

GENEVA SUITES

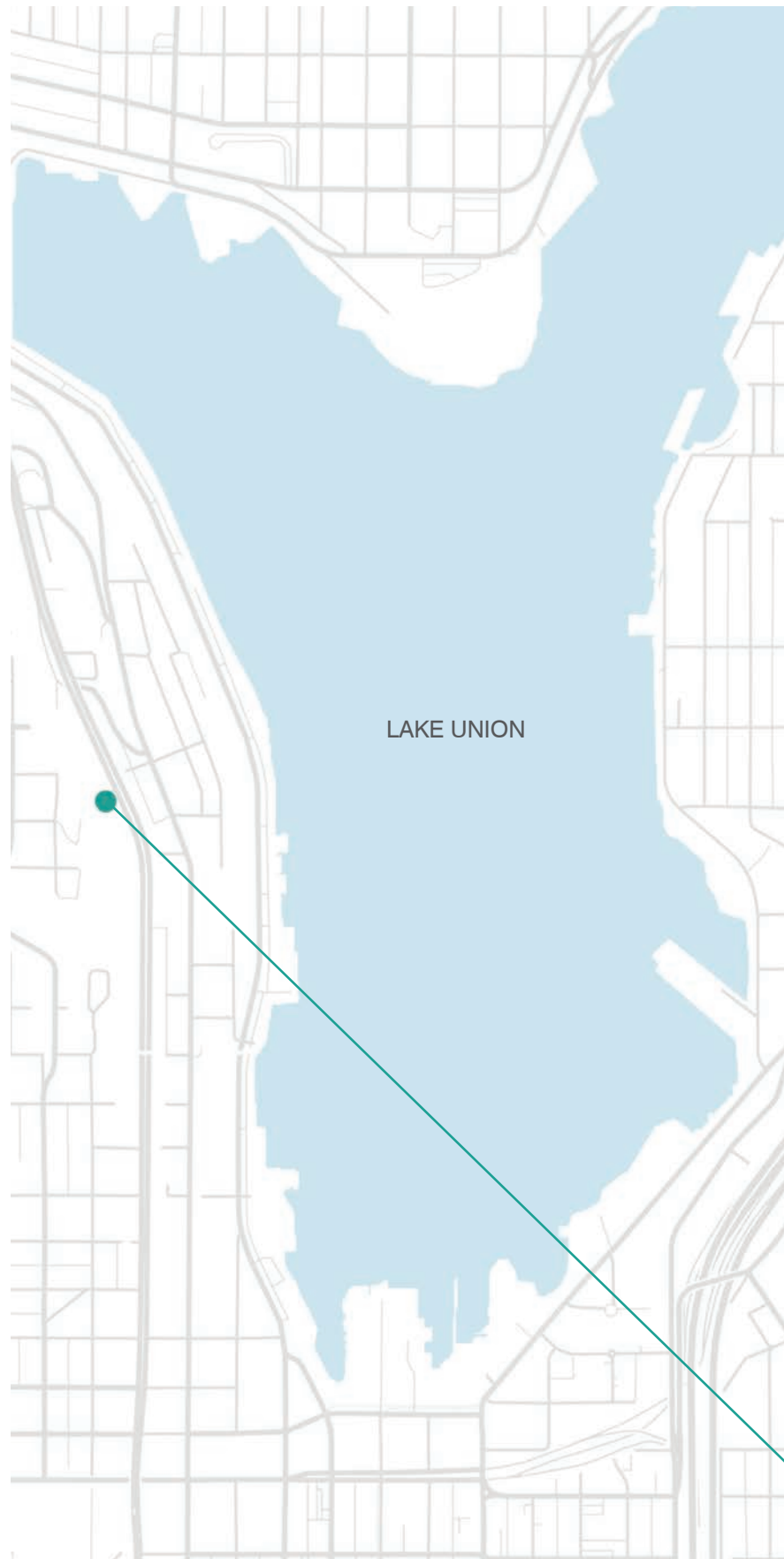
DESIGN RECOMMENDATION

4 OCTOBER 2017

SDCI Project: #3020334



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⊕ VICINITY MAP

DESIGN RECOMMENDATION

20 September 2017

PROJECT ADDRESS:

1945 Aurora Ave. N.

PROJECT TEAM:

Owner

Geneva Apartments LLC
3909 - 51st Ave NE
Seattle, WA 98105
Contact: Cheng-Nan Lin
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Architect

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Landscape Architect

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Bellevue, WA 98008
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Arborist

Tree Solutions Inc
2940 Westlake Ave N #200
Seattle, WA 98109
Phone: 206.528.4670
Contact: Katie Hogan
e/ office@treesolutions.net

Surveyor

Geo Dimensions, Inc
10801 Main Street, Ste 102
Bellevue, WA 98004
Phone: 425.458.4488

SITE 1945 Aurora Ave. N

- 4** Proposal: Summary/ Context Analysis: Zoning
- 5** Context Analysis: Land Use/ Transportation
- 6** Context Analysis: Neighborhood Landmarks
- 7** Context Analysis: Design Cues | Architectural Patterns
- 8** Context Analysis: Vicinity
- 9** Context Analysis: Perspective Views
- 10** Survey
- 11** Shadow Studies + Impacts
- 12** Zoning Data: Seattle Municipal Code: C1-65
- 13** Design Recommendation Guideline
- 15** Perspectives
- 16** Site Plan
- 17** Architectural Floor Plans
- 25** Architectural Section
- 26** Architectural Elevations-Materials
- 30** Landscaping Plans and Concept
- 35** Building Signage
- 36** Building Lighting Plan
- 37** Departures: Development Standard

Overview

Project Information

| | |
|------------------------|---------------------------------------------------------------------------------------------|
| Parcel: | 9301301390 |
| Lot Area: | 9,392 |
| Zoning: | C1-65 |
| Overlay: | None |
| Street Classification: | Aurora Ave N. (Principal Arterial) 6th Ave. N. (Unimproved) Crockett St. (Unimproved) |
| Frequent Transit: | Yes |

The project is a six-story apartment building containing 36 small efficiency dwelling units (SEDU'S). No commercial space is provided.

Project Details:

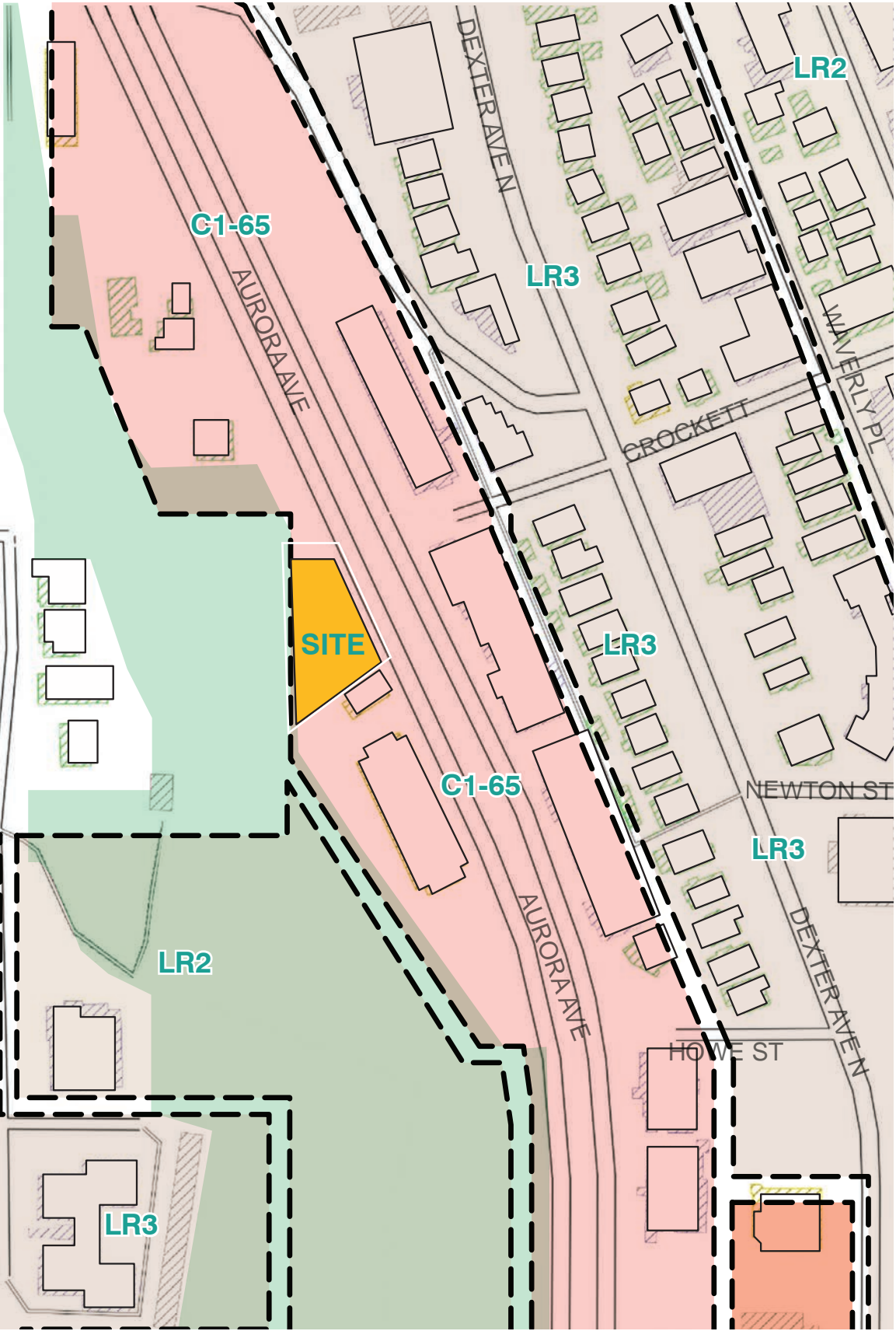
| | |
|-----------------|---------------|
| Units: | 36 SEDU |
| Total GSF: | 31,869 sq. ft |
| Parking Spaces: | 17 |

KEY: Zoning

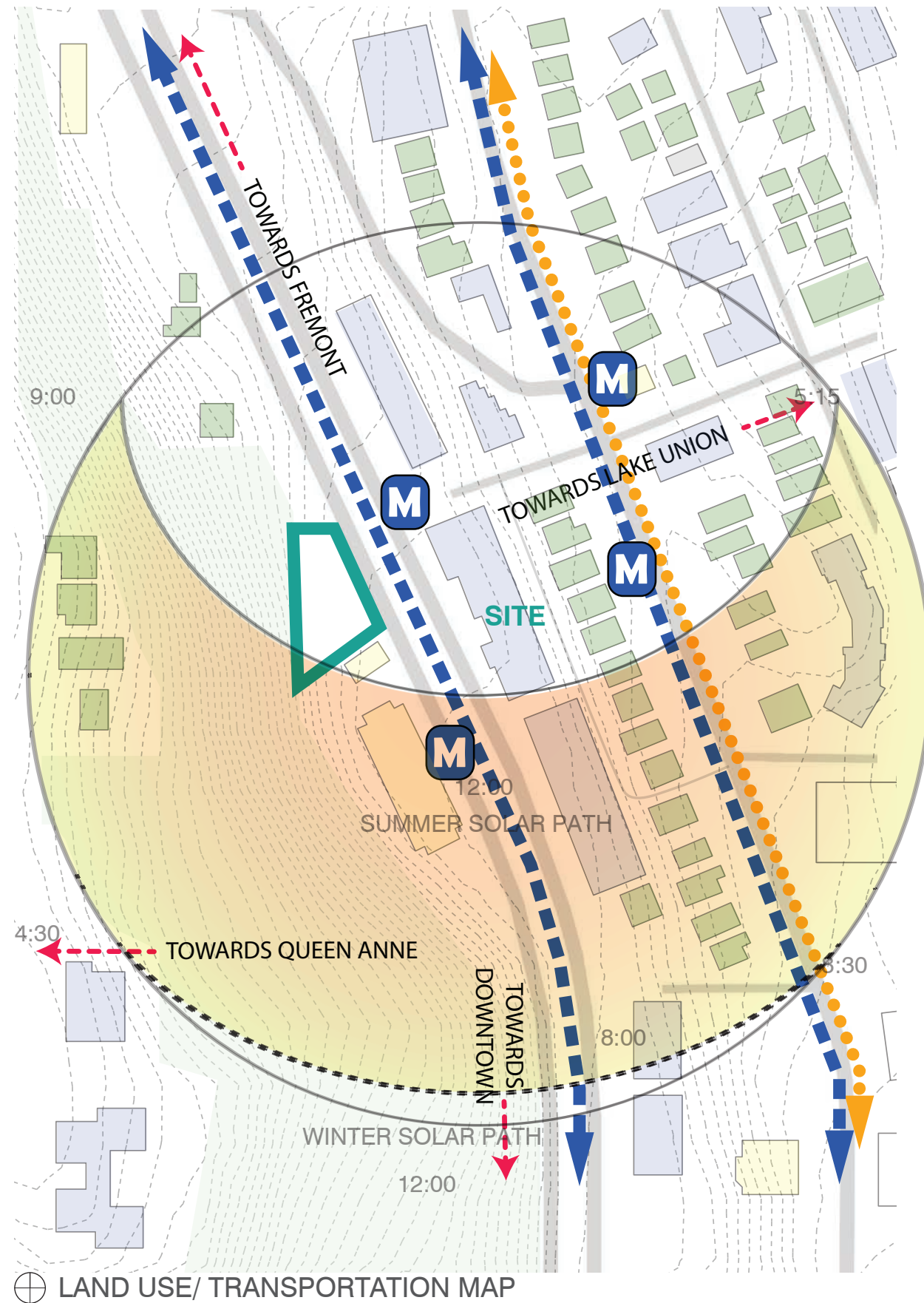
Neighborhood Commercial 1 (C1-65)

Lowrise 2

Lowrise 3



⊕ ZONING AND URBAN CONTEXT MAP



Urban Context




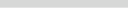



The site is located on one of the major arterial streets in Seattle. To the East, the building is adjacent to an unused r.o.w. and a greenbelt. To the North is unimproved Crockett St. The structures to the South are commercial, including a proposed drive-in espresso stand. Across Aurora, the buildings are multifamily.

The close proximity to Aurora Ave. allows for easy transit: North to the neighborhoods such as Fremont, Ballard, or Greenwood, or South towards South Lake Union and Downtown. Bus Routes 5, 16, and E-line operate along Aurora Ave with the bus stops in close proximity to the site. Pedestrian and bicycle (Dexter Ave. N.) connections across Aurora Ave. N. are located to the North at the Dexter Way N. underpass and to the South at the Ray Moore Bridge. Northbound vehicles can go under Aurora at 6th Ave. N near the Canlis Restaurant.

KEY: Urban Context

-  Single Family Residence
 Multi Family Apartments
 Commercial / Retail
 Other

KEY: SDOT Classification

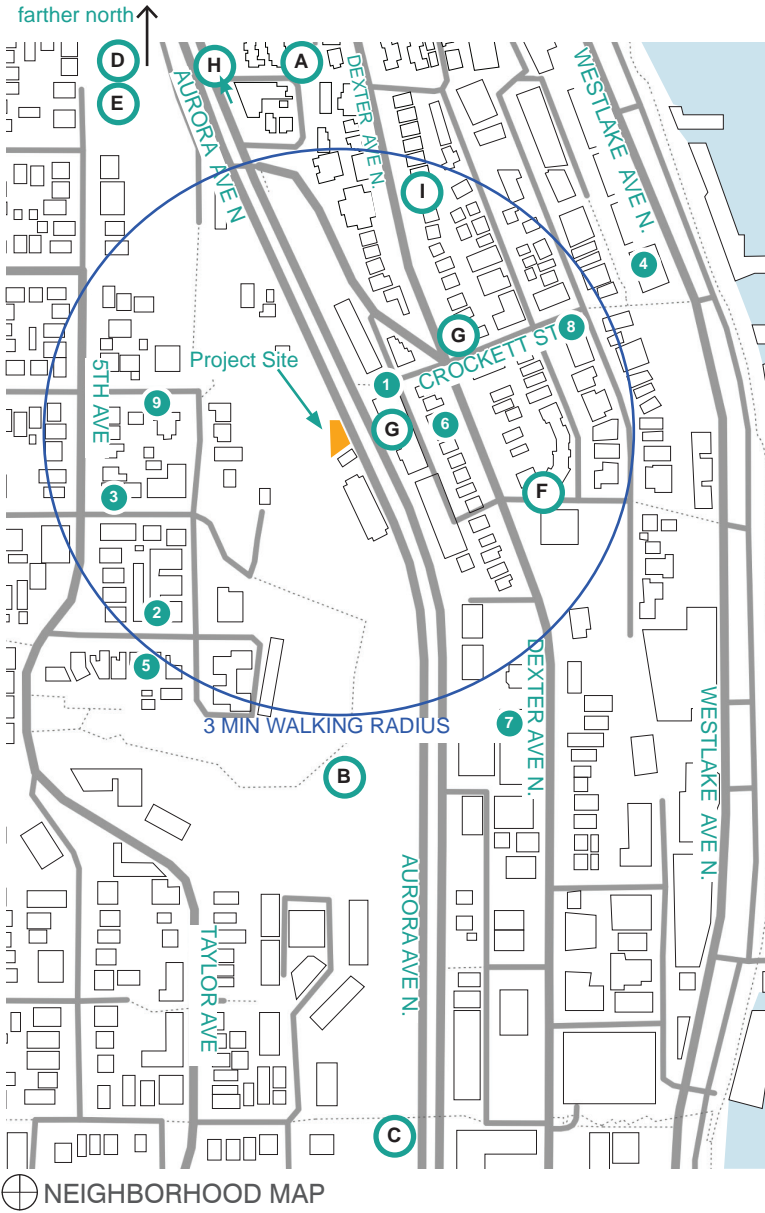
- | | |
|---------------------------------------------------------------------------------------|--------------------|
|  | Green Space |
|  | Primary Arterial |
|  | Secondary Arterial |
|  | Bus Route |
|  | Bus Stop |
|  | Bike Route |
|  | Topography |

Places and Buildings of Interest

The site is located in a no-man’s land between two Seattle neighborhoods. There really isn’t a close connection to any relevant buildings. However, there are a number of places and buildings of interest. To the West, adjacent to the property is the Northeast Queen Anne Greenbelt. A couple of minutes to the North, across Aurora Ave. N., is the Thomas C. Wales Park. To the south, the Ray Moore Bridge allows for East-West pedestrian traffic, as does the tunnel under the Aurora Ave (three minutes walk North from the site).

Neighborhood Urban Features

Two of the main neighborhood features are a steep slope down from Aurora Ave N. towards Lake Union and a steep slope up from Aurora Ave. towards Queen Anne. Due to the prospective views of Lake Union, most of the residential structures incorporate decks and balconies facing west as a part of their design. The presence of sloped terrain also influence the massing arrangements of the buildings, resulting in designs that include stepping of the structures with the slope. Attention is also given to the spaces in front of the buildings., Landscaping is used to screen the buildings from Aurora Ave. N.; making the adjacent streetscape more aesthetically pleasing to the vehicular traffic and the pedestrian eye.



NEIGHBORHOOD MAP

Landmarks, Green spaces, & Residences

- (A) Thomas C. Wales Park, 2401 6th Ave N
- (B) Northeast Queen Anne Greenbelt, 1901 Aurora Ave N
- (C) Ray Moore Bridge
- (D) Aurora Bridge
- (E) Canlis Restaurant, 2576 Aurora Ave N
- (F) Swedish Cultural Center, 1920 Dexter Ave N
- (G) Portal Apartments, 655 Crockett St
- (H) Domaine Apartments, 2483 Birch Ave N
- (I) Private Residence, 2010 5th Ave N

Landmarks



(A) Thomas C. Wales Park - Greenspaces



(B) Northeast Queen Anne Greenbelt - Greenspaces



(C) Ray Moore Bridge - Public Space Landmark



(D) (E) Aurora Bridge - Canlis Restaurant



(F) Swedish Cultural Center - Public Space Landmark



(G) Lyons Grocery - Commercial Landmark

Types of Buildings



G Portal Apartments - Multi-Family Residence



H Domaine Apartments - Multi-Family Condominiums



I Private Residence - Single Family

Treatments

Balconies



1 Balconies - Multi-Family Residence



2 Balconies - Condos

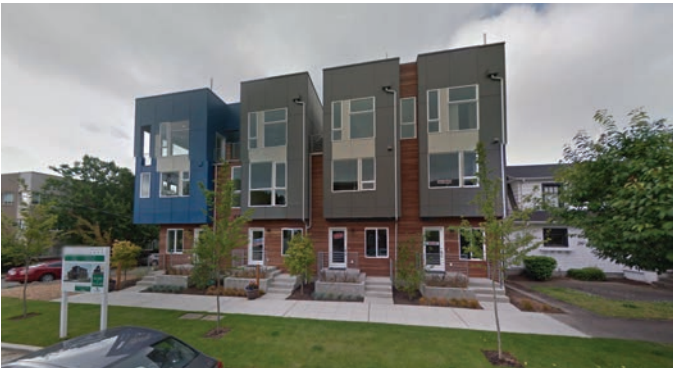


3 Balconies - Single Family Residence

Front Spaces



4 Front Spaces - Multi-Family Residence



5 Front Spaces - Condos



6 Front Spaces - Single Family Residence

Slope Terrain



7 Slope Terrain - Multi-Family Residence



8 Slope Terrain - Condos



9 Slope Terrain - Single Family Residence

Streetscape Context

Buildings along Aurora Ave. N. are residential, mostly 3-4 story apartment/condominium buildings, mixed with small commercial. There is no street parking on the Aurora Ave N. due to the heavy traffic flow. The parking for the apartment structures across Aurora Ave. N. are typically accessed from a street (other then Aurora Ave. N.), or alley, and are in a garage below Aurora Ave. N.

The street facades on the East side of Aurora Ave N. are generally unremarkable. Typically their facades facing Aurora Ave feature a more or less blank wall, concentrating major openings East towards Lake Union.



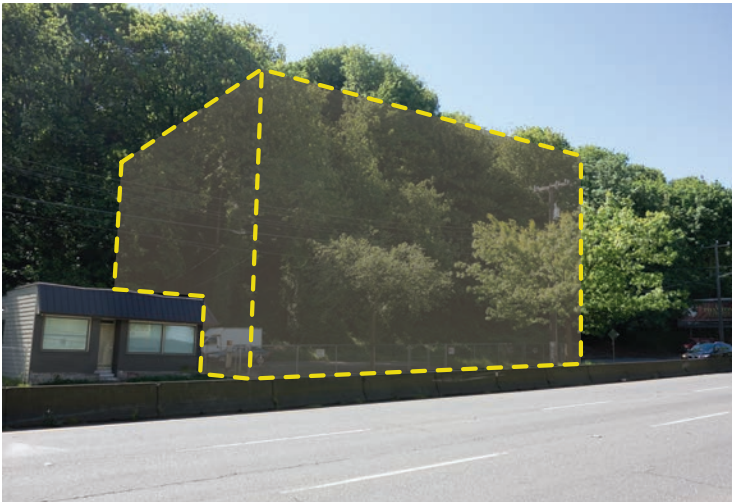
A Facing Southeast



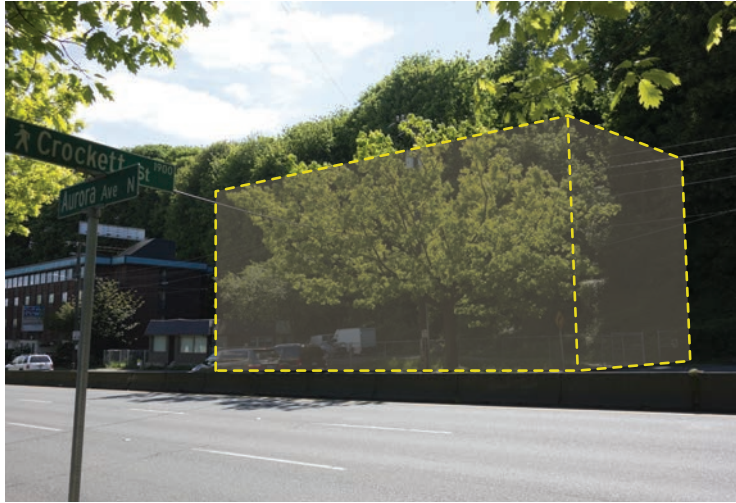
B Facing West



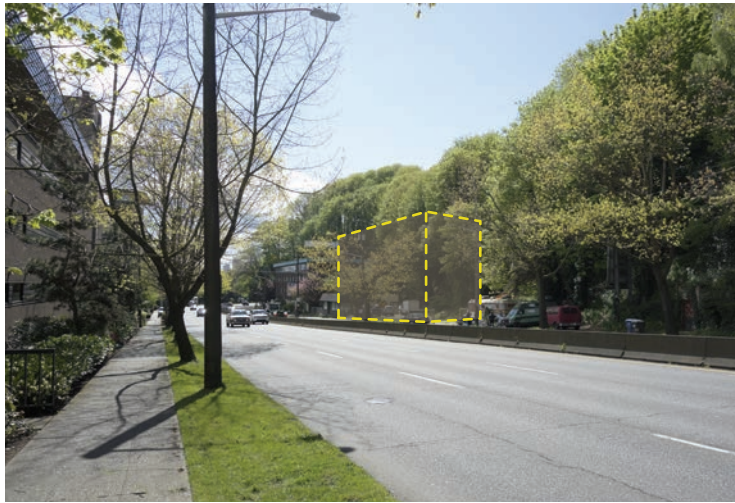
1 Looking West on Aurora Ave N.



C Facing Northwest



E Facing Southwest



F Facing South (not on map)



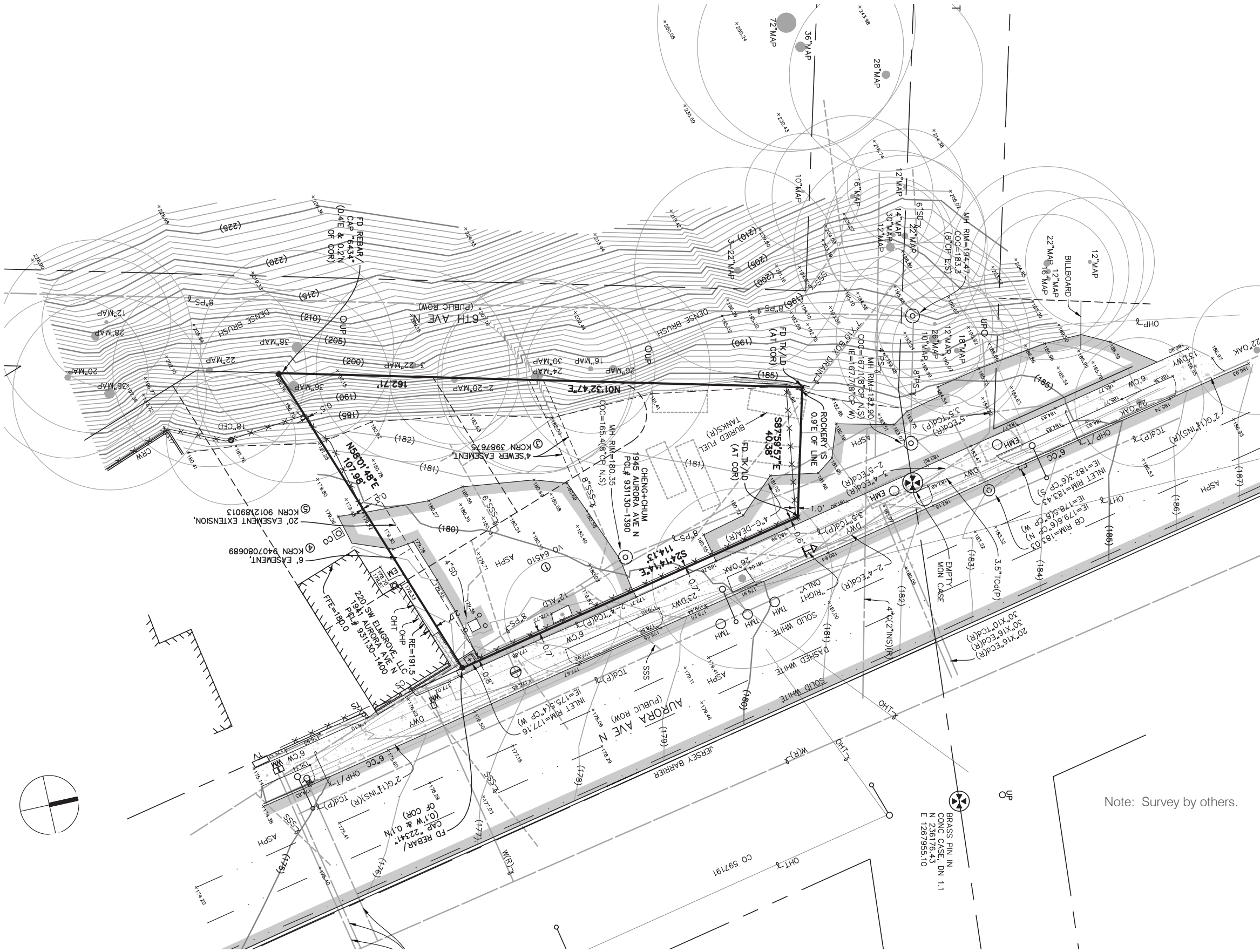
D Facing North



G Aurora Ave N: Traffic Intersection



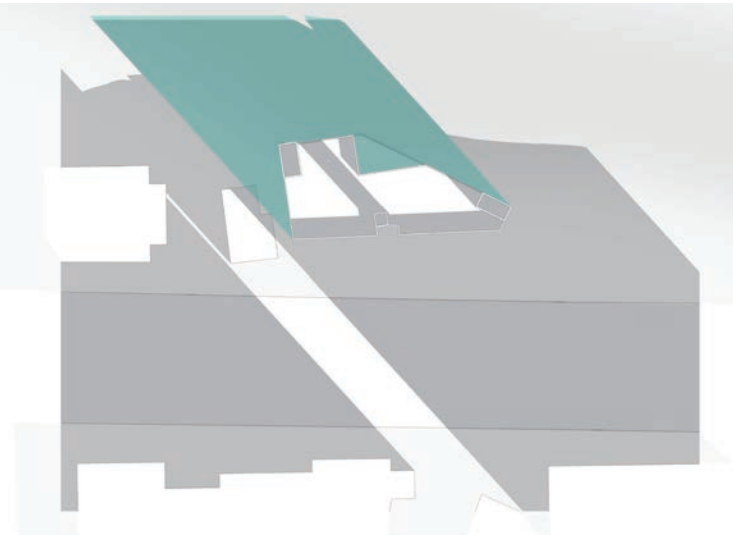
2 Looking East on Aurora Ave N



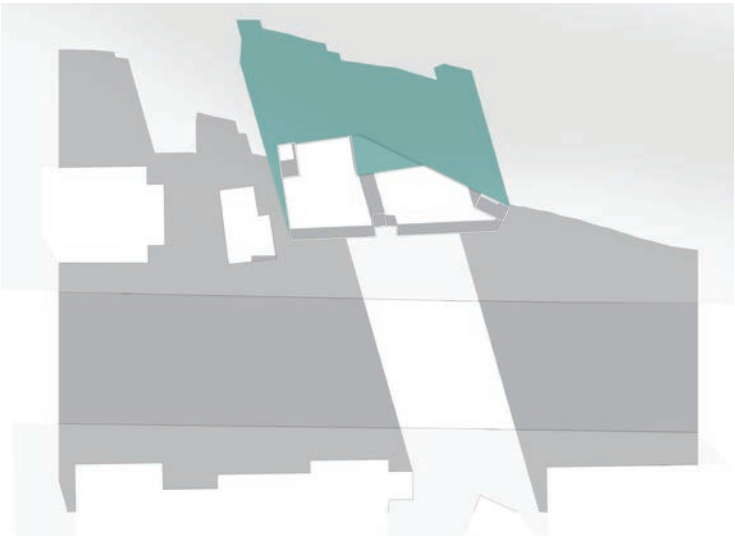
Note: Survey by others.

Shadow Studies: Overview

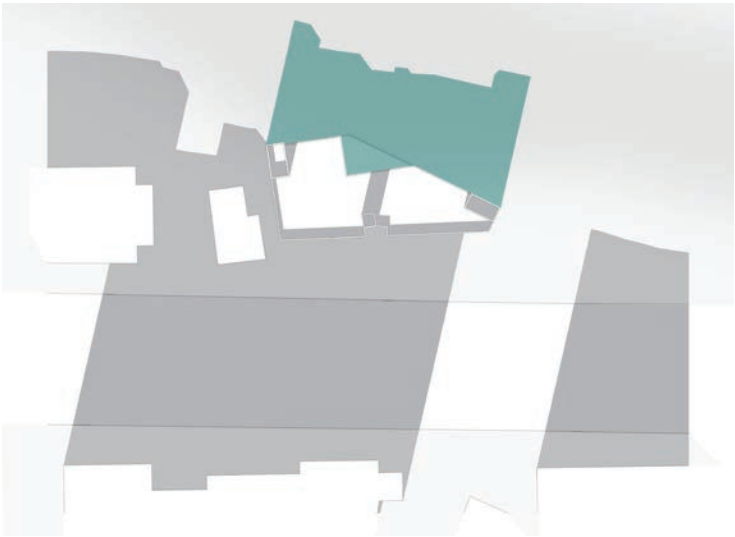
The site primarily has access to the Eastern sun throughout all the seasons of the year. The shadows cast by the buildings across Aurora Ave. N. are negligible. Because of Queen Anne Hill the project location receives limited Spring and Summer afternoon and evening sun and filtered light during the Fall and Winter.



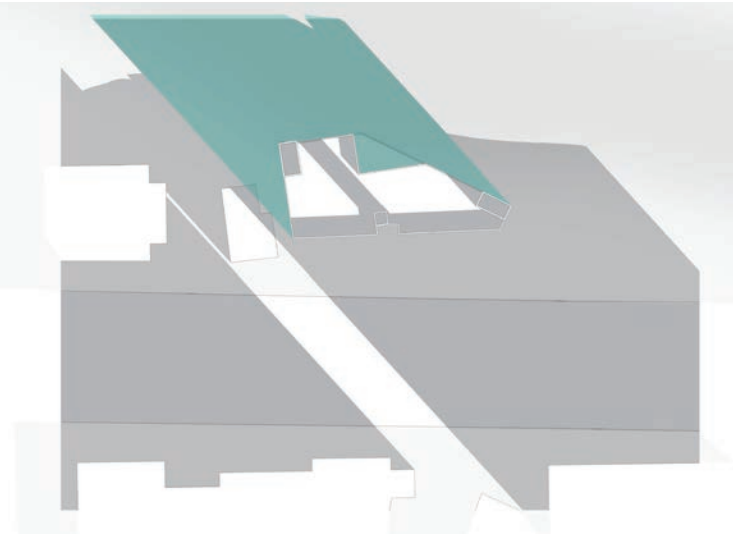
Equinox - March / September 21 at 10am



Equinox - March / September 21 at 12pm



Equinox - March / September 21 at 2pm



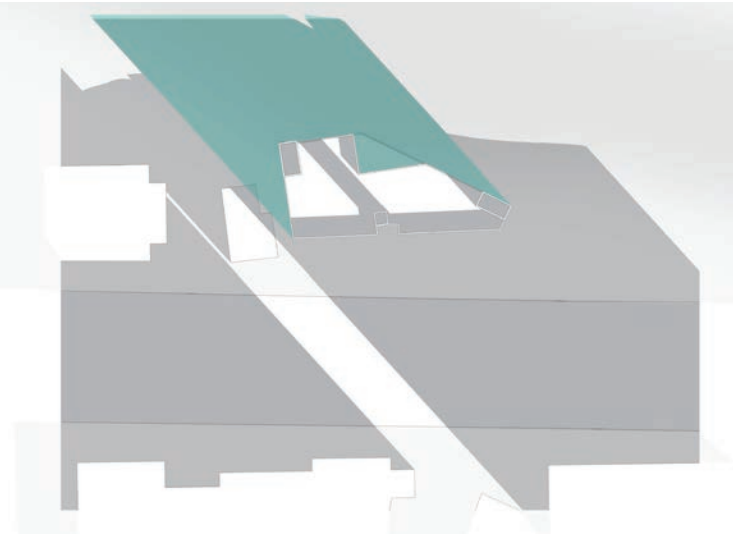
Summer Solstice - June 21 at 10am



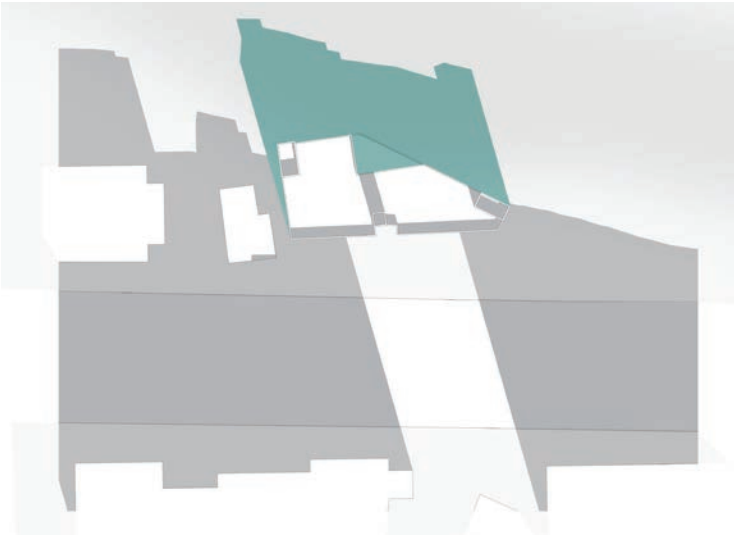
Summer Solstice - June 21 at 12pm



Summer Solstice - June 21 at 2pm



Winter Solstice - December 21 at 10am



Winter Solstice - December 21 at 12pm



Winter Solstice - December 21 at 2pm

SEATTLE MUNICIPAL CODE TITLE 23
SMC 23.47A Commercial

SMC 23.41 Early Project Implementation

012.B Development Sandard Departures

Departures may be granted from any Land Use Code standard or requirement, except for the following: resid. density limits, Floor Area Ratios, max. size of use, structure height, storage of solid waste containers, noise and odor stds., reqs. For streets, alleys, and easements per Chapt. 23.53, definitions, and measurements.

Response:

Departures are requested at this time. See p. 26.

SMC 23.47A Commercial

004 Permitted and Prohibited Uses

Residential uses in mixed use development permitted outright

005 Street Level Uses

C.2. Residential Uses may not occupy more than 20% of the street-level street facing facades when facing an arterial

008 Street Level Development Standards

A.1. Applies to structures that contain residential uses in C zones.

A.2.b. Blank segments of the street-facing façade between 2’ & 8’ above the sidewalk may not exceed 20’ wide

A.2.c. The total blank façade segments may not exceed 40% of the width of the façade of the structure along the street

A.3 Street-level facing facades must be located within 10’ of the property line, unless as otherwise approved

B.2 60% of the street facing façade between 2’ & 8’ shall be transparent ; view into space, or in live work units into 30” deep display windows

B.3 Nonresidential uses must extend an average of at least 30’ and a minimum of 15’ except if the depth requirements would result in a space greater than 50% of the structures footprint

B.3.b Street level non residential uses shall have a floor to floor height of at least 13’

D.1 At least one residential use shall have a visually prominent pedestrian entry

Response:

Departures from SMC 23.47A.008.A & B.. See p. 26.

012 Structure Height

A.1 The maximum height is 65’ per Land Use Map

C.2 Open railings, parapets, and fire walls may extend up to 4’-0” above the maximum height.

C.4 Rooftop features including elevator and stair penthouses & mechanical equipment may not exceed 25% of the roof area.

C.4.F Rooftop features including elevator and stair penthouses & mechanical equipment may extend up to 16’ above applicable height limit

013 Floor Area Ratio

A.3 Above grade parking within or covered by a structure must be included in FAR calculations.

C Maximum FAR for single purpose residential outside SAOD is 4.25

D. Gross floor area below grade not counted against FAR

014 Setbacks

016 Landscaping and Screening Standards

A.2 Landscaping must achieve a Green Factor of 0.30 for any new structure over 4 units

B.1 Street trees are required per SDOT, existing trees count toward the requirement

018 Noise Standards

020 Odor Standards

A. Venting of odors, vapors, smoke, etc. shall be 10’-0” above the finished sidewalk grade, and shall be directed away to the extent possible from residential uses within 50’-0”

022 Light and Glare Standards

A. Exterior lighting shall be shielded from adjacent uses.

B. Interior lighting in parking garages shall be shielded.

024 Amenity Areas

A. Residential amenity areas of 5% of the total gross residential floor area including, but not limited to, decks, balconies, terraces, roof gardens, plazas, courtyards, play areas, or sports courts

B.1 All residents must have access to at least one amenity space

B.2 Amenity spaces may not be enclosed

B.4 Common amenity areas must have a minimum horizontal dimension of 10’ and be a minimum of 250 sq. Ft.

B.6 Private balconies must have a minimum horizontal dimension of 6’ and be a minimum if 60 sq. ft.

030 Required Parking and Loading

A. Access to parking

Response:

Access for parking is proposed from Aurora Av. N. 6th Ave. N. and Crockett St. are unimproved.

033 Transportation Concurrency

All uses shall meet the transportation concurrency level-of-service per chapter 23.52

SMC 23.53 Requirements for Streets, Alleys, and Easements

015 Improvement Requirement for Existing Streets in Residential and Commercial Zones

Street improvements required per SDOT standards

Response:

Right of Way Improvement Exception Requests have been granted for improvements

on 6th Ave. N. and Crockett St.

030 Alley improvement in all zones

Alley improvements required per SDOT standards

035 Structural Building Ovehang

A.1 8’ vertical from sidewalk min.

A.2 1’ horiz, 2’-6” ht., projection for architectural , or decorative features – eaves, etc.

A.4 Window bays/balconies – 8’ above sidewalk, max. 3’ horiz. Projection, 50% open area, 15’ max. length, 2’ separation (see additional specific requirements)

Response:

None proposed.

SMC 23.54 Quantity and Design Standards for Off Street Parking

015 Required Parking

A. Min. parking per SLUC 23.54.015,, Tables A and B, except as modified in this section

K. Bicycle parking required at 1 stall per 4 units for multi-family structures’

Table B, Parking for Residential Uses:

1 space for 2 small efficiency dwelling units (SEDU)

Table E, Parking for Bicycle:

General Sales and Services and Eating and Drinking Establishments – 1/12,000 sf long term,1 /4,000 sf short term; Multi-family – 1 /4 units

Response:

20 parking spaces provided for 40 SEDU’s

030 Parking Space Standards B.1A Minimum medium stalls for residential use (<5 stalls provided))

B.2 75% large stalls for nonresidential (<10 stalls provided)

030 Solid Waste and Recycleable Materials Storage and Access

Table A – 26-50 units - 375 square feet; 0-5,000 sf non-residential – 82 sf

Response:

SEATTLE MUNICIPAL CODE TITLE 25

SMC 25.09.180. Development Standards for Steep slope Areas

B.2 Exception

Response:

Steep slopes on the site are the result of legal grading for Aurora ave. N. We will apply for an exemption

1- Massing Options:

The Board discussed the strengths of the different massing options and strongly supported the hinged massing of Alternate A and the depth with vertical divisions and horizontal balconies shown in Alternate C. The Board preferred a combined massing option which incorporates the hinged massing of Alternate A with Alternate C, as the modified form has the best potential to create architectural presence, address the perceived gateway and respond to the streetscape. The Board directed the applicant to proceed with this modified preferred option. (CS2-A-1, CS2-C, DC2)

- a) Related to the Crockett frontage, the Board supported the projected stair massing element shown in Alternate A and encouraged developing this facade as a corner element to provide a sense of place for the perceived gateway. (CS2-A-1, CS2-C-1, DC2-B)
- b) The Board supported the proposed landscape concept for the roof as shown on page 30 of the packet. Acknowledging that the roof plane will likely be visible, the Board recommended designing the roof as a fifth elevation. (DC2-B-1)
- c) Echoing public comment, the Board requested site sections to clarify the relationship to context and the visibility of the roof for the next meeting. (CS1-C, DC2-B-1)

Response:

To reduce the perceived mass, the building design strategically uses decks, recessed elements, and highlighted building entries.

The most visible facade of the building is the one facing the street. Particular attention will be paid to this side. However, the design will be consistent on all sides of the building including the roof which will be carefully landscaped and it will be treated as the 5th elevation.

An alternate to the open frame at the roof amenity area is shown to the right.

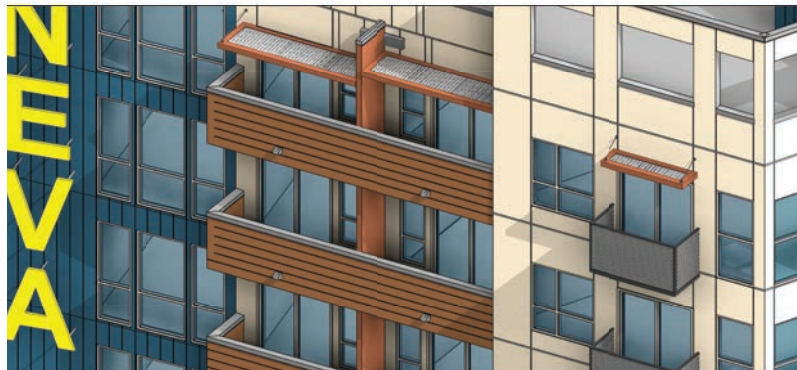
2- Façade Articulation:

The Board supported the conceptual response to each frontage condition and gave guidance for the design development.

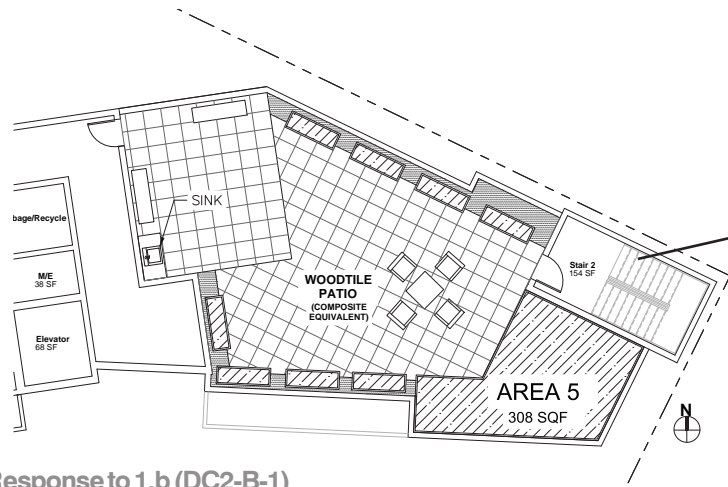
- a) The Board supported the windows at the elevators as they provide façade interest and daylight access and recommended incorporating more windows, where possible, along the Aurora and Crockett frontages. (CS1-B-2, DC2)
- b) For the upper level balconies, the Board viewed the design of the guardrails as an opportunity to create an interesting cascading feature and recommended strengthening the connection between the guardrail design with the green wall feature in order to create a holistic design. (DC2, DC4)
- c) The Board supported detailing of the recessed walls and balcony soffits in a way that is differentiated from the cascading balconies and recommended a simple expression. (DC2-B, DC2-C, DC4)

Response:

The Aurora Ave N facade is broken into five elements (A-B-C-B-A) that project, recede and vary in material and color. The intent is to provide a facade readable at multiple levels: driving by at 40 mph and walking by. There are full length windows on the elevator lobby providing more natural lighting to the lobbies on each floor and creating a shaft of glass and light at the center of the facade. There are two green screen treatments proposed. The first continues the horizontal effect of the balcony siding. The second overlays the balconies and the Level 1 facade with a mesh green screen.



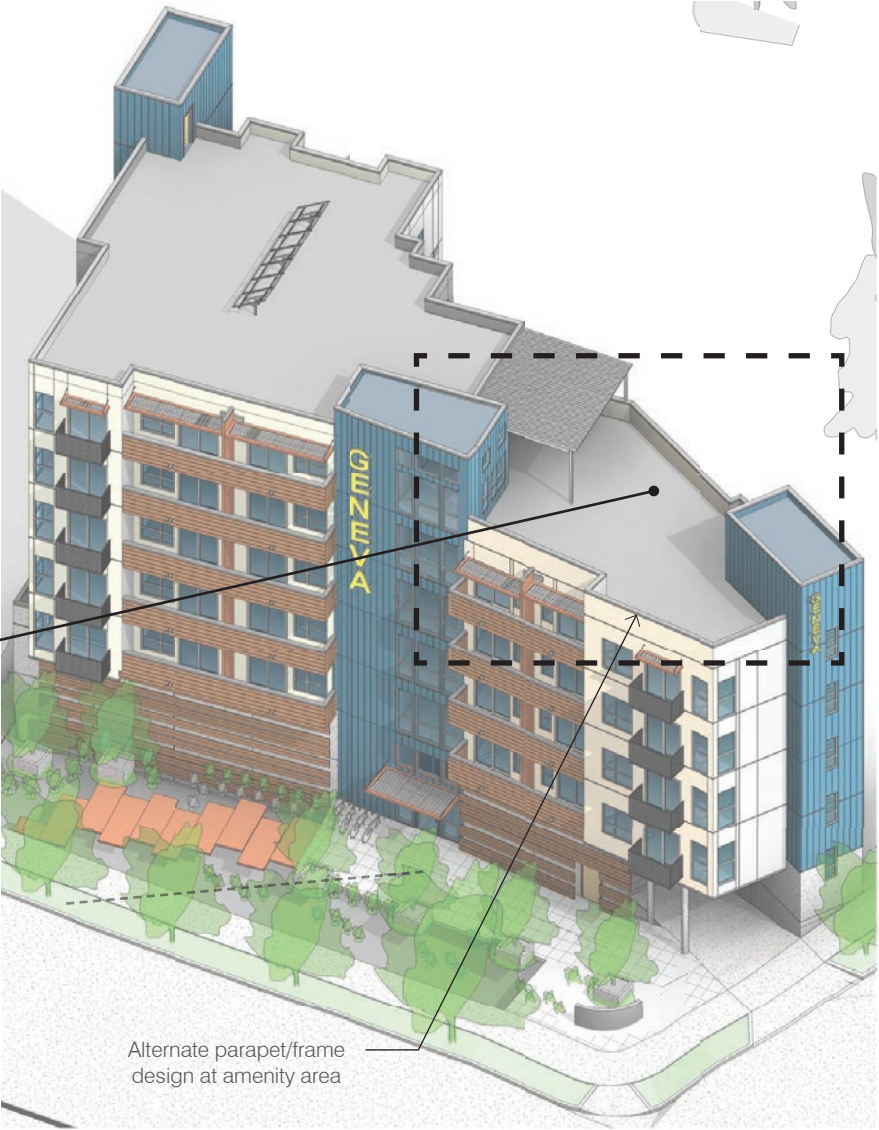
Response to 2.b-2.c (DC2, DC4, DC2-B, DC2-C, DC4)



Response to 1.b (DC2-B-1)



Response to 1.c (CS1-C, DC2-B-1)



Response to 1.a (CS2-A-1, CS2-C-1, DC2-B)



3- Blank Wall/ Green Wall Detailing:

The Board agreed the green wall should read as part of the building identity instead of an applied screening and recommended designing the green screen/ wall as an expressive architectural feature. The Board strongly supported the character represented on page 30 of the packet and gave direction to incorporate modulation, variety, variation and layered planting into the design, similar to what is shown in the precedent images. Related to this feature, the Board indicated preliminary support of the requested departures, provided that the green screen/ wall is further developed as a special architectural feature. The Board requested more information about the green screen/wall detailing for the next meeting. (DC2-B-2, DC2-C, DC2-D, DC4-C-1, DC4-D-1)

Response:

As noted, there are two green screen alternates proposed to cover the blank facade. We feel it's visibility doesn't justify an elaborate green wall, such as on page 30 of the EDG packet. The mesh alternative is simpler and will provide a better base for vegetation. Visually, it also, creates another dimension on the facade. No Standard Departure is required for the blank walls since there are no blank walls greater than 40 that are not covered with screening and landscaping on the wall itself per 23.47A.2.a.5)

4- Trees:

The siting of the structure allows for the retention of one Exceptional Tree and several significant off site trees. Several mature trees are proposed to be removed. The Board supported the proposed retention of the mature vegetation as it provides visual interest and buffering. For the significant trees proposed to be removed, the Board recommended a larger, 3 inch caliper, replacement planting in order an immediate effect upon planting. The Board requested a detailed landscape plan for the site and the right of way, with proposed species and sizes noted for the next meeting. (CS1-D-1, CS2-D-2, DC4-D)

Response:

Required landscaping criteria have been discussed with the landscape architect and arborist and will be collaborated into final landscape design. Note: tree sizes in the front setback are limited by SPU due to the sewer line. (See p.16)

5- Streetscape and Landscape:

the Board recommended developing the site design, mindful of safety and sight lines and gave the following guidance:

- a) The Board discussed the proposed vehicular access and recognized that access from Aurora is a challenge that will need to be addressed through SDCI, SDOT and METRO approval. For the design of the circulation, the Board recommended clarifying wayfinding, sight lights and pedestrian cues, with scoring or paving change to indicate the pedestrian spaces. (PL2-B, PL2-D, PL4-A, DC1-B, DC1-C)
- b) The Board also encouraged bike racks onsite for short term bicycle parking, that could potentially be used for deliveries. (PL4-B)
- c) The Board supported the general landscape design direction and recommended that trees be the feature of the design. The Board also indicated preliminary support of the setback departure, provided that pedestrian lighting, bicycle racks, pedestrian amenities were incorporated. (CS1-D-1, CS2-D-2, DC4)

Response:

The vehicular access has been moved to the north of the site from Crockett st. A bicycle rack at the entry is provided for visitors and residents. Extensive landscaping, benches, and site lighting will be provided between aurora and the building.

6- Lighting and Signage:

The Board agreed that site lighting should be incorporated to provide a well-lit entry way for the recessed elevation and encouraged wash washing lighting to highlight the green screen/wall element. The Board requested a lighting plan, mindful of night light pollution and glare impacts, and a signage plan and street level perspectives for the next meeting. (DC4-B, DC4-C-2)

Response:

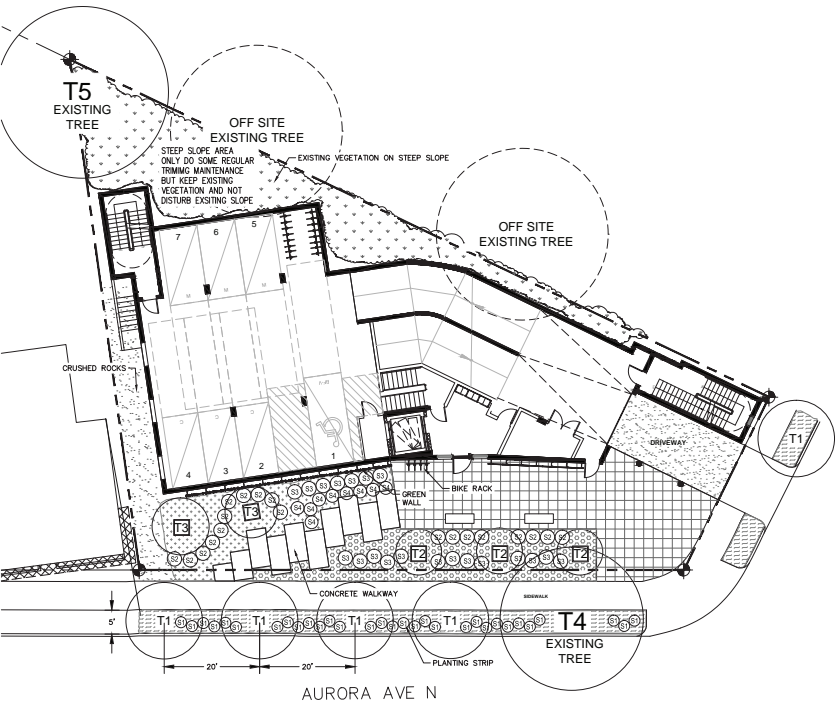
A light plan by the Landscape Architect has been submitted. A signage concept will be provided at the DRB meeting.(See p.26)



Response to 3 (DC-B-2, DC2-C, DC2-D, DC4-C-1, DC4-D-1)



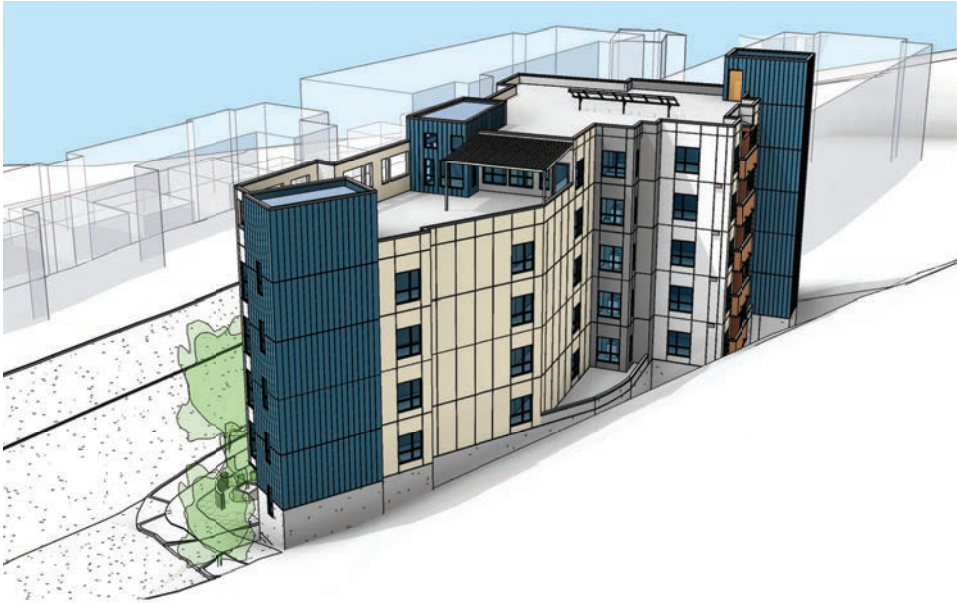
Response to 5&6 (PL2-B&D, PL4-A, DC1-B, DC1-C, PL4-B, CS1-D-1, CS2-D-2, DC4, DC4-B, DC4-C-2)



Response to 4 (CS1-D-1, CS2-D-2, DC4-D)



From Aurora Ave. Looking North



From the Queen Anne Greenbelt Looking Towards Aurora Ave



Looking Towards Garage Entry and Entrance



From Aurora Ave. Looking South



Looking Towards the Queen Anne Greenbelt



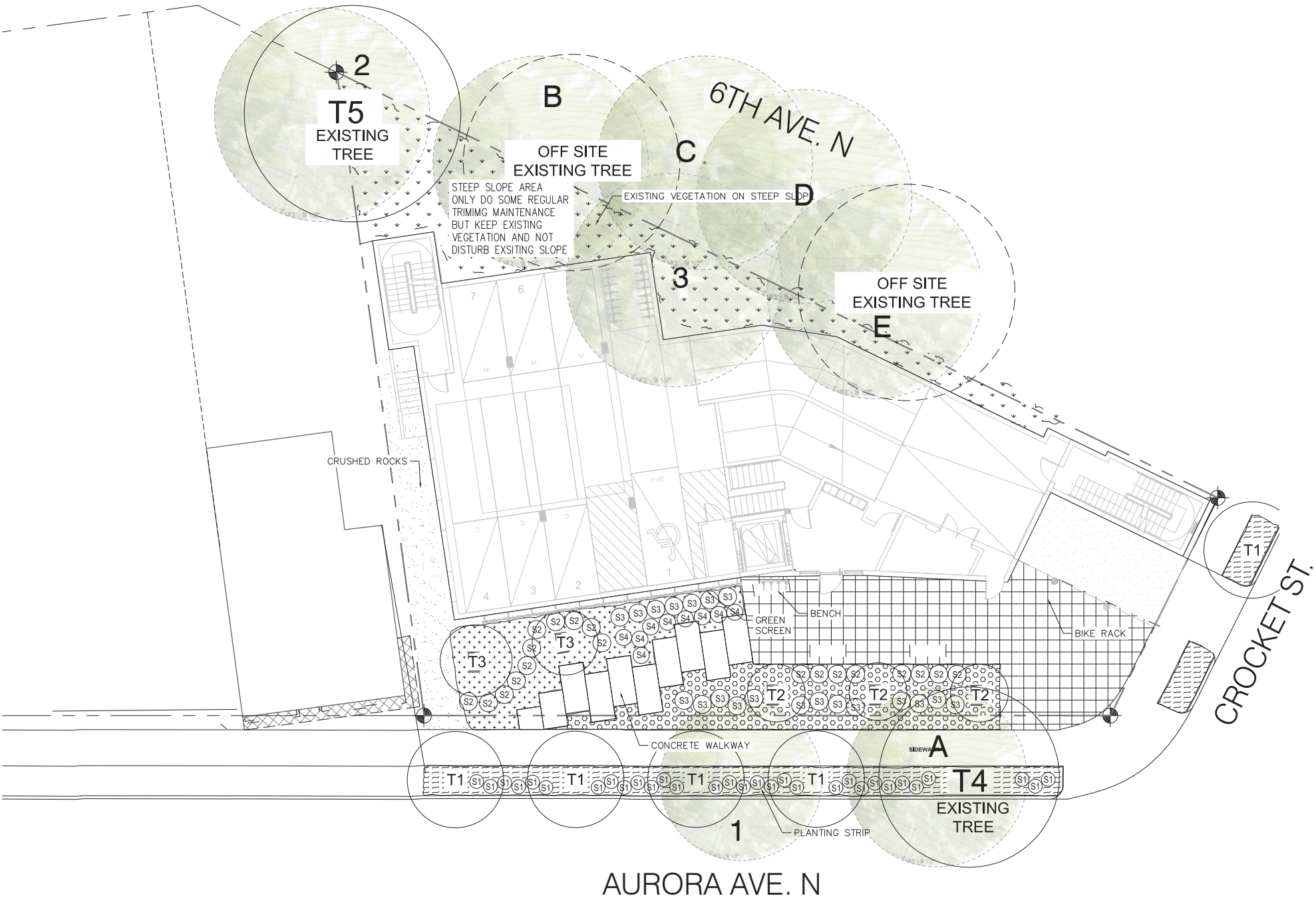
Aerial View Looking Towards Entrance Plaza

Site Plan Conditions

The site is located between Aurora Ave and non-constructed 6th Ave N. running lengthwise from North to South with views partially blocked by neighboring buildings. The property starts up narrow on the North and gains more depth towards South. The site features a steep slope towards the west property line. The views around the site are limited. To the east the views are limited by the vegetation and sloped terrain. The apartment to the east with its mainly blank wall is the most limiting and results only in partial views of lake union. Upper floor views to the Northeast, East and Southeast are mostly open.

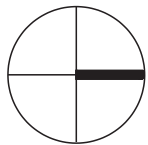
Units are oriented to the East, North, and West towards the most open views. The stairs are to the South and North towards the corners against the steep terrain. The levels that cut into the terrain incorporate the 2-level garage for the occupants.

Notes: Per Arborist's report:
Numbered trees are on-site; lettered tree are off-site.
Trees 1, 3, C, &D to be removed with city permission.
Trees 2, A, B, &E to be retained.

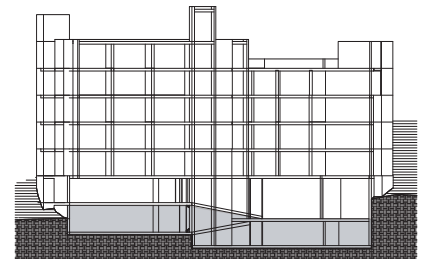
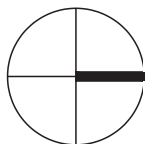
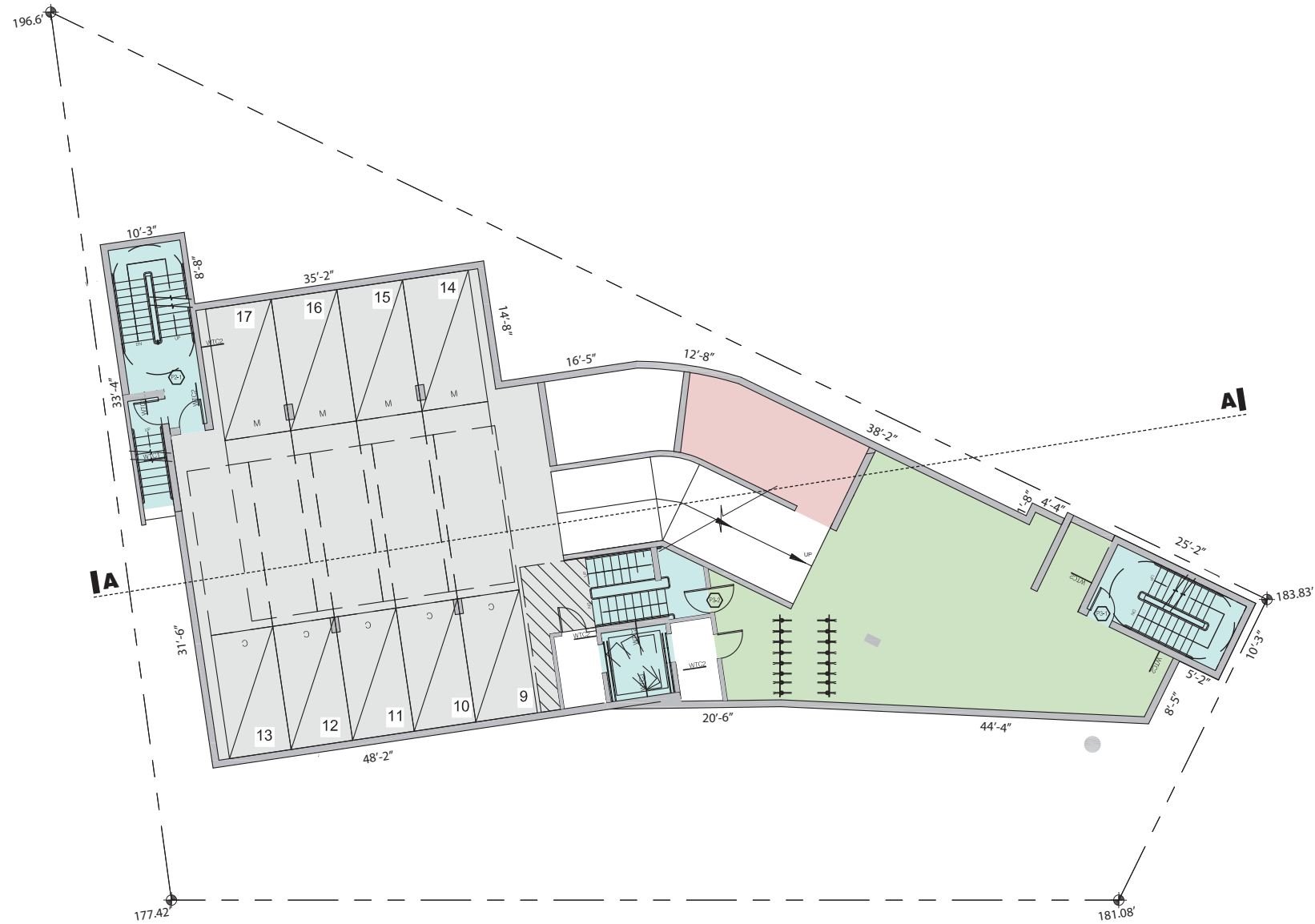


Plan: Site

Scale: N.T.S

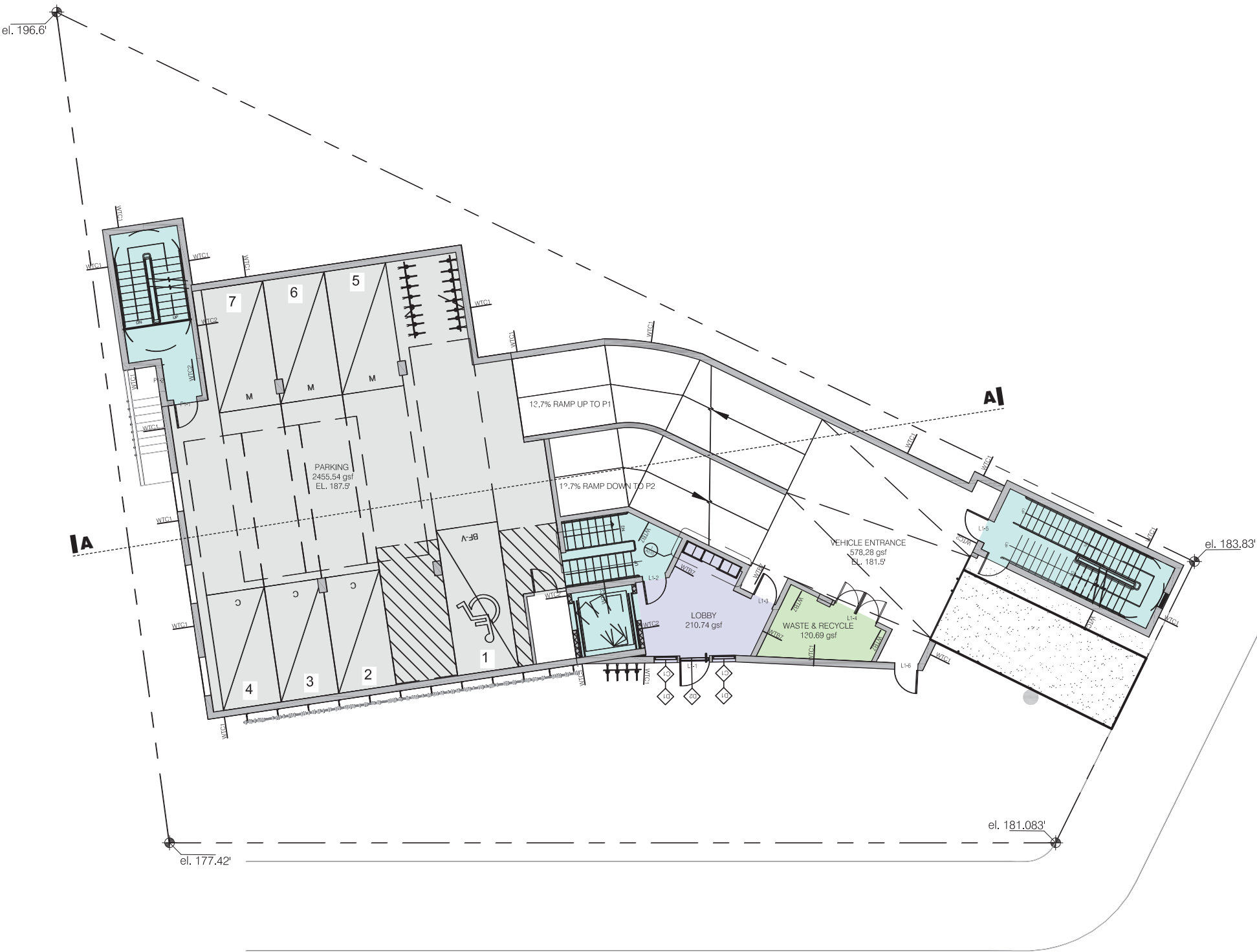


Corridor/Deck



KEY: Program

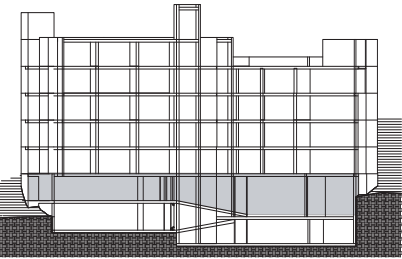
- Stairs/Elevator
- Storage
- Units
- Services
- Entry/Lobby
- Parking
- Corridor/Deck



Plan: LEVEL L01/P1

1/16" = 1'-0"

0 4' 8' 16'

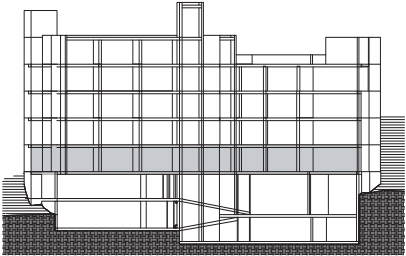
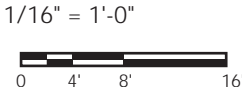


KEY: Program

- Stairs/Elevator
- Storage
- Units
- Services
- Entry/Lobby
- Parking
- Corridor/Deck

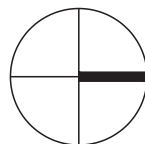
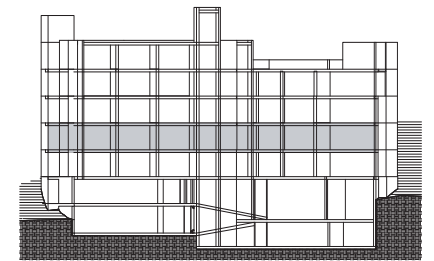


Plan: LEVEL L02





Corridor/Deck

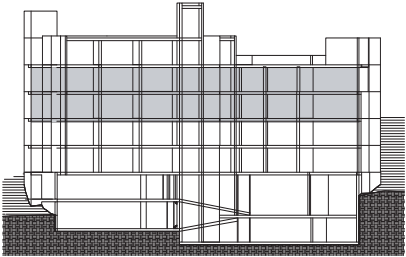
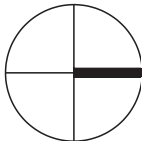
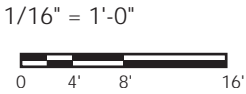


KEY: Program

- Stairs/Elevator
- Storage
- Units
- Services
- Entry/Lobby
- Parking
- Corridor/Deck

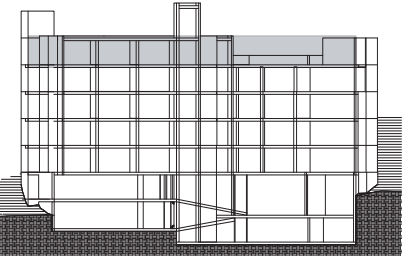


Plan: LEVEL L04/05



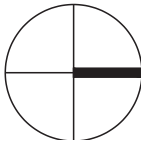
KEY: Program

- Stairs/Elevator
- Storage
- Units
- Services
- Entry/Lobby
- Parking
- Corridor/Deck

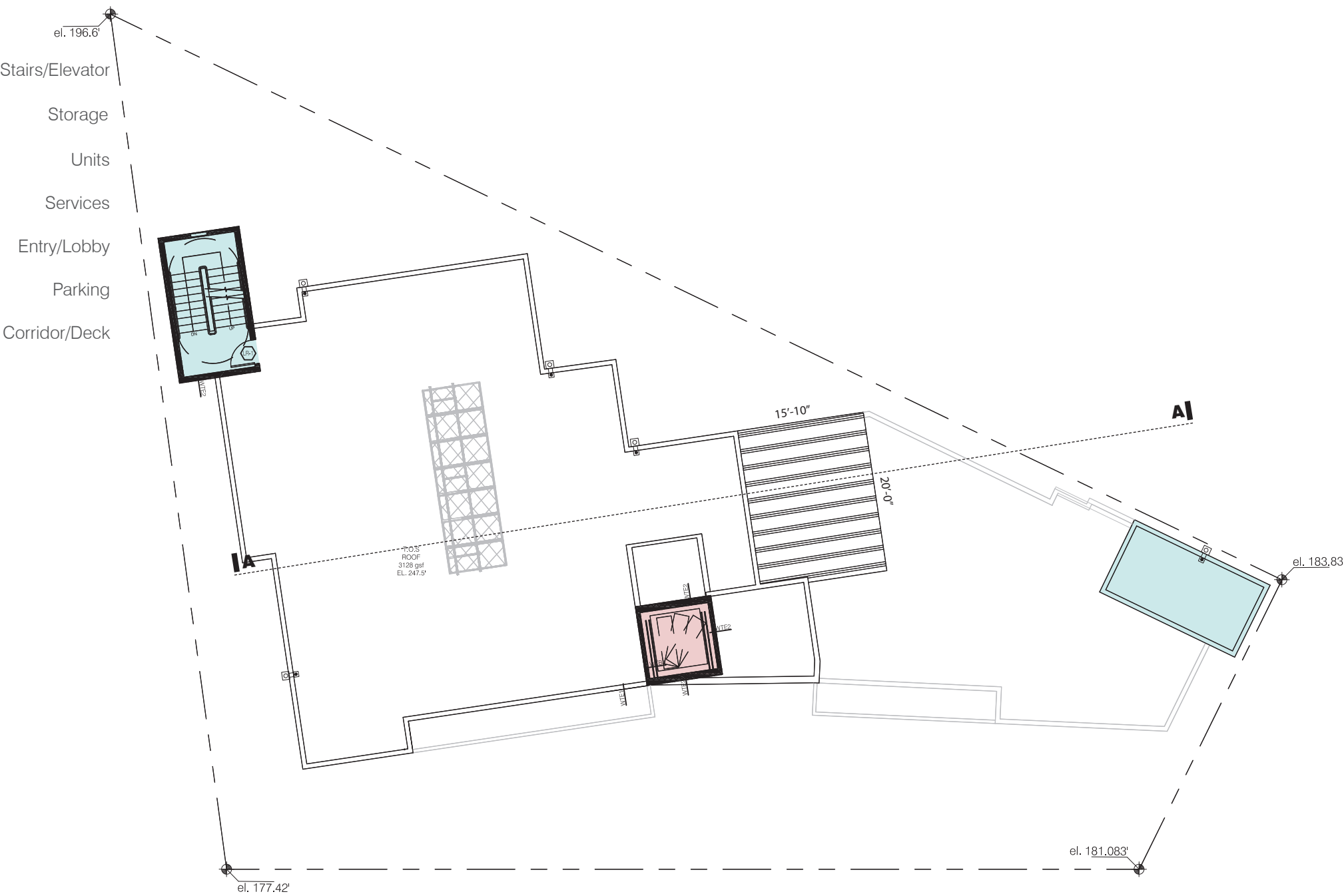


Plan: LEVEL L06

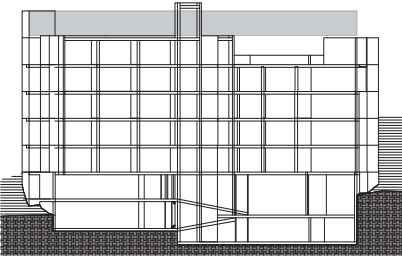
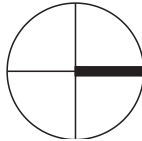
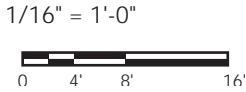
1/16" = 1'-0"



KEY: Program

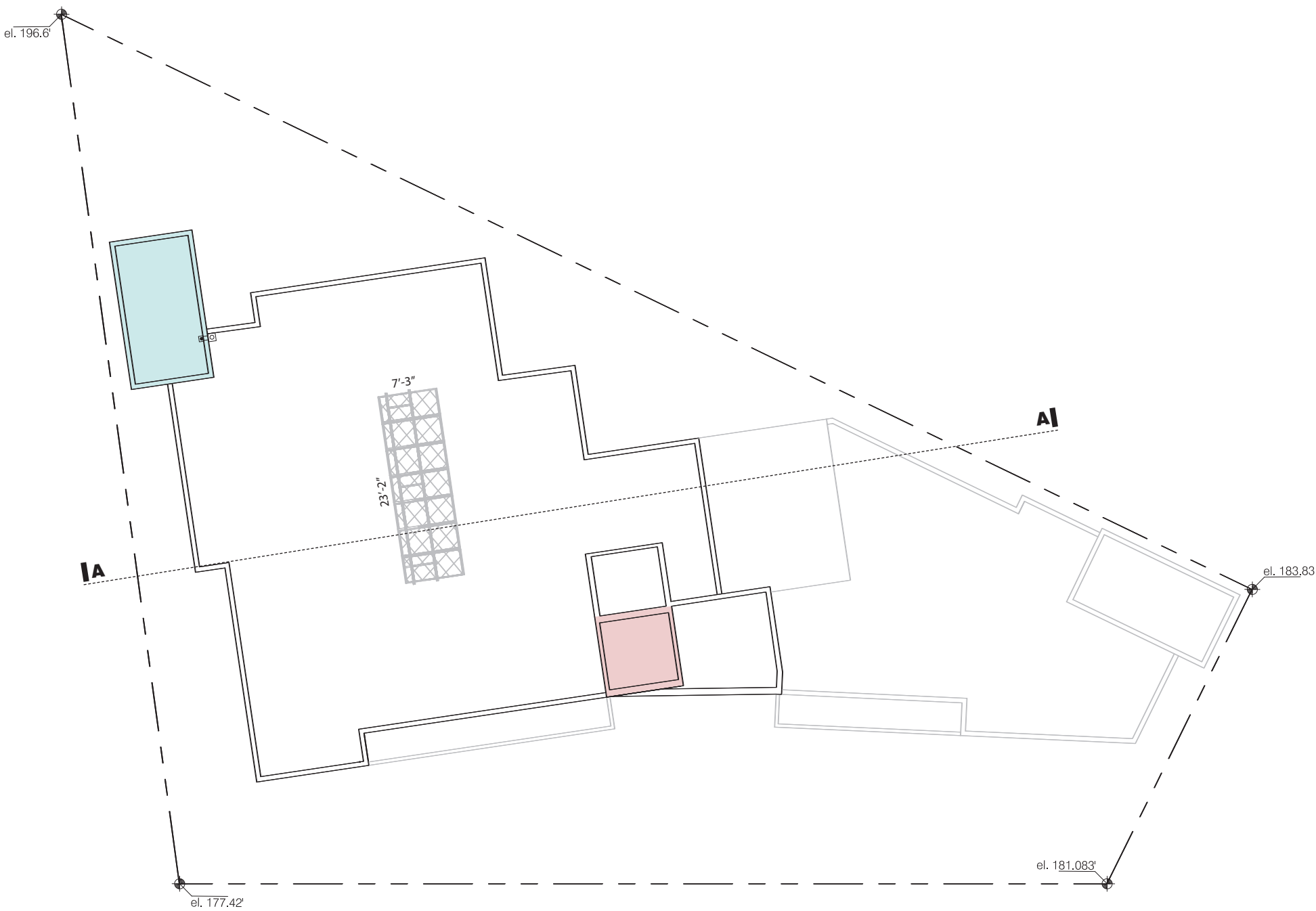


Plan: ROOF

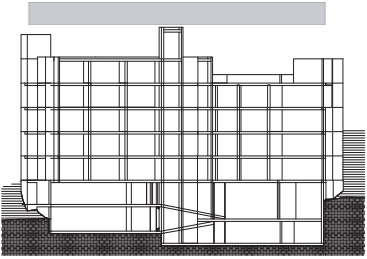
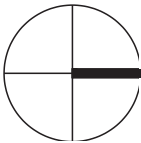
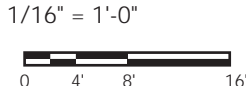


KEY: Program

- Stairs/Elevator
- Storage
- Units
- Services
- Entry/Lobby
- Parking
- Corridor/Deck

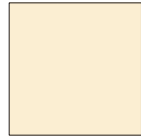


Plan: ROOF T.O.S





FCP(Y)



(03)

Fiber Cement Panel (Yellow)
SW 1668- Pineapple Yellow

FCP(G)



(07)

Fiber Cement Panel (Grey)
SW 7081- Sensuous Grey

AC



(05)

Architectural Concrete

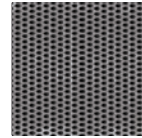
S(TB)



(04)

Steel (Tahoe Blue)

PS



(02)

Perforated Steel
Metal Sales

WS



(01)

Wood T & G Siding
FCP T & G Siding
@ Dividers

GS



(06)

FCP (HORZ. WOOD SIDING)

M1



(08)

Sheet Metal
Vents | Coping
Canopies | Awnings

RV



(09)

Vert. | Horz.
Reveals to match
the field colors

Site
261' - 6 5/8"

Allow Clerestory Ht
251' - 10"

Elev OH
250' - 10"

Allow Ht
247' - 3 1/4"

Level L6
237' - 6"

Level L5
227' - 6"

Level L4
217' - 6"

Level L3
207' - 6"

Level L2
197' - 6"

Level P1
187' - 6"

Level L1 (Lobby)
181' - 6"



Elevation: East

1/16" = 1'-0"



FCP(Y)

Fiber Cement Panel (Yellow)
SW 1668- Pineapple Yellow

FCP(G)

Fiber Cement Panel (Grey)
SW 7081- Sensuous Grey

AC

Architectural Concrete

S(TB)

Steel (Tahoe Blue)

PS

Perforated Steel
Metal Sales

WS

Wood T & G Siding
FCP T & G Siding
@ Dividers

GS

FCP (HORIZ. WOOD SIDING)

M1

Sheet Metal
Vents | Coping
Canopies | Awnings

RV

Vert. | Horz.
Reveals to match
the field colors

Materials/Colors

Facade materials and color were chosen for their module size, texture and orientational difference. They intent of both is to reinforce the facade's 'A-B-C-B-A' parti.

At the two projecting 'A' facades, the FCP siding has a large module (i.e.3' wide x 10' tall). A 'grid' is suggested. The windows and projecting perforated metal balconies add detail and scale. The color is an earthy yellow. The balconies will be painted a warm dark gray.

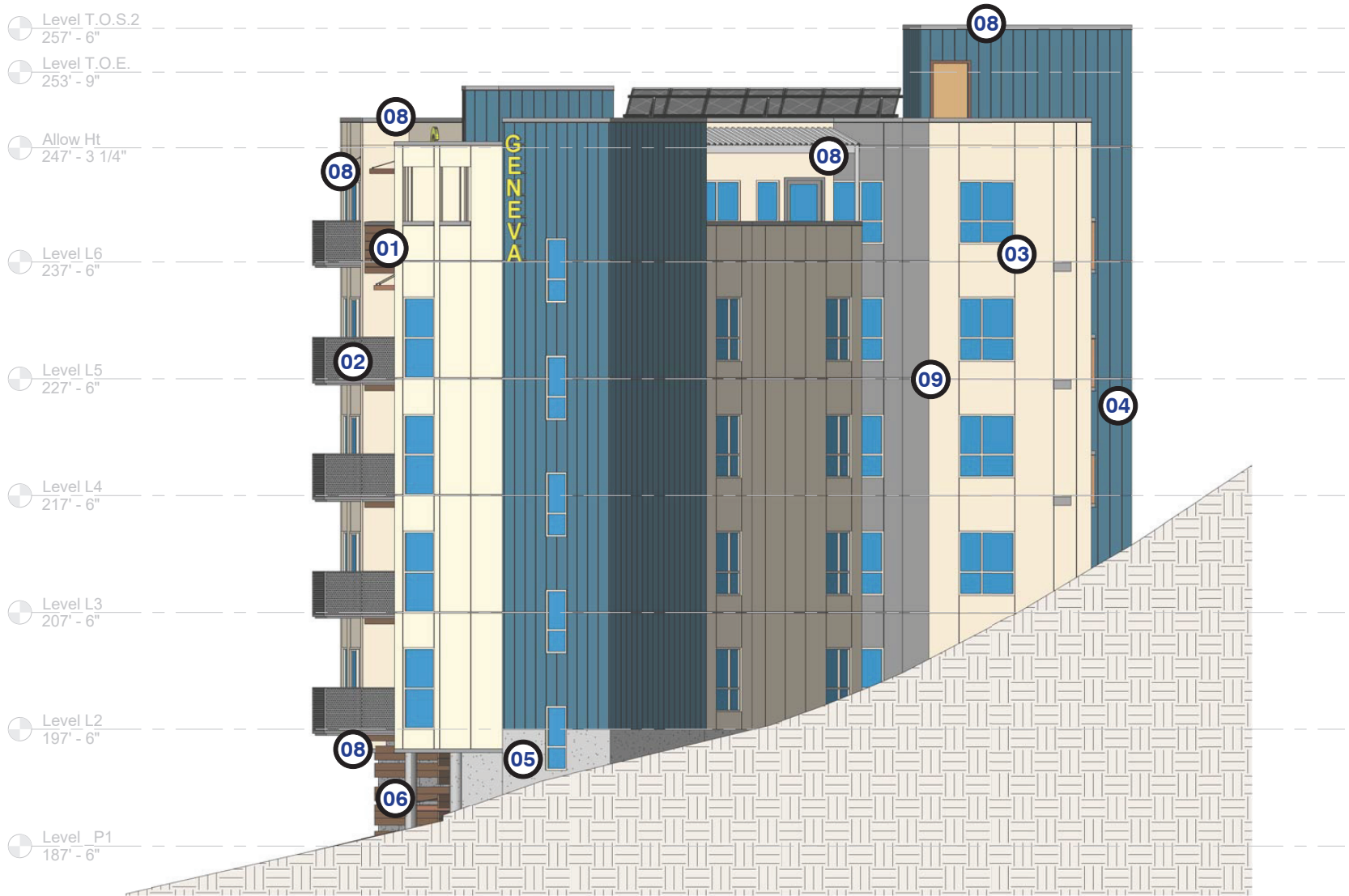
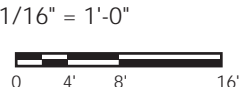
At the two inset 'B' facades, the balcony siding has a smaller scaled horizontal orientation. A linear expression is suggested. The recessed balconies will create shadows emphasizing this. The siding is painted a more natural wood color.





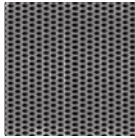




Both the 'A' and 'B' facades merge at the upper floor level linking the with a strong top element.

At the center 'C' facade, the metal siding has a smaller scaled vertical orientation. The glass at the elevator lobbies is inset into it. It extends above the adjacent 'B' facades adding emphasis to it's circulation and entry function. The siding is painted a steel blue to set it off from the more natural colors of the rest of the building.

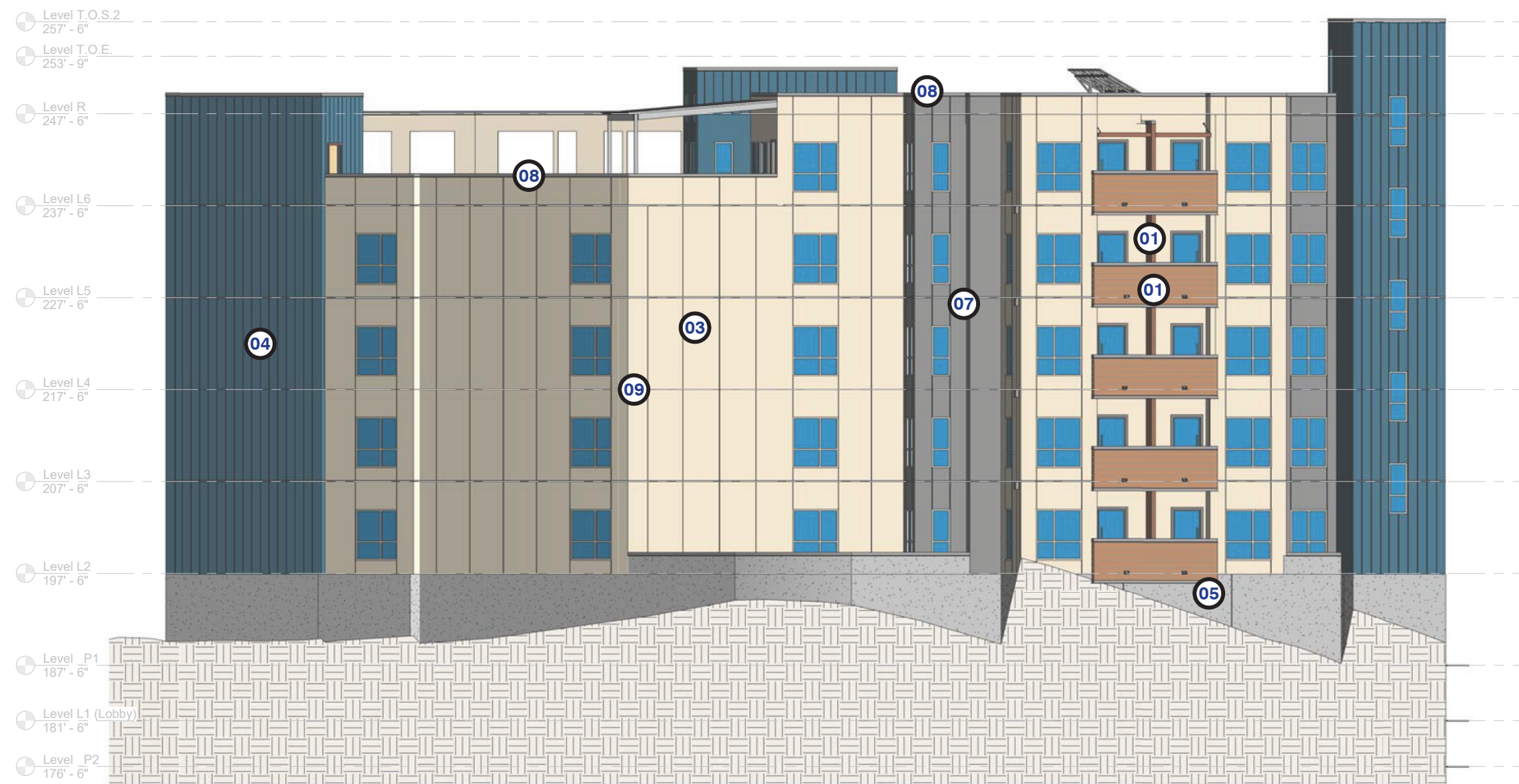
The textural differences reinforce the intended parti, also: smooth-rough-smoothest-rough-smooth.

Elevation: North



| | | | | | | | | |
|---------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------|
| FCP(Y) | FCP(G) | AC | S(TB) | PS | WS | GS | M1 | RV |
|  (03) |  (07) |  (05) |  (04) |  (02) |  (01) |  (06) |  (08) |  (09) |
| Fiber Cement Panel (Yellow) SW 1668- Pineapple Yellow | Fiber Cement Panel (Grey) SW 7081- Sensuous Grey | Architectural Concrete | Steel (Tahoe Blue) | Perforated Steel Metal Sales | Wood T & G Siding FCP T & G Siding @ Dividers | FCP (HORZ. WOOD SIDING) | Sheet Metal Vents Coping Canopies Awnings | Vert. Horz. Reveals to match the field colors |

- Concealed fasteners will be used for the fiber cemenet panels



Elevation: West

1/16" = 1'-0"



FCP(Y)

(03)

Fiber Cement Panel (Yellow)
SW 1668- Pineapple Yellow

FCP(G)

(07)

Fiber Cement Panel (Grey)
SW 7081- Sensuous Grey

AC

(05)

Architectural Concrete

S(TB)

(04)

Steel (Tahoe Blue)

PS

(02)

Perforated Steel
Metal Sales

WS

(01)

Wood T & G Siding
FCP T & G Siding
@ Dividers

GS

(06)

FCP (HORZ. WOOD SIDING)

M1

(08)

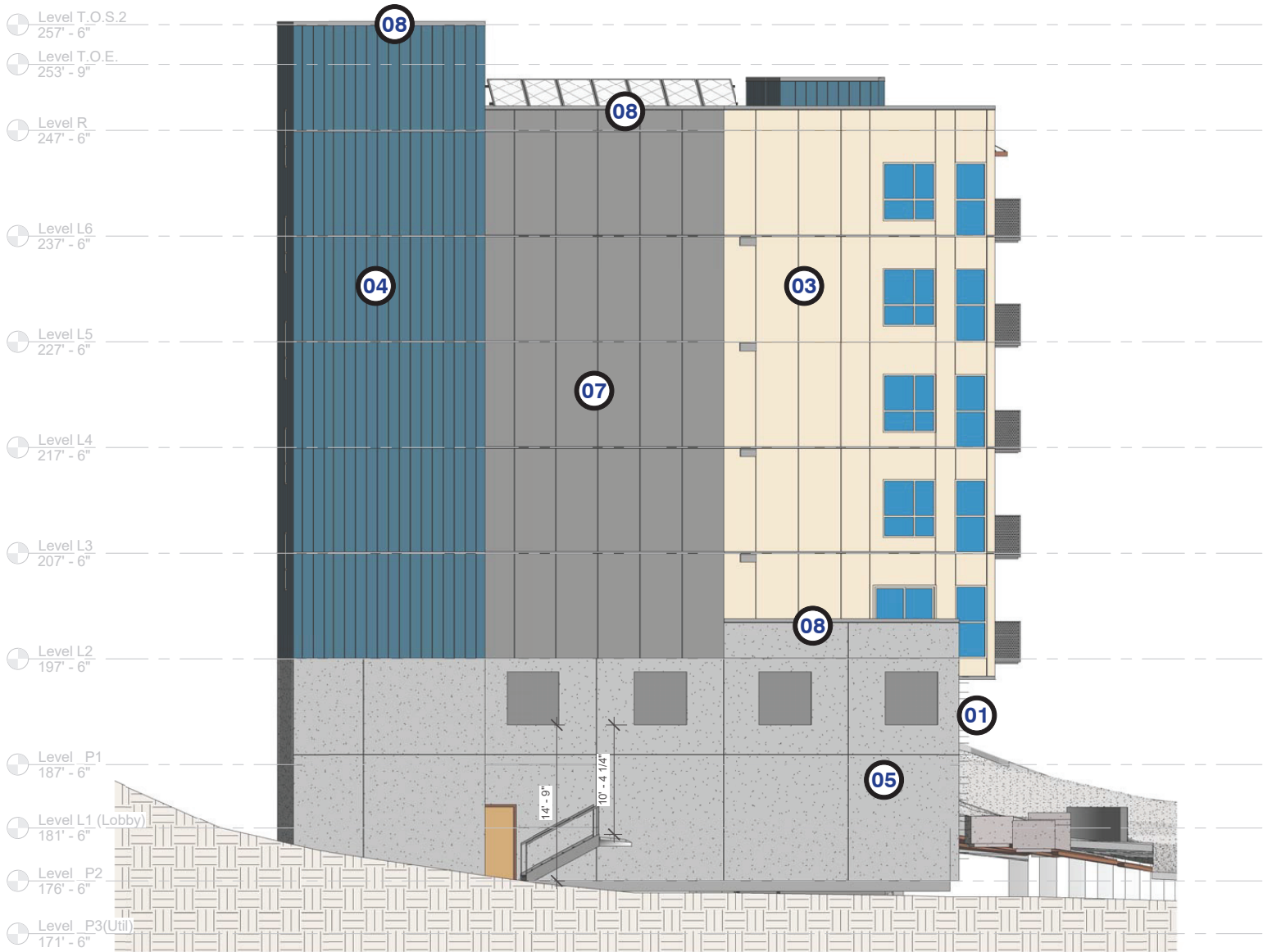
Sheet Metal
Vents | Coping
Canopies | Awnings

RV

(09)

Vert. | Horz.
Reveals to match
the field colors

- Concealed fasteners will be used for the fiber cemenet panels



Elevation: South

1/16" = 1'-0"

0

4'

8'

16'



T1: AMERICAN HORNBEAM



LIVE GREEN WALL ENTRY IMAGE



CORTEN WALL IMAGE



T2: GLOBE NORWAY MAPLE



T3: 'VICTORIA' MAGNOLIA



CORTEN PLANTER



BIKE RACK



S1: HEBE 'MCKEAN'



S2: HEAVENLY BAMBOO



S3: GOLDEN SWORD YUCCA



G1: BLUE FESCUE



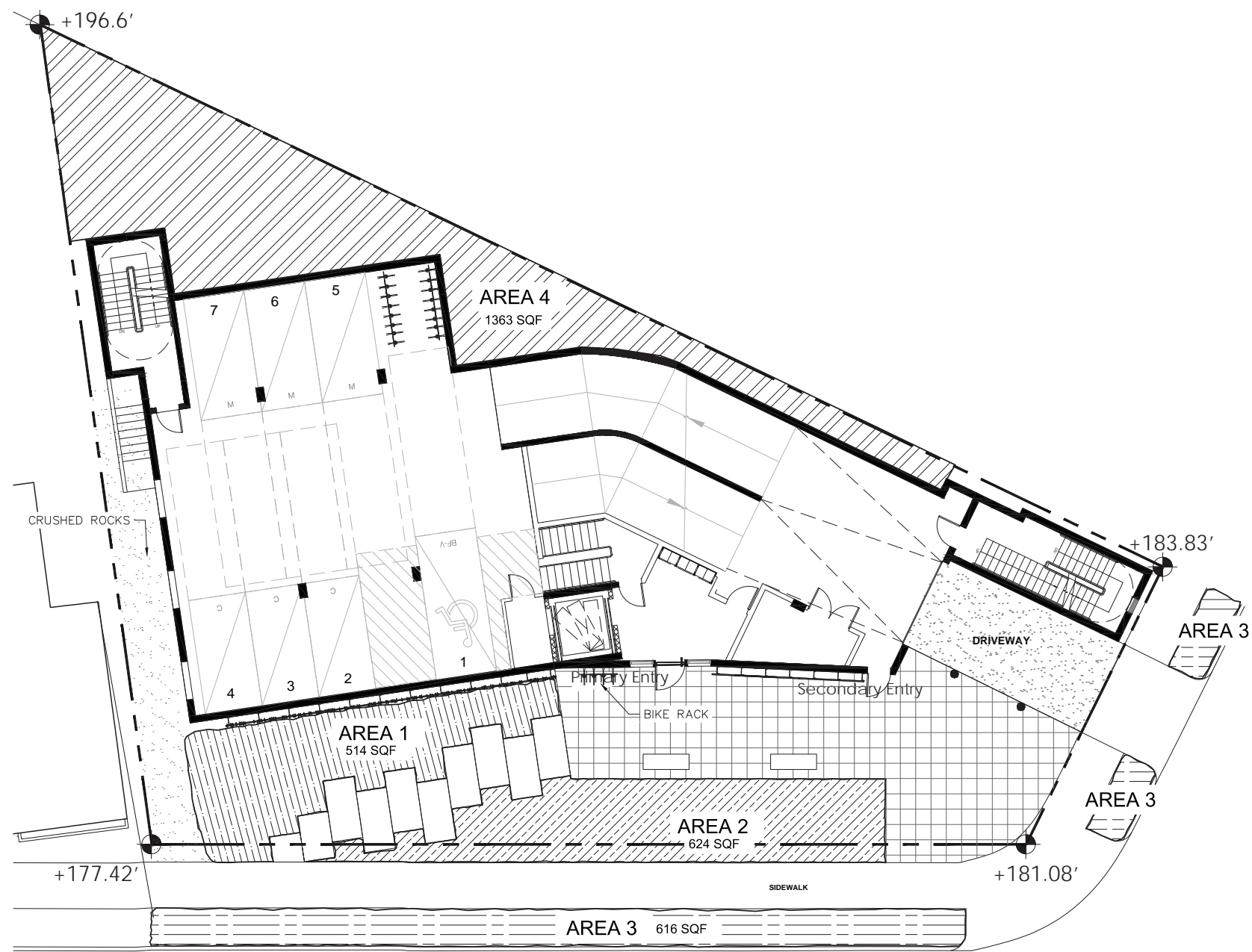
G2: ORANGE GRASS



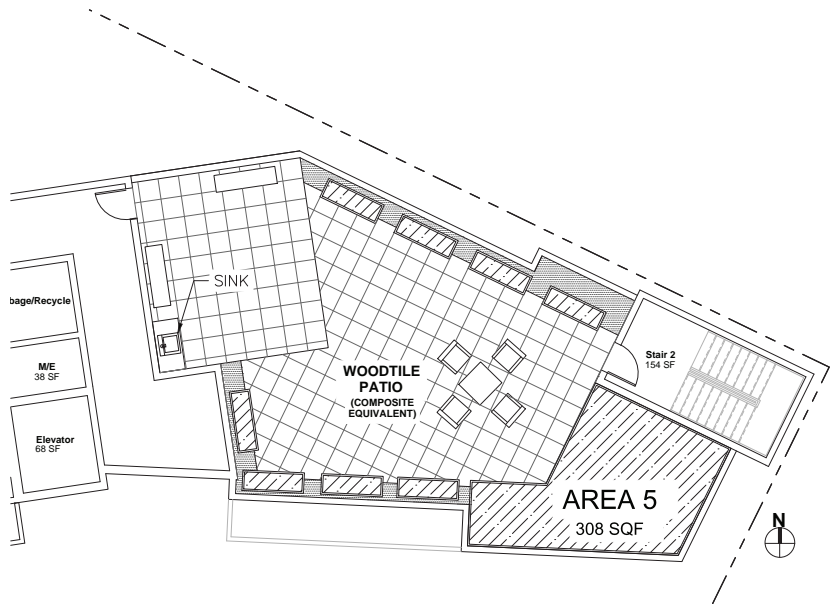
G3: MONDO GRASS



LANDSCAPE DESIGN PLAN
LOTUS LANDSCAPE DESIGN



1 LANDSCAPE PLAN
L-6 PLANTING AREA



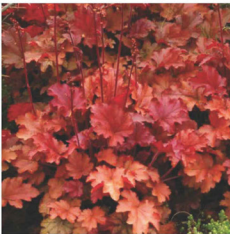
2 ROOF GARDEN
L-6 PLANTING AREA



GREEN WALL PLANTS



DWAFT HEBE



GEORGIA PEACH
CORAL BELLS



LILY TURF



SIERGRAS
BRIZA CAREX
CALAMAGROSTIS



CLEMATIS



DELTA DOWN
CORAL BELLS



GREEN WALL DETAIL
LOTUS LANDSCAPE DESIGN



DECK GREEN WALL



DECK GREEN WALL DETAIL

- PLANTER BOXES WITH ALUMINUM COPING.
- TENANTS CAN CHANGE OR VARY THE PLANTS





From Aurora Ave. Looking North



Entry View



Looking Towards Garage Entry and Entrance



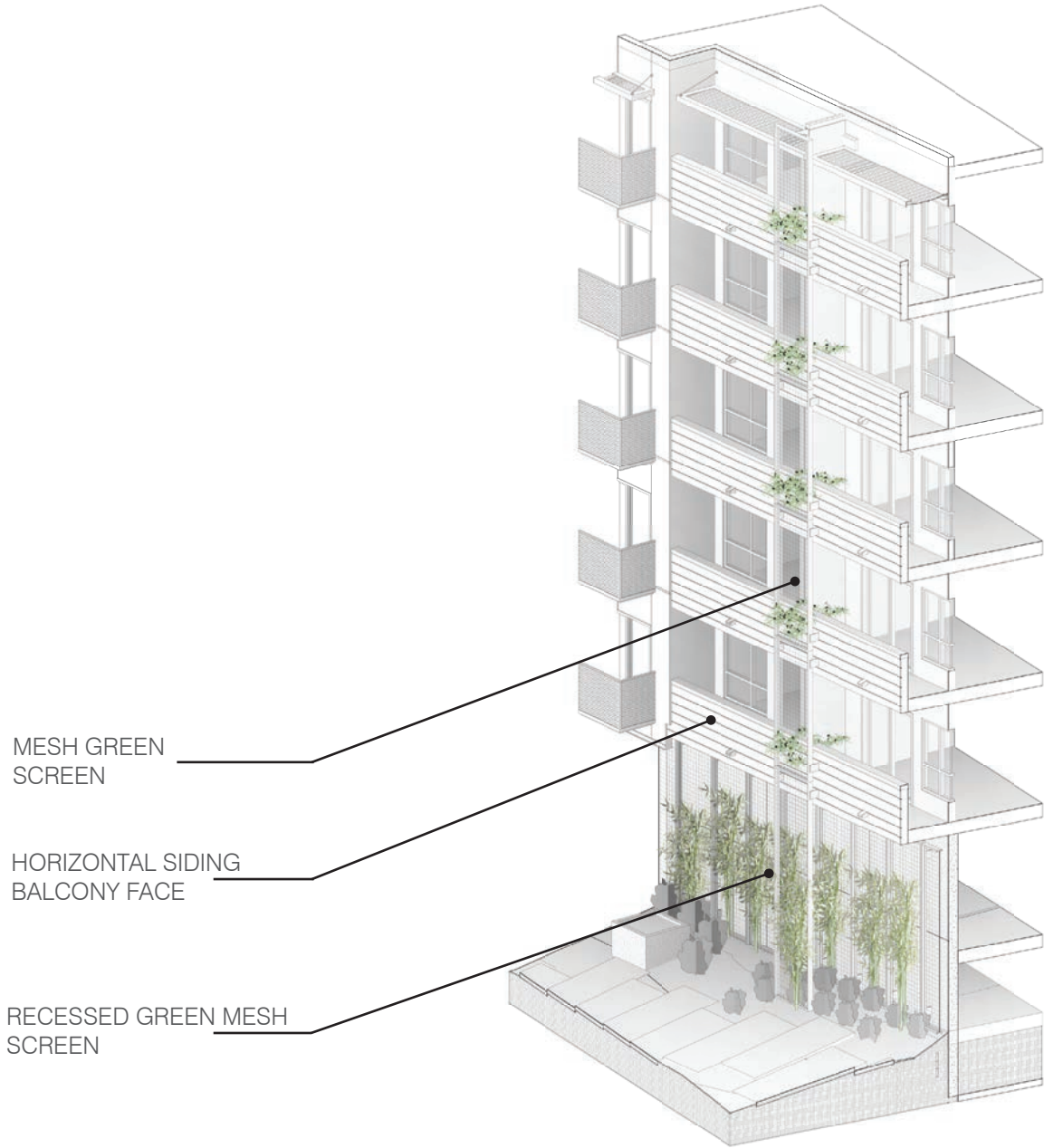
Aerial view From Aurora Ave. Looking South



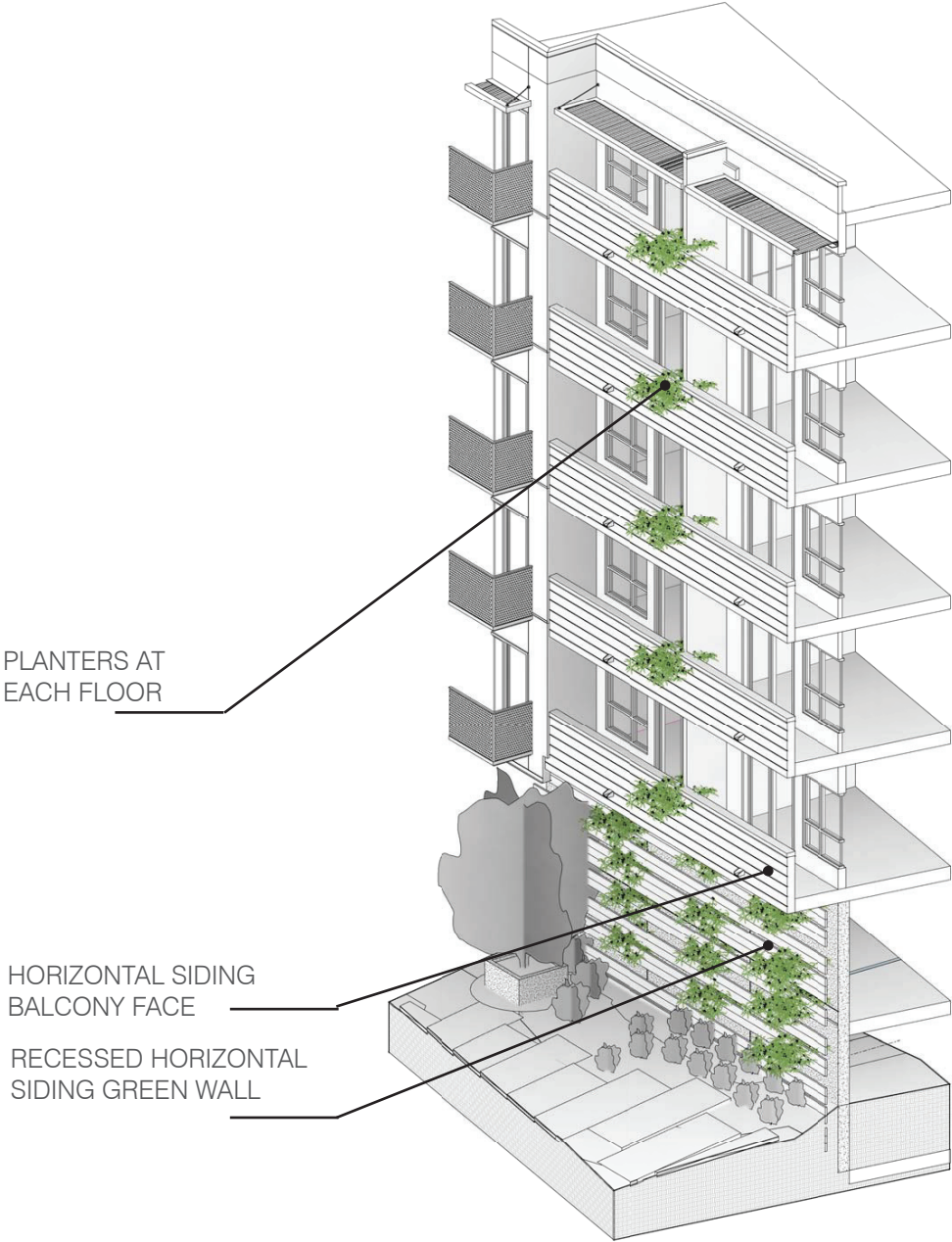
Looking Towards the building entrance



Aerial View Looking Towards Entrance Plaza

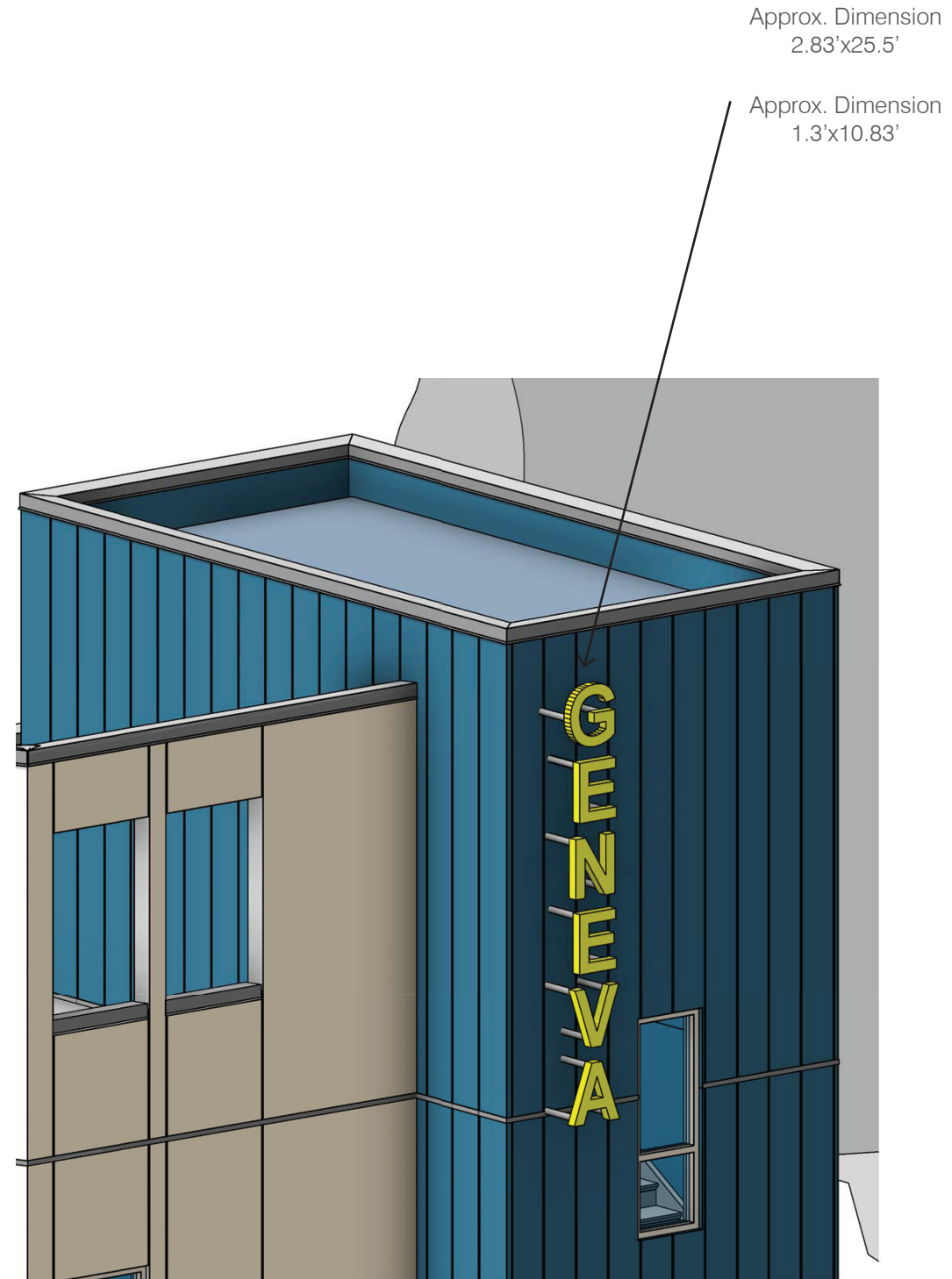


Alternate 2 (Preferred)

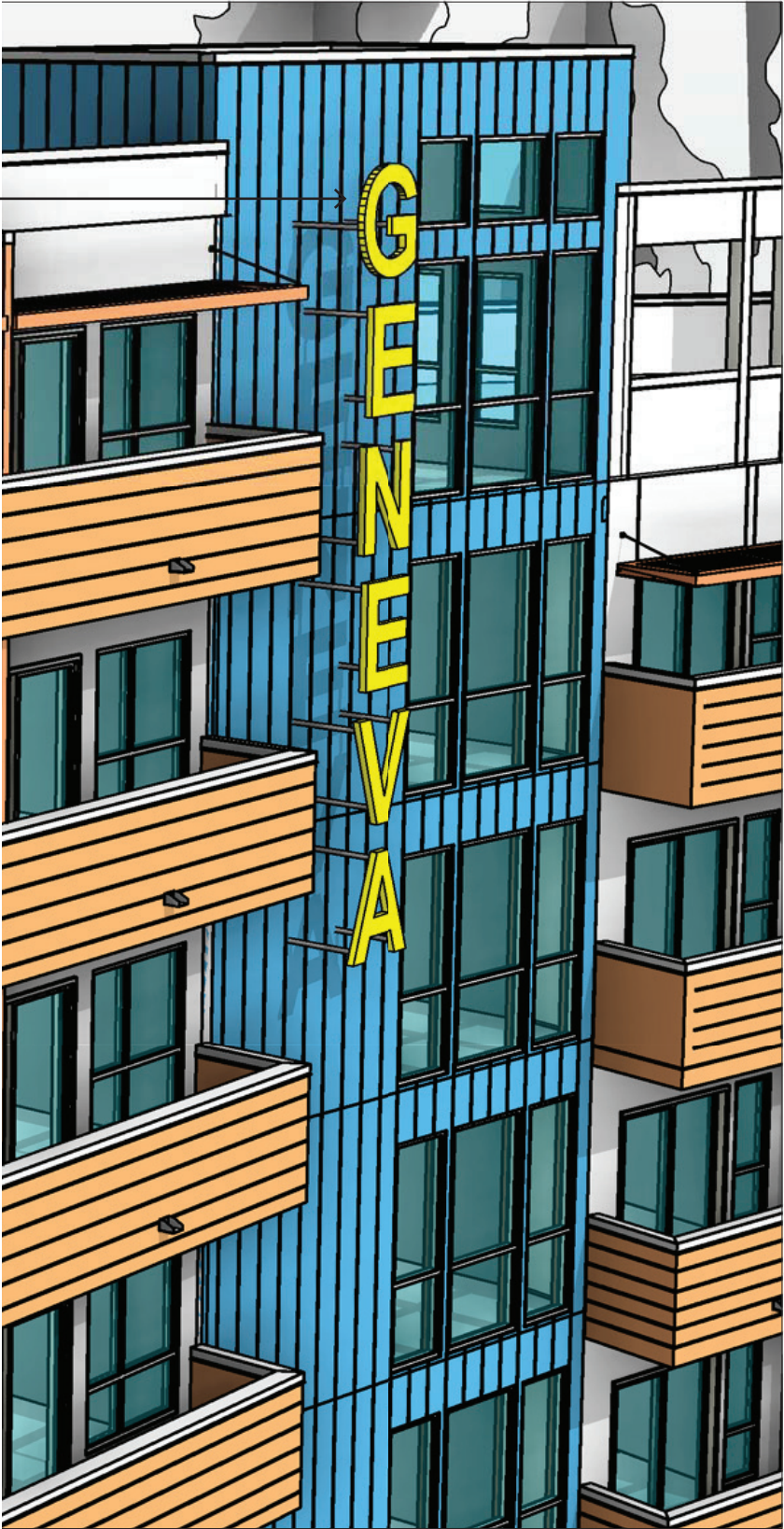


Alternate

The primary building signage will be located as a projecting sign along Aurora Ave N. and the secondary signage will be a projecting sign on the stair tower.



Secondary Building Sign



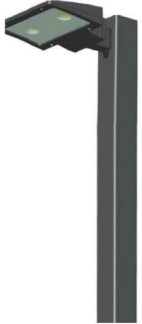
Primary Building Sign



Inspirations



LIVE GREEN WALL LIGHTING



POLE LIGHTING



GARDEN LIGHTING

Walkway Lighting:

It will shine on the walkway. There is no light spillage.

Tree Spot Lighting:

It will shine to the tree in an upward direction. There is no light spillage to the surrounding.

Garden Lighting:

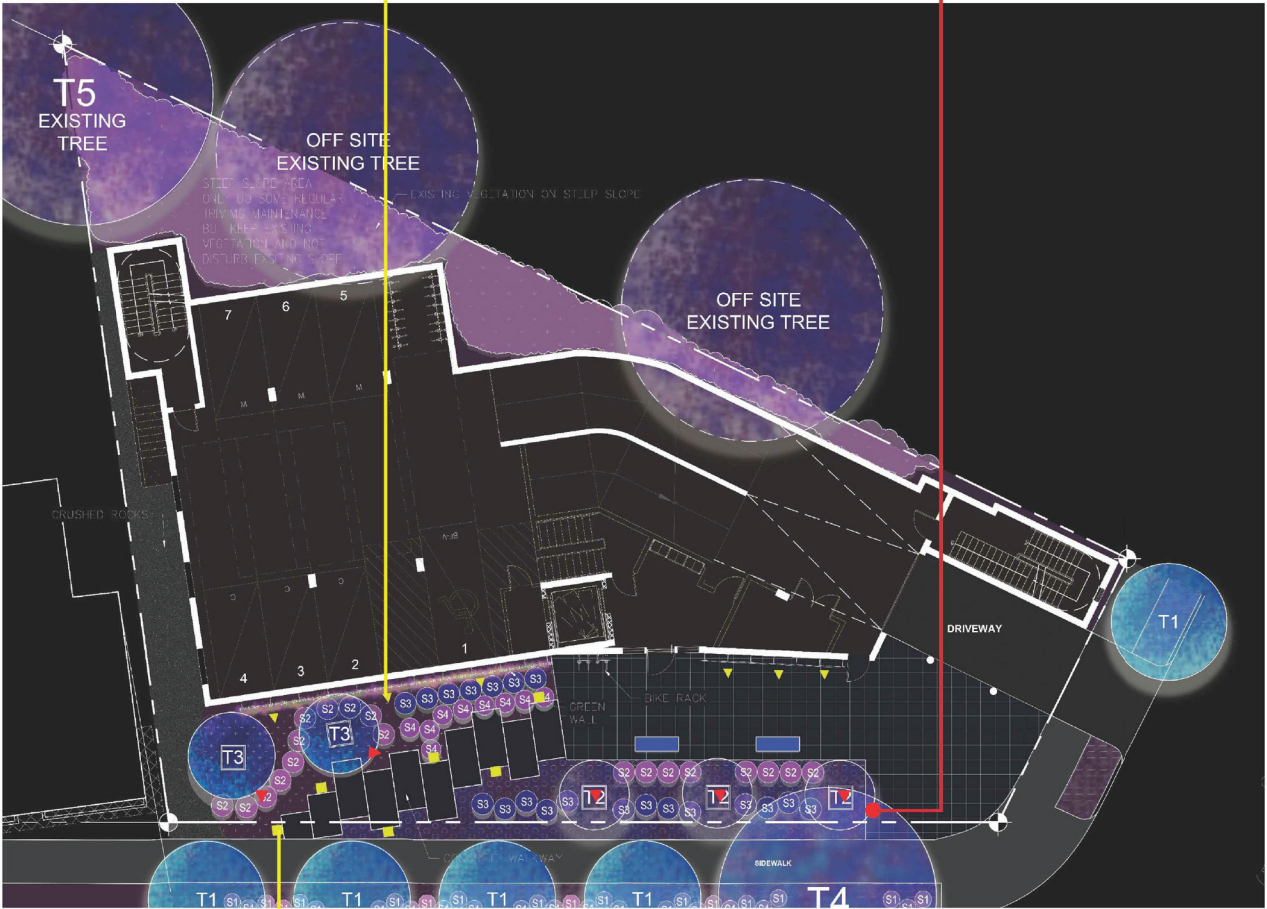
It will shine down to the garden and they have a smaller height. There is no light spillage to the surrounding

Pole Lighting:

It will shine down on the sidewalk and walkway. There is no spillage to the surrounding.

Live Green Wall Lighting:

It will shine up to the green wall in an upward direction. There is no spillage to the surrounding.



LIGHTING PLAN
LOTUS LANDSCAPE DESIGN



WALKWAY LIGHTING



TREE SPOT LIGHTING

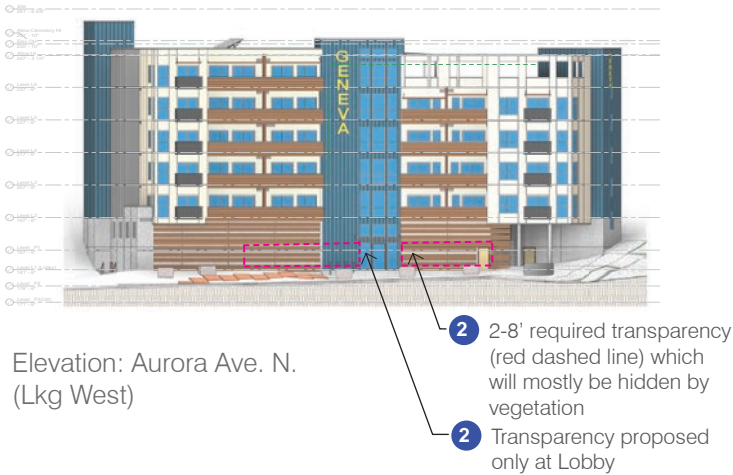
DRB direction|discussion August 31, 2016

DEPARTURE #1

Front Setback (SMC 23.47A.008.A.3): The Code requires street Level facades within 10' of street lot line. The applicant proposes a setback between 15'-6" to 21'-2" of the streetlot line. The Board indicated preliminary support of the request provided that pedestrian lighting, bicycle racks, pedestrian amenities are incorporated. The Board recognized that the departure has the potential to create enhanced open space and a better transition from Aurora Ave. The resulting design has the potential to better meet Design Guidelines CS2- B-2 Connection to the Street, PL1-A-1 Enhancing Open Space and DC3-A Building Open Space Relationship.

DEPARTURE #2

Street Level Transparency (SMC 23.47A.008.B.2): The Code requires 60% of the street facing facade between 2 feet and 8 feet above the sidewalk to be transparent. The applicant proposes less than 60%. The Board indicated preliminary support of the departure as long as the design of the green screen/wall is further developed as a special architectural feature. The resulting design has the potential to create a sense of place and better meet Design Guidelines DC2-A, Architectural Concept Massing, DC2-D-2 Texture and DC3-A Building Open Space Relationship.



PROPOSAL

JUSTIFICATION

- 1 Front Setback. Allow greater then 10' setback from Aurora Ave N. property line
- 2 Transparency. Only transparency on Aurora Ave. facade is at building residential lobby entrance.

- 1 Building is setback to 15'-6" to 21'-2" which sets it back the required distance from a city drain line and easement; allows greater separation from Aurora for units and for parking garage entrance which will provide the opportunity to create enhanced open space and a public area more welcoming to pedestrians and a better transition from Aurora Ave to building entrance; enhances both pedestrian an vehicular safety (Refer to guidelines CS2, PL1, PL2 & PL-3)
- 2 Transparency adds no benefit to the facade which is setback from the street with extensive landscaping in front of it. It would be a distraction to vehicles moving south at 40-50 mph. Commercial uses at this location are virtually inaccessible and would not add to the architectural context and character of the neighborhood, interaction, or connectivity. (Refer to Guidelines CS3, PL1,& PL3)

