

Washington State Convention Center Addition Project

Design Review Board Design Recommendation Meeting 03
01-16-2018

PROPERTY ADDRESS	SITE A 1600 9th Avenue
SDCI PROJECT #	3020176

OWNER Washington State Convention Center 800 Convention Place Seattle, WA 98101	ARCHITECT LMN Architects 801 Second Ave. Suite 501 Seattle, WA 98104	SDCI CONTACT Lisa Rutzick 206-386-9049 lisa.rutzick@seattle.gov
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WSCC EXISTING SITE

WSCC ADDITION PROJECT

The **Washington State Convention Center** Addition offers a transformative opportunity to create an urban convention venue at the forefront of the convention industry, to offer a rich delegate experience which capitalizes on Seattle's unique qualities, and to enrich Seattle's rapidly evolving urban core.

TABLE OF CONTENTS

04
1 DEVELOPMENT
OBJECTIVES &
PROJECT BACKGROUND

06
2 DR 02 RECOMMENDED
CONDITIONS OF
APPROVAL

20
3 DESIGN REVIEW &
UPDATES

55
4 PROPOSED
DEPARTURES

105
5 APPENDIX
A. PLANS & SECTIONS UPDATE
B. MATERIALS UPDATE
C. LANDSCAPE UPDATE
D. GRAPHICS & SIGNAGE UPDATE
E. LIGHTING CONCEPT UPDATE

PROJECT STATUS:

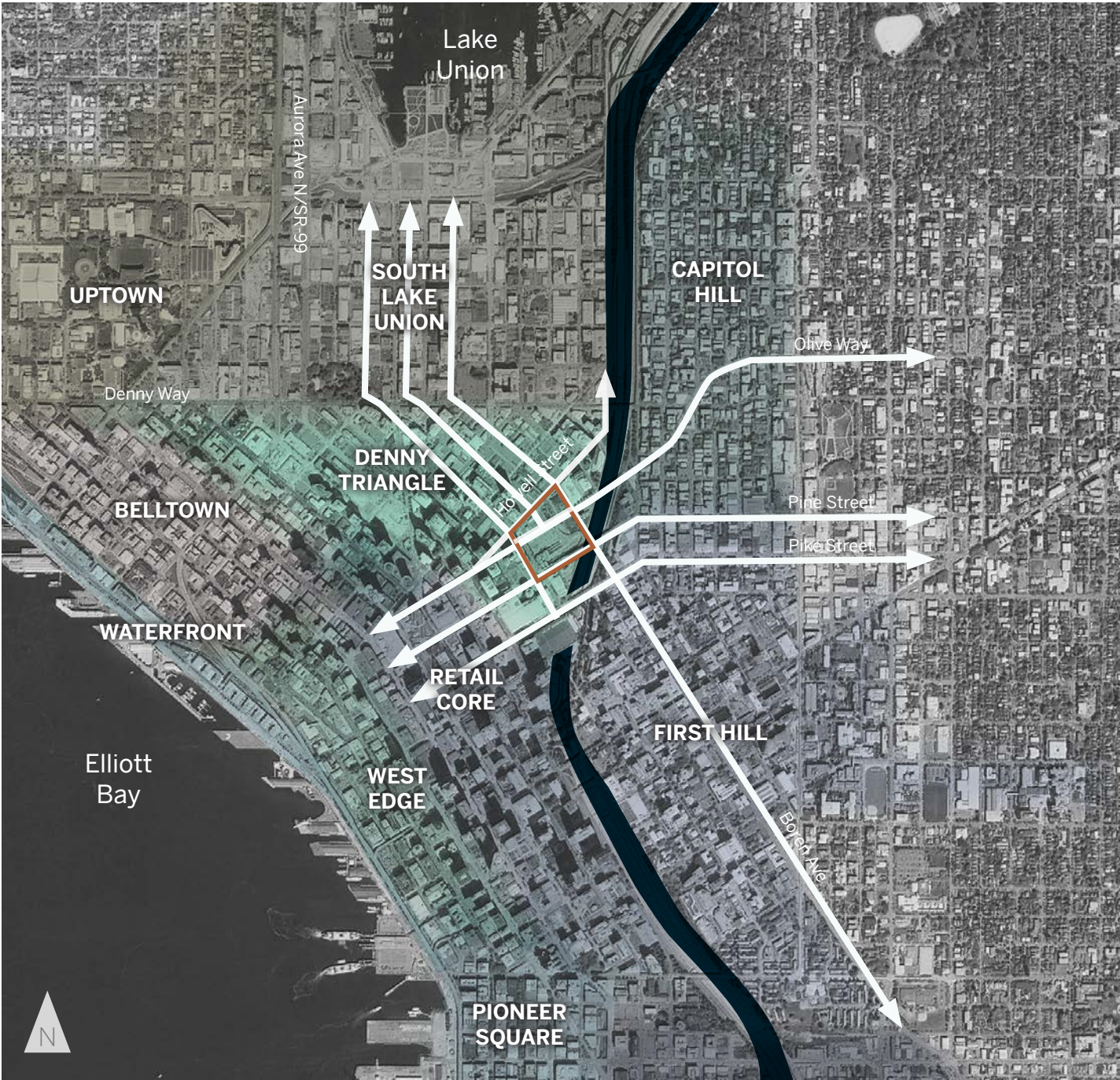
The Design Review Board granted approval to Site A at the Design Recommendation Meeting (DR) #2 on 7/19/16. The approval included comments and recommended conditions from the Board, including both elements of the proposal that were supported and encouraged to not dramatically modify with recommendations to be further studied for review for the Master Use Permit Submittal.

This Design Recommendation Meeting #3 is to cover response to the recommended conditions and updates to departures, including 3 new departures based on further MUP review. Additional information has been provided for reference.

The project has also completed review with the Seattle Design Commission (SDC) for street and alley vacations including public benefits. This proposal includes further design evolution based on their feedback along with general design refinement overall.

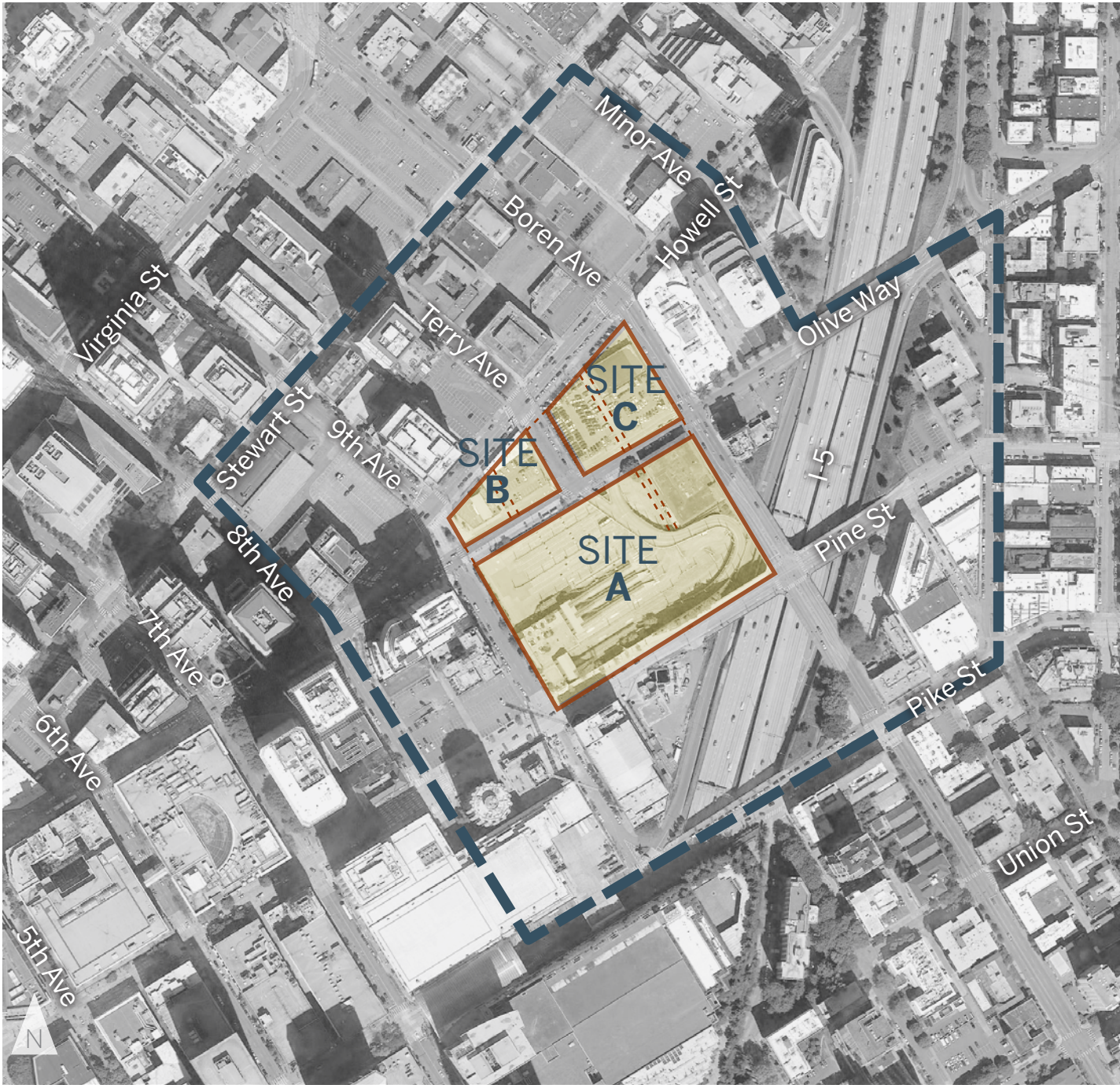
1 DEVELOPMENT OBJECTIVES PROJECT BACKGROUND

VICINITY



STREETS BORDERING PROJECT SITE

9-BLOCK STUDY AREA



- 9-BLOCK STUDY AREA
- WSCC ADDITION PROPERTIES
- STREET/ALLEY VACATION
- AT GRADE SITE AREA INCLUDING FULL VACATIONS

1 DEVELOPMENT OBJECTIVES & PROJECT BACKGROUND

DEVELOPMENT SUMMARY

The proposal is to apply for Master Use Permits for development of a convention center addition on a site consisting of 3 lots: Site A: 1600 9th Avenue, Site B: 920 Olive Way, and Site C 1711 Boren Avenue, that will collectively form the proposed Washington State Convention Center (WSCC) Addition Project. The 3 block site is bounded by Howell Street to the north, Pine Street to the south, 9th Avenue to the west, and Boren Avenue and I-5 to the east. Terry Avenue and Olive Way divide the site on the interior. Below grade street and full alley vacations will be required for this project.

The project will add approximately 1,230,000 SF of gross floor area to the existing Washington State Convention Center. This will include approximately 250,000 SF of new exhibition space, 120,000 SF of meeting rooms, a 60,000 SF Ballroom space, approximately 20 convention center loading bays, and structured parking for up to 717 additional vehicles. The structure height is approximately 150-200ft over 11 stories with 2 additional stories below grade.

Co-development (**DRB approval on 05/17/2016**) on two blocks north of Olive Way (Sites B & C) is proposed as a part of the WSCC Addition project. The co-development sites are planned for a 289ft, 416 unit residential and a 240ft commercial office building.

SITE CONTEXT

The project site is located within the DMC 340/290-400 Downtown Mixed Commercial zone, within the Denny Triangle Urban Center Village. The Downtown Neighborhood Guidelines will apply to this project.

The project site is bordered by the DMC 240/290-400 zone (Denny Triangle Urban Center Village Overlay) to the north, east, and south, and the DOC 2 500/300-500 zone (Commercial Core Urban Center Village Overlay) to the west.

The project occupies the intersection between several distinct and rapidly evolving neighborhoods, including Capitol Hill's Pike/Pine corridor, the Denny Triangle, South Lake Union, First Hill, and the Downtown commercial core.

Capitol Hill's traditional low-rise commercial development is being supplemented with new mid-rise mixed-use buildings. The neighborhood continues to promote a strong pedestrian community, interrupted only by the presence of I-5. The Denny Triangle and First Hill, connected via Boren Avenue, bookend the site to the north and south. Though a product of different eras, both neighborhoods contain higher density, taller residential and commercial development, along with notable institutional buildings. The Downtown neighborhood is the densest and tallest adjacent neighborhood, containing both high-rise commercial and residential development, but also a retail and cultural center for the city.

The site's proximity to Pike and Pine links itself to the waterfront via Pike Place Market and Westlake Center, and to the existing Washington State Convention Center along Ninth Avenue. Other notable landmarks include the historic Paramount Theatre and Worldmark- Camlin Seattle, adjacent to the site across Pine Street and Ninth Avenue. Due to the open space established by the presence of I-5, views to and from the project site to the south and east are both substantial and long-term. Views to the west, particularly from the higher elevations along Pine Street, provide a meaningful glimpse into the heart of the city.

PROCESS OVERVIEW

The Design Recommendation (DR) #3 meeting at the Downtown Design Review Board builds on: EDG #1 (05/19/2015), which focused on the site context and urban design; EDG #2 (07/21/2015), which focused on building massing and street-level designs; and EDG #3 (10/06/2015), during which the project received Early Design Guidance approval. DR #1 (07/19/2016) and DR #2 (07/19/2016) concluded with Design Review Board approval in DR #2. The Design Proposal booklets and SDCI reports from those meetings are available to view at the following link:

<http://www.web6.Seattle.gov/dpd/edms/>

Search Past Reviews by typing in the SDCI project number: 3020176.

NOTE: ALL DESIGNS SHOWN FOR STREET ROW OUTSIDE OF PROPERTY LINES WERE REVIEWED AND APPROVED BY SEATTLE DESIGN COMMISSION AND ARE UNDERGOING CONTINUED REVIEW BY SDOT.

This booklet is for Site A only. Sites B&C were granted DRB Recommendation approval with conditions on 5/17/16.

PROJECT GOALS

- Create a highly efficient design which effectively supports the functional needs of the convention center clients and is competitive in the marketplace.
- Create a unique experience that embodies the special qualities of Seattle, Washington, and the Pacific Northwest.
- Engage the urban framework of downtown Seattle to capitalize on the location at the intersection of major neighborhoods and corridors of the city.
- Create a welcoming street presence that connects the activities of the Convention Center with the pedestrian experience of the adjacent streets.
- Integrate mixed uses such as retail and other possible co-developments, where appropriate, to enrich the urban diversity of the site.
- Create a sustainable design that embraces Seattle's commitment to environmental stewardship.

2 DR 02 RECOMMENDED CONDITIONS OF APPROVAL

DRB CONDITION OF APPROVAL #1 Pine Street Stair & Materials

The soffit under the stair is highly visible and requires quality materials, tight joints and excellent detailing, to ensure metal flatness and a slender stair riser profile. If the soffit is reflective [20], provide a thorough exploration of matte and satin finishes that are less than mirrored, and may perform/maintain better in the northwest winter climate.

RESPONSE - The soffit will use an aluminum composite metal panel system detailed to ensure panel flatness and rigidity. Mock-ups will be done to test materials and finishes for final selection that are appropriate for climate and performance desired. Reflectivity is an important design element to increase interaction with artistic roof graphics and to highlight the landscape and activity of pedestrians below.

DRB CONDITION OF APPROVAL #2 Pine Street Retail Roofs

The three rooftops and associated sidewalls [86] will be visible from the adjacent lobbies and buildings, and possibly be reflected by the soffit above. The roofs should be a durable and sophisticated graphic design, and definitely not a sign or other branded display.

RESPONSE - Intent is for graphics to be designed by a local illustrator and be artful and thoughtful. They will not be branding or advertising. Illustration graphics will be integrated with a durable aluminum metal panel system.

DRB CONDITION OF APPROVAL #3 South Stair and East Ballroom Caps

The translucent top bands shown on pg 20, 39 and 87, should not be a uniform opaque appearance as shown, but rather be translucent and show the structural shadows and light-play within, as stated at the meeting. Exploration of channel glass and other quality translucent materials for these two critical skyline defining elements is encouraged.

RESPONSE - The design was revised to reduce height of opaque materials at the caps of the Hillclimb and East Ballroom. Glazing height has been increased, allowing more visibility to specialty ceiling and structure within.

DRB CONDITION OF APPROVAL #4 East Ballroom 'Landmark Building Graphic'

The signature lighting should be limited to the two facades where the mixing zone reaches the north and south streets, in concert with the light totems and other graphic elements the Board heartily endorsed [65/left]. Any lighting at the east ballroom should be fully internalized, coordinated with the more important ceiling design, and be fully dimable.

RESPONSE - The design was revised to remove the landmark lighting and fully internalize lighting at the east ballroom. This lighting will be fully controllable and dimmable. The window proportion was also revised to reduce the visual impact to the building across the street.

DRB CONDITION OF APPROVAL #5 Boren Avenue Blank Walls

All the ground level concrete along Boren should have a finer grain texture than that shown on pg 32/F; it should be deeply scored more frequently than the 8ft shown on pg 39, and/OR the entire strip of plantings hugging that concrete wall should be 3-5 ft tall (yet not create CPTED issues) (see departure 4b).

RESPONSE - The design was revised to slope the landscape at the Boren ground level up towards the concrete wall to create a larger volume of planting for a greater visual impact. Additional trees and Public Benefit artworks have been added to provide texture and variety. Seating design and lighting elements to be incorporated into the garden in coordination with the selected artist, per SDC guidance. Urban Design Merit artwork to be included at vehicle entry, as developed by selected artist.

DRB CONDITION OF APPROVAL #6 Olive Way Blank Walls / Exit Doors

All the Olive exit doors should ideally be transparent glass, and at minimum the vertical strips of translucent windows above [see pg 37] should be carried down to grade, even if those corresponding aligned doors are translucent glass (see departure 4a).

RESPONSE - The design was revised to bring vertical glazing down to grade in multiple locations at exits. Additional Public Benefits artworks to be developed by artist at (5) glass vitrines, per SDC guidance.

DRB CONDITION OF APPROVAL #7 9th Avenue Blank Walls/Exit Doors

All the 9th Avenue exit doors should ideally be transparent or translucent glass, and at minimum a vertical planting, screen or visually interesting artwork should be installed in the middle of that 42 ft length [81, gray portion], in a door gap that is as wide as possible (see departure 4c).

RESPONSE - The design was revised to include more planting / additional tree at the 9th Avenue exit doors to add visual interest and texture. Further development of the facade materials and metal panels add interest to the facade, and relate to the warm wood of the adjacent market.

SUPPORTED AS PRESENTED:

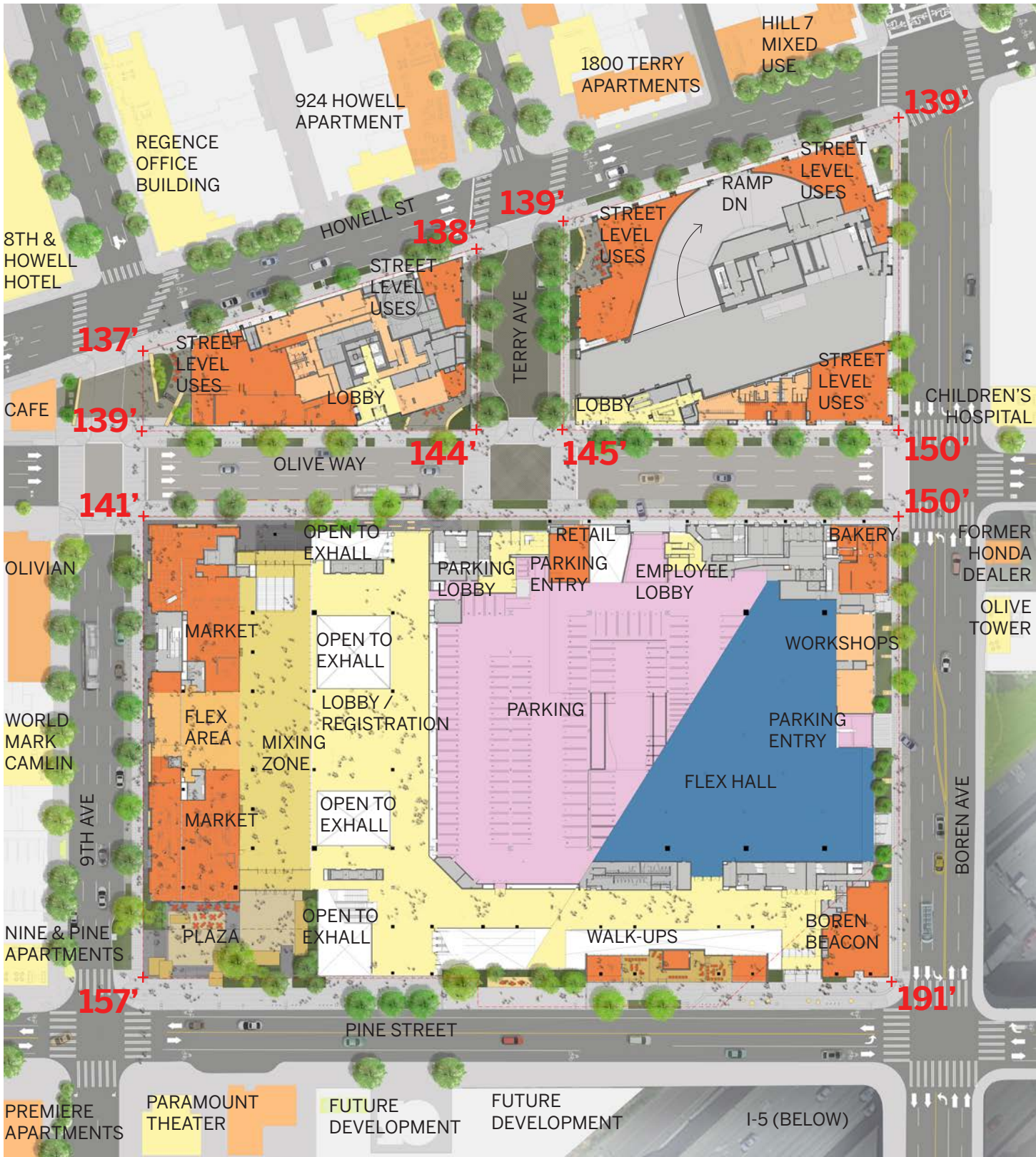
- Retail pavilions, shared deck on Pine Street
- 9th Avenue ground level facade and expression as a distinct pavilion, modulation, seating
- Southwest Plaza materials, benches, lighting
- Olive Way projecting frame, landscape trays
- Overall building modulation and materiality
- Boren Avenue landscape design, seating
- Lighting concept, distribution and fixtures
- Signage and graphics concept
- Roof PV array and color

2 DR 02 RECOMMENDED CONDITIONS OF APPROVAL

DRB #2 SITE PLAN



DRB #3 SITE PLAN



2 DR 02 RECOMMENDED CONDITIONS OF APPROVAL

DRB CONDITION OF APPROVAL #1 Pine Street Stair & Materials

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RESPONSE - The soffit will use an aluminum composite metal panel system detailed to ensure panel flatness and rigidity. Mock-ups will be done to test materials and finishes for final selection that are appropriate for climate and performance desired. Reflectivity is an important design element to increase interaction with artistic roof graphics and to highlight the landscape and activity of pedestrians below.

DRB #3



PERSPECTIVE LOOKING WEST ALONG PINE STREET

2 DR 02 RECOMMENDED CONDITIONS OF APPROVAL

DRB #3



SECTIONAL PERSPECTIVE - LOOKING EAST



SECTIONAL PERSPECTIVE - LOOKING WEST

RESPONSE CONDITION #1: The soffit will use an aluminum composite metal panel system detailed to ensure panel flatness and rigidity.

2 DR 02 RECOMMENDED CONDITIONS OF APPROVAL

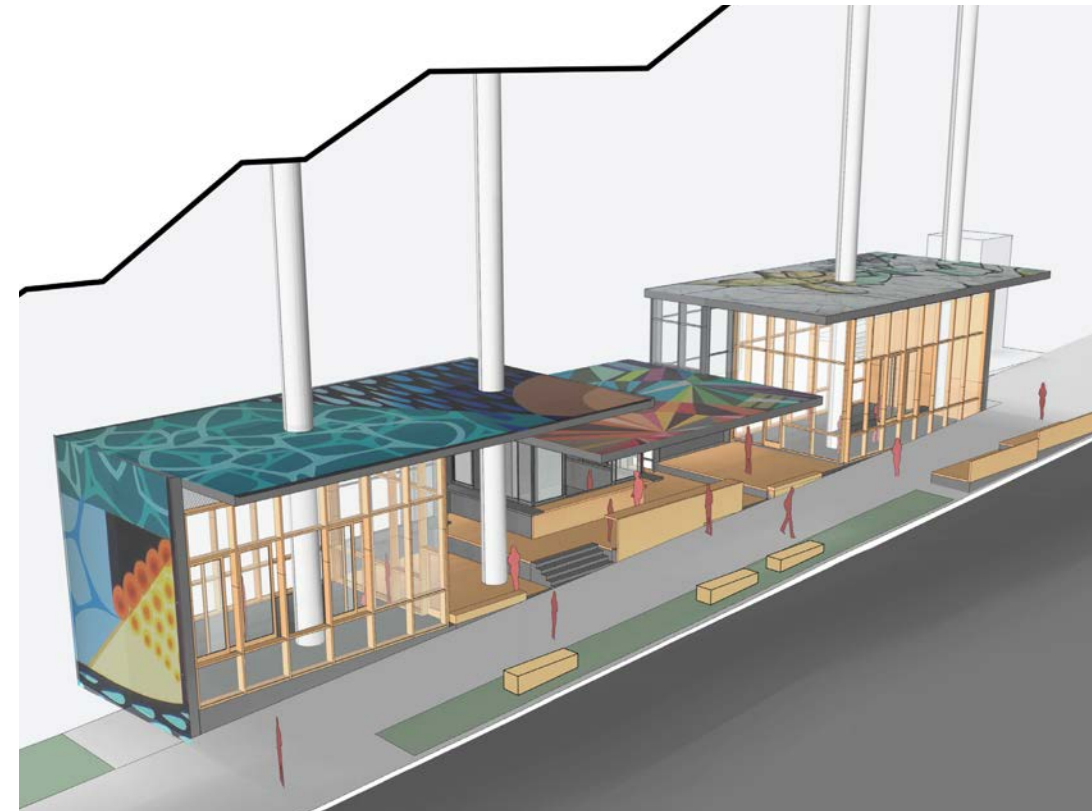
DRB CONDITION OF APPROVAL #2

Pine Street Retail Roofs

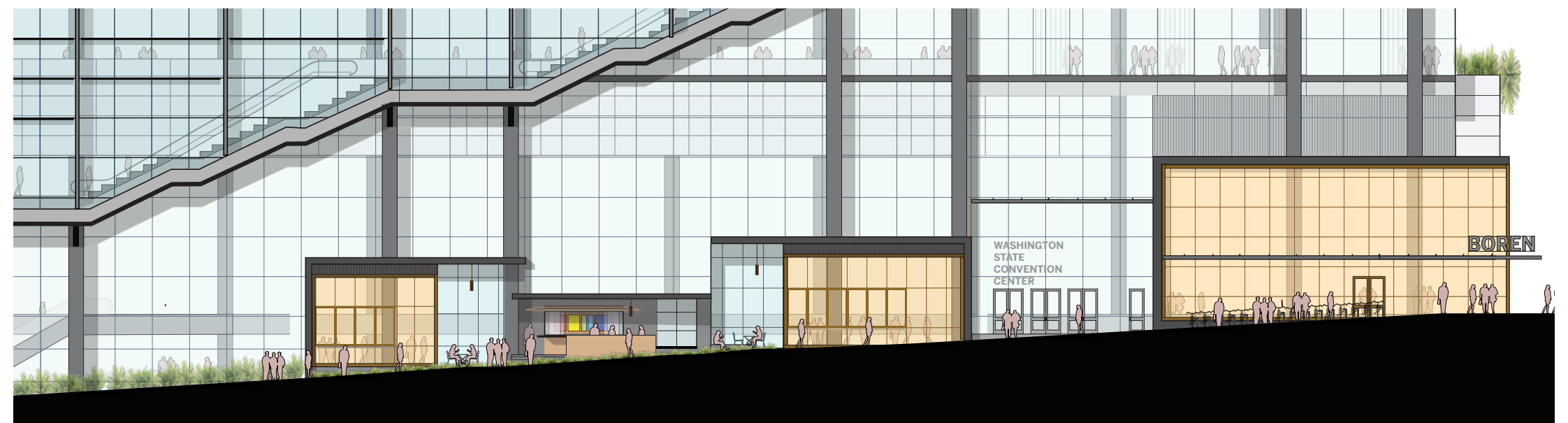
The three rooftops and associated sidewalls [86] will be visible from the adjacent lobbies and buildings, and possibly be reflected by the soffit above. The roofs should be a durable and sophisticated graphic design, and definitely not a sign or other branded display.

RESPONSE - Intent is for graphics to be designed by a local illustrator and be artful and thoughtful. They will not be branding or advertising. Illustration graphics will be integrated with a durable aluminum metal panel system.

DRB#2 - 07-19-2016 - PRESENTATION SLIDE 89



PINE STREET RETAIL AXONOMETRIC

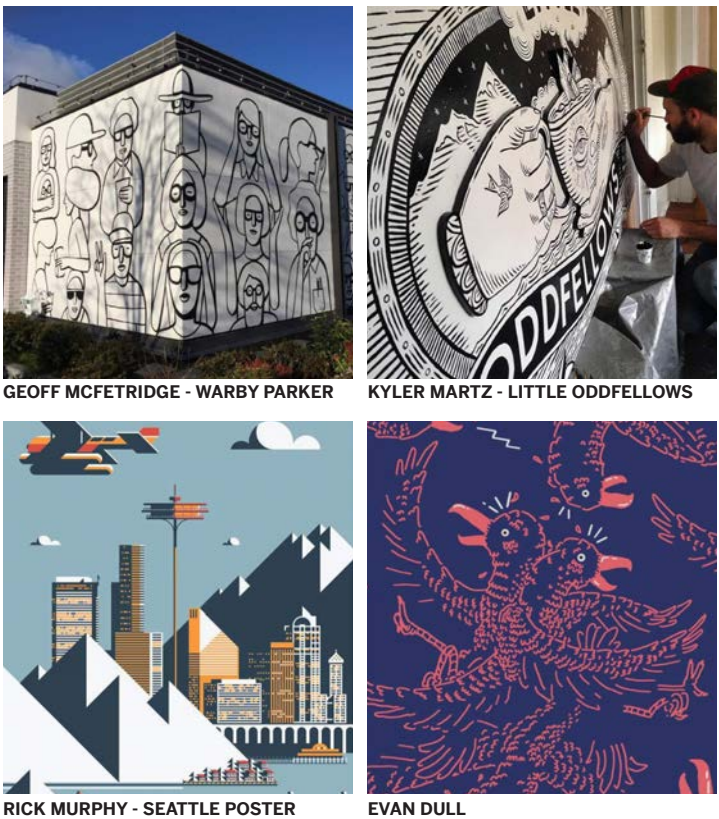


ENLARGED SOUTH ELEVATION - PINE STREET

Note: Landscape along street curb not shown in enlarged elevation for clarity.

2 DR 02 RECOMMENDED CONDITIONS OF APPROVAL

DRB #3 REFER TO RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - A321



LOCAL ILLUSTRATION PRECEDENTS



PINE STREET RETAIL AXONOMETRIC

UPDATED DESIGN: seating bench relocated to more evenly distribute seating along Pine Street and integrate with landscape per SDC guidance.

UPDATED DESIGN: retail massing refined and articulation of canopy and frame

UPDATED DESIGN: seating areas and lean rails refined to make more open to the sidewalk and public per SDC guidance

RESPONSE CONDITION #2: The graphic strategy for these retail panels is to bring interest to the pedestrian experience with the visual language of Capitol Hill.

UPDATED DESIGN: bikeshare station removed and additional benches added at sidewalk per SDC guidance

UPDATED DESIGN: curtainwall type refined



ENLARGED SOUTH ELEVATION - PINE STREET

Note: Landscape along street curb not shown in enlarged elevation for clarity.

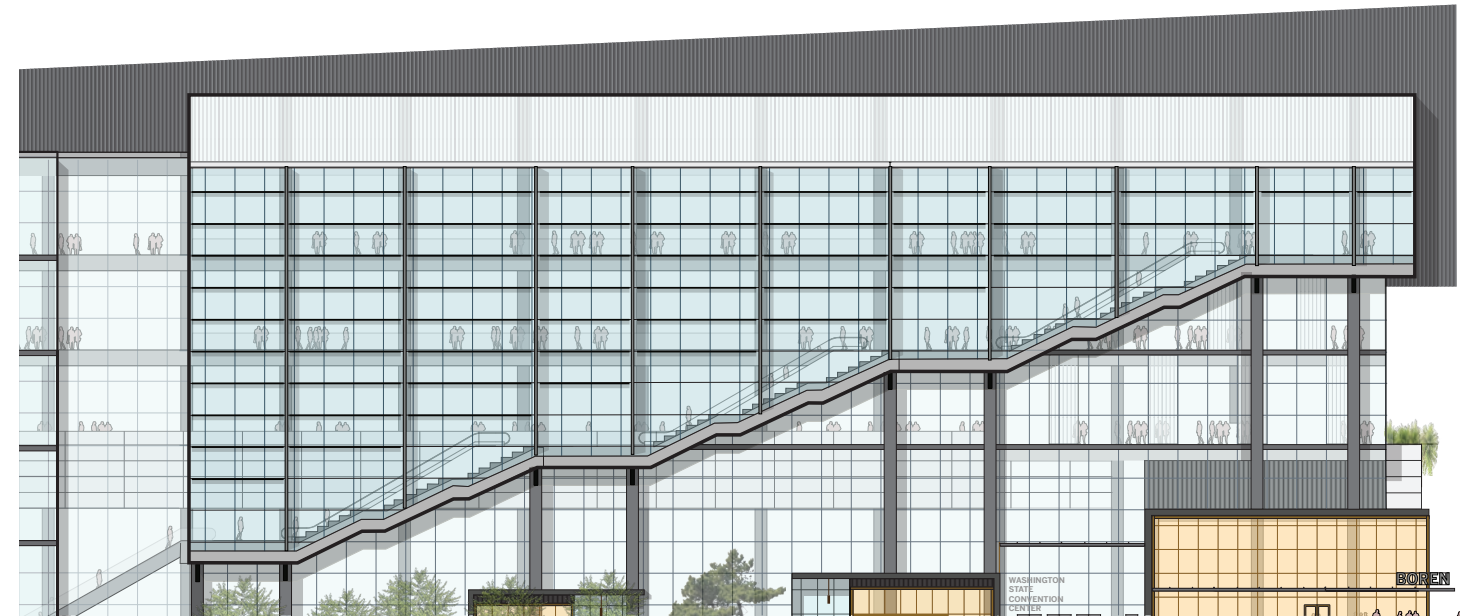
2 DR 02 RECOMMENDED CONDITIONS OF APPROVAL

DRB CONDITION OF APPROVAL #3 South Stair and East Ballroom Caps

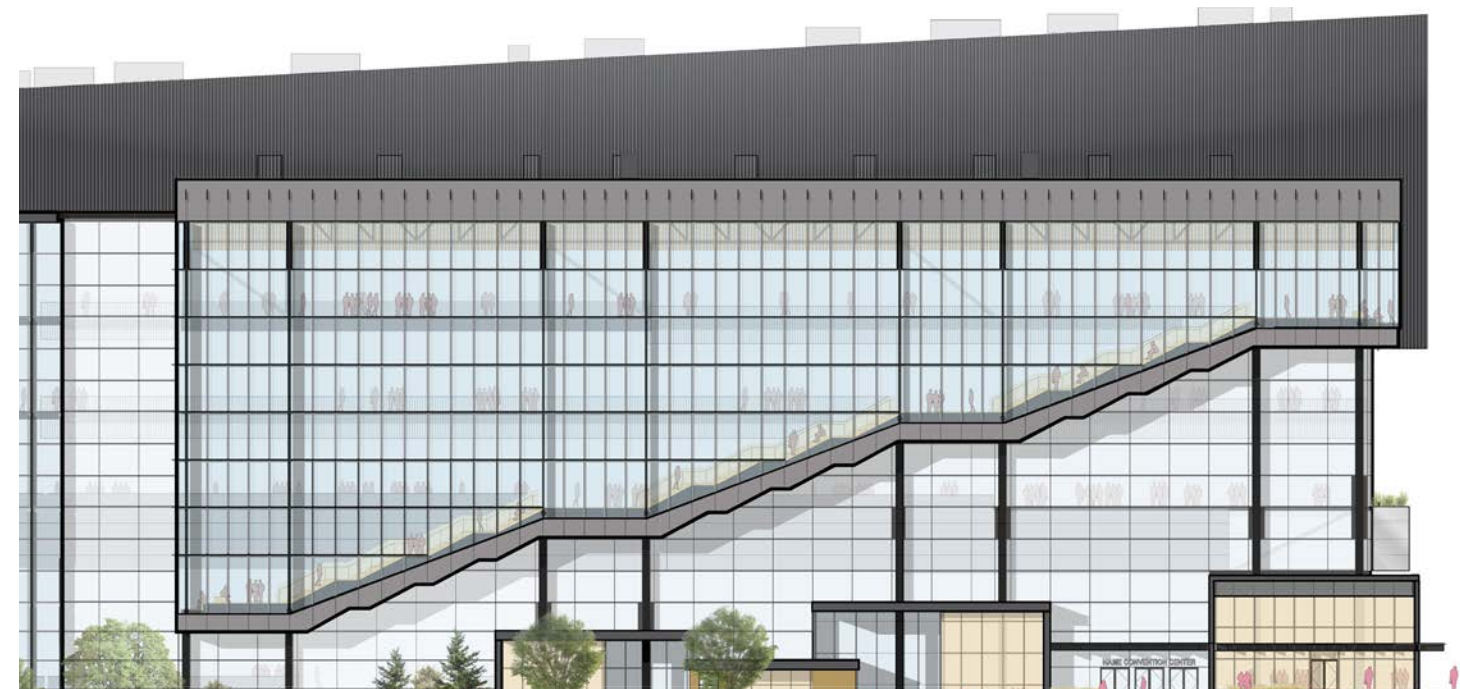
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RESPONSE - The design was revised to reduce height of opaque materials at the caps of the Hillclimb and East Ballroom. Glazing height has been increased, allowing more visibility to specialty ceiling and structure within.

DRB#2 - 07-19-2016 - PRESENTATION SLIDE 12



DRB #3 REFER TO RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - A321



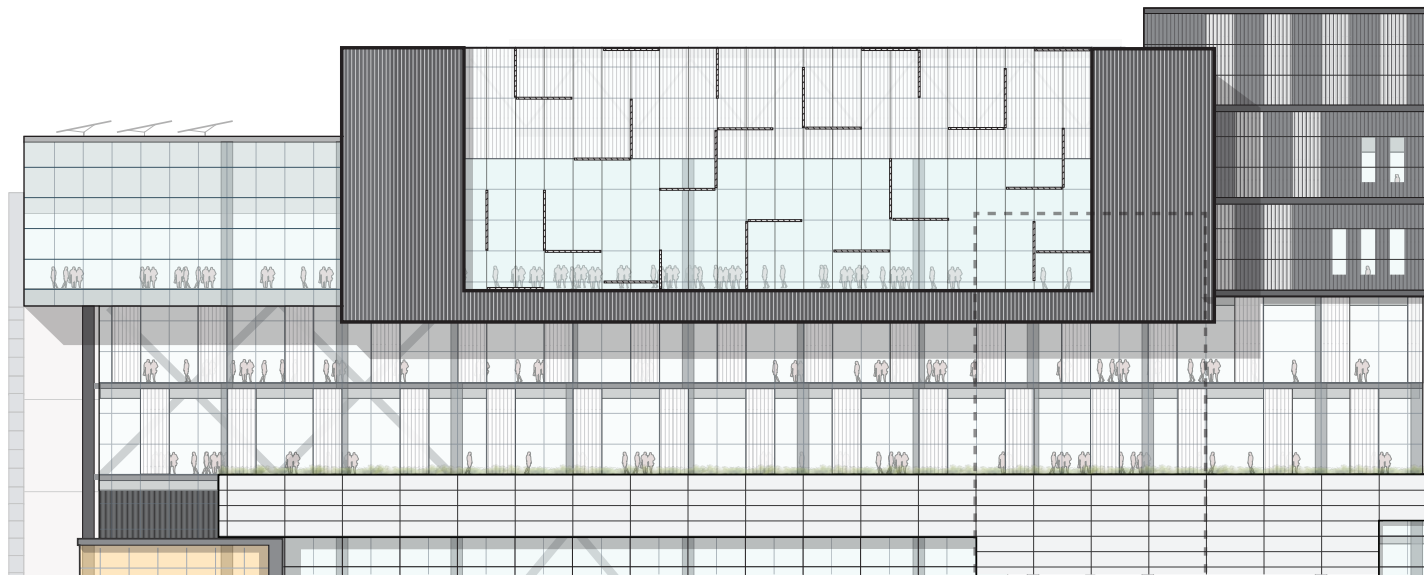
ENLARGED SOUTH ELEVATION - PINE STREET



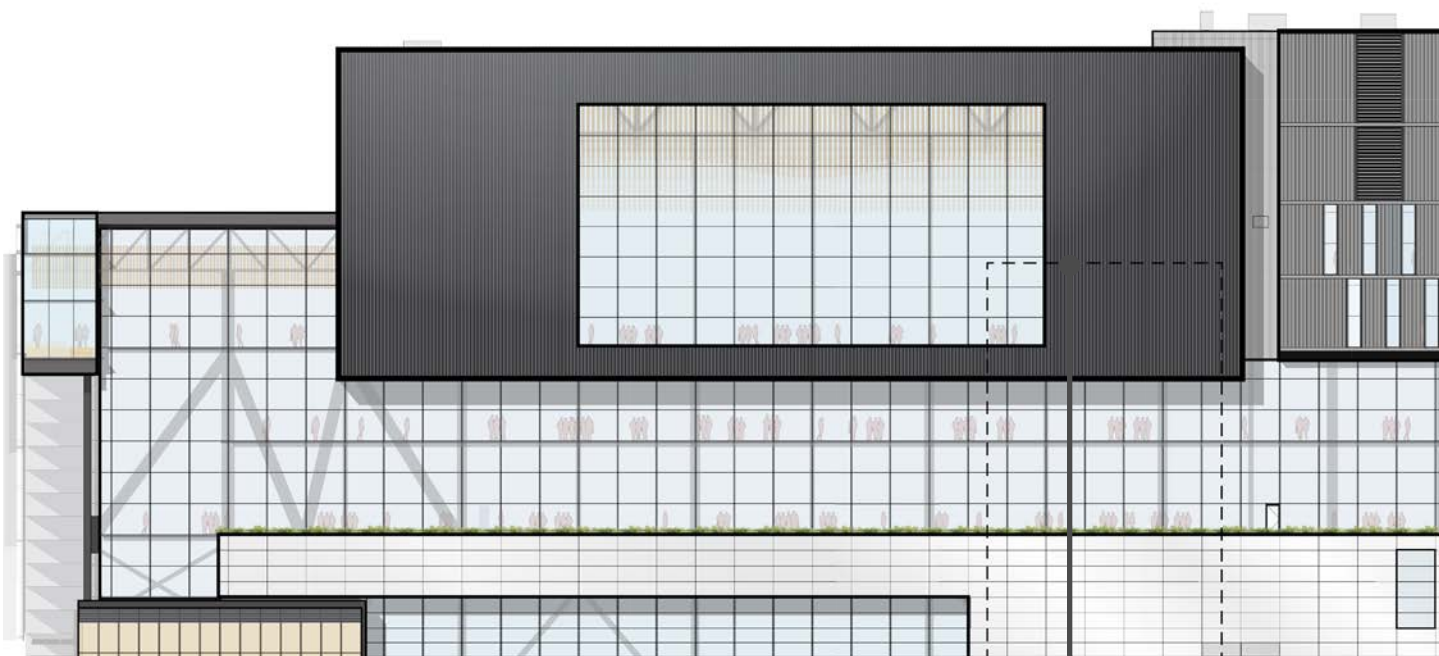
SECTION PERSPECTIVE - SOUTH STAIR

2 DR 02 RECOMMENDED CONDITIONS OF APPROVAL

DRB#2 - 07-19-2016 - PRESENTATION SLIDE 15



DRB #3 REFER TO RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - A322



ENLARGED EAST ELEVATION - BOREN AVENUE



SECTION PERSPECTIVE - EAST BALLROOM

DRB CONDITION OF APPROVAL #3 South Stair and East Ballroom Caps

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DRB CONDITION OF APPROVAL #4 East Ballroom 'Landmark Building Graphic'

The signature lighting should be limited to the two facades where the mixing zone reaches the north and south streets, in concert with the light totems and other graphic elements the Board heartily endorsed [65/left]. Any lighting at the east ballroom should be fully internalized, coordinated with the more important ceiling design, and be fully dimmable.

RESPONSE - The design was revised to remove the landmark lighting and fully internalize lighting at the east ballroom. This lighting will be fully controllable and dimmable. The window proportion was also revised to reduce the visual impact to the building across the street.

RESPONSE CONDITION #4:
window adjusted to reduce impact
(Olive Tower behind)

2 DR 02 RECOMMENDED CONDITIONS OF APPROVAL

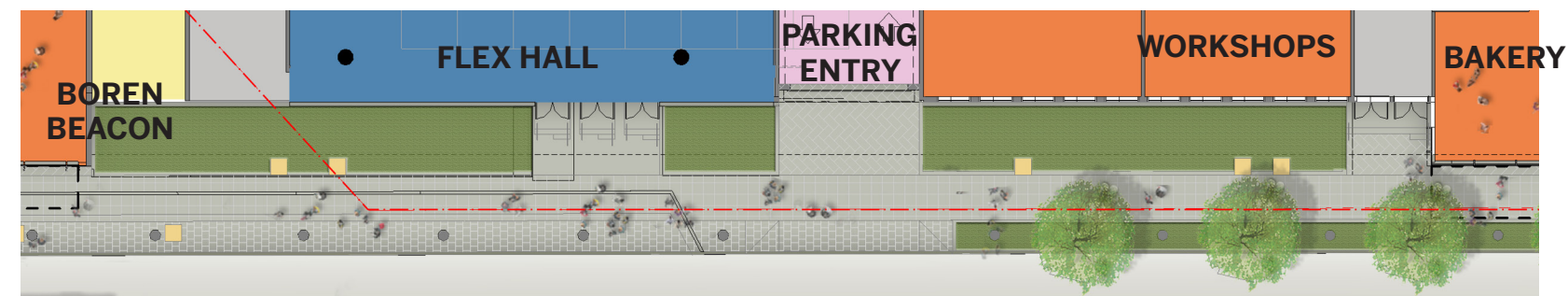
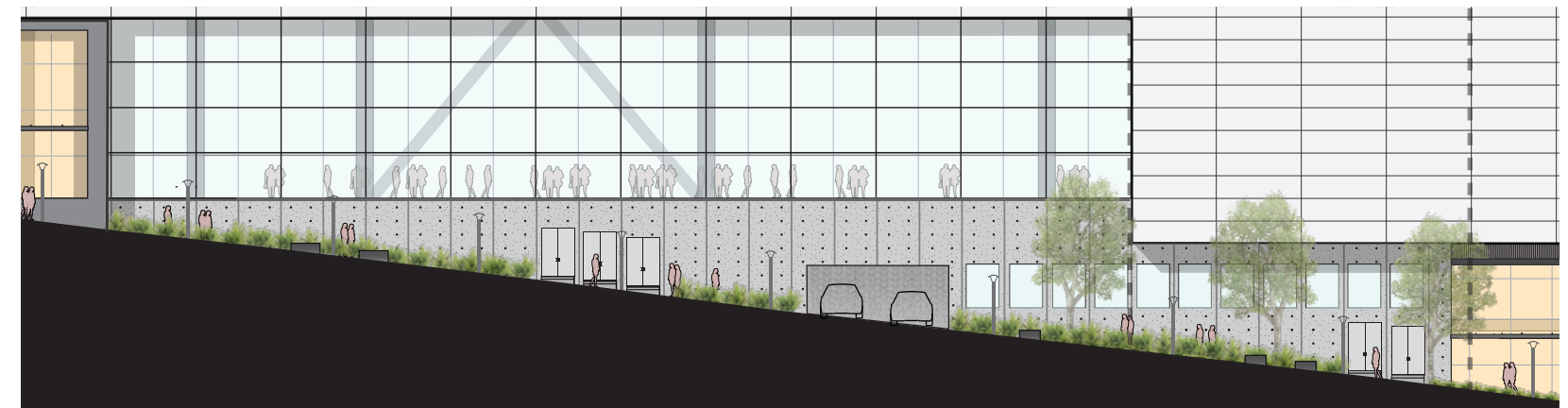
DRB CONDITION OF APPROVAL #5

Boren Avenue Blank Walls

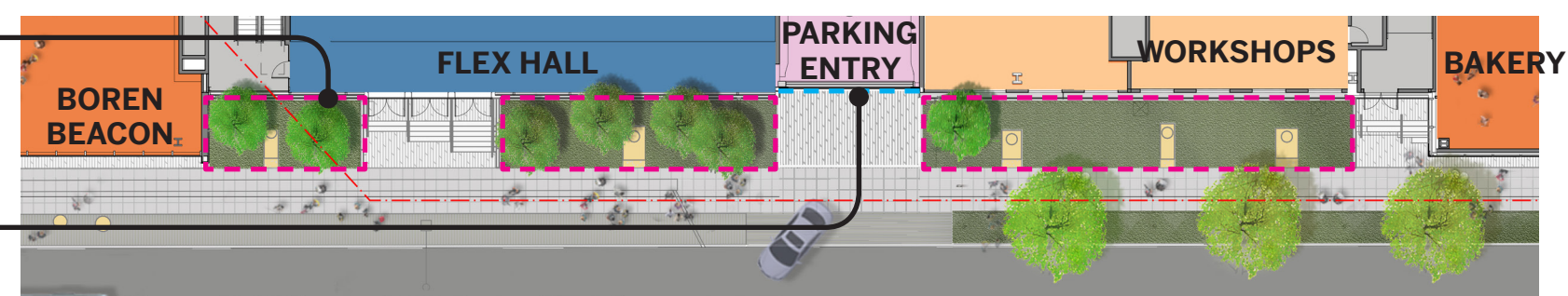
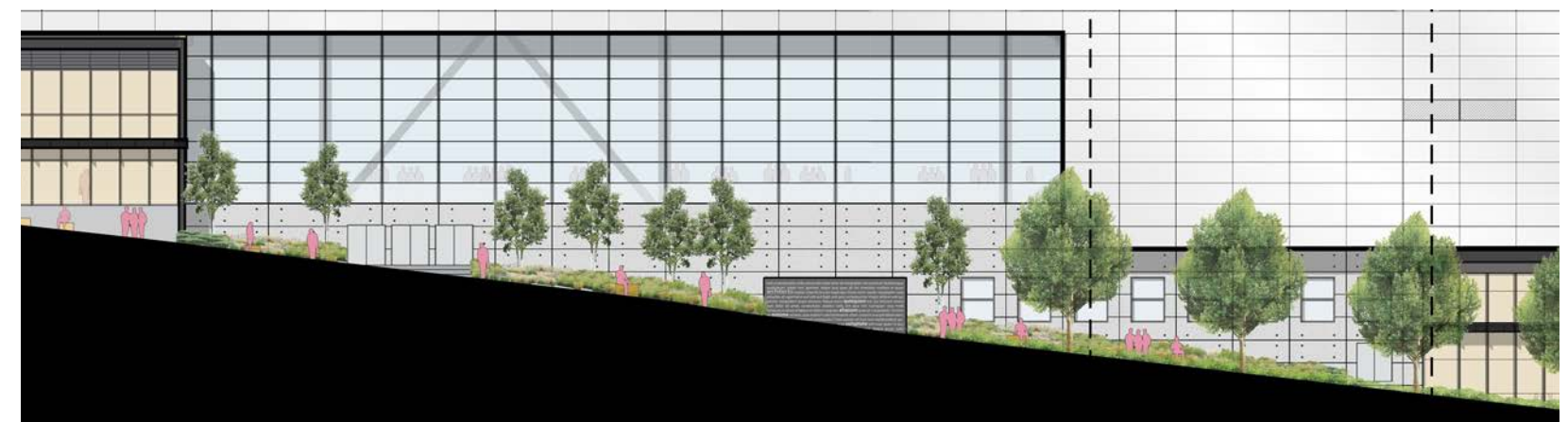
All the ground level concrete along Boren should have a finer grain texture than that shown on pg 32/F; it should be deeply scored more frequently than the 8ft shown on pg 39, and/OR the entire strip of plantings hugging that concrete wall should be 3-5 ft tall (yet not create CPTED issues) (see departure 4b).

RESPONSE - The design was revised to slope the landscape at the Boren ground level up towards the concrete wall to create a larger volume of planting for a greater visual impact. Additional trees and Public Benefit artworks have been added to provide texture and variety. Seating design and lighting elements to be incorporated into the garden in coordination with the selected artist, per SDC guidance. Urban Design Merit artwork to be included at vehicle entry, as developed by selected artist.

DRB #2 - 07-19-2016 - PRESENTATION SLIDE 15



DRB #3 REFER TO RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - A322; L112

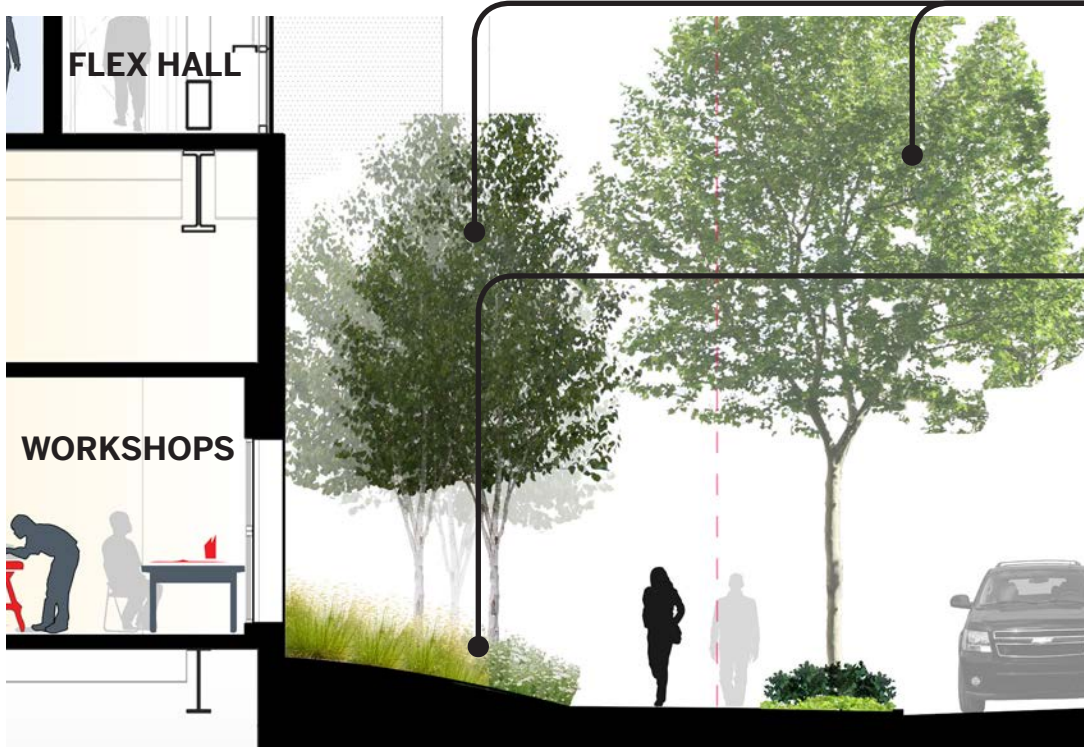


RESPONSE CONDITION #5:
Public Benefit artworks add visual interest and focal points with integrated seating areas; seating design and lighting elements to be incorporated into the garden in coordination with the selected artist, per SDC guidance.

RESPONSE CONDITION #5:
Urban Design Merit artwork to be designed by artist at vehicle entry door; door to be custom perforated illustration

2 DR 02 RECOMMENDED CONDITIONS OF APPROVAL

DRB #3 REFER TO RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - A322; L112



SECTION THROUGH EAST FACADE AND BOREN AVENUE PLANTING



Gaultheria shallon
Salal



Mahonia nervosa
Dwarf Oregon grape



Verbena lilacina 'De La Mina'
Purple Cedros Island Verbena



Polystichum munitum
Western sword fern



Bouteloua gracilis 'Blonde Ambition'
Blonde Ambition Blue Grama Grass



Deschampsia cespitosa
'Goldtau'
Goldtau Tufted Hair Grass



Achillea millefolium 'Sonoma Coast'
Sonoma Coast Yarrow



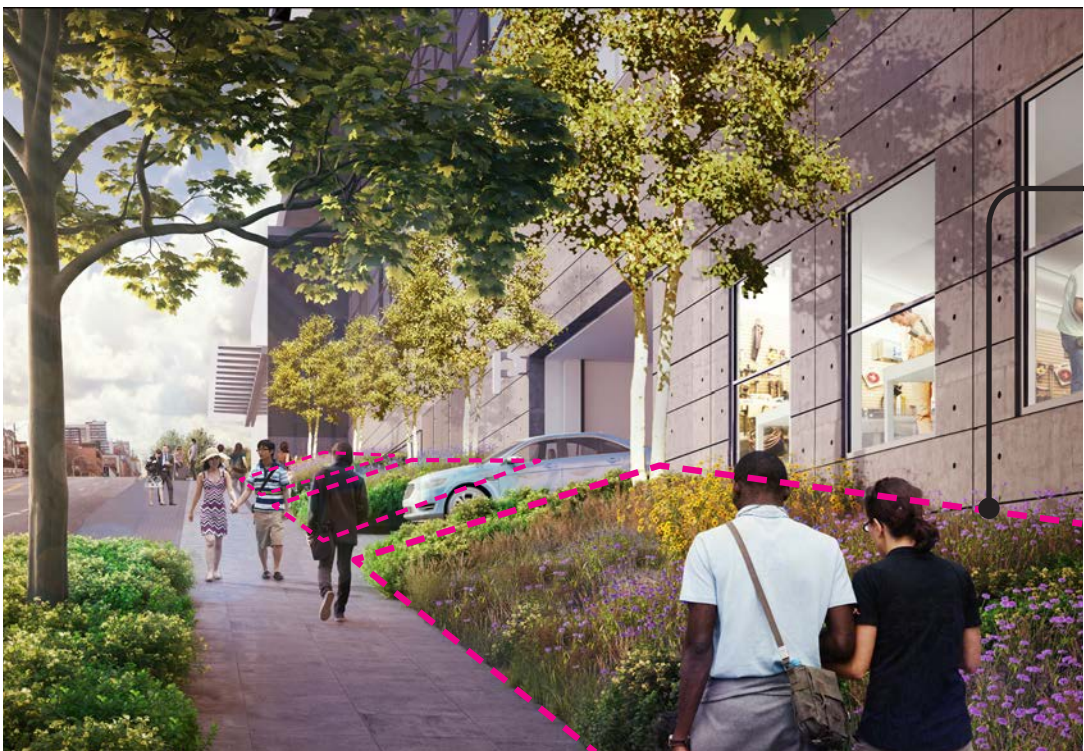
Erigeron glaucus
Wayne Roderick Daisy



Bidens laevis
Smooth Beggartick

RESPONSE CONDITION #5:
additional trees to add variety and texture

RESPONSE CONDITION #5:
sloped planting and landscaping to provide a
greater visual impact



VIEW LOOKING SOUTH ON BOREN AVENUE



VIEW LOOKING NORTH ON BOREN AVENUE

RESPONSE CONDITION #5:
Public Benefit artworks add visual interest
and focal points with integrated seating
areas; seating design and lighting elements
to be incorporated into the garden in
coordination with the selected artist, per
SDC guidance.

RESPONSE CONDITION #5:
Urban Design Merit artwork to be designed
by artist at vehicle entry door; door to be
custom perforated illustration

2 DR 02 RECOMMENDED CONDITIONS OF APPROVAL

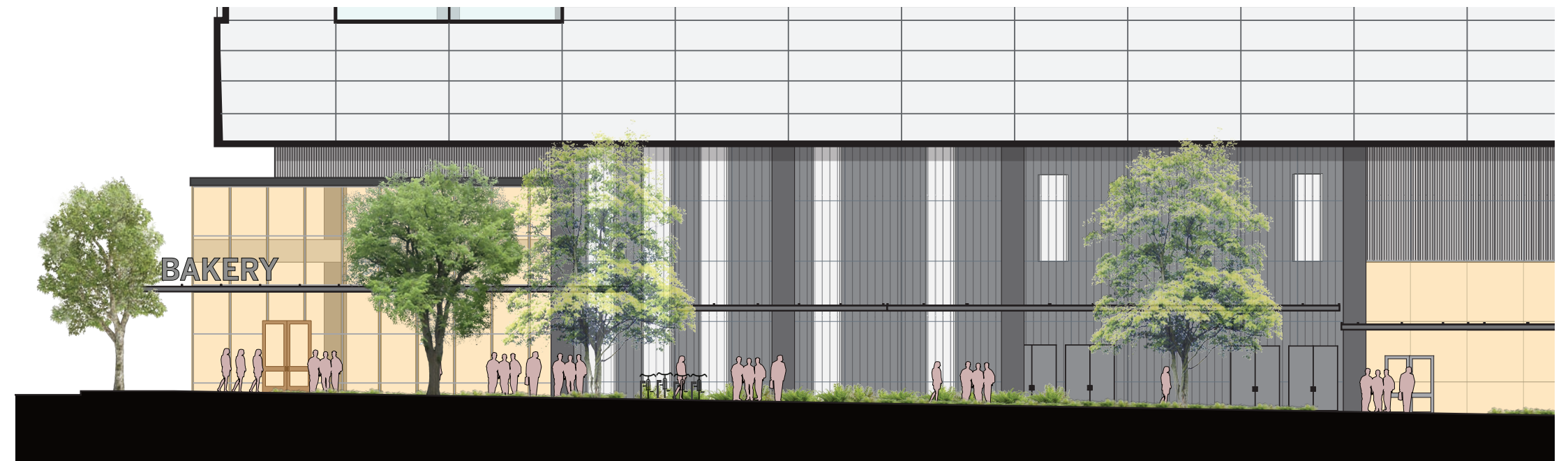
DRB CONDITION OF APPROVAL #6

Olive Way Blank Walls / Exit Doors

All the Olive exit doors should ideally be transparent glass, and at minimum the vertical strips of translucent windows above [see pg 37] should be carried down to grade, even if those corresponding aligned doors are translucent glass (see departure 4a).

RESPONSE - The design was revised to bring vertical glazing down to grade in multiple locations at exits. Additional Public Benefits artworks to be developed by artist at (5) glass vitrines, per SDC guidance.

DRB#2 - 07-19-2016 - PRESENTATION SLIDE 17



DRB #3 REFER TO RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - A323

RESPONSE CONDITION #6: translucent glazing continues to pedestrian grade level at exits

RESPONSE CONDITION #6: artworks to be developed by artist at (5) glass vitrines, per SDC guidance



ENLARGED NORTH ELEVATION - OLIVE WAY

2 DR 02 RECOMMENDED CONDITIONS OF APPROVAL

DRB#2 - 07-19-2016 - PRESENTATION SLIDE 17



DRB #3 REFER TO RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - A323; L111; L112



ENLARGED NORTH ELEVATION - OLIVE WAY



RESPONSE CONDITION #6:
translucent glazing continues to pedestrian
grade level at exits

RESPONSE CONDITION #6: artworks to be
developed by artist at (5) glass vitrines, per
SDC guidance



2 DR 02 RECOMMENDED CONDITIONS OF APPROVAL

DRB CONDITION OF APPROVAL #7 9th Avenue Blank Walls/Exit Doors

All the 9th Avenue exit doors should ideally be transparent or translucent glass, and at minimum a vertical planting, screen or visually interesting artwork should be installed in the middle of that 42 ft length [81, gray portion], in a door gap that is as wide as possible (see departure 4c).

RESPONSE - The design was revised to include more planting / additional tree at the 9th Avenue exit doors to add visual interest and texture. Further development of the facade materials and metal panels add interest to the facade, and relate to the warm wood of the adjacent market.

DRB #2 - 07-19-2016 - PRESENTATION SLIDE 10



DRB #3 REFER TO RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - A324; L111



2 DR 02 RECOMMENDED CONDITIONS OF APPROVAL

DRB #3



Polystichum munitum
Western sword fern



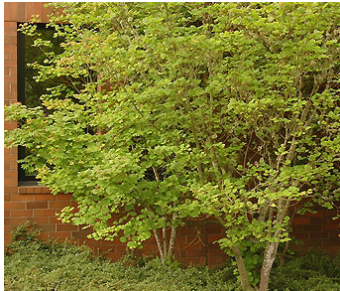
Cornus canadensis
Bunchberry dogwood



Mahonia nervosa
Dwarf Oregon grape



MTLP-5
Metal Panel Wall 1/8"
Aluminum Or Steel Plate Open
Joint Rain Screen System



Acer circinatum
Vine maple

RESPONSE CONDITION #7: additional tree and planting to add visual interest and texture at exit

RESPONSE CONDITION #7: further development of the facade materials and metal panels add interest to the facade, and relate to the warm wood of the adjacent market.

DRB #2



DRB #3









DRB #3



DRB #2



DRB #3



DRB #2





DRB #2



DRB #3



DRB #2

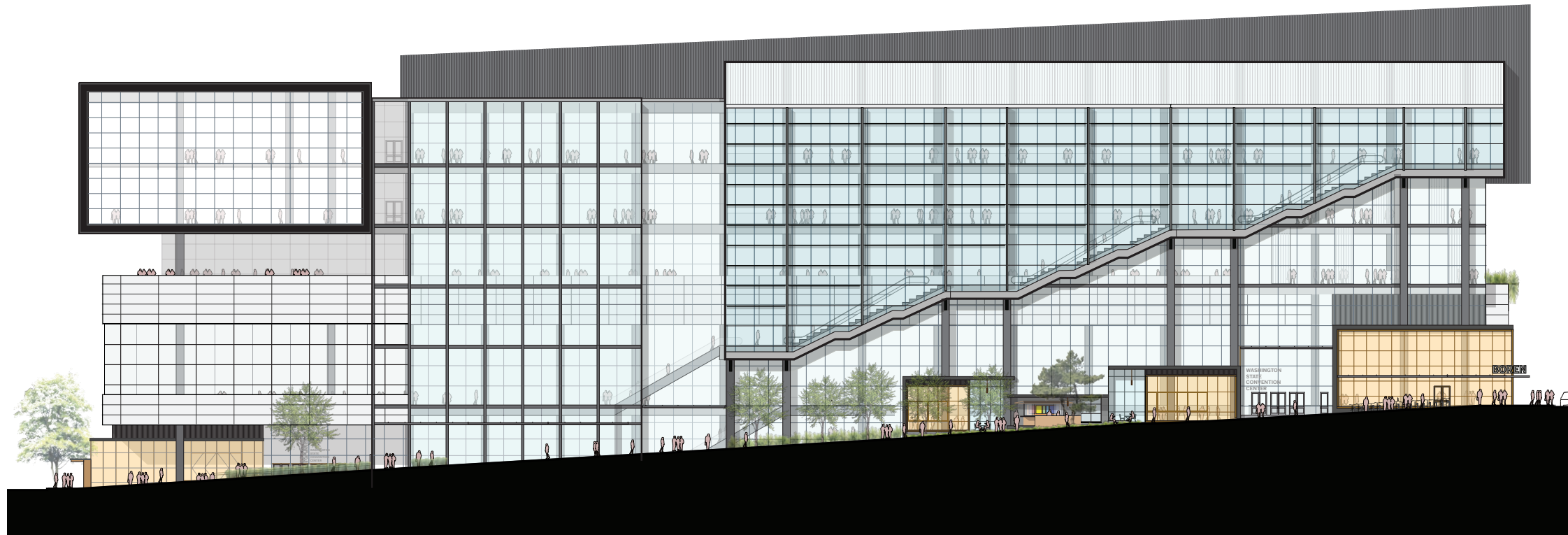


DRB #3

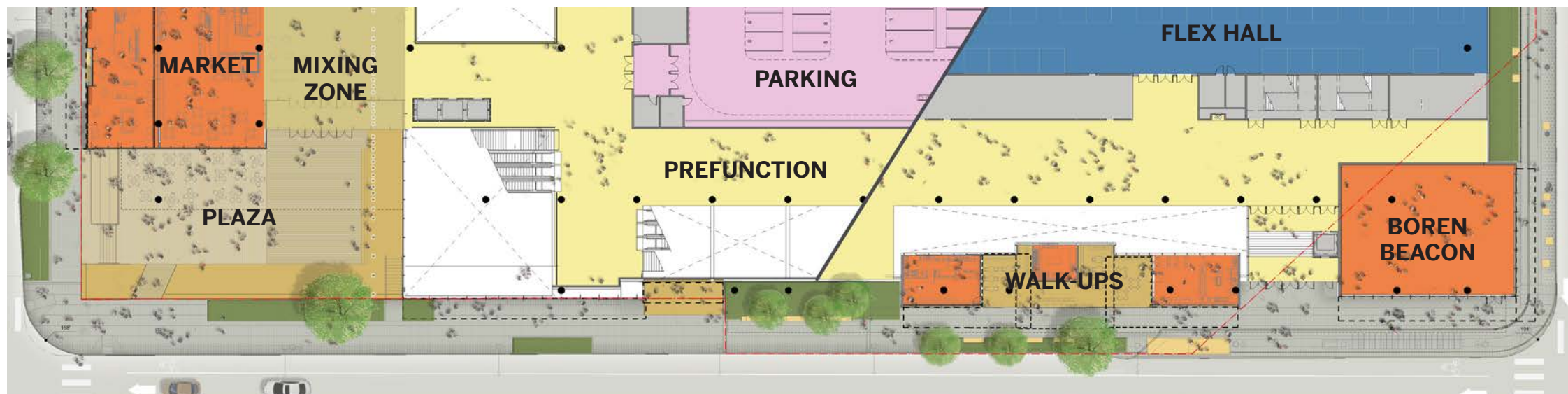


3 DESIGN REVIEW & UPDATES

DRB #2 - 07-19-2016 - PRESENTATION SLIDE 12



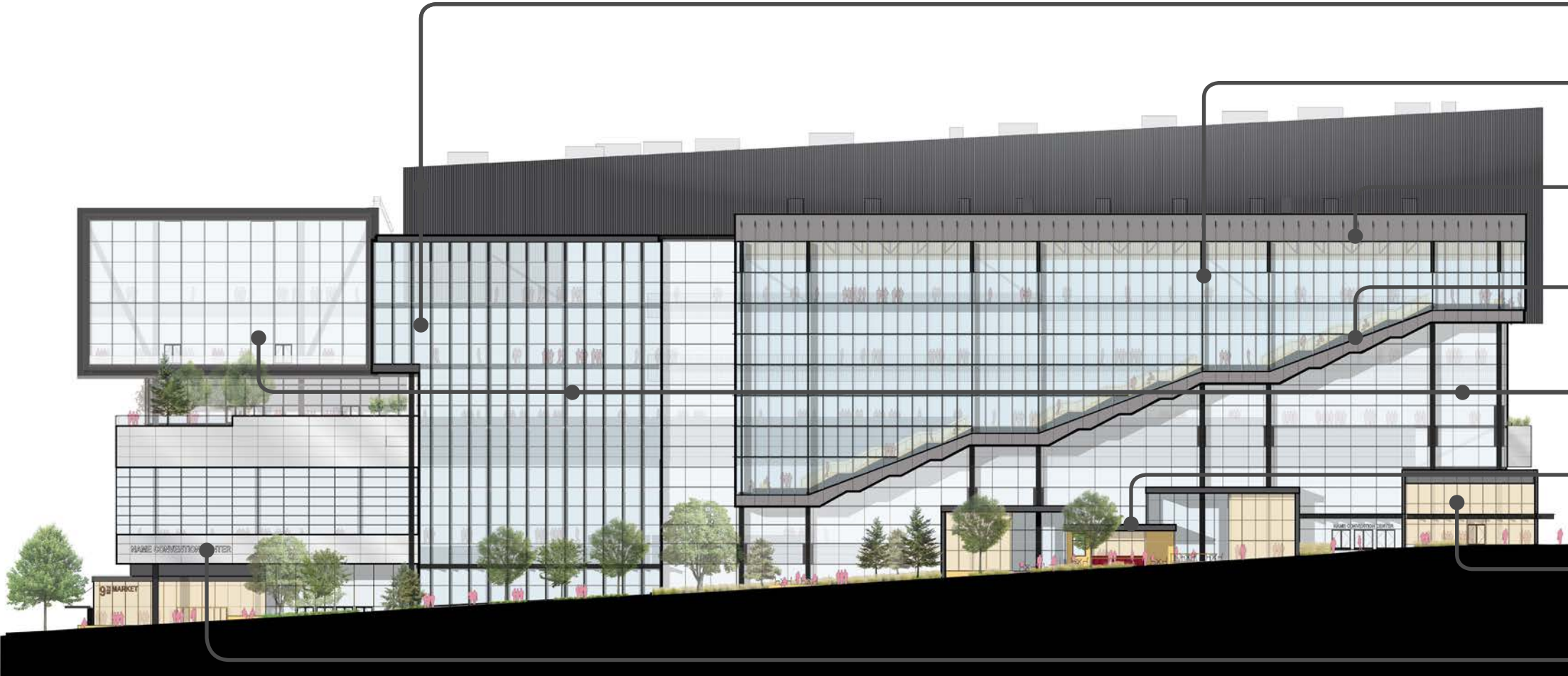
SOUTH ELEVATION - PINE STREET



SITE PLAN

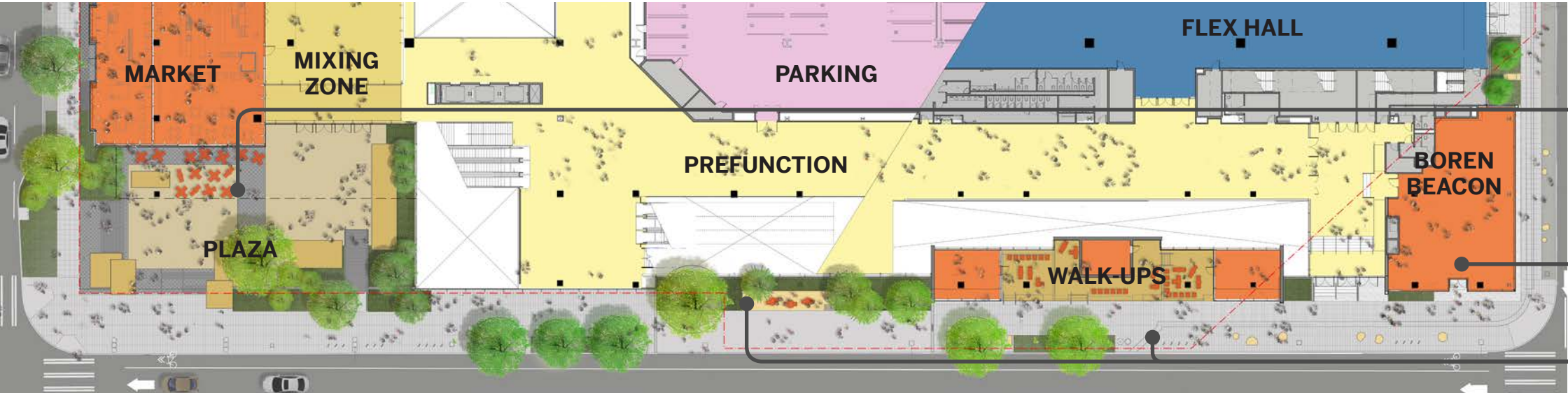
3 DESIGN REVIEW & UPDATES

DRB #3 REFER TO RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - A321; L111; L112



SOUTH ELEVATION - PINE STREET

- UPDATE: massing refined to better frame public plaza and clarify building forms
- UPDATE: reduced quantity of structural fins to allow more clear views from interior to exterior
- RESPONSE CONDITION #3: opaque metal panel at top of hillclimb has been reduced
- RESPONSE CONDITION #1: refined stair and glazing profile to simplify material detailing
- UPDATE: simplified curtain wall structure to provide more transparency
- RESPONSE CONDITION #2: Pine Street retail roofs illustration graphics
- UPDATE: glazing system and soffits refined at retail areas
- UPDATE: landmark lighting removed and signage added to facade to subtly provide clarity and wayfinding

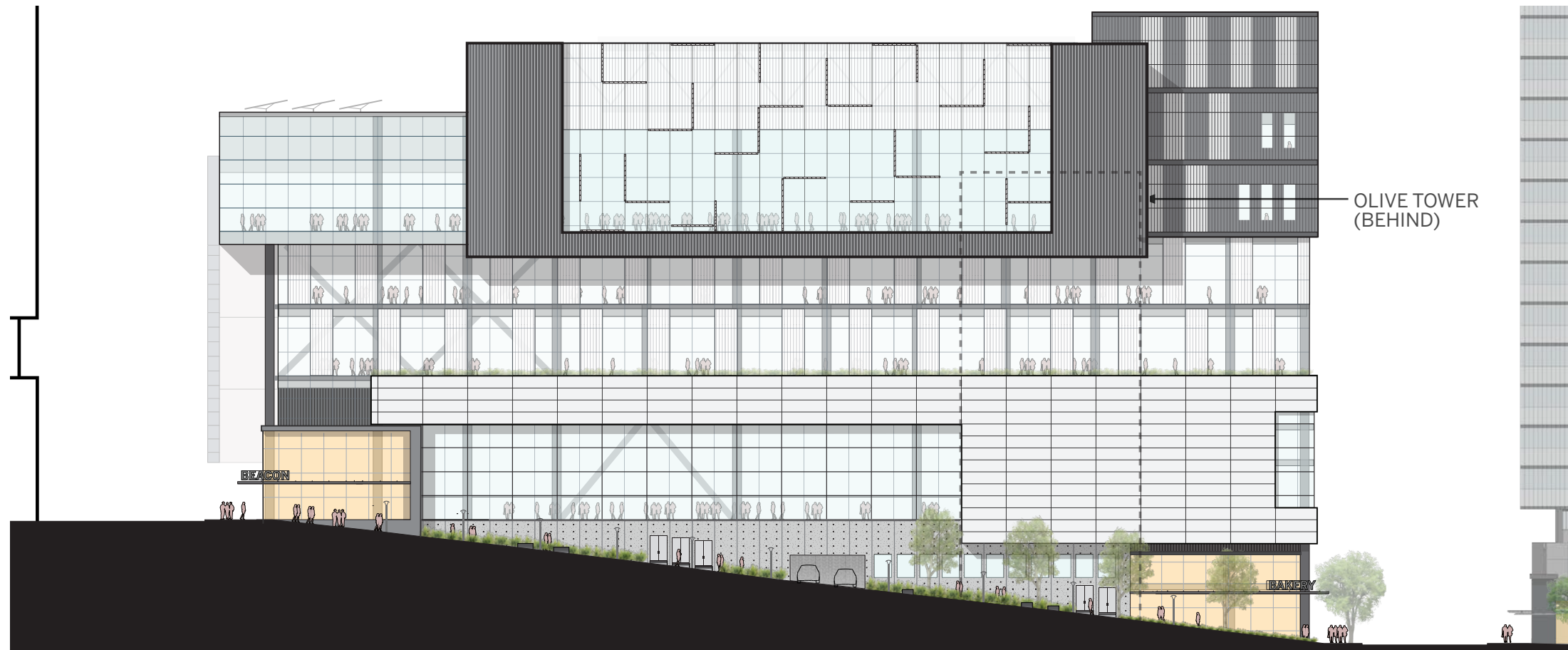


SITE PLAN

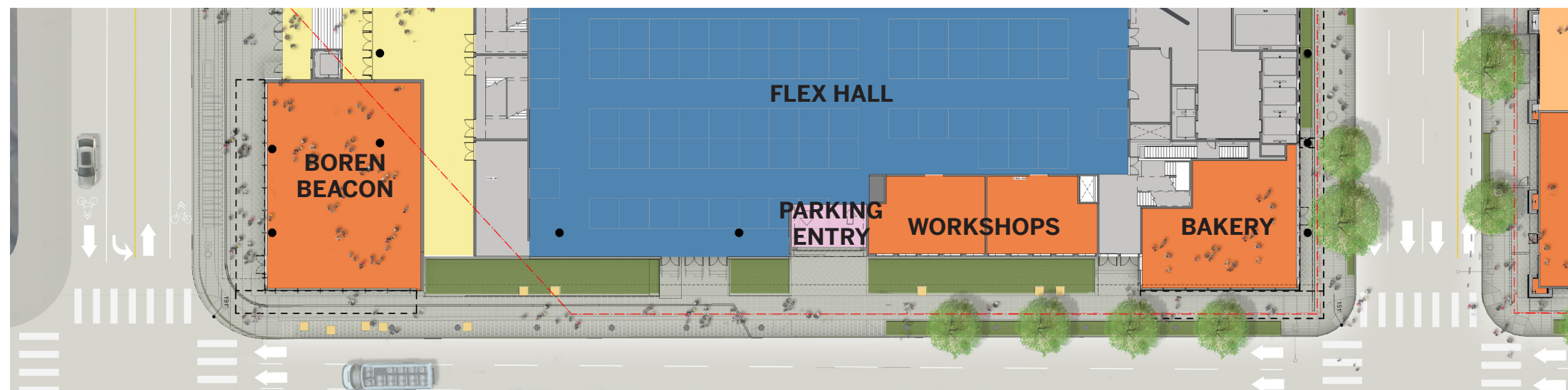
- UPDATE: plaza materials and organization refined to better connect pedestrians with urban streetscape, add seating and planting, and more clearly define spaces, per SDC guidance. A portion of this plaza is also a public benefit.
- UPDATE: re-oriented Boren Beacon massing to better relate to Boren frontage and Capitol Hill
- UPDATE: additional planting added; seating reconfigured per SDC guidance. This area is also a public benefit.

3 DESIGN REVIEW & UPDATES

DRB #2 - 07-19-2016 - PRESENTATION SLIDE 15



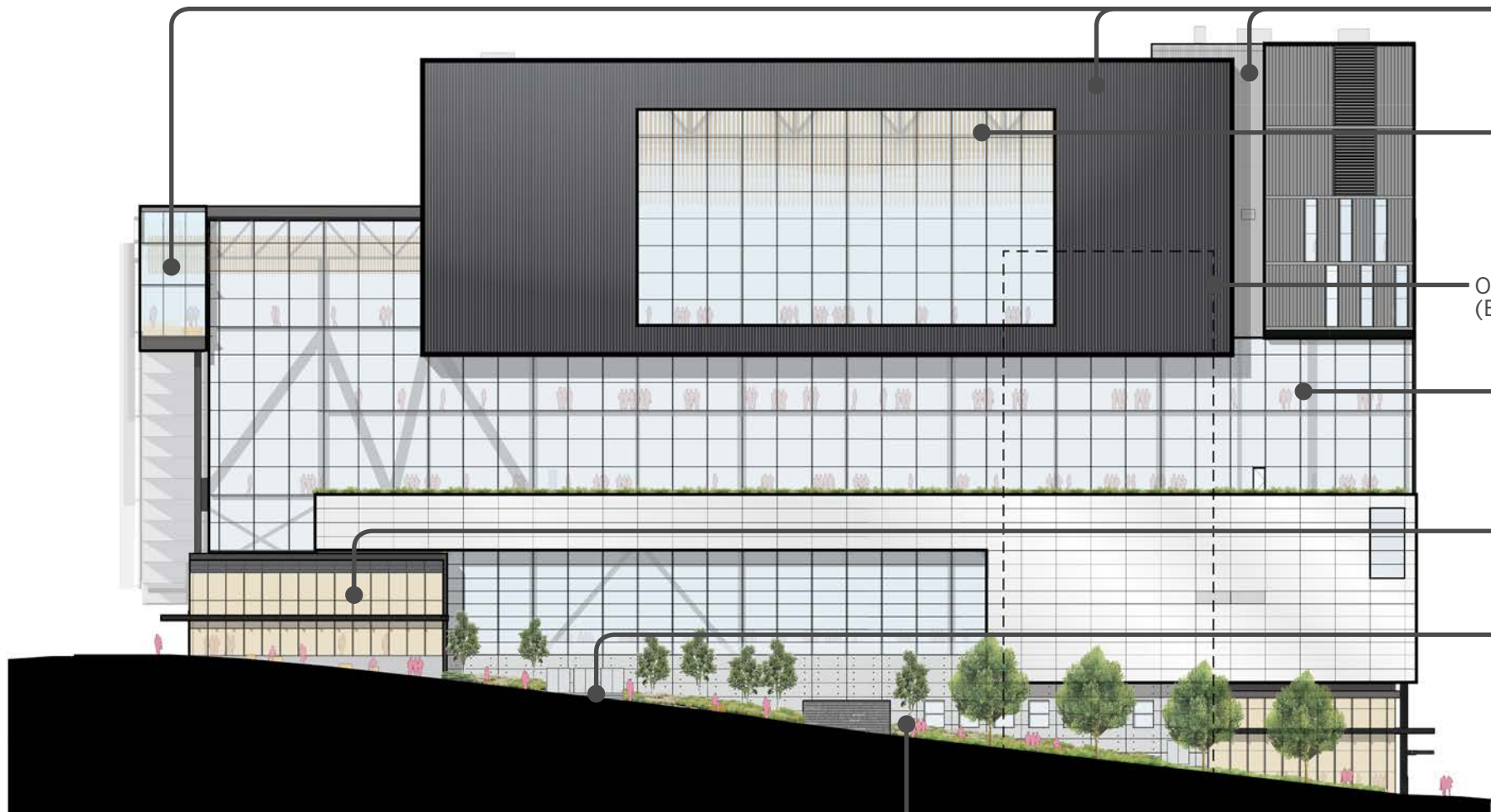
EAST ELEVATION - BOREN AVENUE



SITE PLAN

3 DESIGN REVIEW & UPDATES

DRB #3 REFER TO RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - A322; L112



EAST ELEVATION - BOREN AVENUE

UPDATE: refined massing and proportions to better clarify building forms

RESPONSE CONDITION #3 & 4: additional transparent glazing at top of ballroom; lighting fully internalized and integrated with interior ceiling system. Window proportion adjusted to reduce visual impact.

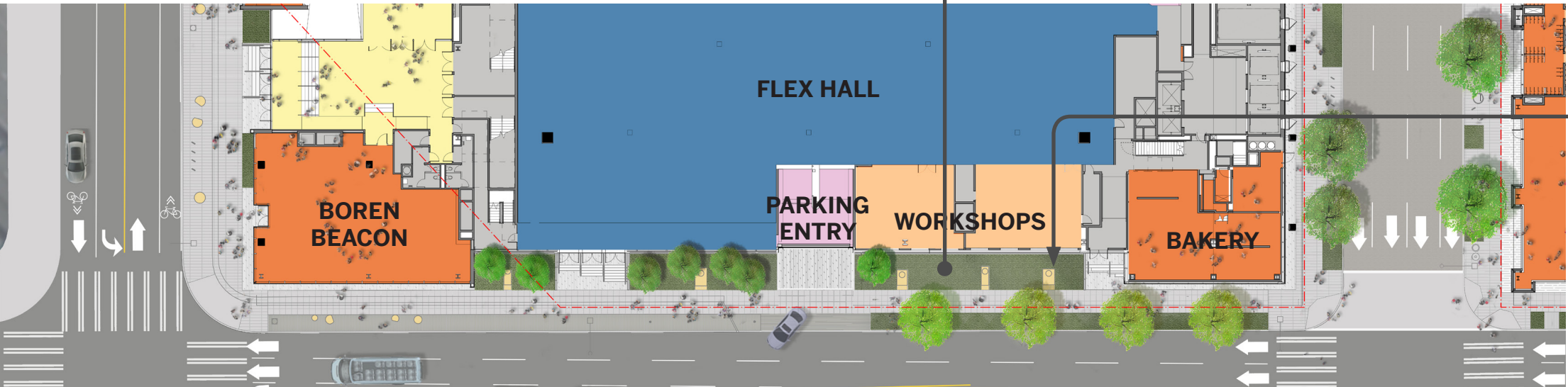
UPDATE: simplified curtain wall structure to provide more transparency

UPDATE: re-oriented Boren Beacon massing to better relate to Boren frontage and Capitol Hill

UPDATE: egress stairs shifted to better distribute planting and seating along Boren frontage

RESPONSE CONDITION #5: additional trees and sloped planting to add variety and texture

RESPONSE CONDITION #5: Public Benefit artworks add visual interest and focal points with integrated seating areas; seating design and lighting elements to be incorporated into the garden in coordination with the selected artist, per SDC guidance. This area is also a public benefit.



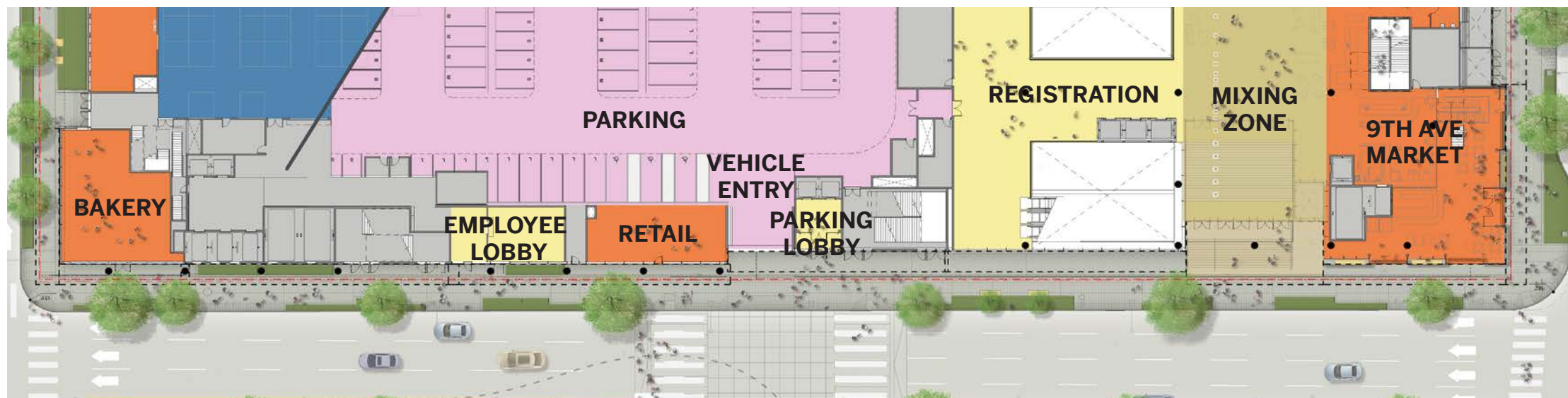
SITE PLAN

3 DESIGN REVIEW & UPDATES

DRB #2 - 07-19-2016 - PRESENTATION SLIDE 17



NORTH ELEVATION - OLIVE WAY



SITE PLAN

3 DESIGN REVIEW & UPDATES

DRB #3 REFER TO RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - A323; L111; L112

NORTH ELEVATION - OLIVE WAY

- UPDATE: refined massing to better articulate urban block patterns and break down scale
- UPDATE: glazing pattern refined to coordinate with adjacent glazing pattern and massing
- UPDATE: planters articulated with wood paneling; locations revised within frame to better provide views from interior spaces; glazing behind revised to animate facade
- UPDATE: simplified curtain wall structure to provide more transparency
- UPDATE: landmark lighting removed and signage added to facade to subtly provide clarity and wayfinding
- UPDATE: window proportion changed to better define massing and address interior functions
- UPDATE: entry canopy more pronounced

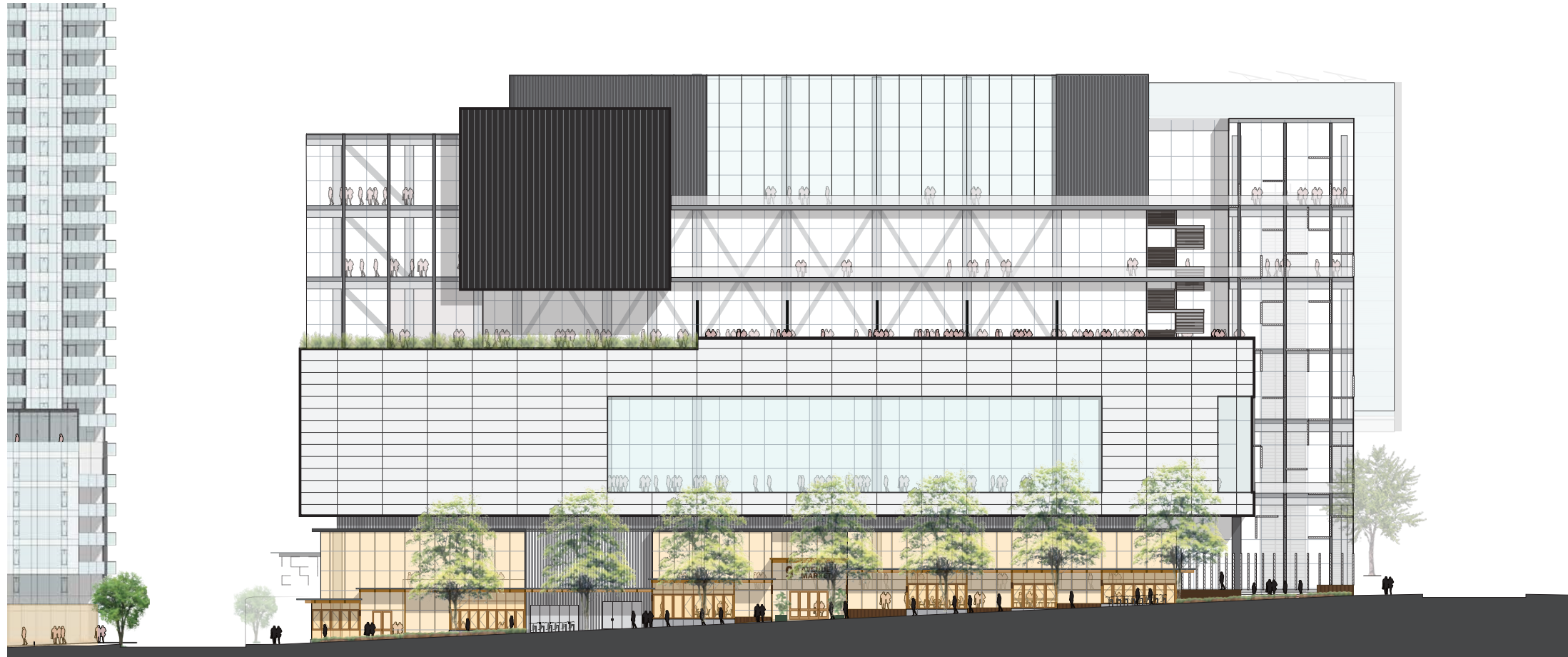
SITE PLAN

- RESPONSE CONDITION #6: translucent glazing continues to pedestrian grade level at exits; artworks to be developed by artist at (5) glass vitrines, per SDCI guidance
- UPDATE: additional area open to below to provide increased visibility for pedestrians into exhibit space and provide visual interest
- UPDATE: additional planting and seating added along Olive Way frontage with potential bus stop
- UPDATE: vehicle entry shifted east to be off axis with Terry Avenue, per SDOT & SDCI feedback, landscape added at terminus.

SITE PLAN

3 DESIGN REVIEW & UPDATES

DRB #2 - 07-19-2016 - PRESENTATION SLIDE 10



WEST ELEVATION - 9TH AVENUE



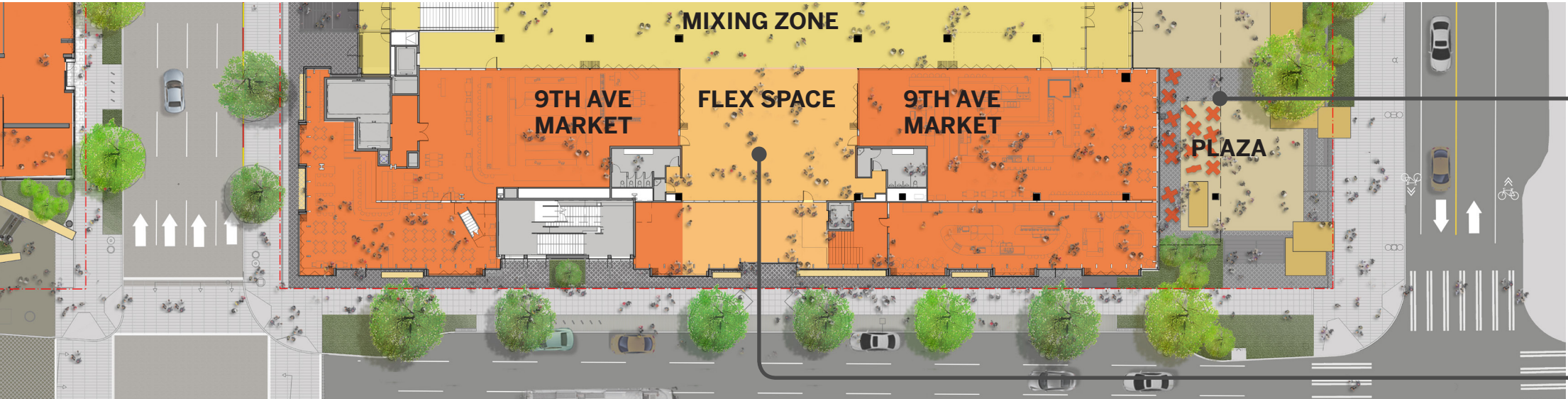
SITE PLAN

3 DESIGN REVIEW & UPDATES

DRB #3 REFER TO RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - A324; L111



WEST ELEVATION - 9TH AVENUE



SITE PLAN

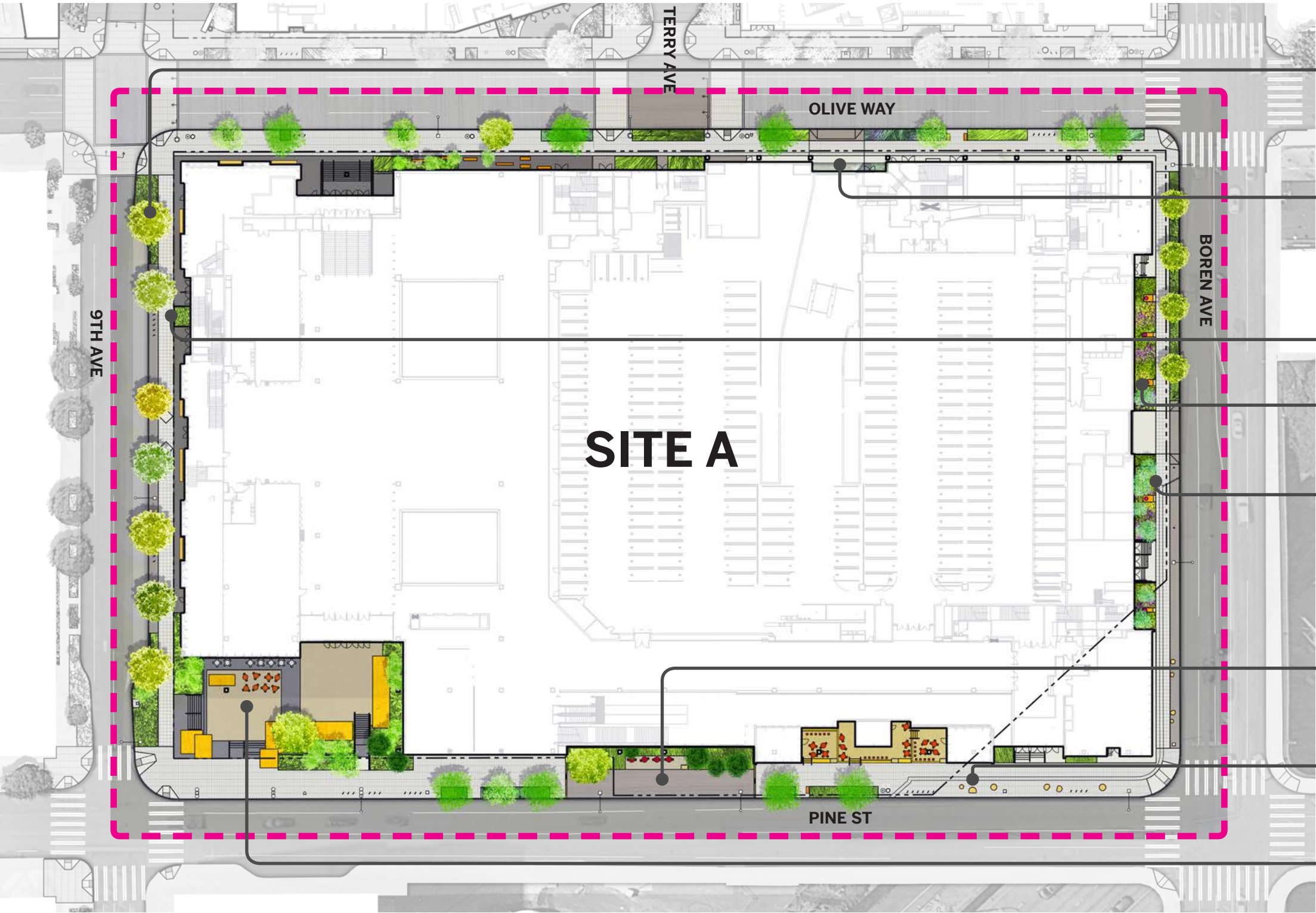
3 DESIGN REVIEW & UPDATES - LANDSCAPE CONCEPT

DRB # 2 - 07-19-2016 - PRESENTATION BOOKLET PAGE 47



3 DESIGN REVIEW & UPDATES - LANDSCAPE CONCEPT

DRB #3



UPDATE: 9th Ave tree locations refined per utility coordination. Species updated per guidance from SDOT Urban Forestry.

UPDATE: Site A garage entry location moved east to misalign from Terry Ave intersection, per SDCI & SDOT guidance.

RESPONSE CONDITION #7: additional tree and planting to add visual interest and texture at exit

UPDATE: Artworks added to Boren Ave Garden per SDC guidance. This area and artworks are also a public benefit.

RESPONSE CONDITION #5: additional trees and sloped planting to add variety and texture

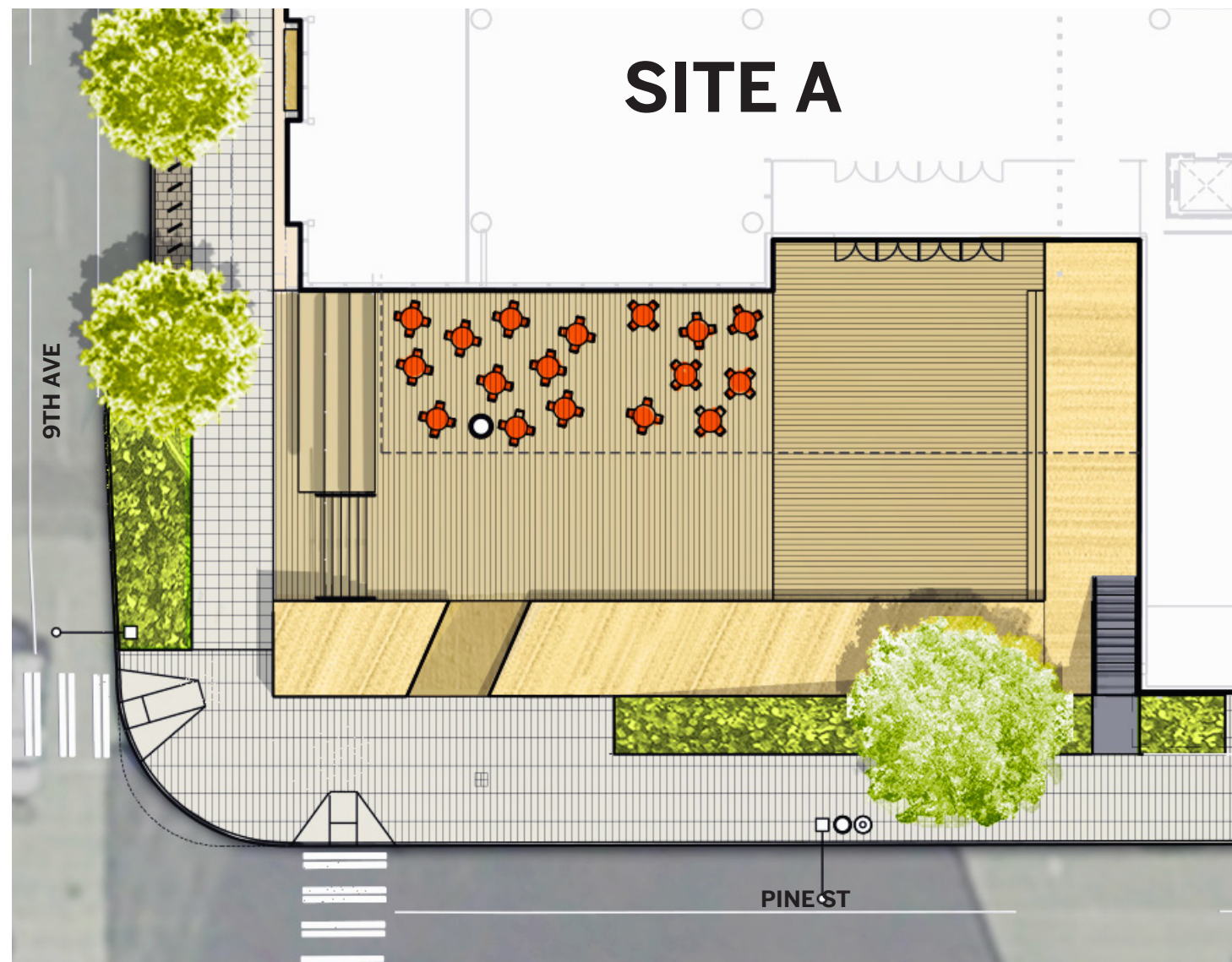
UPDATE: Pine St sungarden expanded and refined per SDC guidance. Paving differentiated to articulate the concept of breaking down the double block length and relate to Terry Ave Green Street design between Olive Way and Howell St. This area is also a public benefit.

UPDATE: Bike share station removed due to the termination of Seattle's Pronto program. Wood seating platforms replaced with custom wood benches, per SDC guidance.

UPDATE: 9th and Pine plaza design refined per SDC guidance. A portion of this area is also a public benefit.

3 DESIGN REVIEW & UPDATES - 9TH & PINE PLAZA

DRB # 2 - 07-19-2016 - PRESENTATION BOOKLET PAGE 70



SITE PLAN - 9TH & PINE PLAZA

DRB #3



3 DESIGN REVIEW & UPDATES - 9TH & PINE PLAZA



PAVING INTEGRATED POETRY



ENGRAVED TEXT PRECEDENT

UPDATE: Plaza surface to be integrated with commissioned poetry by Seattle writers. These poems will provide culturally relevant content to locals and visitors alike.



UPDATE: large specimen tree moved into the plaza (previously shown at sidewalk level), with additional small trees added for visual interest and comfort.

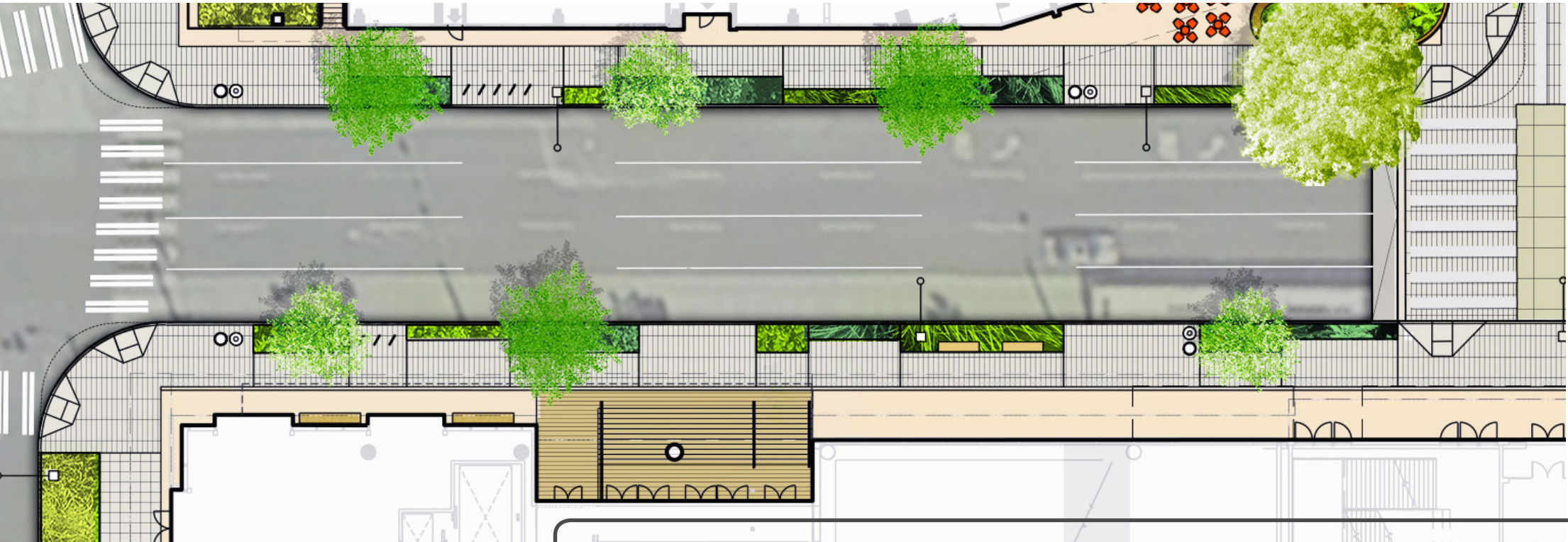
UPDATE: widened all access points to ease pedestrian circulation and better connect the plaza to active urban sidewalks

UPDATE: varied the scale and orientation of fixed seating platforms

UPDATE: plaza materials and organization refined to better connect pedestrians with urban streetscape, add seating and planting, and more clearly define spaces, per SDC guidance

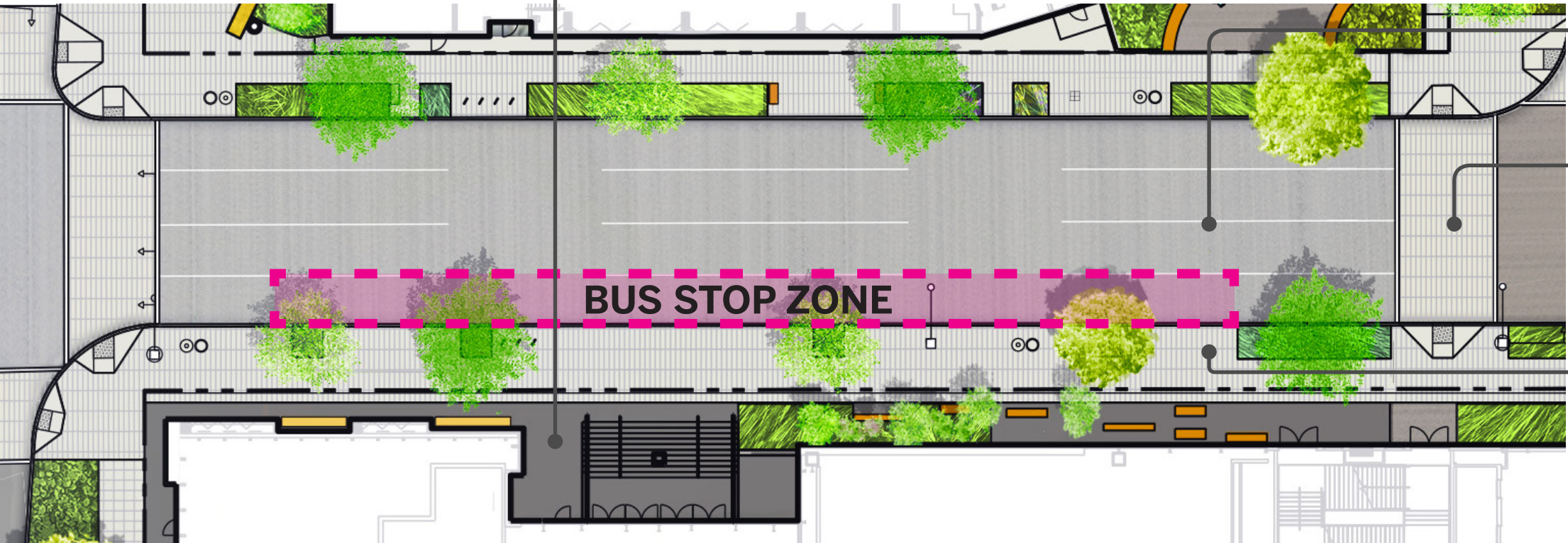
9TH & PINE PLAZA

3 DESIGN REVIEW & UPDATES - OLIVE WAY BUS STOP



DRB 2

UPDATE: Olive Avenue entry stair and 9th Avenue market paving against the building facade updated to dark basalt paving per SDC guidance.



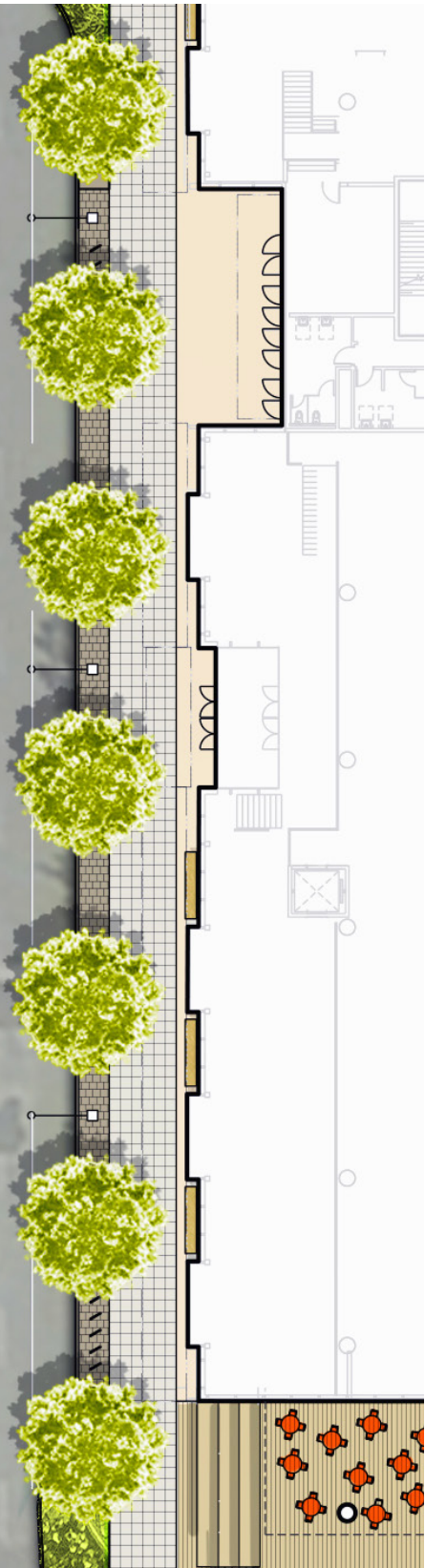
DRB 3

UPDATE: Olive Way roadway paved with City of Seattle standard roadway concrete, per SDOT guidance.

UPDATE: Concrete crosswalks updated to include side striping instead of ladder striping, per SDC guidance.

UPDATE: Olive Way streetscape design updated to accommodate a 180'-0" bus stop, per King County Metro guidance. Design includes trees and open paving along the curb, with a generous planter, wood benches, and overhead weather protection with integrated lighting along the building facade.

3 DESIGN REVIEW & UPDATES - CONVENTION PLACE STATION ART RELOCATION



DRB 2



DRB 3

UPDATE: Through coordination and guidance from 4Culture and SDOT Urban Forestry, the 9th Avenue streetscape design now includes the integration of an existing Convention Place Station artwork, the *Tree Museum*. Art piece includes the relocation of five metal tree grates, corresponding name plaques, and new corresponding trees.

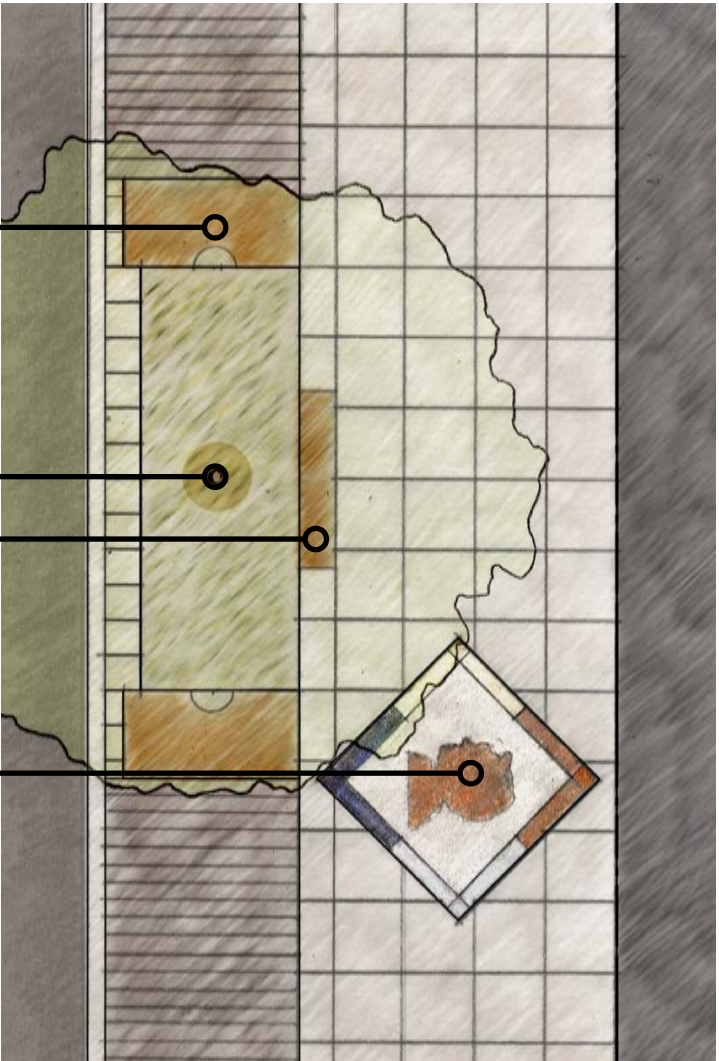
UPDATE: Through coordination and guidance from 4Culture, the 9th Avenue streetscape design now includes the integration of an existing Convention Place Station artwork, the *Traveling Stories*. Art piece includes two granite and concrete profiles, to be inlaid in the 9th Ave sidewalk at the entrance to the 9th Ave market.

Tree grate panel, typical alignment

Corresponding tree

Tree name plaque, typical alignment

Granite and concrete profile, typical alignment



PROFILE, 1 OF 2



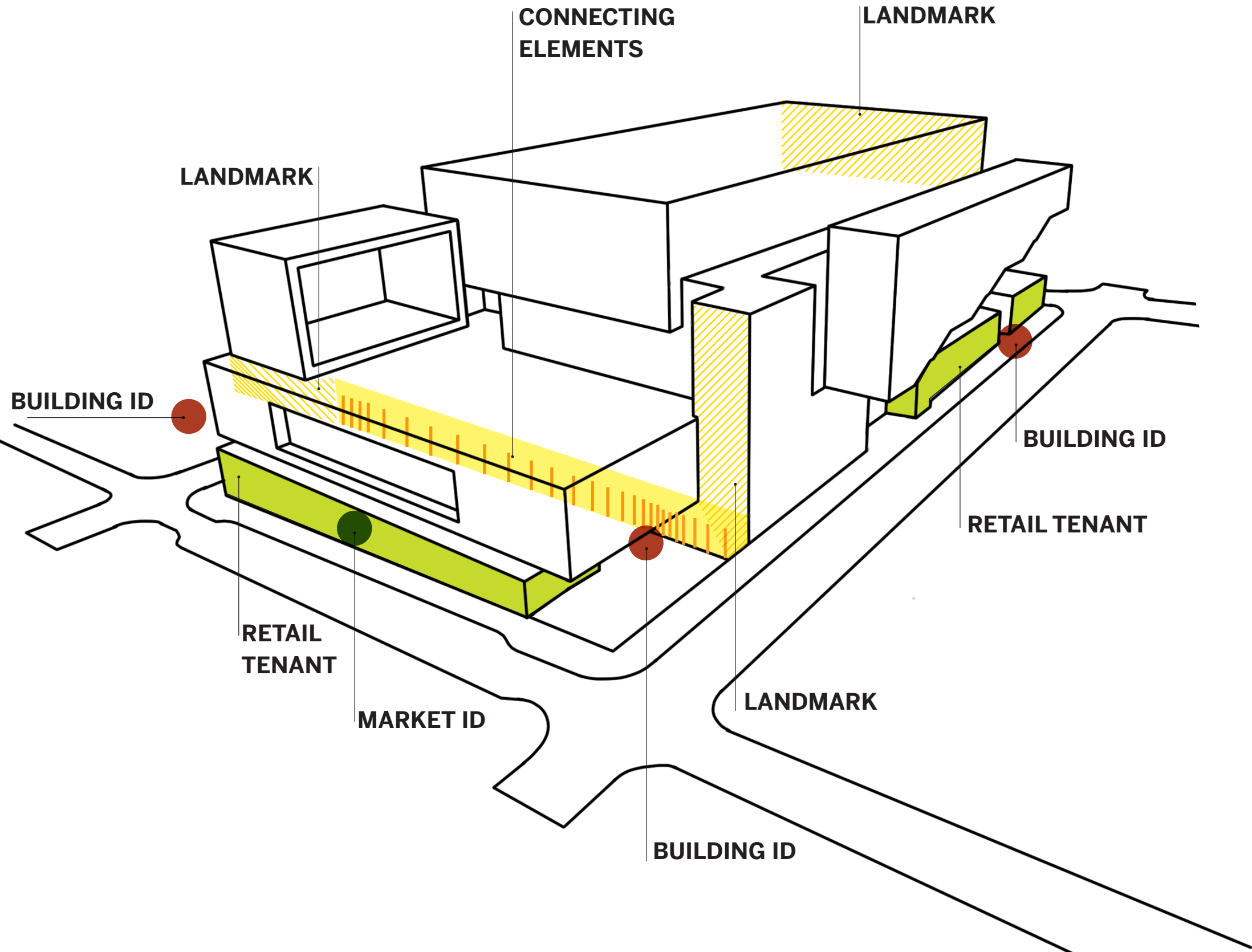
PROFILE, 2 OF 2



TREE GRATE & PLAQUE, 1 OF 5

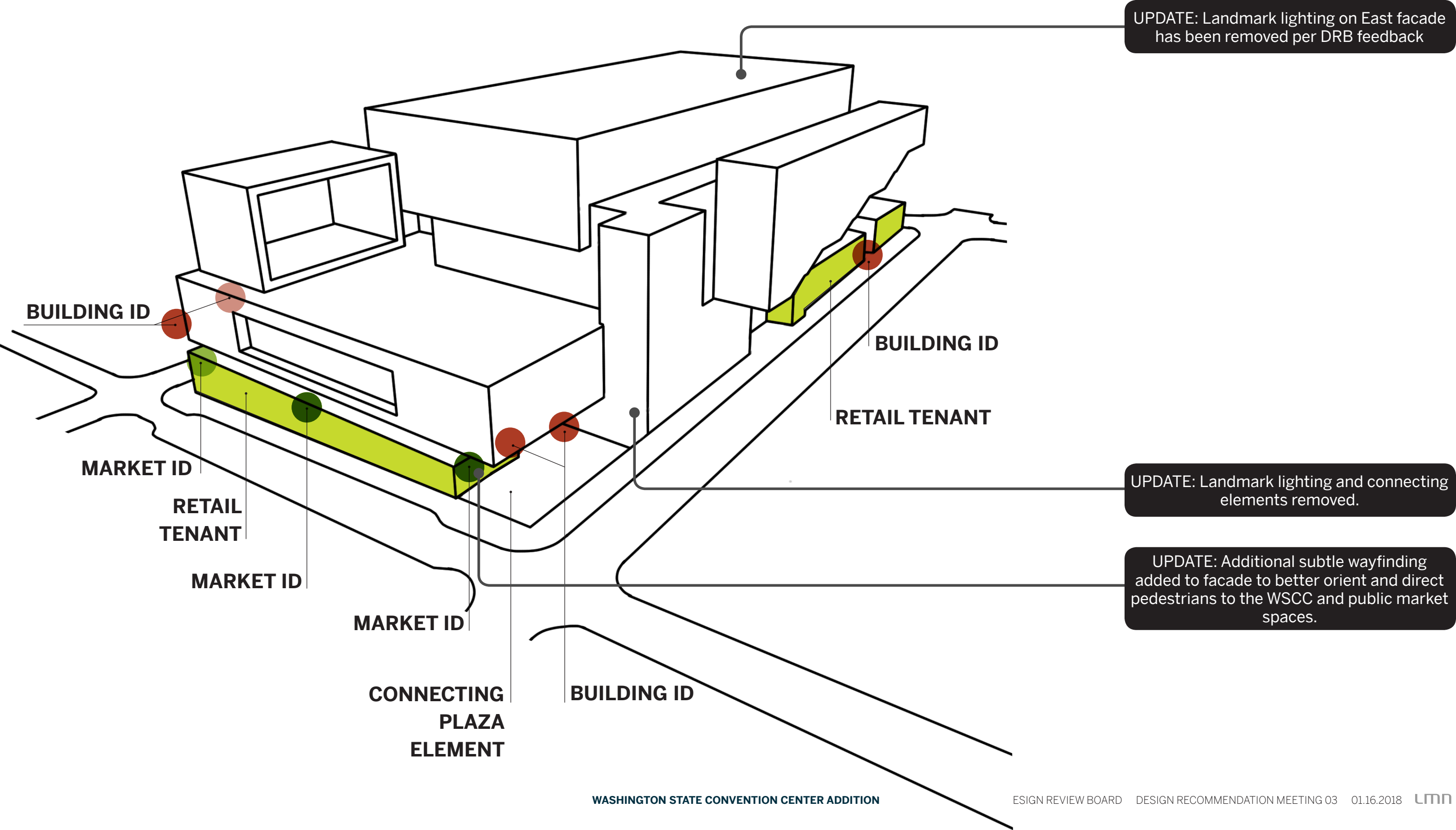
3 DESIGN REVIEW & UPDATES - GRAPHICS & SIGNAGE

DRB # 2 - 07-19-2016 - PRESENTATION SLIDE 42

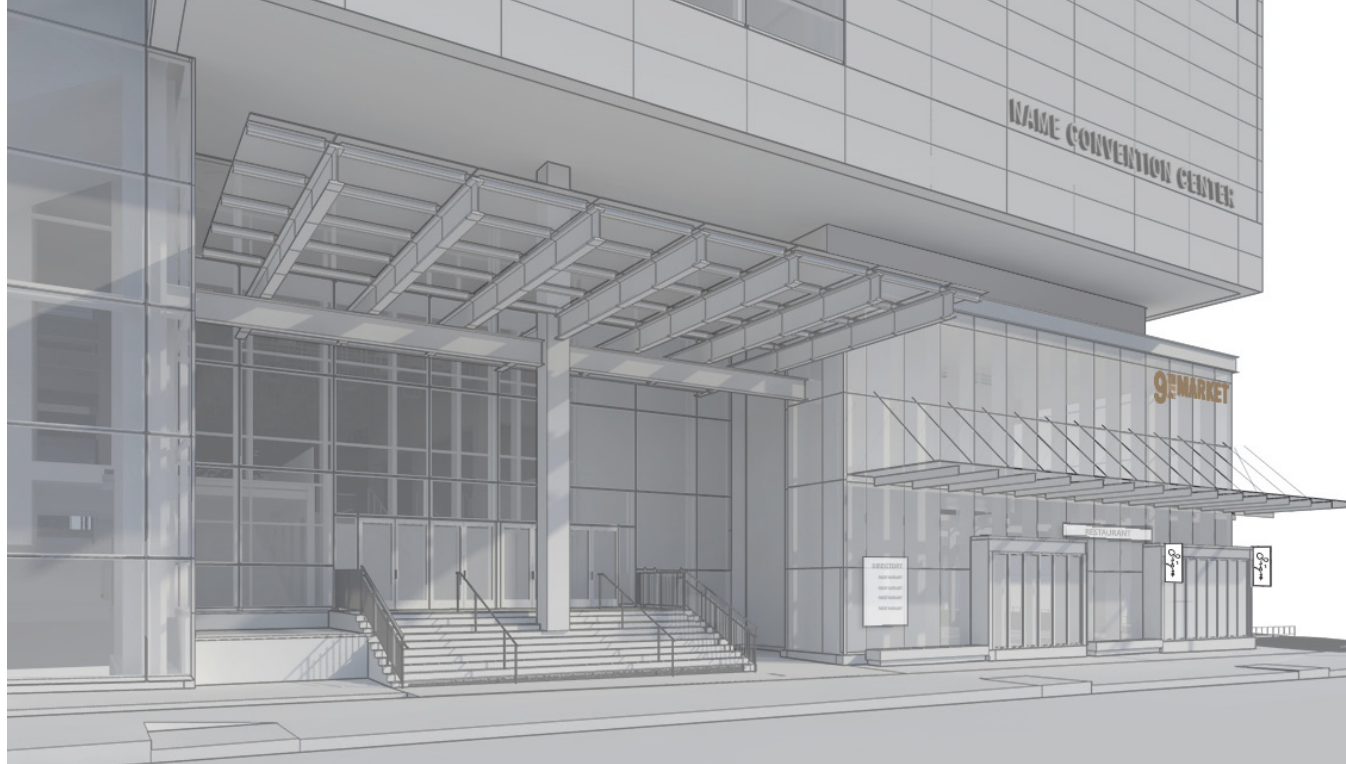


3 DESIGN REVIEW & UPDATES - GRAPHICS & SIGNAGE

DRB #3



3 DESIGN REVIEW & UPDATES - GRAPHICS & SIGNAGE



**BUILDING IDENTIFICATION
NORTH ENTRY - BUILDING NAME**



**BUILDING IDENTIFICATION
EAST ENTRY - BUILDING NAME**



BUILDING IDENTIFICATION



**BUILDING IDENTIFICATION:
DIMENSION, NEUTRAL COLOR AND
MATERIALITY. METALLIC TEXTURES GIVE
A CLEAN, FLEXIBLE PERSONALITY TO THE
CONVENTION FACILITY.**

3 DESIGN REVIEW & UPDATES - GRAPHICS & SIGNAGE



MARKET
VIEW TOWARDS MARKET ENTRY ON 9TH AVENUE



RETAIL
VIEW TOWARDS MARKET TENANTS FROM OLIVE WAY



MARKET
VIEW TOWARDS MARKET FROM PINE STREET



MARKET & RETAIL:
DIMENSION, MATERIALITY AND TEXTURE
HIGHLIGHT THE WARMTH OF RETAIL
SPACES AND THE PERSONALITY OF
INDIVIDUAL TENANTS.



UNIQUE TENANT SIGNS AT INDIVIDUAL
RETAIL SPACES

3 DESIGN REVIEW & UPDATES - LIGHTING CONCEPT

DRB # 2 - 07-19-2016 - PRESENTATION BOOKLET PAGE 93



SITE LIGHTING PLAN



INTERIOR SPACES ILLUMINATE EXTERIOR



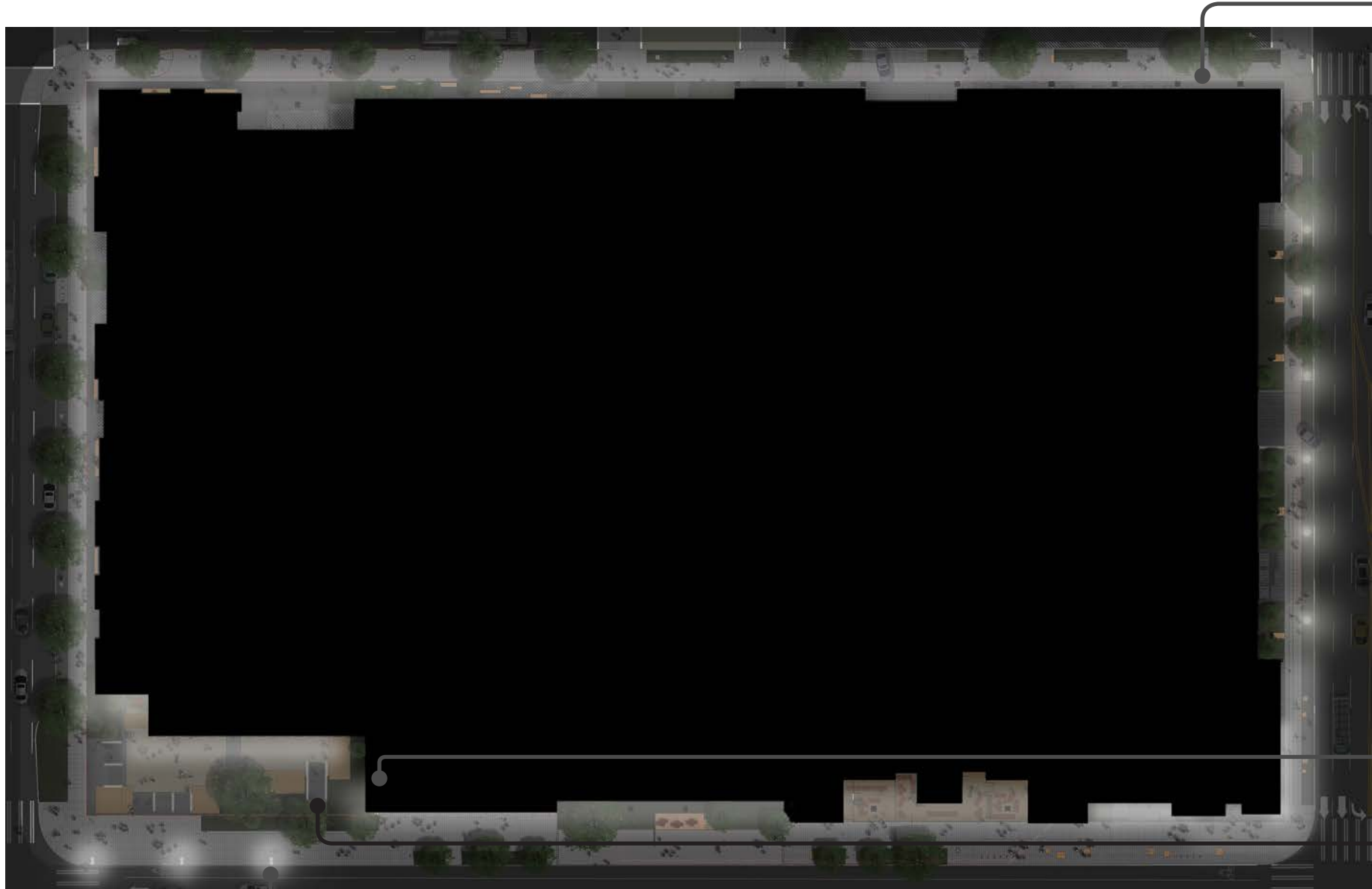
BALLROOM & THEATRICAL LIGHTING



DYNAMIC PREFUNCTION LOBBIES

3 DESIGN REVIEW & UPDATES - LIGHTING CONCEPT

DRB #3



SITE LIGHTING PLAN



BOREN LIGHT
POLE



EVENT TERRACE
LIGHTING

UPDATE: Additional downlighting integrated into all pedestrian canopies surrounding the building

UPDATE: Landmark lighting and connecting elements removed.

UPDATE: Increased quantity of integrated rail lighting at entry plaza

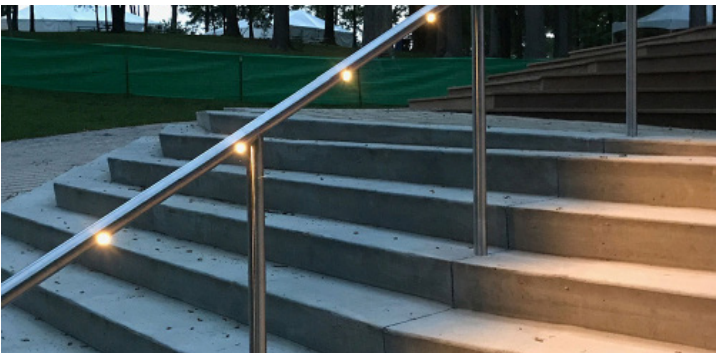
UPDATE: City standard lighting poles introduced to match existing globe lights along Pine Street.



INTEGRATED CANOPY LIGHTING



INTEGRATED BENCH LIGHTING



INTEGRATED RAIL LIGHTING

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4 PROPOSED DEPARTURES - DEPARTURE #1 - Facade Modulation - 23.49.058 B

DRB#2 - 07-19-2016 - BOOK PG 104
RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G122

This departure was approved by the Design Review Board on July 19, 2016.

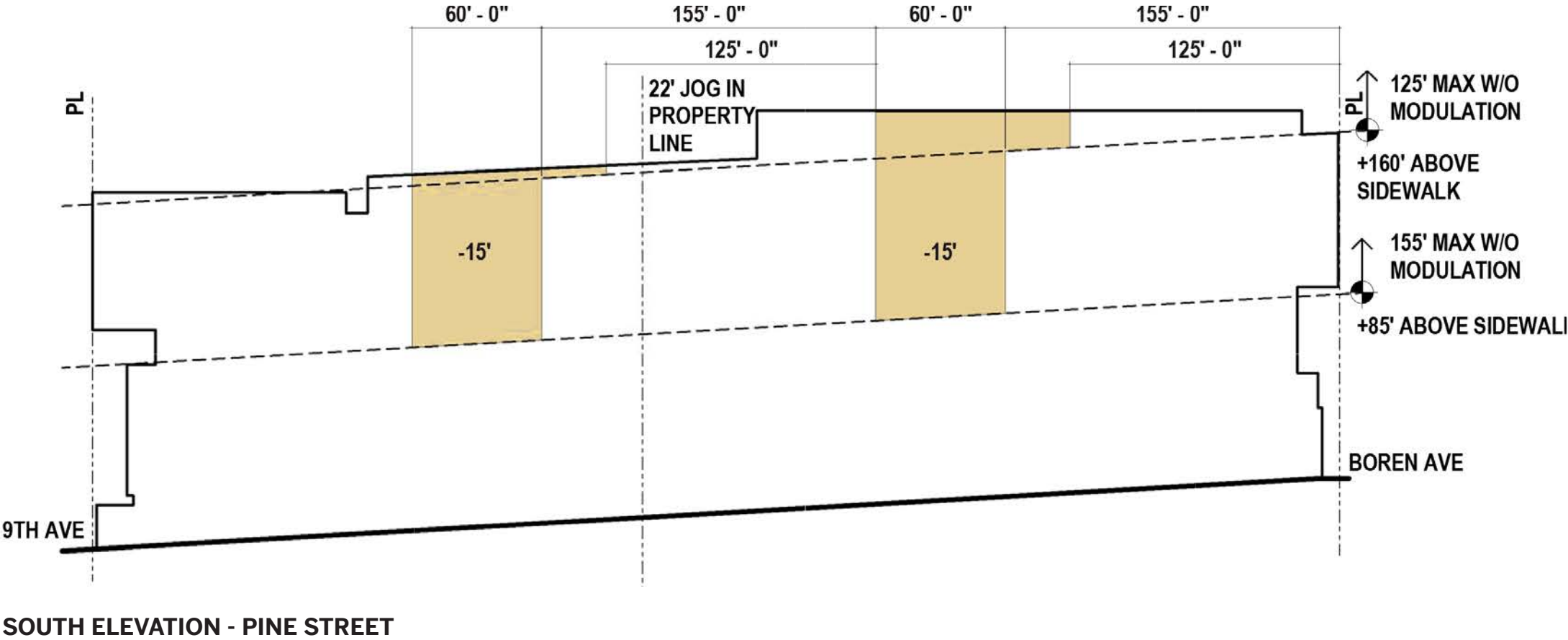
Red text indicates change in departure.

ITEM #	DEVELOPMENT STANDARD	REQUIREMENT	MODIFICATION REQUESTED	RATIONALE
1	23.49.058 B Facade Modulation	<p>Facade modulation is required above a height of 85 feet above the sidewalk of any portion of a structure located within 15 feet of a street lot line. The maximum length of unmodulated facade within 15 feet of a street lot line is 155 feet at a height between 86–160 feet, 125 feet at a height between 161-240 feet.</p> <p>Any portion of a facade exceeding the maximum length of facade prescribed above (listed in 23.49.058 Table A) shall be set back a minimum of 15 feet from the street lot line for a minimum distance of 60 feet before any other portion may be within 15 feet of the street lot line.</p>	Relief from upper level modulation, allowing facade length greater than 155 feet between 37-160 feet 75-160 feet and greater than 125 feet between 67-87 feet 100-130 feet .	<p>The design proposes a greater variety of modulation than that prescribed by the code. This variation of depth and shape extends across a significant surface area of the elevation shown, providing greater visual interest and a More active facade that meets and exceeds the intent of the facade modulation requirements.</p> <p>This departure allows the project to better meet the intent of the design guidelines (described below).</p>

RELEVANT DESIGN GUIDELINES - DEPARTURE #1	
<p>A1.1 Response to context B. A site having dramatic topography or contrasting edge conditions F. Views of the site from other parts of the city or region Adjacent to 1-5, with a significant change in grade from east to west, dramatic topography and unique edge conditions are key elements of the site.</p> <p>The major massing elements take on unique characters that responds to their context - through the use of topography, aperture, and clearly defined edges. The legibility of forms breaks down the scale of the block into distinctive parts which provides personality to the different edges of the site.</p> <p>B4.1 Massing A. Setbacks, projections, and open space B. Relative sizes and shapes of distinct building volumes C. Roof height and forms</p> <p>Distinct setbacks that clearly define building volumes and exterior spaces better relate to scale of surrounding buildings. The roof height is lower than allowed by code, while the collection of volumes provide variety of roof forms throughout the site.</p> <p>B4.2 Coherent design D. Modulation and articulation I. Building base and top</p>	<p>The articulated volumes are united horizontally with finer grain structural module, and vertically with clear layering of forms.</p> <p>C2.1 Modulation of facades A. Fenestration pattern B. Exterior finish materials C. Other architectural elements D. Light fixtures and landscaping elements</p> <p>The larger forms are further articulated with a range of solid and glazed facade systems, materials, and textures. Exposed structure, canopies, and vertical planting create rhythm and scale to the facade</p>

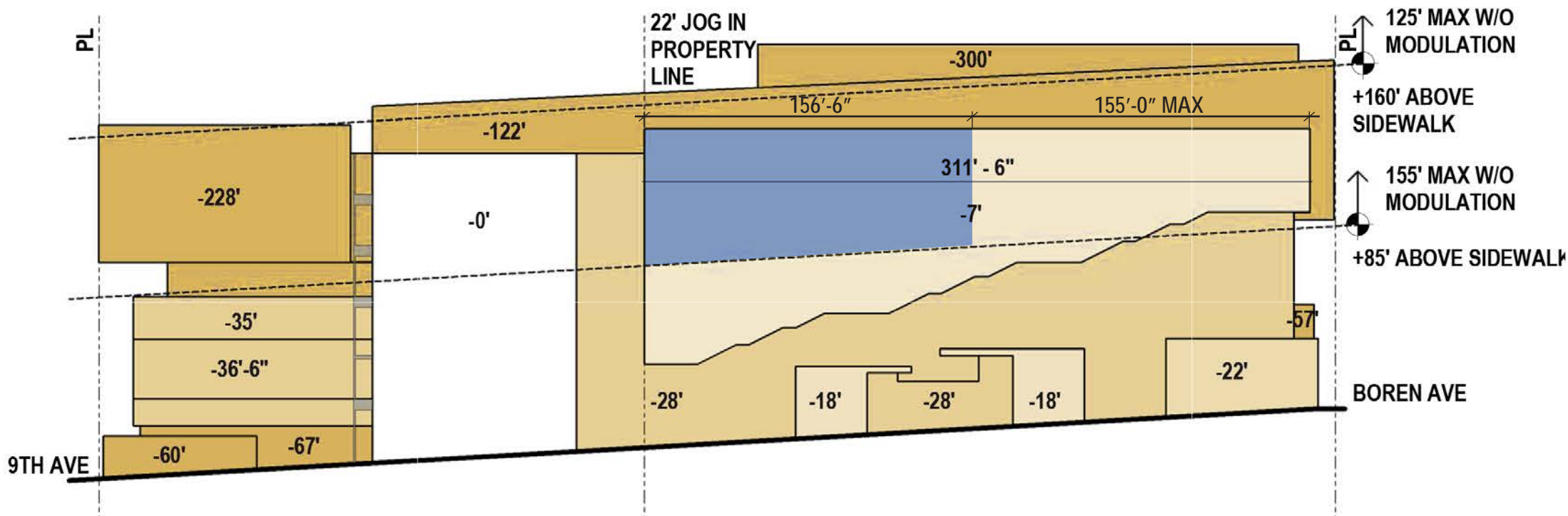
4 PROPOSED DEPARTURES - DEPARTURE #1 - Facade Modulation - 23.49.058 B

CODE BASELINE - DRB#2 - 07-19-2016 - PRESENTATION SLIDE 59



4 PROPOSED DEPARTURES - DEPARTURE #1 - Facade Modulation - 23.49.058 B

DRB#2 - 07-19-2016 - PRESENTATION SLIDE 59



This departure was approved by the Design Review Board on July 19, 2016.

Red text indicates change in departure.

Length of departure request along Pine Street reduced. Facade modulation is consistent with design intent as presented at DRB 2. Adjustments have been made to better articulate building form and create engaging facades.

REQUIREMENT:

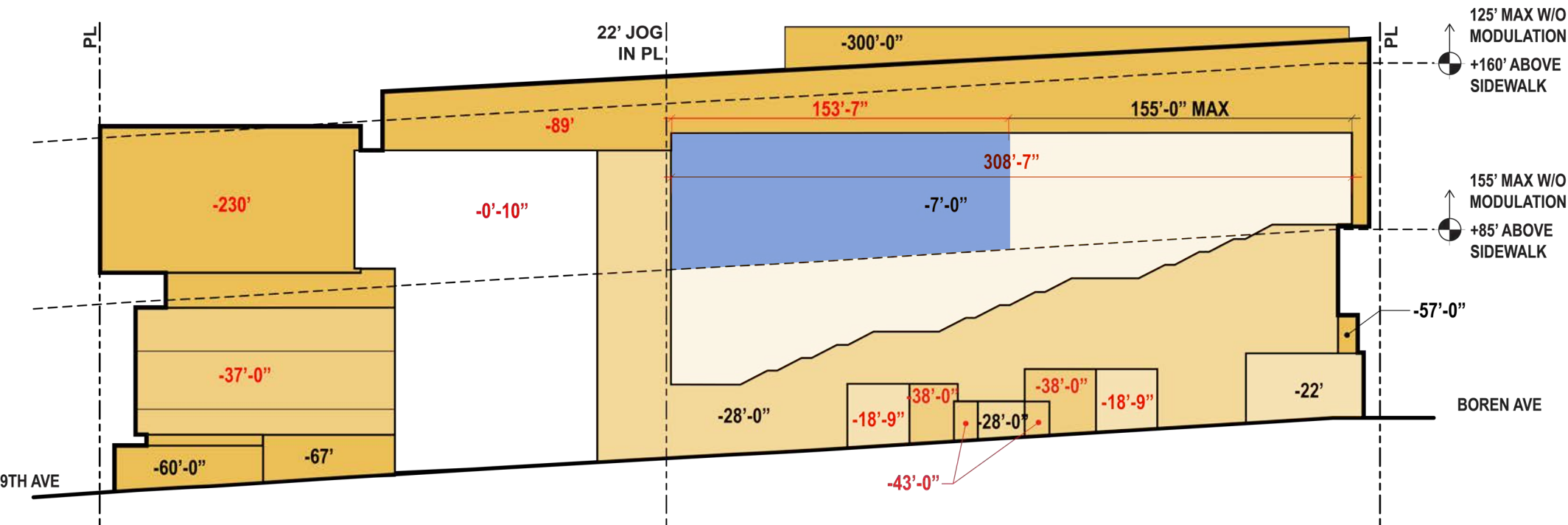
Facade modulation is required above a height of 85 feet above the sidewalk of any portion of a structure located within 15 feet of a street lot line. The maximum length of unmodulated facade within 15 feet of a street lot line is 155 feet at a height between 86–160 feet, 125 feet at a height between 161–240 feet. Any portion of a facade exceeding the maximum length of facade prescribed above (listed in 23.49.058 Table A) shall be set back a minimum of 15 feet from the street lot line for a minimum distance of 60 feet before any other portion may be within 15 feet of the street lot line.

MODIFICATION REQUESTED:

Relief from upper level modulation, allowing facade length greater than 155 feet between 37-160 feet **75-160 feet** and greater than 125 feet between 67-87 feet **100-130 feet**.

- AREAS OF DEPARTURE FROM DEVELOPMENT STANDARD
- CHANGE IN DEPARTURE REQUEST

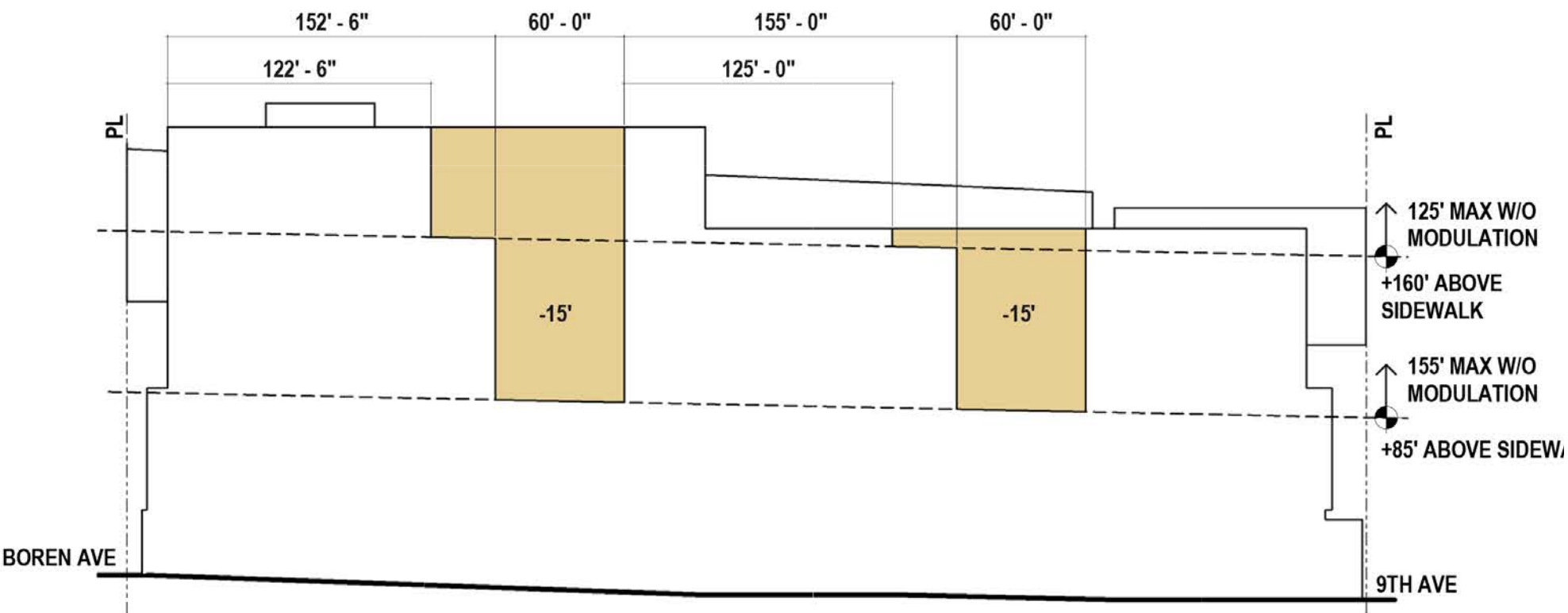
DRB #3 RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G122



SOUTH ELEVATION - PINE STREET

4 PROPOSED DEPARTURES - DEPARTURE #1 - Facade Modulation - 23.49.058 B

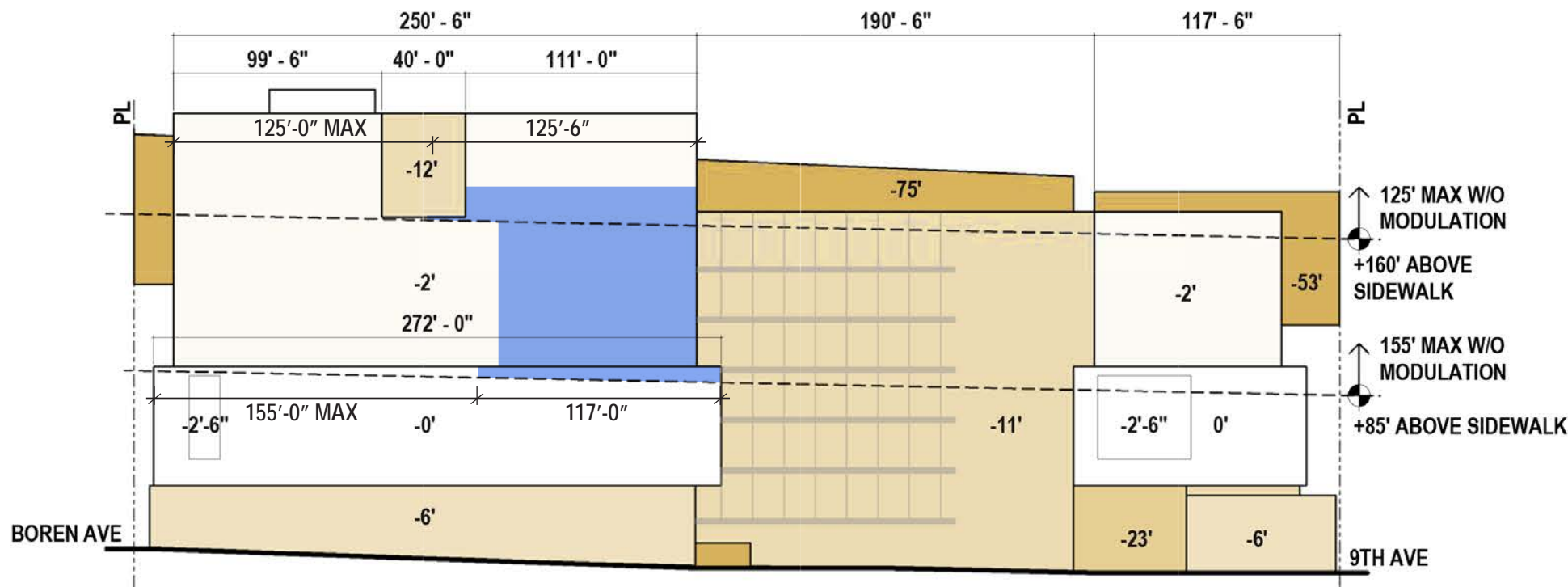
CODE BASELINE - DRB#2 - 07-19-2016 - PRESENTATION SLIDE 60



NORTH ELEVATION - OLIVE WAY

4 PROPOSED DEPARTURES - DEPARTURE #1 - Facade Modulation - 23.49.058 B

DRB#2 - 07-19-2016 - PRESENTATION SLIDE 60



This departure was approved by the Design Review Board on July 19, 2016.

Red text indicates change in departure.

Length of departure request along Olive Way varies slightly, with both minor reductions and minor increases. Facade modulation is consistent with design intent as presented at DRB 2. Adjustments have been made to better articulate building form and create engaging facades.

REQUIREMENT:

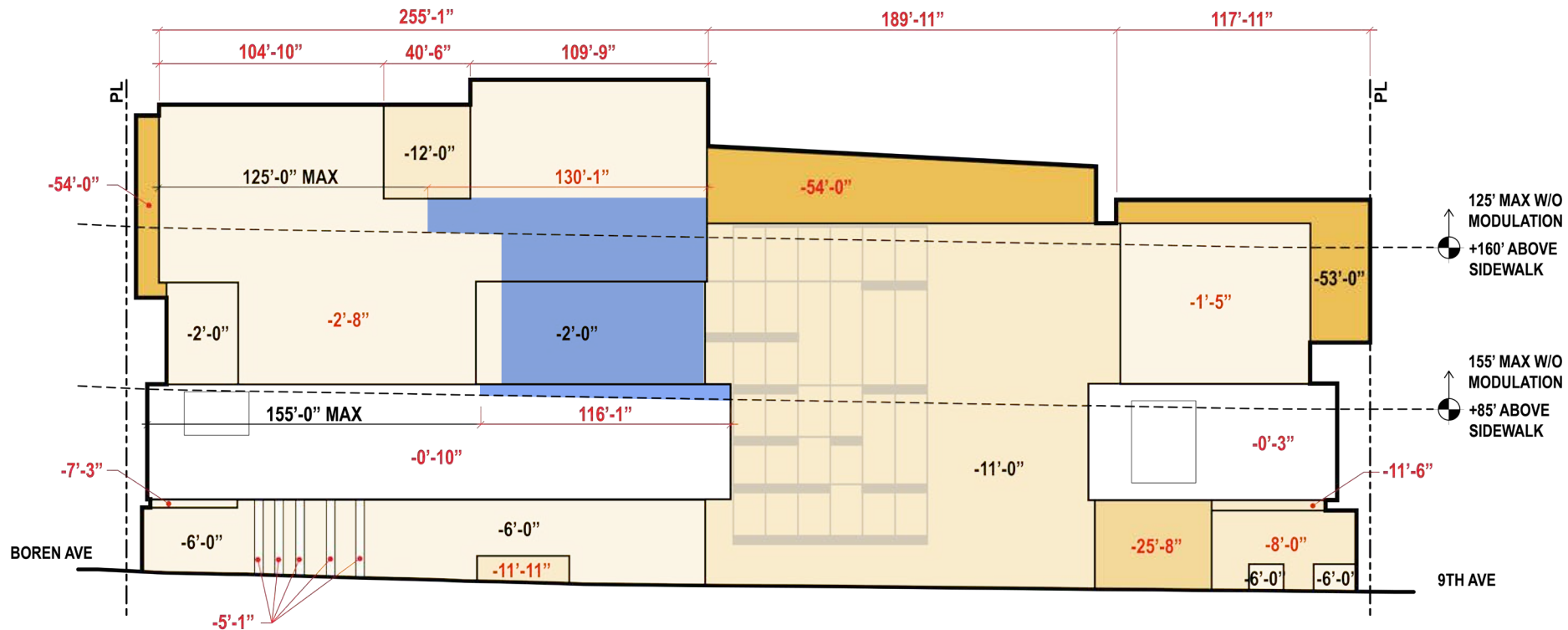
Facade modulation is required above a height of 85 feet above the sidewalk of any portion of a structure located within 15 feet of a street lot line. The maximum length of unmodulated facade within 15 feet of a street lot line is 155 feet at a height between 86–160 feet, 125 feet at a height between 161–240 feet. Any portion of a facade exceeding the maximum length of facade prescribed above (listed in 23.49.058 Table A) shall be set back a minimum of 15 feet from the street lot line for a minimum distance of 60 feet before any other portion may be within 15 feet of the street lot line.

MODIFICATION REQUESTED:

Relief from upper level modulation, allowing facade length greater than 155 feet between 37-160 feet **75-160 feet** and greater than 125 feet between 67-87 feet **100-130 feet**.

- AREAS OF DEPARTURE FROM DEVELOPMENT STANDARD
- CHANGE IN DEPARTURE REQUEST

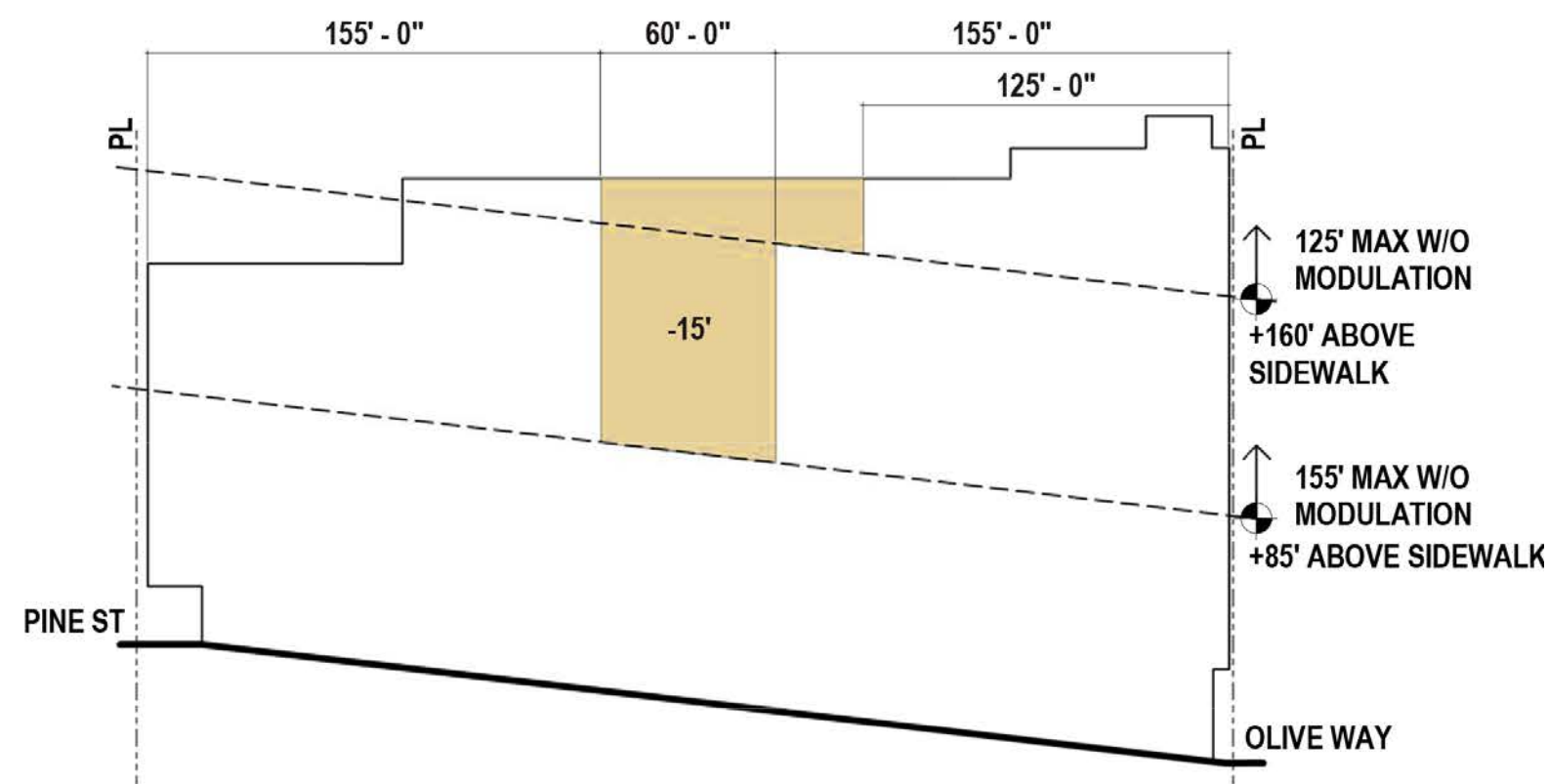
DRB #3 RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G122



NORTH ELEVATION - OLIVE WAY

4 PROPOSED DEPARTURES - DEPARTURE #1 - Facade Modulation - 23.49.058 B

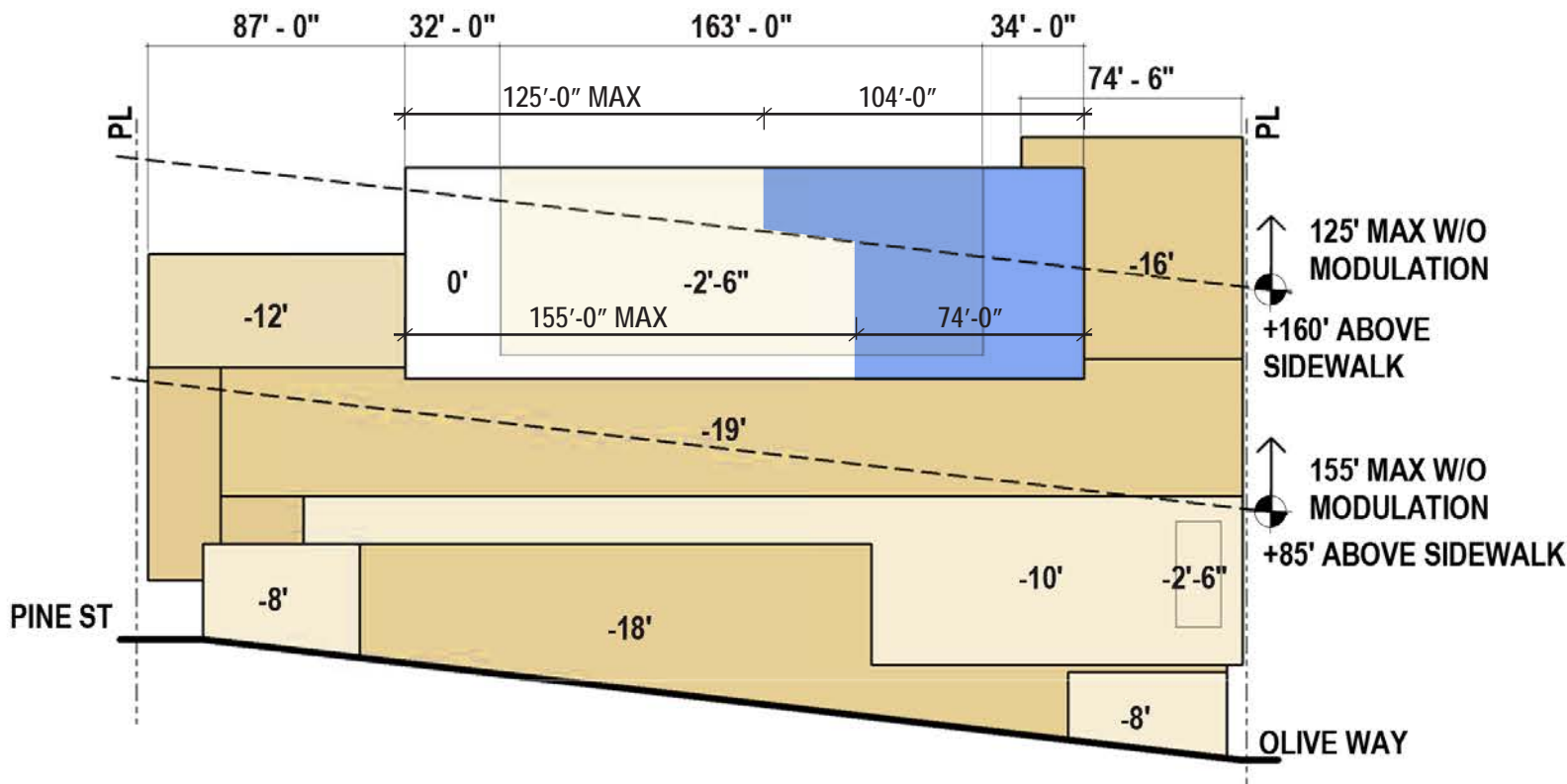
CODE BASELINE - DRB#2 - 07-19-2016 - PRESENTATION SLIDE 61



EAST ELEVATION - BOREN AVENUE

4 PROPOSED DEPARTURES - DEPARTURE #1 - Facade Modulation - 23.49.058 B

DRB#2 - 07-19-2016 - PRESENTATION SLIDE 61



This departure was approved by the Design Review Board on July 19, 2016.

Red text indicates change in departure.

Length of departure request along Boren Avenue varies slightly, with both minor reductions and minor increases. Facade modulation is consistent with design intent as presented at DRB 2. Adjustments have been made to better articulate building form and create engaging facades.

REQUIREMENT:

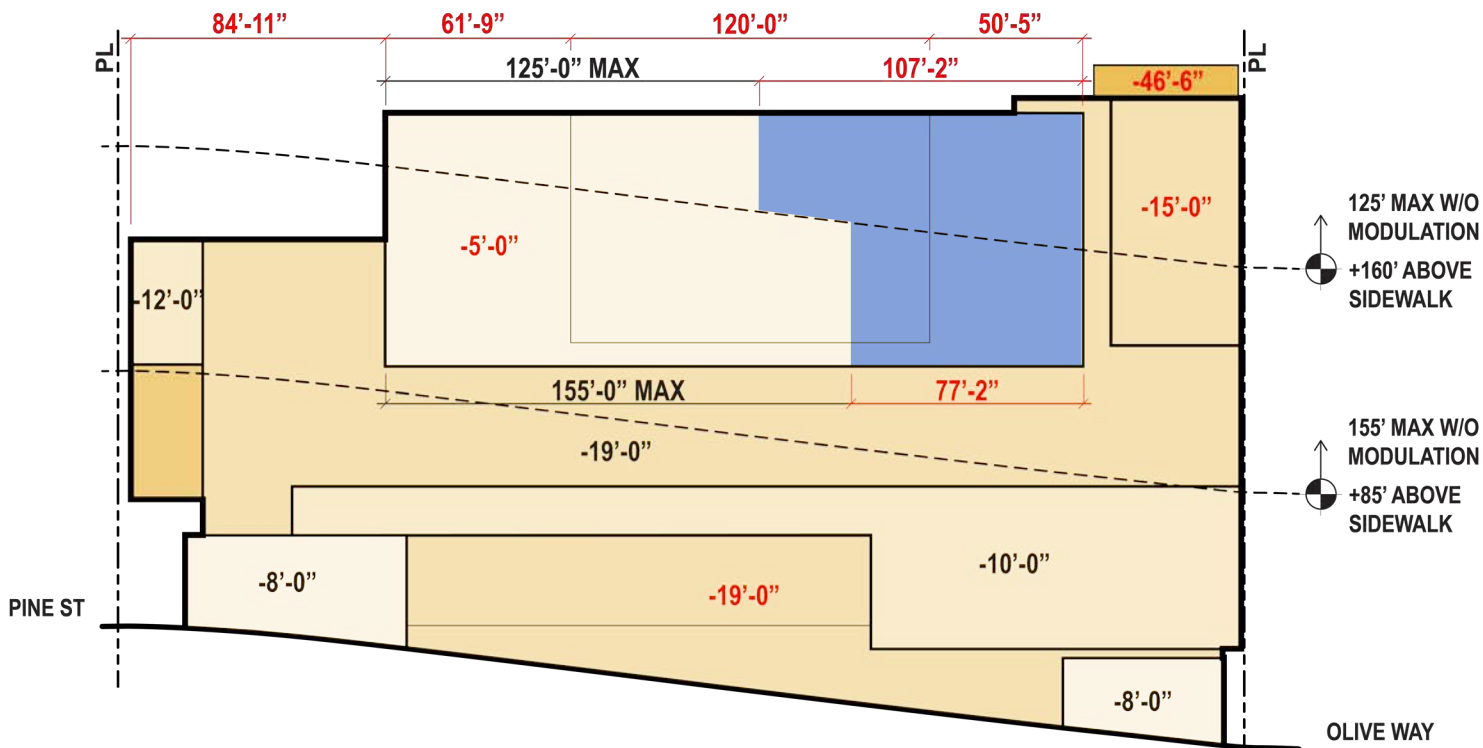
Facade modulation is required above a height of 85 feet above the sidewalk of any portion of a structure located within 15 feet of a street lot line. The maximum length of unmodulated facade within 15 feet of a street lot line is 155 feet at a height between 86–160 feet, 125 feet at a height between 161–240 feet. Any portion of a facade exceeding the maximum length of facade prescribed above (listed in 23.49.058 Table A) shall be set back a minimum of 15 feet from the street lot line for a minimum distance of 60 feet before any other portion may be within 15 feet of the street lot line.

MODIFICATION REQUESTED:

Relief from upper level modulation, allowing facade length greater than 155 feet between 37-160 feet **75-160 feet** and greater than 125 feet between 67-87 feet **100-130 feet**.

- AREAS OF DEPARTURE FROM DEVELOPMENT STANDARD
- CHANGE IN DEPARTURE REQUEST

DRB #3 RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G122



EAST ELEVATION - BOREN AVENUE

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4 PROPOSED DEPARTURES - DEPARTURE #2 - Green Street Upper Level Setback - 23.49.058 G2

DRB#2 - 07-19-2016 - BOOK PG 108
RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G123

This departure was approved by the Design Review Board on July 19, 2016.

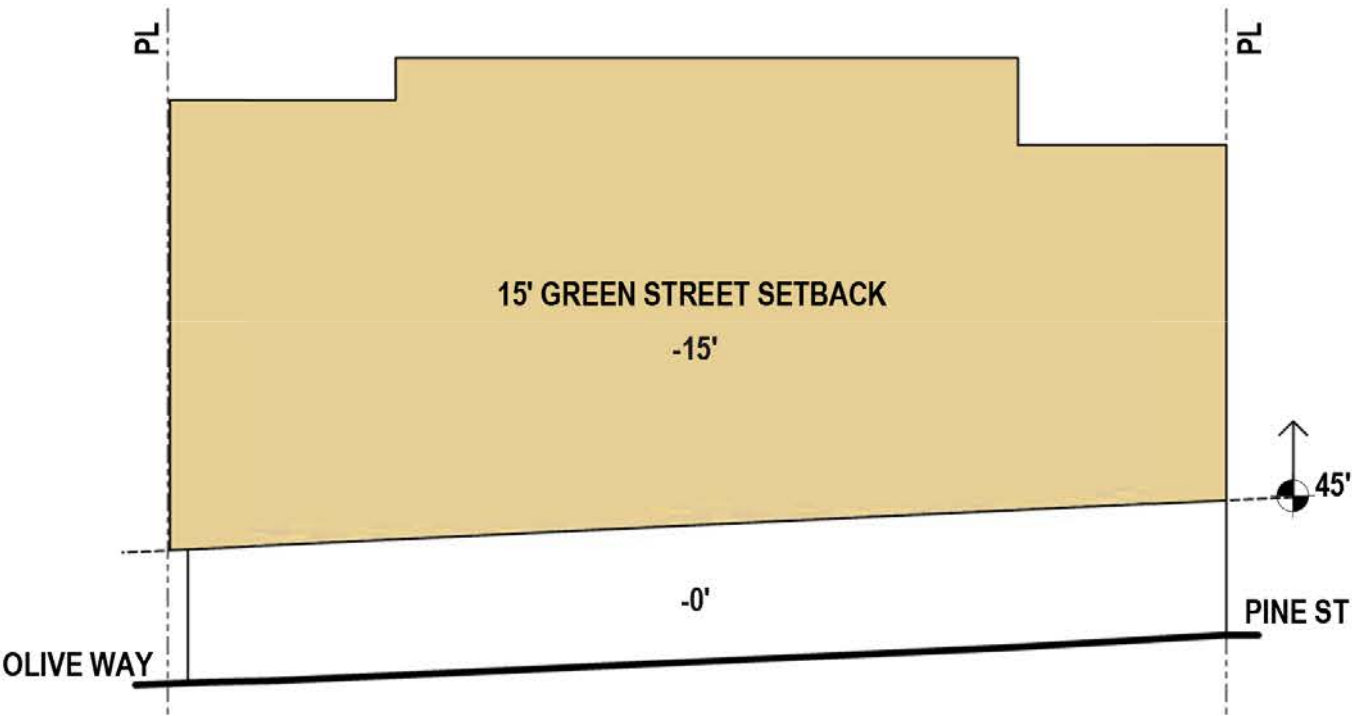
Red text indicates change in departure.

ITEM #	DEVELOPMENT STANDARD	REQUIREMENT	MODIFICATION REQUESTED	RATIONALE
2	23.49.058 G2 Green Street Upper Level Setback	When a lot in a DMC or DOC2 zone is located on a designated green street, a continuous upper-level setback of fifteen (15) feet shall be provided on the street frontage abutting the green street at a height of forty-five (45) feet.	Relief from upper level Green Street setback, allowing a variety of setbacks above 45 feet.	<p>The proposed design provides additional modulation which exceeds the green street upper level setbacks with greater visual interest in both elevation and massing than prescribed by the code. The resulting activation of the building better supports the goals of the green street designation. The upper level meeting room block and the flex hall volume extend into the setback in order to align their north facade with the paramount facade, thereby framing the open space between.</p> <p>This departure allows the project to better meet the intent of the design guidelines (described to the right).</p>

RELEVANT DESIGN GUIDELINES - DEPARTURE #2	
<p>A1.1 Response to context C. Patterns of urban form, such as nearby buildings that have employed distinctive and effective massing compositions</p> <p>B1.1 Adjacent features and networks B. An adjacent landmark or noteworthy building D. Neighboring buildings that have employed distinctive and effective massing compositions</p> <p>B3.2 Features to complement A. Massing and setbacks B. Scale and proportion</p> <p>B4.1 Massing A. Setbacks, projections, and open space B. Relative sizes and shapes of distinct building volumes C. Roof height and forms</p> <p>B4.2 Coherent design D. Modulation and articulation I. Building base and top</p> <p>C2.1 Modulation of facades A. Fenestration pattern B. Exterior finish materials C. Other architectural elements</p>	<p>D2.1 Landscape enhancements D. Distinctively landscape open areas created by building modulation</p> <p>The urban concept considers the multi-block scale of the site, breaking it down to reinforce the familiar grain of the urban fabric. The massing responds to its innate programmatic needs and its location at the intersection of diverse Seattle neighborhoods. Retail is located to activate intersections and define zones. Pedestrian connections echo the urban form of streets and alleys. Planting is used to define edges and characterize zones. The southwest corner of the site marks the primary entry plaza of the convention center. The massing at this intersection and along 9th Avenue is broken down to allow pedestrians and users to enjoy views of Pike Place Market, the waterfront, the Camlin Building and the Paramount Theater. The building will occupy a smaller envelope than possible by code, creating a transition on the edge of downtown to the smaller scale neighborhoods to the east. Terraces, lobbies and retail provide opportunities to compose the building mass appropriate to its context. The massing is broken down into clearly articulated vertical layers of forms and united with a finer grain structural module, which is expressed throughout the entire building.</p> <p>The design incorporates bold civic scale gestures and fine grain articulation at the pedestrian edge to further modulate the facade. Additional layering and depth is expressed through a composition of pedestrian circulation, varying facade systems and materials, retail and landscape zones.</p> <p>A vegetated roof processes stormwater and provides an urban habitat. Non-vegetated roof surfaces allow rainwater harvesting and solar energy collection. Upper level terraces are available to rent by public. Planting is used throughout the site to define edges and characterize zones. Street trees fill the edges of the site with limited planting on Pine Street to maintain view to water.</p>

4 PROPOSED DEPARTURES - DEPARTURE #2 - Green Street Upper Level Setback - 23.49.058 G2

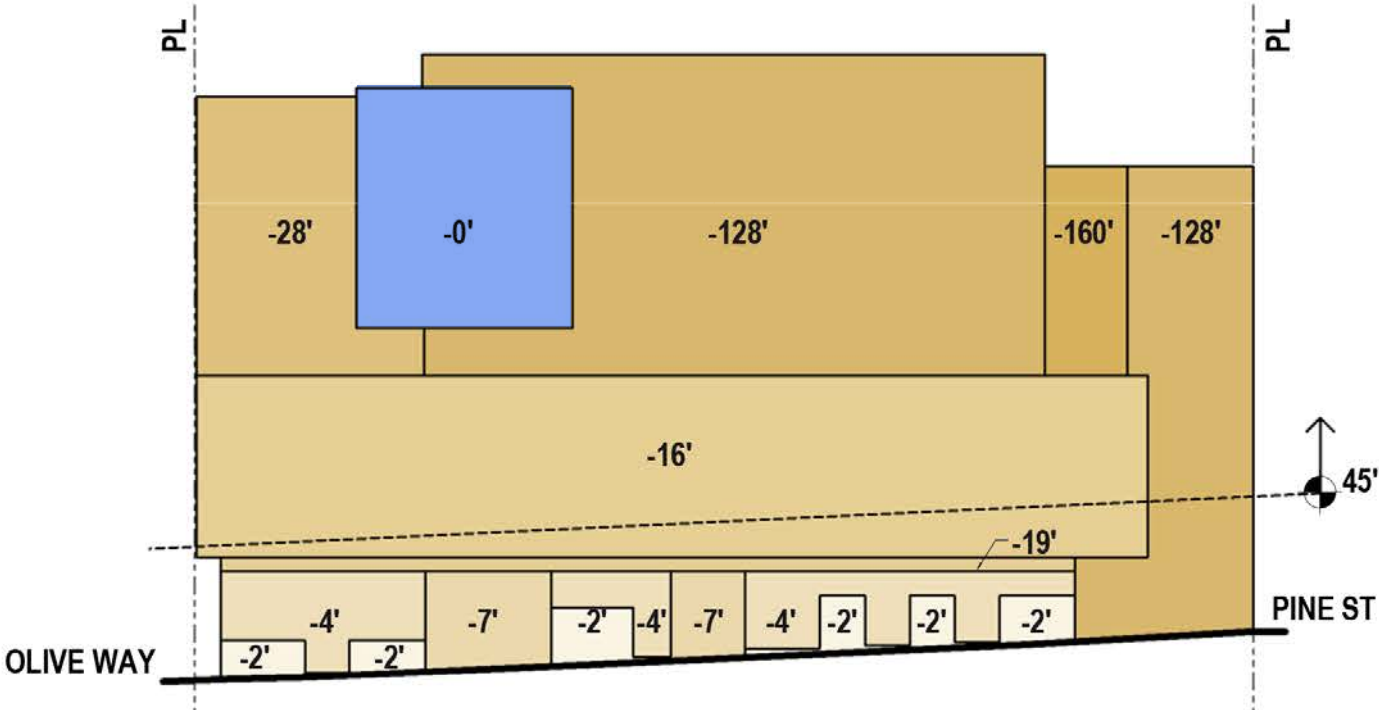
CODE BASELINE - DRB # 2 - 07-19-2016 - PRESENTATION SLIDE 62



WEST ELEVATION - 9TH AVENUE

4 PROPOSED DEPARTURES - DEPARTURE #2 - Green Street Upper Level Setback - 23.49.058 G2

DRB#2 - 07-19-2016 - PRESENTATION SLIDE 62



This departure was approved by the Design Review Board on July 19, 2016.

Red text indicates change in departure.

Setbacks are consistent with design intent of those presented at DRB 2. Minor adjustment to departure area has been made as design has been refined to better articulate clarify forms.

REQUIREMENT:

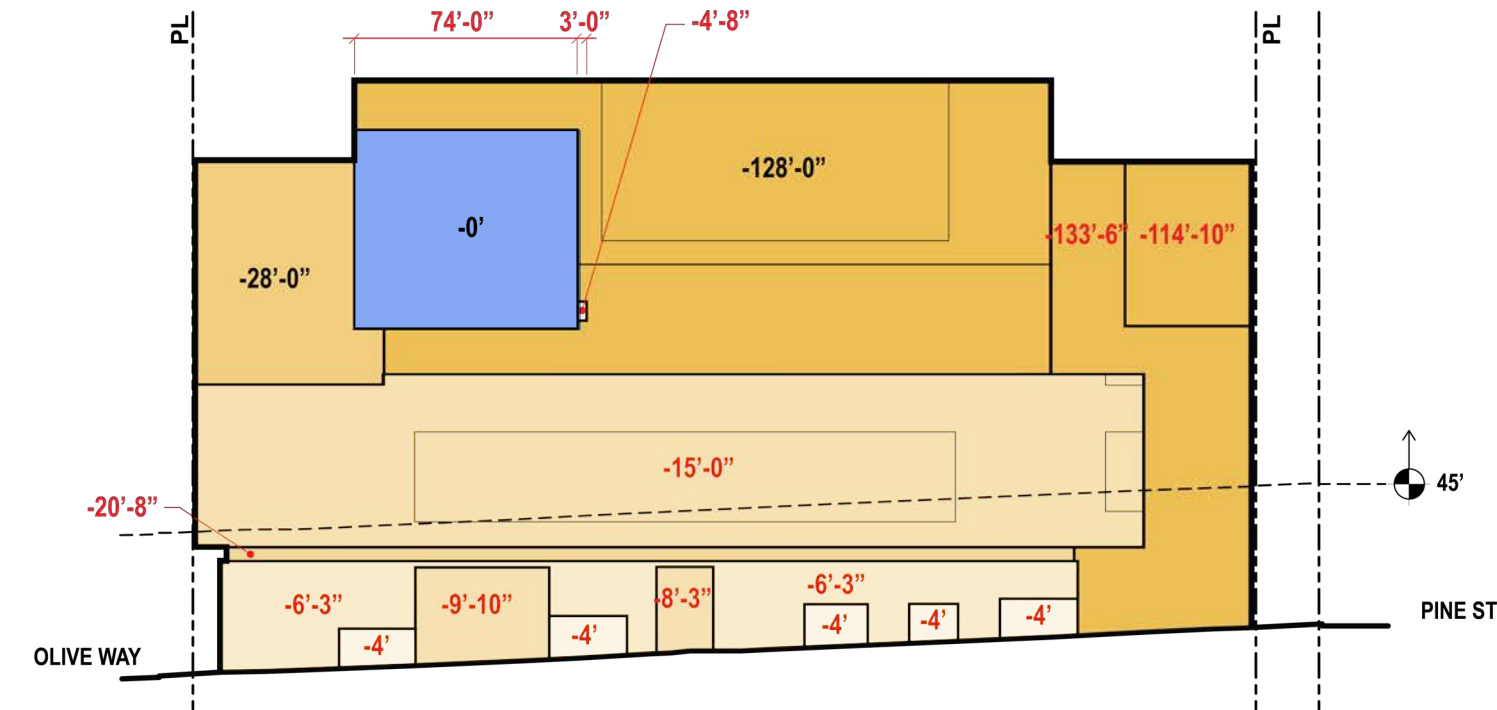
When a lot in a DMC or DOC2 zone is located on a designated green street, a continuous upper-level setback of fifteen (15) feet shall be provided on the street frontage abutting the green street at a height of forty-five (45) feet.

MODIFICATION REQUESTED:

Relief from upper level green street setback, allowing a variety of setbacks above 45 feet.

- AREAS OF DEPARTURE FROM DEVELOPMENT STANDARD
- CHANGE IN DEPARTURE REQUEST

DRB #3 RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G123



WEST ELEVATION - 9TH AVENUE

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4 PROPOSED DEPARTURES - DEPARTURE #3 - Green Street Setback - 23.49.056.F.4

DRB#2 - 07-19-2016 - BOOK PG 109

RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G123; L103

This departure was approved by the Design Review Board on July 19, 2016.

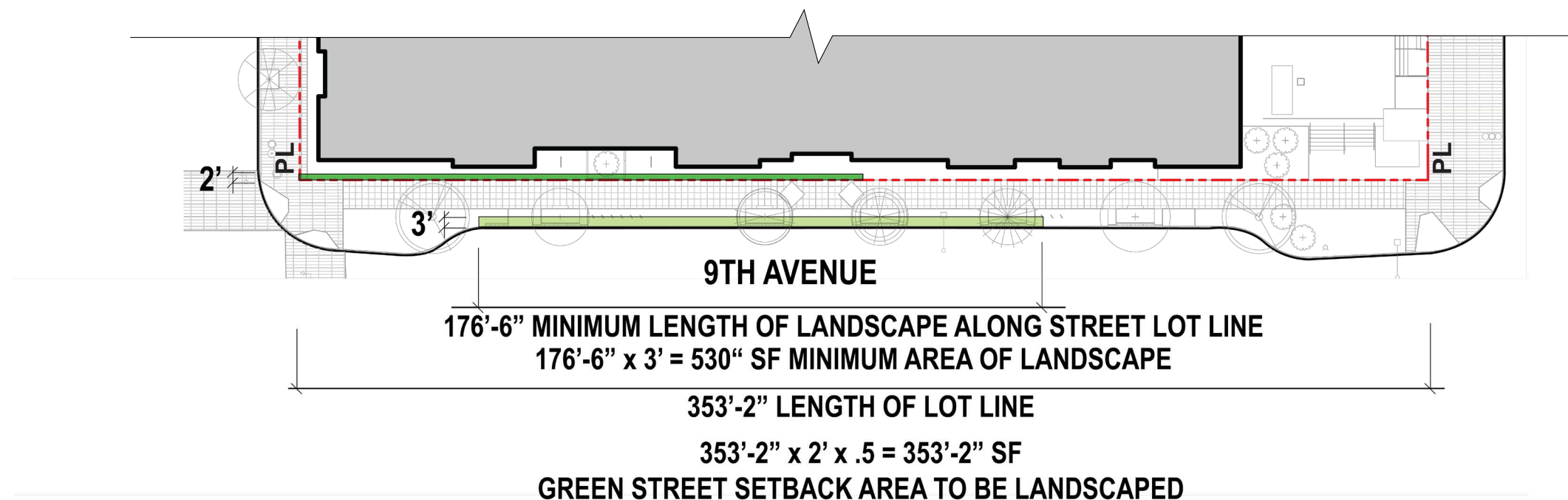
Red text indicates change in departure.

ITEM #	DEVELOPMENT STANDARD	REQUIREMENT	MODIFICATION REQUESTED	RATIONALE
3	23.49.056.F.4 Green Street Setback	<p>Terry and 9th Avenue's Green Street setbacks, a 2 foot wide setback from the street lot line is required.</p> <p>50% of the setback area shall be landscaped.</p> <p>Total area of setback = lot line x 2 feet = 353'-2" X 2' = 706.3 SF 50% of setback area (706.3 SF) = 353.2 SF</p> <p>See sheet L103 for setback and landscaping requirements calculations.</p> <p>See sheet L103 for proposed redistribution of the 50% of setback landscape area along 9th Avenue.</p>	Relief from green street setback requirements along 9th Avenue. Landscape to be redistributed along 9th to permit visual and physical access to retail	<p>9th Avenue is an active frontage for the plaza and retail marketplace. The design maximizes the pedestrian connection into these spaces while providing landscape area adjoining the curb in excess of the standard. 986 additional SF of landscape is provided adjoining the curb.</p> <p>When averaged for frontage, the intent of the setback is met and exceeded. The quantity of planting is provided, but distributed in order to provide larger, denser areas of planting toward the street. Greater visual transparency and access to the retail spaces is provided in addition to egress along 9th Avenue.</p> <p>This departure allows the project to better meet the intent of the design guidelines (described to the right).</p>

RELEVANT DESIGN GUIDELINES - DEPARTURE #3	
<p>A1.1 Response to context C. Patterns of urban form, such as nearby buildings that have employed distinctive and effective massing compositions</p> <p>B1.1 Adjacent features and networks B. An adjacent landmark or noteworthy building D. Neighboring buildings that have employed distinctive and effective massing compositions</p> <p>B3.2 Features to complement A. Massing and setbacks B. Scale and proportion</p> <p>B4.1 Massing A. Setbacks, projections, and open space B. Relative sizes and shapes of distinct building volumes C. Roof height and forms</p> <p>B4.2 Coherent design D. Modulation and articulation I. Building base and top</p> <p>C2.1 Modulation of facades A. Fenestration pattern B. Exterior finish materials C. Other architectural elements</p>	<p>D2.1 Landscape enhancements D. Distinctively landscape open areas created by building modulation</p> <p>The urban concept considers the multi-block scale of the site, breaking it down to reinforce the familiar grain of the urban fabric. The massing responds to its innate programmatic needs and its location at the intersection of diverse Seattle neighborhoods. Retail is located to activate intersections and define zones. Pedestrian connections echo the urban form of streets and alleys. Planting is used to define edges and characterize zones. The southwest corner of the site marks the primary entry plaza of the convention center. The massing at this intersection and along 9th Avenue is broken down to allow pedestrians and users to enjoy views of Pike Place Market, the waterfront, the Camlin Building and the Paramount Theater. The building will occupy a smaller envelope than possible by code, creating a transition on the edge of downtown to the smaller scale neighborhoods to the east. Terraces, lobbies and retail provide opportunities to compose the building mass appropriate to its context. The massing is broken down into clearly articulated vertical layers of forms and united with a finer grain structural module, which is expressed throughout the entire building.</p> <p>The design incorporates bold civic scale gestures and fine grain articulation at the pedestrian edge to further modulate the facade. Additional layering and depth is expressed through a composition of pedestrian circulation, varying facade systems and materials, retail and landscape zones.</p> <p>A vegetated roof processes stormwater and provides an urban habitat. Non-vegetated roof surfaces allow rainwater harvesting and solar energy collection. Upper level terraces are available to rent by public. Planting is used throughout the site to define edges and characterize zones. Street trees fill the edges of the site with limited planting on Pine Street to maintain view to water.</p>

4 PROPOSED DEPARTURES - DEPARTURE #3 - Green Street Setback - 23.49.056.F.4

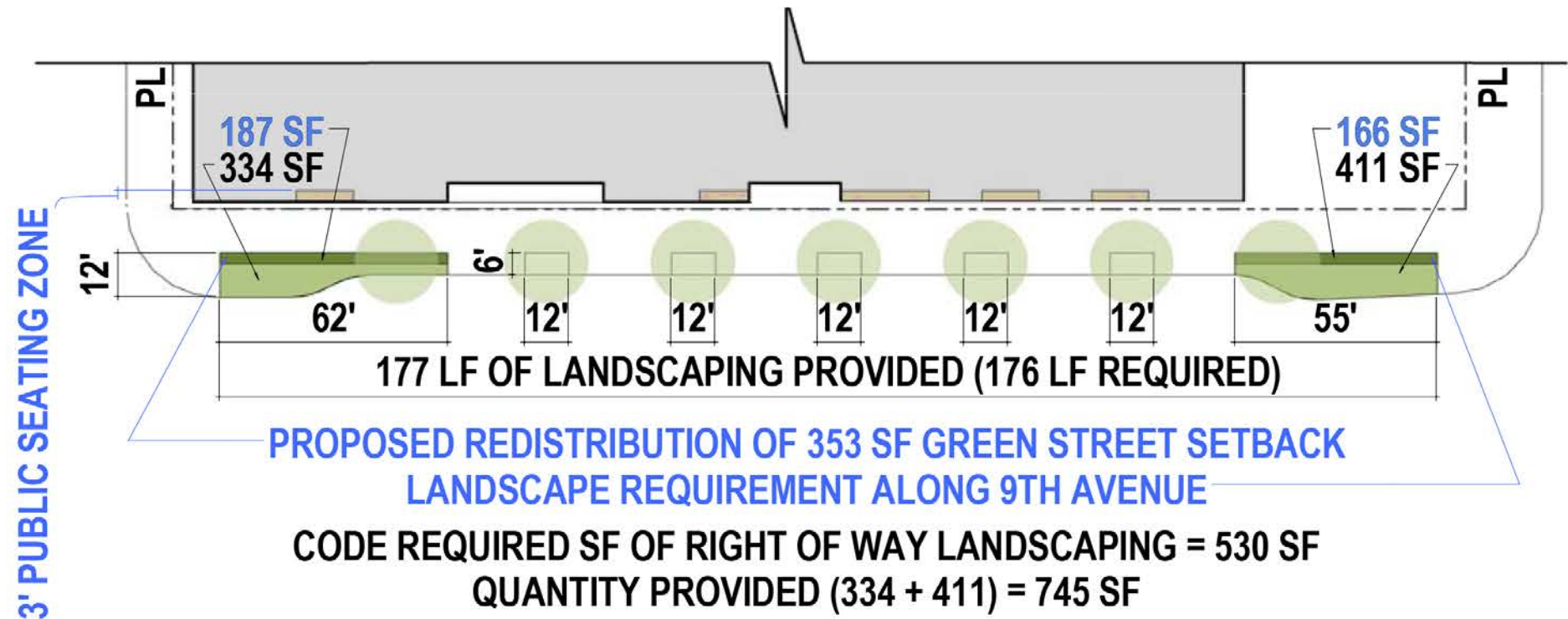
CODE BASELINE



SITE PLAN - 9TH AVENUE

4 PROPOSED DEPARTURES - DEPARTURE #3 - Green Street Setback - 23.49.056.F.4

DRB#2 - 07-19-2016 - PRESENTATION SLIDE 63



This departure was approved by the Design Review Board on July 19, 2016.

Red text indicates change in departure.

Reduced amount of Green Street setback planting to be redistributed; majority is now located adjacent to building facade. Amount of Green Street Setback planting has not changed. Additional planting has been added to provide more visual interest at exit doors consistent with DRB feedback. Additional landscape area has been provided at Right of Way.

REQUIREMENT:

Terry and 9th Avenues green street setbacks, a 2-foot wide setback from the street lot line is required. 50% of the setback area shall be landscaped.

Total area of setback = Lot line X 2 feet
= 353'-2" x 2' = 706.3SF
50% of setback area (706.3 SF) = 353.2 SF

See sheet L103 for setback and landscaping requirements calculations.

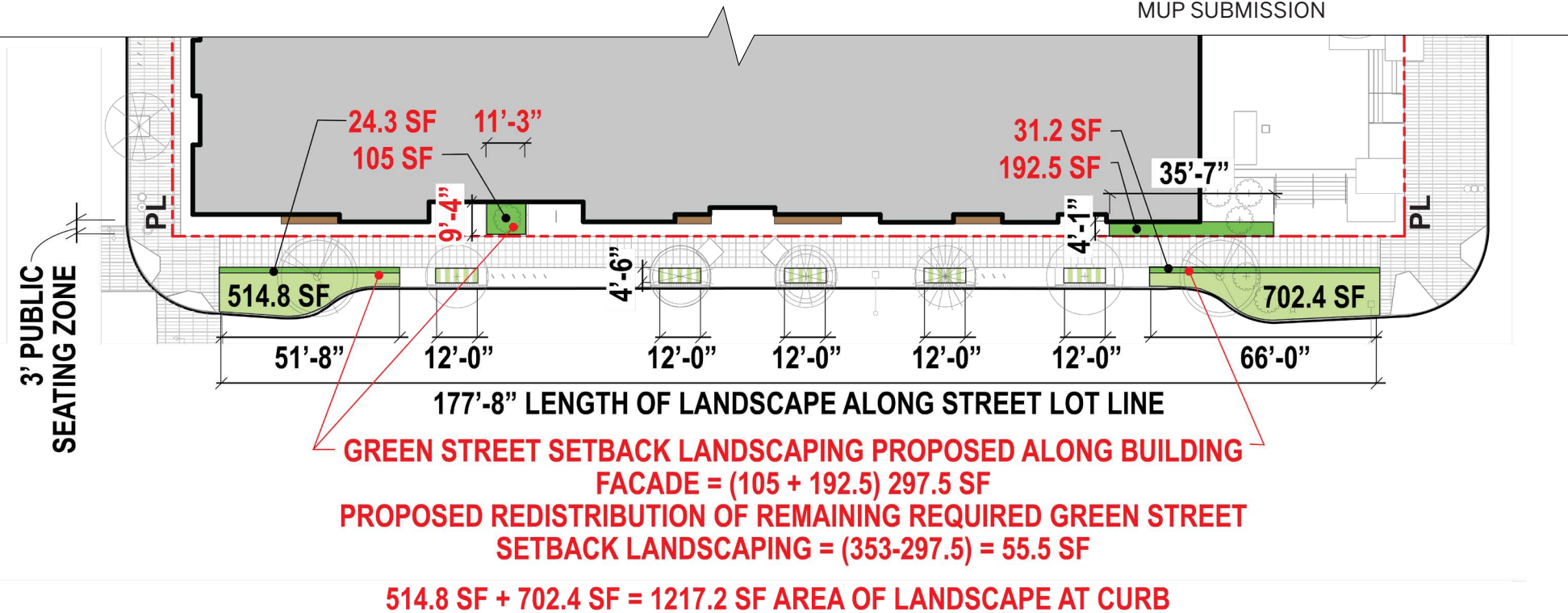
See sheet L103 for proposed redistribution of the 50% of setback landscape area along 9th Avenue.

MODIFICATION REQUESTED:

Relief from green street setback requirements along 9th Avenue. Landscape to be re-distributed along 9th to permit visual and physical access to retail.

- AREAS OF DEPARTURE FROM DEVELOPMENT STANDARD
- CHANGE IN DEPARTURE REQUEST

DRB #3 RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G123; L103; L160; L161 *THIS DEPARTURE UPDATED SINCE MUP SUBMISSION



SITE PLAN - 9TH AVENUE

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4 PROPOSED DEPARTURES - DEPARTURE #4 - Blank Facade Limits - 23.49.056.D

DRB#2 - 07-19-2016 - BOOK PG 111
RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G124

This departure was approved by the Design Review Board on July 19, 2016.

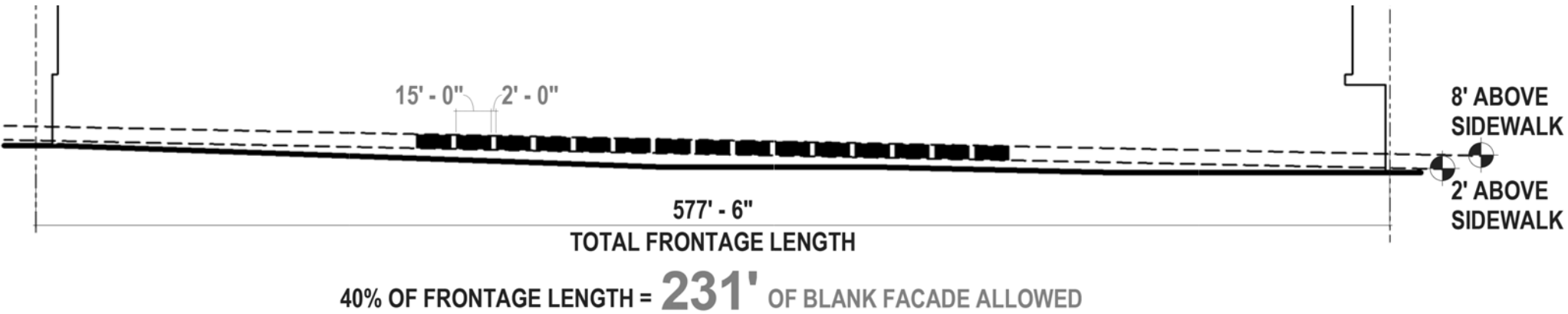
Red text indicates change in departure.

ITEM #	DEVELOPMENT STANDARD	REQUIREMENT	MODIFICATION REQUESTED	RATIONALE
4	23.49.056.D Blank Facade Limits	<p>Blank facade limits apply to the area of the facade between 2 feet and 8 feet above the sidewalk, except that where the slope along the street frontage of the facade exceeds 7.5 percent, blank facade limits apply to the area of the facade between 4 feet and 8 feet above sidewalk grade.</p> <p>Class I pedestrian streets and designated green streets. Blank facades shall be no more than 15 feet wide except segments with garage doors which may be as wide as the driveway plus 5 feet. May be increased to 30ft by the director if enhanced with visual interest features. Total width shall not exceed 40% of facade.</p> <p>Class II pedestrian streets. Blank façade segments shall be no more than 30 feet wide, except for garage doors, which may be as wide as the driveway plus 5 ft. May be increased to 60ft by the director if enhanced with visual interest features. Total width shall not exceed 70% of facade, or 75% on slopes greater than 7.5%.</p>	Relief from blank facade requirements along Boren, Olive, and 9th.	<p>The convention center program requires a significant amount of emergency egress and vertical transportation capacity to safely serve the large number of staff and visitors to the facility. Where blank facades are present at specific areas of emergency egress and vertical transportation, the use of shadowboxes, translucent glazing, and vertical vegetation are proposed to provide added visual depth, pattern, and consistency with adjacent window systems. In other areas to the extent possible, additional transparency has been provided extensively beyond code requirements.</p> <p>This departure allows the project to better meet the intent of the design guidelines (described below).</p>

RELEVANT DESIGN GUIDELINES - DEPARTURE #4	
<p>C2.1 Modulation of facades C. Other architectural elements D. Light fixtures and landscaping elements</p> <p>Canopies, structure, planters, site stairs, overhangs, and vertical planting work together to soften the pedestrian experience and create an increased sense of modulation.</p> <p>C3.1 Desirable facade elements A. Retail spaces B. Visibility into building interiors D. Landscaped or raised bed planted with vegetation that will grow up a vertical trellis or frame installed to obscure or screen the wall's blank surface E. High quality public art F. Small setbacks, indentations G. Different textures, color, or material H. Specialty lighting, a canopy, awning, horizontal trellis to add visual interest</p> <p>Many of the architectural moves feature desirable facade elements, particularly at the street level. Retail spaces have been provided on all facades above code requirements. Visibility has been provided into building interiors where permissible, including glimpses into the inner workings of elevators. Landscape has taken form both in horizontal and vertical applications. Smaller moves and modulations, textures, colors, materials, and canopies have been integrated into the architecture on all facades.</p>	<p>D2.1 Landscape enhancements D. Distinctively landscape open areas created by building modulation G. Provide framework such as trellis or arbor for plants to grown on H. Incorporate upper story planter boxes or roof planters</p> <p>Modulation helps to provide significant pockets of planting at grade. The facade along boren also features a planting framework for Vertical vegetation.</p> <p>E3.1 Methods of integrating service areas A. Plan service areas for less visible locations</p> <p>Service areas have primarily been located below grade and away from public view.</p>

4 PROPOSED DEPARTURES - DEPARTURE #4 - Blank Facade Limits - 23.49.056.D

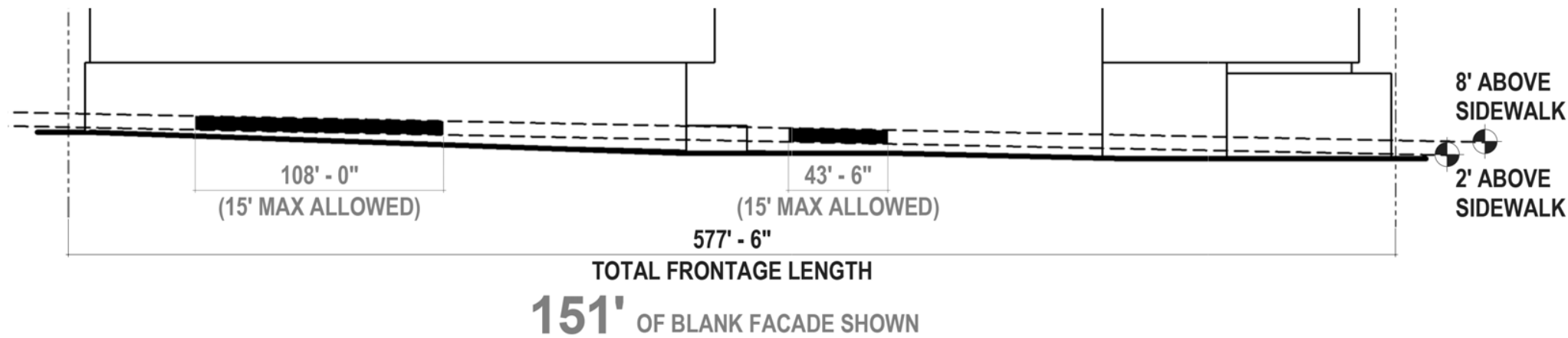
CODE BASELINE - DRB#2 - 07-19-2016 - PRESENTATION SLIDE 64



NORTH ELEVATION - OLIVE WAY

4 PROPOSED DEPARTURES - DEPARTURE #4 - Blank Facade Limits - 23.49.056.D

DRB#2 - 07-19-2016 - PRESENTATION SLIDE 64



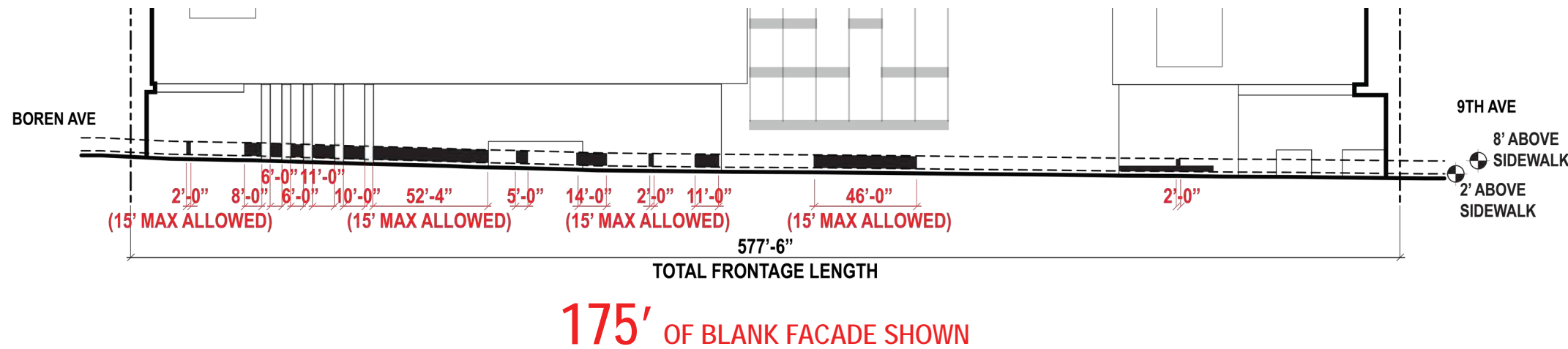
This departure was approved by the Design Review Board on July 19, 2016.

Red text indicates change in departure. Overall length of Blank Facade increased, though individual lengths reduced. More variation added to facade. Facade adjusted to accommodate garage entry shifted from Terry Avenue axis.

REQUIREMENT:
Blank facade limits apply to the area of the facade between 2 feet and 8 feet above the sidewalk, except that where the slope along the street frontage of the facade exceeds 7.5 Percent, blank facade limits apply to the area of the facade between 4 feet and 8 feet above sidewalk grade. Class I pedestrian streets and designated green streets. Blank facades shall be no more than 15 feet wide except segments with garage doors which may be as wide as the driveway plus 5 feet. May be increased to 30 feet by the director if enhanced with visual interest features. Total width shall not exceed 40% of facade. Class II pedestrian streets. Blank facade segments shall be no more than 30 feet wide, except for garage doors, which may be as wide as the driveway plus 5 feet. May be increased to 60ft by the director if enhanced with visual interest features. Total width shall not exceed 70% of facade, or 75% on slopes greater than 7.5%.

MODIFICATION REQUESTED:
Relief from blank facade requirements along Boren, Olive, and 9th.

DRB #3 RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G124; A311; A312



BLANK FACADE (23.49.056.D)			
TOTAL FRONTAGE (LF)	TOTAL BLANK FACADE (LF)	GARAGE ENTRY (LF)	% OF BLANK FACADE INCLUDING GARAGE ENTRY
577.42	175	22	34% (40% ALLOWED)

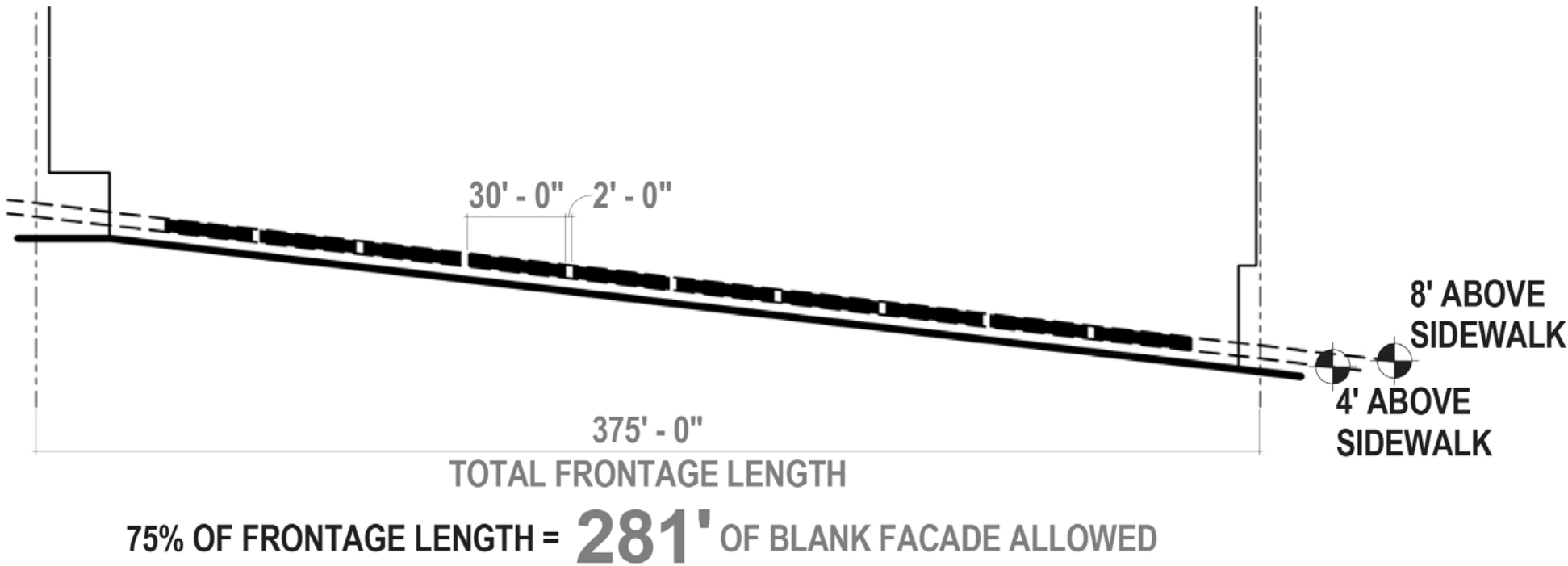
175 LF + 22 LF = 197 LF
197 LF / 578 LF = .34 = 34%

NORTH ELEVATION - OLIVE WAY

CHANGE IN DEPARTURE REQUEST

4 PROPOSED DEPARTURES - DEPARTURE #4 - Blank Facade Limits - 23.49.056.D

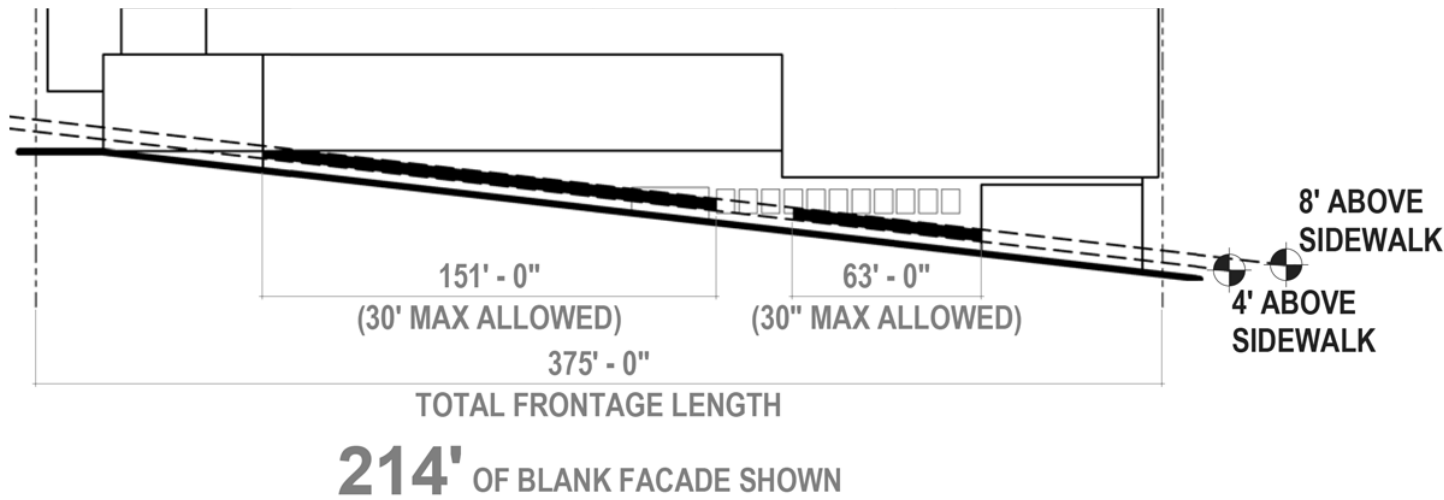
CODE BASELINE - DRB#2 - 07-19-2016 - PRESENTATION SLIDE 65, 66



EAST ELEVATION - BOREN AVENUE

4 PROPOSED DEPARTURES - DEPARTURE #4 - Blank Facade Limits - 23.49.056.D

DRB#2 - 07-19-2016 - PRESENTATION SLIDE 65, 66



This departure was approved by the Design Review Board on July 19, 2016.

Red text indicates change in departure.

Overall length of Blank Facade did not change. Individual lengths of blank facade adjusted to better balance landscape and egress along frontage and provide a more consistent rhythm / visual interest with planting.

REQUIREMENT:

Blank facade limits apply to the area of the facade between 2 feet and 8 feet above the sidewalk, except that where the slope along the street frontage of the facade exceeds 7.5 Percent, blank facade limits apply to the area of the facade between 4 feet and 8 feet above sidewalk grade. Class I pedestrian streets and designated green streets. Blank facades shall be no more than 15 feet wide except segments with garage doors which may be as wide as the driveway plus 5 feet. May be increased to 30 feet by the director if enhanced with visual interest features. Total width shall not exceed 40% of facade. Class II pedestrian streets. Blank facade segments shall be no more than 30 feet wide, except for garage doors, which may be as wide as the driveway plus 5 feet. May be increased to 60ft by the director if enhanced with visual interest features. Total width shall not exceed 70% of facade, or 75% on slopes greater than 7.5%.

MODIFICATION REQUESTED:

Relief from blank facade requirements along Boren, Olive, and 9th.

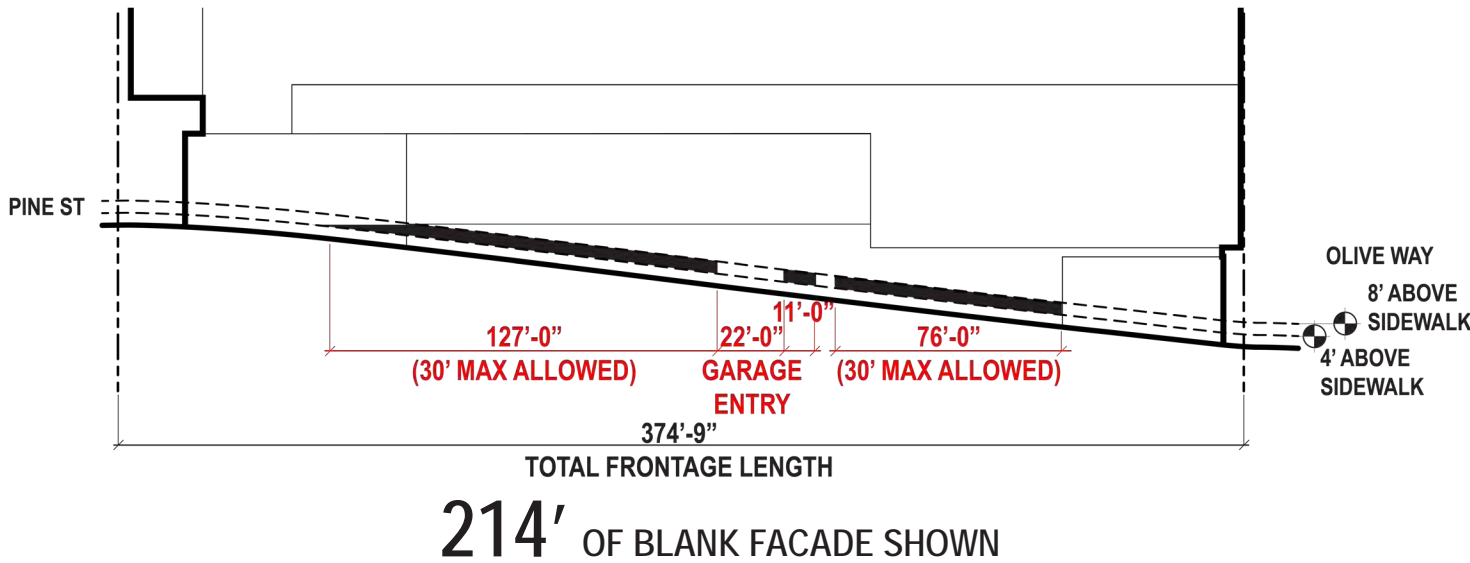
BLANK FACADE (23.49.056.D)			
TOTAL FRONTAGE (LF)	TOTAL BLANK FACADE (LF)	GARAGE ENTRY (LF)	% OF BLANK FACADE INCLUDING GARAGE ENTRY
374.75	214	22	63% (75% ALLOWED)

214 LF + 22 LF = 236 LF

236 LF / 374.75 LF = .63 = 63%

CHANGE IN DEPARTURE REQUEST

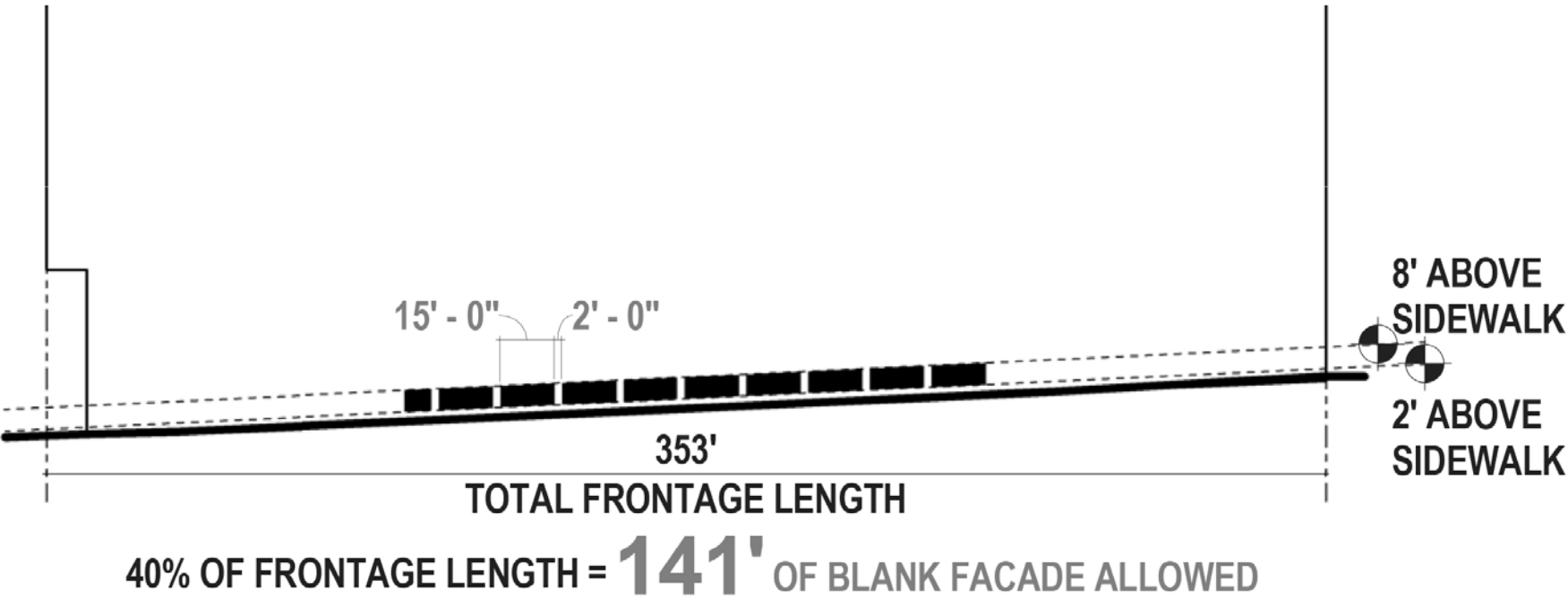
DRB #3 RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G124; A313; A314



EAST ELEVATION - BOREN AVENUE

4 PROPOSED DEPARTURES - DEPARTURE #4 - Blank Facade Limits - 23.49.056.D

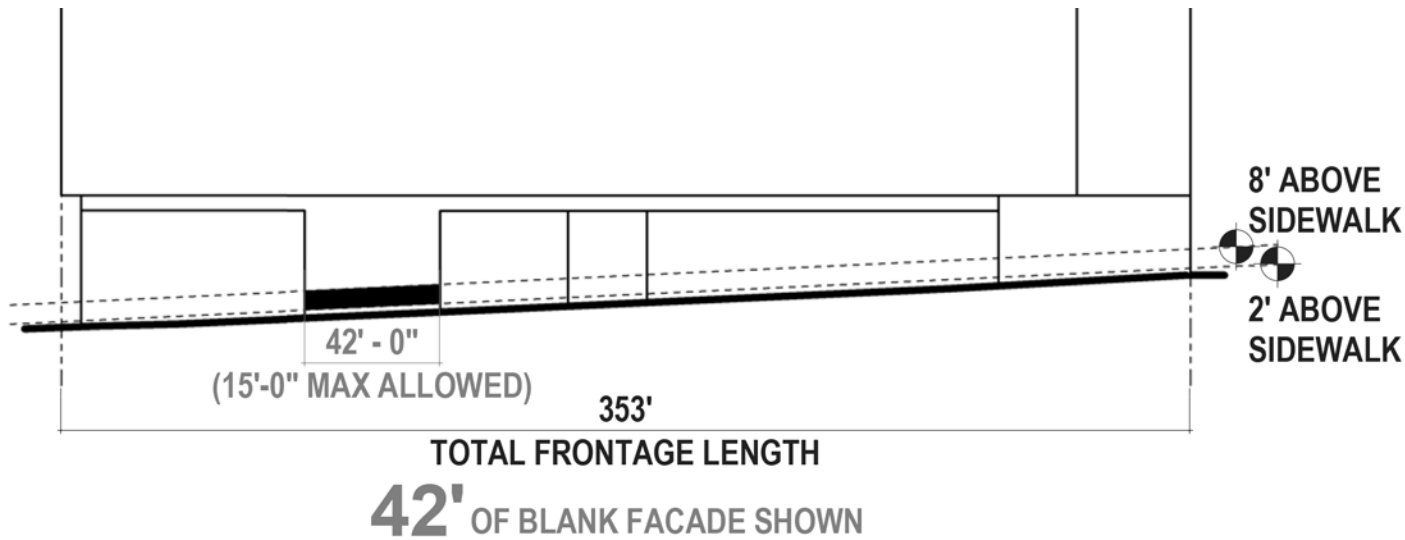
CODE BASELINE - DRB#2 - 07-19-2016 - PRESENTATION SLIDE 65, 66



WEST ELEVATION - 9TH AVENUE

4 PROPOSED DEPARTURES - DEPARTURE #4 - Blank Facade Limits - 23.49.056.D

DRB#2 - 07-19-2016 - PRESENTATION SLIDE 65, 66



This departure was approved by the Design Review Board on July 19, 2016.

Red text indicates change in departure. Overall length of Blank Facade increased. Individual small lengths of blank facade added to better articulate market protruding boxes. Facade adjusted to accommodate additional planting and tree at exits per DRB feedback to provide visual interest and texture.

REQUIREMENT:

Blank facade limits apply to the area of the facade between 2 feet and 8 feet above the sidewalk, except that where the slope along the street frontage of the facade exceeds 7.5 Percent, blank facade limits apply to the area of the facade between 4 feet and 8 feet above sidewalk grade. Class I pedestrian streets and designated green streets. Blank facades shall be no more than 15 feet wide except segments with garage doors which may be as wide as the driveway plus 5 feet. May be increased to 30 feet by the director if enhanced with visual interest features. Total width shall not exceed 40% of facade. Class II pedestrian streets. Blank facade segments shall be no more than 30 feet wide, except for garage doors, which may be as wide as the driveway plus 5 feet. May be increased to 60ft by the director if enhanced with visual interest features. Total width shall not exceed 70% of facade, or 75% on slopes greater than 7.5%.

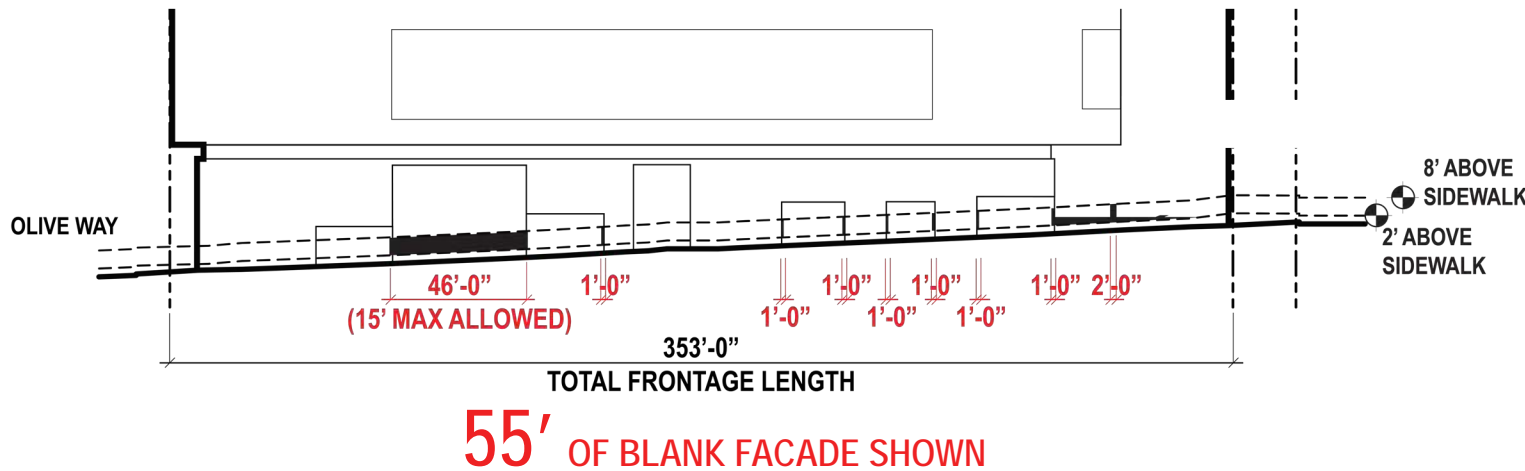
MODIFICATION REQUESTED:

Relief from blank facade requirements along Boren, Olive, and 9th.

BLANK FACADE (23.49.056.D)			
TOTAL FRONTAGE (LF)	TOTAL BLANK FACADE (LF)	GARAGE ENTRY (LF)	% OF BLANK FACADE INCLUDING GARAGE ENTRY
353.2	55	0	16% (40% ALLOWED)

55 LF / 353.2 LF = .155 = 16%

DRB #3 RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G124; A317; A318



WEST ELEVATION - 9TH AVENUE

CHANGE IN DEPARTURE REQUEST

4 PROPOSED DEPARTURES - DEPARTURE #5 - Overhead Weather Protection - 23.49.018

DRB#2 - 07-19-2016 - BOOK PG 114

RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G125

This departure was approved by the Design Review Board on July 19, 2016.

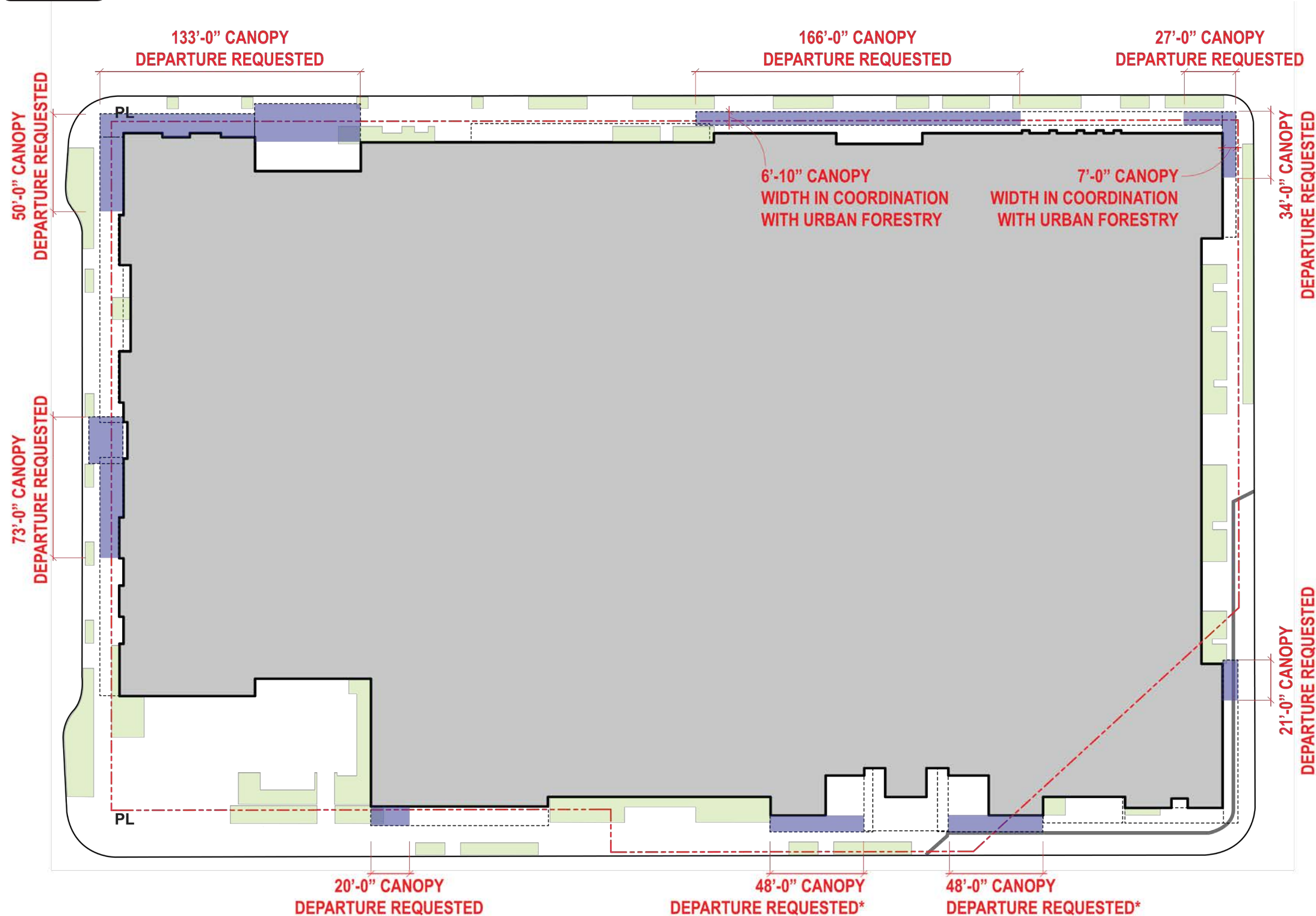
Red text indicates change in departure.

ITEM #	DEVELOPMENT STANDARD	REQUIREMENT	MODIFICATION REQUESTED	RATIONALE
5	23.49.018 Overhead Weather Protection	<p>Continuous overhead weather protection is required along the entire street frontage of a lot except along those portions of the structure facade that:</p> <ol style="list-style-type: none">1. Are located farther than five feet from the street property line or widened sidewalk on private property2. Abut a bonused open space amenity feature3. Are separated {...} By a landscaped area at least two feet in width4. Are driveways into structures or loading docks <p>Minimum dimension of eight (8) feet measured horizontally from the building wall or must extend to a line two (2) feet from the curb line, whichever is less.</p> <p>The installation of overhead weather protection shall not result in any obstructions in the sidewalk area.</p> <p>The lower edge of the overhead weather protection must be a min. 10 feet and a max. of 15 feet above the sidewalk.</p>	Variation of overhead weather protection heights at specialty zones	<p>The proposed canopies have been designed to highlight retail and major entries into the project. They promote clear wayfinding and create greater visual interest than that required by code. The remaining canopies have been designed to align with the structural dimensions and promote a more harmonious integration with architectural features.</p> <p>Landscaping and trees have been provided along the street edge to enhance the pedestrian experience. The sidewalk along Boren Ave is 15'-6" wide, street trees are required to be centered 3'-6" from the curb. An 8' canopy requirement allows only 4' between the edge of the canopy and center of the tree. Urban Forestry recommends providing 5' from the center of the tree to the edge of the canopy for proper growth. Pulling the canopy back to 7' from the building face provides coverage without limiting the growth of the trees.</p> <p>This departure allows the project to better meet the intent of the design guidelines (described below).</p>

RELEVANT DESIGN GUIDELINES - DEPARTURE #5	
<p>A1.1 Response to context B. A site having dramatic topography or contrasting edge conditions</p> <p>The canopies take on unique character in response to their context and building Program. They interact with the changing topography to create a variety of engaging spatial conditions and to highlight building features such as retail and entry points.</p> <p>B1.1 Adjacent features and networks E. Elements of the pedestrian network nearby</p> <p>While maintaining a continuous network of overhead weather protection on site, the canopy system relates to adjacent features through gentle overtures toward surrounding streets (Terry terminus) and pedestrian flow (Pine Street massing and hill climb).</p> <p>B4.1 Massing A. Setbacks, projections, and open space B. Relative sizes and shapes of distinct building volumes</p> <p>The building massing is framed, highlighted, and complemented by the canopy designs. Canopies help to define and activate important interactive areas while helping building volumes relate to contextual scale.</p>	<p>B4.2 Coherent design D. Modulation and articulation I. Building base and top</p> <p>Canopies are used in some instances to help unify building elements (9th Avenue Green Street retail market) and in others to help distinguish the building into clearly articulated and layered forms.</p>

4 PROPOSED DEPARTURES - DEPARTURE #5 - Overhead Weather Protection - 23.49.018

DRB #3 RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G125



This departure was approved by the Design Review Board on July 19, 2016.

Red text indicates change in departure. Additional length of canopy added to departure request, including previously approved canopies that are not required by code. Canopies are consistent with design intent presented at DRB 2. Adjusted to better highlight entries / major retail spaces, provide variety to building facade, and address changing grade along frontage.

- REQUIREMENT:**
- Continuous overhead weather protection is required along the entire street frontage of a lot except along those portions of the structure facade that:
- Are located farther than five feet from the street property line or widened sidewalk on private property
 - Abut a bonused open space amenity feature
 - Are separated {...} By a landscaped area at least two feet in width
 - Area driveways into structures or loading docks
 - Minimum dimension of eight (8) feet measured horizontally from the building wall or must extend to a line two (2) feet from the curb line, whichever is less.
 - The installation of overhead weather protection shall not result in any obstructions in the sidewalk area.
 - The lower edge of the overhead weather protection must be a Min. 10 Feet and a Max. Of 15 feet above the sidewalk.

MODIFICATION REQUESTED:

Variation of overhead weather protection heights as specialty zones, such as retail and building entries.

Note: Location of dimension is revised per MUP corrections for all canopies

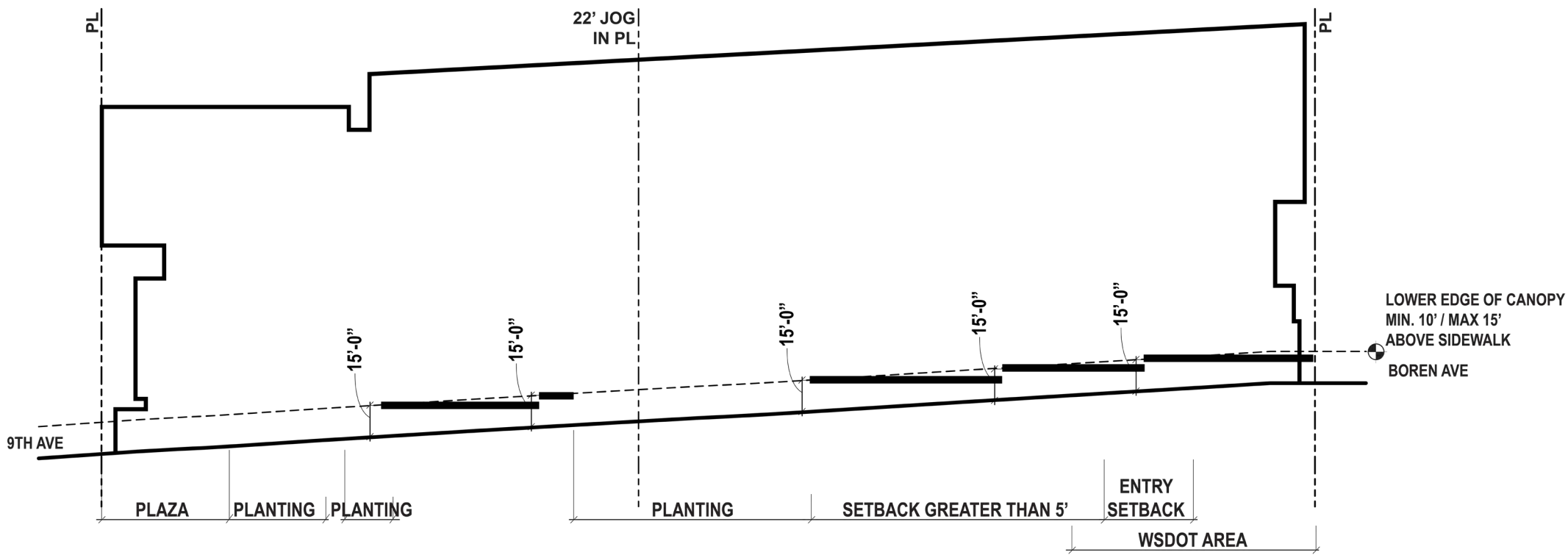
*** Same location as approved by DRB, but added to departure request for canopies not required by code**

- AREAS OF DEPARTURE FROM DEVELOPMENT STANDARD
- CHANGE IN DEPARTURE REQUEST

SITE PLAN

4 PROPOSED DEPARTURES - DEPARTURE #5 - Overhead Weather Protection - 23.49.018

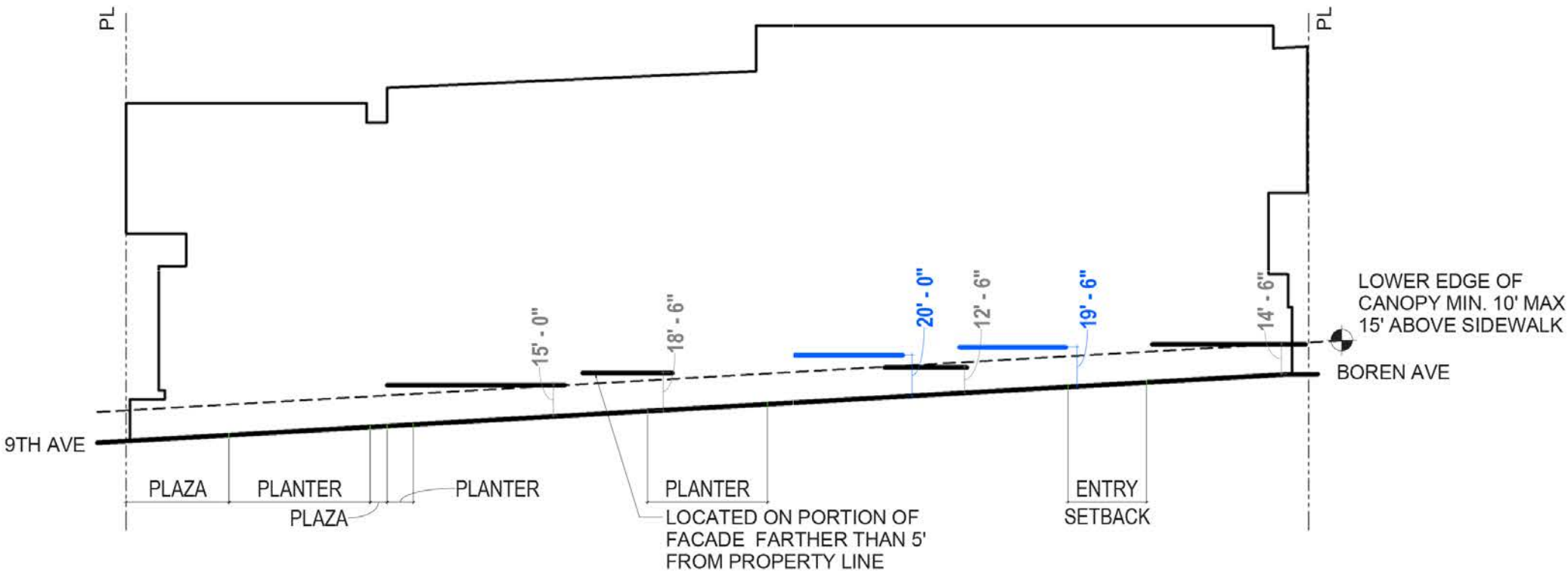
CODE BASELINE - DRB#2 - 07-19-2016 - PRESENTATION SLIDE 67



SOUTH ELEVATION - PINE STREET

4 PROPOSED DEPARTURES - DEPARTURE #5 - Overhead Weather Protection - 23.49.018

DRB#2 - 07-19-2016 - PRESENTATION SLIDE 67



This departure was approved by the Design Review Board on July 19, 2016.

Red text indicates change in departure. Additional length of canopy added to departure request, including previously approved canopies that are not required by code. Canopies are consistent with design intent presented at DRB 2. Adjusted to better highlight entries / major retail spaces, provide variety to building facade, and address changing grade along frontage.

- REQUIREMENT:**
- Continuous overhead weather protection is required along the entire street frontage of a lot except along those portions of the structure facade that:
- Are located farther than five feet from the street property line or widened sidewalk on private property
 - Abut a bonused open space amenity feature
 - Are separated {...} By a landscaped area at least two feet in width
 - Area driveways into structures or loading docks
 - Minimum dimension of eight (8) feet measured horizontally from the building wall or must extend to a line two (2) feet from the curb line, whichever is less.
 - The installation of overhead weather protection shall not result in any obstructions in the sidewalk area.
 - The lower edge of the overhead weather protection must be a Min. 10 Feet and a Max. Of 15 feet above the sidewalk.

MODIFICATION REQUESTED:

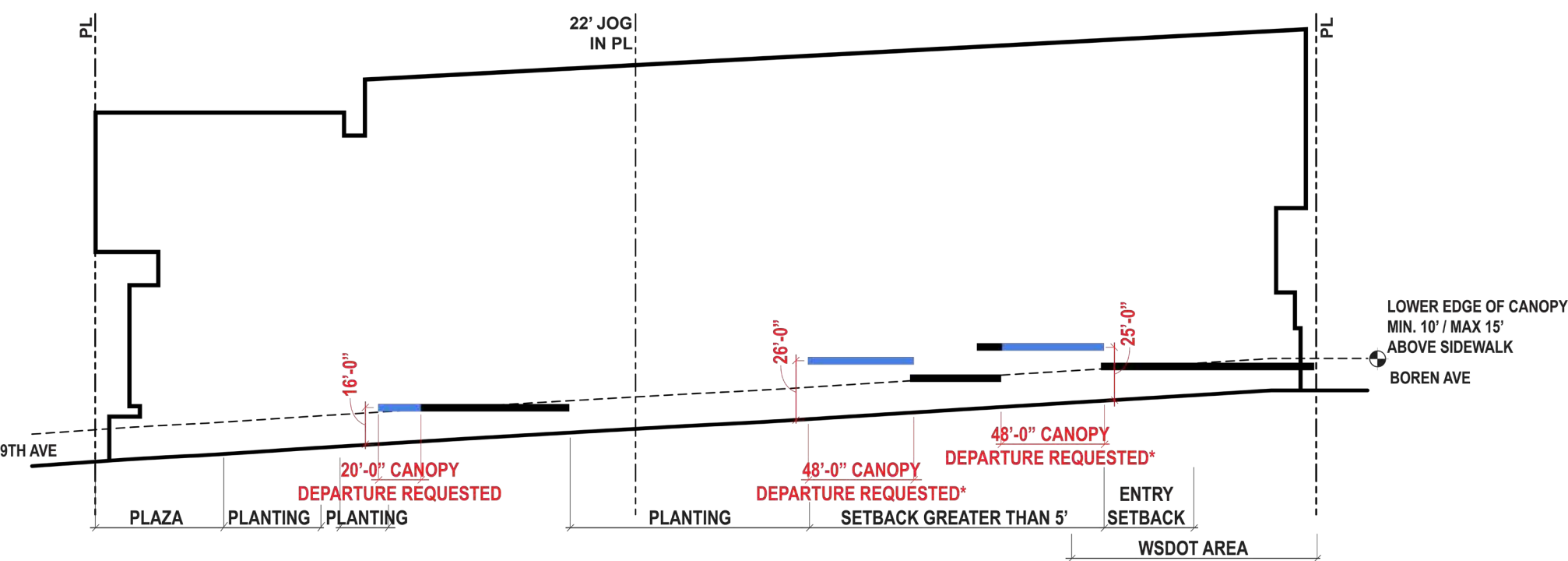
Variation of overhead weather protection heights as specialty zones, such as retail and building entries.

Note: Location of dimension is revised per MUP corrections for all canopies

* Same location as approved by DRB, but added to departure request for canopies not required by code

- AREAS OF DEPARTURE FROM DEVELOPMENT STANDARD
- CHANGE IN DEPARTURE REQUEST

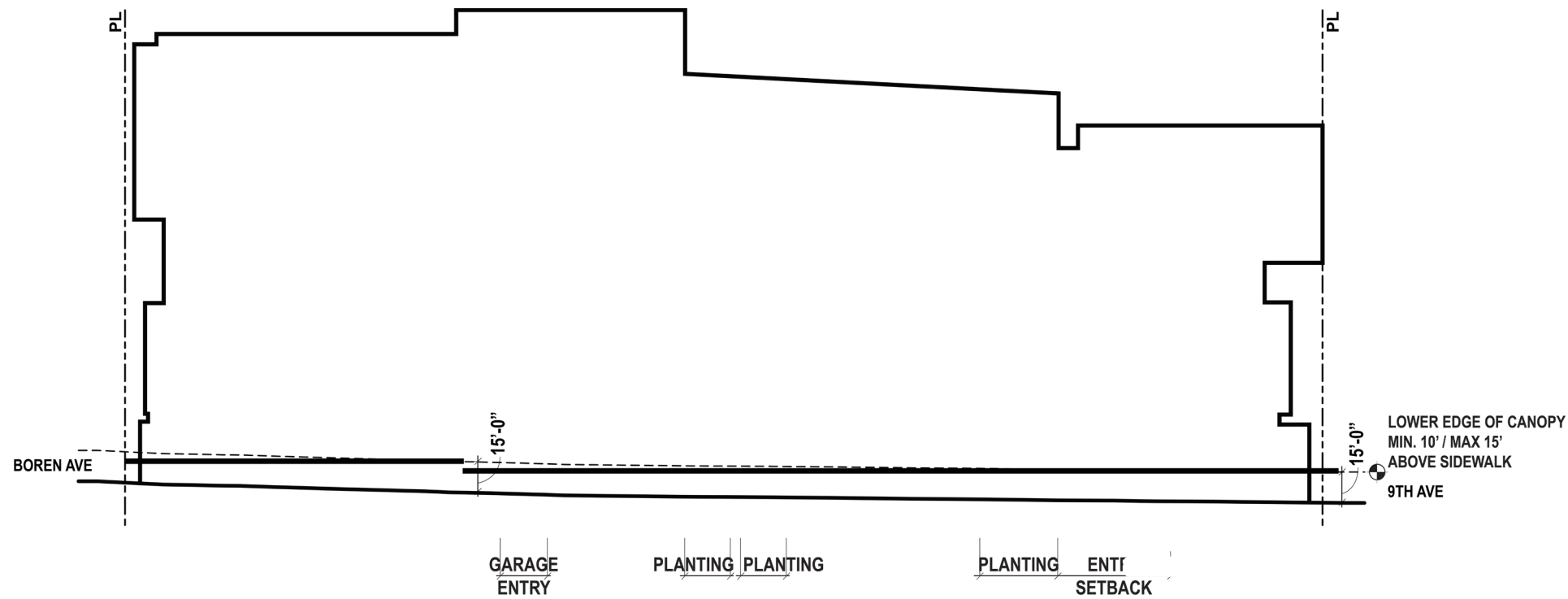
DRB #3 RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G125; A315; A316



SOUTH ELEVATION - PINE STREET

4 PROPOSED DEPARTURES - DEPARTURE #5 - Overhead Weather Protection - 23.49.018

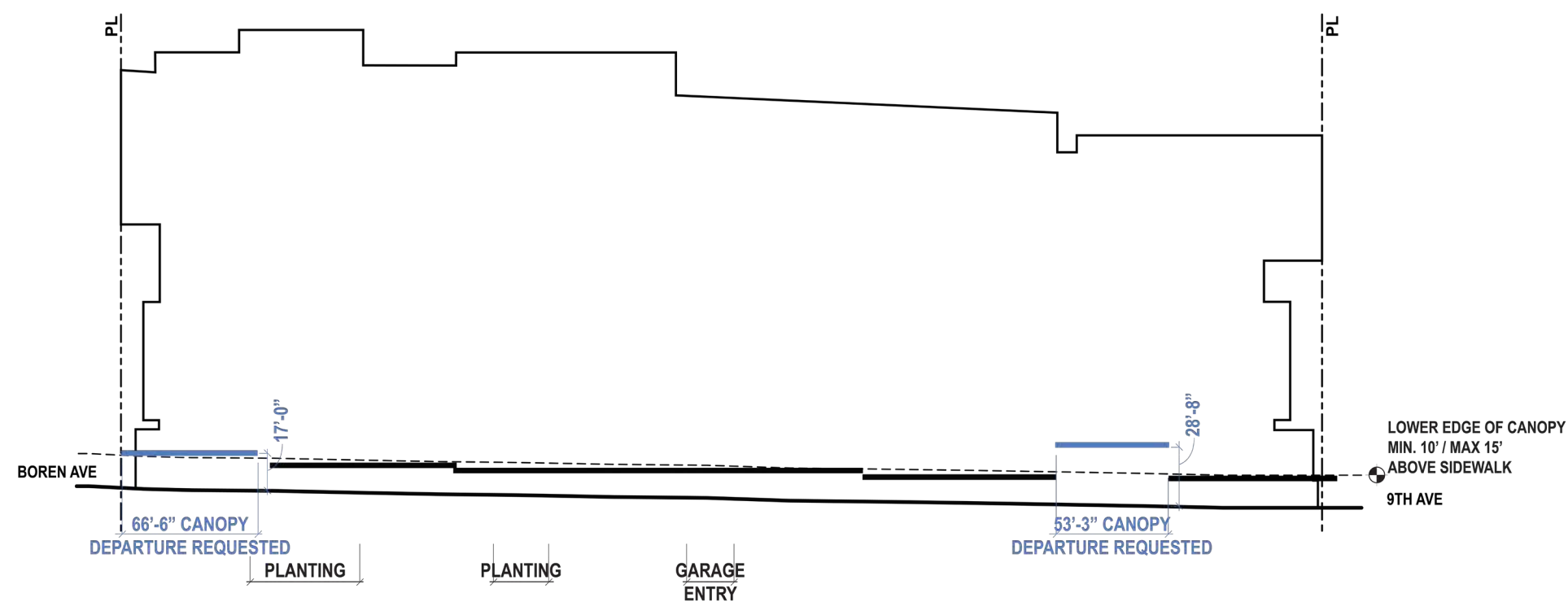
CODE BASELINE



NORTH ELEVATION - OLIVE WAY

4 PROPOSED DEPARTURES - DEPARTURE #5 - Overhead Weather Protection - 23.49.018

DRB#2 - 07-19-2016

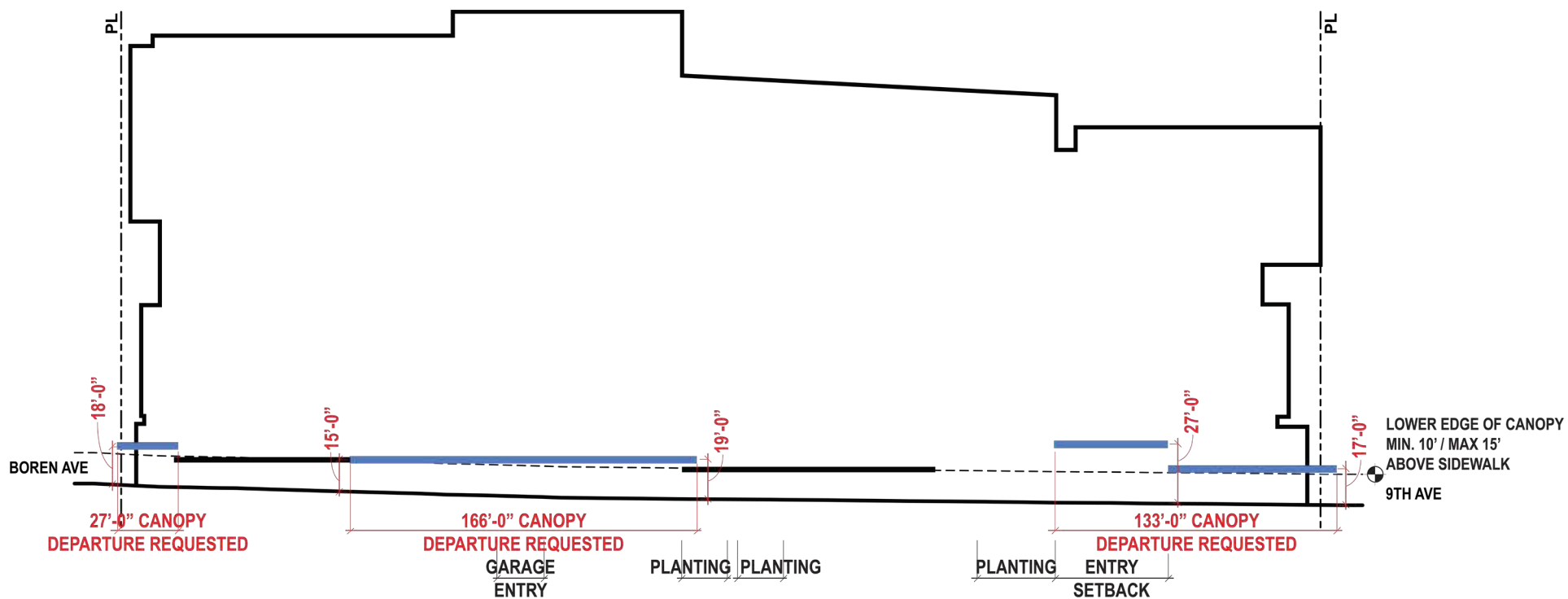


This departure was approved by the Design Review Board on July 19, 2016.

Red text indicates change in departure. Additional length of canopy added to departure request, including previously approved canopies that are not required by code. Canopies are consistent with design intent presented at DRB 2. Adjusted to better highlight entries / major retail spaces, provide variety to building facade, and address changing grade along frontage.

- REQUIREMENT:**
- Continuous overhead weather protection is required along the entire street frontage of a lot except along those portions of the structure facade that:
- Are located farther than five feet from the street property line or widened sidewalk on private property
 - Abut a bonused open space amenity feature
 - Are separated {...} By a landscaped area at least two feet in width
 - Area driveways into structures or loading docks
 - Minimum dimension of eight (8) feet measured horizontally from the building wall or must extend to a line two (2) feet from the curb line, whichever is less.
 - The installation of overhead weather protection shall not result in any obstructions in the sidewalk area.
 - The lower edge of the overhead weather protection must be a Min. 10 Feet and a Max. Of 15 feet above the sidewalk.

DRB #3 RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G125; A311; A312



MODIFICATION REQUESTED:

Variation of overhead weather protection heights as specialty zones, such as retail and building entries.

Note: Location of dimension is revised per MUP corrections for all canopies

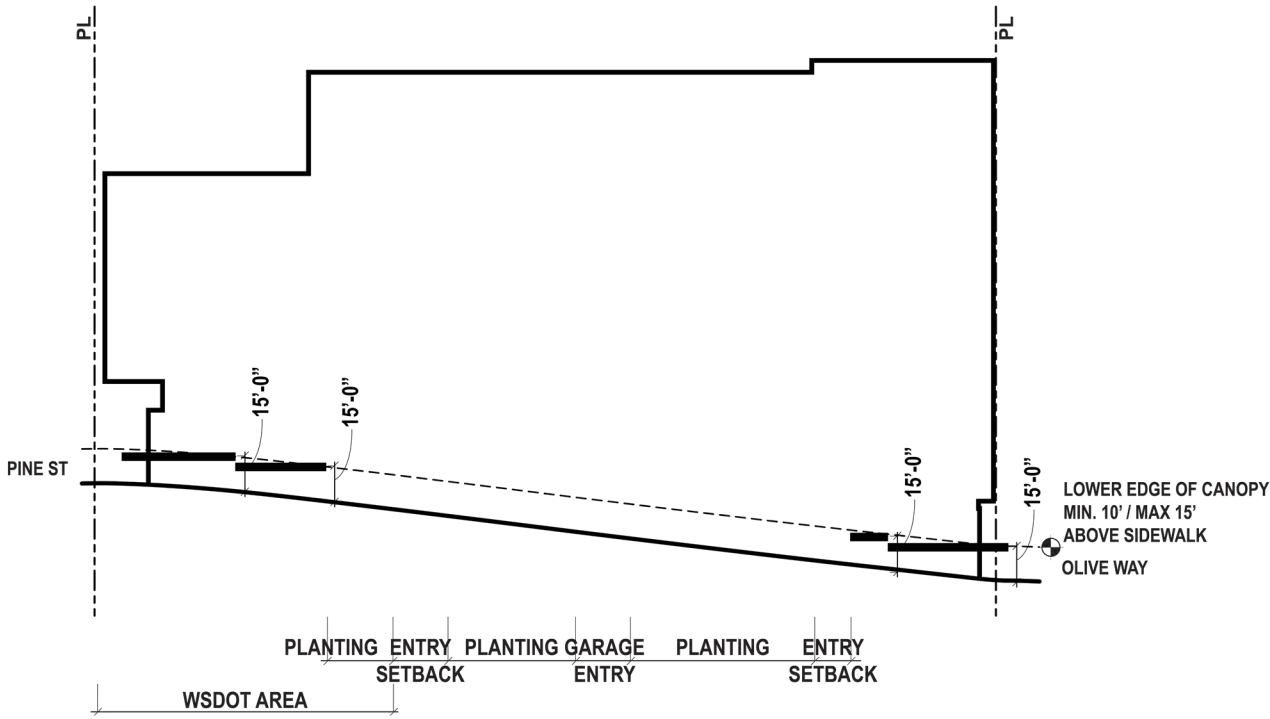
*** Same location as approved by DRB, but added to departure request for canopies not required by code**

- AREAS OF DEPARTURE FROM DEVELOPMENT STANDARD
- CHANGE IN DEPARTURE REQUEST

NORTH ELEVATION - OLIVE WAY

4 PROPOSED DEPARTURES - DEPARTURE #5 - Overhead Weather Protection - 23.49.018

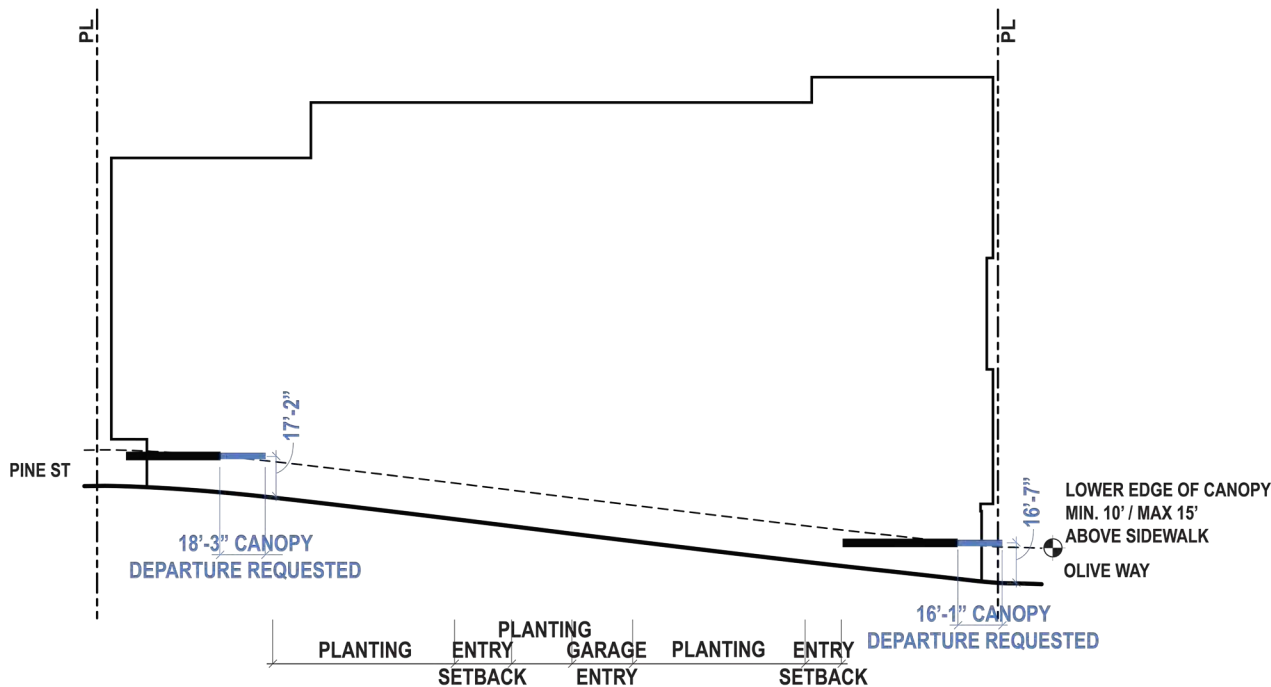
CODE BASELINE



EAST ELEVATION - BOREN AVENUE

4 PROPOSED DEPARTURES - DEPARTURE #5 - Overhead Weather Protection - 23.49.018

DRB#2 - 07-19-2016

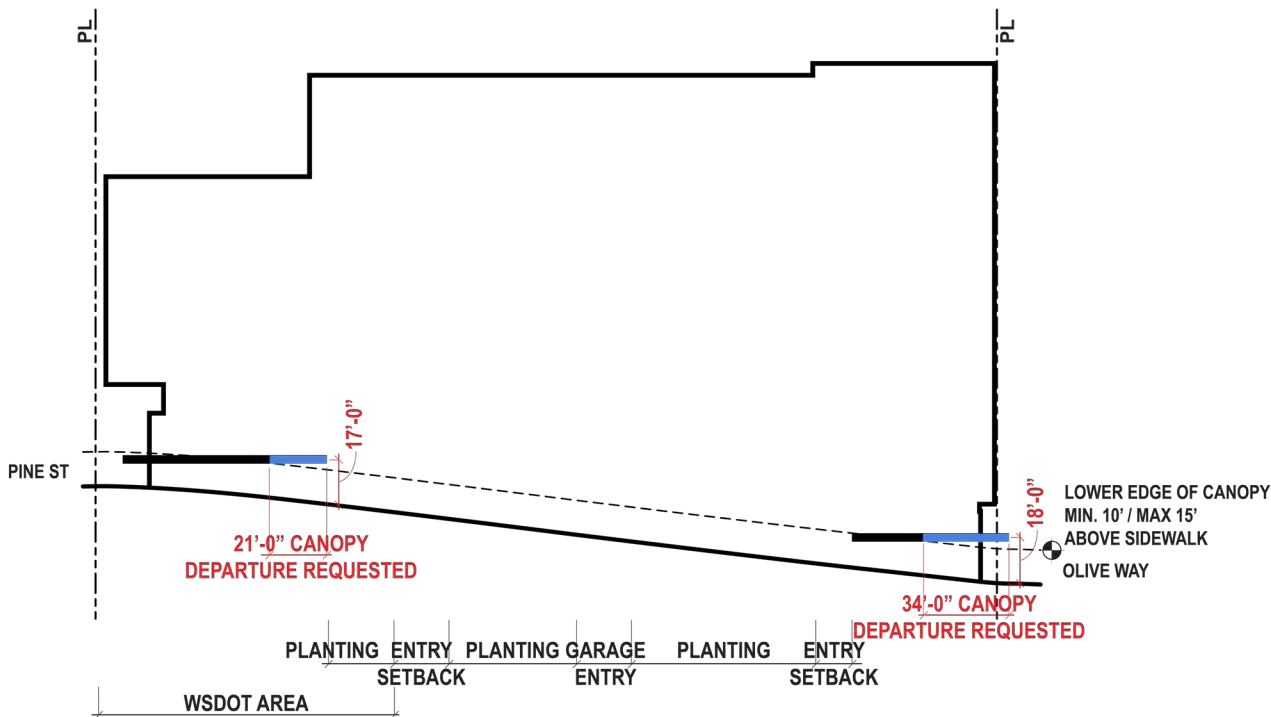


This departure was approved by the Design Review Board on July 19, 2016.

Red text indicates change in departure. Additional length of canopy added to departure request, including previously approved canopies that are not required by code. Canopies are consistent with design intent presented at DRB 2. Adjusted to better highlight entries / major retail spaces, provide variety to building facade, and address changing grade along frontage.

- REQUIREMENT:**
- Continuous overhead weather protection is required along the entire street frontage of a lot except along those portions of the structure facade that:
- Are located farther than five feet from the street property line or widened sidewalk on private property
 - Abut a bonused open space amenity feature
 - Are separated {...} By a landscaped area at least two feet in width
 - Area driveways into structures or loading docks
 - Minimum dimension of eight (8) feet measured horizontally from the building wall or must extend to a line two (2) feet from the curb line, whichever is less.
 - The installation of overhead weather protection shall not result in any obstructions in the sidewalk area.
 - The lower edge of the overhead weather protection must be a Min. 10 Feet and a Max. Of 15 feet above the sidewalk.

DRB #3 RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G125; A313; A314



MODIFICATION REQUESTED:

Variation of overhead weather protection heights as specialty zones, such as retail and building entries.

Note: Location of dimension is revised per MUP corrections for all canopies

* Same location as approved by DRB, but added to departure request for canopies not required by code

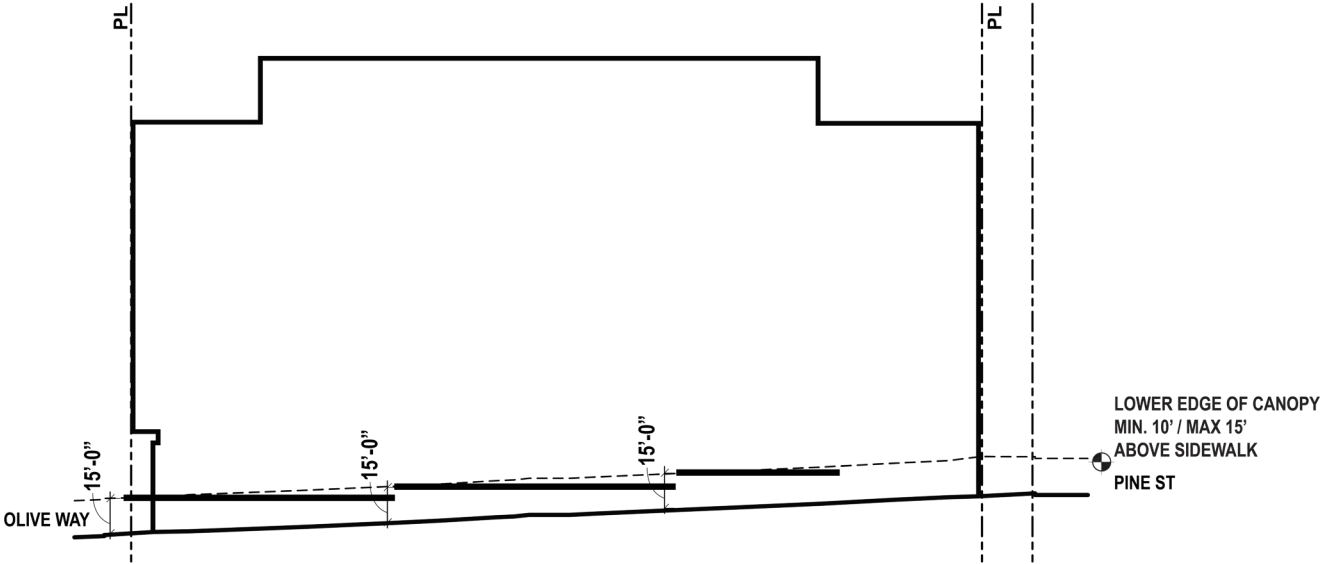
AREAS OF DEPARTURE FROM DEVELOPMENT STANDARD

CHANGE IN DEPARTURE REQUEST

EAST ELEVATION - BOREN AVENUE

4 PROPOSED DEPARTURES - DEPARTURE #5 - Overhead Weather Protection - 23.49.018

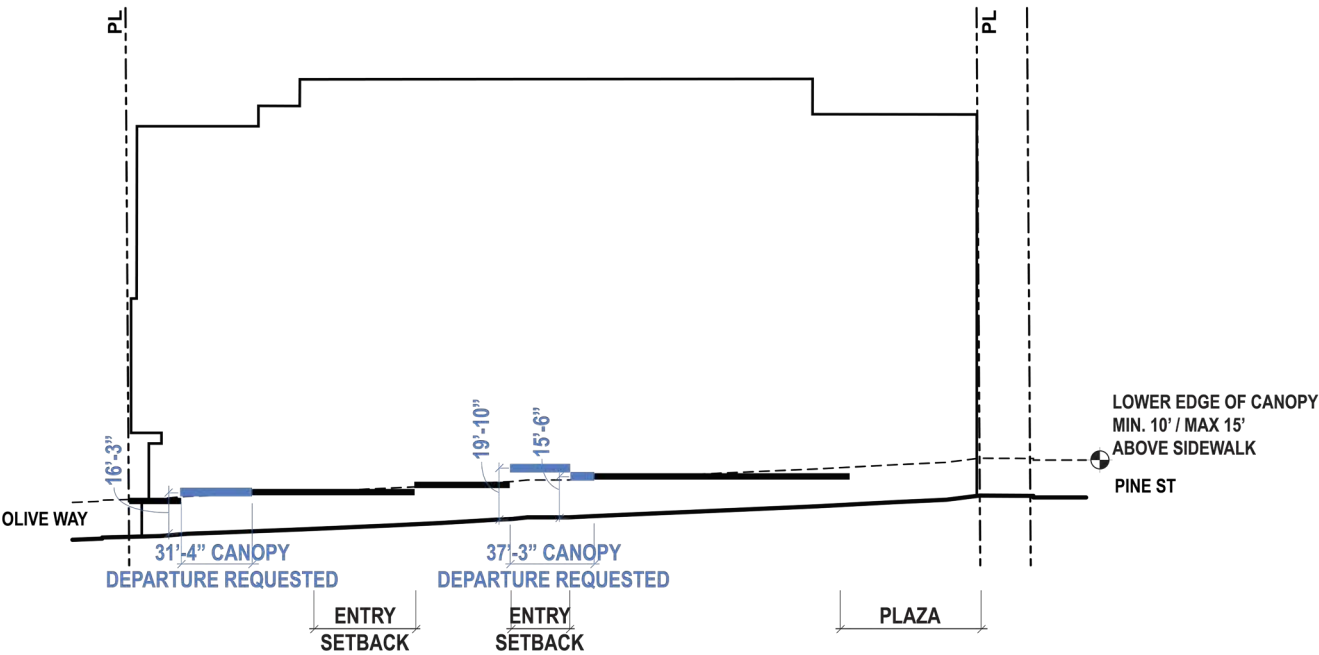
CODE BASELINE



WEST ELEVATION - 9TH AVENUE

4 PROPOSED DEPARTURES - DEPARTURE #5 - Overhead Weather Protection - 23.49.018

DRB#2 - 07-19-2016

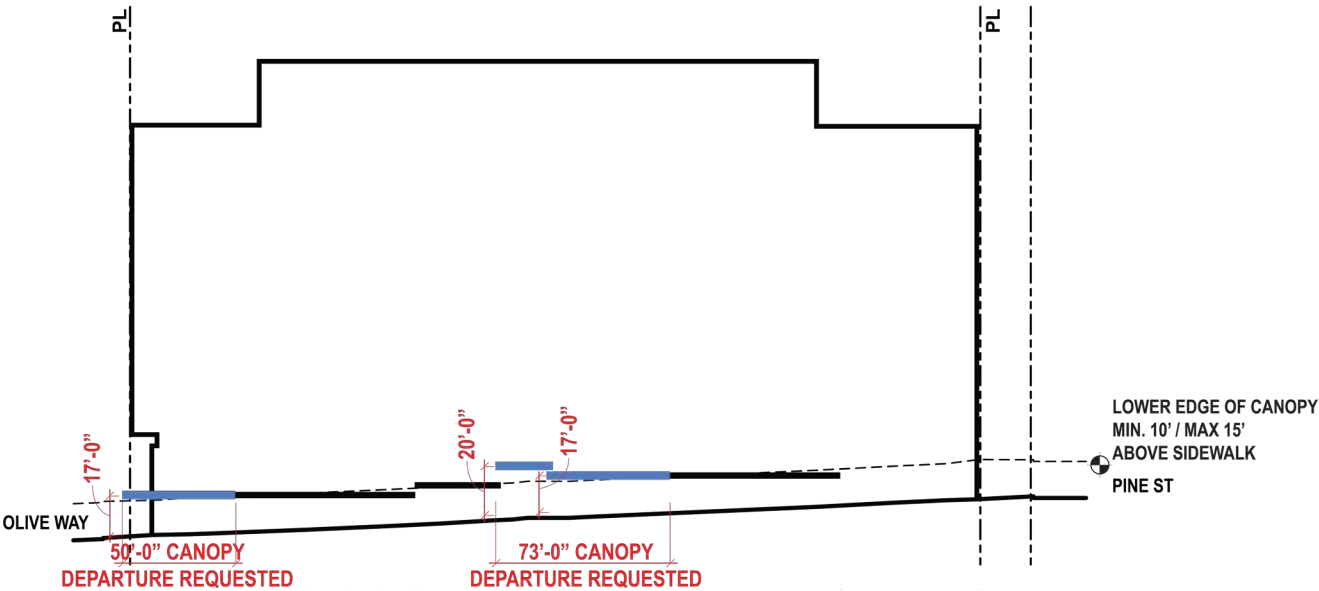


This departure was approved by the Design Review Board on July 19, 2016.

Red text indicates change in departure. Additional length of canopy added to departure request, including previously approved canopies that are not required by code. Canopies are consistent with design intent presented at DRB 2. Adjusted to better highlight entries / major retail spaces, provide variety to building facade, and address changing grade along frontage.

- REQUIREMENT:**
- Continuous overhead weather protection is required along the entire street frontage of a lot except along those portions of the structure facade that:
- Are located farther than five feet from the street property line or widened sidewalk on private property
 - Abut a bonused open space amenity feature
 - Are separated {...} By a landscaped area at least two feet in width
 - Area driveways into structures or loading docks
 - Minimum dimension of eight (8) feet measured horizontally from the building wall or must extend to a line two (2) feet from the curb line, whichever is less.
 - The installation of overhead weather protection shall not result in any obstructions in the sidewalk area.
 - The lower edge of the overhead weather protection must be a Min. 10 Feet and a Max. Of 15 feet above the sidewalk.

DRB #3 RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G125; A317; A318



MODIFICATION REQUESTED:

Variation of overhead weather protection heights as specialty zones, such as retail and building entries.

Note: Location of dimension is revised per MUP corrections for all canopies

* Same location as approved by DRB, but added to departure request for canopies not required by code

- AREAS OF DEPARTURE FROM DEVELOPMENT STANDARD
- CHANGE IN DEPARTURE REQUEST

WEST ELEVATION - 9TH AVENUE

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UPDATED TEXT HIGHLIGHTED IN RED

4 PROPOSED DEPARTURES - DEPARTURE #6 - Landscaping In Street Right-Of-Way (Pine Street) - 23.49.056 F1

DRB#2 - 07-19-2016 - BOOK PG 116

RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G126

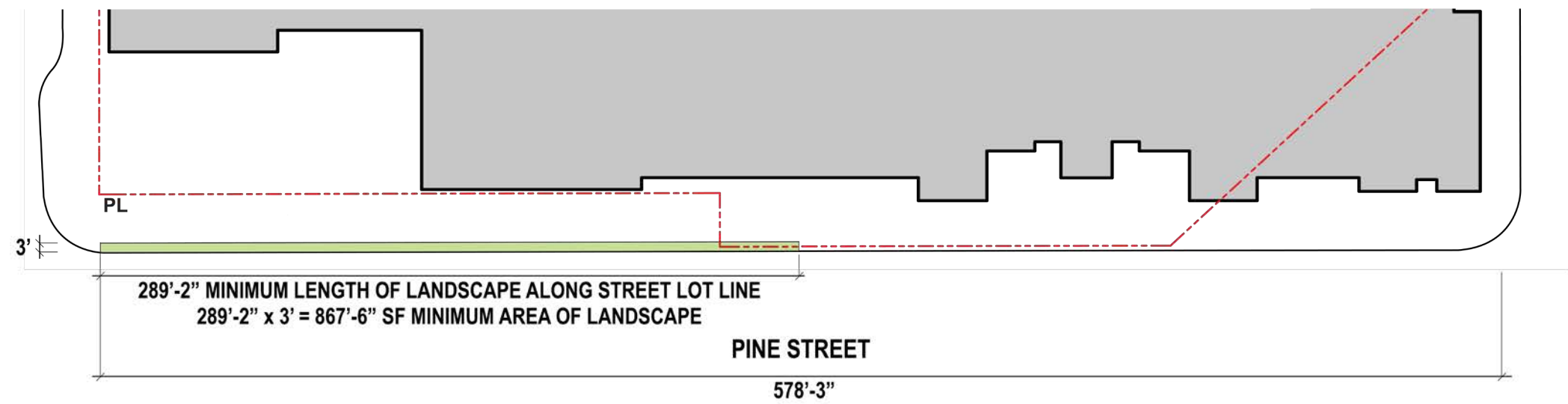
This departure was approved by the Design Review Board on July 19, 2016.

Red text indicates change in departure.

ITEM #	DEVELOPMENT STANDARD	REQUIREMENT	MODIFICATION REQUESTED	RATIONALE
6	23.49.056.F.1 Landscaping In Street Right-Of-Way	<p>Landscape area shall be at least 18 inches wide and shall be located in the public right-of-way along the entire length of the street lot line, except for building entrances, vehicular access or other connections between sidewalk and the lot, provided that the exceptions may not exceed 50% of the total length of the street lot line(s).</p> <p>As an alternative to locating the landscape at the street lot line, all or a portion of the required landscaped area may be provided in the sidewalk area within 5 feet of the curb line.</p> <p>Total length of landscape required along lot line: 50% of 578.25 LF = 289.13 LF</p> <p>Note: the minimum requirements nets 867 SF of landscaped area (1.5 FT x 578.25 FT).</p>	<p>Landscape adjoining the curb is 6'-6" wide and covers 110.5 LF.</p> <p>Additional landscape of 207.07 LF (44.58 +25.08 + 108.25 + 11.33 + 17.83 LF) provided in the sidewalk area but is located more than 5 feet from the curb line</p> <p>Total length of landscape provided: 110.5 LF + 209.4 LF = 317.57 LF</p> <p>162 LF of the lot line is over WSDOT Highway. Additional load cannot be Added to the existing structure. To meet the recommended standards, propose deducting the length of the WSDOT lot line from the calculations.</p> <p>Deducting the LF over WSDOT: 578.25 LF - 162 LF = 416.25 LF Total length of landscape required: 50% Of 416.25 LF = 208.13 LF Total length of landscape provided is 317.57 LF</p> <p>Note: counting only the landscape areas adjacent to the curb line and within sidewalk, the project provides 2656.57 SF of landscape area or 306% of the required minimum based on the full length of the street lot line measured parallel to the street curb line.</p>	<p>Per Design Review Guidelines for Downtown Development</p> <p>A1.2: Response to Planning Efforts D2.1: Landscape Enhancements D: Distinctively landscape open areas created by building modulation</p> <p>Planting design for Pine Street enhances interest and diversity along this two block stretch, building on the character of the larger Denny Triangle Neighborhood Green Streets where informal character and variety help define the neighborhood character. Large planting areas along building edge capitalize and support building modulation and incorporates additional trees. The largest of the planters marks the locations of the former intersection of Pine Street and Terry Avenue, helping to break down the scale of the double block for pedestrians. Native species of trees and shrub planting also contribute to rich native character of Denny Triangle Neighborhood. In place of planting over WSDOT bridge structure, other pedestrian amenities enhance pedestrian connections and support a continuous experience. Irregular planting spacing on both sides of the sidewalk also preserves and enhances iconic view to Pike Place and Elliott Bat marking this location as an important moment for orienting pedestrians between Capitol Hill, Downtown, Denny Triangle Neighborhood and First Hill. The planting design also supports pedestrian connections into and out of the new plaza at the intersection of 9th Avenue and Pine Street. Planting helps frame the plaza edge while keeping the sidewalk clear at this important pedestrian crossing.</p>

4 PROPOSED DEPARTURES - DEPARTURE #6 - Landscaping In Street Right-Of-Way (Pine Street) - 23.49.056 F1

CODE BASELINE

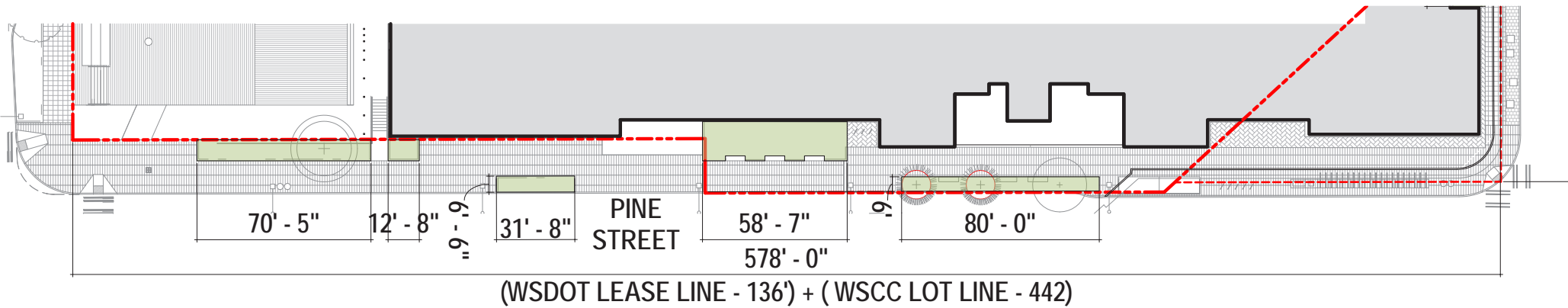


SITE PLAN - PINE STREET

4 PROPOSED DEPARTURES - DEPARTURE #6 - Landscaping In Street Right-Of-Way (Pine Street) - 23.49.056 F1

DRB#2 - 07-19-2016 - PRESENTATION SLIDE 68

LENGTH OF LANDSCAPE REQUIRED = 289.13 LF
LENGTH OF LANDSCAPE REQUIRED W/ WSDOT AREA DEDUCTED = 220.96 LF
LENGTH OF LANDSCAPE PROVIDED = 253.2 LF



This departure was approved by the Design Review Board on July 19, 2016.

Red text indicates change in departure.

Length of landscape has increased. Additional landscape added to soften and provide variety to streetscape along more of the lot line. Pine Street sun garden enlarged. Calculation of required landscape and WSDOT area has been refined.

REQUIREMENT (PINE STREET):

Landscape area shall be at least 18 inches wide and shall be located in the public right-of-way along the entire length of the street lot line, except for building entrances, vehicular access or other connections between sidewalk and the lot, provided that the exceptions may not exceed 50% of the total length of the street lot line(s).

As an alternative to locating the landscape at the street lot line, all or a portion of the required landscaped area may be provided in the sidewalk area within 5 feet of the curb line.

Total length of landscape required along lot line: 50% of 578.25 LF = 289.13 LF

Note: The minimum requirements nets 867 SF of landscaped area (1.5 FT x 578.25 FT).

MODIFICATION REQUESTED (PINE STREET):

Landscape adjoining the curb is 6'-6" wide and covers 110.5 LF.

Additional landscape of 209.4 LF (44.58 + 25.08 + 113.42 + 11.33 + 17.83 LF) provided in the sidewalk area but is located more than 5 feet from the curb line

Total length of landscape provided: 110.5 LF + 209.4 LF = 317.57 LF

162 LF of the lot line is over WSDOT Highway. Additional load cannot be added to the existing structure. To meet the recommended standards, Propose deducting the length of the WSDOT lot line from the calculations.

Deducting the LF over WSDOT:

578.25 LF - 162 LF = 416.25 LF

Total length of landscape required:

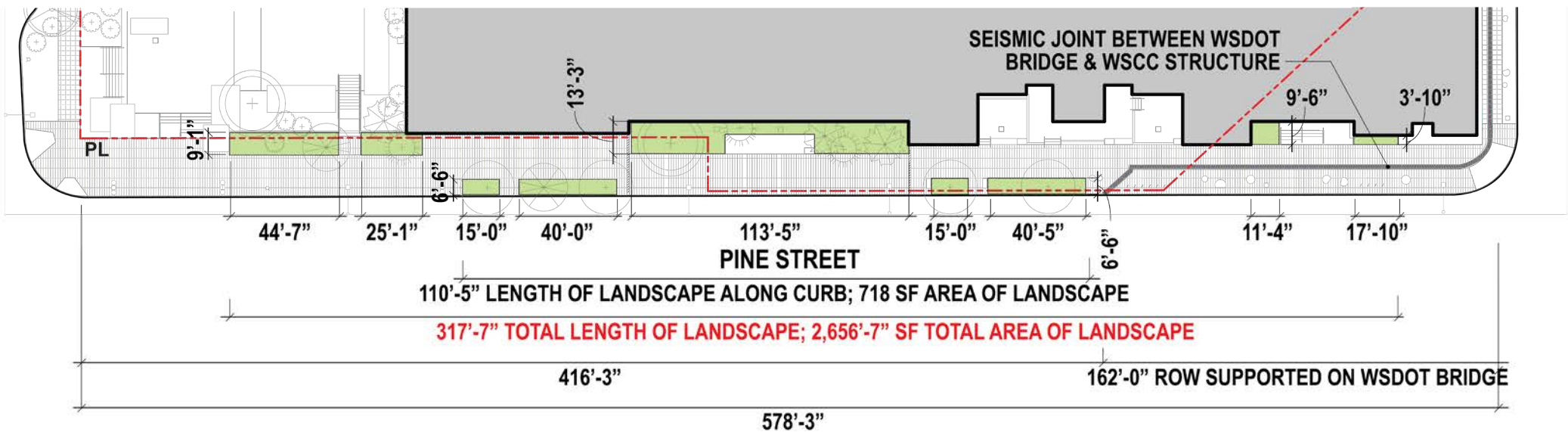
50% Of 416.25 LF = 208.13 LF

Total length of landscape provided is 317.57 LF

Note: counting only the landscape areas adjacent to the curb line and within sidewalk, the project provides 2,656.57 SF of landscape area or 306% of the required minimum based on the full length of the street lot line measured parallel to the street curb line.

DRB #3 RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G126; L160

LENGTH OF LANDSCAPE REQUIRED = 289.13 LF
LENGTH OF LANDSCAPE REQUIRED W/ WSDOT AREA DEDUCTED = 208.13 LF
LENGTH OF LANDSCAPE PROVIDED = 317.57 LF



SITE PLAN - PINE STREET

AREAS OF DEPARTURE FROM DEVELOPMENT STANDARD
CHANGE IN DEPARTURE REQUEST

4 PROPOSED DEPARTURES - DEPARTURE #7 - Landscaping In Street Right-Of-Way (Boren Avenue) - 23.49.056 F1

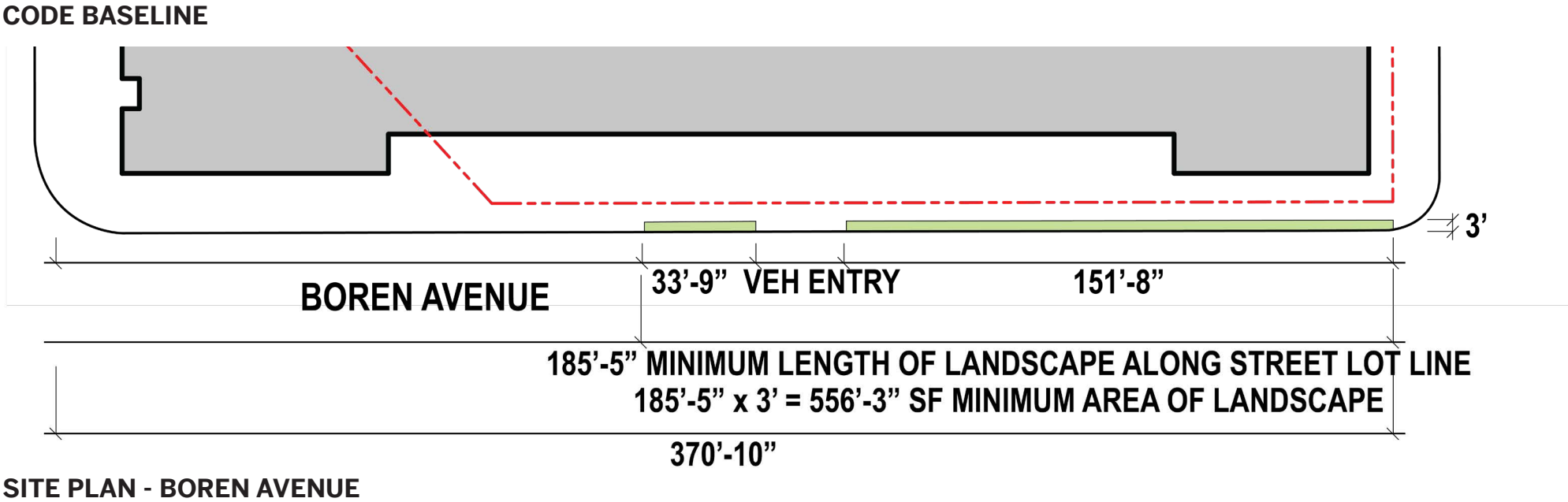
DRB#2 - 07-19-2016 - BOOK PG 116

DRB #3 RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G126

This departure was approved by the Design Review Board on July 19, 2016.

Red text indicates change in departure.

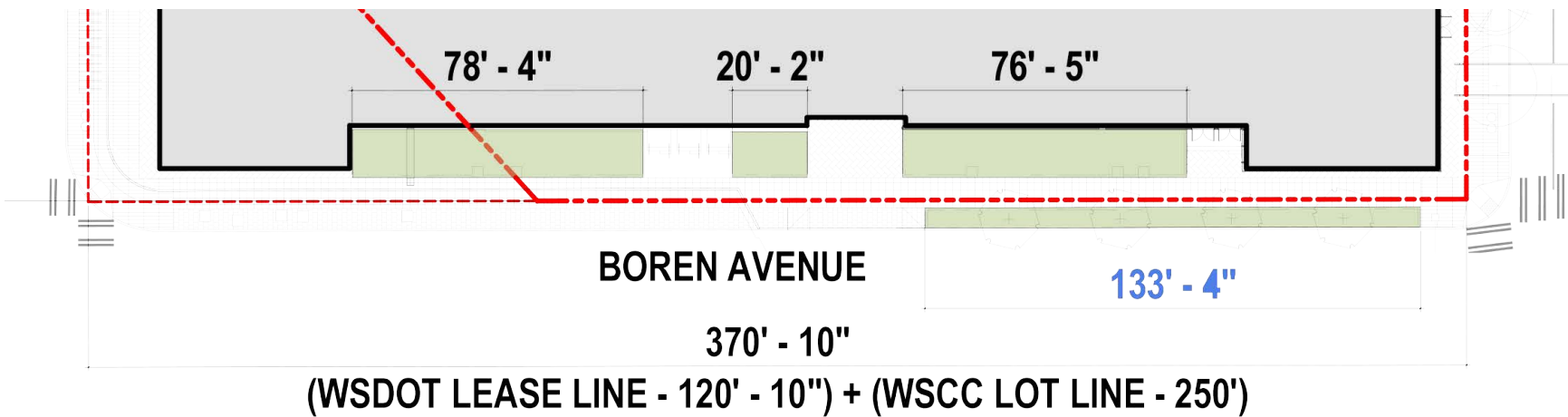
ITEM #	DEVELOPMENT STANDARD	REQUIREMENT	MODIFICATION REQUESTED	RATIONALE
7	23.49.056.F.1 Landscaping In Street Right-Of-Way (Boren Avenue)	<p>Landscape area shall be at least 18 inches wide and shall be located in the public right-of-way along the entire length of the street lot line, except for building entrances, vehicular access or other connections between sidewalk and the lot, provided that the exceptions may not exceed 50% of the total length of the street lot line(s).</p> <p>Total length of landscape required along lot line: 50% of 370.83 LF = 185.42 LF</p> <p>Note: the minimum requirement nets 556.25 SF of landscaped area (1.5 FT x 370.83 FT).</p>	<p>Landscape area is 5'-6" wide and covers 133.33 LF.</p> <p>180.58 LF of the lot line is over WSDOT Highway, and additional load cannot be added to the existing structure. To meet the recommended standards, propose deducting the length of the WSDOT lot line from the calculations.</p> <p>Deducting the LF over WSDOT: 370.83 LF - 180.58 LF = 190.25 LF</p> <p>Total length of landscape required: 50% of 190.25 LF = 95.13 LF</p> <p>Total length of landscape provided is 133.33 LF</p> <p>Note: Counting only the landscape areas adjacent to or beginning within 5 FT of the curb line, the project provides 733 SF of landscape area or 132% of the required minimum based on the full length of the street lot line measured parallel to the street curb line. The is a continuous planter along the length of the building at Level 8.</p>	<p>Per Design Review Guidelines for Downtown Development</p> <p>A1.2: Response to Planning Efforts D2.1: Landscape Enhancements D: Distinctively landscape open areas created by building modulation G: Provide framework such as trellis or arbor for plants to grow on H: Incorporate upper story planter boxes or roof planters</p> <p>Planting design for Boren Avenue maximizes a strong protective edge of planting where possible given the existing constraints of the WSDOT Overpass. Additional planting along the building facade creates a rich two-sided experience that enhances pedestrian connectivity. Additional planting incorporated into the building facade system and a planted terrace above establish a rich green character that supports the larger goals on the Denny Triangle Neighborhood.</p>



4 PROPOSED DEPARTURES - DEPARTURE #7 - Landscaping In Street Right-Of-Way (Boren Avenue) - 23.49.056 F1

DRB#2 - 07-19-2016 - PRESENTATION SLIDE 68

LENGTH OF LANDSCAPE REQUIRED = 185.42 LF
LENGTH OF LANDSCAPE REQUIRED W/ WSDOT AREA DEDUCTED = 125 LF
LENGTH OF LANDSCAPE PROVIDED = 133.33 LF



This departure was approved by the Design Review Board on July 19, 2016.

Red text indicates change in departure.

Length of landscape has not changed. Calculation of required landscape and WSDOT area has been refined.

REQUIREMENT (BOREN AVENUE):
Landscape area shall be at least 18 inches wide and shall be located in the public right-of-way along the entire length of the street lot line, except for building entrances, vehicular access or other connections between sidewalk and the lot, provided that the exceptions may not exceed 50% of the total length of the street lot line(s).

Total length of landscape required along lot line: 50% of 370.83 LF = 185.42 LF

MODIFICATION REQUESTED (BOREN AVENUE):
Landscape area is 5'-6" wide and covers 133.33 LF.

180.58 LF of the lot line is over WSDOT Highway, and additional load cannot be added to the existing structure. To meet the recommended standards, propose deducting the length of the WSDOT lot line from the calculations.

Deducting the LF over WSDOT:
370.83 LF - 180.58 LF = 190.25 LF

Total length of landscape required:
50% of 190.25 LF = 95.13 LF

Total length of landscape provided is 133.33 LF

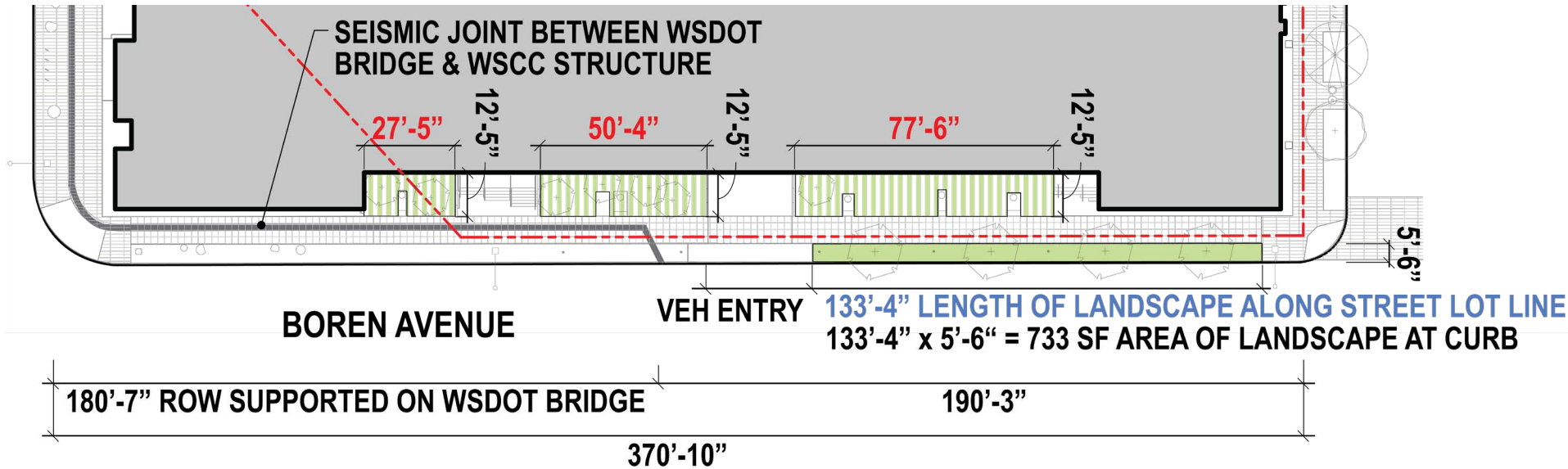
Note: Counting only the landscape areas adjacent to or beginning within 5 FT of the curb line, the project provides 733 SF of landscape area or 132% of the required minimum based on the full length of the street lot line measured parallel to the street curb line. There is a continuous planter along the length of the building at Level 8.

AREAS OF DEPARTURE FROM DEVELOPMENT STANDARD

CHANGE IN DEPARTURE REQUEST

DRB #3 RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G126; L160

LENGTH OF LANDSCAPE REQUIRED = 185.42 LF
LENGTH OF LANDSCAPE REQUIRED W/ WSDOT AREA DEDUCTED = 95.13 LF
LENGTH OF LANDSCAPE PROVIDED = 133.33 LF



SITE PLAN - BOREN AVENUE

4 PROPOSED DEPARTURES - DEPARTURE #8 - Landscaping In Street Right-Of-Way (Olive Way) - 23.49.056 F1

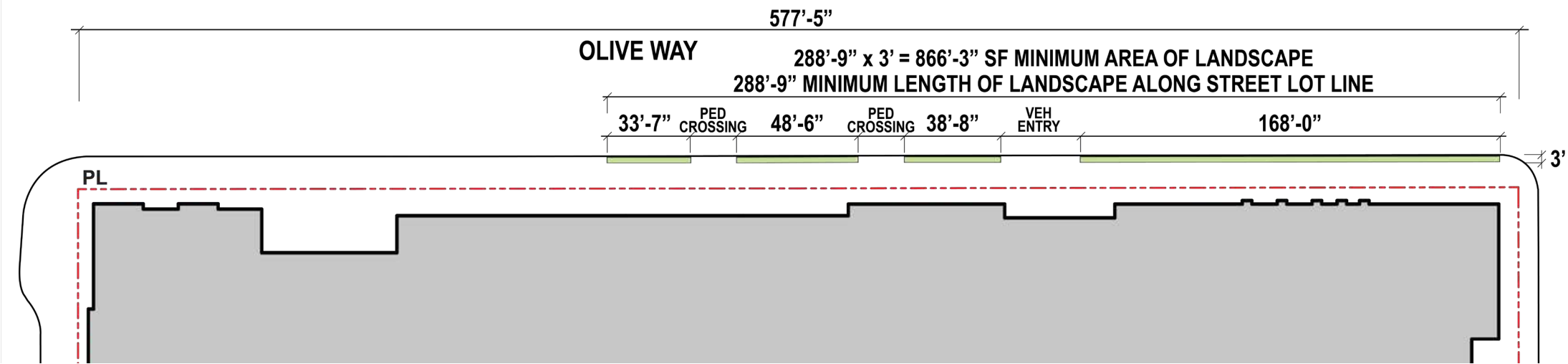
ITEM #	DEVELOPMENT STANDARD	REQUIREMENT	MODIFICATION REQUESTED	RATIONALE
8	23.49.056.F.1 Landscaping In Street Right-Of-Way (Olive Way)	<p>Landscape area shall be at least 18 inches wide and shall be located in the public right-of-way along the entire length of the street lot line, except for building entrances, vehicular access or other connections between sidewalk and the lot, provided that the exceptions may not exceed 50% of the total length of the street lot line(s).</p> <p>Total length of landscape required along lot line: 50% of 577.42 LF = 288.71 LF</p> <p>Note: the minimum requirement nets 866.13 SF of landscaped area (1.5 FT x 577.42 FT).</p>	<p>Landscape area is 6'-6" wide and covers 225.83 LF.</p> <p>The western half of Olive Way will be a major bus stop, and metro requirements for loading/unloading zones prevent continuous planting areas. Street trees are strategically located in the loading zone to minimize interference with passenger movement.</p> <p>Counting only the landscape areas adjacent to or beginning within 5 ft of the curb line, the project provides 1,467.9 SF of landscape area or 169% of the required minimum based on the full length of the street lot line measured parallel to the street curb line.</p> <p>An additional 737.6 SF of planting area within the lot line and directly adjacent to the sidewalk is proposed.</p>	<p>Per Design Review Guidelines for Downtown Development</p> <p>A1.2: Response to Planning Efforts D2.1: Landscape Enhancements D: Distinctively landscape open areas created by building modulation G: Provide framework such as trellis or arbor for plants to grow on H: Incorporate upper story planter boxes or roof planters</p> <p>The planting design for Olive Way responds to the site's transit function. To allow for efficient passenger movement, planting at the bus stop will be limited to street trees. Additional landscaping and benches will be provided for waiting passengers. Additional planting in the building facade directly above establishes a rich green character that supports the larger goals of the Denny Triangle Neighborhood.</p>

4 PROPOSED DEPARTURES - DEPARTURE #8 - Landscaping In Street Right-Of-Way (Olive Way) - 23.49.056 F1

CODE BASELINE

LENGTH OF LANDSCAPE REQUIRED =

288.71 LF



REQUIREMENT (OLIVE WAY):

Landscape area shall be at least 18 inches wide and shall be located in the public right-of-way along the entire length of the street lot line, except for building entrances, vehicular access or other connections between sidewalk and the lot, provided that the exceptions may not exceed 50% of the total length of the street lot line(s).

Total length of landscape required along lot line: 50% of 577.42 LF = 288.71 LF

Note: the minimum requirement nets 866.13 SF of landscaped area (1.5 FT x 577.42 FT).

MODIFICATION REQUESTED (OLIVE WAY):

Landscape area is 6'-6" wide and covers 225.83 LF.

The western half of Olive Way will be a major bus stop, and metro requirements for loading/unloading zones prevent continuous planting areas. Street trees are strategically located in the loading zone to minimize interference with passenger movement.

Counting only the landscape areas adjacent to or beginning within 5 ft of the curb line, the project provides 1,467.9 SF of landscape area or 169% of the required minimum based on the full length of the street lot line measured parallel to the street curb line.

An additional 737.6 SF of planting area within the lot line and directly adjacent to the sidewalk is proposed.

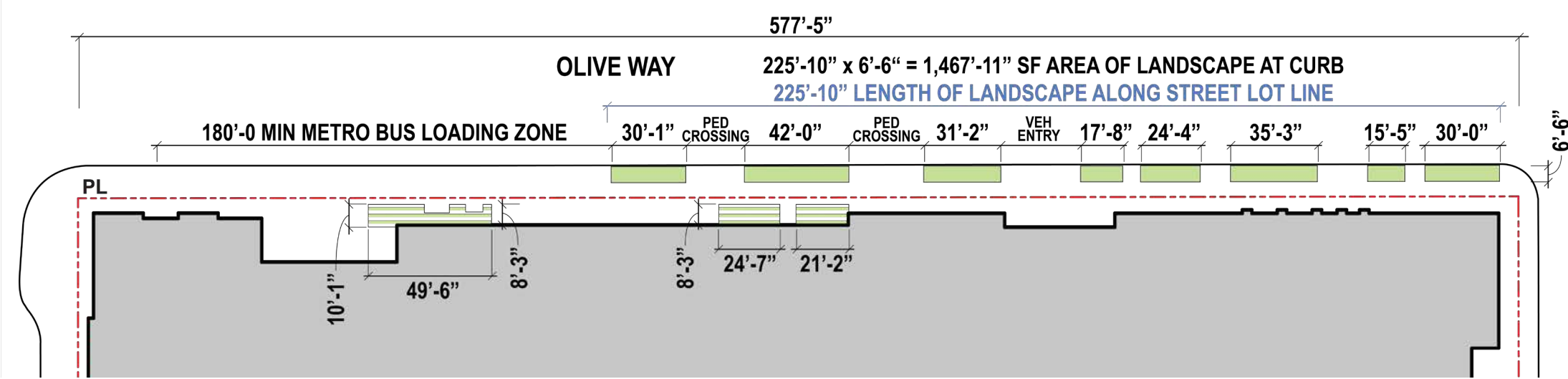
DRB #3 RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G127; L160

LENGTH OF LANDSCAPE REQUIRED =

288.71 LF

LENGTH OF LANDSCAPE PROVIDED =

225.83 LF



AREAS OF DEPARTURE FROM DEVELOPMENT STANDARD

SITE PLAN - OLIVE WAY

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4 PROPOSED DEPARTURES - DEPARTURE #9 - Facade Setback Limits - 23.49.056.B.2

RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G128

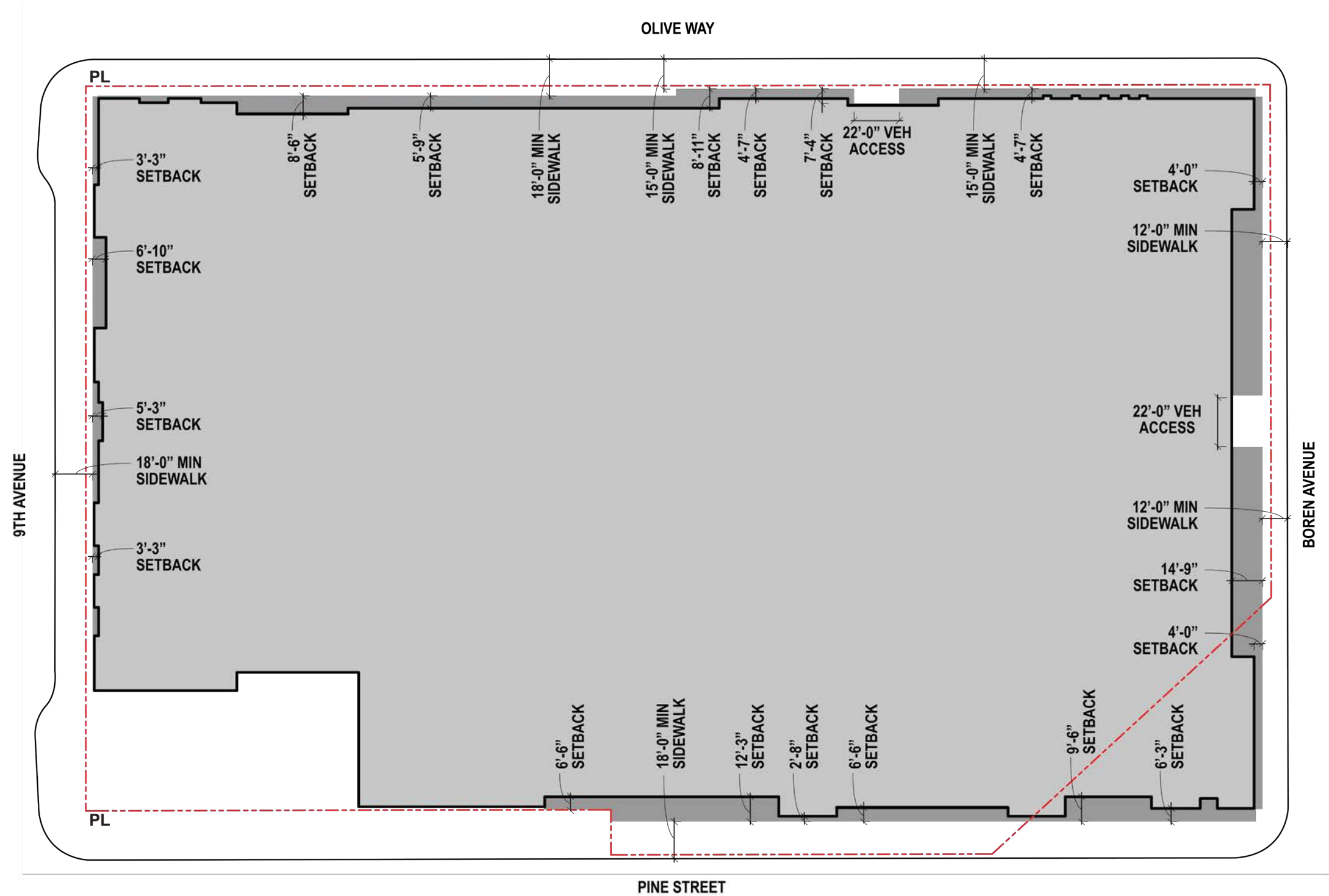
NEW DEPARTURE

ITEM #	DEVELOPMENT STANDARD	REQUIREMENT	MODIFICATION REQUESTED	RATIONALE
9	23.49.056.B.2. Facade Setback Limits	<p>The following setback limits apply on streets not requiring property line facades:</p> <p>A. The portion of a structure subject to setback limits shall vary according to the structure height and required minimum facade height, as follows:</p> <p>1) ... If the structure is greater than 15 feet in height, the setback limits apply to the facade between an elevation of 15 feet above sidewalk grade and the minimum facade height...</p> <p>B. The maximum area of all setbacks between the street lot line and facade along each street frontage shall not exceed the area derived by multiplying the averaging factor by the width of the street frontage of the structure along that street. The averaging factor is five on class i pedestrian streets and ten on class ii pedestrian streets and designated green streets.</p> <p>C. The maximum width, measured along the street lot line, of any setback area exceeding a depth of 15 feet from the street lot line shall not exceed 80 feet, or 30 percent of the lot frontage on that street, whichever is less.</p> <p>D. The maximum setback of the facade from the street lot lines at intersections is 10 feet. The minimum distance the facade must conform to this limit is 20 feet along each street.</p> <p>E. Any exterior public open space that meets The downtown amenity standards, whether it receives a bonus or not, and any outdoor common recreation area required for residential uses, is not considered part of a setback.</p> <p>F. If a sidewalk is widened into the lot as a condition to development, setback standards shall be measured to the line established by the new sidewalk width rather than the street lot line.</p>	Requesting greater facade setback area on Pine Street and Olive Way	<p>The design proposes a greater variety of modulation than that prescribed by the code. This variation of depth and shape extends across significant areas of the facade on Pine Street and Olive Way, providing greater visual interest and a more active facade with retail, seating areas and landscape that exceeds the intent of the facade setback requirements.</p> <p>The design incorporates bold civic scale gestures and fine grain articulation at the pedestrian edge to further modulate the facade. The double block (Terry setback) is articulated to maintain historic urban patterns. Pine St. is articulated on the same scale as it's neighboring Capitol Hill. Additional layering and depth is expressed through a composition of pedestrian circulation, varying facade systems and materials, retail and landscape zones. Places to pause and experience views are created through the relationship between the exterior and interior, entries are highlighted and space is provided for potential bus seating.</p> <p>This departure allows the project to better meet the intent of the design guidelines (described below).</p>

RELEVANT DESIGN GUIDELINES - DEPARTURE #9		
<p>A1.1 Response to context ...Distinct features to which building design should respond</p> <p>C. Patterns of urban form; such as nearby buildings that have employed distinctive and effective massing compositions</p> <p>B3.3 Pedestrian amenities at the ground level Setting portions of the building back slightly to create spaces conducive to pedestrian-oriented activities such as vending, resting, sitting or dining. Further articulate street level facade to provide an engaging experience via:</p> <p>I. Street furniture and signage systems</p> <p>J. Lighting and landscaping</p> <p>K. Overhead weather protection</p> <p>C1.3 Street level uses</p> <p>A. Reinforce existing retail concentrations</p> <p>B. Vary in size, width and depth</p> <p>C. Enhance main pedestrian links between areas</p> <p>D. Establish new pedestrian activity</p>	<p>C1.2 Retail orientation</p> <p>C1.3 Street level articulation and pedestrian activity</p> <p>E. Open facades</p> <p>F. Multiple building entries</p> <p>G. Windows/merchandising</p> <p>H. Merchandising display windows</p> <p>I. Open space with landscape or art features</p> <p>J. Interesting exterior finish materials</p> <p>D1.1 Pedestrian enhancements</p> <p>A. Open space elements should enhance pedestrian oriented urban environment</p> <p>B. Preferable open space to south and west of tower development</p> <p>C. Orient public open space to receive maximum direct sunlight, provide shading</p> <p>D. Landscape design should allow visibility into and out of the open space</p>	<p>D1.2 Open space features</p> <p>A. Visual and pedestrian access into the site from sidewalk</p> <p>B. Walking surfaces of attractive pavers</p> <p>D. Retail spaces that “spill out”</p> <p>F. Landscaping that enhances space</p> <p>H. Site furniture, art, amenities and seating</p> <p>D2.1 Landscape enhancements</p> <p>A. Emphasize entries with special planting, paving and/or lighting</p> <p>D. Landscape open areas created by building modulation</p> <p>I. Provide identity and reinforce desired feeling of intimacy and quiet</p>

4 PROPOSED DEPARTURES - DEPARTURE #9 - Facade Setback Limits - 23.49.056.B.2

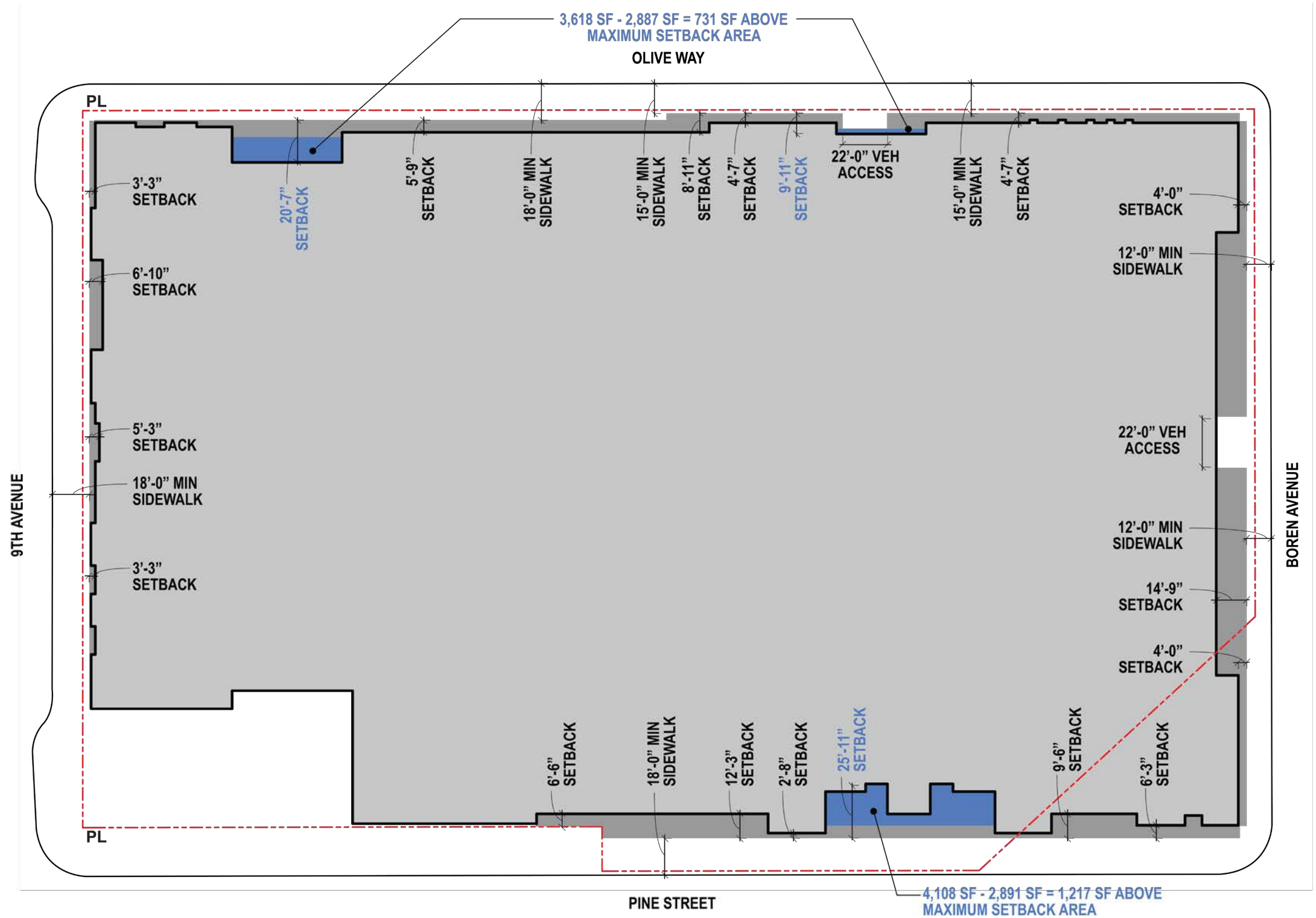
CODE BASELINE



SITE PLAN

4 PROPOSED DEPARTURES - DEPARTURE #9 - Facade Setback Limits - 23.49.056.B.2

DRB #3 RESPONSE TO MUP CORRECTION NOTICE #3 - 11-17-2017 - G128



SITE PLAN

REQUIREMENT:
The following setback limits apply on streets not requiring property line facades:
A. The portion of a structure subject to setback limits shall vary according to the structure height and required minimum facade height, as follows:
1) ... If the structure is greater than 15 feet in height, the setback limits apply to the facade between an elevation of 15 feet above sidewalk grade and the minimum facade height...
B. The maximum area of all setbacks between the street lot line and facade along each street frontage shall not exceed the area derived by multiplying the averaging factor by the width of the street frontage of the structure along that street. The averaging factor is five on Class I pedestrian streets and ten on Class II pedestrian streets and designated green streets.
C. The maximum width measured along the street lot line of any setback area exceeding a depth of 15 feet from the street lot line shall not exceed 80 feet, or 30 percent of the lot frontage on that street, whichever is less.
D. The maximum setback of the facade from the street lot lines at intersections is 10 feet. The minimum distance the facade must conform to this limit is 20 feet along each street.
E. Any exterior public open space that meets the Downtown Amenity Standards, whether it receives a bonus or not, and any outdoor common recreation area required for residential uses, is not considered part of a setback.
F. If a sidewalk is widened into the lot as a condition to development, setback standards shall be measured to the line established by the new sidewalk width rather than the street lot line.

MODIFICATION REQUESTED:
Requesting greater facade setback area on Pine Street and Olive Way

FACADE SETBACK (23.49.056.B.2)		
STREET	MAX SETBACK AREA	PROPOSED SETBACK AREA
PINE STREET	2,891 SF	4,108 SF
OLIVE WAY	2,887 SF	3,618 SF
9TH AVENUE	3,532 SF	1,123 SF
BOREN AVENUE	3,748 SF	3,348 SF

AREAS OF DEPARTURE FROM DEVELOPMENT STANDARD

4 PROPOSED DEPARTURES - DEPARTURE #10 - Parking Aisles - 23.54.030.E

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4 PROPOSED DEPARTURES - DEPARTURE #10 - Parking Aisles - 23.54.030.E

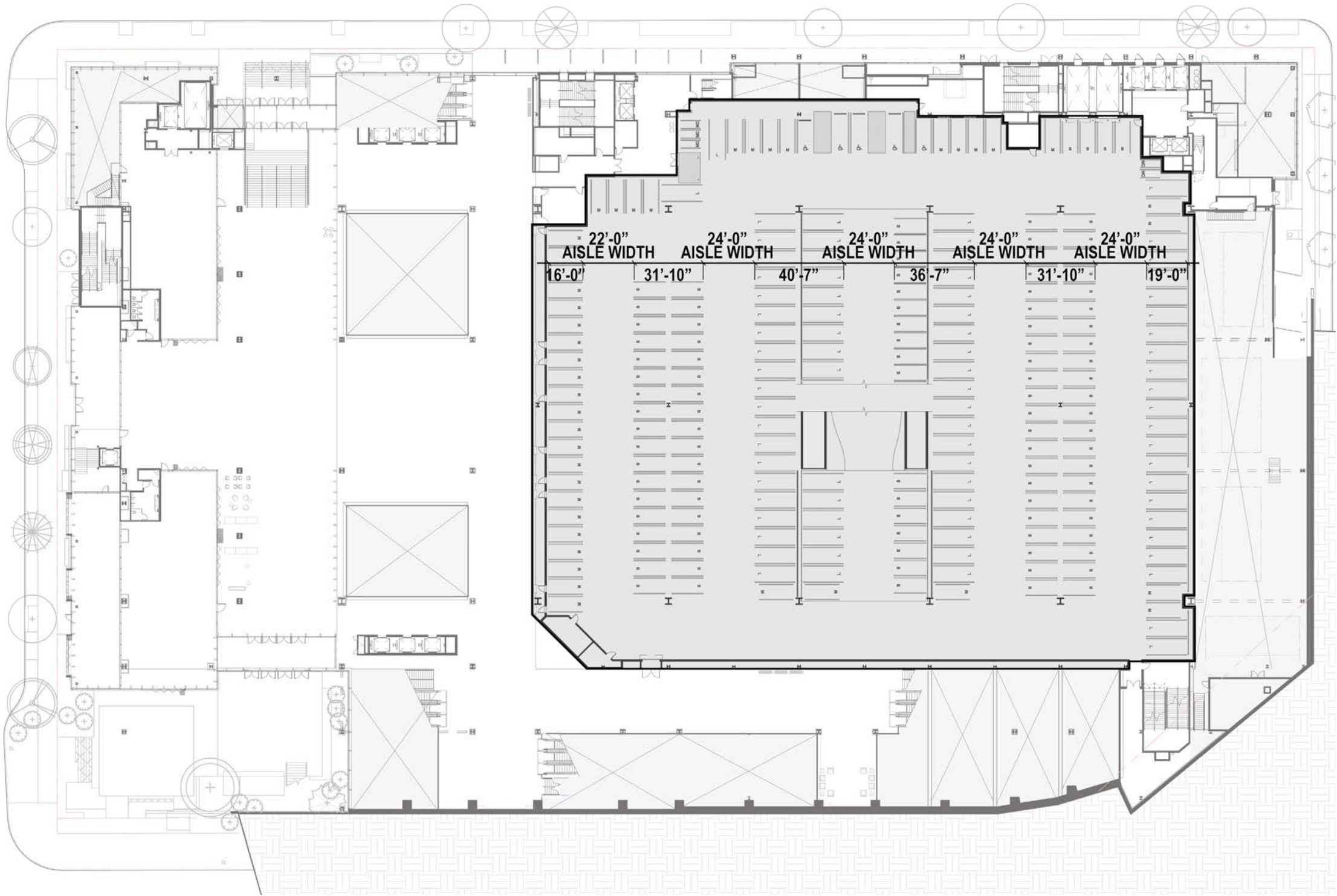
DRB #3

NEW DEPARTURE

ITEM #	DEVELOPMENT STANDARD	REQUIREMENT	MODIFICATION REQUESTED	RATIONALE																		
10	23.54.030.E Exhibit C Parking Aisles	<p>Parking aisles shall be provided according to the requirements of Exhibit C for 23.54.030:</p> <table><tr><th>A</th><th>B</th><th>C</th><th>D</th></tr><tr><th>PARKING ANGLE</th><th>STALL WIDTH</th><th>STALL LENGTH</th><th> AISLE WIDTH</th></tr><tr><td rowspan="3">90 DEGREES</td><td>7.5</td><td>15.0</td><td>20.0</td></tr><tr><td>8.0</td><td>16.0</td><td>22.0</td></tr><tr><td>8.5</td><td>19.0</td><td>24.0</td></tr></table> <p>Minimum aisle widths shall be provided for the largest vehicles served by the aisle.</p> <p>Turning and maneuvering areas shall be located on private property, except that alleys may be credited as aisle space.</p> <p>Aisle slope shall not exceed 17 percent provided that the Director may permit a greater slope if the criteria in subsections 23.54.030.D.3.a, 23.54.030.D.3.b, and 23.54.030.D.3.c are met.</p>	A	B	C	D	PARKING ANGLE	STALL WIDTH	STALL LENGTH	AISLE WIDTH	90 DEGREES	7.5	15.0	20.0	8.0	16.0	22.0	8.5	19.0	24.0	Requesting 2.5% reduced parking aisle widths within structured parking area.	The design proposes minor reduction in aisle width in order to reduce the overall footprint and impact of parking within the structure. Reduction on parking area contributes to the clarity of the building structure, informing the overall building form and facade articulation.
A	B	C	D																			
PARKING ANGLE	STALL WIDTH	STALL LENGTH	AISLE WIDTH																			
90 DEGREES	7.5	15.0	20.0																			
	8.0	16.0	22.0																			
	8.5	19.0	24.0																			
RELEVANT DESIGN GUIDELINES - DEPARTURE #10																						
<p>B4.2 Coherent Interior/Exterior Design H. building garage and entries</p> <p>E2.1. Integrate parking facilities Minimize the visibility of at-grade parking structures or accessory parking garages. The parking portion of a structure should be architecturally compatible with the rest of the building and streetscape.</p> <p>E2.2 Parking Structure Entrances Design vehicular entries to parking structure so that they do not dominate the street frontage of a building. Subordinate the garage entrance to the pedestrian entrance in terms of size, prominence on the street-scape, location, and design emphasis. J. Recess the garage entry portion of the facade or extend portions of the structure over the garage entry to help conceal it.</p>																						

4 PROPOSED DEPARTURES - DEPARTURE #10 - Parking Aisles - 23.54.030.E

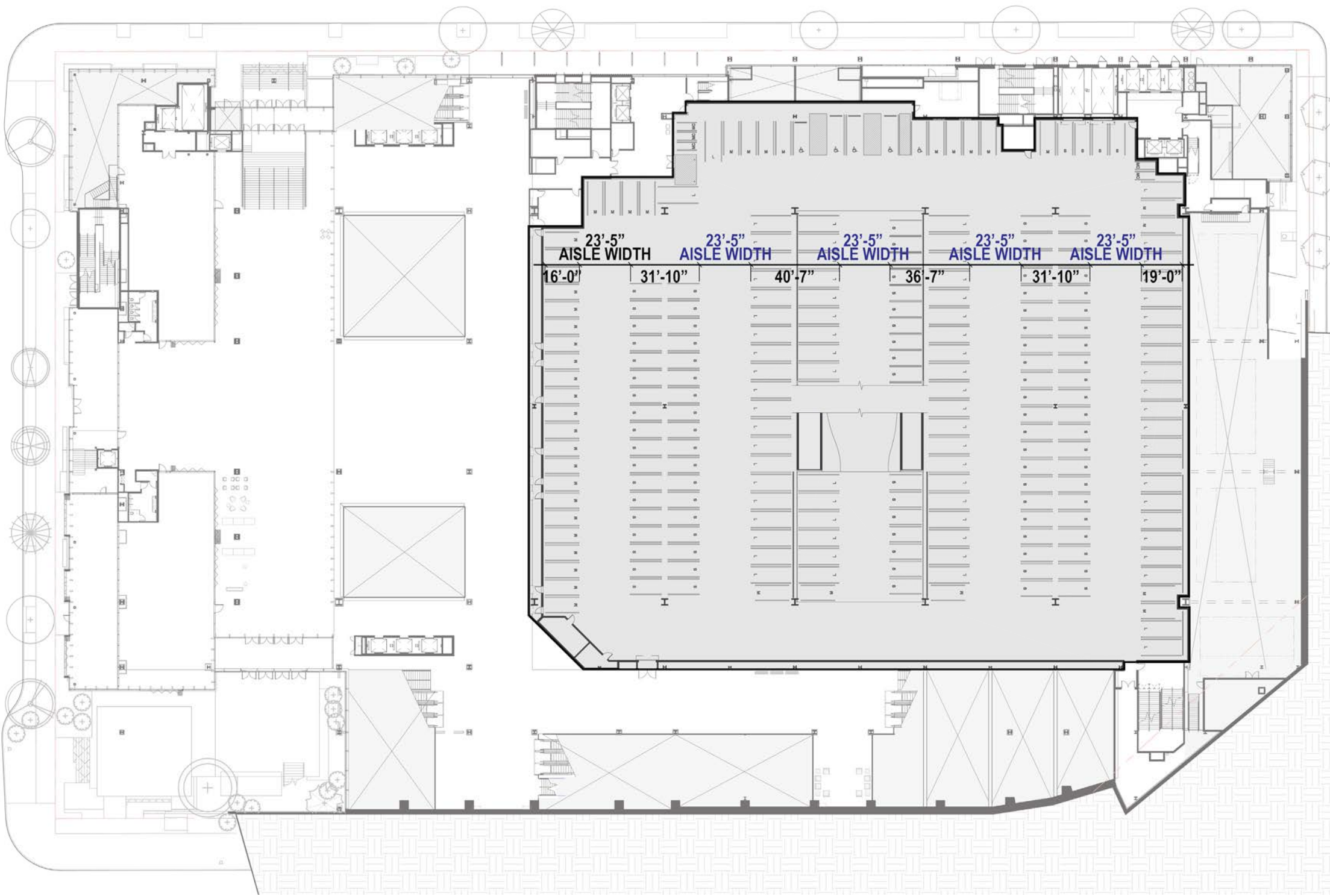
CODE BASELINE



LEVEL 2 - TYPICAL PARKING LAYOUT

4 PROPOSED DEPARTURES - DEPARTURE #10 - Parking Aisles - 23.54.030.E

DRB #3



LEVEL 2 - TYPICAL PARKING LAYOUT

REQUIREMENT:
 Parking aisles shall be provided according to the requirements of Exhibit C for 23.54.030:

A	B	C	D
PARKING ANGLE	STALL WIDTH	STALL LENGTH	AISLE WIDTH
90 DEGREES	7.5	15.0	20.0
	8.0	16.0	22.0
	8.5	19.0	24.0

Minimum aisle widths shall be provided for the largest vehicles served by the aisle.

Turning and maneuvering areas shall be located on private property, except that alleys may be credited as aisle space.

Aisle slope shall not exceed 17 percent provided that the Director may permit a greater slope if the criteria in subsections 23.54.030.D.3.a, 23.54.030.D.3.b, and 23.54.030.D.3.c are met.

MODIFICATION REQUESTED:
 Requesting 2.5% reduced parking aisle widths within structured parking area.

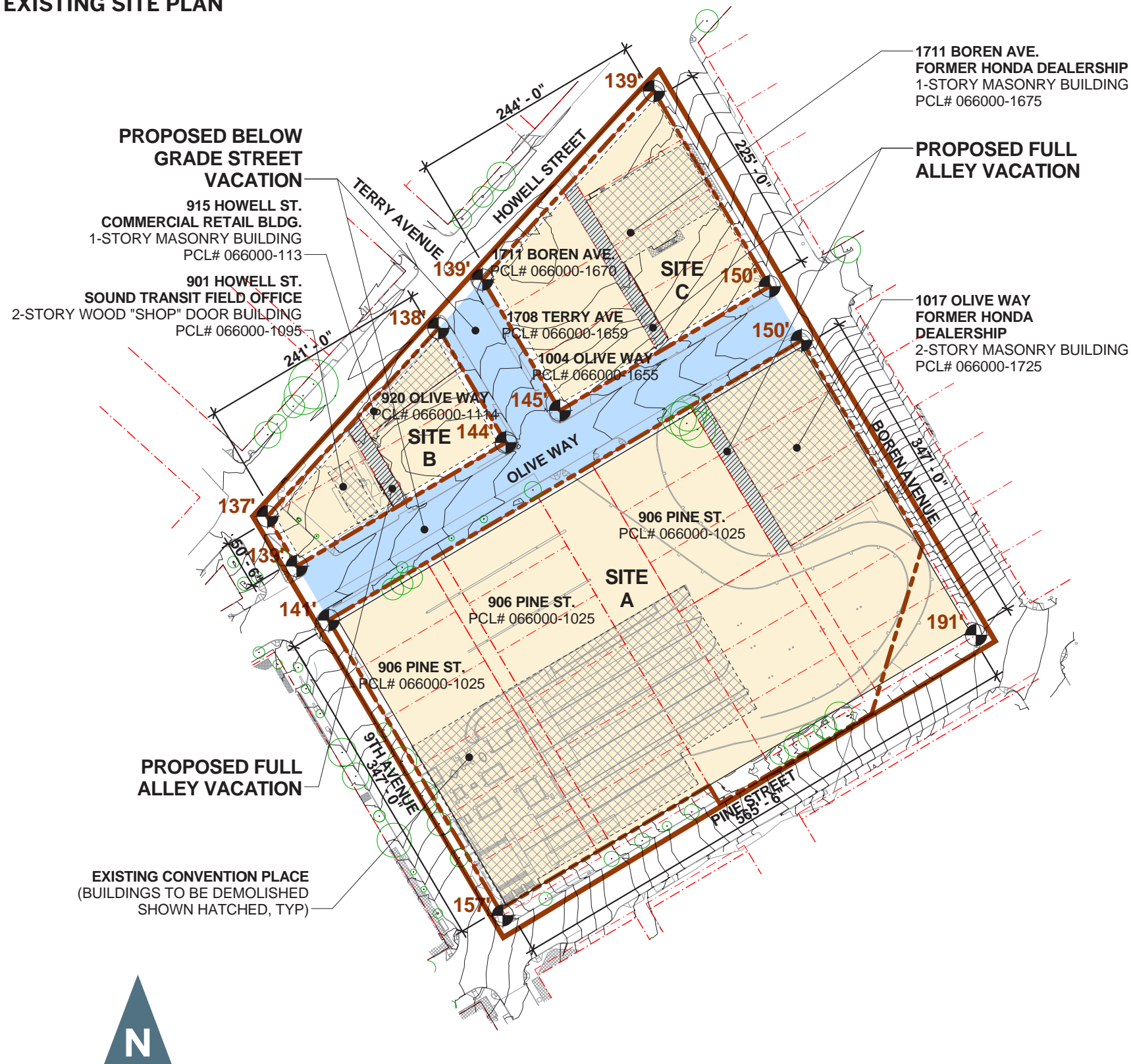
AREAS OF DEPARTURE FROM DEVELOPMENT STANDARD

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5 APPENDIX - DEVELOPMENT OBJECTIVES & PROJECT BACKGROUND

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EXISTING SITE PLAN



EXISTING SITE AREA

The project site consist of 3 blocks bounded by Howell Street to the north, Pine Street to the south, 9th Avenue to the west, and Boren Avenue and I-5 to the east. Terry Avenue and Olive Way divide the site in the interior. The site slopes significantly from the highest point at the intersection of Boren Avenue and Pine Street where they cross over 1-5 on the southeast corner of the site to the lowest point at the intersection of 9th Avenue and Howell Street on the northwest corner of the site.

Proposed vacations include remaining alleys on Sites A, B, & C, as well as subterranean vacations of Terry Avenue and Olive Way.

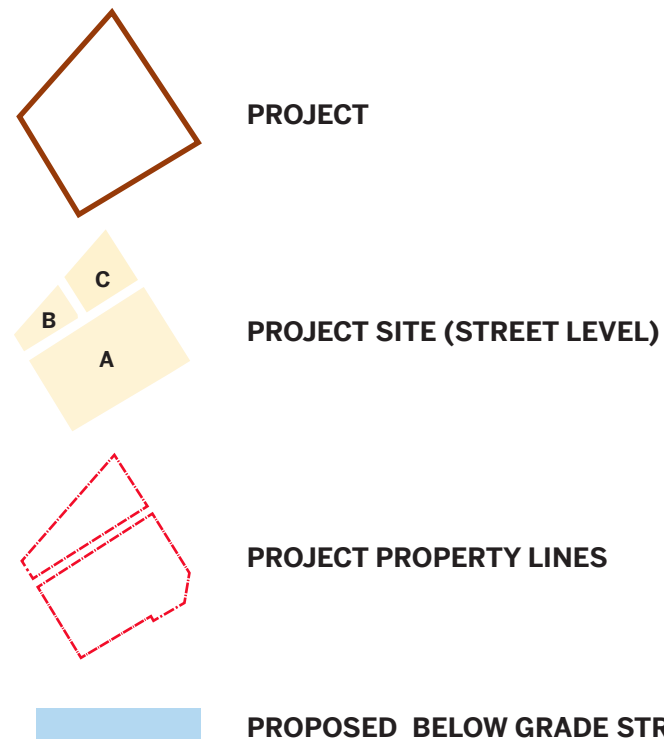
EXISTING BUILDINGS

The current use on the site is the Convention Place Station. There is also a vacant former Honda Dealership on Site A. Sites B and C also contain vacant former Honda dealership facilities and a small commercial retail building.

EXISTING LANDSCAPE

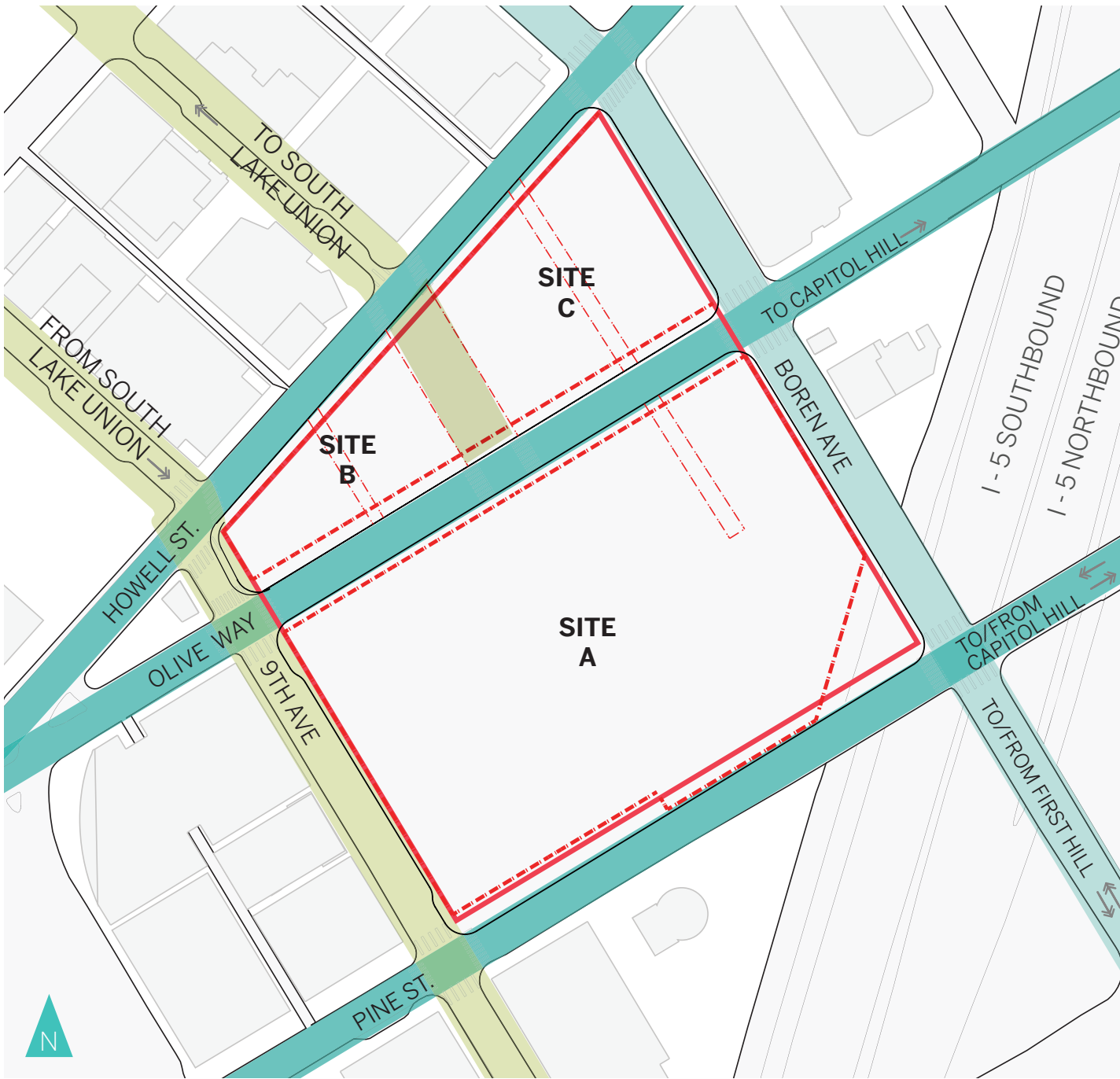
The Arborist collected data (06/25/2015) on all trees of significant size, 6" and greater. The research concluded that no exceptional trees are located on the site.

DOCUMENT KEY



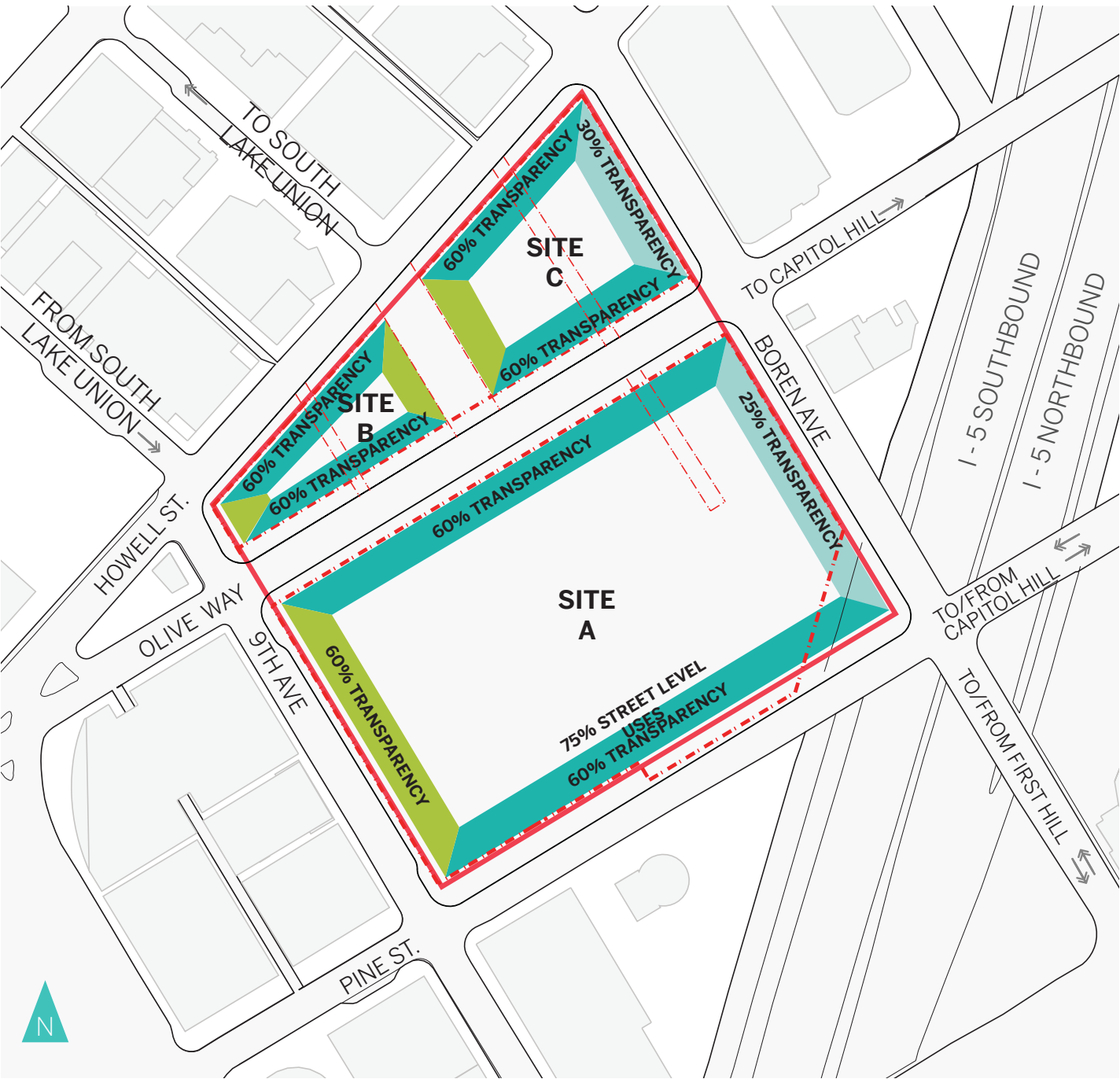
5 APPENDIX - DEVELOPMENT OBJECTIVES & PROJECT BACKGROUND

STREET CLASSIFICATIONS SUMMARY



- PRINCIPAL TRANSIT STREET
- PRINCIPAL ARTERIAL
- GREEN STREET
- PROJECT AREA
- PROPERTY LINE
- PROPOSED FULL ALLEY VACATION

FRONTAGE CLASSIFICATION SUMMARY



- CLASS I STREET FRONTAGE
- CLASS II STREET FRONTAGE
- GREEN STREET FRONTAGE
- PROJECT AREA
- PROPERTY LINE
- PROPOSED FULL ALLEY VACATION

DOWNTOWN OVERLAY MAPS Map 1B Street Classifications

NOTE Landscape Requirement per Denny Triangle Urban Center Village Code applies to all frontages

5 APPENDIX - DEVELOPMENT OBJECTIVES & PROJECT BACKGROUND

PROPERTY ADDRESSES

SITE A	SITE B	SITE C
1600 9th Avenue	920 Olive Way	1711 Boren Avenue

KING COUNTY PARCEL NUMBERS

SITE A	SITE B	SITE C
#0660001025	#0660001095	#0660001655
#0660001700	#0660001113	#0660001659
#0660001725	#0660001114	#0660001670
		#0660001675

ZONING SMC 23.49.056 MAP A

SITE A/B/C
Denny Triangle Urban Center Village
Downtown Mixed Commercial – DMC 340/290-400

SITE AREA

SITE A	SITE B	SITE C
202,509 SF*	25,551 SF*	50,979 SF*

* Includes vacated alleys

DESIGN REVIEW SMC 23.41 Required

STRUCTURE HEIGHT 23.49.008

NON-RESIDENTIAL MAXIMUM HEIGHT

340ft

RESIDENTIAL MAXIMUM HEIGHT

290-400ft depending on incentives

ROOFTOP FEATURES

15ft above the applicable height limit.

STREET-LEVEL USE 23.49.009 MAP 1G

PINE STREET – REQUIRED*

OLIVE WAY – Not Required

9TH AVENUE – Not Required

* Minimum 75% of each street frontage must be occupied by qualifying uses & located within 10 ft of the street property line or line established by the new sidewalk width

One or more of the uses listed in subsection 23.49.009.A are required at street level on all lots abutting streets designated on Map 1G. Required street-level uses shall meet the standards of this Section 23.49.009.

The following uses qualify as required street-level uses:

- General sales and services;
- Human service uses and child care centers;
- Retail sales, major durables;
- Entertainment uses**;
- Museums, and administrative offices within a museum expansion space meeting the requirement of subsection 23.49.011.B.1.h;
- Libraries;
- Elementary and secondary schools, and colleges, except on lots zoned DRC;
- Public atria;
- Eating and drinking establishments;
- Arts facilities; and
- Religious facilities; and
- Bicycle parking, provided that the use does not exceed 30 percent of the frontage 23.49.009.B or 50 feet, whichever is less.

** 23.84A.010 - “E”

“Entertainment use” means a commercial use in which recreational, entertainment, athletic, and/or cultural opportunities are provided for the general public, either as participants or spectators. Entertainment uses include the following uses:

6.a - “Lecture and meeting hall” means a theater and spectator sports facility intended and expressly designed for public gatherings such as but not limited to commercial spaces available for rent or lease for the purpose of holding meetings or the presentation of public speeches.

FLOOR AREA RATIO 23.49.011 SMC-CHART A1

DMC 340/290-400
FAR Base = 5
FAR Max = 10

TOTAL
SITE A + SITE B + SITE C = 279,039

279,039 x 10 (Max FAR) = 2,790,390 sq ft

OVERHEAD WEATHER PROTECTION & LIGHTING 23.49.018

Required along the entire street frontage facade located within 5ft of property line or widened sidewalk except: where separate by landscaped areas at least two feet in width, or at driveways into structures of loading docks. Lower ledge must be between 10ft and 15ft above the sidewalk.

DENNY TRIANGLE URBAN CENTER VILLAGE 23.49.056F

Provide landscaping in sidewalk area of the right of way as a square footage of 1.5 times the length of the street lot line. Must be 18” wide, along entire length of street lot line, except at building entrances, vehicular access (not to exceed 50% of the length of the lot line.

In addition, a 2ft wide setback from the street lot line is required along Terry & 9th Green Streets within the Denny Triangle Urban Center Village. Averaging may be allowed. 50% of the setback area shall be landscaped.

PARKING 23.49.019

MINIMUM REQUIRED
None

PROPOSED

717 stalls

NON-RESIDENTIAL MAXIMUM ALLOWED
1 per 1000 SF except with special exception.

PARKING LOCATION WITHIN STRUCTURES

Parking above street level is permitted if separated along all street frontages of the structure by another use.

Parking at street level is permitted if separated by other uses on Class 1 Pedestrian Streets, and at least 30% separated by other uses on Class 11 Pedestrian Streets.

ACCESSORY PARKING

Permitted outright in areas shown on Map 1I if they contain a total of 20 or fewer parking spaces on the lot.
23.49.045

BICYCLE PARKING

Bicycle parking required 1 space per 5,000SF of gross floor area of office or retail over 10,000 SF. Shower facility required for structures containing 250,000 GFA of office use.

1 space for every 2 dwelling units of residential use.

CURB CUT LOCATION

Per Director as a Type 1 decision SMC 23.49.019.H.1.c

LOADING BERTHS

Off-street loading berths required per SMC 23.54.035
TABLE A

5 APPENDIX - DEVELOPMENT OBJECTIVES & PROJECT BACKGROUND

MINIMUM SIDEWALK WIDTH 23.49.022 MAP 1C

PINE STREET – 18 FT (When on a one-way street, only the side with transit stops shall be 18ft, the other side shall be 15ft.)

PINE STREET PROPOSED WIDTH - ~20 FT

OLIVE WAY – 9TH AVE TO TERRY AVE - 15-18 FT (When on a one-way street, only the side with transit stops shall be 18ft, the other side shall be 15ft.)

OLIVE WAY – 9TH AVE TO TERRY AVE
PROPOSED WIDTH - ~19 FT

OLIVE WAY - TERRY AVE TO BOREN AVE - 12 FT

OLIVE WAY – TERRY AVE TO BOREN AVE
PROPOSED WIDTH - ~16 FT

BOREN AVENUE – 12 FT

BOREN AVE PROPOSED WIDTH - ~14 FT

9TH AVENUE – VARIABLE, Green Street has additional requirements per Denny Triangle Urban Center Village Downtown Code**

9TH AVE PROPOSED WIDTH - 18 FT

** SEE DENNY TRIANGLE URBAN CENTER VILLAGE-
23.49.056F (PG. 10)

STREET FACADE & STREET SETBACKS 23.49.056 MAP 1F

PINE STREET – Class I

OLIVE WAY – Class I

BOREN STREET – Class II

9TH AVENUE – GREEN STREET

FACADE TRANSPARENCY REQUIREMENTS SMC 23.49.056C

Class I & Green Streets = minimum 60%
Class II Streets = minimum 30%

BLANK FACADE LIMITS SMC 23.49.056D

Class I & Green Streets = 15ft max
Class II Streets = 30ft max

FACADE SETBACK LIMITS SMC 23.49.56 B

If the structure is greater than 15 feet in height, the setback limits apply to the portion facade between an elevation of 15 feet above sidewalk grade and the minimum facade height established in subsection 23.49.056.A.

The maximum area of all setbacks between the street lot line and facade along each street frontage = (averaging factor) x (width of street frontage).

The averaging factor is five (5) on Class I pedestrian streets and ten (10) on Class II pedestrian streets and designated green streets.

The Maximum setback of the facade from the street lot lines at intersections is 10 feet. The minimum distance the facade must conform to this limit is 20 feet along each street.

DOWNTOWN AMENITY STANDARDS SEPTEMBER 2014

L. Urban Plaza
Urban plazas are relatively large, strategically located open spaces that denote important downtown places, create a public focus for surrounding development, increase access to light and air at street level, and provide points of orientation within downtown.
Eligibility Condition: 6,000SF min

UPPER-LEVEL DEVELOPMENT STANDARDS 23.49.058

NON-RESIDENTIAL USE ABOVE 160 FT IN HEIGHT

Green Street Setback
9th Avenue & Terry Avenue Continuous upper-level setback of 15ft on the street frontage abutting the green street at a height of 45ft

Facade Modulation
Required above 85ft from the sidewalk for any portion of a structure located within 15ft of a street property line.

Maximum Facade Width
0-85ft = No Limit
86-160ft = 155ft
161-240ft = 125ft
241-500ft = 100ft

UPPER LEVEL WIDTH LIMIT
For portions of structures in non-residential use above 160ft where any story above 85ft exceeds 15,000SF. Upper-level width limit is required on lots that exceed 200ft in width and depth. Any portion of a building above 240ft shall be 145ft along the general n/s axis of a site (parallel to the Avenues). This portion shall be separated horizontally from any other portion by 80ft at all points.

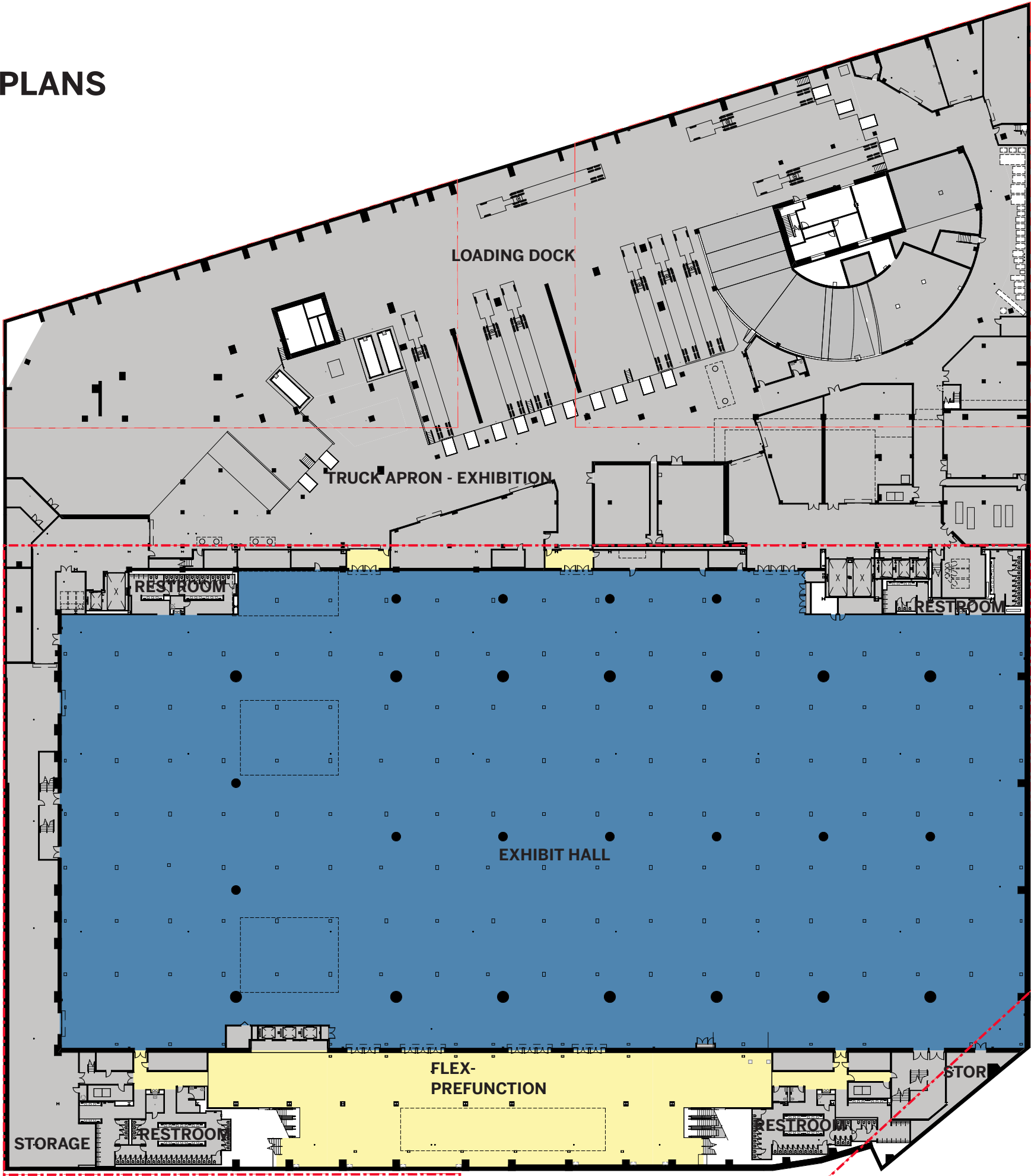
5 APPENDIX - FLOOR PLANS

SITE PLAN



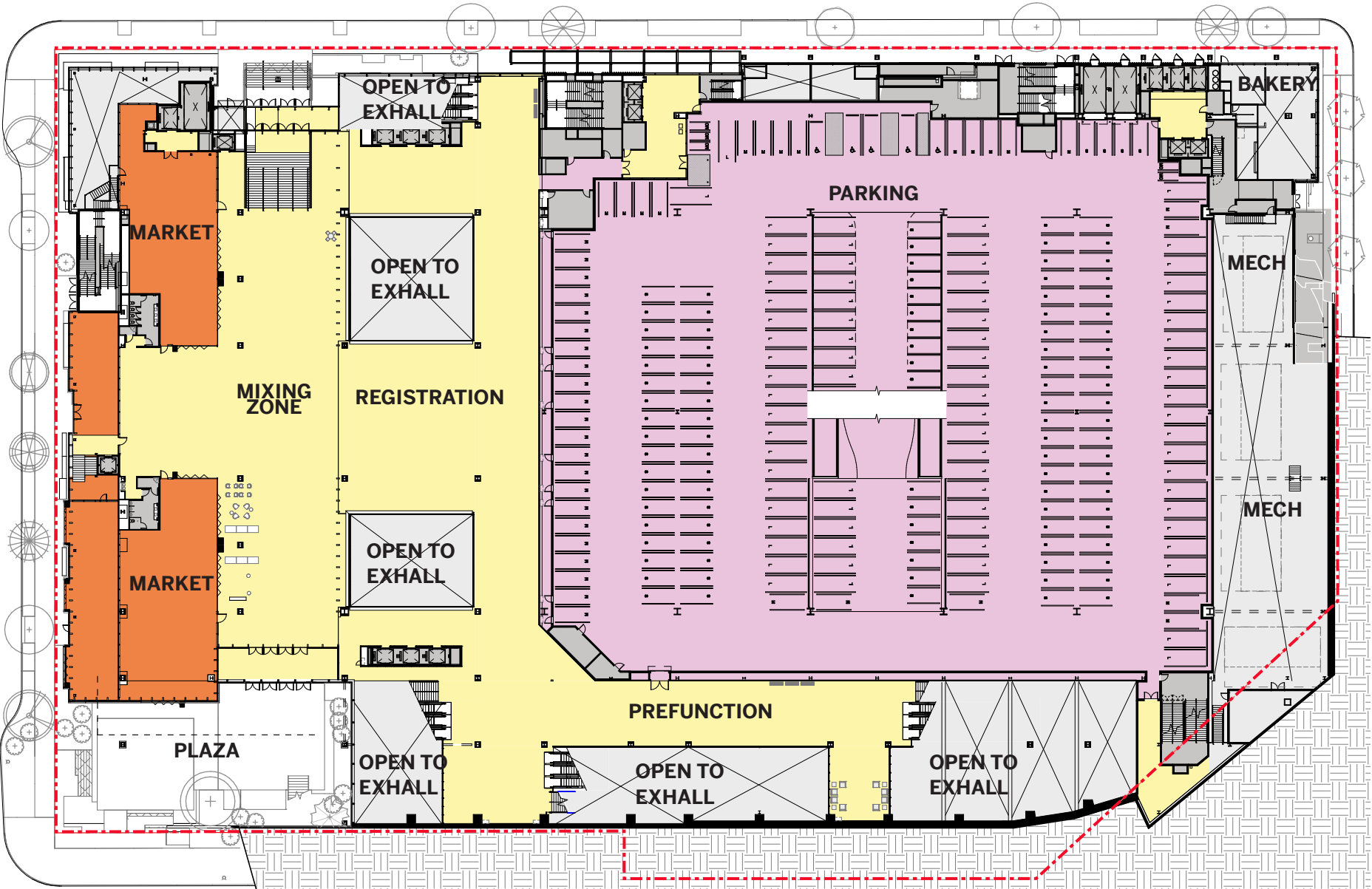
5 APPENDIX - FLOOR PLANS

LEVEL B2



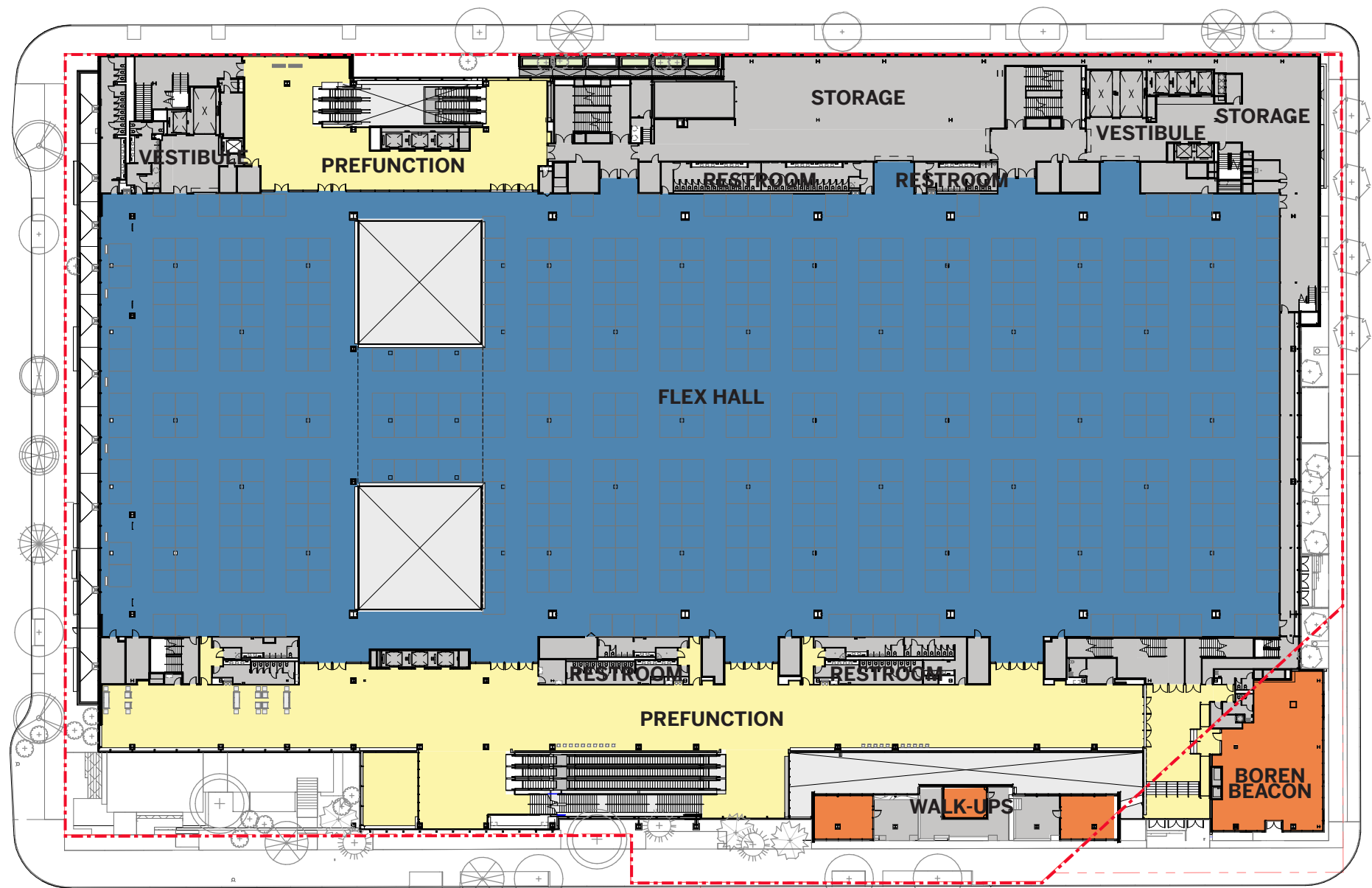
5 APPENDIX - FLOOR PLANS

LEVEL 2



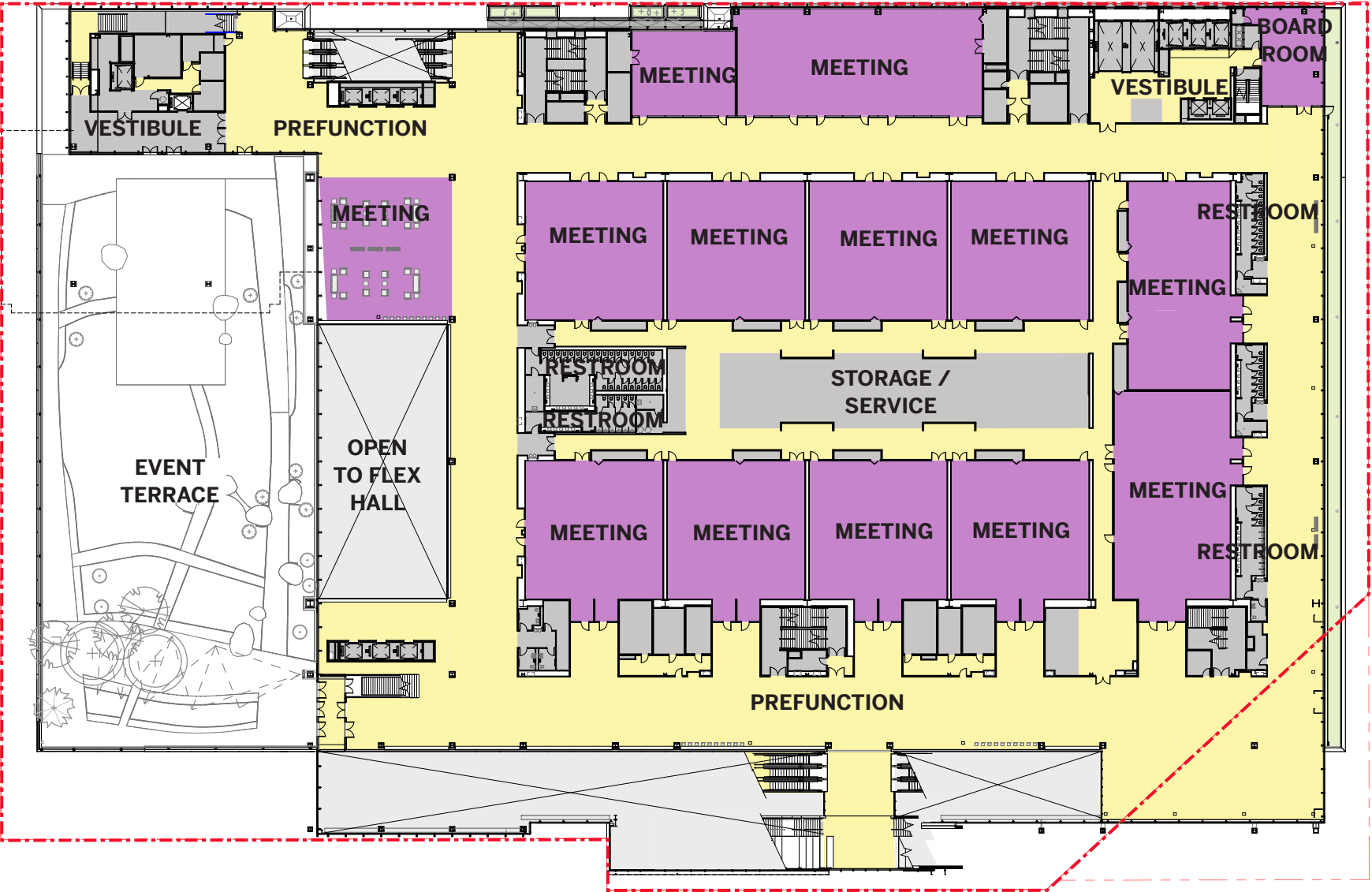
5 APPENDIX - FLOOR PLANS

LEVEL 5



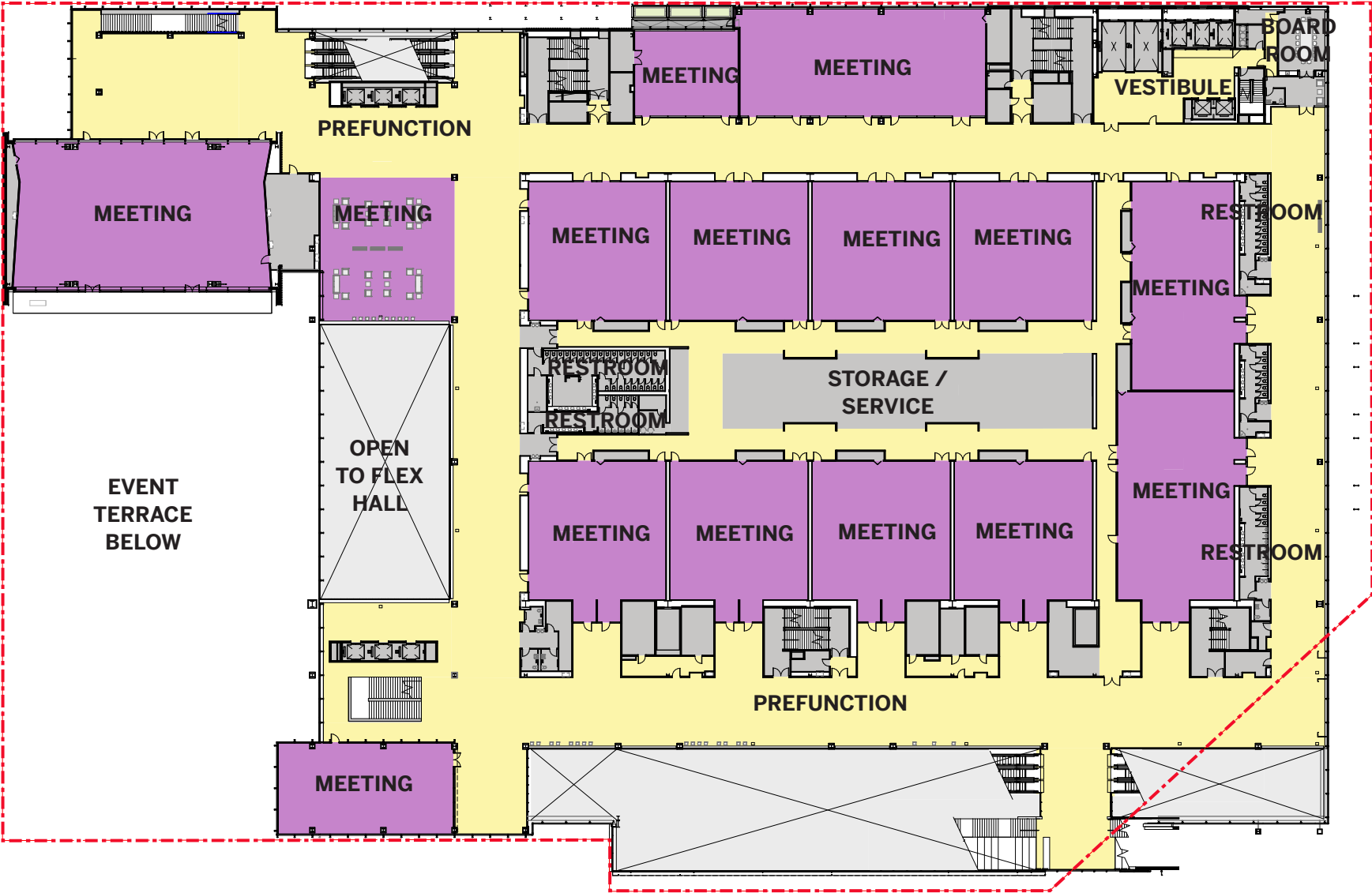
5 APPENDIX - FLOOR PLANS

LEVEL 8



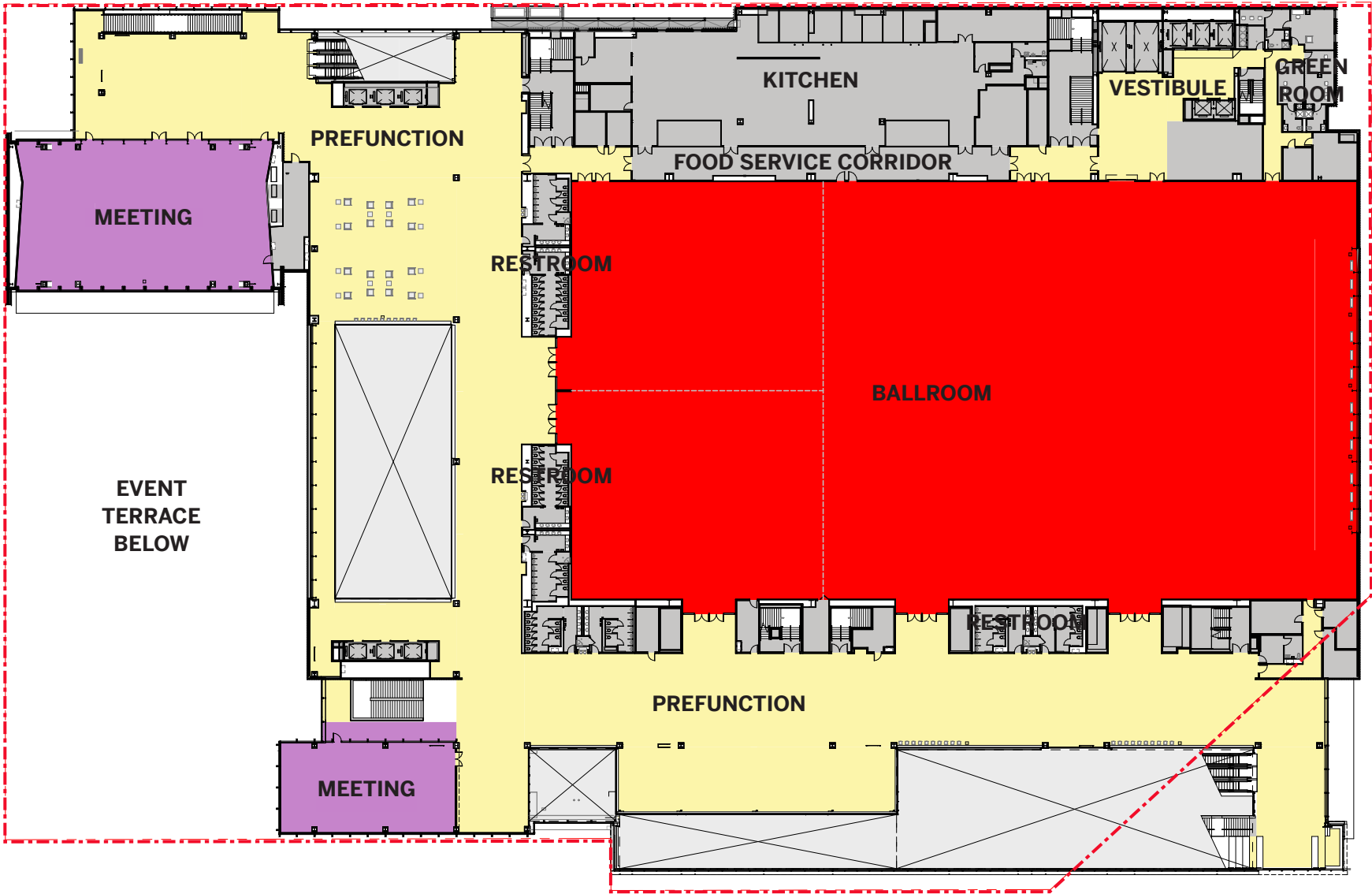
5 APPENDIX - FLOOR PLANS

LEVEL 9



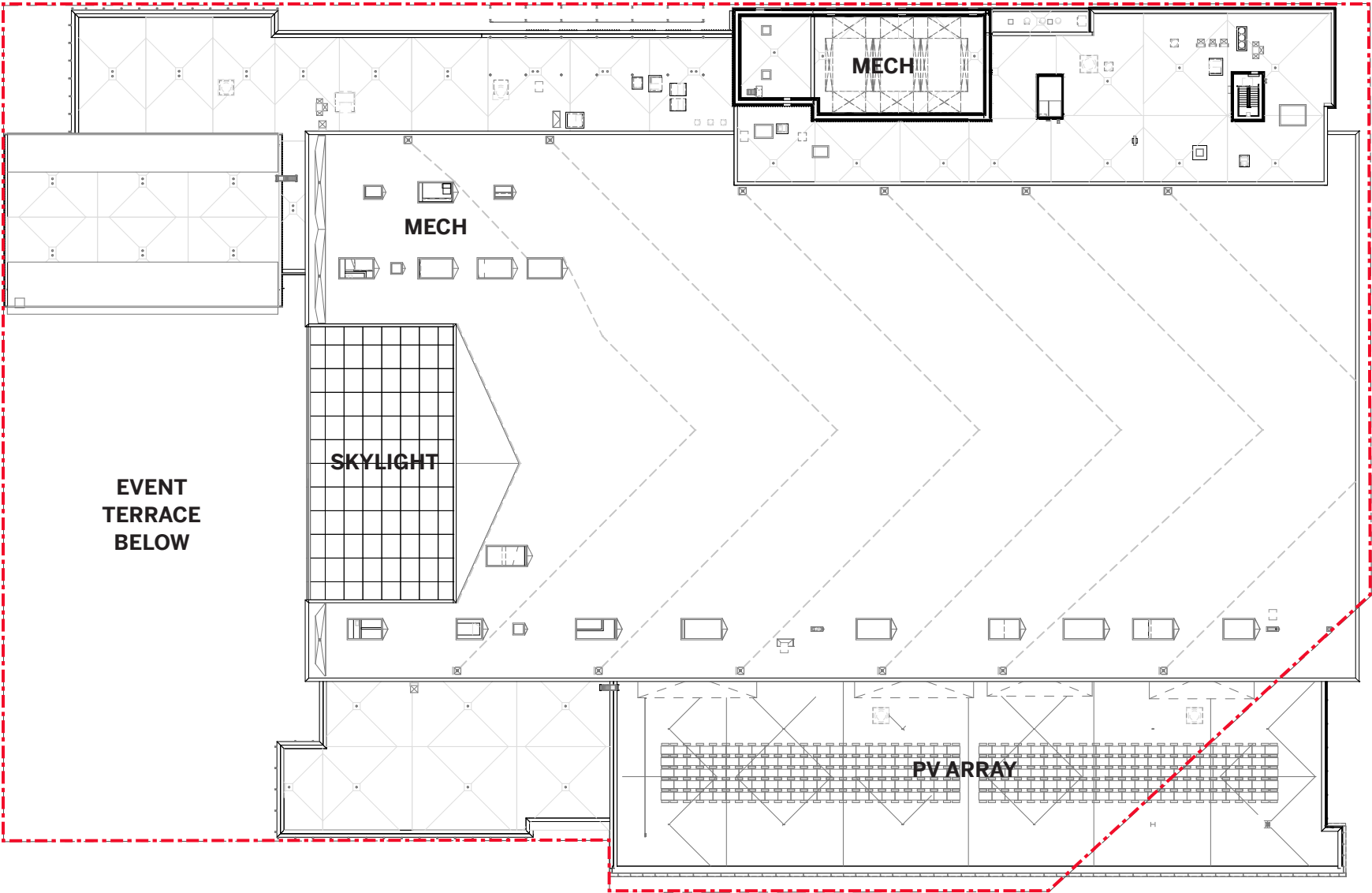
5 APPENDIX - FLOOR PLANS

LEVEL 10

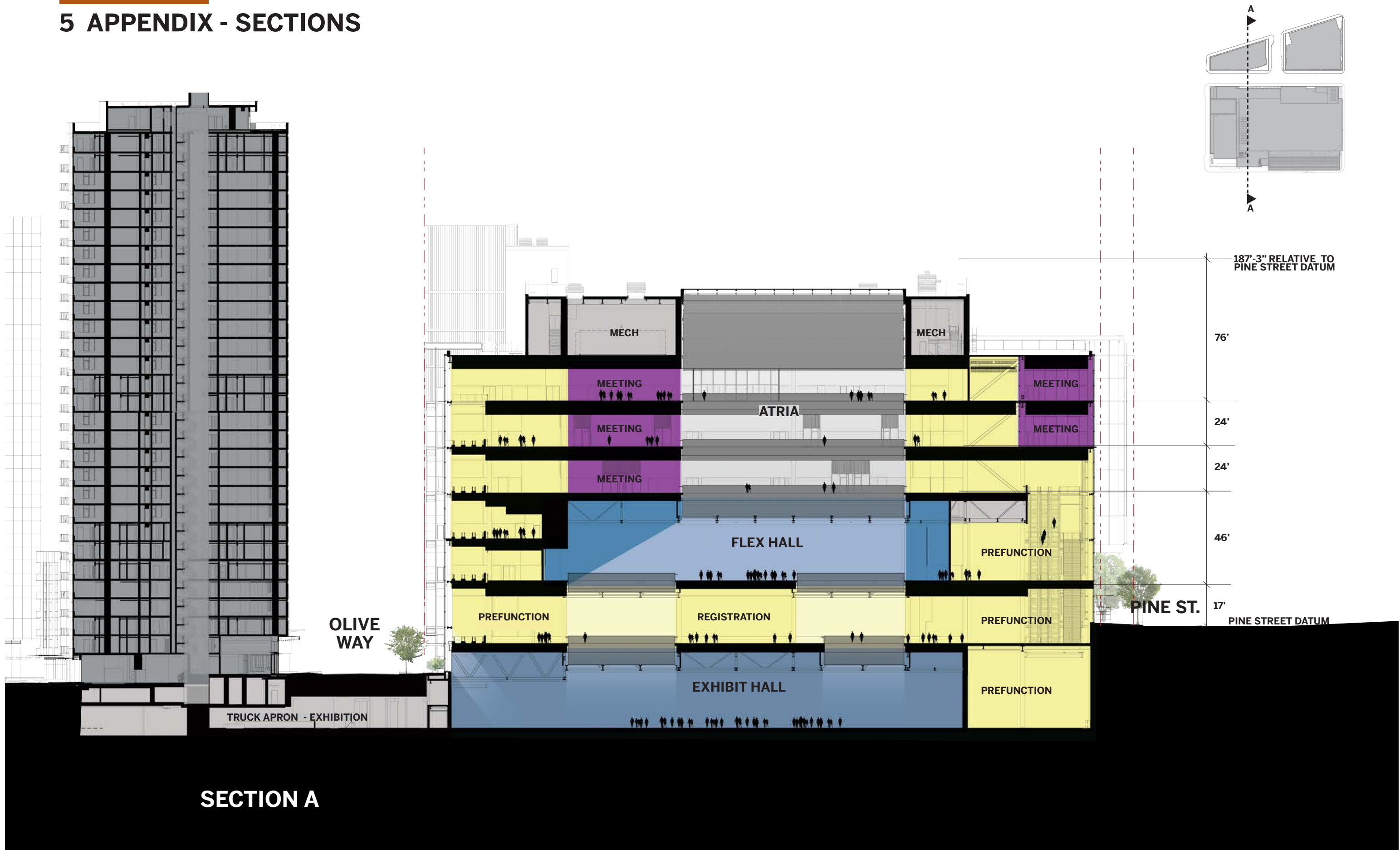


5 APPENDIX - FLOOR PLANS

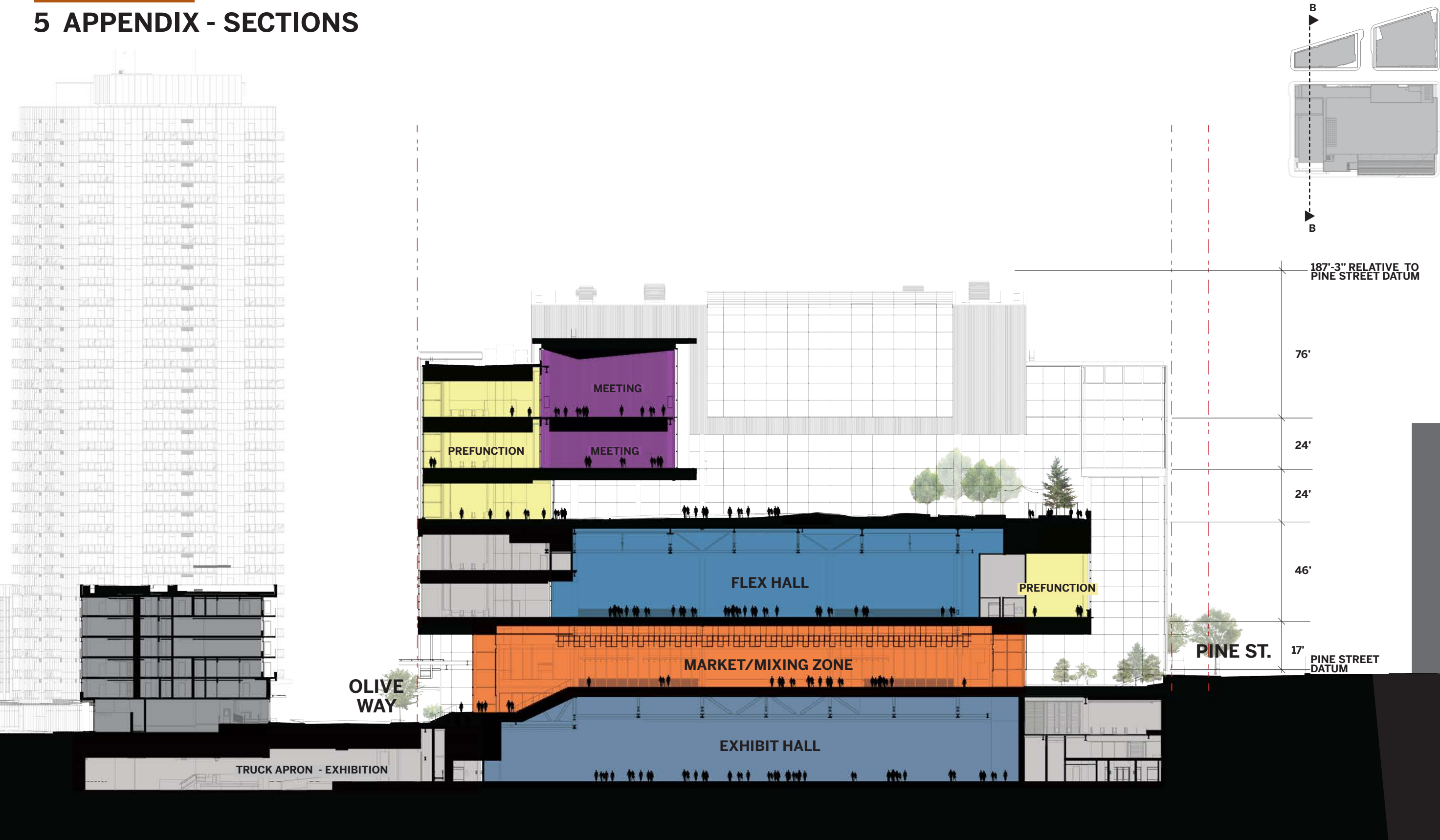
LEVEL 13



5 APPENDIX - SECTIONS

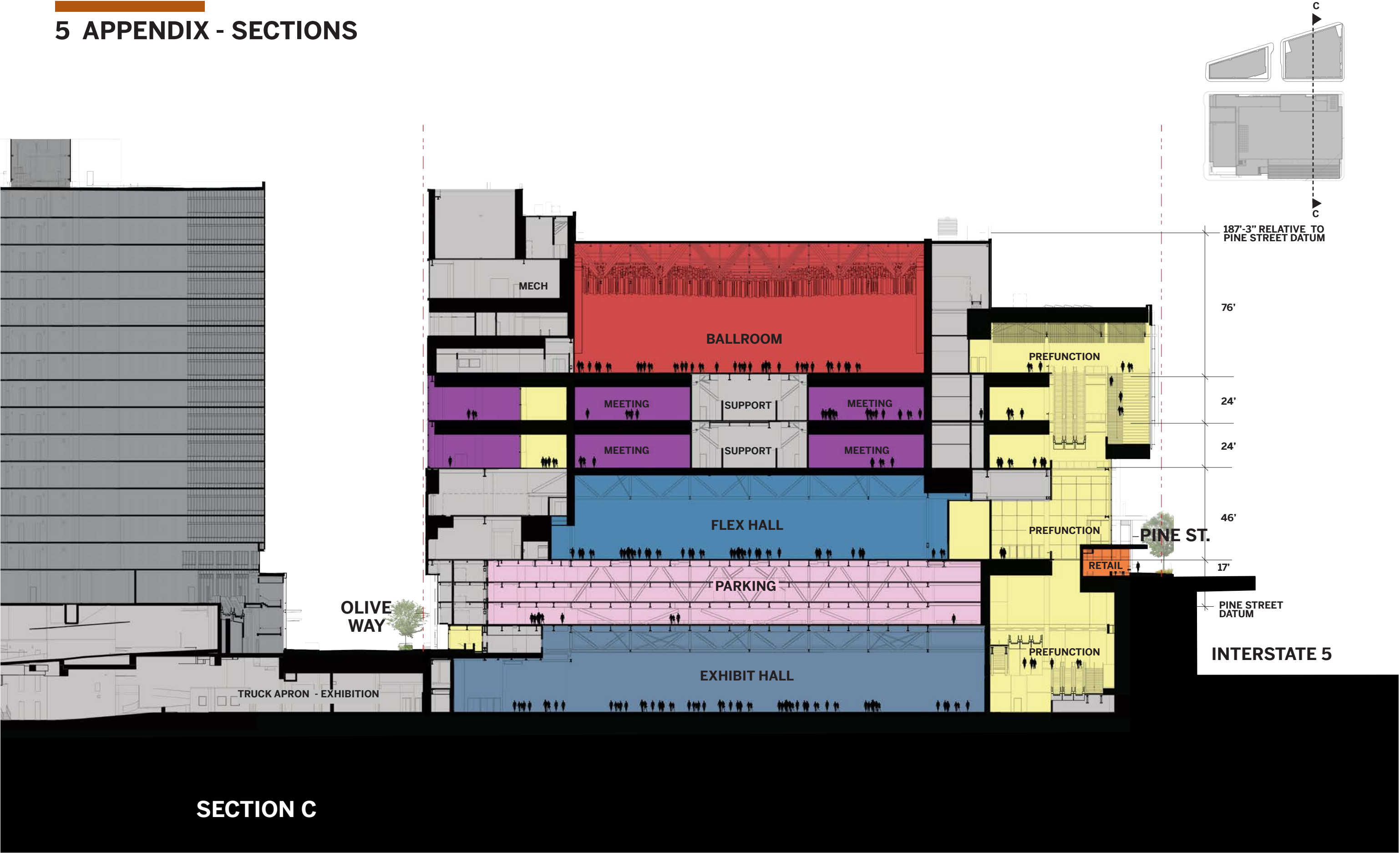


5 APPENDIX - SECTIONS

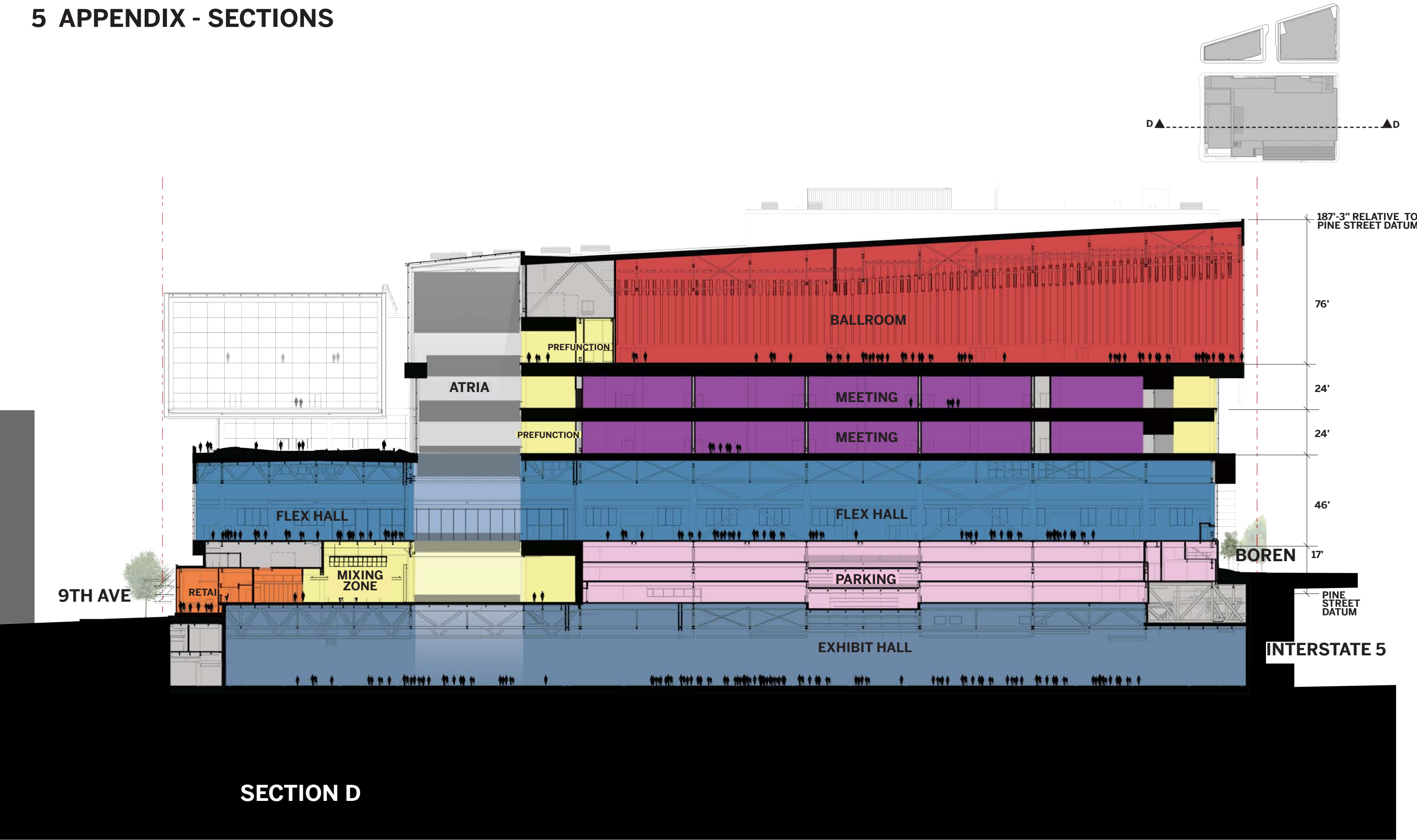


SECTION B

5 APPENDIX - SECTIONS



5 APPENDIX - SECTIONS



5 APPENDIX - MATERIALS



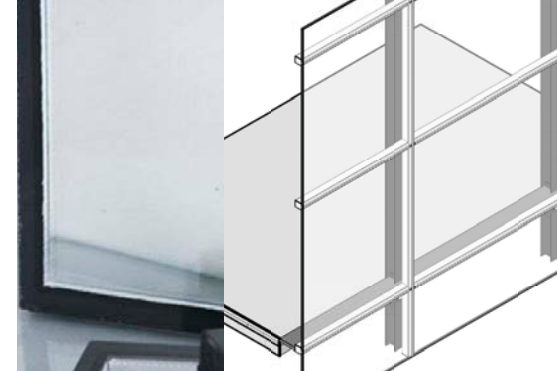
ACIP-1
ARCHITECTURAL CAST IN PLACE
CONCRETE WALL



CMPST MTLP-1
ALUMINUM CURTAINWALL SYSTEM



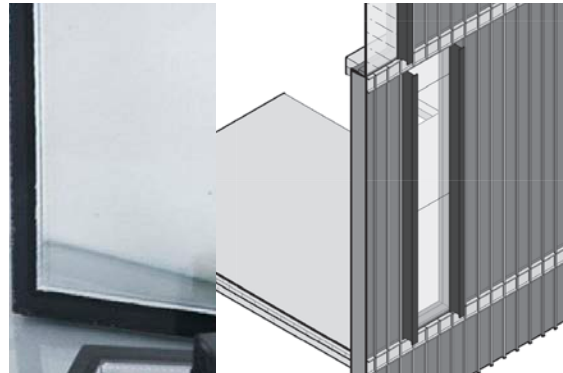
COL COVER-1
EXTERIOR METAL COLUMN AND BEAM COVER



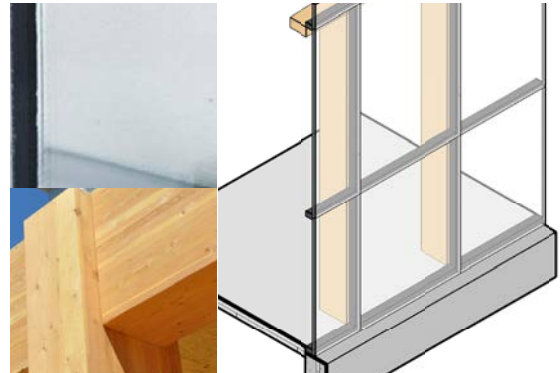
CW-1
4" WIDE ALUMINUM CURTAIN WALL SYSTEM
SUPPORTED BY INTERIOR STEEL



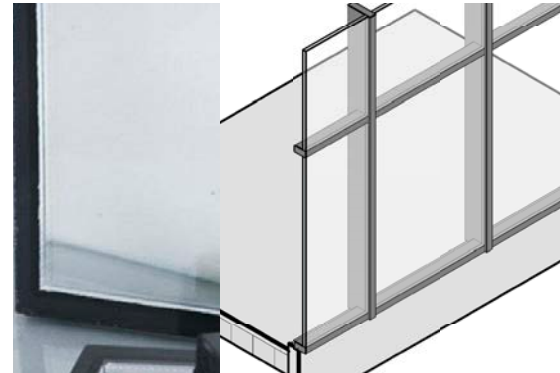
CW-2
4" WIDE ALUMINUM CURTAIN WALL SYSTEM
SUPPORTED BY EXTERIOR STEEL



CW-3
ALUMINUM CURTAINWALL SYSTEM;
TRANSLUCENT GLAZING WHEN NOTED ON
ELEVATIONS



CW-4
ALUMINUM CURTAINWALL SYSTEM
SUPPORTED BY INTERIOR WOOD



CW-5
ALUMINUM CURTAINWALL SYSTEM



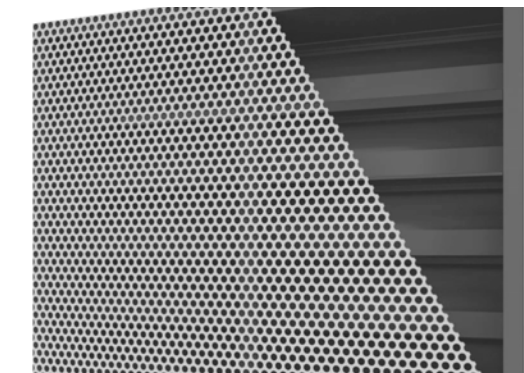
FGW WD-1
FOLDING WOOD FRAMED GLASS WALL SYSTEM



GDR-3B
48" GLASS GUARDRAIL WITH TOP CAP



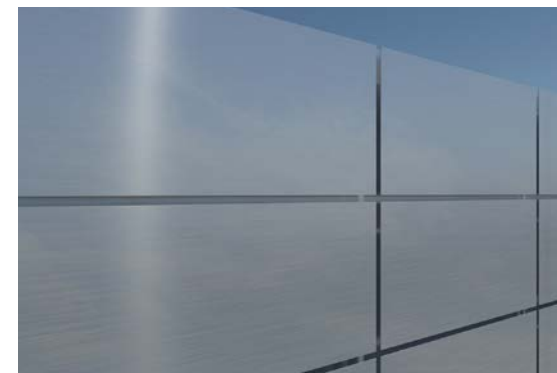
GL CAN-1
ALUMINUM MULLION ON STEEL "T" SYSTEM



LVR-1
ALUMINUM LOUVER COVERED WITH
PERFORATED STAINLESS STEEL PANEL



LVR-2
ALUMINUM LOUVER



MTLP-1A
FLAT PLATE STAINLESS STEEL WALL PANEL



MTLP-2
2.5" DEEP STANDING SEAM WALL PANEL

5 APPENDIX - MATERIALS



MTLP-3A
ARTISTIC ILLUSTRATED FLAT PLATE WALL
SYSTEM
*SPECIFIC GRAPHIC TO BE DETERMINED



MTLP-5
METAL PANEL WALL 1/8" ALUMINUM OR
STEEL PLATE OPEN JOINT RAIN SCREEN
SYSTEM



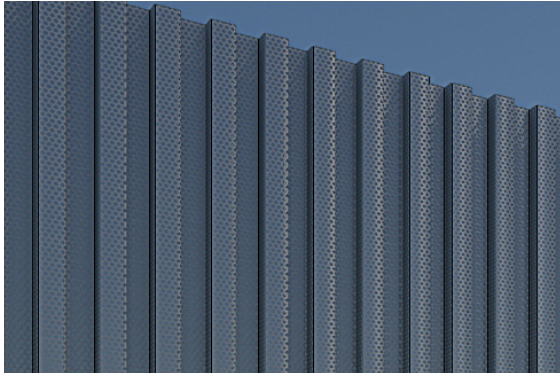
MTLP-7
HIGH GLOSS FLUSH FORMED INTERLOCKING
WALL PANEL



MTLP-8
SNAP ON 5" DEEP BATTEN SEAM AT 6" OC



MTLP-10A
FORMED CORRUGATED METAL PANEL SYSTEM



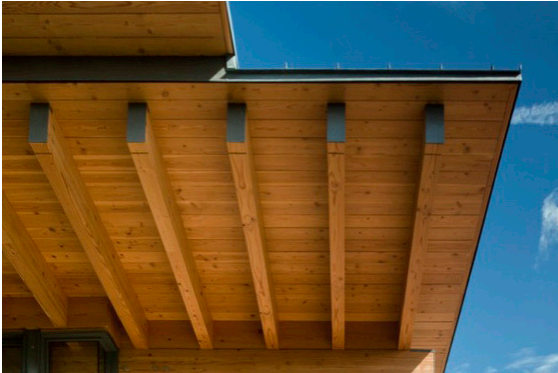
MTLP-10B
PERFORATED FORMED CORRUGATED METAL
PANEL SYSTEM



OHCD
.05" ALUMINUM INTERLOCKING FLAT SLATS
*SPECIFIC GRAPHIC TO BE DETERMINED

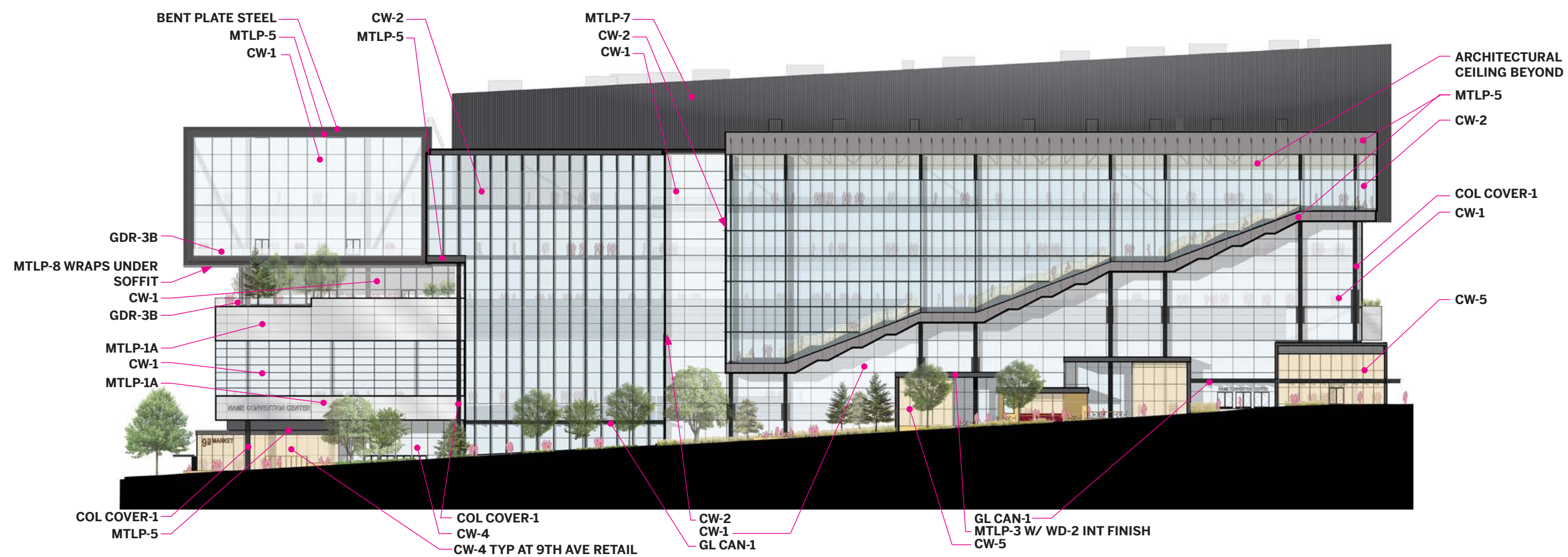
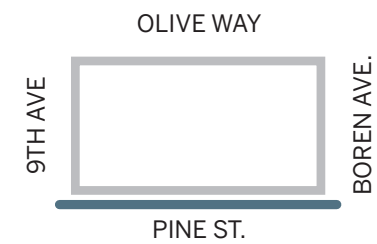


WD-4
ACCOYA RADIATA PINE EXTERIOR WOOD
WALL PANELS

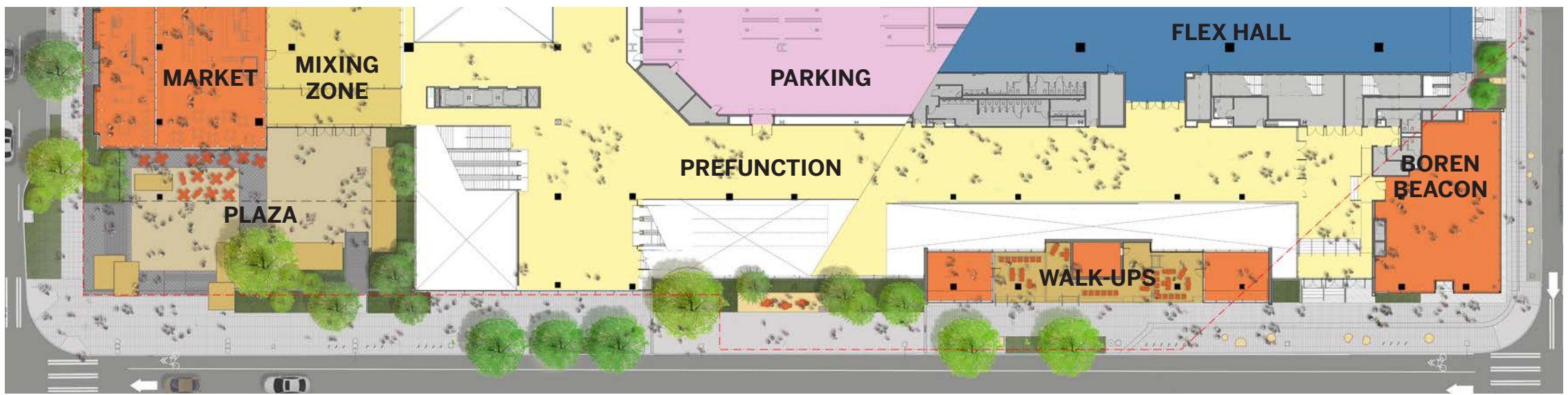


WD CAN-1
WOOD FRAMED CANOPY

5 APPENDIX - ELEVATIONS & MATERIALS

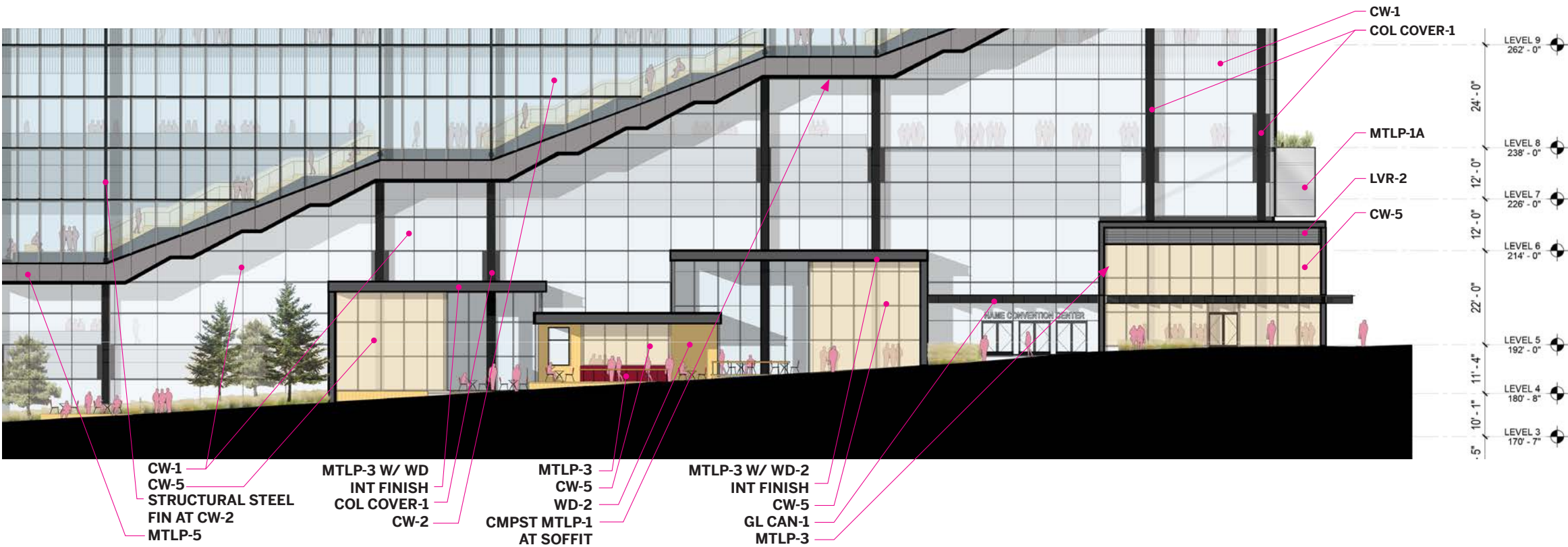
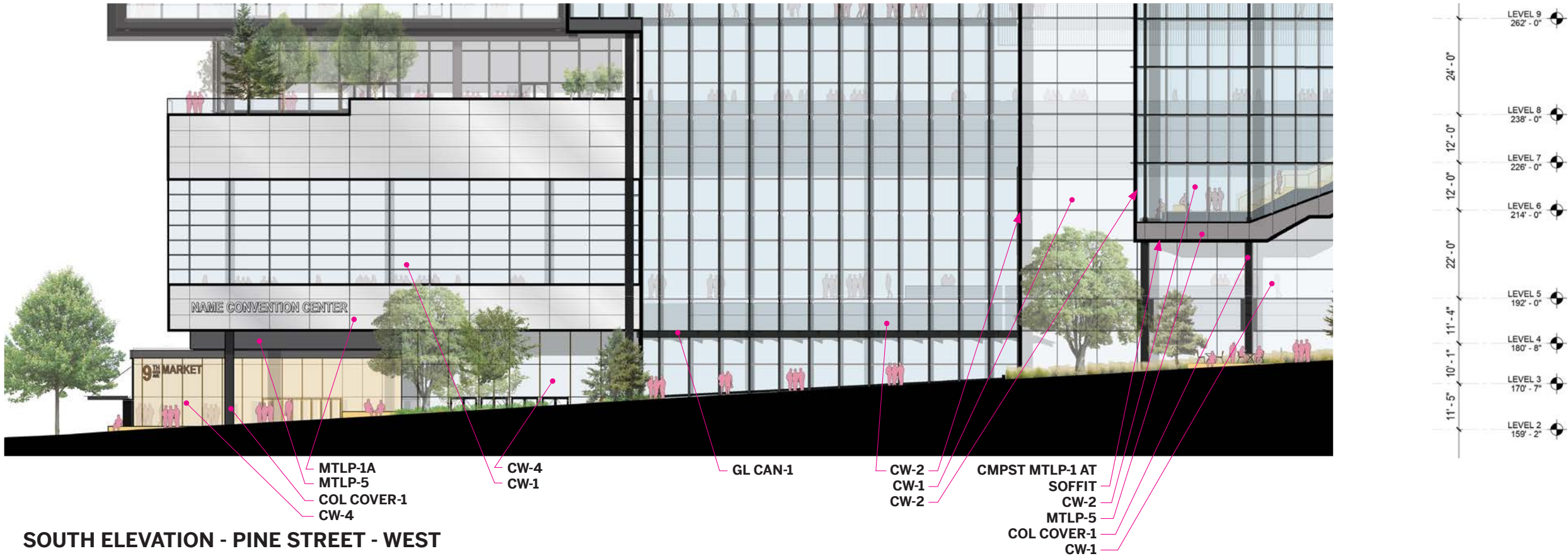


SOUTH ELEVATION - PINE STREET



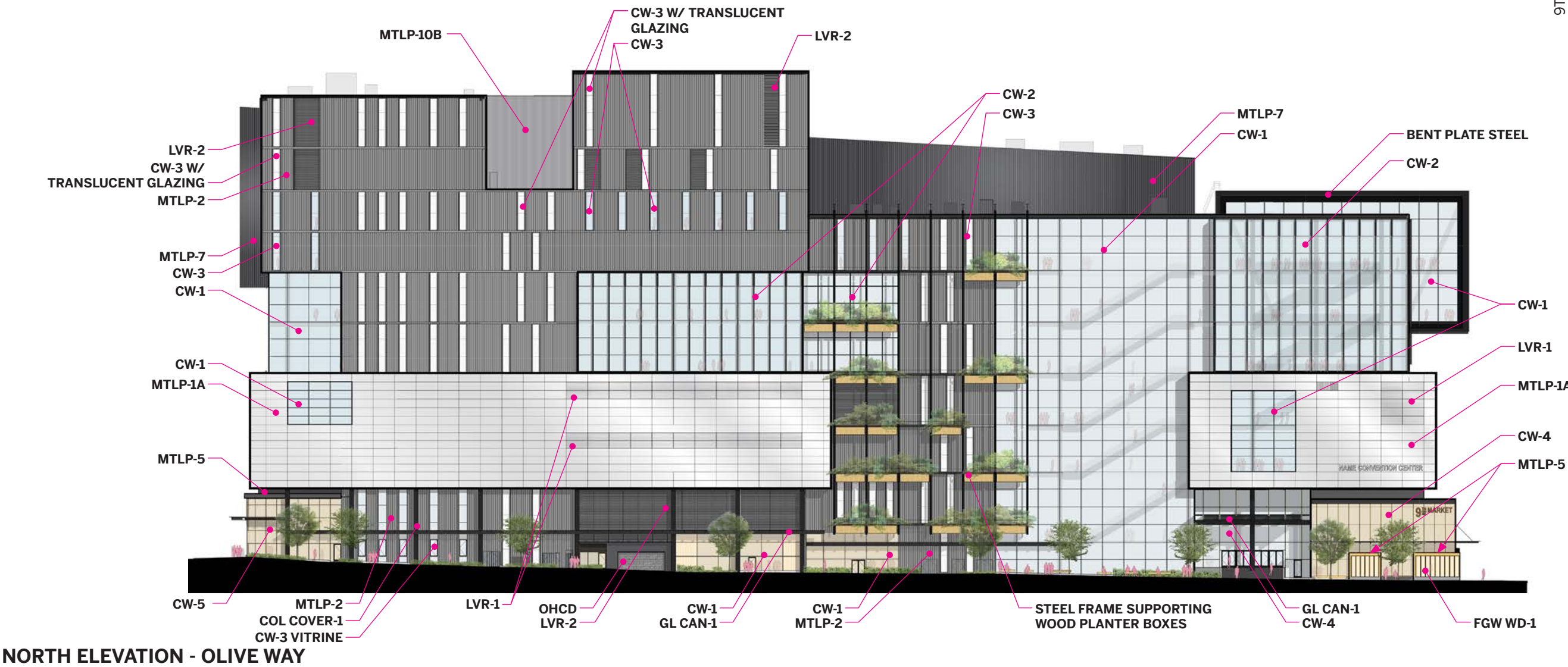
SITE PLAN

5 APPENDIX - ELEVATIONS & MATERIALS



Note: Landscape along street curb not shown in enlarged elevation for clarity.

5 APPENDIX - ELEVATIONS & MATERIALS

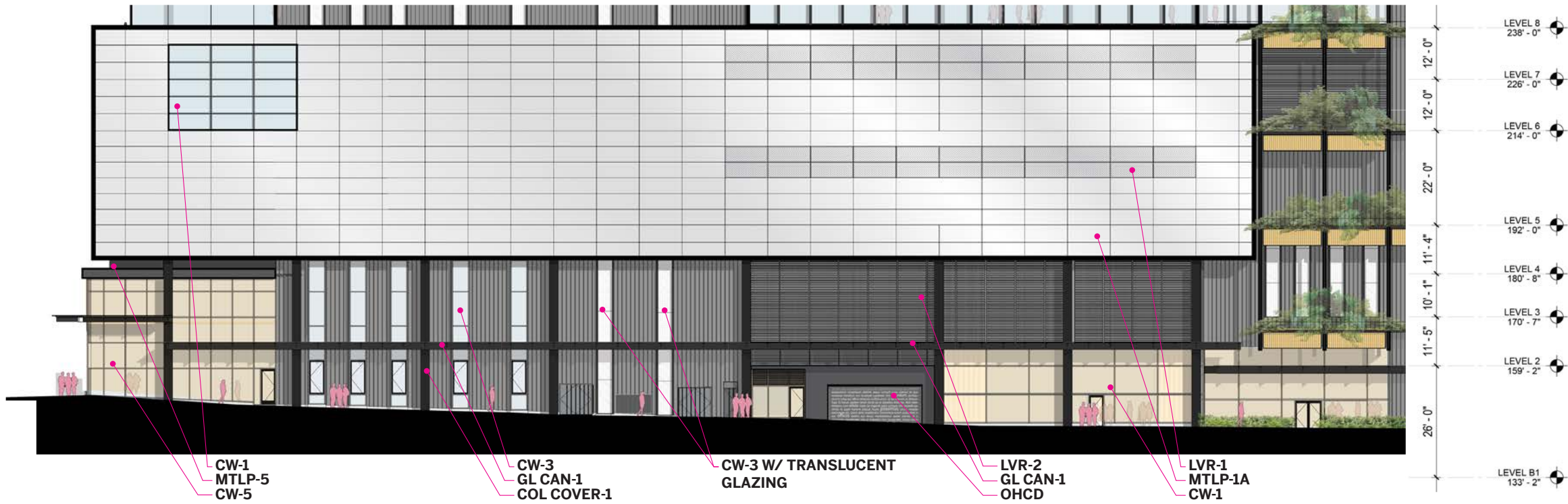


NORTH ELEVATION - OLIVE WAY

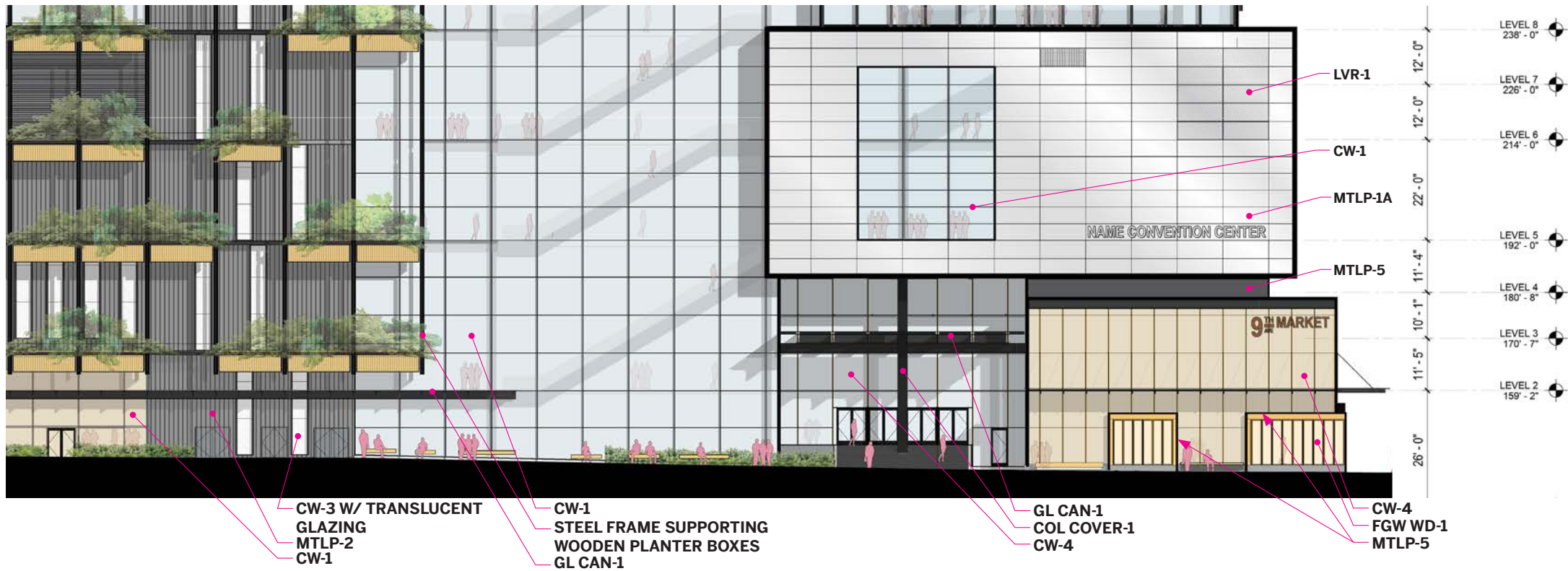


SITE PLAN

5 APPENDIX - ELEVATIONS & MATERIALS



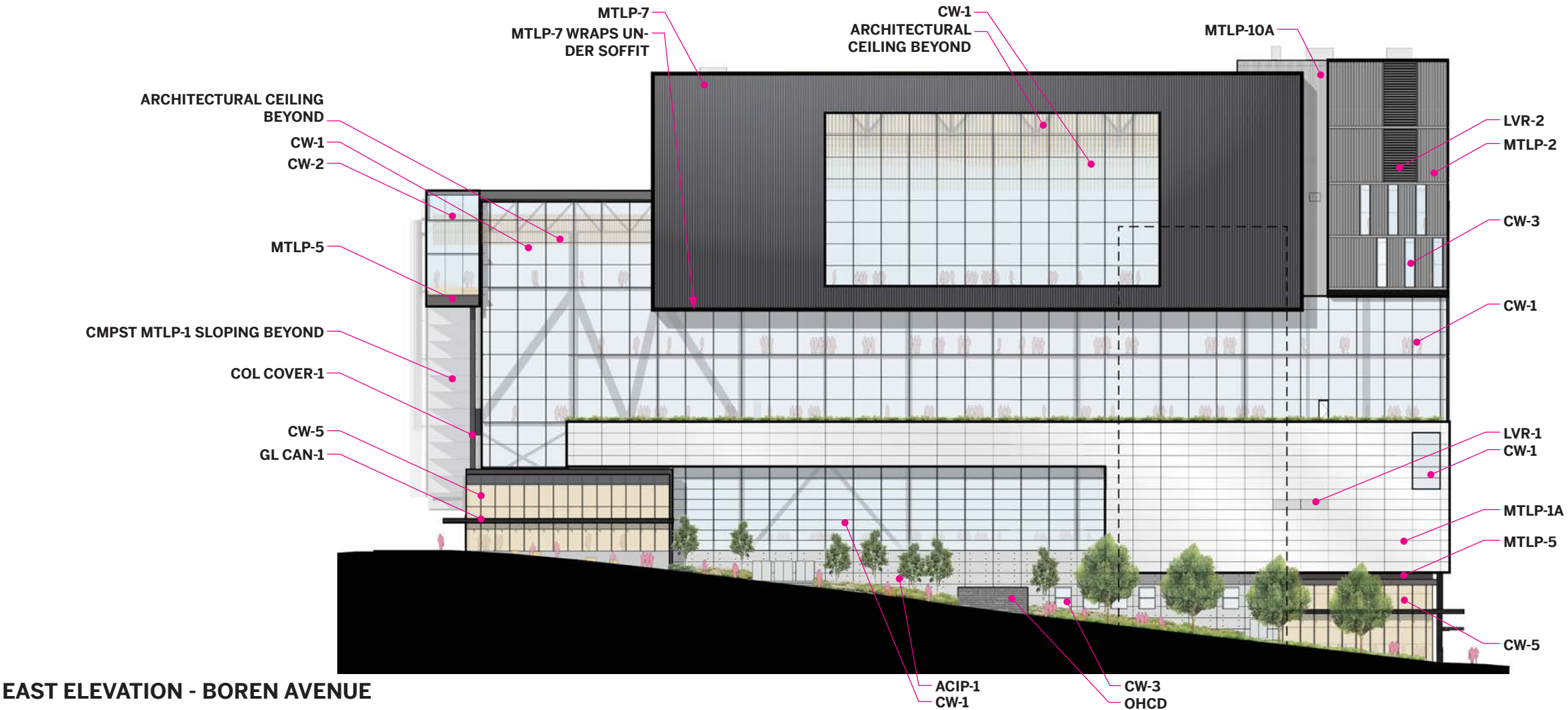
NORTH ELEVATION - OLIVE WAY - EAST



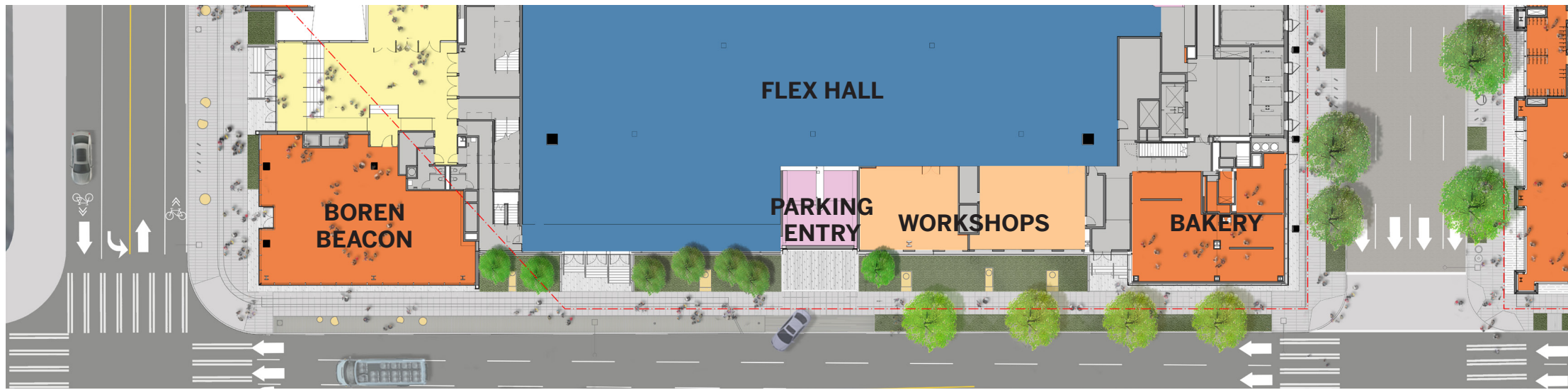
NORTH ELEVATION - OLIVE WAY - WEST

Note: Landscape along street curb not shown in enlarged elevation for clarity.

5 APPENDIX - ELEVATIONS & MATERIALS



EAST ELEVATION - BOREN AVENUE

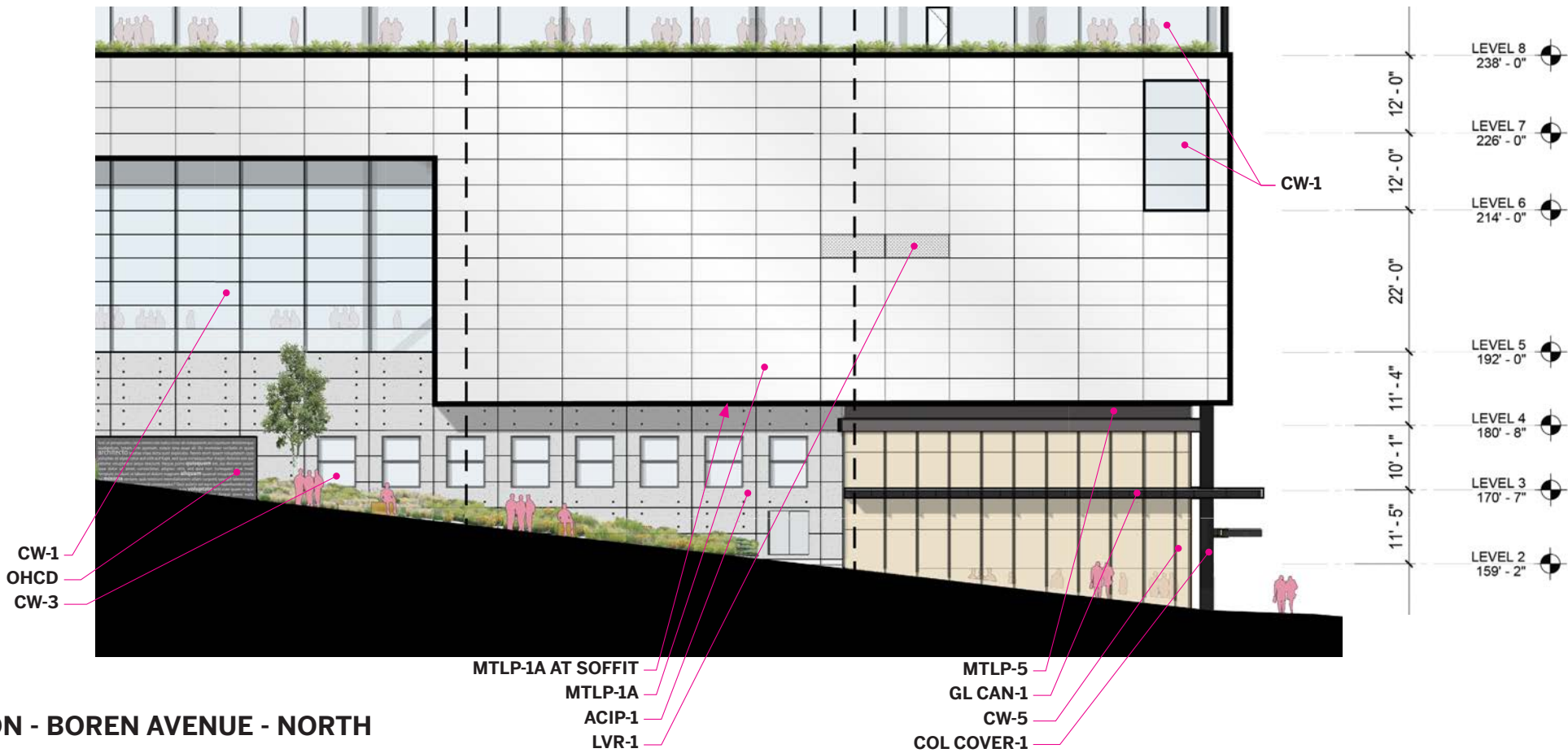


SITE PLAN

5 APPENDIX - ELEVATIONS & MATERIALS



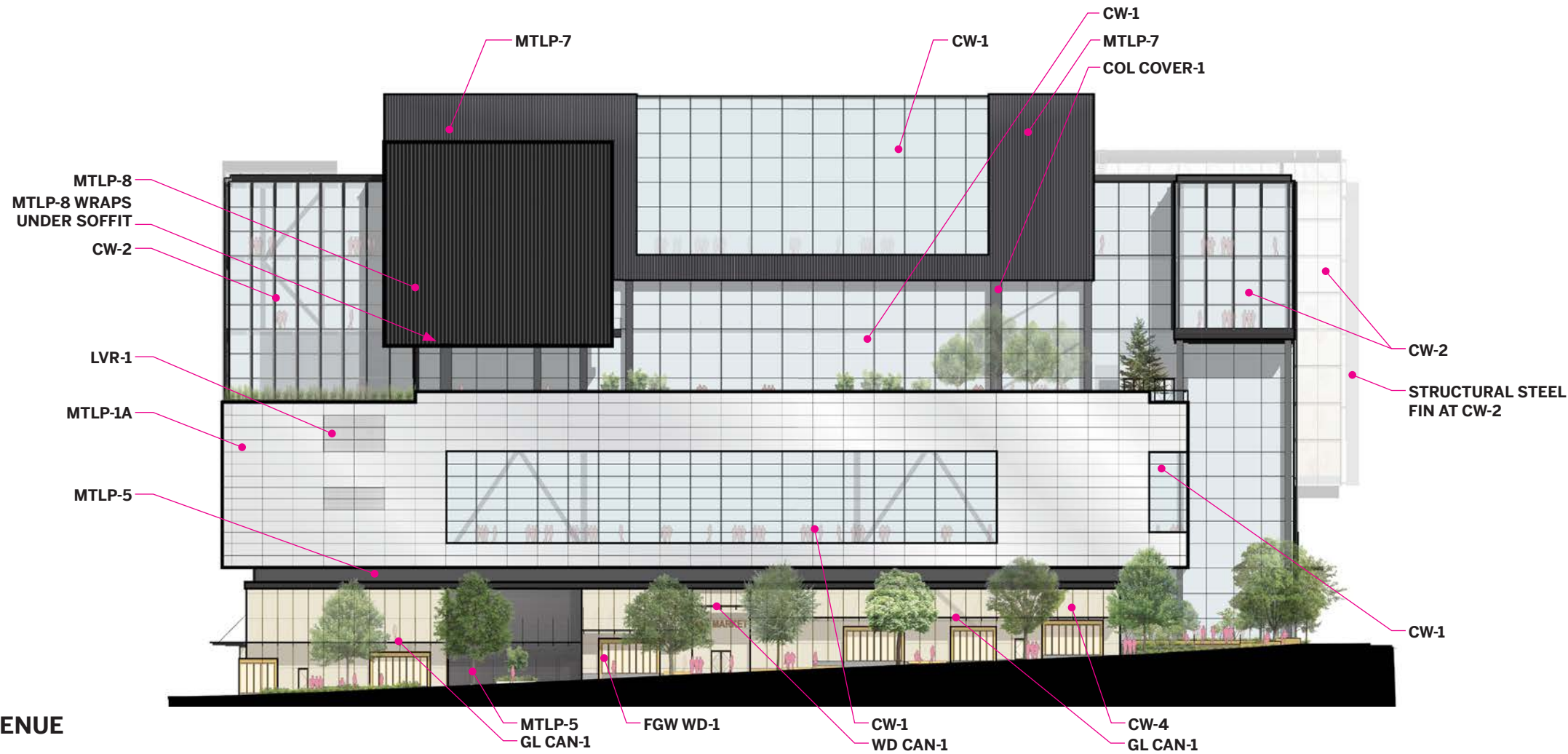
EAST ELEVATION - BOREN AVENUE - SOUTH



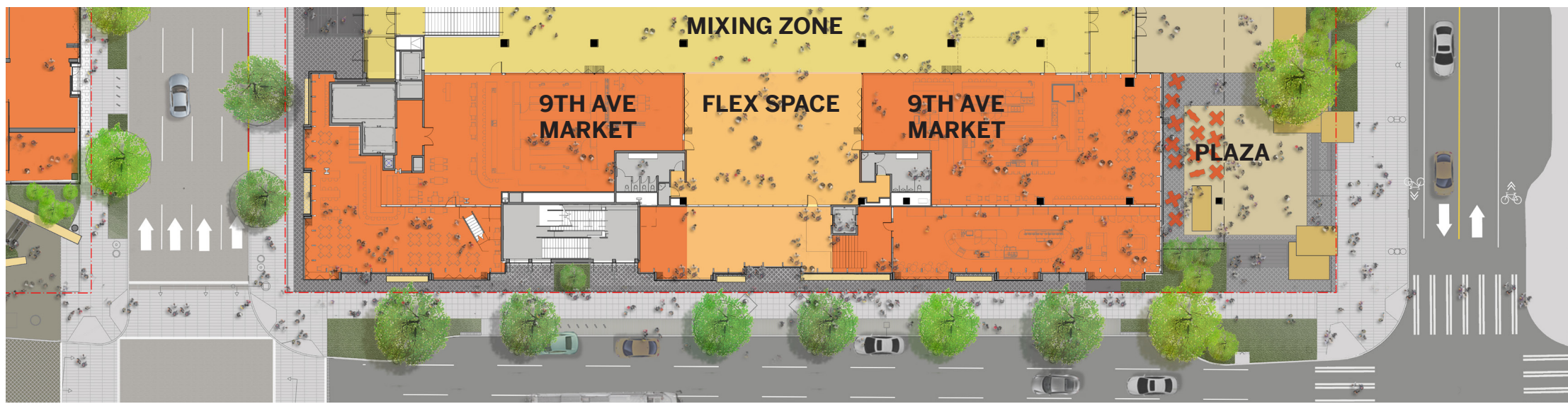
EAST ELEVATION - BOREN AVENUE - NORTH

Note: Landscape along street curb not shown in enlarged elevation for clarity.

5 APPENDIX - ELEVATIONS & MATERIALS



WEST ELEVATION - 9TH AVENUE

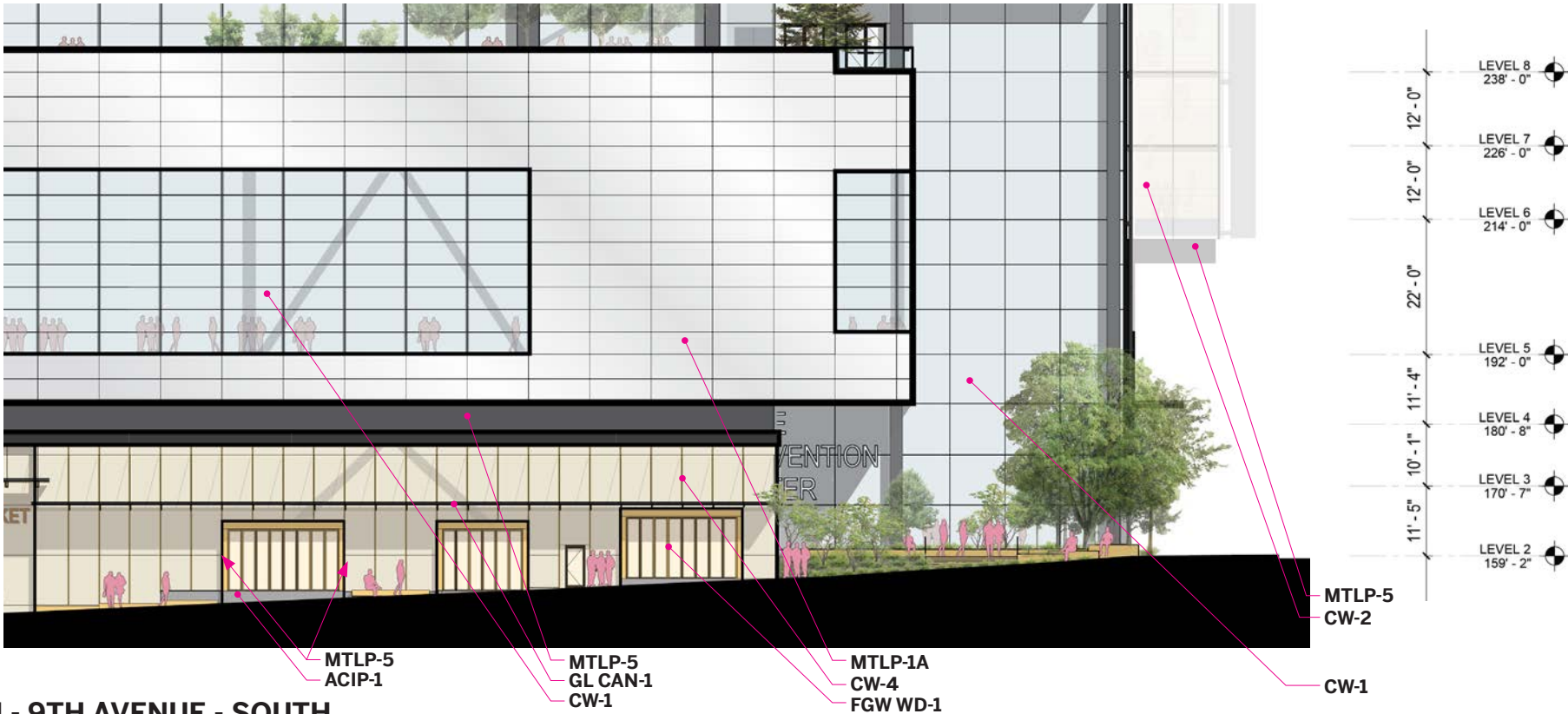


SITE PLAN

5 APPENDIX - ELEVATIONS & MATERIALS



WEST ELEVATION - 9TH AVENUE - NORTH



WEST ELEVATION - 9TH AVENUE - SOUTH

Note: Landscape along street curb not shown in enlarged elevation for clarity.

5 APPENDIX - LANDSCAPE CONCEPT

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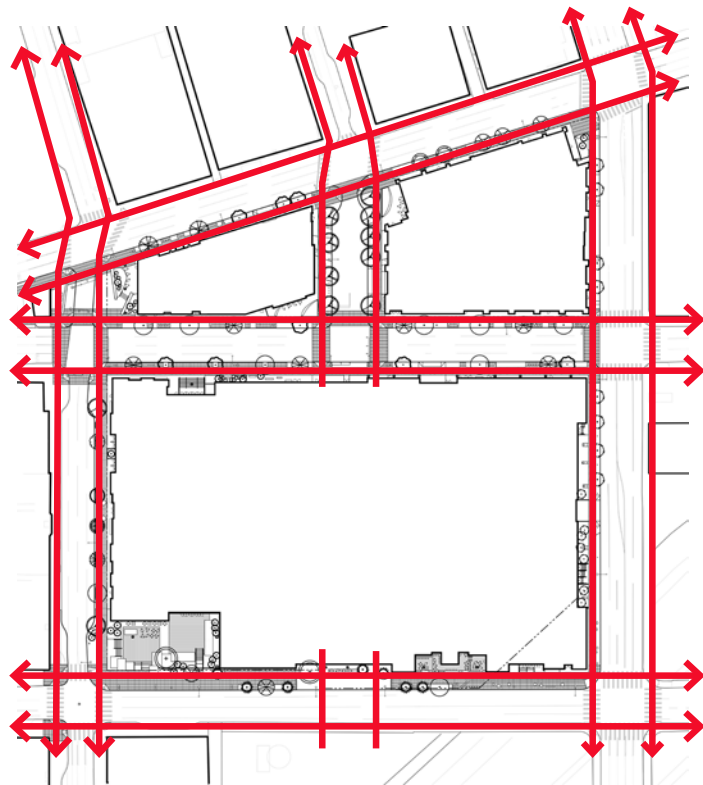
5 APPENDIX - LANDSCAPE CONCEPT

INTRODUCTION

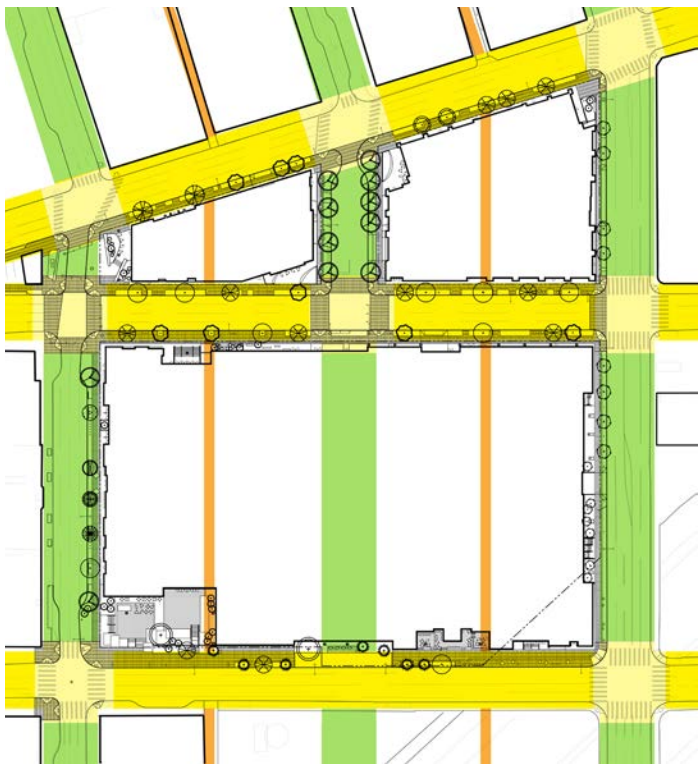
Critical to the success of this urban convention center is the ability to balance the scale and extents of the building required by its program with the grain and texture of the surrounding city. Rather than appearing as a super-block project, the streets and building will relate to the grain and texture of their surrounding neighborhoods. The building edges will create a continuously active experience on all sides for visitors and residents alike.

The site design strengthens the connectivity and continuity of the city streets. A distinct approach to avenues and streets offers a shift in character that builds on city standards to offer an additional layer of orientation rooted in the city's basic relationship between hills and Elliott Bay.

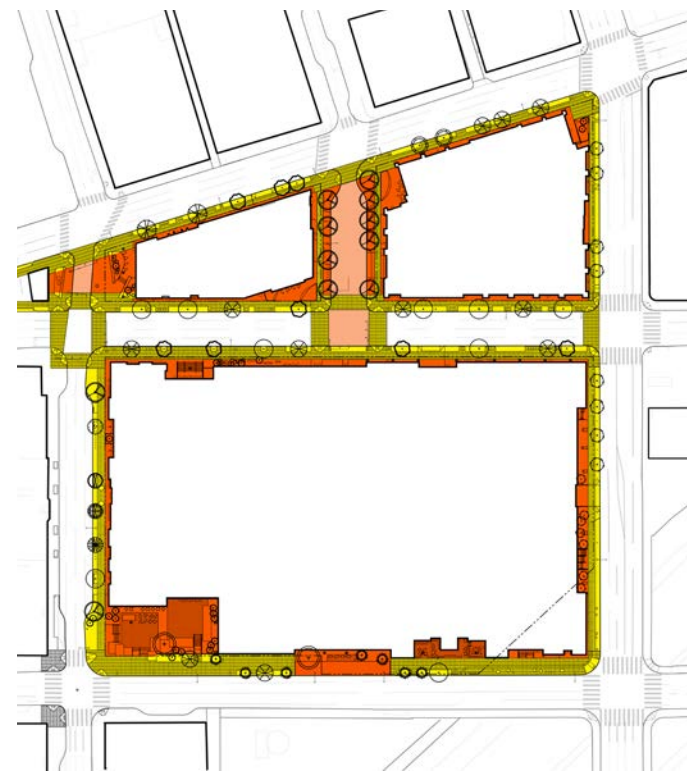
The project reinforces the continuity of the city streets by clearly articulating the edge between the street and WSCC Addition. This allows for a wider variety of conditions within the WSCC Addition footprint and supports multiple identities around the multi-block project, while extending the larger continuity of city streets. An additional layer of site elements offers pedestrian amenities that bridge the public programs of the WSCC Addition and the experience of convention visitors.



URBAN CONNECTIVITY AND CONTINUITY



ORIENTING STREETS AND AVENUES



INDEPENDENT SIDEWALKS








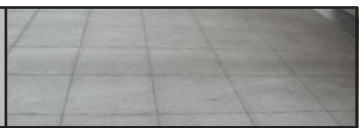





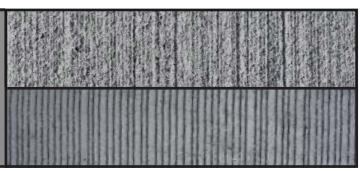



VARIETY AND MULTIPLE IDENTITIES WITHIN PROPERTY

5 APPENDIX - LANDSCAPE CONCEPT

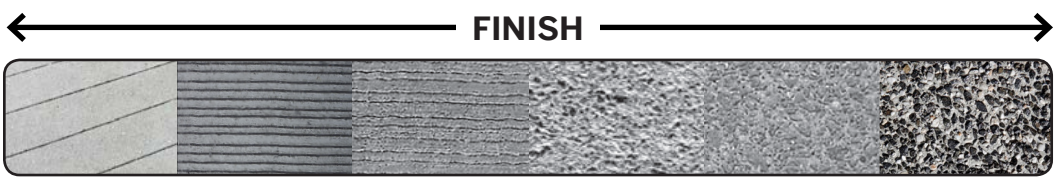


5 APPENDIX - LANDSCAPE CONCEPT












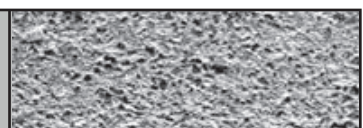


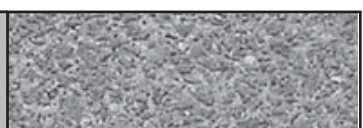
Right of Way Paving Materials

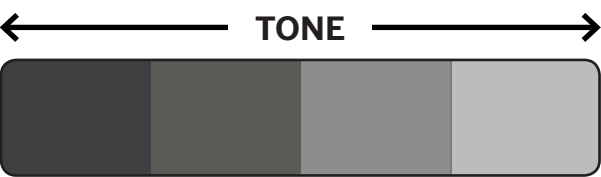
ZONE	MATERIAL	COLOR	FINISH
	HILL STREETS Color: City Standard "light gray" Finish: Light broom		
	AVENUES Color: City Standard "light gray" Finish: Medium broom		
	LANDSCAPE ZONE Color: Integral Color - Charcoal Finish: Heavy trowel at 6" o.c.		
	GREEN STREETS Color: Integral Color - Charcoal Sidewalk Finish: Heavy broom Roadway Finish: Raked		
	TREE PITS Material: Stabilized gravel Color: Gray and Black mixed		

Concrete Paving Gradients


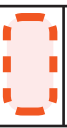



Property Line Paving Materials

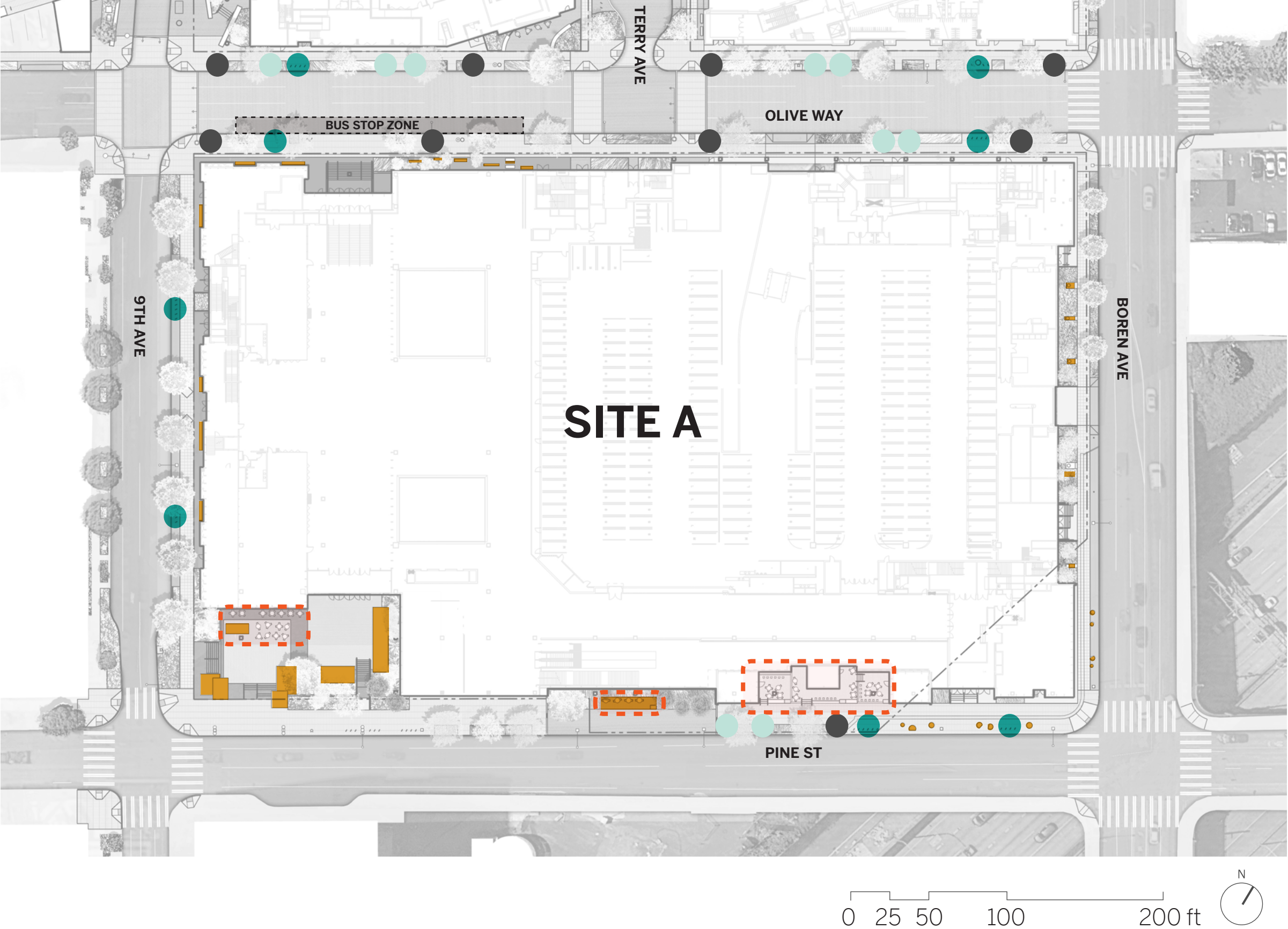
ZONE	MATERIAL	COLOR	FINISH
	WOOD DECK Material: Kebony and Accoya		
	WSCC WEST Material: Basalt Finish: Flamed, Dark Gray		
	WSCC EAST Color: City Standard "light gray" Finish: Water Wash		
	OFFICE TOWER Color: City Standard "light gray" Finish: Light sand blast		
	RESIDENTIAL TOWER Color: City Standard "light gray" Finish: Exposed aggregate		



Additional Distinctions

	VEHICLE RATED SIDEWALK CONCRETE
	CPS ART EMBEDDED INTO PAVING
	OFFSITE PAVING PROPOSED AS PUBLIC BENEFIT

5 APPENDIX - LANDSCAPE CONCEPT



5 APPENDIX - LANDSCAPE CONCEPT

Primary Site A Street Furnishings



Planting Protection Fence
(Adjoining all planting areas with landscaping at sidewalks)



Wood ROW bench
Spring Bench
Duluth Timber Company
(various salvaged wood species)



Bicycle Rack
Westport, No-Scratch
Sportworks



Trash/Recycling Receptacle
RR-55 series
Urban Accessories



Moveable Tables and Chairs
Various models and finish colors
Fermob

Custom Wood Street Furnishings & Seating Platforms



Kebony



Douglas fir



9th Ave & Pine St plaza custom seating platforms

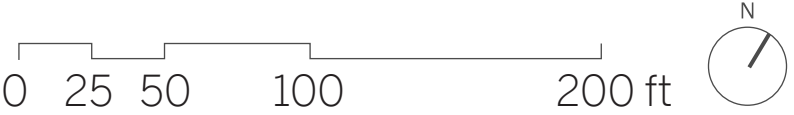
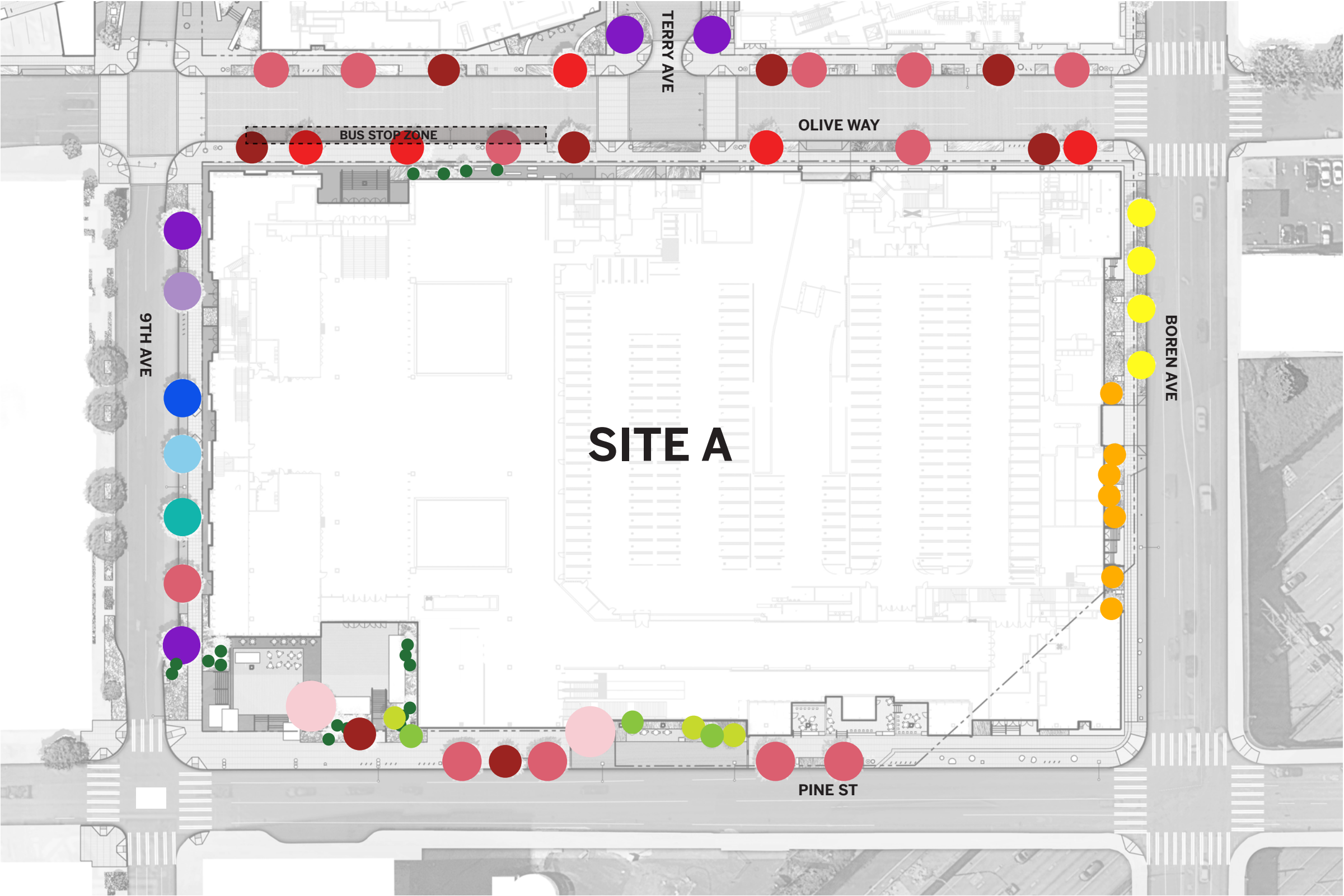


Custom reclaimed timber benches located on Olive Way between 9th Ave and Terry Ave



Custom reclaimed timber benches located at the corner of Pine St and Boren Ave

5 APPENDIX - LANDSCAPE CONCEPT



5 APPENDIX - LANDSCAPE CONCEPT

Site A Street Trees



Acer circinatum
Vine maple



Acer macrophyllum
Bigleaf maple



Acer platanoides
Norway maple



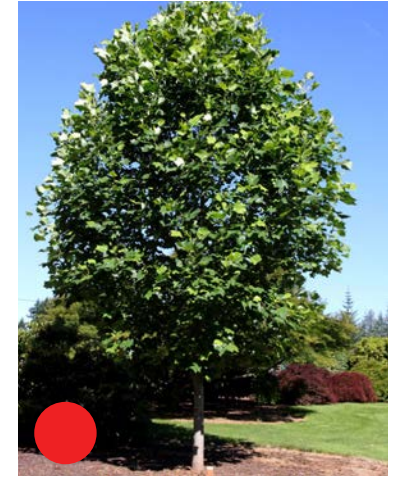
Acer saccharum 'Bonfire'
Sugar maple 'Bonfire'



Betula papyrifera
'Renaissance reflection'
Paper birch



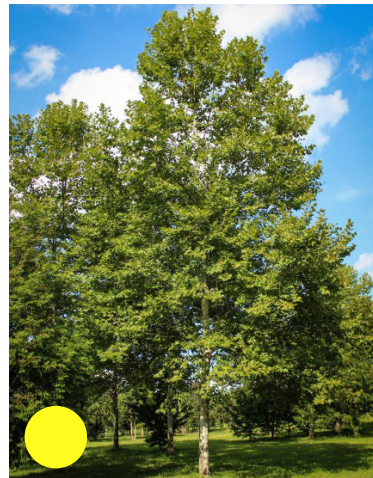
Ginkgo biloba
Ginkgo



Liriodendron tulipifera
'Emerald City'
Emerald City tulip tree



Pinus contorta var.
contorta
Shore pine



Platanus x acerifolia
'Columbia'
Columbia plane tree



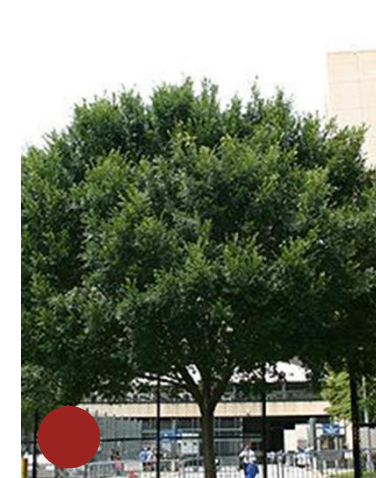
Pseudotsuga menziesii
Douglas fir



Quercus rubra
Northern red oak



Tilia cordata
Little leaf linden

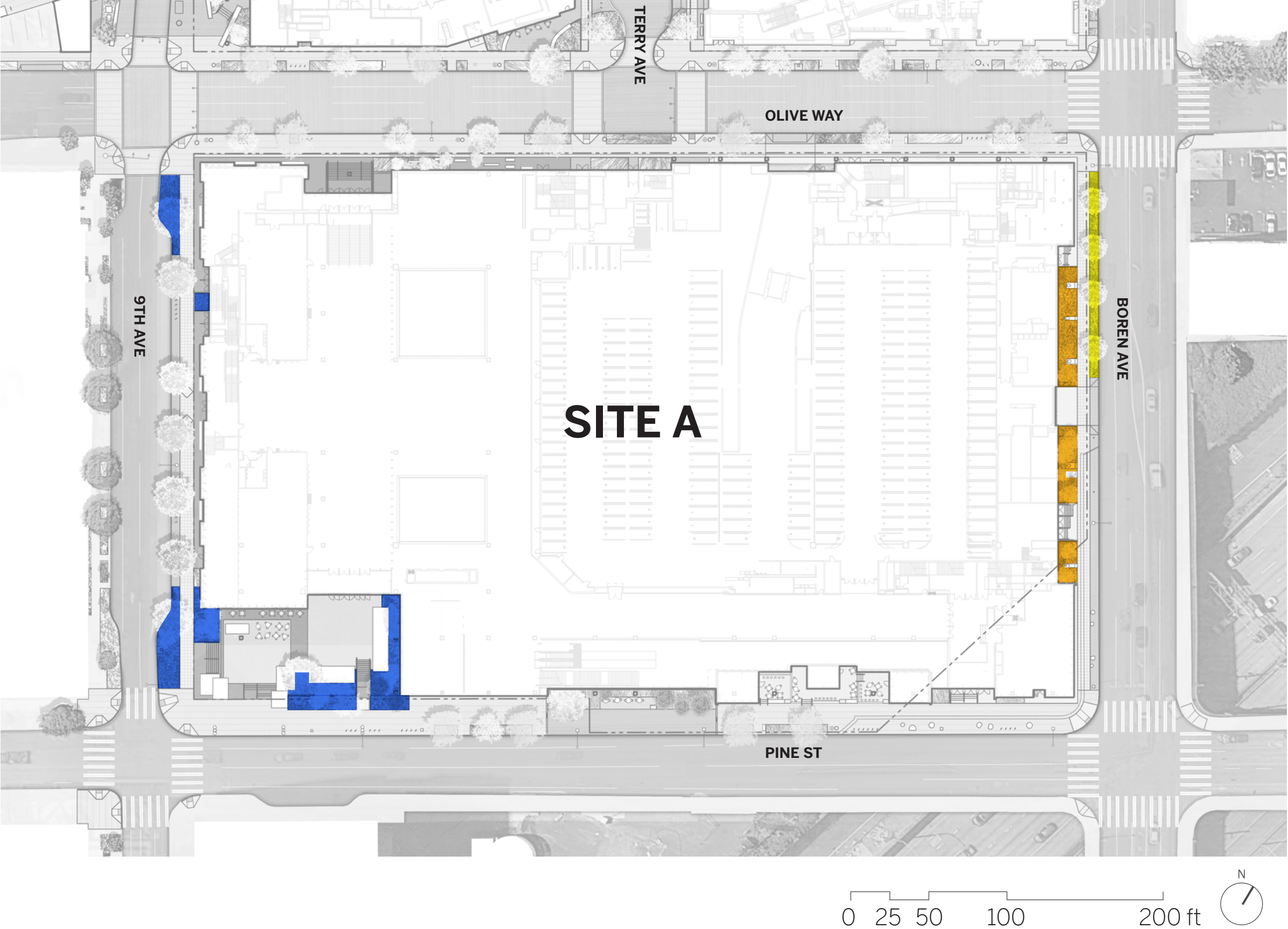


Ulmus parvifolia 'Emer II'
Allee elm



Zelkova serrata 'Green Vase'
Japanese zelkova

5 APPENDIX - LANDSCAPE CONCEPT



5 APPENDIX - LANDSCAPE CONCEPT

9th Avenue Streetscape, Pine and 9th Plaza



Camassia leictlinii
Californian white camass



Cornus stolonifera 'Kelseyi'
Kelsey's Dwarf Red-Osier Dogwood



Cornus stolonifera 'Arctic Fire'
Red-osier dogwood



Deschampsia cespitosa 'Schottland'
Scottish tufted hair grass



Dryopteris filix-max 'Robusta'
Robust male fern



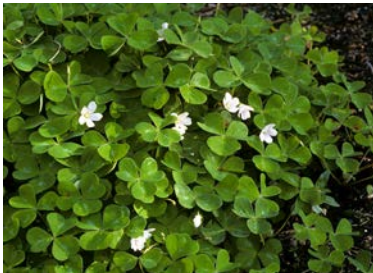
Fragaria Vesca
Wild Strawberry



Gaultheria shallon
Salal



Mahonia nervosa
Dwarf Oregon grape



Oxalis oregana
Redwood sorrel



Polystichum munitum
Western sword fern



Thuja plicata 'Whipcord'
Whipcord western red cedar



Vaccinium corymbosum 'Patriot'
Patriot early season blueberry



Vaccinium ovatum 'Scarlet Ovation'
Evergreen huckleberry

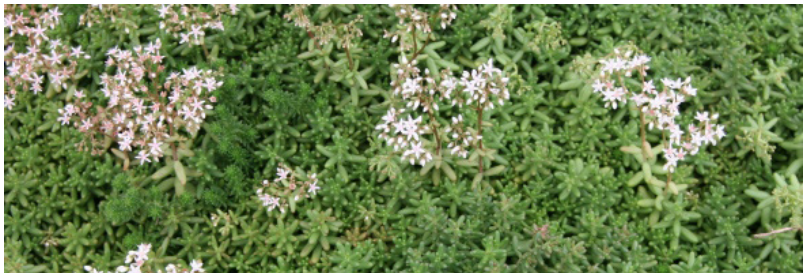


Viola adunca
Hookedspur violet

Boren Ave Streetscape



Ceanothus griseus var. horizontalis 'Yankee Point'
Yankee Point California lilac



Sedum album
White stonecrop

Boren Ave Garden



Gaultheria shallon
Salal



Polystichum munitum
Western sword fern



Mahonia nervosa
Dwarf Oregon grape



Achillea millefolium 'Sonoma Coast'
Sonoma Coast Yarrow



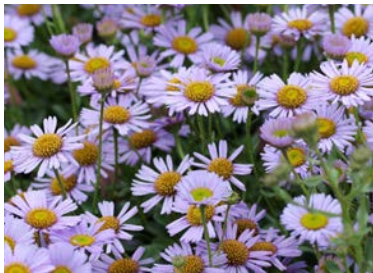
Deschampsia cespitosa 'Goldtau'
Goldtau Tufted Hair Grass



Bidens laevis
Smooth Beggartick



Bouteloua gracilis 'Blonde Ambition'
Blonde Ambition Blue Grama Grass

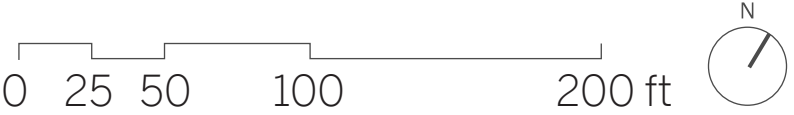
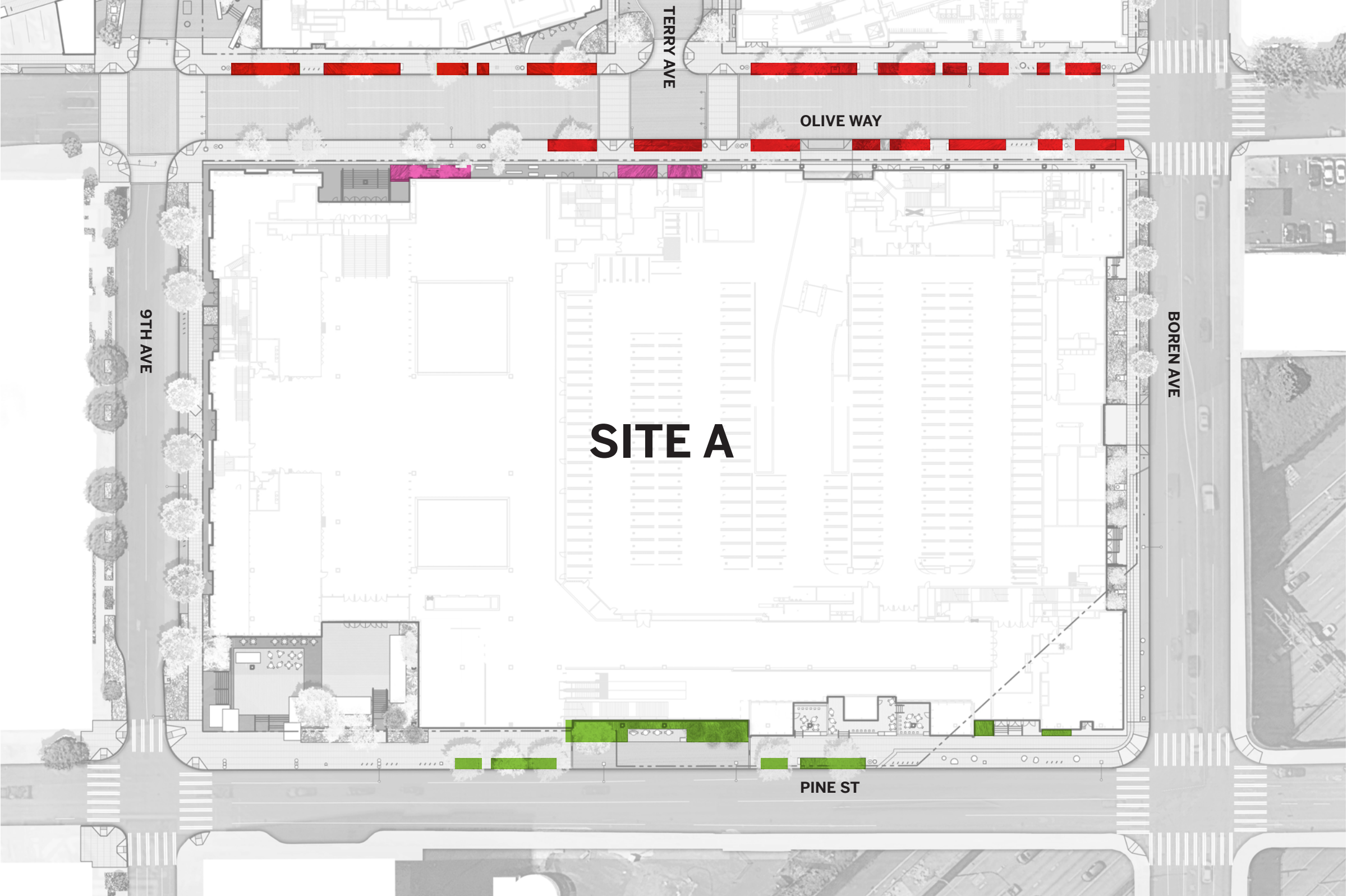


Erigeron glaucus
Wayne Roderick Daisy



Verbena lilacina 'De La Mina'
Purple Cedros Island Verbena

5 APPENDIX - LANDSCAPE CONCEPT



5 APPENDIX - LANDSCAPE CONCEPT

Olive Way Streetscape Planting



Arctostaphylos uva-ursi
'Massachusetts'
Massachusetts kinnikinnick



Camassia leictlinii
Californian white camass



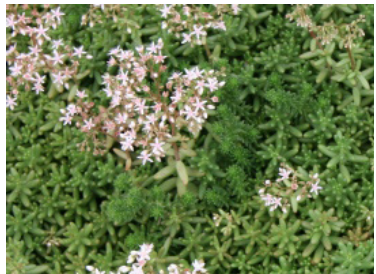
Cornus sericea 'Arctic Fire'
Arctic Fire red twig dogwood



Dryopteris filix-max 'Robusta'
Robust male fern



Gaultheria shallon
Salal



Sedum album
White stonecrop

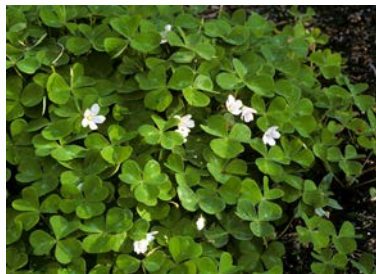


Vaccinium ovatum
'Thunderbird'
Thunderbird evergreen huckleberry

Olive Way Bus Stop Streetscape Planting



Mahonia nervosa
Dwarf Oregon grape



Oxalis oregana
Redwood sorrel



Polystichum munitum
Western sword fern

Pine St Streetscape Planting



Achillea millefolium 'Pink Grapefruit'
Pink grapefruit yarrow



Bouteloua gracilis 'Blonde Ambition'
Blonde Ambition Blue Grama Grass



Deschampsia cespitosa 'Goldtau'
Goldtau Tufted Hair Grass



Erigeron glaucus
Wayne Roderick Daisy



Gaultheria shallon
Salal



Mahonia aquifolium 'Apollo'
Salal



Vaccinium ovatum
'Thunderbird'
Thunderbird evergreen huckleberry

5 APPENDIX - LANDSCAPE CONCEPT



5 APPENDIX - LANDSCAPE CONCEPT

Terrace Planting



Calluna vulgaris 'silver knight'
Silver Knight Scotch heather



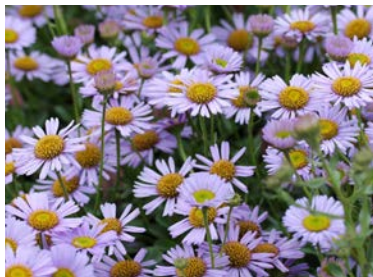
Camassia quamash
Camas



Erica carnea
'Springwood Pink'
Winter heath



Erica carnea
'Springwood White'
Winter heath



Erigeron glaucus
Wayne Roderick Daisy



Lavendula x intermedia
'Provence'
Provence French lavender



Monardella villosa
Coyote mint



Salvia chiapensis
Chiapas sage



Blechnum spicant
Deer fern



Cornus canadensis
Bunchberry dogwood



Polystichum munitum
Western sword fern



Mahonia nervosa
Dwarf Oregon grape



Gaultheria shallon
Salal



Dryopteris filix-max 'Robusta'
Robust male fern



Tellima grandiflora
Bigflower tellima



Vaccinium 'Northsky'
Northsky Blueberry



Vaccinium angustifolium
'Burgundy'
Burgundy Wild Lowbush Blueberry



Vaccinium ovatum
'Thunderbird'
Thunderbird Evergreen Huckleberry



Bouteloua gracilis 'Blonde Ambition'
Blonde Ambition Blue Grama Grass



Deschampsia cespitosa
'Schottland'
Scottish tufted hair grass



Nassella tenuissima
Mexican Feather Grass

Level 8 Planting



Acer circinatum
Vine maple



Arctostaphylos uva-ursi
'Massachusetts'
Massachusetts kinnikinnick



Cornus sericea 'Arctic Fire'
Arctic Fire red twig dogwood



Gaultheria shallon
Salal



Mahonia nervosa
Dwarf Oregon grape

5 APPENDIX - LANDSCAPE CONCEPT



PLANTER AT OLIVE WAY FACADE



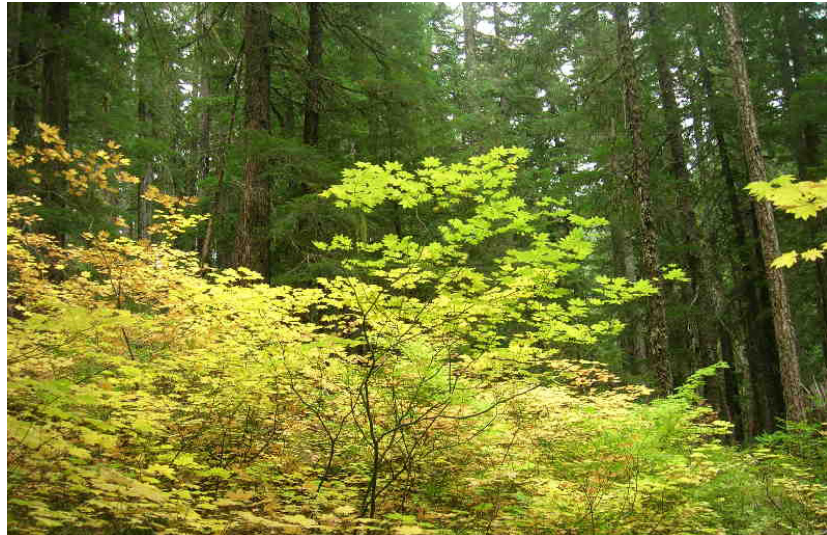
NORTHWEST FOREST PLANTER CHARACTER



DETAIL: PLANTER FRAME

5 APPENDIX - LANDSCAPE CONCEPT

Olive Way elevated facade planters



Acer circinatum
Vine maple



Sambucus racemosa
Red elderberry



Aruncus dioicus
'Zweiweltenkind'
Goat's beard



Arctostaphylos uva-ursi
'Massachusetts'
Massachusetts kinnikinnick



Cornus canadensis
Bunchberry dogwood



Cornus sericea *'Arctic Fire'*
Arctic Fire red twig dogwood



Dryopteris filix-max *'Robusta'*
Robust male fern



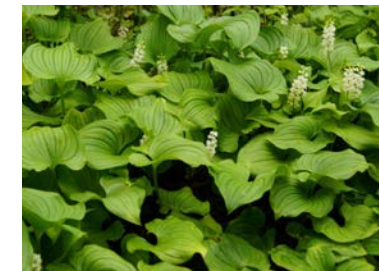
Gaultheria shallon
Salal



Linnaea borealis
Twinflower



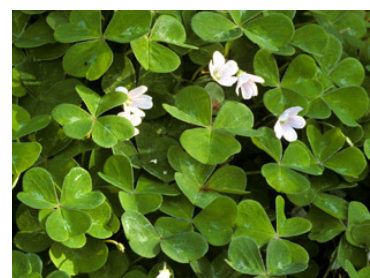
Mahonia nervosa
Dwarf Oregon grape



Maianthemum dilatatum
False lily of the valley



Oplopanax horridus
Devil's club



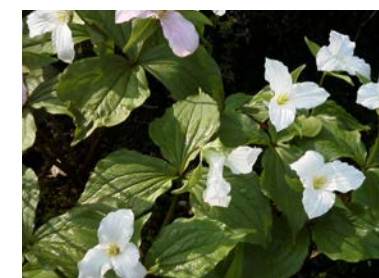
Oxalis oregana
Redwood sorrel



Polystichum munitum
Western sword fern



Smilacina racemosa
Solomon's plume



Trillium ovatum
Coast trillium



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