

## RECOMMENDATION MEETING

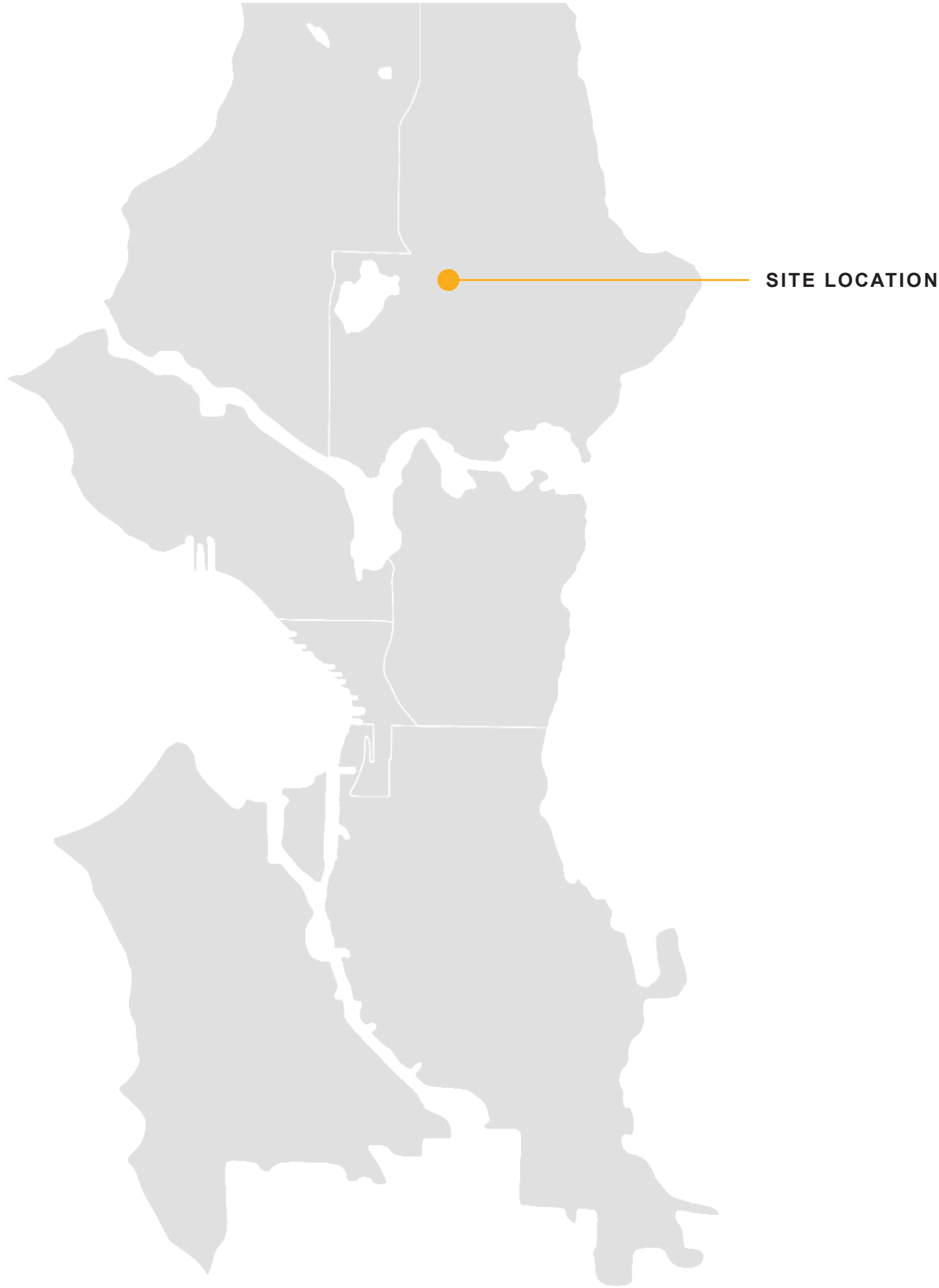
February 8th, 2016, 6:30 PM

DPD # 3020416  
6921 Roosevelt Way NE  
Seattle, WA 98115

Applicant:  
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9515 15th Ave NE  
Seattle, WA 98115  
Contact: Tim Carter

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VICINITY MAP

EXISTING SITE

The project site consists of three (3) parcels (APN's: 9528102175, 9528102180, and 952810286) located on the southwest corner of NE 70th Street and Roosevelt Way NE. South of the subject parcels on the same block of Roosevelt Way is an existing 3-story apartment building. The parcel west of the proposed project site contains two (2) 3-story structures listed as a business and condominiums by the King County Department of Assessments. Directly across Roosevelt Way NE is a 3-story self-storage building. The combined area of the subject parcels is approximately 10,226 SF and measures roughly 132' wide by 80' deep. The site slopes from the northeast corner to the southwest corner, with an overall grade change in this direction of approximately 12 feet. Three (3) existing 1-story buildings currently occupy the site.

ZONING AND OVERLAY DESIGNATION

The project parcel is zoned NC2-40 and is located within the Roosevelt Residential Urban Village. This zoning designation continues to the north for several blocks and continues south for two blocks where it transitions up to NC3P-65. The zoning steps down to SF-5000 two parcels to the west. Similarly, 2 parcels east of Roosevelt Way NE the zoning steps down to SF-5000. To be certain, no SF-5000 parcels are located directly adjacent to any of the subject parcels. Additionally, it is important to note that the subject parcels do not have a pedestrian zoning overlay and therefore a commercial use is not required. Finally, the subject parcels are located within the Roosevelt Light Rail Station Overlay and as a result parking is not required.

DEVELOPMENT OBJECTIVES

The owner proposes the construction of a new residential apartment building with approximately 72 small efficiency dwelling units. The existing buildings on the project parcels will be demolished. The site is within the Roosevelt Light Rail Station Overlay, and as a result is not required to provide parking. The objective for these apartments is to provide upscale, yet affordable, housing for the Roosevelt neighborhood. The demographic that will benefit most from this housing will be students and wage earners in the neighborhood that can't afford the more expensive rents of nearby properties; city-dwellers seeking a pedestrian-oriented lifestyle; and people that commute to the University of Washington and downtown businesses. These small efficiency apartments will add to the variety of multifamily housing types in the neighborhood and complement the diverse residential community that defines Roosevelt.

NEIGHBORHOOD DEVELOPMENT

The immediate blocks in the zone are a mix of multi-family apartment buildings, small businesses, and single-family homes. A vibrant commercial area is located four blocks south at NE 65th Street and Roosevelt Way NE where there is a grocery store, several restaurants and shops, as well frequent buses connecting to the University District, Downtown Seattle, and beyond. A new Link Light Rail Station located three blocks south of the proposed site is opening in 2021. In general, the area is very pedestrian friendly and there are numerous restaurants, stores, and parks within walking distance of the project site.



SITE LOCATION

6921 Roosevelt Way NE  
Seattle, WA 98115

PROJECT PROGRAM

Site Area: 10,226 SF  
Number of Residential Units: Approx. 72  
Number of Parking Stalls: None  
Proposed Bike Parking: Approx. 55 Stalls  
Total Area: 31,266 SF  
Total Area Above Grade: 27,526 SF  
Allowable FAR = 4.0  
Anticipated FAR = 2.97



**EXISTING SITE CONDITIONS:**

Proposed Project Site:

- Three (3) parcels located on the southwest corner of NE 70th Street and Roosevelt Way NE
- Combined area = 10,226 SF and measures roughly 132' wide by 80' deep

Topography:

- 12'-9" slope across site from northeast corner to southwest corner
- 6'-10" slope along Roosevelt Way frontage from north to south
- 2'-9" slope along NE 70th frontage from east to west

Adjacent Buildings and Uses:

- Existing 3-story multi-family structures immediately south & west of site (zoned NC2-40)

Solar Access & Views:

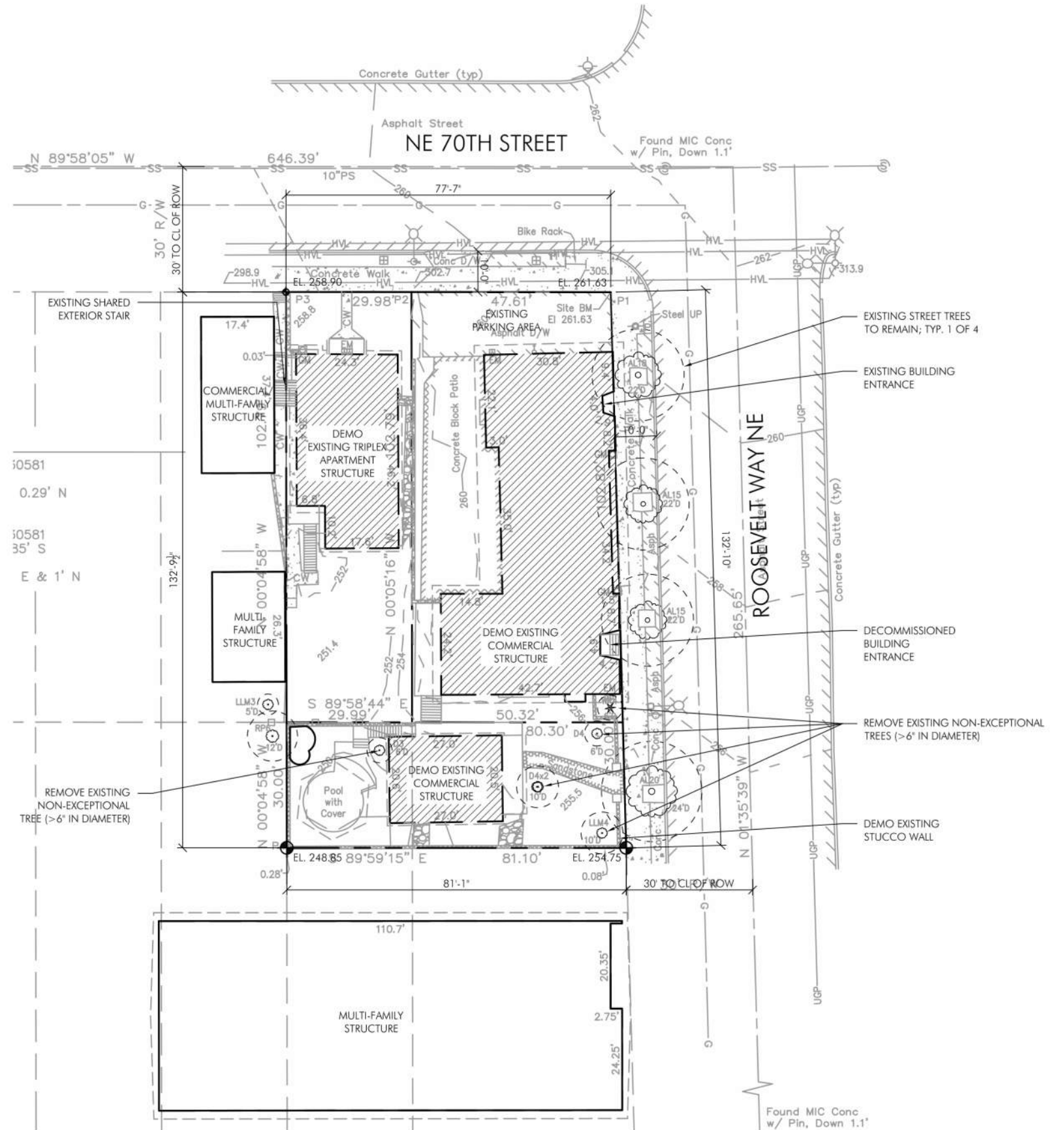
- The site has good solar access due to existing topography and the generous setback and existing landscape buffer at the south property line.
- Exceptional territorial views of Green Lake, downtown Seattle, and Mt. Rainer from the upper reaches of the site.
- Neighboring multi-family structures directly west of the site have completely blank facades.

Allowable Structure Height:

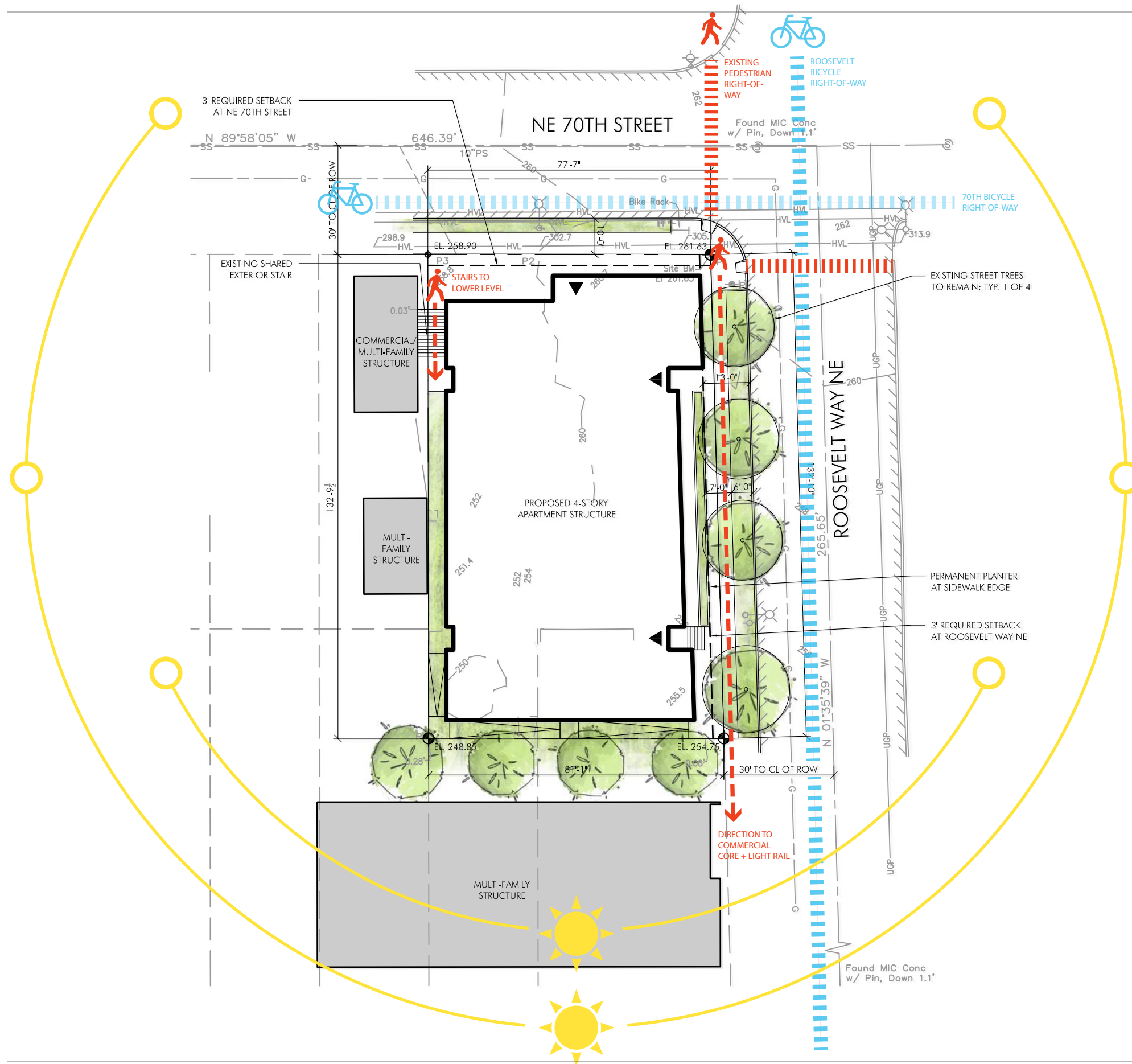
- NC2-40 zoning allows for a 40'-0" structure height
- 4' bonus for rooftop features
- 16' bonus for stair/elevator penthouses

Allowable Building Area:

- 2.0 Base FAR
- 4.0 Max FAR with Roosevelt Station Overlay = 10,226 sf x 4.0 = 40,904 sf







## PROPOSED SITE CONDITIONS:

### Setback Requirements:

- No setbacks are required at the south and west property lines (5'-0" minimum proposed for 25% openings in the adjacent exterior walls)
- 3'-0" required ROW setback at NE 70th Street
- 3'-0" required ROW setback at Roosevelt Way NE
- 10'-0" required setback from existing High Voltage Lines at NE 70th Street (13'-6" proposed for working clearances)

### Traffic and Circulation:

- NE 70th Street is a minor arterial with a connection over I-5 to Green Lake to the west
- Roosevelt Way NE is a principal arterial with time-limited parking on the east side of the street opposite the project site and un-timed parking on the west side of the street immediately adjacent to the site.
- There is a traffic light at the intersection of NE 70th Street and Roosevelt Way NE
- Roosevelt Way NE is one-way south from NE 75th street
- Bus lines serve Roosevelt Way NE. The nearest stop is less than 200' south of the site.
- The future Roosevelt Light Rail Station will open in 2021 and is located less than one quarter mile south of the site

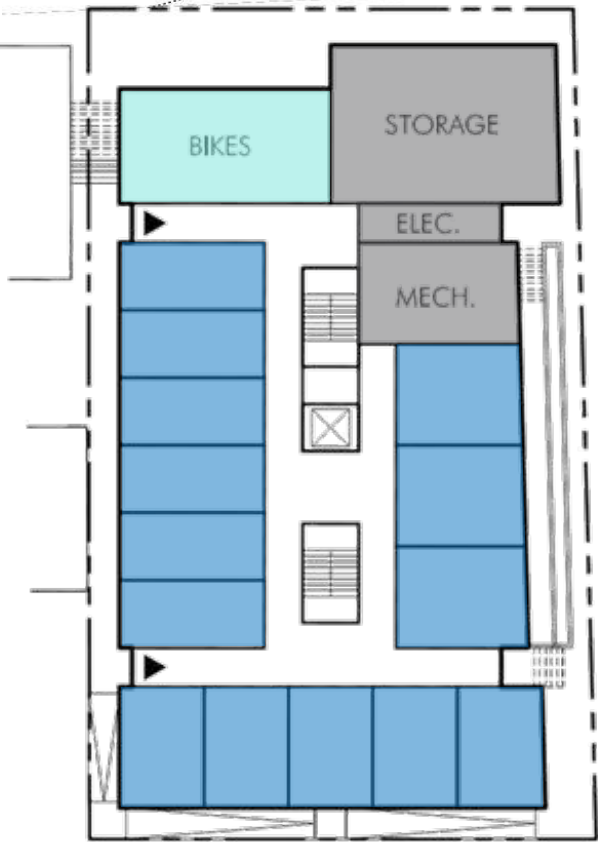
### Streetscape:

- NE 70th Street has a 6'-0" sidewalk with an additional 3'-6" planting strip that is filled in with asphalt adjacent to the site. A 3'-0" required ROW setback may allow for wider sidewalks along this edge in the future.
- Roosevelt Way NE has a 6'-0" sidewalk with an additional 3'-6" planting strip that is filled in with asphalt adjacent to the site. A 3'-0" required ROW setback will allow for wider sidewalks along this edge.
- There are (4) existing street trees along Roosevelt Way NE adjacent to the site

### Neighborhood Patterns and Potential:

- Roosevelt Way NE is zoned to promote commercial development and is a principal pedestrian street, but the project site is outside of the pedestrian area (no commercial use is required).
- The intersection of NE 70th Street and Roosevelt Way NE is not a recognized neighborhood "gateway." The nearest gateway is 5 blocks south of the site at NE 65th Street and Roosevelt Way NE. The proposed project will anchor and activate the corner with pedestrian oriented uses at the street level and poised building massing above.
- NE 70th Street transitions to a quiet residential street east of the 3-story mini-storage business across Roosevelt Way NE from the site.
- From two parcels west of the site along NE 70th Street to the bridge over I-5 are single family homes
- NE 70th Street experiences substantial bike and pedestrian traffic to and from Green Lake. The proposed project will provide entrances along this edge for bicyclists and pedestrians.

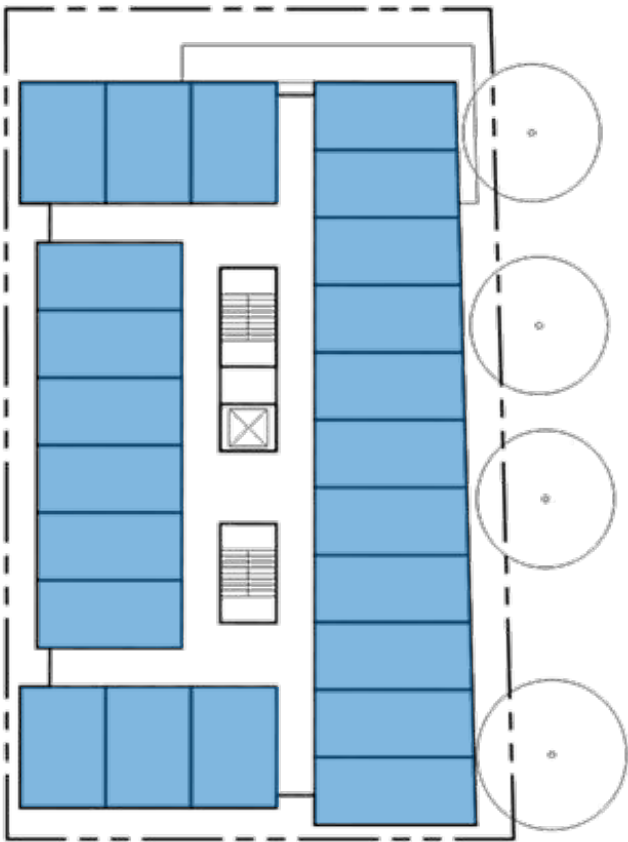




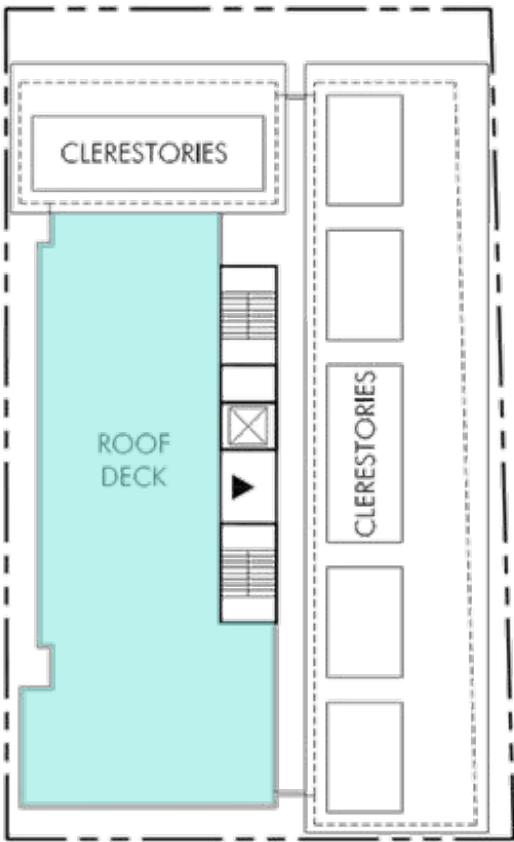
BASEMENT LEVEL 7771 GSF (14 UNITS)



FIRST LEVEL 7802 GSF (15 UNITS)

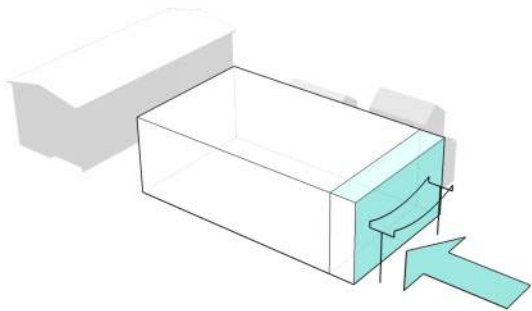


SECOND AND THIRD LEVELS 16,142 GSF (46 UNITS)

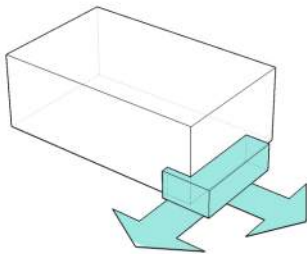


ROOF LEVEL 541 GSF

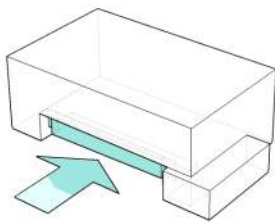




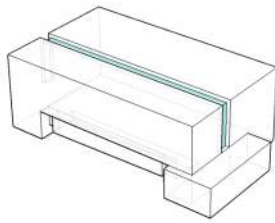
STEP BACK



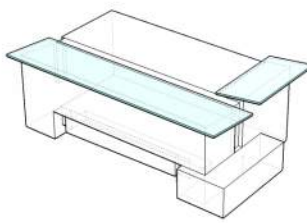
HOLD THE CORNER



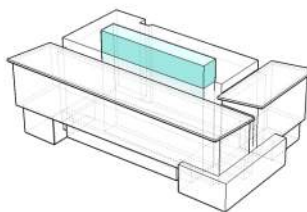
RECESS GROUND UNITS



EXPRESS CIRCULATION



EMBRACE PUBLIC



CENTRALIZE CIRCULATION



VIEW TOWARDS SOUTHEAST CORNER



VIEW FROM INTERSECTION OF ROOSEVELT AND NE 70TH



VIEW FROM NE 70TH

AMENITY AREA SERVICE AREA DWELLING UNIT



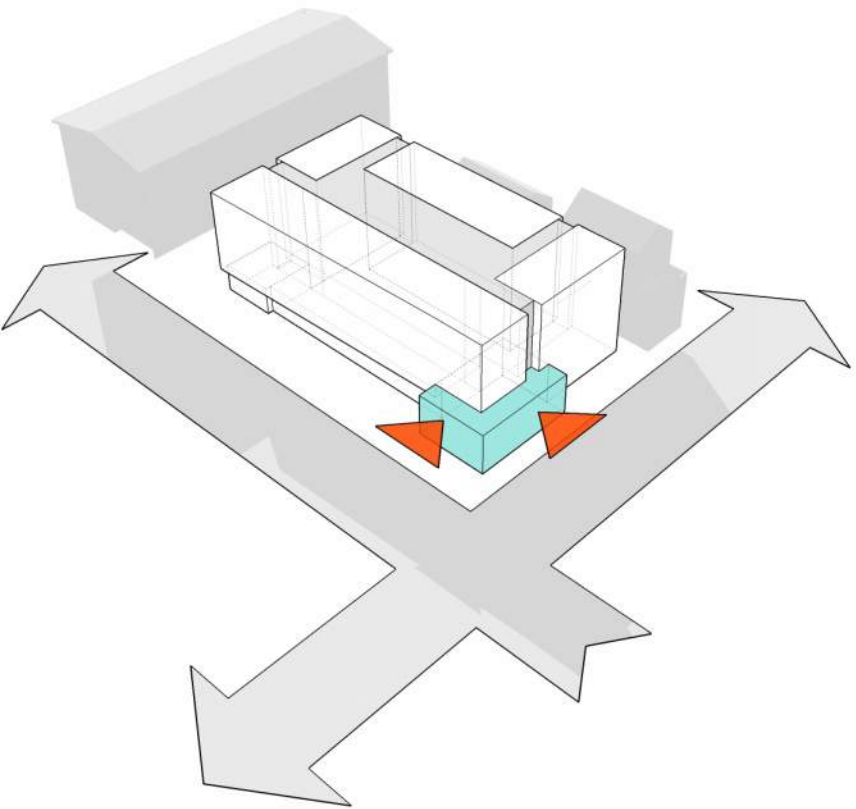
EARLY DESIGN GUIDANCE & BOARD RECOMMENDATIONS

MASSING, DESIGN CONCEPT AND CONTEXT RESPONSE

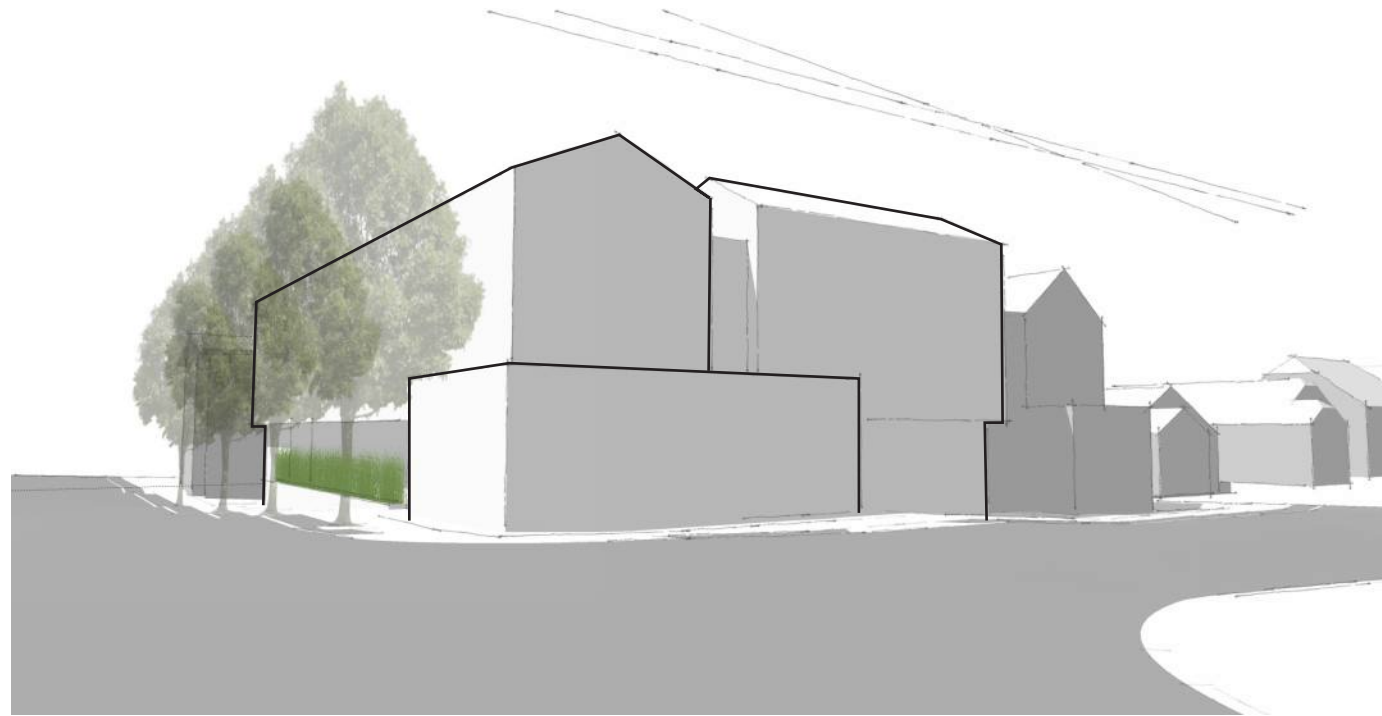
- PROJECT MASSING AND ROOF FORMS
- BIKE STORAGE LOCATION
- AMENITY SPACE

STREETScape AND STREET LEVEL USES

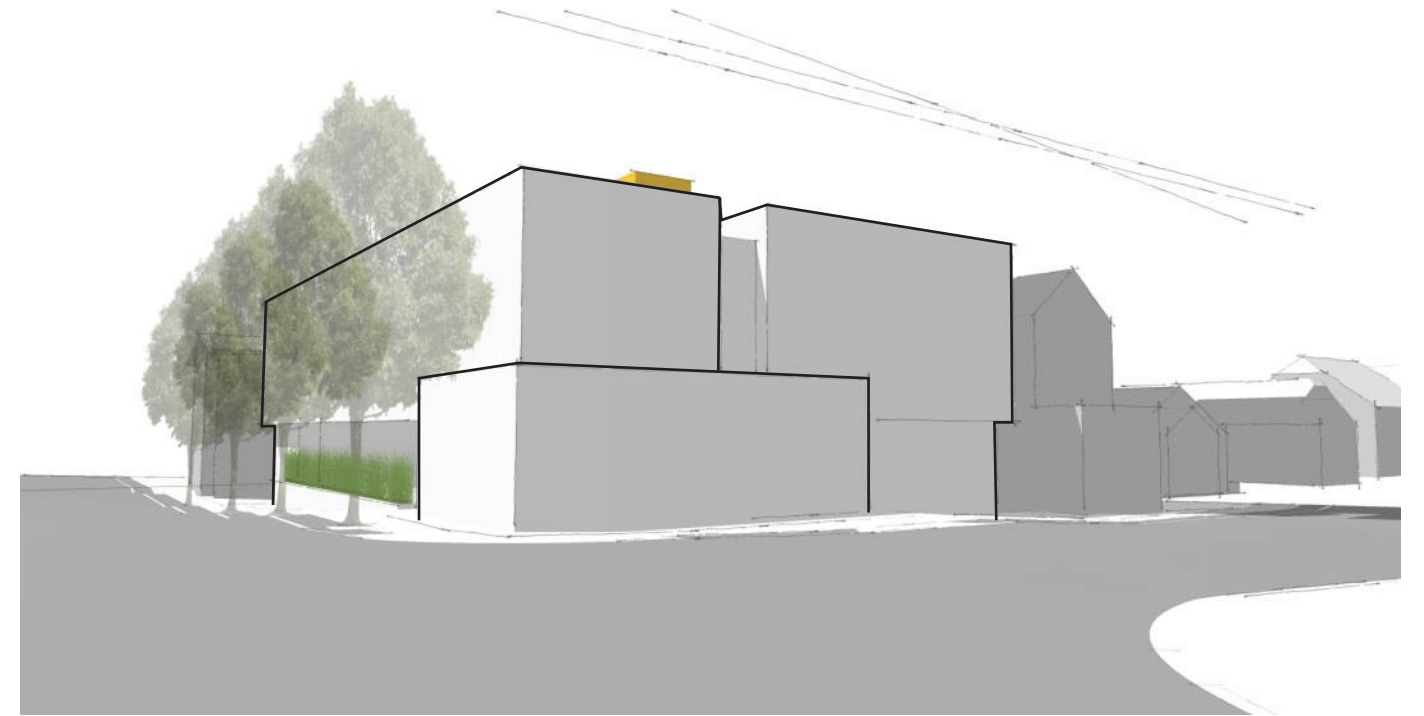
- STREETSCAPE
- ACTIVE USES AT CORNER
- ELIMINATION OF BASEMENT UNITS
- BUILDING EDGE ACTIVATION
- TRASH/RECYCLING



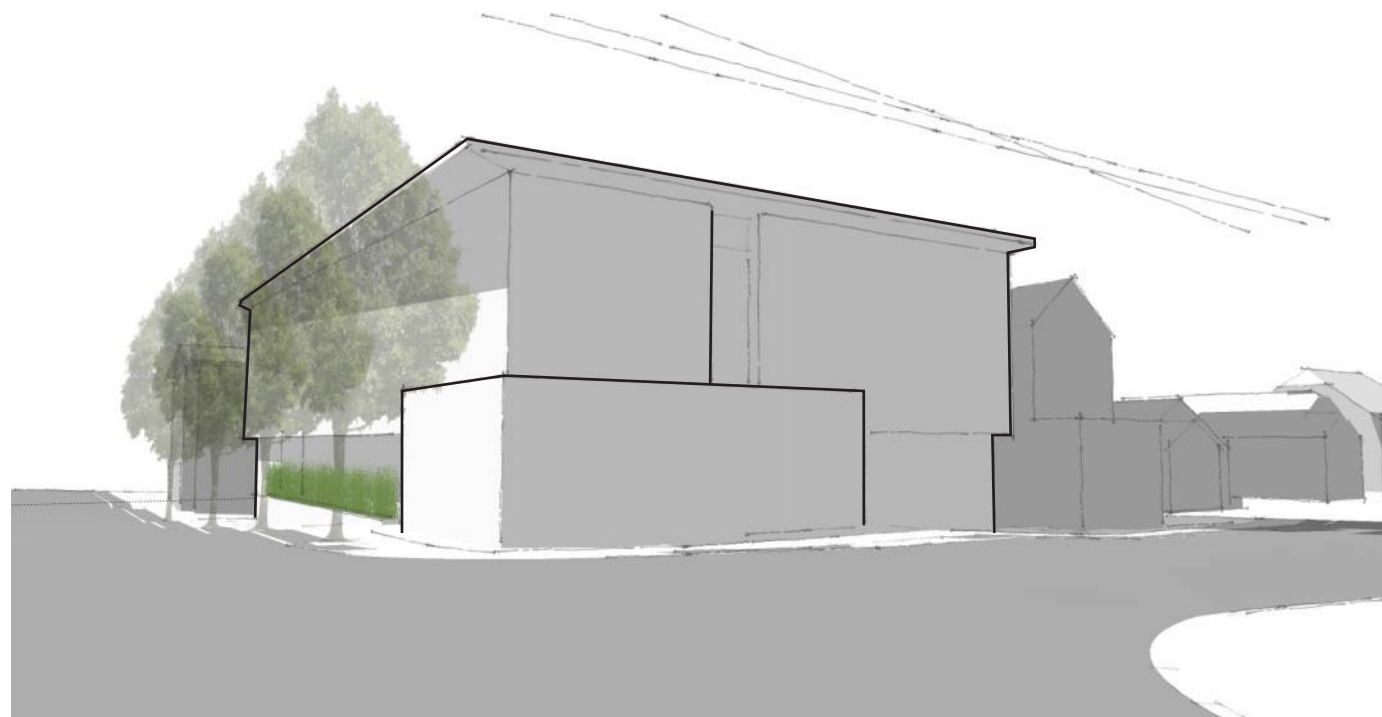




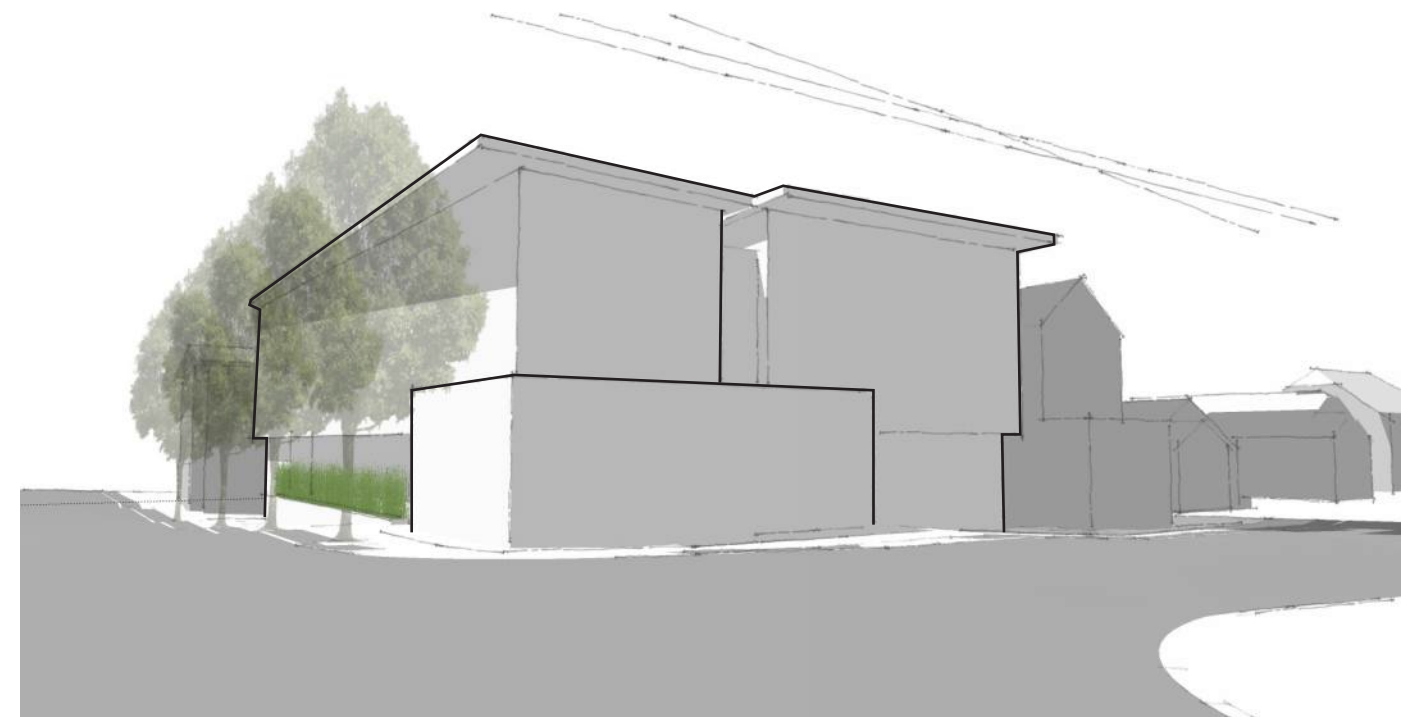
GABLED ROOF



FLAT ROOF



SHED ROOF - EXTENDED



SHED ROOF - LOWER PITCH (PREFERRED)





PREFERRED OPTION 1 - SINGLE STORY @ CORNER



OPTION 2 - DOUBLE STORY @ CORNER

### MASSING STUDIES

Recognizing the importance of the building's relationship to the block and in particular to the intersection of Roosevelt and NE 70th, the massing at the northeast corner was carefully considered. A two-story projecting massing option was explored that would give the building more weight at the northeast corner. While visually more prominent, this massing appears disproportionate with the overall building mass and clips the dominant upper level horizontal mass. Furthermore, the two-story corner mass is not reflective of the programming behind its walls. The second floor of the mass would have to remain apartment units to meet the client's pro forma. Therefore, what could appear from the exterior as a unified mass, would not programmatically be on the interior. Lastly, the two-story mass as explored and in relation to the existing 6' sidewalks could feel foreboding to pedestrians.

The preferred option retains a one-story projecting brick mass at the corner and is architecturally more cohesive for several reasons. First, it clearly anchors the northeast corner of the site, but at a more pedestrian scale. Second, as encouraged by the board at the edg meeting, a secondary rooftop amenity is proposed for the top of this mass which will put eyes on and activities closer to the intersection of Roosevelt and NE 70th. Third, the massing is a genuine reflection of the internal arrangement of the building here. Finally, building entrances are more clearly defined. This one-story mass wraps into the adjacent entrance on Roosevelt and now also includes a prominent entrance for the bicyclists at NE 70th.





PREFERRED OPTION 1 - SINGLE STORY @ CORNER



OPTION 2 - DOUBLE STORY @ CORNER



EAST ELEVATION



NORTH ELEVATION



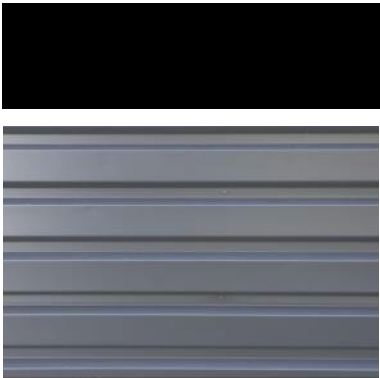
① CEDAR SIDING



② FIBER-CEMENT PANEL  
SW "IRON ORE" (#7069)



③ STACKED BOND  
GRAY BRICK



④ APE SPAN METAL SIDING  
COOL OLD TOWN GRAY W/  
COOL MATTE BLACK INFILL



⑤ FIBER-CEMENT PANEL  
SW "GUSTO GOLD" (#6904)



⑥ ARCHITECTURAL  
CONCRETE



⑦ VINYL WINDOWS



⑧ BLACK ALUMINUM  
STOREFRONT





WEST ELEVATION



SOUTH ELEVATION



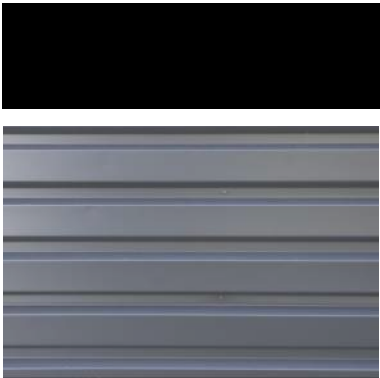
① CEDAR SIDING



② FIBER-CEMENT PANEL  
SW "IRON ORE" (#7069)



③ STACKED BOND  
GRAY BRICK



④ AEP SPAN METAL SIDING  
COOL OLD TOWN GRAY W/  
COOL MATTE BLACK INFILL



⑤ FIBER-CEMENT PANEL  
SW "GUSTO GOLD" (#6904)



⑥ ARCHITECTURAL  
CONCRETE



⑦ VINYL WINDOWS



⑧ BLACK ALUMINUM  
STOREFRONT

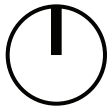


BASEMENT FLOOR PLAN



FIRST FLOOR PLAN

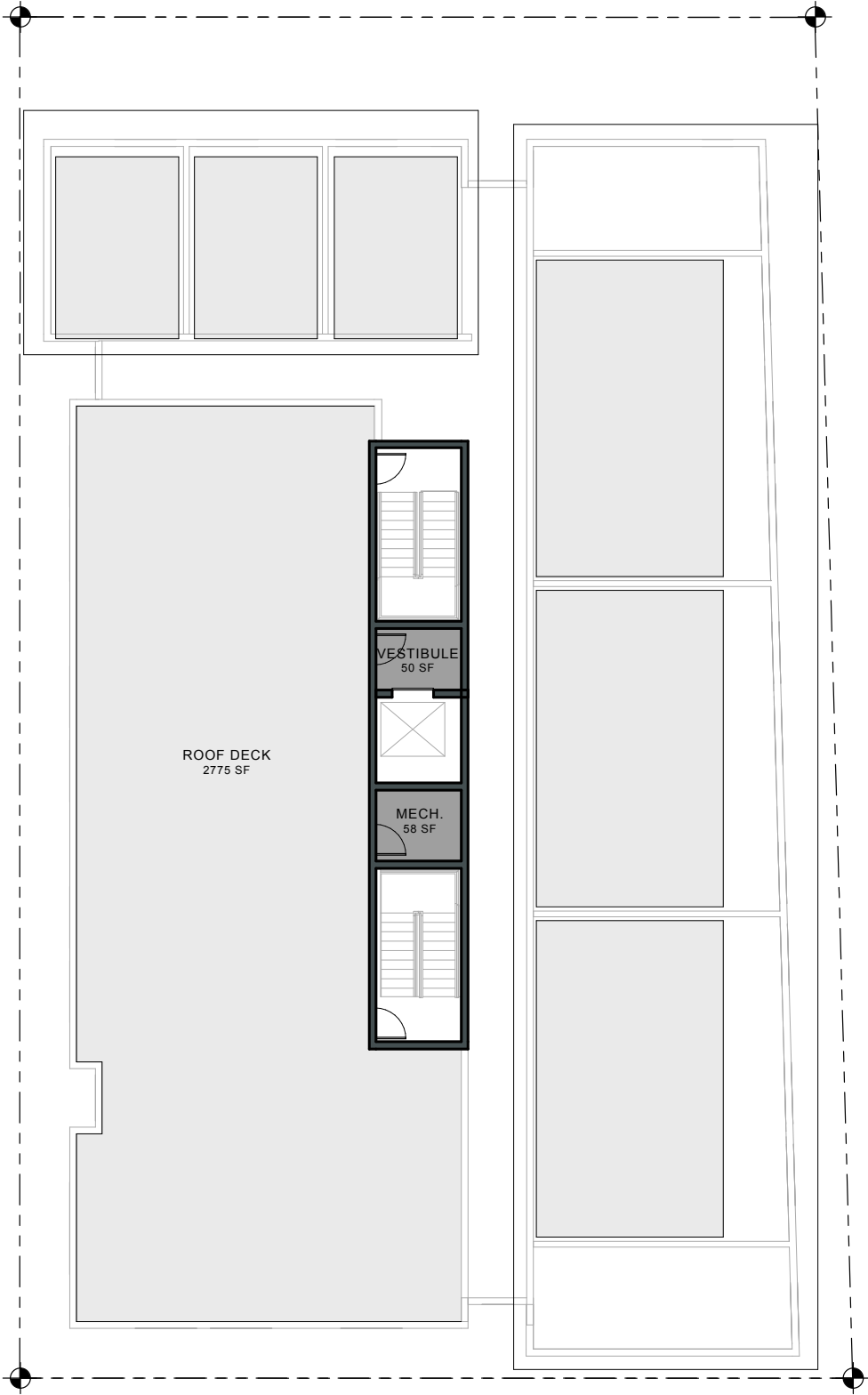
AMENITY AREA SERVICE AREA DWELLING UNIT





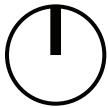


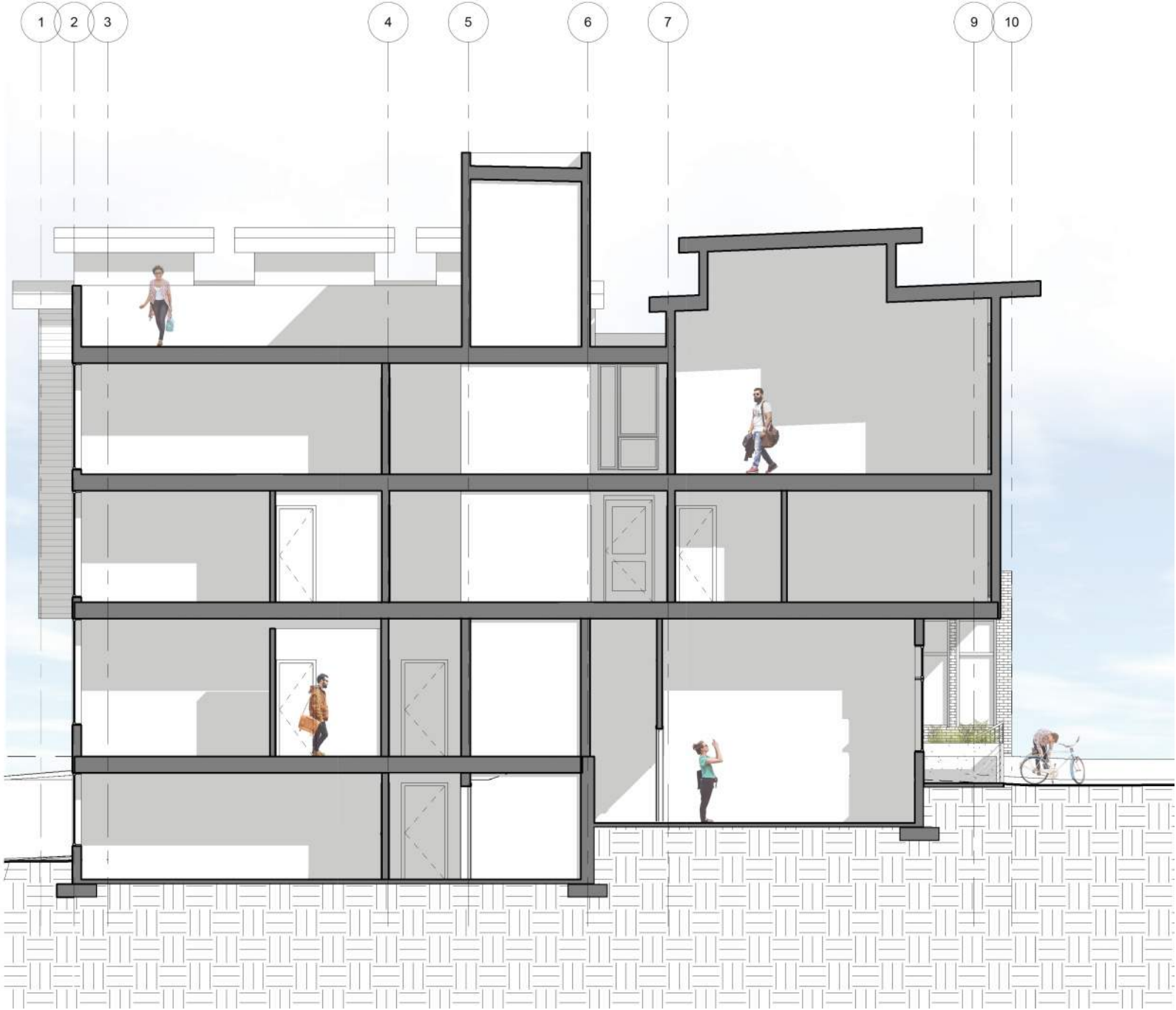
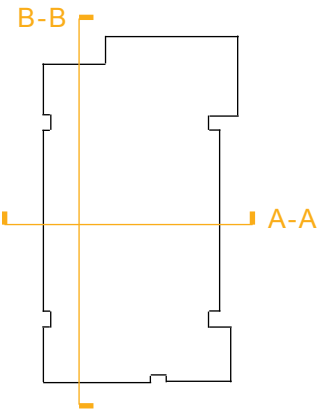
2ND & 3RD FLOOR PLAN



ROOF PLAN

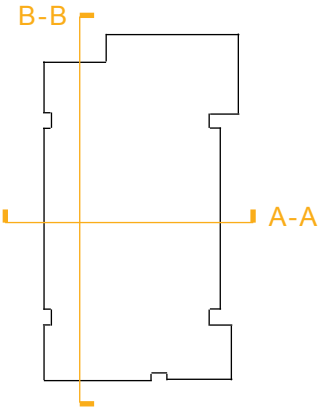
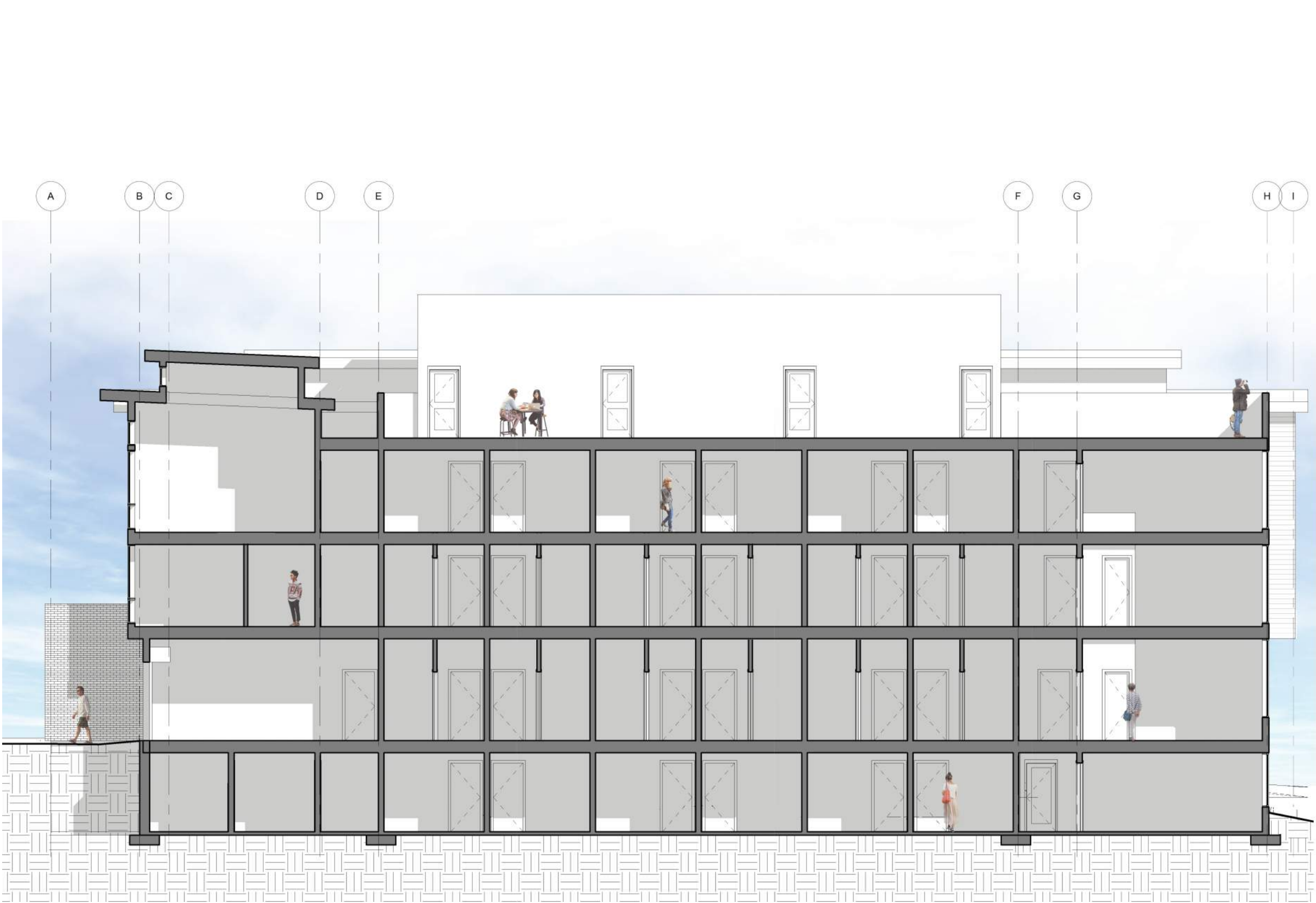
AMENITY AREA SERVICE AREA DWELLING UNIT



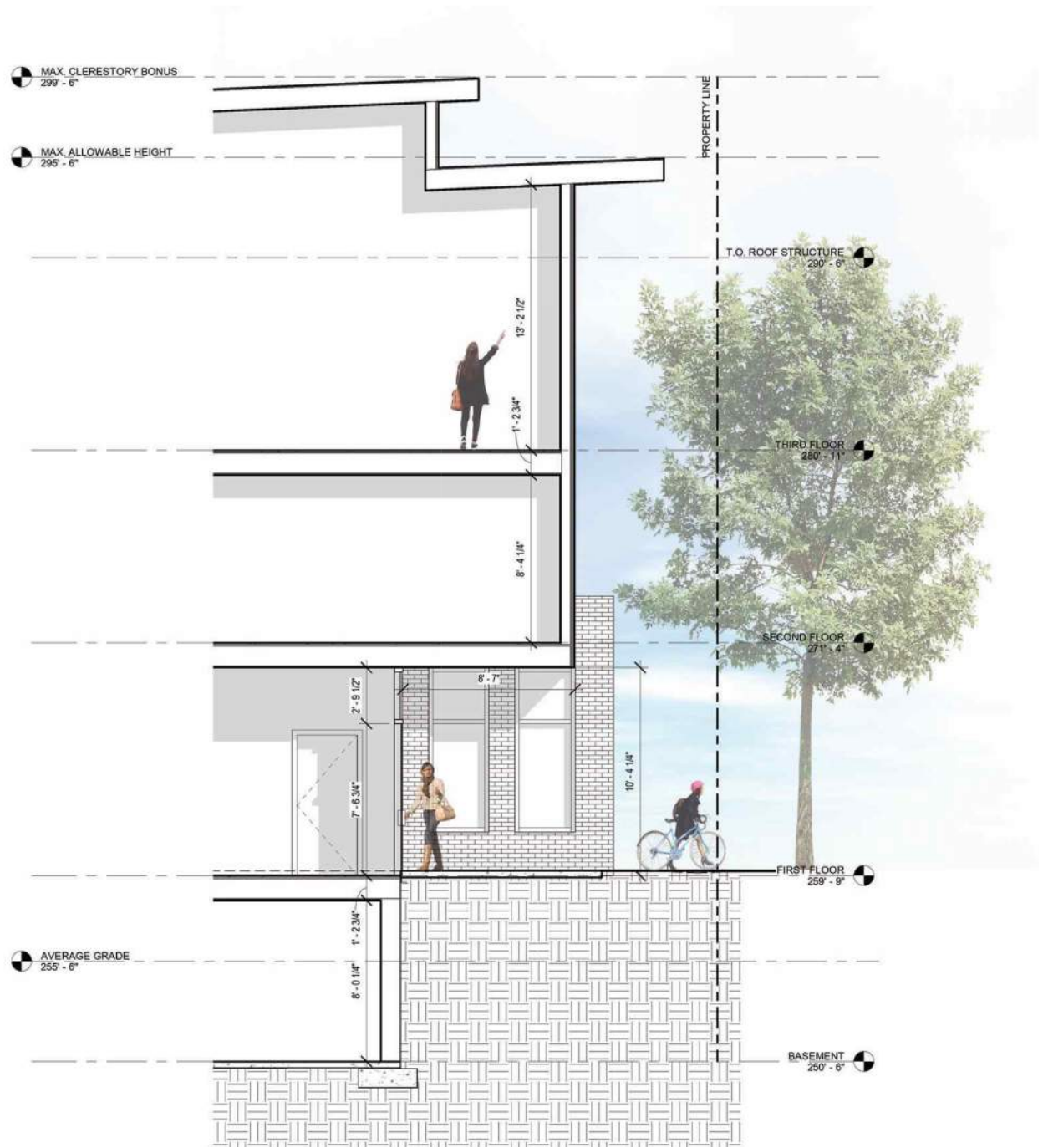
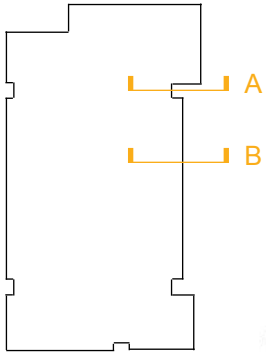


TRANSVERSE SECTION (A-A)

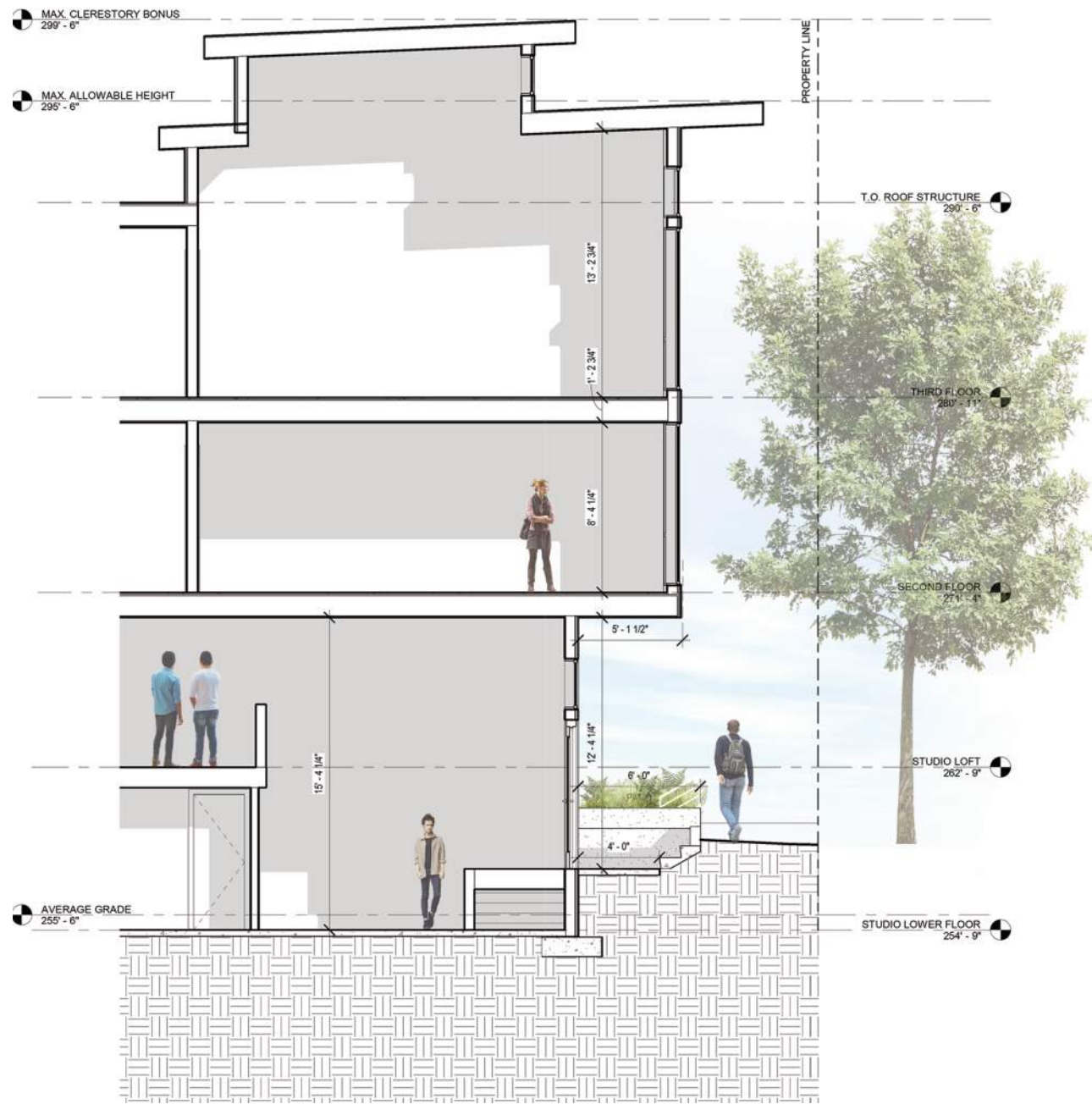




LONGITUDINAL SECTION (B-B)

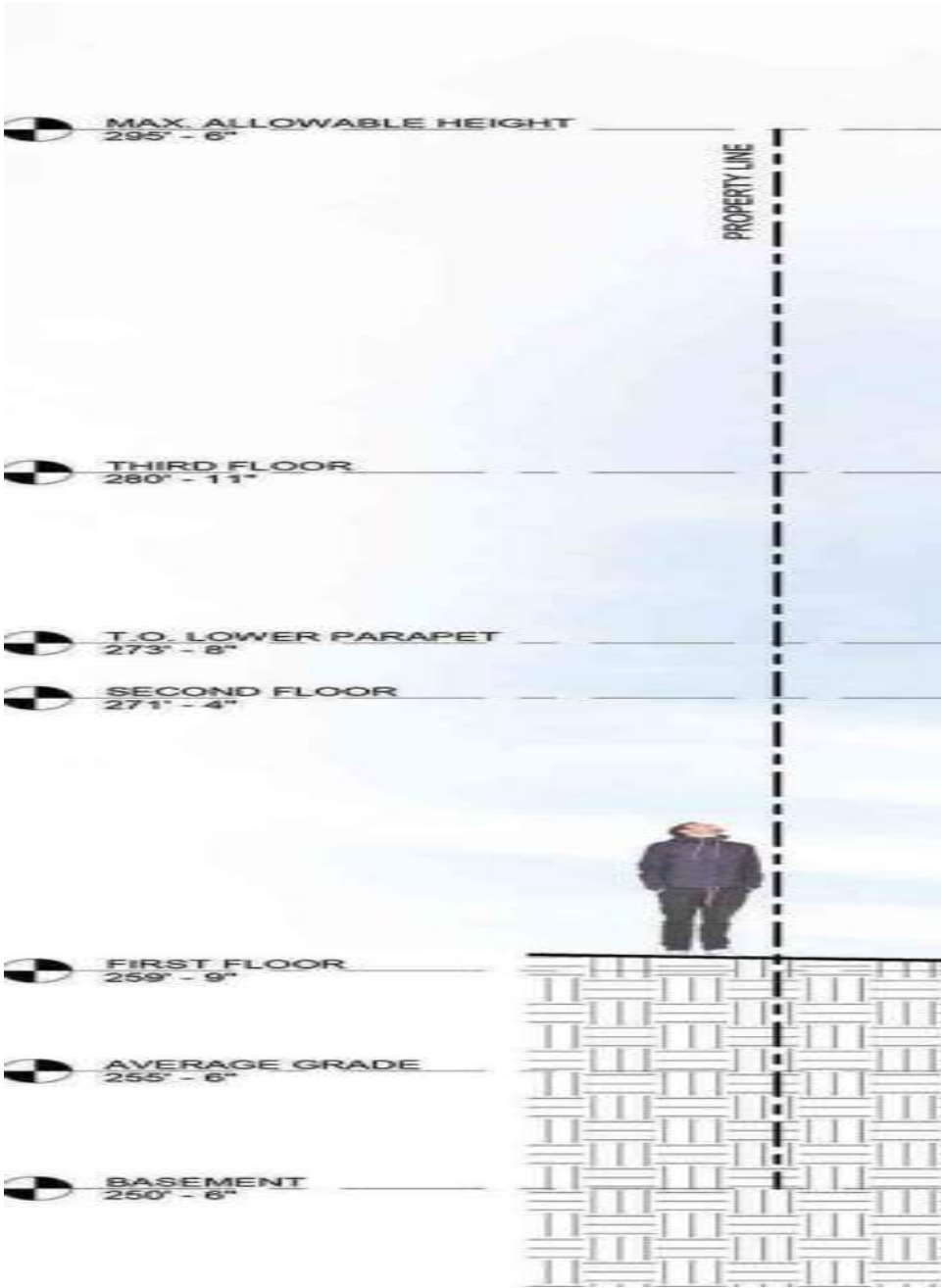


PRIMARY ENTRY SECTION (A)

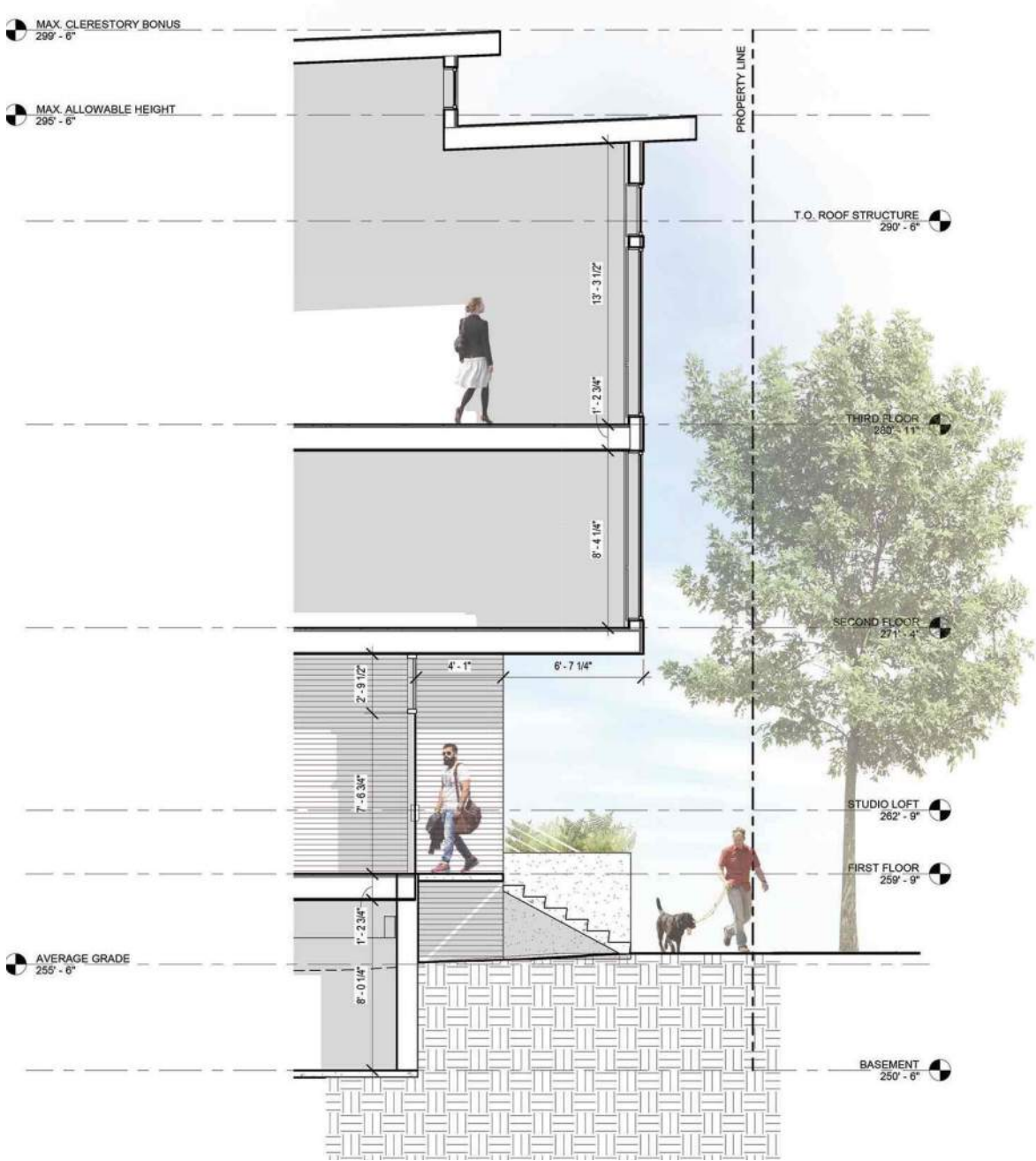


STOOP SECTION (B)

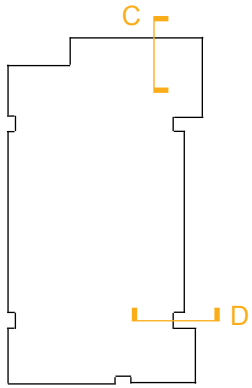




BIKE AMENITY SECTION (C)



SECONDARY ENTRY SECTION (D)





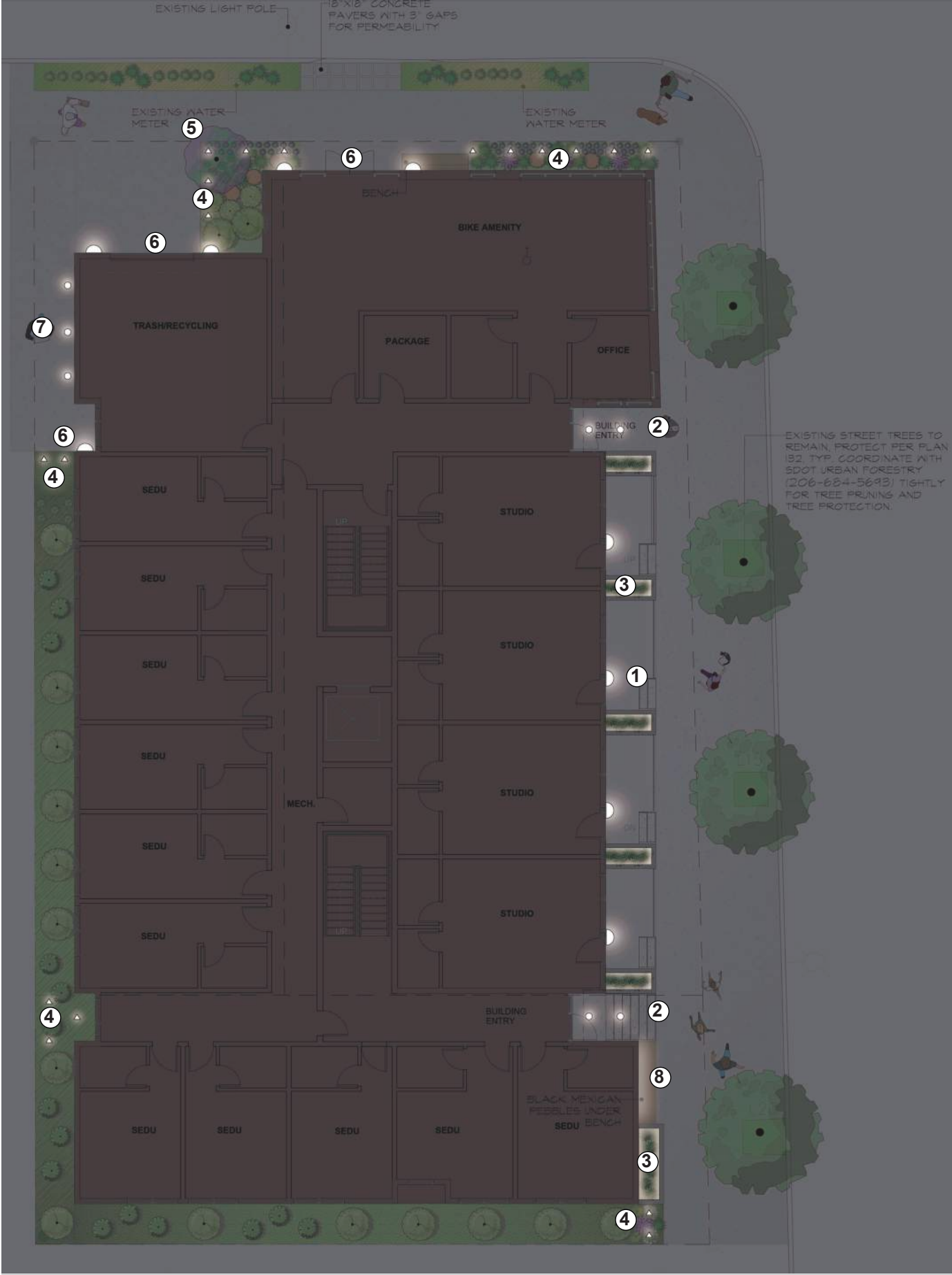
PROPOSED PLANTING

- ① *Cuperessus macrocarpa* - Wilma Goldcrest Cypress
- ② *Phormium 'Shiraz'* - Shiraz New Zealand Flax
- ③ *Phyllostachys nigra* - Black Bamboo
- ④ *Uncinia rubra 'Firedance'* - Firedance Sedge
- ⑤ *Nassella tenuissima* - Mexican Feather Grass
- ⑥ *Equisetum hyemale* - Horsetail
- ⑦ *Lysimachia nummularia 'Aurea'* - Yellow Creeping Jenny
- ⑧ *Cercidiphyllum japonicum 'Rotfuchs'* - Red Fox Katsura
- ⑨ *Ophiopogon planiscapus 'Nigrescens'* - Black Mondo Grass
- ⑩ *Imperata cylindrica 'Red Baron'* - Japanese Blood Grass
- ⑪ *Pachysandra terminalis 'Silver Edge'* - Japanese Spurge
- ⑫ *Pennisetum orientale* - Oriental Fountain Grass
- ⑬ *Leucothoe fontanesiana 'Rainbow'* - Rainbow Dropping Fetterbush
- ⑭ *Berberis thunbergii 'Orange Rocket'* - Orange Rocket Barberry
- ⑮ *Cornus alba 'Buds Yellow'* - Bud's Yellow Dogwood
- ⑯ *Acer japonicum 'Autumn Moon'* - Autumn Moon Maple









① ENTRY SCONCES @ STUDIO UNITS



② RECESSED CAN LIGHTS



③ PLANTER BOX ROPE LIGHTING



④ LANDSCAPE LIGHTING



⑤ TREE UP-LIGHTING



⑥ ENTRY SCONCES @ SECONDARY ENTRIES



⑦ WOOD "WALL-WASH" DOWN-LIGHTING



⑧ BACK-LIT STEEL SIGNAGE





EAST FACADE ALONG ROOSEVELT





BACK-LIT STEEL SIGN



BENCH INTEGRATED WITH LANDSCAPE AND ENTRY SEQUENCE



ROOSEVELT SIDEWALK AT SOUTH BUILDING ENTRANCE





ROOSEVELT AND NE 70TH CORNER VIEW





CEDAR WRAPPING AT ENTRIES



LANDSCAPE PLANTERS TO DEFINE SPACE



PRIMARY ENTRY AT NORTH





ROOSEVELT WALKING VIEW FROM SOUTHWEST





VIEW FROM PRIVATE STOOPS TOWARDS ROOSEVELT



URBAN SIDEWALK PATIOS



OPEN STEEL-RAILINGS



LIGHTING INTEGRATED WITH PLANTERS





VIEW FROM 70TH TOWARDS BIKE ENTRY





REAR VIEW FROM ADJACENT PARKING LOT



MUTED PALETTE ALONG NEIGHBORING PROPERTIES WITH YELLOW CIRCULATION COLOR AT RECESSES







NORTH FACADE AS SEEN DRIVING SOUTH ON ROOSEVELT



TALL PROJECTION SIGNAGE ALONG ROOSEVELT



LIGHTING INCORPORATED WITH PROJECTION SIGN



GUIDELINE	DESCRIPTION	RELEVANT SUBCATEGORIES	BOARD RECOMMENDATION CATEGORY
CS2 - Urban Pattern and Form	Strengthen the most desirable forms, characteristics, and patterns of the streets, block faces, and open spaces in the surrounding area.	B. Adjacent Sites, Streets, and Open Spaces 1. Site Characteristics 2. Connection to the Street C. Relationship to the Block 1. Corner Sites	Massing, Design Concept, & Context Response Streetscape & Street Level Uses
CS3 - Architectural Context and Character	Contribute to the architectural character of the neighborhood.	A. Emphasizing Positive Neighborhood Attributes 1. Fitting Old and New Together 2. Contemporary Design 3. Established Neighborhoods 4. Evolving Neighborhoods I. Emphasizing Positive Neighborhood Attributes i. Roosevelt High School Architectural Heritage ii. Reinforce Vibrant Streetscape iii. Street Walls Facing Arterial Streets	Massing, Design Concept, & Context Response
PL2 - Walkability	Create a safe and comfortable walking environment that is easy to navigate and well-connected to existing pedestrian walkways and features.	B. Safety and Security 1. Eyes on the Street 2. Lighting for Safety 3. Street-Level Transparency	Streetscape & Street Level Uses
PL3 - Street-Level Interaction	Encourage human interaction and activity at the street-level with clear connections to building entries and edges.	A. Entries 1. Design Objectives 2. Ensemble of Elements B. Residential Edges 1. Security and Privacy 2. Ground-level Residential II. Human and Commercial Activity	Streetscape & Street Level Uses
PL4 - Active Transportation	Incorporate design features that facilitate active forms of transportation such as walking, bicycling, and use of transit.	B. Planning Ahead for Bicyclists 1. Early Planning 2. Bike Facilities 3. Bike Connection	Streetscape & Street Level Uses
DC1 - Project Uses and Activity	Optimize the arrangement of uses and activities on site.	A. Arrangement of Interior Uses 1. Visibility 2. Gathering Places 3. Flexibility 4. Views and Connections C. Parking and Service Uses 2. Visual Impacts 4. Service Uses	Massing, Design Concept, & Context Response Streetscape & Street Level Uses
DC2 - Architectural Concept	Develop an architectural concept that will result in a unified and functional design that fits well on the site and within its surroundings.	A. Massing 1. Site Characteristics and Uses 2. Reducing Perceived Massing B. Architectural and Facade Composition 1. Facade Composition 2. Blank Walls D. Scale and Texture 1. Human Scale 2. Texture	Massing, Design Concept, & Context Response
DC3 - Open Space Concept	Integrate open space design with the design of the building so that each complements the other.	I, Open Space Character i. Views and Solar Access	Massing, Design Concept, & Context Response
DC4 - Exterior Elements and Finishes	Use appropriate and high quality elements and finishes for the building and its open spaces.	C. Lighting 1. Functions 2. Avoiding Glare	Streetscape & Street Level Uses



MASSING, DESIGN CONCEPT, AND CONTEXT RESPONSE:

BOARD RECOMMENDATION

PROJECT MASSING

The Board noted that overall the massing concepts and siting of the building respected the adjacencies and responded appropriately to the neighborhood context. The Board supported the preferred option (Option 3) with a broken mass along NE 70th and a projecting corner mass. The intersection of the two masses should be resolved, especially where the roof lines interact. The Board recommended clarifying the overall massing, noting that cues could be taken from the simplified massing presented in Option 1.

BIKE STORAGE

The bike storage should be located at grade for convenience, and could be located along the streetscape with a high level of transparency to provide an active, interesting use.

AMENITY SPACE

The Board supported the amenity space located over the lobby at the corner, noting that it could strengthen the overall massing concept, enhance the relationship of the building to the street, and provide eyes on the street.

SUMMARY RESPONSE

Several roof studies were preformed to analyze the ideal roof form in terms of scale, contextual relationship, and overall aesthetics. Of these options, the double shed uses modulation to decrease the appearance of the overall massing. The shed also responds more appropriately to the roof profiles of the neighborhood while maintaining a proportion that is appropriate for the size of the building. The pitch of these shed roofs was lowered so that when viewed from Roosevelt, their datum line would be more aligned than they were at the EDG presentation.

The bike storage has been moved from the basement level and has relocated at grade at the focal corner of the project. It has also been integrated with a lobby/ lounge space to create an overall, open “bike amenity” room that has large corner glazing to create a visual connection to NE 70th and Roosevelt.

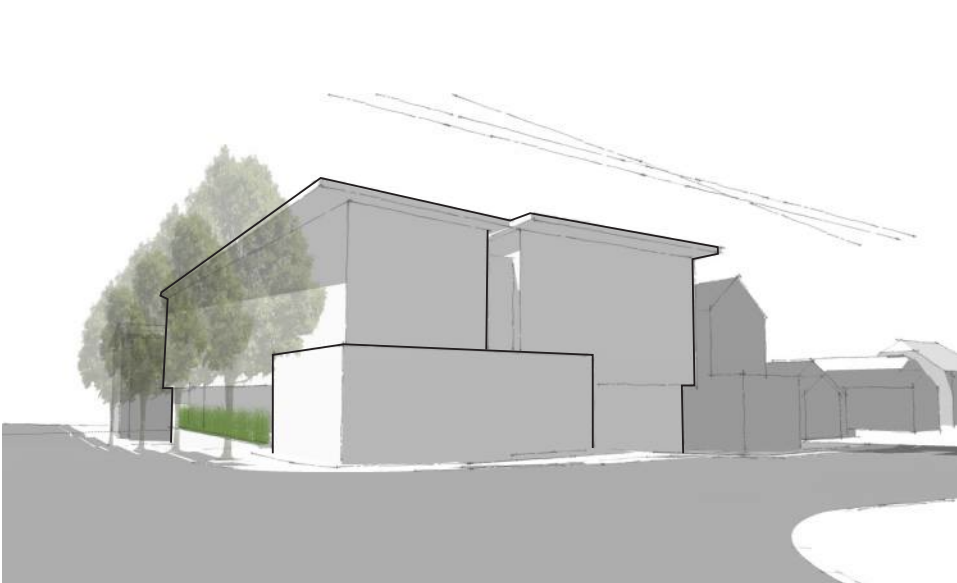
The amenity deck has been refined in the proposed design. The windows and railings have been further developed to provide a gracious, usable deck space while retaining privacy for the adjacent units. Green roof is proposed at the eastern and western edges of this deck to uphold this separation from adjacent units while softening the edges of this amenity space.

DESIGN GUIDELINES

CS2-B, CS3-A  
DC2-A, DC2-B

PL4-B  
DC1-A

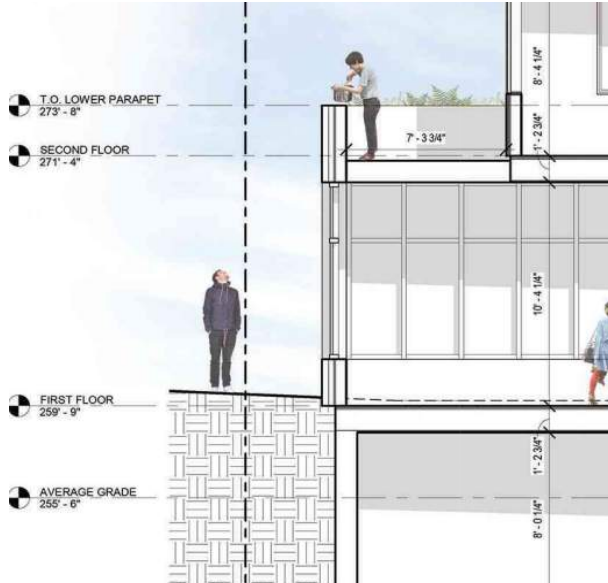
CS2-C, CS3-A  
PL2-B  
DC2-A, DC3-I



CS2-B, CS3-A, DC2-A, DC2-B



PL4-B, DC1-A



CS2-C, CS3-A, PL2-B, DC2-A, DC3-I



STREETSCAPE AND STREET LEVEL USES:

BOARD RECOMMENDATION	SUMMARY RESPONSE	DESIGN GUIDELINES
<p><b>STREETSCAPE</b></p> <p><i>The Board agreed that the spaces at street level should be designed to engage and activate the streetscape. The design and programming of spaces along Roosevelt Way NE should support active uses that establish a relationship with the pedestrian realm. (CS2-B, DC1-A, PL2-B, P PL3-II)</i></p>	<p>Three primary materials are proposed for the building – metal panels at the street-facing facades, wood at the recessed and protected street-level volumes, and fiber cement panels at the rear facades. The metal panels are proposed as a gray, horizontal ribbed panel to express the horizontal movement of the design. Vinyl windows are generally proposed, with storefront glazing occurring at the protruding amenity volume to increase visual connectivity. This volume, as it is “anchoring” the corner, is proposed to be a “heavy” gray, brick constructed in a stacked-bond assembly. Stained cedar will be used along the street-level studios to add visual warmth and additional texture at the pedestrian level. This cedar also extends to the underside of the soffit, as well as continues into the building to strengthen the connectivity between interior and exterior spaces. The cementitious panels proposed are two distinct colors; a dark, iron color for the general building mass, and a bright accent color at circulation recesses. This adds moments of excitement to the facade, and also helps the building functions read diagrammatically from the exterior.</p>	<p>CS2-B DC1-A PL2-B, PL3-II</p>
<p><b>ACTIVE USES AT CORNER</b></p> <p><i>Locating active uses at the corner is appropriate, as it works to engage the corner and is responsive to the massing. The Board noted that the bike lounge is the most active use in the proposed program, and supported locating the bike lounge at the corner. The use and programming of the lobby/bike lounge should be further developed to provide active uses that relate to the streetscape. The Board requested more information on the potential programming and design of the space.</i></p>	<p>The bike storage has been relocated from the basement level to a “bike amenity space” on the main level at the northeast corner of the building. This open room serves as both a lounge for residents, and as a general bike storage and maintenance space. Large areas of glazing wrap the corner, and the interior layout of auxiliary spaces has been revised to make this active amenity space visible from the exterior. Interior and exterior lighting of this space will provide visual interest to passersby.</p>	<p>PL2-B, PL3-A DC1-A, DC3-I</p>



CS2-B, DC1-A, PL2-B, PL3-II



CS2-B, DC1-A, PL2-B, PL3-II



STREETSCAPE AND STREET LEVEL USES:

BOARD RECOMMENDATION	SUMMARY RESPONSE	DESIGN GUIDELINES
<p><b>BUILDING EDGE ACTIVATION</b> <i>The Board did not support the basement units along Roosevelt, as they felt it isolated the street-facing facade and was a detriment to the pedestrian experience. They suggested stoops, split stoops, or to consider live-work spaces that would offer a more commercial character. Any live-work should be designed to activate the street.</i></p> <p><b>TRASH / RECYCLING</b> <i>The Board supported the location of the waste storage on NE 70th, and encouraged the applicant to explore a split-level trash room to minimize the impacts to the pedestrian realm and valuable street frontage. (DC1-C)</i></p>	<p>The basement units have been eliminated from the plan, instead proposing four street-level studio apartments along Roosevelt. These apartments would each have a small concrete porch and entrance that faces the street. Rather than having a large bioretention planter running the length of these units, there are now five smaller planters that separate and privatize the proposed stoops. These planters also soften the pedestrian edge of the Roosevelt sidewalk. The stoops are elevated or sunken, depending on each unit's relationship to the sidewalk topography, and would be framed with metal open rails to add more detail at the pedestrian scale.</p> <p>The waste storage is still located along NE 70th, but has remained a single level room due to the impracticalities of a split-level trash storage room and the project's existing topography. The exterior mass of the room is proposed to be cedar-clad to increase the warmth of the corner. The current curb cut is also proposed to be eliminated and planted, therefore softening the edges with additional landscaping. A small tree is also proposed at this corner, further softening the trash/recycling exit.</p>	<p>PL2-B, PL3-B, PL3-II DC2-A</p> <p>DC1-C</p>



PL2-B, PL3-B, PL3-II, DC2-A



PL2-B, PL3-B, PL3-II, DC2-A



DC1-C







## APPENDIX





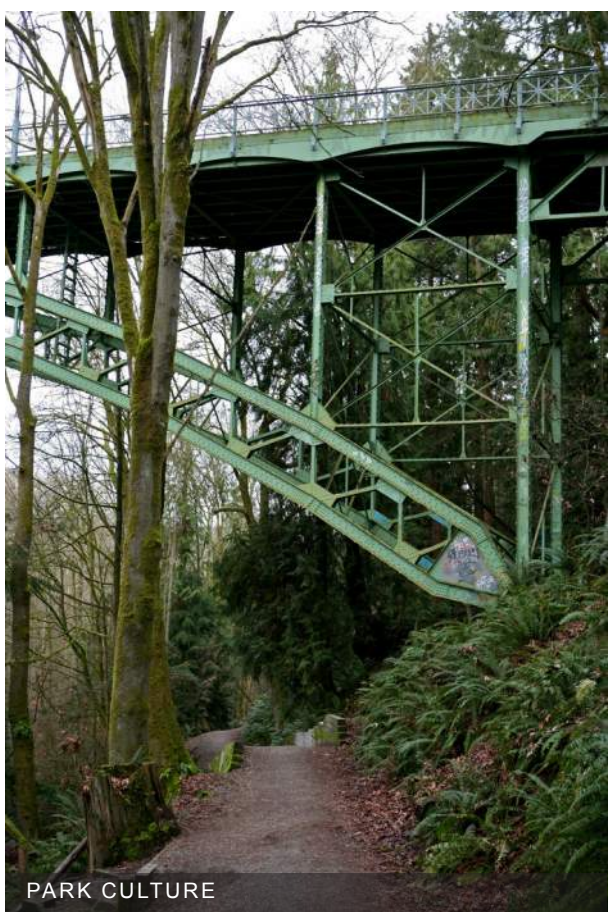
HISTORIC ROOSEVELT COMMERCIAL DISTRICT



CURRENT ROOSEVELT COMMERCIAL DISTRICT



BULL MOOSE FESTIVAL



PARK CULTURE



U-DISTRICT FARMER'S MARKET



PROXIMITY TO GREEN LAKE PARK



HISTORIC ROOSEVELT HIGH SCHOOL



RELATIONSHIP TO UNIVERSITY OF WASHINGTON



LIGHT-RAIL STATION (PROPOSED FOR 2021)



LOCAL BUSINESSES





## MULTI-FAMILY CONTEXT

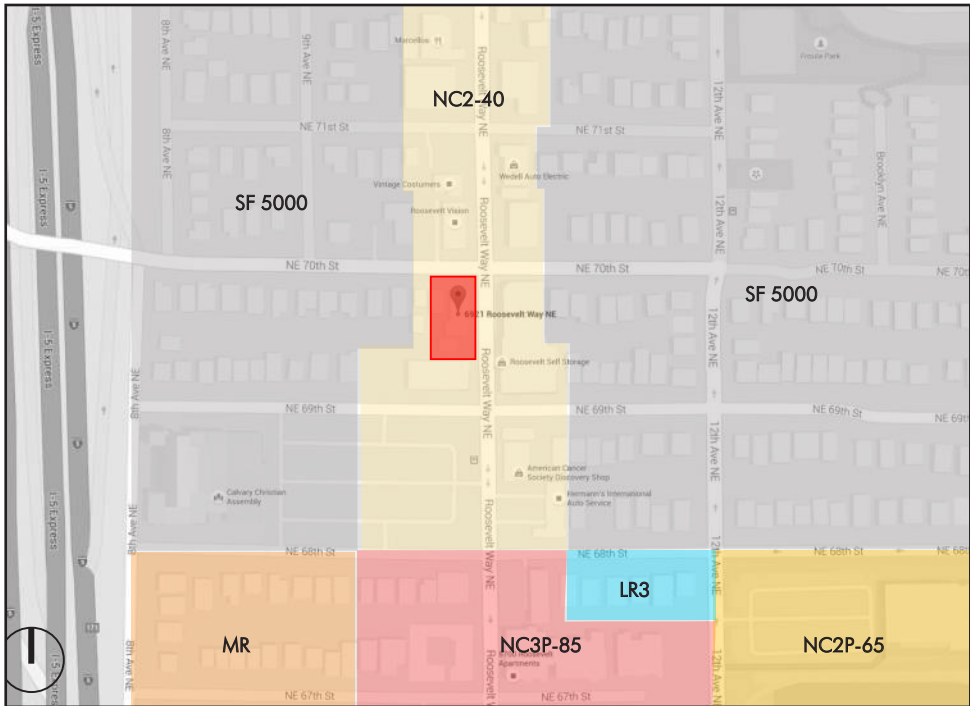
The Roosevelt neighborhood is experiencing a period of profound building activity for multifamily projects, in particular apartment units to meet the demands of the rental market. This proposal will draw from the precedents of the multifamily buildings in the neighborhood, both existing, proposed and currently under construction. These buildings and proposals are diverse in scale and appearance but share common traits, such as simple forms and good quality materials like architectural concrete, brick, fiber cement, and rain screen siding systems. The historical apartment buildings typically offer no interaction with the public sphere. Newer buildings place a priority on interacting with the public at the sidewalk.







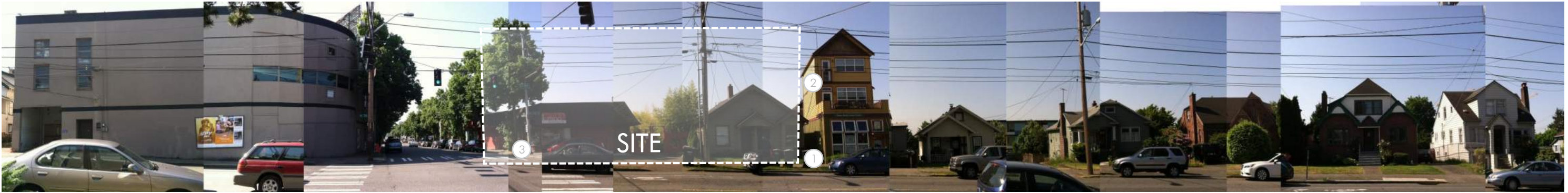








Ⓐ NE 70<sup>TH</sup> ST NORTH PANORAMIC



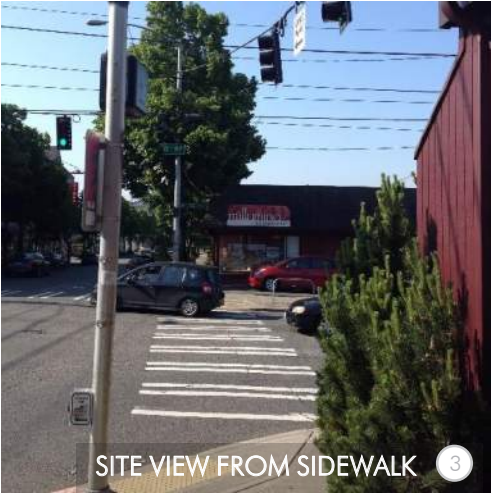
Ⓑ NE 70<sup>TH</sup> ST SOUTH PANORAMIC



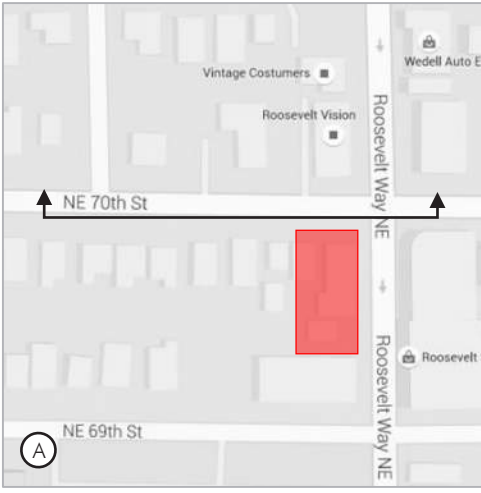
PATHWAY WITH NEIGHBORS 1



BLANK FACADES 2



SITE VIEW FROM SIDEWALK 3



PERSPECTIVE MAPS







← (A) ROOSEVELT WAY NE: EAST PANORAMIC →



← (B) ROOSEVELT WAY NE: WEST PANORAMIC →



PERSPECTIVE MAPS



GREEN BUFFER WITH NEIGHBOR

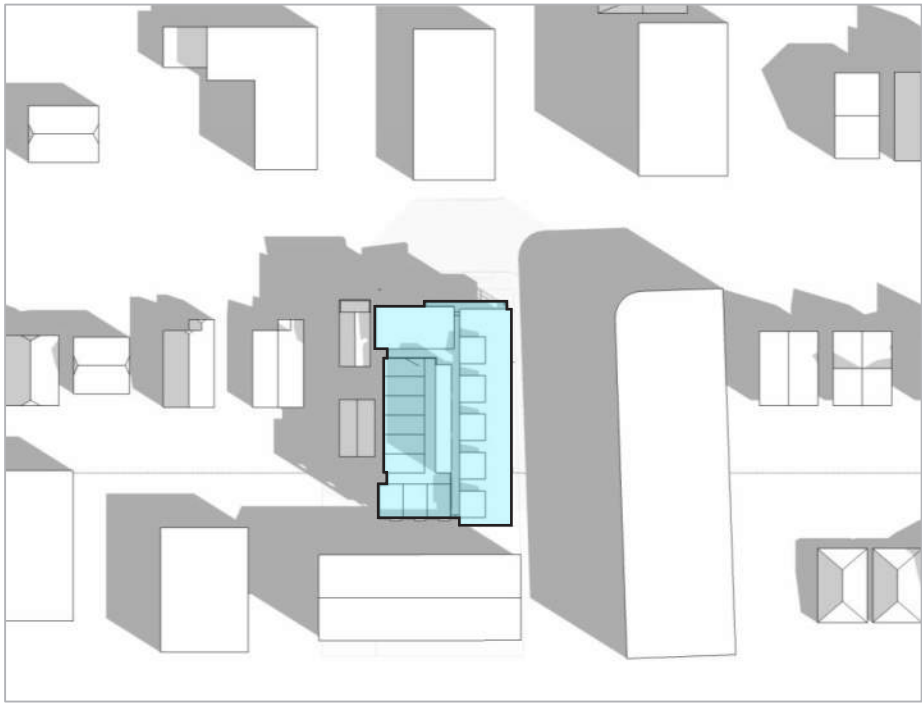


UPDATED NEIGHBOR

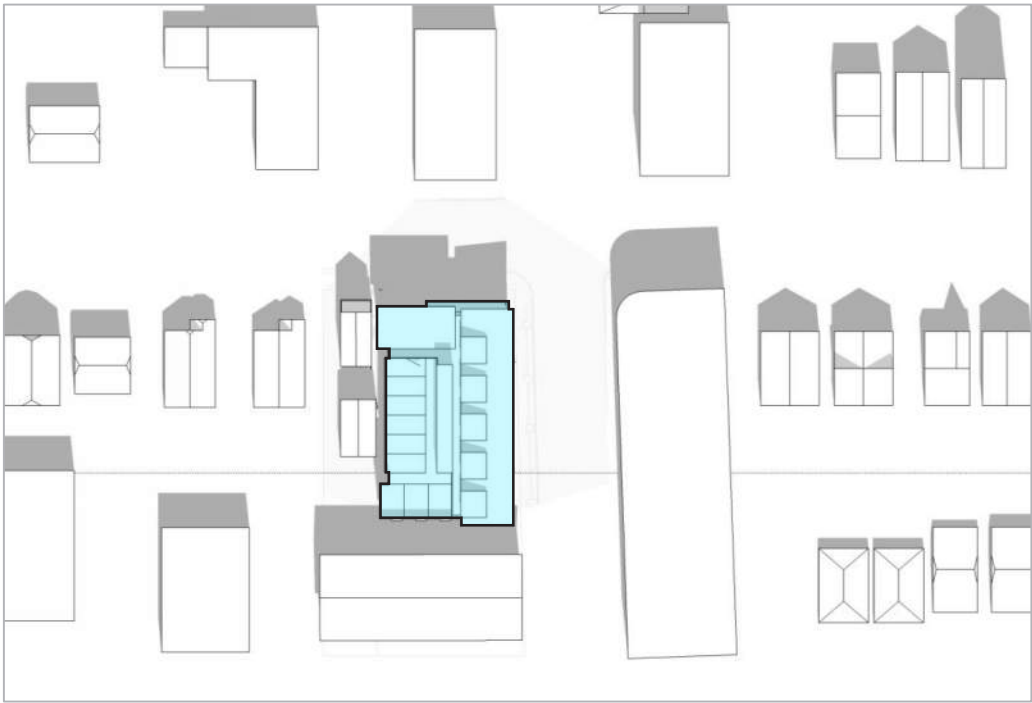


NEIGHBORHOOD SIGNAGE

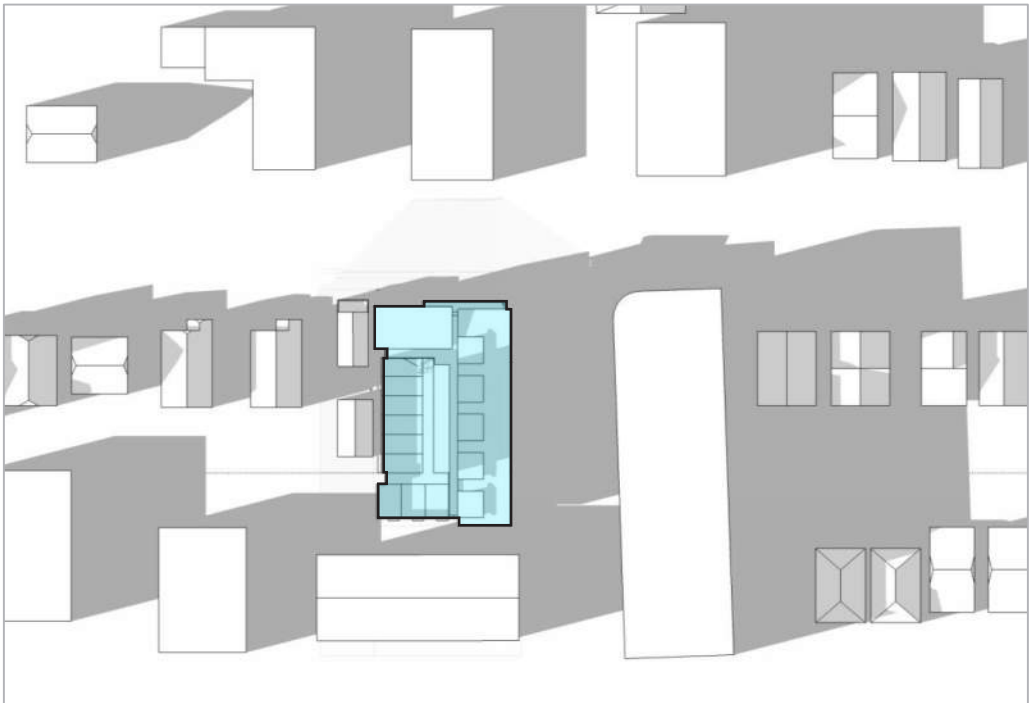




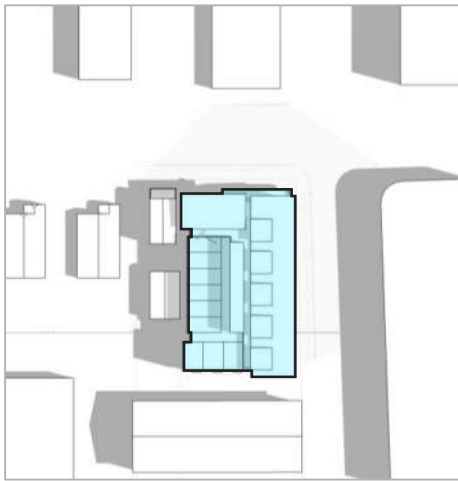
MARCH/SEPTEMBER 21, 9 AM



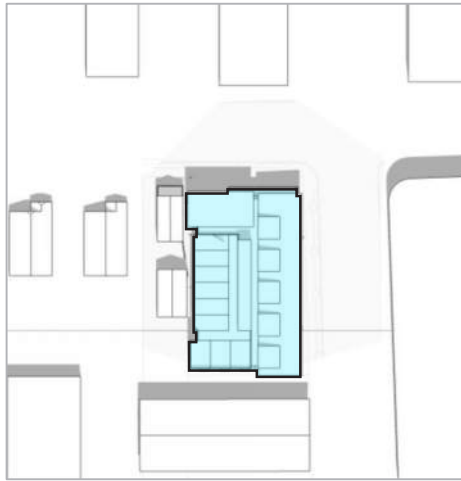
MARCH/SEPTEMBER 21, 12 PM



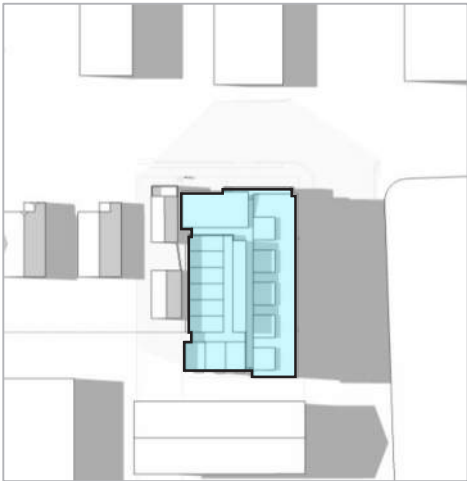
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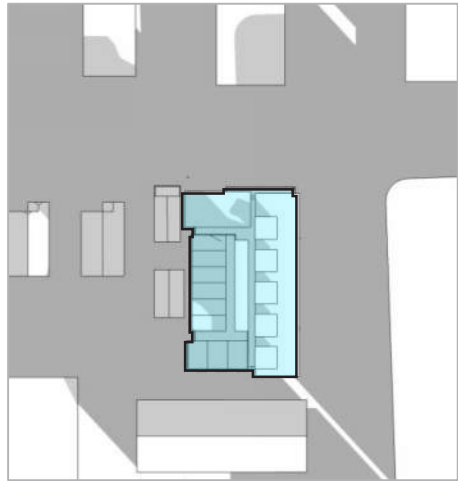
JUNE 21, 9 AM



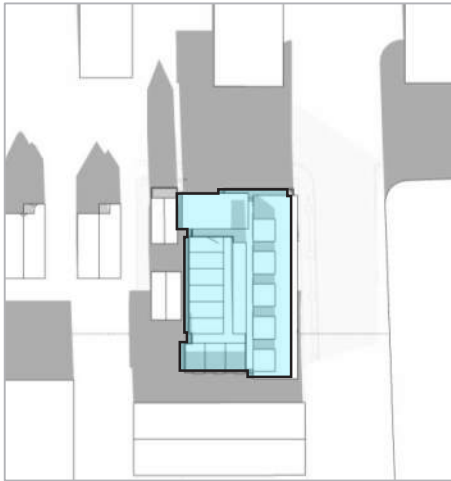
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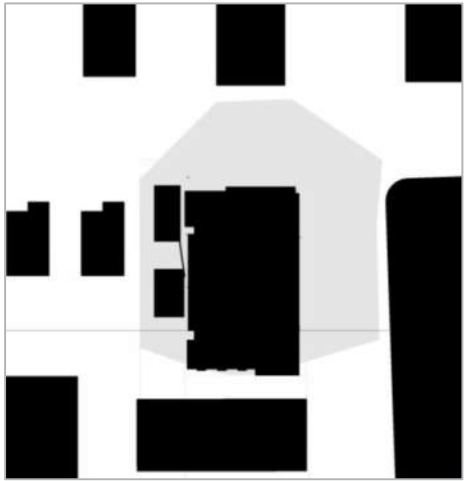
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DECEMBER 21, 9 AM

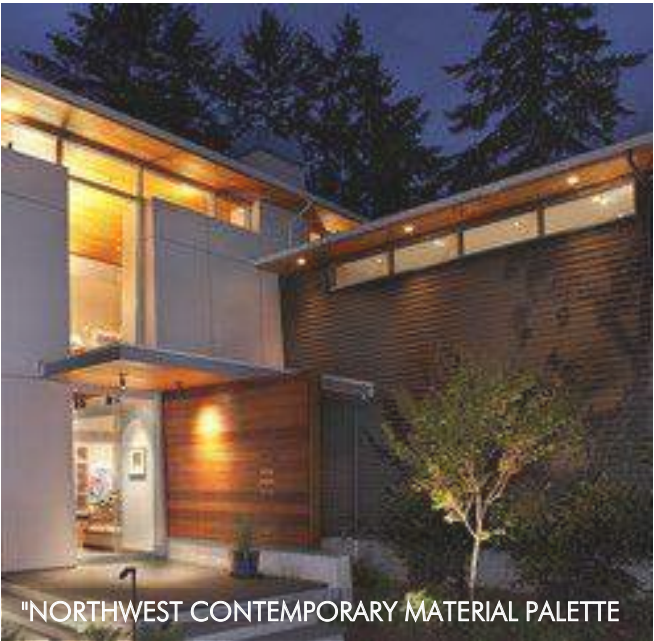
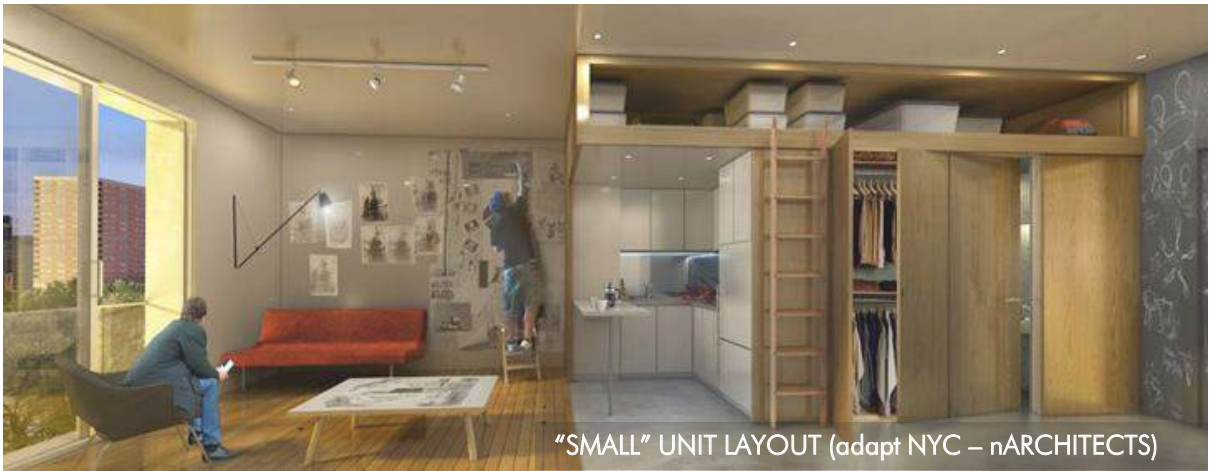


DECEMBER 21, 12 PM



DECEMBER 21, 6 PM











THANK YOU

