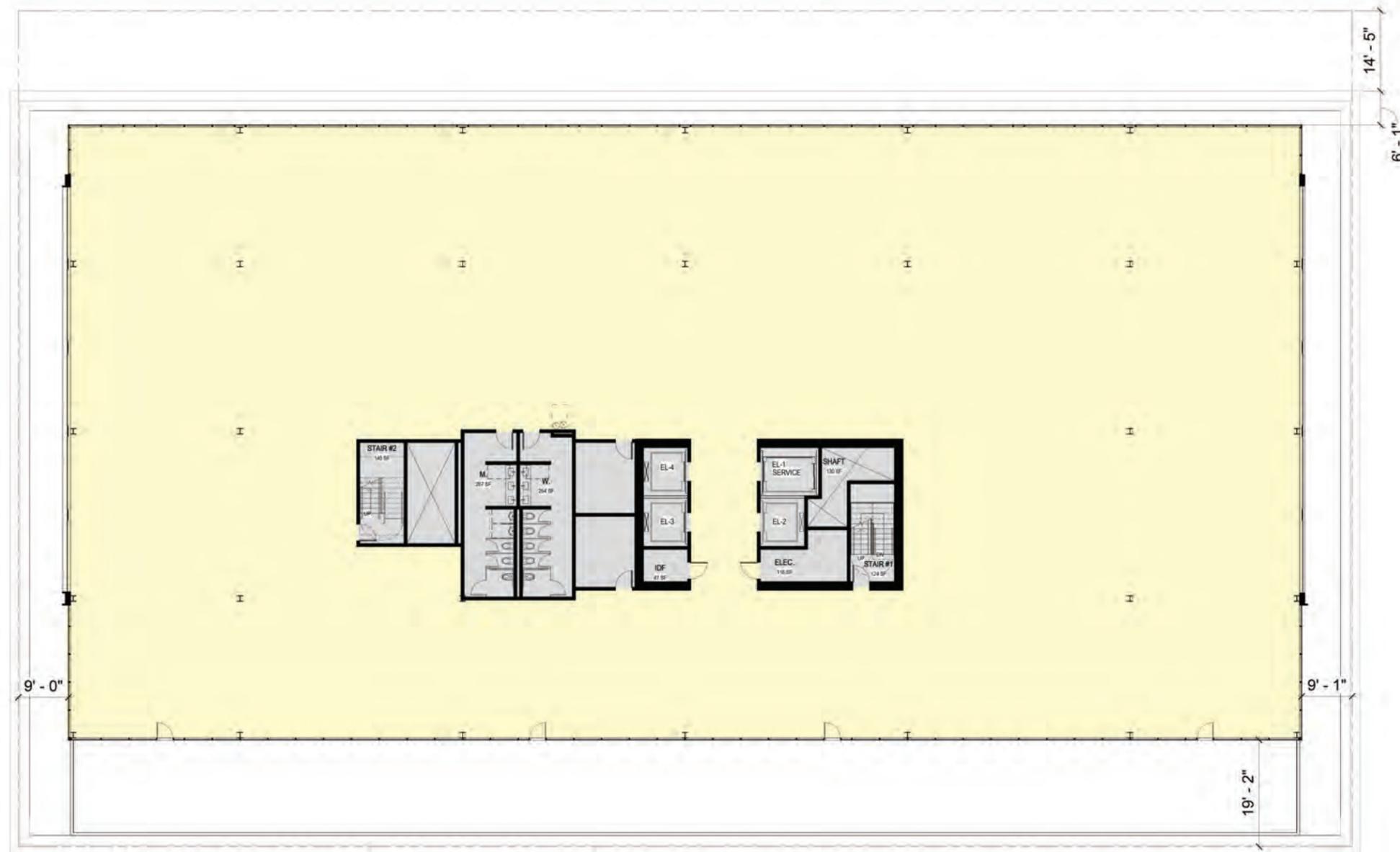
Floors 2-5



ALASKAN WAY

MARION STREET

MADISON STREET

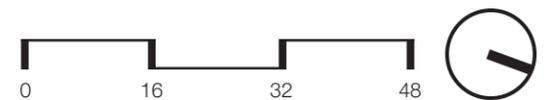


LEGEND

- Retail
- Office
- Core / Circulation
- Open Space

WESTERN AVENUE

Floor 6



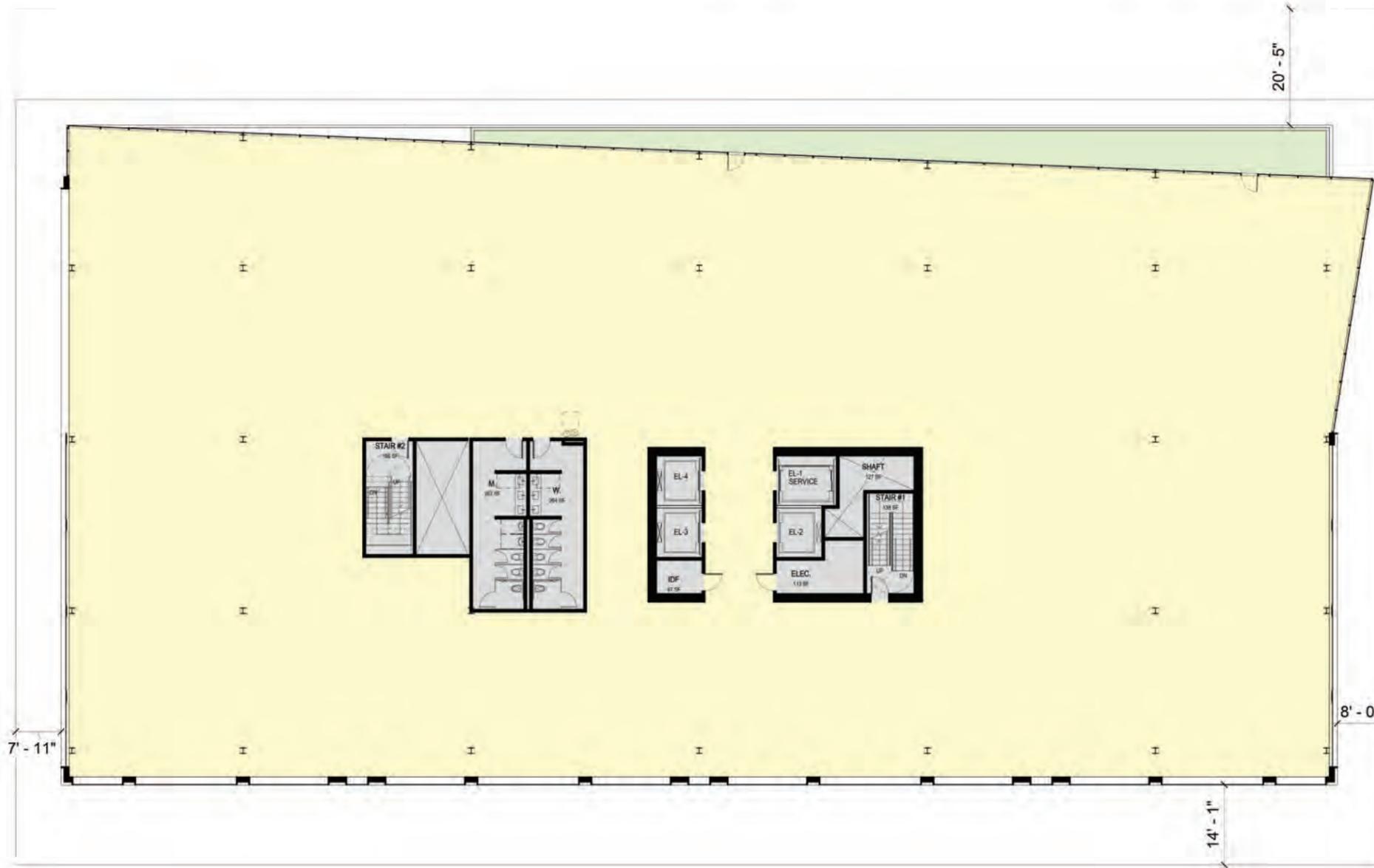
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MUP FLOOR PLANS

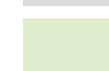
ALASKAN WAY

MARION STREET

MADISON STREET

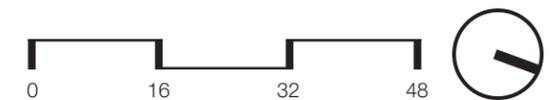


LEGEND

-  Retail
-  Office
-  Core / Circulation
-  Open Space

WESTERN AVENUE

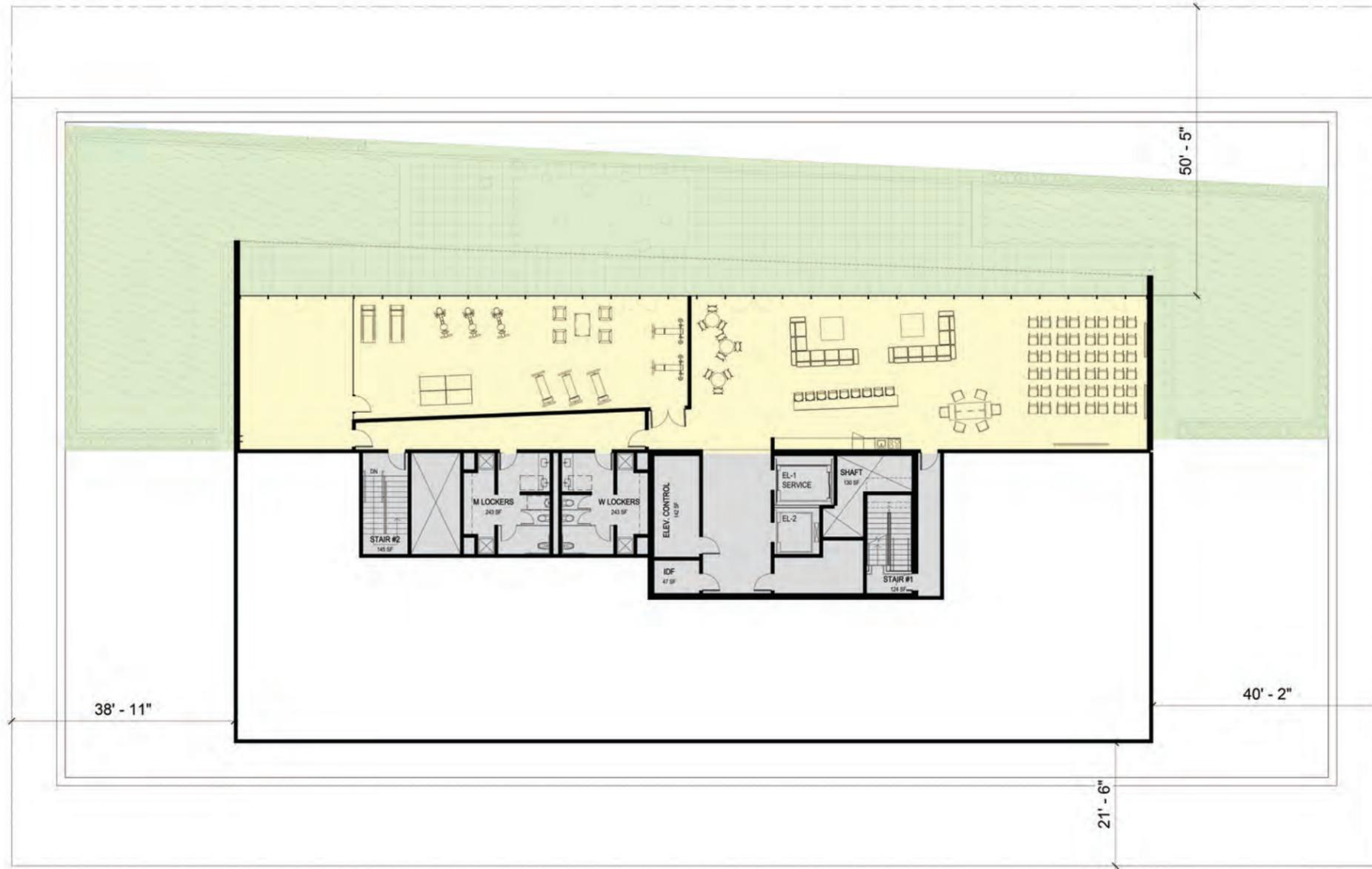
Floor 7



ALASKAN WAY

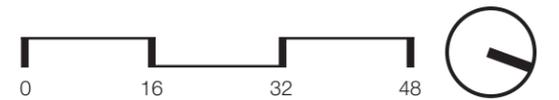
MARION STREET

MADISON STREET



LEGEND

- Retail
- Office
- Core / Circulation
- Open Space



WESTERN AVENUE

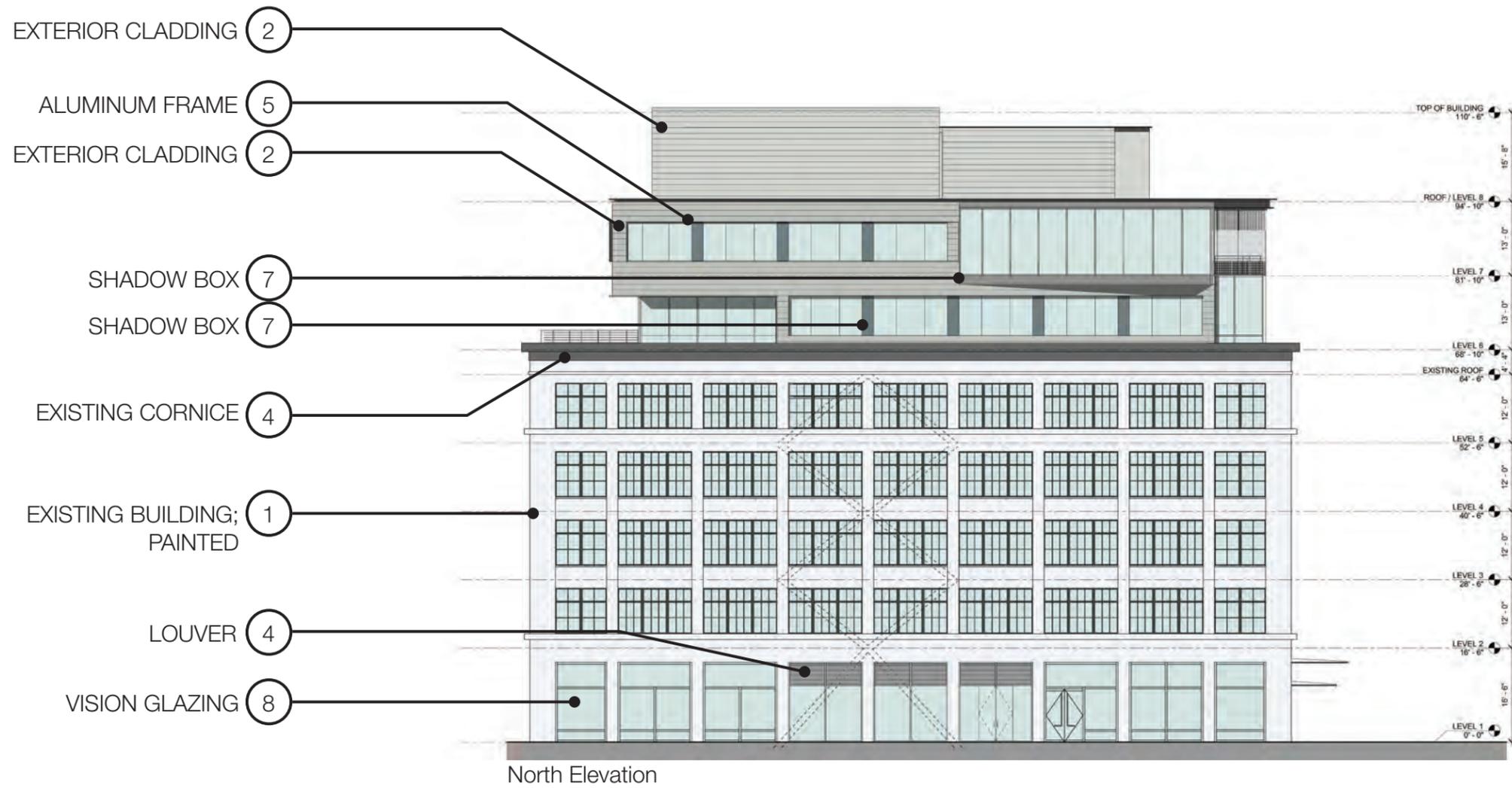
Floor 8

8.0

MUP ELEVATIONS



West Elevation



8.0

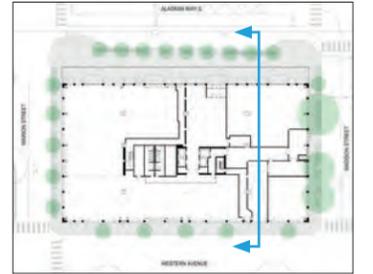
MUP ELEVATIONS



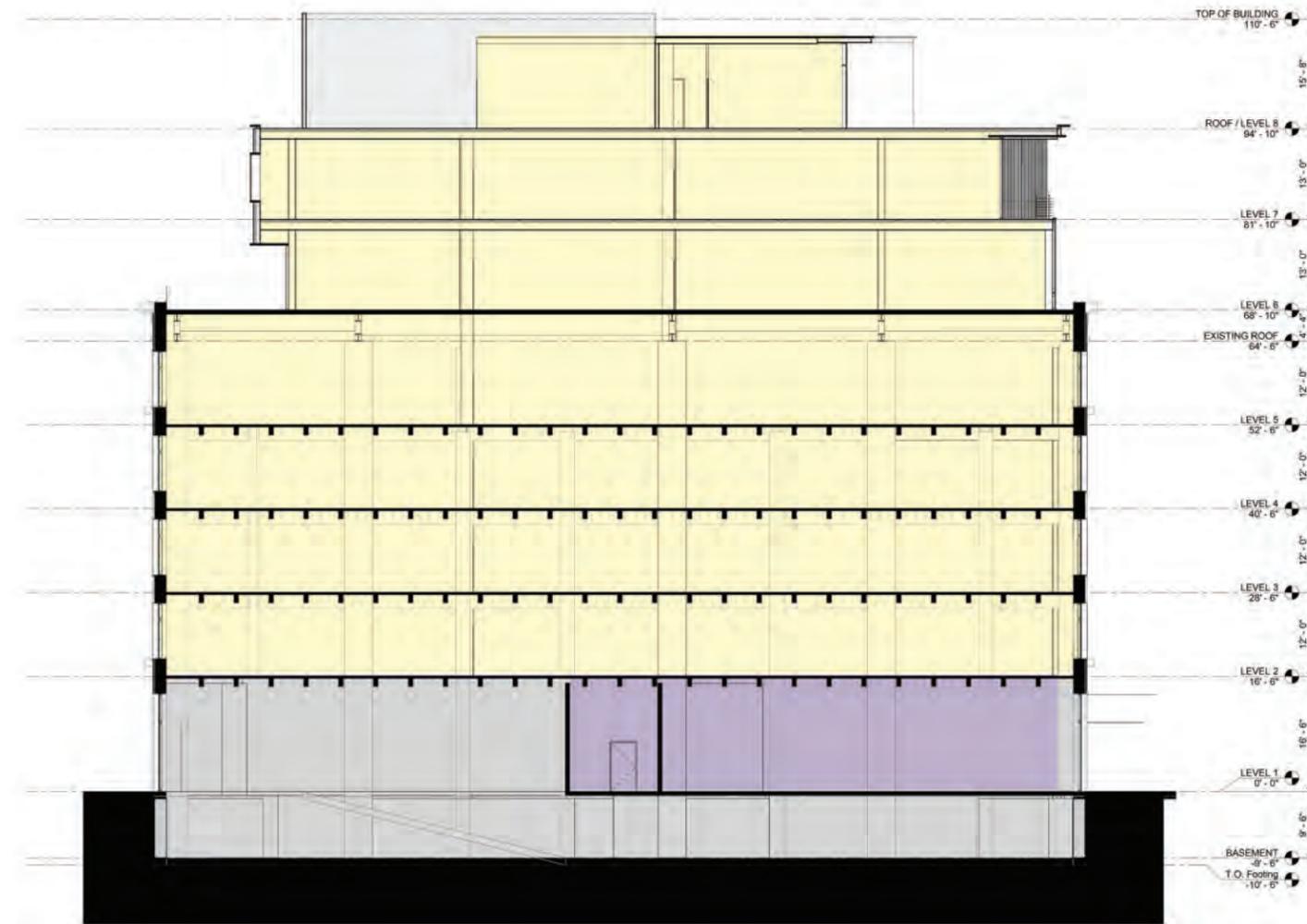


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MUP ELEVATIONS

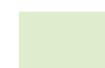


Key Plan



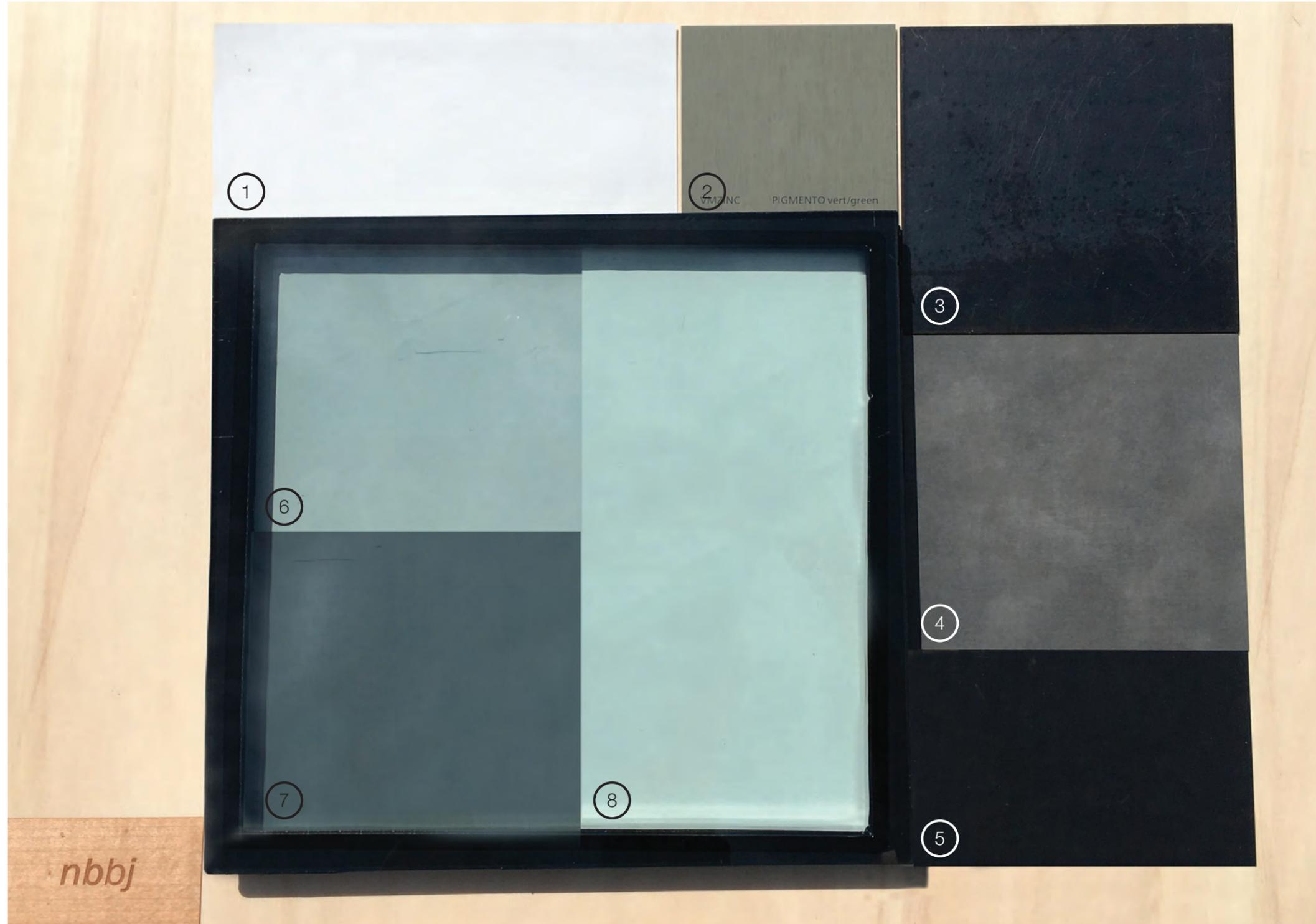
West - East Section

LEGEND

-  Retail
-  Office
-  Core / Circulation
-  Open Space

9.0

MATERIAL & COLOR PALETTE



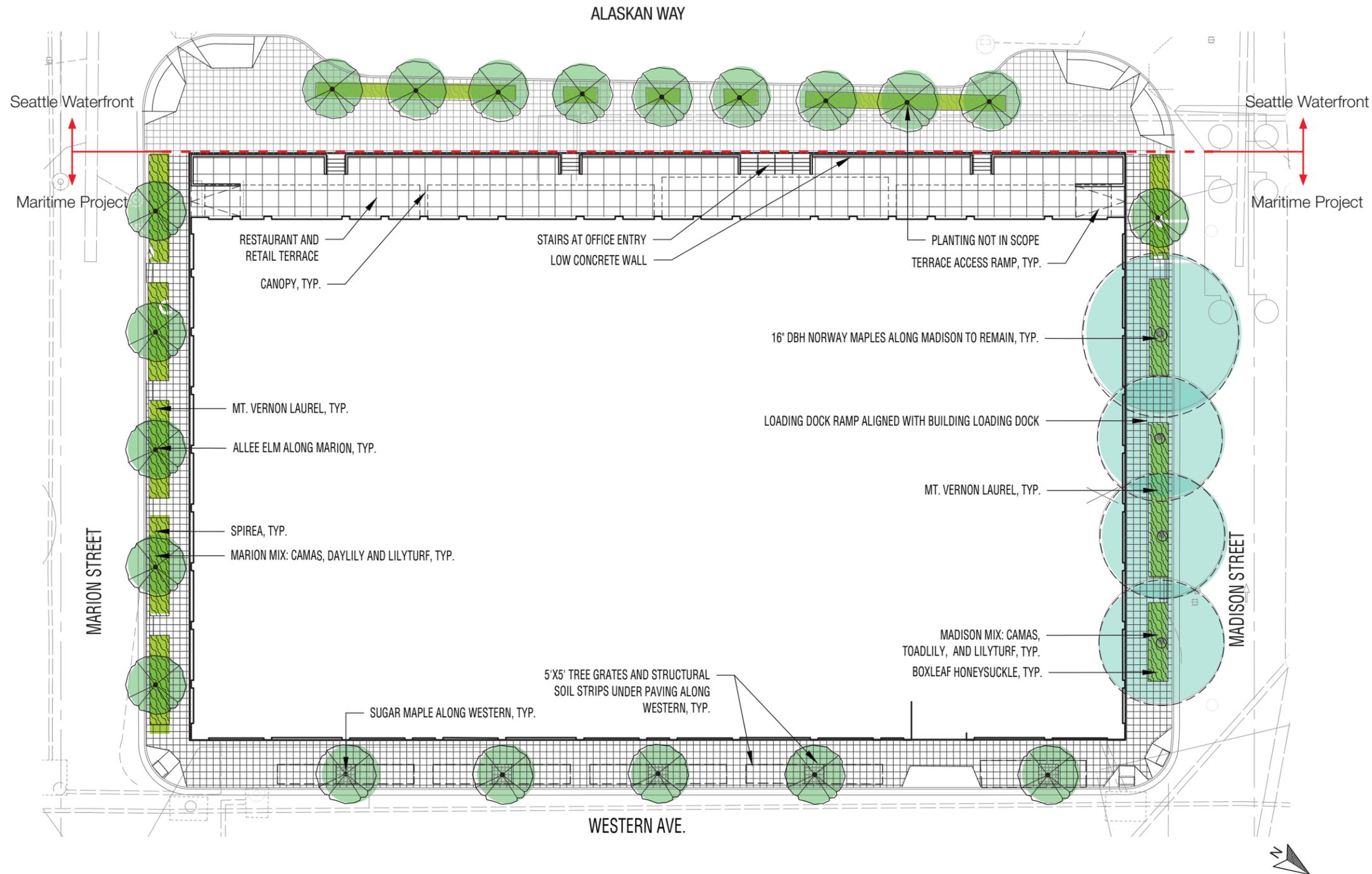
1. Benjamin Moore - Stone White 2120-70
2. Painted Metal Panel - Pigmento Green
3. Benjamin Moore - Shaker Gray 1594
4. Steel - Blackened
5. Aluminum Frame - Black
6. Shadowbox - Back painted vision glass light gray
7. Shadowbox - Back painted Vision Glass dark gray
8. Glazing - Vision Glass

* Materials subject to Landmark Preservation Board review and approval

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10.0

LANDSCAPE ARCHITECTURE



COMPOSITE LANDSCAPE PLAN

The Maritime building project will have new streetscape on all four surrounding streets that conforms to the character of the adjoining blocks. Marion Street, Western Avenue and Madison Street each have 12-foot-wide sidewalks which will receive new SDOT standard concrete pavement. The sidewalks will be regraded to comply with ADA standards. New shrub planting beds will be located in tree pits under the existing Norway Maple trees on Madison Street, and under the proposed trees on Marion Street. Tree pits with tree grates and structural soil under the sidewalk will be installed along Western. There will be a 12-inch-wide strip of concrete pavement along the existing curb for the convenience of passengers getting out of cars. Breaks in the planting strip will provide access to the sidewalk for passengers of parked cars or people being dropped off. A wider break in the planting strip on Madison Street will provide access from the proposed on-street loading space to the loading entrance.

Street trees and shrub plantings will be held back from the underground parking entry on Western Avenue. On Alaskan Way the building will have a 13-foot-wide raised terrace with decorative paving for pedestrian circulation and the use of adjacent retail tenants. The new office entry will have a wide set of stairs, and ADA access at the north and south ends of the raised terrace. Along the edge of the terrace will be a two-foot-high wall with a 32-inch-high rail along the front. The streetscape along Alaskan Way is part of the new Seattle Waterfront Project and is not part of the project. The projects will be coordinated.

GROUND LEVEL PLANTING



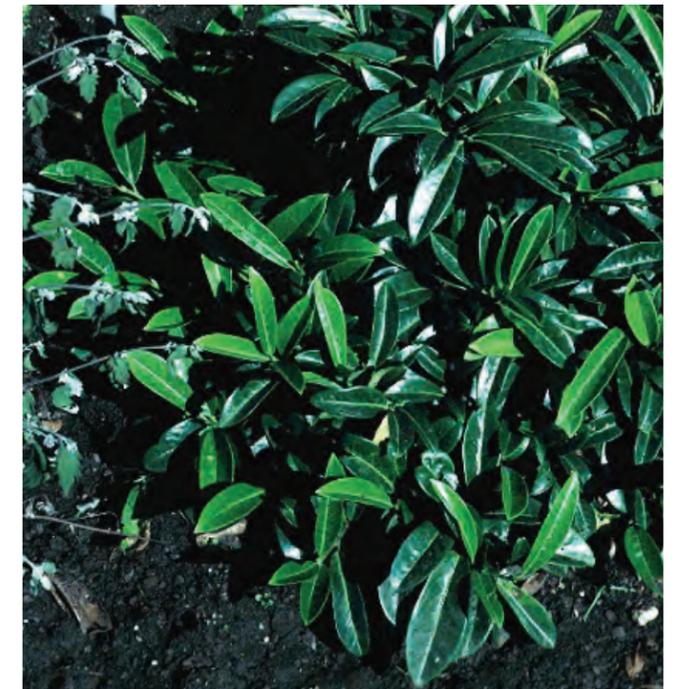
LEGACY SUGAR MAPLE



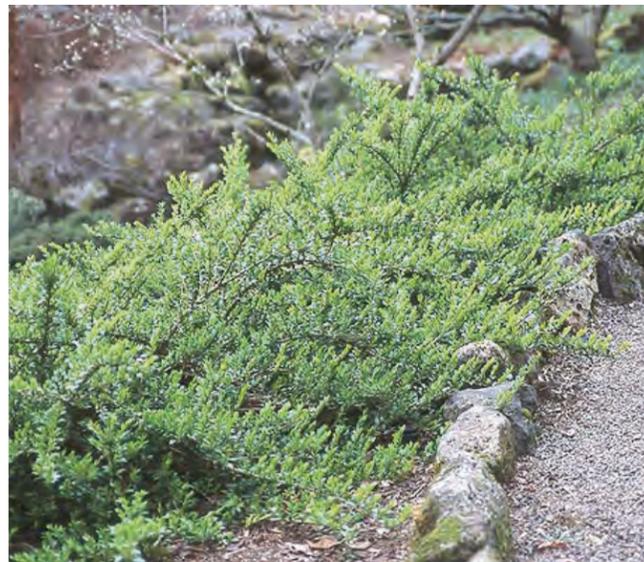
ALLEE ELM



SHIROBANA SPIREA



MT. VERNON LAUREL



BOXWOOD HONEYSUCKLE



STELLA D'ORO DAYLILY



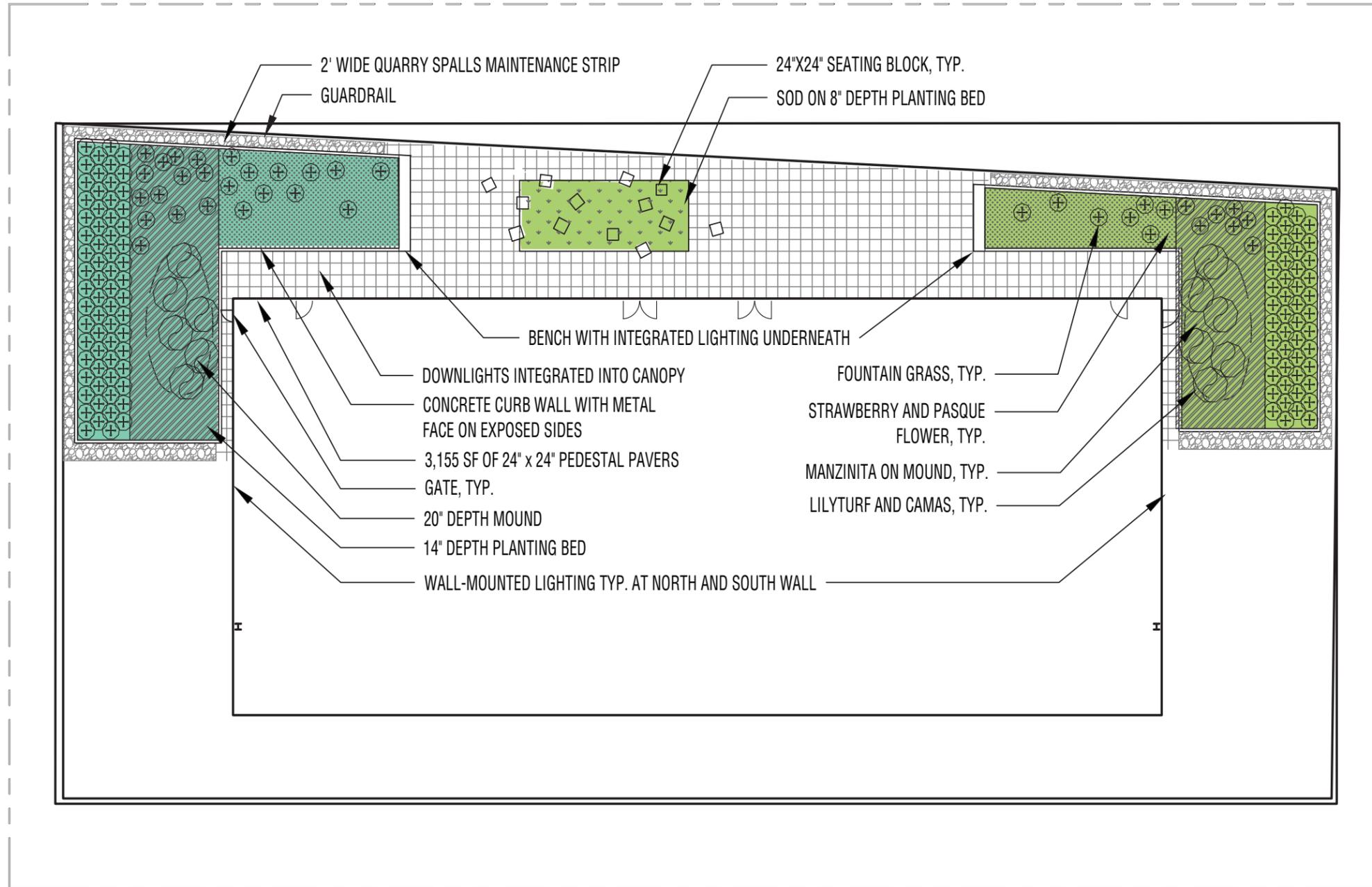
TOADLILY



CAMAS

10.0

LANDSCAPE ARCHITECTURE



FLOOR 8



STONE BLOCK SEATING



INTENSIVE GREEN ROOF PLANTING

ROOFTOP PLANTING



MANZANITA



HAMELN DWARF FOUNTAIN GRASS



CALIFORNIA FUSCHA



BEACH STRAWBERRY



CAMAS



LILY TURF

10.0

LANDSCAPE ARCHITECTURE

ROOFTOP PLANT LIST

Shrubs and Groundcovers

Symbol	Qua.	Scientific Name	Common Name	Size	Notes
		<i>ARCTOSTAPHYLOS DENSIFLORA</i> 'HOWARD MCMIN'	HOWARD MCMIN VINE HILL MANZANITA	5 GAL. POT	
		<i>FRAGARIA CHILOENSIS</i>	BEACH STRAWBERRY	4" POT, 10" O.C.	
		<i>LIRIOPE MUSCARI</i> 'BIG BLUE'	LILYTURF	1 GAL. POT, 18" O.C.	
		<i>CAMASSIA LEICHTLINII</i>	LEICHTLIN'S CAMAS	BULB, 6" O.C.	EVENLY MIX THROUGH LILYTURF
		<i>PENNISETUM ALOPECUROIDES</i> 'HAMELN'	DWARF FOUNTAIN GRASS	2 GAL. POT, 3' O.C.	
		<i>ZAUSCHNERIA CALIFORNICA</i>	CALIFORNIA FUSCHA	1 GAL. POT, 30" O.C.	

ON-GRADE PLANT LIST

Trees

Symbol	Qua.	Scientific Name	Common Name	Size	Notes
	5	<i>ACER SACCHARUM</i> 'LEGACY'	LEGACY SUGAR MAPLE	2" CAL., B&B	BRANCHING ABOVE 6', TYP.
	6	<i>ULMUSPARVIFLORA</i>	ALLEE ELM	2" CAL., B&B	BRANCHING ABOVE 6', TYP.
		<i>WATERFRONT PROJECT TREES</i>	NOT IN SCOPE		
	4	<i>EXISTING TREES TO REMAIN</i>			

Shrubs and Groundcovers

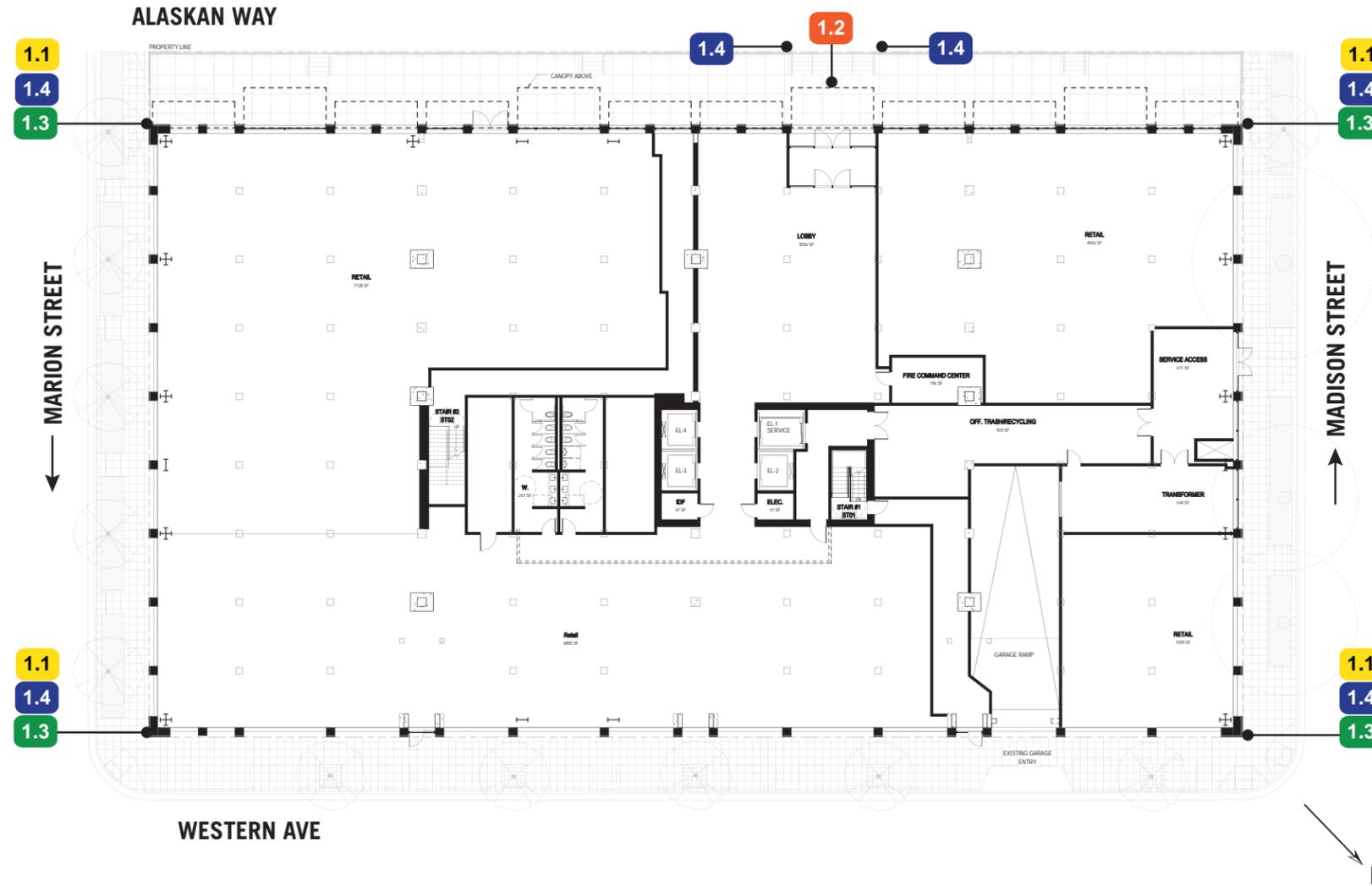
Symbol	Qua.	Scientific Name	Common Name	Size	Notes
		Madison Mix			
	10	<i>CAMASSIA LEICHTLINII</i>	LEICHTLIN'S CAMAS	BULB, 6" O.C.	
	160	<i>HEMEROCALLIS</i> 'STELLA D'ORO'	STELLA D'ORO DAYLILY	1 GAL. POT, 12" O.C.	
	130	<i>LIRIOPE MUSCARI</i> 'BIG BLUE'	LILYTURF	1 GAL. POT, 12" O.C.	
	18	<i>LONICERA PILEATA</i>	BOXLEAF HONEYSUCKLE	2 GAL. POT, 3' O.C.	
	146	<i>PRUNUS LAUROCERASUS</i> 'MT VERNON'	MT VERNON LAUREL	2 GAL, 18" O.C.	
	48	<i>SPIRAEA JAPONICA</i> 'SHIROBANA'	SHIROBANA SPIREA	2 GAL., 18" O.C.	
		Marion Mix			
	10	<i>CAMASSIA LEICHTLINII</i>	LEICHTLIN'S CAMAS	BULB, 6" O.C.	
	120	<i>TRICYRTIS HIRTA</i>	JAPANESE TOADLILY	1 GAL. POT, 12" O.C.	
	100	<i>LIRIOPE MUSCARI</i> 'BIG BLUE'	LILYTURF	1 GAL. POT, 12" O.C.	

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11.0

SIGNAGE CONCEPT PLAN

SITE OVERVIEW

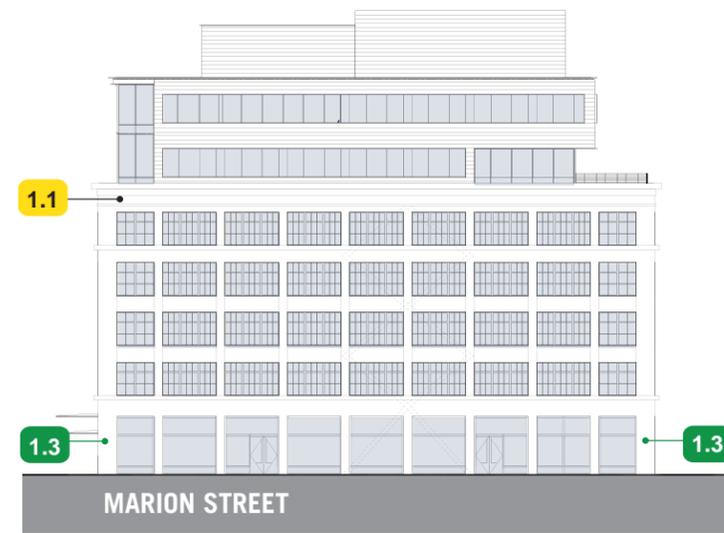
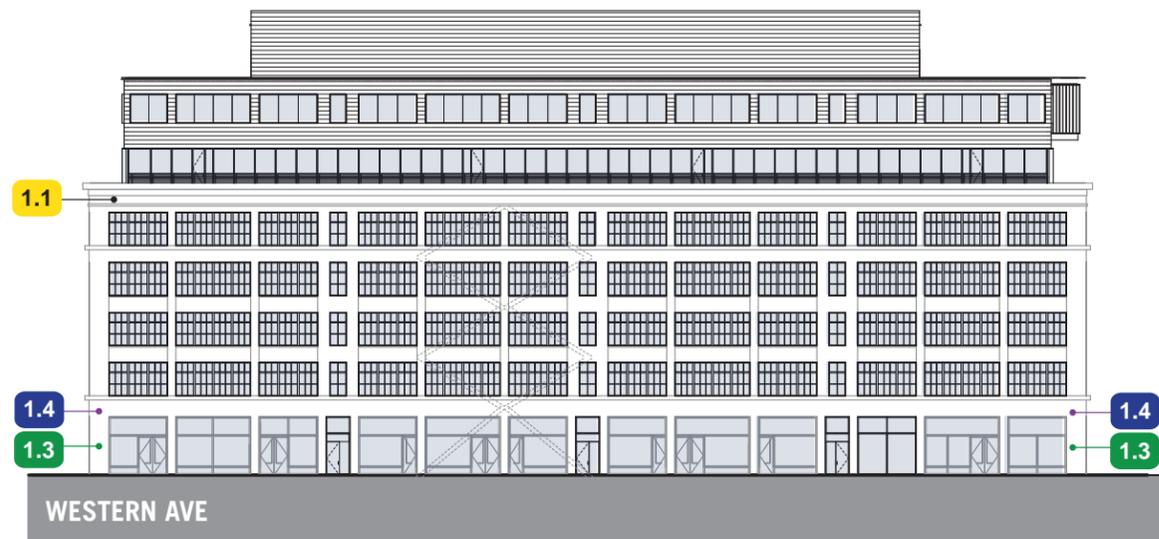
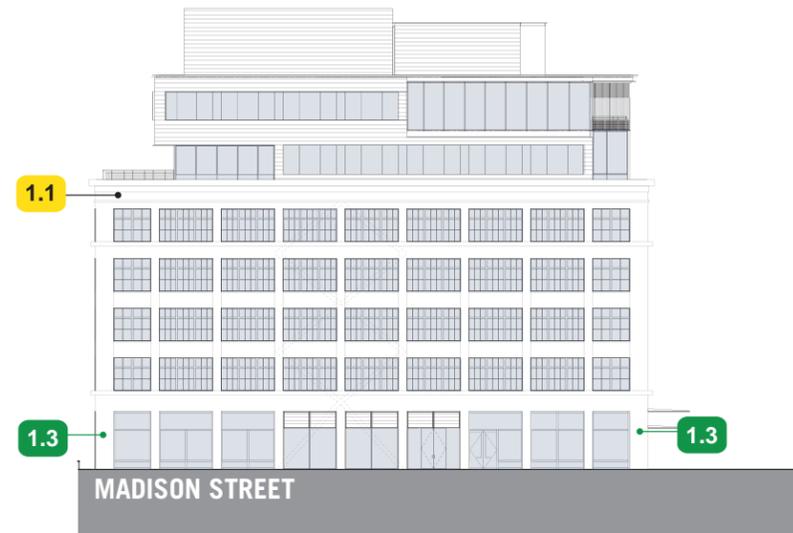
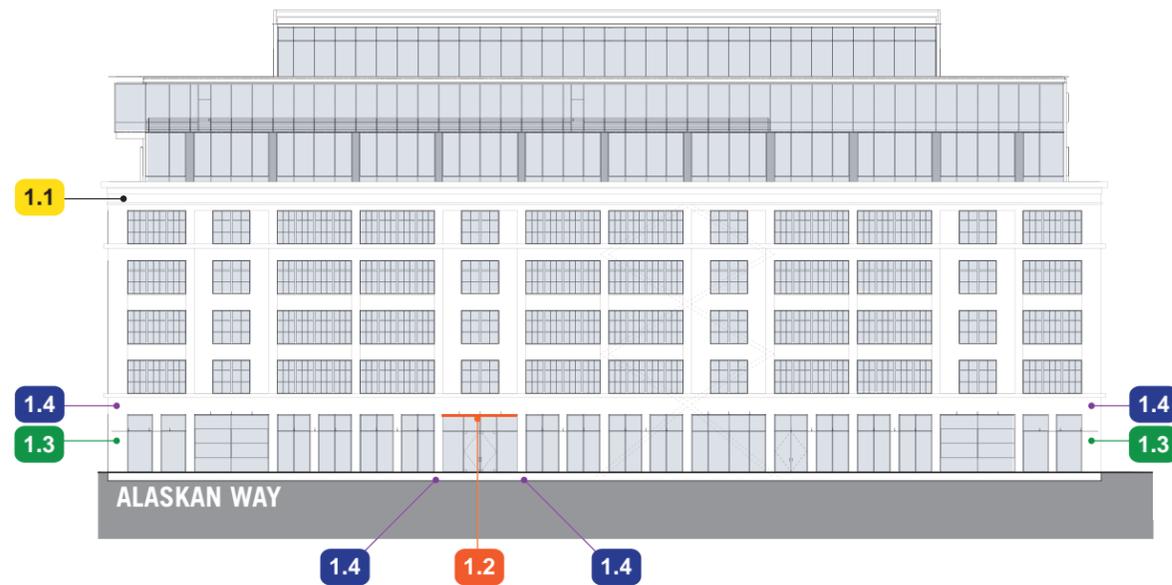


SIGN TYPE OVERVIEW

EXTERIOR

- 1.1 BUILDING TOP ID
- 1.2 CANOPY BUILDING ID
- 1.3 PEDESTRIAN BUILDING ID
- 1.4 ADDRESS ID

SIGNAGE LOCATION PLAN

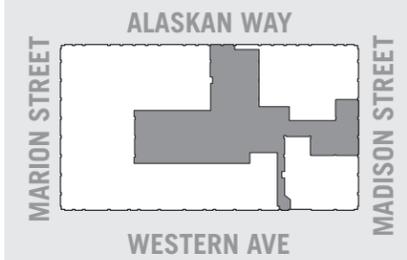


SIGN TYPE OVERVIEW

EXTERIOR

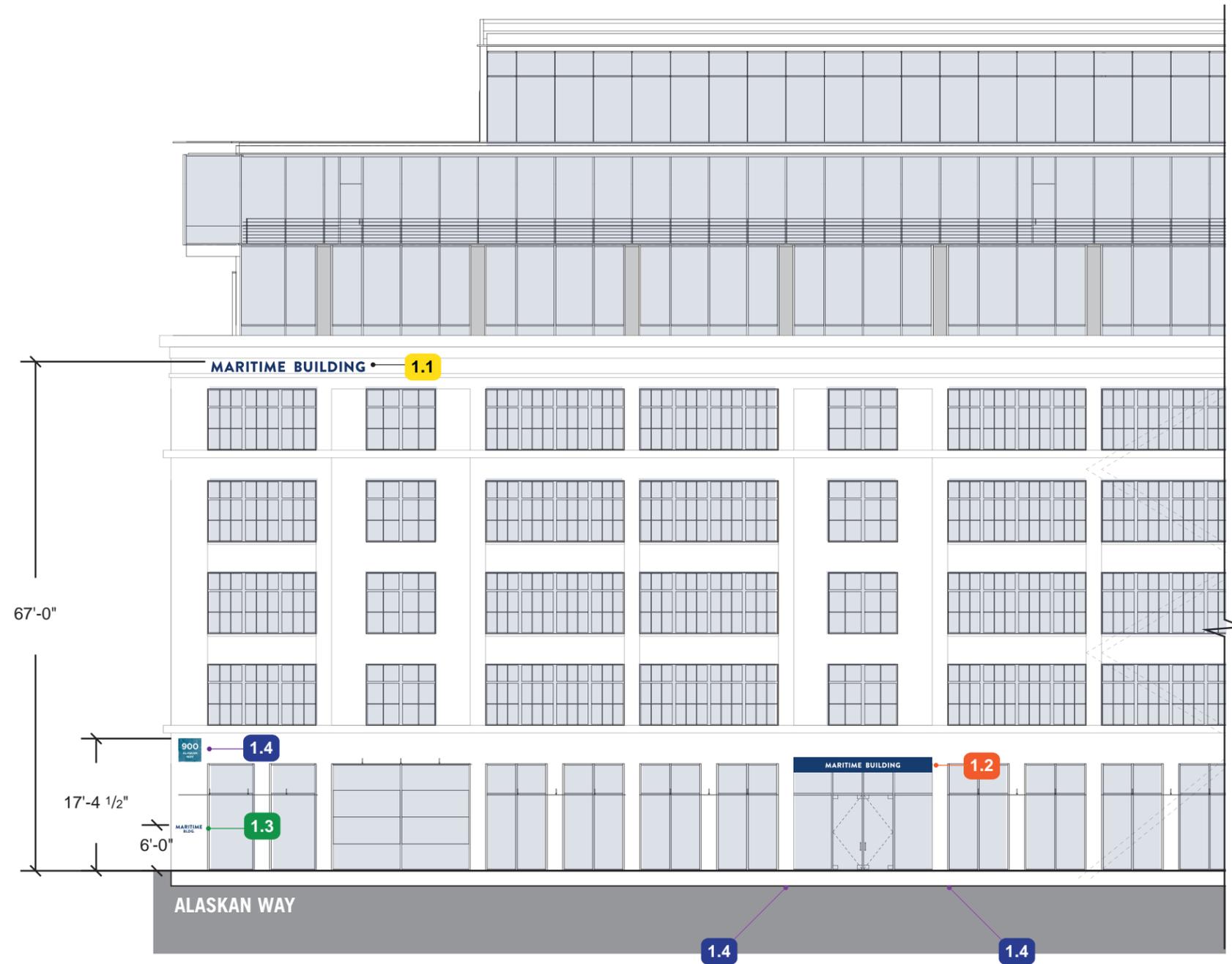
- 1.1** BUILDING TOP ID
- 1.2** CANOPY BUILDING ID
- 1.3** PEDESTRIAN BUILDING ID
- 1.4** ADDRESS ID

SITE PLAN



11.0

SIGNAGE CONCEPT PLAN

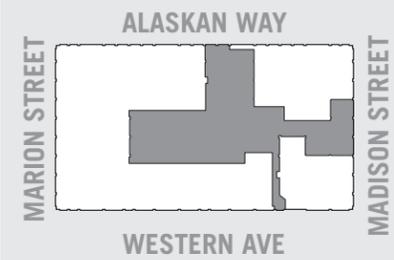


SIGN TYPE OVERVIEW

EXTERIOR

- 1.1 BUILDING TOP ID
- 1.2 CANOPY BUILDING ID
- 1.3 PEDESTRIAN BUILDING ID
- 1.4 ADDRESS ID

SITE PLAN



SAMPLE ELEVATION

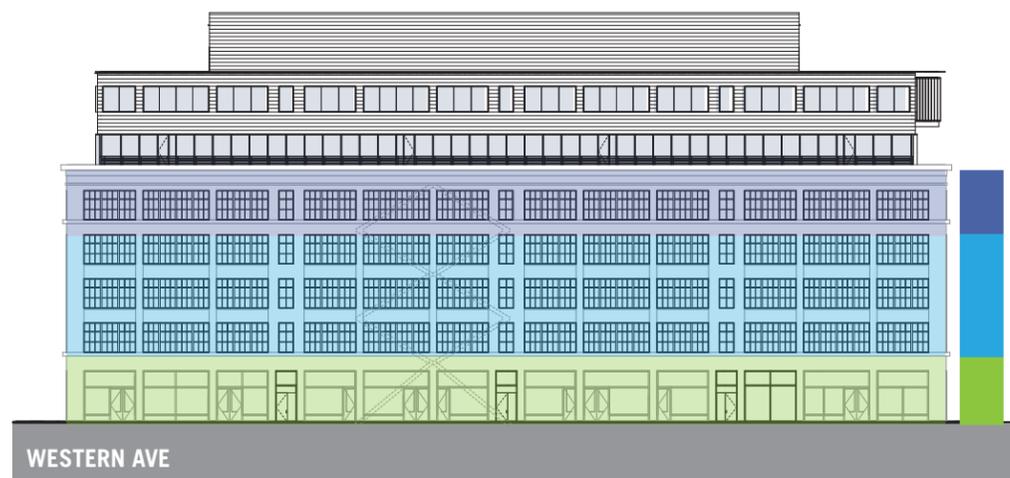
SCALE: 1/16" = 1'

SIGNAGE CONCEPT PLAN

SIGHT LINE ANALYSIS & POTENTIAL TENANT SIGNAGE POSSIBILITIES



Consistent for all 4 facades



Consistent for all 4 facades

SIGNAGE SIGHT LINE ZONES

- APPROACH VIEW
- VEHICULAR / PEDESTRIAN VIEW
- PEDESTRIAN VIEW

PROSPECTIVE TENANT SIGNAGE ZONES

- ANCHOR OFFICE TENANT
- OFFICE TENANT
- RETAIL TENANT

SIGNAGE MATERIALS LOOK & FEEL



11.0

SIGNAGE CONCEPT PLAN

LOGO LOCK-UPS FOR SIGNAGE

MARITIME BUILDING

MARITIME BLDG.

MARITIME BUILDING - THE BRAND

The signage is informed by the building's brand. Represented here is how the brand translates for use in signage - including logo lock-ups and color palette.

STANDARD COLORS



Dark Blue

PANTONE 294C
C100, M69, Y7, K30
R0, G47, B108
RAL 5002
#2 MP4526 Anil Blue



Blue

PANTONE 7707C
C100, M18, Y12, K52
R0, G97, B127
RAL 5019
#115 MP00375 Guantanamo Teal



Light Blue

PANTONE 7696C
C56, M9, Y9, K21
R99, G153, B174
RAL 5024
#71 MP05753 Daisy Blue

ACCENT COLORS



Yellow Green

PANTONE 458C
C5, M4, Y73, K7
R217, G199, B86
#3 MP01508



White

C0, M0, Y0, K0
R255, G255, B255

MARITIME BUILDING - THE BRAND

The lighting for the Maritime Building project will address the historical context of the building as a landmark, while developing functional and aesthetic lighting for its new occupants and enriching the neighborhood. Quality lighting is a result of a layered and modulated lighting system that is strategically designed to accentuate building form and materials while addressing visual tasks. Careful and deliberate planning of the lighting will provide a system that supports the beauty and integrity of the landmark building while enhancing comfort and performance for the new occupants. In the case of the Maritime project, quality lighting also refers to lighting that helps address and achieve sustainability goals for the project.

Good Visibility

Good visibility is important for the comfort of occupants in any of the various space types in the building. Good visibility is the combination of both adequate lighting levels as well as visual acuity. It is the consideration of both illuminance criteria and ratios for the space that create an environment with good visibility and enhance the feeling of safety.

Luminance Contrast

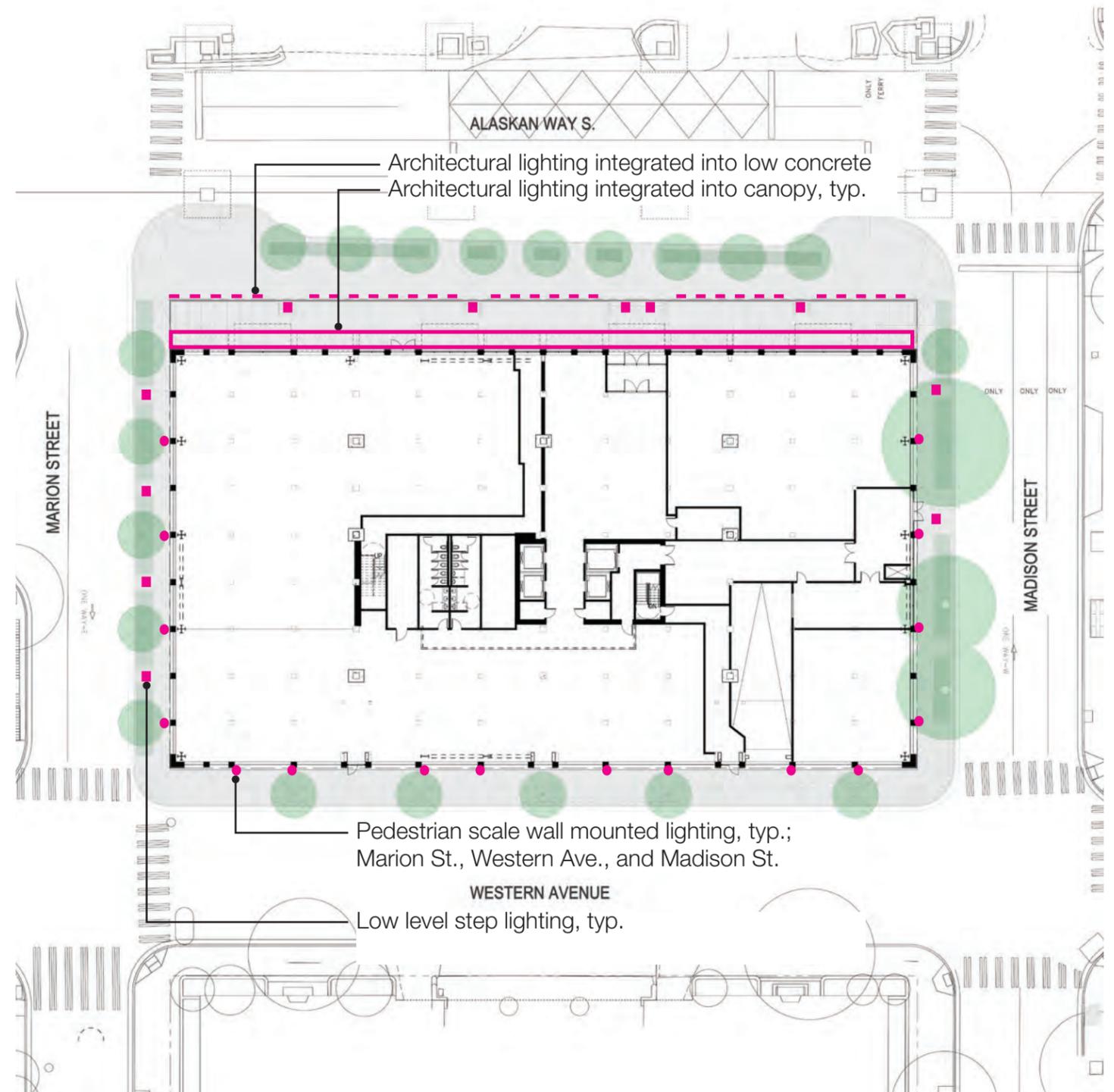
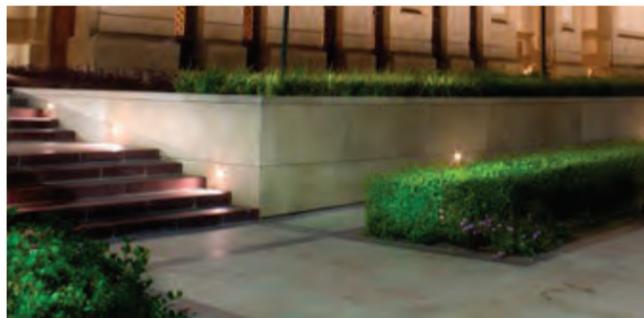
Related to visual acuity, balancing the luminance contrast between objects can help the eye more readily distinguish between them. Carefully considering luminance contrast can be used strategically in areas of visual interest to highlight architectural features or important points for wayfinding.

Color Temperature and Color Rendition

The color of light and the ability to see colors well are important considerations in the selection of lamps and fixtures for the Maritime project. Color temperature can be used to manipulate the perception of depth and enhance the building's architecture and nighttime presence. A warm color temperature of 3000K will be used throughout the exterior of the project to evoke a warm, inviting environment and create uniformity in the exterior appearance of the building.

Visual Cues

Efficient wayfinding is supported by lighting that reveals a simple, clear, and uncluttered building form. Visual cues accented by light help to reinforce basic building plan organization and create a hierarchy of visual information for the occupants to process.



12.0

RENDERINGS





12.0

RENDERINGS





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13.0

HALA DESIGN OPTION



Beacon Capital Partners wish to participate in the Housing Affordability and Livability Agenda (HALA) program when adopted. The submitted MUP design will be updated and amended to increase the height of the proposed addition by one full floor. Also, the width of the addition will be increased locating the east façade just inside the parapet of the existing Maritime Building. As a result, the design team will request a departure from the required upper level façade modulation.

The overall architectural expression of a modern lightweight addition described in the MUP submittal will be maintained with this increased height. The mass of the additional floor will project beyond the lower floor retaining the goal to create a strong horizontal expression on the west façade. At a total of eight floors plus an amenity / mechanical pavilion, the new Maritime Building remains in scale to other waterfront buildings in the neighborhood.

Complete and substantial renovation of the existing Maritime Building – approximately 185,200 GSF existing office space.

Addition of three new office floors – approximately 84,900 GSF.

Addition of a rooftop amenity pavilion.
Landscaped terraces on level 7 and the rooftop
Street level retail of approximately 23,000 GSF.

Total development is approximately 277,500 GSF.

Approximately 85 parking stalls located on the existing basement level.

13.0

HALA DESIGN OPTION

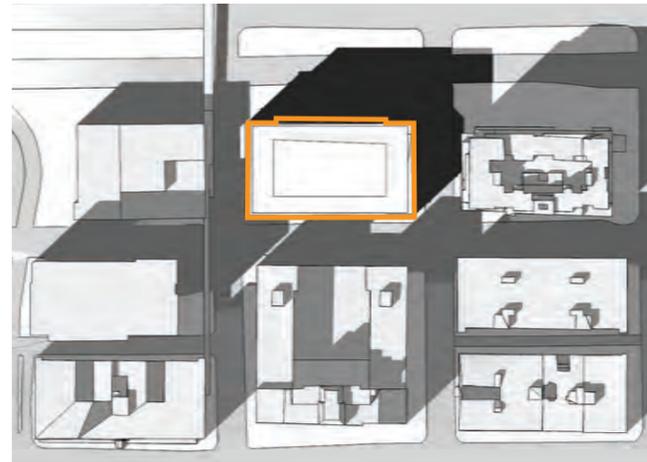
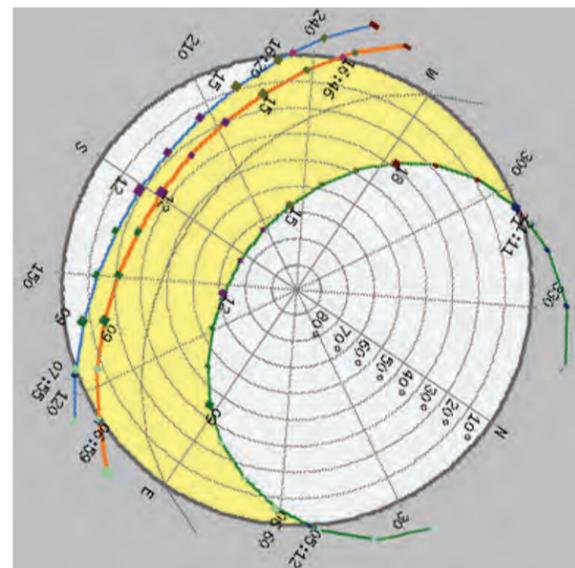
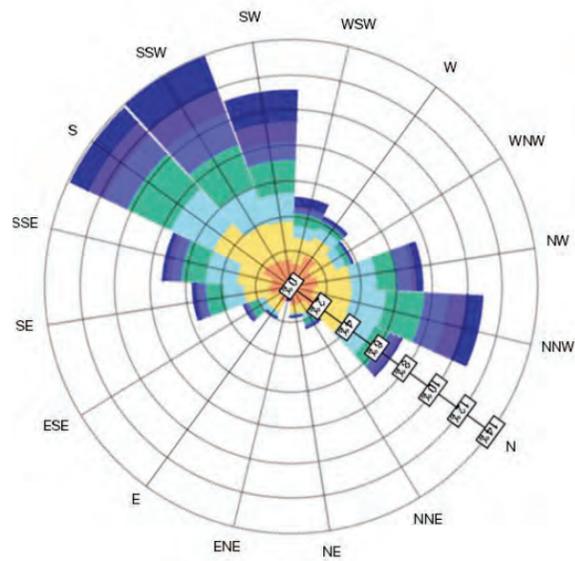


Option 2



Preferred Option

SITE SOLAR ANALYSIS - HALA DESIGN OPTION



Mar/Sep 21 _ 10 a.m.



Mar/Sep 21 _ 12 p.m.



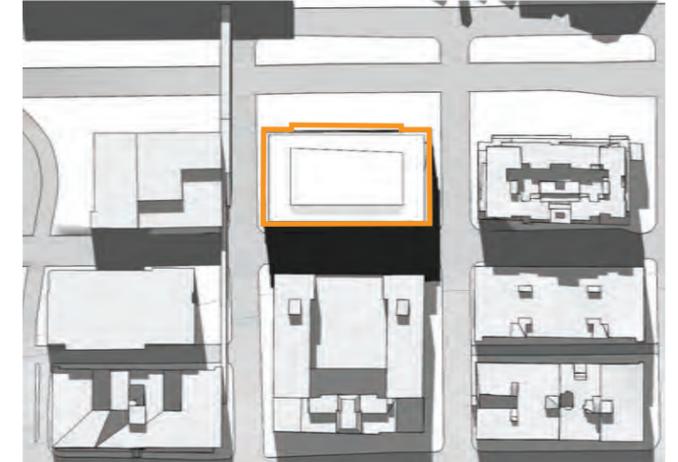
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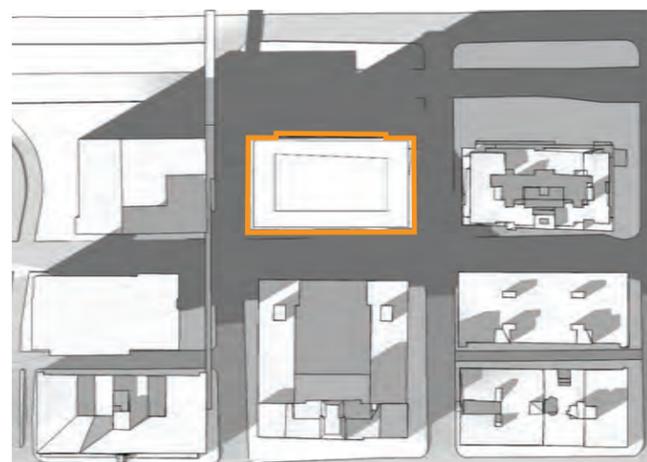
June 21 _ 10 a.m.



June 21 _ 12 p.m.



June 21 _ 4 p.m.



Dec 21 _ 10 a.m.



Dec 21 _ 12 p.m.



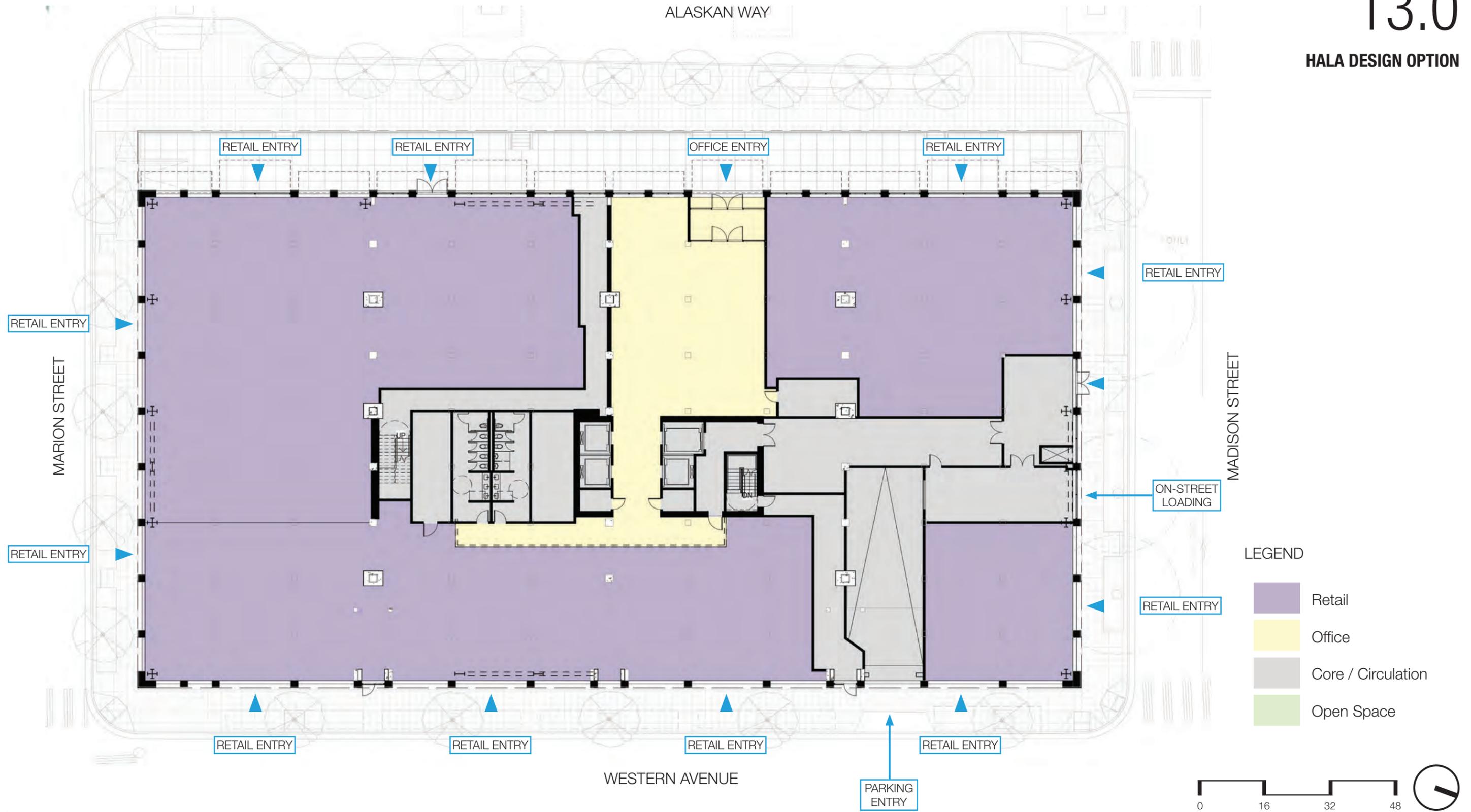
Dec 21 _ 4 p.m.

13.0

HALA DESIGN OPTION

ALASKAN WAY



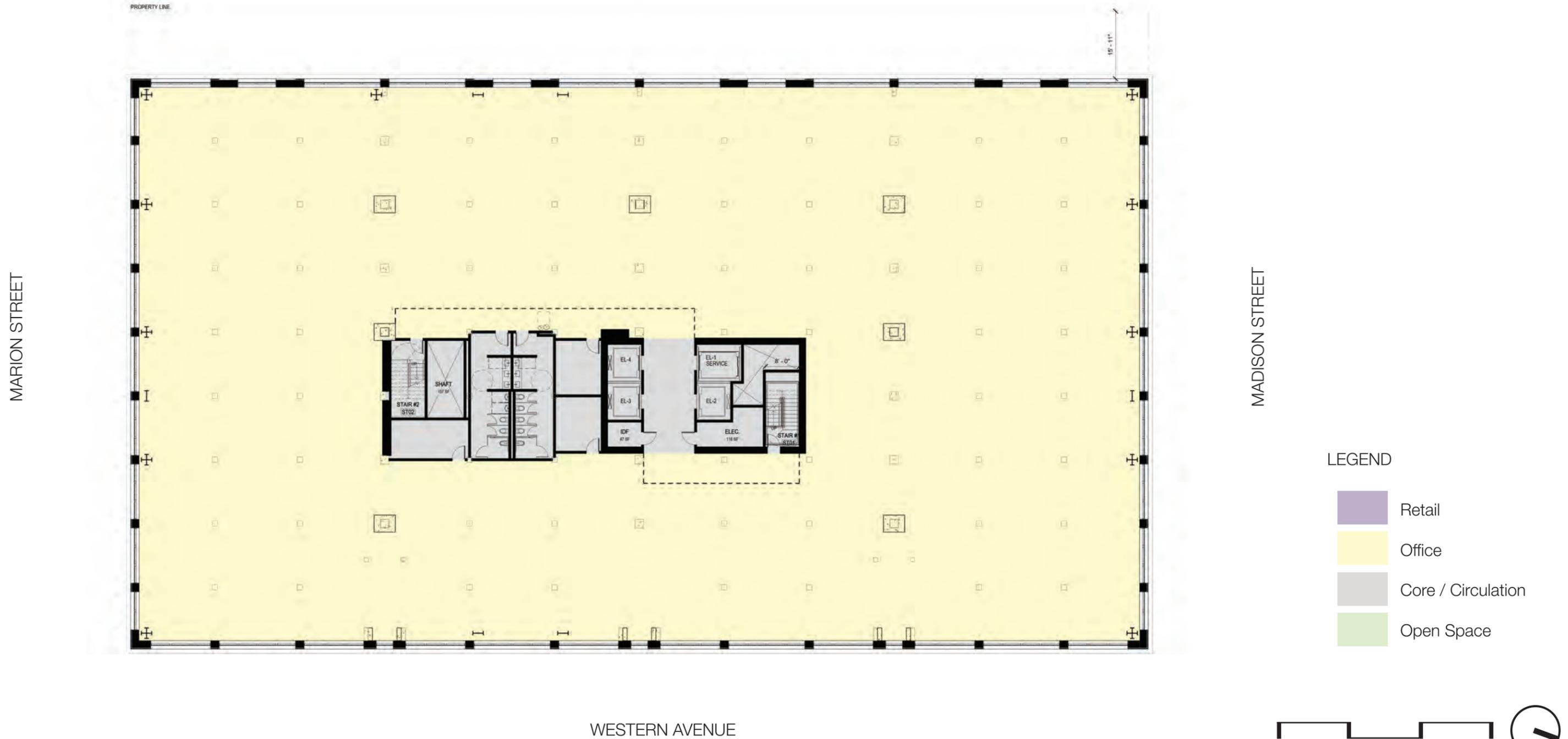


Ground Floor Plan

13.0

HALA DESIGN OPTION

ALASKAN WAY

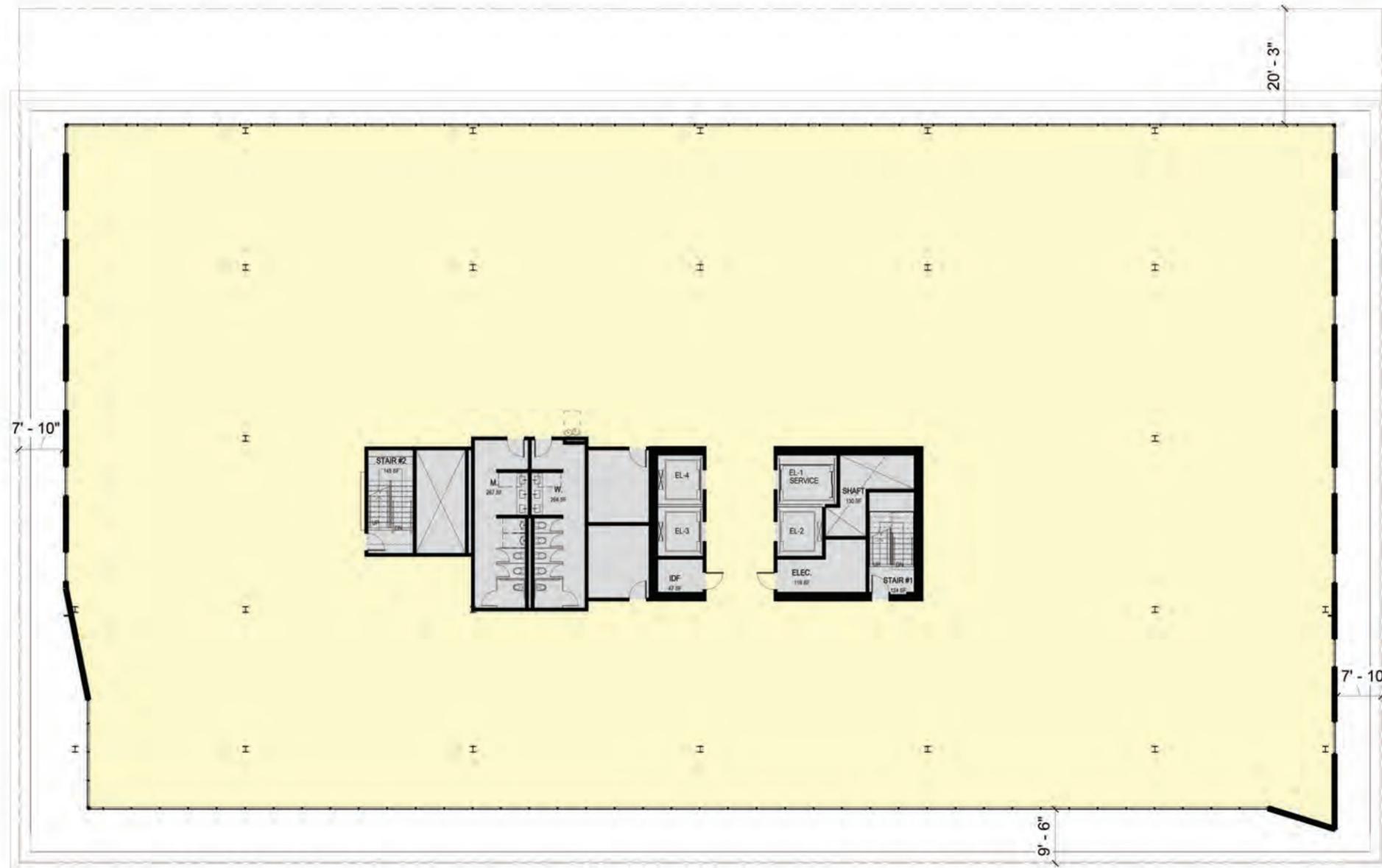


Floors 2-5

ALASKAN WAY

MARION STREET

MADISON STREET

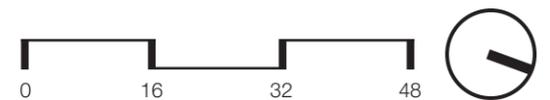


WESTERN AVENUE

LEGEND

- Retail
- Office
- Core / Circulation
- Open Space

Floor 6



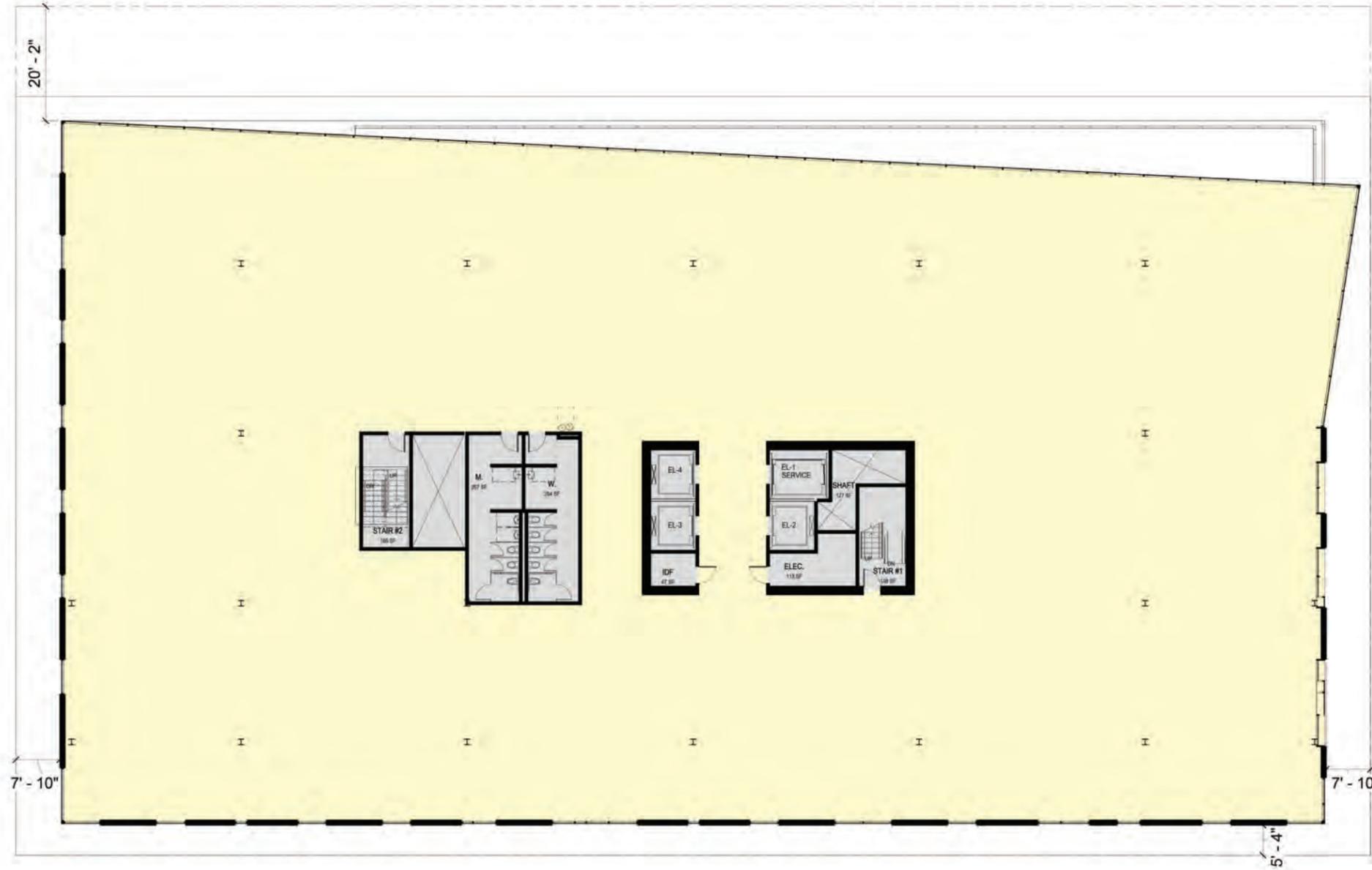
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HALA DESIGN OPTION

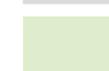
ALASKAN WAY

MARION STREET

MADISON STREET

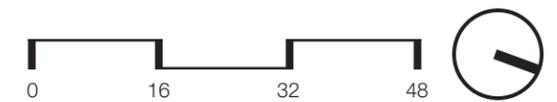


LEGEND

-  Retail
-  Office
-  Core / Circulation
-  Open Space

WESTERN AVENUE

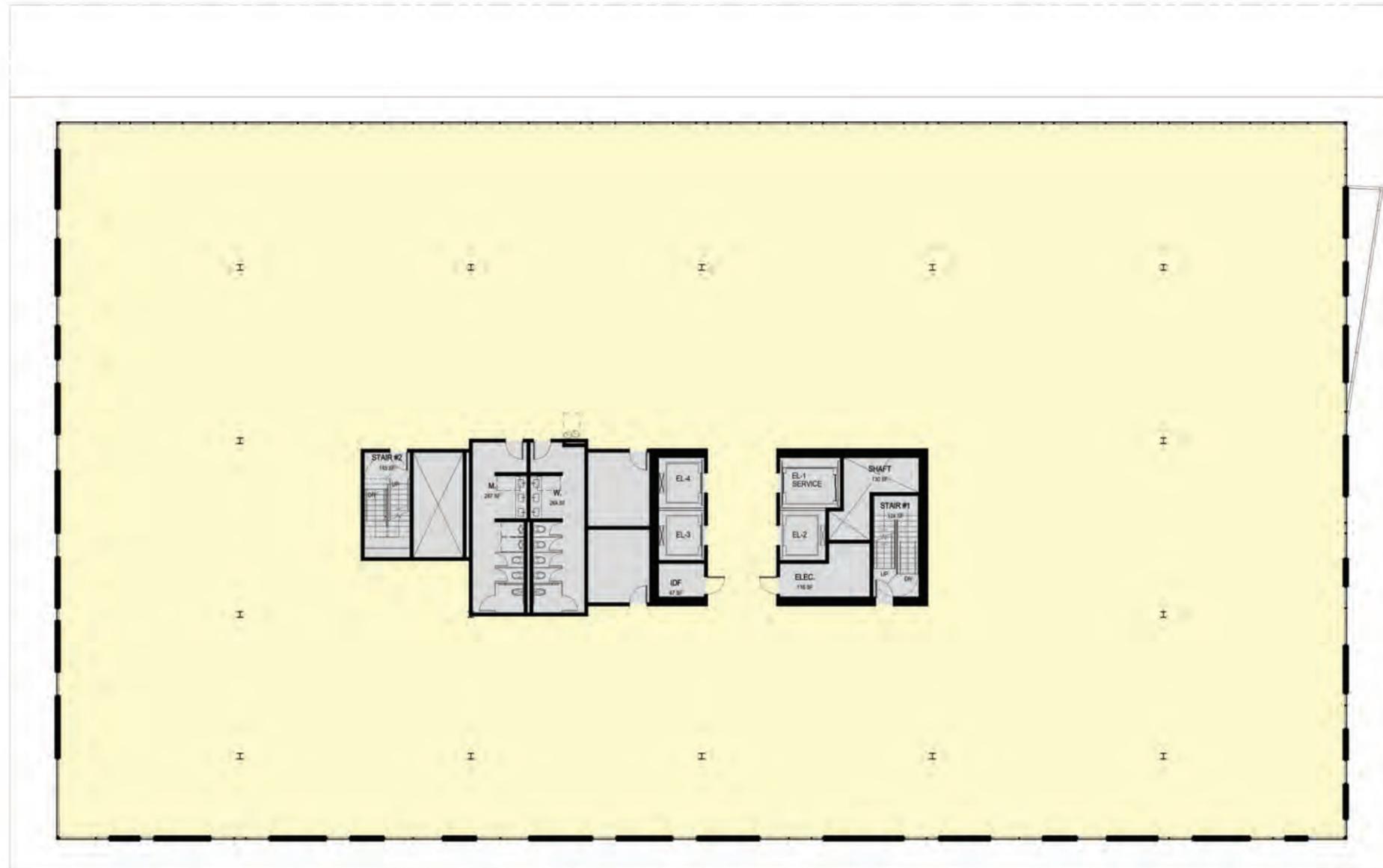
Floor 7



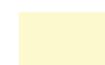
ALASKAN WAY

MARION STREET

MADISON STREET



LEGEND

-  Retail
-  Office
-  Core / Circulation
-  Open Space

WESTERN AVENUE

Floor 8



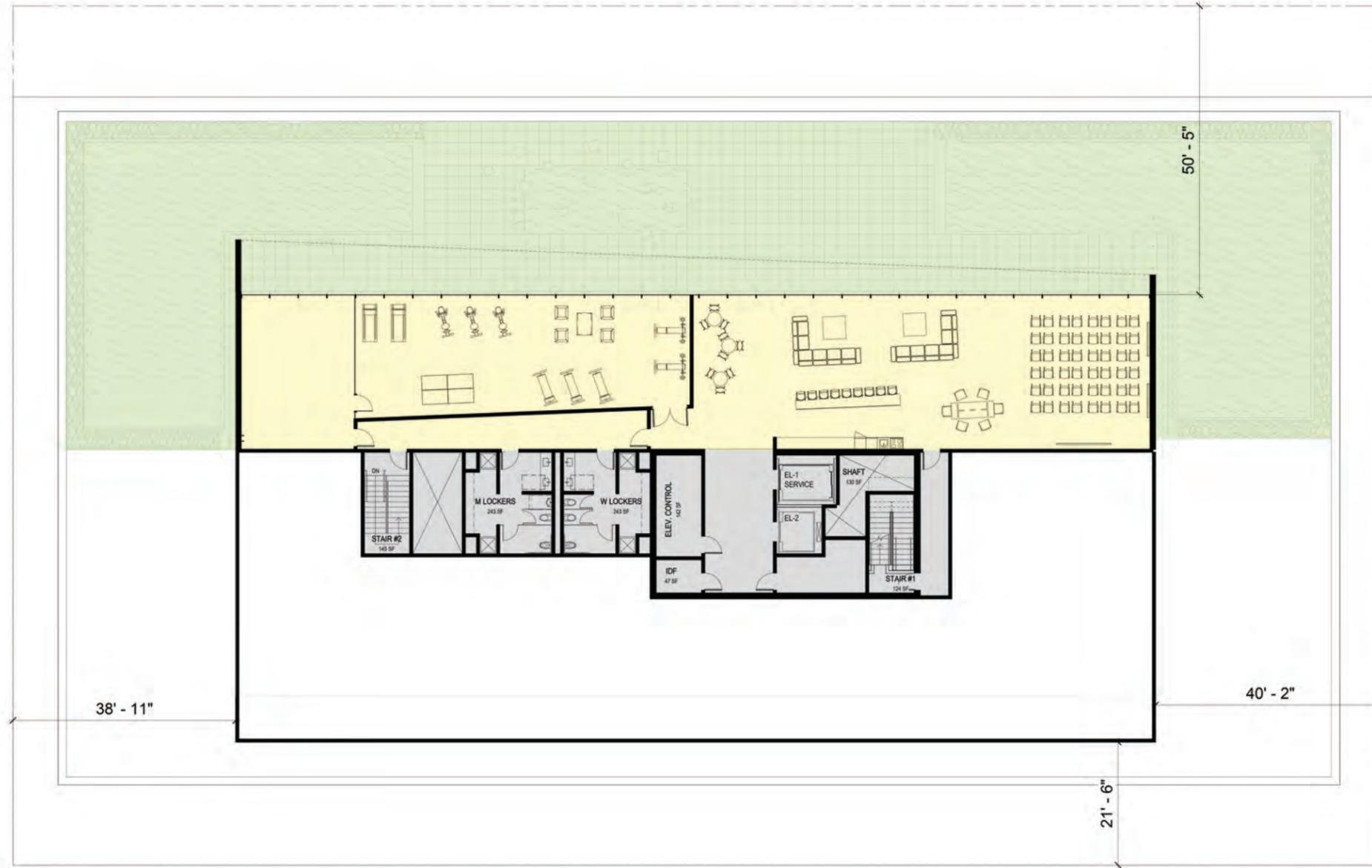
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HALA DESIGN OPTION

ALASKAN WAY

MARION STREET

MADISON STREET

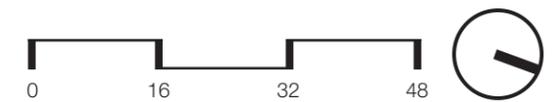


LEGEND

- Retail
- Office
- Core / Circulation
- Open Space

WESTERN AVENUE

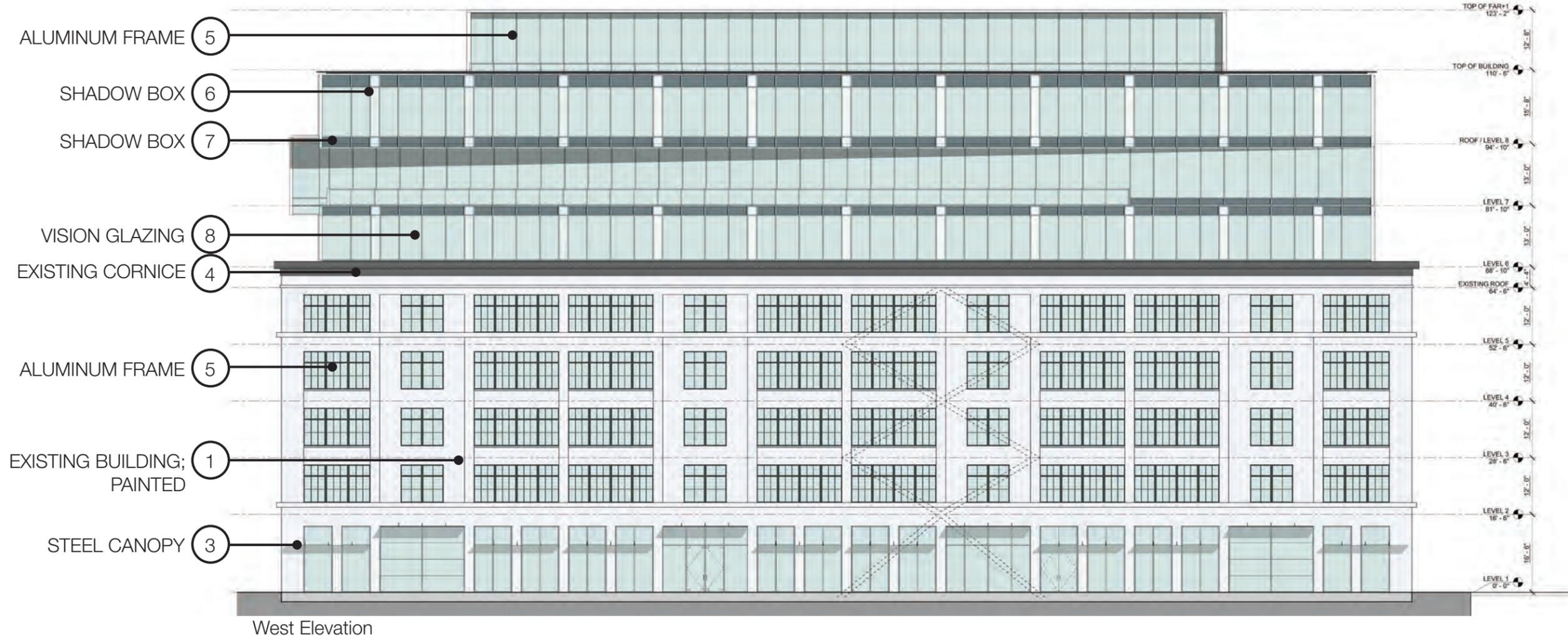
Level 9



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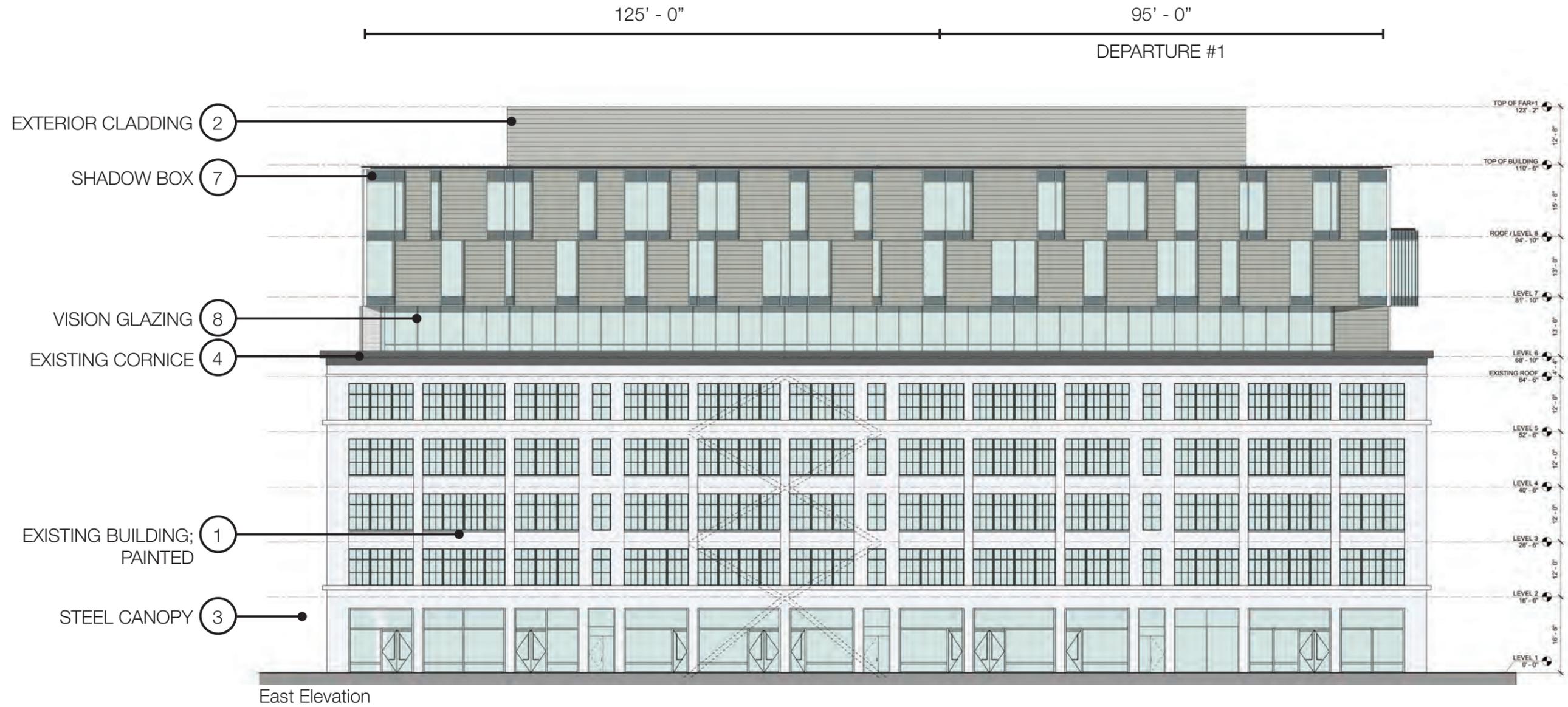
HALA DESIGN OPTION

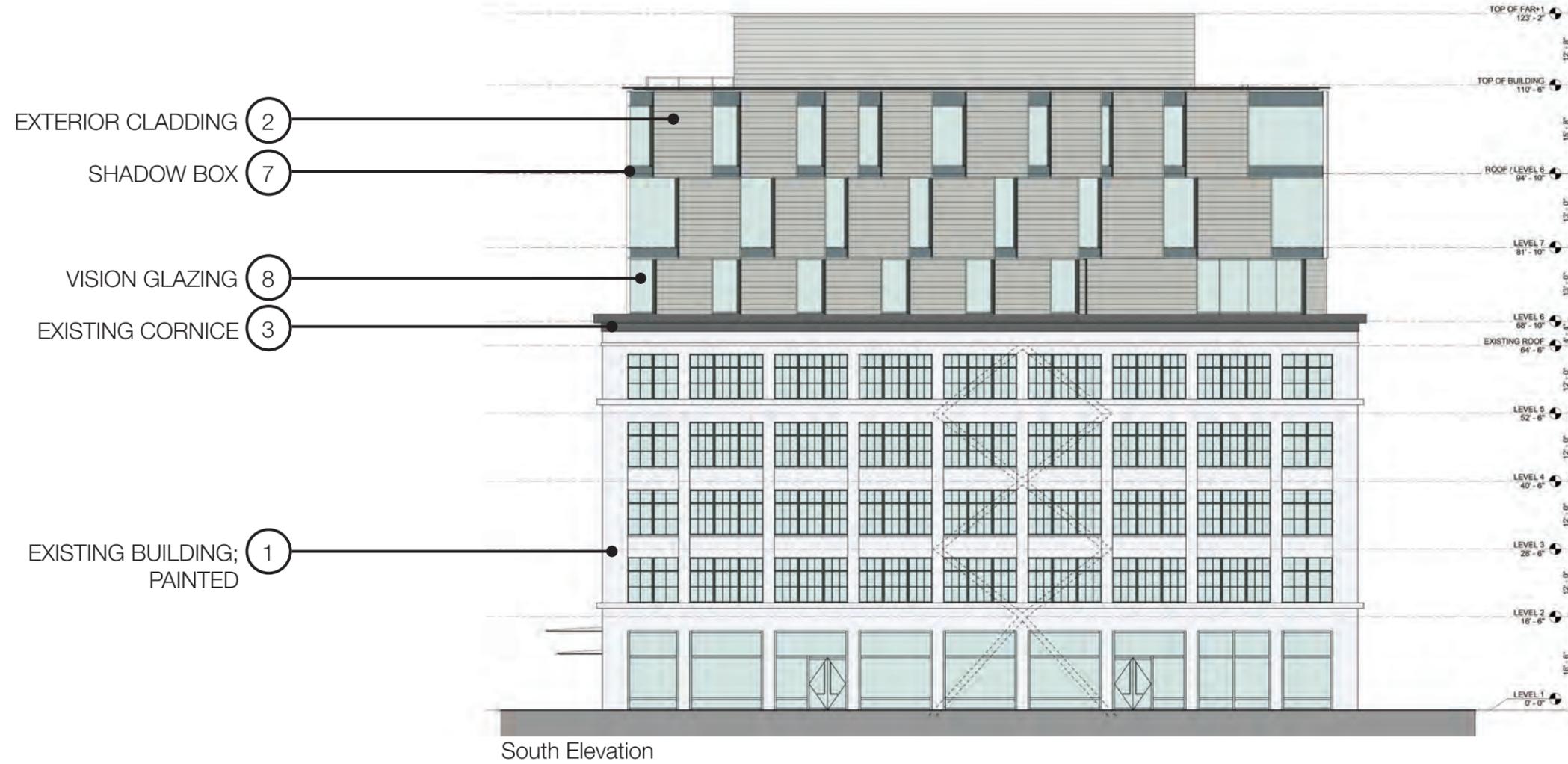




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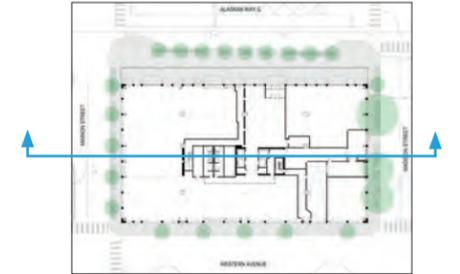
HALA DESIGN OPTION



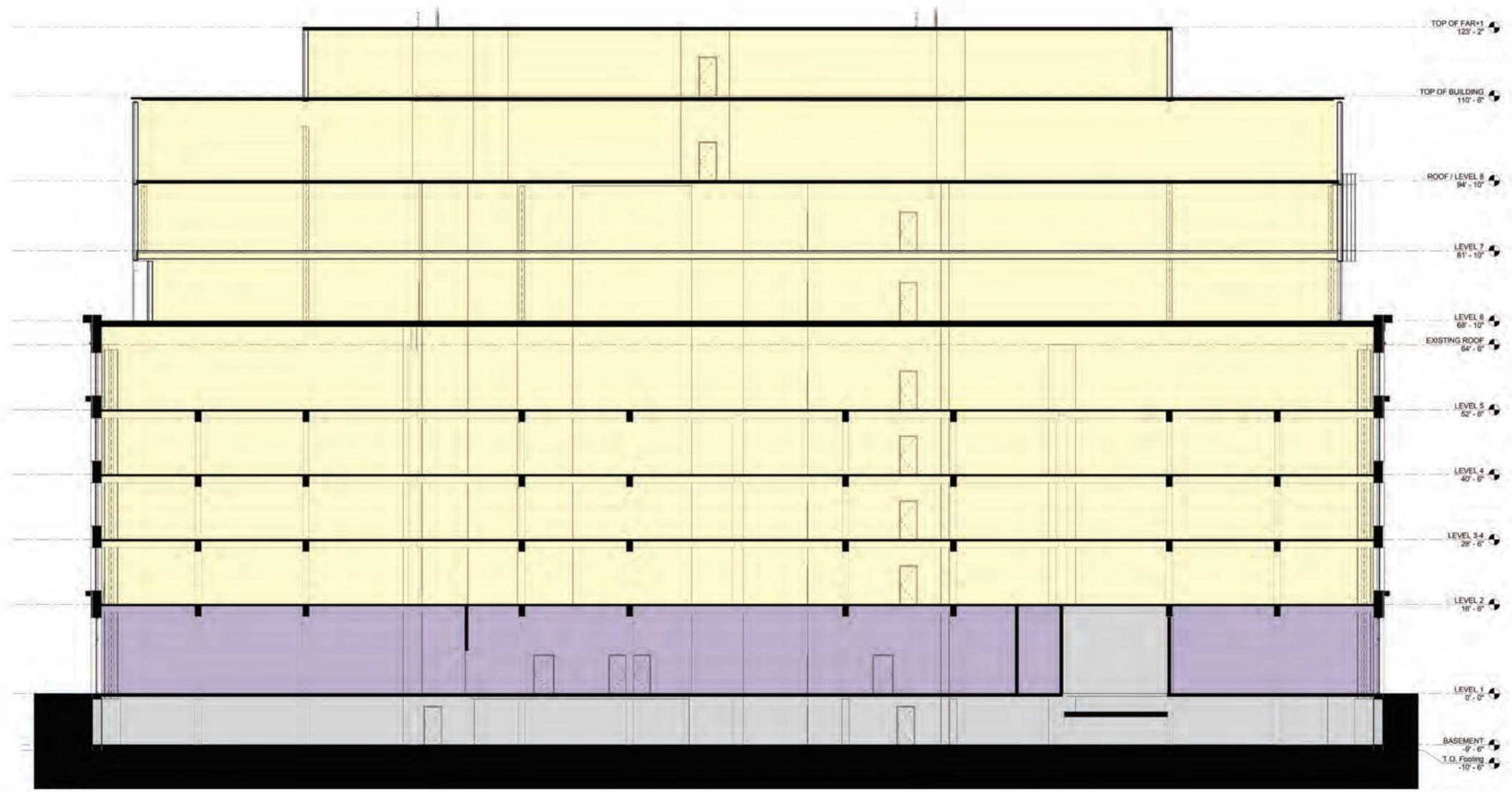


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HALA DESIGN OPTION



Key Plan



LEGEND

-  Retail
-  Office
-  Core / Circulation
-  Open Space

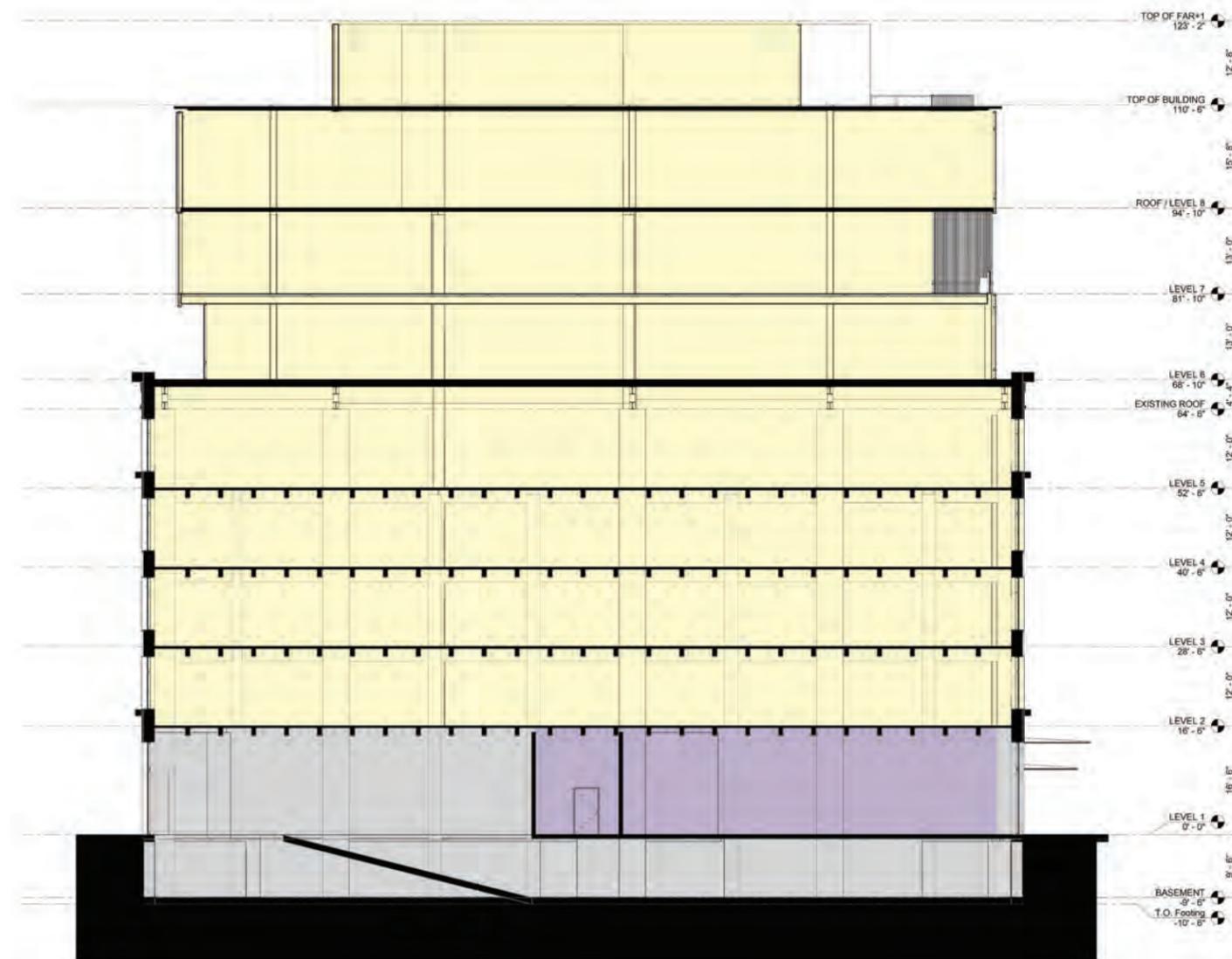
North - South Section

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HALA DESIGN OPTION

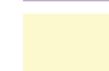


Key Plan



West - East Section

LEGEND

-  Retail
-  Office
-  Core / Circulation
-  Open Space

13.0

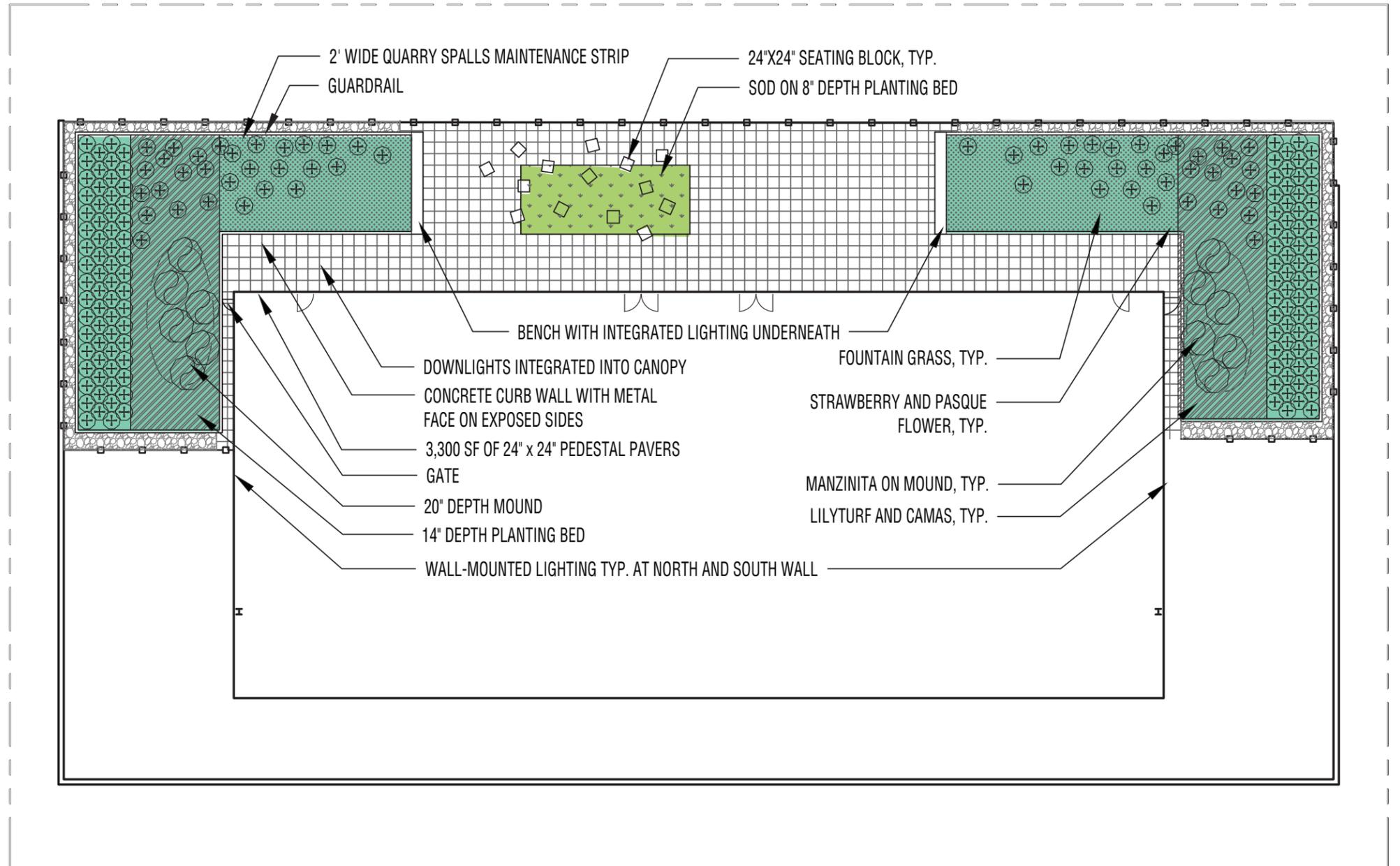
HALA LANDSCAPE ARCHITECTURE



STONE BLOCK SEATING



INTENSIVE GREEN ROOF PLANTING



FLOOR 8 - HALA

ROOFTOP PLANTING



MANZANITA



HAMELN DWARF FOUNTAIN GRASS



CALIFORNIA FUSCHA



BEACH STRAWBERRY



CAMAS



LILY TURF

13.0

HALA DESIGN OPTION



13.0

HALA DESIGN OPTION



13.0

HALA DESIGN OPTION



13.0

HALA DESIGN OPTION



14.0

HALA DESIGN OPTION

DEPARTURES

CODE SECTION 23.49.058

Subject

Downtown Mixed Commercial: Upper-level development standards

Requirements Summary

Façade modulation per Table 23.49.058A is required above a height of sixty (60) feet above the sidewalk for any portion of a structure located within fifteen (15) feet of a street property line. No modulation is required for portions of a façade set back fifteen (15) feet or more from a street property line.

Any portion of a façade exceeding the maximum length of façade prescribed on Table 23.49.058A shall be set back a minimum of fifteen (15) feet from the street property line for a minimum distance of sixty (60) feet before any other portion may be within fifteen (15) feet of the street property line.

Departure Request #1

Departure #1 - Façade Modulation along Western Avenue

Proposed façade length: 220 feet

Maximum façade length: 125 feet

See page 68 for elevation diagram.

Rationale

In order to maintain the horizontal proportionality requested by the Landmarks Board, the HALA option extends the proposed massing to within 15 feet of the property line along Western Avenue, Madison and Marion Streets. Along Western Avenue, the first additional floor has been inset to respect the cornice line of the existing building and to enhance the horizontal proportions of the floors above. Along Marion and Madison Streets, façade articulation and patterning echo the proportionality of the existing facades below, while providing a clear distinction between the original structure and new addition.

Design Guidelines Supported:

A-1.c

Site Planning and Massing; patterns of urban form.

A-2.a

Site Planning and Massing; sculpt or profile the facades.

B-1.b

Architectural Expression; respond to neighborhood context.

B-1.d

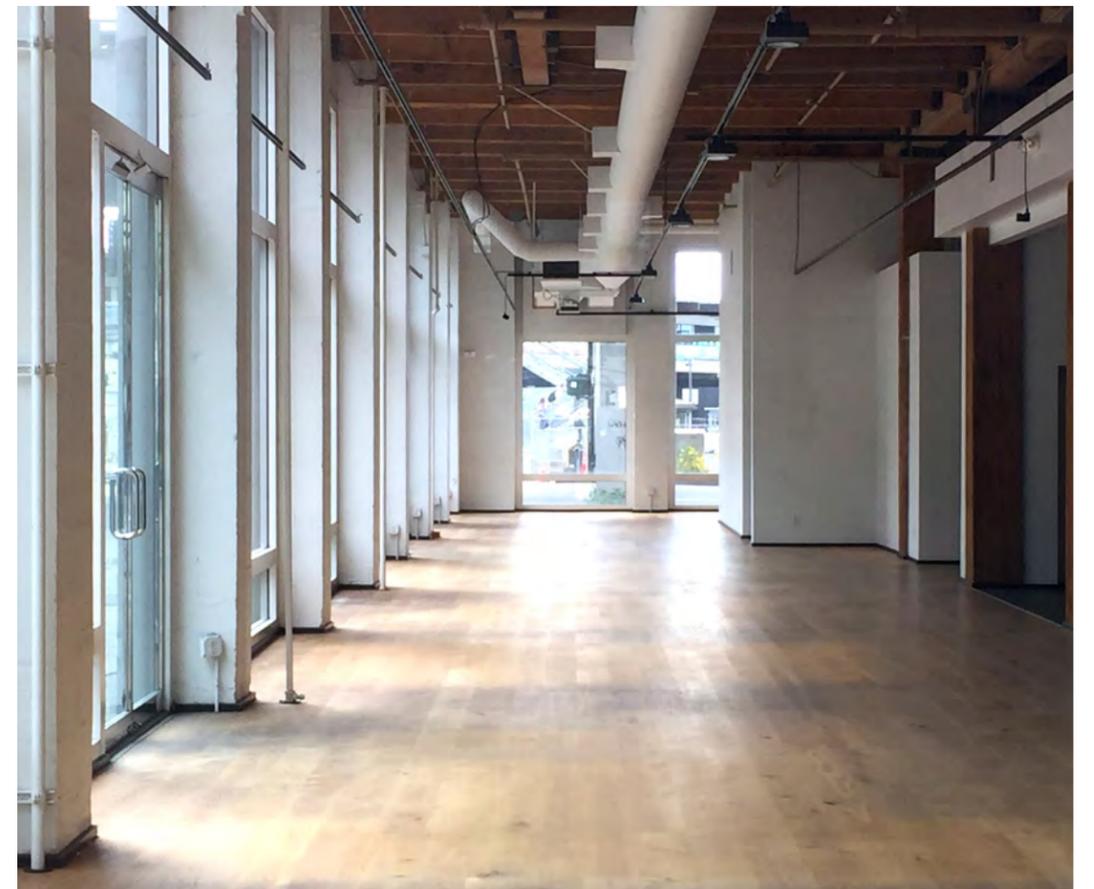
Architectural Expression; respond to neighboring buildings with effective massing compositions.

B-3

Architectural Expression; reinforce the positive urban form & architectural attributes of the immediate area.

B-4

Architectural Expression; design a well-proportioned and unified building.





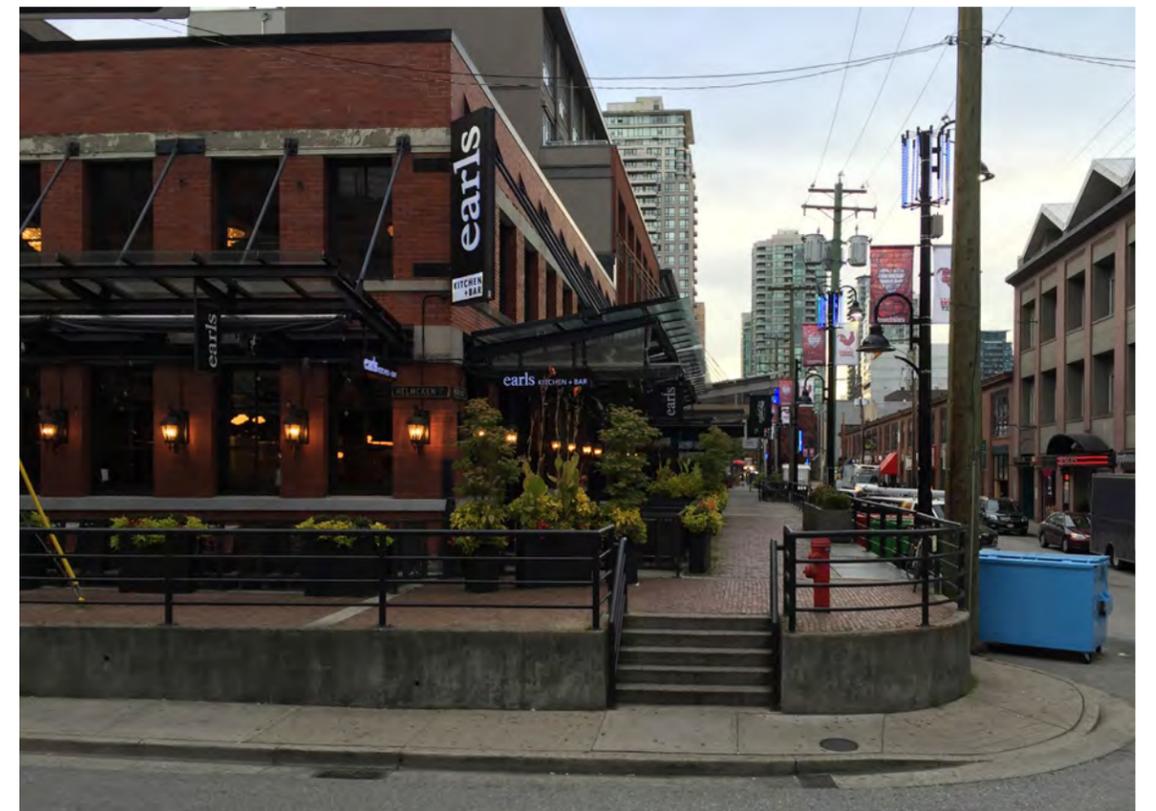




WESTERN AVENUE - RETAIL



WESTERN AVENUE - RETAIL



3 CONCEPTS AT EDG & GENERAL BOARD RESPONSE

GENERAL BOARD COMMENTS

“Continue to develop a landscape plan for all four sides of the structure that would take into account the volumes of people (and dogs) utilizing the areas for a variety of purposes”

“**Maintain the existing scale** along Western Avenue”

“Relocating the loading area for the structure to Madison Street was a good move”

“There was support for the location and configurations of the two proposed lobbies”

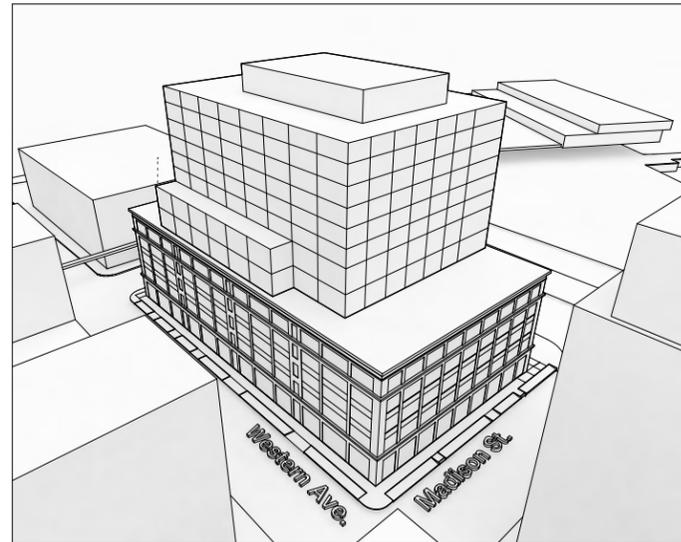
“The applicants preferred scheme showed promise, although the **stepping back up the topographic slope** conveyed in the second conceptual scheme was not without its appeal”

“The raised dining terrace and circulation space along the ground floor of the waterfront facade was conceptually strong”

“The north and south facades of the additional structure **need not be symmetrical**; support was shown for exploring more extensive opportunities for balconies as part of the composition of the residential floors”

“The Board members would like to see **explorations of asymmetrical upper facades** and optional alternatives to the overhanging cantilevered office space and a clear demonstration of the superiority of specific ways the upper and lower structures might otherwise interlock at the Maritime Building’s cornice line.”

CONCEPT 1



Board Response at EDG

- Least preferable due to the lack of setback at Alaskan Way from existing structure.

CONCEPT 2



Board Response at EDG

- The Board responded favorably to this scheme due to it’s response to the site’s natural topography

CONCEPT 3 | PREFERRED



Board Response at EDG

- The Board responded favorably to this scheme, but showed concern around the office floors overhanging the cornice of the existing structure.

“WE ARE LANDMARKED...”



PRECEDENTS



Penthouse and Renovation - Vienna, Austria



Metropolis Center - Bucharest, Romania



564 Beatty Street - Vancouver, BC

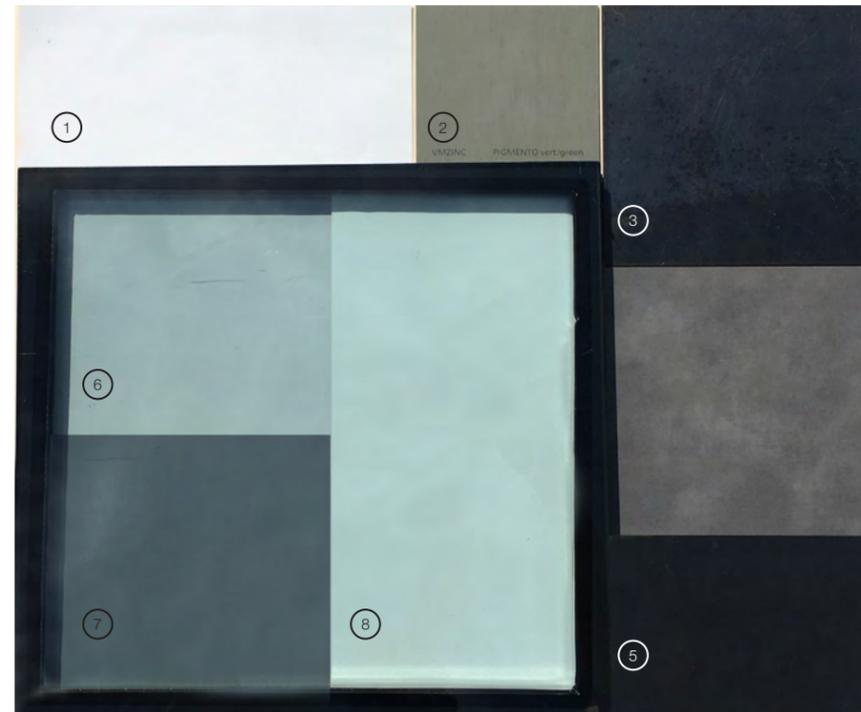
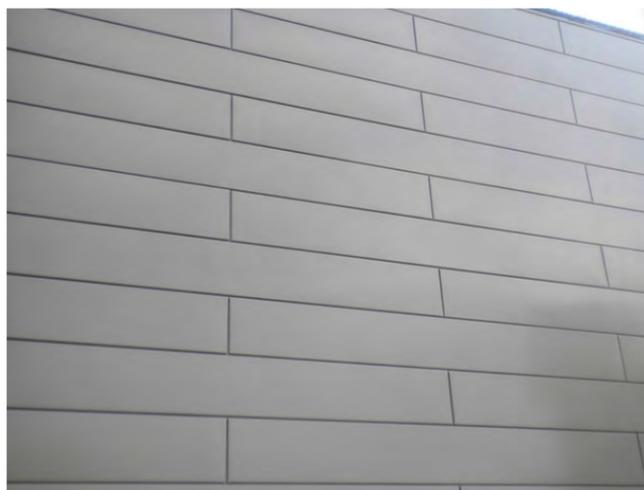
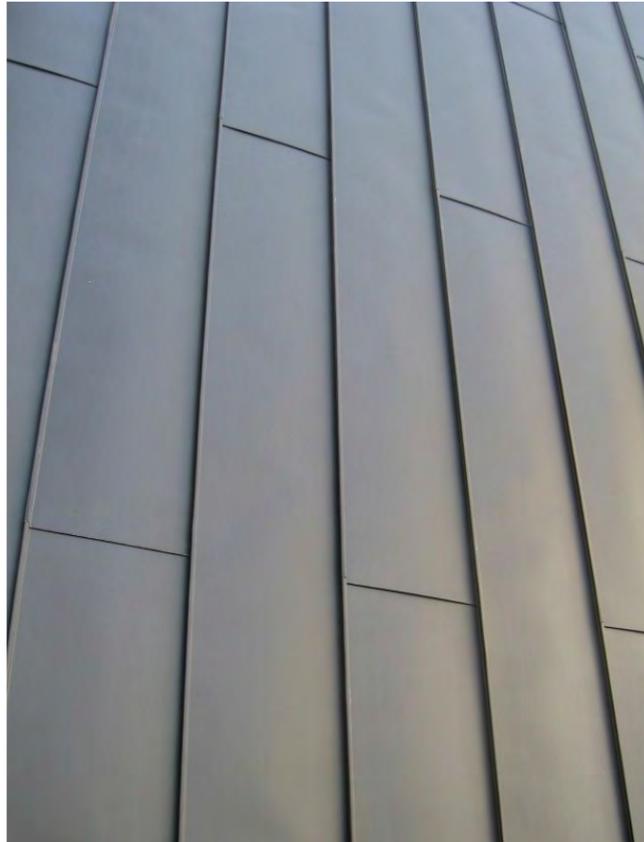


330 Hudson Street - New York, NY

MASSING OPTION #3



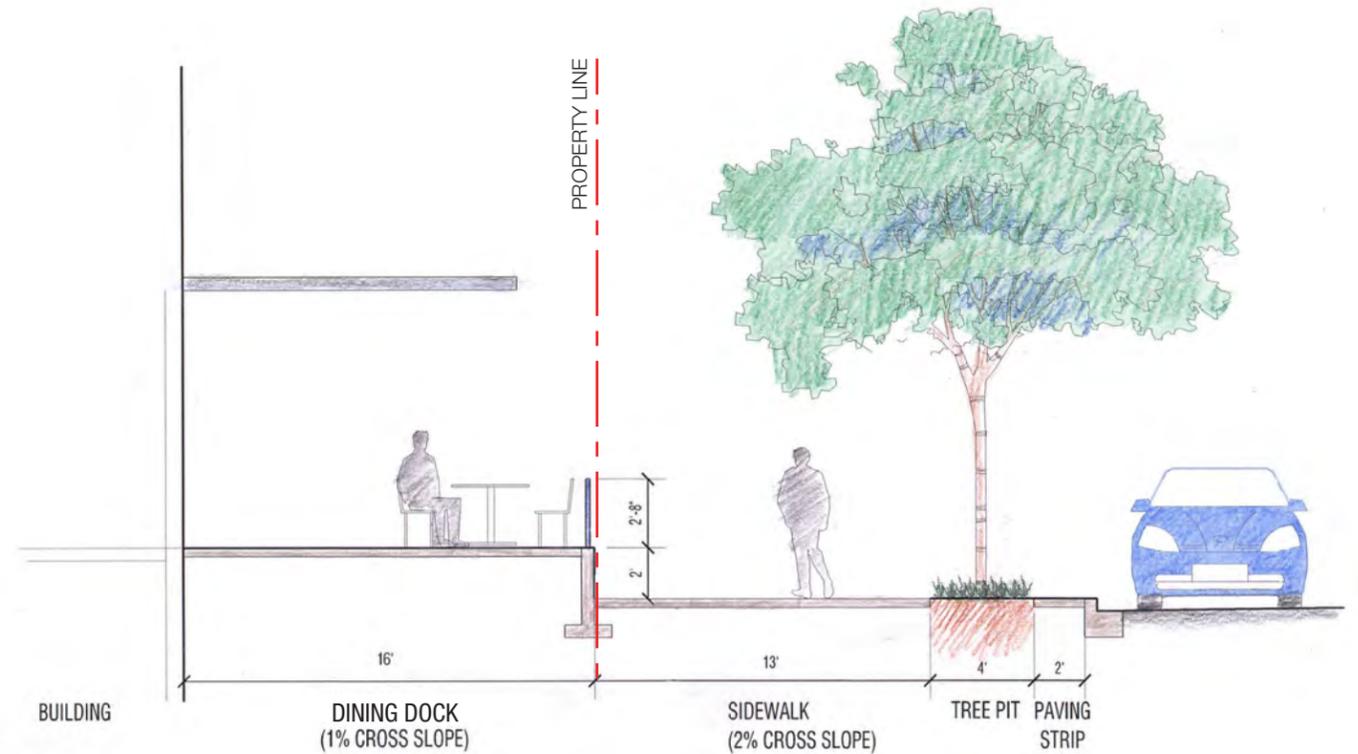
MATERIAL & COLOR PALETTE



1. Benjamin Moore - Stone White 2120-70
2. Painted Metal Panel - Pigmento Green
3. Benjamin Moore - Shaker Gray 1594
4. Steel - Blackened
5. Aluminum Frame - Black
6. Shadowbox - Back painted vision glass light gray
7. Shadowbox - Back painted Vision Glass dark gray
8. Glazing - Vision Glass

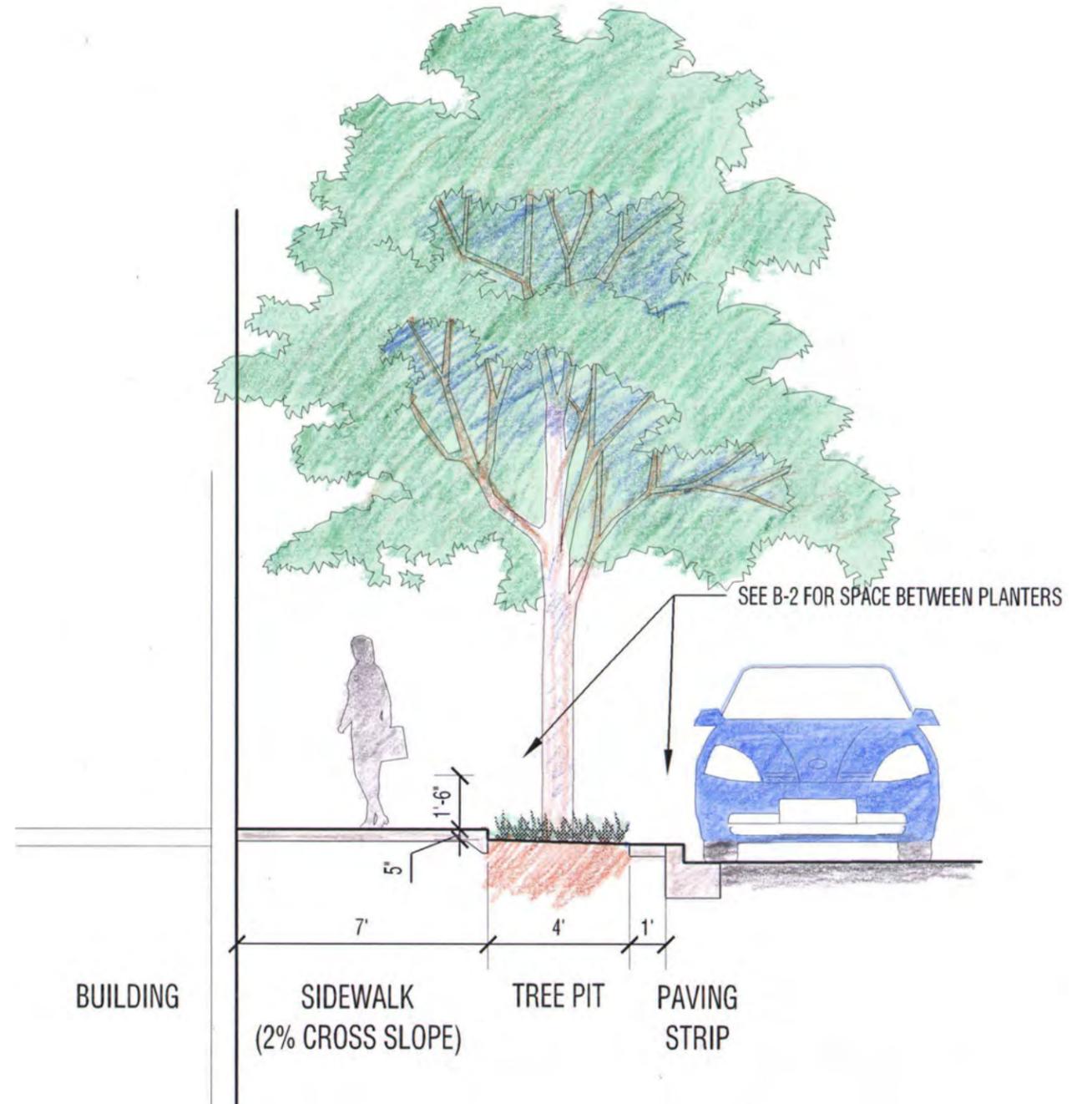
STREETSCAPE DEVELOPMENT

ALASKAN WAY



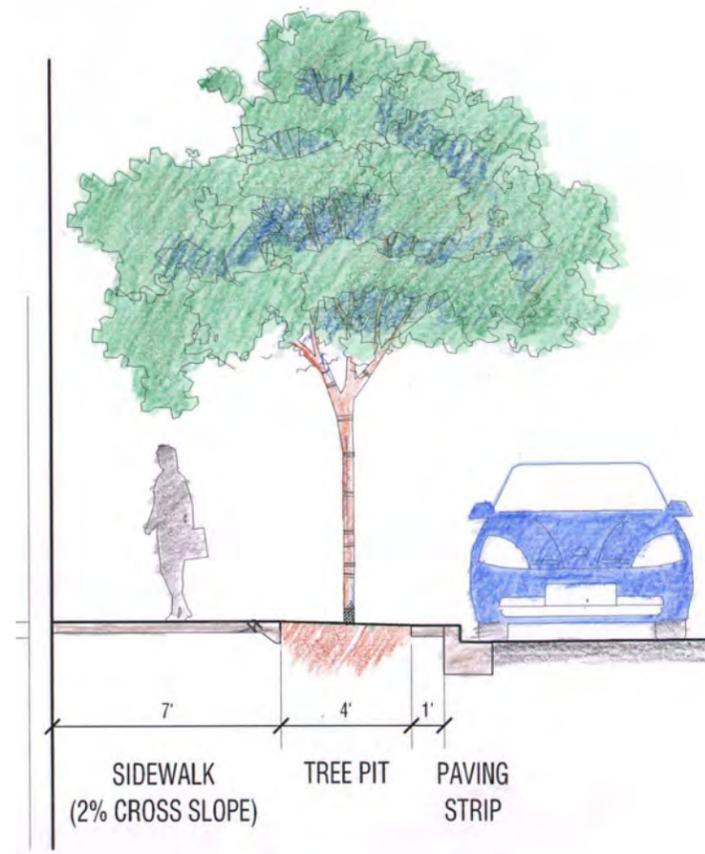
STREETSCAPE DEVELOPMENT

MARION/MADISON



STREETScape DEVELOPMENT

WESTERN AVENUE





HALA DESIGN OPTION

DEPARTURES

CODE SECTION 23.49.058

Subject

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