

6616 8TH AVE NE

DPD PROJECT #3020076
STREAMLINED DESIGN REVIEW MEETING
APRIL 29, 2015



caron

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ROOSEVELT APARTMENT

PROJECT INTRODUCTION

SITE INFORMATION:

- Site Address: 6616 8th Ave NE
- DPD Project #: 3020076
- Parcel(s): 952810-2915
- Site Area: 4,047 SF
- Overlay Designation: Frequent Transit, Light Rail, Roosevelt
- Parking Requirement: None
- Legal Description: Lots 1-2 of Block 57 Woodlawn add to Green Lake 1 & N 1.67 FT of 2

DEVELOPMENT STATISTICS:

- Zoning: MR1.2
- Lot Size: 4,047 SF
- FAR: 12.950 SF
- Proposed FAR: 9.585 SF
- Residential Units: 20
- Parking Stalls: None required. Provided min. 11 bicycle spaces, required.

PROJECT PROPOSAL

DEVELOPMENT OBJECTIVES:

The proposed project is a development of an infill Mid-rise site in the Roosevelt neighborhood which will take advantage of future nearby light rail transit. The site being located directly across the street from Interstate 5 is an opportunity to create a project that can act as a signifier of the neighborhood to travelers along the freeway above. The proposed project uses unique geometry and color to create a distinct 5 story building containing 20 apartment units. The unit types range from traditional 1 bedroom units to small efficiency dwelling units. Being in the station overlay no parking is required and none will be provided, however, ample, secure bicycle storage will be provided inside of the building.

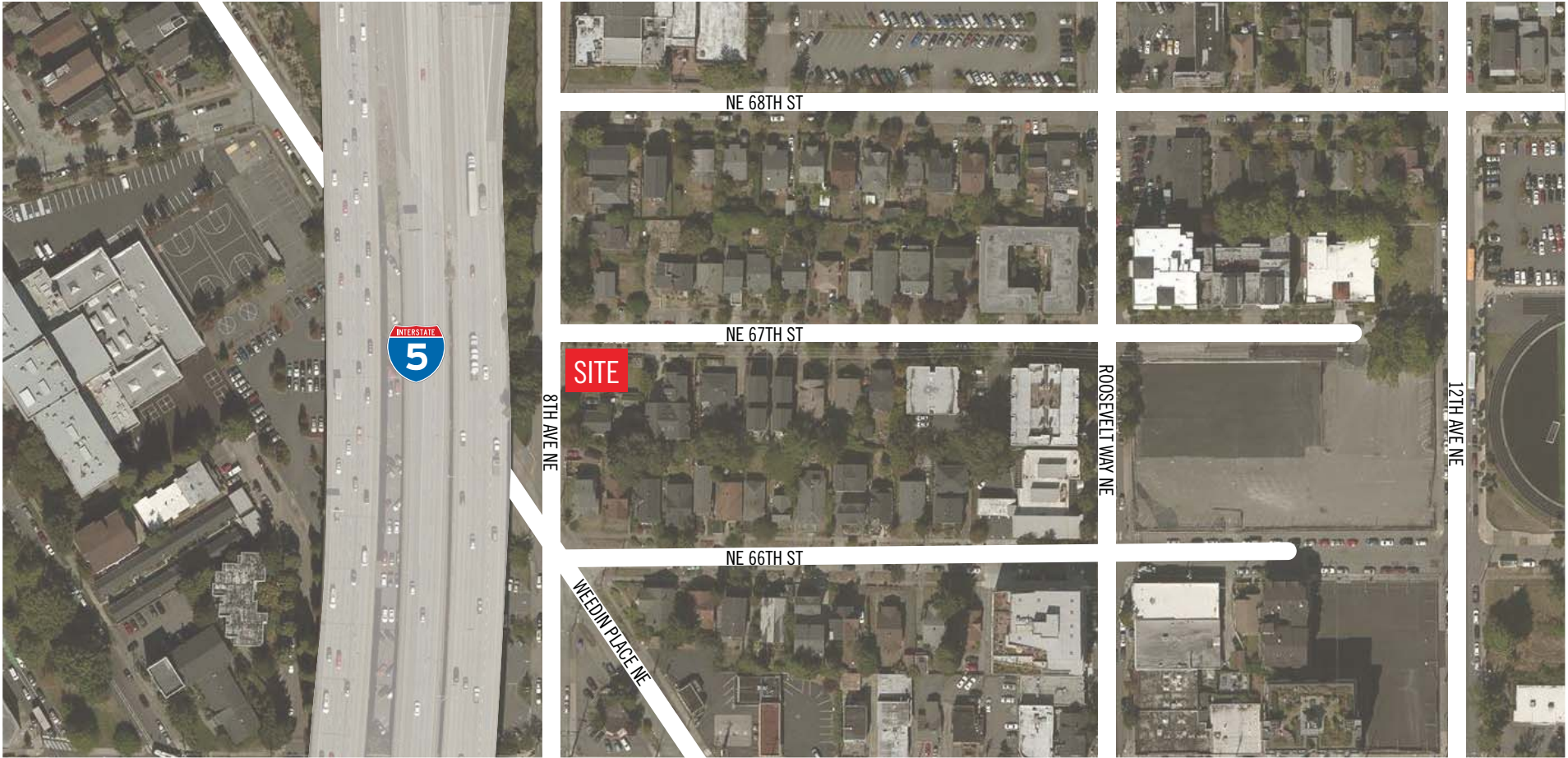
CONTEXT:

The site is located on the western edge of the Roosevelt neighborhood on the southeastern corner of the intersection of 8th Avenue NE and NE 67th Street. The site is bounded by construction of a 6-story 260 unit development to the north, Interstate 5 to the west, future development of an approximately 7-story 75 unit apartment structure to the south, and a 6-unit townhouse development to the east. The neighborhood is undergoing rapid redevelopment in anticipation of the completion of the light rail station one block to the east at Roosevelt Way NE between NE 67th Street and NE 66th Street. Other nearby neighborhood amenities include Roosevelt High School, Green Lake, and Froula Park, Whole Foods, Safeway and Roosevelt P-patch.

The site is zoned MR 1.2 requiring the inclusion of affordable housing units meeting the requirements of SMC chapter 23.58A to achieve the higher FAR allowed in MR zones. No commercial is proposed in the adjacent developments surrounding the site on 8th Ave and 67th St. and this proposal keeps with that standard.

DEVELOPMENT STATISTICS CONTINUED:

LEVEL	FAR SF	# UNITS	USE
ROOF		0	
MEZZANINE	526	0	Residential
5	1791	4	Residential
4	1791	4	Residential
3	1844	4	Residential
2	1844	4	Residential
1	1933	4	Residential
BASEMENT	111	0	Storage / Utilities
TOTAL	9,696	20	



AERIAL 9 BLOCK MAP

SURROUNDING USES & COMMUNITY NODES



1

GREEN LAKE PARK



2

WHOLE FOODS



3

ROOSEVELT HIGH SCHOOL



4

CALVARY CHRISTIAN ASSEMBLY



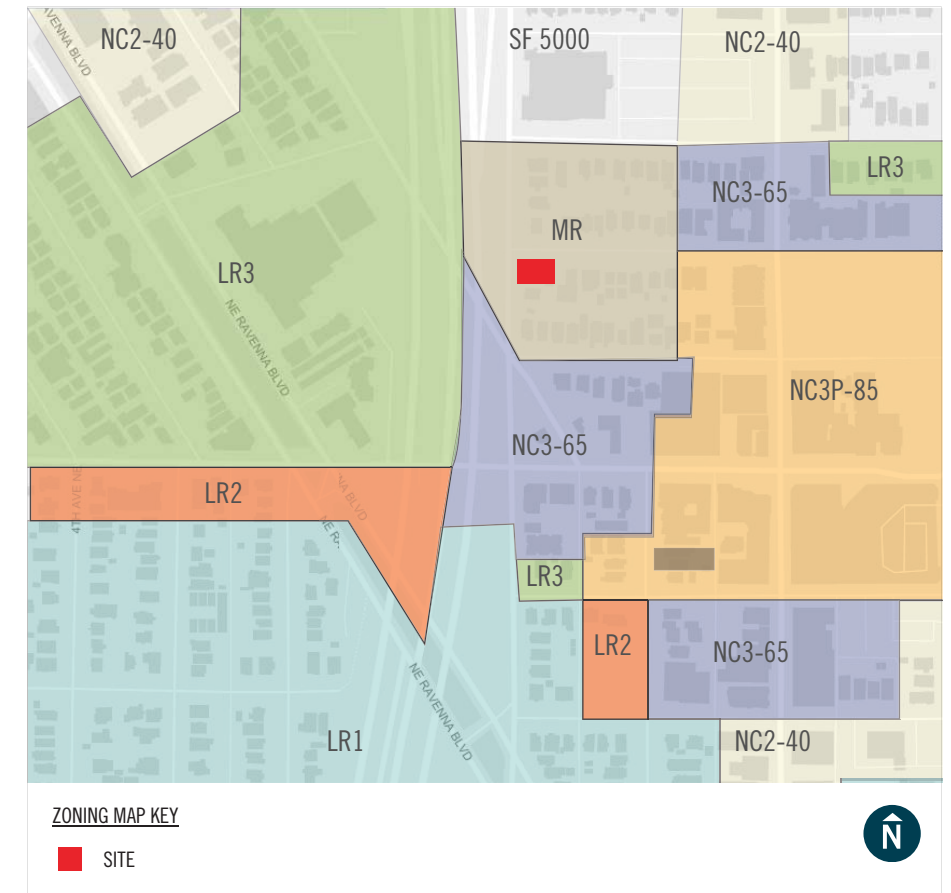
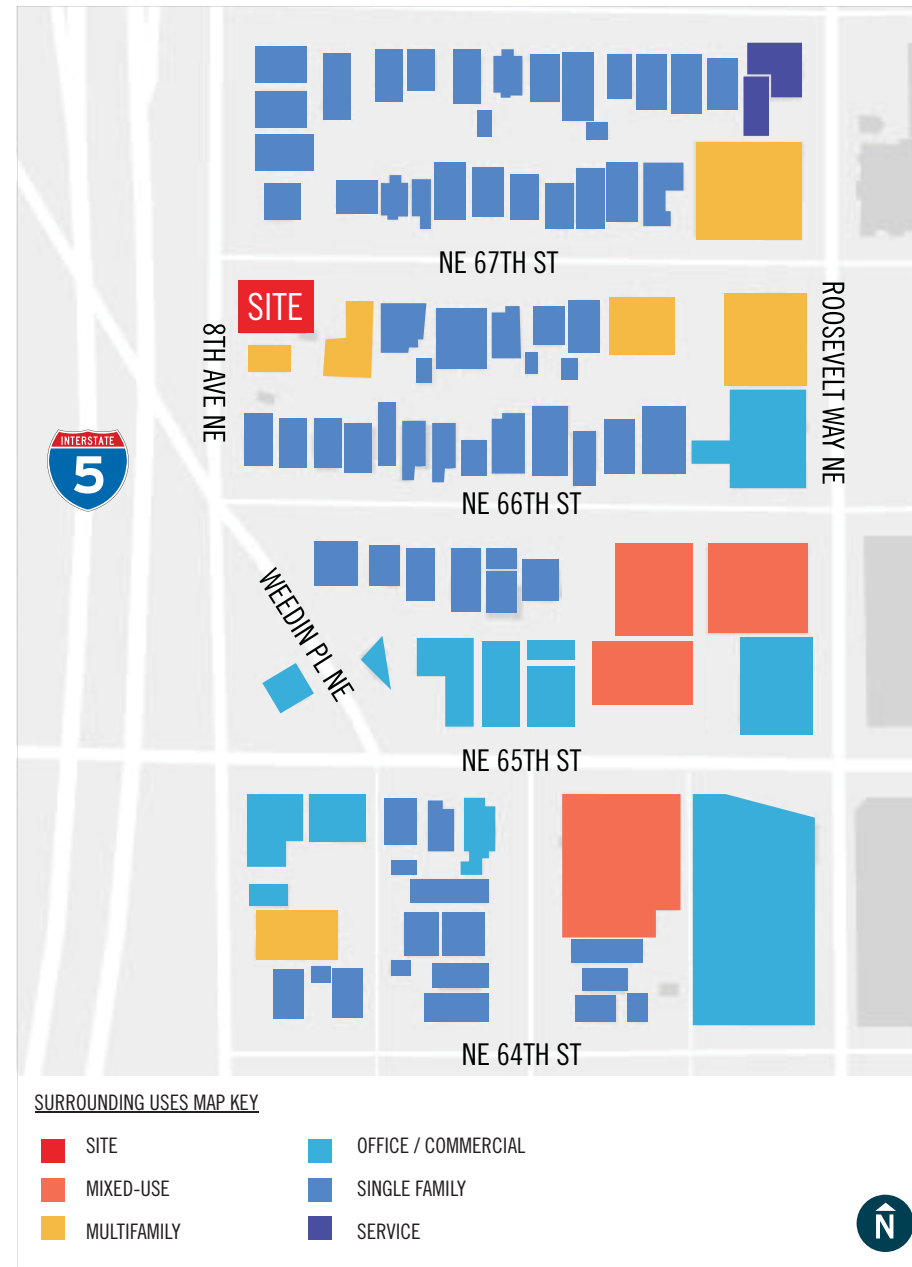
5

FROULA PARK



6

ROOSEVELT P-PATCH COMMUNITY GARDENS





1

KAVELA APARTMENTS AT 845 NE 66TH ST



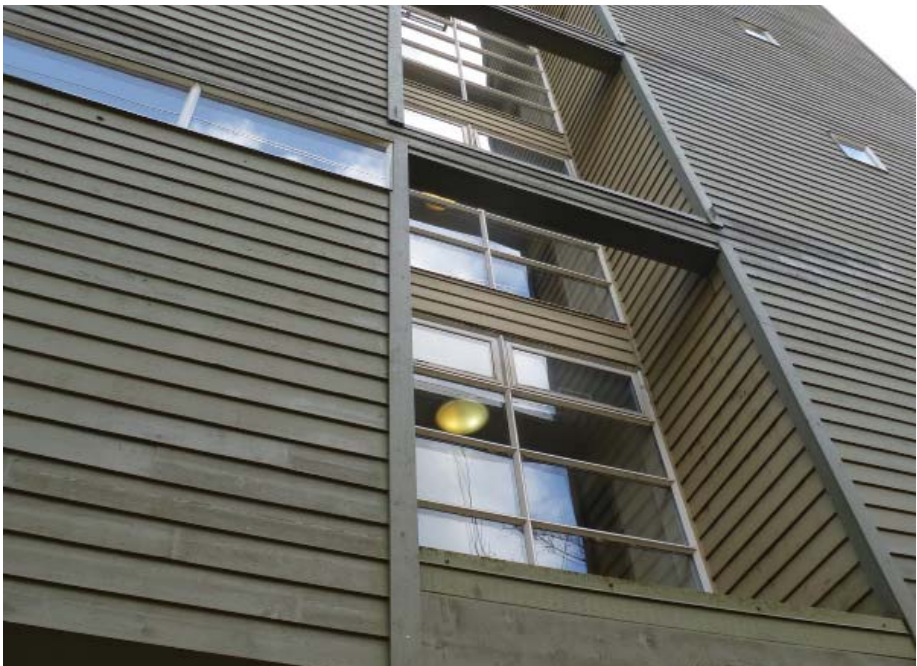
2

858 NE 67TH ST & ROOSEVELT WAY NE



3

TOWNHOUSES AT NE 66TH ST AND WEEDON PL NE



4

NE 67TH ST





1

ROOSEVELT SQUARE, EAST OF SITE



2

ROOSEVELT WAY NE AND 65TH ST



3

8TH AVE NE & WEEDON PL NE



4

FUTURE ELEANOR APARTMENTS AT
NE 67TH ST AND 8TH AVE NE



5

NE 65TH ST AND 12TH AVE NE, T.O.D SITE AND FUTURE
ROOSEVELT LIGHT RAIL STATION



STREETSCAPES

FACING PROJECT SITE



FUTURE APARTMENTS

SINGLE FAMILY HOME

8TH AVE NE, LOOKING EAST



8TH AVE NE, LOOKING WEST



NE 67TH ST, LOOKING NORTH

FUTURE APARTMENTS

PROJECT SITE



NE 67TH ST, LOOKING SOUTH

SINGLE FAMILY HOME

TOWNHOUSES

SITE ANALYSIS

The site slopes evenly down from east to west approximately 12 feet. The high side of the site is currently excavated for a level back yard for the existing structure.

Currently there is a rental house on the property which will be demolished. A setback is required at the rear of the lot which will keep the building away from the shorter less dense townhouse development to the east. A garage on the adjacent southern property sits on the property line and acts as a retaining wall for the grade at the back yard. No parking exists on the existing site due to this excavation.

Transmission power lines run the length of the site along 8th Ave NE and other power lines run the length of the site along NE 67th St. Safety setbacks from these utility lines restrict the potential for modulation and deck spaces on the proposed building.

Solar exposure will be quite restricted by the taller proposed development to the south. A zoning setback above the 42-foot height limit will help give southern facing units daylight and results in additional deck spaces for some units.



1

NE 67TH ST, LOOKING SOUTH



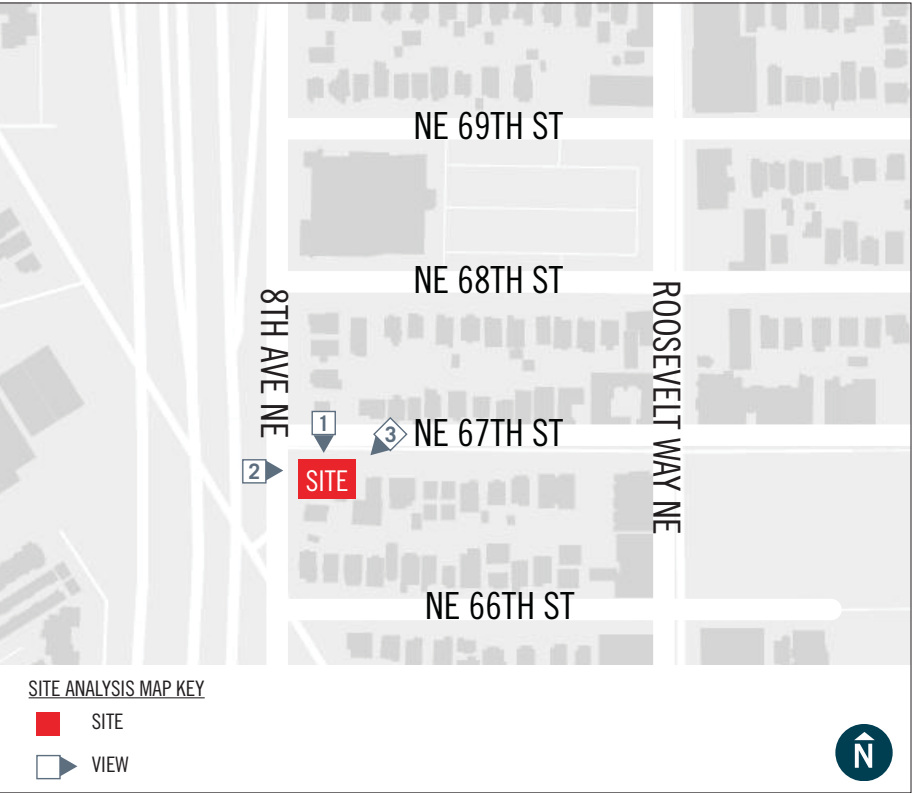
2

8TH AVE NE, LOOKING EAST

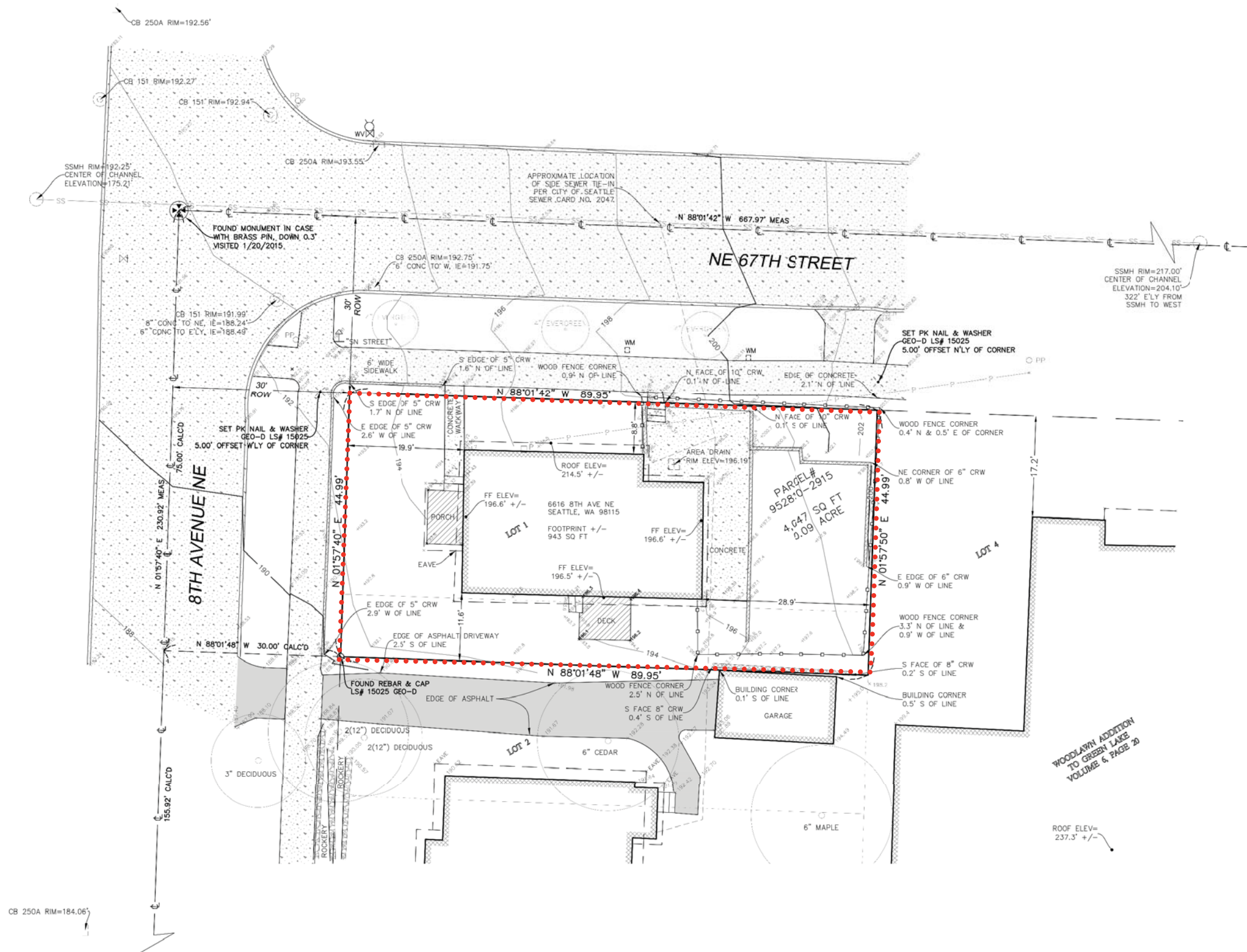


3

NE 67TH ST, LOOKING SOUTH

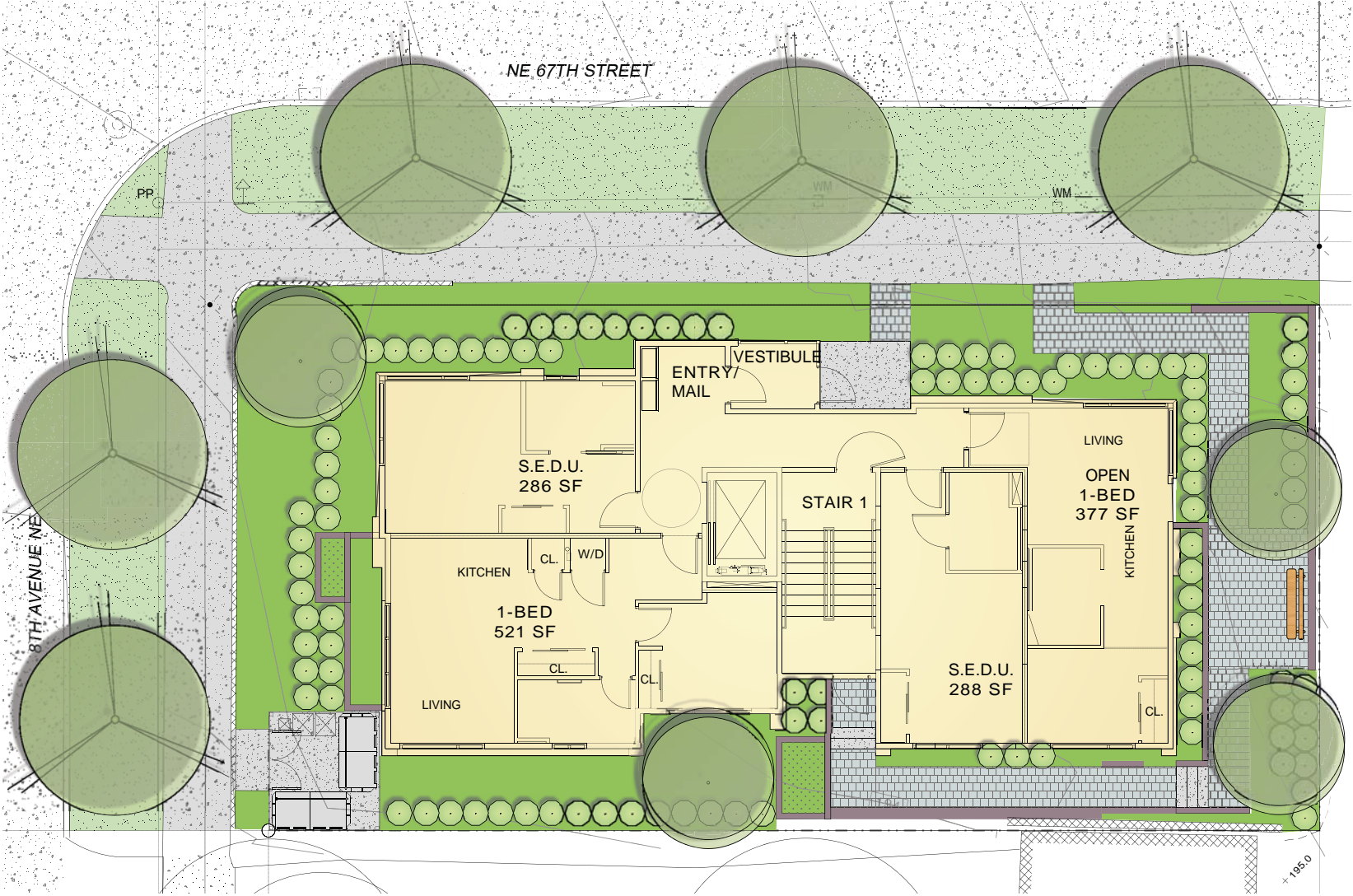


SURVEY



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LANDSCAPE PLAN



APPLICABLE ZONING		SUB-SECTION	REQUIREMENT	PROVIDED
Structure Height	23.45.514	Table B	Base height in MR zones is 60 feet.	See below.
		G	In MR zones the base height is increased by 5 feet if # of stories which are not more than 4 feet above finished grade does not exceed 6 and one of the following conditions is met: <div><div>1.</div><div>The FAR exemption provided in Section 23.45.510.E.4 is used;</div></div> <div><div>2.</div><div>All stories in the structure except those used for parking, have floor to ceiling heights of 9 feet or more</div></div> <div><div>3.</div><div>The lot is split between a MR zone and an NC zone and the base height of the NC zone is 65 feet or more.</div></div>	The proposed structure does include a basement level that does not rise more than 4 feet above the finish grade and is exempt from inclusion in FAR calculations per SMC23.45.510.E.4 and thus meets the requirements of section G for additional structure height. The proposed structure is within this additional height limit.
Setbacks and Separations	23.45.518	Table B	Front and side setback from street lot lines: 7 foot average setback; 5 foot minimum setback Rear Setback: 15 feet from a rear lot line that does not abut an alley Side Setback from interior lot line: 42 feet or less in height: 7 foot average, 5 foot minimum; Above 42 feet in height: 10 foot average, 7 foot minimum	The proposed structure is within the setbacks listed on three sides of the project. The structure encroaches into the side street-facing setback to include space for the building entry. The weighted average setback in accordance with this section of the code is 6.48’ from the lot line. The portion of the structure that contains the entry vestibule also encroaches into the 5 foot minimum setback by 2 feet. The projected entry vestibule stands proud of the rest of the structure to give the viewer a definitive point of access to the building. The entry is defined by a different material palate and glazing pattern. Having the entry facing NE 67th Ave is a safer option as it is (or will be) a more active street than 8th Ave NE, which faces the freeway overpass. The entry vestibule is only located on the first floor and contains no floor space above on other levels. An adjustment request for the reduction of setbacks is given in the adjustments section of this packet.
		I	Unenclosed decks and balconies may project a maximum of 4 feet into required setbacks if each one is: <div><div>1.</div><div>No closer than 5 feet to any lot line</div></div> <div><div>2.</div><div>No more than 20 feet wide</div></div> <div><div>3.</div><div>Separated from other decks and balconies on the same facade of the structure by a distance equal to at least ½ the width of the projection</div></div>	Unenclosed decks projecting from the east facade meet this requirement and add a functional private outdoor space for their respective apartment unit. The decks project 3 feet 9 inches and measure 10 feet 9 inches wide. There are no other projections on this facade.
Amenity Area	23.45.522	C	The required amenity area in MR zones is equal to 5 percent of the total gross floor area of a structure in residential use.	The amenity area provided is equal to or greater than 5 % of the gross floor area in residential use.
		D	Required amenity areas shall meet the following requirements: <div><div>1.</div><div>All units shall have access to a common amenity area</div></div> <div><div>2.</div><div>In MR zones no more than 50 percent of the amenity area may be enclosed, and this enclosed area shall be provided as common amenity area.</div></div> <div><div>3.</div><div>Projections into amenity areas: structural projection that do not provide floor area may extend up to 2 feet into an amenity area if they are at least 8 feet above finished grade.</div></div> <div><div>4.</div><div>Private amenity area: There is no minimum dimension for private amenity areas, except that if a private amenity area abuts a side lot line that is not a street lot line, the minimum horizontal dimension measured from the side lot line is 10 feet.</div></div> <div><div>5.</div><div>Common Amenity area for apartments shall meet the following conditions:<div><div>a.</div><div>No common amenity area shall be less than 250 square feet in area, and shall have a minimum horizontal dimension of 10 feet.</div></div><div><div>b.</div><div>The common amenity area shall be improved as follows: at least 50 percent of the area provided at ground level shall be landscaped with grass, ground cover or bushes and trees. Elements that enhance the usability and livability of the space for residents shall be provided.</div></div></div></div>	Both common and private amenity areas are proposed on this project. The common amenity area is accessible to all residents and includes a paved path and patio space which will have a privacy screen against the street. There are also decks and balconies accessory to some units which will also provide amenity space.

DESIGN GUIDELINES

CONTEXT & SITE: CS1 NATURAL SYSTEMS AND SITE FEATURES

I. Responding to Site Characteristics

Response: The site is the southeast corner lot on NE 67th St. and 8th Ave NE. Directly across the street from 8th Ave NE is the Interstate 5 freeway. The proposed 5-story building will be highly visible from traffic on the freeway going both into and out of the city. The area along NE 67th St. is undergoing rapid development of similar sized and larger projects. The project has many main unit windows facing NE 67th which will is being developed into the more activated street. The proposed structure is setback from adjacent lots to the rear and the south. Upper level zoning setbacks from the lot to the south will afford the proposed building more solar exposure. The site will also receive good western solar exposure due to the location and setback of the freeway.

CS1 URBAN PATTERN & FORM

- I. Streetscape Compatibility
- II. Corner Lots
- III. Height, Bulk and Scale

Response: The proposed project is in an MR zone with no commercial spaces proposed in the vicinity. The character of the surrounding street is predominantly apartment or small multifamily dwellings. Entries to these buildings are defined with unique window patterns and gates or doors. The proposed entry is located on NE 67th St. which is more active than 8th Ave NE. The entry vestibule stands proud of the rest of the structure and is accessed via a simple walkway from the sidewalk. The windows and materials enclosing the entry are unique from the rest of the building. The siding pattern above the entry also is differentiated and framed by other siding elements to reflect the stacking of units. Although not located on a “gateway” intersection, the proposed project seeks to be a signifier of the neighborhood to those travelling on the freeway above. The unique geometrical framing and façade is easily seen from the freeway and has the potential to be a visual queue to those travelling to and from the city. As such the building is located close to 8th Ave. leaving a setback to the east for an open amenity space facing the shorter townhouse structures.

CS3 ARCHITECTURAL CONTEXT & CHARACTER

I. Architectural Context

Response: As mentioned the area along NE 67th is undergoing rapid development from a single family residential scale to a large multifamily scale with several projects of varying size either under construction or planned. No one style is predominant, but both new and old apartment buildings are defined by vertical blocks of color or fenestration used to define the stacking of units. Although not in the commercial core the proposed project intends to follow this model by breaking up the façade along the stacks of units. Material changes are informed by the location of apartments versus public corridors. Framing and material texture is used to accent the division between units or spaces within the units.

PUBLIC LIFE: PL2 WALKABILITY

I. Pedestrian Open Spaces & Entrances

Response: Although the project is located outside of the commercial core the proposed project intends to maintain the buffer of landscaping between the sidewalk and the building as a way to enhance the streetscape. Pathway lighting will be subtle and unobtrusive to adjacent properties. See above for explanation of the pedestrian entry.

PL3 STREET-LEVEL INTERACTION

- I. Human Interaction
- II. Transition between Residence & Street

Response: See above. The entry vestibule as a structure acts as the transition between the sidewalk and the street. Occupied space within the building is setback from the street for privacy.

DESIGN CONCEPT: DC1 PROJECT USES & ACTIVITIES

I. Parking and Vehicle Access

Response: The project is located in the Station Overlay and no parking is required or proposed.

DC2 ARCHITECTURAL CONCEPT

I. Architectural Concept & Consistency

Response: TThe proposed project seeks to be a signifier of the neighborhood viewed by those travelling at speed along the freeway. The unique geometrical façade is intended to grab the viewer’s attention for the short moment it is seen so as to make the viewer associate the building with that particular place. The project is divided into quadrants, each generally facing a cardinal direction and each framed by a block of color to reflect the stacking of units on the floor plate. The framing elements on the two western quadrants facing the freeway are different depending on viewer’s perspective coming from either the north or the south. The angled siding and framed element facing north and the boxed element facing south are mirrored on the two eastern quadrants.

DC3 OPEN SPACE CONCEPT

I. Residential Open Space

Response: Proposed open space is used to separate the building from a lower-intensive use adjacent to the east. The townhouses to the east also have open space facing west, so there is larger net space between the actual buildings. The common space faces NE 67th St. and will have a low fence to act as a separation. Positioning the amenity space on the eastern side of the property takes advantage of the structure as a noise barrier. The common space will have a patio which is large enough for a small gathering along with a pathway to the rear of the lot and landscaping for privacy of the adjacent property. Part of the open space between the building and the common area will be a private amenity space devoted to the unit on the ground floor. There are also decks and balconies on some units for additional private outdoor space.

DC4 EXTERIOR ELEMENTS & FINISHES

I. Exterior Finish Materials

Response: Durable, long lasting materials will be used throughout the project. The material palette consists of fiber cement panel, metal corrugated infill panels and concrete. Color is pivotal in drawing attention from a distance. The color palette uses a bold red in a white field to attract the eye. Neutral grays are used in a playful manner which belies their usual coolness in tone. The cascading variation of grays draws attention to the entry and operates with its own signaling effect to denote the difference in use inside the building from that of apartment units.

RENDERINGS



RENDERINGS



RENDERINGS



RENDERINGS



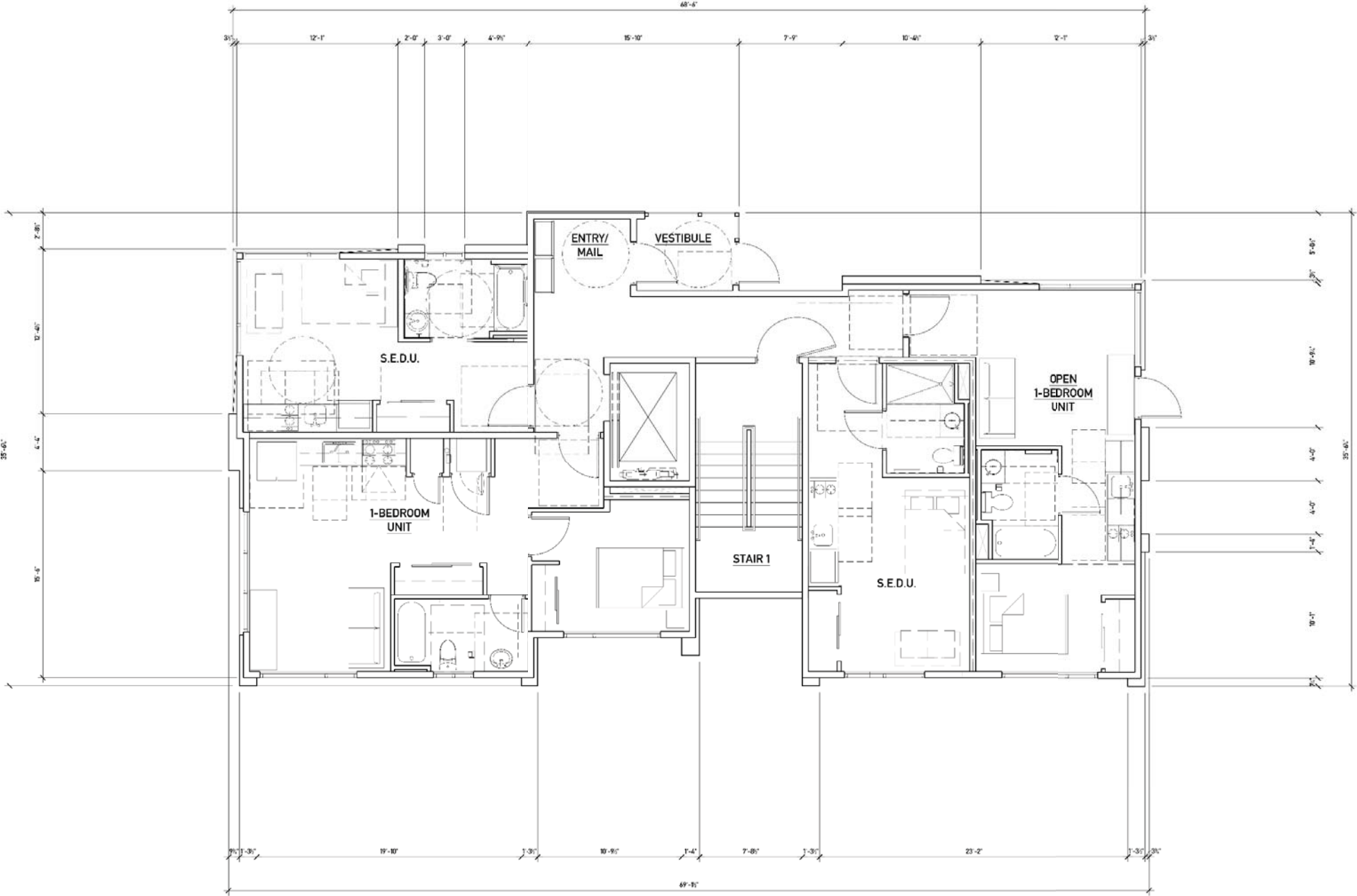
RENDERINGS



RENDERINGS



FLOOR PLAN | LEVEL 1



1 LEVEL ONE PLAN
1/4" = 1'-0"



FLOOR PLAN | LEVEL 2-3

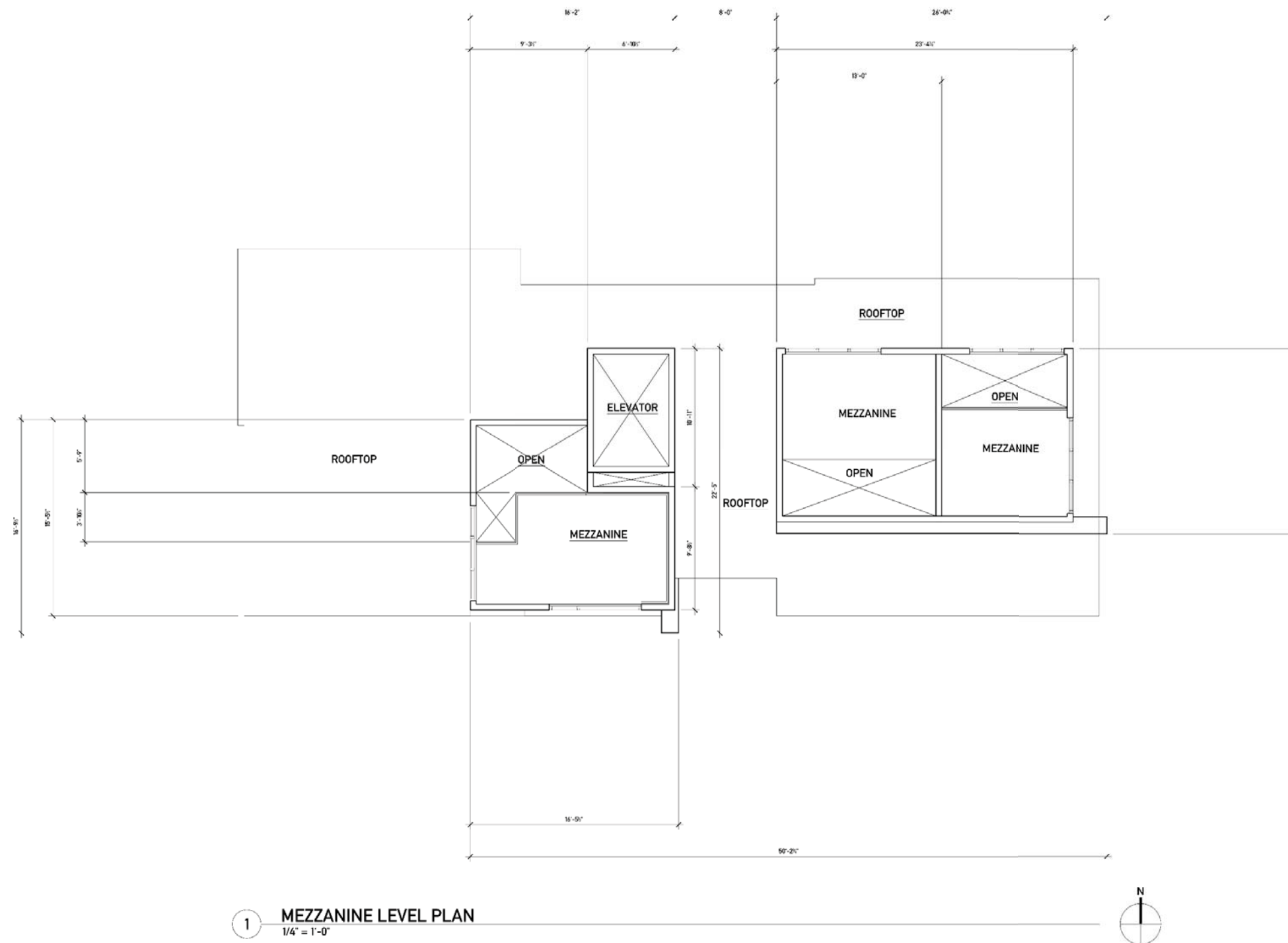


FLOOR PLAN | LEVEL 4-5

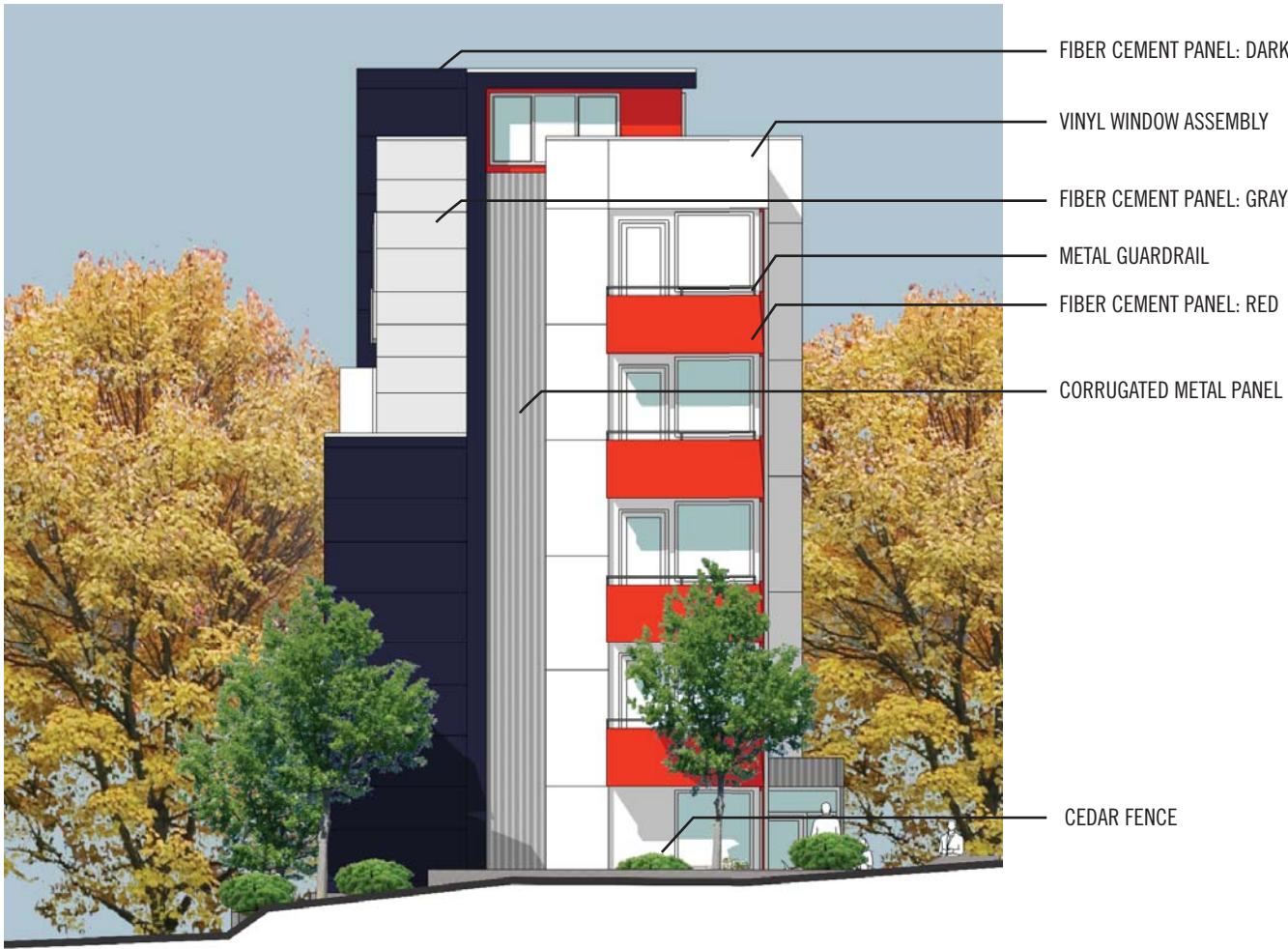
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FLOOR PLAN | MEZZANINE



ELEVATIONS | EAST & WEST



EAST ELEVATION



WEST ELEVATION



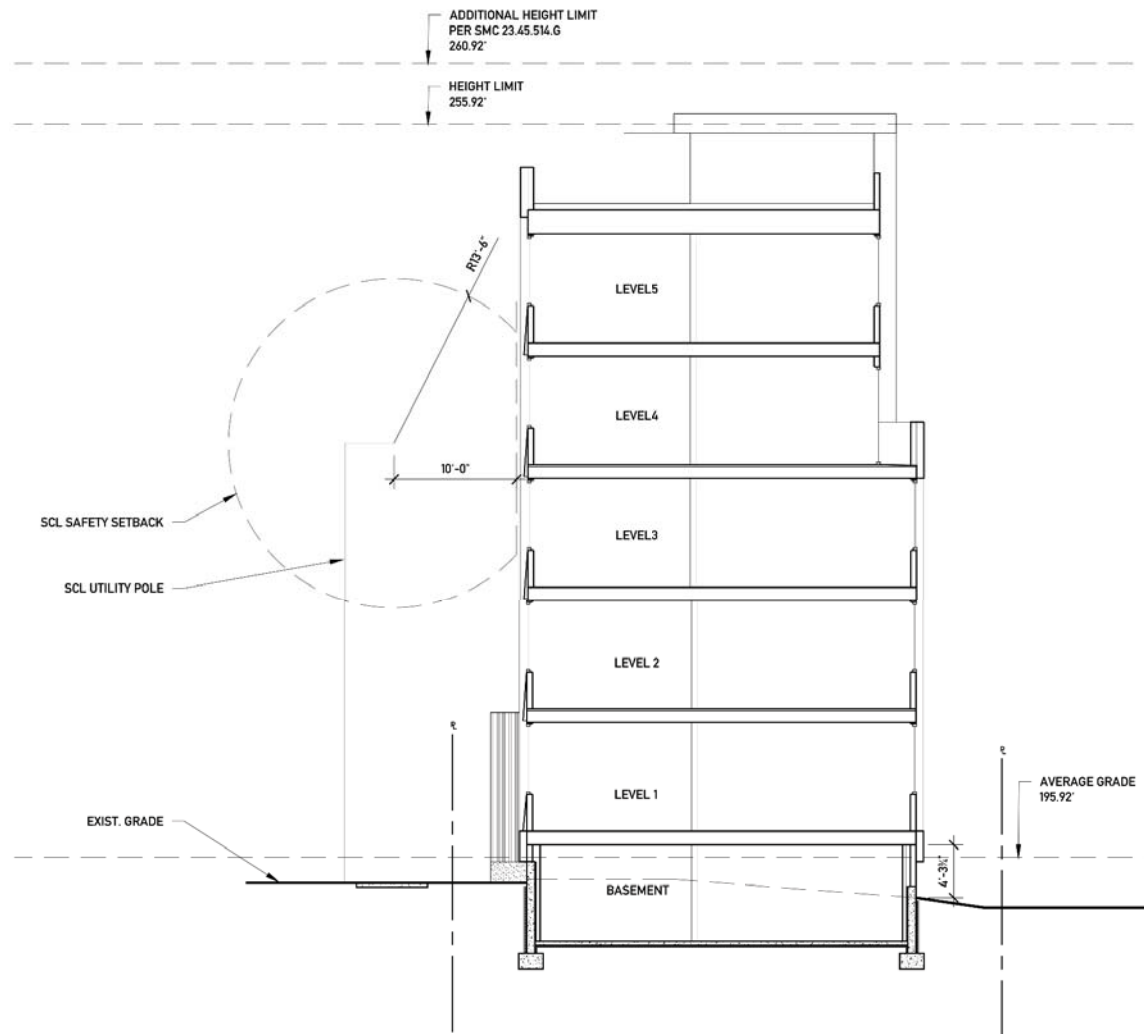
ELEVATIONS | NORTH



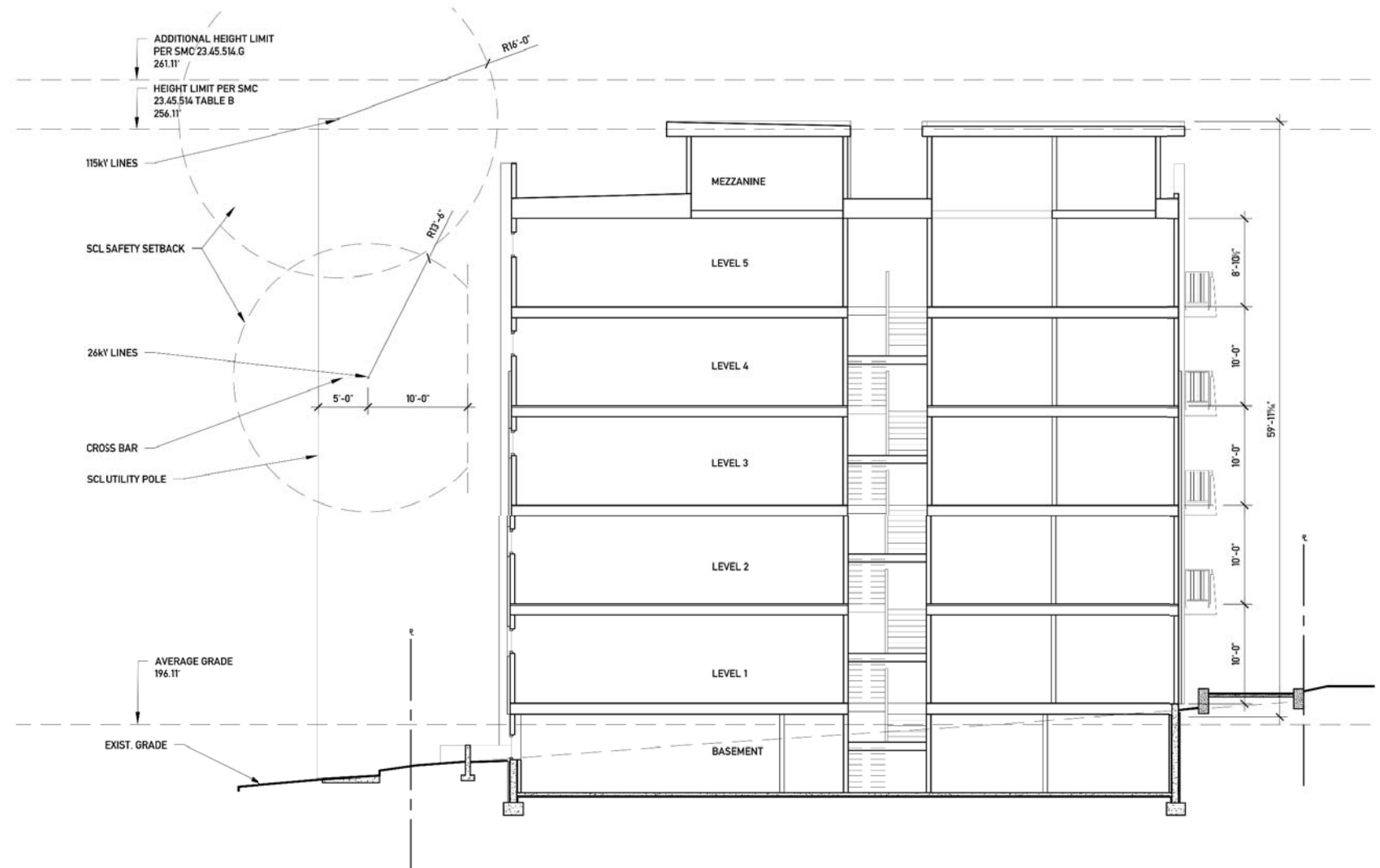
ELEVATIONS | SOUTH



SECTIONS



B CROSS SECTION



A LONGITUDINAL SECTION



ADJUSTMENT REQUEST

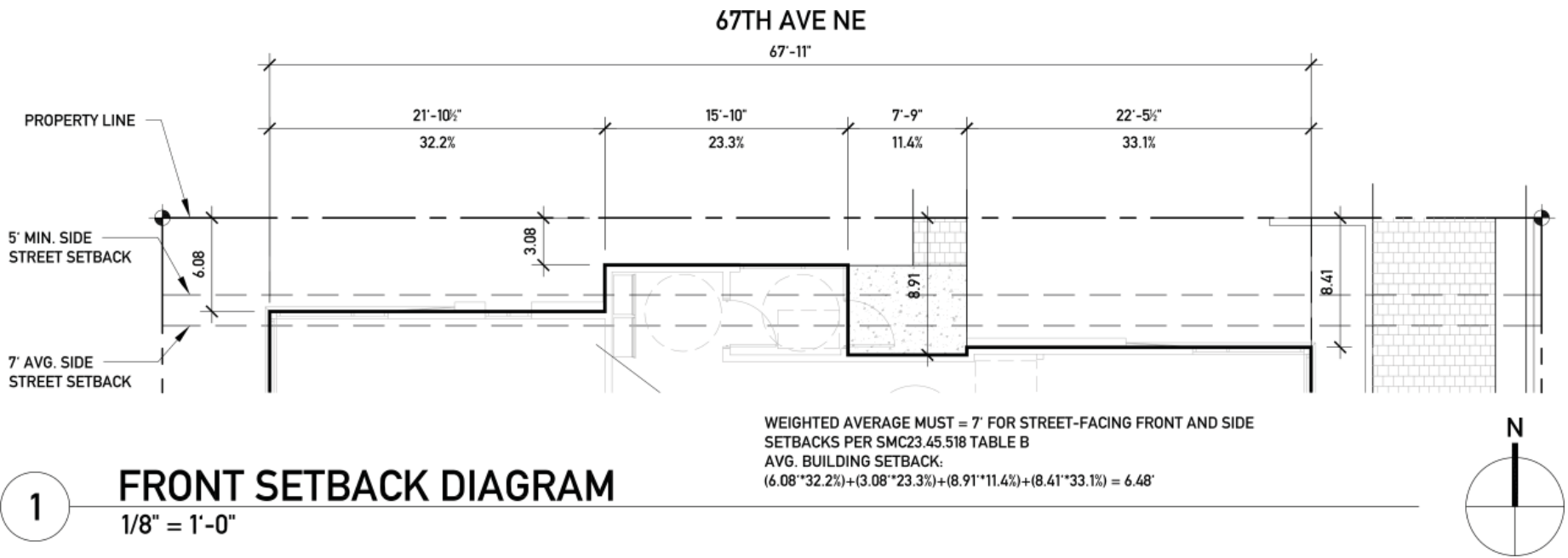
FRONT SETBACK ADJUSTMENT REQUEST

SMC 23.45.518 – Table B: Setbacks in MR zones
Front and side setback from street lot lines shall be 7 foot average; 5 foot minimum

Response: The proposed design has a weighted average building setback of 6.48' from the property line, which is less than the code requirement of 7 feet average. However, the proposed minimum building setback is 3.08' from the property line, which is also less than the code requirement for minimum setback.

To accommodate a unique and definitive entry that is also functional in the small building footprint we are proposing that a small portion of the building extend beyond the minimum setback from the street. The portion of the building enclosing the entry is the minimum needed to accommodate the required vestibule and turning radius into the building. This entry extension will only be at the ground level with no portion overhead extending as far into the setback. To keep the average setback closer to the code requirement 44.5% of the proposed building is setback 8.91-8.41 feet from the street.

The SDR process allows setbacks be reduced by a maximum of 50% through an adjustment. We request that the street minimum setback be reduced by 40% (2 feet) and the average setback be reduced by 10% (9 inches) to accommodate the building entry.



ADJUSTMENT REQUEST

REAR SETBACK ADJUSTMENT REQUEST

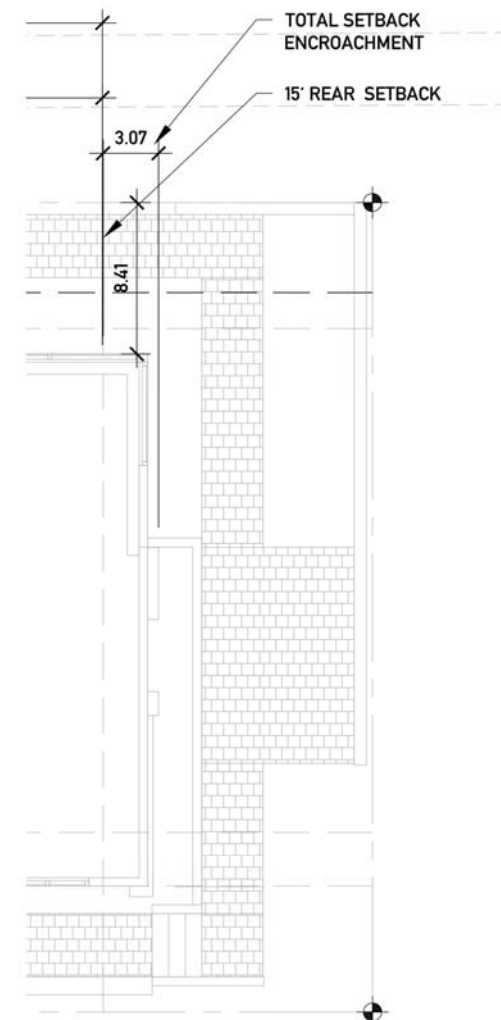
SMC 23.45.518 – Table B: Setbacks in MR zones

Rear setback if no alley shall be 15 feet.

Response: The proposed design has a rear setback of 11.93' which is less than the code requirement of 15'.

The architectural concept of this project as discussed in the design guidelines has resulted in a form that is unique and seeks to be an identifiable part of the neighborhood. The safety setbacks required of the high voltage power lines that run along 8th Ave NE if applied to the zoning envelope would result in a façade that is broken down into many small and awkward planes. To maintain a consistent character and form the building is located 3.07 feet at maximum into the rear setback with most of the structure more than 12.5' away from the rear property line. This allows the street-facing façade to remain in one plane to present a consistent façade. The distance between the rear property line and the building is still the largest setback on the site and accommodates the common amenity area and landscaped areas for privacy.

The SDR process allows setbacks be reduced by a maximum of 50% through an adjustment. We request that the rear setback be reduced by 20.05% (3.07 feet) to maintain the architectural concept while keeping the structure outside of the safety setbacks imposed by the utility lines.



REQUIRED REAR SETBACK PER SMC23.45.518 TABLE B: 15' IF NO ALLEY.

1 REAR SETBACK DIAGRAM

1/8" = 1'-0"