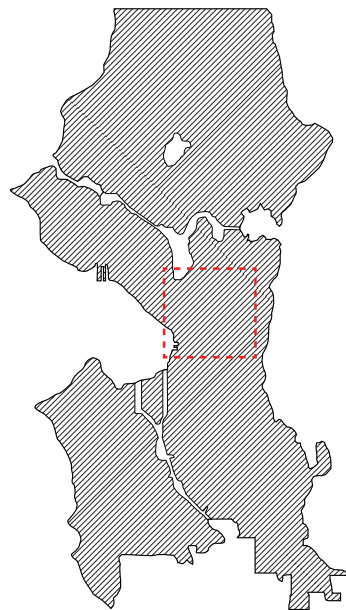
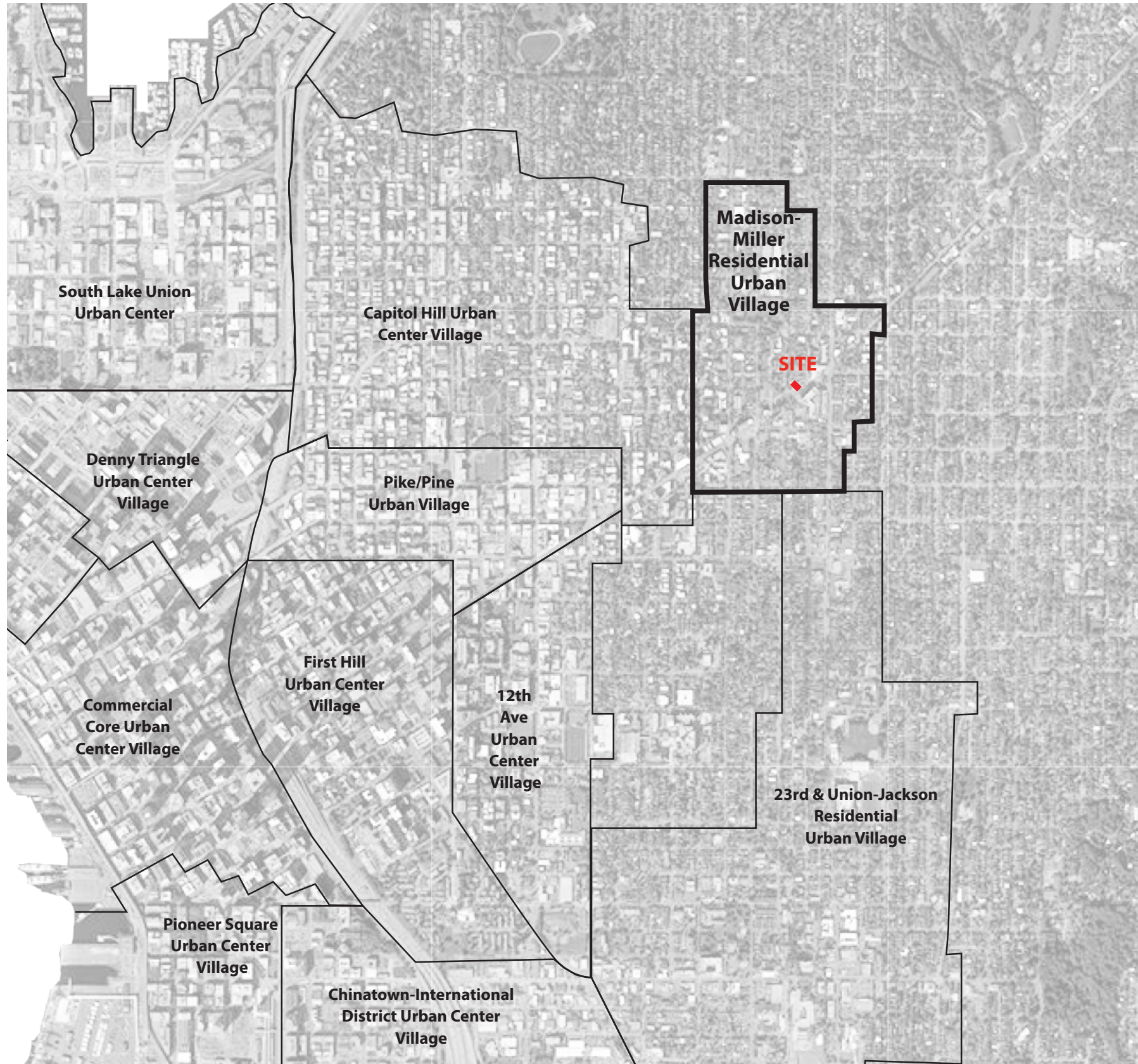


2100 E MADISON: MIXED-USE BUILDING
EARLY DESIGN GUIDANCE MEETING • JULY 22, 2015
2100 E MADISON ST • PROJECT NO. 3020124





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Project Information

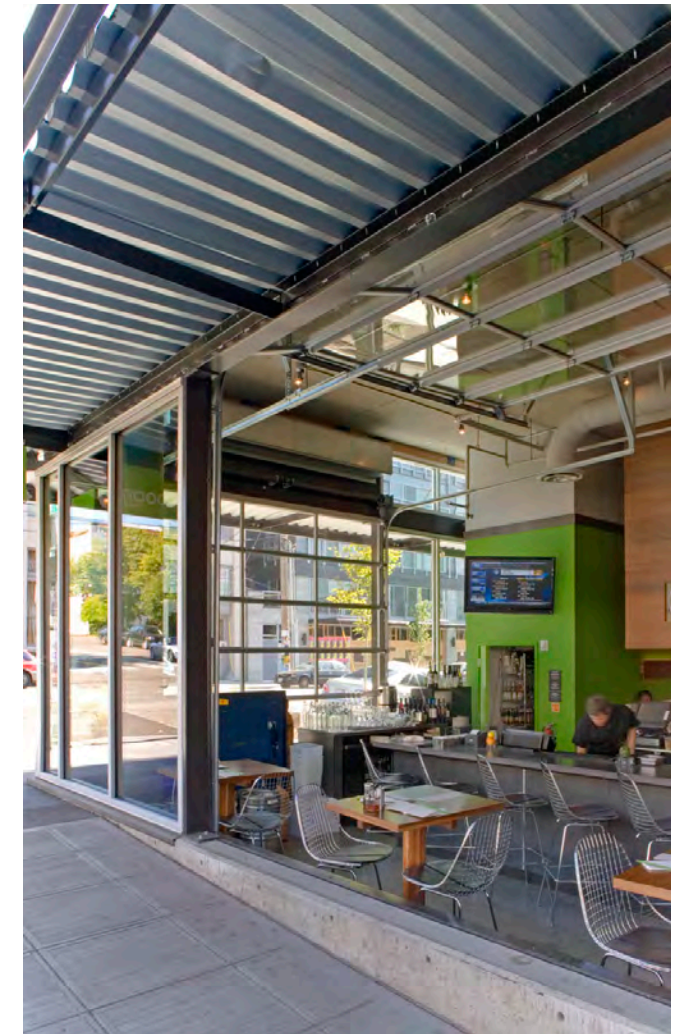
Property Address:	2100 E Madison St Seattle WA
Owner:	2100 East Madison Holdings LLC
Developer:	JCMV LLC T (206) 404-4019
Architect:	Weinstein A+U LLC Daniel Goddard T (206) 443-8606

PROPOSAL AND PROJECT SUMMARY

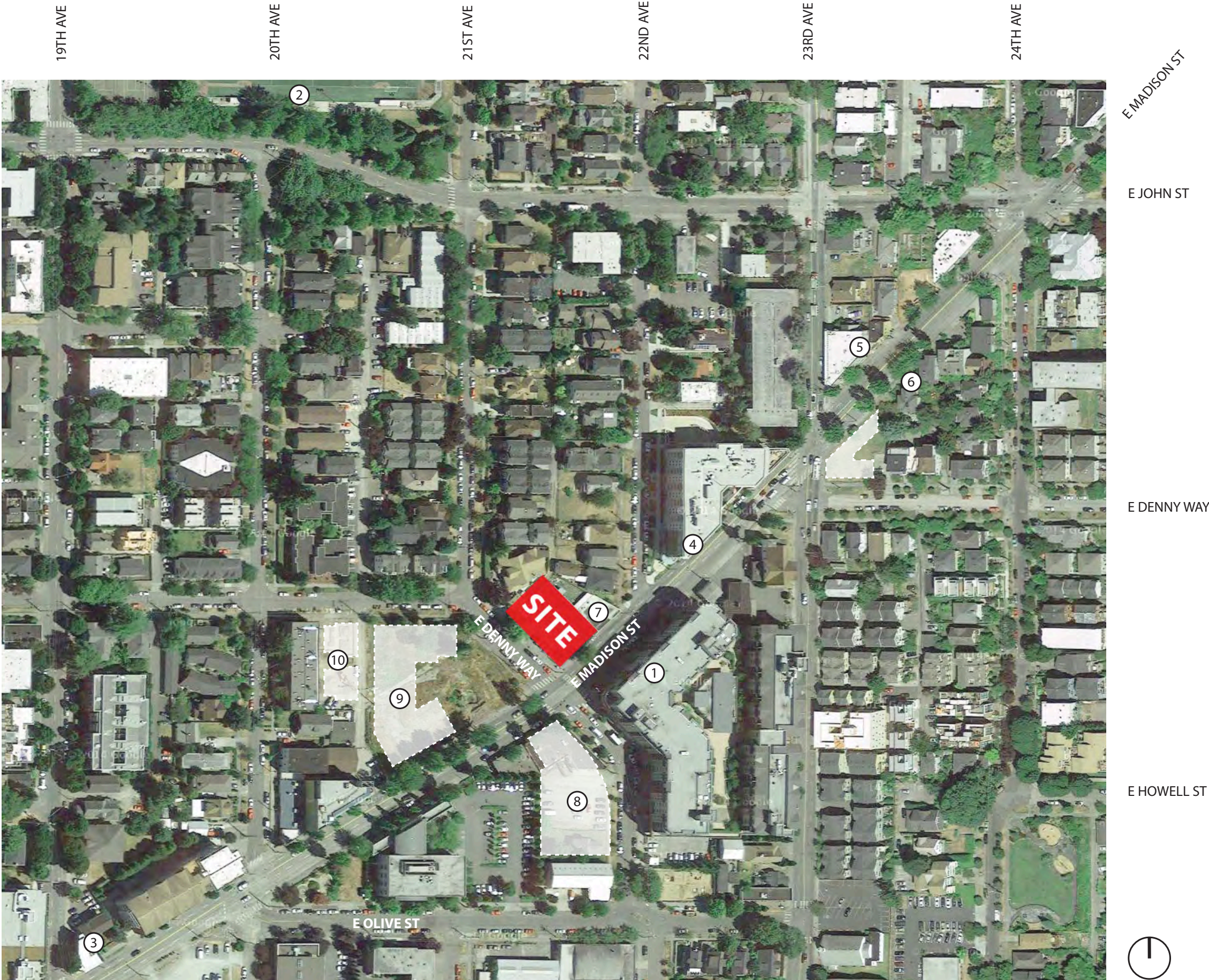
The proposed project will be a six-story, market rate apartment building approximately 65-feet in height and approximately 39,325-SF in area (above grade). A below grade parking accounts for an additional 8,400-SF in area. The building contains 50 residential apartment units, a resident lobby, exterior landscaped roof deck, balconies, and patios. There are three retail spaces, totalling approximately 3,800 SF. Two of the retail spaces can be combined to create a larger commercial space on East Madison Street. As the project is located in an urban village, parking is not required per SBC 23.54.015. However, 22 residential spaces are being designed for this building.

The project's development objectives are as follows:

- Provide a high quality living environment for residents convenient to work, leisure, recreation, and transit
- Enhance the quality of the streetscapes with an appropriately scaled façade that reflects the neighborhood's growth and ambitions
- Be sensitive to the light and view access of neighboring buildings
- Enhance the quality of the pedestrian environment in response to the specific demands of each street frontage
- Provide a lively environment for urban residents, including accommodations for retail and restaurants
- Be a good neighbor. This will inform the project in terms of:
 - Commercial level streetscape design
 - Landscape design and material selection
 - Exterior lighting design
 - Parking access
 - Solid waste and recycling storage



SITE CONTEXT



Location
 The proposed project is located on the far east side of Seattle's Capitol Hill neighborhood on the north corner of the intersection of E Madison Street and E Denny Way. The site is located within the Madison-Miller Residential Urban Village.

The project site is comprised of a single parcel bounded by E Madison Street to the southeast, E Denny Way to the southwest, a narrow unimproved alley to the northwest, and abuts a commercial structure to the northeast. The site is approximately 110' southwest to northeast and 80' southeast to northwest with a total site area of approximately 8,800-sf.

- 1. Safeway, Subway, Madison Street Animal Hospital
- 2. Miller Playfield
- 3. El Gallito
- 4. Queen Bee Tasty Food & Coffee, Aegis Living
- 5. Emerald City Crossfit, Cafe Shar
- 6. Crush
- 7. De' Charlene Beauty Shop and Boutique / Central Area Chamber of Commerce
- 8. 2051 E. Madison (Future)
- 9. 2020 E. Madison (Future)
- 10. 2005 E. Denny Way (Future)

EXISTING SITE PLAN

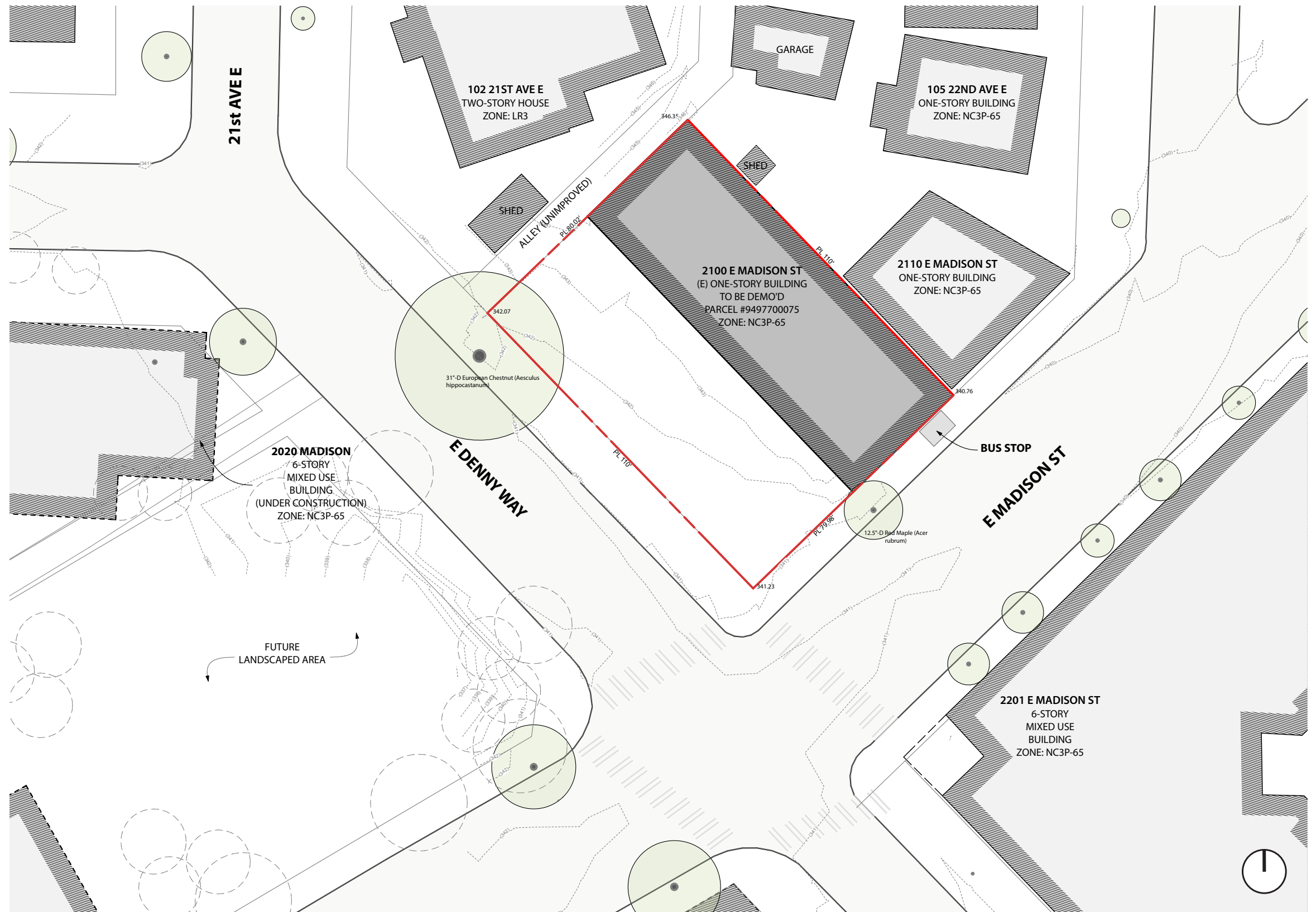
Existing Uses & Structures

Until recently the site has been the location of a Courtesy Tire Center comprised of a single-story, concrete block structure abutting the northeast property line with a surface parking lot separating it from E Denny Way. The tenant has vacated the building and no uses currently occupy the site.

Physical Features & Context

The property is on the east side of the crest of Capitol Hill along a relatively flat section of E Madison Street. The site's topography is very benign with a gentle downward slope from north to south of approximately three feet, with the frontages along E Madison Street and E Denny Way nearly flat. The site's frontage along E Madison is approximately 80' in length with only about nine-feet between the curb and property line. A King County Metro bus shelter is located to the east end of the property's frontage and a single street tree (a 12.5" caliper maple) is located just to the east of an 18' curb cut at the site's midpoint. The site's E Denny Way frontage is its longest at 110' and most generous with approximately 15' between the curb and property line. A large street tree (a 31" caliper Horse Chestnut) is located near the alley and north of a 20' curb cut.

As noted above, the site is bounded by public rights-of-way to the southeast (E Madison Street), to the southwest (E Denny Way) and to the northwest (alley). A small single-story commercial structure containing De Charlene's Beauty & Boutique, a salon and beauty school, abuts the site to the northeast and is the only lot immediately adjacent to the site. Across the alley to the northeast is a two-story wood-framed residential structure that currently houses the Madison Inn Work Release. The lot across E Denny Way to the southwest (currently vacant) was the former home of Deano's Grocery and is soon to be re-developed as a six-story mixed-use apartment building. On the opposite side of E Denny Way's intersection with E Madison Street is the former location of the Twilight Exit, which is currently under construction for a six-story mixed-use apartment building. Directly opposite the site across E Madison is The Summit at Madison Park, a six-story mixed-use apartment building home to Safeway grocery store.



EXISTING AERIAL VIEW



Project Site Under Current Zoning
 Neighboring Developments with permits or in construction

* Zoning Heights Shown As Approximation \ Not to Scale

SITE CONTEXT: LAND USE

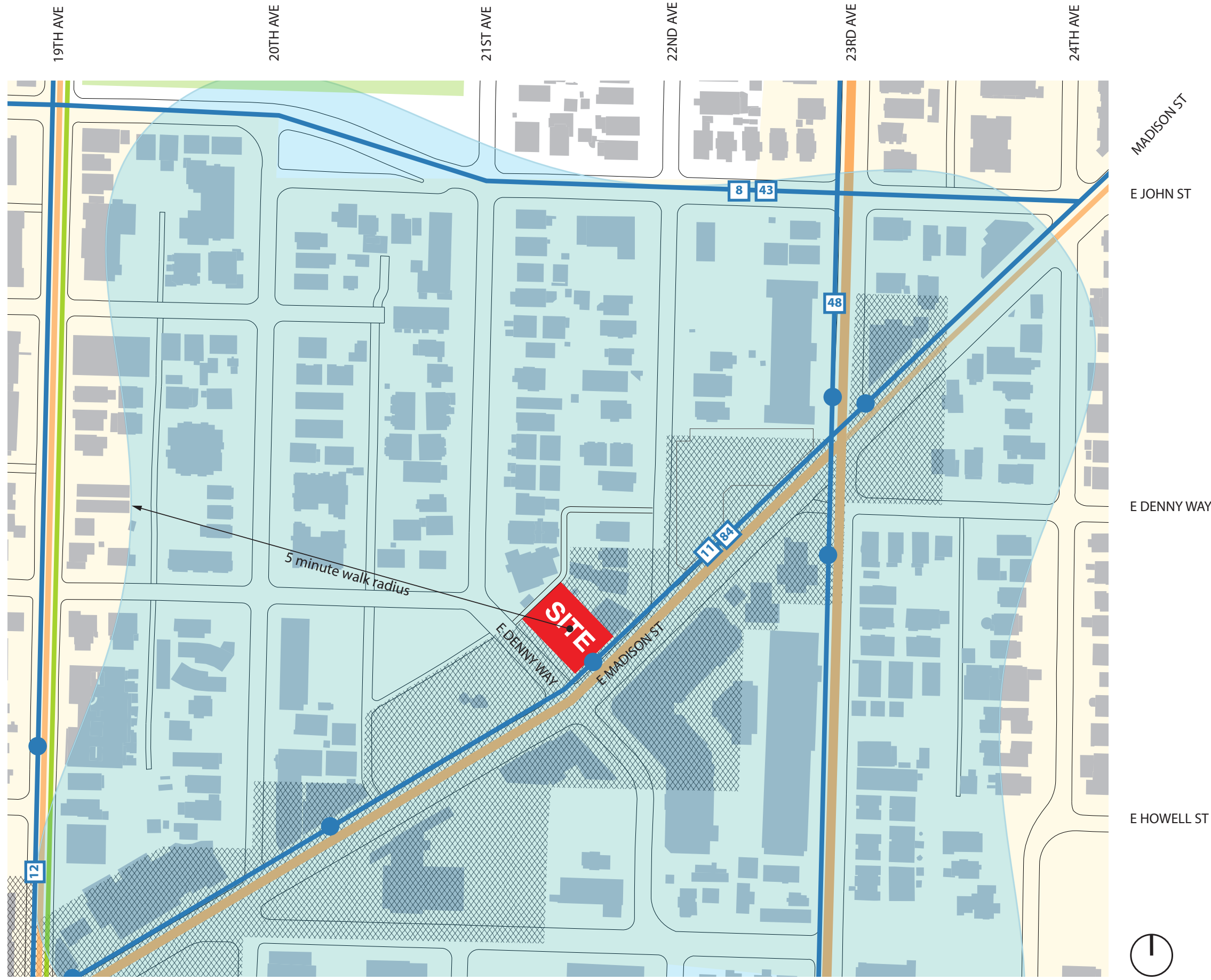
As observed at the introduction of the site and its surrounding, the proposed project is located at a rapidly changing intersection and is the last of the corners to be proposed for redevelopment. Consistent with the zoning of the neighborhood, E Madison Street is lined by a number of large footprint mixed-use and multi-family buildings. The residential grain of the neighborhood to the north of the site is still visible, but is also changing with a number of small apartment buildings typical of Capitol Hill and an increasing number of townhouse units largely constructed over the past 15 years.



Legend

- Future Planned Development / Under Construction
- Single Family Residential
- Multi Family Residential
- Mixed Use
- Retail
- Office
- Religious
- School / Institutional
- Utility
- Parks / Open Space

SITE CONTEXT: TRANSIT MODES



Several bus lines are proximate to the site, including a stop in front of the site along E Madison Street that serves Metro routes 11 and 84 that provide convenient access to the Downtown retail core and Madison Valley. A block and a half north, on E John Street, are two additional bus lines providing additional service to the Downtown retail core and the University of Washington (Metro 43); and connections to Lower Queen Anne via South Lake Union, as well as south to Rainier Valley (Metro 8). The Metro 48 bus line is also located two blocks east of the site along 23rd Avenue and provides connections to Ballard via the University of Washington to the north and Mount Baker to the south.

The character of E Madison Street near the site provides a moment of calm along an otherwise busy arterial. The relative flatness of E Madison Street coupled with a slight bend in its geometry, mature street trees on either side of the street west of the site, and on-street parking work to create a pedestrian environment that is conducive to walking. This environment is bolstered by the emergence of a nearby grocery store, retail, restaurants and several religious centers all located within a 5-minute walk of the project site along E Madison. Miller Playfield, to the north, is also within a 5-minute walk of the site, with the Miller community center, Meany Middle School and a small neighborhood commercial center located just outside the walk radius.

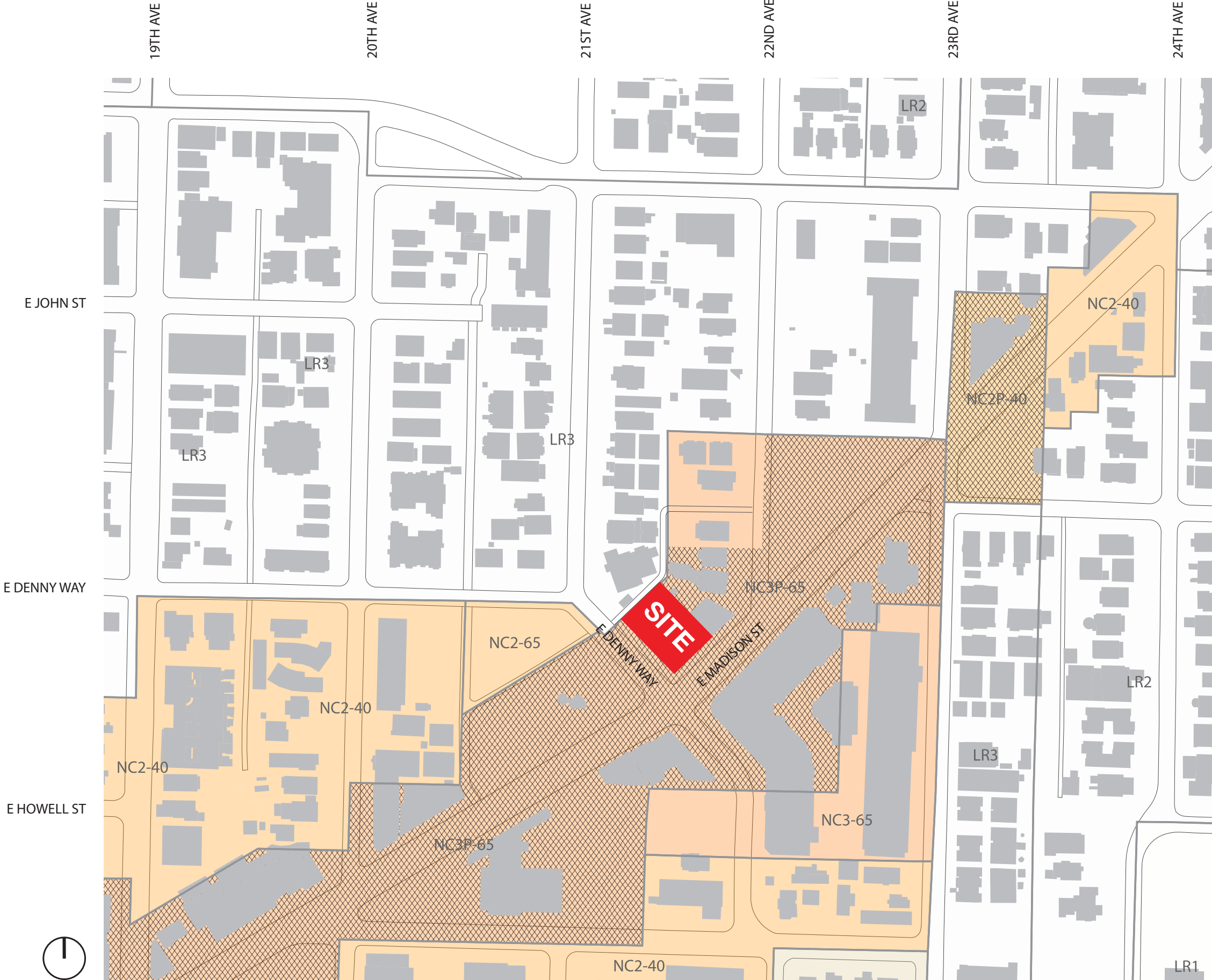
Legend

- Main Car Arterials:**
 - Principal Arterial / Regional Connector
 - Minor Arterial / Commercial Connector
- Frequent Transit Corridors
- Pedestrian Overlay
- 5 Minute Pedestrian Walkshed
- Bus Routes
- Bus Lines 2
- Current Bicycle Routes

CONTEXT: EXISTING ZONING

Existing Zoning

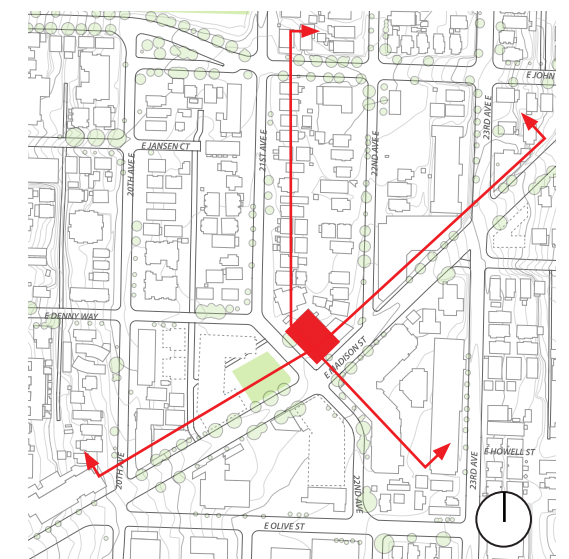
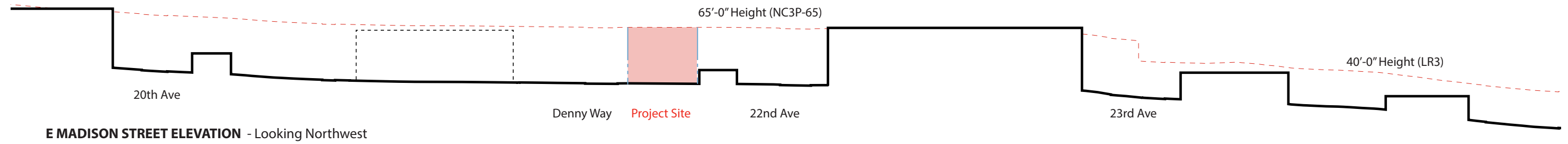
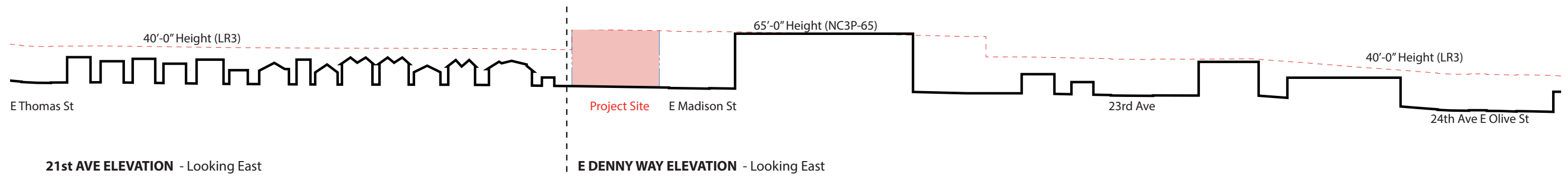
The project site is located within the NC3P-65 zone within in the Madison-Miller Residential Urban Village. It is located along the E Madison Street corridor, which serves as the commercial spine for the neighborhoods to its north and south, which typically are zoned LR3. The project site abuts on other parcel to the northeast which is also zoned NC3P-65. The parcels to the south (across E Madison Street) and west (across E Denny Way) of the site are also zoned NC3P-65. The parcel across the alley to the north of the site is zoned LR3.



Existing Zoning Legend

- NC1
- NC2
- NC3
- LR1 & LR2
- Zone Boundaries
- Pedestrian Overlay

CONTEXT: ZONING ENVELOPE



CONTEXT: E MADISON

The site's location on E Madison Street presents views to Lake Washington and the Cascades beyond. Just two blocks east of the site, E Madison ceases to be classified as a major arterial, giving way to 23rd Avenue and transforming instead into smaller scale residential. Vehicular traffic is still prominent at this end of Madison.

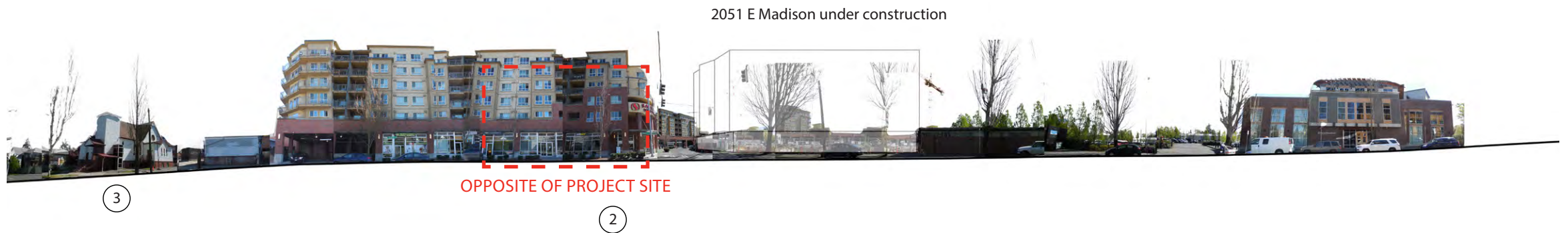
The character of E Madison Street adjacent to the site presents conditions found elsewhere along E Madison Street. More recent construction near the project site includes the 6-story Aegis on Madison (a block to the northeast), the Summit at Madison Park (opposite the site), a four-story office building for Planned Parenthood and a 5-story office building to the southwest. Further transformation is in the works with a 95-unit mixed-use building under construction at 2051 E Madison Street and another 160+ mixed-use building in permitting for 2020 E Madison.



PROJECT SITE

①

E Madison St - Looking Northwest

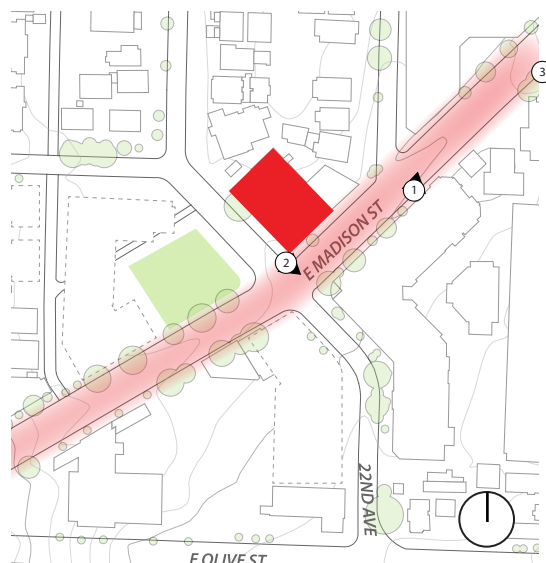


2051 E Madison under construction

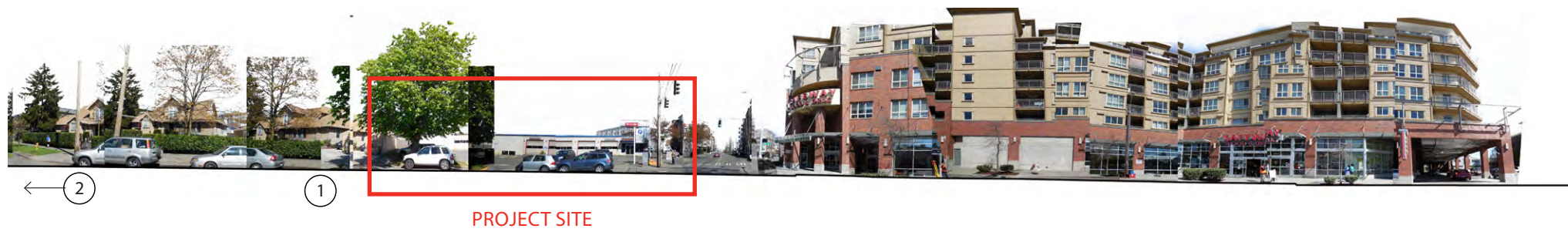
OPPOSITE OF PROJECT SITE

E Madison St - Looking Southeast

1. Aegis Living
2. The Summit at Madison Park
3. Madison Temple Church of God



CONTEXT: E DENNY WAY

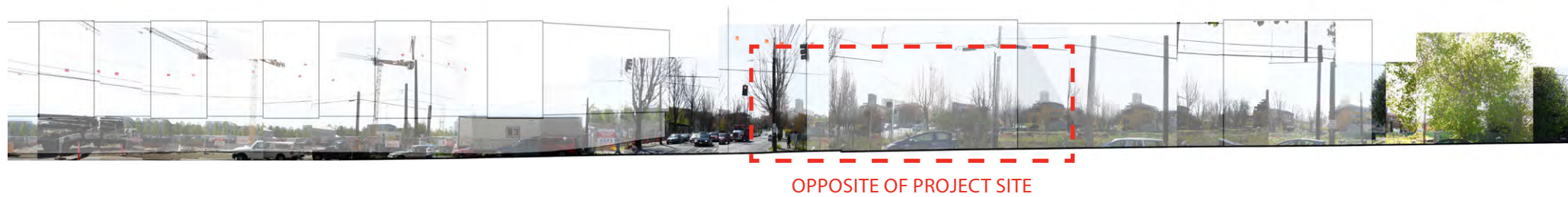


E Denny Way - Looking Northeast

Bordering the site to the southwest, E Denny Way also forms the dividing line between residential and commercial zoning on the north side of E Madison Street. Immediately adjacent to the site are low-rise attached and semi-detached townhouses and apartment buildings intermixed with older single family dwellings further to the north of the site. Consistent with their exclusively residential uses, the streets are typically quiet and generously planted with on street parking on either side of the streets. Residences new and old are typically set back from the street with landscaped areas delineating the boundary between public and private.

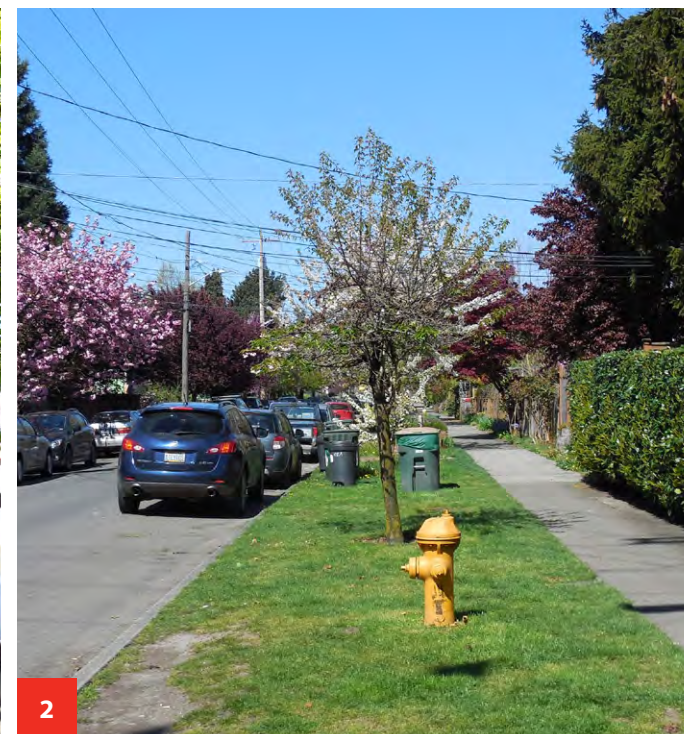
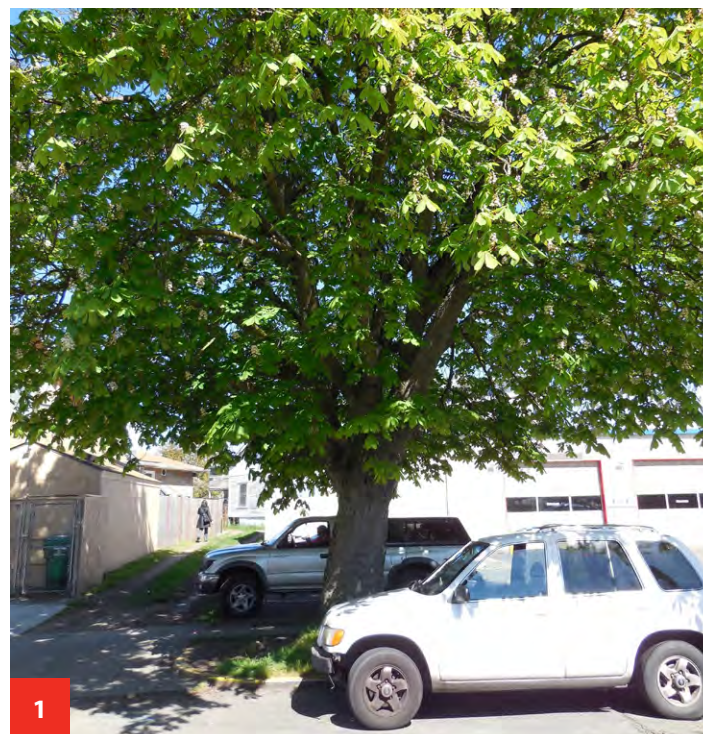
2051 E Madison under construction

2020 E Madison under construction

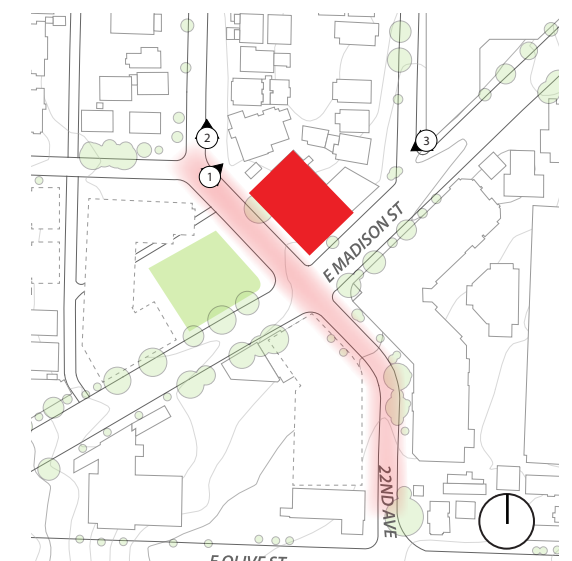


E Denny Way - Looking Southwest

Across E Madison Street, E Denny Way changes into 22nd Avenue. 22nd Avenue is flanked by six-story mixed-use apartment buildings for its first block south of E Madison before giving way to a neighborhood comprised primarily of older single-family dwellings



1. View of site and alley from Denny
2. 21st Ave Streetscape
3. Northeast Neighbors on 22nd Ave E



CONTEXT: EXISTING SITE

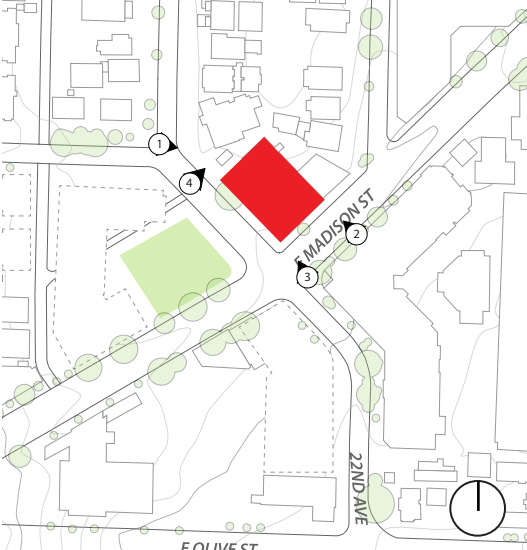
As previously indicated, the site was home to a Courtesy Tire Center until recently. The site is approximately 80' (along E Madison Street) by 110' (along E Denny Way). The existing structure and associated paved areas will be demolished, as the proposed project will occupy nearly the entire site.

The existing property has two curb cuts—one along each street frontage. The curb cut accessed from E Denny Way is approximately 20' in width and located just west of the site's midpoint. The curb cut located along E Madison Street is approximately 18' wide and located just to the west of the site's midpoint.

There is no landscaping currently located on the project site, but a relatively wide planting strip runs the length of the E Denny Way frontage interrupted only by the curb cut. The planting strip is largely grass except for a large 31" caliper Horse Chestnut tree located at its north end approximately 6'-8' south of the alley. The frontage along E Madison is fully paved except for a 12.5" maple tree located in a 4'x10' tree well just east of the curb cut.



- 1. West corner of site
- 2. View across E Madison
- 3. South corner of site
- 4. View of Alley



CONTEXT: ADJACENT NEIGHBORS



1



2



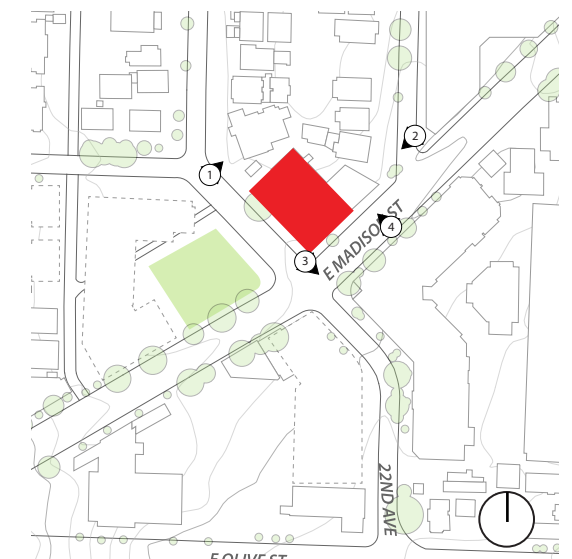
3



4

To the east of the site is a single-story wood-framed commercial building (De Charlene's Beauty & Boutique). Across the alley to the northeast is a two-story wood-framed residential structure that currently houses the Madison Inn Work Release. The parcel's across E Denny Way to the southwest and on the opposite side of E Denny Way's intersection with E Madison Street are both vacant and in the process of being redeveloped—the former in for construction permitting and the latter currently under construction. Both will be six-story mixed-use apartment buildings. Opposite the site across E Madison is The Summit at Madison Park, a six-story mixed-use apartment building containing Safeway grocery store and several small retail spaces.

1. Washington Work Releases
2. 2051 E Madison (Future)
3. 2020 E Madison (Future)
4. De' Charlene Beauty Shop



SITE ANALYSIS

Topography

- Gently sloping from north to south
 - 3' change in elevation with the site's high point located at its north corner
- Relatively flat along both street frontages
 - 1' change in elevation along E Denny Way, dropping to south
 - Less than 4" change in elevation along E Madison Street, dropping to the east

Neighboring buildings

- Single-story wood-framed commercial building (DeCharlene's Beauty & Boutique) to the east
- 6-story mixed-use apartment building to the south (The Summit at Madison Park)
- Two 6-story mixed-use apartments buildings under construction/in for permit to the west and south of the site

Solar Access

- Good light access to the west will be diminished by proposed construction
- Good light access to the south due to street geometry

Views

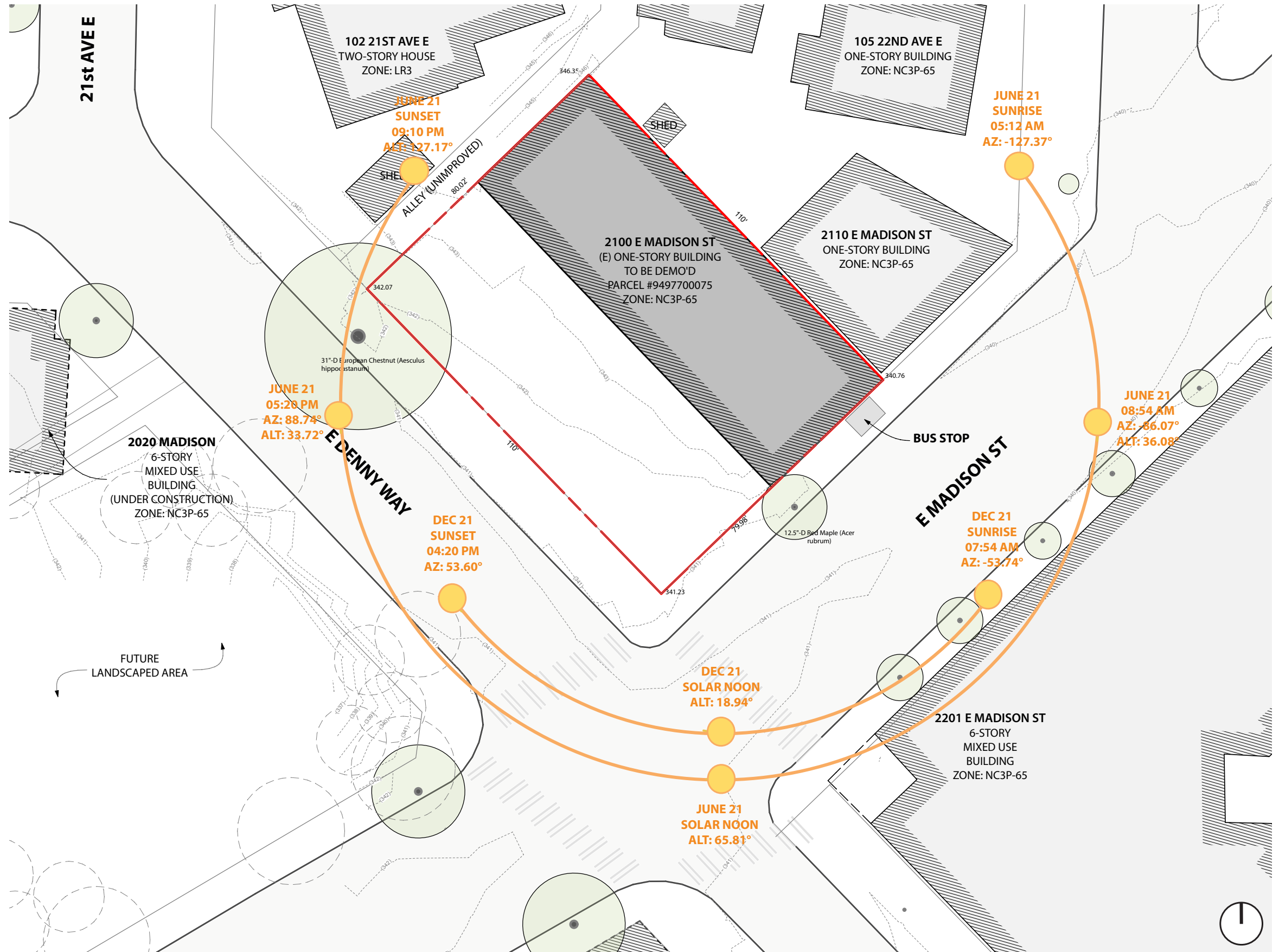
- Territorial views to the north and east of the project site with potential views of the Cascades and Lake Washington possible at the upper levels

Structure height

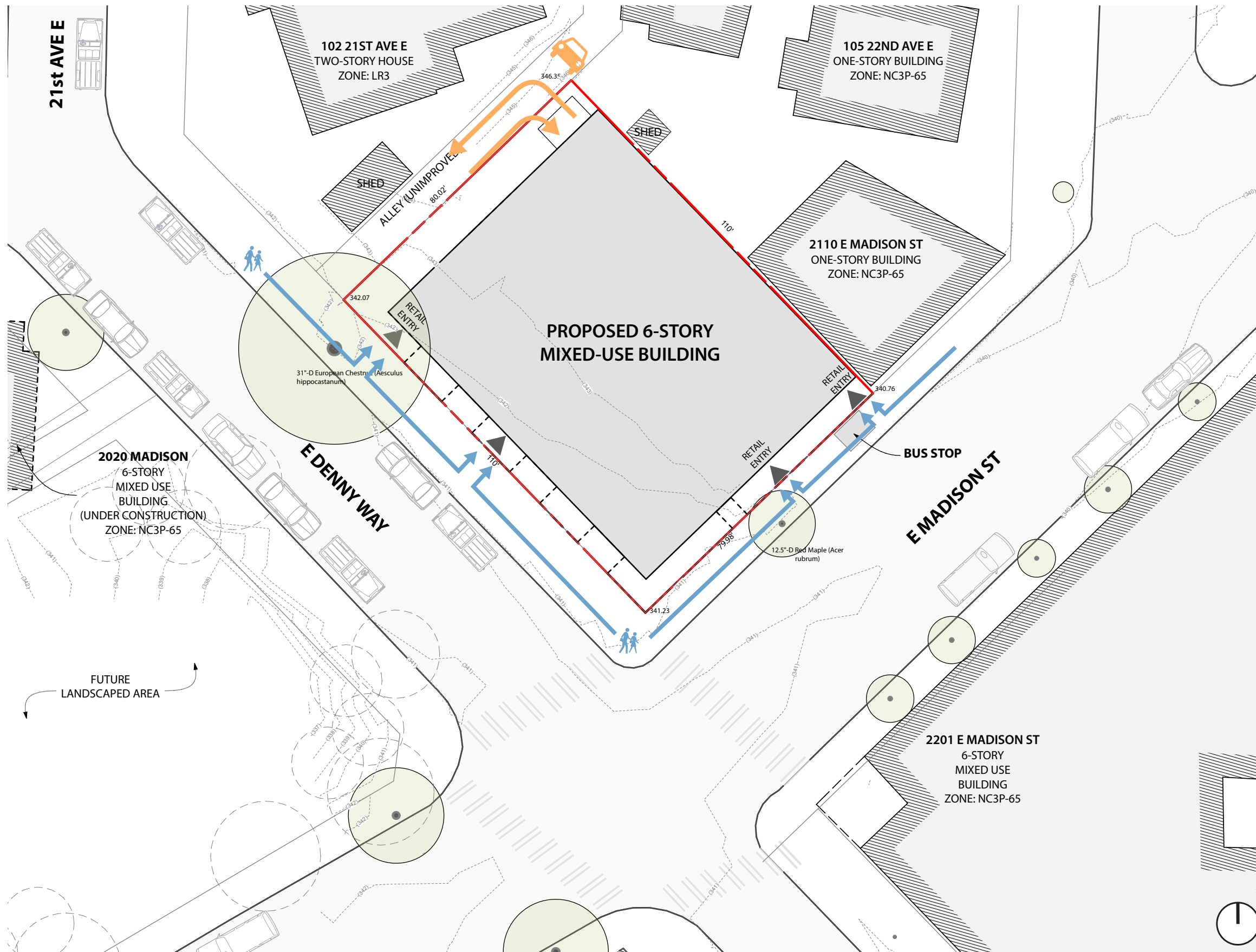
- Zoned as NC3P-65 with a base height limit of 65'
- 13' floor to floor minimum required at street level

Allowable building area

- The maximum FAR for the building site is 4.75, with residential uses limited to 4.25
- Site area: 8,800-sf
- Maximum allowable area: 41,800-sf
- Maximum allowable residential area: 37,400-sf



PRELIMINARY SITE PLAN



Setback Requirements

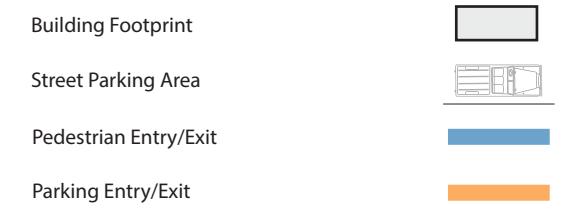
- No setback requirements from adjacent NC-zoned lot
- Variable setback from LR3 residential zones across an alley:
 - No setback up to 13'
 - 15' setback above 13' up to 40'
 - An additional setback of 1' for each 2' above 40'
- No high-voltage overhead powerlines located adjacent to site

Traffic & Circulation

- E Madison Street is an arterial and has continuous sidewalks on each side of street
- E Denny Way is a residential street and has continuous sidewalks on each side of street
- On-street parking located on either side of E Madison Street (except at bus stop) and E Denny Way
- Proposed parking access and trash collection from alley

Streetscape

- E Denny Way has continuous ±6' wide planting strips located between sidewalk and on-street parking with a mix of street tree species and sizes
- E Madison has narrow sidewalk and intermittent street trees with on-street parking except where bus stops are located



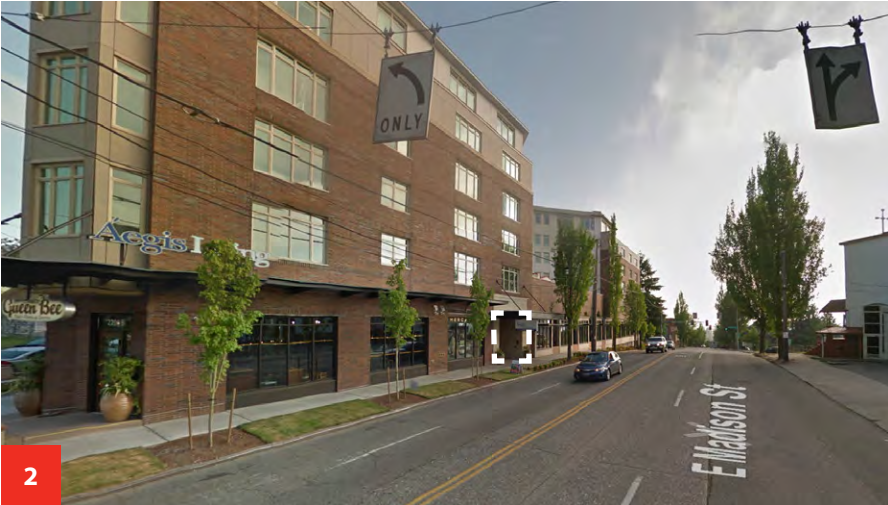
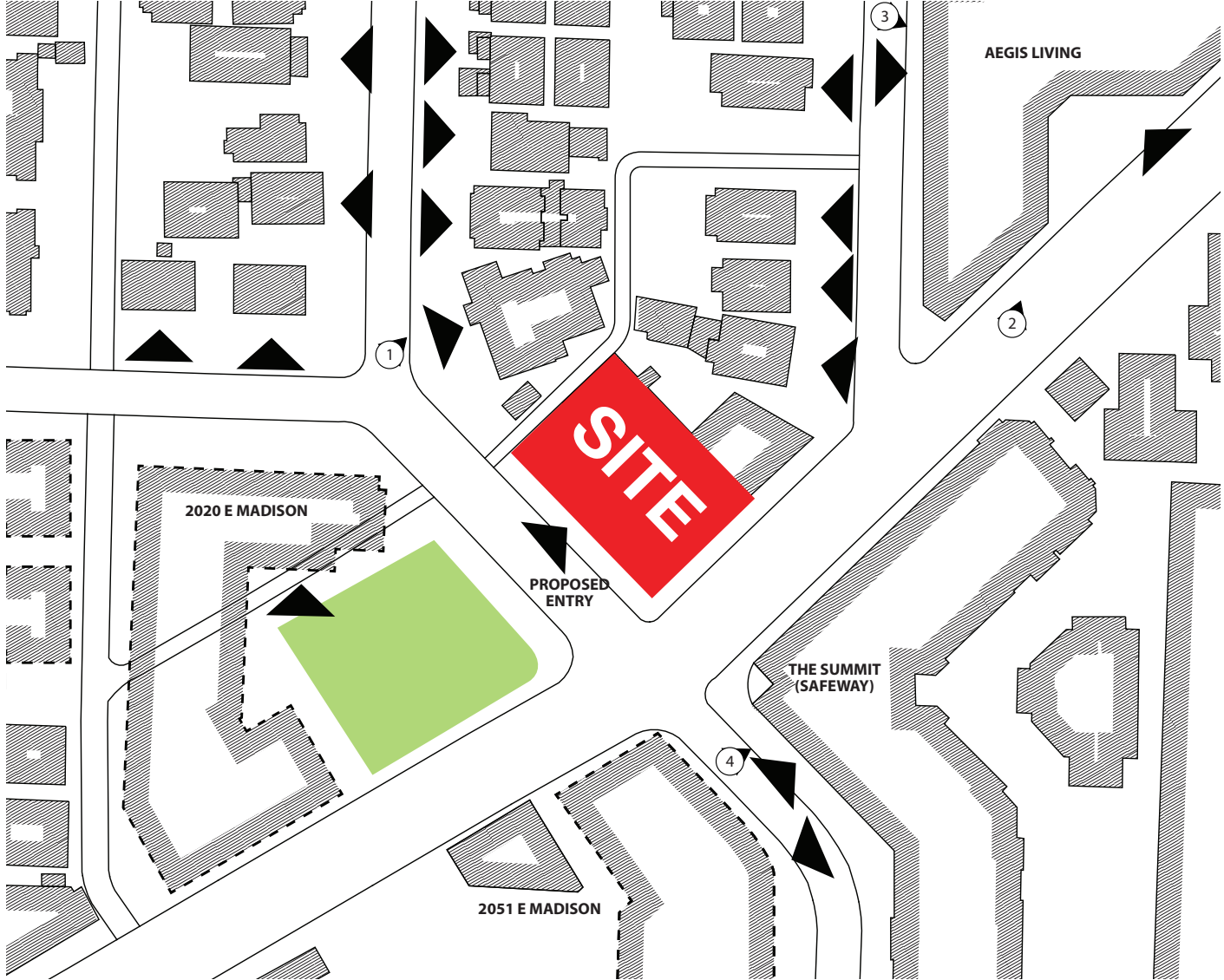
RESIDENTIAL ENTRY CONTEXT

Main Residential Entrance

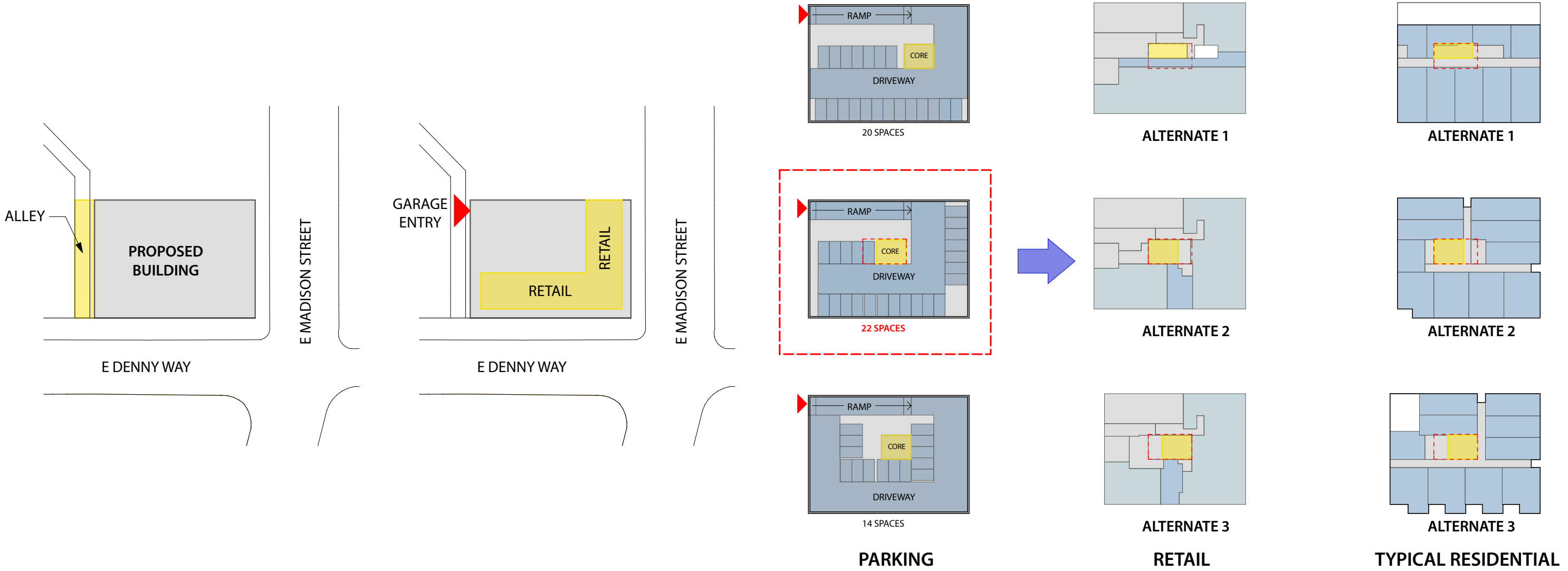
Contextually, the main residential entrances of the neighboring buildings are along the north-south avenues. E Denny Way, 21st Avenue E, and 22nd Avenue E are primarily residential, with a strong pattern of "front doors" looking onto the street. Given the arterial nature, E Madison Street's patterning is largely retail and commercial. East of the site on Madison, there is a residential entry for the Aegis Living building. However, the main residential entry (both pedestrian and vehicular) for this assisted living and memory care facility is from 22nd Avenue E.

At the site, the sidewalk on Madison is currently 9' - 6" in width and the sidewalk on Denny is currently 14' - 8". The proposed building include a 6' - 0" setback for the building at grade for both street frontages. Given the wider sidewalk and the consistency of the residential pattern, locating the main residential entry along E Denny Way is highly desirable.

It should be noted that the future development at 2020 E Madison Street incorporates an open landscaped area. The proximity of the entry to the park across the street will promote a dynamic synergy at this location in the neighborhood.



SITE ANALYSIS: PARKING DIAGRAMS



If the lot abuts an improved alley, access to parking shall be from the alley in NC zones, per SMC 23.47A.032.A.1.a.

To maximize the potential retail frontage along Madison Street and Denny Way, locate the driveway entrance and ramp to the northeast of the site.

No parking is required per SMC 23.54.015. Parking space dimensions and percentages are not applicable. 7.5' X 15' compact stalls assumed.

The most efficient garage layout provides for 22 parking spaces.

The vertical core location informs the plan layouts for the commercial (level 1) and the residential floors (levels 2 - 6).

ELEVATOR AND STAIR CORE (TO GARAGE)
 POSSIBLE CORE LOCATION

DESIGN PROPOSAL: ALTERNATE 1

Design Alternate 1 provides a residential entry off of East Madison Street. There are two retail spaces along Madison and one located along East Denny Way. The residential units are arranged along a double-loaded corridor, with units oriented to the northeast and to the southwest. The northeast property line is adjacent to another parcel, therefore it is set back fifteen feet at the residential levels to allow for 75% unprotected openings (sprinklered building).

Advantages:

- No design review departures requested.

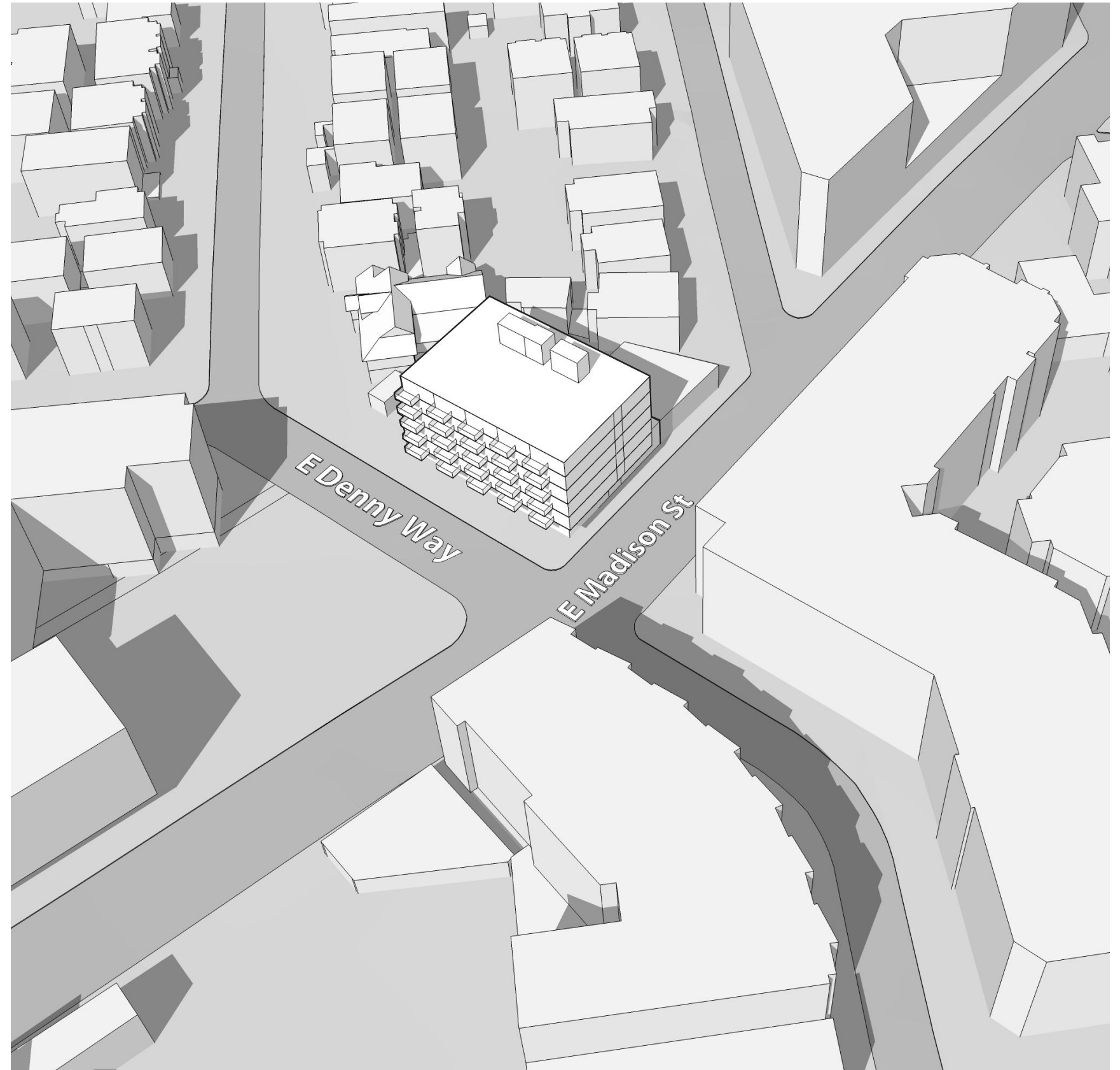
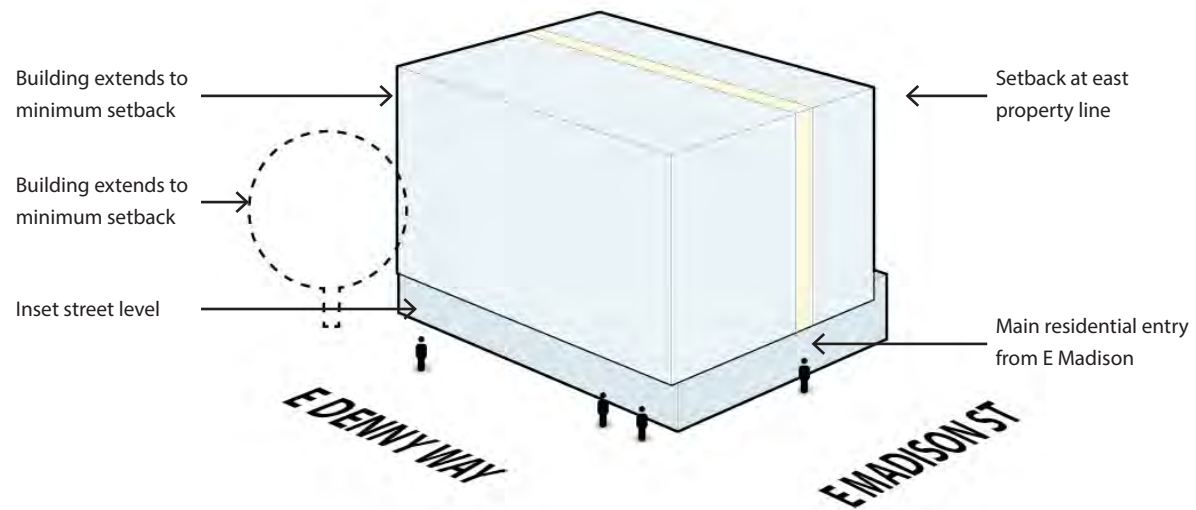
Disadvantages:

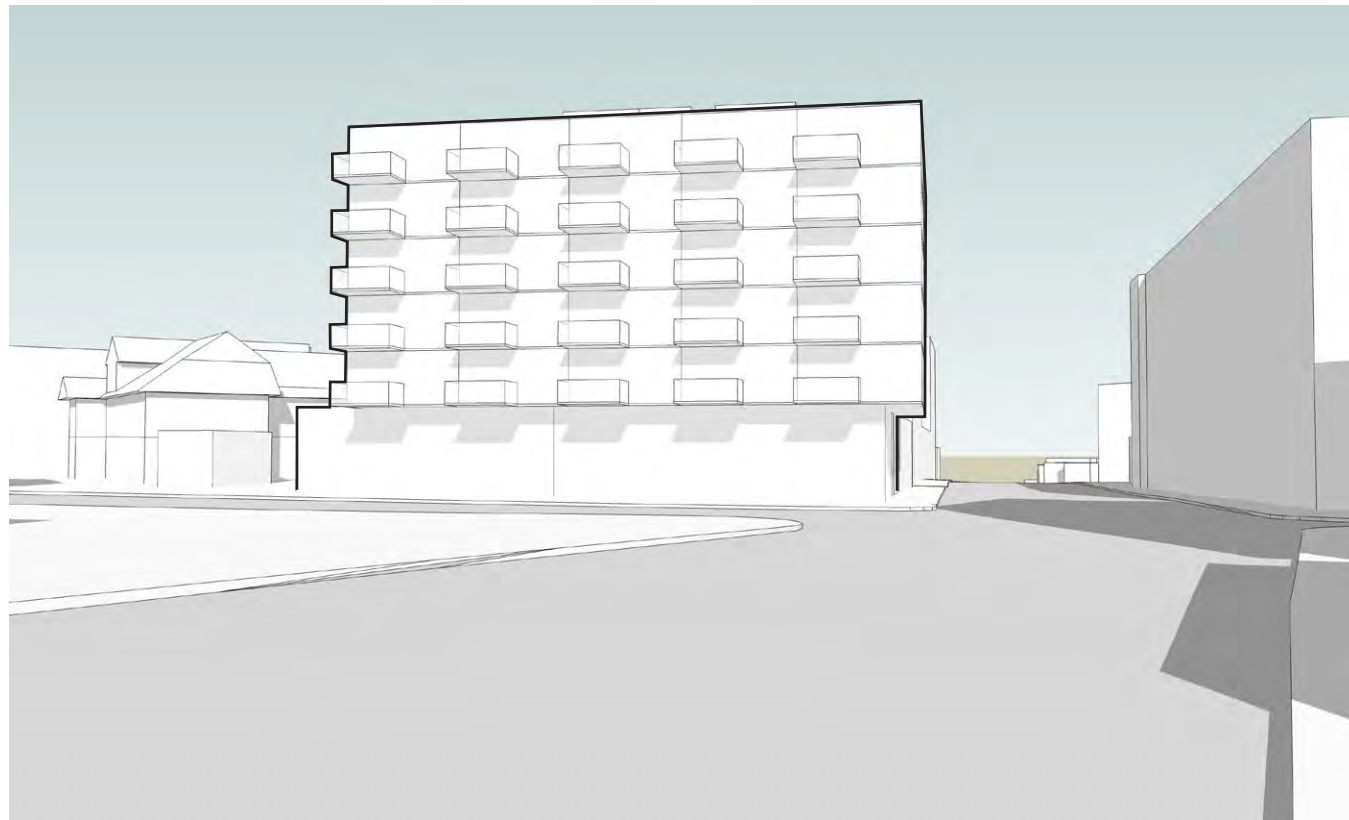
- Many of the units are oriented northeast; potential for view blockage if adjacent property is developed.
- Residential entry off of Madison is not as contextual as an entry off of Denny Way.
- Alley façade compliant with setback requirements, however does not consider non-conforming structure to the north.
- Tree at northwest corner of the site will need to be removed.
- Unit count is 5 less than other configurations (45 instead of 50).
- FAR utilization is 85%, which may not yield a feasible project.

Summary

Stories:	6 (5-over-1 + 1 level below grade)
Unit Count:	45
Commercial Area:	4,083 SF
FAR utilization:	85.6%
Amenity Area:	2,940 SF (750 SF roof deck, 2,190 SF level 2 patio [common and private])
Parking:	
Residential:	not required; 24 spaces provided
Commercial:	not required
Ground Floor Uses:	
Madison Street:	residential entry and retail
Denny Way:	retail

This option is code compliant.

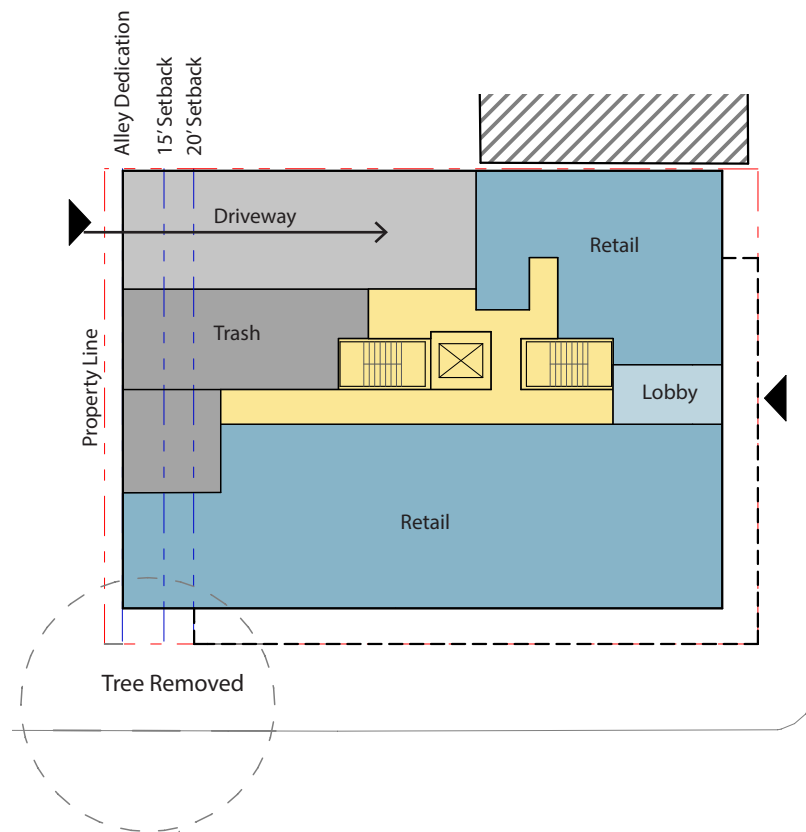




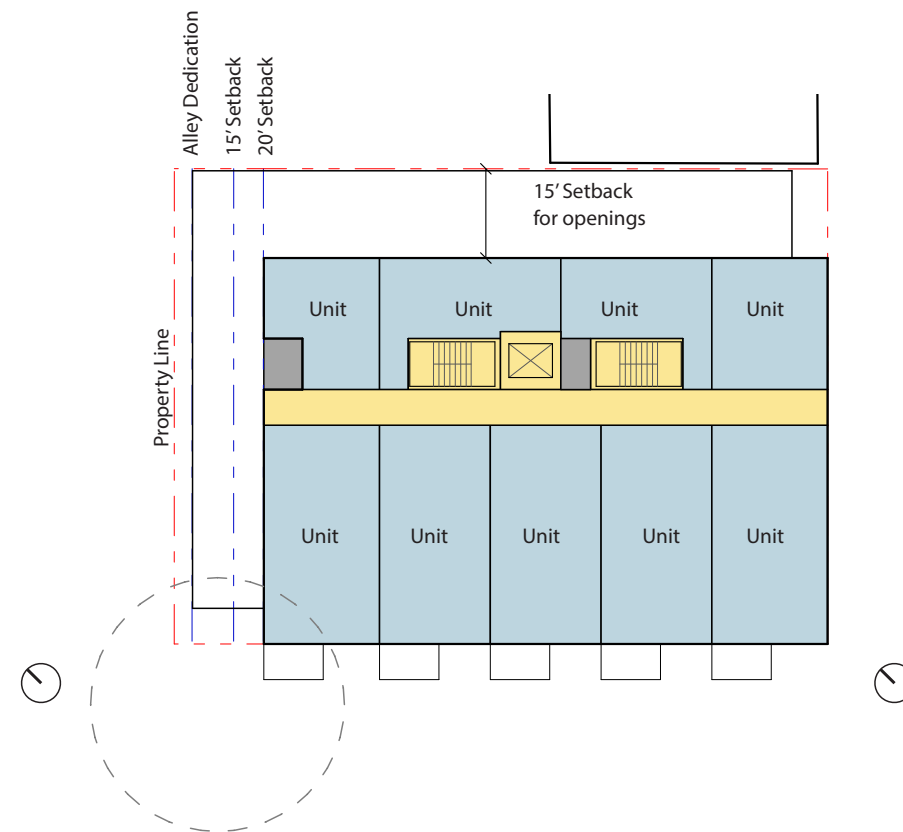
Looking East on Madison



Looking Across Madison Along Denny Way



Street Level Plan (Level 1)



Typical Residential Plan (Levels 2 - 6)



Looking Down Denny Way

DESIGN PROPOSAL: ALTERNATE 2

Design Alternate 2 provides a residential entry off of East Denny Way. Locating the residential entry off of Denny is preferred (per the SDOT Street Classification Map, Madison Street is classified as a principal arterial). There are two retail spaces along Madison, and one retail space along Denny to the north of the residential entry. The residential units are oriented in a "T" scheme. This maximizes the units with street frontage, and does not require openings along the northeast of the site, which is adjacent to another NC-65 parcel. The residential levels are set back from the northwest property line at the alley by twenty feet, allowing for a uniform elevation at the alley façade.

Advantages:

- Residential lobby off of Denny Way is preferred.
- Development under construction across from Denny is also a mixed-use building, with an open space across from the lobby.

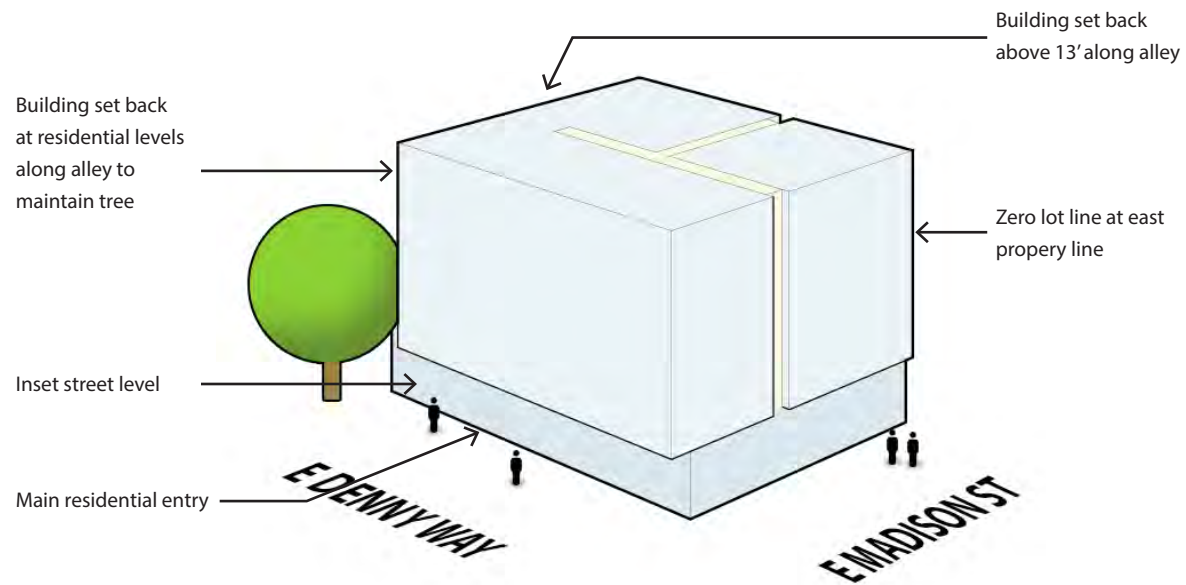
Disadvantages:

- Alley façade compliant with setback requirements, however does not consider non-conforming structure to the north.

Summary

Stories:	6 (5-over-1 + 1 level below grade)
Unit Count:	50
Commercial Area:	4,097 SF
FAR utilization:	99%
Amenity Area:	2,688 SF (750 SF roof deck, 879 SF level 2 patio, balconies)
Parking:	
Residential:	not required; 24 spaces provided
Commercial:	not required
Ground Floor Uses:	
Madison Street:	retail
Denny Way:	residential entry and retail

This option is code compliant.

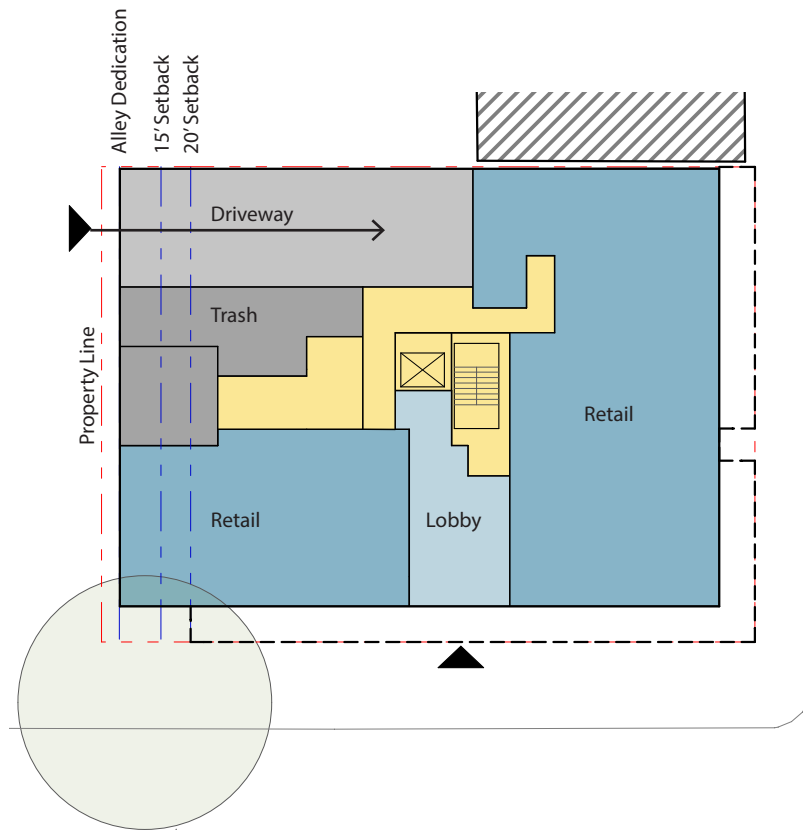




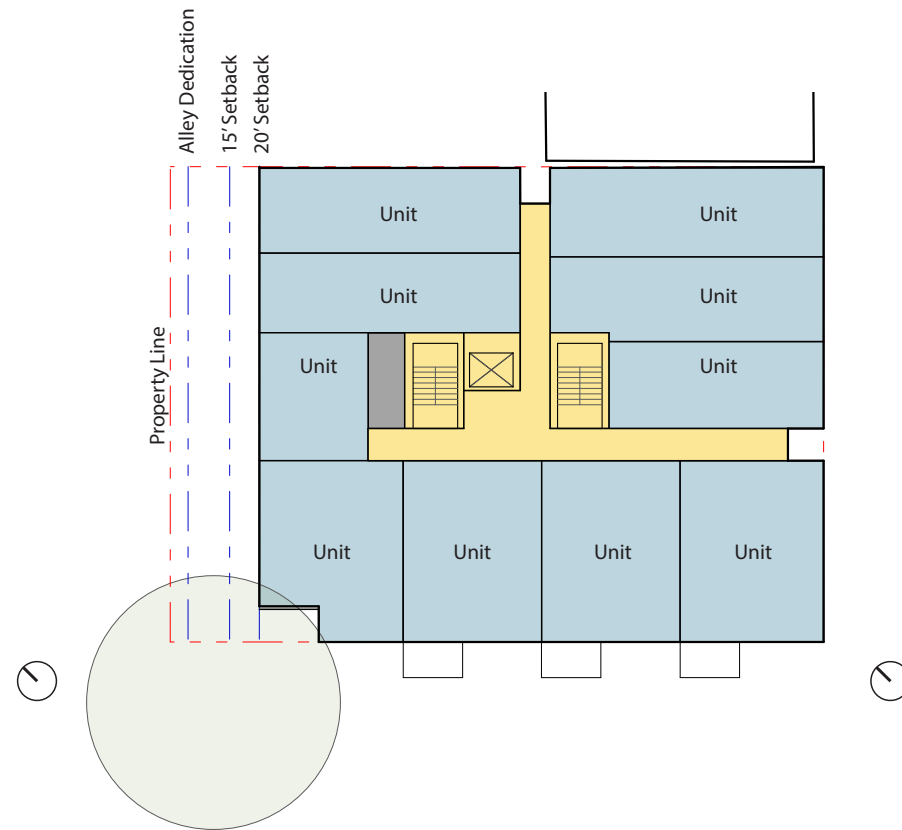
Looking East on Madison



Looking Across Madison Along Denny Way



Street Level Plan (Level 1)



Typical Residential Plan (Levels 2 - 6)



Looking Down Denny Way

DESIGN PROPOSAL: ALTERNATE 3

Design Alternate 3 locates the residential entry off Denny Way. Denny is the preferred location for a residential entry (per the SDOT Street Classification Map, Madison Street is classified as a principal arterial). There are two retail spaces along Madison, and a retail space along Denny to the north of the residential entry. The residential units are oriented in a "T" scheme. This maximizes the units with street frontage, and does not require openings along the northeast of the site, which has an adjacent to another NC-65 parcel. The residential levels are set back from the northwest property line at the alley by twenty feet, allowing for a uniform elevation at the alley façade.

Due to the desire to retain the 31" Chestnut tree at the west of the site, the building has been pulled back from the property line at the alley an additional 7' from the required alley setback. The building erodes at the west corner to allow for the tree canopy to meet the building. To maintain a façade rhythm due to the deference to the tree, inset balconies are utilized along the elevation along Denny Way. In response to the non-conforming two-story house across the alley, the residential levels of the building are set back approximately 30' at the north corner of the site. A departure for the alley setback required above 40' is being requested for the remainder of the alley elevation (request to maintain the 15' setback that is required from 13' - 40').

Potential Departure:

- Alley setback above 40'

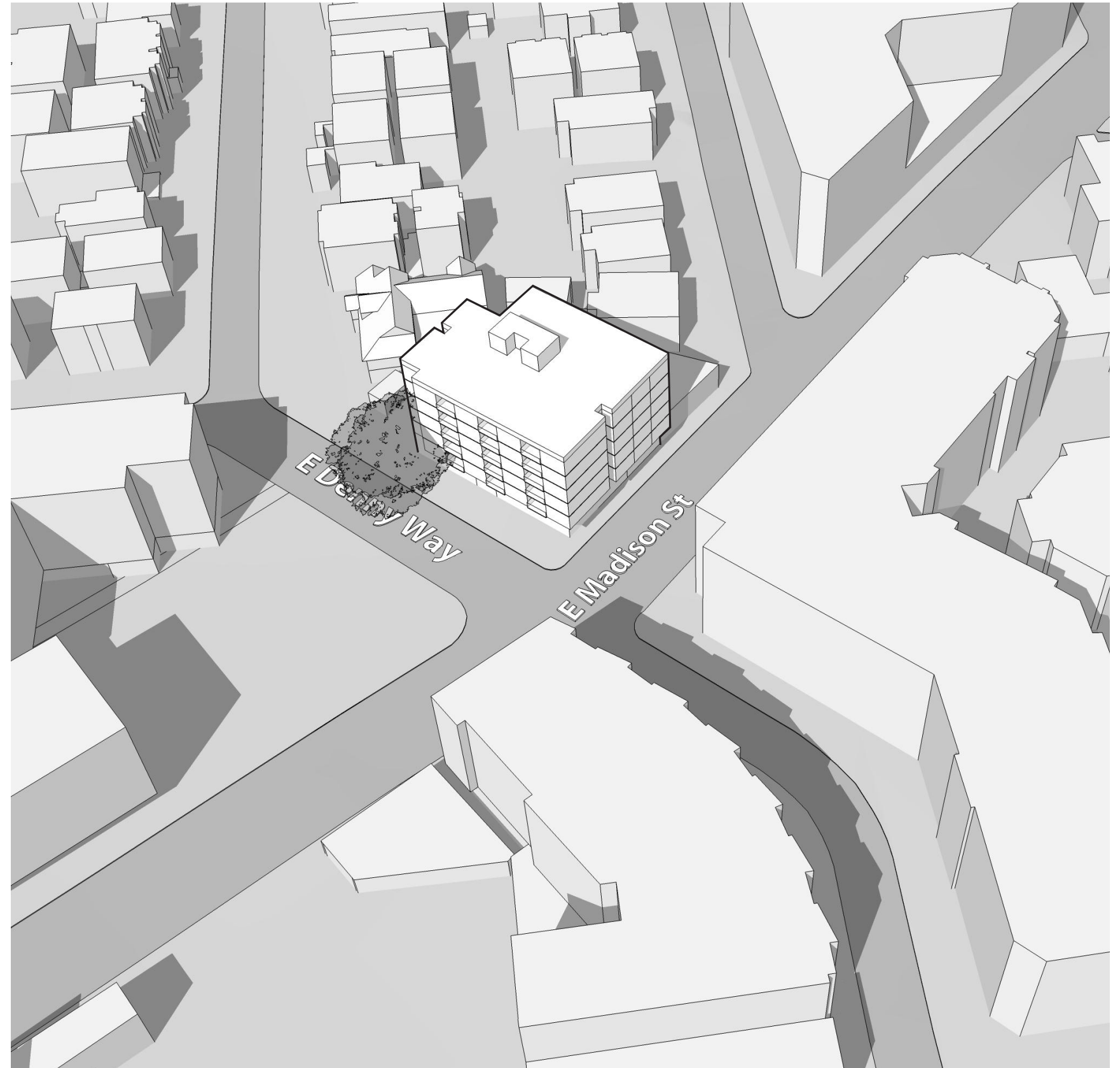
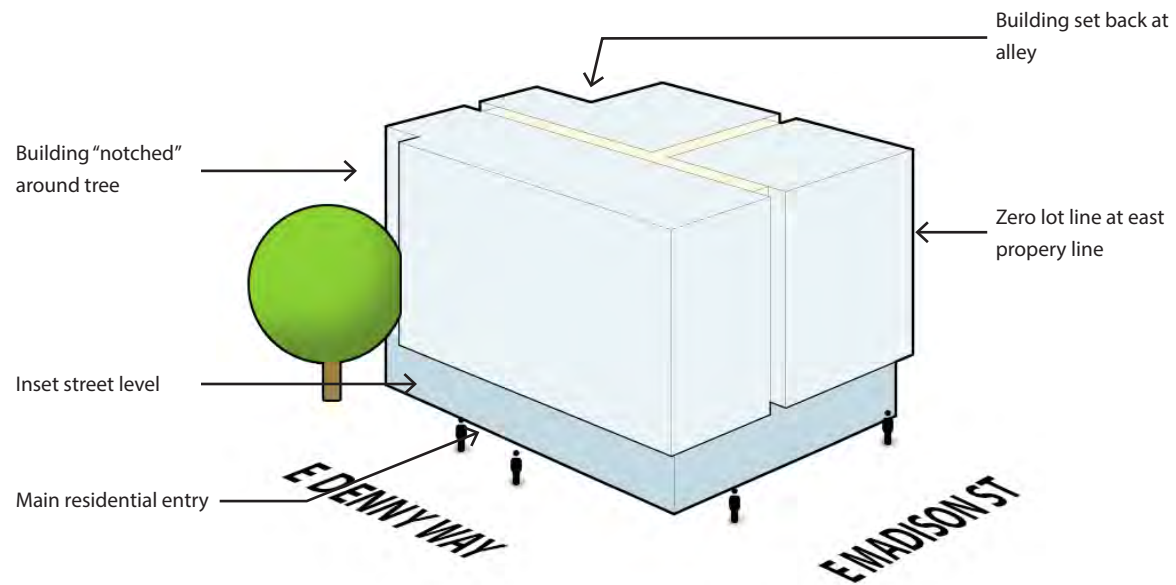
Advantages:

- Residential lobby off of Denny Way is preferred
- Development under construction across from Denny is also a mixed-use building, with an open space across from the lobby.
- Tree at northwest corner of the site can remain.

Summary

Stories: 6 (5-over-1 + 1 level below grade)
 Unit Count: 50
 Commercial Area: 3,797 SF
 FAR utilization: 94%
 Amenity Area: 2,232 SF
 (750 SF roof deck, 490 SF level 2 patio, balconies)

Parking:
 Residential: not required; 22 spaces provided
 Commercial: not required
 Ground Floor Uses:
 Madison Street: retail
 Denny Way: residential entry and retail

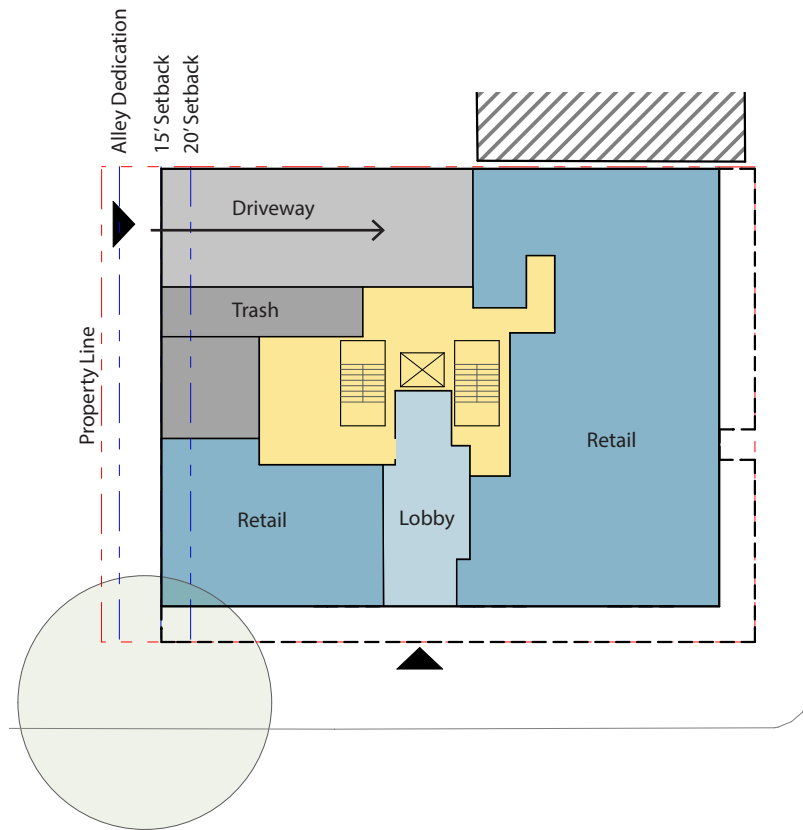




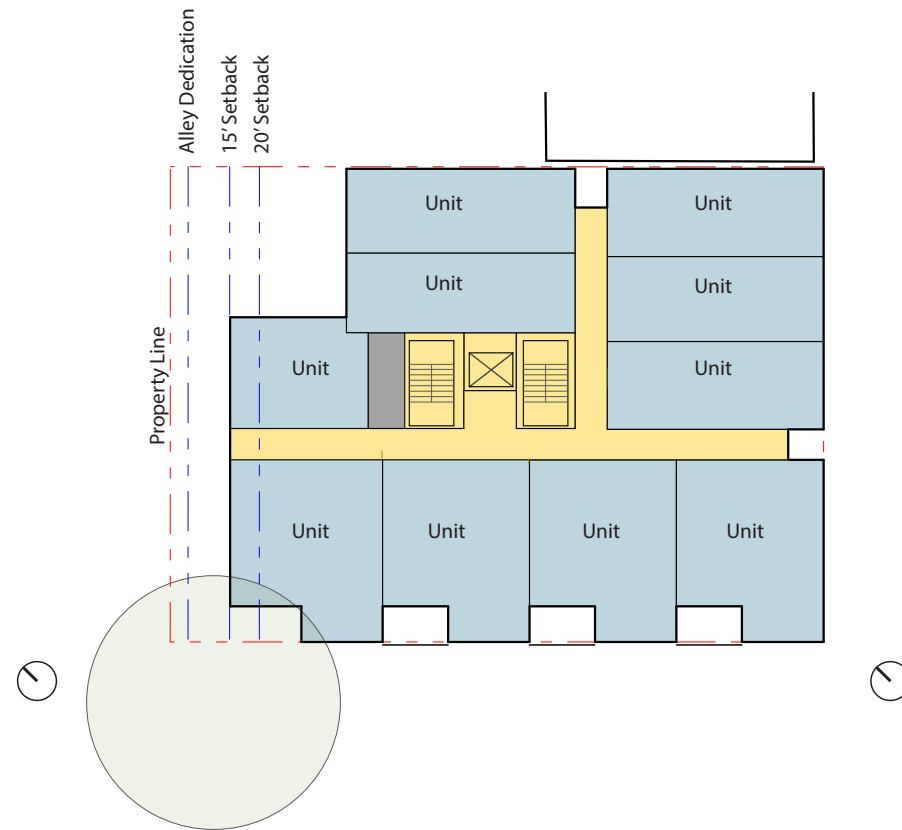
Looking East on Madison



Looking Across Madison Along Denny Way



Street Level Plan (Level 1)



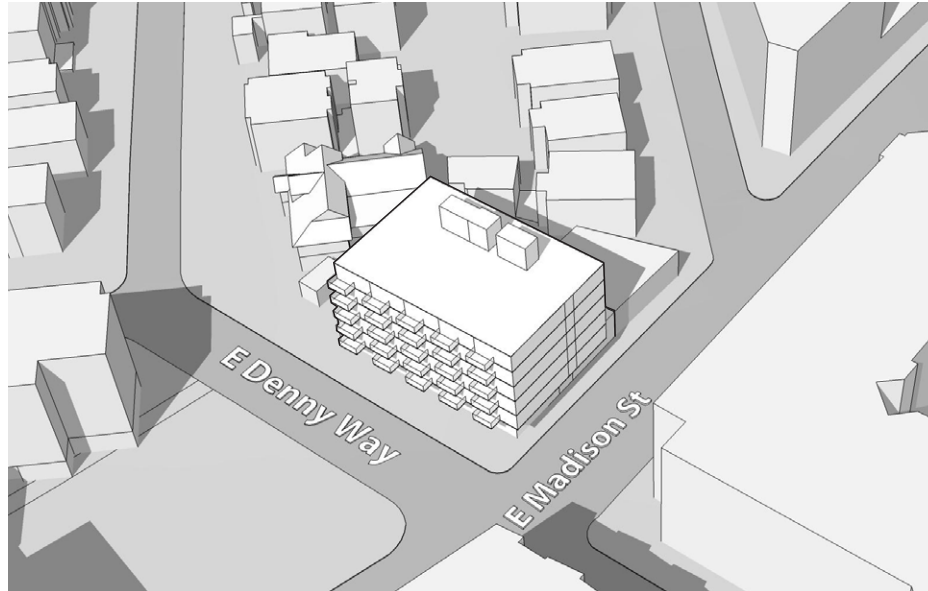
Typical Residential Plan (Levels 2 - 6)



Looking Down Denny Way

ARCHITECTURAL CONCEPTS: COMPARISON

ALTERNATE 1



Summary:

Stories:	6 (5-over-1 + 1 level below grade)
Unit Count:	45
Commercial Area:	4,083 SF
FAR utilization:	85.6%
Amenity Area:	2,940 SF (roof deck, level 2 patio)
Parking:	
Residential:	not required; 24 spaces provided
Commercial:	not required
Ground Floor Uses:	
Madison Street:	residential entry and retail
Denny Way:	retail

Disadvantages:

- Many of the units are oriented northeast; potential for view blockage if adjacent property is developed.
- Residential entry off of Madison is not as contextual as an entry off of Denny Way.
- Alley façade compliant with setback requirements, however does not consider non-conforming structure to the north.
- Tree at northwest corner of the site will need to be removed.
- Unit count is 5 less than other configurations (45 instead of 50).
- FAR utilization is 85%, which may not yield a feasible project.

ALTERNATE 2



Summary:

Stories:	6 (5-over-1 + 1 level below grade)
Unit Count:	50
Commercial Area:	4,097 SF
FAR utilization:	99%
Amenity Area:	2,688 SF (roof deck, level 2 patio, balconies)
Parking:	
Residential:	not required; 24 spaces provided
Commercial:	not required
Ground Floor Uses:	
Madison Street:	retail
Denny Way:	residential entry and retail

Advantages:

- Residential lobby off of Denny Way is preferred.
- Development under construction across from Denny is also a mixed-use building, with an open space across from the lobby.

Disadvantages:

- Alley façade compliant with setback requirements, however does not consider non-conforming structure to the north.

PREFERRED



Summary:

Stories:	6 (5-over-1 + 1 level below grade)
Unit Count:	50
Commercial Area:	3,797 SF
FAR utilization:	94%
Amenity Area:	2,232 SF (roof deck, level 2 patio, balconies)
Parking:	
Residential:	not required; 22 spaces provided
Commercial:	not required
Ground Floor Uses:	
Madison Street:	retail
Denny Way:	residential entry and retail

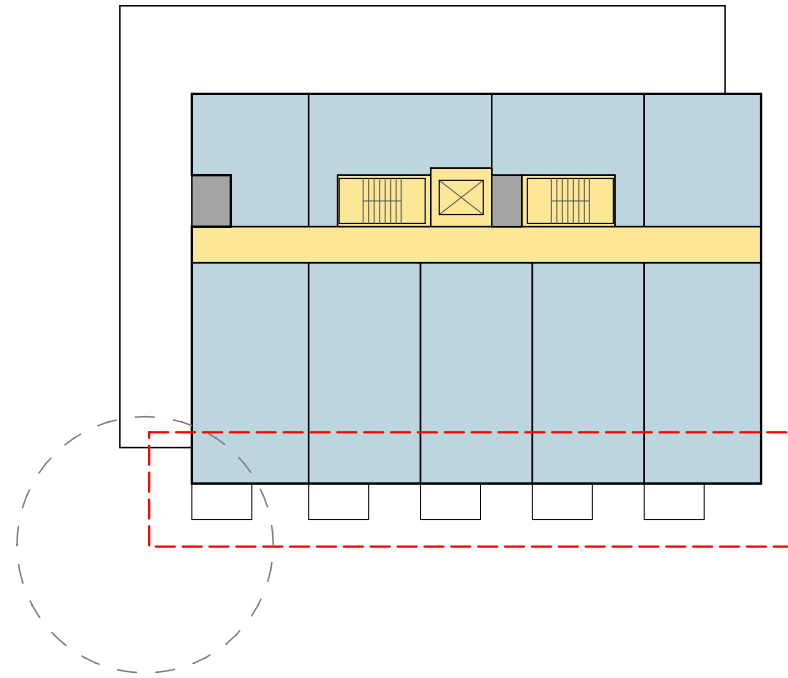
Potential Departures

- Alley setback above 40'

Advantages:

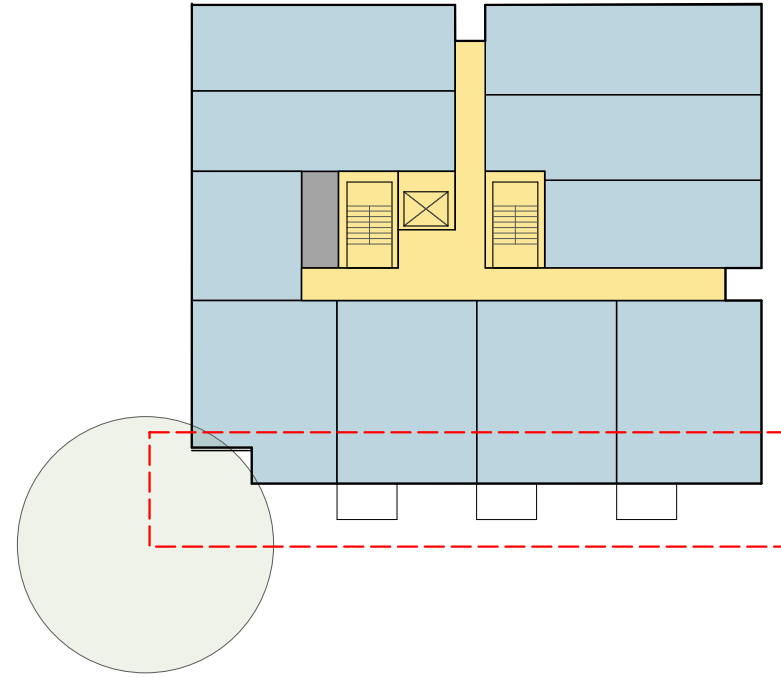
- Residential lobby off of Denny Way is preferred.
- Development under construction across from Denny is also a mixed-use building, with an open space across from the lobby.
- Tree at northwest corner of the site can remain.

ALTERNATE 1



Design rationale:
Attached balconies along Denny

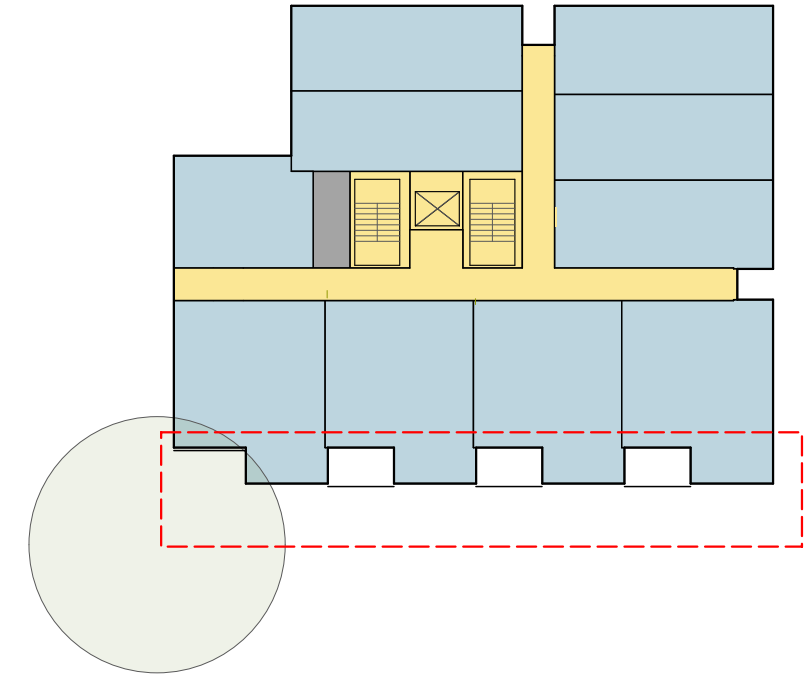
ALTERNATE 2



Design rationale:
Attached balconies along Denny

Notch at corner to provide relief for the Chestnut tree; lose one bay of balconies (provide Juliette balcony). Remainder of balconies are attached.

PREFERRED



Design rationale:
Inset balconies along Denny

Notch at corner to provide relief for the Chestnut tree; lose one bay of balconies (provide Juliette balcony). Follow the notch for tree and articulate façade to allow for inset balconies.

Balcony Placement

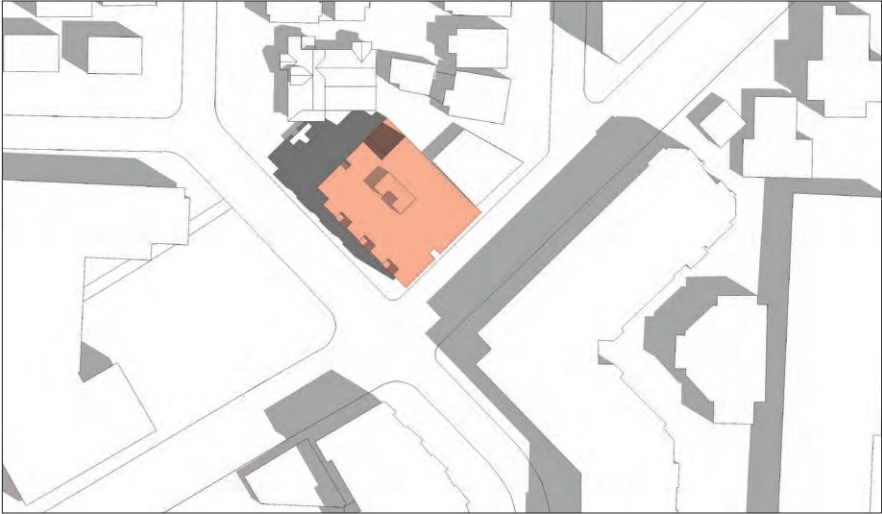
The corner of East Madison Street and East Denny Way is a busy and significant intersection. As such, the design team feels that it is imperative to maintain a strong corner architecturally for the building.

Balconies can be an amenity for residential units; however, careful placement is necessary as balconies may not be desirable in some locations due to exposure and noise.

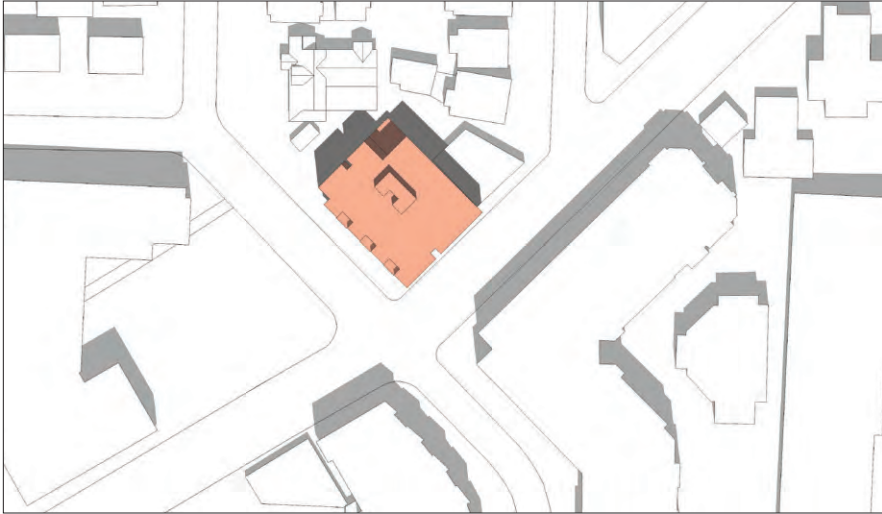
In analyzing the site, the optimal location for balconies is for the units along Denny Way. These balconies will benefit from southwest exposure and are along a quieter street. Madison was determined not to be an appropriate location for balconies, given the noise and soot from the busy traffic conditions. Balconies are not currently being designed for the units along the alley, due to the proximity to the neighboring LR3 zone.

PREFERRED ALTERNATE SHADOW STUDY

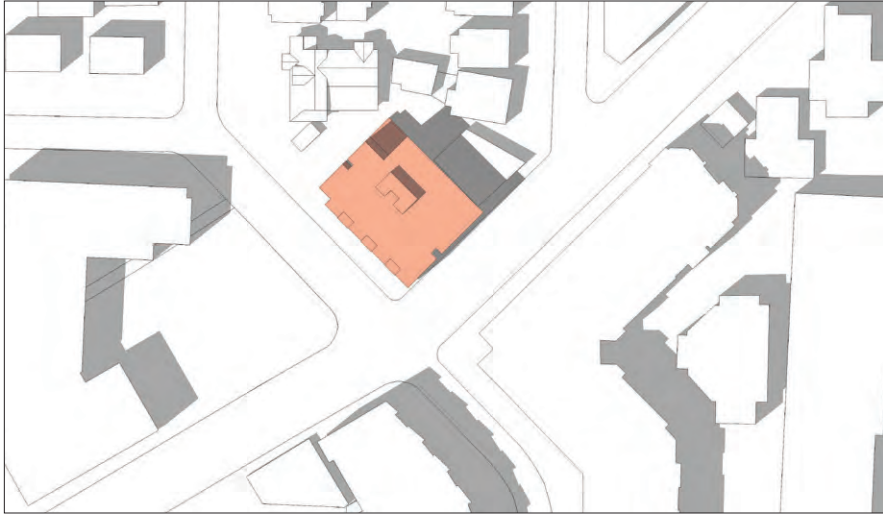
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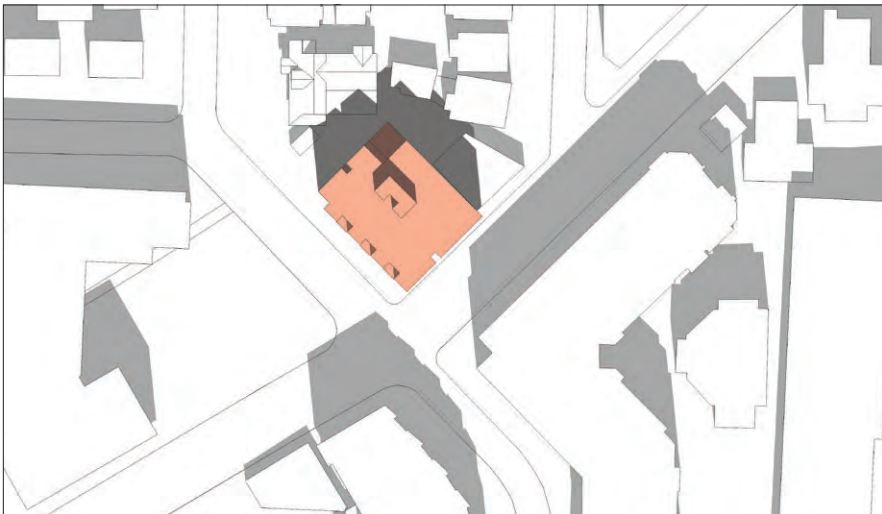
Noon



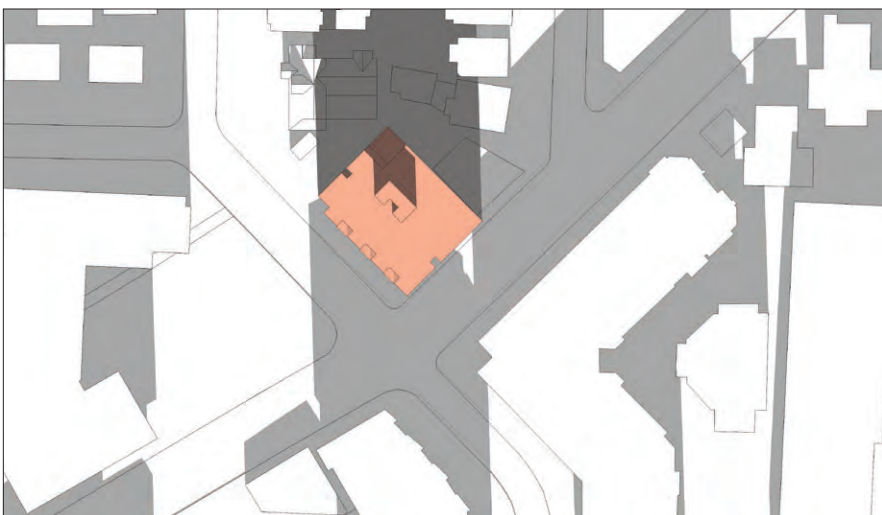
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June 21



March/ September 21

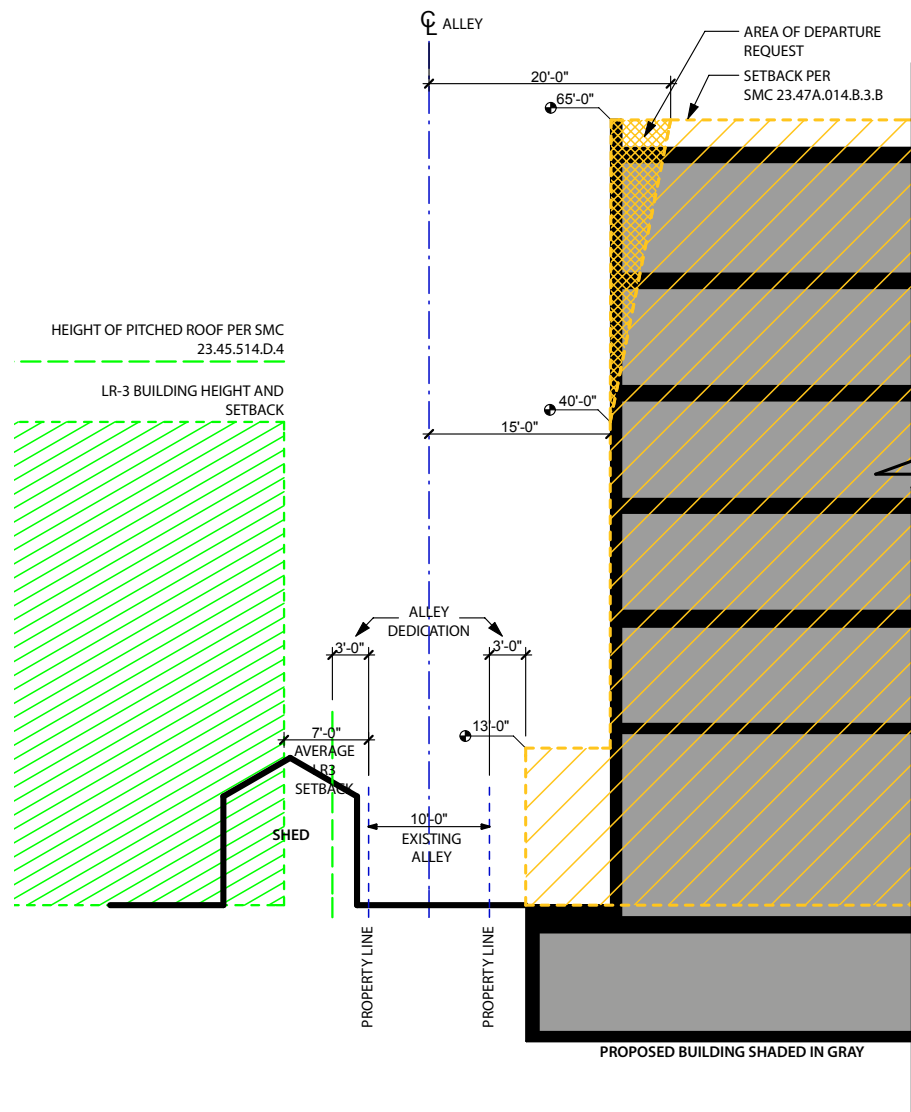


December 21

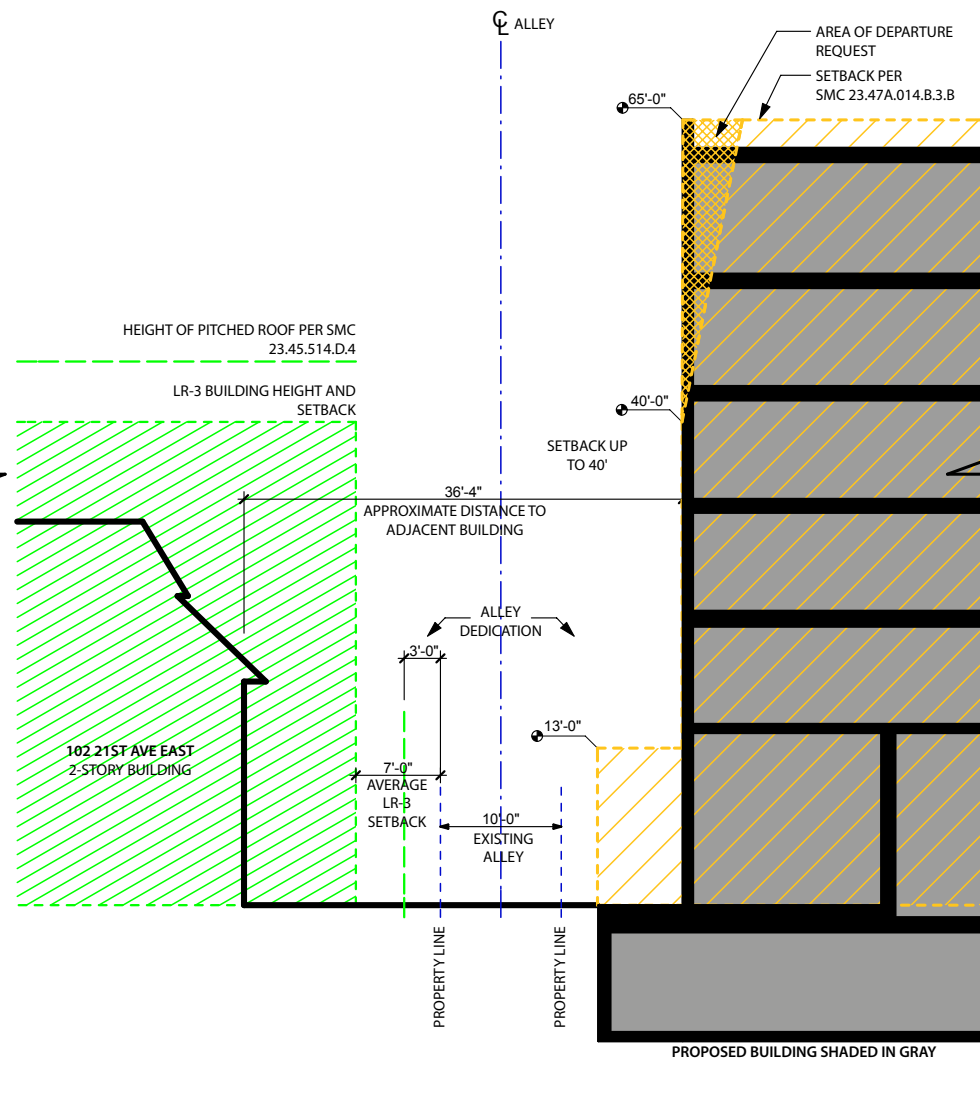


Landscape Plan
scale 1/8" = 1' - 0"

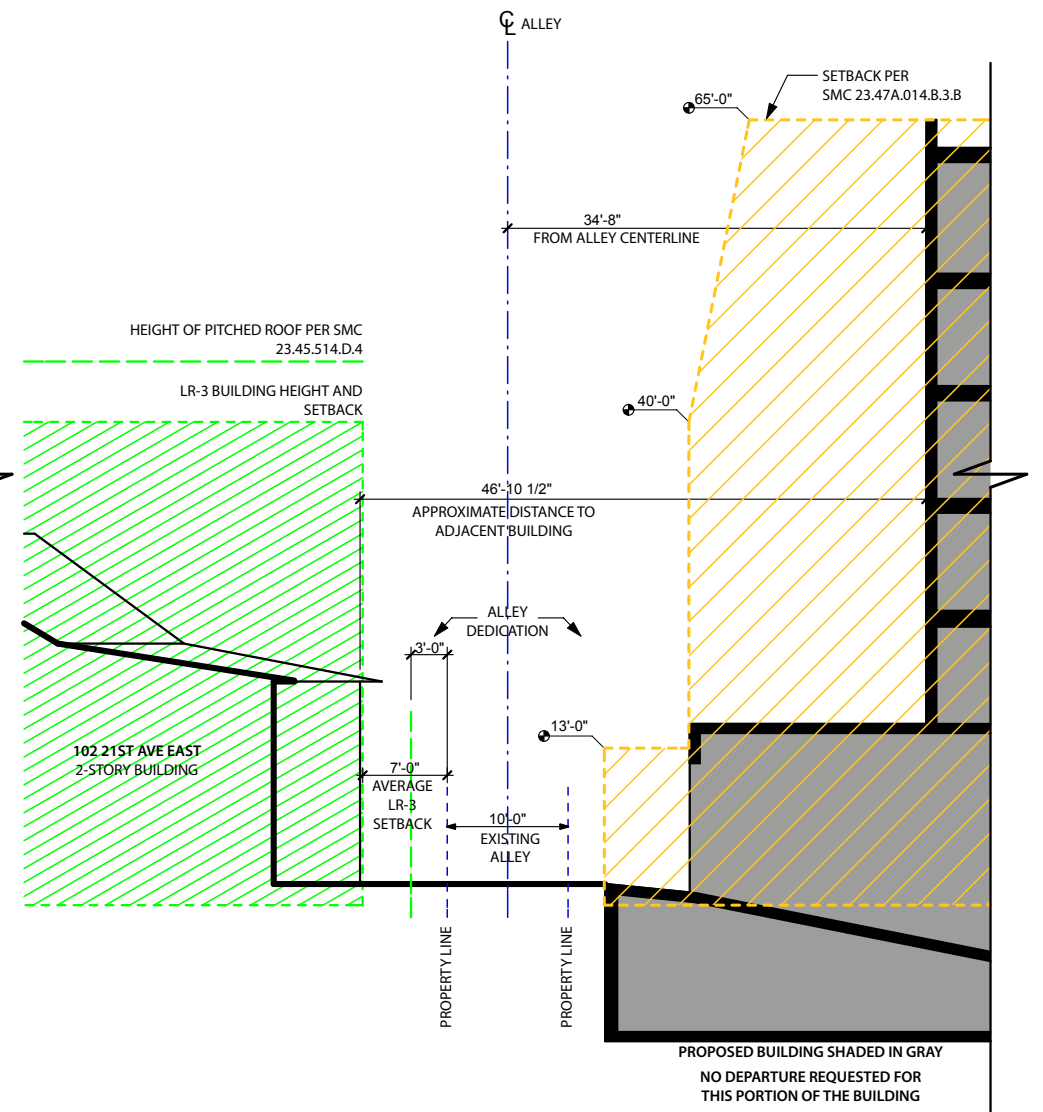
DEVELOPMENT DEPARTURE DIAGRAMS






Section A Through Retail



Section B Midpoint of Site



Section C Through Driveway

-  NC3-65 BUILDING ENVELOPE PER SMC 23.47A.014 SETBACK REQUIREMENTS
-  LR3 BUILDING ENVELOPE PER SMC 23.45.514 TABLE A AND SMC 23.45.518 TABLE A
-  AREA OF DEPARTURE REQUEST

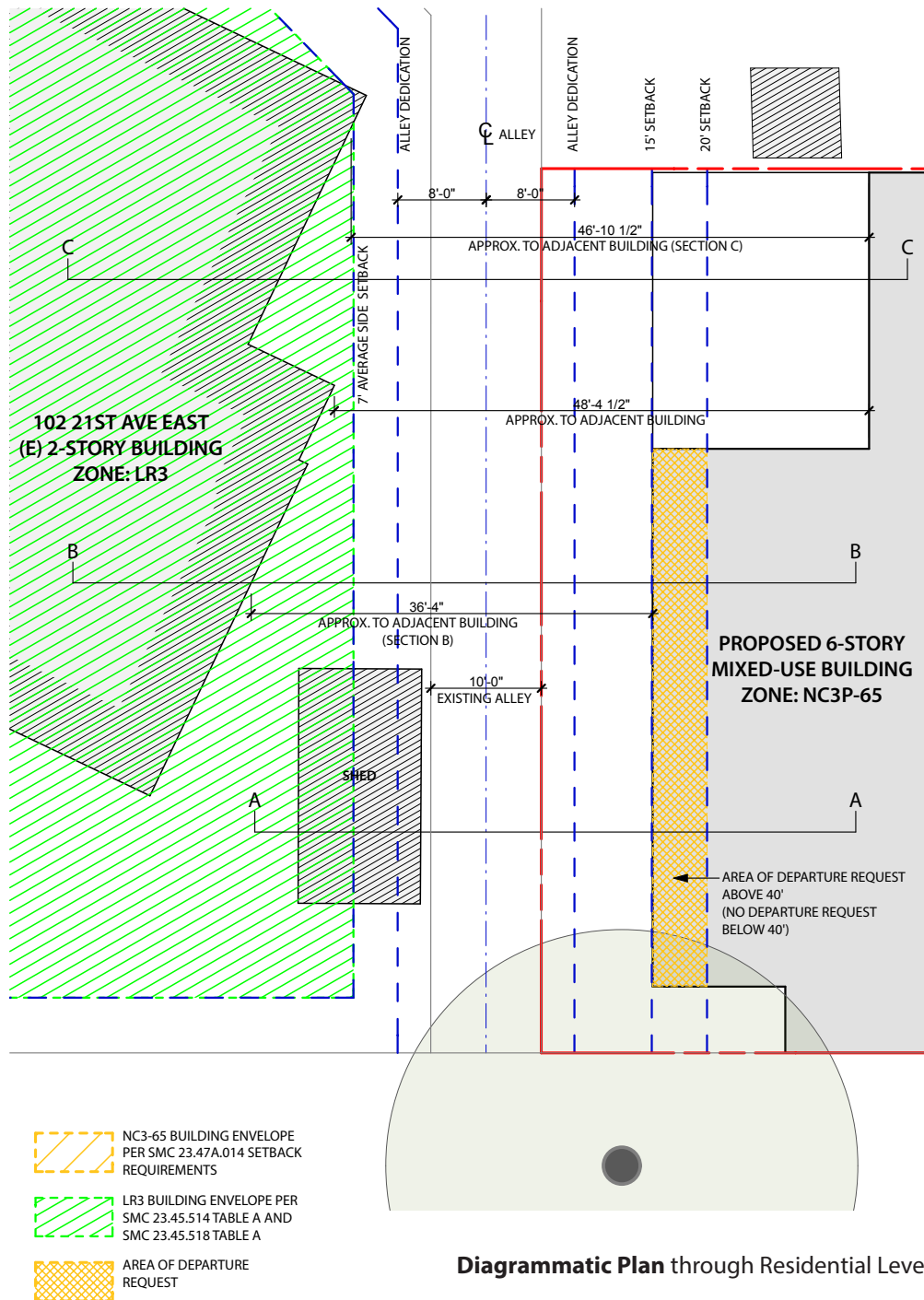
DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
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1 23.47A.014.B.3.b
Setback requirements. For lots abutting or across the alley from residential zones, the setback for heights greater than 40' is 15' + a rate of 2' for every 10'.

Request a departure from the setback requirement over 40'.

The parcel to across the alley is zoned LR-3. The NW facade is articulated to provide relief at the alley elevation. This results in significant portions of the elevation that are in excess of the required setback requirements, specifically in response to the non-conforming structure to the north. A departure is being requested for the setback requirements for portions that are above 40 feet in height; these portions will be maintained at setback of fifteen feet.

The departures noted here have been identified as potentially necessary to achieve the preferred design alternate. If the Board indicates their willingness to consider the departures, additional study and refinement of the design will be conducted in advance of the Design Recommendation meeting.



Diagrammatic Plan through Residential Level



Existing conditions in unimproved alley

POTENTIAL DESIGN GUIDELINES

Absent neighborhood specific Design Guidelines, the citywide Seattle Design Guidelines will serve to guide the design and development of the project. Potential applicable Design Guidelines have been identified to the right.

SEATTLE DESIGN GUIDELINES		DESIGN RESPONSE
CS1 Natural Systems and Site Features <i>Use natural systems and features of the site and its surroundings as a starting point for project design.</i>		
B. SUNLIGHT AND NATURAL VENTILATION		
B2	Daylight and Shading: Maximize daylight for interior and exterior spaces and minimize shading on adjacent sites through the placement and/or design of structures on the site.	The massing of the building in the preferred design maximizes daylight to the residential units. In addition, the proposed setback minimizes the impact on daylight access to the single family house north of the site.
B3	Managing Solar Gain: Manage direct sunlight falling on south and west facing facades through shading devices and existing or newly planted trees.	The preferred design retains the existing Chestnut tree at the west of the site, which will help manage solar gain to five units.
D. PLANTS AND HABITAT		
D1	On-Site Features: Incorporate on-site natural habitats and landscape elements such as: existing trees, native plant species or other vegetation into project design and connect those features to existing networks of open spaces and natural habitats wherever possible. Consider relocating significant trees and vegetation if retention is not feasible.	The preferred design retains the existing 31" diameter Chestnut tree at the west of the property.
CS2 Urban Pattern and Form <i>Strengthen the most desirable forms, characteristics, and patterns of the streets, block faces, and open spaces in the surrounding area.</i>		
A. LOCATION IN THE CITY AND NEIGHBORHOOD		
A1	Sense of Place: Emphasize attributes that give Seattle, the neighborhood, and/or the site its distinctive sense of place. Design the building and open spaces to enhance areas where a strong identity already exists, and create a sense of place where the physical context is less established. Examples of neighborhood and/or site features that contributed to a sense of place include patterns of streets or blocks, slopes, sites with prominent visibility, relationships to bodies of water or significant trees, natural areas, open spaces, iconic buildings or transportation junctions, and land seen as a gateway to the community.	The preferred design alternate emphasizes the street levels of East Madison Street and Denny Way with a strong, continuous retail presence and high visibility at the corner. A variety of spaces can be accommodated, from smaller retail spaces to a restaurant. These will provide needed amenities to the growing area. The alignment of the residential entry opposite an open space (currently under construction on the south side of Denny Way) helps to establish the pedestrian residential presence along Denny.
A2	2. Architectural Presence: Evaluate the degree of visibility or architectural presence that is appropriate or desired given the context, and design accordingly. A site may lend itself to a "high-profile" design with significant presence and individual identity, or may be better suited to a simpler but quality design that contributes to the block as a whole. Buildings that contribute to a strong street edge, especially at the first three floors, are particularly important to the creation of a quality public realm that invites social interaction and economic activity. Encourage all building facades to incorporate design detail, articulation and quality materials.	The design creates a strong and active street edge. The residential portion of the building cantilevers over the street level on both Denny Way and East Madison. This creates a much needed extension of perceived width at the sidewalk, particularly along Madison where the existing sidewalk is 10'. The zone adjacent to the retail and restaurant spaces at the sidewalk will both encourage activity for the building and enhance the pedestrian experience.
B. ADJACENT SITES, STREETS, AND OPEN SPACES		
B1	Site Characteristics: Allow characteristics of sites to inform the design, especially where the street grid and topography create unusually shaped lots that can add distinction to the building massing.	The site is at a highly visible corner at East Madison Street and East Denny Way. It is at a bend on Madison Street, which makes the building a focal point while traveling east along Madison. The preferred alternate considers this in both use and façade design along Madison and Denny.
B2	Connection to the Street: Identify opportunities for the project to make a strong connection to the street and carefully consider how the building will interact with the public realm. Consider the qualities and character of the streetscape--its physical features (sidewalk, parking, landscape strip, street trees, travel lanes, and other amenities) and its function (major retail street or quieter residential street)--in siting and designing the building.	The building is set back from the property line along E Madison Street and E Denny Way. Functionally, this widens the sidewalk, creating an enhanced pedestrian and user experience. Both streets will include street trees and planting strips in the sidewalk, and the overhang of the building's cantilevered levels will create a generous pedestrian zone.
C. RELATIONSHIP TO THE BLOCK		

SEATTLE DESIGN GUIDELINES

DESIGN RESPONSE

C1	Corner Sites: Corner sites can serve as gateways or focal points; both require careful detailing at the first three floors due to their high visibility from two or more streets and long distances. Consider using a corner to provide extra space for pedestrians and a generous entry, or build out to the corner to provide a strong urban edge to the block.	The bend in Madison Street provides a unique visible corner for the building. Rather than erode the corner, it is maintained, with the street level use set back to enhance the pedestrian and sidewalk experience.
D. HEIGHT, BULK, AND SCALE		
D1	Existing Development and Zoning: Review the height, bulk, and scale of neighboring buildings as well as the scale of development anticipated by zoning for the area to determine an appropriate complement and/or transition. Note that existing buildings may or may not reflect the density allowed by zoning or anticipated by applicable policies.	The proposed building is similar in height to those of neighboring buildings. The building is set back at street level, to reduce the building's footprint and provide an enhanced pedestrian experience.
D2	Existing Site Features: Use changes in topography, site shape, and vegetation or structures to help make a successful fit with adjacent properties; for example siting the greatest mass of the building on the lower part of the site or using an existing stand of trees to buffer building height from a smaller neighboring building.	The preferred design hopes to retain the existing 31" diameter chestnut tree. The form of the building is influenced by the tree's location and size.
D3	Zone Transitions: For projects located at the edge of different zones, provide an appropriate transition or complement to the adjacent zone(s). Projects should create a step in perceived height, bulk and scale between the anticipated development potential of the adjacent zone and the proposed development. Factors to consider:	The parcel is adjacent to an LR-3 zone across the alley. Where there is the potential for the neighboring building (non-conforming) and the proposed building to be the closest, the preferred design steps the residential portion of the building back substantially beyond the required setbacks.
	a. Distance to the edge of a less (or more) intensive zone;	In addition to the stepping at the residential levels, the entire building is set back from the alley dedication by an additional seven feet.
	c. The type of separation from adjacent properties (e.g. separation by property line only, by an alley or street or open space, or by physical features such as grade change);	The building is separated from the adjacent LR-3 property by an alley.
D4	Massing Choices: Strive for a successful transition between zones where a project abuts a less intense zone. In some areas, the best approach may be to lower the building height, break up the mass of the building, and/or match the scale of adjacent properties in building detailing. It may be appropriate in other areas to differ from the scale of adjacent buildings but preserve natural systems or existing features, enable better solar exposure or site orientation, and/or make for interesting urban form.	The building is separated from the LR-3 property by an alley, and the preferred design further steps the building back an additional seven feet from the alley dedication. At the upper floors, the building breaks down the mass of the building, creating an articulation of massing that is more similar to that of a less dense zoning.
D5	Respect for Adjacent Sites: Respect adjacent properties with design and site planning to minimize disrupting the privacy and outdoor activities of residents in adjacent buildings.	The location and the residences and commercial spaces are designed to minimize disruption to the adjacent neighbors.
CS3	Architectural Context and Character <i>Contribute to the architectural character of the neighborhood.</i>	
A. EMPHASIZING POSITIVE NEIGHBORHOOD ATTRIBUTES		
A2	Contemporary Design: Explore how contemporary designs can contribute to the development of attractive new forms and architectural styles; as expressed through use of new materials or other means.	In this growing neighborhood, the preferred design will seek to be a focal point by supporting commercial spaces that can be enjoyed by all neighbors. The design of the building will enhance these activities, as will the choices of the exterior materials.
A4	Evolving Neighborhoods: In neighborhoods where architectural character is evolving or otherwise in transition, explore ways for new development to establish a positive and desirable context for others to build upon in the future.	Given its location on a prominent corner, the building will become an "anchor" building in the neighborhood. Its modest size will require a discrete palette of materials, allowing for the activities that the building supports to have the primary role.

POTENTIAL DESIGN GUIDELINES

SEATTLE DESIGN GUIDELINES		DESIGN RESPONSE
PL1	Connectivity <i>Complement and contribute to the network of open spaces around the site and the connections among them.</i>	
A. NETWORK OF OPEN SPACES		
A2	Adding to Public Life: Seek opportunities to foster human interaction through an increase in the size and/or quality of project-related open space available for public life. Consider features such as widened sidewalks, recessed entries, curb bulbs, courtyards, plazas, or through-block connections, along with place-making elements such as trees, landscape, art, or other amenities, in addition to the pedestrian amenities listed in PL1.B3.	The building is set back from the property line, allowing for a wider pedestrian zone. This will not only enhance the experience for the commercial spaces, it will also promote a livelier sidewalk. The current sidewalk along Madison is particularly narrow, especially given its proximity to vehicular traffic. The Chestnut tree on Denny is intended to be preserved, helping to establish a quieter pedestrian experience and an area of respite.
B. WALKWAYS AND CONNECTIONS		
B2	Pedestrian Volumes: Provide ample space for pedestrian flow and circulation, particularly in areas where there is already heavy pedestrian traffic or where the project is expected to add or attract pedestrians to the area.	As mentioned previously, the set back of the building along E Madison Street and E Denny Way will allow provide space to enhance pedestrian flow and circulation.
B3	Pedestrian Amenities: Opportunities for creating lively, pedestrian oriented open spaces to enliven the area and attract interest and interaction with the site and building should be considered. Visible access to the building's entry should be provided. Examples of pedestrian amenities include seating, other street furniture, lighting, year-round landscaping, seasonal plantings, pedestrian scale signage, site furniture, art work, awnings, large storefront windows, and engaging retail displays and/or kiosks.	The pedestrian zone on E Denny Way and E Madison Street are envisioned to include seating areas, trees, and planting strips. There will be a large degree of transparency into both the commercial spaces and the residential lobby. The upper levels of the building will cantilever over the sidewalk, creating a canopy at street level.
PL2	Walkability <i>Create a safe and comfortable walking environment that is easy to navigate and well-connected to existing pedestrian walkways and features.</i>	
A. ACCESSIBILITY		
A1	Access for All: Provide access for people of all abilities in a manner that is fully integrated into the project design. Design entries and other primary access points such that all visitors can be greeted and welcomed through the front door. Refrain from creating separate "back door" entrances for persons with mobility limitations.	All retail spaces and the residential entry will be designed to be fully accessible for use by people of all mobility types.
B. SAFETY AND SECURITY		
B1	Eyes on the Street: Create a safe environment by providing lines of sight and encouraging natural surveillance through strategic placement of doors, windows, balconies and street-level uses.	Entries and doors are situated and designed to allow for "eyes on the street" and high visibility. This is integral for security and for visibility into the retail areas.
B3	Street-Level Transparency: Ensure transparency of street-level uses (for uses such as nonresidential uses or residential lobbies), where appropriate, by keeping views open into spaces behind walls or plantings, at corners, or along narrow passageways. Choose semi-transparent rather than opaque screening.	The street-level uses are designed to be as transparent as possible. A high degree of interaction between the street and the interior uses is desired for this project.
C. WEATHER PROTECTION		
C1	Locations and Coverage: Overhead weather protection is encouraged and should be located at or near uses that generate pedestrian activity such as entries, retail uses, and transit stops. Address changes in topography as needed to provide continuous coverage the full length of the building, where possible.	The building is set back approximately six feet from the property line on E Denny Way and E Madison Street on level 1, with the residential levels built out to the property line. This creates weather protection at the sidewalks.

SEATTLE DESIGN GUIDELINES

DESIGN RESPONSE

PL3 Street-Level Interaction

Encourage human interaction and activity at the street-level with clear connections to building entries and edges.

A. ENTRIES

- A1 Design Objectives: Design primary entries to be obvious, identifiable, and distinctive with clear lines of sight and lobbies visually connected to the street. Scale and detail them to function well for their anticipated use and also to fit with the building of which they are a part, differentiating residential and commercial entries with design features and amenities specific to each.

Entries will be designed to be easily identifiable, with appropriate scale and signage.

C. RETAIL EDGES

- C1 Porous Edge: Engage passersby with opportunities to interact visually with the building interior using glazing and transparency. Create multiple entries where possible and make a physical and visual connection between people on the sidewalk and retail activities in the building.
- C2 Visibility: Maximize visibility into the building interior and merchandise displays. Consider fully operational glazed wall-sized doors that can be completely opened to the street, increased height in lobbies, and/or special lighting for displays.
- C3 Ancillary Activities: Allow space for activities such as sidewalk vending, seating, and restaurant dining to occur. Consider setting structures back from the street or incorporating space in the project design into which retail uses can extend.

Given the nature of Seattle, people enjoy the opportunity to be outside as often as possible. Operable glazing will allow the "outside in" during opportune weather, and at the least, allow for a visual connection to the sidewalk and exterior activities.

The interior of the building will be quite transparent, allowing for an indirect interaction with users on the street. This helps provide viable retail and commercial spaces for the tenants, and creates interesting views for pedestrians just passing by.

The area adjacent to the building may be utilized for sidewalk seating for future tenants. This will enhance the life on the street, creating a positive space for both building users and pedestrians.

PL4 Active Transportation

Incorporate design features that facilitate active forms of transportation such as walking, bicycling, and use of transit.

A. ENTRY LOCATIONS AND RELATIONSHIPS

- A1 Serving all Modes of Travel: Provide safe and convenient access points for all modes of travel.
- A2 Connections to All Modes: Site the primary entry in a location that logically relates to building uses and clearly connects all major points of access.

The building will be convenient for those that are walking, biking, bussing, or driving. Pedestrian access includes those on foot and those requiring mobility assistance.

The main residential entry is easily viewed from the corner of E Madison Street and E Denny Way. It is located on Denny, as this is the quieter, more residential street.

C. PLANNING AHEAD FOR TRANSIT

- C1 Influence on Project Design: Identify how a transit stop (planned or built) adjacent to or near the site may influence project design, provide opportunities for placemaking, and/or suggest logical locations for building entries, retail uses, open space, or landscaping. Take advantage of the presence of transit patrons to support retail uses in the building.

Currently, a bus stop is located on E Madison Street near the east corner of the building. We are advocating for the relocation of the bus stop and relocating the stop at the far side of the intersection would be consistent with Metro's standard practices. The project team will work with King County Metro and SDOT in considering options.

DC1 Project Uses and Activities

Optimize the arrangement of uses and activities on site.

A. ARRANGEMENT OF INTERIOR USES

- A1 Visibility: Locate uses and services frequently used by the public in visible or prominent areas, such as at entries or along the street front.

Three retail spaces are located at the street front, in addition to the residential entry. They will be clearly identified to encourage ease of way-finding.

B. VEHICULAR ACCESS AND CIRCULATION

- B1 Access Location and Design: Choose locations for vehicular access, service uses, and delivery areas that minimize conflict between vehicles and non-motorists wherever possible. Emphasize use of the sidewalk for pedestrians, and create safe and attractive conditions for pedestrians, bicyclists, and drivers by:

The building is inset from the property line along Denny Way and Madison Street to encourage sidewalk use for pedestrians. Driveway entry is located off of the alley, minimizing conflict between vehicles, pedestrians, and bicycles.

- a. using existing alleys for access or, where alley access is not feasible, choosing a location for street access that is the least visually dominant and/or which offers opportunity for shared driveway use;

The driveway is located off of the alley, at the point in the site furthest from the alley intersection with Denny Way.

POTENTIAL DESIGN GUIDELINES

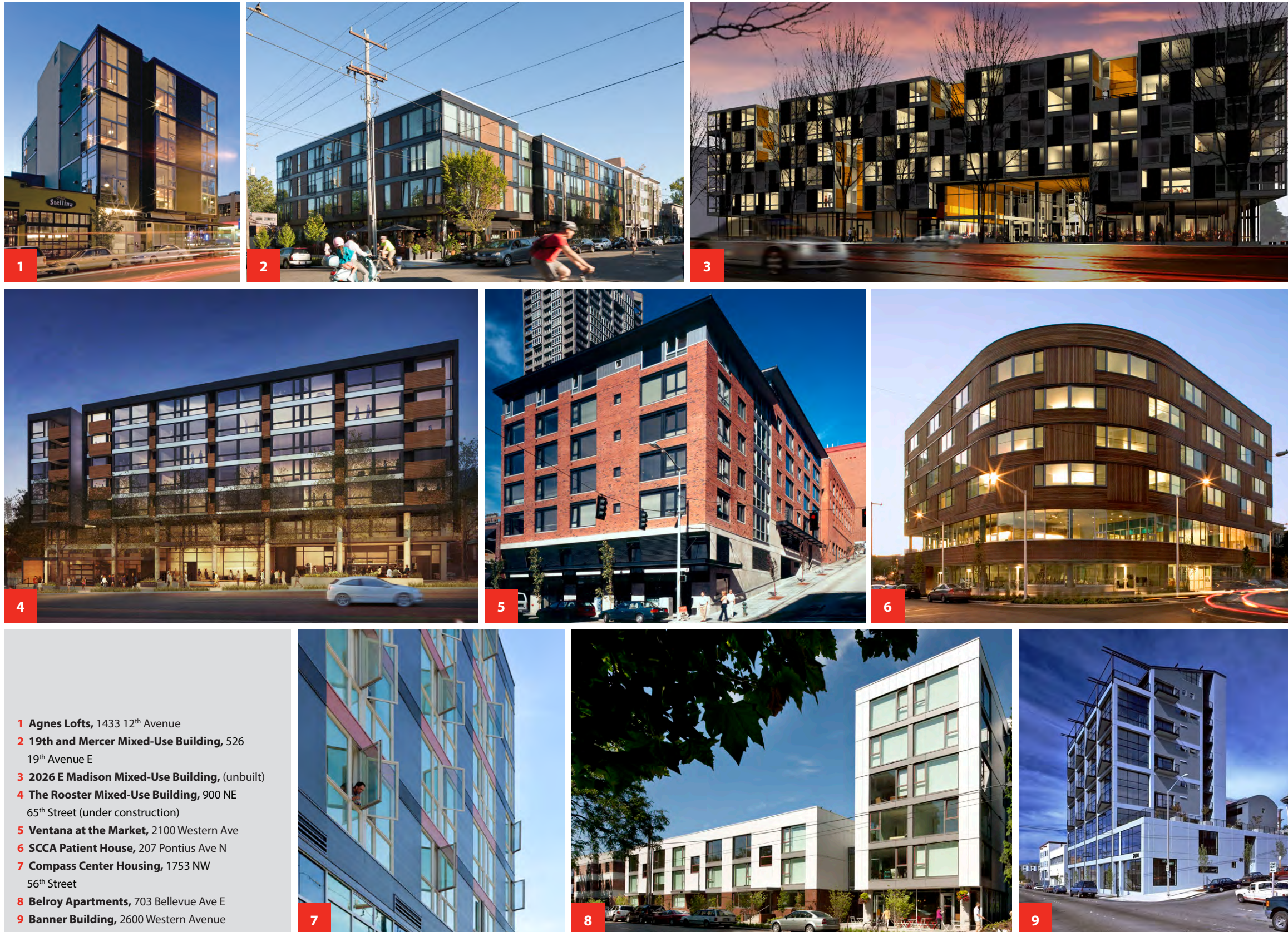
SEATTLE DESIGN GUIDELINES		DESIGN RESPONSE
C. PARKING AND SERVICE USES		
C1	Below-Grade Parking: Locate parking below grade wherever possible. Where a surface parking lot is the only alternative, locate the parking in rear or side yards, or on lower or less visible portions of the site.	Parking for residents (not required per zoning code) is located below grade. The covered accessible van parking space is located at grade off the alley, as the parking level does not allow for the required van height.
C4	Service Uses: Locate and design service entries, loading docks, and trash receptacles away from pedestrian areas or to a less visible portion of the site to reduce possible impacts of these facilities on building aesthetics and pedestrian circulation. Where service facilities abut pedestrian areas or the perimeter of the property, maintain an attractive edge through screening, plantings, or other design treatments.	Vehicle entries and trash receptacles will be located off of the alley, reducing pedestrian impact as much as possible.
DC2 Architectural Concept <i>Develop an architectural concept that will result in a unified and functional design that fits well on the site and within its surroundings.</i>		
A. MASSING		
A2	Reducing Perceived Mass: Use secondary architectural elements to reduce the perceived mass of larger projects. Consider creating recesses or indentations in the building envelope; adding balconies, bay windows, porches, canopies or other elements; and/or highlighting building entries.	The massing of the building is reduced as the retail level on both Denny Way and Madison Street are inset from the envelope of the building. Balconies are added to the quieter elevation of Denny Way, while the Madison elevation includes a recess to bring in light. The recess divides the building approximately 30%/70% along the 80' length of the building.
B. ARCHITECTURAL AND FACADE COMPOSITION		
B1	Facade Composition: Design all building facades -- including alleys and visible roofs -- considering the composition and architectural expression of the building as a whole. Ensure that all facades are attractive and well-proportioned through the placement and detailing of all elements, including bays, fenestration, and materials, and any patterns created by their arrangement. On sites that abut an alley, design the alley facade and its connection to the street carefully. At a minimum, consider wrapping the treatment of the street-facing facade around the alley corner of the building.	The facades are carefully designed to consider both the interior function and the exterior composition. Articulation responds to the site chestnut tree, street noise, and interior environment. The elevation at the alley responds not only to the unit design, but also to the adjacent residential zone.
C. SECONDARY ARCHITECTURAL FEATURES		
C1	Visual Depth and Interest: Add depth to facades where appropriate by incorporating balconies, canopies, awnings, decks, or other secondary elements into the facade design. Add detailing at the street level in order to create interest for the pedestrian and encourage active street life and window shopping (in retail areas). Detailing may include features such as distinctive door and window hardware, projecting window sills, ornamental tile or metal, and other high-quality surface materials and finishes.	Balconies will be incorporated into the quieter, residential elevation along Denny Way. The cantilevered residential building creates a partially covered sidewalk, creating an inviting pedestrian experience. Careful attention will be taken to the treatment of the soffit condition underneath this "canopy".
DC3 Open Space Concept <i>Integrate open space design with the design of the building so that each complements the other.</i>		
B. OPEN SPACE USES AND ACTIVITIES		
B4	Multifamily Open Space: Design common and private open spaces in multifamily projects for use by all residents to encourage physical activity and social interaction. Some examples include areas for gardening, children's play (covered and uncovered), barbecues, resident meetings, and crafts or hobbies.	The project will incorporate a common area roof deck for all residents. The lobby is situated between two retail spaces, and potentially may have a direct connection if there is a synergy of use. Even without a direct connection, the proximity of the entrances will promote interaction between the residents and the businesses. The lobby is sized to encourage waiting and conversation.

REPRESENTATIVE PROJECTS

Weinstein A+U is recognized as one of the Northwest's leading design firms and has continually demonstrated design excellence on a broad array of projects for State, City, Federal, private, and not-for-profit clients. We are passionate about our city and the shaping of its urban neighborhoods through the integration of architecture and urban design is central to our practice.

Well-designed and thoughtful urban housing is a special concern of ours, and we have worked aggressively to advance the expectations of mixed-use projects in Seattle, both technically and aesthetically. While each project presents very specific challenges, a number of recurring themes inform much of our work and form the basis of our approach to housing design:

- All of our buildings are situational and are inseparable from their sites. They sit comfortably amongst their neighbors, drawing from established precedents while looking to the future.
- Well-designed unit plans are essential to the success of a housing project. While the functionality of each unit type is important, the organization of units across a floor plate and their influence on building elevations is equally important.
- Appropriately located and proportioned open space is a significant design determinant for most mixed-use and urban housing projects.
- We avoid arbitrary façade embellishment. Instead we utilize the organization of individual units and their aggregation to establish the pattern and rhythm of multi-family facades that is furthered informed by site organization and orientation. Plans correlate to elevations and variation occurs within an established system.
- The constrained budgets for typical mixed-use projects demand careful consideration of a project's primary orientation and configuration to provide cost effective sustainable design strategies.
- The scale and proportion of new mixed-use buildings must address, but need not directly reflect, those of adjacent structures. Plan, section, and elevation strategies should be integrated to provide a comprehensible "read" of the building's composition and organization.



- 1 Agnes Lofts**, 1433 12th Avenue
- 2 19th and Mercer Mixed-Use Building**, 526 19th Avenue E
- 3 2026 E Madison Mixed-Use Building**, (unbuilt)
- 4 The Rooster Mixed-Use Building**, 900 NE 65th Street (under construction)
- 5 Ventana at the Market**, 2100 Western Ave
- 6 SCCA Patient House**, 207 Pontius Ave N
- 7 Compass Center Housing**, 1753 NW 56th Street
- 8 Belroy Apartments**, 703 Bellevue Ave E
- 9 Banner Building**, 2600 Western Avenue