

# Washington State Convention Center Addition Project: Co-Development

Design Review Board Recommendation Meeting 01  
05-17-2016

|                  | SITE B        | SITE C          |
|------------------|---------------|-----------------|
| PROPERTY ADDRESS | 920 Olive Way | 1711 Boren Ave. |
| SDCI PROJECT #   | 3018096       | 3020177         |

| OWNER  | ARCHITECT   | SDCI CONTACT   |
|--|---|--|
| Washington State<br>Convention Center<br>800 Convention Place<br>Seattle, WA 98101 | LMN Architects<br>801 Second Ave.<br>Suite 501<br>Seattle, WA 98104 | Garry Papers<br>206-684-0916<br>garry.papers@seattle.gov |







WSSC EXISTING SITE

WSSC ADDITION PROJECT

SITE B

SITE C



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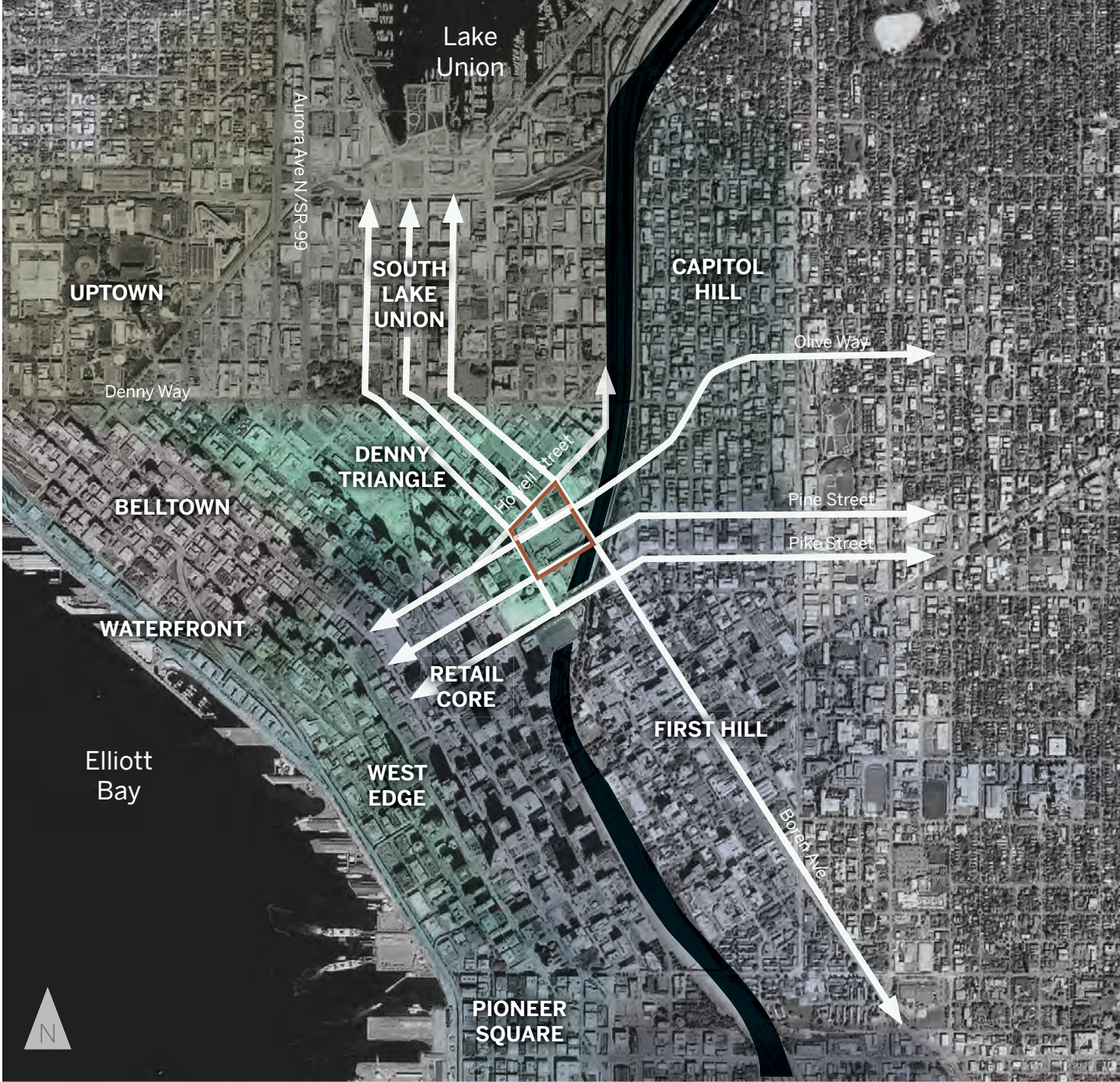
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1 DEVELOPMENT OBJECTIVES

PROJECT BACKGROUND

VICINITY



STREETS BORDERING PROJECT SITE

9-BLOCK STUDY AREA



- 9-BLOCK STUDY AREA
- WSCC ADDITION PROPERTIES
- FULL STREET / ALLEY VACATION
- AT GRADE SITE AREA INCLUDING VACATIONS



# 1 DEVELOPMENT OBJECTIVES    PROJECT BACKGROUND

## DEVELOPMENT SUMMARY

The proposal is to apply for Master Use Permits for development of a convention center addition on a site consisting of 3 lots: Site A: 1600 9th Avenue, **SITE B:** 920 Olive Way, and **SITE C** 1711 Boren Avenue, that will collectively form the proposed Washington State Convention Center (WSCC) Addition Project. The 3 block site is bounded by Howell Street to the north, Pine Street to the south, 9th Avenue to the west, and Boren Avenue and I-5 to the east. Terry Avenue and Olive Way divide the site on the interior. Below grade street and full alley vacations will be required for this project.

The project will add approximately 1,230,000 sf. of gross floor area to the existing Washington State Convention Center. This will include approximately 250,000 sf. of new exhibition space, 120,000 sf. of meeting rooms, a 60,000 sf. Ballroom space, approximately 20 convention center loading bays, and structured parking for up to 800 additional vehicles. The structure height is approximately 150-200ft over 11 stories with 2 additional stories below grade. **The WSCC Addition project (Site A) was reviewed by the DRB on May 3, 2016.**

Co-development on two blocks north of Olive Way (Site B and Site C) is proposed as part of the WSCC Addition project through the use of a potential Planned Community Development (PCD; a City of Seattle Provision: SMC 23.49.036).

**SITE B** is planned for a 290 ft., 409 unit residential tower with 65ft. podium.

**SITE C** is planned for a 240 ft. commercial building with approximately 500,000 sf of office use. Site C will also include the loading entrance and ramp for the WSCC Addition to access the convention center loading bays below Site B and Site C.

## PLANNED COMMUNITY DEVELOPMENT

A public meeting was held on September 2, 2015 providing an overview of the Planned Community Development process and soliciting public comment on the public's priorities for the list of potential public benefit priorities. The Director of the Department of Planning and Development in turn issued a list of priorities from the list of PCD items outlined in the Land Use Code.

## SITE CONTEXT

The project site is located within the DMC 340/290-400 Downtown Mixed Commercial zone, within the Denny Triangle Urban Center Village. The Downtown Neighborhood Guidelines will apply to this project.

The project site is bordered by the DMC 240/290-400 zone (Denny Triangle Urban Center Village Overlay) to the north, east, and south, and the DOC 2 500/300-500 zone (Commercial Core Urban Center Village Overlay) to the west.

The project occupies the intersection between several distinct and rapidly evolving neighborhoods, including Capitol Hill's Pike/Pine corridor, the Denny Triangle, South Lake Union, First Hill, and the Downtown commercial core.

Capitol Hill's traditional low-rise commercial development is being supplemented with new mid-rise mixed-use buildings. The neighborhood continues to promote a strong pedestrian community, interrupted only by the presence of I-5. The Denny Triangle and First Hill, connected via Boren Avenue, bookend the site to the north and south. Though a product of different eras, both neighborhoods contain higher density, taller residential and commercial development, along with notable institutional buildings. The Downtown neighborhood is the densest and tallest adjacent neighborhood, containing both high-rise commercial and residential development, but also a retail and cultural center for the city.

The site's proximity to Pike and Pine links itself to the waterfront via Pike Place Market and Westlake Center, and to the existing Washington State Convention Center along Ninth Avenue. Other notable landmarks include the historic Paramount Theatre and Worldmark-Camlin Seattle, adjacent to the site across Pine Street and Ninth Avenue. Due to the open space established by the presence of I-5, views to and from the project site to the south and east are both substantial and long-term. Views to the west, particularly from the higher elevations along Pine Street, provide a meaningful glimpse into the heart of the city.

## PROCESS OVERVIEW

This Design Recommendation (DR) #1 meeting before the Downtown Design Review Board, builds on the EDG #1 held on 05/19/2015, which focused on the site context and urban design, EDG #2 held on 07/21/2015 which focused on building massing and street level designs, and EDG #3 held on 10/06/2015, during which the project received Early Design Guidance approval. The Design Proposal booklet and SDCI report from those meetings are available to view at the following link:

<http://www.web6.seattle.gov/dpd/edms/>

Search Past Reviews by typing in the SDCI project number: 3020176 (Site A), 3018096 (Site B), 3020177 (Site C).

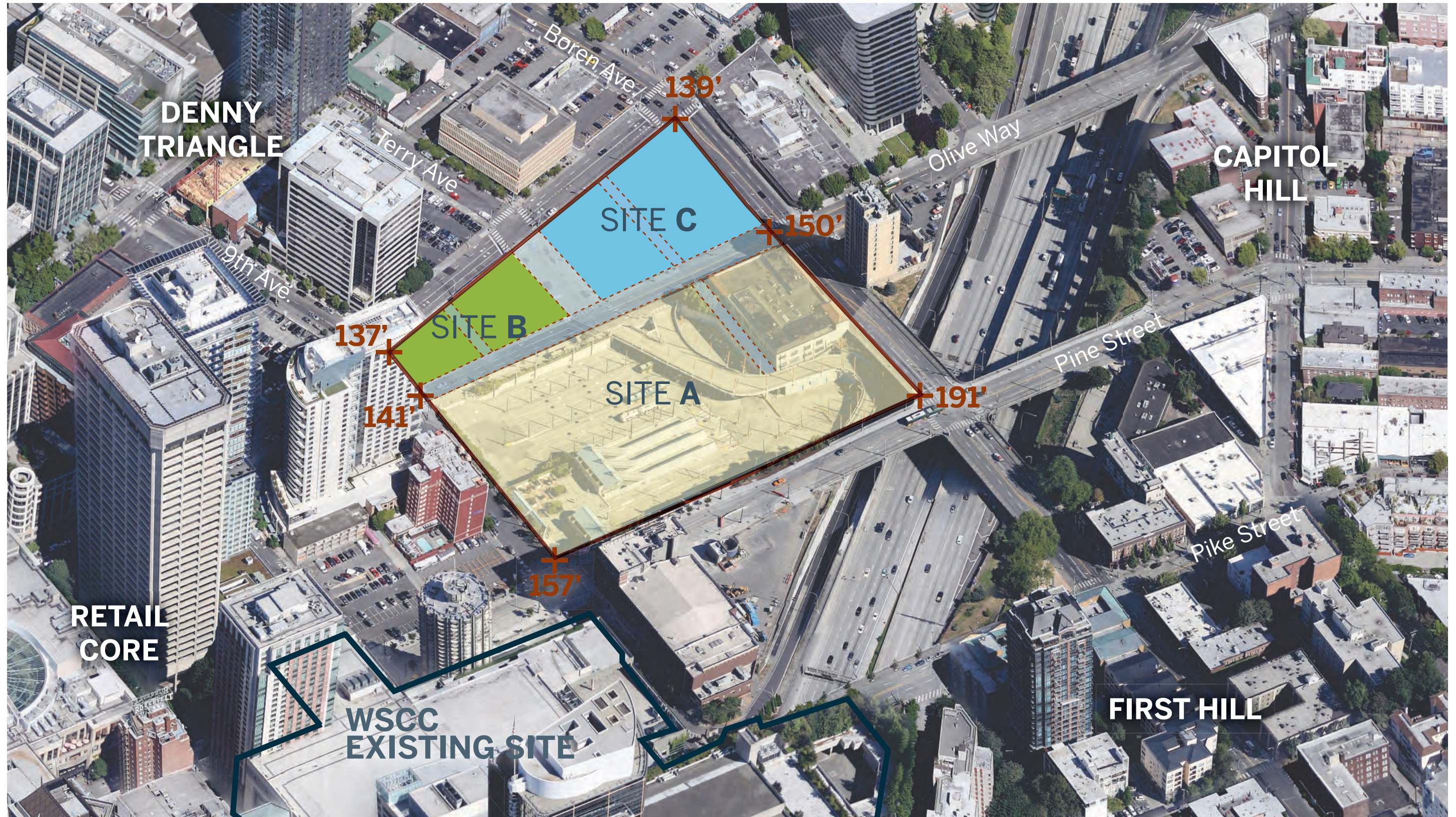
**NOTE:** ALL DESIGN SHOWN FOR STREET ROW OUTSIDE OF PROPERTY LINES ARE PRELIMINARY AND PROVIDED FOR DRB CONTEXT. ALL THOSE ELEMENTS WILL BE SUBJECT TO SUBSEQUENT SDOT AND DESIGN COMMISSION REVIEW.

THIS BOOKLET IS FOR SITE B & C ONLY.  
SITE A WAS REVIEWED ON 5/03/16.



# 1 DEVELOPMENT OBJECTIVES PROJECT BACKGROUND

## WSCC URBAN CONTEXT





1 DEVELOPMENT OBJECTIVES

PROJECT BACKGROUND





# 1 DEVELOPMENT OBJECTIVES PROJECT BACKGROUND

**NOTE** Map + building reference images show a sample from EDG #1 book. Refer to EDG #1 book for remain buildings numbered, but not shown here.



**906 NINTH AVE.**  
Convention Station Place transit facility



**1017 OLIVE WAY**  
Honda of Seattle (Vacant)



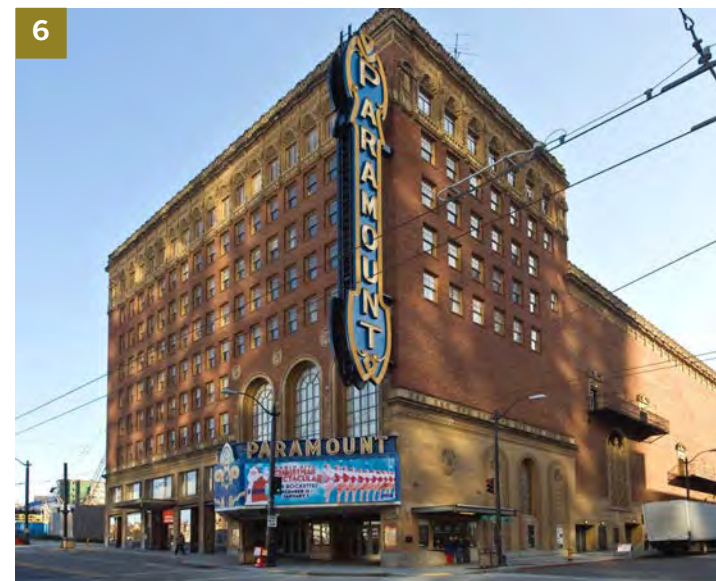
**915 HOWELL STREET**  
Retail/Commercial



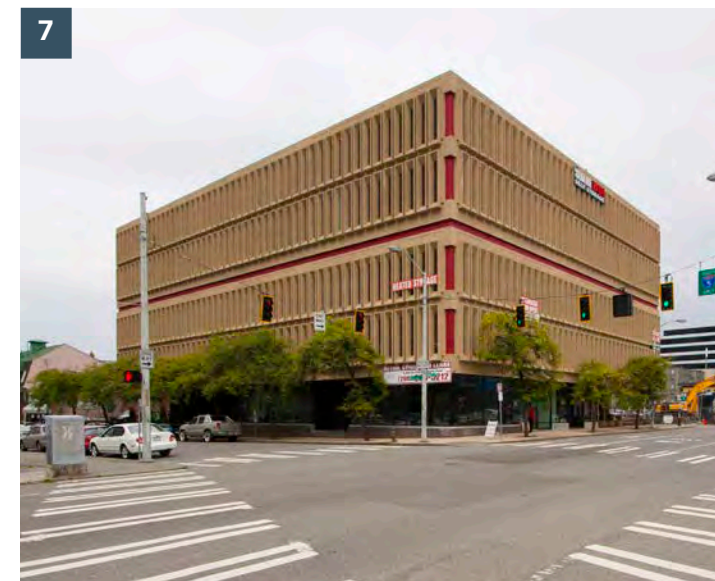
**1711 BOREN AVE.**  
Honda of Seattle auto sales (Vacant)



**1619 9TH AVE.**  
Worldmark Seattle: The Camlin



**911 PINE STREET**  
Paramount Theatre



**737 OLIVE WAY**  
Seattle Vault Self-Storage



**1100 OLIVE WAY**  
Seattle Children's Cancer Research



# 1 DEVELOPMENT OBJECTIVES PROJECT BACKGROUND

**NOTE** Map + building reference images show a sample from EDG #1 book. Refer to EDG #1 book for remain buildings numbered, but not shown here.



**1626 BOREN AVE.**  
Olive Tower Apartments



**1701 MINOR AVE.**  
Metropolitan Parks Office Tower



**1800 NINTH AVE.**  
Regence BlueShield/Amazon



**809 OLIVE WAY**  
The Olivian Apartment High-Rise



**1823 TERRY AVE.**  
Aspira Apartments, 37-story apartment building



# 1 DEVELOPMENT OBJECTIVES PROJECT BACKGROUND



1601 9TH AVE.

Nine & Pine Apartments: Mixed-Use Residential



1099 STEWART AVE. & 1050 HOWELL STREET

Hill 7 Office & Hotel

Under Construction: DPD #3013130



1007 STEWART STREET

Office

Under Construction: DPD 016095



1812 BOREN AVE.

Tilt 49: Mixed-Use Building

Under Construction: DPD #3016574



1823 MINOR AVE.

Kinects: Office & Hotel

In Development: DPD #3004848



# 1 DEVELOPMENT OBJECTIVES PROJECT BACKGROUND



815 PINE STREET

The Premiere: Mixed-Use Retail/Residential



808 HOWELL STREET

8th & Howell Hotel

In Development: DPD # 3016917



737 OLIVE WAY

Olive 8, 39 story mixed-use with  
condominium residential and hotel uses.



1900 9TH AVE.

Seattle Children's Research Institute



700 UNION STREET

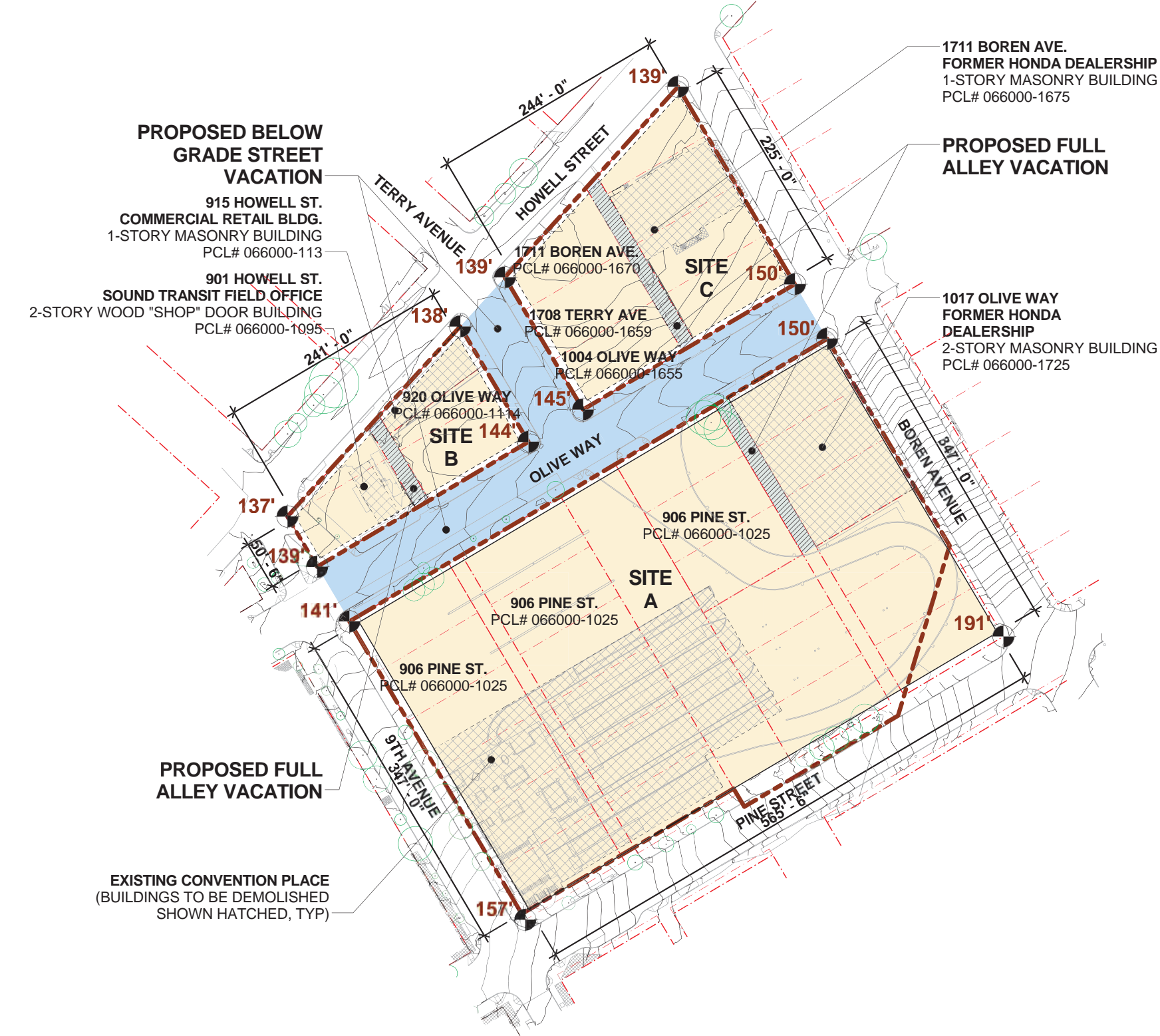
ACT Theatre



1 DEVELOPMENT OBJECTIVES

PROJECT BACKGROUND

EXISTING SITE PLAN



EXISTING SITE AREA

The project site consist of 3 blocks bounded by Howell Street to the north, Pine Street to the south, 9th Ave. to the west, and Boren Ave. and I-5 to the east. Terry Ave. and Olive Way divide the site in the interior. The site slopes significantly from the highest point at the intersection of Boren Ave. and Pine Street where they cross over I-5 on the southeast corner of the site to the lowest point at the intersection of 9th Ave. and Howell Street on the northwest corner of the site.

Proposed vacations include remaining alleys on Site A, B, & C, as well as a full vacation of Terry Ave. (R.O.W. to remain open to sky) and a subterranean vacation of Olive Way.

EXISTING BUILDINGS

The current uses on the site are most notably the Convention Place Station, along with a former Honda dealership on the block south of Olive Way. The blocks on the north of Olive Way also contain former Honda dealership facilities, a small commercial retail building, and a Sound Transit field office. The Honda facilities are now vacant.

EXISTING LANDSCAPE

The Arborist collected data (06/25/2015) on all trees of significant size, 6" and greater. The research concluded that no Exceptions trees are located on the site.

DOCUMENT KEY

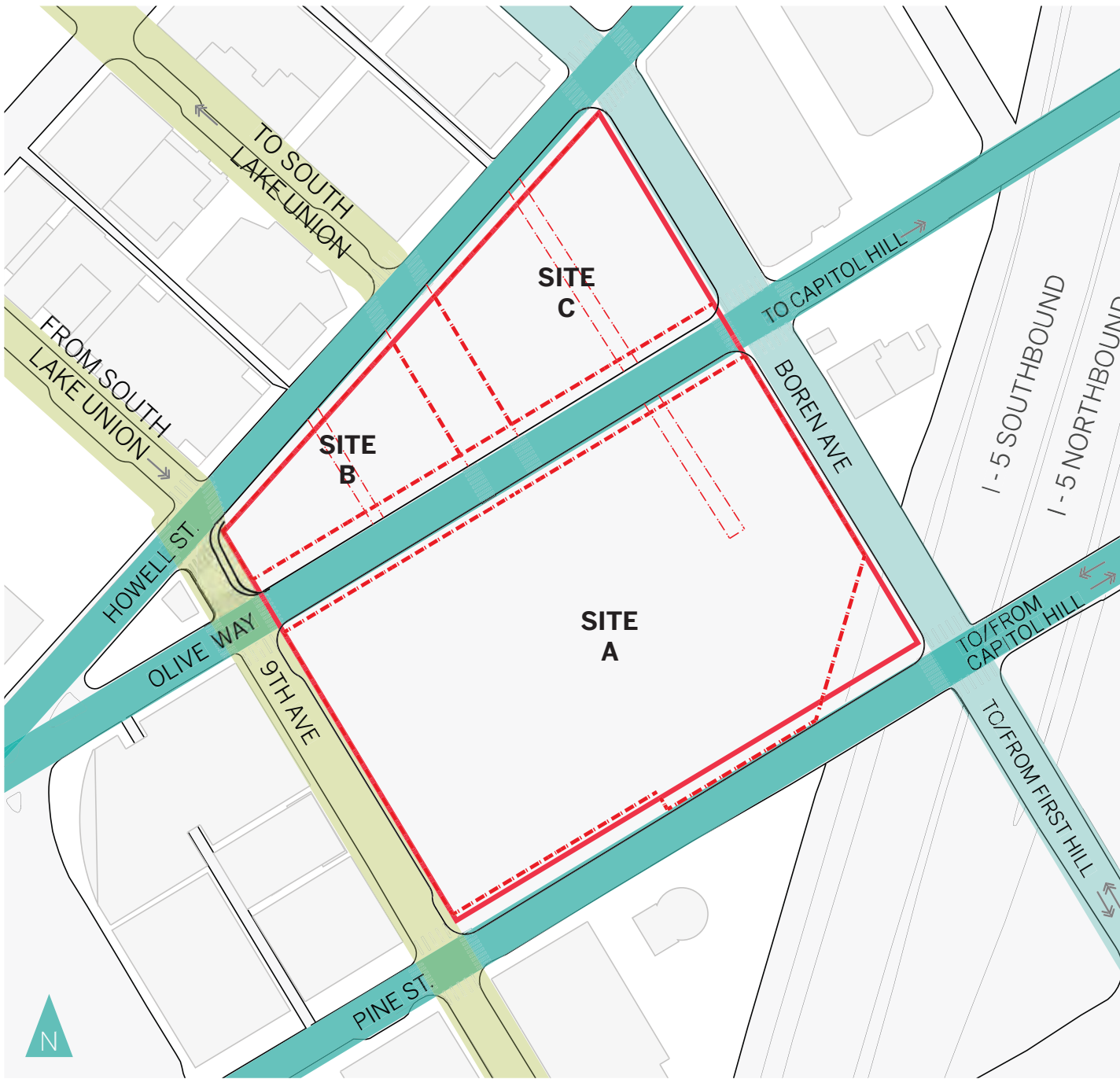




# 1 DEVELOPMENT OBJECTIVES

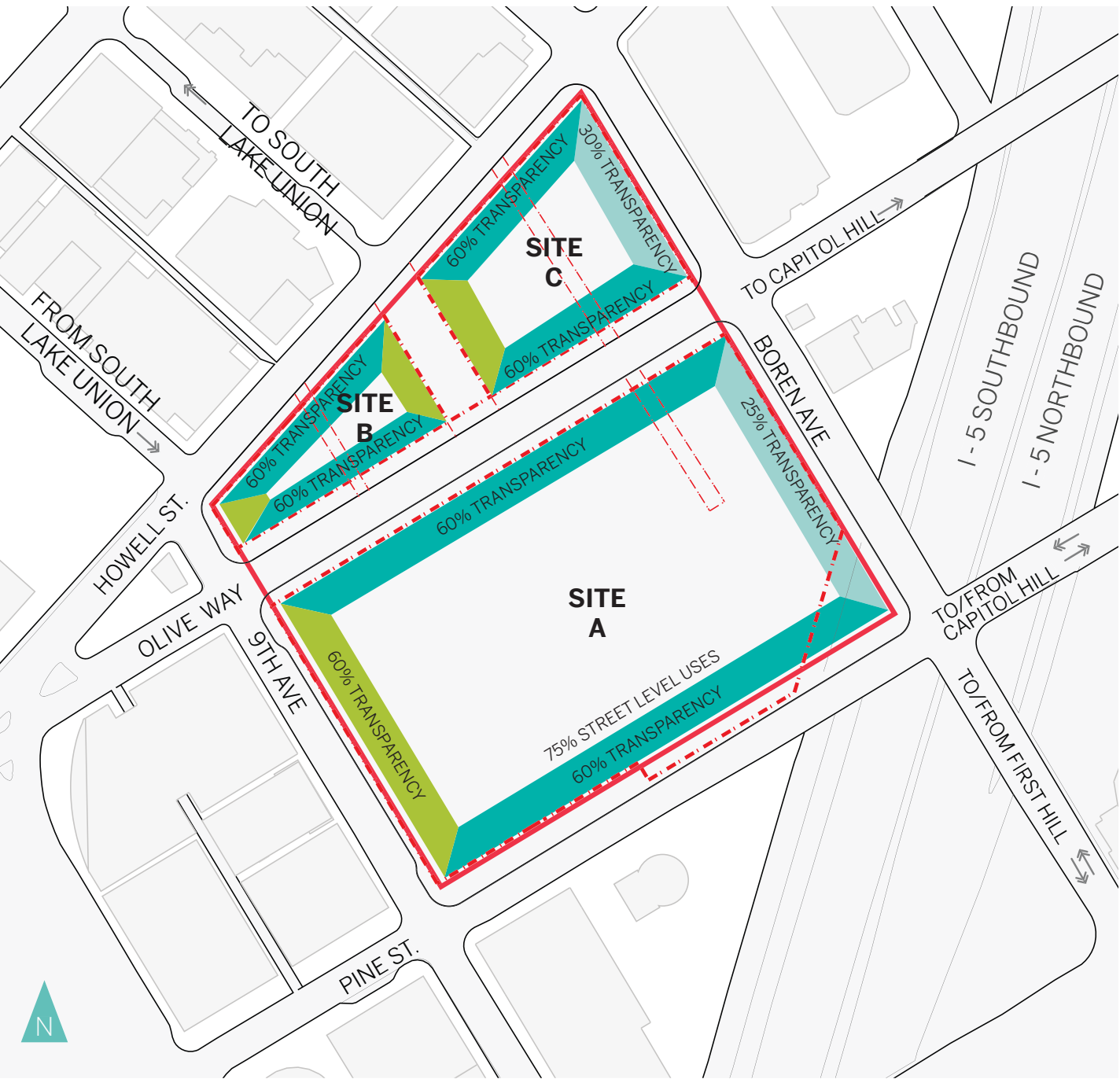
## PROJECT BACKGROUND

STREET CLASSIFICATIONS SUMMARY



- PRINCIPAL TRANSIT STREET
- PRINCIPAL ARTERIAL
- GREEN STREET
- PROJECT AREA
- PROPERTY LINE
- PROPOSED FULL ALLEY VACATION

FRONTAGE CLASSIFICATION SUMMARY



- CLASS I STREET FRONTAGE
- CLASS II STREET FRONTAGE
- GREEN STREET FRONTAGE
- PROJECT AREA
- PROPERTY LINE
- PROPOSED FULL ALLEY VACATION



1 DEVELOPMENT OBJECTIVES

PROJECT BACKGROUND

SITE B - RESIDENTIAL

PROPERTY ADDRESSES

| SITE A        | SITE B        | SITE C          |
|---------------|---------------|-----------------|
| 1600 9th Ave. | 920 Olive Way | 1711 Boren Ave. |

KING COUNTY PARCEL NUMBERS

| SITE A      | SITE B      | SITE C      |
|-------------|-------------|-------------|
| #0660001025 | #0660001095 | #0660001655 |
| #0660001700 | #0660001113 | #0660001659 |
| #0660001725 | #0660001114 | #0660001670 |
|             |             | #0660001675 |

ZONING SMC 23.49.056 MAP A

SITE A/B/C  
Denny Triangle Urban Center Village  
Downtown Mixed Commercial – DMC 340/290-400

SITE AREA

| SITE A      | SITE B     | SITE C     |
|-------------|------------|------------|
| 202,509 sf* | 25,551 sf* | 50,979 sf* |

\* Includes vacated alleys

DESIGN REVIEW SMC 23.41

Required

STRUCTURE HEIGHT 23.49.008

Max height = 400 feet  
The proposed height is approximately 290 feet.

STREET-LEVEL USE 23.49.009 MAP 1G

9TH AVE.– Not Required  
HOWELL STREET – Not Required  
OLIVE WAY – Not Required  
TERRY AVE. - 75% Required  
Street level use is provided to exceed development standards along Howell Street and Olive Way. See proposed departure at Terry Ave.

FLOOR AREA RATIO 23.49.011

Base = 5, Max = 10 - Site ~ 25,551 SF  
Exemptions: Residential use, street level use, below-grade use.  
Proposed FAR is approximately 0. NOTE: Under the PCD the combined FAR for sites A, B, and C is approximately 14.3.

RESIDENTIAL UNITS: 409

RESIDENTIAL AMENITY AREA 23.49.016

5 % of the total residential gross floor area  
Required = 17,530 SF  
Proposed total = 17,610 SF  
Proposed Indoor = 8,859 SF  
Proposed Outdoor = 8,751 SF  
Amenity area is proposed for certain interior areas of the building, along with exterior amenity space on the podium and tower rooftop.

OVERHEAD WEATHER PROTECTION & LIGHTING 23.49.018

Required along Terry Ave., 9th Ave., Olive Way, Boren Ave., and Howell St.  
Canopies are proposed along the entire building per development standards, except along 9th Ave. where a large overhang and open space is provided.

CAR PARKING 23.49.019.E

None required, none provided.

BICYCLE PARKING 23.49.019.E

Residential - 1 space per every two dwelling units  
Retail - 1 space per 5,000 SF of retail space  
Bicycle parking has been proposed to meet development standards.

MINIMUM SIDEWALK WIDTH 23.49.022

9TH Ave. = Varies per Green Street requirements  
HOWELL STREET = 15 feet  
OLIVE WAY = 15 feet  
TERRY Ave. = Varies per Green Street requirements  
The building has been setback from the property line to meet or exceed the sidewalk width requirements.

MINIMUM FACADE HEIGHT 23.49.056.A

25 feet on Class 1 Pedestrian:  
Olive Way, 9th Ave., Terry Ave., Howell Street  
15 feet on Class 2 Pedestrian: N/A  
The proposed facade height meets development standards.

FACADE SETBACK LIMITS 23.49.056.B.2

Required from 15’-0” to 25’-0” along Class 1 and Green Streets: Olive Way, 9th Ave., Terry Ave., Howell Street  
The proposed building setback along 9th Ave. is beyond the 10 foot maximum to create a larger outdoor space for seating or dining adjacent to active street level uses. See potential departure request. The proposed setback at the corner of Olive Way and Terry Ave. opens up the terminus of the green street and directs pedestrians towards the entrance of the convention center and 9th Ave.

FACADE TRANSPARENCY 23.49.056.C

Class 1 = 60% transparency required between 2’-0” and 8’-0” above sidewalk:  
The proposed design meets the facade transparency development standards for the site.

BLANK FACADE LIMITS 23.49.056.D

Class 1 and Green Street:  
Maximum width of blank facade = 15’-0”:  
There are two areas for potential departure requests. A reduced area for gas meters on Howell Street with architectural screening. There is also an area on Terry Ave. for mechanical use setback from the sidewalk.

STREET TREES 23.49.056.E

Required on all streets  
Street trees are proposed along all sides per development standards

LANDSCAPING 23.49.056.F

Area provided shall be 1.5 times the length of the street lot line: 23.49.056.F.1  
Green Street planting: 23.49.056.F.2  
Landscaping in setbacks of area more than 300 SF require 20% landscape: 23.49.056.F.3  
Landscaping is provided to meet development standards.

GREEN STREET SETBACK 23.49.056.F4

2’-0” setback required  
50 % of setback shall be landscaped  
Green Street setbacks are provided to meet development standards.

UPPER LEVEL GREEN STREET SETBACK 23.49.058.G

15 foot setback required above 45 feet (along 9th Ave. and Terry Ave.)  
The entire tower is setback beyond what is required by code to open up Terry Ave. and provide greater solar access. A portion of the podium is within the required setback in order to clarify the massing and intersections of building forms along the shift in the street grid. See potential departure request.

MAXIMUM TOWER WIDTH

The maximum tower width for portions of a structure above a height of 85 feet shall be 120 feet or 80% of the width of the lot measured in the Avenues (9th and Terry)  
The proposed maximum tower width meets development standards.

SOLID WASTE STORAGE 23.54.040

Residential  
575 SF plus 4 SF for each additional unit above 100  
Retail  
50% of the requirement for nonresidential development in mixed use development that contains both residential and nonresidential use  
A space smaller than the required area for solid waste storage is being proposed. A solid waste management plan is being proposed.



1 DEVELOPMENT OBJECTIVES

PROJECT BACKGROUND

SITE C - OFFICE

**STRUCTURE HEIGHT** 23.49.008  
Maximum height = 340ft  
The proposed height is approximately 240 feet.

**STREET-LEVEL USE** 23.49.009 MAP 1G  
BOREN AVE. – Not Required  
HOWELL STREET – Not Required  
OLIVE WAY – Not Required  
TERRY AVE. – 75% Required  
Street level use is provided to exceed development standards along Boren Ave., Howell Street, and Olive Way.  
See proposed departure at Terry Ave.

**FLOOR AREA RATIO** 23.49.011 SMC  
Base = 5, Max = 10 - Site ~ 50,980 SF  
Exemptions: Residential use, street level use, below grade  
Proposed FAR is approximately 9.7. NOTE: Under the PCD the combined FAR for sites A, B, and C is approximately 14.3.

**OPEN SPACE** 23.49.016  
20 SF required per 1,000 SF of office area.  
Open space is proposed to meet development standards.  
Two large terraces will be accessible from the second floor lobby space.

**OVERHEAD WEATHER PROTECTION & LIGHTING** 23.49.018  
Canopies are proposed along the entire building per development standards.

**CAR PARKING** 23.49.019.E  
None required, none provided.

**BICYCLE PARKING** 23.49.019.E  
1 space per 5000 SF of office area use up to 50 spaces.  
1 space per 10,000 SF office area above 50 spaces.  
Bicycle parking has been proposed to meet development standards.

**SHOWER FACILITY** 23.49.019.F  
1 shower per gender per 250,000 SF of office  
The shower facility has been proposed to meet the development standards.

**OFF STREET LOADING** 23.49.019.G  
Off street loading required = Low Use (Table A 23.54.035)  
Off street loading bays are proposed to meet development standards.

**MINIMUM SIDEWALK WIDTH** 23.49.022  
BOREN Ave. = 12 feet  
HOWELL STREET = 15 feet  
OLIVE WAY = 15 feet  
TERRY Ave. = Varies per Green Street requirements  
The building design has been setback from the property line to meet or exceed sidewalk width requirements.

**MINIMUM FACADE HEIGHT** 23.49.056.A  
25 feet on Class 1 Pedestrian:  
Olive Way, Terry Ave., Howell Street  
15 feet on Class 2 Pedestrian: Boren Ave.  
Minimum facade heights are proposed to meet development standards.

**FACADE SETBACK LIMITS** 23.49.056.B.2  
Required from 15'-0" to 25'-0" along Class 1 Green Streets:  
Olive Way, Terry Ave., Howell Street  
Required from 0'-0" to 15'-0" along Class 2: Boren Ave.  
The proposed setback at Boren Ave. responds to the shift in the street grid at Howell Street. The form of the tower is carried through to the ground, adding clarity to the overall design and providing for a more generous sidewalk. The same design approach is carried through on Terry Ave. See potential departure requests.

**FACADE TRANSPARENCY** 23.49.056.C  
Class 1 = 60% transparency required between 2'-0" and 8'-0" above sidewalk: Howell Street, Olive Way, Terry Ave.  
Class 2 = 30% Transparency required between 2'-0" and 8'-0" above sidewalk: Boren Ave.  
The proposed design meets and exceeds the facade transparency development standards for the site.

**BLANK FACADE LIMITS** 23.49.056.D  
Class 1 and Green Street:  
Maximum width of blank facade = 15'-0":  
Howell Street, Olive Way, Terry Ave.  
Class 2: Maximum width of blank facade = 30'-0":  
Boren Ave.  
The proposed design meets development standards for the site.

**STREET TREES** 23.49.056.E  
Required on all streets  
Street trees are proposed along all sides per development standards.

**LANDSCAPING** 23.49.056.F  
Area provided shall be 1.5 times the length of the street lot line: 23.49.056.F  
Green Street planning: 23.49.056.F.2  
Landscaping in setbacks of area more than 300 SF require 20% landscape: 23.49.056.F.3  
The landscaping proposed has been designed to meet development standards.

**GREEN STREET SETBACK** 23.49.056.F4  
2'-0" setback required  
50 % of setback shall be landscaped  
Green Street setbacks are provided to meet development standards

**MAXIMUM LENGTH OF UNMODULATED FACADE** 23.49.058.B  
Within 15'-0" of property line  
155 feet between 86'-0" and 160'-0" (Max length of unmodulated facade)  
125 feet between 161'-0" and 240'-0" (Max length of unmodulated facade)  
See proposed departure request. The Howell Street facade modulation is proposing a 6ft. setback over a larger area than required per development standards.

**UPPER LEVEL GREEN STREET SETBACK** 23.49.058.G  
15 foot setback required above 45 feet (along Terry Ave.)  
The tower is setback beyond what is required to offer more space and light for the green street.

**SOLID WASTE STORAGE** 23.54.040  
Office required: 500 SF of shared storage  
Retail required: 125 SF of shared storage  
The proposed solid waste storage exceeds development standards.



## 2 EDG 03 COMMENT SUMMARY

### OVERALL COMMENTS

#### 9A - CONTEXT ANALYSIS

At EDG#3, the Board discussed design elements at the pedestrian scale in great detail, and the recommendation to manifest pedestrian activation and interest at all locations on all perimeters of all three blocks, using varied materials, added activities or nodes for users near passive spaces, visual interest and texture at all loading doors (when closed), vehicle portals and non-retail frontages.

#### 9B - VIEWPOINTS

At EDG#3, the Board appreciated the multiple perspective views provided, but recommended additional views along Terry Ave. looking toward the Olive facade that is the Terry Green Street terminus, developing an intentional and scaled elevation response to that terminus condition, and showing the pedestrian edges of Terry Ave. and the adjacent building ground floors in detail. Fully conveying all three facades holistically may require 2-3 perspectives.

#### 10C - TERRY AVE. & TRUCK PLAZA

At the EDG#3, the Board restated the design approach for the plaza along Terry Ave. between Howell and Olive should be pedestrian first, ensuring north/south connections when vehicles are present and creating a usable space during CCX events. Therefore, the sidewalks on both sides should be consistent connectors and generous, outside any required truck movements (which should be minimized); the sidewalks, planting buffers and setbacks shown on pg 48 were supported. The Board also restated this space is a critical visual and pedestrian link from the Terry Green Street to the CCX facility.

The Board continued to have concerns about the quantity and timing of truck movements on Terry plaza, and supported the eroded ground floor massing at Block B southeast corner, but recommended the presence of the Block B loading doors be more subordinate to the corner entrance lobby (pg 61)

### SITE B COMMENTS - RESIDENTIAL

#### 9C - STREET GRID

At EDG#3, the Board was generally in support of a sizable (approximately 38' x 59') public plaza at the northeast corner of 9th and Olive (as shown on pg.45, upper right), as it occupies and marks a special pedestrian crossroads location. The Board will review the detailed design features of this plaza at subsequent meetings. See comments under 11h for the podium massing adjacent to this plaza. See Departure #4 for Board comments about the projections proposed along the 9th Ave. Green Street.

#### 10E - MASSING OPTIONS

At EDG#3, the Board supported the overall massing scheme for the CCX structure as presented, with important revisions along Olive discussed under departure #1. The Board supported the Block B west plaza and massing as shown on pg 61, and more importantly via the model; the Board recommended more perspectives of this block from the west, to confirm a superior response to the “flatiron” condition.

The Board supported the 3-part tower massing of Block C, but recommended the vertical recesses be deeper. The Block C office podium was less well resolved, and the Board recommended the height, scale and stepping of the podium needs more study, possibly a dramatic change of materials and/or taller, and deeper offsets at the gasket to the tower. The Board agreed the two-story scale along Olive and part of Boren (shown on pg 27) was more successful than the one on Howell.

#### 11E - TERRY AVE. GREEN STREET TERMINUS

At EDG#3, the Board supported crosswalks and tabled intersection shown on pg 47, while those R.O.W improvements are for SDOT review and confirmation. See 11e above for comments on the Terry terminus. The Board agreed the block B lobby should be more prominent on Terry, and recommended additional perspective renderings (see comments under 9b).

#### 11H - SITES B & C; CO-DEVELOPMENT

At EDG#3, the Board supported the massing, tower proportions and development of bock B, with a tall podium, and expressed gasket. Also see comments under 10e.

#### 4A - TOWER MASSING AND PODIUM

**MUP 01** - Contrary to DRB endorsement of the EDG#3 elevations (EDG#3 booklet pg 60/right) and guidance to maintain “a tall podium, and expressed gasket” (EDG report 11h), the Howell tower facade runs straight down to grade, uninterrupted, leaving a compressed, residual ground level that is not the tower lobby or a use related to the tower (this tower identity-to-grade approach is valid on the Olive facade, per A310, but with further refinements). Per guidance, re-instate the basic composition from EDG pg 61, to add a legible podium, 2-3 stories in proportion to the tall mass above, and with a substantial ‘gasket’ or setback at the transition to the tower.

**MUP 02** - Carrying the tower facade language down to level 2 (on Howell and Olive) was preliminarily supported only if the rest of the podium ground floor scale was revised, and the joint/transition between the brick podium and metal panel tower is legible and well-composed. Neither has occurred; the ground level especially on Olive remains too compressed, and the tower/podium overlap is crude.

#### 4B - TOWER MASSING AND PODIUM

**MUP 01** - At EDG #3 the tower above the base was broken into 2 masses with offset heights/parapets and different cladding treatments, as shown on pg 24, 60/61. The DRB endorsed pursuing this 2-part massing per guidance 10e and 11h, and did not mention a more unified, singular mass, which is now shown (elevations and G301). Please increase the roof and plan offsets to create 2 legible masses to ‘slenderize’ the tower, and design more dramatic elevational/material distinction to reinforce the 2 forms.

**MUP 02** - Raising a roof screen does not make 2 forms; the tower has lost all sense of verticality shown at EDG#3, it is blocky and displays no secondary slenderizing, is made up of 4 equal facades with identical proportions/materials/ colors. see g301 and elevations; revise.

#### 4C - TOWER MASSING AND PODIUM

**MUP 01** - Most of the podium is flush to the PL’s and overhangs the setback ground floor about 3ft (A201/202), creating a very squat and compressed ground level scale and pedestrian experience; this is true on all sides except the SE corner, and is especially demonstrated on Olive at grid RG, where the overhanging soffit is only 8 ft clear of sidewalk. Consistent with the EDG #3 elevations seen by

the DRB, shift the overhang up to level 3, to increase the ground scale of the entire block to 2 stories. Level 2 can remain residential, but with adjusted unit designs that evoke a ‘transom’ appearance to the storefronts below.

**MUP 02** - MUP 01 comment is still valid; podium does not match the model with no-overhang or tall ground floor proportion shown to DRB, nor drawings on pg 60/61. reduce to PL’s at least level 2 overhangs on west portion of Olive and Howell; the east portion may be acceptable.

#### 5A - WEST PLAZA OPENNESS, PUBLIC ACCESS

**MUP 01** - Per drawings on G105, L302, and A201/202, the west ‘plaza’ appears to be nominally 36 x approx. 60 ft of “public open space” inside the required sidewalks, at this critical pedestrian crossroads. The building to the east overhangs more than 50% of the 36 ft width (please provide a dimension string from the west PL), with a soffit height of only approx. 12ft. This is not consistent with the size and openness to the sky of the plaza shown to the DRB on pg 61/left, nor with the DRB guidance 9c. The openness and sunlight of this space is especially critical if this plaza is to fulfill a PCD public benefit item. Eliminate the columns at grid RAA and recess any overhanging portion significantly (15-20ft), and lift the soffit consistent with correction #4 above, to create an attractive, sunlit public space. A slight overhang of the west storefront is warranted for weather protection and architectural expression at the flatiron, axial view.

**MUP 02** - MUP 01 comment still valid as no change to MUP drawings; revise per Lindsay King email dated 4/26/16.

NOTE: per g000, the west facade forming a flat-iron focus/terminus looking east on Olive, remains generic and uninspired; no special character/color/materials...an exercise in flat glass at the podium, and unit stacking in the tower; respond better to DRB guidance 11h, paragraph 2.

#### 5B - WEST PLAZA OPENNESS, PUBLIC ACCESS

**MUP 01** - Per planting plan L161, 3 sides of the ‘plaza’ consist of a 4-12 ft wide planter, with approx 18” tall hedges, and one small 3 ft wide access apron 30 ft off the intersection; this fully privatizes the space from the adjacent sidewalks (although the perspectives on G000 show no hedges/ barriers). Eliminate/ reduce/Redesign the planters to maximize public access to the plaza and adjoining retail,



## 2 EDG 03 COMMENT SUMMARY

especially along the west side. Revise any planter edges at the NW corner to respect pedestrian desire lines from the Howell crosswalk traveling to Olive, regardless of the PL's.

### 7 - DESIGN REVIEW

**MUP 02** - Eliminate departure #8 and improve retail viability; shift blank gas meter off the strategic southwest corner more toward mid-block (possibly in the tower return between grids RF and RG), and dogleg the exit stair corridor to provide more retail depth facing onto the plaza.

### SITE C COMMENTS - OFFICE

#### 10E - MASSING OPTIONS

At EDG#3, the Board supported the overall massing scheme for the CCX structure as presented, with important revisions along Olive discussed under departure #1. The Board supported the Block B west plaza and massing as shown on pg 61, and more importantly via the model; the Board recommended more perspectives of this block from the west, to confirm a superior response to the "flatiron" condition.

The Board supported the 3-part tower massing of Block C, but recommended the vertical recesses be deeper. The Block C office podium was less well resolved, and the Board recommended the height, scale and stepping of the podium needs more study, possibly a dramatic change of materials and/or taller, and deeper offsets at the gasket to the tower. The Board agreed the two-story scale along Olive and part of Boren (shown on pg 27) was more successful than the one on Howell.

#### 11F - HOWELL STREET

At the EDG#3, the Board re-affirmed that the screening of the truck ramp in the middle of block C (pg 59), and any perimeter back-of-house on either block (pg 60), should be a sophisticated glass design, providing an excellent pedestrian experience and interest by using varying patterns or materials.

#### 11G - SITE C, NORTHEAST BLOCK

At the EDG#3, the Board applauded the sizable decreases in the two truck portals (48 ft on Boren; 26 ft on Terry), and supported the approach of integrating and recessing the loading doors into the podium bays, as shown on pg 58/59. See 10e for office podium comments.

#### 12B - GENERAL

At the EDG#3, the board summarized that the project's site plan and massing have improved, the corners along Olive and Boren are stronger, and that some areas of the transparency are adequate. However, the Board recommended that the other areas need additional refinement. Several key edges of the ground floor perimeter need more attention. The lower and middle facade along Boren should be much more visually compelling. The Board stated the massing refinements to the Co-development blocks are going in the right direction, but need further refinement, especially at the podium, gasket and facades along Howell and Olive for block C.

#### 4A - TOWER MASSING AND PODIUM

**MUP 01** - The Howell tower facade runs straight down to grade, uninterrupted. Per guidance, redesign to add a legible podium, preferably 2-3 stories in proportion to the 240 ft tall mass above, and with a substantial 'gasket' or setback at the transition.

#### 4B - TOWER MASSING AND PODIUM

**MUP 01** - The 2 masses above the base are split by the central vertical reveal (pending material/color clarifications; does "LVR-1" or some other vertical/contrasting material carry all the way down the reveal?), but it should be more than 17 ft wide to break the monolithic reading of the identical claddings on the 2 masses, AND/OR...dramatically change the materiality of one of the tower masses, including offset parapets, taller gaskets (currently only level 2/3) and other techniques cited in 11g.

**MUP 02** - Issue remains: Per G320/321 material notes, the 2 tower masses have exactly the same GL-1 & GL-2 materials as the predominant cladding; how will one be "vertical expression" and the other "horizontal expression" per your memo? Deep fins and horizontal slab lines? Except for one portion at the northwest, the mullion and glass proportions shown are also identical, so the tower reads as one unified cladding, contrary to DRB guidance and correction #1. The vertical reveals on Terry and Boren are also identical cladding, rather than the advised contrasting material/louvers; issue remains.

#### 5B - LACK OF TOWER MODULATION

**MUP 01** - The approx. 2 ft deep divot with canted wall plane proposed is barely perceptible on the 120x 240 ft flat Howell wall (which is NW facing and will not benefit from much shadow-play); this is contrary to the explicit DRB guidance under Departure #3/pg 41 "one larger & deeper (than 3 ft) recess" and the recess "should extend to the parapet" to make a legible break at the roof edge. This departure/modulation proposal is going backwards from DRB guidance and is not sufficient on such a large, undifferentiated plane; Redesign with a substantially deeper, legible recess to at least partially comply with Code.

**MUP 02** - The revised, singular and wider recess as shown on g106 is an improvement, however the cladding inside the recess is identical to the adjacent walls; revise to provide some contrast and improve legibility of this modulation. also, the 6 ft depth is less than half the 15 ft code requirement; the DRB will evaluate.

### EDG 03 MODELS





### 3 EDG 03 COMMENT RESPONSES OVERALL COMMENTS

#### COMMENT 9A - CONTEXT ANALYSIS

The Board discussed design elements at the pedestrian scale in great detail, and the recommendation to manifest pedestrian activation and interest at all locations on all perimeters of all three blocks, using varied materials, added activities or nodes for users near passive spaces, visual interest and texture at all loading doors (when closed), vehicle portals and non-retail frontages.

**RESPONSE** - The proposed building and site design has been further refined to address the board's comments and express the texture and character of the pedestrian experience. In addition, further detail on materials, landscape, lighting and environmental graphics has been included in this package.

#### EDG 03



SITE PLAN



3 EDG 03 COMMENT RESPONSES OVERALL COMMENTS

RECOMMENDATION



NOTES

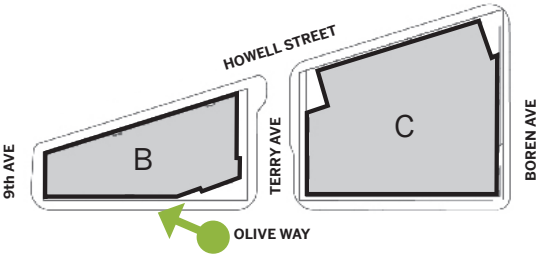
- A, B, C** - garage and loading doors have been developed to be integrated into the podium and provide texture, visual interest for pedestrians when opened or closed.
- D, E** - Activity and passive spaces, change of texture, paired with landscape and seating for lingering near building entries, corners, or associated retail spaces.
- F, G** - Non-retail frontages designed to be integrated into architectural expression of the building, provide continuity for pedestrians, and provide visual interest to interior spaces
- H** - plaza has been further developed to include features to be used by general public as well as activated by associated retail



3 EDG 03 COMMENT RESPONSES OVERALL COMMENTS



SITE B - STREET LEVEL VIEW LOOKING WEST ALONG OLIVE WAY

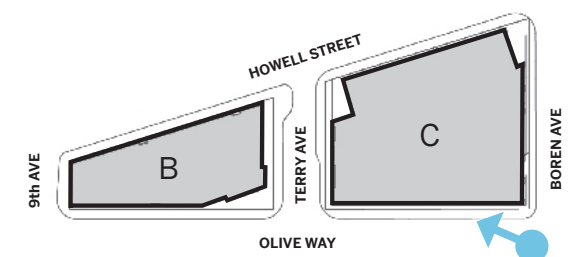




### 3 EDG 03 COMMENT RESPONSES OVERALL COMMENTS



**SITE C** - STREET LEVEL VIEW LOOKING WEST ALONG OLIVE WAY





### 3 EDG 03 COMMENT RESPONSES OVERALL COMMENTS

#### COMMENT 9B - VIEWPOINTS

At EDG#3, the Board appreciated the multiple perspective views provided, but recommended additional views along Terry Ave. looking toward the Olive facade that is the Terry Green Street terminus, developing an intentional and scaled elevation response to that terminus condition, and showing the pedestrian edges of Terry Ave. and the adjacent building ground floors in detail. Fully conveying all three facades holistically may require 2-3 perspectives.

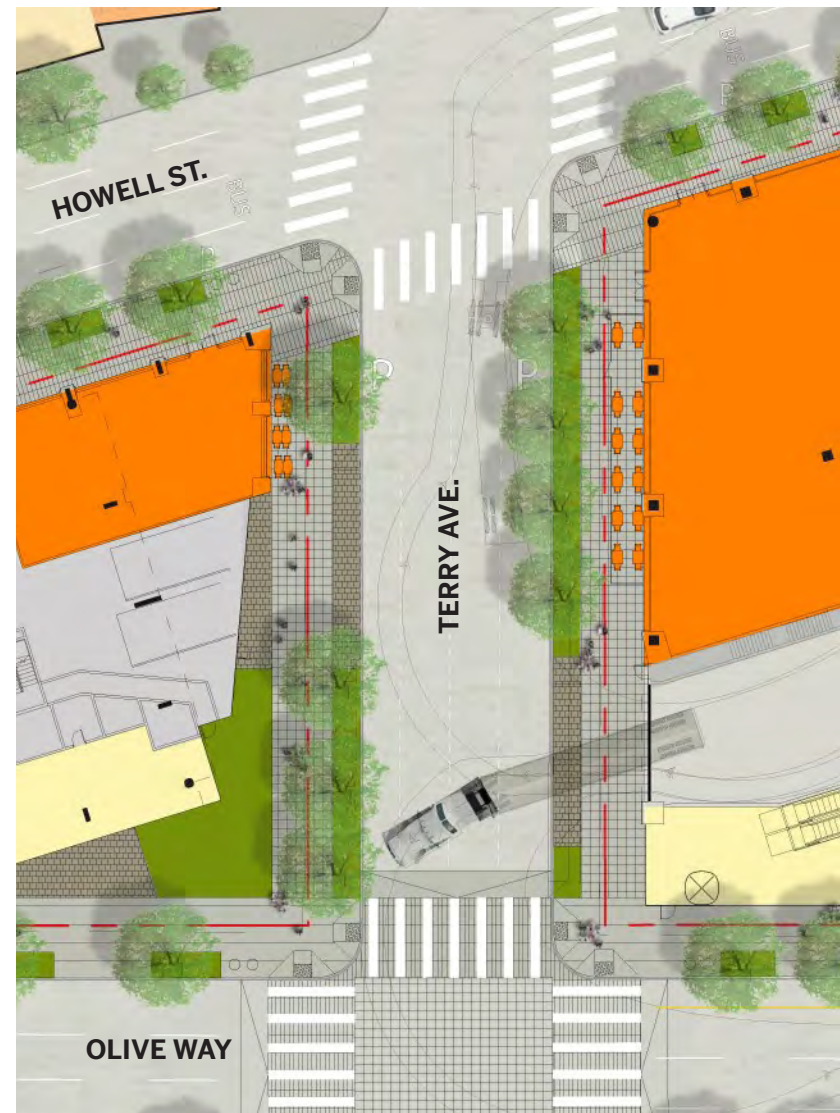
#### COMMENT 10C - TERRY AVE. & TRUCK PLAZA

The Board restated the design approach for the plaza along Terry Ave. between Howell and Olive should be pedestrian first, ensuring north/south connections when vehicles are present and creating a usable space during CCX events. Therefore, the sidewalks on both sides should be consistent connectors and generous, outside any required truck movements (which should be minimized); the sidewalks, planting buffers and setbacks shown on pg 48 were supported. The Board also restated this space is a critical visual and pedestrian link from the Terry Green Street to the CCX facility.

The Board continued to have concerns about the quantity and timing of truck movements on Terry plaza, and supported the eroded ground floor massing at Block B southeast corner, but recommended the presence of the Block B loading doors be more subordinate to the corner entrance lobby (pg 61)

**RESPONSE** - Additional views have been developed to illustrate the pedestrian experience between the Co-Development buildings along Terry Ave (pg. 25). The sidewalks along both sides of Terry Ave. provide a consistent and generous north-south connection. Wide pedestrian pathways along with a landscape buffer has been provided where possible to screen pedestrians from truck movements which have been minimized. A new pedestrian activated plaza is located at the corner of Terry Ave. and Howell Street on Site C to provide generous pedestrian nodes along the green street for adjacent street level uses. The loading doors for the proposed towers have been recessed from the buildings' facades to be more subordinate to the street level uses and lobby and reduce visual impact. Loading and service areas have been minimized to provide as much street level use as possible.

EDG 03



SITE PLAN AT TERRY AVE.

RECOMMENDATION



loading doors recessed

pedestrian zones expanded



3 EDG 03 COMMENT RESPONSES

OVERALL COMMENTS



STREET LEVEL VIEW LOOKING  
NORTHEAST AT **SITE C**, AT TERRY  
AVE. AND OLIVE WAY



STREET LEVEL VIEW LOOKING SOUTH AT TERRY AVE.



STREET LEVEL VIEW LOOKING  
SOUTHWEST AT **SITE B**, AT TERRY AVE.  
AND HOWELL ST.



### 3 EDG 03 COMMENT RESPONSES

SITE B - RESIDENTIAL

**COMMENT 10E** - MASSING OPTIONS

At EDG#3, the Board supported the overall massing scheme for the CCX structure as presented, with important revisions along Olive discussed under departure #1. The Board supported the Block B west plaza and massing as shown on pg 61, and more importantly via the model; the Board recommended more perspectives of this block from the west, to confirm a superior response to the “flatiron” condition.

**COMMENT 11H** - MASSING OPTIONS

At EDG#3, the Board supported the massing, tower proportions and development of bock B, with a tall podium, and expressed gasket.

**RESPONSES** - An additional view has been provided on page 25 for the west face of the residential tower. The amenity pavilion on the podium roof has been revised to orient towards 9th Ave. and create more visual interest at the “flatiron” condition. The tall podium remains with a two-story ground floor expression along Olive Way which transitions to a taller one-story expression along Howell Street. The amenity pavilion trellis and the multi-layered facade create a taller, narrower western facade.

**EDG 03**



AERIAL VIEW LOOKING EAST



### 3 EDG 03 COMMENT RESPONSES **SITE B** - RESIDENTIAL

#### RECOMMENDATION



Flatiron condition of podium emphasized through contrast of materials and integration of pavilion trellis into west podium facade

STREET LEVEL VIEW LOOKING EAST ALONG HOWELL ST.



### 3 EDG 03 COMMENT RESPONSES    SITE B - RESIDENTIAL

**COMMENT 11E** - TERRY AVE. GREEN STREET TERMINUS

At EDG#3, the Board supported crosswalks and tabled intersection shown on pg 47, while those R.O.W. improvements are for SDOT review and confirmation. See 11e for comments on the Terry terminus. The Board agreed the block B lobby should be more prominent on Terry, and recommended additional perspective renderings (see comments under 9b).

**RESPONSE** - A larger setback was created at the loading area for the residential building on Terry Ave. to give the building street level use at Terry Ave. and Olive Way more visual prominence for pedestrians walking south towards the convention center addition project. Requested perspectives on page 29.

EDG 03



STREET LEVEL VIEW LOOKING SOUTH AT TERRY AVE.



3 EDG 03 COMMENT RESPONSES **SITE B** - RESIDENTIAL

RECOMMENDATION



STREET LEVEL VIEW LOOKING SOUTH AT TERRY AVE.

recessed loading  
doors emphasize  
street level use at Olive  
and Terry corner



STREET LEVEL VIEW LOOKING SOUTH AT TERRY AVE.



### 3 EDG 03 COMMENT RESPONSES SITE B - RESIDENTIAL

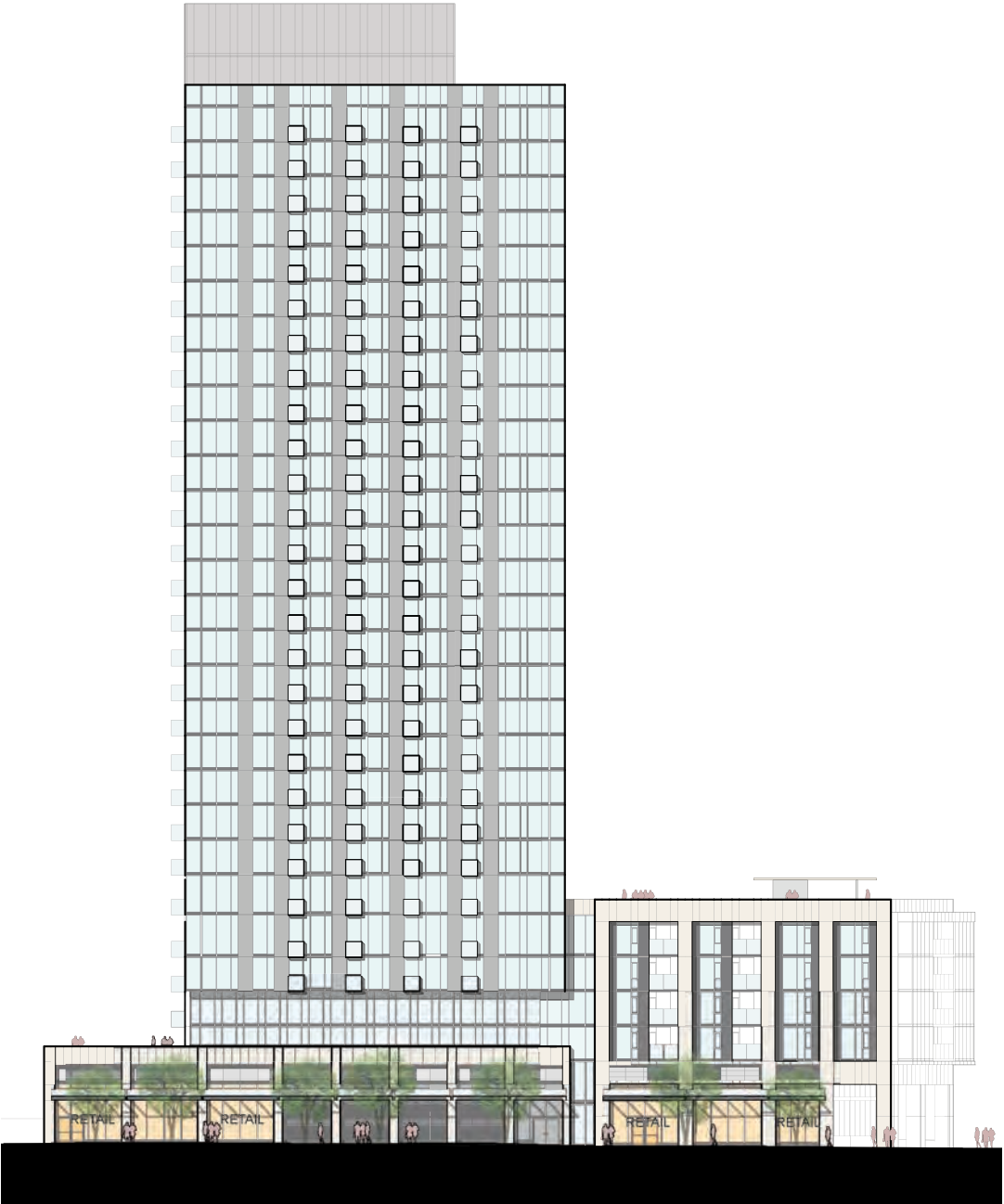
**COMMENT 4A** - TOWER MASSING AND PODIUM

**MUP 01** - Contrary to DRB endorsement of the EDG#3 elevations (EDG#3 booklet pg 60/right) and guidance to maintain “a tall podium, and expressed gasket” (EDG report 11h), the Howell tower facade runs straight down to grade, uninterrupted, leaving a compressed, residual ground level that is not the tower lobby or a use related to the tower (this tower identity-to-grade approach is valid on the Olive facade, per A310, but with further refinements). Per guidance, re-instate the basic composition from EDG pg 61, to add a legible podium, 2-3 stories in proportion to the tall mass above, and with a substantial ‘gasket’ or setback at the transition to the tower.

**MUP 02** - Carrying the tower facade language down to level 2 (on Howell and Olive) was preliminarily supported only if the rest of the podium ground floor scale was revised, and the joint/transition between the brick podium and metal panel tower is legible and well-composed. Neither has occurred; the ground level especially on Olive remains too compressed, and the tower/podium overlap is crude.

**RESPONSE** - The base of the podium has been revised to express a continuous setback at the street level. Relocating this “gasket” at the grade allowed the podium to remain at a constant height which strengthens the building mass on the site and gives more clarity to the tower and the visual transition between responding to the two forms and the influence of the street grids. The ground scale podium has been raised to create a two story expression along Olive Way. At Howell Street the ground floor scale remains at one story before transitioning to two stories as it turns the corner along 9th Avenue. A transition between the brick podium and glass tower occurs where the tower sets back for the balconies.

**EDG 03**



ELEVATION AT HOWELL ST.



3 EDG 03 COMMENT RESPONSES

SITE B - RESIDENTIAL

RECOMMENDATION





3 EDG 03 COMMENT RESPONSES

SITE B - RESIDENTIAL

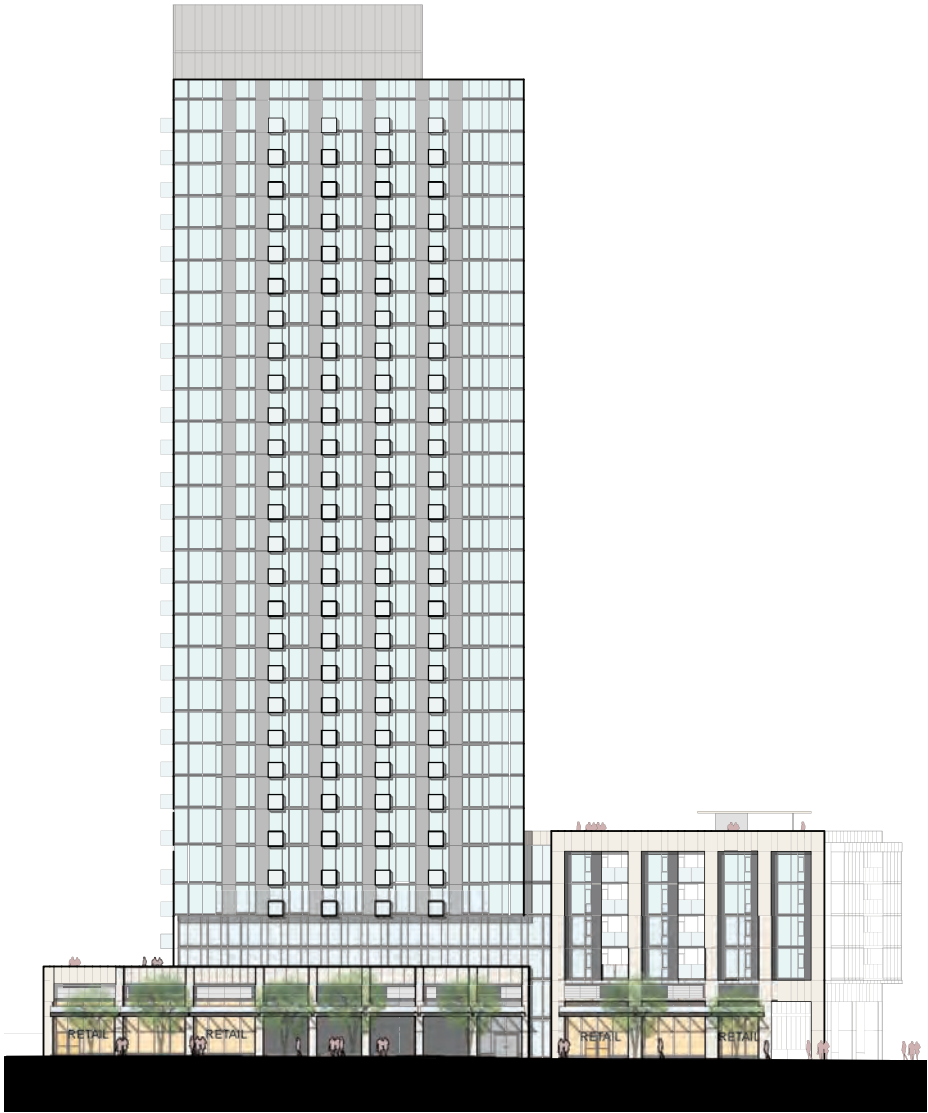
COMMENT 4B - TOWER MASSING AND PODIUM

**MUP 01** - At EDG #3 the tower above the base was broken into 2 masses with offset heights/parapets and different cladding treatments, as shown on pg 24, 60/61. The DRB endorsed pursuing this 2-part massing per guidance 10e and 11h, and did not mention a more unified, singular mass, which is now shown (elevations and G301). Please increase the roof and plan offsets to create 2 legible masses to 'slenderize' the tower, and design more dramatic elevational/material distinction to reinforce the 2 forms.

**MUP 02** - Raising a roof screen does not make 2 forms; the tower has lost all sense of verticality shown at EDG#3, it is blocky and displays no secondary slenderizing, is made up of 4 equal facades with identical proportions/materials/ colors. See g301 and elevations; revise.

**RESPONSE** - The tower proportions have been revised to give prominence to the west facing "flatiron" condition. The tower plan has remained similar to EDG #3. The facades have been redesigned to accentuate the vertical expression created by setting back the glass at the balconies. The proposed recommended design now has slender vertical expressions on all four facades, more texture and responds to the "flatiron" condition by creating symmetry and a more unified gesture at the west facade. The EDG#3 design achieved a two mass reading by raising the south and east screenwalls at the top of the tower. This gave less prominence to the "flatiron" condition which is the most visible and prominent facade of the tower.

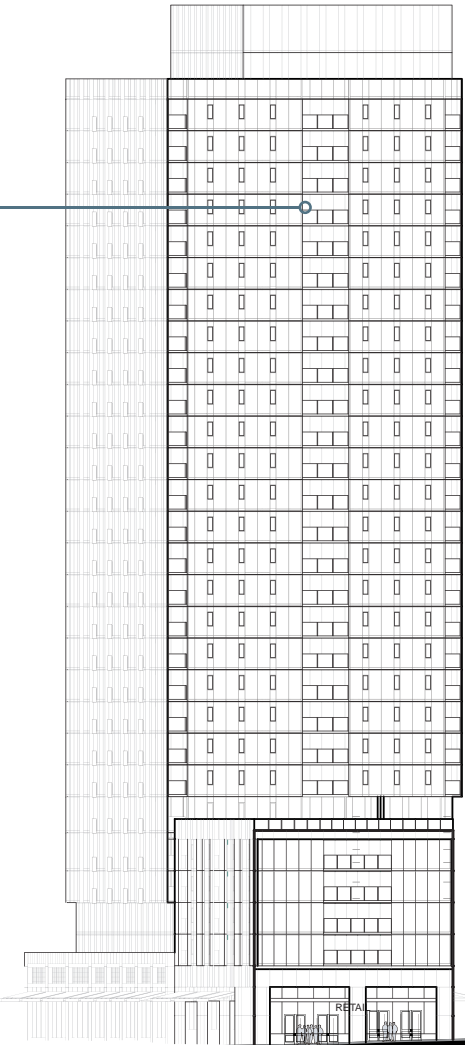
EDG 03



ELEVATION AT HOWELL ST.



wide flat facade



no unified gesture towards the "flatiron" condition



3 EDG 03 COMMENT RESPONSES

SITE B - RESIDENTIAL

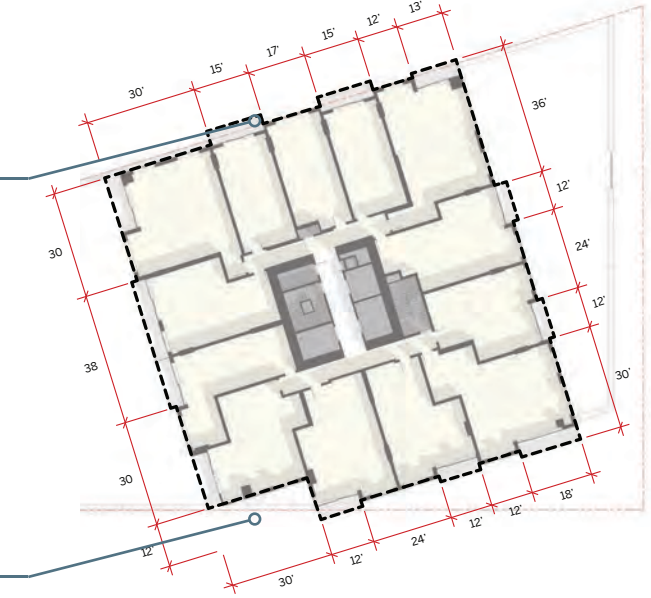
RECOMMENDATION



ELEVATION AT HOWELL ST.

projecting balconies and recessed facade accentuate vertical expressions on all four facades

step in facade setback further to create symmetry and a more unified gesture on the west facade



unified gesture in response to the "flatiron" condition





### 3 EDG 03 COMMENT RESPONSES SITE B - RESIDENTIAL

**COMMENT 4C** - TOWER MASSING AND PODIUM

**MUP 01** - Most of the podium is flush to the PL's and overhangs the setback ground floor about 3ft (A201/202), creating a very squat and compressed ground level scale and pedestrian experience; this is true on all sides except the SE corner, and is especially demonstrated on Olive at grid RG, where the overhanging soffit is only 8 ft clear of sidewalk. Consistent with the EDG #3 elevations seen by the DRB, shift the overhang up to level 3, to increase the ground scale of the entire block to 2 stories. Level 2 can remain residential, but with adjusted unit designs that evoke a 'transom' appearance to the storefronts below.

**MUP 02** - MUP 01 comment is still valid; podium does not match the model with no-overhang or tall ground floor proportion shown to DRB, nor drawings on pg 60/61. reduce to PL's at least level 2 overhangs on west portion of Olive and Howell; the east portion may be acceptable.

**RESPONSE** - A two story ground floor expression has been created along the entire Olive Way facade to increase the ground floor proportion. The raised overhang allows for more opportunities to vary the canopies and break down the length of the facade at the pedestrian level.

EDG 03



ENLARGED ELEVATION AT OLIVE WAY



### 3 EDG 03 COMMENT RESPONSES **SITE B** - RESIDENTIAL

#### RECOMMENDATION



ENLARGED ELEVATION AT OLIVE WAY

overhang raised to level 3 to create a continuous two story ground floor expression.



### 3 EDG 03 COMMENT RESPONSES    SITE B - RESIDENTIAL

**COMMENT 5A** - WEST PLAZA OPENNESS, PUBLIC ACCESS

**MUP 01** - Per drawings on G105, L302, and A201/202, the west 'plaza' appears to be nominally 36 x approx. 60 ft of "public open space" inside the required sidewalks, at this critical pedestrian crossroads. The building to the east overhangs more than 50% of the 36 ft width (please provide a dimension string from the west PL), with a soffit height of only approx. 12ft. This is not consistent with the size and openness to the sky of the plaza shown to the DRB on pg 61/left, nor with the DRB guidance 9c. The openness and sunlight of this space is especially critical if this plaza is to fulfill a PCD public benefit item. Eliminate the columns at grid RAA and recess any overhanging portion significantly (15-20ft), and lift the soffit consistent with correction #4 above, to create an attractive, sunlit public space. A slight overhang of the west storefront is warranted for weather protection and architectural expression at the flatiron, axial view.

**MUP 02** - MUP 01 comment still valid as no change to MUP drawings; revise per Lindsay King email dated 4/26/16.

NOTE: per g000, the west facade forming a flat-iron focus/terminus looking east on Olive, remains generic and uninspired; no special character/color/materials...an exercise in flat glass at the podium, and unit stacking in the tower; respond better to DRB guidance 11h, paragraph 2.

**RESPONSE** - In response to Lindsay King's email the west edge of the residential podium has been raised an additional level above the plaza along 9th Ave. to closer match the proposed design at EDG 03. The west facing glass projects out beyond the brick to celebrate the axial view and connects with the pavilion above. Longer balconies have been provided at the edges of the west face to lighten the visual mass of the overhang.

EDG 03



ENLARGED ELEVATION  
AT WEST FACE OF OLIVE WAY

RECOMMENDATION



setback raised to level  
three

columns minimized



### 3 EDG 03 COMMENT RESPONSES **SITE B** - RESIDENTIAL



STREET LEVEL VIEW FACING SOUTHEAST, LOOKING AT 9TH AVE. PLAZA



### 3 EDG 03 COMMENT RESPONSES SITE B - RESIDENTIAL

**COMMENT 5B - WEST PLAZA OPENNESS, PUBLIC ACCESS**

**MUP 01** - Per planting plan L161, 3 sides of the 'plaza' consist of a 4-12 ft wide planter, with approx 18" tall hedges, and one small 3 ft wide access apron 30 ft off the intersection; this fully privatizes the space from the adjacent sidewalks (although the perspectives on G000 show no hedges/ barriers). Eliminate/ reduce/Redesign the planters to maximize public access to the plaza and adjoining retail, especially along the west side. Revise any planter edges at the NW corner to respect pedestrian desire lines from the Howell crosswalk traveling to Olive, regardless of the PL's.

**COMMENT 9C - STREET GRID**

At EDG#3, the Board was generally in support of a sizable (approximately 38' x 59') public plaza at the northeast corner of 9th and Olive (as shown on pg.45, upper right), as it occupies and marks a special pedestrian crossroads location. The Board will review the detailed design features of this plaza at subsequent meetings. See comments under 11h for the podium massing adjacent to this plaza. See Departure #4 for Board comments about the projections proposed along the 9th Ave. Green Street.

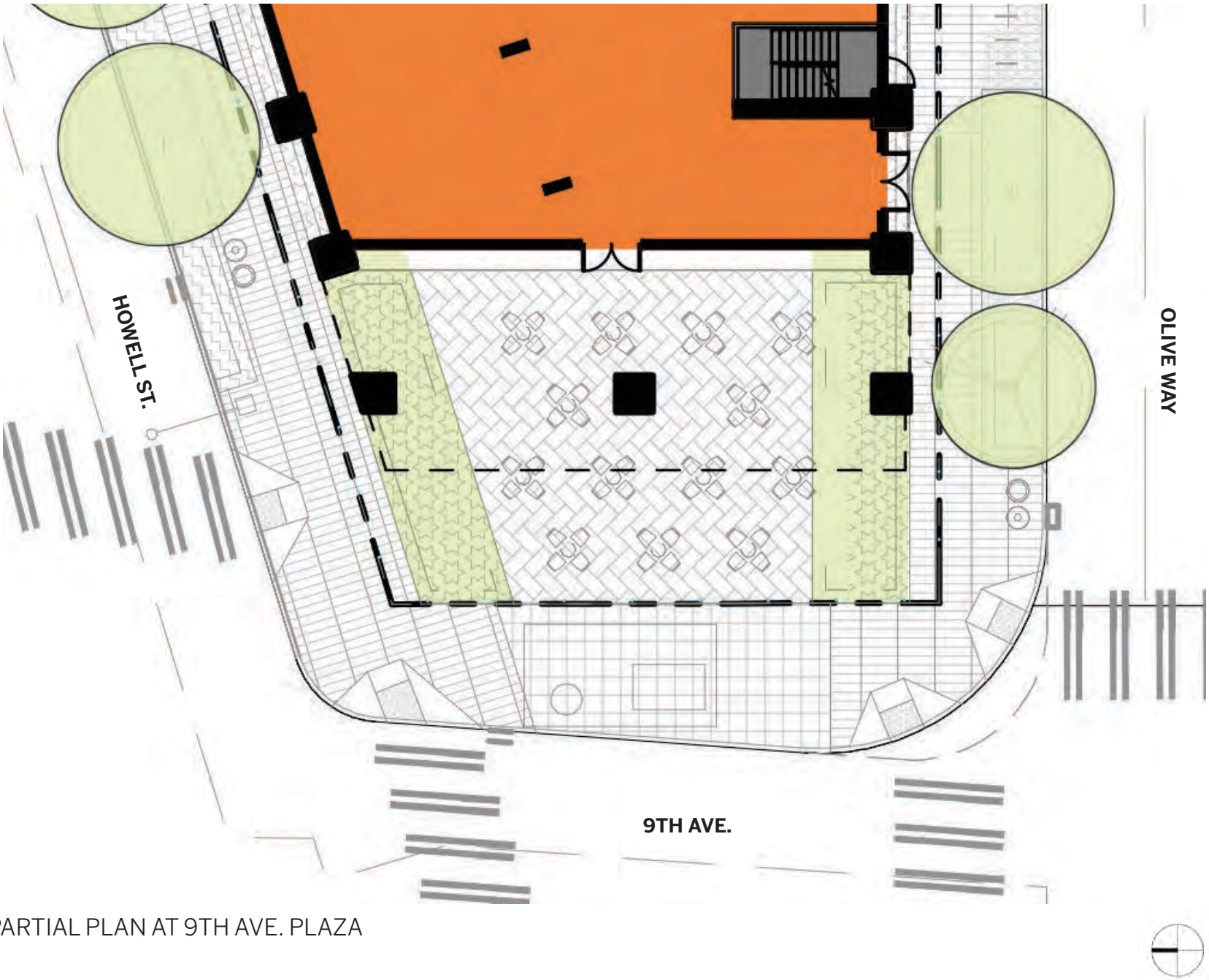
**RESPONSE** - In response to Lidsay King's email, the plaza landscape design has been revised to provide better access for pedestrians through the space. Landscape and street furnishings are designed to be open to the public while still allowing for scheduled private use.

**COMMENT 7 - DESIGN REVIEW**

**MUP 02** - Eliminate departure #8 and improve retail viability; shift blank gas meter off the strategic southwest corner more toward mid-block (possibly in the tower return between grids RF and RG), and dogleg the exit stair corridor to provide more retail depth facing onto the plaza.

**RESPONSE** - Gas meters have been relocated closer to a mid-block location on Howell St. to keep from street level use entrances.

EDG 03



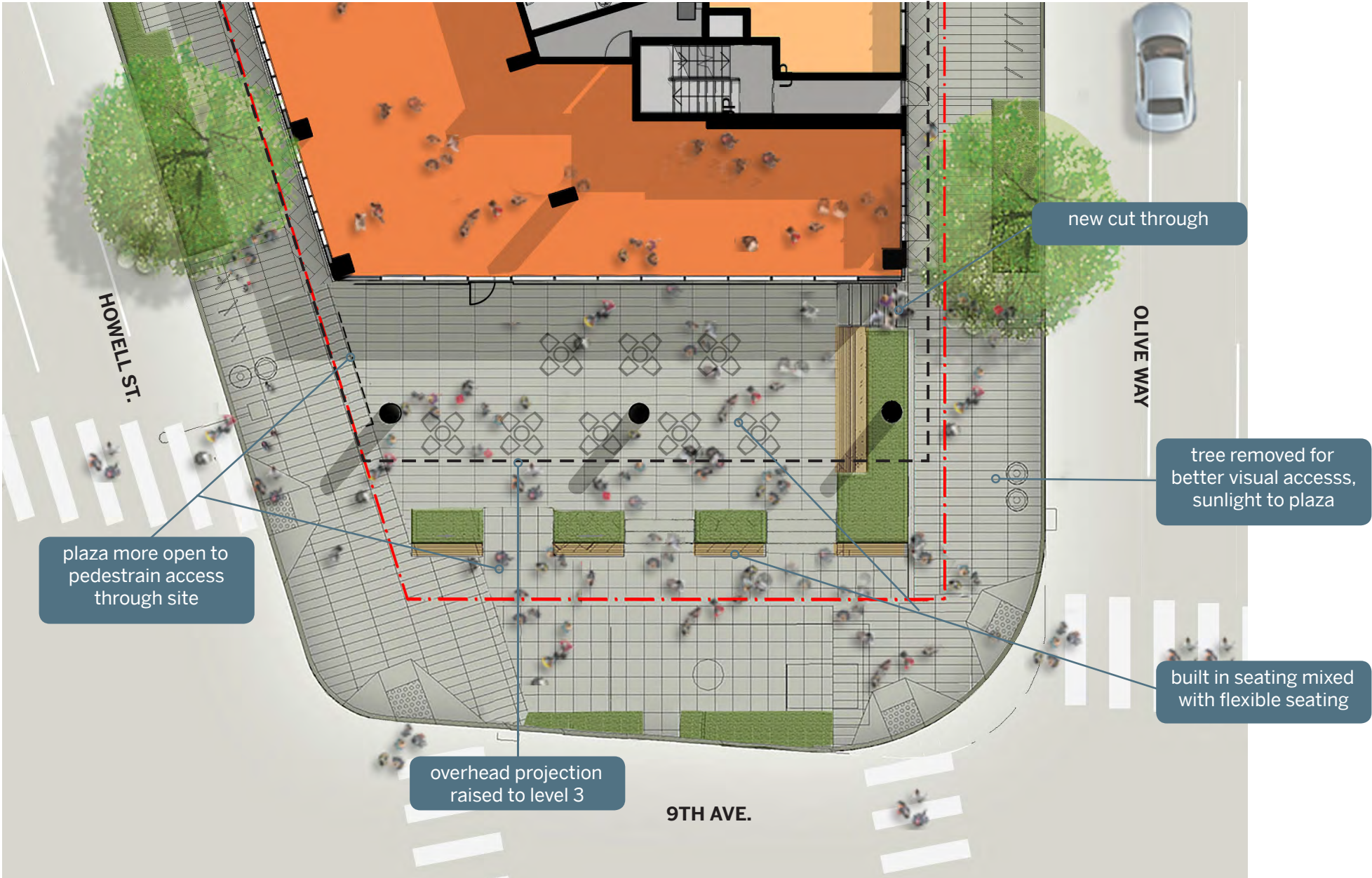
PARTIAL PLAN AT 9TH AVE. PLAZA



3 EDG 03 COMMENT RESPONSES

SITE B - RESIDENTIAL

RECOMMENDATION



PARTIAL PLAN AT 9TH AVE. PLAZA



# 3 EDG 03 COMMENT RESPONSES

## SITE C - OFFICE

### COMMENT 10E - MASSING OPTIONS

The Board supported the 3-part tower massing of Block C, but recommended the vertical recesses be deeper. The Block C office podium was less well resolved, and the Board recommended the height, scale and stepping of the podium needs more study, possibly a dramatic change of materials and/or taller, and deeper offsets at the gasket to the tower. The Board agreed the two-story scale along Olive and part of Boren (shown on pg 27) was more successful than the one on Howell.

### COMMENT 4B - TOWER MASSING AND PODIUM

**MUP 01** - The 2 masses above the base are split by the central vertical reveal (pending material/color clarifications; does “LVR-1” or some other vertical/contrasting material carry all the way down the reveal?), but it should be more than 17 ft wide to break the monolithic reading of the identical claddings on the 2 masses, AND/OR...dramatically change the materiality of one of the tower masses, including offset parapets, taller gaskets (currently only level 2/3) and other techniques cited in 11g.

**MUP 02** - Issue remains: Per G320/321 material notes, the 2 tower masses have exactly the same GL-1 & GL-2 materials as the predominant cladding; how will one be “vertical expression” and the other “horizontal expression” per your memo? Deep fins and horizontal slab lines? Except for one portion at the northwest, the mullion and glass proportions shown are also identical, so the tower reads as one unified cladding, contrary to DRB guidance and correction #1. The vertical reveals on Terry and Boren are also identical cladding, rather than the advised contrasting material/louvers; issue remains.

**RESPONSE** - The proportions of the 3-part tower massing have been updated to clarify how the forms resolve the shifting grids at the street level. The north mass along Howell Street is a thinner mass with vertical fins, the south mass floats above the podium and has horizontal reveals at the spandrel glass. The two masses are separated by a neutralglass reveal with no horizontal or vertical expressions. The Howell Street facade has been updated with a stronger two story podium expression with a transition to a one story expression to help break up the length of the facade. The podium along Olive Way and Boren Ave. have been updated to read as a two story height with a strong rhythm of one story stone piers accentuated with trees to create a second story canopy.

EDG 03



STREET LEVEL VIEW LOOKING WEST ON HOWELL ST.



3 EDG 03 COMMENT RESPONSES

SITE C - OFFICE

RECOMMENDATION



STREET LEVEL VIEW LOOKING WEST ON HOWELL ST.

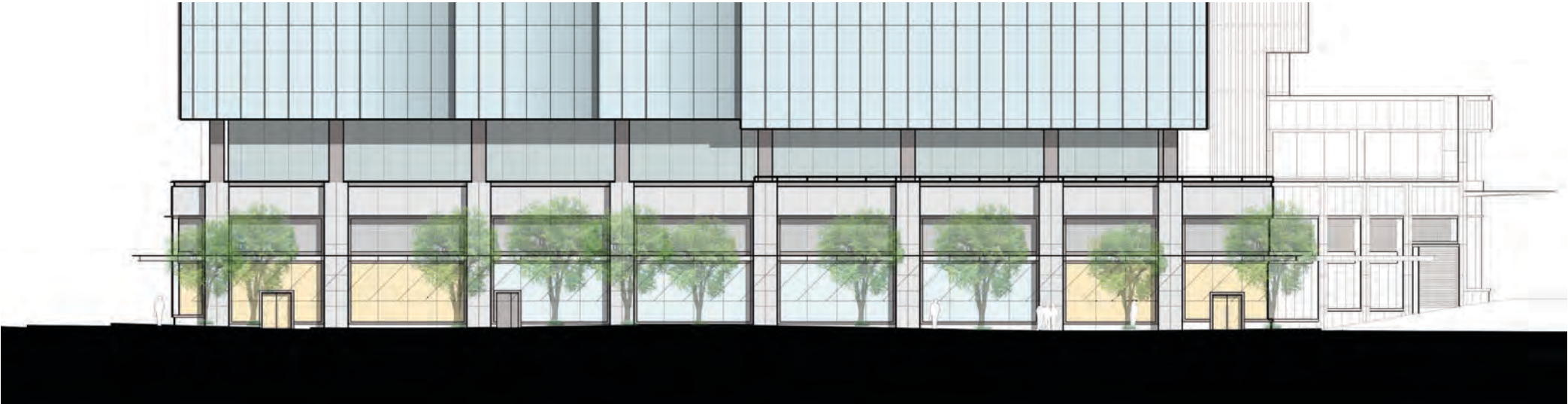


### 3 EDG 03 COMMENT RESPONSES SITE C - OFFICE

**COMMENT 11F - HOWELL STREET**  
At the EDG#3, the Board re-affirmed that the screening of the truck ramp in the middle of bock C (pg 59), and any perimeter back-of-house on either block (pg 60), should be a sophisticated glass design, providing an excellent pedestrian experience and interest by using varying patterns or materials.

**RESPONSE** - The glass at the truck ramp along Howell Street is proposed to be a clear vision glass to match the adjacent storefront to visually integrate into the architectural language of the building and provides continuity for pedestrians at the street level. This area has been minimized significantly from EDG 03 to provide more street level use at the sidewalk. The activity of the trucks moving down the ramp provides visual interest and moments of curiosity for pedestrians.

EDG 03



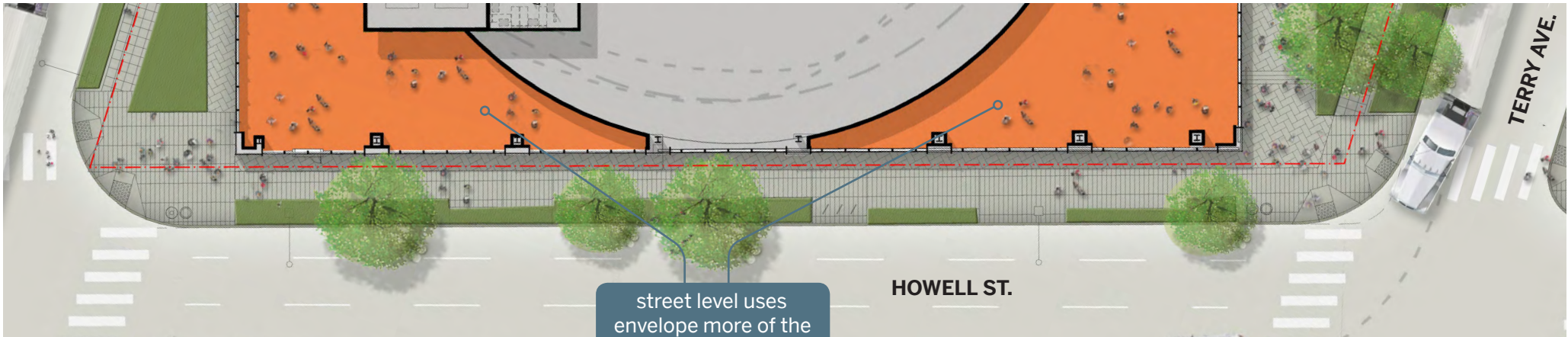
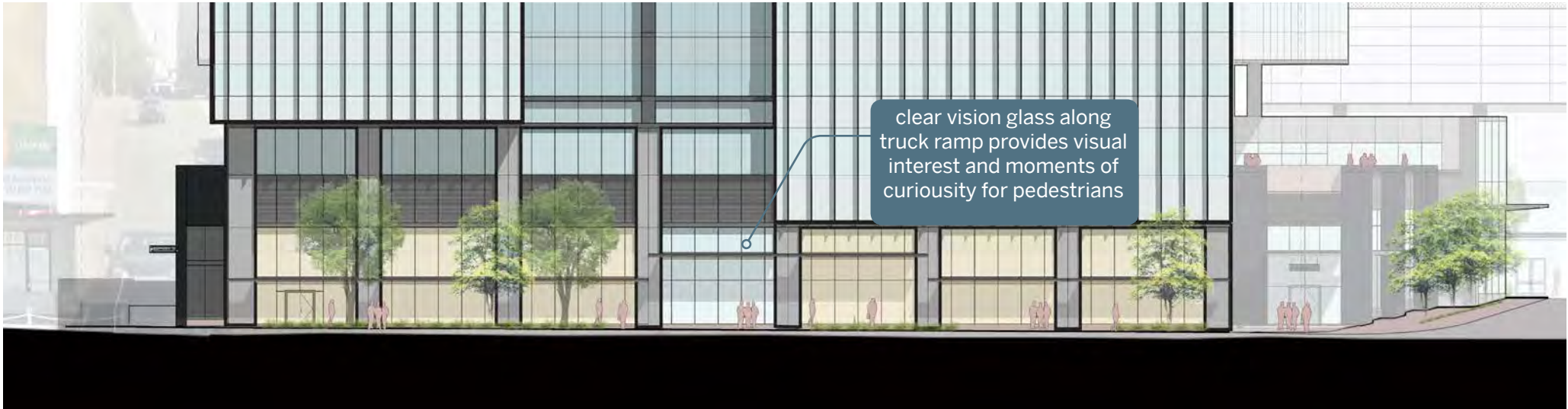
ENLARGED ELEVATION AT HOWELL ST.



3 EDG 03 COMMENT RESPONSES

SITE C - OFFICE

RECOMMENDATION



ENLARGED ELEVATION AT HOWELL ST.





### 3 EDG 03 COMMENT RESPONSES SITE C - OFFICE

#### COMMENT 11G - TERRY AVE.

At the EDG#3, the Board applauded the sizable decreases in the two truck portals (48 ft on Boren; 26 ft on Terry), and supported the approach of integrating and recessing the loading doors into the podium bays, as shown on pg 58/59. See 10e for office podium comments.

**RESPONSE** - The loading doors remain recessed from the building facade and are the same width as proposed at the EDG 03. In addition, the loading doors are now more integrated into the rhythm of the podium facade.

#### EDG 03



ENLARGED ELEVATION AT BOREN AVE.

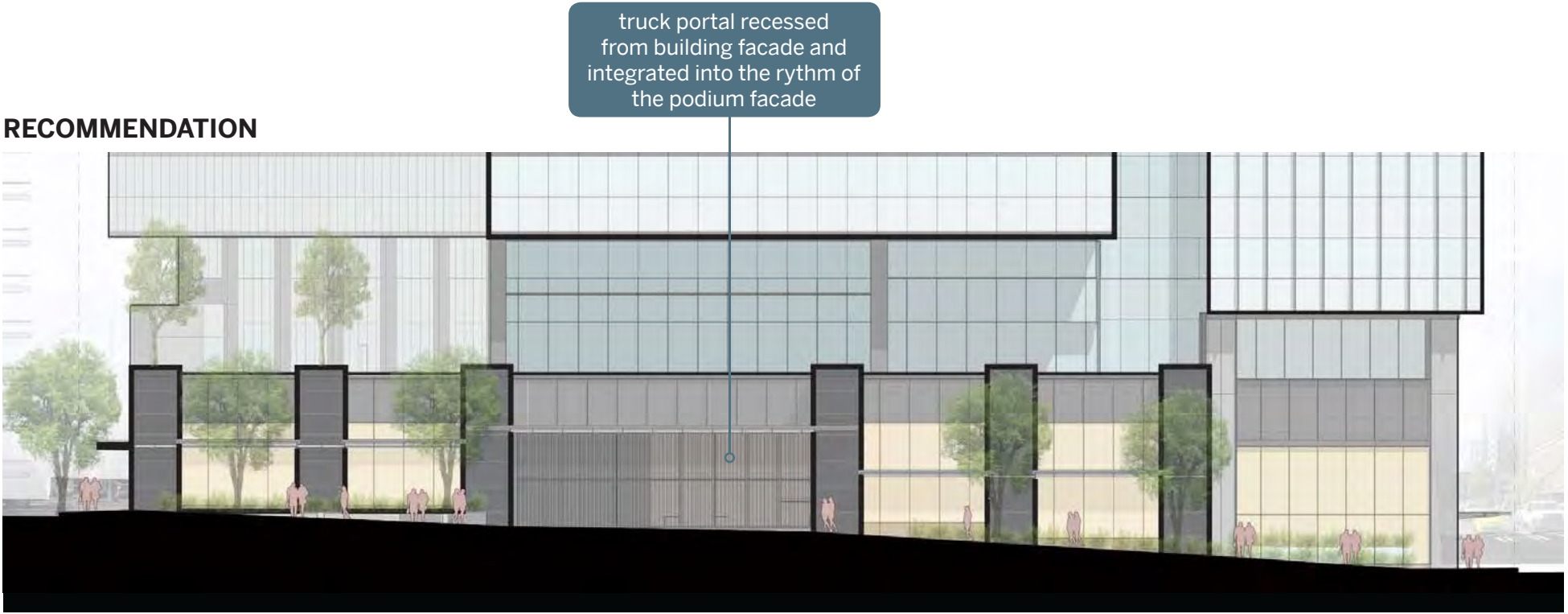


STREET LEVEL VIEW LOOKING EAST ON TERRY AVE.



# 3 EDG 03 COMMENT RESPONSES SITE C - OFFICE

## RECOMMENDATION



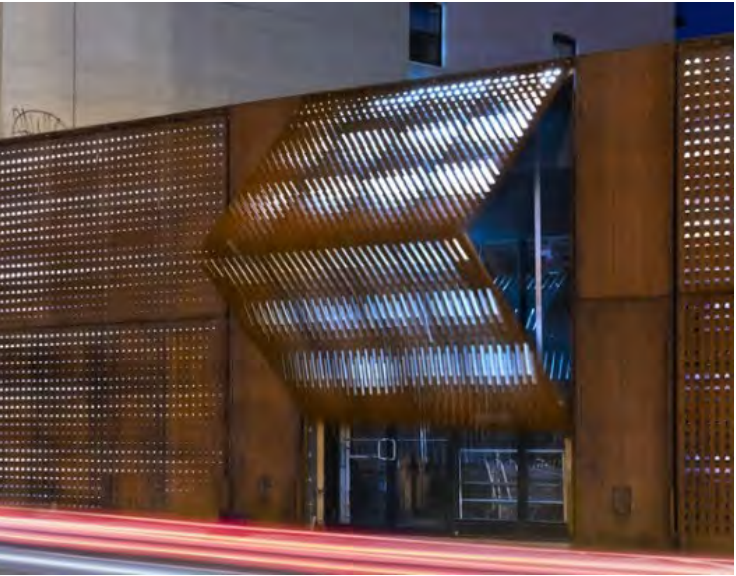
ENLARGED ELEVATION AT BOREN AVE.



STREET LEVEL VIEW LOOKING EAST ON TERRY AVE.



GARAGE DOOR MATERIAL PRECEDENTS FOR BOREN AVENUE





### 3 EDG 03 COMMENT RESPONSES

SITE C - OFFICE

**COMMENT 12B - GENERAL**

At the EDG#3, the board summarized that the project’s site plan and massing have improved, the corners along Olive and Boren are stronger, and that some areas of the transparency are adequate. However, the Board recommended that the other areas need additional refinement. Several key edges of the ground floor perimeter need more attention. The lower and middle facade along Boren should be much more visually compelling. The Board stated the massing refinements to the Co-development blocks are going in the right direction, but need further refinement, especially at the podium, gasket and facades along Howell and Olive for block C.

**RESPONSE** - The Howell Street facade has been updated with a stronger two story podium expression with a transition to a one story expression to help break up the overall length of the facade. The podium along Olive Way and Boren Ave. has been updated to read as a two story height with a strong rhythm of one story stone piers accentuated with trees to create a second story canopy.

EDG 03



ELEVATION AT HOWELL ST.



RECOMMENDATION



ELEVATION AT HOWELL ST.

two story podium  
expression with  
transition to a one  
story expression to  
break up length of  
facade

RECOMMENDATION



ELEVATION AT BOREN AVE.

piers accentuated  
with trees to create  
second story canopy

trees aligned with  
piers create strong  
rhythm along facade



# 3 EDG 03 COMMENT RESPONSES SITE C - OFFICE

**COMMENT 4A - TOWER MASSING AND PODIUM**

**MUP 01** - The Howell tower facade runs straight down to grade, uninterrupted. Per guidance, redesign to add a legible podium, preferably 2-3 stories in proportion to the 240 ft tall mass above, and with a substantial 'gasket' or setback at the transition.

**COMMENT 5B - LACK OF TOWER MODULATION**

**MUP 01** - The approx. 2 ft deep divot with canted wall plane proposed is barely perceptible on the 120x 240 ft flat Howell wall (which is NW facing and will not benefit from much shadow-play); this is contrary to the explicit DRB guidance under Departure #3/pg 41 "one larger & deeper (than 3 ft) recess" and the recess "should extend to the parapet" to make a legible break at the roof edge. This departure/modulation proposal is going backwards from DRB guidance and is not sufficient on such a large, undifferentiated plane; Redesign with a substantially deeper, legible recess to at least partially comply with Code.

**MUP 02** - The revised, singular and wider recess as shown on g106 is an improvement, however the cladding inside the recess is identical to the adjacent walls; revise to provide some contrast and improve legibility of this modulation. also, the 6 ft depth is less than half the 15 ft code requirement; the DRB will evaluate.

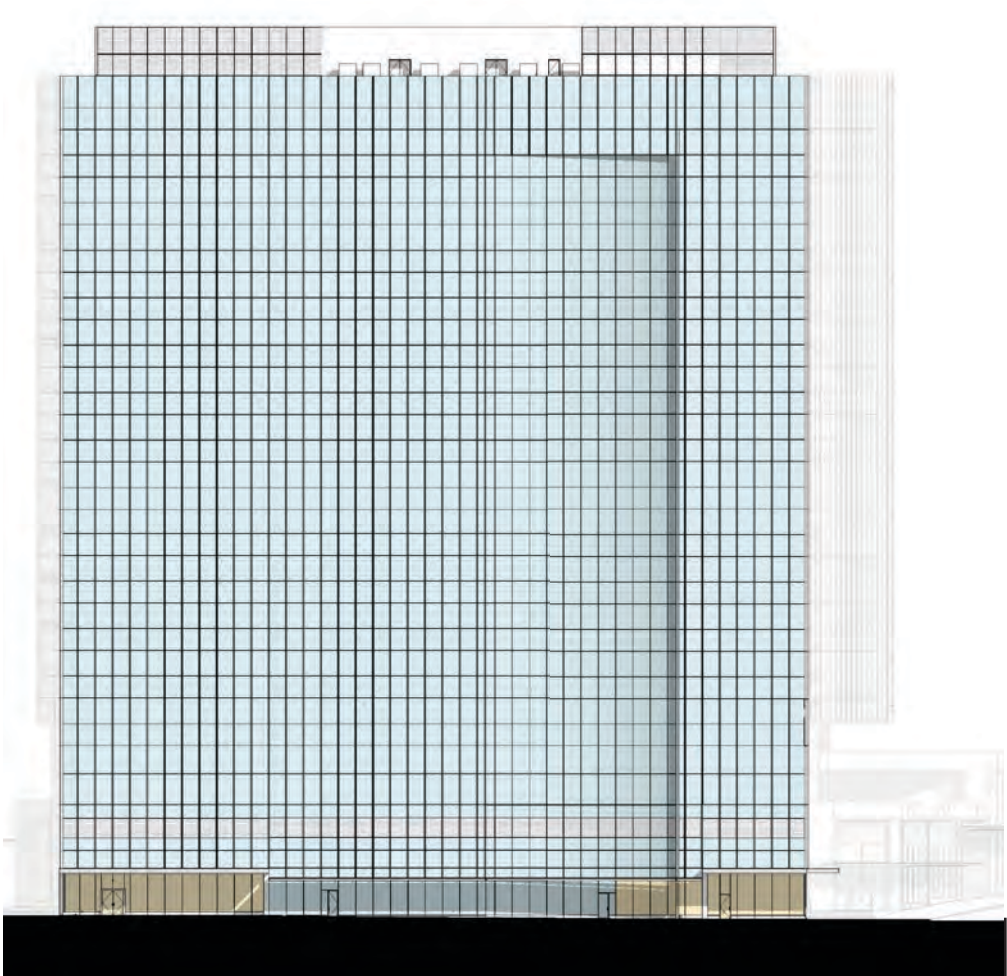
**RESPONSE** - The Howell Street facade has been updated with a stronger two story podium expression with a transition to a one story expression to bring more variety to the facade closer to the street level. A larger single 6 feet recess is being proposed above the podium expression to break the length of the facade. The facade east of the recess is located at the property line while the facade west of the recess is 3 feet back from the property line. The facade is broken into three pieces on three different planes and breaks the facade down so that the proportions closely replicate the scale of the buildings across Howell St. A vertical expression is created by providing a 4" fin at every mullion, except at the recessed facade where the glass will be neutral.

EDG 03



ELEVATION AT HOWELL ST.

MUP



ELEVATION AT HOWELL ST.

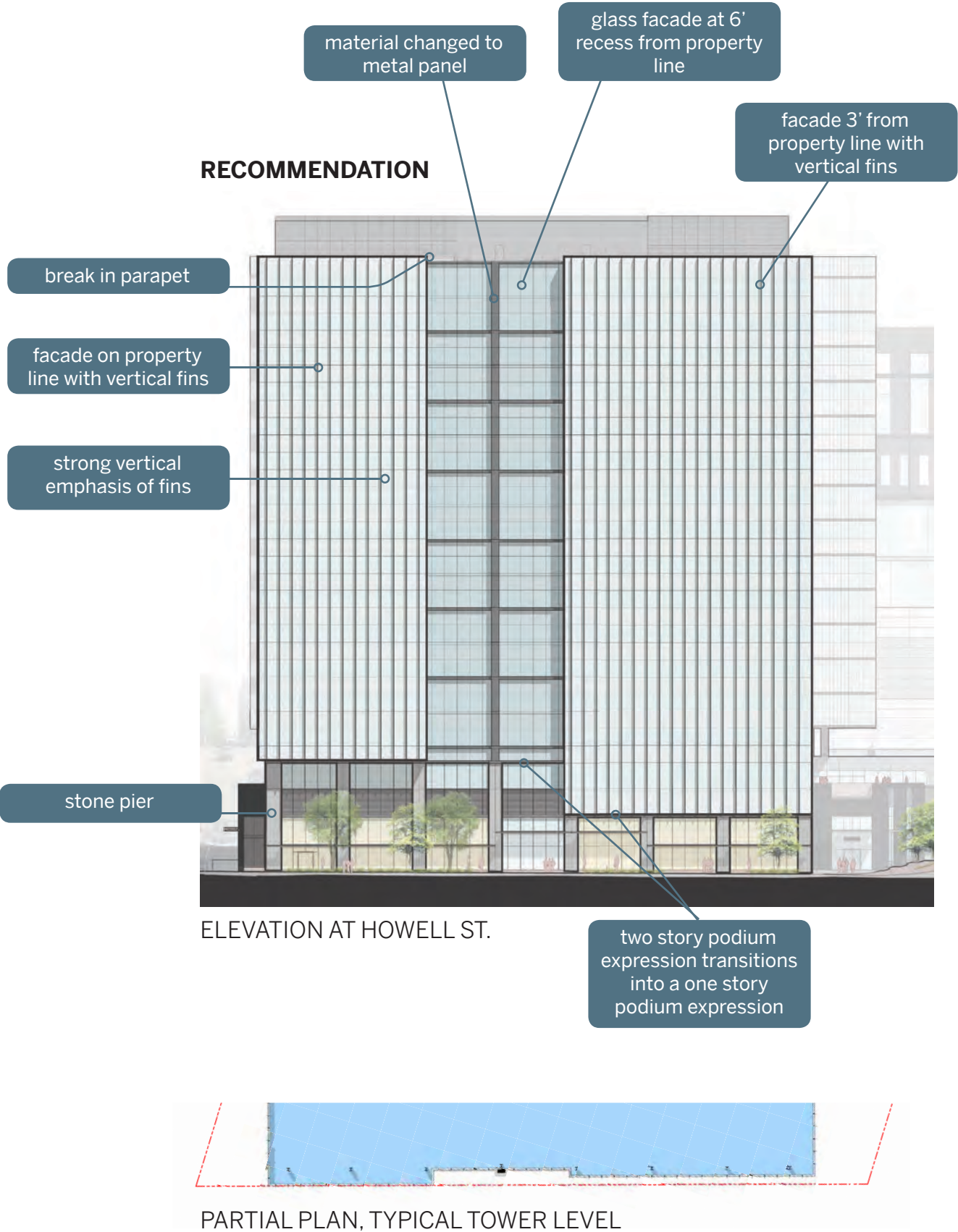


3 EDG 03 COMMENT RESPONSES

SITE C - OFFICE



HOWELL ST. FACADE STUDIES





# 4 DESIGN RECOMMENDATION



## 4 DESIGN RECOMMENDATION SITE PLAN







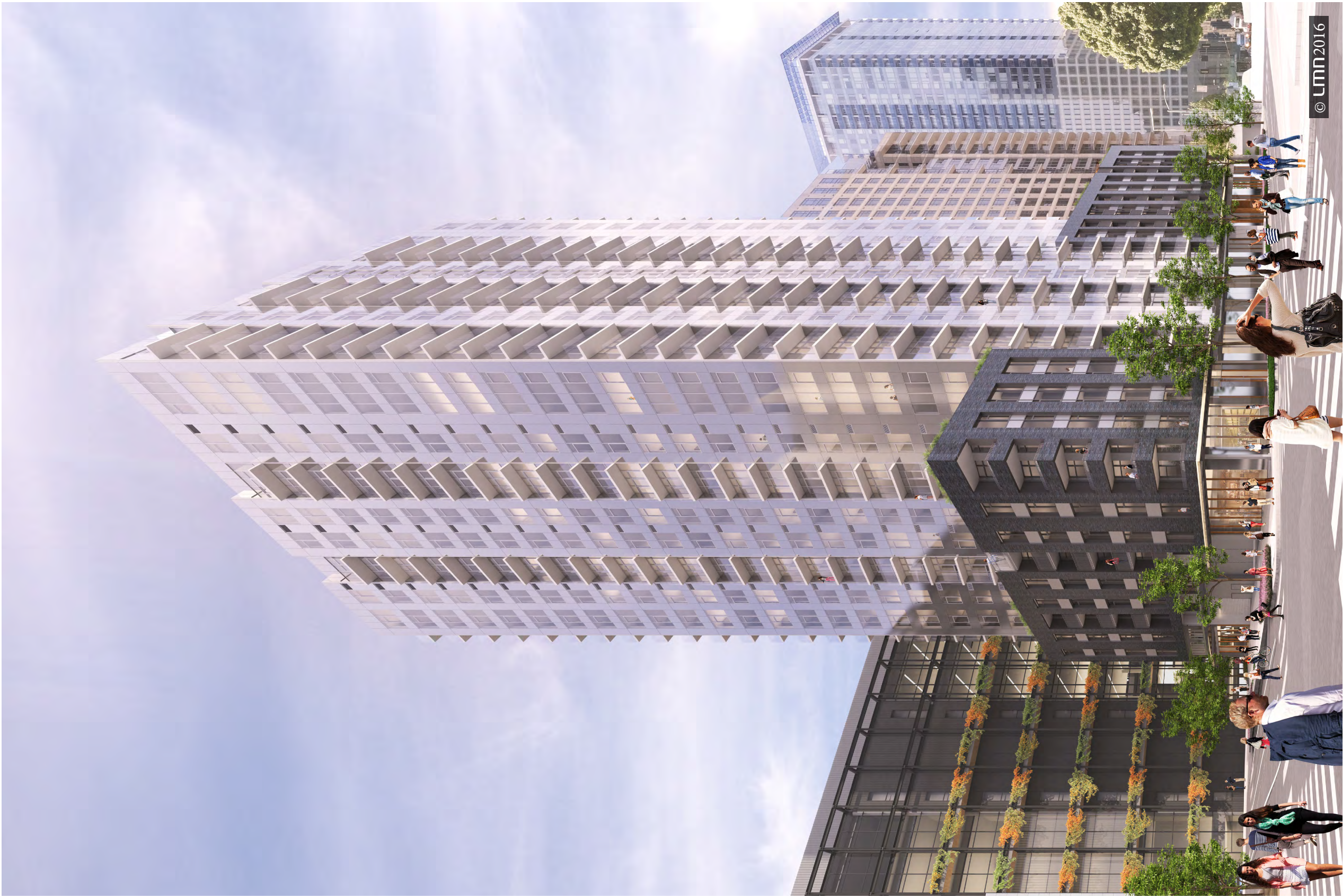
VIEW LOOKING SOUTHEAST AT 9TH AVE. AND HOWELL STREET





VIEW LOOKING NORTHEAST AT 9TH AVE. AND OLIVE WAY





VIEW LOOKING SOUTHWEST AT TERRY AVE. AND HOWELL ST.





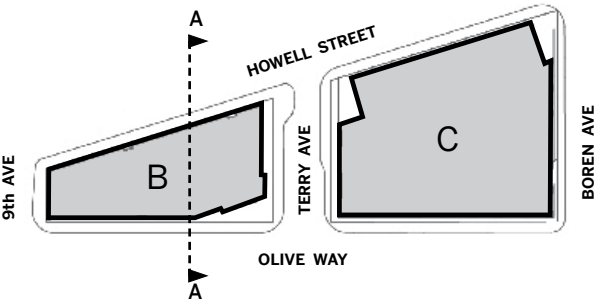
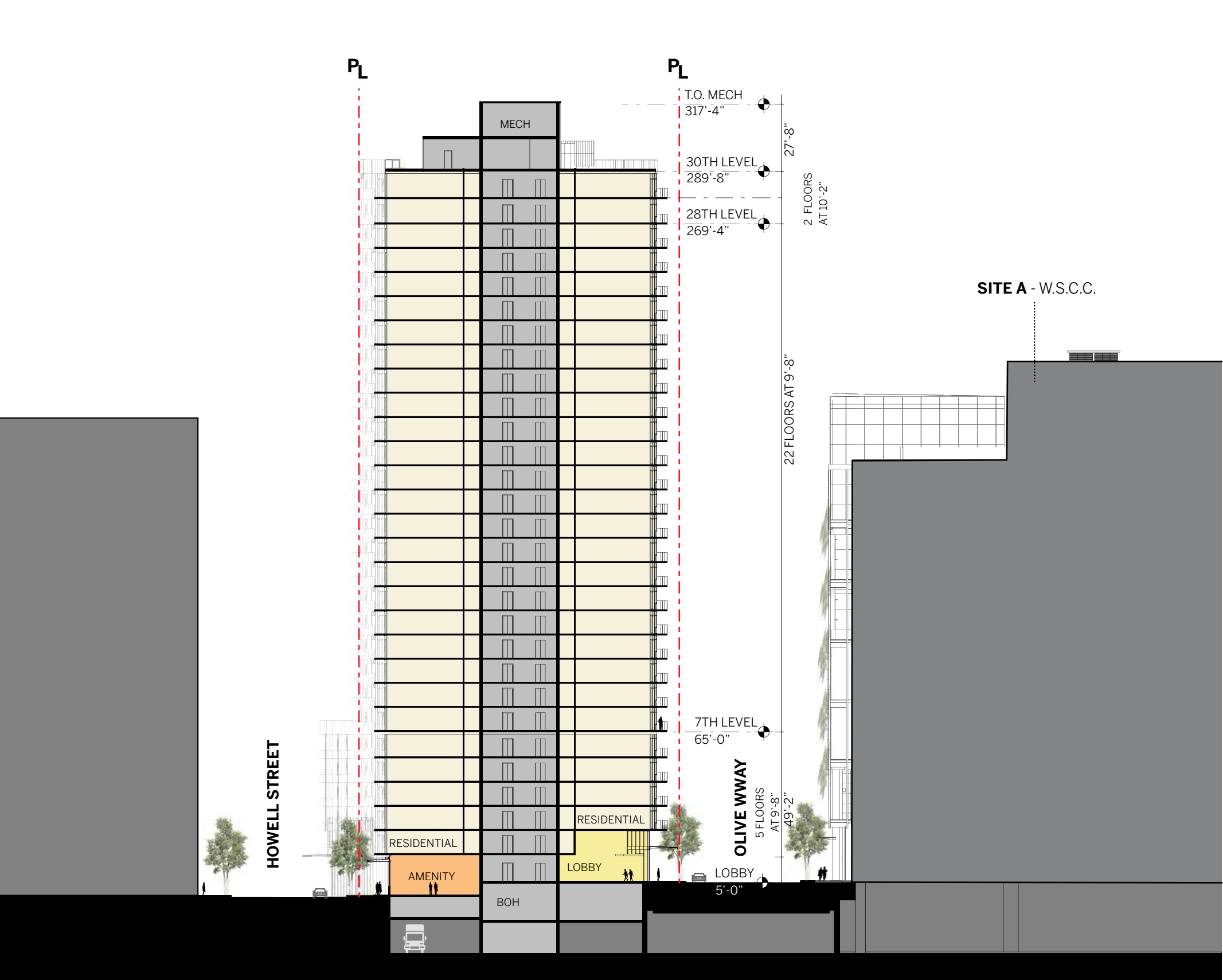
VIEW LOOKING NORTHWEST AT TERRY AVE. AND OLIVE WAY



4 DESIGN RECOMMENDATION

SECTIONS

SITE B - RESIDENTIAL

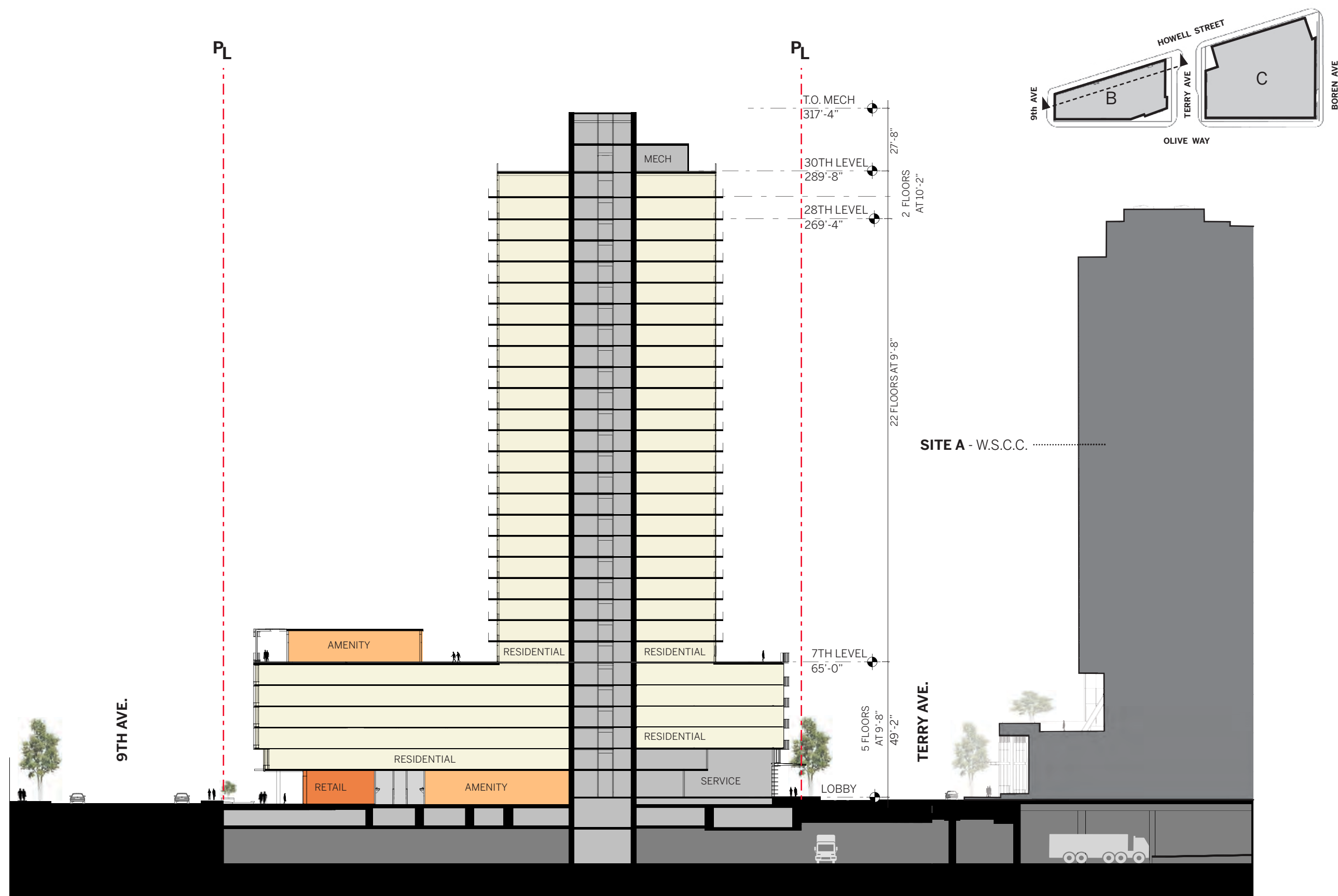




4 DESIGN RECOMMENDATION

SECTIONS

SITE B - RESIDENTIAL





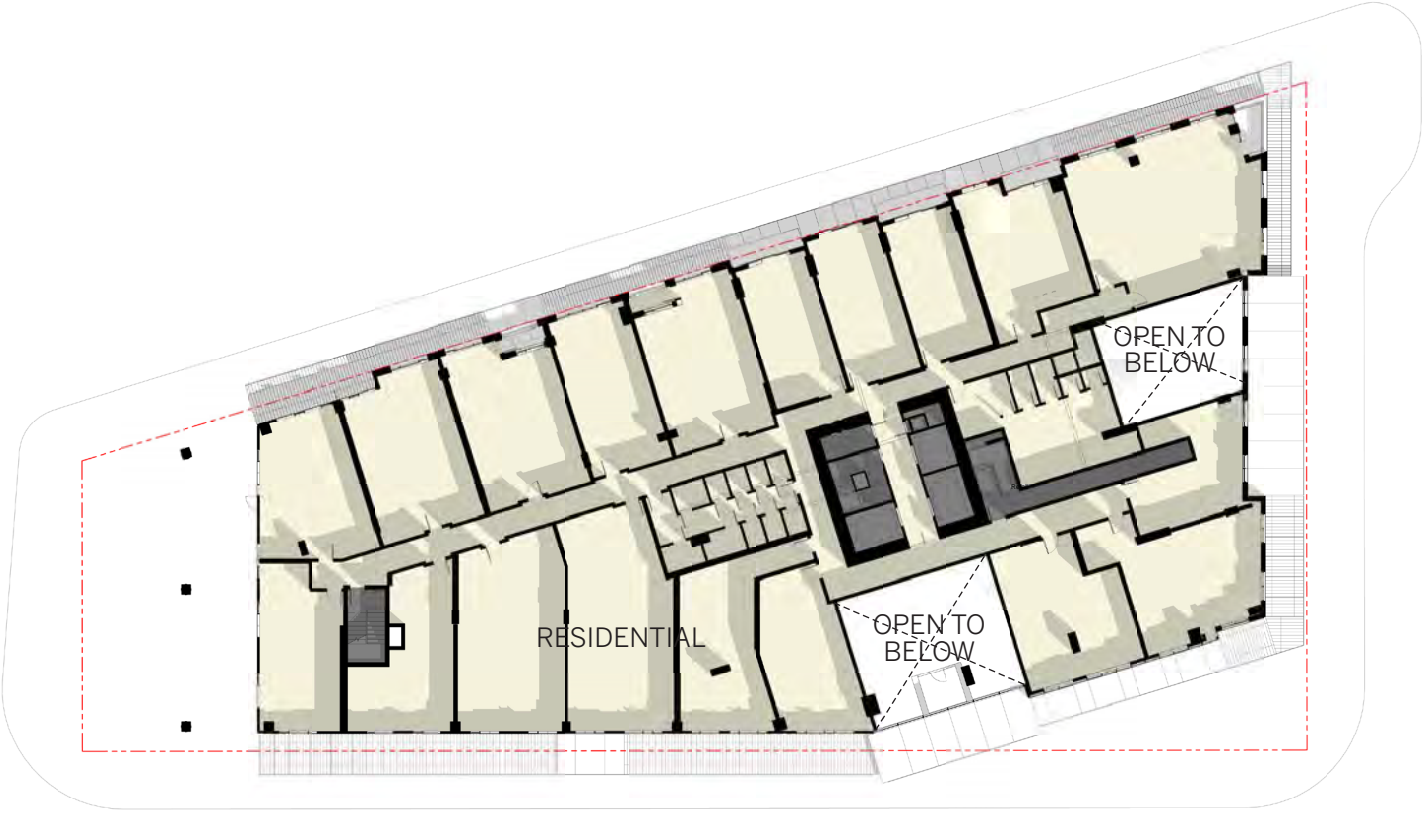
4 DESIGN RECOMMENDATION

FLOOR PLANS

SITE B - RESIDENTIAL



LEVEL 1

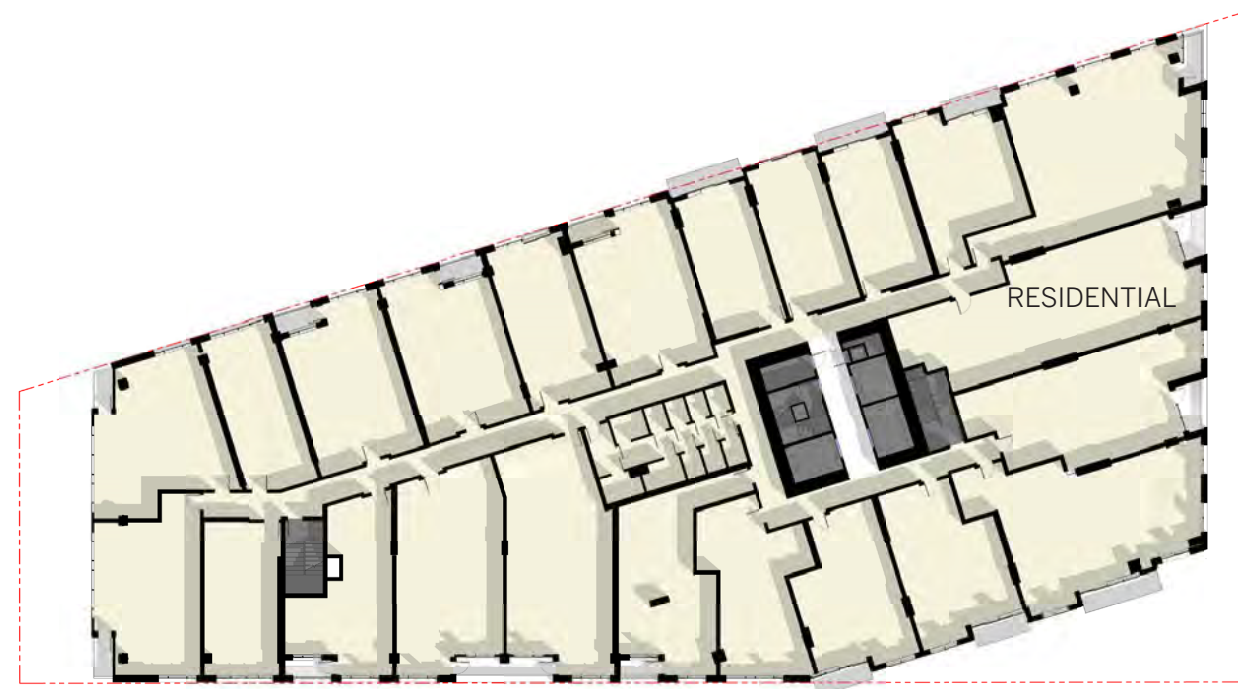


LEVEL 2

\*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2020.



## 4 DESIGN RECOMMENDATION FLOOR PLANS **SITE B** - RESIDENTIAL

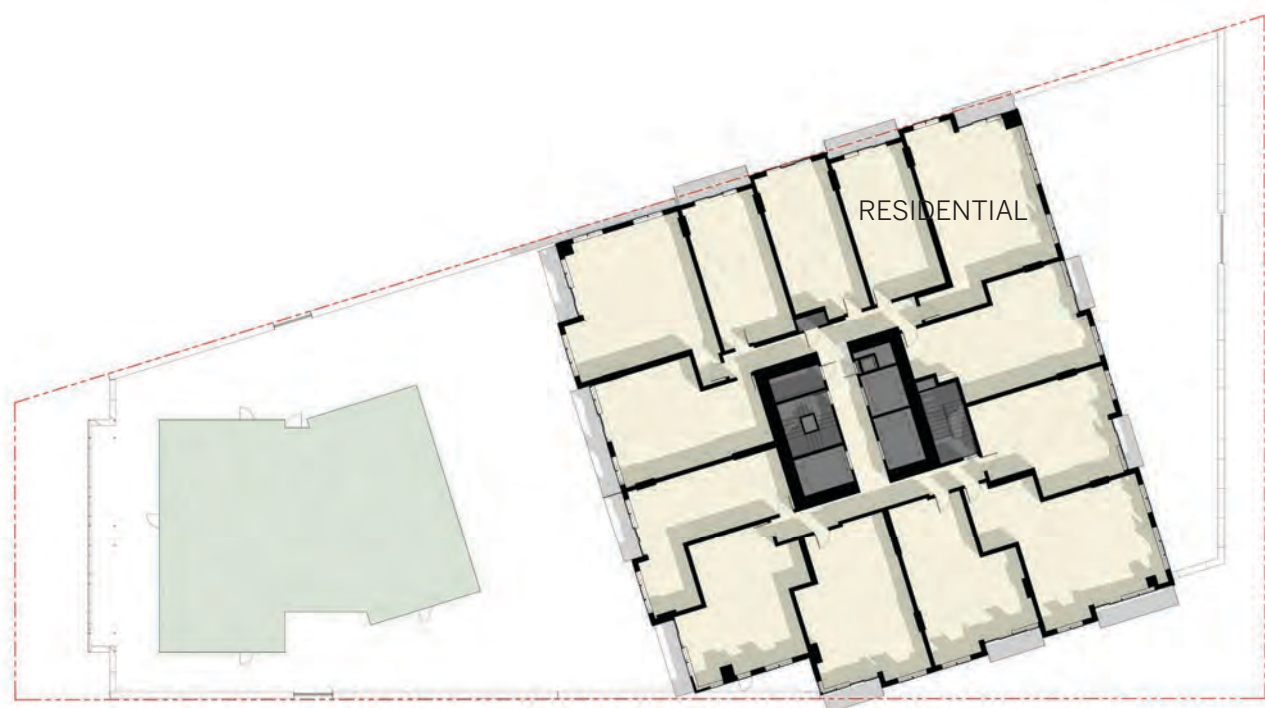


LEVEL 3-6

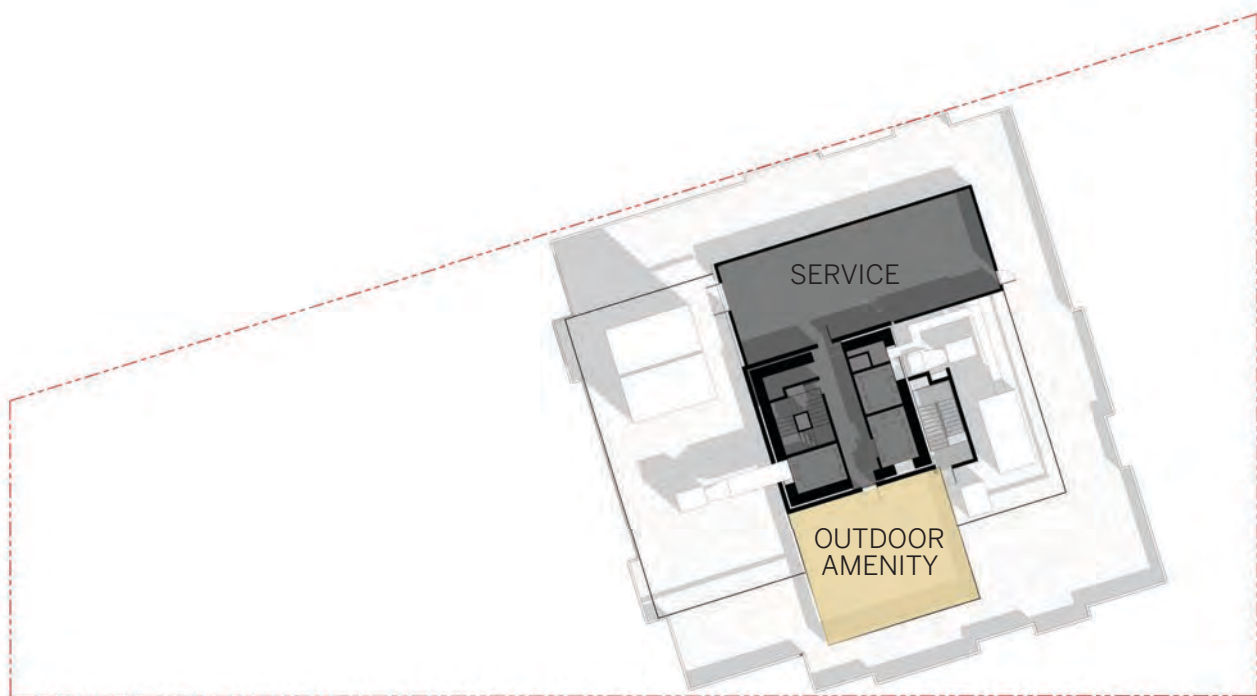


LEVEL 7





LEVEL 8-29



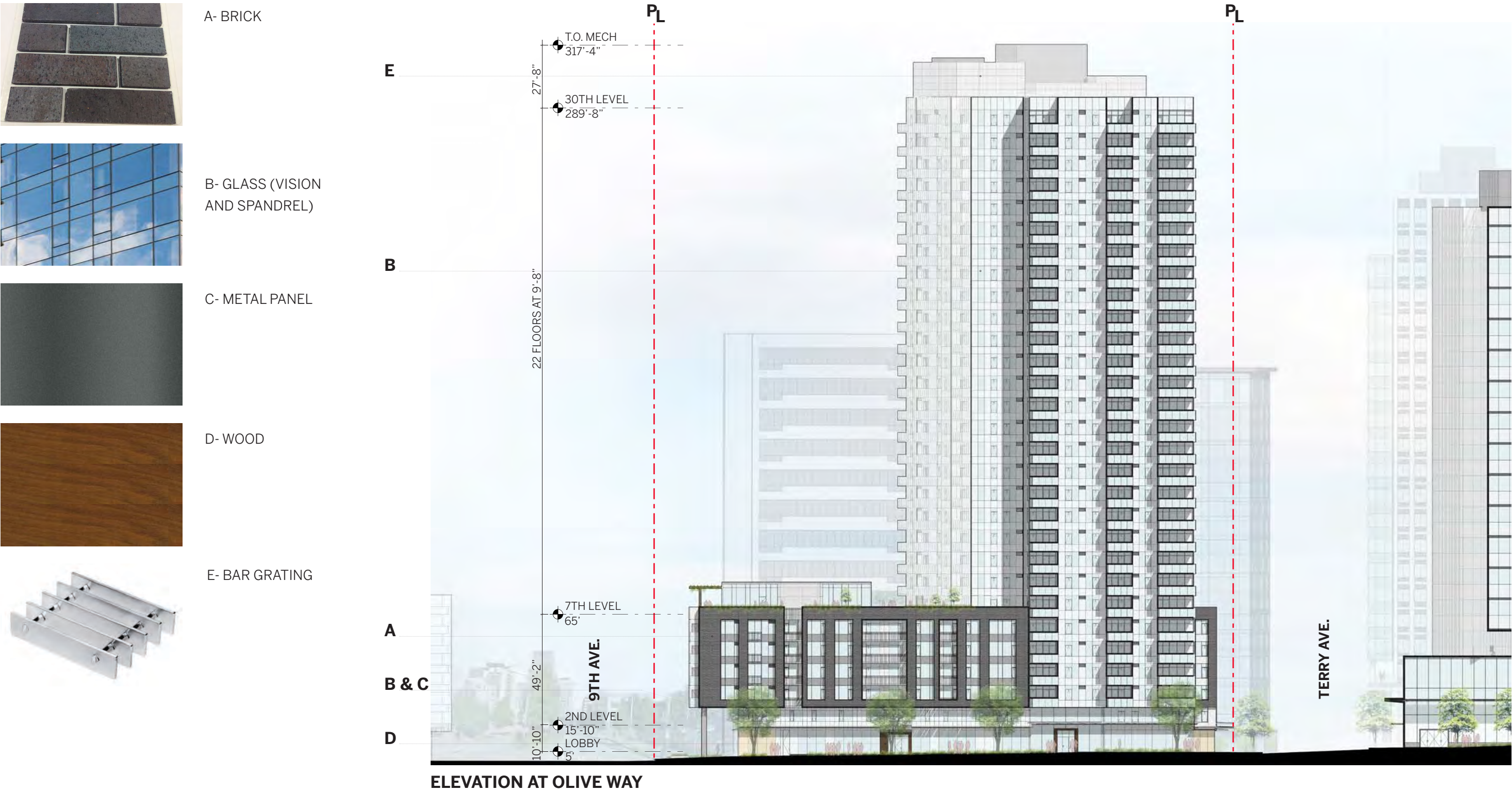
LEVEL 30







# 4 DESIGN RECOMMENDATION ELEVATIONS SITE B - RESIDENTIAL

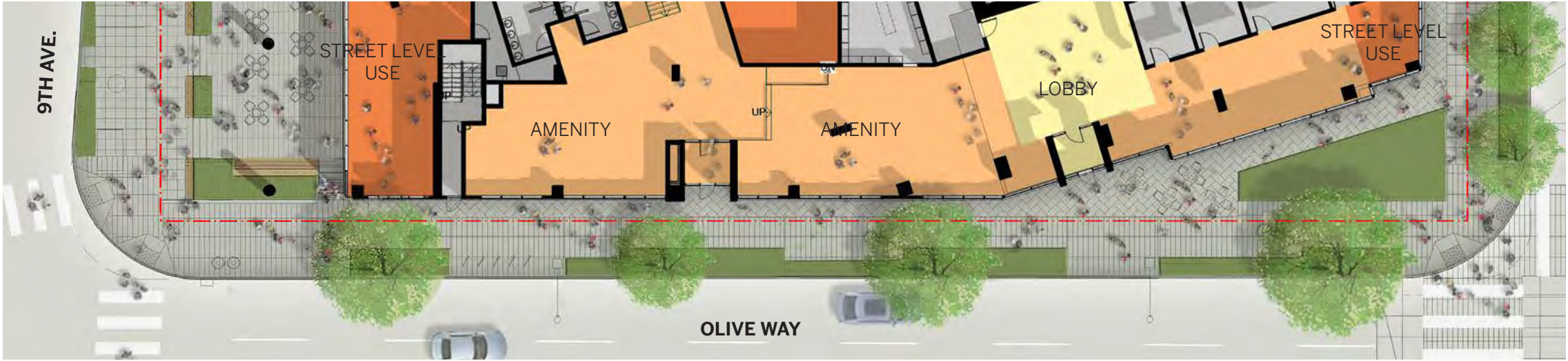




4 DESIGN RECOMMENDATION

ENLARGED ELEVATIONS

SITE B - RESIDENTIAL



\*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2020.



4 DESIGN RECOMMENDATION

ELEVATIONS

SITE B - RESIDENTIAL



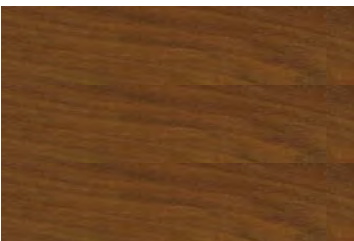
A- BRICK



B- GLASS (VISION  
AND SPANDREL)



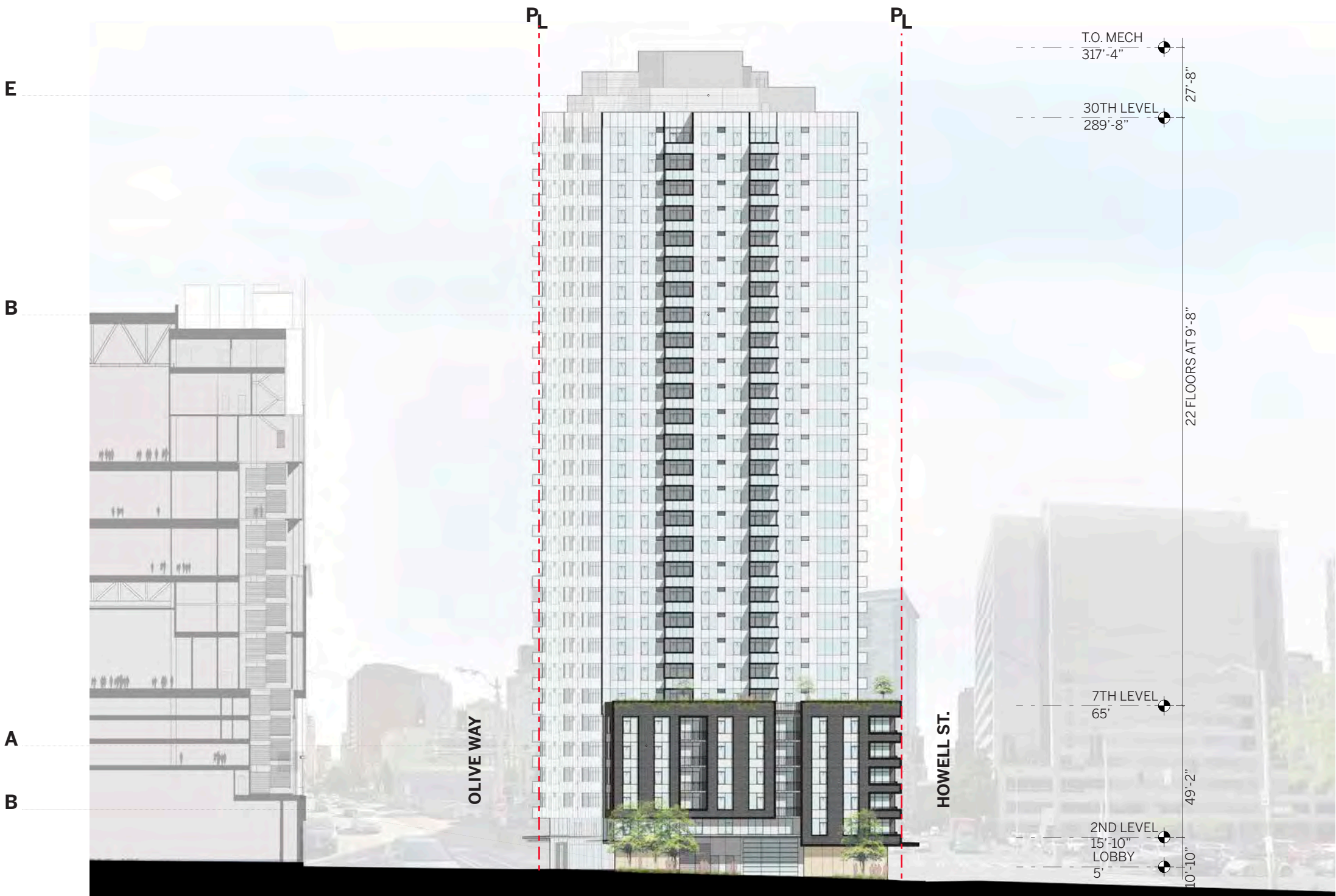
C- METAL PANEL



D- WOOD

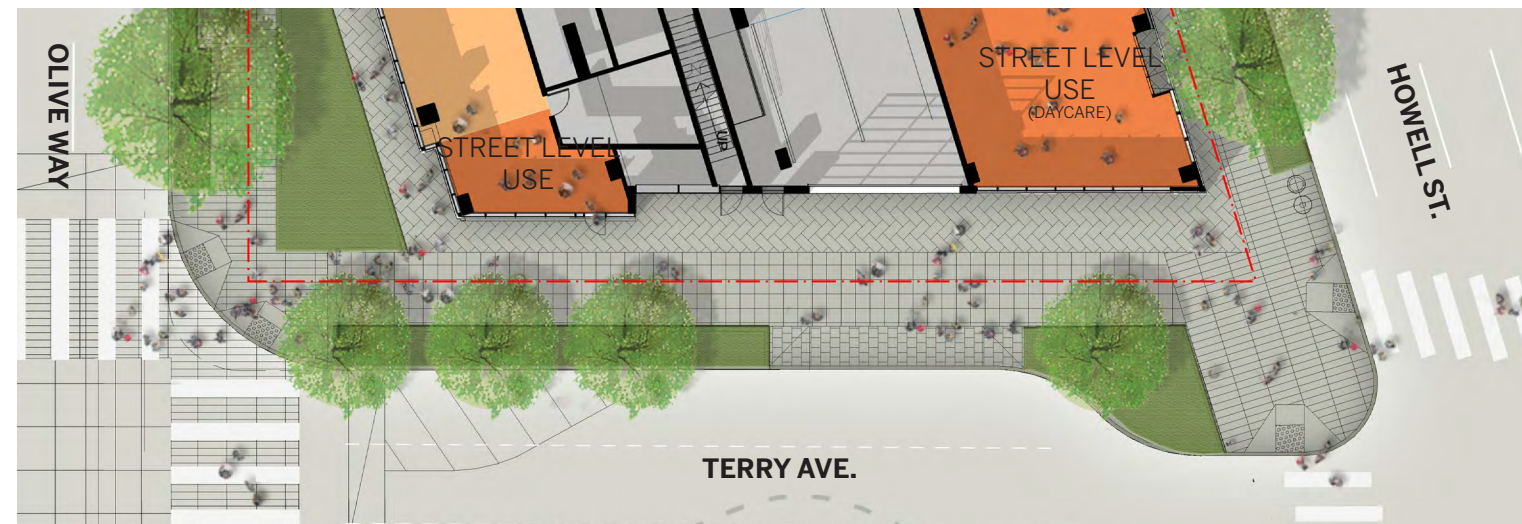


E- BAR GRATING





## 4 DESIGN RECOMMENDATION ENLARGED ELEVATIONS **SITE B** - RESIDENTIAL



\*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2020.





4 DESIGN RECOMMENDATION

ELEVATIONS

SITE B - RESIDENTIAL



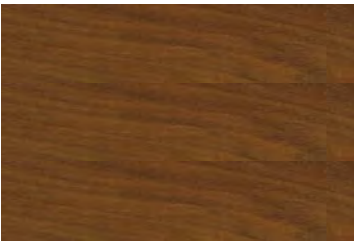
A- BRICK



B- GLASS (VISION  
AND SPANDREL)



C- METAL PANEL



D- WOOD



E- BAR GRATING



ELEVATION AT 9TH AVE.



4 DESIGN RECOMMENDATION

ENLARGED ELEVATIONS

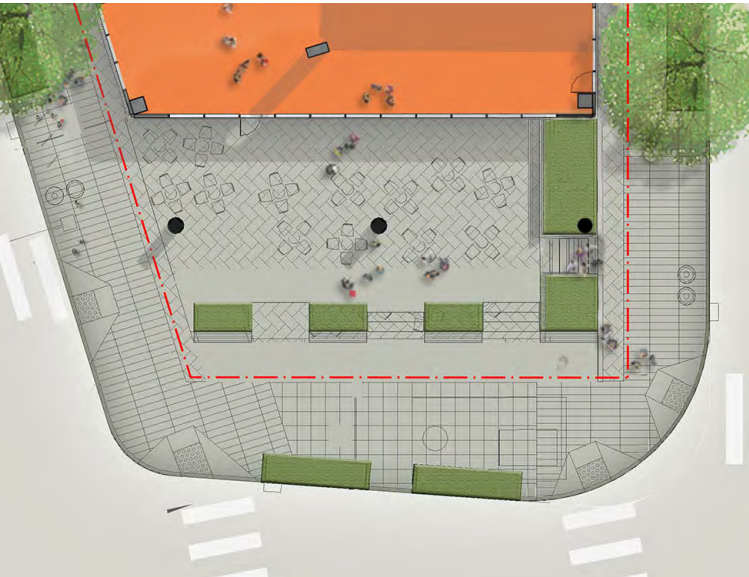
SITE B - RESIDENTIAL



PREFERRED PLAZA DESIGN



ALTERNATE PLAZA DESIGN



\*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2020.





4 DESIGN RECOMMENDATION

ELEVATIONS

SITE B - RESIDENTIAL



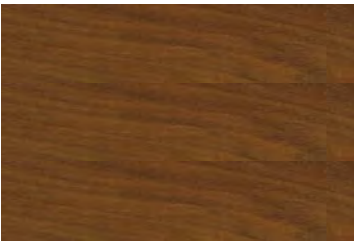
A- BRICK



B- GLASS (VISION  
AND SPANDREL)



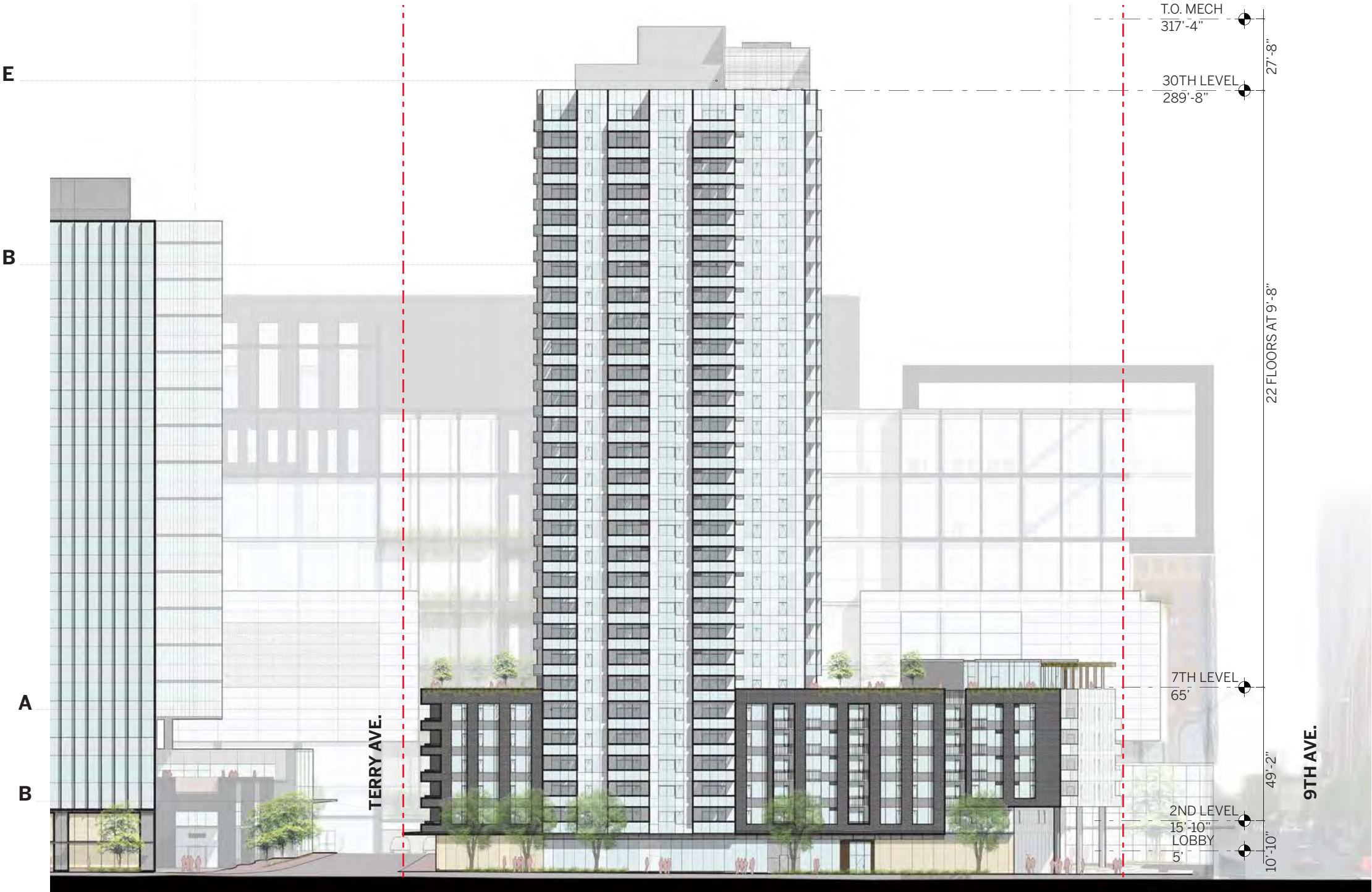
C- METAL PANEL



D- WOOD



E- BAR GRATING

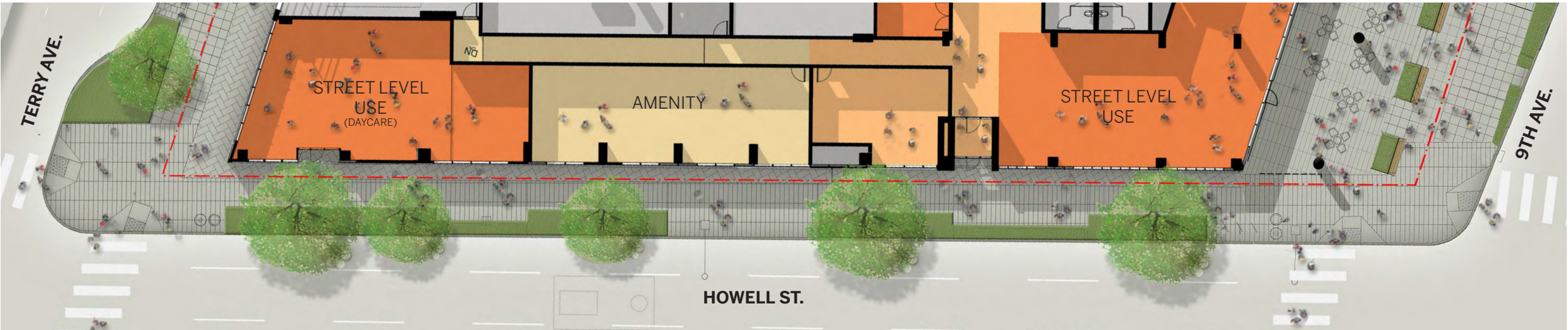




4 DESIGN RECOMMENDATION

ENLARGED ELEVATIONS

SITE B - RESIDENTIAL



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## 4 DESIGN RECOMMENDATION RENDERINGS **SITE B** - RESIDENTIAL



VIEW LOOKING SOUTHEAST AT 9TH AVE. AND HOWELL ST.



## 4 DESIGN RECOMMENDATION RENDERINGS **SITE B** - RESIDENTIAL



VIEW LOOKING SOUTH AT TERRY AVE.



## 4 DESIGN RECOMMENDATION RENDERINGS **SITE B** - RESIDENTIAL



VIEW LOOKING WEST ON OLIVE WAY



## 4 DESIGN RECOMMENDATION RENDERINGS **SITE B** - RESIDENTIAL



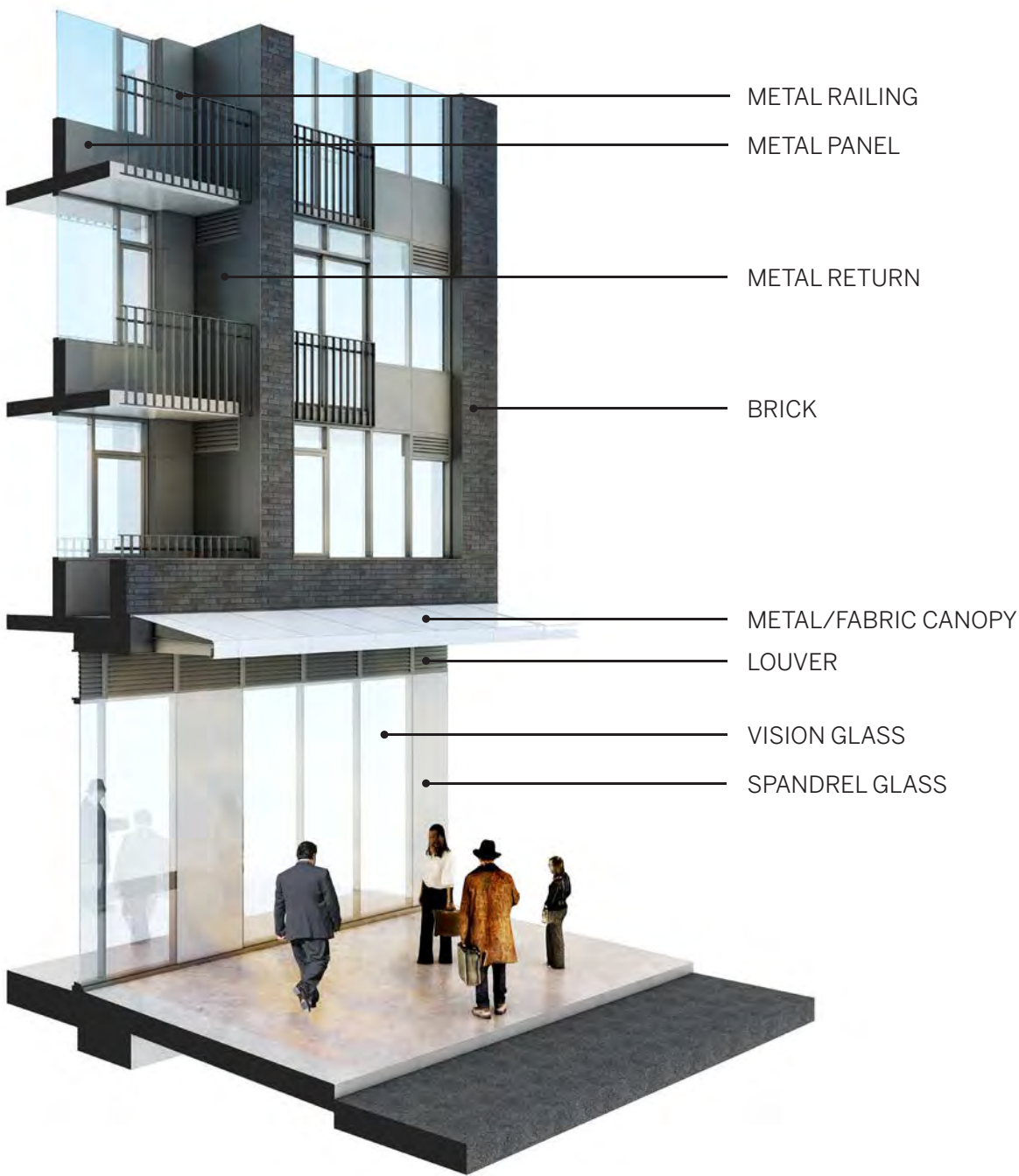
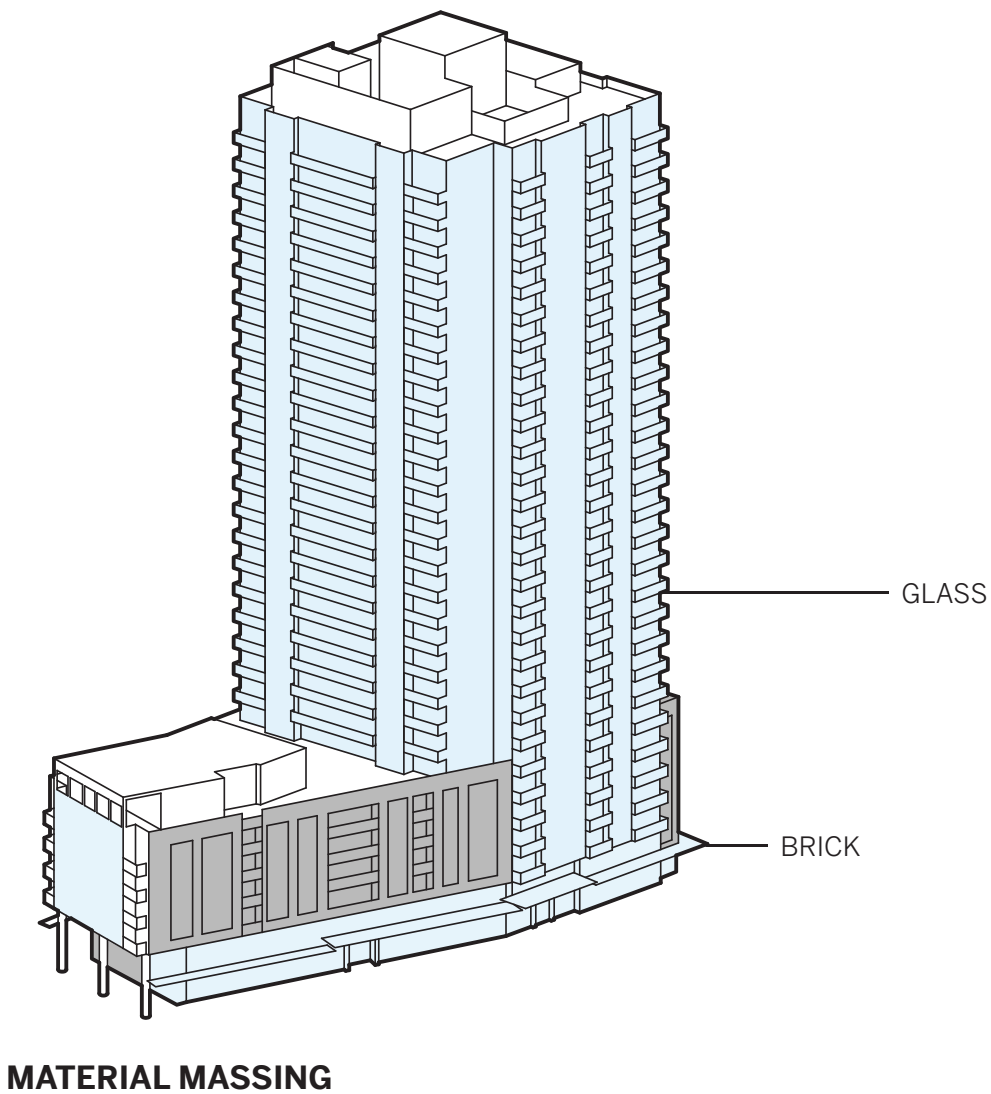
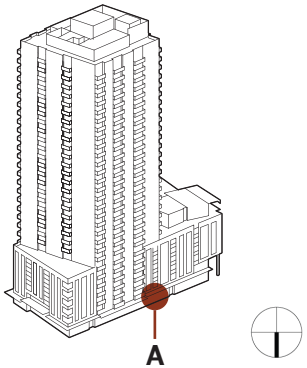
VIEW LOOKING NORTH AT OLIVE WAY



4 DESIGN RECOMMENDATION

MATERIAL CONCEPT

SITE B - RESIDENTIAL



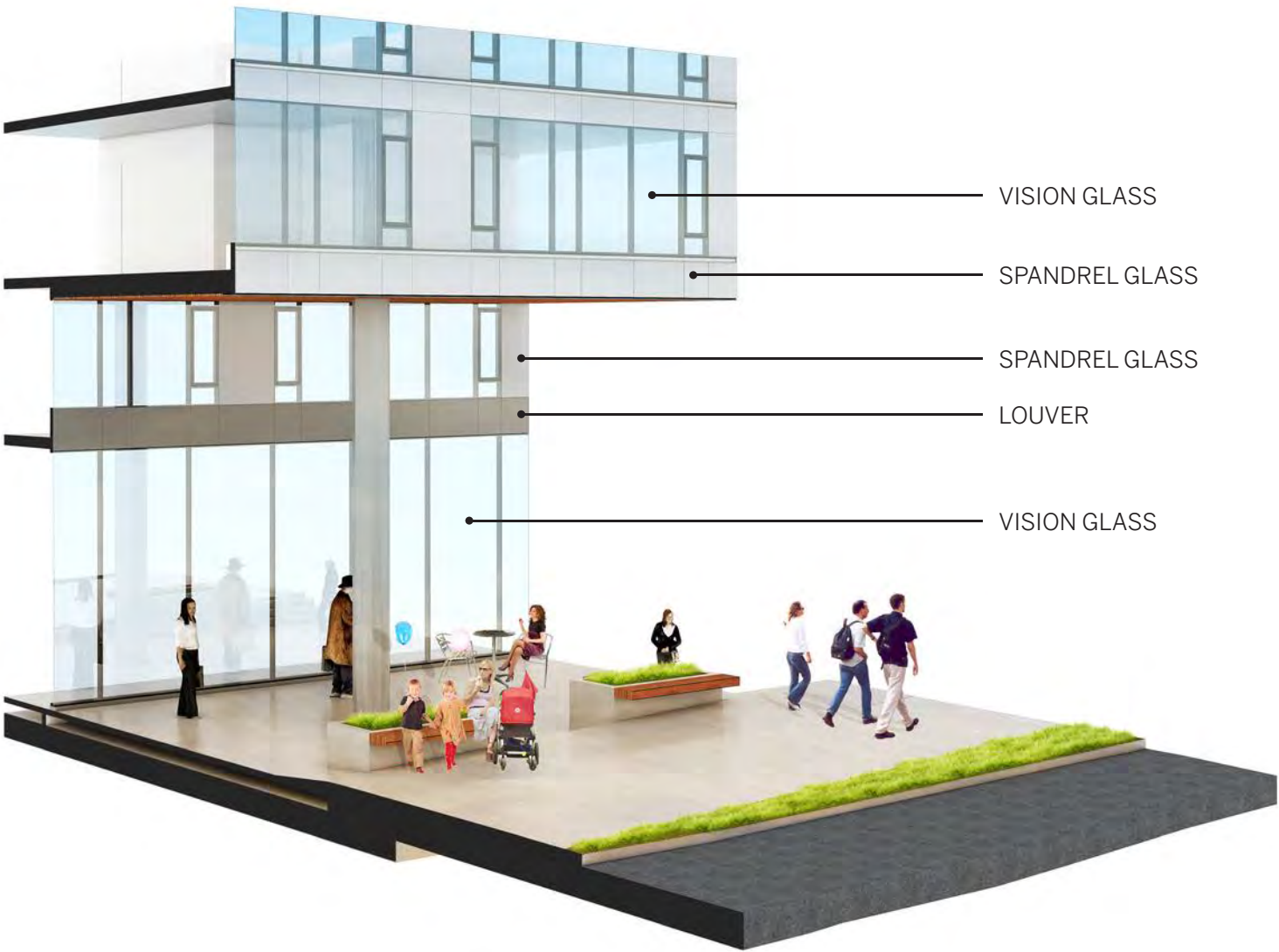
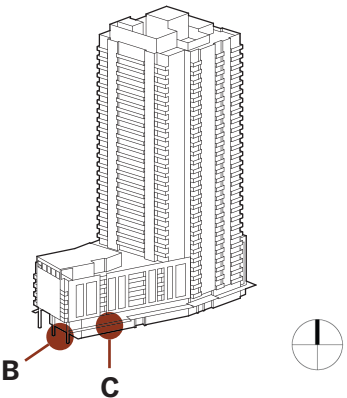
A. HOWELL ST.



4 DESIGN RECOMMENDATION

MATERIAL DETAIL

SITE B - RESIDENTIAL



B. 9TH AVE. PLAZA



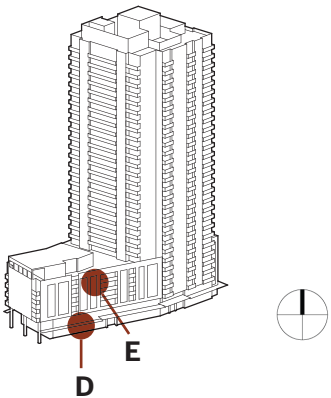
C. OLIVE WAY



4 DESIGN RECOMMENDATION

MATERIAL DETAIL

SITE B - RESIDENTIAL

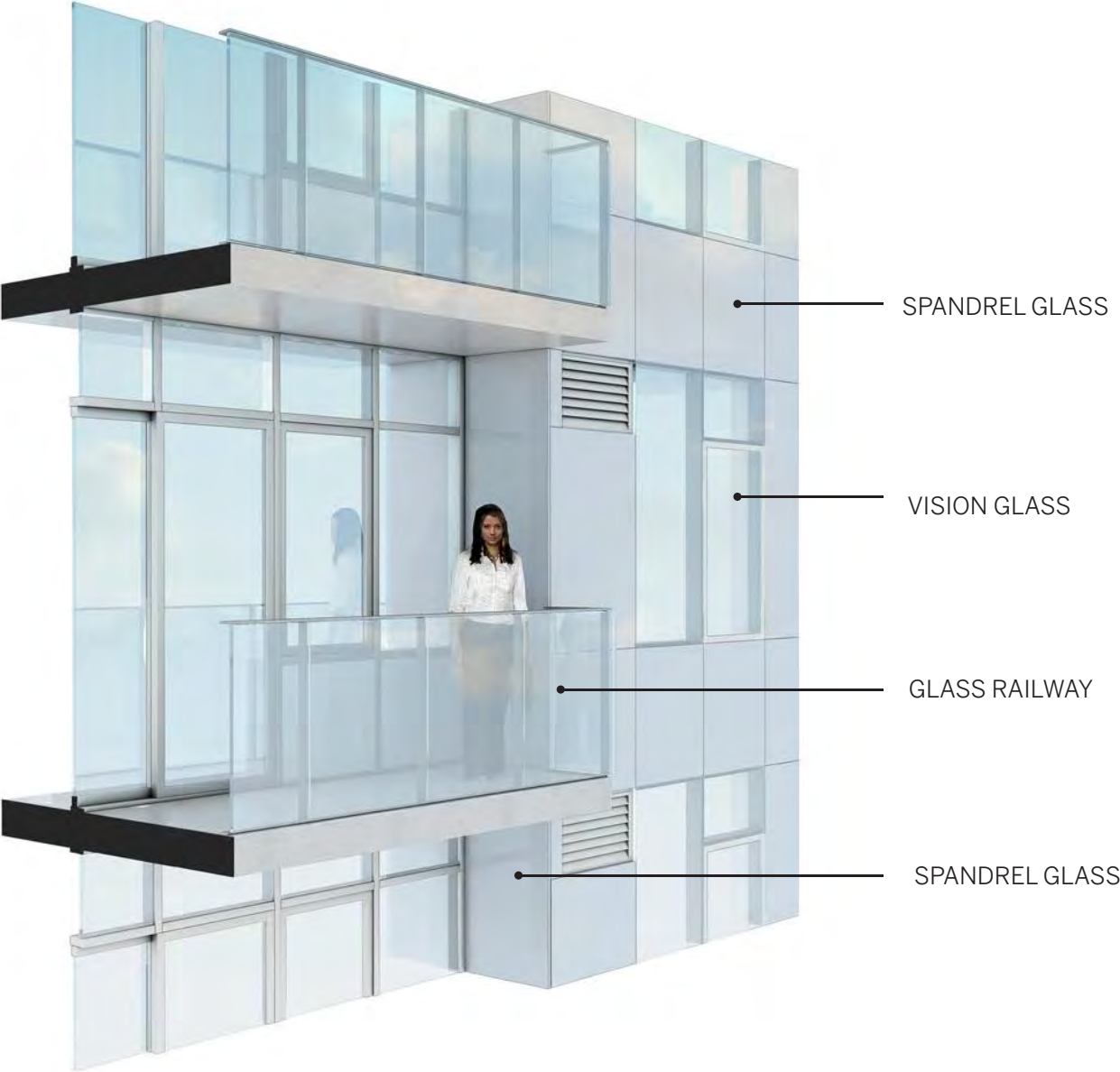
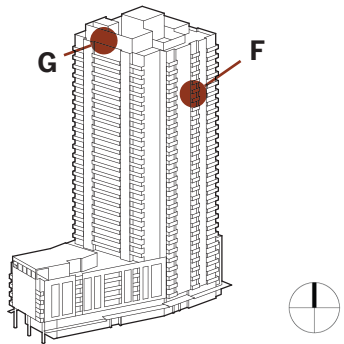


D. SECTION PERSPECTIVE AT TOLIVE WAY ENTRANCE

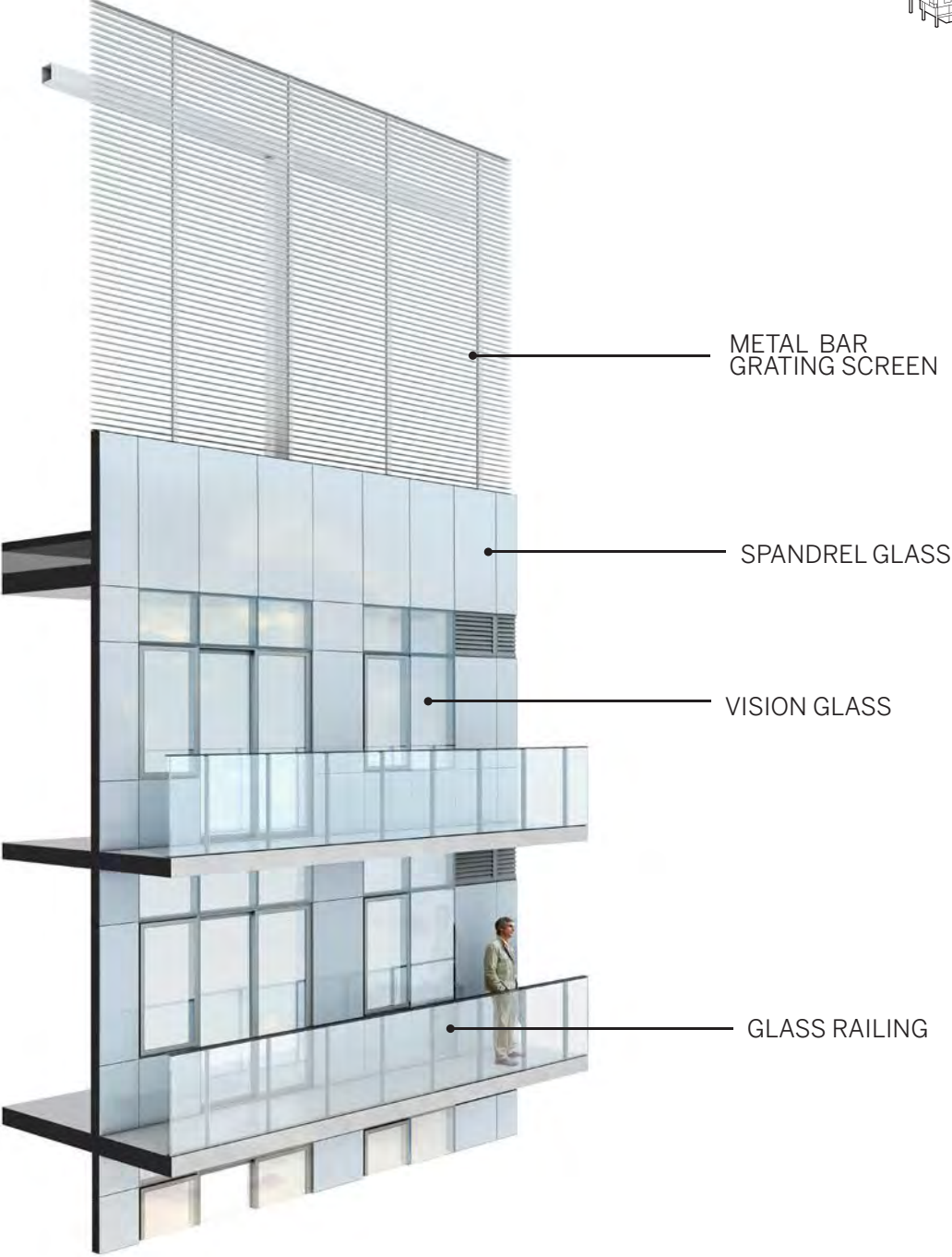


E. SECTION PERSPECTIVE AT TOP OF PODIUM



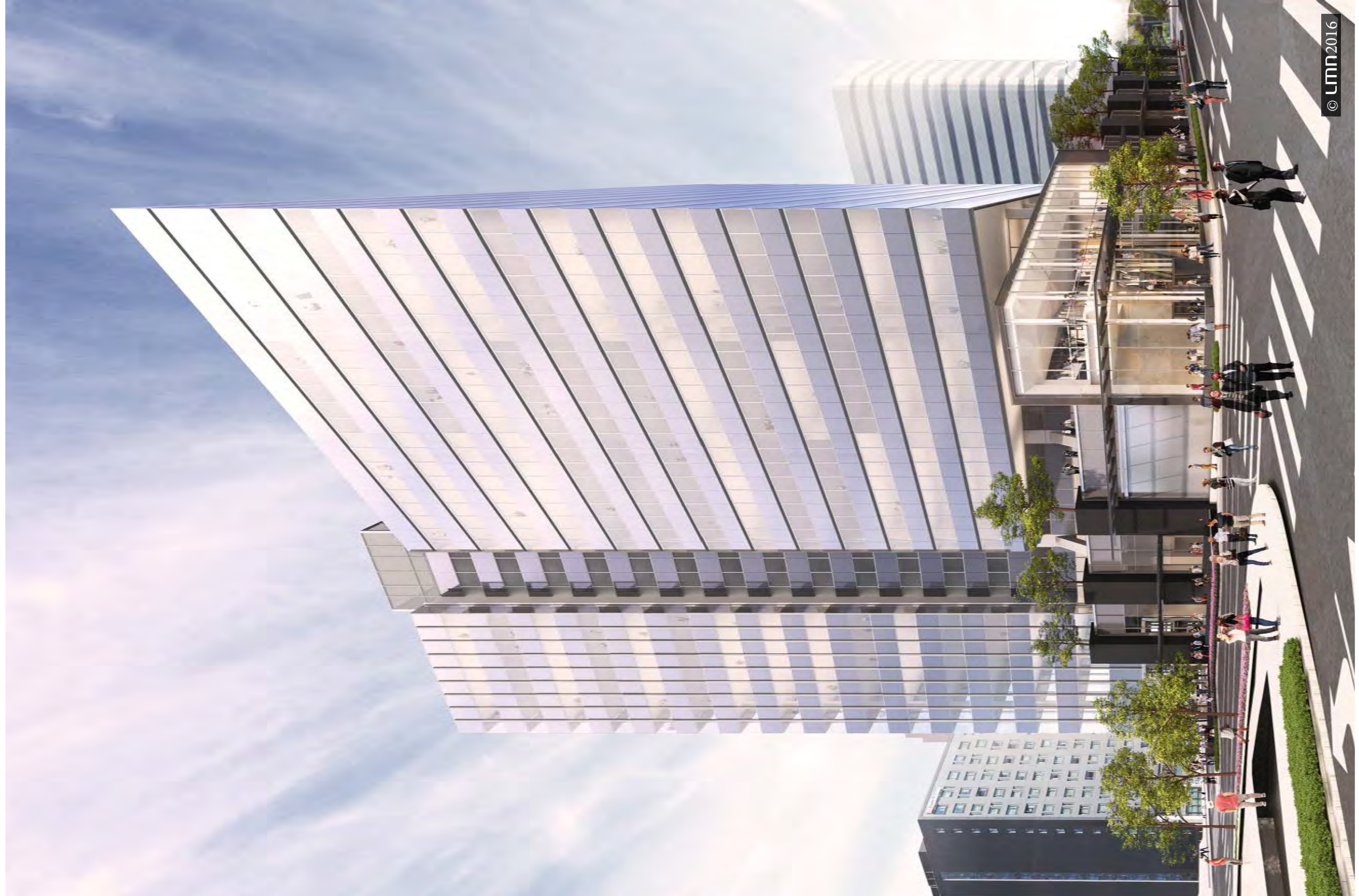


F. SECTION PERSPECTIVE AT TOWER



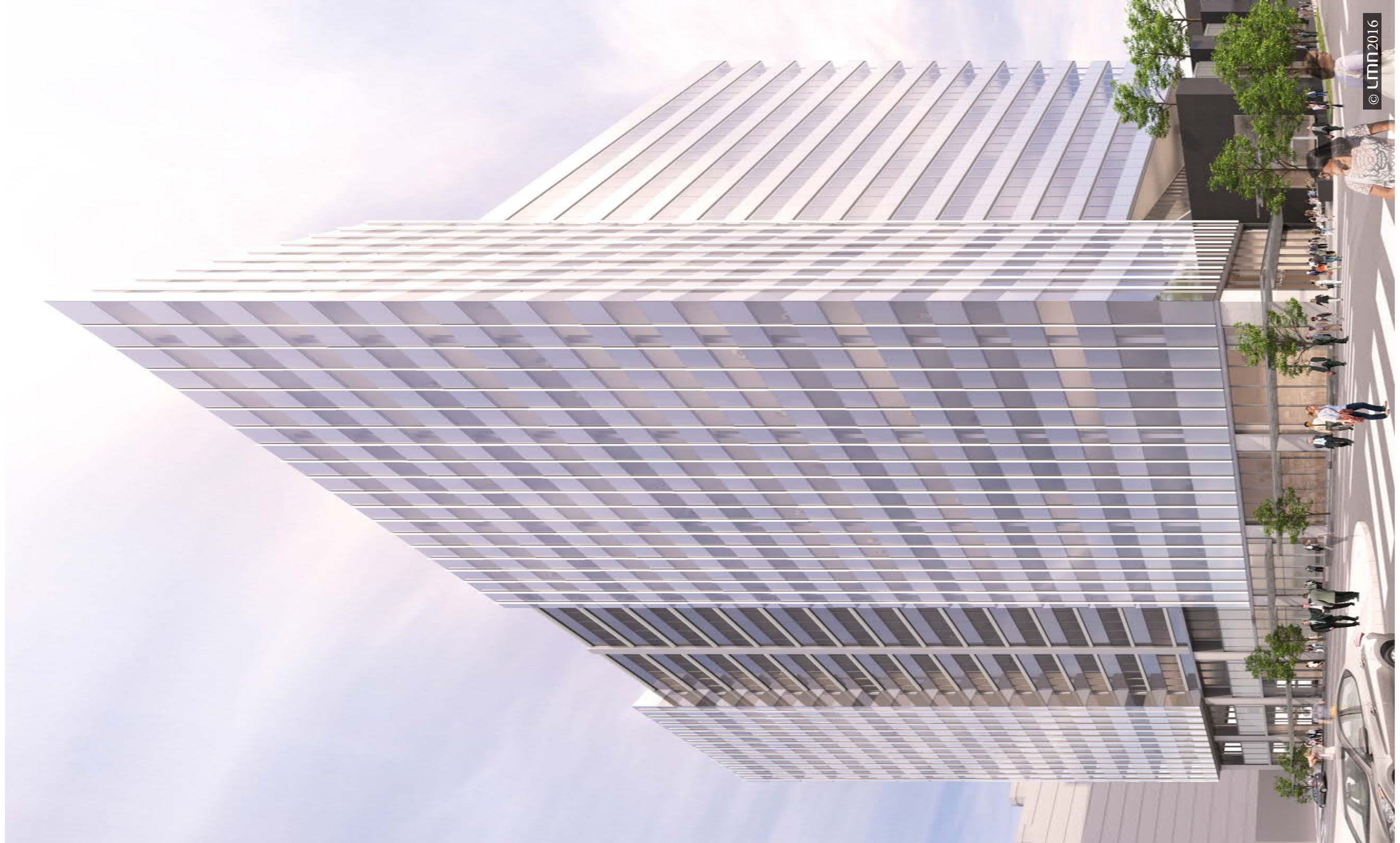
G. SECTION PERSPECTIVE AT TOP OF TOWER





VIEW LOOKING NORTHEAST AT TERRY AVE. AND OLIVE WAY

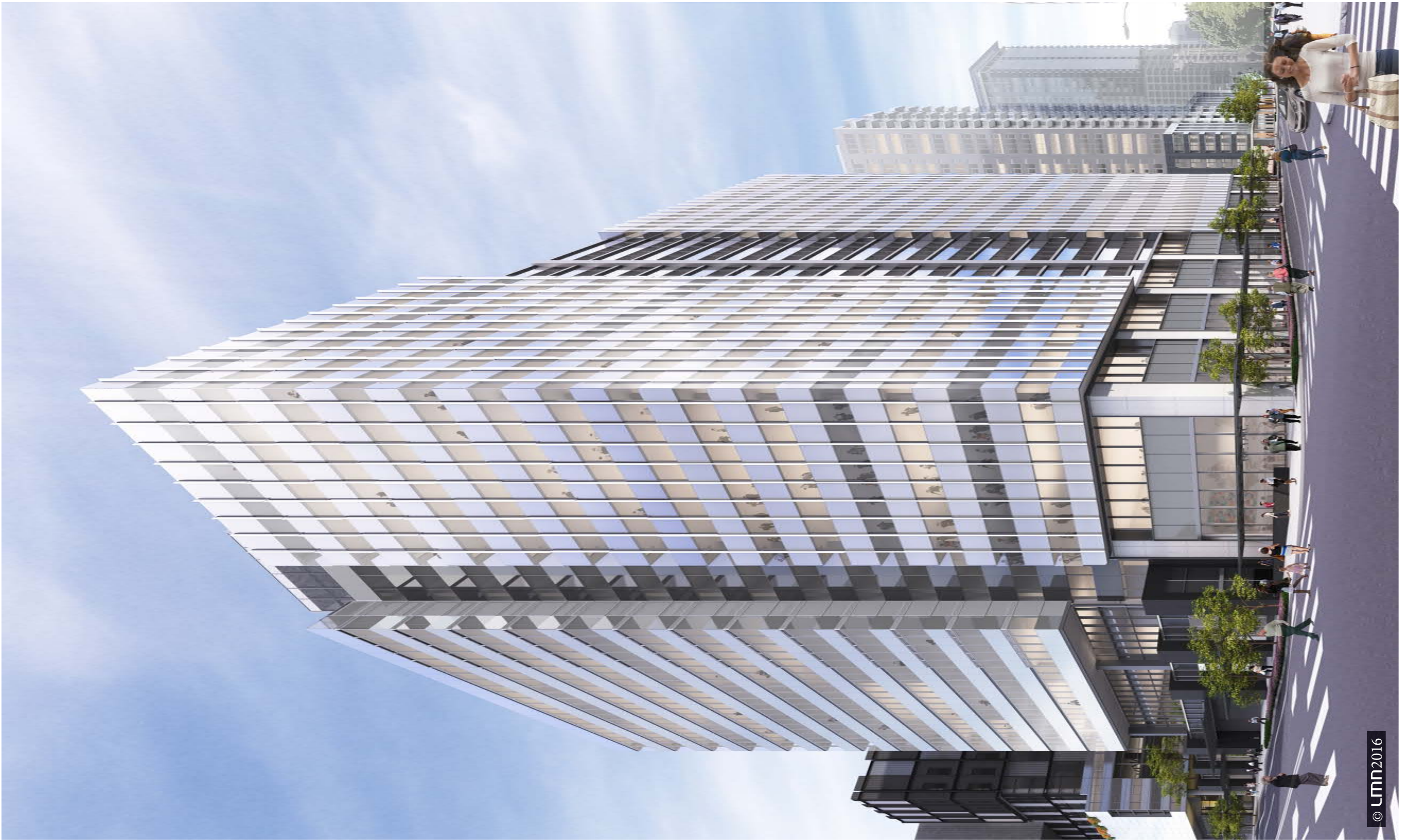




VIEW LOOKING SOUTHEAST AT TERRY AVE. AND HOWELL ST.

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VIEW LOOKING SOUTHWEST AT BOREN AVE. AND HOWELL ST.





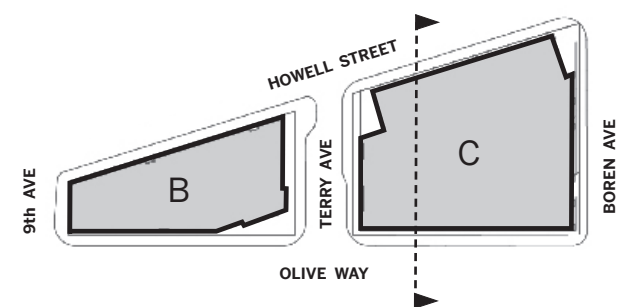
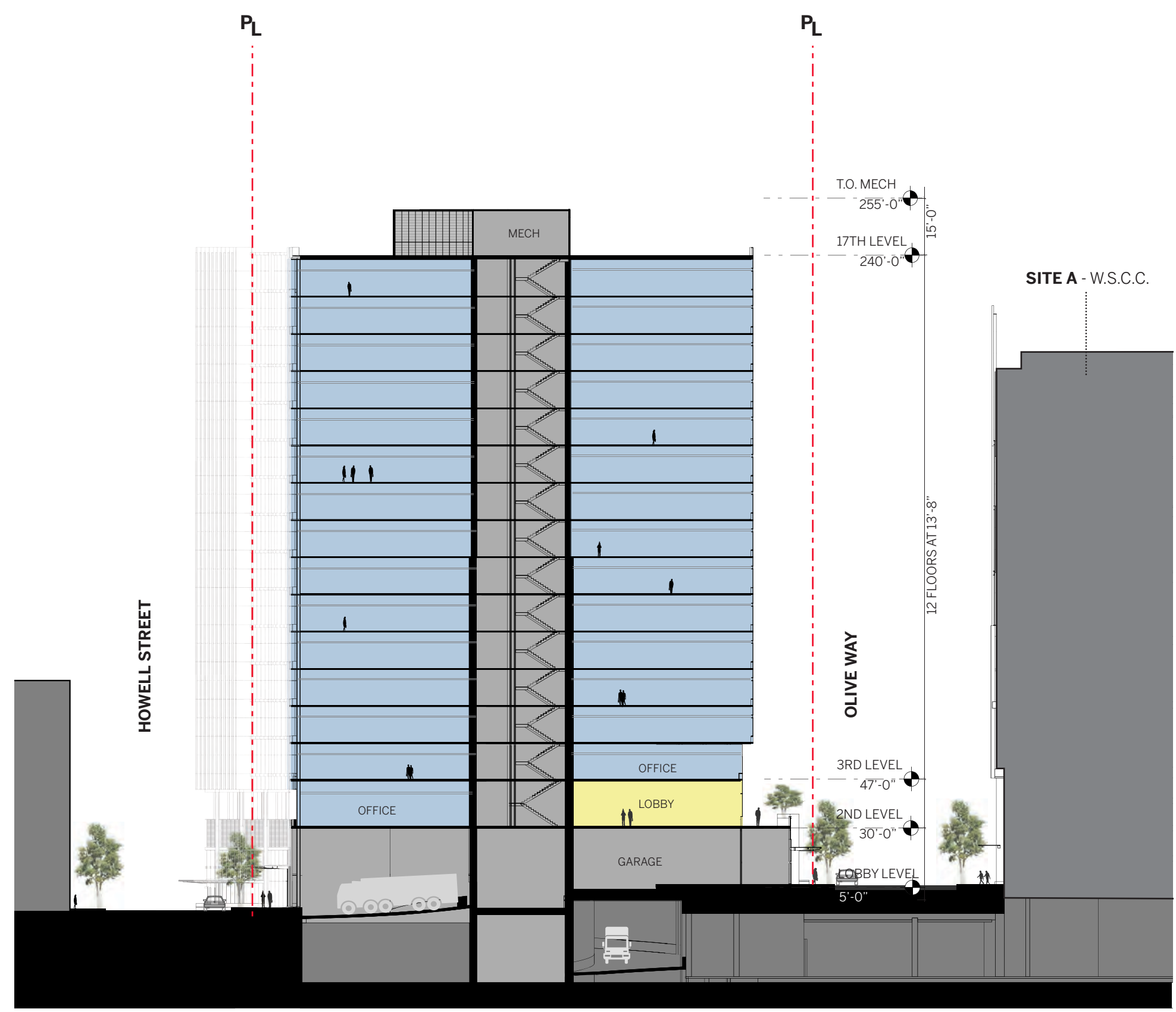
VIEW LOOKING WEST AT BOREN AVE. AND OLIVE WAY



4 DESIGN RECOMMENDATION

SECTIONS

SITE C - OFFICE



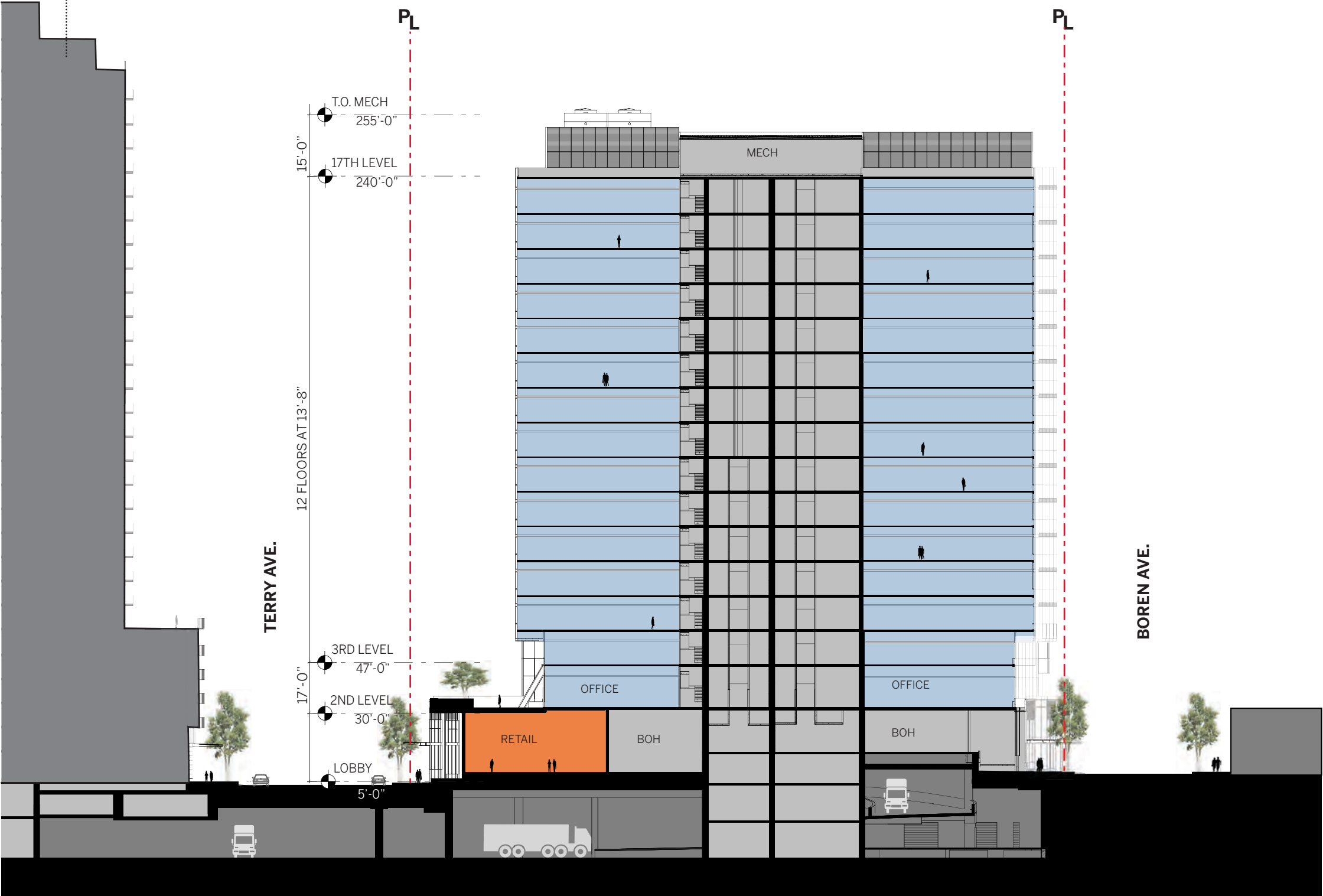


4 DESIGN RECOMMENDATION

SECTIONS

SITE C - OFFICE

SITE B - RESIDENTIAL

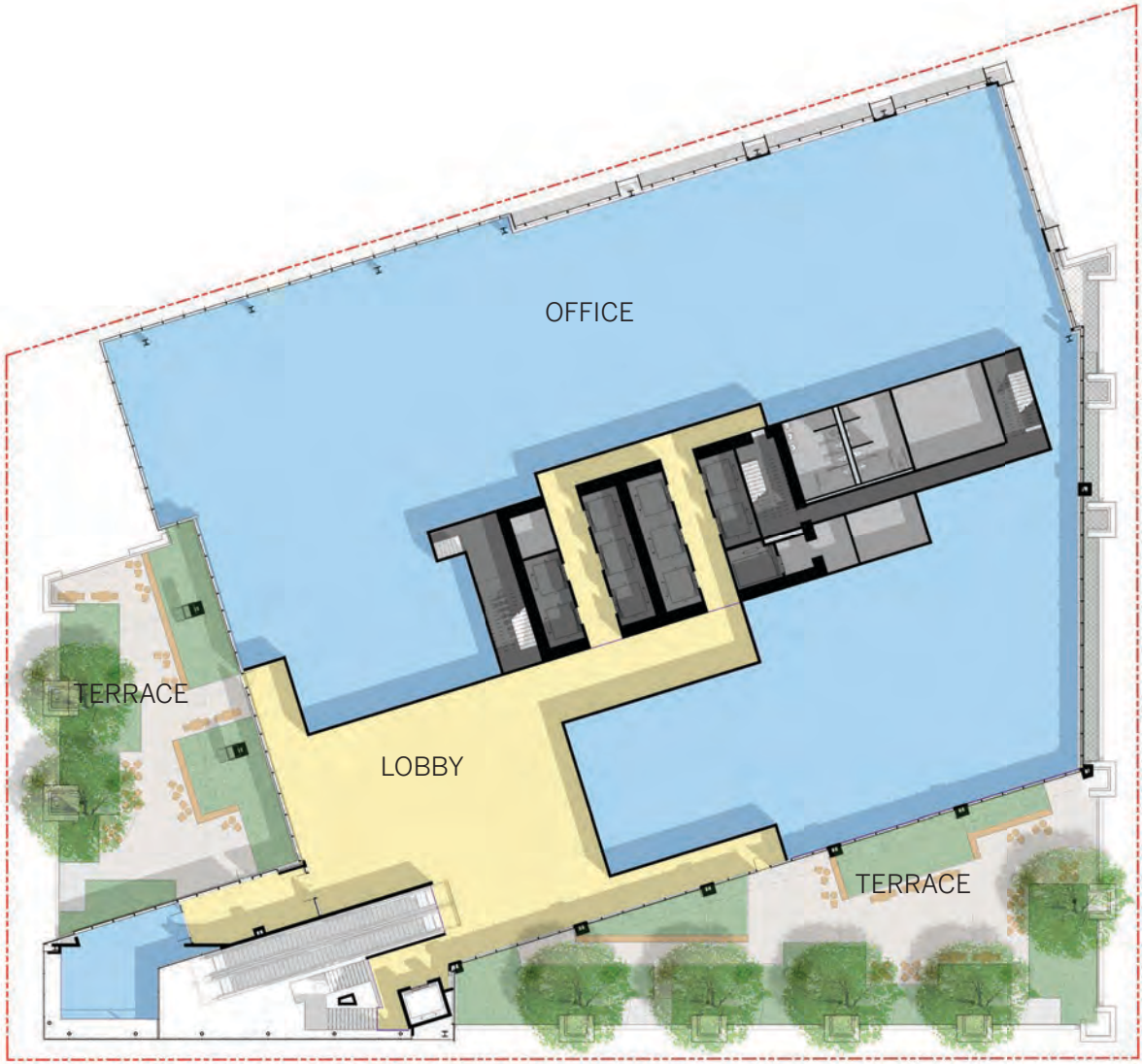




4 DESIGN RECOMMENDATION FLOOR PLANS SITE C - OFFICE



LEVEL 1



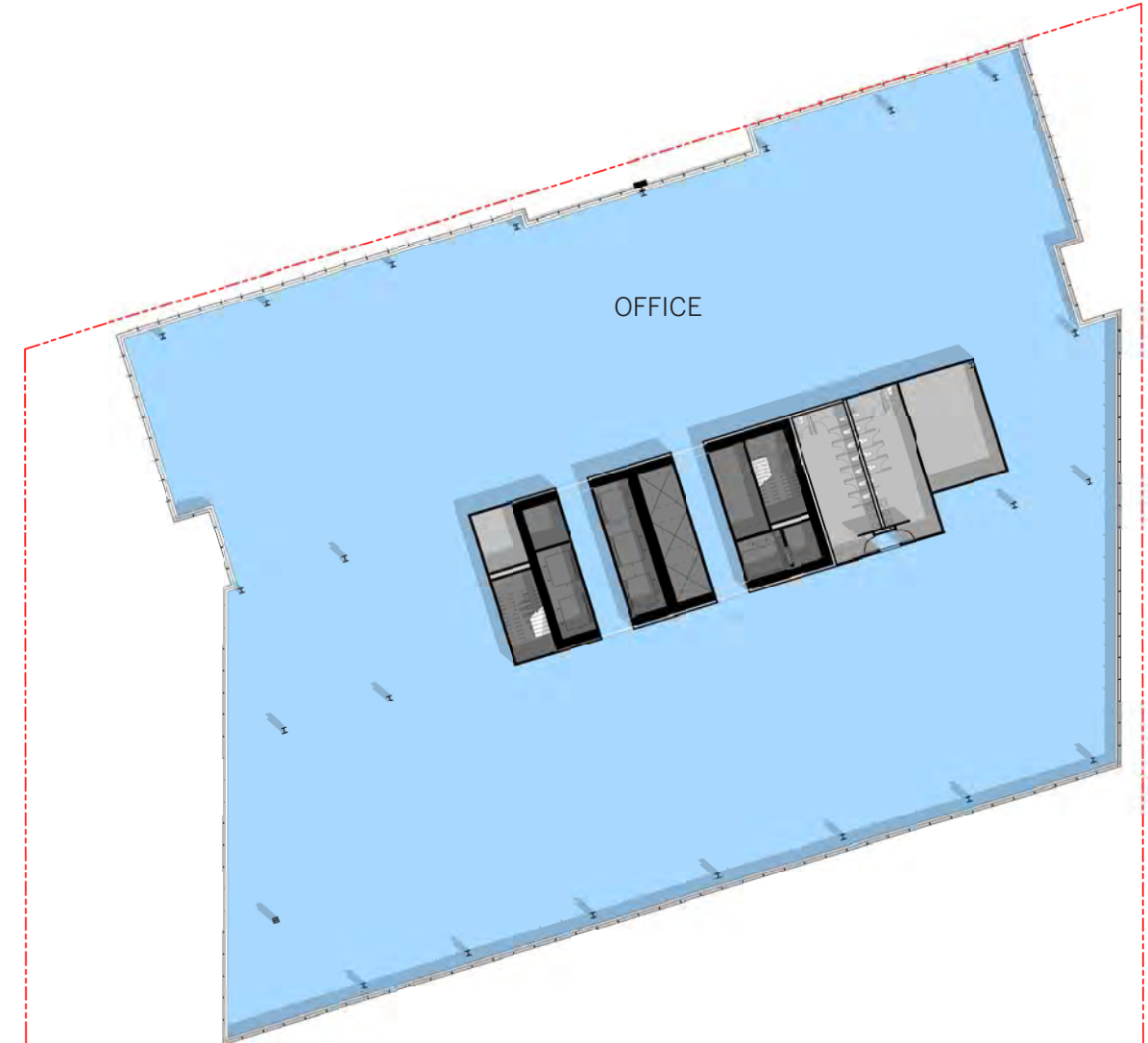
LEVEL 2



## 4 DESIGN RECOMMENDATION FLOOR PLANS SITE C - OFFICE

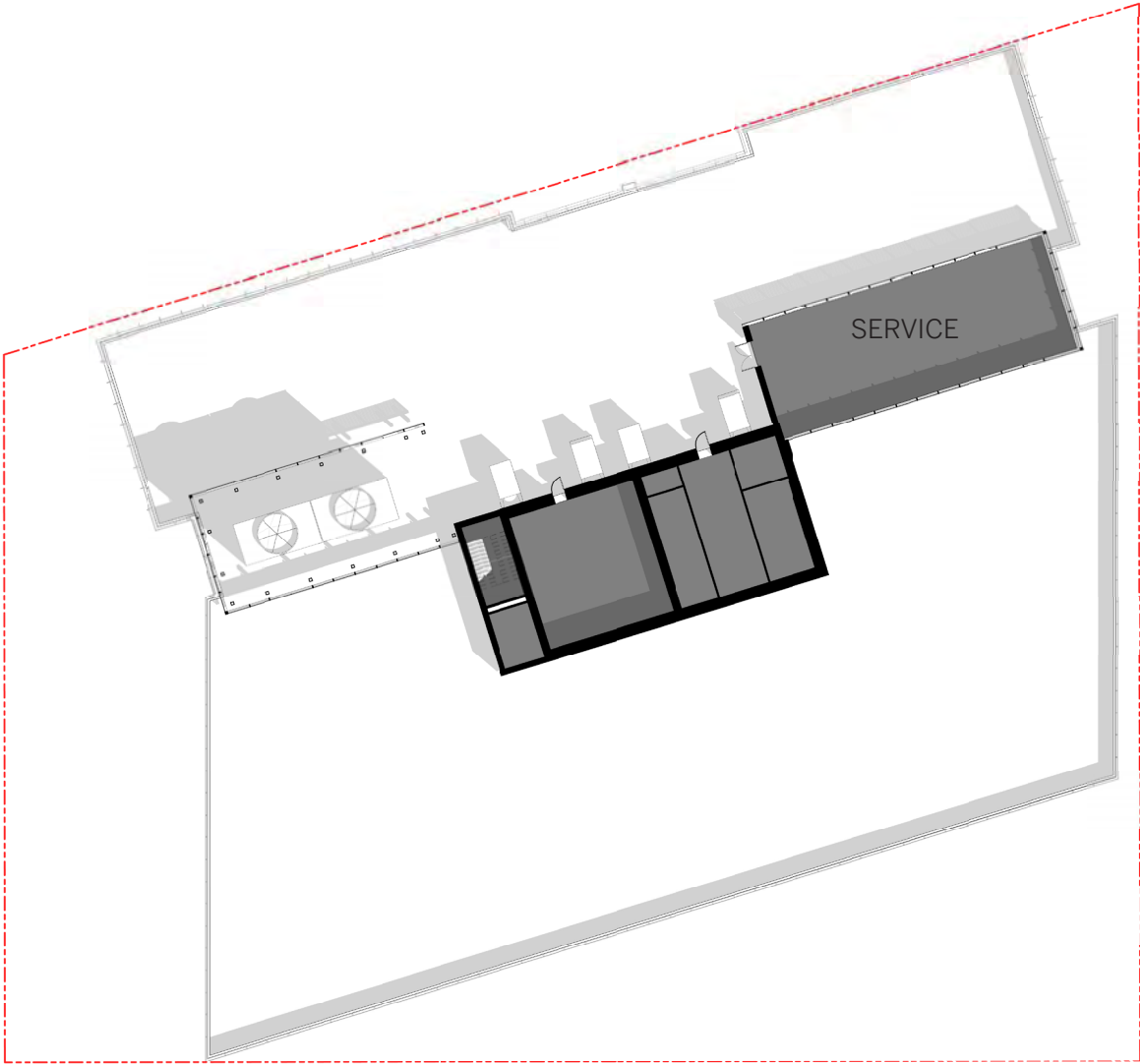


LEVEL 3



LEVEL 4-16





LEVEL 17 ROOF PLAN









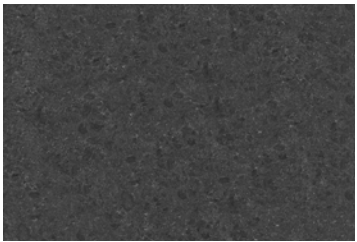
4 DESIGN RECOMMENDATION

ELEVATIONS

SITE C - OFFICE



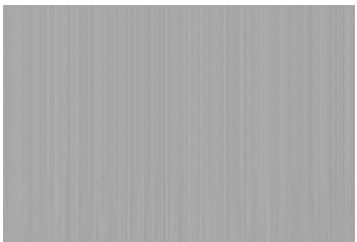
A- STONE



B- STONE BLACK



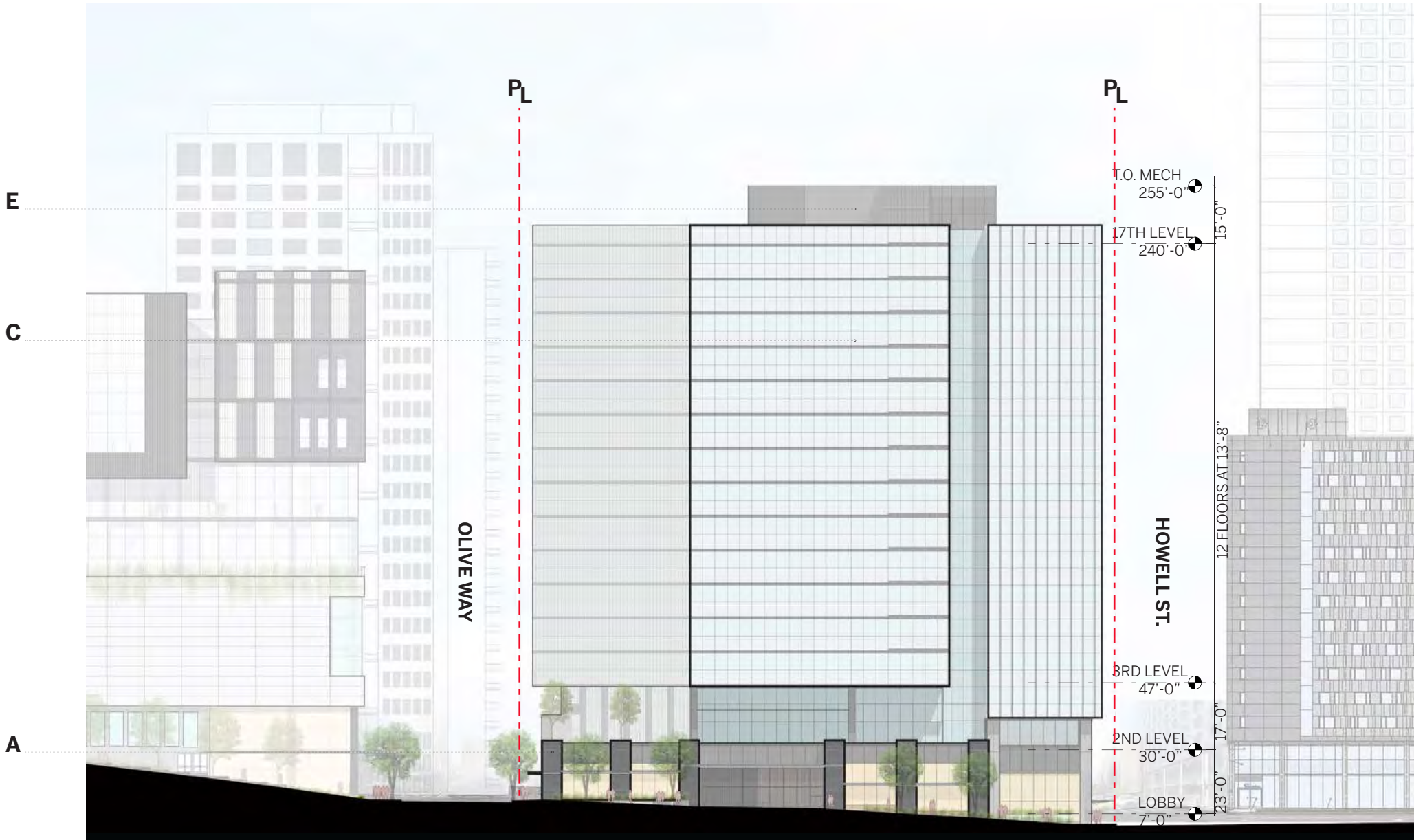
C- GLASS  
(VISION AND  
SPANDREL)



D- METAL PANEL



E- BAR GRATING



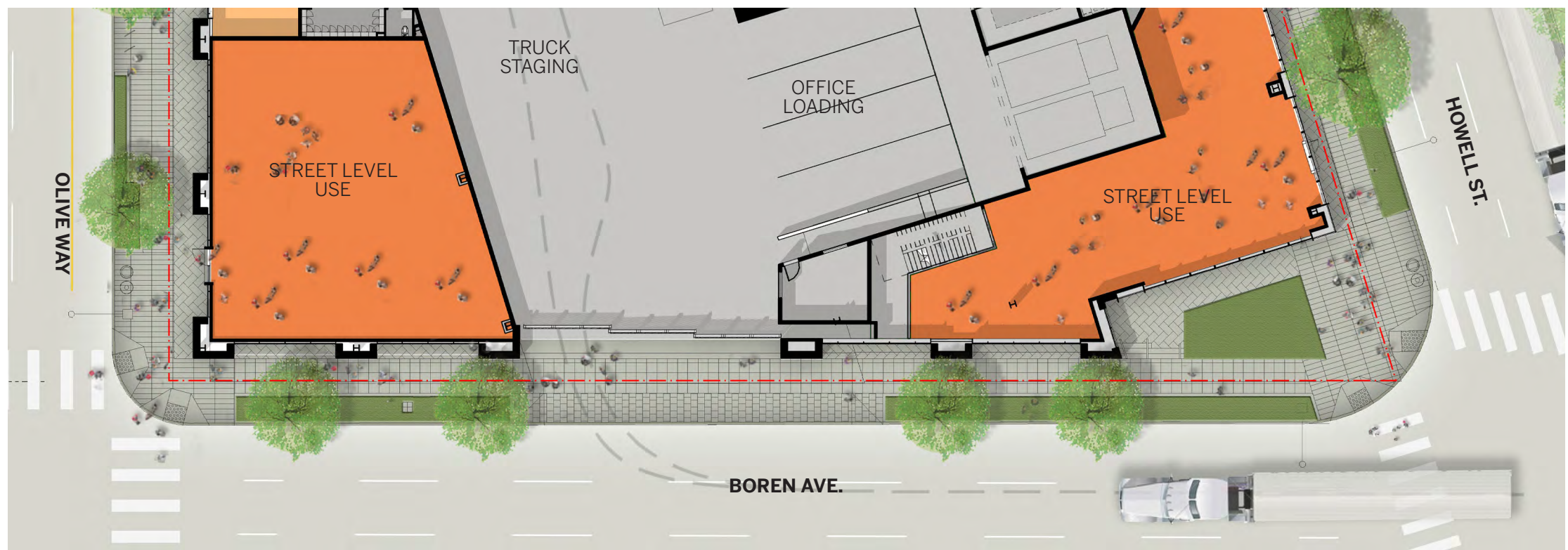
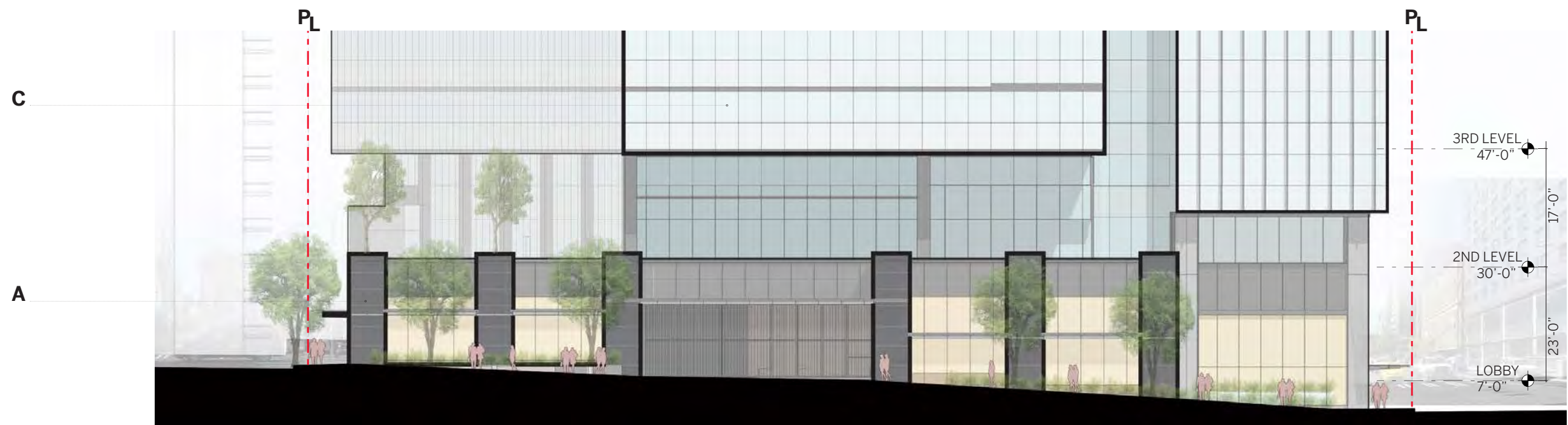
ELEVATION AT BOREN AVE.



4 DESIGN RECOMMENDATION

ELEVATIONS

SITE C - OFFICE



\*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2020.



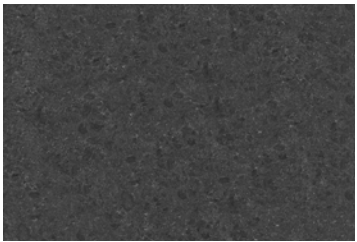
4 DESIGN RECOMMENDATION

ELEVATIONS

SITE C - OFFICE



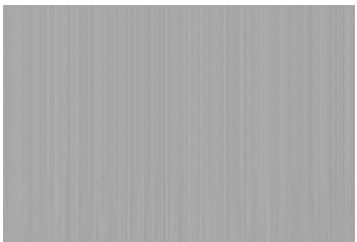
A- STONE



B- STONE BLACK



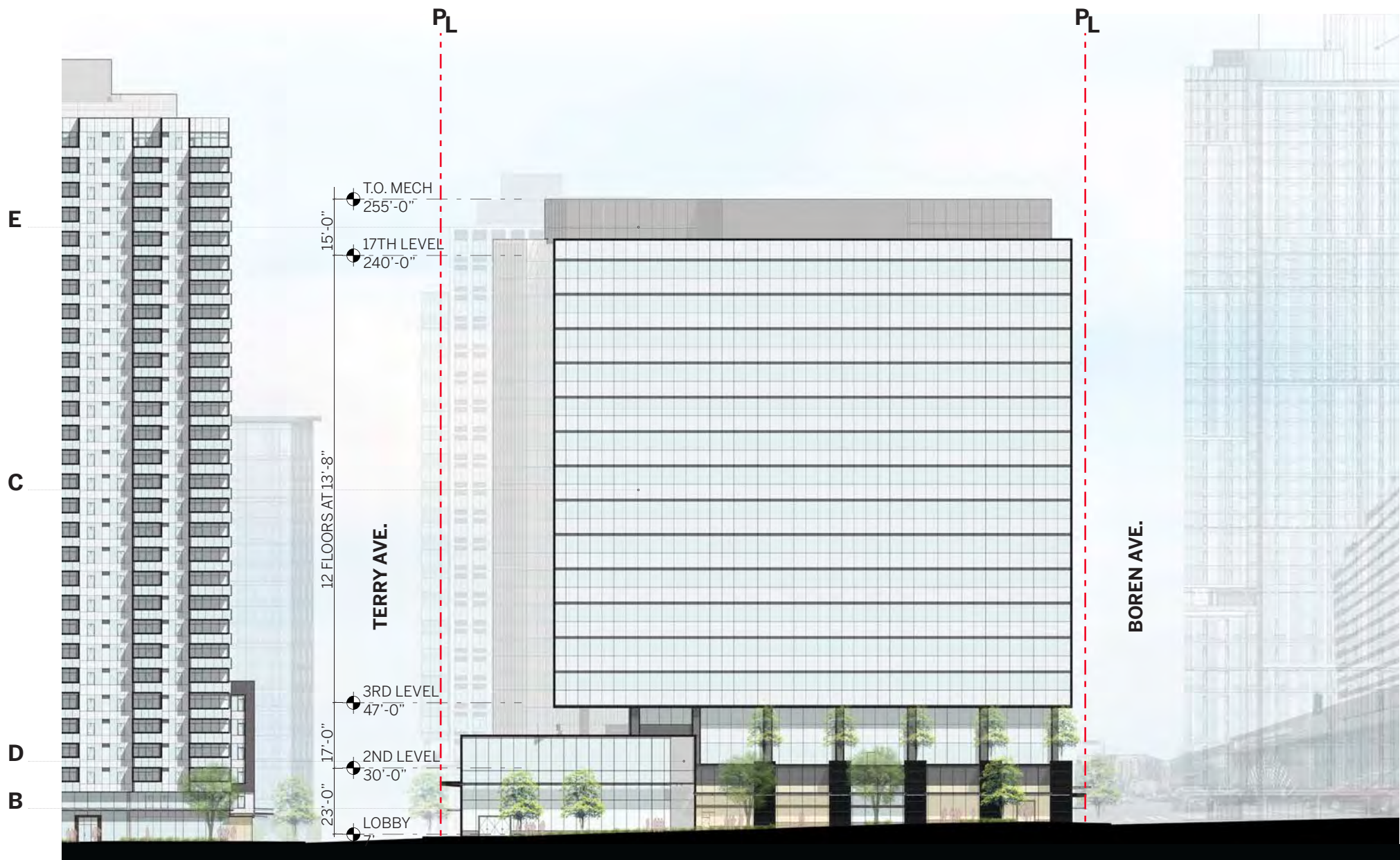
C- GLASS  
(VISION AND  
SPANDREL)



D- METAL PANEL



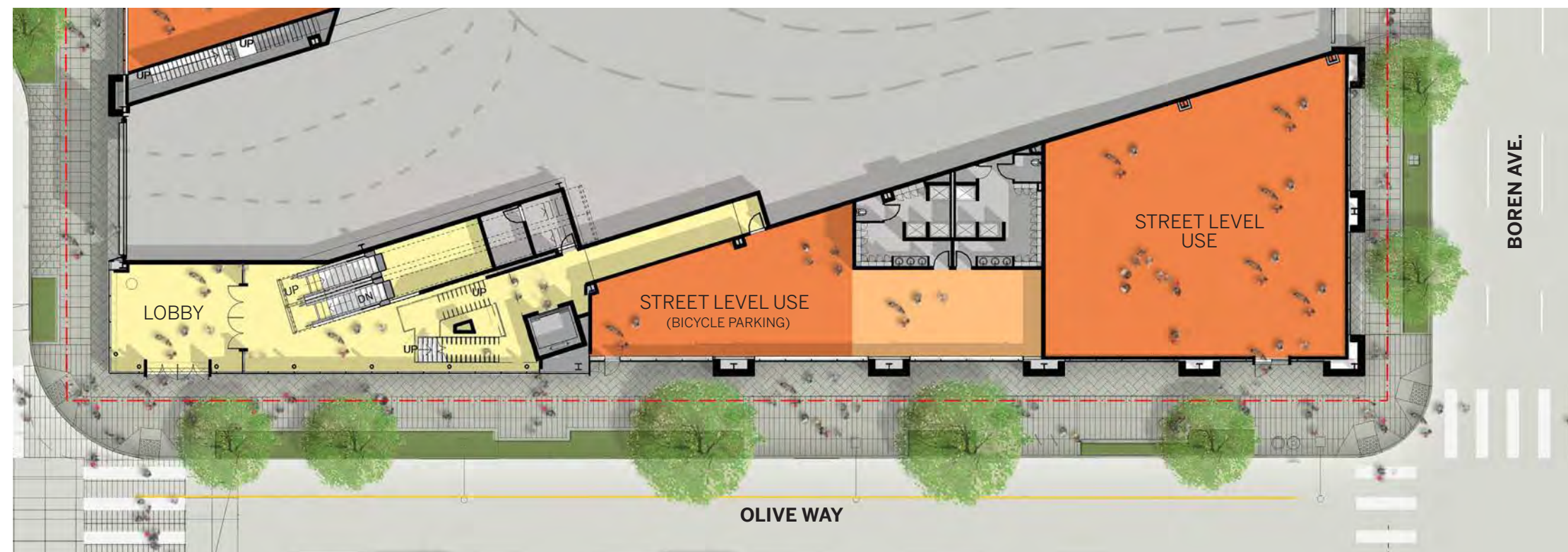
E- BAR GRATING



ELEVATION AT OLIVE WAY



## 4 DESIGN RECOMMENDATION ELEVATIONS SITE C - OFFICE



\*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2020.



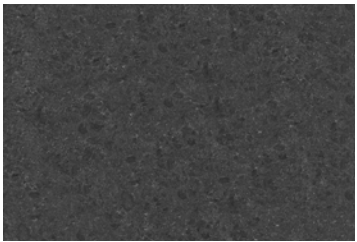
4 DESIGN RECOMMENDATION

ELEVATIONS

SITE C - OFFICE



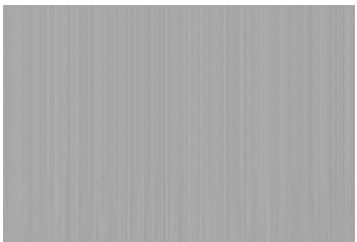
A- STONE



B- STONE BLACK



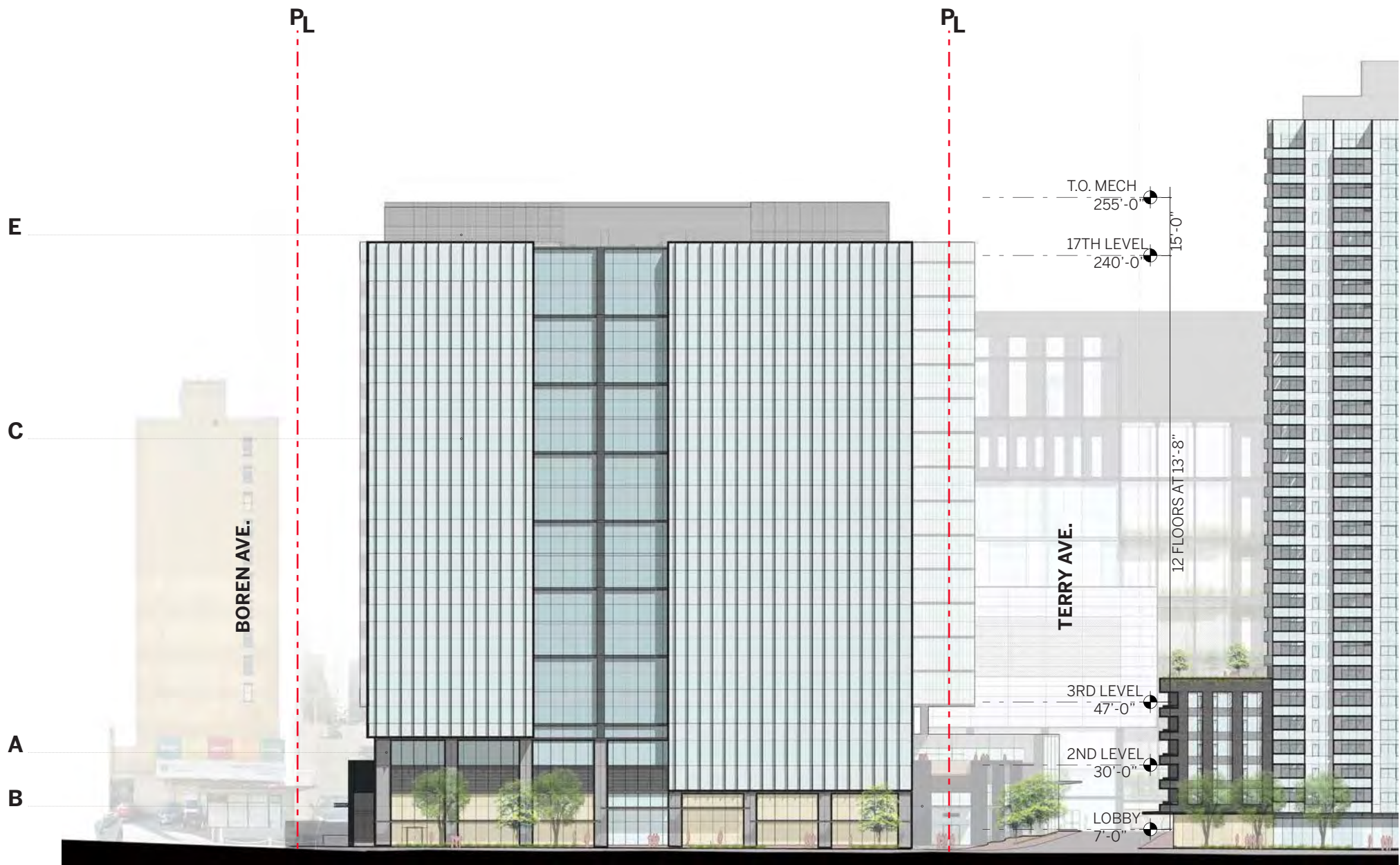
C- GLASS  
(VISION AND  
SPANDREL)



D- METAL PANEL



E- BAR GRATING



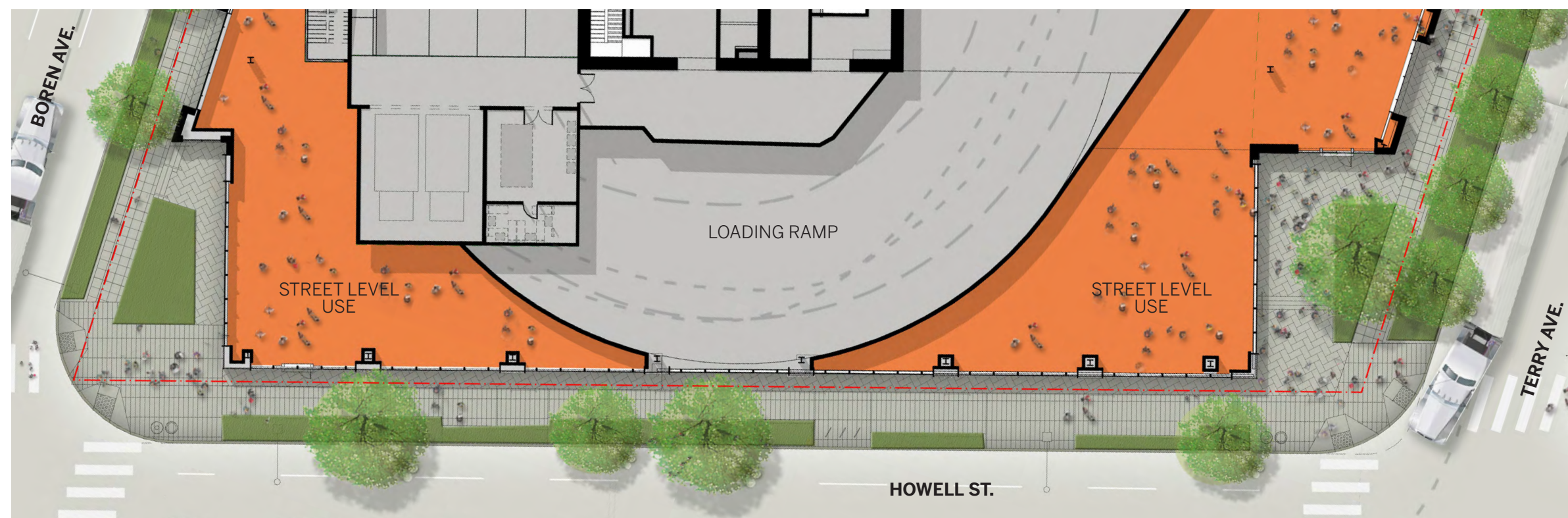
ELEVATION AT HOWELL ST.



4 DESIGN RECOMMENDATION

ELEVATIONS

SITE C - OFFICE



\*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2020.



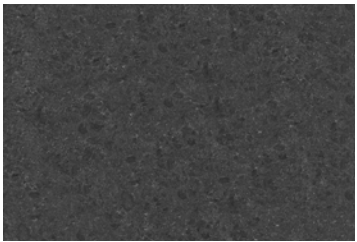
4 DESIGN RECOMMENDATION

ELEVATIONS

SITE C - OFFICE



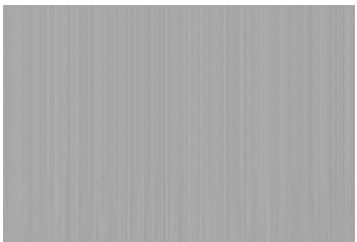
A- STONE



B- STONE BLACK



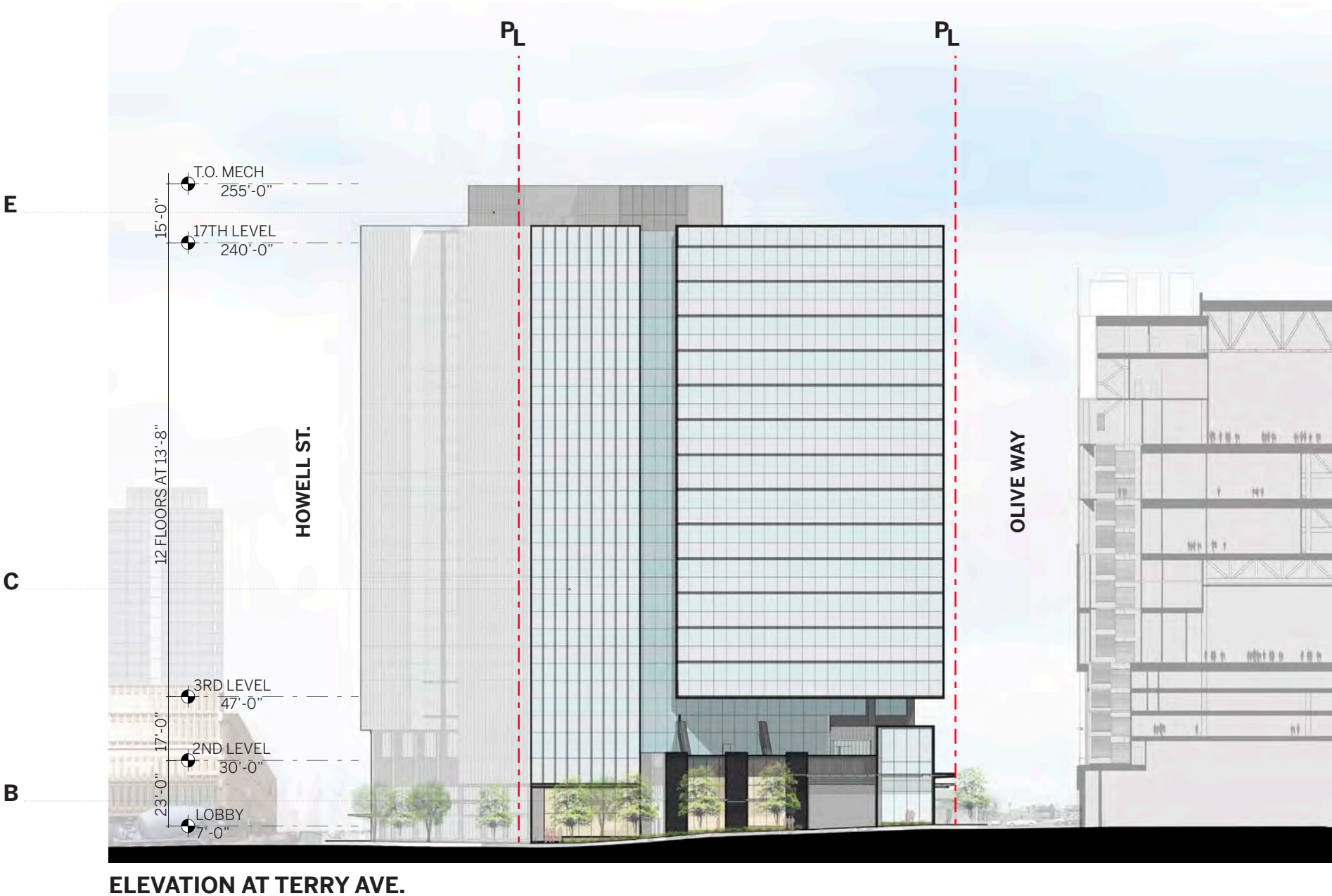
C- GLASS  
(VISION AND  
SPANDREL)



D- METAL PANEL



E- BAR GRATING

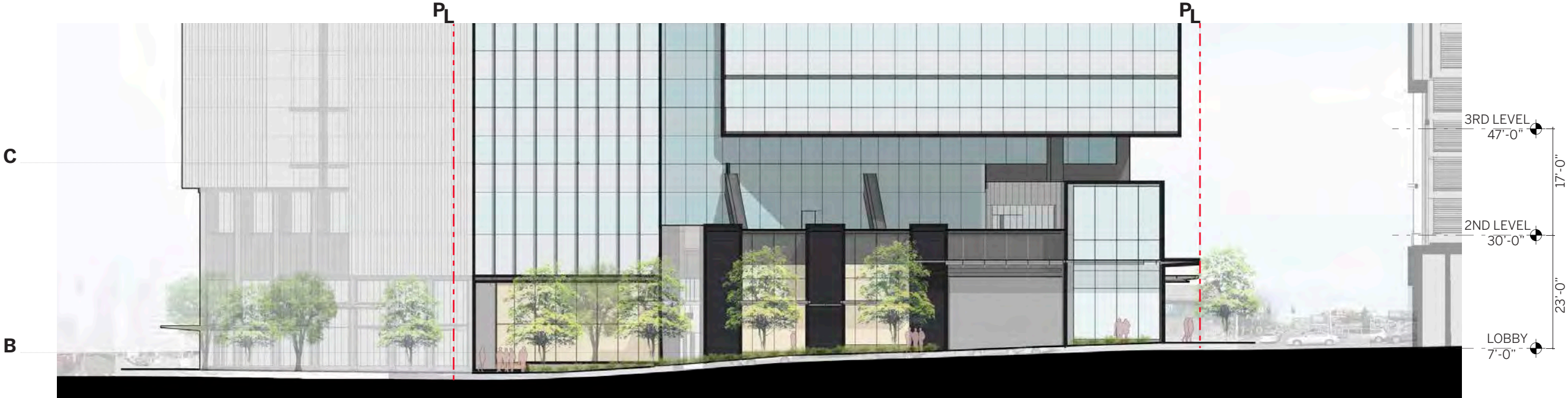




4 DESIGN RECOMMENDATION

ELEVATIONS

SITE C - OFFICE



\*Specific street level use reflects current thinking and is subject to change based on market at opening, planned for 2020.









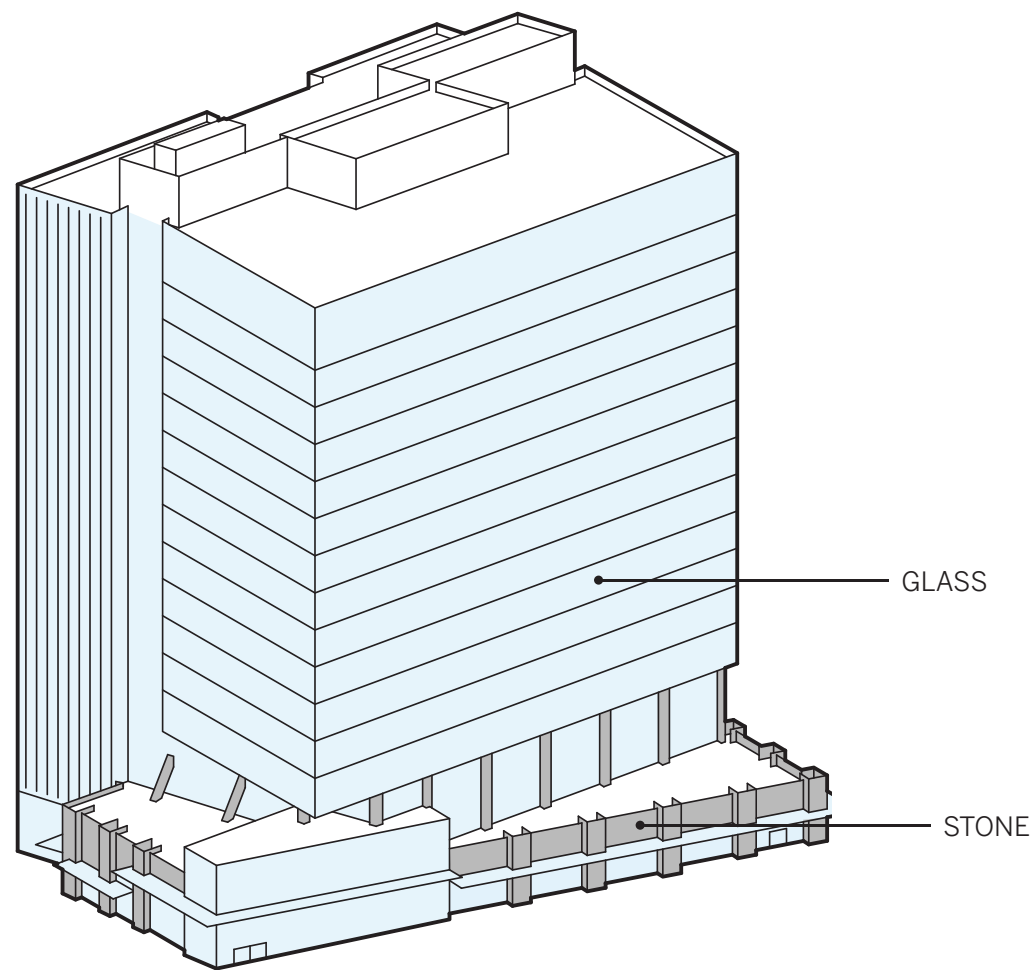
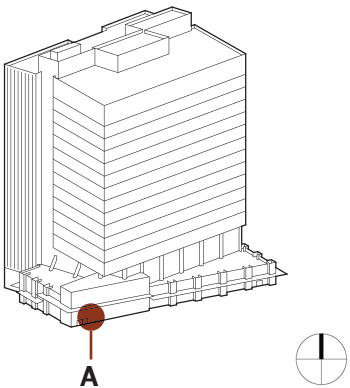
VIEW LOOKING WEST AT OLIVE WAY



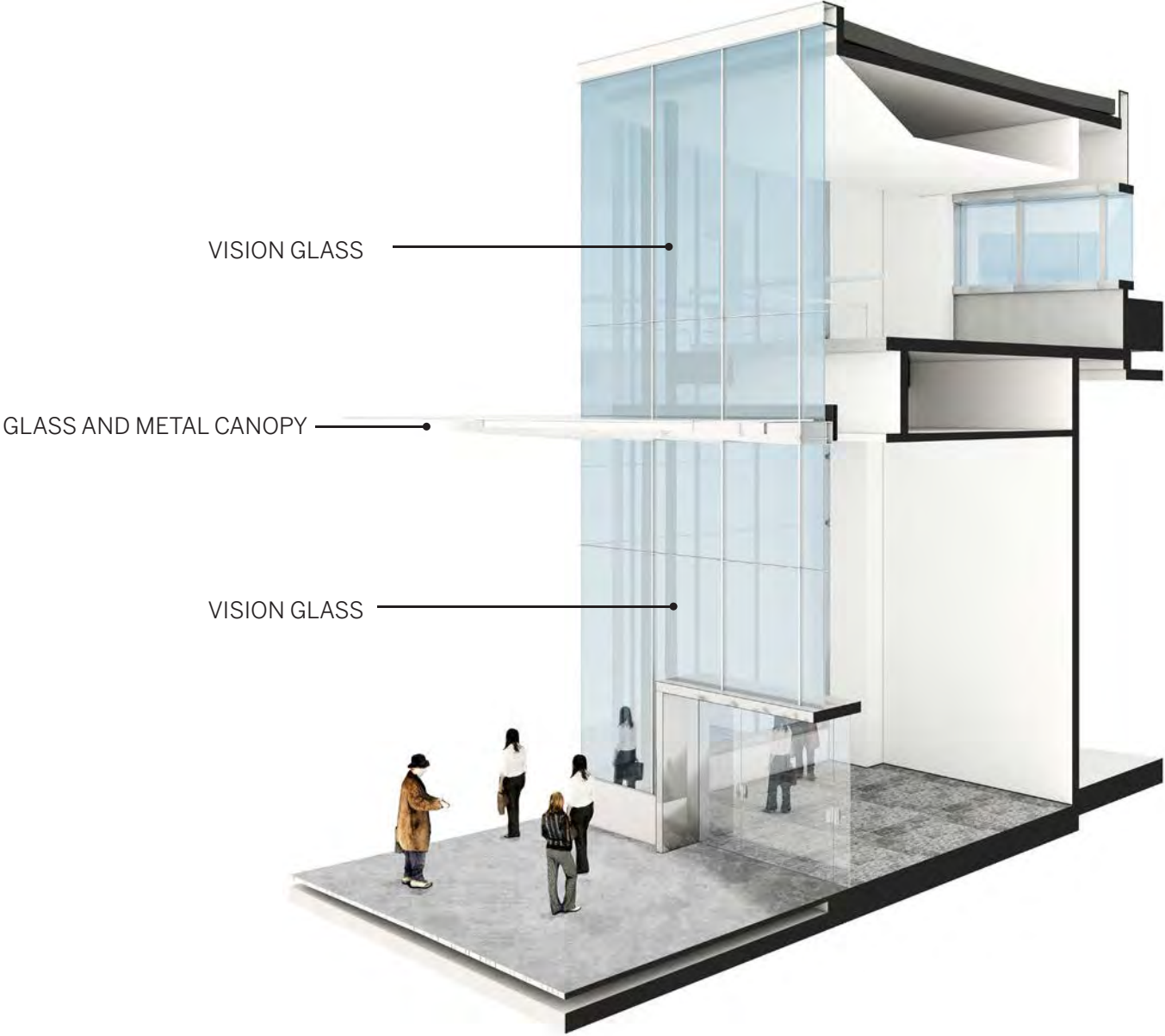
4 DESIGN RECOMMENDATION

MATERIAL CONCEPT

SITE C - OFFICE



MATERIAL MASSING



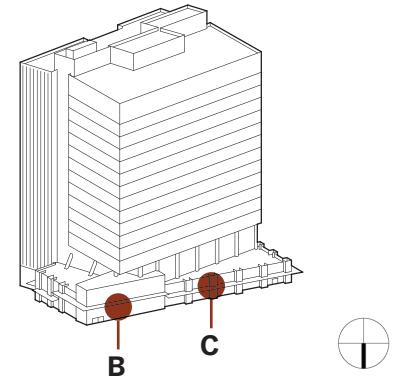
A. OLIVE WAY ENTRANCE



## 4 DESIGN RECOMMENDATION

## MATERIAL DETAILS

## SITE C - OFFICE

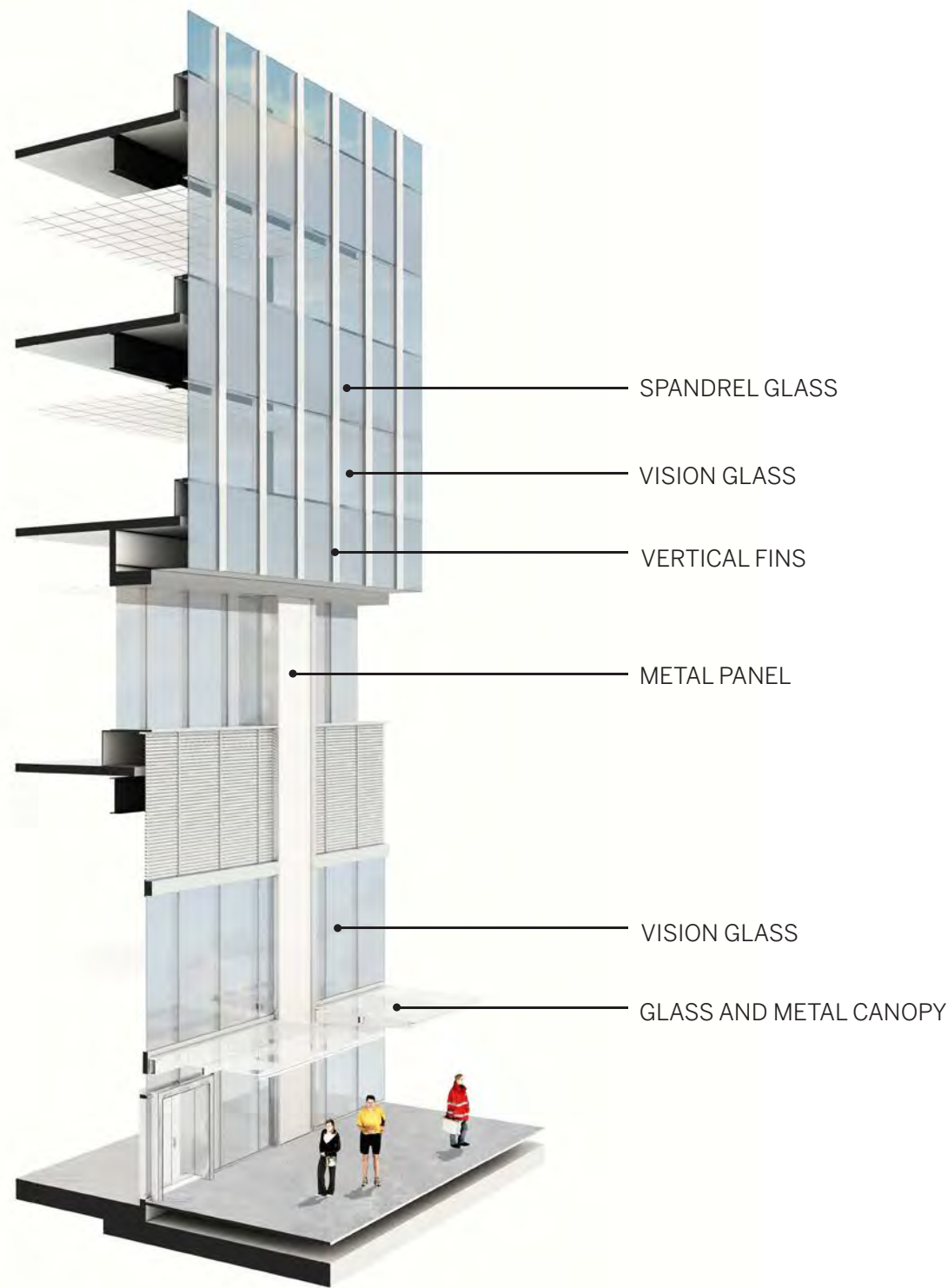
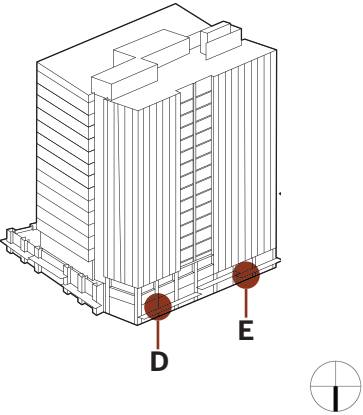


B. OLIVE WAY LOBBY

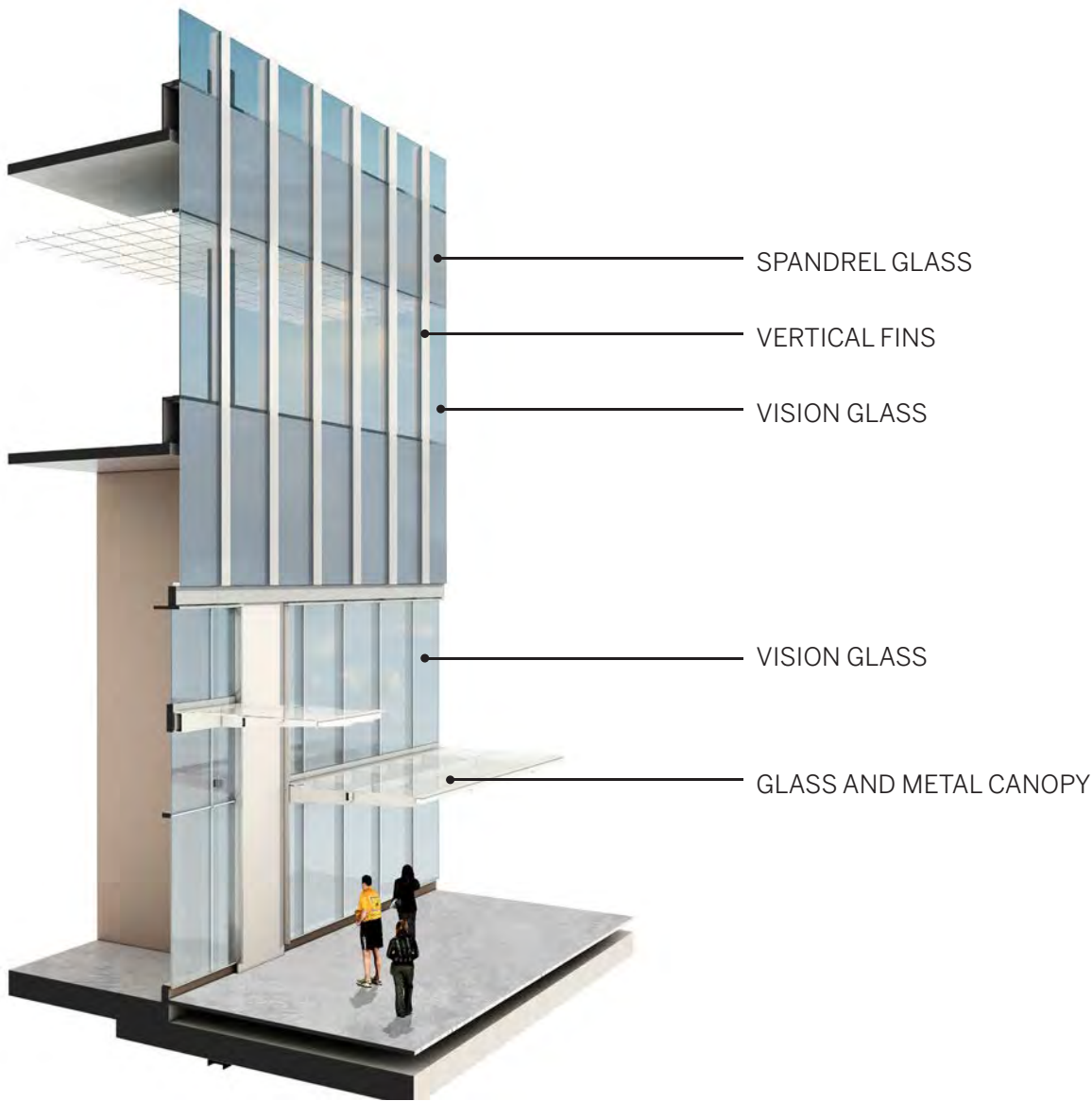


C. OLIVE WAY FACADE



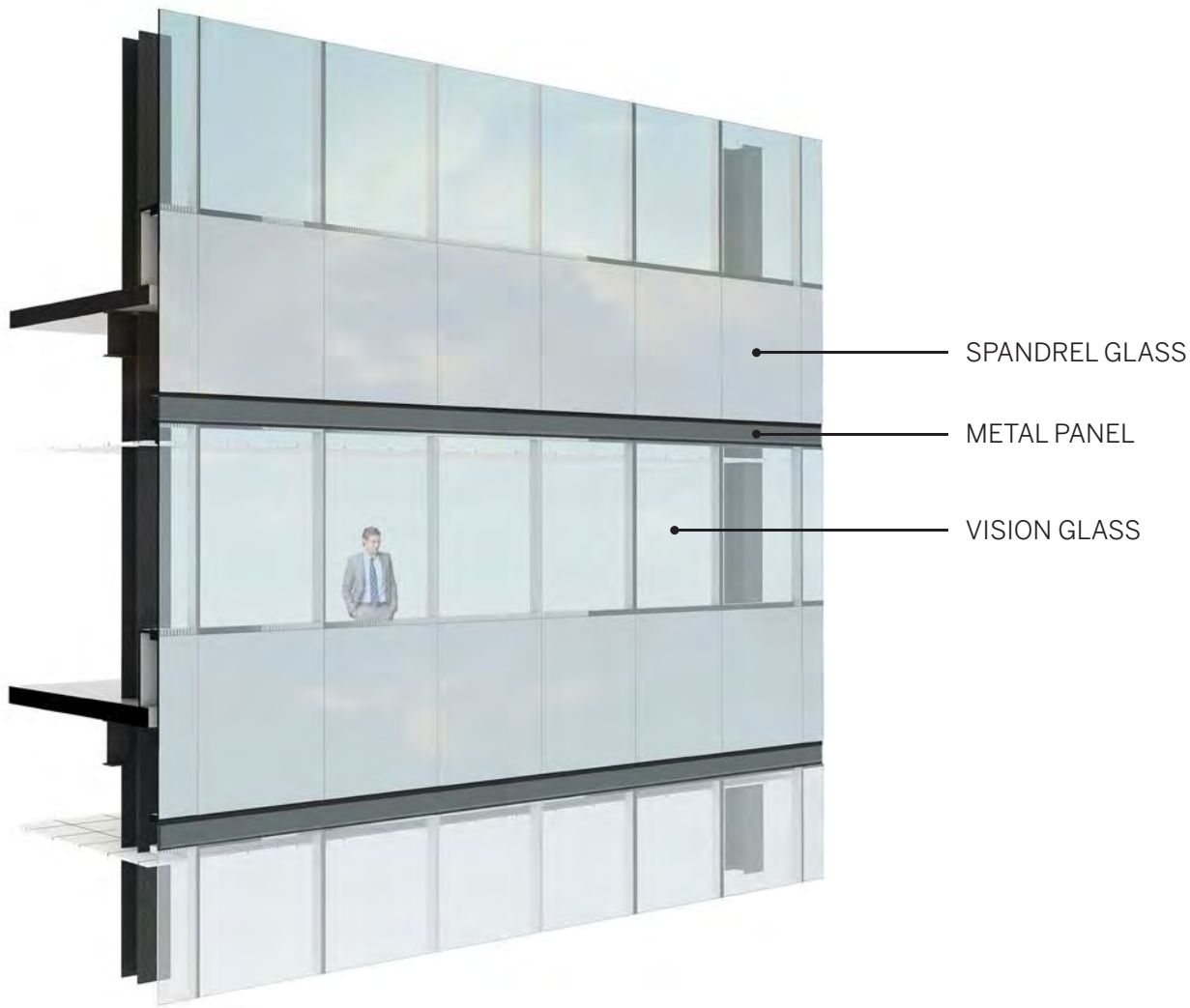
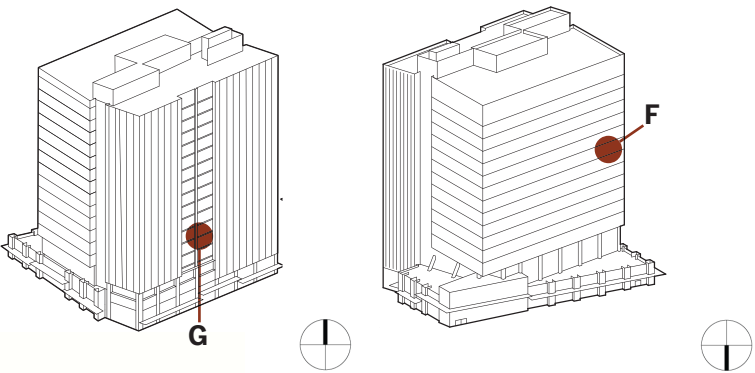


D. HOWELL STREET FACADE

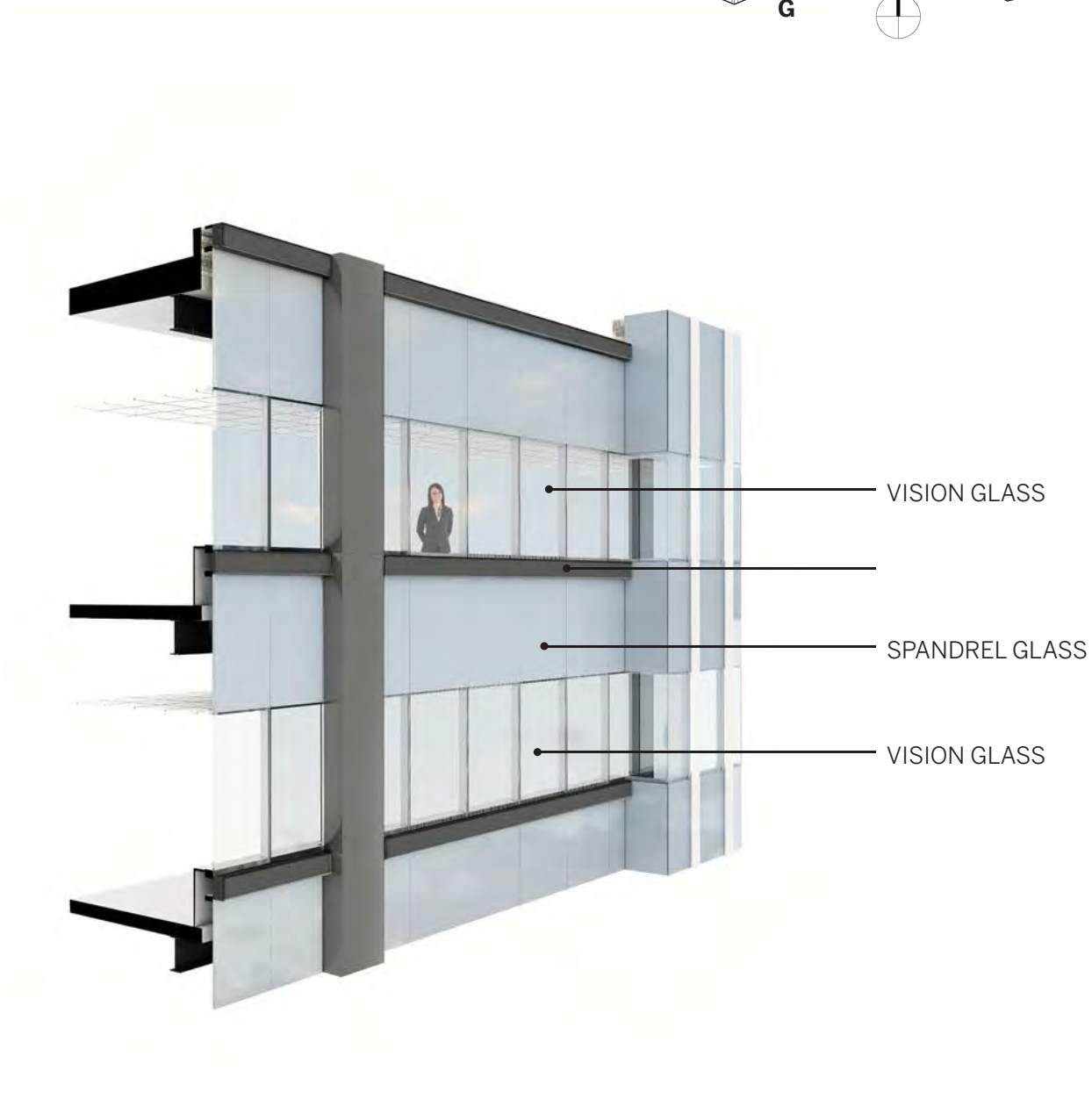


E. HOWELL STREET PODIUM





F. TYPICAL TOWER



G. RECESSED FACADE ON HOWELL ST.



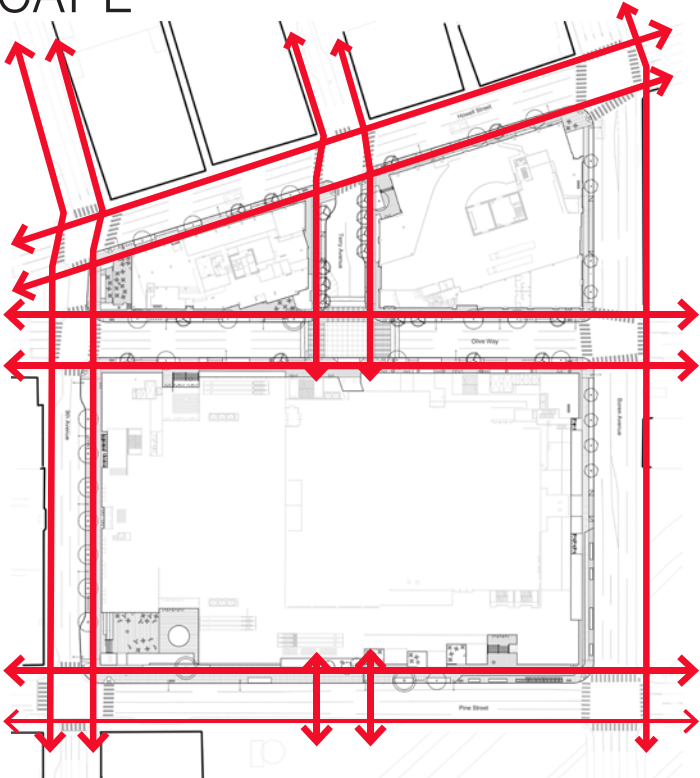
4 DESIGN RECOMMENDATION LANDSCAPE

INTRODUCTION

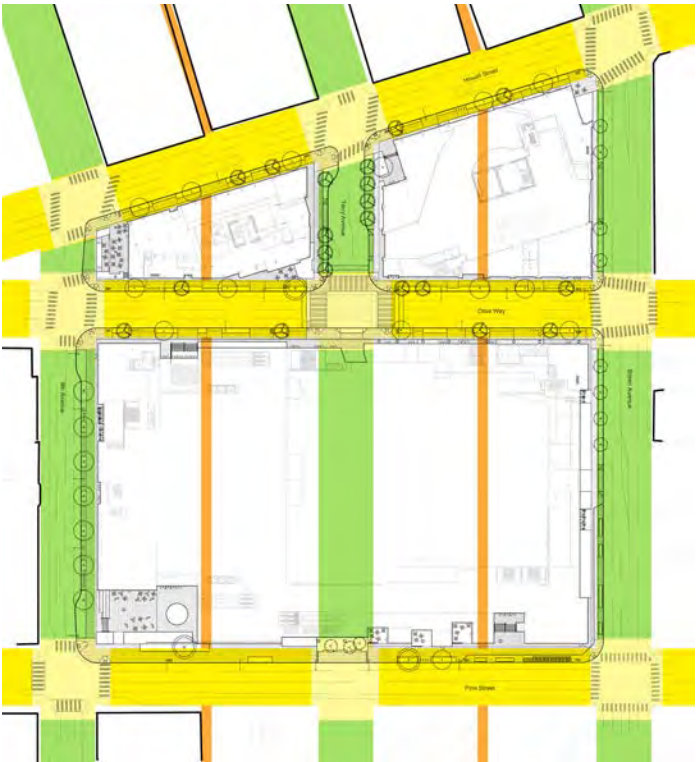
Critical to the success of this urban convention center is the ability to balance the scale and extents of the building required by its program with the grain and texture of the surrounding city. Rather than appearing as a superblock project, the streets and building will relate to the grain and texture of their surrounding neighborhoods. The building edges will create a continuously active experience on all sides for visitors and residents alike.

The site design for the WSCC Addition looks to ensure the connectivity and continuity of the city streets. A distinct approach to avenues and streets offers a shift in character that builds on city standards to offer an additional layer of orientation rooted in the city's basic relationship between hills and Elliot Bay.

The project reinforces the continuity of the city streets by clearly articulating the edge between the street and WSCC Addition. This allows for a wider variety of conditions within the WSCC Addition and support multiple identities around the multi-block project without compromising the larger continuity of city streets. An additional layer of site elements offers pedestrian amenities that bridge the public programs of the WSCC Addition and the experience of convention visitors.



URBAN CONNECTIVITY AND CONTINUITY



ORIENTING STREETS AND AVENUES



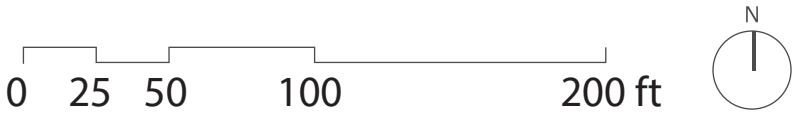
INDEPENDENT SIDEWALKS



VARIETY AND MULTIPLE IDENTITIES WITHIN PROPERTY



4 DESIGN RECOMMENDATION LANDSCAPE



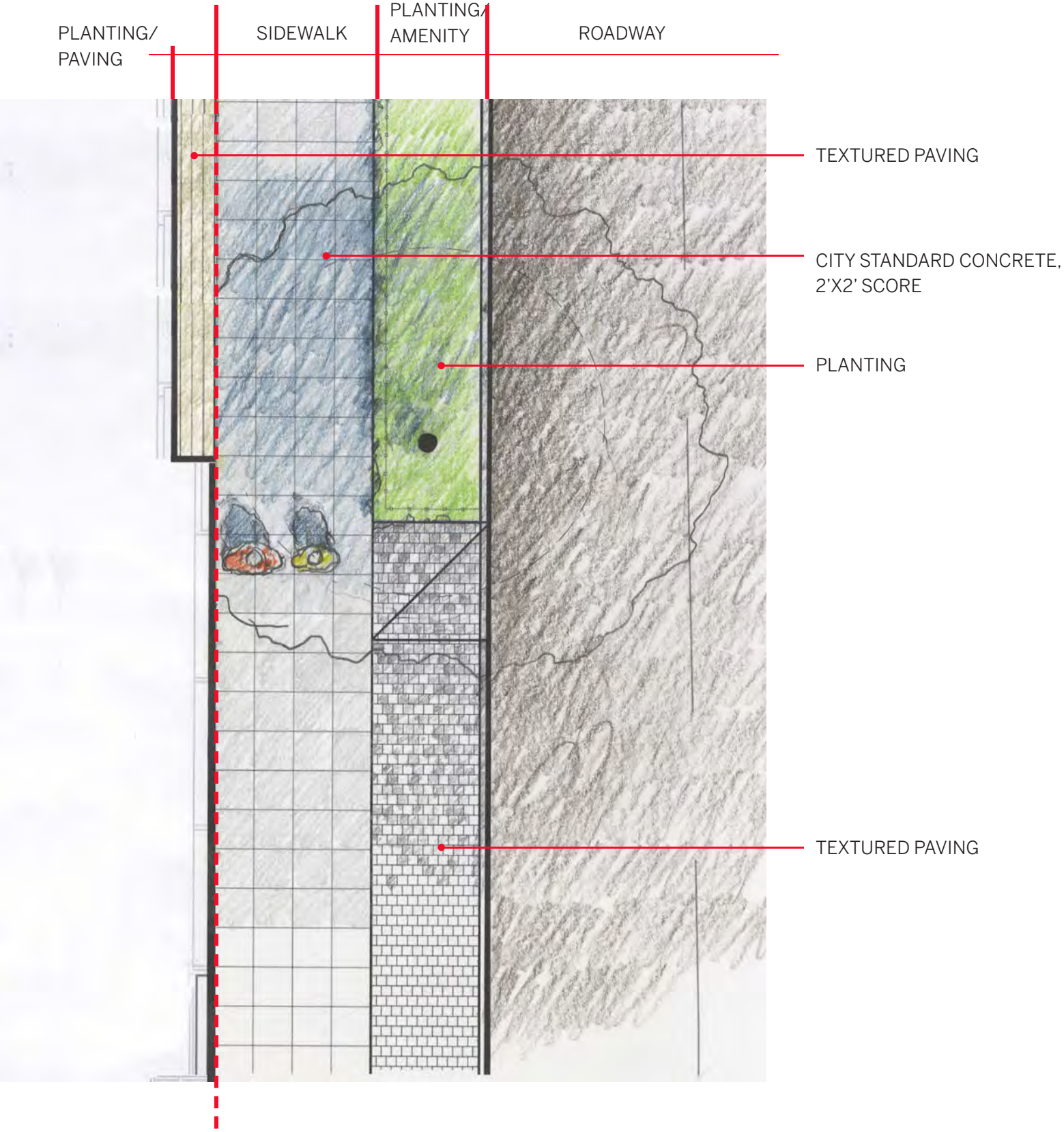
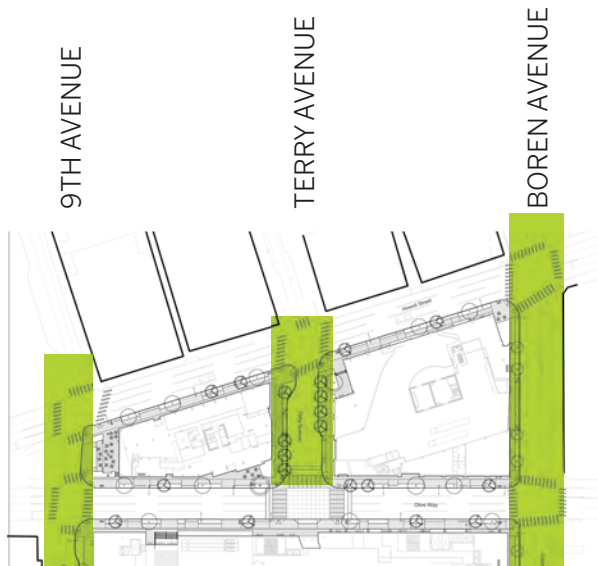


# 4 DESIGN RECOMMENDATION

## LANDSCAPE

**AVENUES**

The project’s approach to Avenues (Boren Avenue, Terry Avenue, and 9th Avenue) looks to maximize planting and tree canopy coverage with a consistent and lush character.



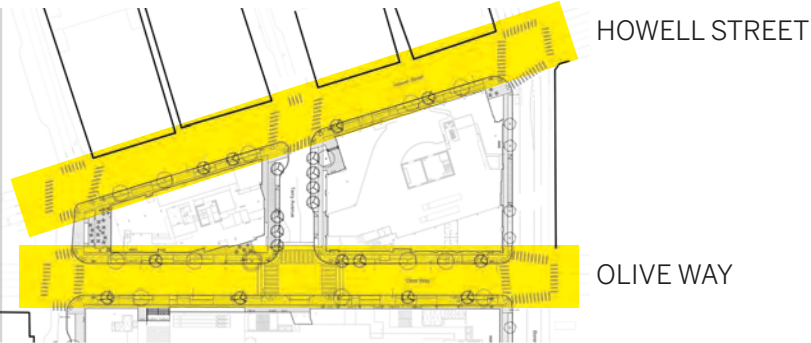


# 4 DESIGN RECOMMENDATION

## LANDSCAPE

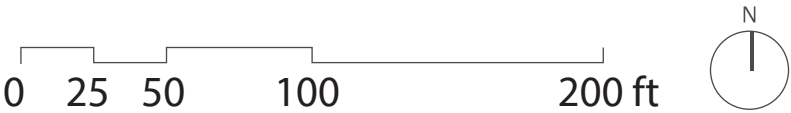
### STREETS

The project’s approach to Streets (Pine Street and Olive Way) celebrates these important connections from Capitol Hill down to Elliot Bay. A linear paving pattern and varied tree species, spacing, and planting support a more diverse experience on the two-block lengths of the new WSCC building. Iconic views down to Pike Place and the water beyond are preserved to help orient and invite visitors to explore the city.





4 DESIGN RECOMMENDATION LANDSCAPE





# 4 DESIGN RECOMMENDATION LANDSCAPE

## Avenue Trees (Terry Avenue and Boren Avenue)



Fraxinus latifolia ●  
Oregon Ash



Platanus x acerifolia 'Columbia' ●  
Columbia Planetree

## Street Trees (Howell Street and Olive Way)



Acer macrophyllum ●  
Bigleaf Maple



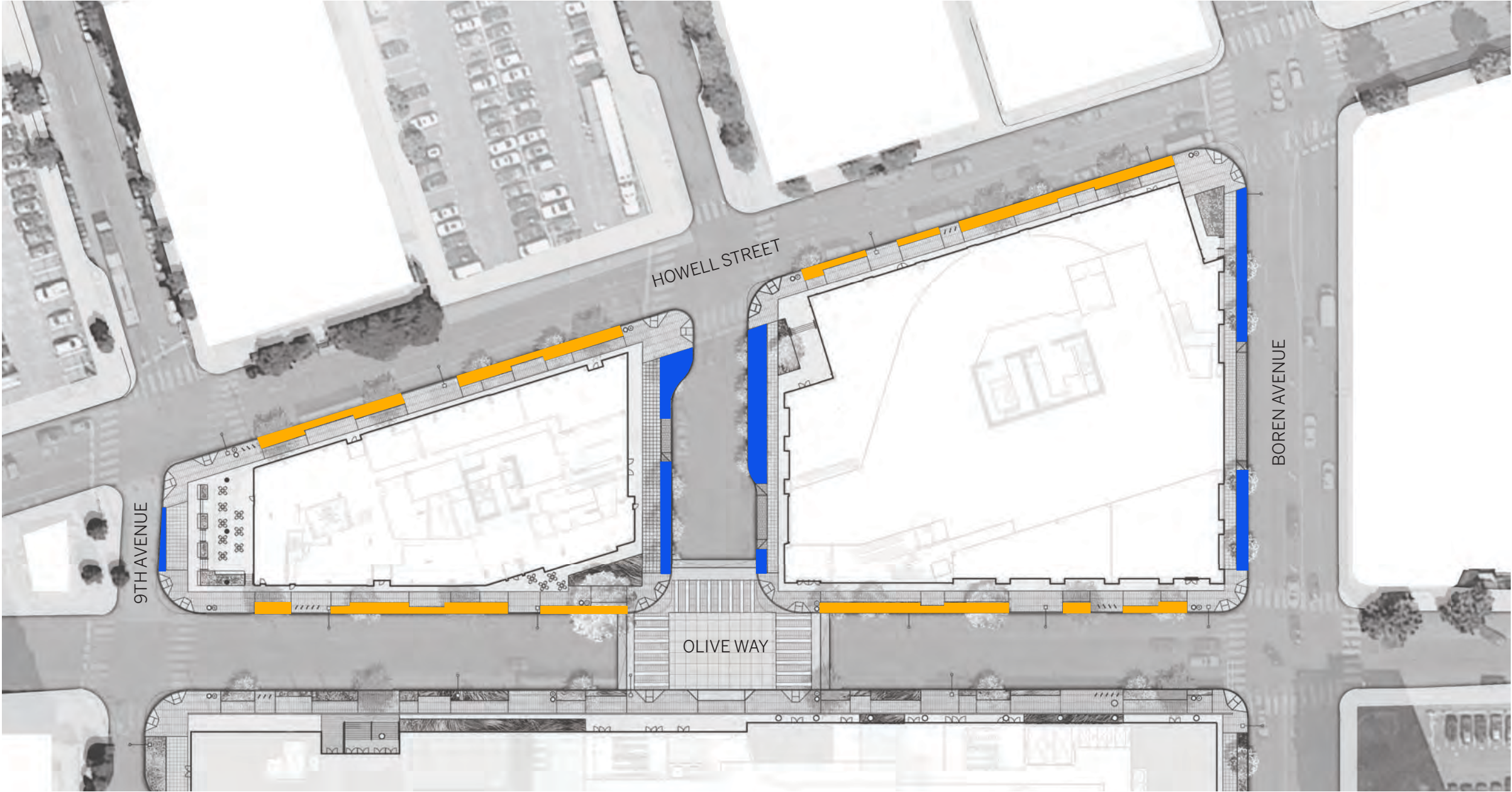
Zelkova serrata 'Green Vase' ●  
Japanese Zelkova



Fraxinus latifolia ●  
Oregon Ash



4 DESIGN RECOMMENDATION LANDSCAPE





## 4 DESIGN RECOMMENDATION LANDSCAPE

### Avenue Planting (Terry Avenue and Boren Avenue)



Arctostaphylos uva-ursi  
'Massachusetts'  
Kinnikinnick



Gaultheria shallon  
Salal



Mahonia aquifolium 'Compacta'  
Compact Oregon Grape

### Street Planting (Howell Street and Olive Way)



Arctostaphylos uva-ursi  
'Massachusetts'  
Kinnikinnick



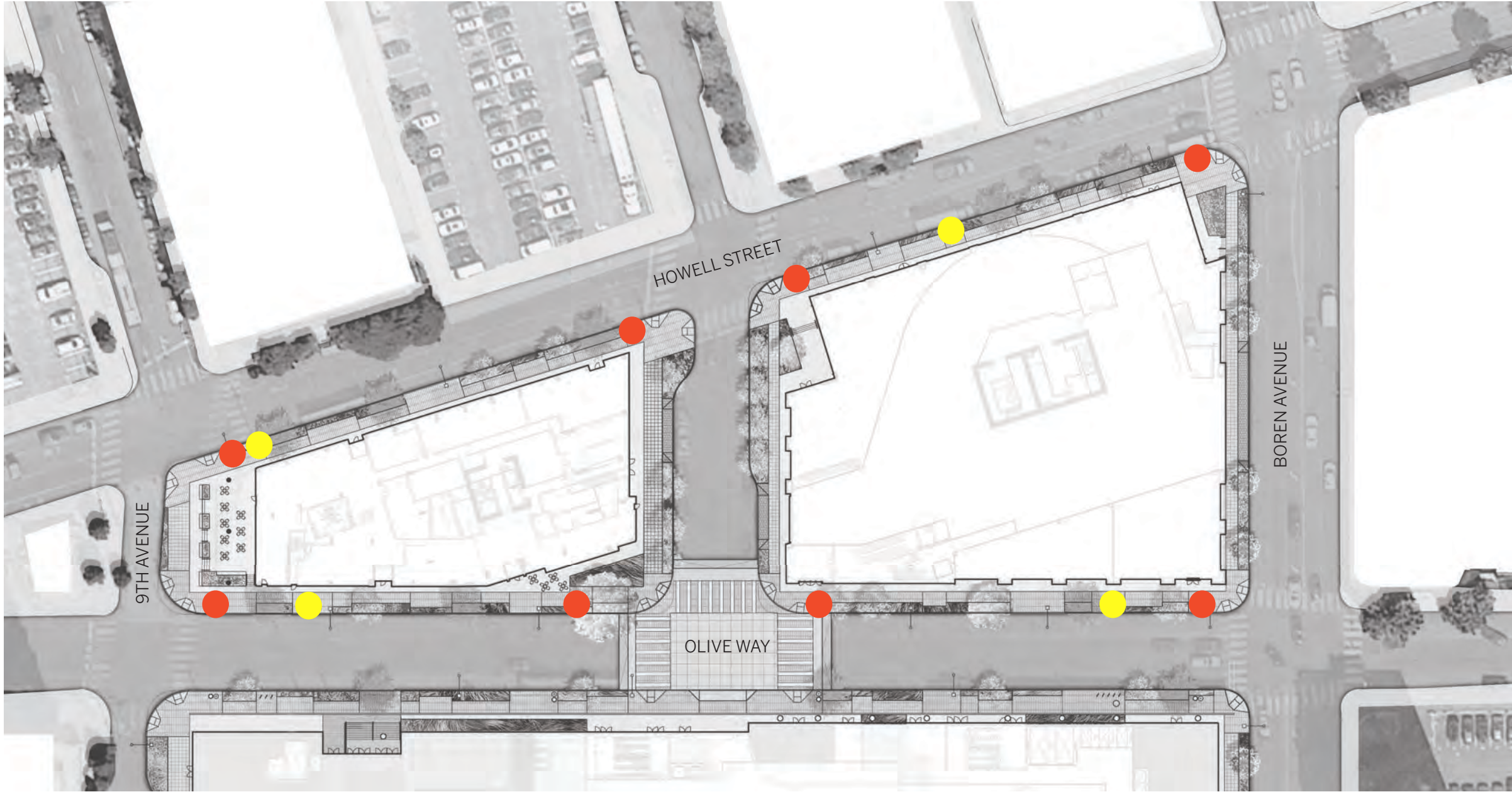
Mahonia nervosa  
Dwarf Oregon Grape



Polystichum munitum  
Sword Fern



4 DESIGN RECOMMENDATION LANDSCAPE





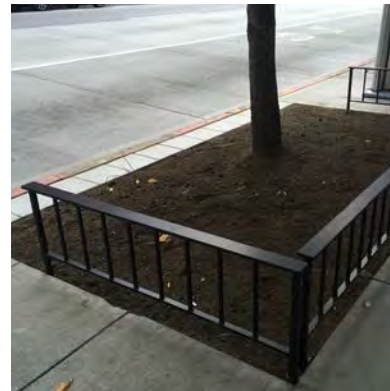
## 4 DESIGN RECOMMENDATION LANDSCAPE



Tofino Bicycle Rack ●



Trash/Recycling Receptacle ●

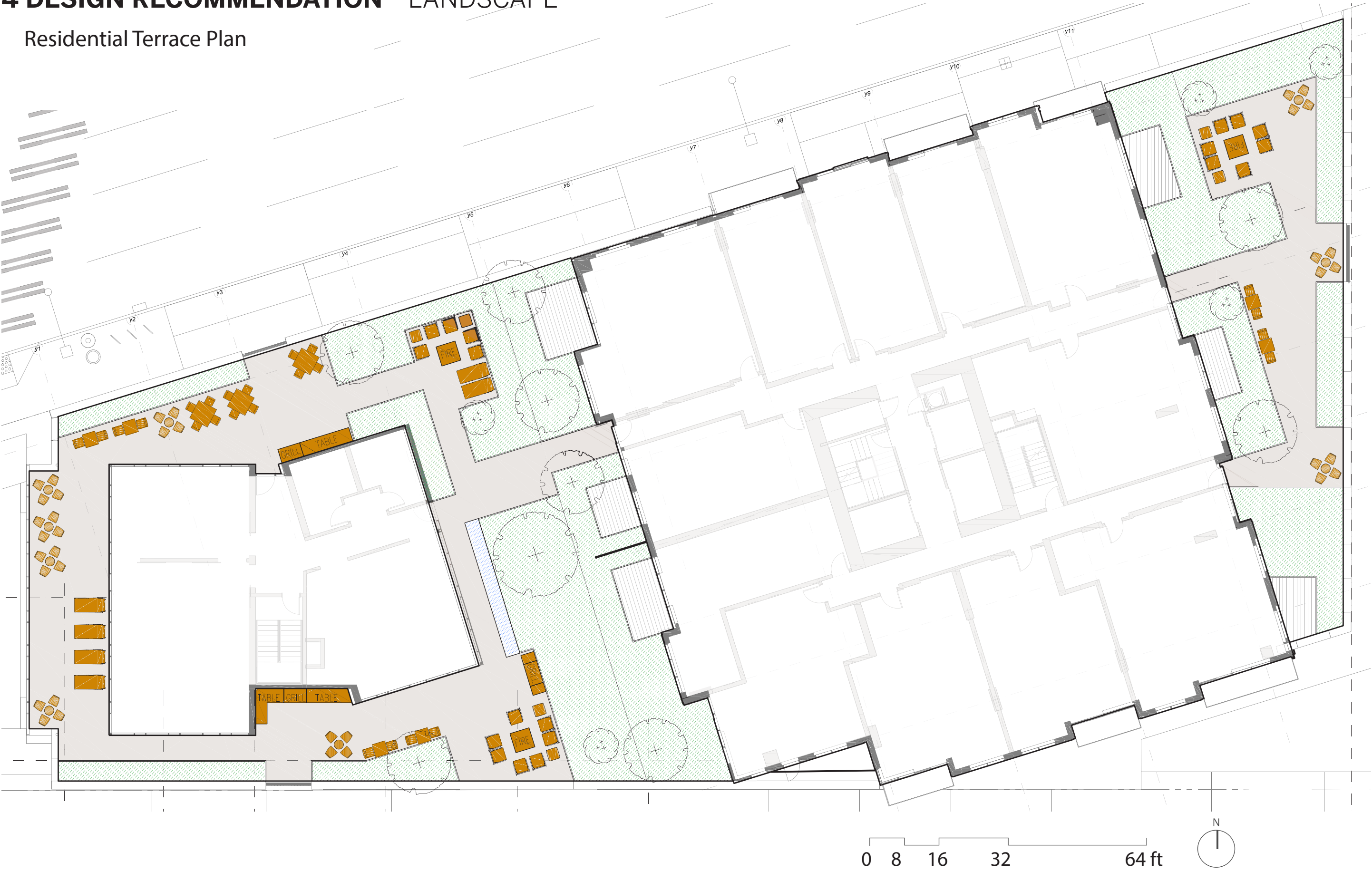


Planting Protection Fence  
(Adjoining all planting area)



**4 DESIGN RECOMMENDATION** LANDSCAPE

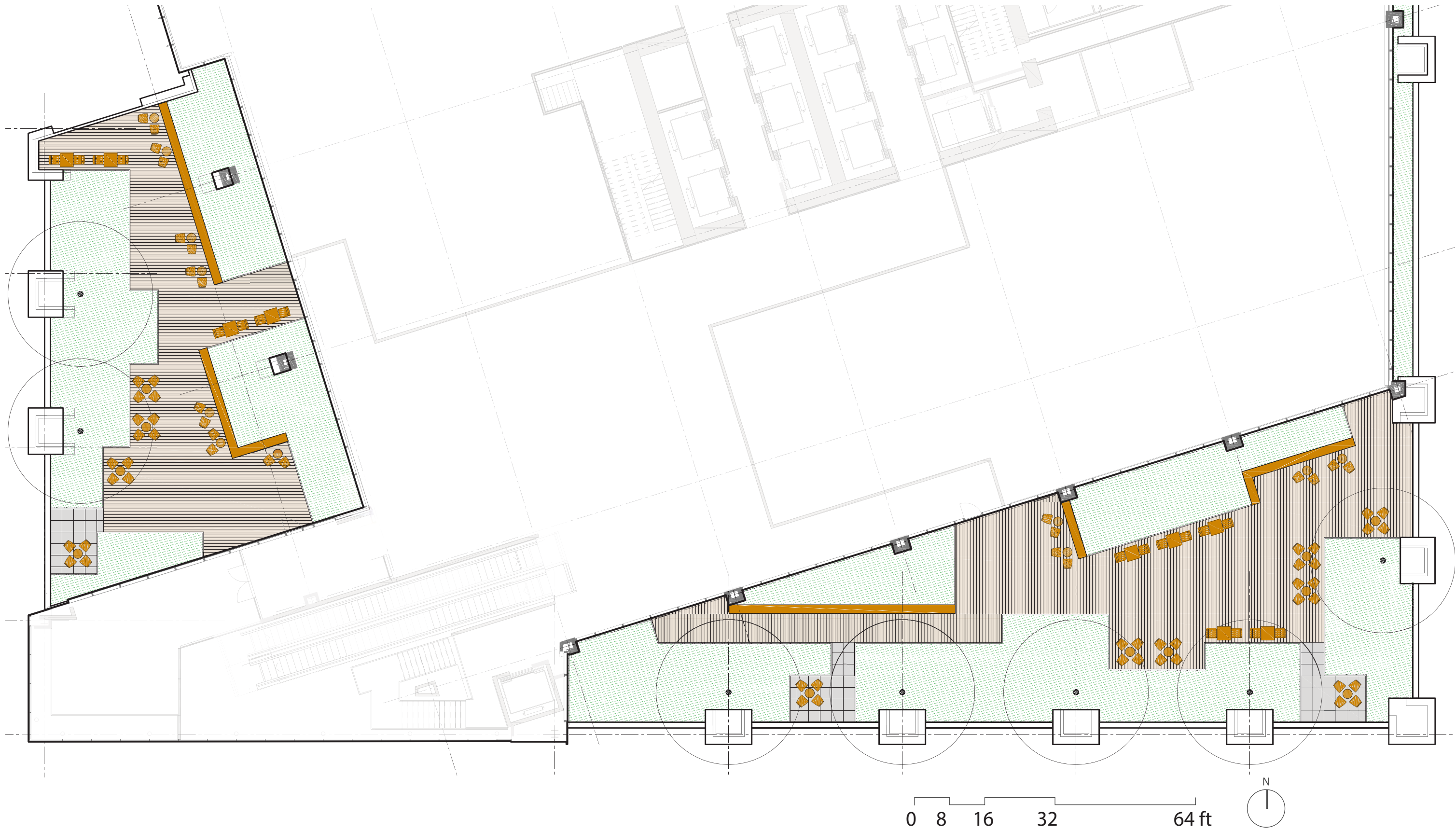
Residential Terrace Plan





**4 DESIGN RECOMMENDATION** LANDSCAPE

Commerical Terrace Plan





## 4 DESIGN RECOMMENDATION LANDSCAPE

### Residential and Commerical Terrace Trees



*Acer circinatum*  
Vine Maple



*Amelanchier alnifolia*  
Western Serviceberry



*Cornus 'Venus'*  
Venus Hybrid Dogwood



*Zelkova serrata 'Green Vase'*  
Japanese Zelkova

### Residential and Commercial Terrace Shrubs



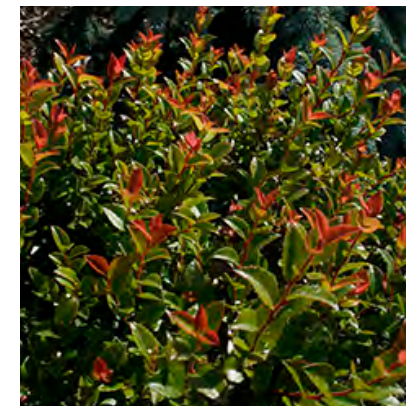
*Arctostaphylos columbiana*  
Hairy Manzanita



*Garrya elliptica 'James Roof'*  
Silk Tassel

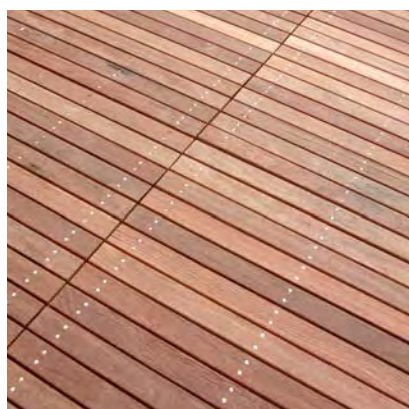


*Philadelphus lewisii 'Blizzard'*  
Blizzard Mock Orange



*Vaccinium ovatum 'Scarlet Ovation'*  
Scarlet Ovation Evergreen Huckleberry

### Residential and Commercial Terrace Hardscape



Hardwood Decking



## 4 DESIGN RECOMMENDATION LANDSCAPE

### Residential and Commerical Terrace Planting

Perennial/Groundcover, Sun



*Arctostaphylos uva-ursi*  
Wood's Compact Kinnikinnick



*Camassia quamash*  
Small Camas



*Fragaria vesca*  
Wild Strawberry



*Iris tenax*  
Oregon Iris



*Lewisia columbiana*  
Columbia Lewisia



*Gaultheria shallon*  
Salal



*Sedum oreganum*  
Oregon Stonecrop

### Residential and Commerical Terrace Planting

Perennial/Groundcover, Shade



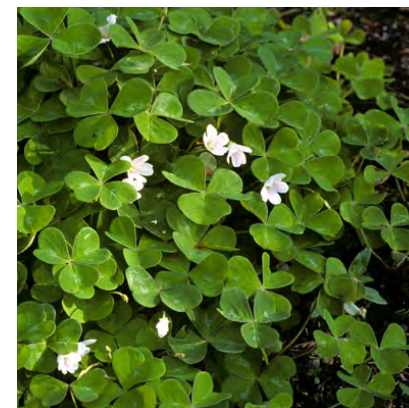
*Gymnocarpium dryopteris*  
Oak Fern



*Mahonia nervosa*  
Dwarf Oregon Grape



*Maianthemum dilatatum*  
False Lily-of-the-Valley



*Oxalis oregana*  
Redwood Sorrel



*Polystichum munitum*  
Sword Fern

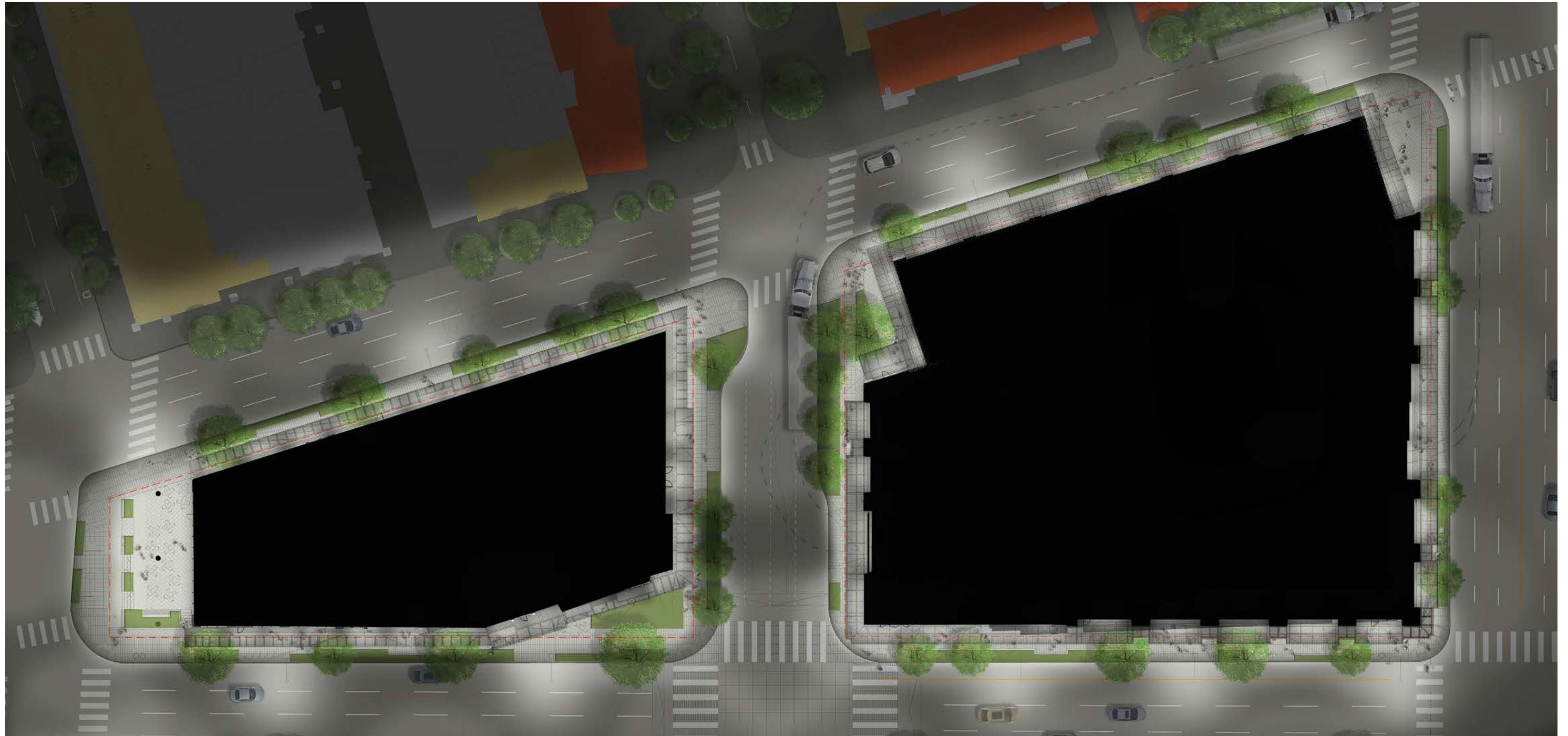


*Smilacina racemosa*  
Solomon's Plume



## 4 DESIGN RECOMMENDATION LIGHTING DESIGN

### LIGHTING DESIGN SITE PLAN





# 4 DESIGN RECOMMENDATION LIGHTING DESIGN

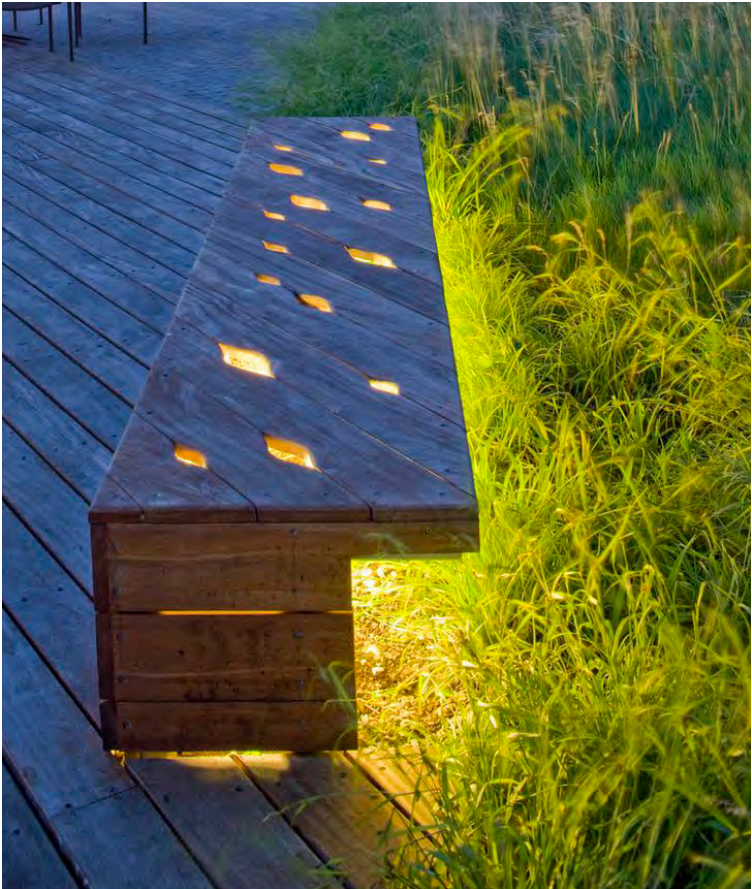
## LIGHTING REFERENCE



INTEGRATED CANOPY LIGHTING



INTEGRATED SITE LIGHTING



INTEGRATED LANDSCAPE LIGHTING



INTEGRATED SOFFIT LIGHTING



5 DESIGN GUIDELINES

SITE B - RESIDENTIAL

SITE PLANNING & MASSING

**A-1 Respond to the Physical Environment**  
Develop an architectural concept and compose the building’s massing in response to geographic conditions and patterns of urban form found beyond the immediate context of the building site.

The proposal includes building massing, streetscape, landscape, and public spaces composed to strengthen the surrounding urban framework and its location at the intersection of a multiplicity of diverse Seattle neighborhoods.



**Reinforce the Positive Urban Form & Architectural Attributes of the Immediate Area**

Consider the predominant attributes of the immediate neighborhood and reinforce desirable siting patterns, massing arrangements, and street-scape characteristics of nearby development.

The proposal infuses the attributes of the civic scale of downtown with the vibrancy of adjacent neighborhoods like Capitol Hill, reinforcing active urban streets with dynamic architectural character.



ARCHITECTURAL EXPRESSION

**B-2 Create a Transition in Bulk & Scale**  
Compose the massing of the building to create a transition to the height, bulk, and scale of development in neighboring or nearby less intensive zones.

The proposal occupies a smaller envelope than is possible by code, creating a transition on the edge of downtown. Rotated towers respond to the Denny Triangle grid while the podiums of both towers form a transition to the Olive Way corridor from downtown.



THE STREET-SCAPE

**C-1 Promote Pedestrian Interaction**

Spaces for street level uses should be designed to engage pedestrians with the activities occurring within them. Sidewalk-related spaces should be open to the general public and appear safe and welcoming.

The street-scape is designed to promote a vibrant urban pedestrian experience. Views into the building along with landscape elements, pedestrian amenities, street level lobbies, and retail activate the street.



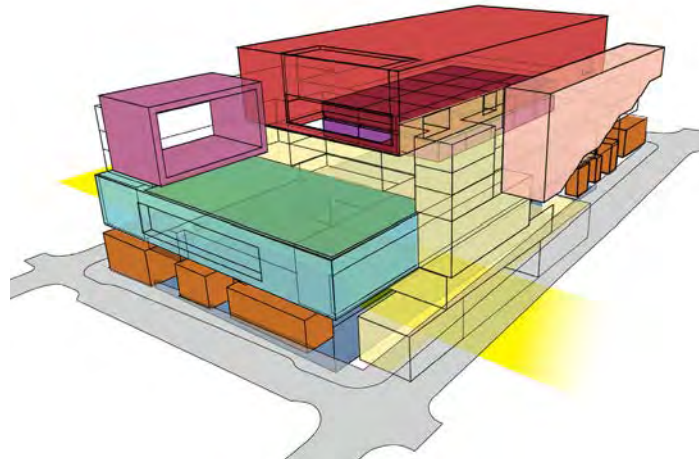


## 5 DESIGN GUIDELINES **SITE B** - RESIDENTIAL

### C-2 Design Facades of Many Scales

Design architectural features, fenestration patterns, and materials compositions that refer to the scale of human activities contained within. Building facades should be composed of elements scaled to promote pedestrian comfort, safety, and orientation.

The proposal incorporates architectural features that will respond to the scale of the pedestrian as well as the larger urban form.



### C-3 Provide Active–Not Blank–Facades

Buildings should not have large blank walls facing the street especially near sidewalks.

The proposal carefully considers the layout within the building to limit the amount of blank facades, particularly at the pedestrian level. Pedestrian edges are designed to allow visual access/visibility to both the public and amenity spaces of the building. This strategy sustains visual interest all along the pedestrian path, enhancing the overall experiential quality at street level.



## PUBLIC AMENITIES

### D-1 Provide Inviting & Usable Open Space

Design public open spaces to promote a visually pleasing, safe, and active environment for workers, residents, and visitors. Views and solar access from the principal area of the open space should be especially emphasized.

The proposal has provided opportunities for open space that provide both an amenity to the occupants of the facility, as well as contributing to a vibrant inviting urban street-scape.



### D-3 Enhance Elements that Define the Place

Provide special elements on the facades, within public open spaces, or on the sidewalk to create a distinct, attractive, and memorable “sense of place” associated with the building.

Each unique condition contributes to the urban framework. The site’s distinct edges create the potential for special moments enriched by the partnering of the building and the street-scape, as exemplified by the 9th Ave. plaza and the plazas on Terry Ave., along with architecturally-integrated landscaping.





## 5 DESIGN GUIDELINES **SITE B** - RESIDENTIAL

### VEHICULAR ACCESS & PARKING

#### **E-3 Minimize the Presence of Service Areas**

Locate service areas for trash dumpsters, loading docks, mechanical equipment, and the like away from the street front where possible. Screen from view those elements which for programmatic reasons cannot be located away from the street front.

The proposal carefully integrates loading and other service areas into the facility by directing them below-grade, thereby minimizing their street presence, shielding their adverse qualities and promoting a positive pedestrian experience. Loading doors have been minimized and setback to promote the street level activity along the main building facade.





5 DESIGN GUIDELINES

SITE C - OFFICE

SITE PLANNING & MASSING

A-1 Respond to the Physical Environment

Develop an architectural concept and compose the building’s massing in response to geographic conditions and patterns of urban form found beyond the immediate context of the building site.

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THE STREET-SCAPE

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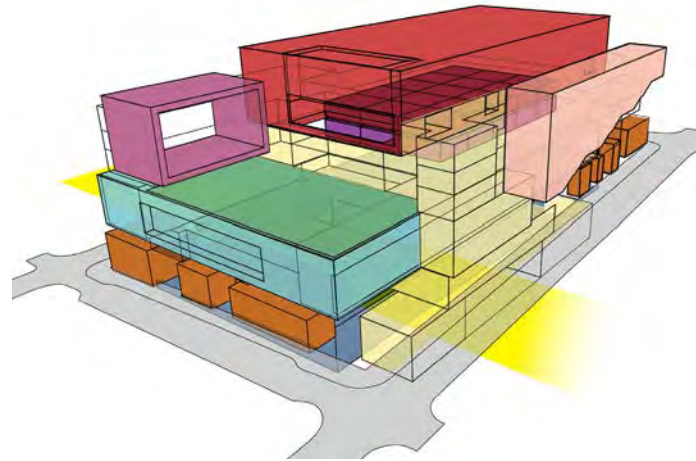


## 5 DESIGN GUIDELINES SITE C - OFFICE

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Each unique condition contributes to the urban framework. The site’s distinct edges create the potential for special moments enriched by the partnering of the building and the street-scape, as exemplified by the 9th Ave. plaza and the plazas on Terry Ave., along with architecturally-integrated landscaping.





## 5 DESIGN GUIDELINES SITE C - OFFICE

### VEHICULAR ACCESS & PARKING

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Locate service areas for trash dumpsters, loading docks, mechanical equipment, and the like away from the street front where possible. Screen from view those elements which for programmatic reasons cannot be located away from the street front.

The proposal carefully integrates loading and other service areas into the facility by directing them below-grade, thereby minimizing their street presence, shielding their adverse qualities and promoting a positive pedestrian experience. Loading doors have been minimized and setback to promote the street level activity along the main building facade.





6 PROPOSED DEPARTURES

DEPARTURES

SITE B - RESIDENTIAL

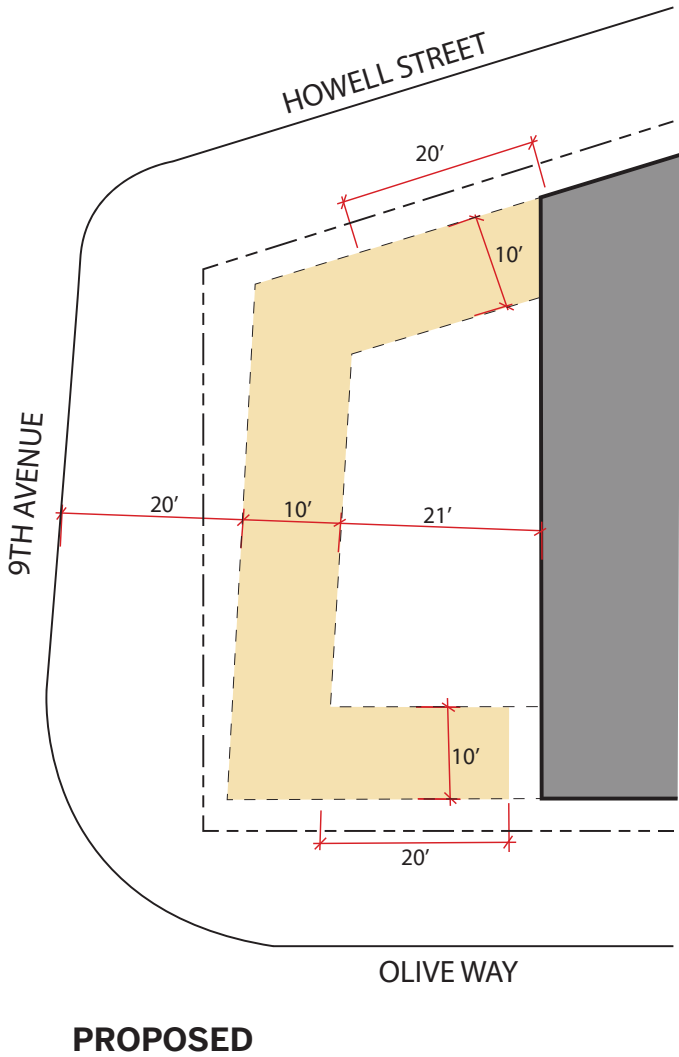
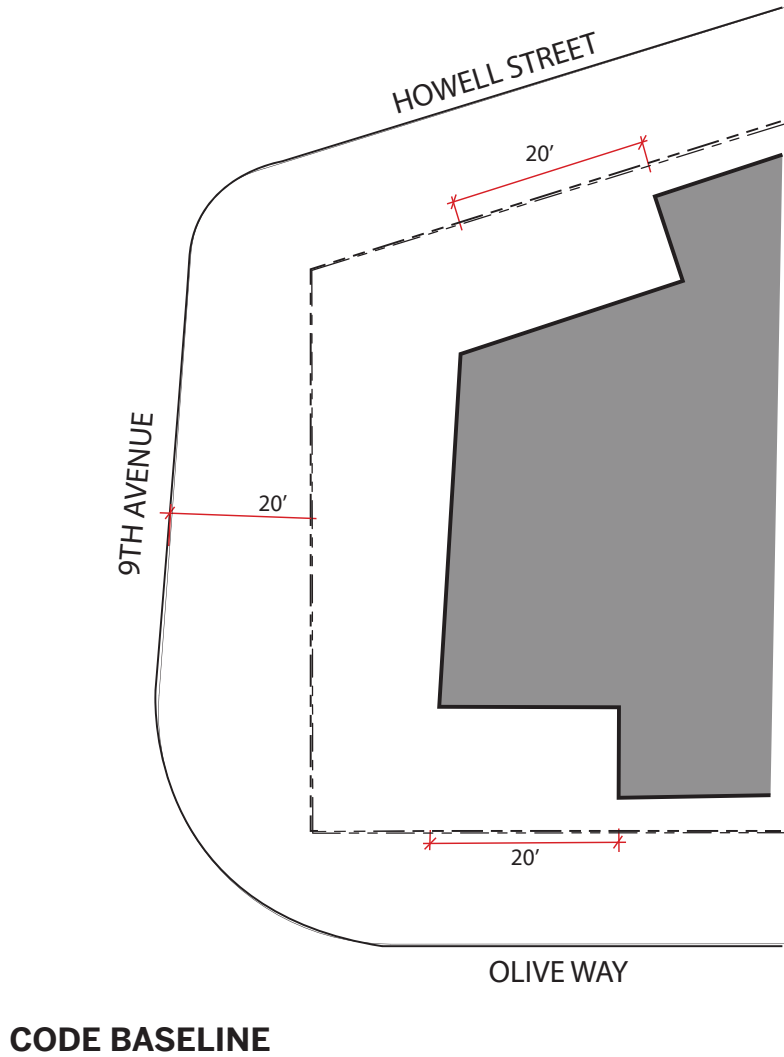
| Item # | Development Standard                  | Requirement   | Modification Requested   | Rationale   |
|--------|---------------------------------------|---|--|---|
| 1      | 23.49.056.B.2<br>Facade Setback Limit | The maximum setback of the facade at an intersection is 10 feet. The minimum distance the facade must conform is 20 feet from the corner along each street. | Relief from the 10 foot maximum setback limit at the intersections of Howell street/ ninth Ave. and Olive way/ninth Ave. The modification is requesting an additional 21' of setback from the code baseline. | The building setback along 9th Ave. creates a larger outdoor amenity for seating or dining adjacent to active street level uses. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guideline section. |

Relevant Design Guidelines - Departure #1

B3.1 building orientation - orient open space toward street intersections and towards street fronts with the highest pedestrian activity (open space oriented towards intersection and active streets)

B3.3 pedestrian amenities at the ground level - setting the building back to create space adjacent to the sidewalk conducive to pedestrian-oriented activities (building setback to create space adjacent to the sidewalk)

D1.1 pedestrian enhancements  
B. Preferable open space locations are to the south and west of tower development, or where the siting of the open spaces would improved solar access to the sidewalk (located on the west facade at the sidewalk)  
C. Orient to receive the maximum direct sunlight possible, while providing shade in the warmer months. (setback and overhang balance the solar needs)





6 PROPOSED DEPARTURES

DEPARTURES

SITE B - RESIDENTIAL

| Item # | Development Standard                  | Requirement   | Modification Requested  | Rationale   |
|--------|---------------------------------------|---|---|---|
| 3      | 23.49.056.B.2<br>Facade Setback Limit | The maximum setback of the facade at an intersection is 10 feet. The minimum distance the facade must conform is 20 feet from the corner along each street. | Relief from the 10 foot maximum setback limit at the intersection of Olive Way and Terry Ave. The modification is requesting an additional 10' of setback from the code baseline. | The building is set back at the corner of Olive Way and Terry Ave. to provide a wider sidewalk and planted area along the green street with better solar access. The corner creates a transition to the north entry of the proposed convention center beyond and provides a generous terminus to the green street. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guideline section. |

Relevant Design Guidelines - Departure #3

A1.1 response to context

A. A change in street grid alignment that yields a site having a nonstandard shape (facade orientation responds to shift in street grid)

D. Access to direct sunlight-seasonally or at particular times of day (facade is set back to allow for solar access)

A1.2 response to planning efforts

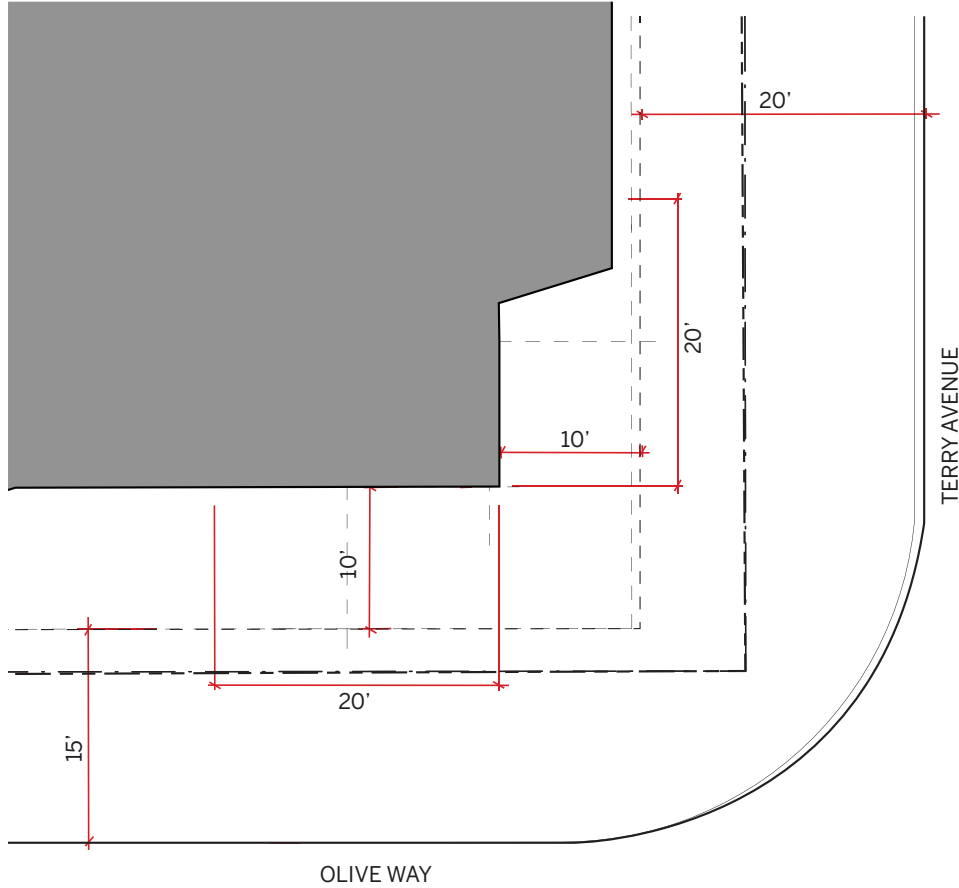
(Denny triangle neighborhood, transitional environment, design for future context - informality, use of green streets and pocket planting, strengthen pedestrian connections)

D1.1 pedestrian enhancements

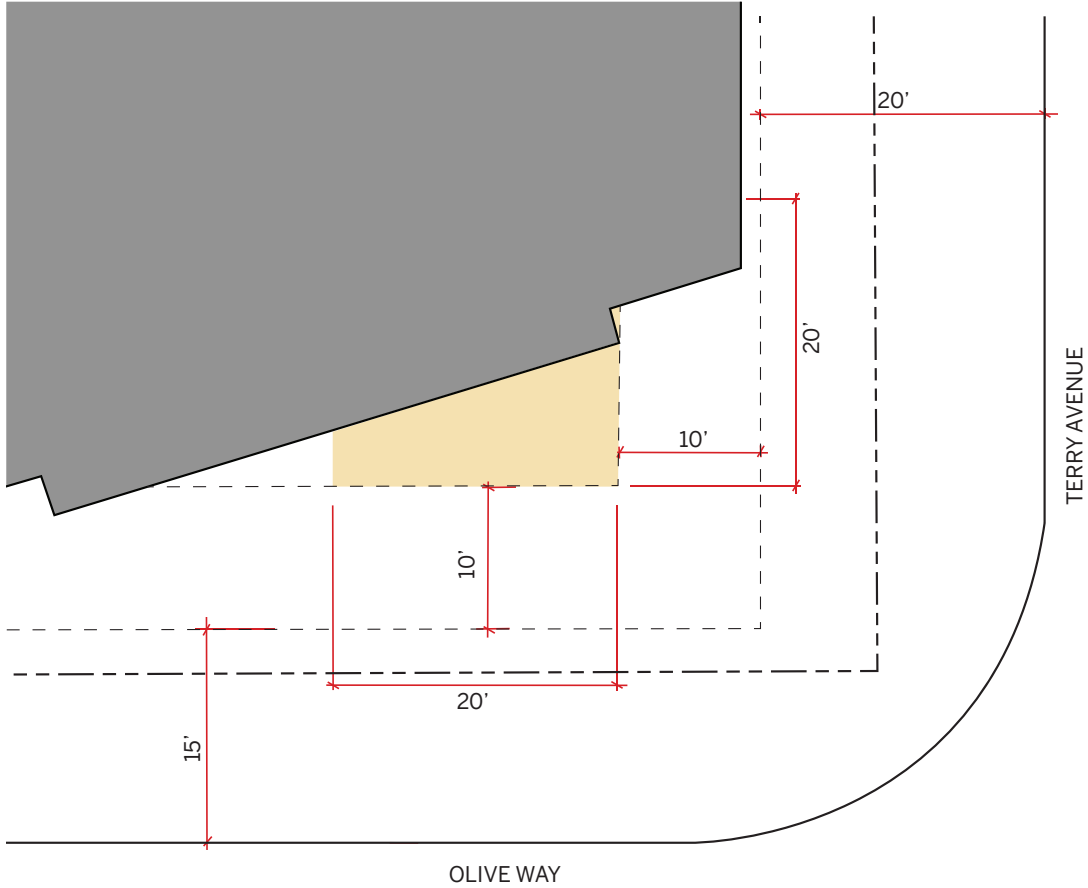
A. Open space elements should enhancement a pedestrian oriented, urban environment that has the appearance of stability, quality, and safety (clearly defined edges at active intersection)

B. Preferable open space locations are to the south and west of tower development, or where the siting of the open spaces would improved solar access to the sidewalk (located on the south facade at the sidewalk)

D. The design of landscaping should allow visibility into and out of the space (maintain visibility while providing a sense of protected enclosure)



CODE BASELINE



PROPOSED



6 PROPOSED DEPARTURES

DEPARTURES

SITE B - RESIDENTIAL

| Item # | Development Standard          | Requirement   | Modification Requested   | Rationale   |
|--------|-------------------------------|---|--|---|
| 4      | 23.49.009<br>Street Level Use | A minimum of 75% of each street frontage at the street level where street level uses are required, must be occupied by uses listed in subsection 23.49.009.a. | Relief from the 75% street level use required along Terry Ave. | The building lobby is located along Olive Way and shares frontage along Terry Ave. and is designed to be an open space that is connected with the retail spaces beyond to bring more activity and use in the building lobby and at the street level along Terry. The loading is a necessary building use and is reduced to a minimum dimension. Locating the loading in this location allows us to provide more street level use on the other sides of the building. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guideline section. |

Relevant Design Guidelines - Departure #4

C1.1 street level uses

A. Reinforce existing retail concentrations;

B. Vary in size, width, and depth

C. Enhance main pedestrian links between areas; and

D. Establish new pedestrian activity

C1.3 street level articulation for pedestrian activity

- setting the building back to create space adjacent to the sidewalk conducive to pedestrian-oriented activities

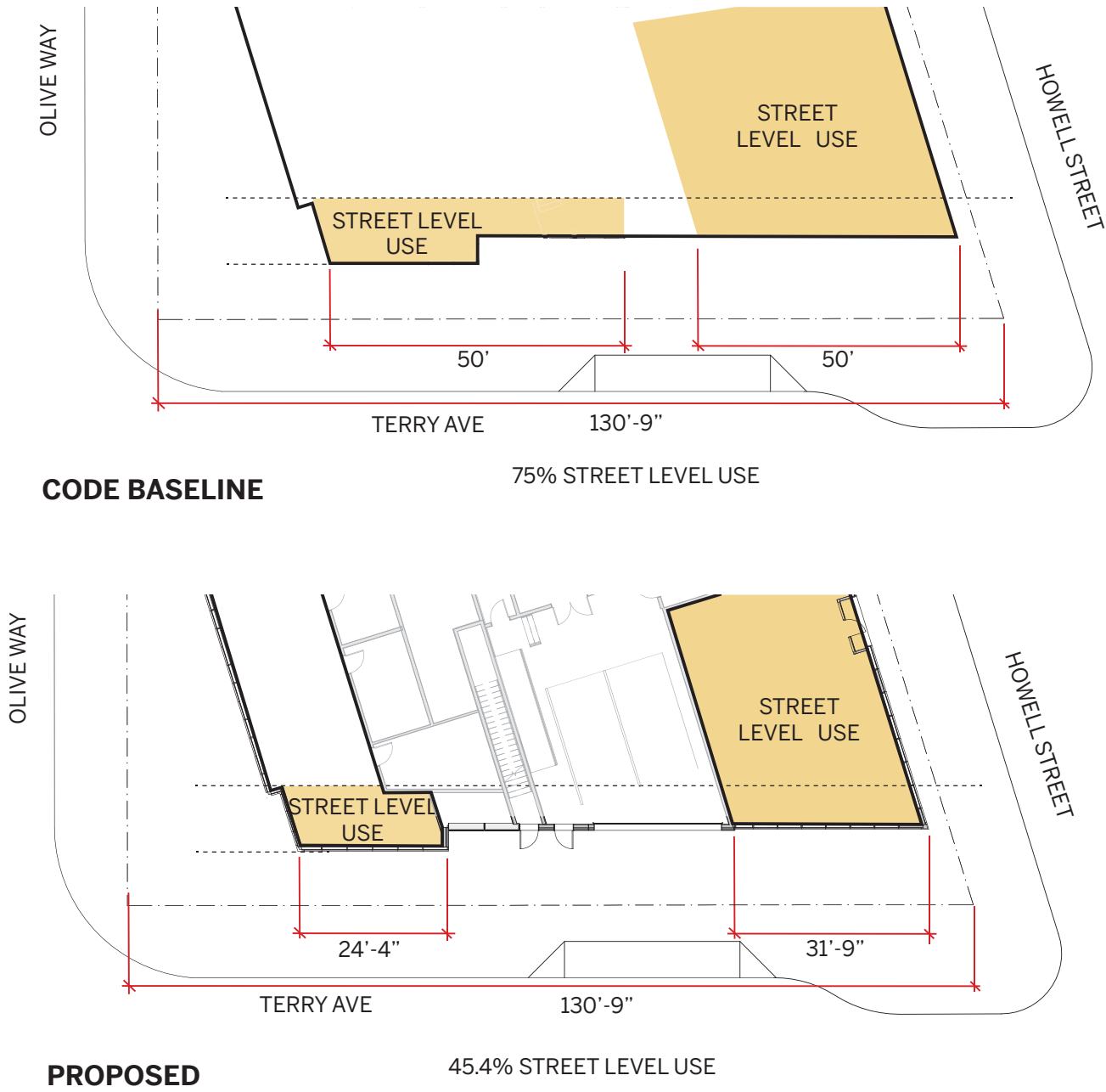
G. Windows that encourage pedestrians to look into building interior

I. Street front open space

E3.1 integrating service areas

A. Plan service areas for less visible locations

B. Screen service areas to be less visible





6 PROPOSED DEPARTURES

DEPARTURES

SITE B - RESIDENTIAL

| Item # | Development Standard                               | Requirement  | Modification Requested  | Rationale   |
|--------|--|--|---|---|
| 5      | 23.49.056.F.4<br>Upper Level Green Street Set-back | On a designated green street, a continuous upper level setback of 15 feet is required for portions of the structure above a height of 45 feet. | Relief from the setback of 15 feet for portions of the structure above a height of 45 feet. | The entire tower is setback beyond what is required by code to open up the green street and provide greater solar access. A portion of the podium is within the required setback in order to clarify the massing and intersection of building forms along the shift in the street grid at Howell street. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guideline section. |

Relevant Design Guidelines - Departure #5

A1.1 response to context

A. A change in street grid alignment that yields a site having a nonstandard shape

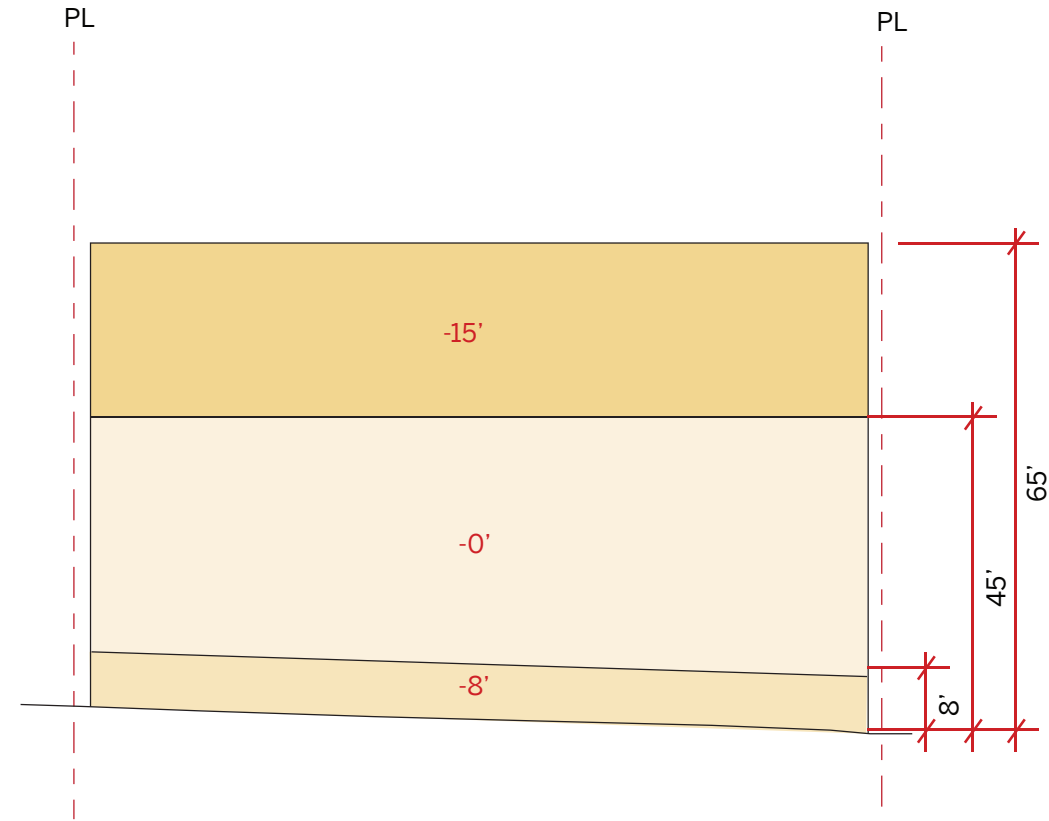
C. Patterns in urban form, such as nearby buildings that employ distinctive and effective massing compositions

D. Access to direct sunlight-seasonally or at particular times of day

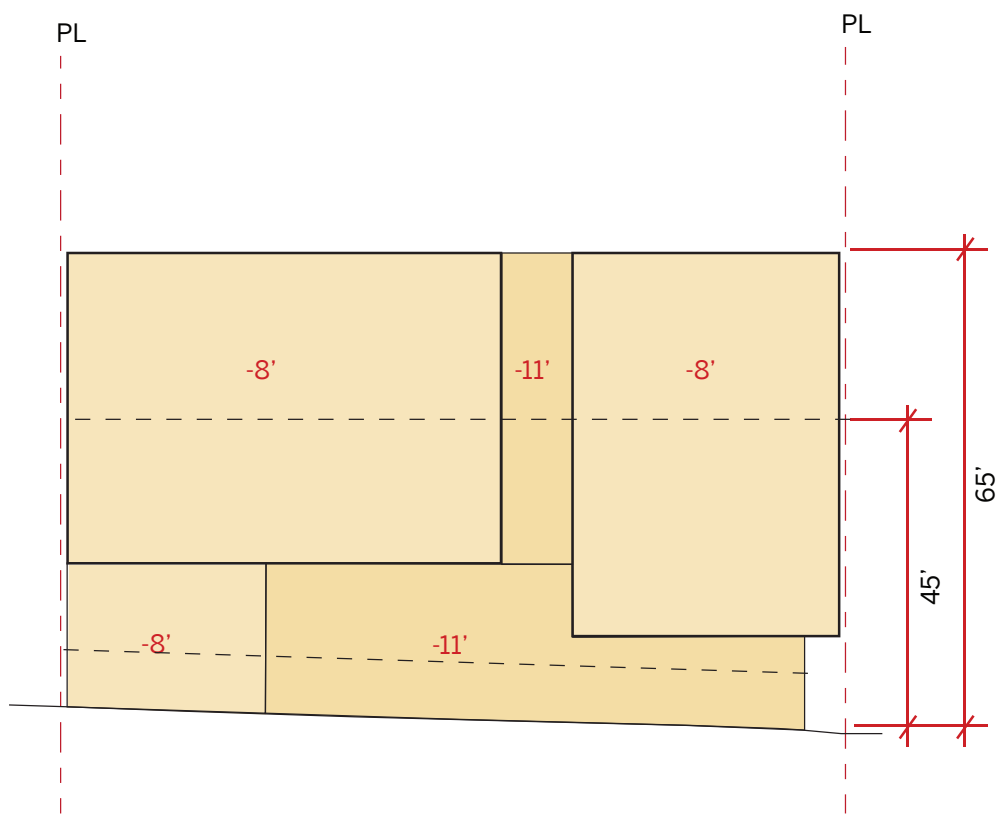
B4.1 massing

A. Setbacks, projections, and open space

B. Relative sizes and shapes of distinct building volumes



CODE BASELINE



PROPOSED



6 PROPOSED DEPARTURES

DEPARTURES

SITE B - RESIDENTIAL

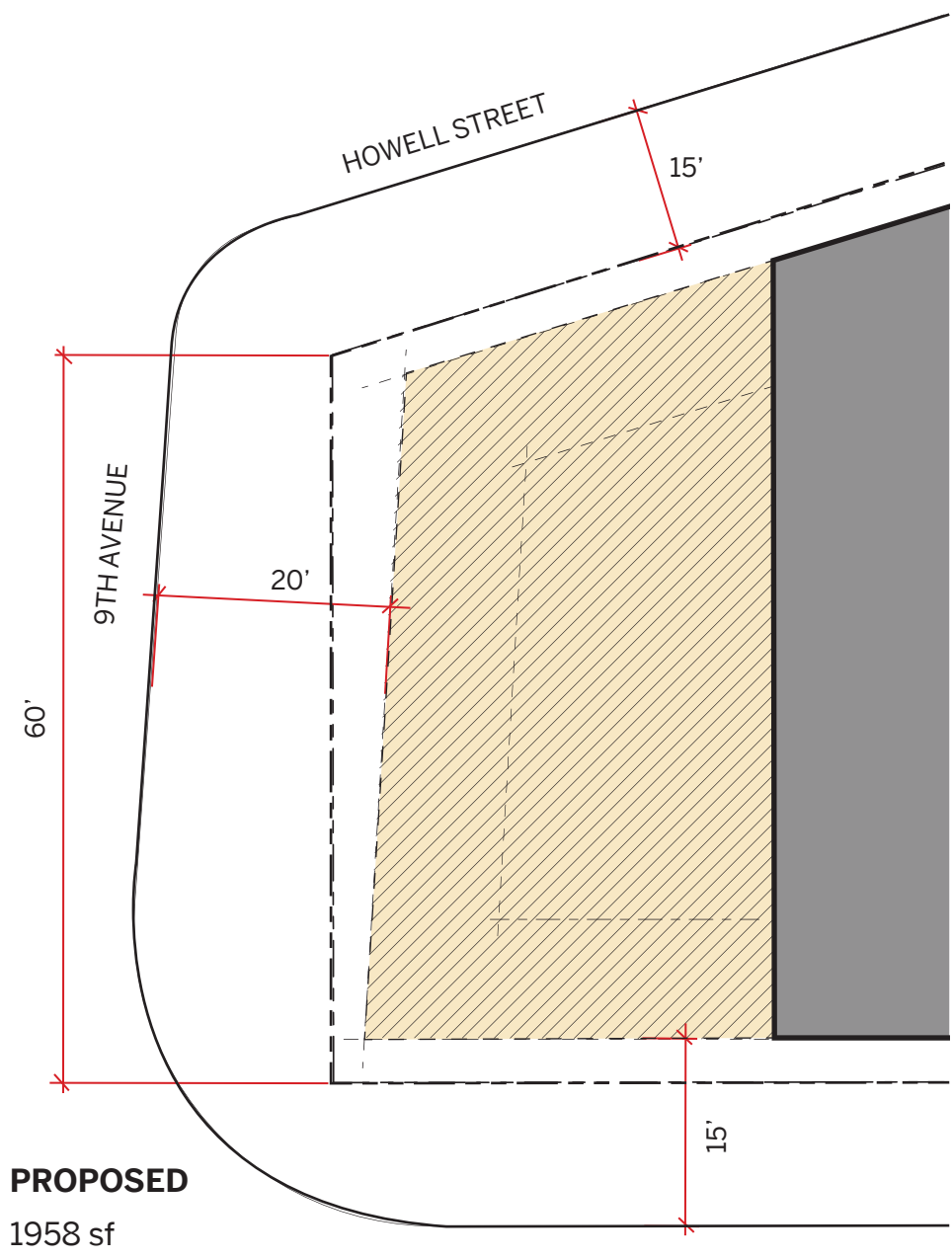
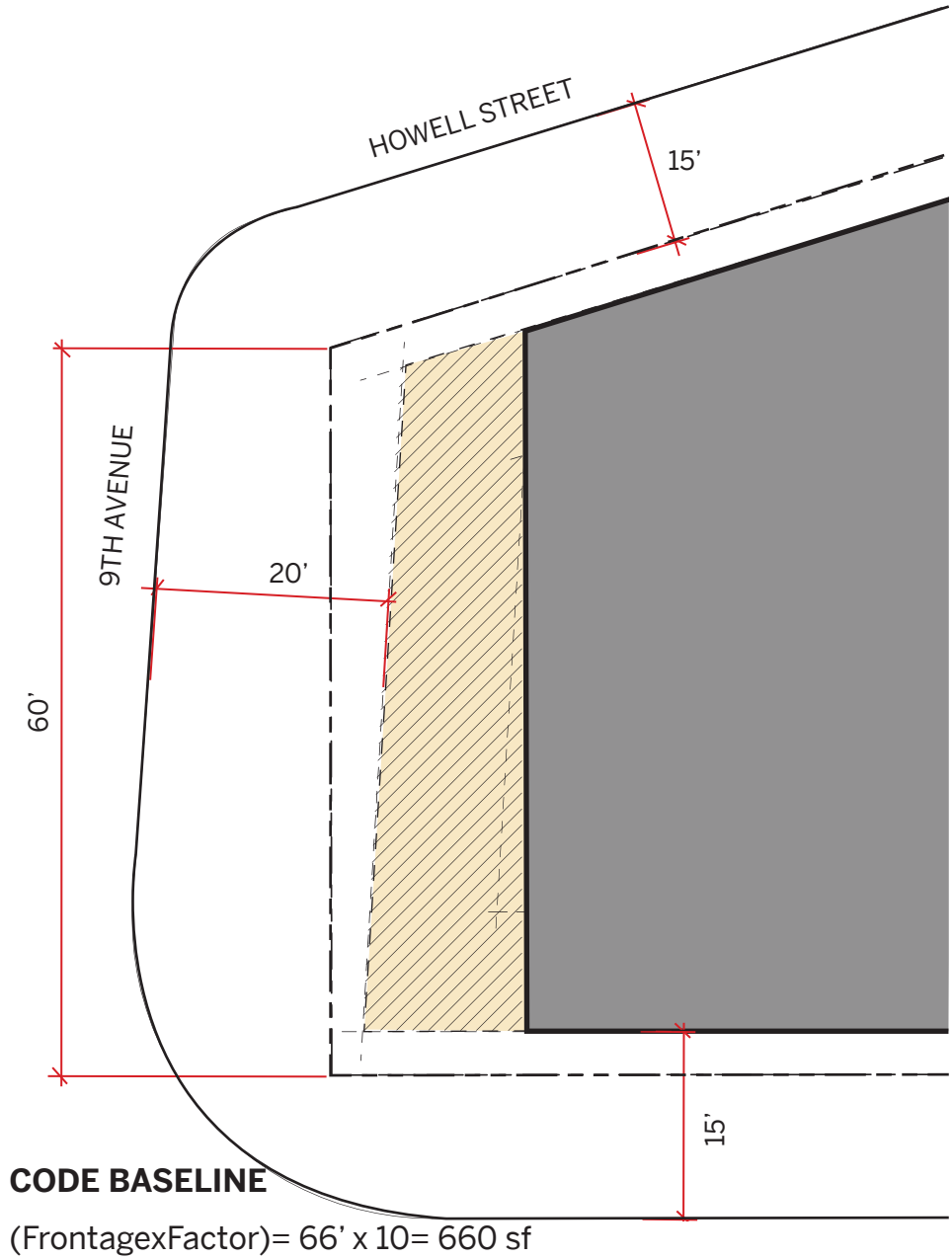
| Item # | Development Standard                     | Requirement   | Modification Requested   | Rationale  |
|--------|--|---|--|--|
| 6      | 23.49.056.C.2.b<br>Facade Setback Limits | The maximum area of all setbacks between the street lot line and facade along each street frontage of a lot shall not exceed the area derived by multiplying the averaging factor by the width of street frontage of the structure along that street. | Proposed area of setback to be greater than limit allowed per factor calculations. | The building setback along 9th Ave. creates a larger public outdoor amenity adjacent to active street level uses. The building setback is supported by the design review board. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guideline section. |

Relevant Design Guidelines - Departure #6

B3.3 pedestrian amenities at the ground level - setting the building back to create space adjacent to the sidewalk conducive to pedestrian-oriented activities (building setback to create space adjacent to the sidewalk)

B4.1 massing  
A. Setbacks, projections, and open space (set backs and projections define building volumes, and exterior open spaces)

D1.1 pedestrian enhancements  
B. Preferable open space locations are to the south and west of tower development, or where the siting of the open spaces would improved solar access to the sidewalk (located on the west facade at the sidewalk)  
C. Orient to receive the maximum direct sunlight possible, while providing shade in the warmer months. (setback and overhang balance the solar needs)





6 PROPOSED DEPARTURES

DEPARTURES

SITE B - RESIDENTIAL

| Item # | Development Standard                         | Requirement  | Modification Requested  | Rationale   |
|--------|--|--|---|---|
| 7      | 23.49.056.D.2.a<br>Blank Facade Requirements | Blank facades shall be no more than 15 feet wide except segments with garage doors may exceed a width of 15 feet and may be as wide as the driveway plus 5 feet. | The blank facade at the gas meters along Howell St. is proposed to be 16 feet wide. A 24 feet wide blank facade is proposed at Terry Ave. for mechanical systems and an exit stair. | The gas meters and mechanical systems are a necessary building service and have been reduced to a minimum dimension. The gas meters have been placed away from the street level use and pedestrian activated storefront along Howell St. The mechanical louvers are grouped with the exit stair to provide as much street level use and transparency as possible along Terry Ave. |

Relevant Design Guidelines - Departure #7

C1.1 street level uses

A. Reinforce existing retail concentrations;

B. Vary in size, width, and depth

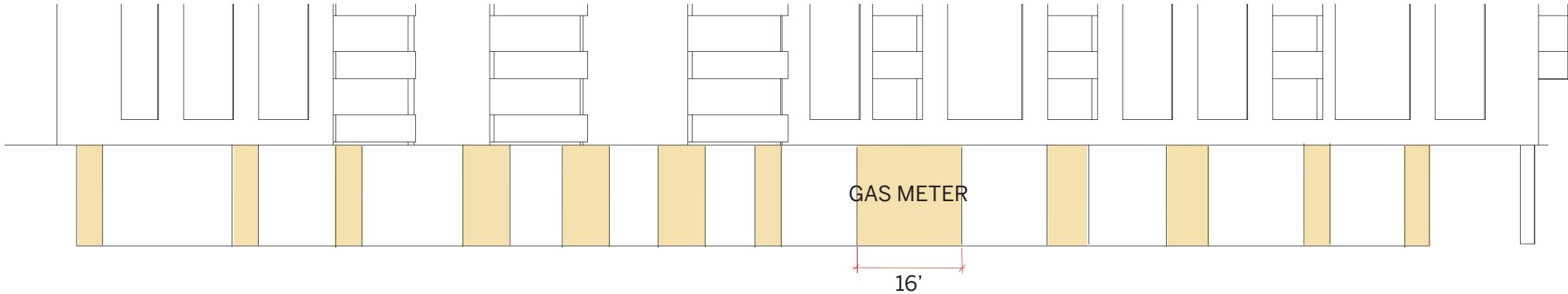
C. Enhance main pedestrian links between areas; and

D. Establish new pedestrian activity

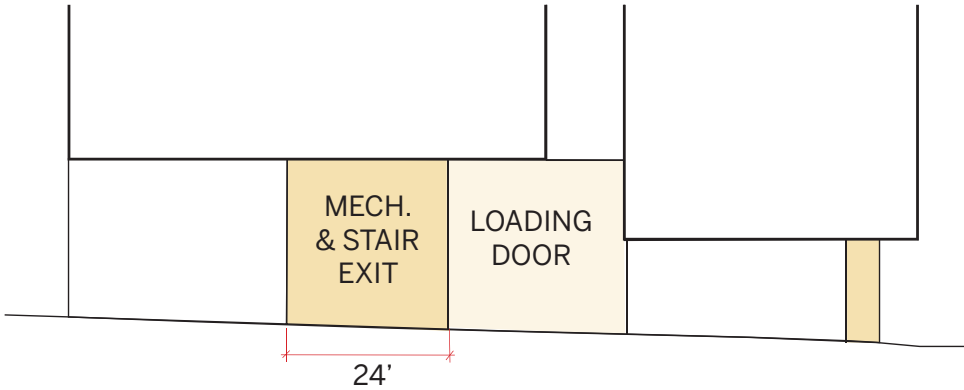
E3.1 integrating service areas

A. Plan service areas for less visible locations

B. Screen service areas to be less visible



HOWELL STREET ELEVATION



TERRY AVE. ELEVATION



6 PROPOSED DEPARTURES

DEPARTURES

SITE B - RESIDENTIAL

| Item # | Development Standard                   | Requirement   | Modification Requested  | Rationale  |
|--------|--|---|---|--|
| 1      | 23.49.056.B.2D<br>Facade Setback Limit | The maximum setback of the facade at an intersection is 10 feet. The minimum distance the facade must conform is 20 feet from the corner along each street. | Relief from the minimum setback of the facade from the street lot line at intersections. The modification is requesting an additional 13' of setback from the baseline. | The proposed design responds to the shift in the street grid at Howell Street, reflected in the massing of the tower, the form of the tower is carried through to the ground, adding clarity to the overall design and providing for a more generous sidewalk. The additional pedestrian space at grade eases the transition through the intersection and allows for better visibility across the changing street grid. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guideline section. |

Relevant Design Guidelines - Departure #1

A1.1 response to context

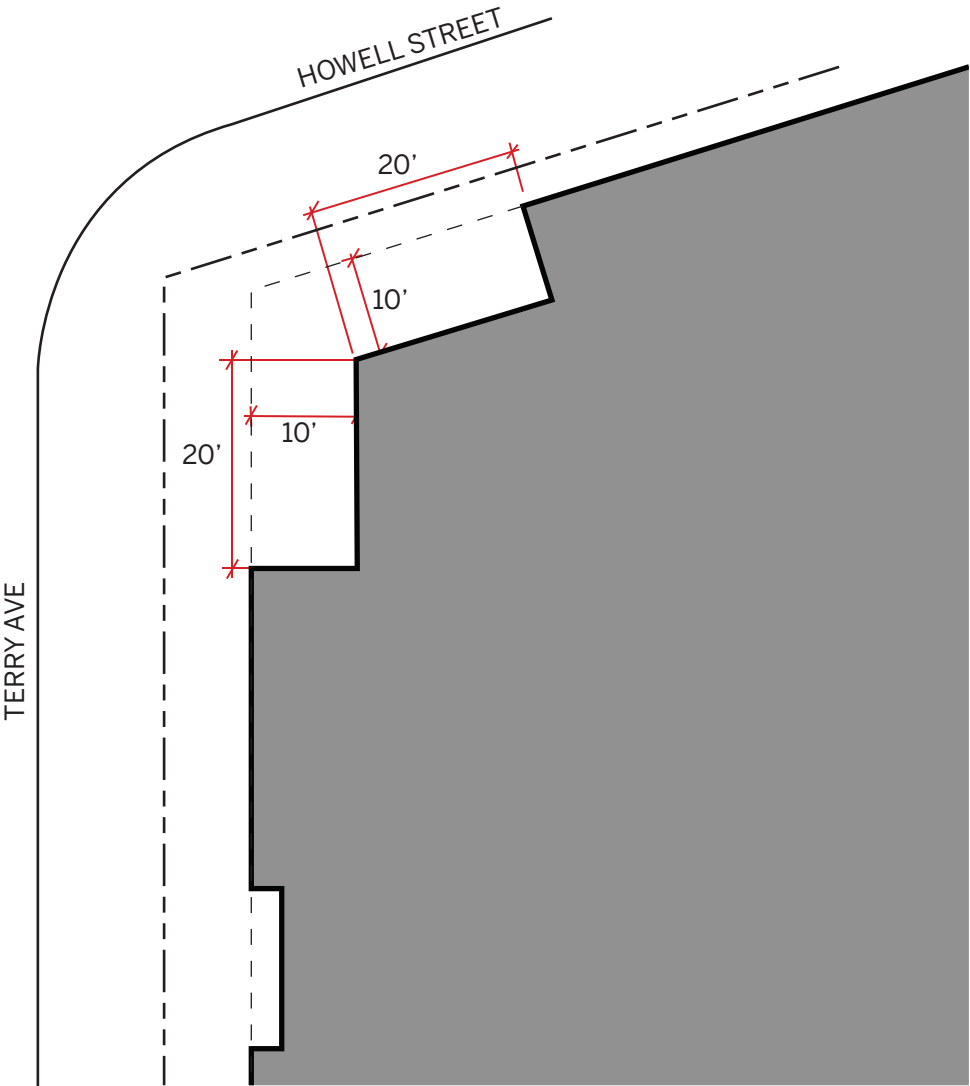
A. A change in street grid alignment that yields a site having a nonstandard shape

D. Access to direct sunlight-seasonally or at particular times of day

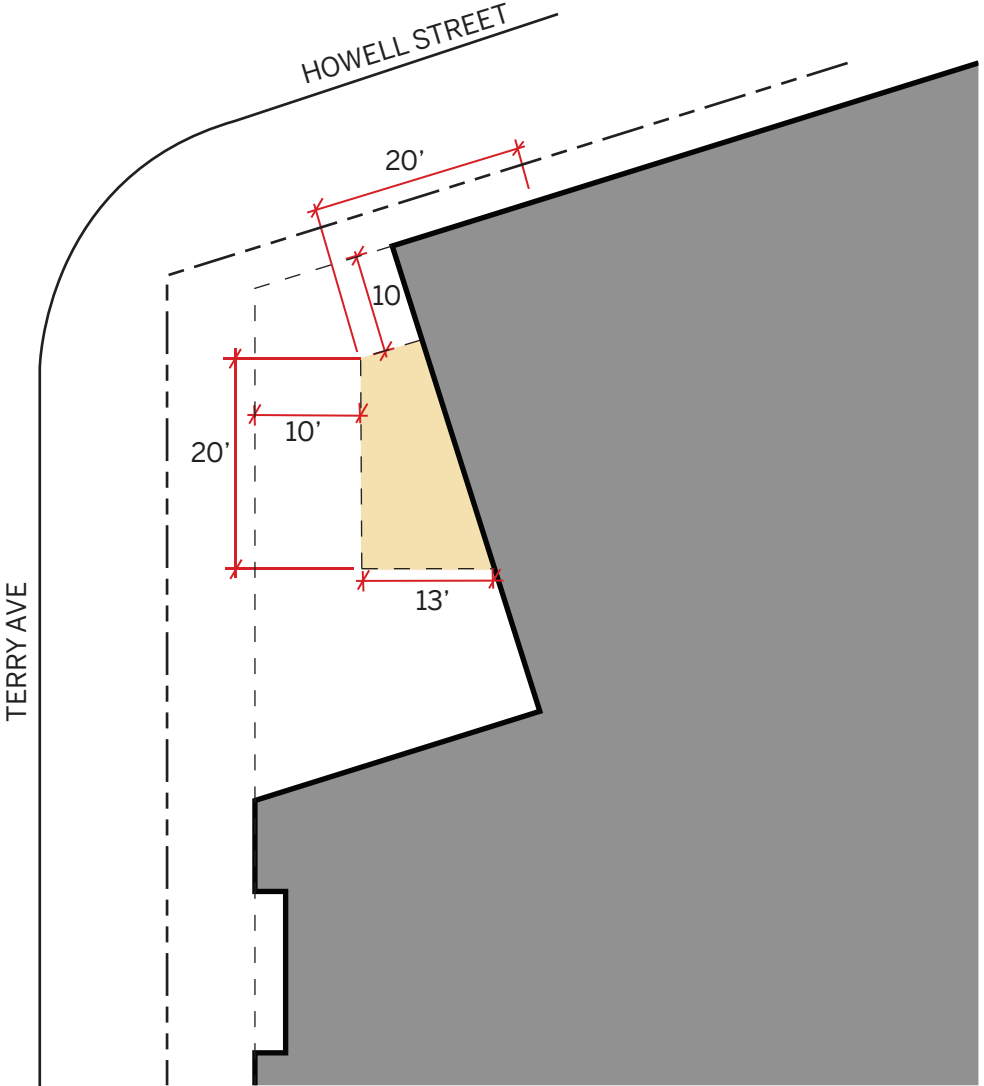
B4.1 massing

A. Setbacks, projections, and open space

B. Relative sizes and shapes of distinct building volumes



CODE BASELINE



PROPOSED



6 PROPOSED DEPARTURES

DEPARTURES

SITE C - OFFICE

| Item # | Development Standard              | Requirement  | Modification Requested   | Rationale  |
|--------|-----------------------------------|--|--|--|
| 2      | 23.49.058.C<br>Unmodulated Facade | Facade modulation is required above a height of 85 feet above the sidewalk of any portion of a structure located within 15 feet of a street lot line. The maximum length of unmodulated facade within 15 feet of a street lot line is 155 feet at a height between 86-160 feet, 125 feet at a height between 161-240 feet, and 100 feet at a height between 241-500 feet. Any portion of a facade exceeding the maximum length of a facade prescribed above (listed in 23.49.058 table a) shall be set back a minimum of 15 feet from the street lot line for a minimum distance of 60 feet before any other portion may be within 15 feet of the street lot line. | Relief from the upper level facade modulation requirements between 85' and 240'. The design is proposing a 54'-6" wide modulation that is setback 6' from the property line starting at 41' and extending up to 240'. This provides an additional 1,545 sf of modulation along the full tower width at Howell Street. The design is proposing a 6 foot setback at the modulation rather than the required 15 foot setback. | The proposed facade modulation supports the overall building massing response to the shift in the street grid at Howell Street, by providing a clear frame of reference to both grids. The proposed modulation occurs over a larger area than required by code, and creates the simplified singular move encouraged by the design review board. The size and scale of the setback, a direct response to the building across Howell Street to the north, defines itself as a distinct element from the rest of the building facade. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guidelines section. |

Relevant Design Guidelines - Departure #2

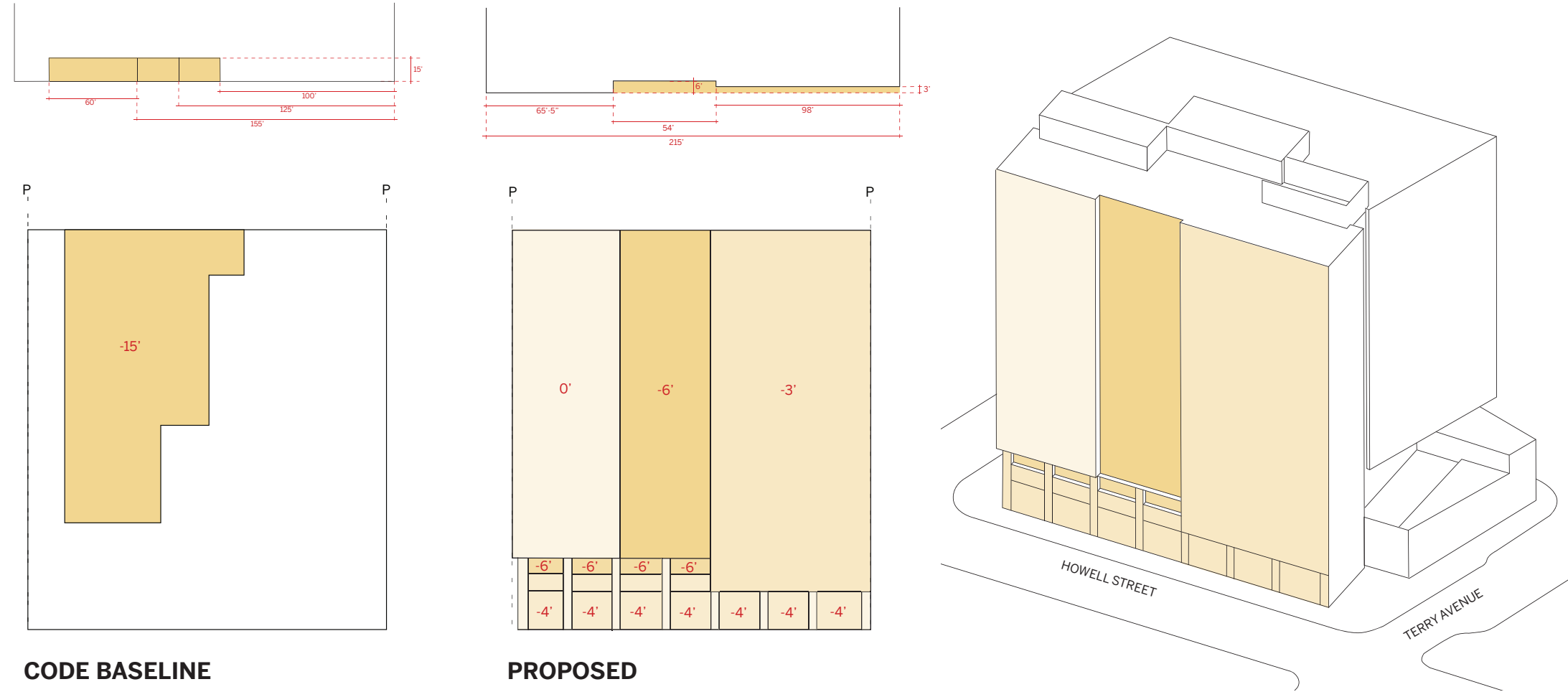
A1.1 response to context

A. A change in street grid alignment that yields a site having a nonstandard shape

B4.1 massing

A. Setbacks, projections, and open space (set backs and projections define building volumes, and exterior open spaces)

B. Relative sizes and shapes of distinct building volumes (distinct volumes relate to scale of surrounding context)





6 PROPOSED DEPARTURES

DEPARTURES

SITE C - OFFICE

| Item # | Development Standard          | Requirement  | Modification Requested  | Rationale   |
|--------|-------------------------------|--|---|---|
| 3      | 23.49.009<br>Street Level Use | A minimum 75% of each street frontage at street level where street level uses are required must be occupied by uses listed in subsection 23.49.009.a required street level use shall be located within 10 feet of street line. | Relief from the 75% street level use requirement along Terry Ave., and from street level use within 10 feet of street lot line. | The building is providing loading egress along Terry Ave. (being reviewed under type 1 application process) with the smallest building opening possible. The main pedestrian entrance is located at the corner of Terry Ave. and Olive Way with the desire to bring part of the lobby frontage along Terry to activate the facade at street level. This frontage is a small contribution to the over-all width along Terry Ave. The remaining frontages along Howell Street, Olive Way and Boren Ave. have been maximized for street level uses, where none are required by code. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guidelines section. |

Relevant Design Guidelines - Departure #3

C1.1 street level uses

A. Reinforce existing retail concentrations;

B. Vary in size, width, and depth

C. Enhance main pedestrian links between areas; and

D. Establish new pedestrian activity

C1.2 retail orientation

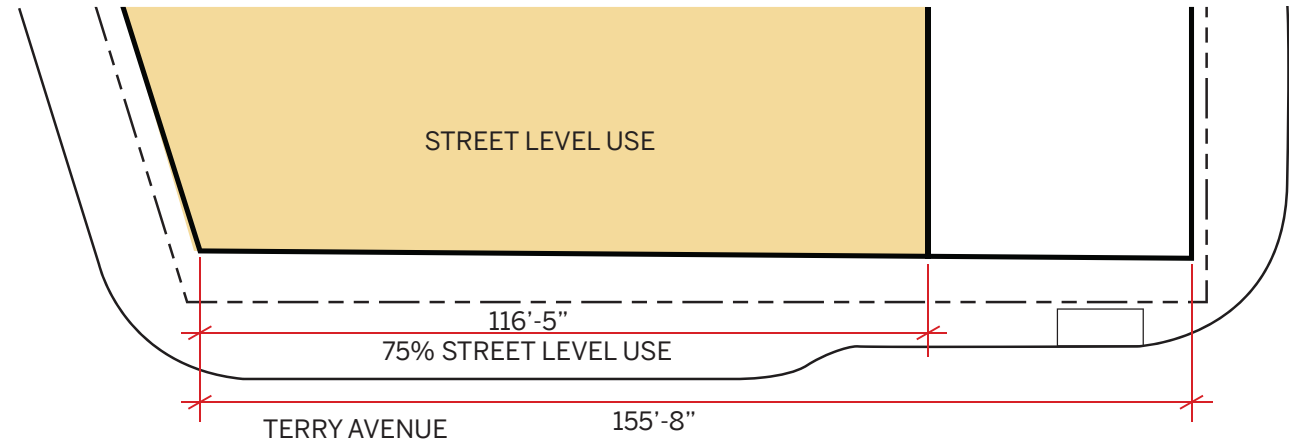
Concentrating the street level uses along Terry at the corner of Howell, allows a potential retail tenant to “spill-out” onto the sidewalk where there is greater depth and more visibility.

C1.3 street level articulation for pedestrian activity

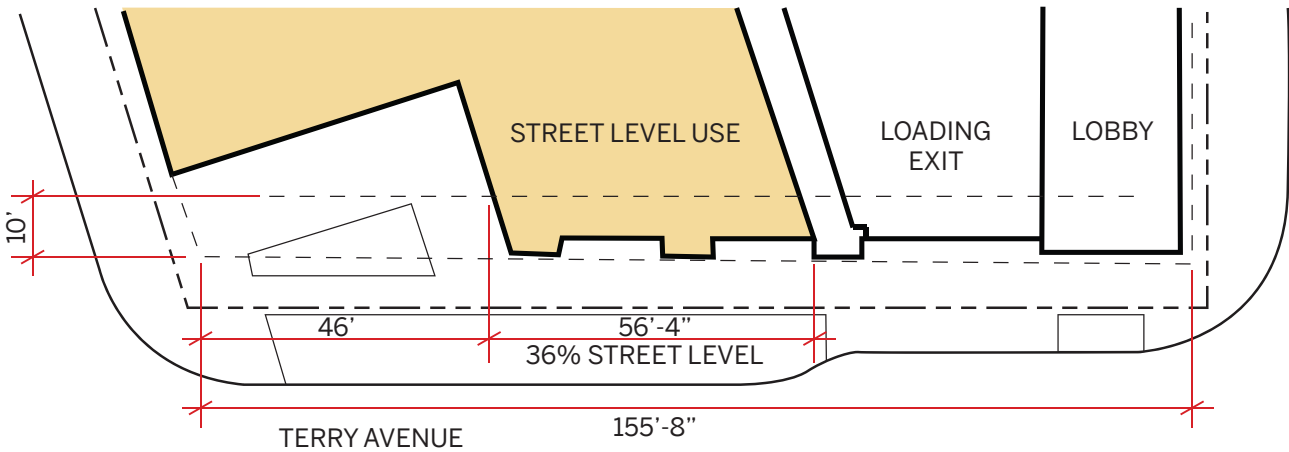
- setting the building back to create space adjacent to the sidewalk conducive to pedestrian-oriented activities

G. Windows that encourage pedestrians to look into building interior

I. Street front open space



CODE BASELINE



PROPOSED



6 PROPOSED DEPARTURES

DEPARTURES

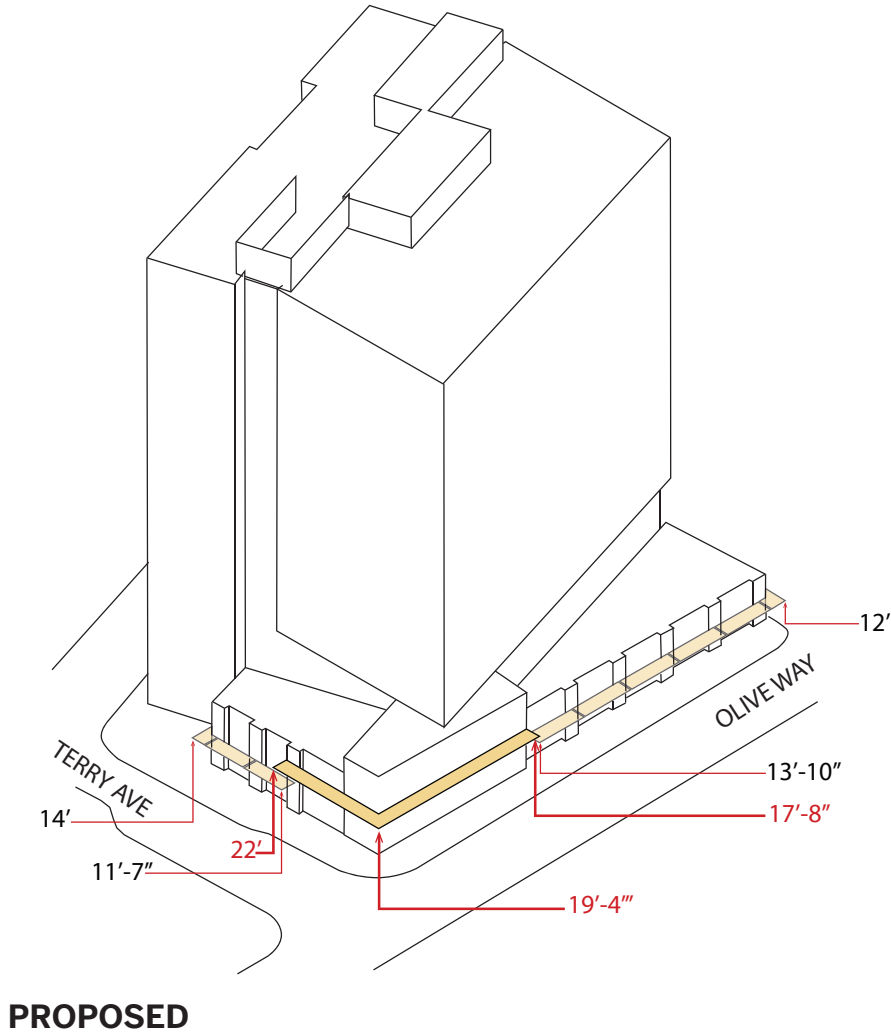
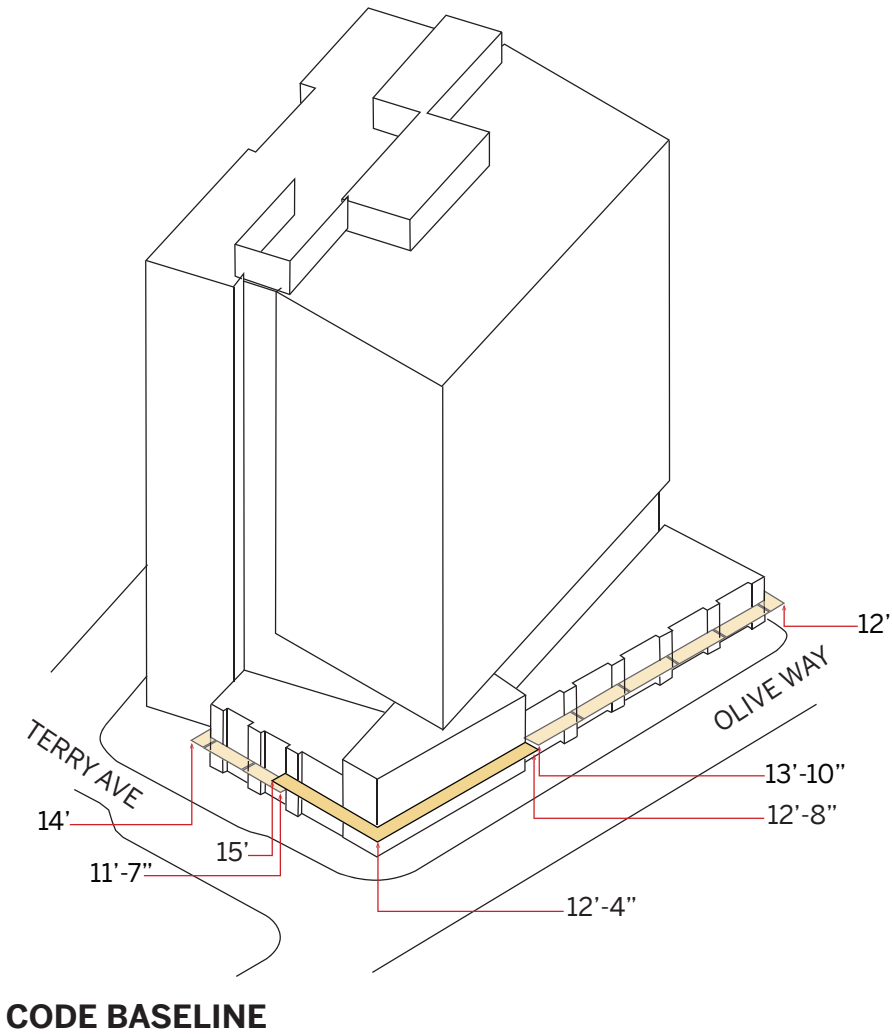
SITE C - OFFICE

| Item # | Development Standard                     | Requirement   | Modification Requested   | Rationale  |
|--------|--|---|--|--|
| 4      | 23.49.018<br>Overhead Weather Protection | The lower edge of the overhead weather protection must be a minimum of 10 feet and a maximum of 15 feet above the sidewalk. | Relief from the 15 feet maximum height limit above the sidewalk. | A taller canopy has been provided to signal the pedestrian entrance to the building and over the loading door for truck exiting. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guidelines section. |

Relevant Design Guidelines - Departure #4

A1.1 response to context  
B. A site having a dramatic topography or contrasting edge conditions

The canopies are sited to reinforce the massing and modulation of the building facade, stepping along the grade in larger increments





6 PROPOSED DEPARTURES

DEPARTURES

SITE C - OFFICE

| Item # | Development Standard | Requirement   | Modification Requested  | Rationale  |
|--------|----------------------|---|---|--|
| 5      | SMC.23.49.058 F4A    | A 2 foot wide setback from the street lot line is required along the Terry and 9th Ave. green streets. Total 2 foot wide setback area required along Terry Ave. green street. Lot line x 2 feet: 159.75 LF X 2= 319.50 SF | No landscape area is provided adjoining the building but additional landscape area of 159.75 SF is provided at the right-of-way edge. Total landscape provided at R.O.W. edge= 947 SF | Continuous overhead protection is provided along the Terry Ave. building facade providing weather protection for pedestrians. The required 159.75 square feet of landscaped area is provided within the Terry Ave. right of way. |
|        | SMC.23.49.058.F4B    | 50% of setback area shall be landscaped. Total landscape required: 50% of 319.50= 159.75 SF   | Green street setback requirement= 159.75 SF<br><br>Total landscape required in street R.O.W. = 239.63 SF<br><br>Additional landscape area= 947SF - 159.75SF - 239.63SF= 547.62SF      |  |

Relevant Design Guidelines - Departure #5

Per design review guidelines for downtown development:

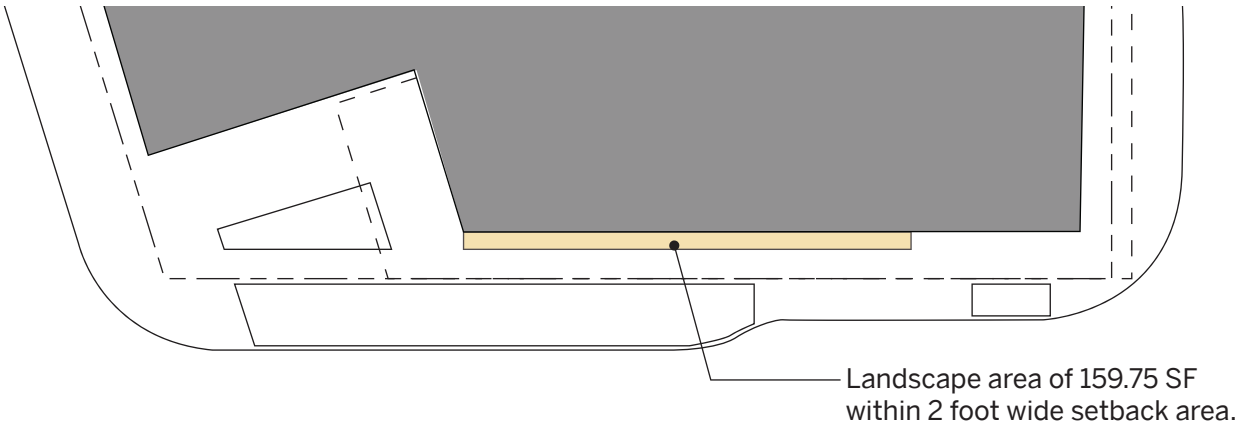
A.2: Response to planning efforts

C5: Overhead weather protection

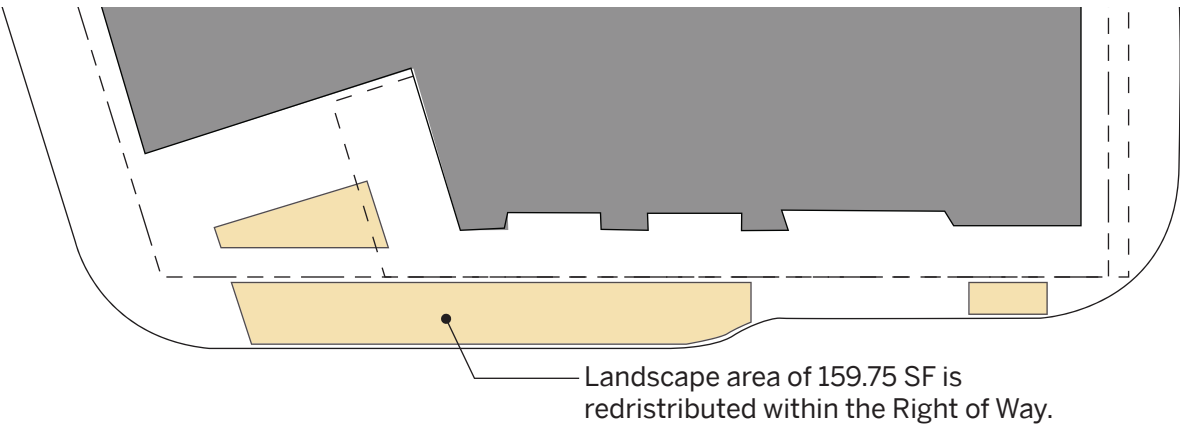
D1.1: Pedestrian enhancements

A. Open space elements should enhance a pedestrian oriented, urban environment that has the appearance of stability, quality, and safety.

D. The design of landscaping should allow visibility into and out of the space



CODE BASELINE



PROPOSED



6 PROPOSED DEPARTURES

DEPARTURES

SITE C - OFFICE

| Item # | Development Standard                   | Requirement   | Modification Requested   | Rationale  |
|--------|--|---|--|--|
| 6      | 23.49.056.B.2D<br>Facade Setback Limit | The maximum setback of the facade at an intersection is 10 feet. The minimum distance the facade must conform is 20 feet from the corner along each street. | Relief from the minimum setback of the facade from the street lot line at intersections. The modification is requesting an additional 13' of setback from the code baseline. | The proposed design responds to the shift in the street grid at Howell Street, reflected in the massing of the tower, the form of the tower is carried through to the ground, adding clarity to the overall design and providing for a more generous sidewalk at an otherwise sharply angled intersection. The additional pedestrian space at grade eases the transition through the intersection and allows for better visibility across the changing street grid. This departure allows the project to better meet the intent of the design guidelines described in the relevant design guideline section. |

Relevant Design Guidelines - Departure #6

A1.1 response to context

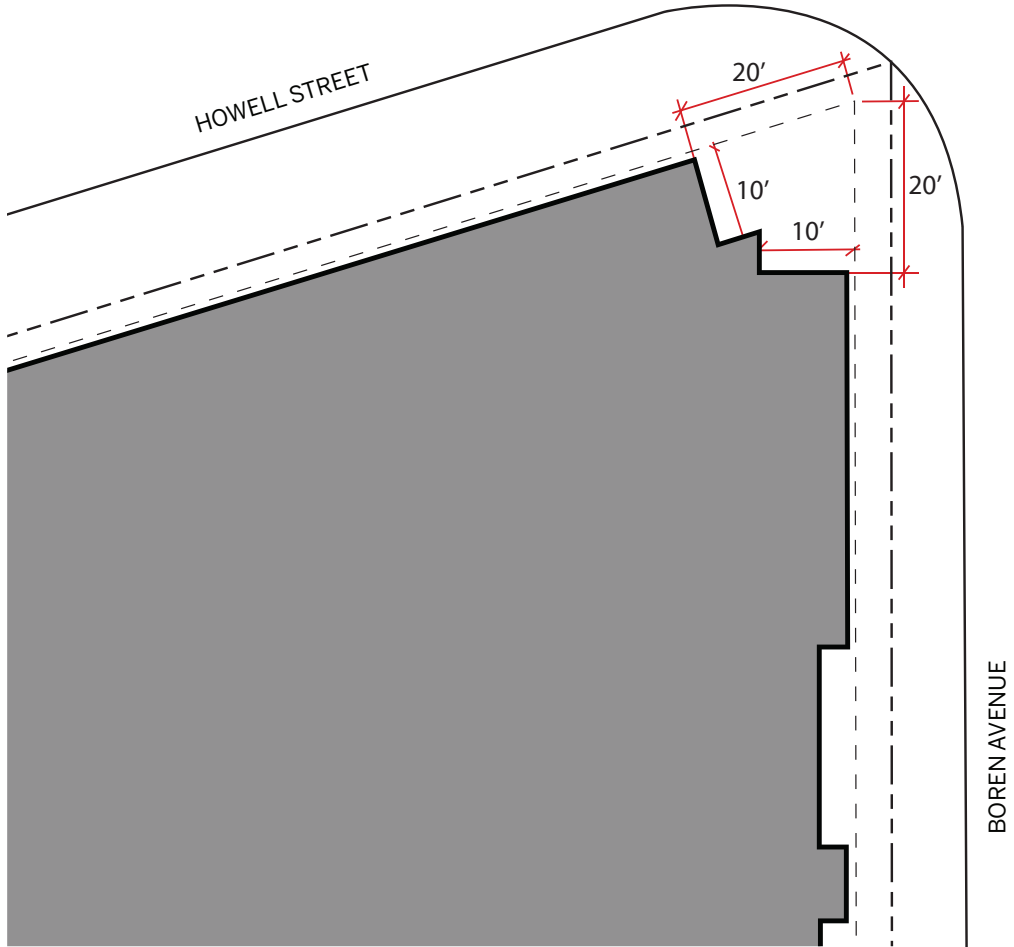
A. A change in street grid alignment that yields a site having a nonstandard shape

D. Access to direct sunlight-seasonally or at particular times of day

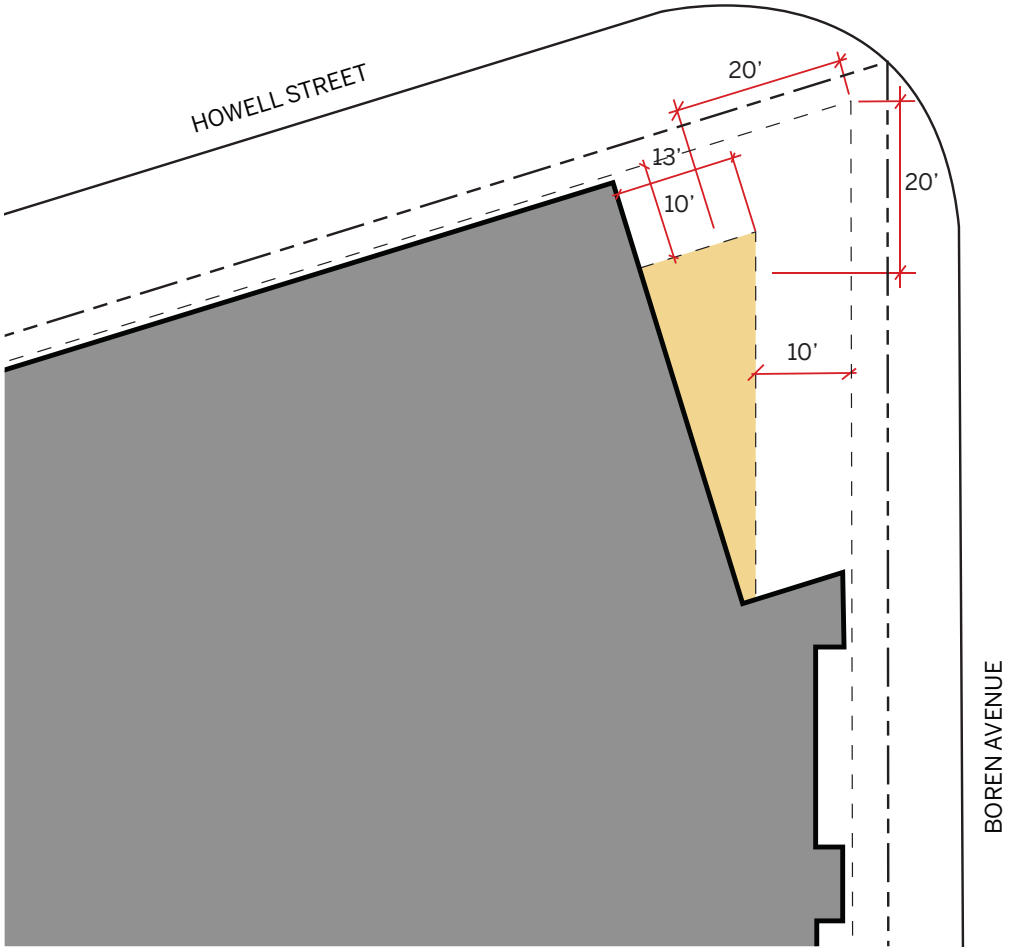
B4.1 massing

A. Setbacks, projections, and open space

B. Relative sizes and shapes of distinct building volumes



CODE BASELINE



PROPOSED







# Site-B Signage





# Site-B Signage





# Site-C Signage

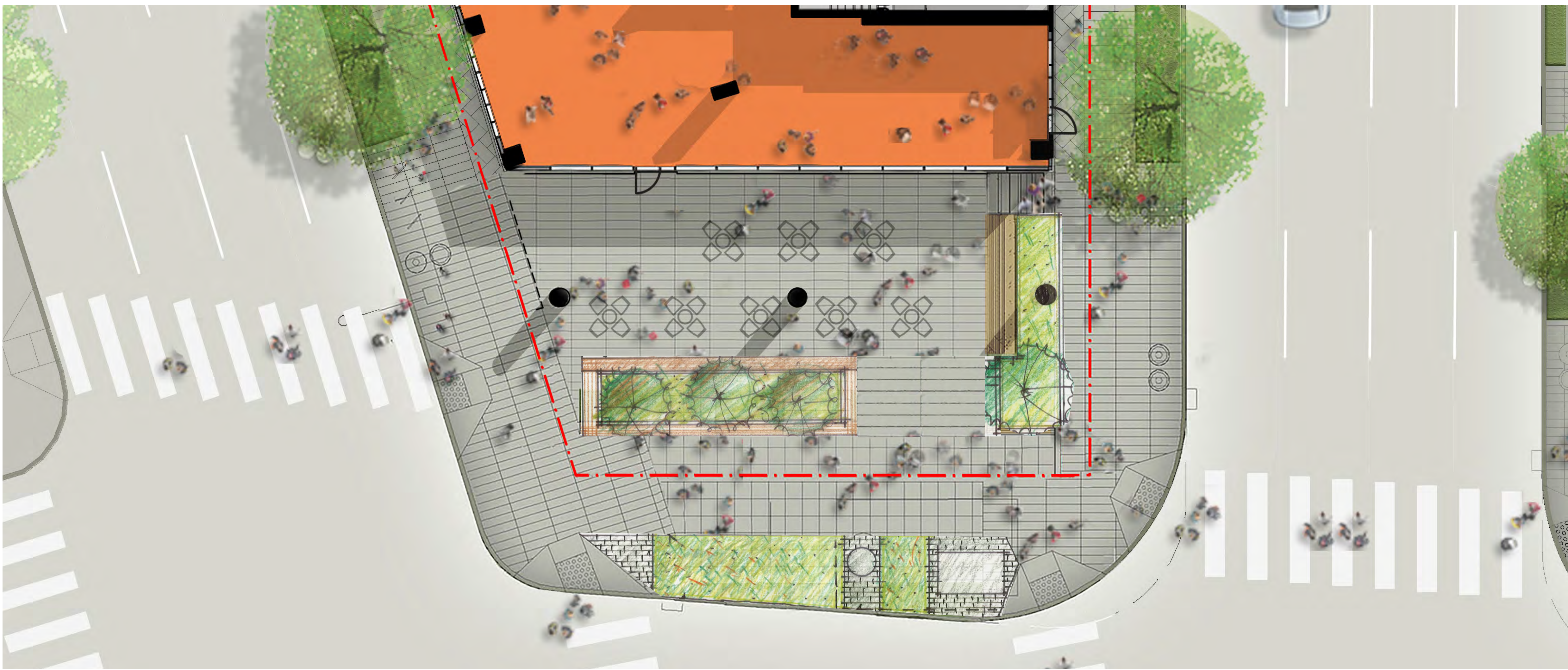




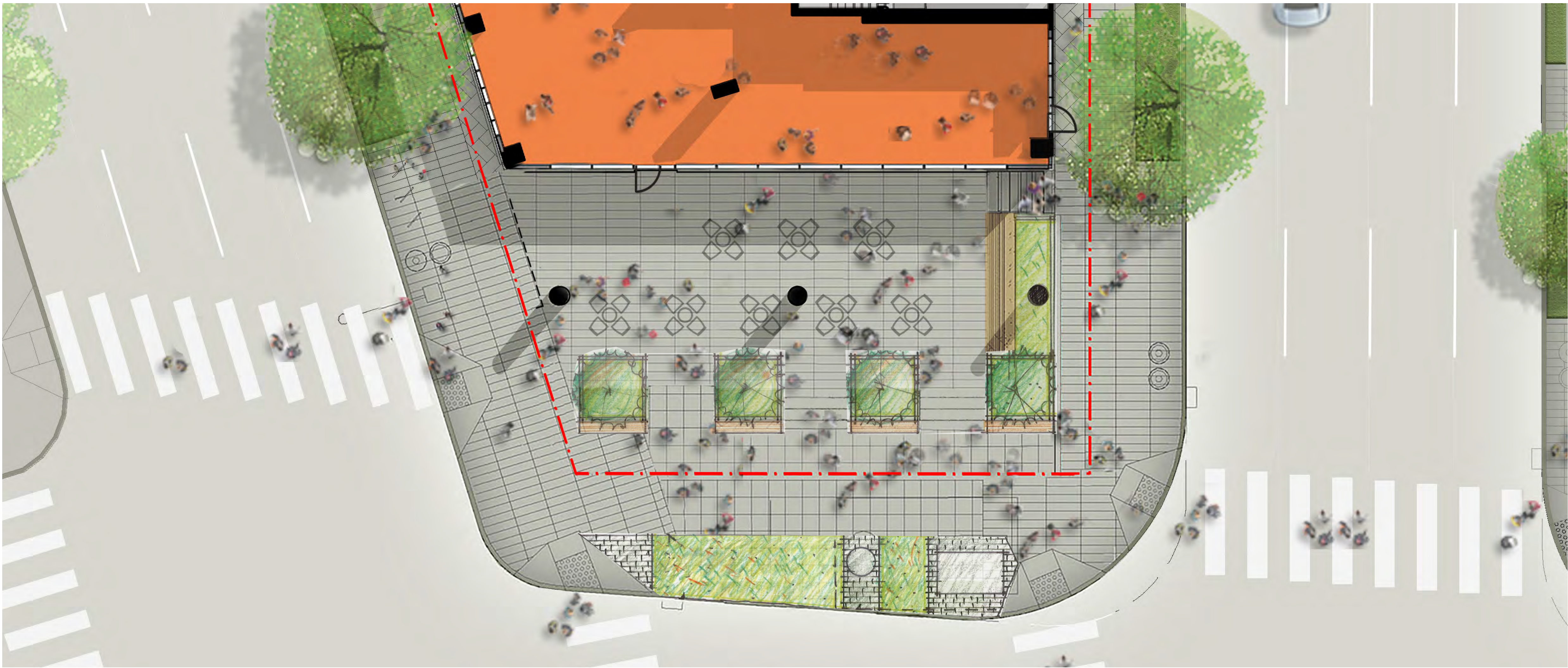
# Site-C Signage



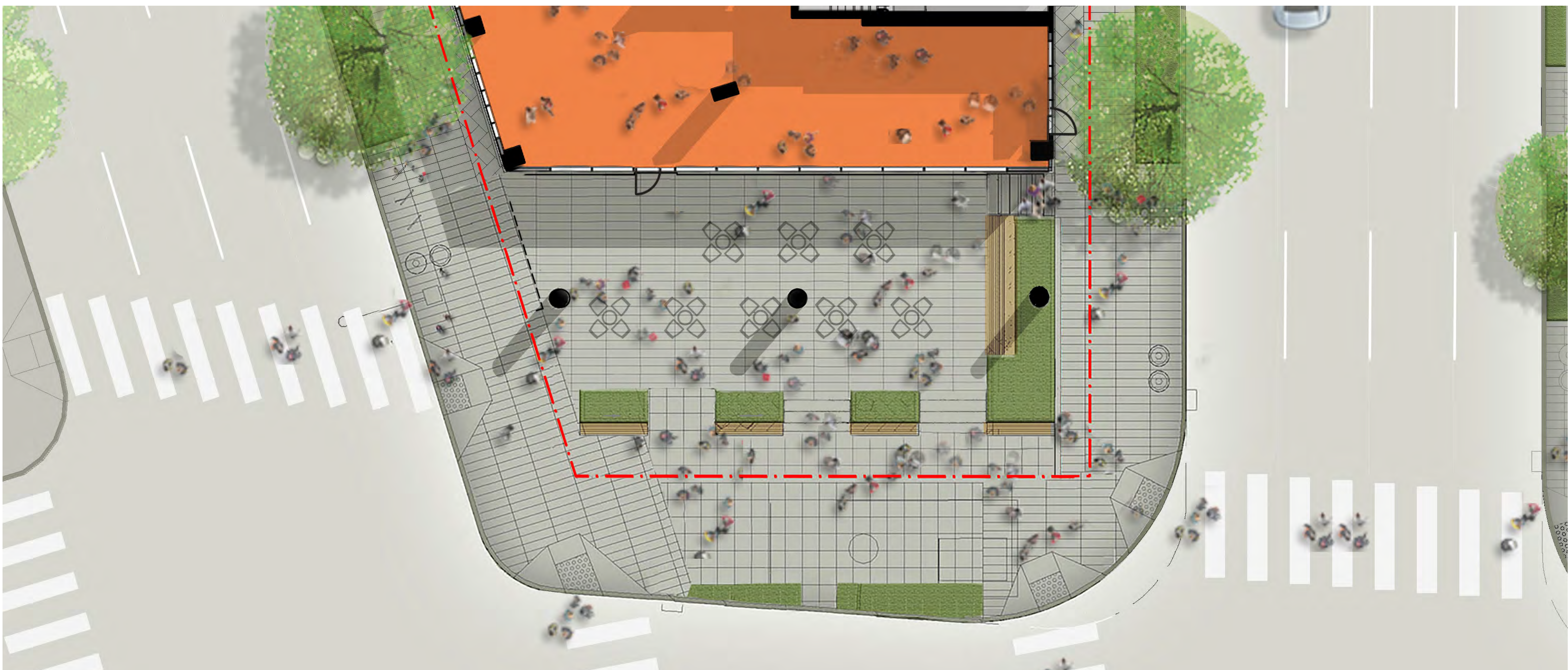




9th Avenue Plaza  
- Alternate 2



9th Avenue Plaza  
- Alternate 1



9th Avenue Plaza  
- Current Design