



14TH & MADISON
1401 EAST MADISON

DESIGN REVIEW RECOMMENDATION
DPD #3020065 / 13 APRIL 2016



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PROJECT INFORMATION

ADDRESS: 1401 E MADISON STREET
DPD PROJECT #: 3020065

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DEVELOPMENT OBJECTIVES

The proposed project is 6-story mixed-use residential building with below-grade parking. The basic program includes:

- 137 RESIDENTIAL UNITS
- 83,000 SF OF NET RENTABLE AREA
- 3,800 SF OF STREET-LEVEL COMMERCIAL AREA
- 81 BELOW-GRADE PARKING STALLS

PROJECT GOALS

CREATE A DENSE, VIBRANT PLACE TO LIVE IN CAPITOL HILL

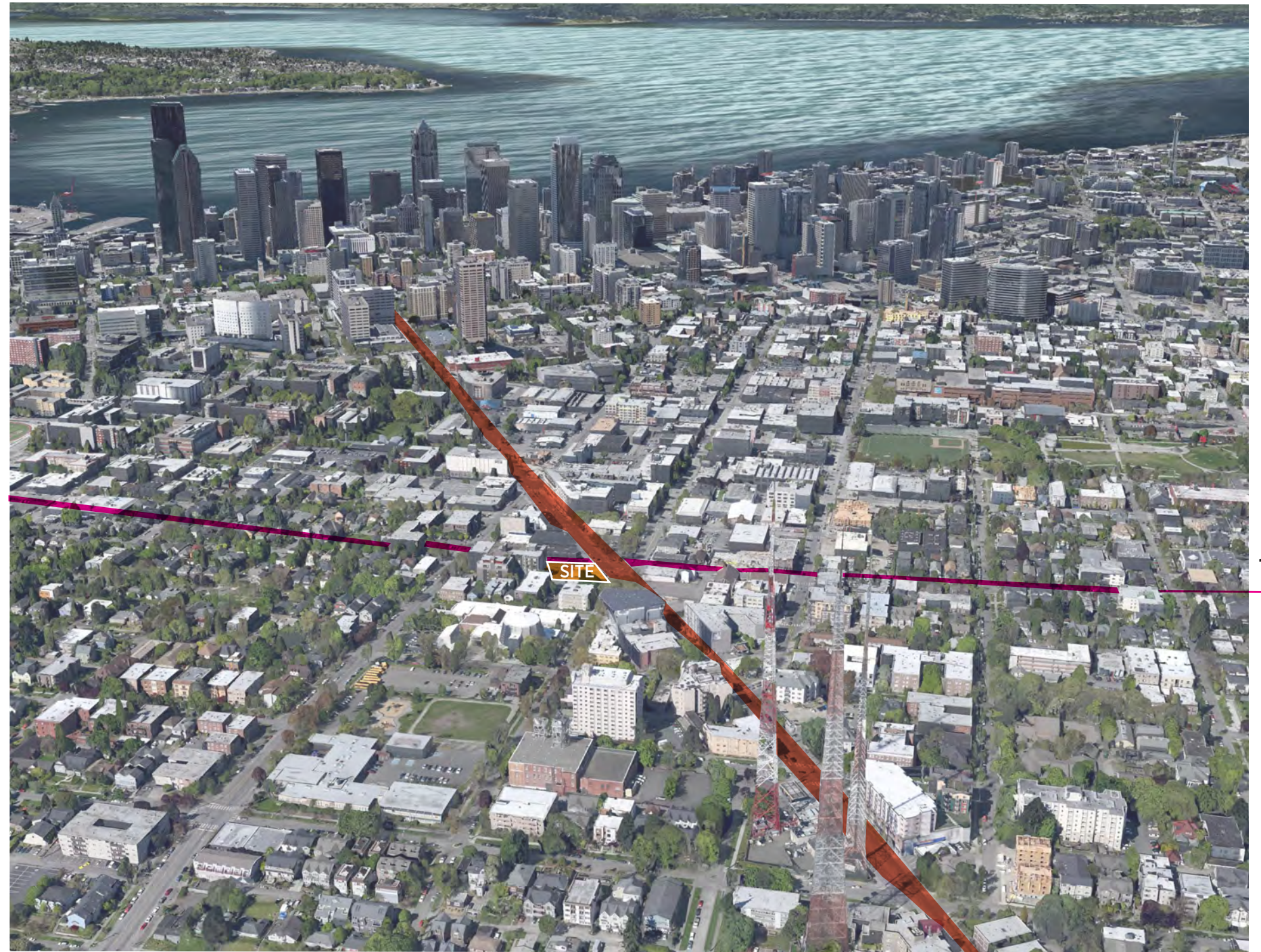
- Improve the pedestrian experience along 14TH Avenue through the use of landscaping and building overhangs and/or canopies.
- Mitigate the auto-oriented nature of Madison Street through development of the street edge, while maintaining residential privacy.

CONTRIBUTE TO THE CHARACTER OF THE NEIGHBORHOOD

- The architectural character of the project is informed by the neighboring development. Massing and siting is appropriate to the street and relates to the neighborhood context.

ADDRESS THE SITE CONTEXT

- Understand and respond to its multiple contexts: the hard edge of the street along the north and west edges (14TH and Madison); the low-rise character to the east of the project site, especially as it transitions up Pike; and its corner location.
- Reinforce the open space connections and respond to the topography.



14TH AVE

E MADISON ST

4.0 CONTEXT ANALYSIS

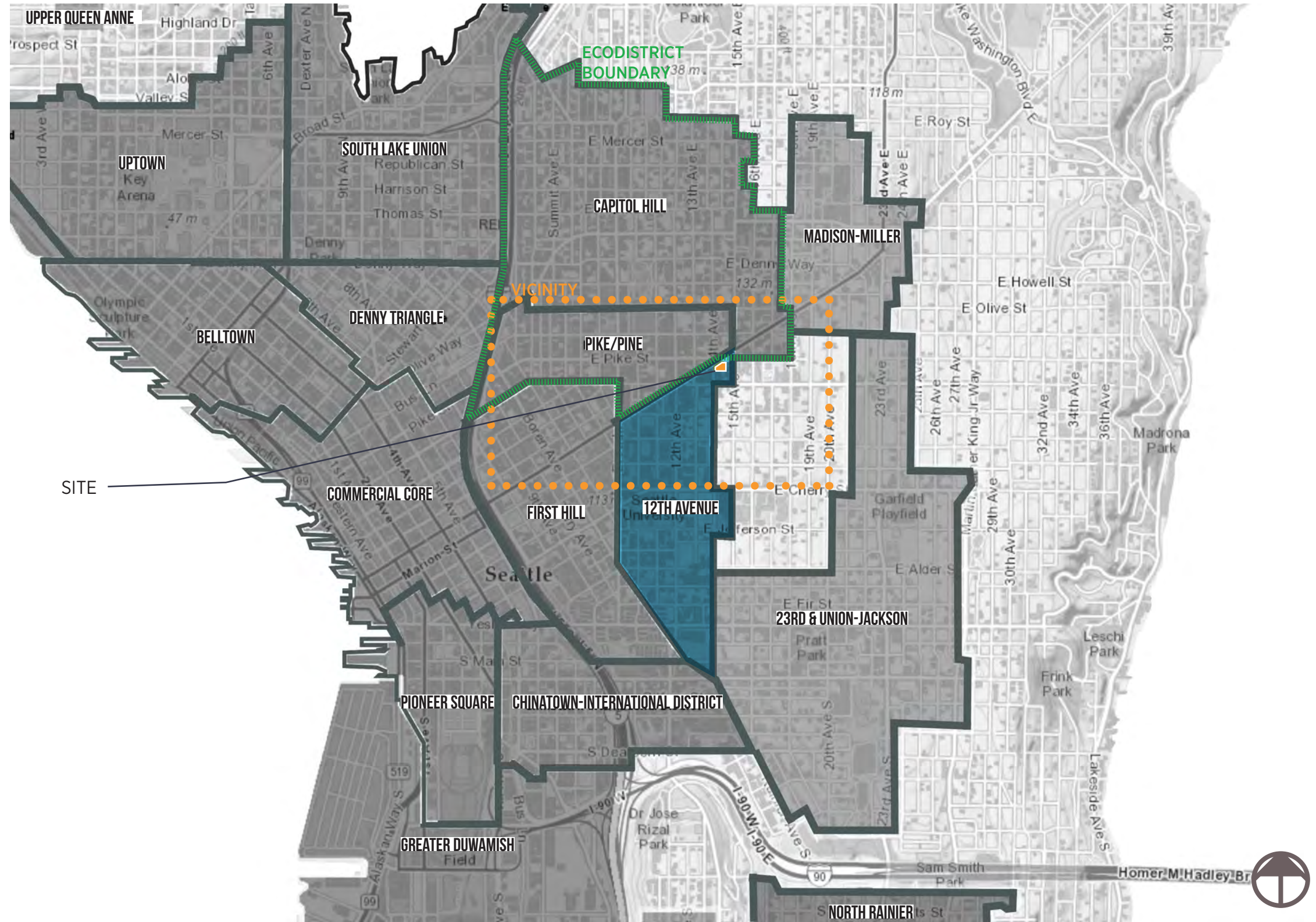
ZONING & OVERLAY DESIGNATIONS

The site is located in Capitol Hill, one of the most vibrant neighborhoods in Seattle. The neighborhood is comprised of a mix of uses, with most new development being residential in nature.

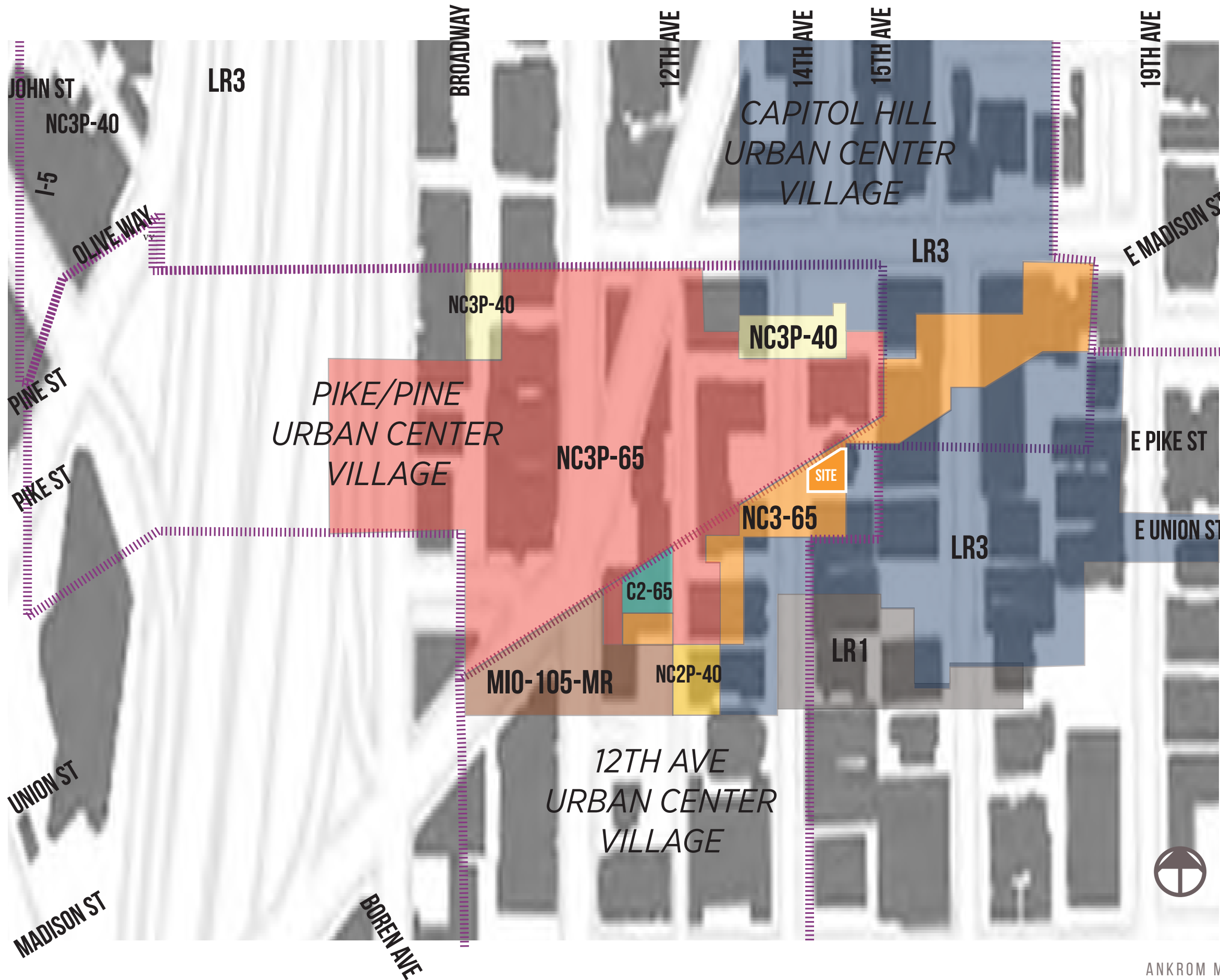
It is located within the 12TH Avenue Urban Center Village. It borders the Pike/Pine Urban Center Village to the north and the Capitol Hill Urban Center Village to the northeast.

The site is also located across the street from the Capitol Hill EcoDistrict boundary, a framework for sustainability applied at a neighborhood scale.

The site is zoned Neighborhood Commercial 3 with a 65 foot height limit (NC3-65).



4.0 CONTEXT ANALYSIS



ZONING SUMMARY

KING COUNTY PARCEL NUMBER
7836800025-09

CODE: Seattle Municipal Code, Title 23 Land Use Code

DESIGN GUIDELINES
City of Seattle Design Guidelines

ZONING CLASSIFICATION (MAP 111)
NEIGHBORHOOD COMMERCIAL NC3-65

OVERLAY ZONING: 12Th Avenue Urban Center Village

SITE AREA
Total Lot Area = 26,068 SF (per survey)

STREET CLASSIFICATION: ARTERIAL, TRANSIT
East Madison Street: Principal Arterial, Major Transit Street
14th Avenue: Collector Arterial, Local Transit Street
East Pike Street: No Classification

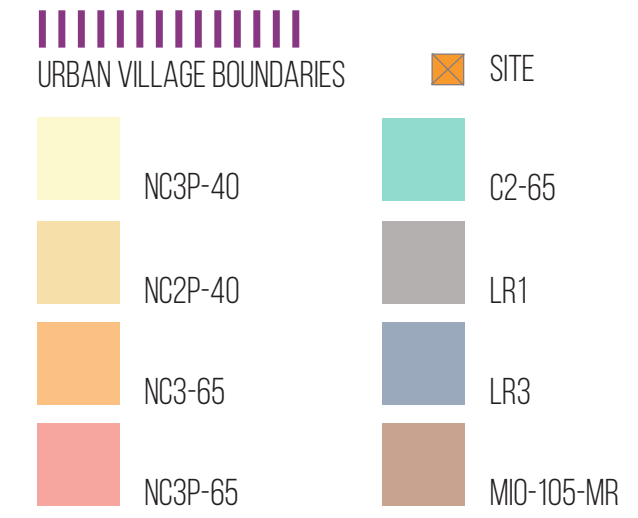
PERMITTED USES (23.47A.004)
Residential, office, hotel, general retail sales, and multipurpose retail sales.

FAR (23.47A.013)
Total Mixed-Use Structure: 4.75
Total Single Use: 4.25

MAXIMUM ALLOWABLE AREA (23.47A.013)
(Site Area x FAR) 26,068 SF x 4.75 FAR = 123,823SF

PARKING
No minimum requirement

MAXIMUM ALLOWABLE AREA (23.47A.013)
(Site Area x FAR)
26,068 SF x 4.75 FAR = 123,823SF



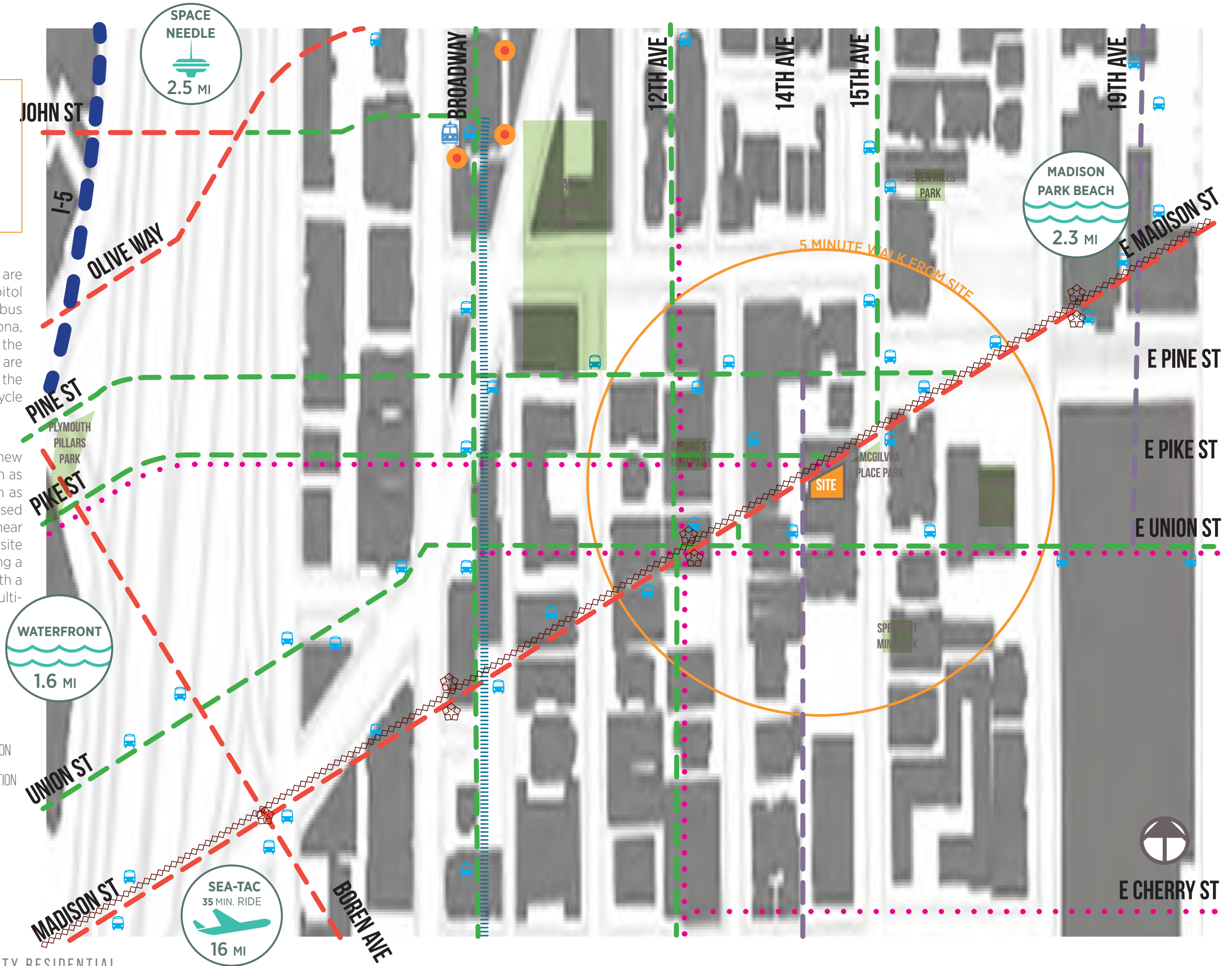
4.0 CONTEXT ANALYSIS

TRANSPORTATION, SITE VIEWS & FEATURES

Walking, bicycling, and taking the public transit are all excellent alternatives to driving in the Capitol Hill neighborhood. The site is surrounded by bus routes providing direct access to and from Madrona, Madison Valley, Downtown, North Capitol Hill, and the University District. Dedicated nearby bicycle lanes are on 12th Avenue, Pine and Union Streets. In general, the neighborhood is extremely bicycle friendly and bicycle use is high.

Northwest of Cal Anderson Park is the site of the new light rail station, which connects riders as far south as the SEA-TAC airport, and in the future as far north as Northgate. Our site is also located along the proposed Bus Rapid Transit Corridor along Madison. It is also near the future Streetcar station along Broadway. Our site is remarkably well located for those who are seeking a car free lifestyle. Our project will treat the street with a sensitivity towards traveling by bicycle, foot and multi-modal public transit.

- INTERSTATE
 - PRINCIPAL ARTERIALS*
 - MINOR ARTERIALS*
 - COLLECTOR ARTERIALS*
 - BIKE LANE
 - FUTURE STREETCAR LINE
 - PROPOSED BUS RAPID TRANSIT CORRIDOR**
 - SITE
 - MAJOR BUS STOPS
 - FUTURE LIGHT RAIL STATION
 - FUTURE STREETCAR STATION
 - PROPOSED BRT STATION
- *Per Seattle Arterial Classifications Planning Map
**Per SDOT Seattle Transit Master Plan



4.0 CONTEXT ANALYSIS

SITE VIEWS

Upper level floors and roof deck have potential panoramic views toward downtown to the west, with secondary views of the Space Needle to the northwest of the site. Additional views from the site include Queen Anne and the Space Needle.

STREET CHARACTERISTICS

EAST MADISON STREET- (Northwest of Project Site)

- Principal Arterial Street – Serves as the principal route for the movement of traffic through the City. (Seattle Arterial Classifications Map).
- Major Transit Street (Transit Classification Map).
- Madison is characterized as a fast-moving, busy street with high traffic and high noise in each direction of the two lane street. There is currently off peak parking in front of the site. The change in grade presents an access challenge.

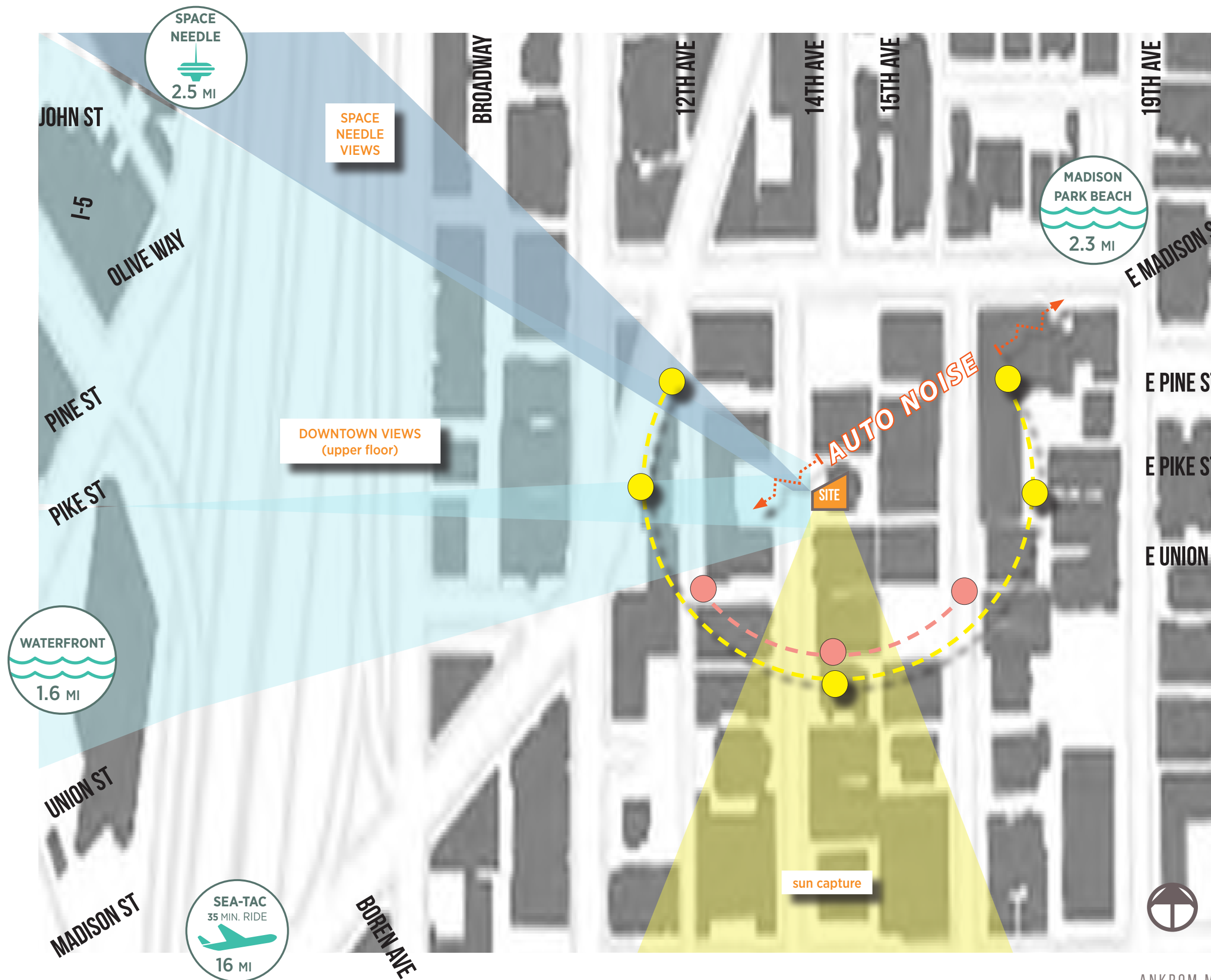
14TH AVENUE - (West of Project Site)

- Collector Arterial Street – Collects and distributes traffic from Principal and Minor Arterials to Access Streets or directly to local destinations (Seattle Arterial Classifications Map).
- Local Transit Street (Transit Classification Map)

EAST PIKE STREET - (North of Project Site)

- Unclassified Street
- Arterial/Access Street – provides project access from the arterial network. (Seattle Transit Classifications Map, Seattle Arterial Classifications Map)
- Pike, at this location, is typically used for immediate neighborhood access. There is a significant pedestrian curbcut to allow for a crossing to Madison and McGilvra Place Park.

- 21 JUNE 2015: SUNRISE 5:11 AM, SUNSET 9:11 PM
- 22 DECEMBER 2015: SUNRISE 7:55 AM, SUNSET 4:21 PM



4.0 CONTEXT ANALYSIS

NODES, NOTABLE PATTERNS & DESIGN CUES

THE MADISON AVENUE BUILDING RESPONSE TO THE TOPOGRAPHY HOLDING THE STREET EDGE

Buildings along Madison Avenue all hold the street edge, regardless of whether they hold residential or retail uses. They step up with the topography. Modulation, if any, occurs at building entries. The level and type of transparency varies depending on use. In general, a punched opening denotes residential uses, the use of storefront denotes retail uses.

In general, newer residential buildings have their primary entry off of Madison Avenue, using the busy street for secondary means of egress only. This is also true of the main commercial and retail entries: these entries mainly occur at the corner or just off the corner at the intersecting street. Older buildings, especially those that are all residential in function, have their primary entry along Madison. Often recessed, these entries utilize stairs to mitigate the grade change.

There has been some softening and creative treatment of the building edges along Madison; however most hold the edge firmly. We propose to use some landscaping to soften the transition between the building edge and Madison as the building ramps up toward East Pike Street and to hold the corner firmly at the retail edge. Keeping in line with the newer building precedents, we also propose to create an inviting residential building entrance off of Madison, just as the building turns the corner east, right on E Pike Street.

✘ SITE



VIVA



TRACE LOFTS



CITIZEN



1305 E MADISON



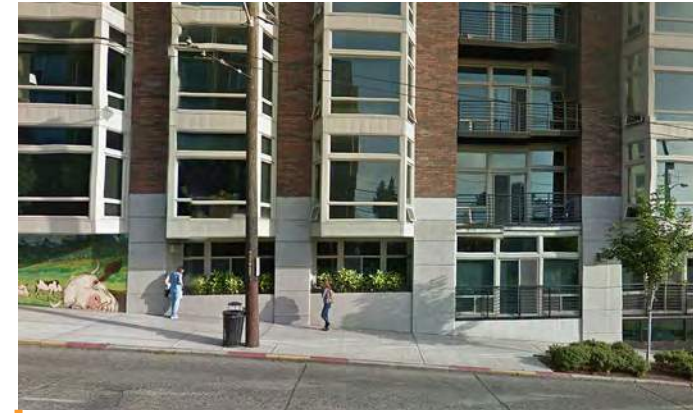
PEARL APARTMENTS



CENTRAL CO-OP



1700 MADISON



LAWRENCE LOFTS



BULLITT CENTER



1605 E MADISON



1785 E MADISON



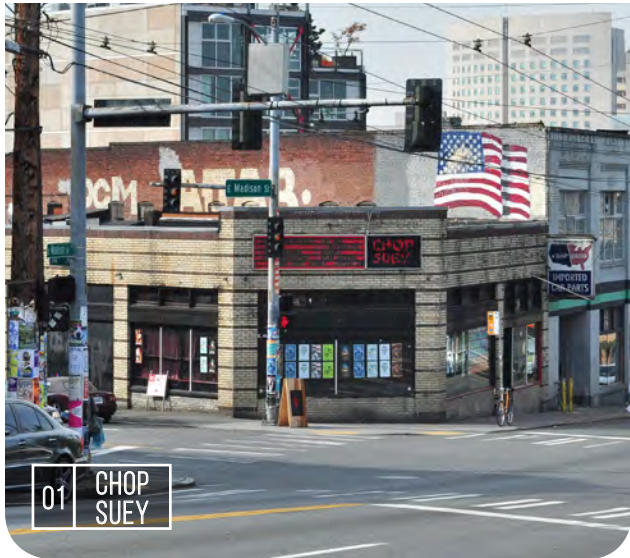
4.0 CONTEXT ANALYSIS

SURROUNDING USES & STRUCTURES

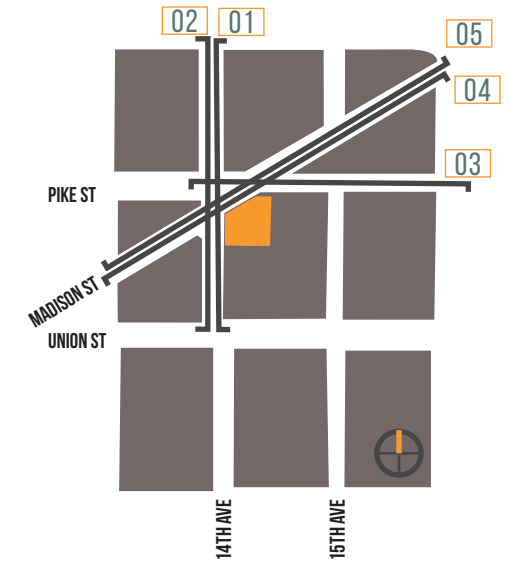
Surrounding uses and structures include an eclectic mix of old and new residential and commercial developments. The surrounding context continues to evolve, with a large mixed-use residential development just completed and a new one in the planning stages across the street. While there are a stock of classically detailed brick buildings, most new buildings tend to use modern materials and massing to add a new character to the neighborhood.



- 1 CHOP SUEY
- 2 BANK OF AMERICA
- 3 ARTIFICIAL LIMB CO (COMMERCIAL BUILDING)
- 4 COMMERCIAL BUILDING
- 5 REO FLATS
- 6 1420 E MADISON (FUTURE MIXED-USE/FORMER TACO TIME SITE)
- 7 PEARL APARTMENTS
- 8 BULLITT FOUNDATION
- 9 MCGILVRA PLACE PARK
- 10 IMPERIAL APARTMENTS
- 11 TEMPLE DE HIRSCH SINAI
- 12 QUALMAN APARTMENTS
- 13 CHLOE APARTMENTS
- 14 DIESEL/BAR SUE
- 15 RESTAURANT ZOE
- 16 1310 E UNION LOFTS
- 17 PONY BAR
- 18 TRACE LOFTS



4.0 CONTEXT ANALYSIS





5.0 EXISTING SITE CONDITIONS

SURROUNDING USES & STRUCTURES

IMMEDIATE CONTEXT

As commented on previous pages, the site is surrounded by a wide range of structures and uses, from parks, restaurants, nightlife, as well as a wide range of housing—all using a mix of materials and styles.

- BAR/NIGHTLIFE
- RESTAURANTS/CAFE
- RELIGIOUS INSTITUTIONS
- OFFICE
- MULTI-FAMILY RESIDENTIAL
- BUSINESS
- EDUCATION



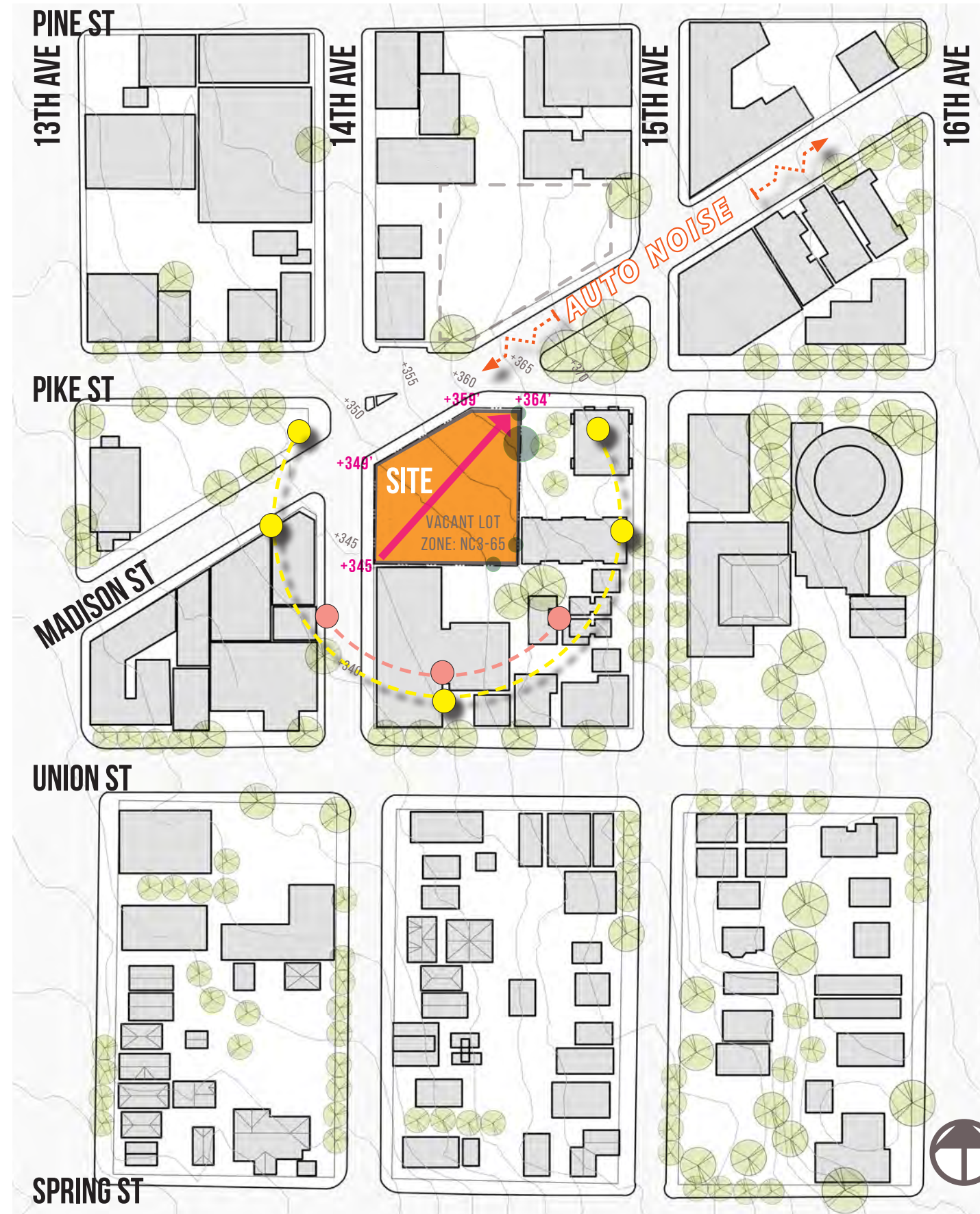
TOPOGRAPHY,
PHYSICAL FEATURES
& LANDSCAPE
ELEMENTS

IMMEDIATE CONTEXT

Located at 1401 E Madison Street, the site area is approximately 26,068 SF and fronts 14TH Avenue, East Madison Street and East Pike Street.

The site slopes uphill from a low point at the southwest corner to a high point at the northeast corner. There is an elevation gain of almost 20 feet between the southwest and northeast corner.

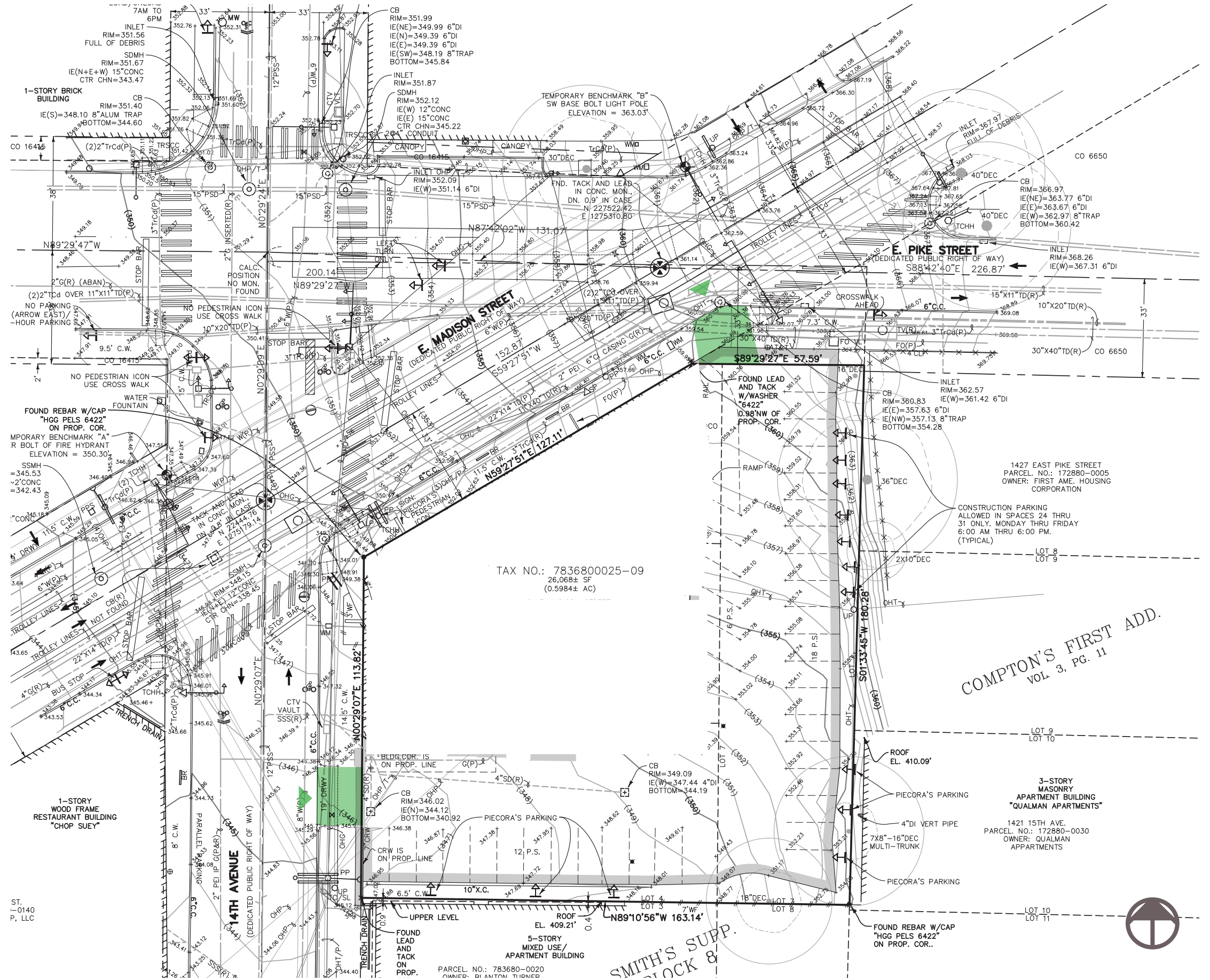
There are no structures on site.



5.0 EXISTING SITE CONDITIONS

The site is currently vacant.

Access to the site currently occurs in two places: along 14TH Avenue and East Madison Street

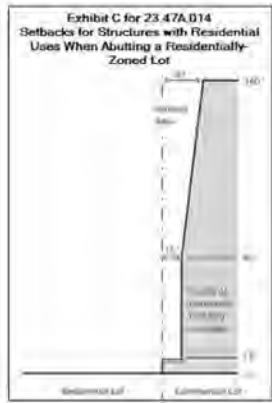


Existing Vehicular Entry

SITE PHOTOS



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LAND USE CODE ANALYSIS		
SITE ADDRESS	1401 E MADISON, SEATTLE, WA 98122	
SITE AREA	26,069 SF / 0.594 ACRES	
SITE ZONING	NC3-65 NEIGHBORHOOD COMMERCIAL	
OVERLAY ZONE	12TH AVENUE URBAN CENTER VILLAGE	
STREET DESIGNATIONS	14TH AVENUE (COLLECTOR ARTERIAL / ARTERIAL STREET), MADISON STREET (PRINCIPAL ARTERIAL / MAJOR TRANSIT STREET)	
ADJACENT PROPERTY ZONING	EAST: LR3, SOUTH: NC3-65	
LAND USE CODE SECTION	DESCRIPTION/REQUIREMENT	COMPLIANCE/REFERENCE
23.47A.004 TABLE A. PERMITTED AND PROHIBITED USES	RESIDENTIAL USES, DRINKING ESTABLISHMENTS, RESTAURANTS, OFFICES, RETAIL PERMITTED OUTRIGHT	COMPLIANT
23.47A.005 STREET LEVEL USES	C. RESIDENT USES AT STREET-LEVEL PERMITTED ANYWHERE.	COMPLIANT
23.47A.008 STREET LEVEL DEVELOPMENT STANDARDS	A2. BLANK FACADES. THE TOTAL OF ALL BLANK FACADE SEGMENTS MAY NOT EXCEED 40% OF THE WIDTH OF THE FACADE	COMPLIANT. SEE SHEET G0.04
	A3. STREET-LEVEL STREET-FACING FACADES MUST BE LOCATED WITHIN 10 FEET OF THE STREET LOT LINE UNLESS PLAZAS OR OTHER OPEN SPACES ARE PROVIDED.	COMPLIANT. SEE SHEET A2.01 & A2.02
	B2. FOR STRUCTURES WITH STREET LEVEL NON-RESIDENTIAL USES, 60% OF FACADE BETWEEN 2' AND 8' ABOVE SIDEWALK SHALL BE TRANSPARENT AND DESIGNED TO ALLOW UNOBSTRUCTED VIEWS FROM OUTSIDE INTO STRUCTURE.	COMPLIANT. SEE SHEET G0.04
	B3. NONRESIDENTIAL USES SHALL EXTEND AN AVERAGE DEPTH OF AT LEAST 30' AND A MINIMUM DEPTH OF 15' FROM STREET-FACING FACADE. MUST HAVE A MINIMUM FLOOR-TO-FLOOR HEIGHT OF 13'.	COMPLIANT. SEE SHEETS A2.01, A3.01, A3.02
23.47A.012 STRUCTURE HEIGHT	MAY NOT EXCEED 85' (MEASURED FROM GRADE PLANE).	COMPLIANT. SEE SHEET G0.03 FOR CALCULATIONS. ALSO SHEETS A3.01-A3.04
	OPEN RAILINGS, PLANTERS, PARAPETS MAY EXTEND UP TO 4' ABOVE THE HEIGHT LIMIT.	COMPLIANT. SEE SHEETS A3.01-A3.04
	SOLAR COLLECTORS AND MECHANICAL EQUIPMENT MAY EXTEND UP TO 7' ABOVE THE HEIGHT LIMIT.	COMPLIANT. SEE SHEET A2.07
23.47A.013 FLOOR-AREA RATIO	MEASURE AREA BOUNDED BY THE INSIDE SURFACE OF THE EXTERIOR WALL.	COMPLIANT. SEE SHEET G0.03
	TOTAL PERMITTED FOR ALL USES WITHIN A MIXED-USE STRUCTURE: 4.75	COMPLIANT. SEE SHEET G0.03
	TOTAL PERMITTED FOR ANY SINGLE USE WITHIN A MIXED-USE STRUCTURE: 4.25	COMPLIANT. SEE SHEET G0.03
	PROJECT NOT WITHIN STATION AREA OVERLAY DISTRICT.	COMPLIANT. SEE SHEET G0.03
23.47A.014 SETBACK REQUIREMENTS	FIFTEEN FEET FOR PORTIONS OF STRUCTURES ABOVE 13 FEET IN HEIGHT TO A MAXIMUM OF 40 FEET, AND FOR EACH PORTION OF STRUCTURE ABOVE 40 FEET, AN ADDITIONAL SETBACK AT THE RATE OF 2 FEET FOR EVERY 10 FEET BY WHICH THE HEIGHT EXCEEDS 40 FEET.	DEPARTURE REQUESTED. SEE REQUESTS #1 AND #2 THIS SHEET
		
23.47A.014 UTILITY SETBACKS (PROJECT ZONED EAST OF SITE IS LR3)	OVERHEAD POWER LINES RUN PARALLEL ALONG 14TH AND MADISON, RESPECTIVELY. SCL REQUIRES A SETBACK FROM THE PRIMARY ELECTRICAL DISTRIBUTION ZONE FOR MAINTENANCE PURPOSES.	COMPLIANT. SEE SHEET A1.01

NOTE: refer to MUP drawing set for compliance diagrams and calculations

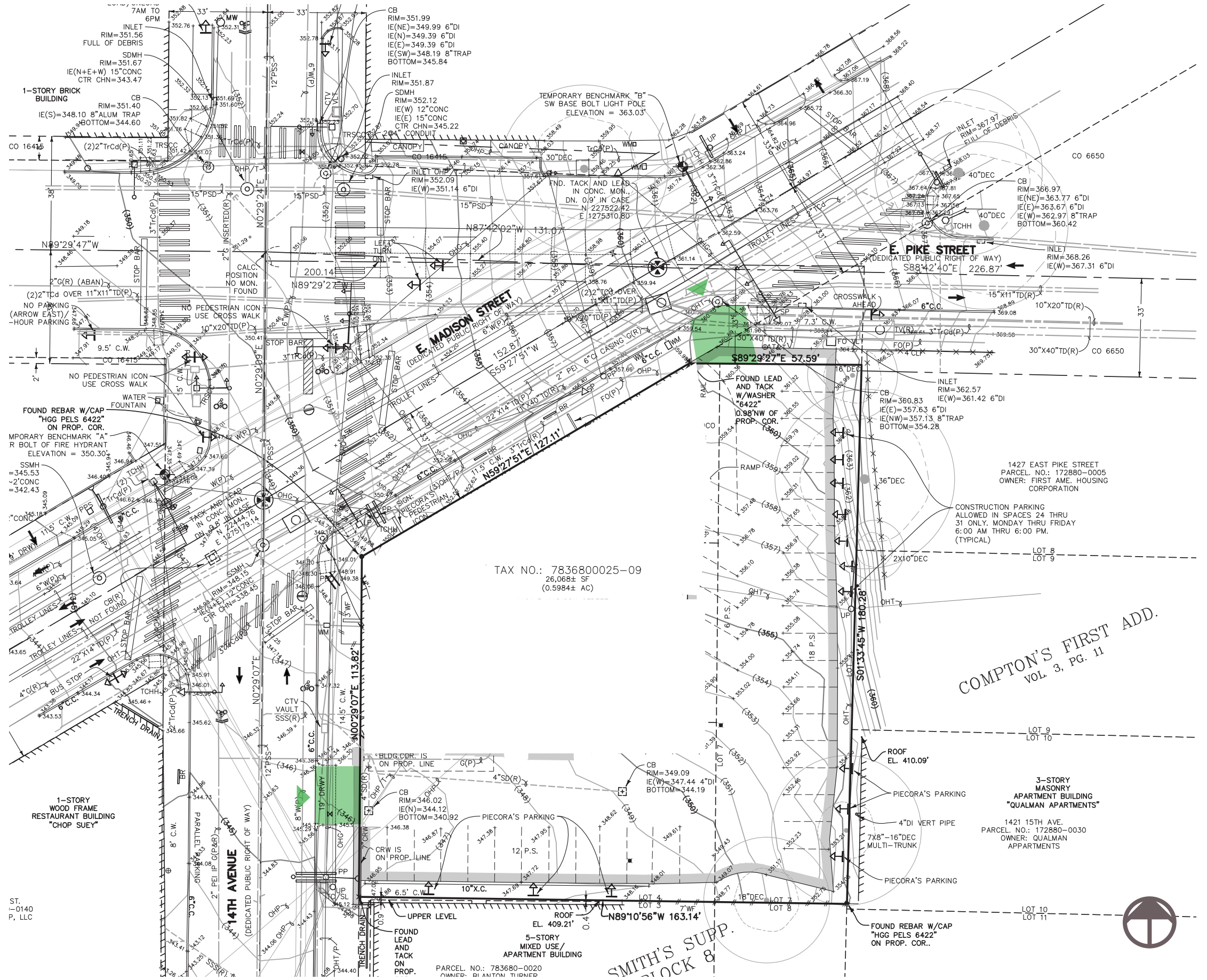
23.47A.016 LANDSCAPING REQUIREMENTS	REQUIRES GREEN AREA FACTOR SCORE OF .30 OR GREATER.	COMPLIANT. SEE SHEET L2.00
	STREET TREES ARE REQUIRED	COMPLIANT. SEE SHEET L1.00, L1.01, DR L1.02
23.47A.020 ODOR STANDARDS	THE VENTING OF ODORS, VAPORS, SMOKE, CINDERS, DUST, GAS AND FUMES SHALL BE AT LEAST 10' ABOVE FINISHED SIDEWALK GRADE, AND DIRECTED AWAY TO THE EXTENT POSSIBLE FROM RESIDENTIAL USES WITHIN 50' OF THE VENT.	COMPLIANT. SEE SHEET A2.07
23.47A.024 RESIDENTIAL AMENITY AREAS	REQUIRED IN AN AMOUNT EQUAL TO 5 PERCENT OF THE STRUCTURE'S GROSS FLOOR AREA IN RESIDENTIAL USE.	COMPLIANT. SEE SHEET G0.04
	AMENITY AREA MAY NOT BE ENCLOSED.	COMPLIANT. SEE SHEET G0.04
	COMMON RECREATION AREA MUST HAVE A MINIMUM HORIZONTAL DIMENSION OF AT LEAST OF 10' AND CANNOT BE LESS THAN 250 SF	COMPLIANT. SEE SHEET G0.04
	PRIVATE BALCONIES AND DECKS MUST HAVE MINIMUM AREA OF 60 SF AND NO HORIZONTAL DIMENSION CAN BE LESS THAN 6 FEET.	COMPLIANT. SEE SHEET G0.04
23.47A.030 PARKING SPACE STANDARDS	PARKING IS REQUIRED FOR RESIDENTIAL USES IN COMMERCIAL ZONES BY TABLE B.	COMPLIANT
	PARKING IS REQUIRED FOR NONRESIDENTIAL USES IN COMMERCIAL ZONES BY TABLE A.	COMPLIANT
23.47A.030 BICYCLE PARKING	NO PARKING FOR MOTOR VEHICLES IS REQUIRED FOR RESIDENTIAL OR NON-RESIDENTIAL USES (OTHER THAN INSTITUTIONS) IN COMMERCIAL AND MULTI-FAMILY ZONES WITHIN URBAN CENTERS (23.54.015 TABLE B.M.)	NOT REQ'D BUT PROVIDED. SEE COMPLIANCE CALCULATIONS THIS SHEET
	BICYCLE PARKING IS REQUIRED FOR SPECIFIED USES BY TABLE D.	COMPLIANT. SEE THIS SHEET FOR CALCULATIONS, AND SHEET A2.P2
23.47A.030 VEHICLE STALL REQUIREMENTS	SALES AND SERVICE, GENERAL - 1 PER 12,000 SF LONG TERM, 1 PER 2,000 SF SHORT TERM (IN URBAN CENTER)	COMPLIANT. SEE THIS SHEET FOR CALCULATIONS, AND SHEET A1.01
	RESIDENTIAL - 1 PER 4 UNITS.	COMPLIANT. THIS SHEET FOR CALCULATIONS, AND SHEET A2.P2
	ONCE THE FIRST 50 BICYCLE SPACES ARE PROVIDED, ADDITIONAL SPACES ARE REQUIRED AT .5 THE RATIO SHOWN IN TABLE D.	COMPLIANT. SEE THIS SHEET FOR CALCULATIONS, AND SHEET A2.P2
	RESIDENTIAL: 60% OF STALLS TO BE MEDIUM STALLS (8'X16')	COMPLIANT. SEE THIS SHEET FOR CALCULATIONS, AND SHEET A2.P1 & A2.P2
23.47A.030 LOADING BERTH REQUIREMENTS	NON-RESIDENTIAL: WHEN 20 OR MORE PARKING SPACES ARE PROVIDED A MIN. OF 35% OF STALLS TO BE SMALL STALLS (7.5'X15') AND 35% OF STALLS TO BE LARGE STALLS (8.5'X19')	COMPLIANT. SEE THIS SHEET FOR CALCULATIONS, AND SHEET A2.P1
	ACCESSIBLE STALLS: (1) 19' ACCESSIBLE STALL REQUIRED	COMPLIANT. SEE SHEET A2.P1 & A2.P2
	DRIVEWAY WIDTH FOR NON-RESIDENTIAL: 22' MIN - 25' MAX WIDTH	COMPLIANT. SEE SHEET A2.P1
	DRIVEWAY WIDTH FOR RESIDENTIAL: 20' MIN	COMPLIANT. SEE SHEET A2.P1 & A2.P2
23.47A.030 LOADING BERTH REQUIREMENTS	ACCESS TO PARKING SHALL BE FROM THE ALLEY IF THE LOT ABUTS AN ALLEY IMPROVED TO THE STANDARDS OF SECTION 23.53.030	N/A
	IF LOT ABUTS 2 OR MORE STREETS, ACCESS IS PERMITTED ACROSS ONE OF THE SIDE STREET LOT LINES PER DIRECTOR (23.47A.032 A.2.B).	COMPLIANT. SEE SHEET A1.01
23.54.040 SOLID WASTE AND RECYCLABLE MATERIALS STORAGE SPACE	RESIDENTIAL DEVELOPMENT MORE THAN 100 DWELLING UNITS - 575 SQUARE FEET PLUS 4 SQUARE FEET FOR EACH ADDITIONAL UNIT ABOVE 100	COMPLIANT. SEE THIS SHEET FOR CALCULATIONS, AND SHEET A2.0P1
	MIXED USE DEVELOPMENT THAT CONTAINS BOTH RESIDENTIAL AND NON-RESIDENTIAL USES SHALL MEET THE STORAGE SPACE REQUIREMENTS SHOWN IN TABLE A FOR 23.54.040 FOR RESIDENTIAL DEVELOPMENT, PLUS 50 PERCENT OF THE REQUIREMENT FOR NONRESIDENTIAL DEVELOPMENT - 5,001-15,000 SQUARE FEET NON-RESIDENTIAL AREA REQUIRES 125 SQUARE FEET STORAGE SPACE.	COMPLIANT
	IN MIXED-USE DEVELOPMENTS, STORAGE SPACE FOR GARBAGE MAY BE SHARED BETWEEN RESIDENTIAL AND NON-RESIDENTIAL USES, BUT SEPARATE SPACES FOR RECYCLING SHALL BE PROVIDED.	COMPLIANT. SEE SHEET A2.0P1
	FOR RESIDENTIAL DEVELOPMENT OVER 100 DWELLING UNITS, THE REQUIRED MINIMUM AREA FOR STORAGE SPACE MAY BE REDUCED BY 15% IF THE STORAGE AREA PROVIDED HAS A MINIMUM HORIZONTAL DIMENSION OF 20'.	COMPLIANT. SEE THIS SHEET FOR CALCULATIONS, AND SHEET A2.0P1
23.54.040 SOLID WASTE AND RECYCLABLE MATERIALS STORAGE SPACE	COLLECTION LOCATION SHALL NOT BE WITHIN A BUS STOP OR WITHIN THE STREET RIGHT-OF-WAY AREA ABUTTING A VEHICULAR LANE DESIGNATED AS A SOLE TRAVEL LANE FOR A BUS.	COMPLIANT. SEE SHEETS A1.01 & A2.P1

7.0 SITE PLAN

EXISTING SITE CONDITIONS

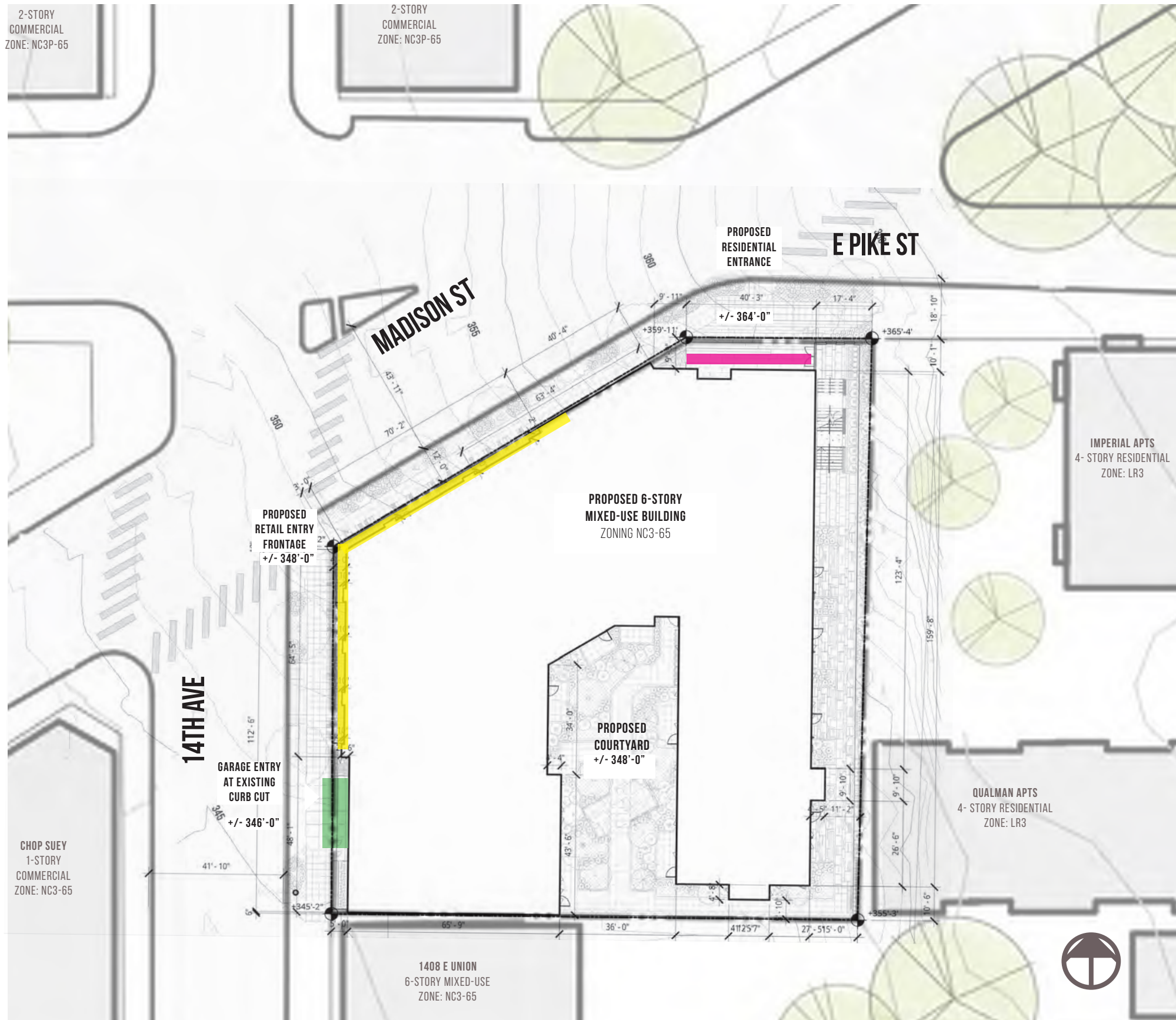
The site is currently vacant.

Access to the site currently occurs in two places: along 14TH Avenue and East Madison Street



Existing Vehicular Entry

SITE PLAN



To allow for the strongest, most viable retail, we propose a highly transparent commercial street front along 14th and Madison. The proposed retail entry/entries will mainly be along 14th or at the building corner due to the grade change along Madison. The proposed residential entry is along E Pike, allowing for some visual and physical separation from vehicle-oriented Madison. The residential entry will be setback from the property line to help with the slight grade change (+/- 4') along E. Pike. As the grade steps up along Madison (+/- 10'), a landscape buffer is proposed to help with the transition from retail to residential uses.

A break in the Madison street frontage separates retail from residential, with the proposed residential entry off of E Pike Street. Conceptually, the E Pike Street frontage signifies the start of the private "Mews", and is the beginning of a pedestrian experience that is more residential in character. Also, in keeping with the established Madison Avenue building precedents, the primary residential entry is located off the busy street.

The proposed vehicular entry is along 14th Avenue. Due to the limited street frontage and pedestrian curb ramps/crossing along E Pike there is no room for vehicular entry at this location. Moreover, a vehicular entry along the heavily trafficked Madison Street frontage would negatively affect the streetscape and potential pedestrian safety. The most logical location is at the southeast corner of the site.

- Residential Entry
- Vehicular Entry at Existing Curb Cut
- Retail Entry

8.0 ITEMIZED RESPONSE TO EDG

1. MASSING, HEIGHT, BULK & SCALE

a. *Discussing the concept of the two scales, the ‘urban edge’ and the ‘private mews’ the Board recommended the applicant explore ways to differentiate these expressions beyond fenestration patterns and material detailing. Develop the design to provide a dramatic differentiation between the two forms.*

CS2:A2 Architectural Presence

CS2:C1 Corner Sites

DC2: B1 Façade Composition

DC2:C1 Visual Depth and Interest

b. *The Board was concerned with the eastern massing transition to the streetscape. Noting that the softer mews space appears thoughtfully considered to meet the needs of the resident, the Board directed the applicant to also create a strong visual connection to the streetscape for pedestrians. Refine the transition to the neighborhood and focus on articulation consistent with the overall architectural concept.*

CS2:A2 Architectural Presence

CS2:B2 Connection to the Street

PL2: B3 Street-Level Transparency

DC2:C1 Visual Depth and Interest

RESPONSE

The building massing strengthens the existing urban fabric by reducing its mass and scale as it transitions to the east. Occupying the 14th & Madison facade, the “urban edge” massing has a strong, uniform expression, characterized by rationality, regularity and the interplay of shadow and light. As the building form transitions to the low-rise residential neighborhood to the east, the massing breaks down in scale to nearly **half**. The “private mews” massing is characterized by playfulness, patterning, variegated shades of color, and a depth to its facade.

The urban edge facade is set up as a **rationalist, regular, expressed structural grid**, first composed of board-formed concrete pilasters at the ground-floor retail level. The retail corner at 14th & Madison is highly transparent, to accentuate the corner and provide a large area of glazing at the street. Directly above, at the residential levels,

long, linear lines of white, integrated-color cement board in which a grid of large, black vinyl windows are placed, further reinforces the rationalist expression of the structure on the facade. The windows themselves are typically composed of six lites: with two of the lites containing operable casements. Where the window width varies, the **proportions** of the casement and/or fixed lites **remain the same**. The windows are arranged in alternating pattern of casement/fixed, fixed/casement, casement/fixed, fixed/casement, and so on, across the urban edge facade. The addition of black metal aluminum accent **solar shade fins** expresses the grid even further and also reduces heat gain and glare for residents. These black metal fins extend past the facade a minimum of 4 inches, adding additional depth and shadow lines to the urban edge facade.

In contrast, the private mews is differentiated by several design elements: most notably **color, pattern, form and facade composition**. In contrast to the long, linear white horizontality of the urban edge facade, the private mews utilizes large expanses of dark-colored integrated-color cement board panels. **Staggered punched window openings** of varying sizes punctuate the private mews massing.

To create a strong visual connection to the streetscape and McGilvra Place Park, we removed three residential units directly above the lobby to create a dramatic, **3-story transparent volume**. This volume is clearly bounded by a horizontal and vertical “container” frame, which is a material continuation of the north and south elevations. Adding visual depth and interest to the facade, this frame acts as a **contextual datum** for pedestrians traveling along E Pike Street, further breaking down the scale and volume of the private mews for passersby. Both the transparent volume and container frame create a strong, human-scaled, visual connection to the surrounding streetscape and serve to further differentiate the private mews from the urban edge expression.

c. *Acknowledging the close proximity of the adjacent Qualman Apartments, the Board directed the applicant to provide a window mapping/privacy study. When developing the design, consider the privacy of the neighboring structure.*

CS2:D5 Respect for Adjacent Sites

DC2: B1 Façade Composition

RESPONSE

Site photos and as built measurements were used to approximate the location of the Qualman Apartment windows. Windows on the east facade are positioned to minimize the visual adjacencies and sightlines to the Qualman Apartments. We have positioned the egress stair (stair encroachment requires departure) directly across from a major grouping of the Qualman windows. We have also located other windows within our project to best maintain the privacy of both buildings. See Section 18.0.

2. CONNECTION TO PARK

The Board encouraged developing the design concept of the private mews to play off the relationship to the nearby McGilvra Place Park. Explore how the massing, and circulation, can visually interact with the nearby park. Develop the design to enhance and contribute to the character of both sites.

CS2:B3 Character of Open Space

DC3: B3 Connections to Other Open Space

RESPONSE

As mentioned in the response to 1a/b, to create a strong visual connection to the streetscape and McGilvra Place Park, we removed three residential units directly above the lobby to create a dramatic, **3-story transparent volume**. This volume is clearly bounded by a horizontal and vertical “container” frame, which is a material continuation of the north and south elevations. Adding visual depth and interest to the facade, this frame acts as a **contextual datum** for pedestrians traveling along E Pike Street, further breaking down the scale and volume of the private mews for passersby. Both the transparent atrium volume and container frame create a strong, human-scaled, visual connection to the surrounding streetscape and serve to further differentiate the private mews from the urban edge expression.

The atrium reveals our amenity space, which will be an active component of the project. This triple height volume also allows for additional light to penetrate into the ground floor space, which connects with our residential courtyard.

By setting back the project (close to 20 feet at the NE corner, nearest to the park) at the ground floor levels, we create a generous open space buffer between our project and the park. Additionally, we have located an **art gate component** at the north end of the private mews stair to further activate the streetscape at this location.

3. GROUND FLOOR USES & ENTRIES

a. The Board was concerned with the proposed ground floor connections. Recognizing that E Madison St is transitioning and more developments are being constructed with retail, the Board encouraged the applicant to consider more retail and transparency to engage and interact with the streetscape. Develop this frontage to create a pedestrian oriented street edge.

CS1:C TOPOGRAPHY

CS2:B ADJACENT SITES, STREETS, AND OPEN SPACES

PL3: C RETAIL EDGES

RESPONSE

We have maximized the glazing along E Madison to provide both views into the building and views out to life on the street. Due to the extreme grade change of E Madison street (+11 feet from the corner of 14th and Madison to the corner of Madison and E Pike St), retail entry(ies) are not feasible along this frontage without creating a series of carves/ramps into this portion of the site, which would detract from the pedestrian environment and streetscape. Furthermore, we feel retail entries are best suited along 14TH AVE, continuing the successful pattern of development that is currently occurring in the neighborhood.

As Madison transitions to E Pike ST, we have softened the transition from building edge to sidewalk through the use of integrated planters. These planters also mark the start of the residential zone. We have also located a portion of our amenity space along the North end of the Madison ST edge. We have developed this area to be analagous to the idea of a “coffee shop” bar. With counter height and bar seating, this “perch” adds an additional “see and be seen” element off the lobby space, further activating the Madison streetscape. See Section 18.0.

b. The Board was also concerned with the character and function of the lobby/leasing area and urged the applicant to further develop the design so that this frontage will provide activity and support street interaction. Provide interior renderings of the area and more information on the function.

PL2:B3 Street-Level Transparency

PL3: C RETAIL EDGES

RESPONSE

See above for development of the “coffee shop” perch at the Madison ST edge of the lobby/leasing area. We have also developed the frontage. See Section 18.0

c. The Board supported the elevated residential entry and stressed the importance of accessibility when refining the design. Study the circulation and the lobby entry to explore opportunities to use the existing site topography and create a strong accessible connection to the street.

CS1:C TOPOGRAPHY

CS2:B ADJACENT SITES, STREETS, AND OPEN SPACES

PL2:A1 Access for All

PL3:A1 Design Objectives (ENTRIES)

RESPONSE

Approximately 10-12 feet of the residential entry at the northeast corner of the site meets the sloping sidewalk grade, and is fully accessible from the street.

The Board discussed the vehicular entry location and related departure request and directed the applicant to thoughtfully consider the potential conflict between different travel modes. In developing the design, create clear lines of sight.

PL4:A ENTRY LOCATIONS AND RELATIONSHIPS

DC1:B VEHICULAR ACCESS AND CIRCULATION

RESPONSE

We are requesting a departure from the sight triangle requirement. A parabolic mirror at the garage entrance along 14th is proposed to provide advance warning for vehicles and pedestrians. This departure minimizes the overall effect of the parking garage entry DC1:B (Vehicular Access) by eliminating any jogs at the frontage, allowing for a cleaner retail and garage entry face. This departure also helps meet the intent of CS2 (Architectural Context & Character) and CS3 (Urban Pattern and Form) with the integration of a clean, modern, uniform face for pedestrians to safely walk by, eliminating any visual impact of the driveway. To also help with safety, we have used a different scoring pattern at the driveway that is clearly differentiated from the sidewalk (PL4A (Entry Location & Relationships)).

4. CONNECTIVITY & OPEN SPACES:

a. The Board strongly recommended the applicant create stronger connections between the various open spaces and site circulation. Explore opportunities to connect with, or enhance, the uses and activities of other nearby open space where appropriate. Consider animating the courtyard level with ground level activity and entries. At the next meeting, the Board would like to see more information on the site circulation, detailed sections and perspectives should be provided.

CS2:B3 Character of Open Space

PL3:B1 Security and Privacy

PL3:C3 Ancillary Activities

DC3:A1 Interior/Exterior Fit

DC3:B3 Connections to Other Open Space

DC3:C1 Reinforce Existing Open Space

RESPONSE

We have ground level unit entries at the courtyard and private mews. We have created a dramatic 3-story space that connects all of our amenity spaces, both from the interior and exterior, at the private mews, courtyard and streetscape. We have also set back the ground floor entrance from the sidewalk, providing a larger, more welcoming residential entrance. We have also provided an art screen/gate at the private mews entry and across from McGilvra Place Park, enhancing the experience of both sites.

Similar to the ground floor lobby area, we have also setback the retail frontage to allow for a uniform, gracious street front. This setback provides additional room for pedestrian movement that is protected by the weather. Also see Section 18.0.

b. The Board also discussed the visual presence of open spaces from the street. Develop the design to provide a strong connection to the street and visual access for the pedestrian. Explain the design intent for the treatment of topographical changes.

CS2:B ADJACENT SITES, STREETS, AND OPEN SPACES

PL2:B2 Lighting for Safety

PL3:B1 Security and Privacy

DC3:A1 Interior/Exterior Fit

DC3:B3 Connections to Other Open Space

DC3:C1 Reinforce Existing Open Space

RESPONSE

We have developed the design to provide a strong connection to the street, while maintaining safety for residents.

We have also provide a major bicycle amenity to residents that is accessed off the private mews exterior stair. This stair is equipped with a bicycle runnel to allow for convenient access for bicycles to access the amenity.

See above responses for further elaboration on the visual connection between open spaces, particular McGilvra Place Park.

5. MATERIALS

Recognizing that this building will mark the transition of E Madison St and is a prominent site, the Board encouraged durable, high quality materials. The Board strongly supported the quality of materials and large windows shown in the precedent images in the packet and urged the applicant to consider durability and detailing of the materials.

DC4:A BUILDING MATERIALS

RESPONSE

The majority of the street-facing elevations use high quality, integral color cement composite panel. Metal panel siding is also used where there is a major change in plane. See elevations.

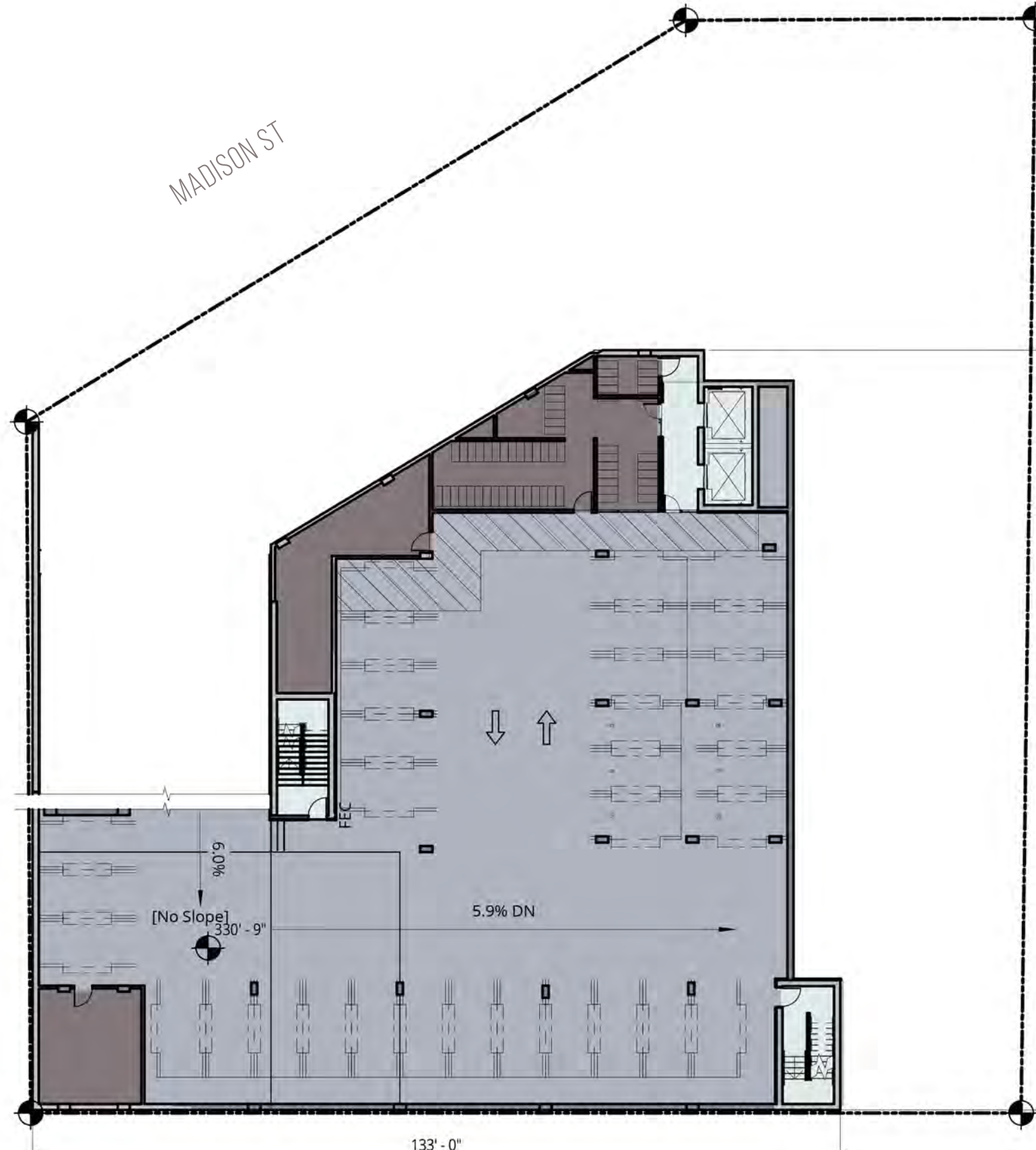
LEVEL P2 PLAN

- Residential
- Leasing/Lobby/Amenity
- Commercial/Retail
- Circulation
- Service/Back of House
- Parking

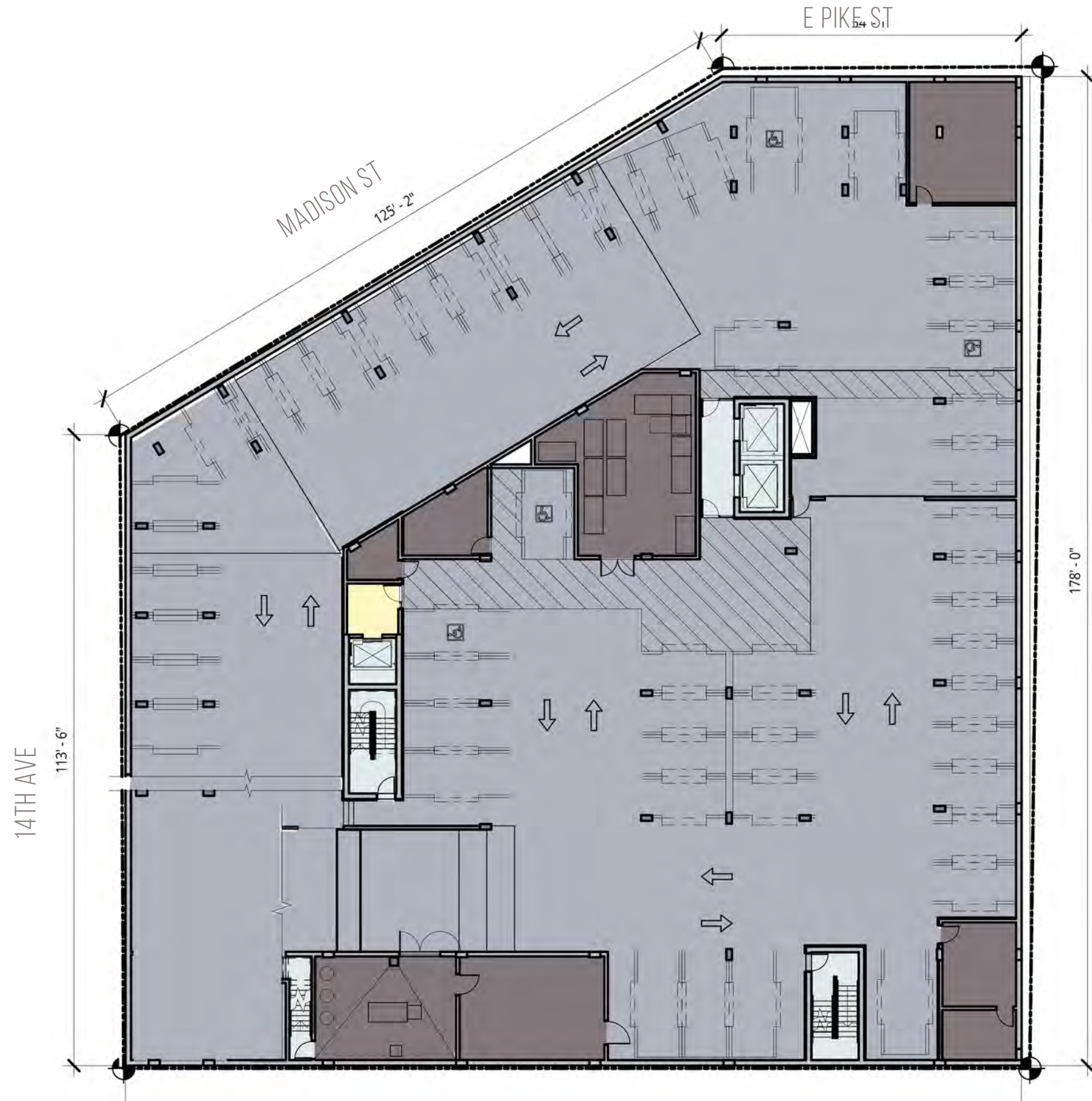
14TH AVE

MADISON ST

E PIKE ST



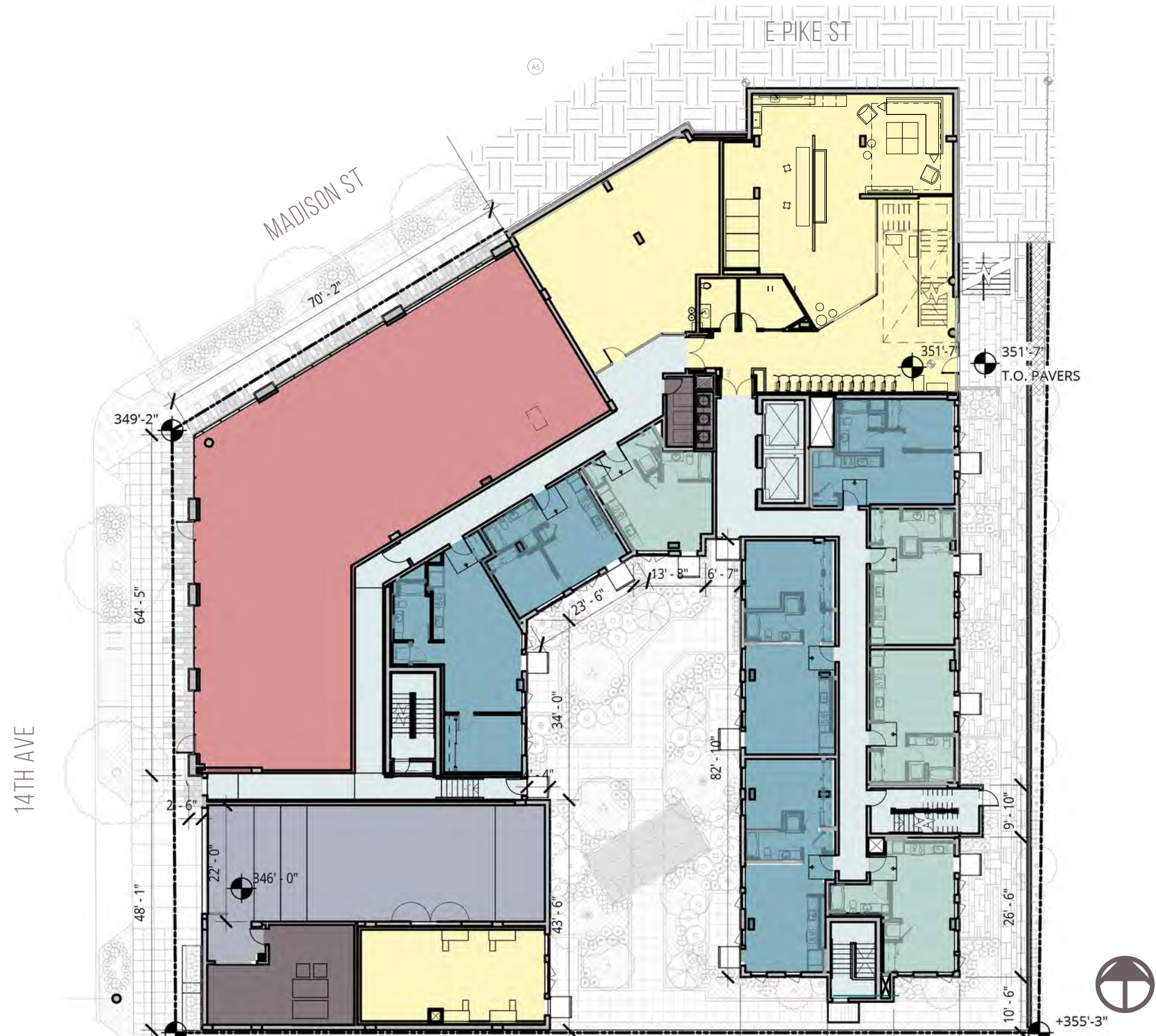
LEVEL P1 PLAN



- Residential
- Leasing/Lobby/Amenity
- Commercial/Retail
- Circulation
- Service/Back of House
- Parking

LEVEL 01 PLAN

- Residential
- Leasing/Lobby/Amenity
- Commercial/Retail
- Circulation
- Service/Back of House
- Parking



LEVEL 02 PLAN



- Residential
- Leasing/Lobby/Amenity
- Commercial/Retail
- Circulation
- Service/Back of House
- Parking



LEVEL 03 PLAN

- Residential
- Leasing/Lobby/Amenity
- Commercial/Retail
- Circulation
- Service/Back of House
- Parking



LEVELS 04-06 PLAN



- Residential
- Leasing/Lobby/Amenity
- Commercial/Retail
- Circulation
- Service/Back of House
- Parking



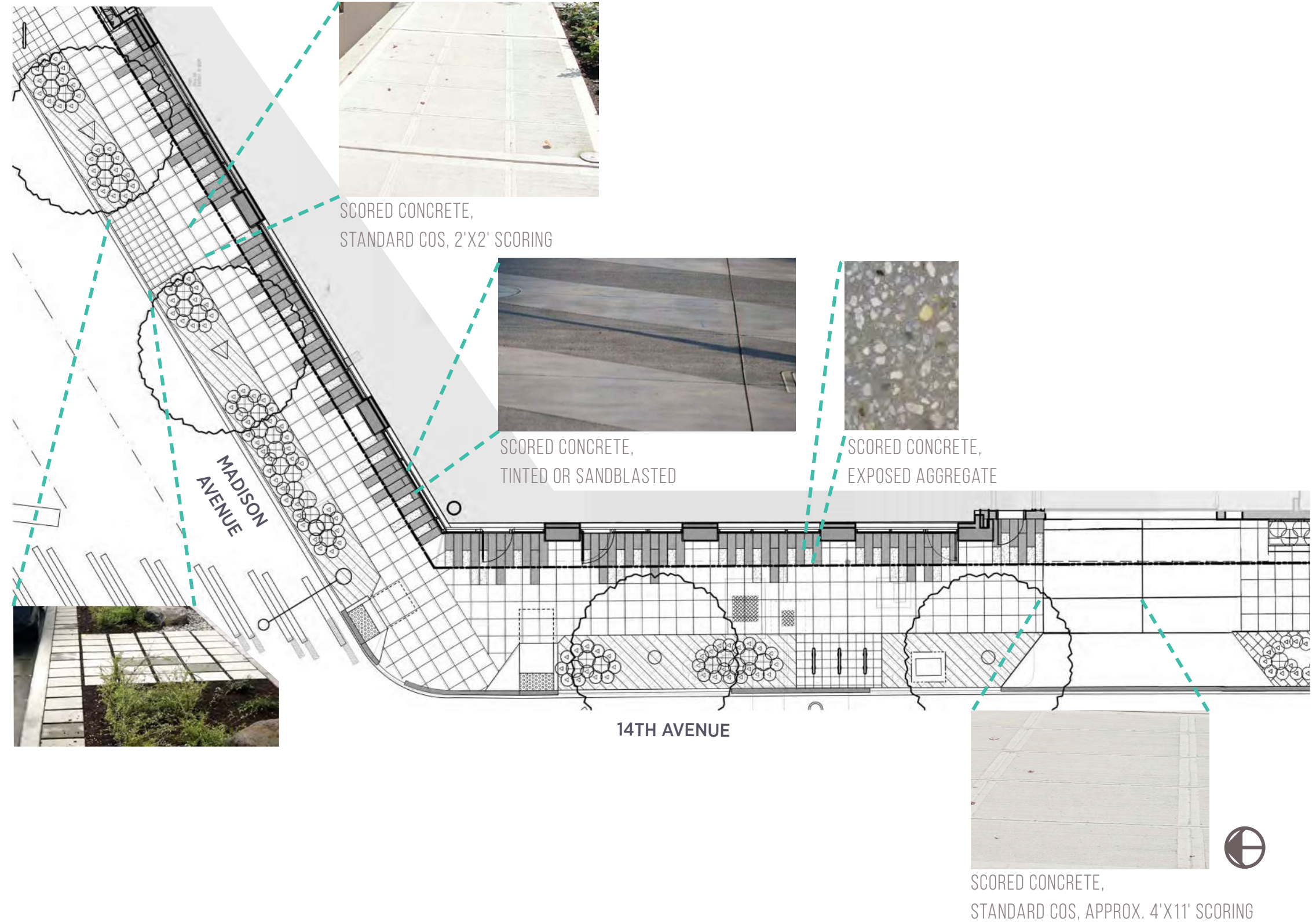
GROUND PLANE
ELEMENTS:
E. PIKE ST

GROUNDPLANE ELEMENTS

A human scaled approach was taken to the groundplane element design. A mix of scoring patterns and textures are implemented for visual interest (retail) and safety (driveway).

GROUND FLOOR TREATMENT

A. PAVING: 14TH & MADISON



GROUND FLOOR TREATMENT

A. PAVING: PRIVATE MEWS (EAST PIKE STREET)



Maidenhair Fern /
Adiantum aleuticum
along edge



Deer Fern /
Blechnum spicant
massed in middle



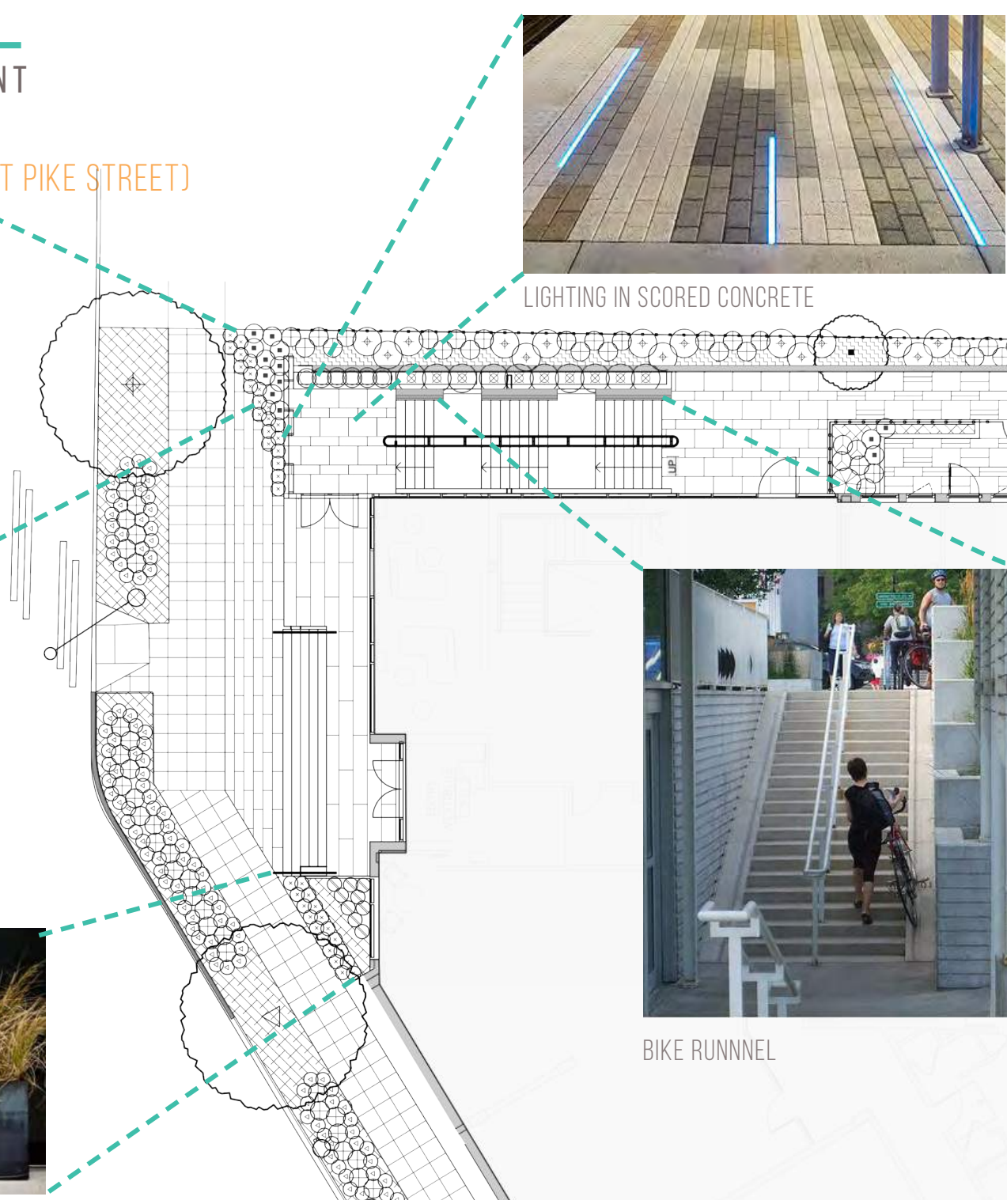
BLACKENED STEEL PLANTER



LIGHTING IN SCORED CONCRETE



BIKE RUNNEL



GROUND PLANE
ELEMENTS:
14TH & MADISON

10.0 COMPOSITE LANDSCAPE / HARDSCAPE PLAN

GROUND FLOOR

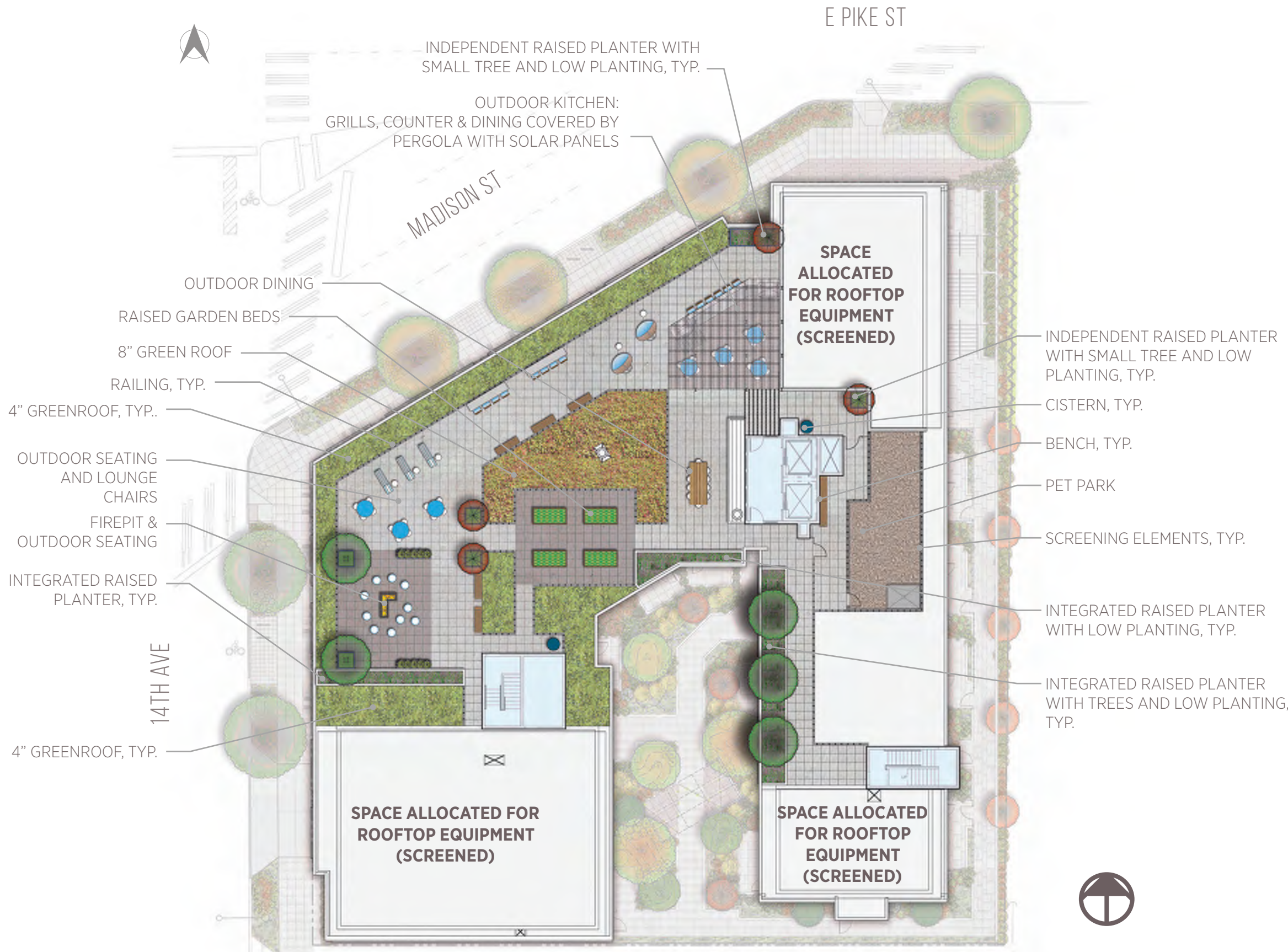
GROUNDPLANE

A generous building setback along 14th Avenue offers opportunities for landscape design that responds to pedestrian wayfinding mechanisms as well as the experience to the user. The streetscape along 14th Avenue will accommodate a planting strip that compliments the planting to the recently completed development to the south. The 6.5'-wide (min.) planter strip will have new street trees, low shrubs and groundcover. The selected plant palette will take into account the transparency for the proposed retail along 14th Ave., as it transitions to the E. Madison intersection.

The potential for accent paving may also reinforce the pedestrian experience while also act as a wayfinding mechanism, once the interior program is defined. A small planting area between the property line and building façade will help provide a soft, strategic buffer along the sidewalk and interior private-programming, when needed. As the pedestrian experience transitions onto E. Madison, a 5.5'-wide planter strip will accommodate new street trees, low shrubs and groundcover. The planting will reflect the transitioning from the retail component to the residential entry/lobby space, as well as the private Mews along the east property-line, at E. Pike. To navigate the grade down to the private mews, the design team will integrate tiered planters, a staircase with a bike runnel and adequate lighting to provide a safe and energetic space. Private terraces will be defined by the use of special accent paving and privately reinforce with the use of buffered planting and potential fence/gate elements.



ROOF PLAN



COURTYARD

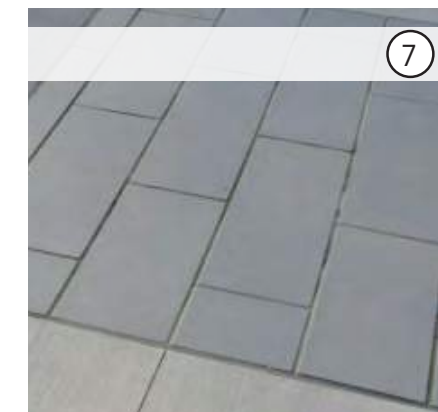
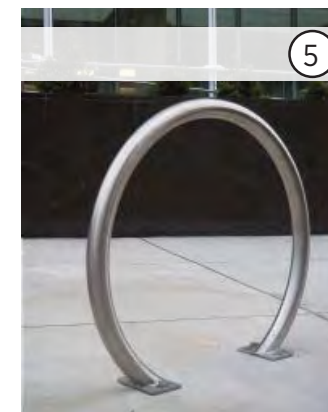
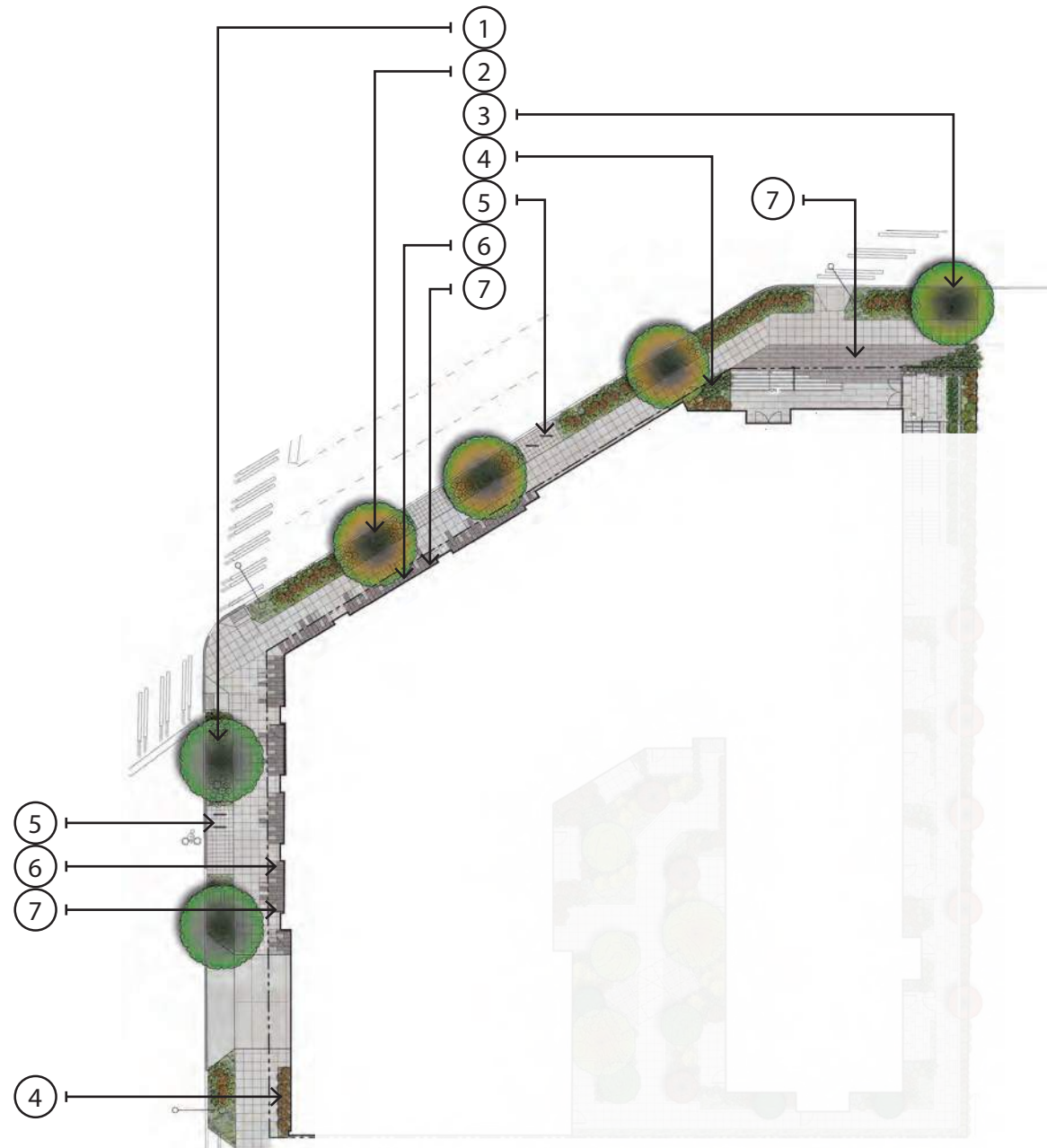
The private interior courtyard will be surrounded by private terraces that will be buffered by planting. The vegetated oasis will be reinforced by incorporating low berming to provide another organic element while adding to the verticality and variety of the planting scheme. The planting will consists of a plant palette that plays with the senses with various textures, colors, fragrances and scales. The focal point of the courtyard will be an open-cabana structure that will have seating and ambient lighting. This semi-open air structure will provide the user a special place to reflect, socialize and experience the meandering landscaped oasis, while also providing an ambient beacon for those tenants that surround the courtyard.

ROOFDECK

The roof deck terrace will include such program elements as the following: areas allocated for pet parks, semi-open trellis structures that help define activated areas for outdoor dining/socializing, outdoor kitchen/BBQ, small and large group seating, and areas for active p-patch/food cultivation, green roof components as well as raised planters. The pedestrian wayfinding will be reinforced by different paver colors and textures as well as the use of decking to signify special spaces and sense of arrival.

There will also be areas for rooftop equipment at the south end of the site.

14TH & MADISON
URBAN EDGE



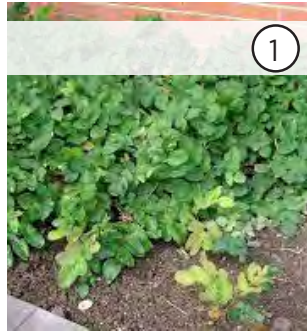
PRIVATE MEWS

Fireglow
Japanese Maple

PLANTING AT PROPERTY LINE



Kelsey & Yellowtwig
Dogwoods



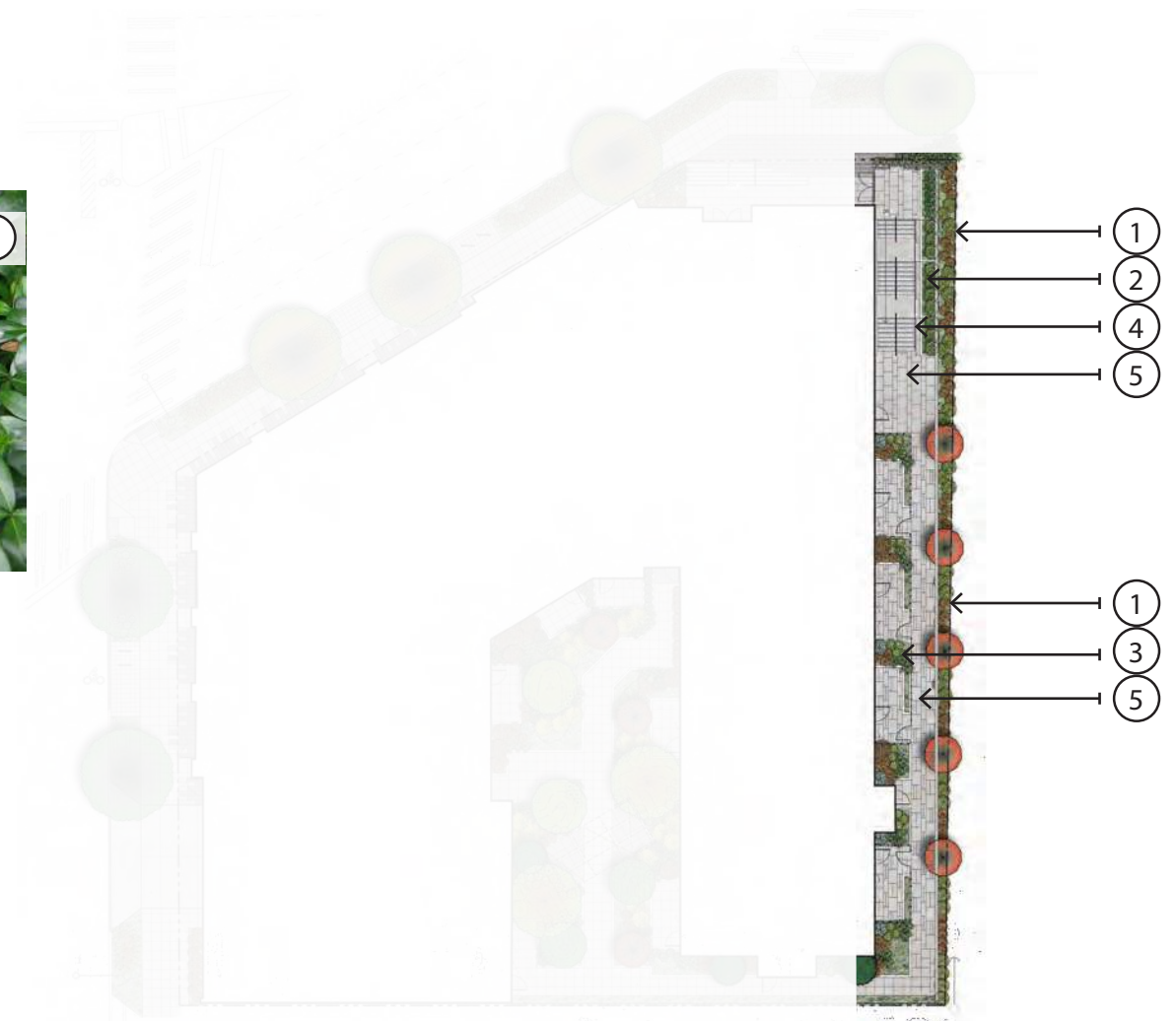
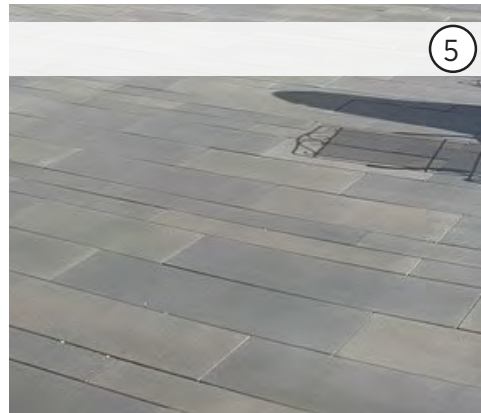
PLANTING IN STEPPED PLANTERS AT STAIRS



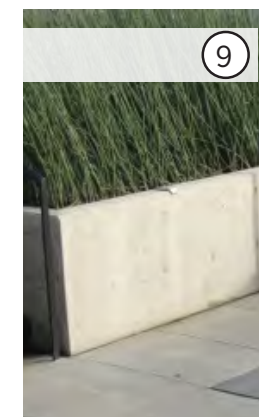
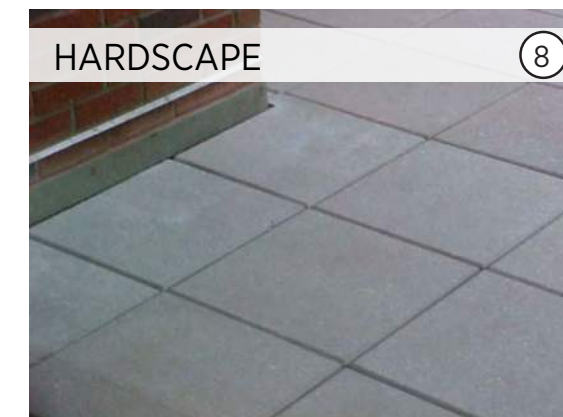
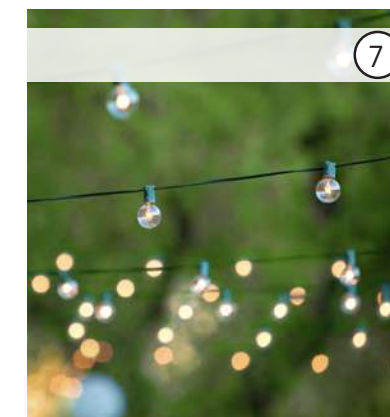
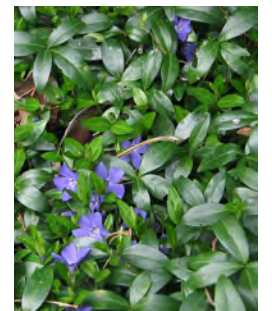
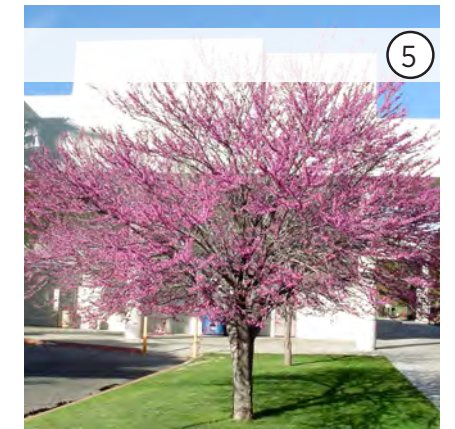
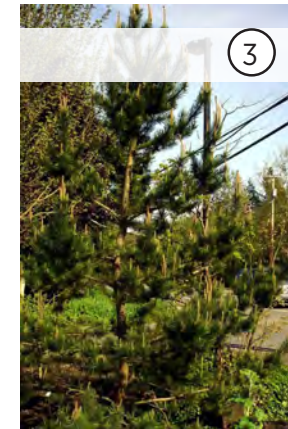
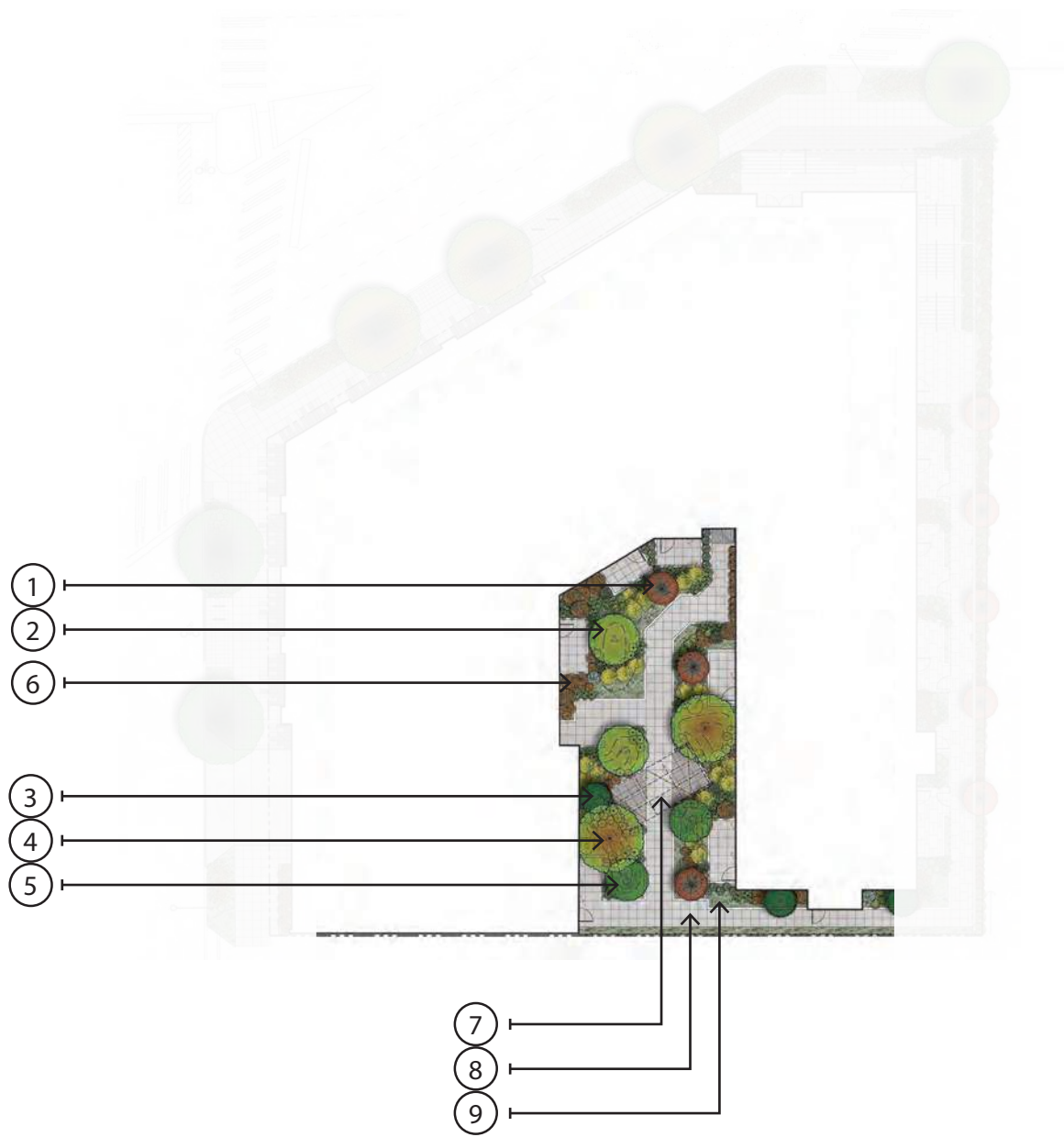
PLANTING IN RAISED PLANTERS AT PRIVATE PATIOS



HARDSCAPE



COURTYARD



PLANTING ①



②



③



④



⑤



⑥



ROOF DECK

SITE ELEMENTS ⑦



⑧



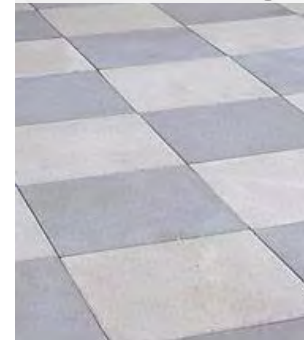
⑨



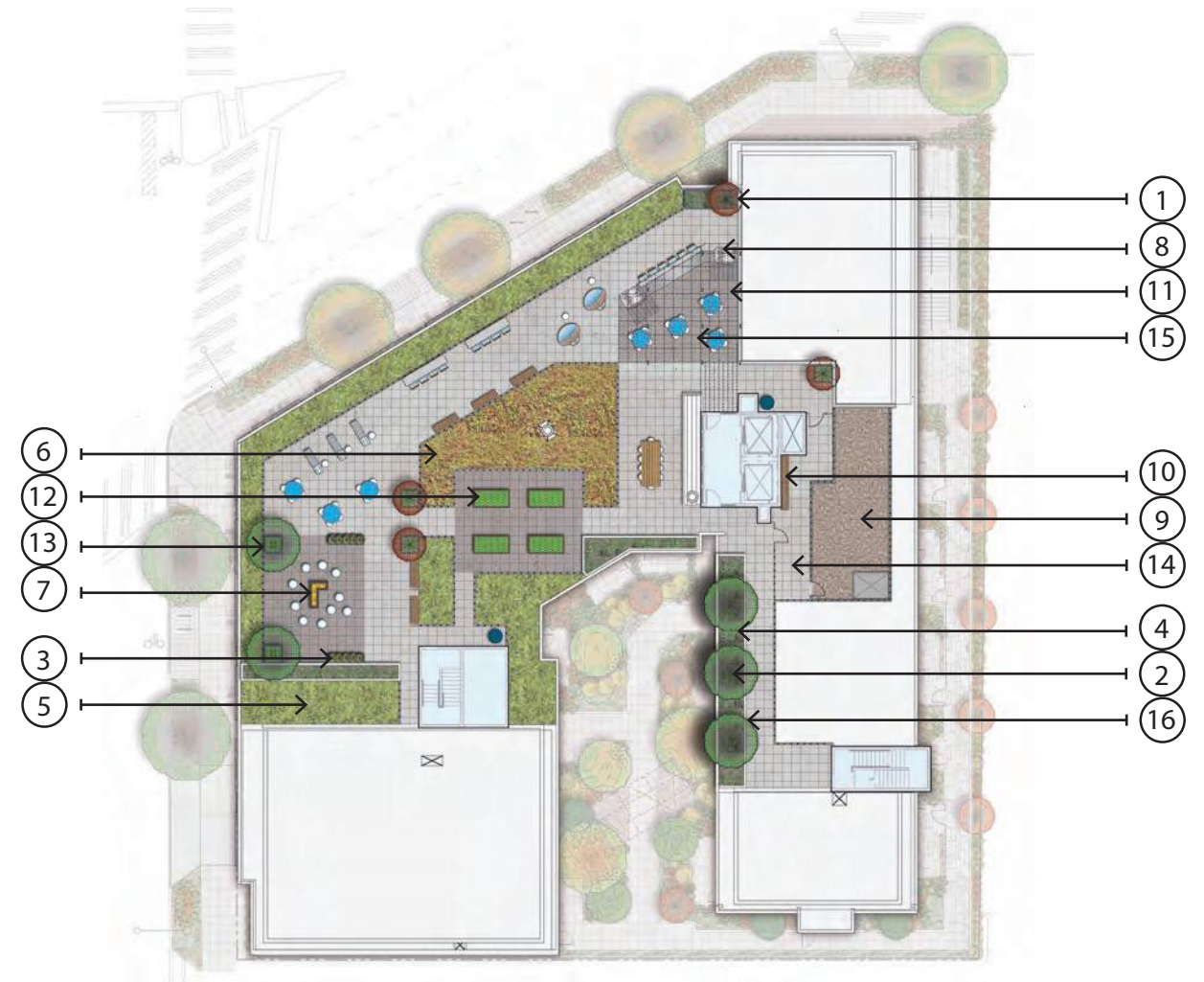
HARDSCAPE ⑭



⑮



⑯



11.0 ELEVATIONS



NORTH ELEVATION



MADISON STREET ELEVATION

11.0 ELEVATIONS



WEST ELEVATION



SOUTH ELEVATION

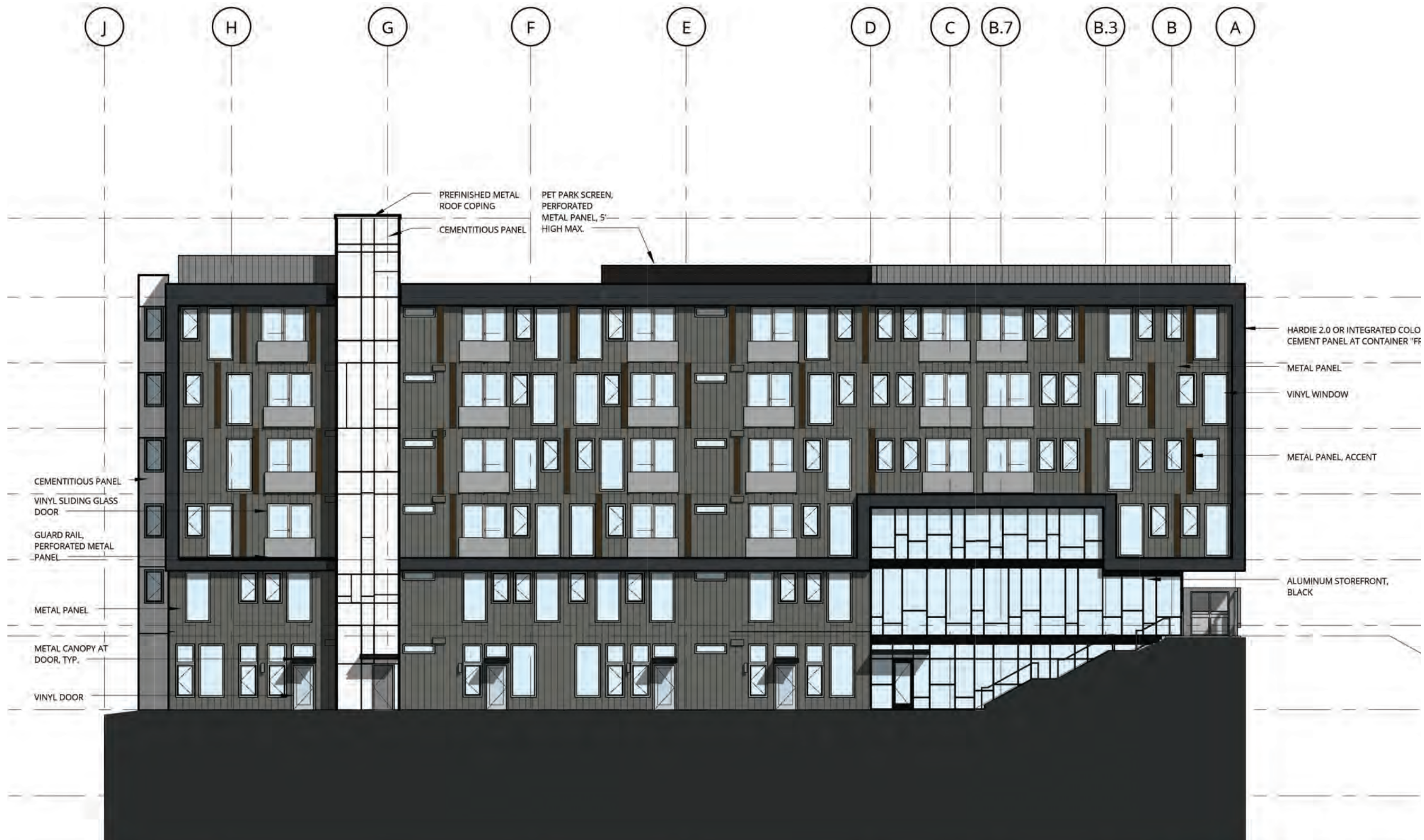
11.0 ELEVATIONS



COURTYARD WEST ELEVATION

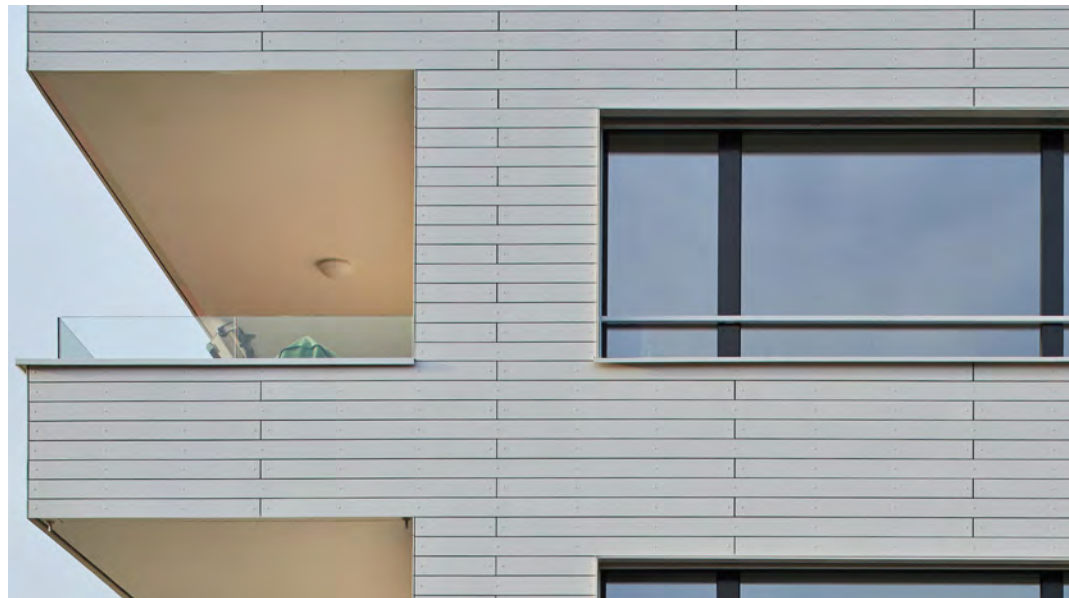


COURTYARD EAST ELEVATION

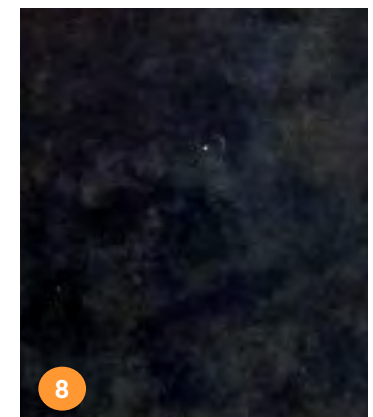
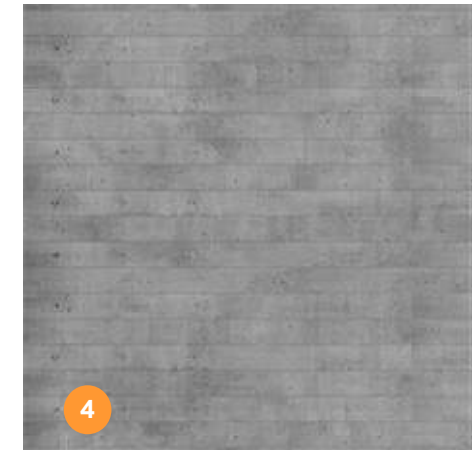
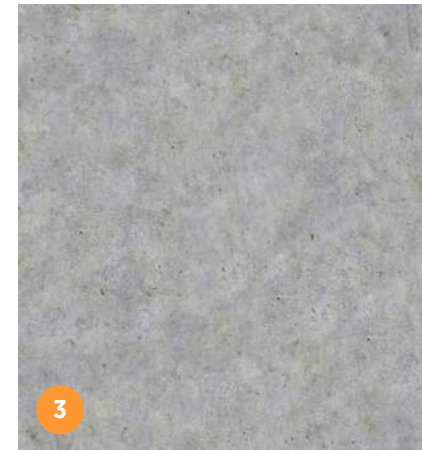
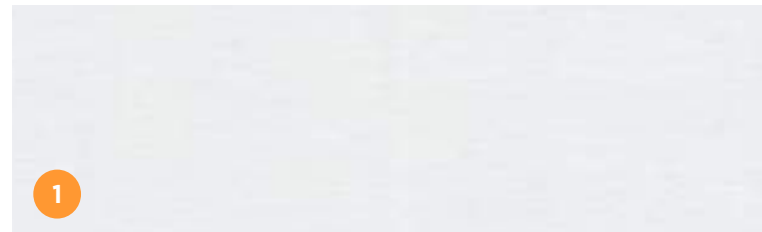


EAST ELEVATION

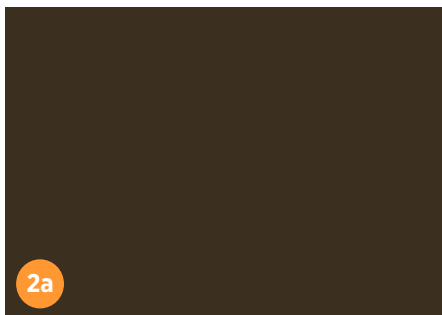
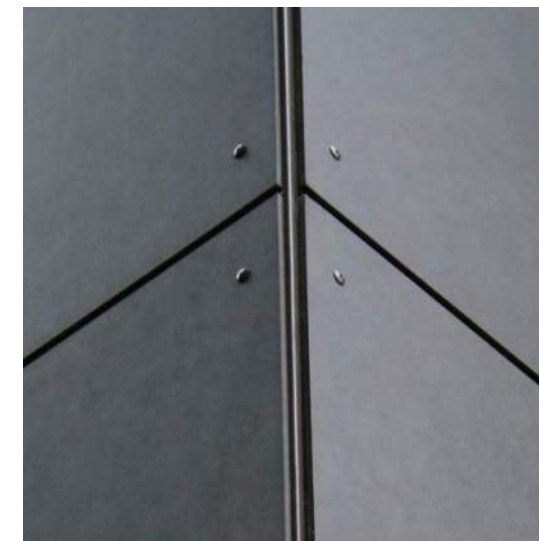
14TH & MADISON
URBAN EDGE



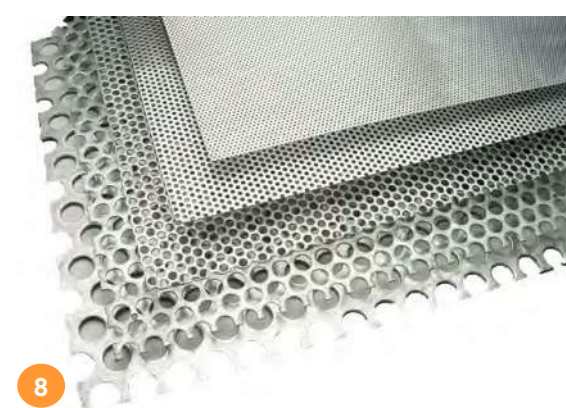
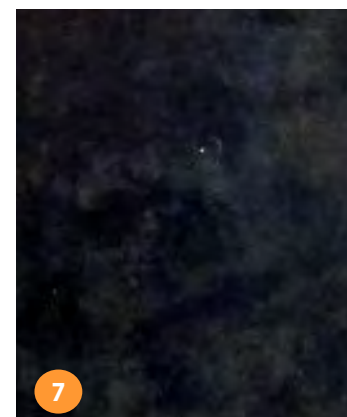
1. Integrated Color Cement Composite Panel Siding (14th & Madison):
White Onyx
2. Metal Siding, 12" Panel:
Cool Matte Black
3. Concrete, Architectural Board Formed
4. Concrete, Board Formed (alternate)
5. Prefinished Metal Trim / Painted Steel, Black
6. High Performance Vinyl Windows, Black
7. Storefront, Anodized Black Aluminum
8. Steel, blackened



PRIVATE MEWS

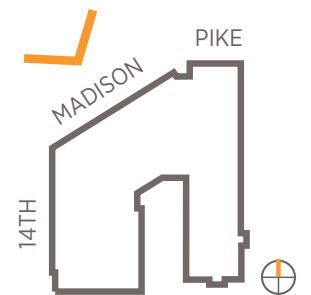


1. Integrated Color Cement Composite Panel Siding (private mews north elevation):
Black Opal
- 2a. Metal Siding, 12" Panel:
Cool Weathered Copper
- 2b. Metal Siding, 12" Panel:
Cool Zinc Gray
3. Fiber Cement Siding:
Hardie 2.0, Painted White (to match white onyx)
Hardie 2.0, Painted White (to match black opal)
4. High Performance Vinyl Windows, Black
5. High Performance Vinyl Windows, White
6. Storefront, Anodized Black Aluminum
7. Steel, blackened
8. Perforated Metal (Juliets, screens/art screen)



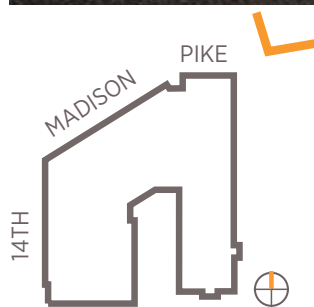


VIEW TOWARDS SOUTHEAST



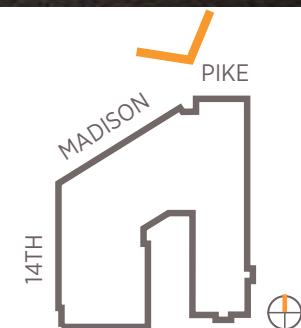


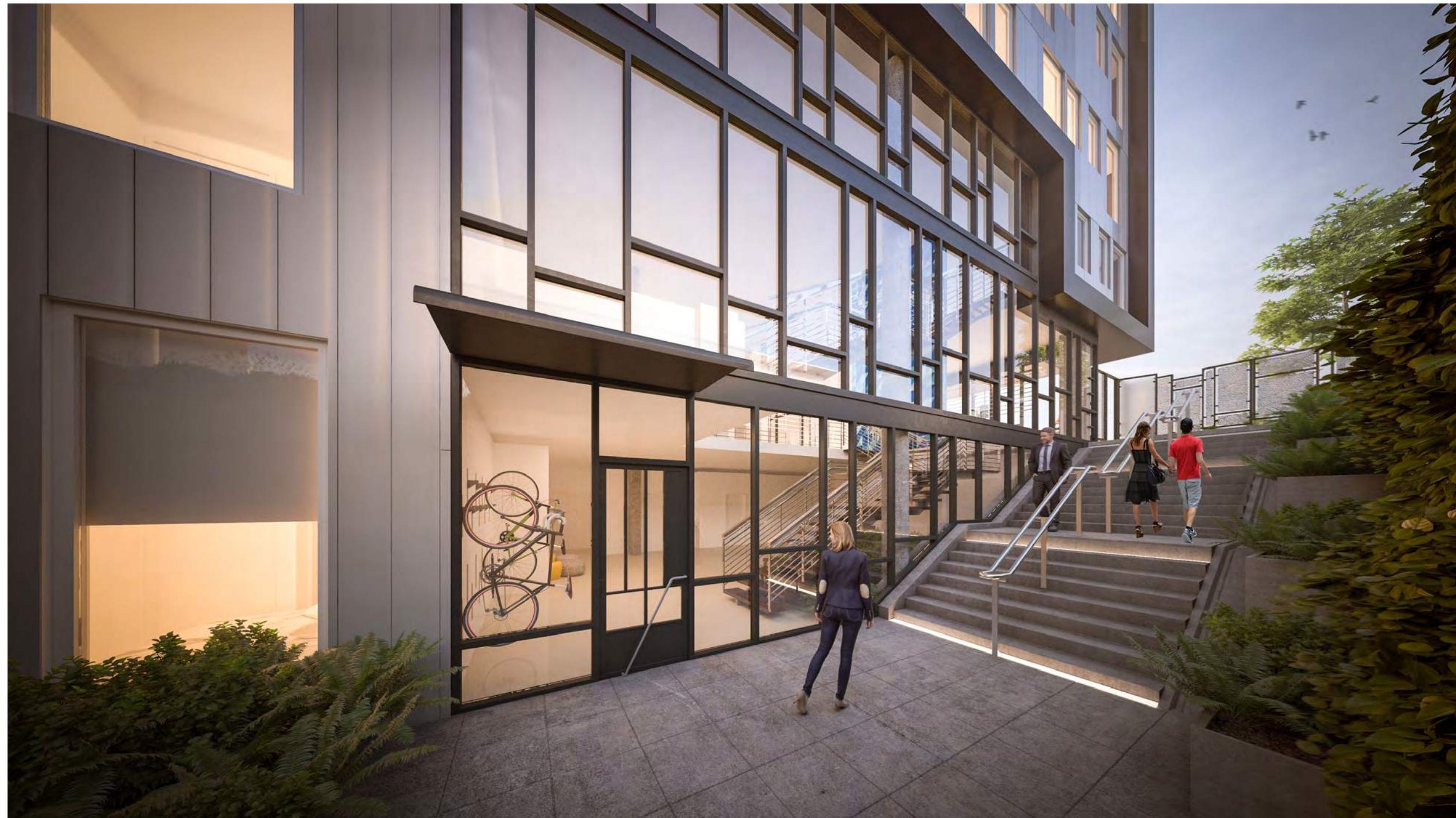
VIEW TOWARDS SOUTHWEST FROM MCGILVRA PLACE PARK



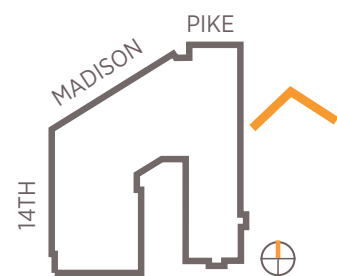


VIEW TOWARDS RESIDENTIAL ENTRY





VIEW TOWARDS NORTH FROM PRIVATE MEWS

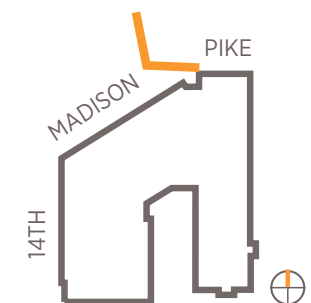


13.0 RENDERINGS

NOTE: Glazing at this location is slightly translucent and/or fritted to signify a change in use (residential fitness amenity is located beyond this window), creating visual interest for pedestrians and cars along Madison, as well as privacy for residents using the fitness room.

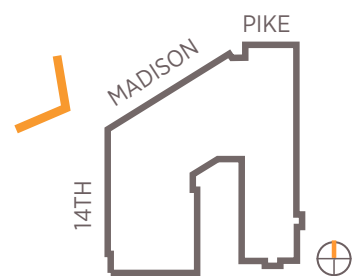


VIEW TOWARDS SOUTHWEST ALONG MADISON



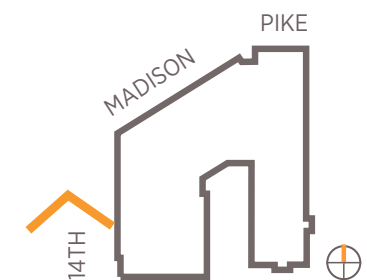


VIEW TOWARDS CORNER OF 14TH & MADISON



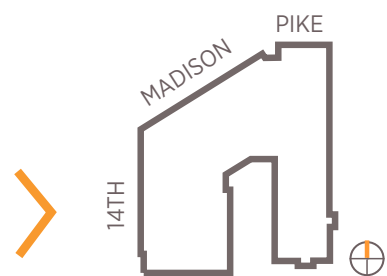


VIEW TOWARDS MADISON FROM 14TH



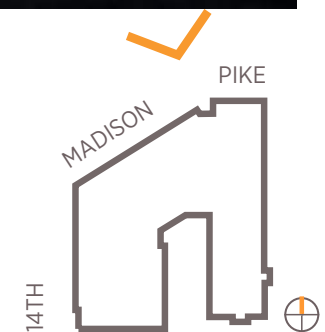


VIEW TOWARDS 14TH FROM WEST





VIEW TOWARDS PIKE AND MADISON

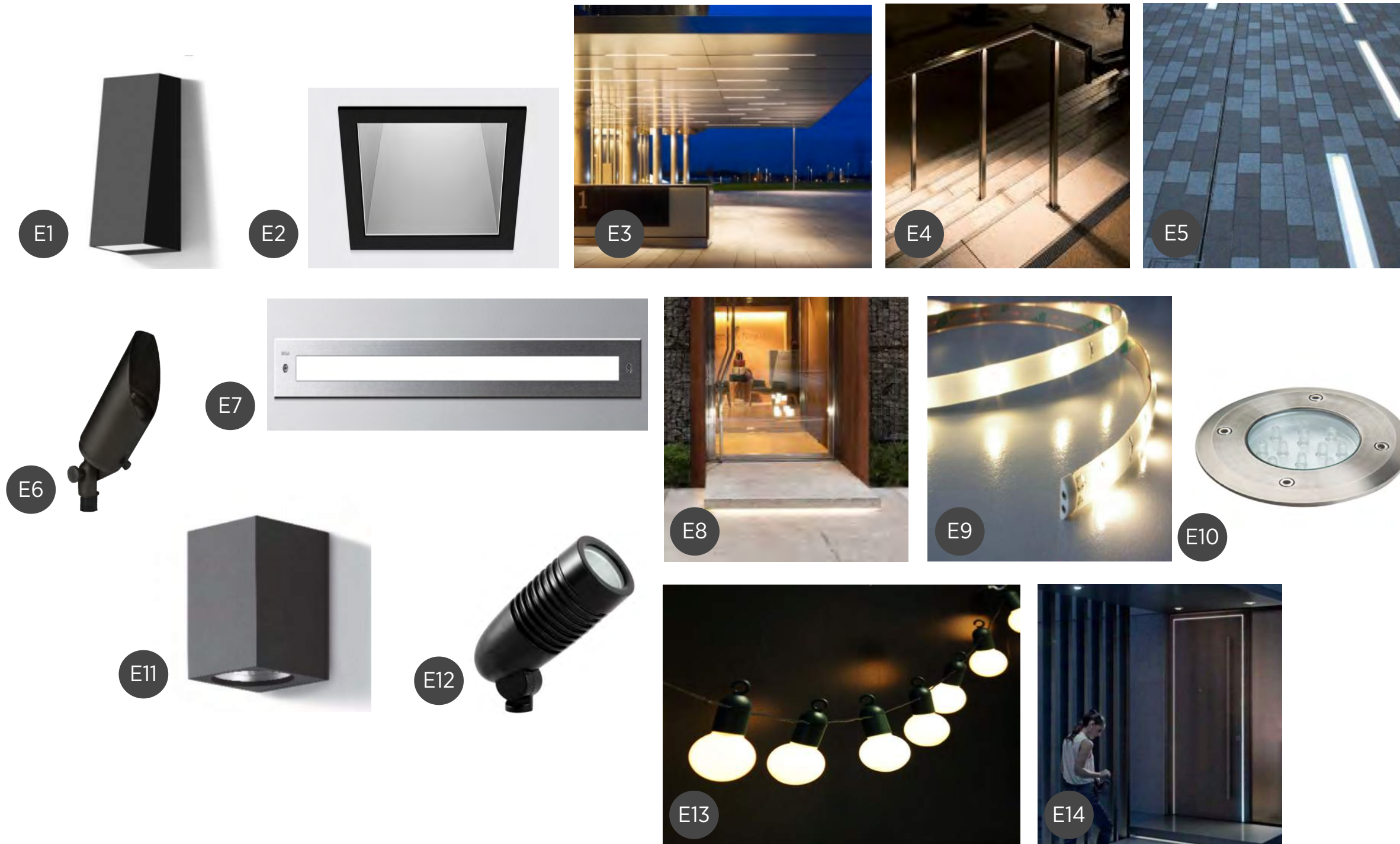


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14.0 EXTERIOR LIGHTING PLAN



14.0 EXTERIOR LIGHTING PLAN



- E1. LED Surface Wall Sconce
- E2. LED Down Light
- E3. LED Linear Down Light
- E4. LED Linear Hand Rail Light
- E5. LED Linear In-Grade Light
- E6. LED Landscape Accent Light
- E7. LED Step Light
- E8. LED Step Light
- E9. Wet Location LED Strip Light
- E10. LED Up-Light
- E11. LED Wall Light
- E12. LED Bullet Light
- E13. LED Festoon Light
- E14. LED Continuous Linear In Grade Light

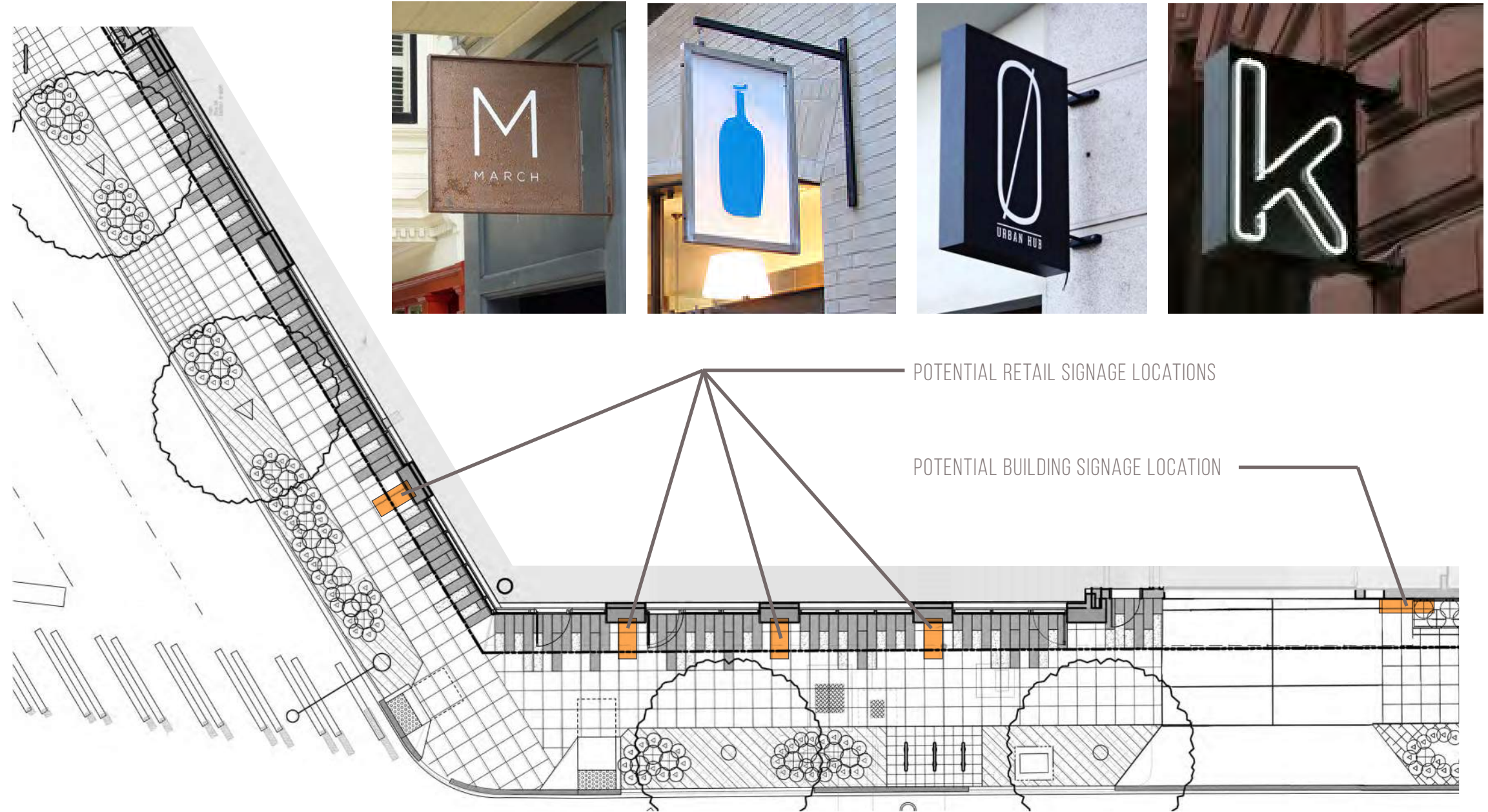
EXTERIOR LIGHTING

The exterior lighting is designed to coordinate with the building façade. Lighting for ground level illumination will include LED down lighting at main building entrances, canopies, wall sconces and soffits for general illumination. Landscape up-lights will be provided at trees and landscaping. Linear LED lighting will be provided under benches. Table mounted task lighting to illuminate BBQ prep/work surface areas will be provided. A combination of low-level step lights or bollards will be provided along curbs, planters, and pathways for general way finding and egress illumination.

The downlights at the exterior unit entries as well as the step lighting used at the eastern wall planter are specified with a frosted diffuser to minimize glare and provide a more comfortable light output for the residents. Both specifications not only offer diffused shielding but are also low output fixtures to ease the worry of light pollution. Recessed step lights are typically mounted 1-2 ft. above grade.

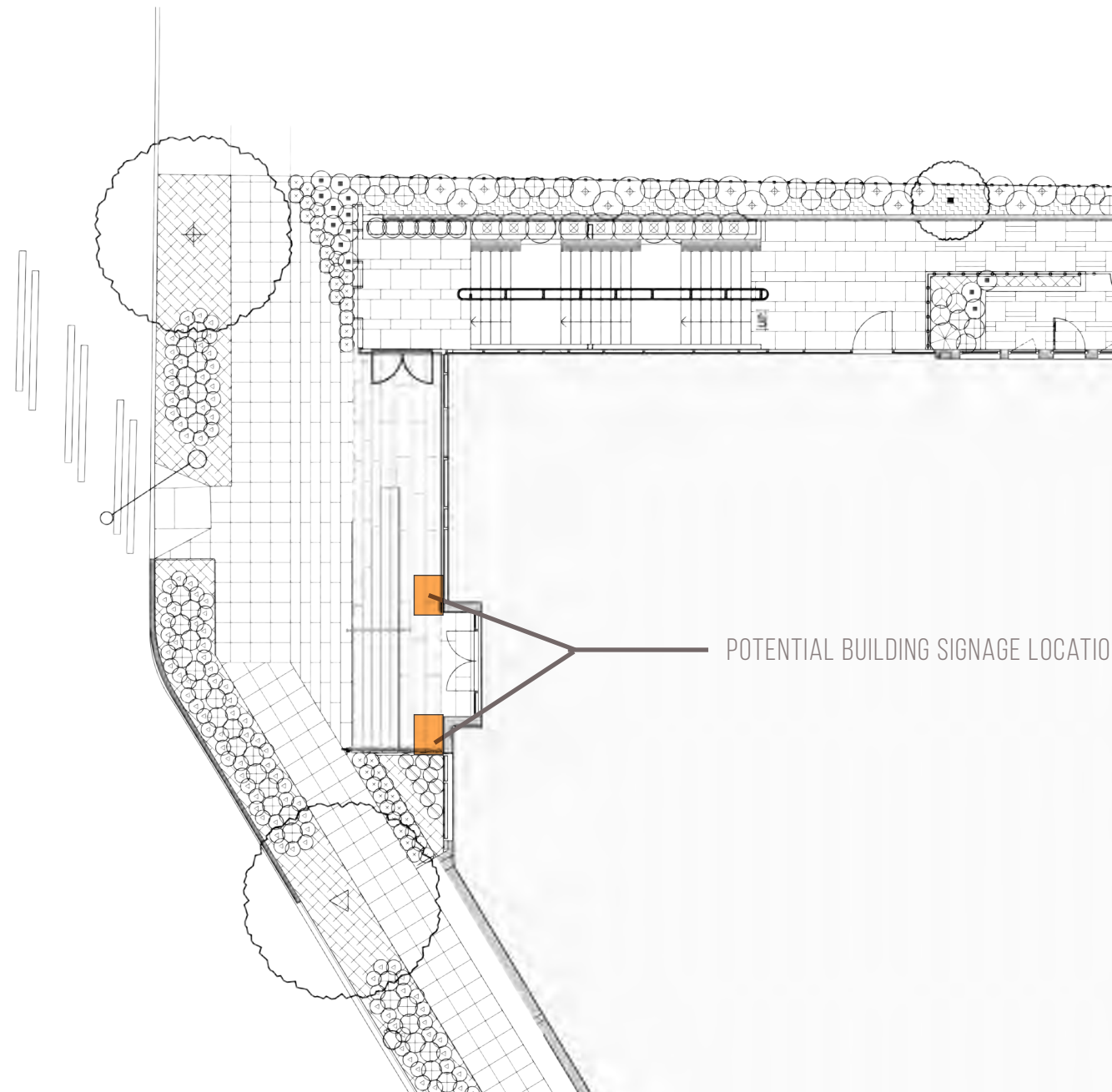
RETAIL SIGNAGE CONCEPTS

SIGNAGE



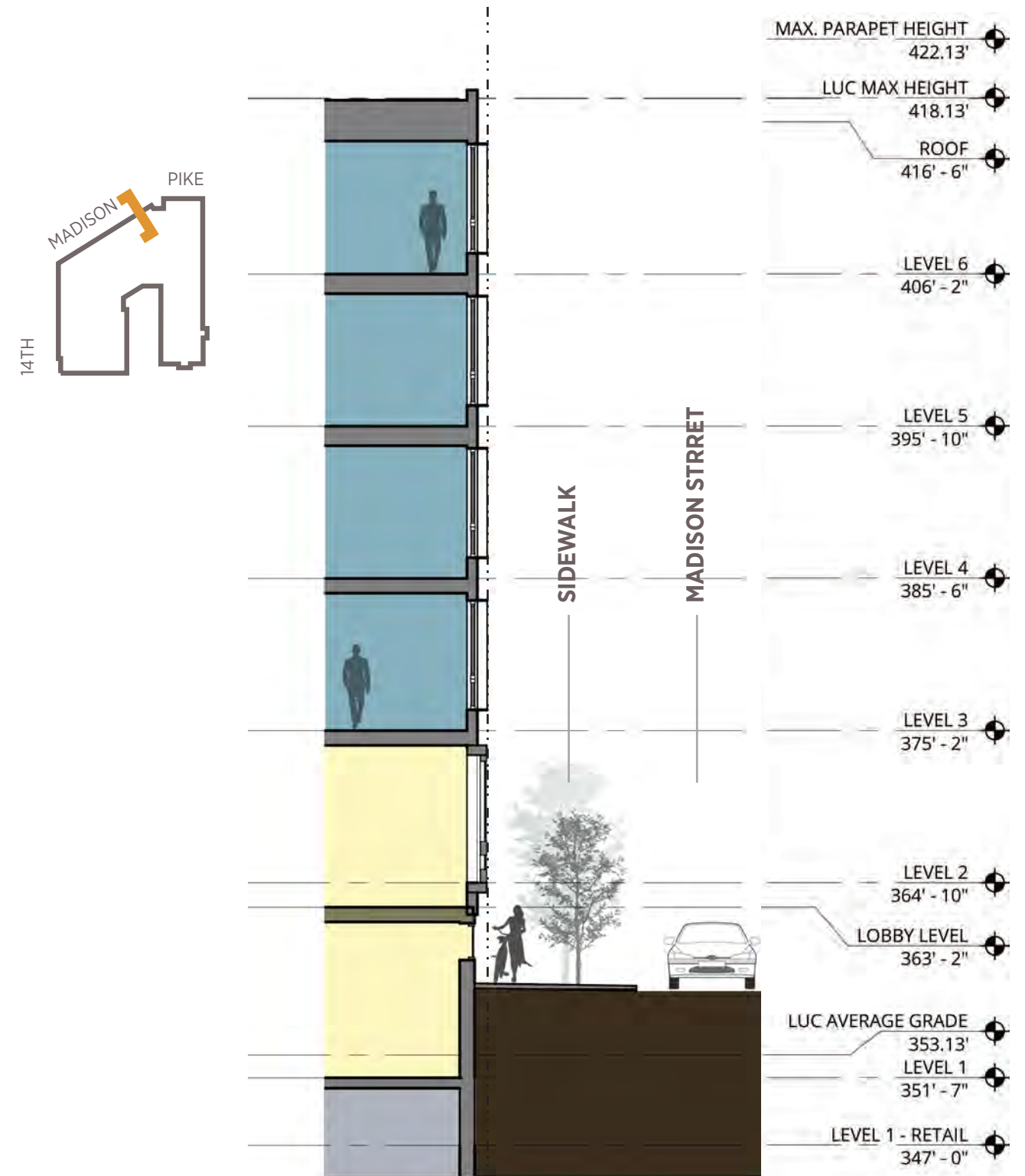
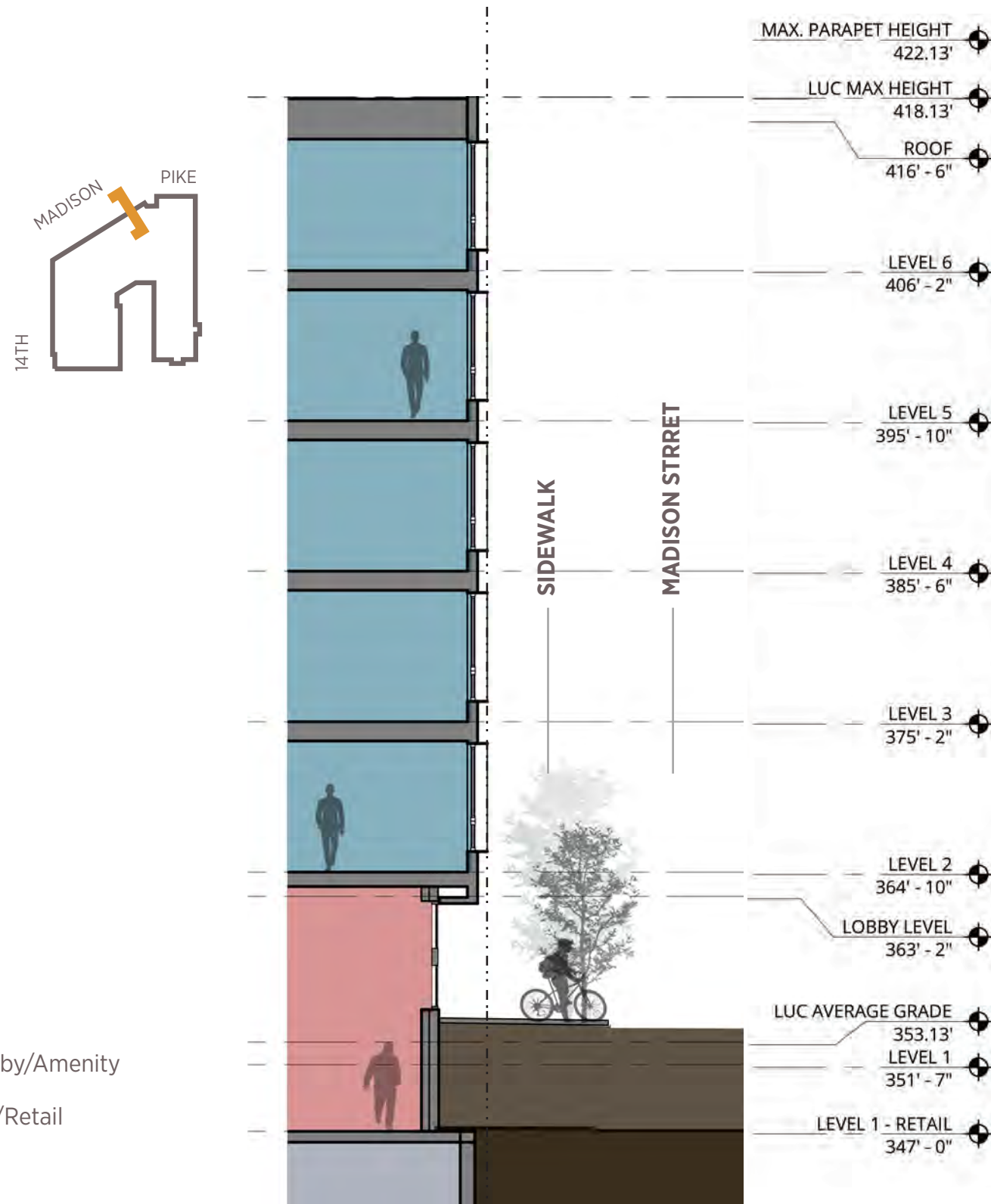
EAST SIGN ELEVATION $\frac{3}{32}'' = 1'-0''$

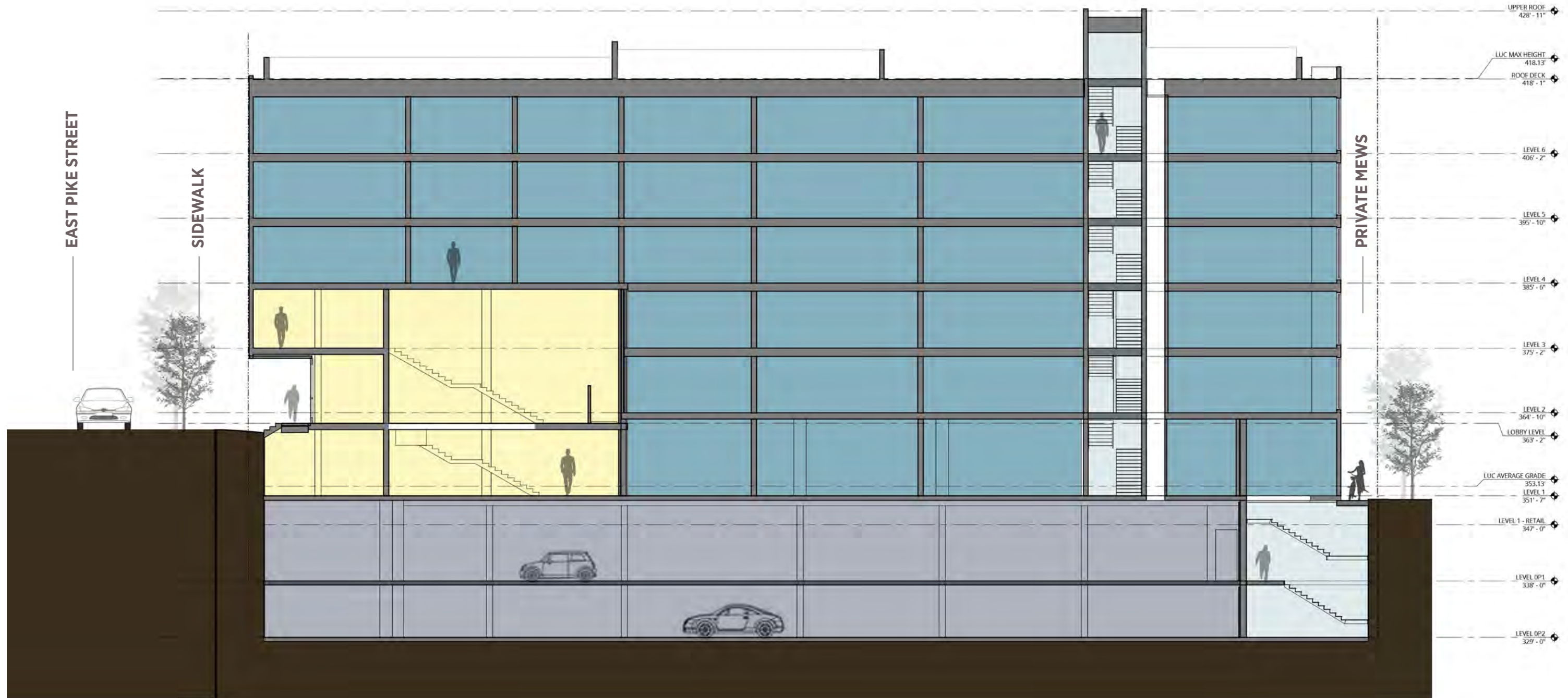
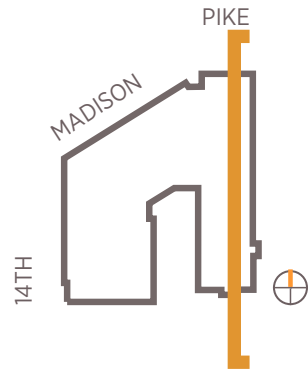
BUILDING SIGNAGE CONCEPTS



16.0 BUILDING SECTIONS

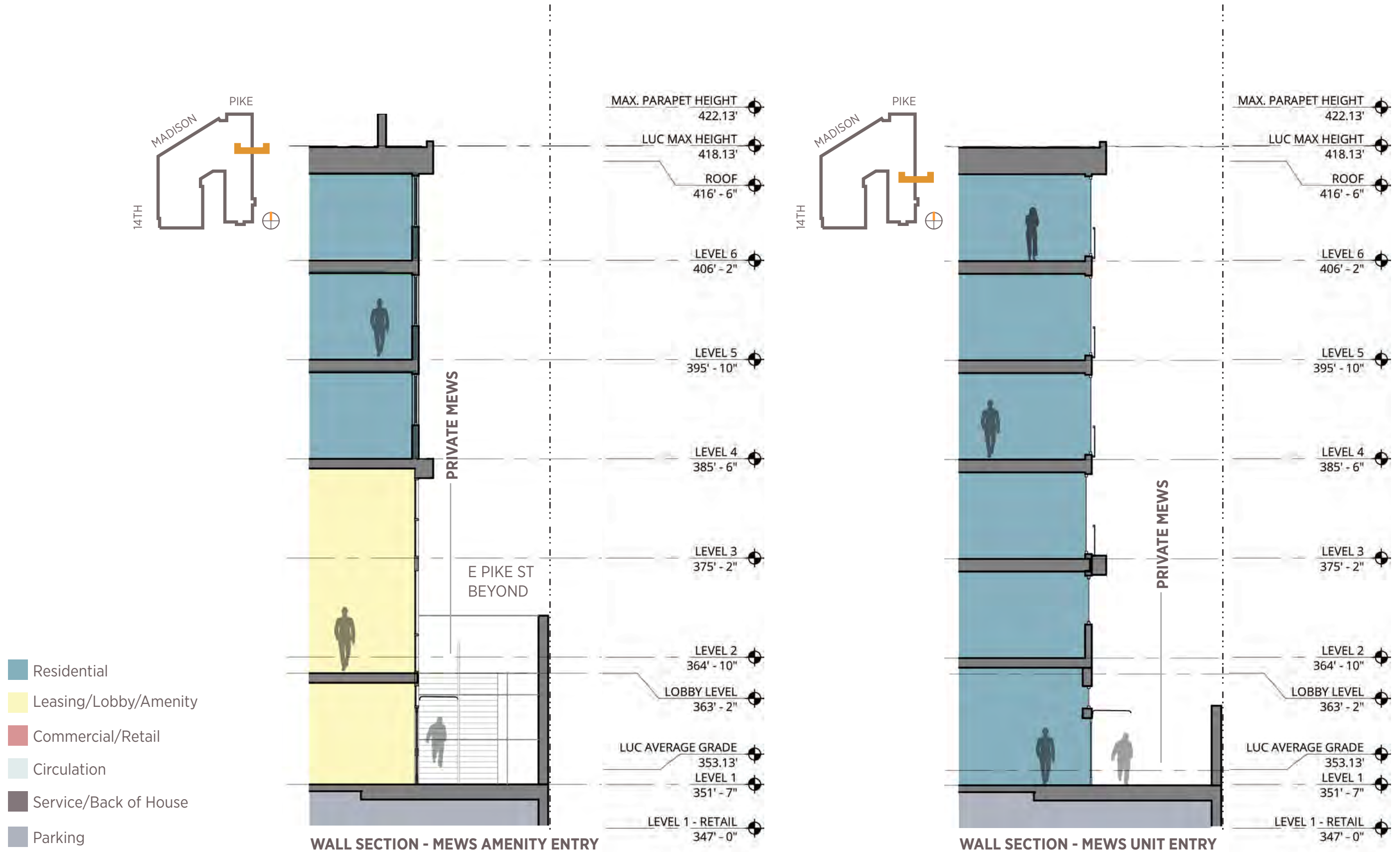
- Residential
- Leasing/Lobby/Amenity
- Commercial/Retail
- Circulation
- Service/Back of House
- Parking

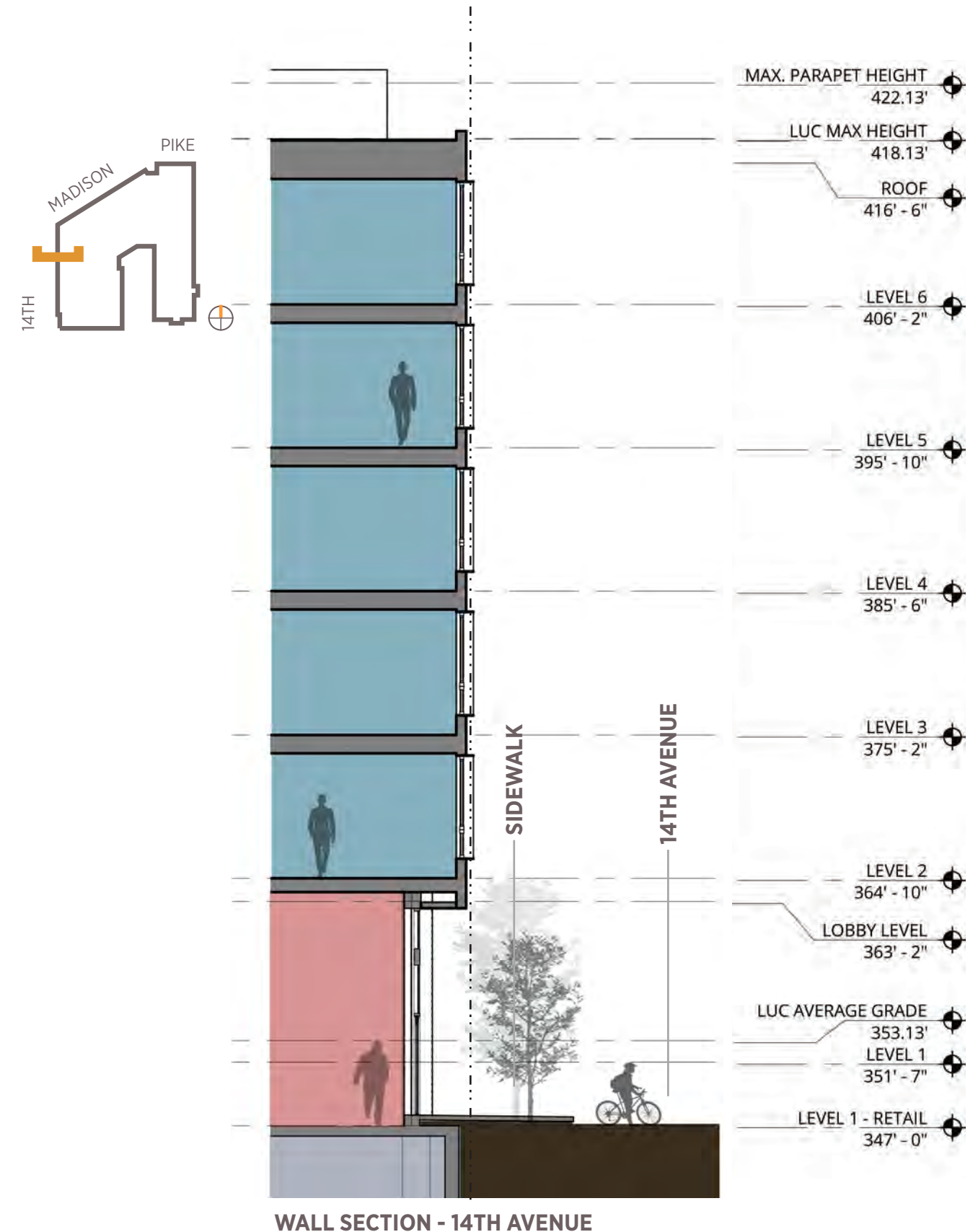
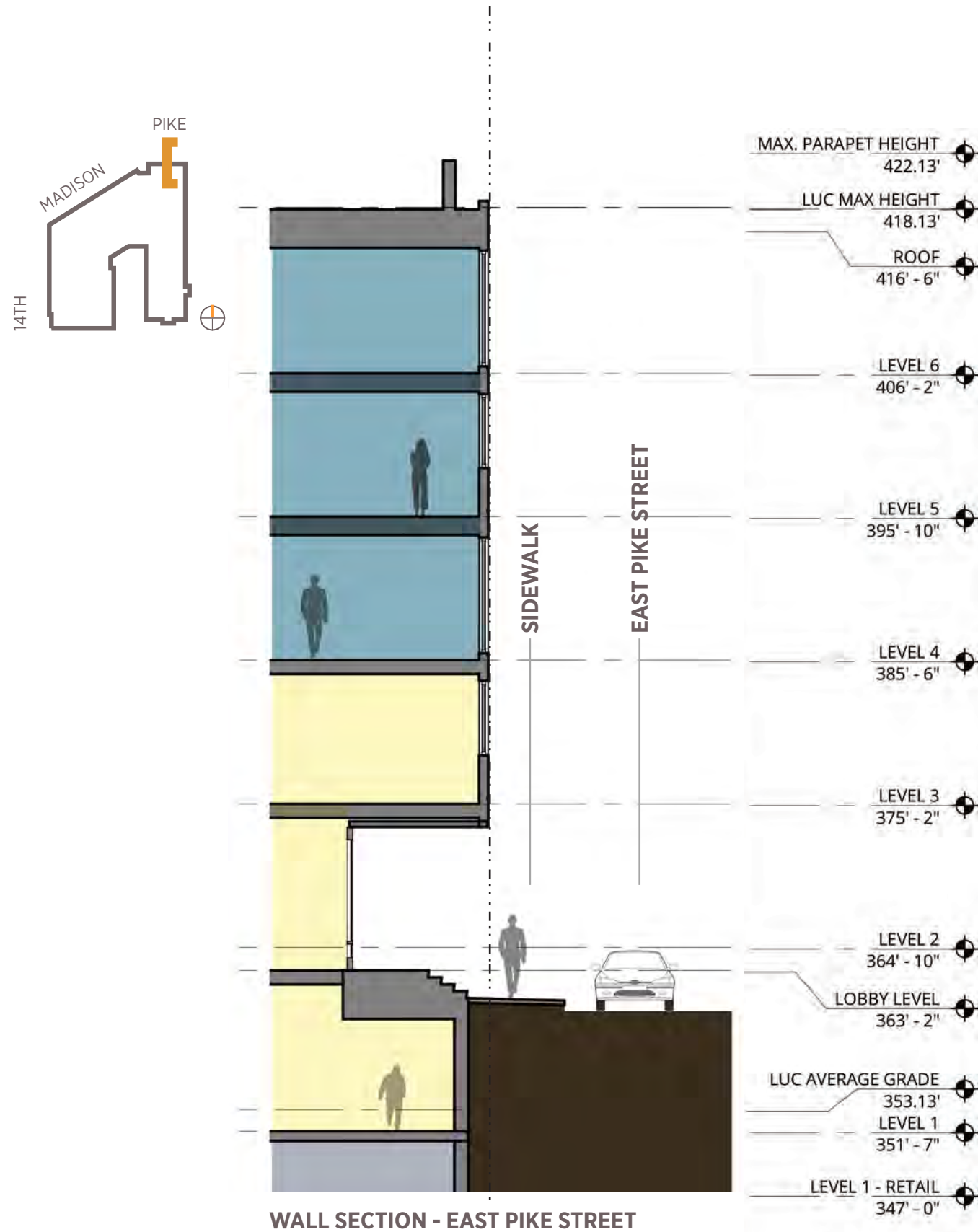




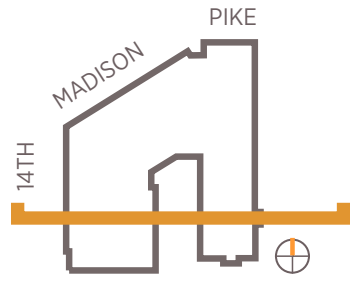
BUILDING SECTION - NORTH - SOUTH

16.0 BUILDING SECTIONS



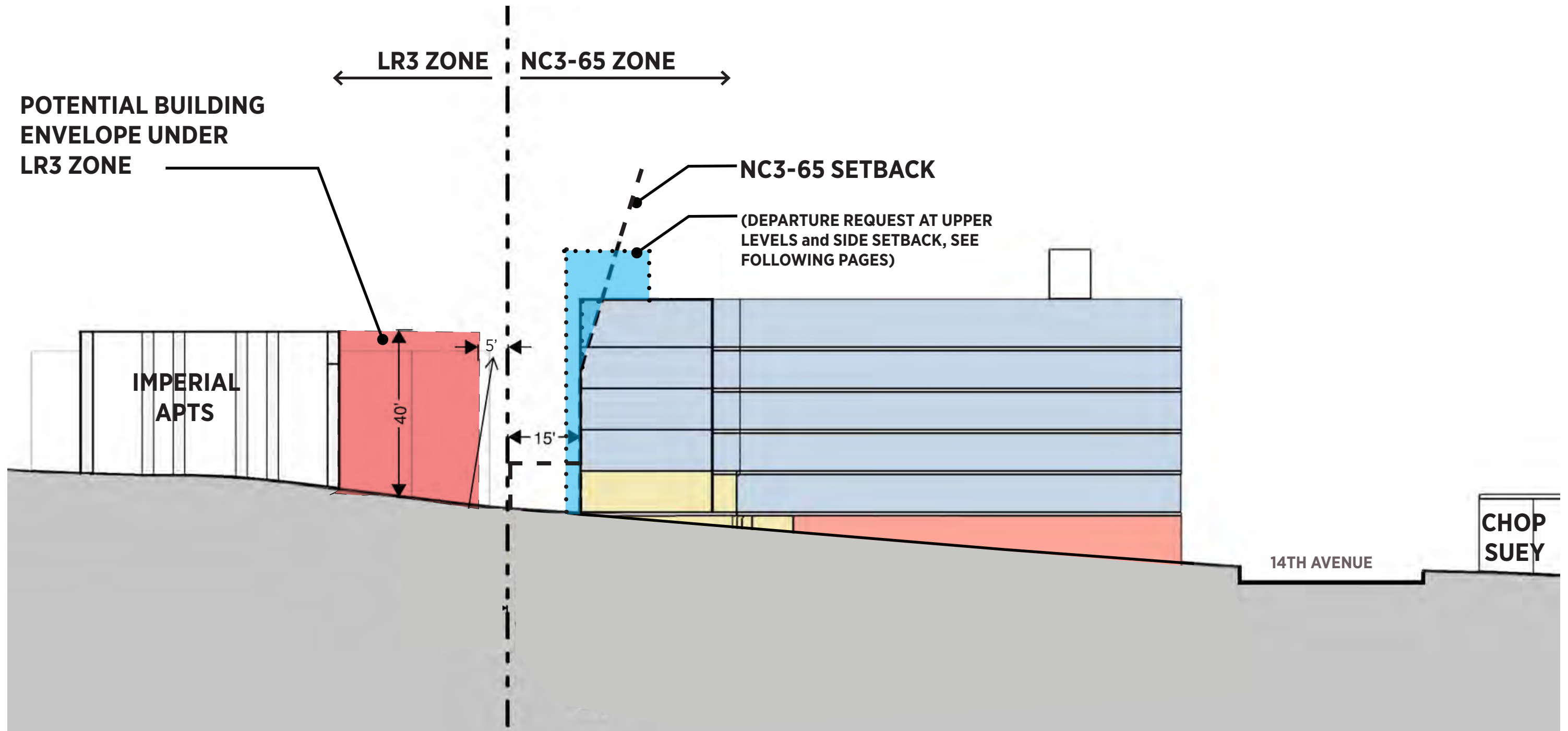


16.0 BUILDING SECTIONS



BUILDING SECTION - EAST - WEST

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ZONING HEIGHT DIAGRAM

AT PINE STREET
NOT TO SCALE

DEPARTURE #1

REQUEST #1:

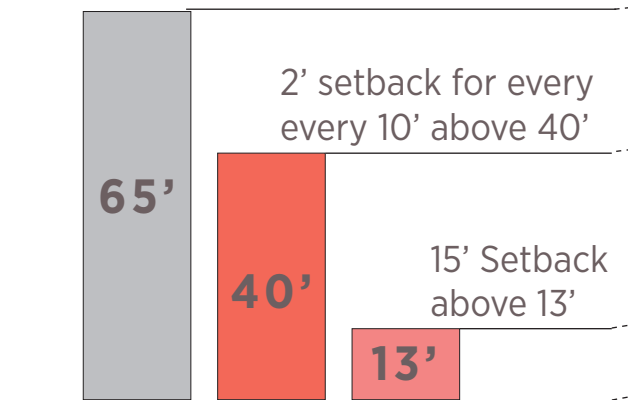
We propose a portion of the structure approximately 170' in length to encroach up to 6' into the required setback for the portion of the building above 40' in height.

RATIONALE:

The building proposal includes setbacks at the ground level in the NC3 zone where no setbacks are required. It also includes almost 20 feet of setback at the north end of the property where only 15 feet is required, serving as a generous buffer between the NC3 and LR3 zones, allowing for greater privacy and open space between buildings.

In order to offset the loss of square footage at the ground level, we request a departure at the uppermost two levels. This departure allows for an architecturally consistent form of the proposed massing, meeting the intent of the Design Guidelines DC2 (Architectural Concept) and DC3 (Open Space Concept). Given the grade change with the property to the east, we believe the proposed setback achieves the intent of the side setback requirement (light and air, stepping down to a residential zone).

NC3-65 Max Zoning Height

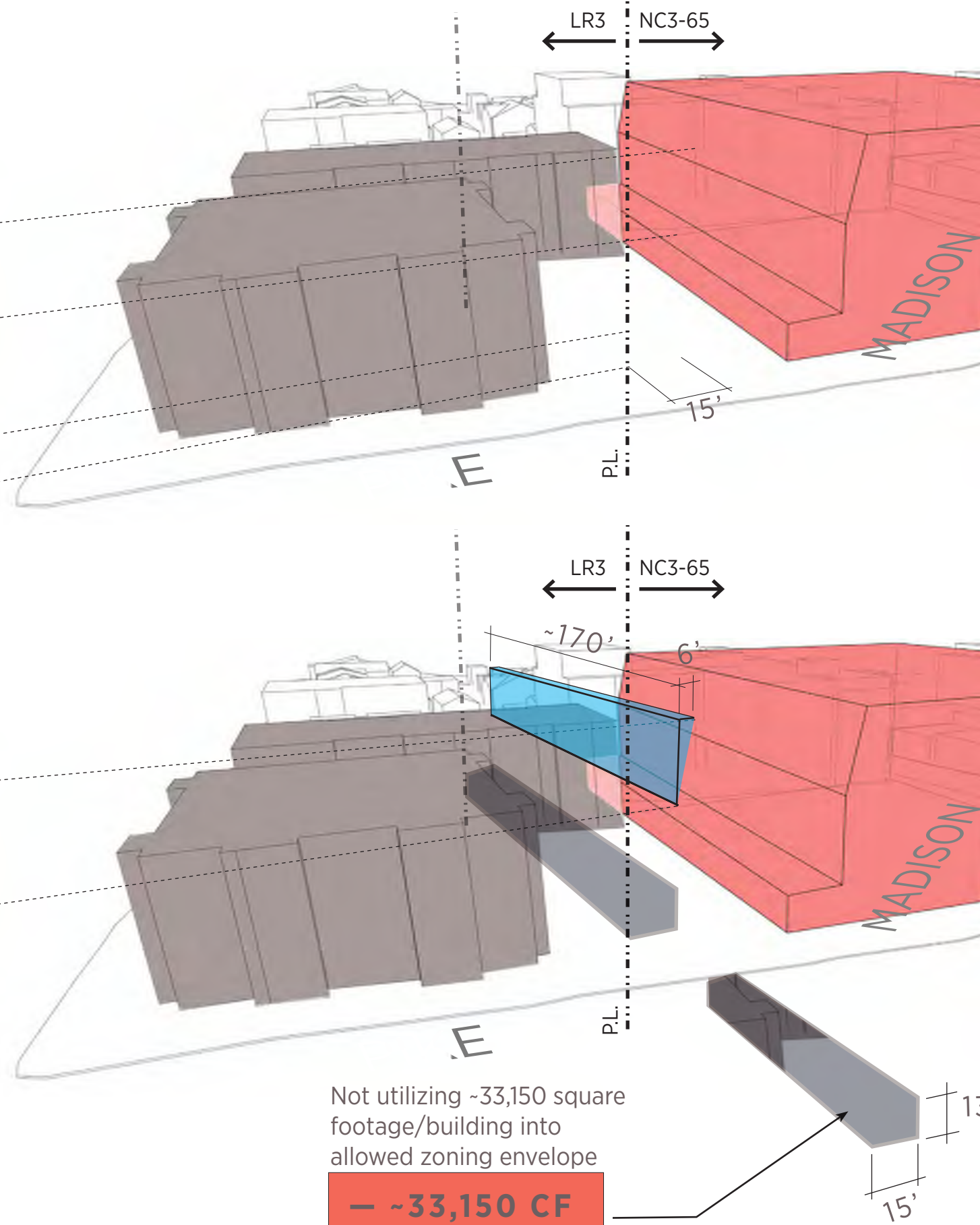


CODE-COMPLIANT BUILDING ENVELOPE

NC3-65 Max Zoning Height

We propose a portion of the structure approx. 170' in length to encroach up to 6' into the required [2' setback for every 10' above 40'] setback.

+ ~12,750 CF



Not utilizing ~33,150 square footage/building into allowed zoning envelope

- ~33,150 CF

DEPARTURE REQUEST #1 DIAGRAM

-33,150
 +12,750

 -20,400 CF difference

17.0 DEPARTURES

DEPARTURE #2

REQUEST #2:

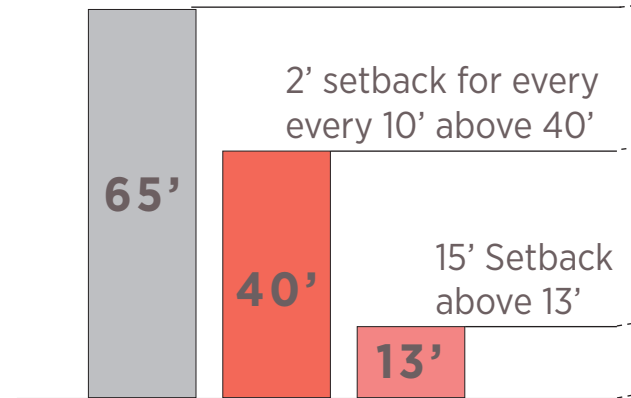
We propose a portion of the structure approximately 10' in length to encroach up to 5' into the required setback for the rear egress stair tower for the portion of the building above 13' in height.

RATIONALE:

The building proposal includes setbacks at the ground level in the NC3 zone where no setbacks are required. It also includes almost 20 feet of setback at the north end of the property where only 15 feet is required, serving as a generous buffer between the NC3 and LR3 zones, allowing for greater privacy and open space between buildings.

In order to meet the intent of Design Guidelines CS2 (Urban Pattern & Form) and DC2 (Architectural Concept) as well as create an efficient building plan (corridor alignments and circulation) we request a departure to allow the egress stair tower to encroach into the setback. This encroachment allows us to respect and increase the visual privacy of the adjacent residents of the Qualman Apartments (our neighbor to the east). Special treatment in the of the stair tower will be provided.

NC3-65 Max Zoning Height



CODE-COMPLIANT BUILDING ENVELOPE

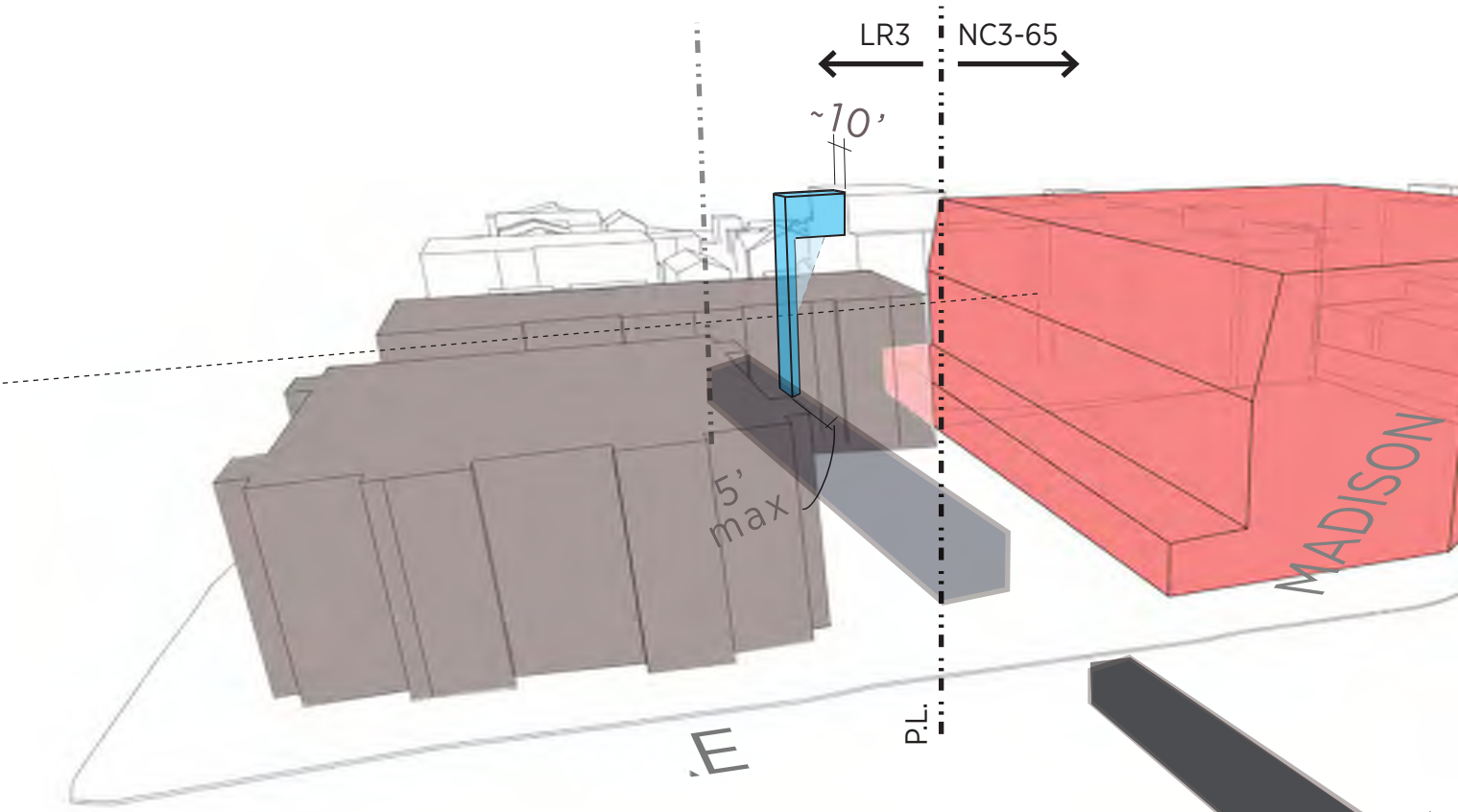
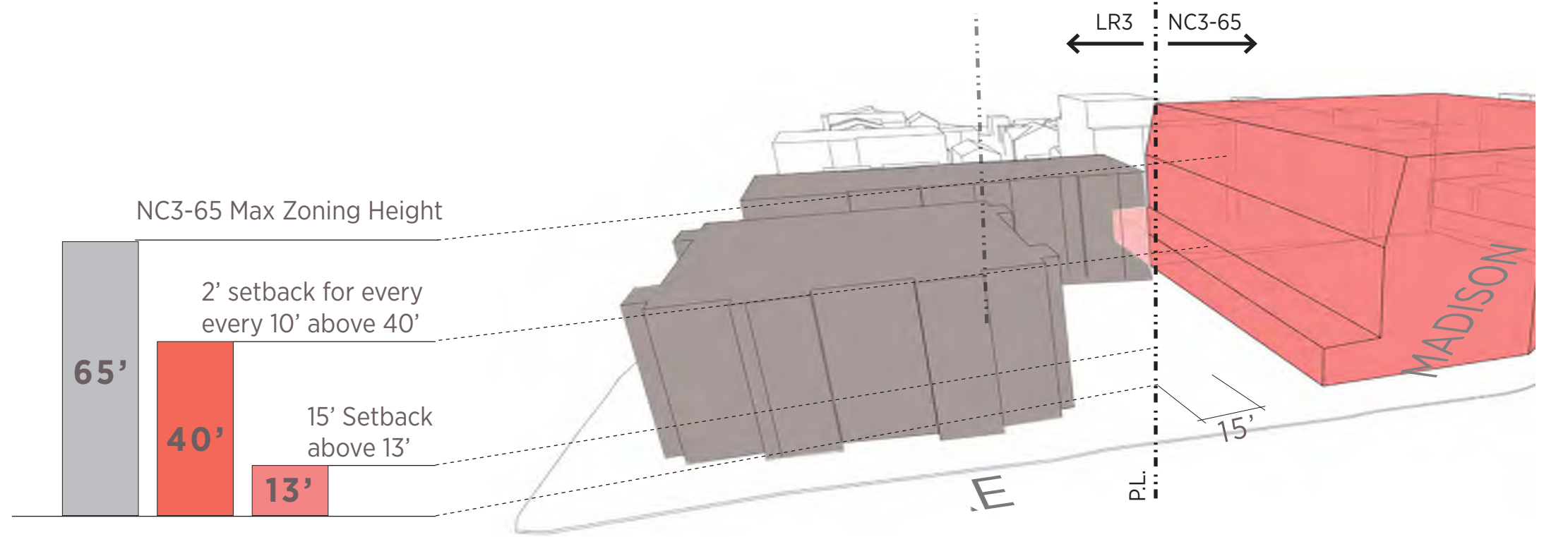
NC3-65 Max Zoning Height

We propose a portion of the stair enclosure, approx. 10' in length to encroach:
 - up to 5' into the required setback above 13' to 40'
 - up to 6' into the required [2' setback for every 10' above 40'] setback.

+ ~2,900 CF

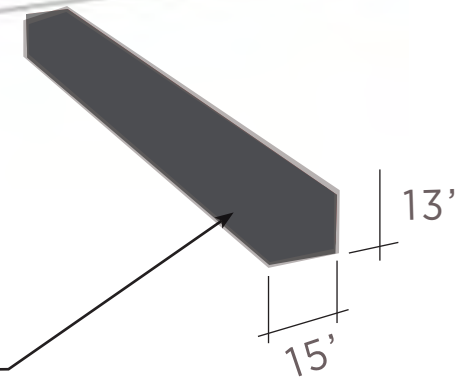
-33,150
+2,900
-30,250 CF difference

-33,150
+2,900
+12,750
-17,500 CF for both departures



Not utilizing ~33,150 square footage/building into allowed zoning envelope

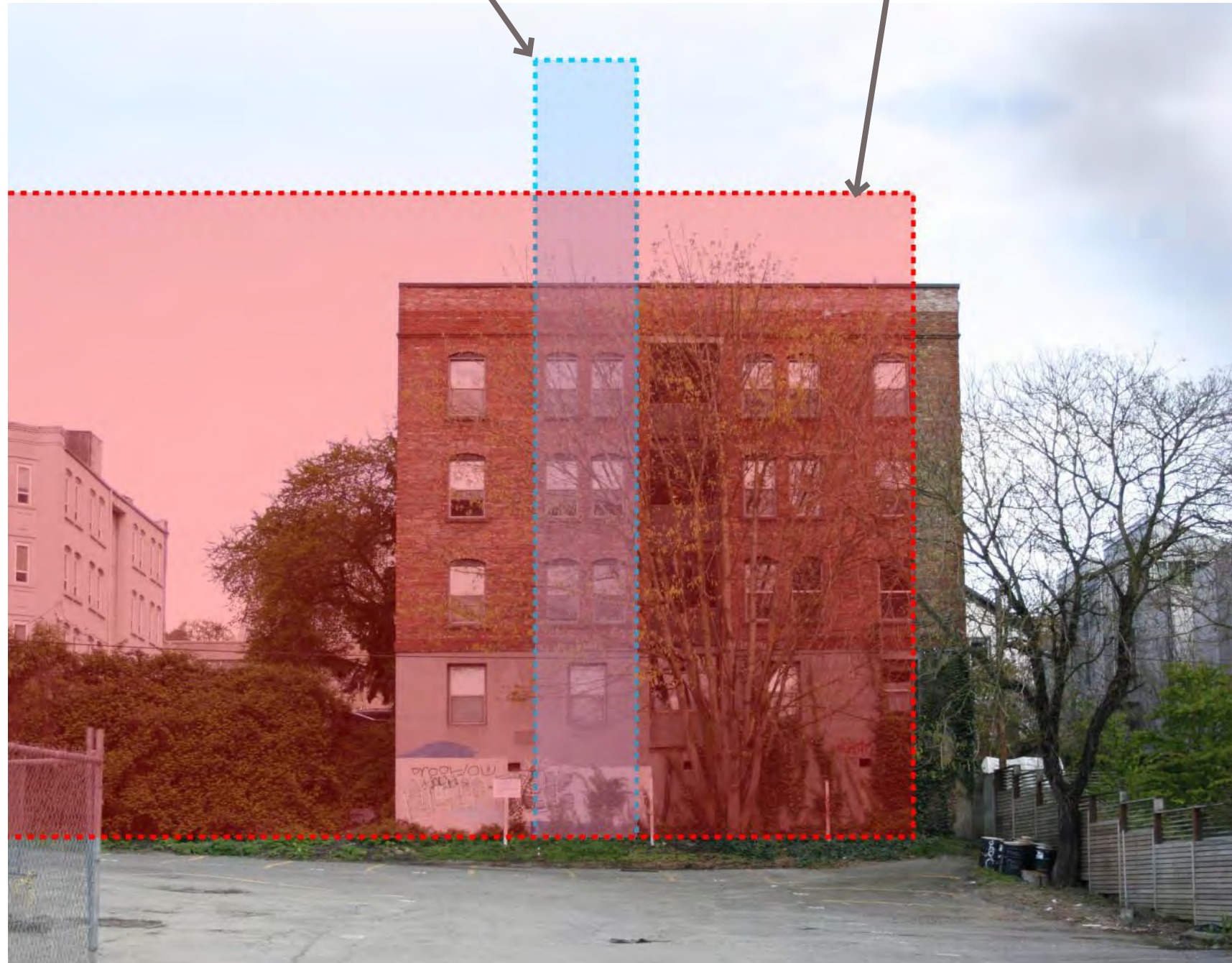
- ~33,150 CF



DEPARTURE #2
BUILDING ENVELOPE
DIAGRAM

Stair Tower (with departure #2)
-Allows for greater privacy
between both neighbors.
-Allows for more light to reach
Qualman apartments and
neighbors to SE.

Building Envelope



SQUARE FOOTAGE OFFSET

By not building into the code allowed building envelope adjacent to the LR3 zone (15 feet wide, 13 feet high allowed), we are creating a better, more unified building massing while also being a good neighbor, giving open space back to the neighborhood and respecting our residentially zoned neighbor.

This results in approximately 33,150 SF that we are not using; however, we request two departures to offset the loss, as shown to the left. Even with two departures, we are still leaving ~17,500 SF unused to allow for a better, more unified massing.

DEPARTURE REQUEST #2 DIAGRAM

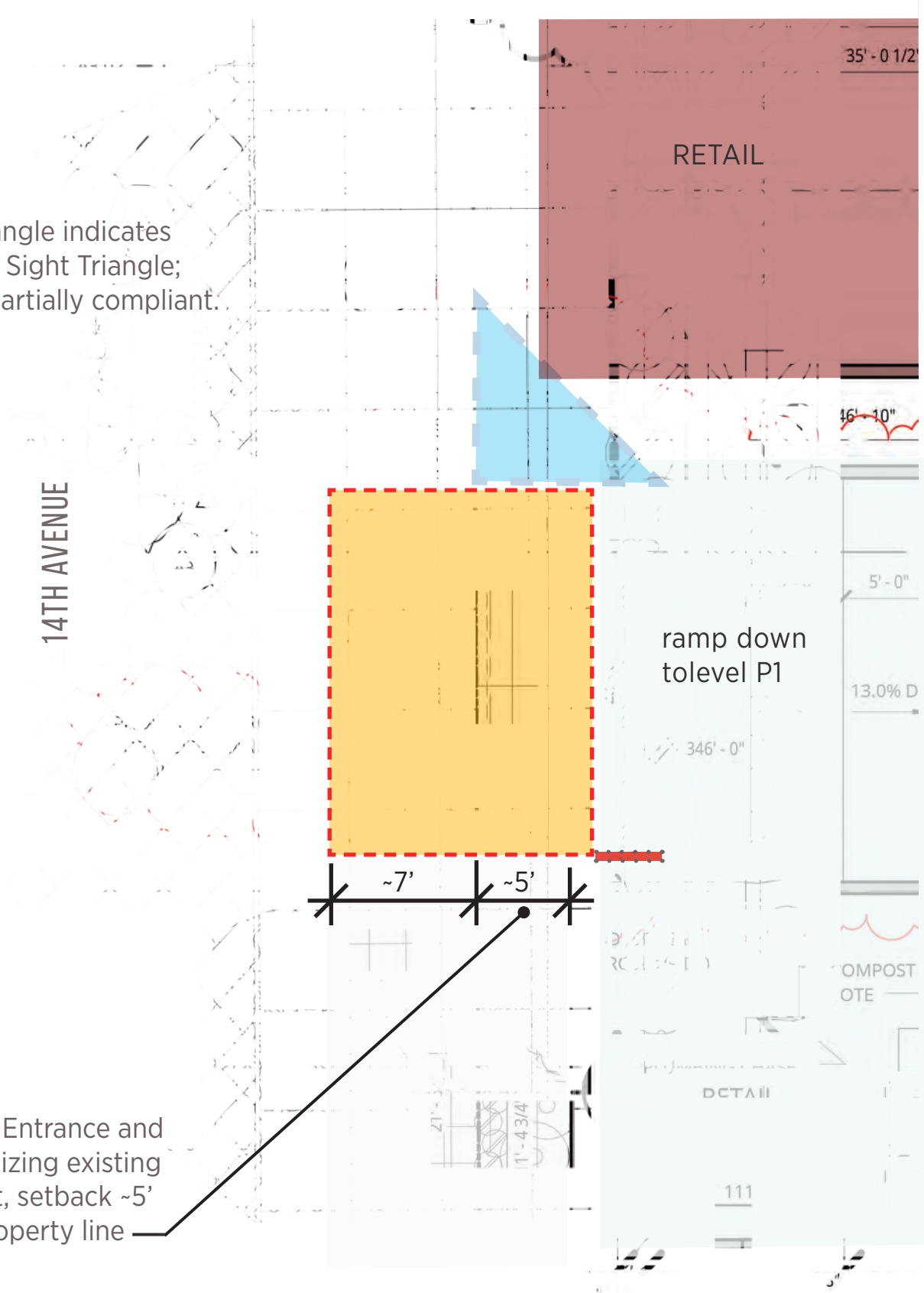
CODE-COMPLIANT
BUILDING ENVELOPE
DIAGRAM



Blue Triangle indicates required Sight Triangle; we are partially compliant.

Parking Entrance and Exit, utilizing existing curb cut, setback ~5' from property line

Note: See landscape plan for paving and landscape species.



SEATTLE POLICE DEPARTMENT EAST PRECINCT | use of mirrors



CHLOE | use of mirrors | no site triangle

NEIGHBORHOOD PRECEDENTS

REQUEST #3:

Per SMC 23.54.030.G. For two way driveways 22 feet wide or more, a sight triangle on the side of the driveway used as an exit shall be provided, and shall be kept clear of any obstruction for a distance of 10 feet from the intersection of the driveway.

The site triangle required is indicated in blue at the diagram to the left. We are requesting a departure from the sight triangle requirement. A parabolic mirror at the garage entrance is proposed to provide advance warning for vehicles and pedestrians.

RATIONALE

The property line is approximately 14'-6" from the edge of the sidewalk. The driveway entrance is setback approximately 5 feet from the property line. A parabolic mirror at the garage entrance along 14th is proposed to provide advance warning for vehicles and pedestrians. This departure minimizes the overall effect of the parking garage entry DC1:B (Vehicular Access) by eliminating any jogs at the frontage, allowing for a cleaner retail and garage entry face. This departure also helps meet the intent of CS2 (Architectural Context & Character) and CS3 (Urban Pattern and Form) with the integration of a clean, modern, uniform face for pedestrians to safely walk by, eliminating any visual impact of the driveway. To also help with safety, we have used a different scoring pattern at the driveway that is clearly differentiated from the sidewalk (PL4A (Entry Location & Relationships).



TRACE NORTH | no use of mirrors | no apparent site triangle

WINDOW MAPPING/
PRIVACY STUDY WITH
DEPARTURE #2

We located the egress stair tower (requires departure) directly across from a major grouping of windows in the Qualman Apartments. We located the rest of windows to have minimal impact on Qualman window adjacencies. Furthermore, windows are also screened with ground level landscaping.

As described in Departure #2 on page 70, the location of the Private Mews egress stair tower (requires departure) better meets the intent of the design guidelines.

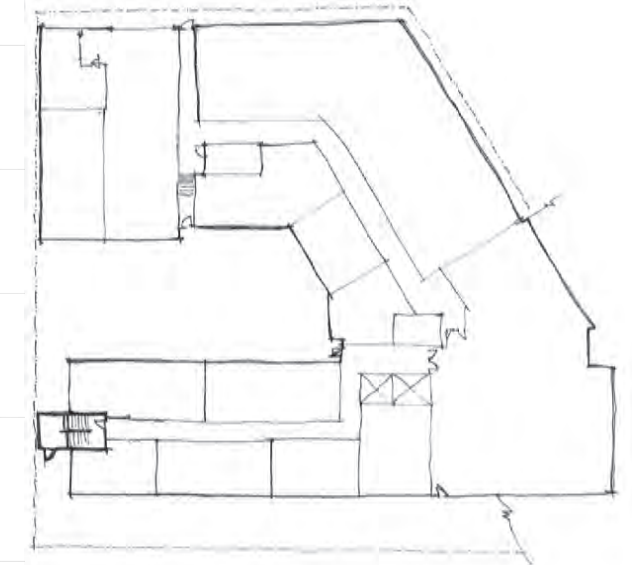
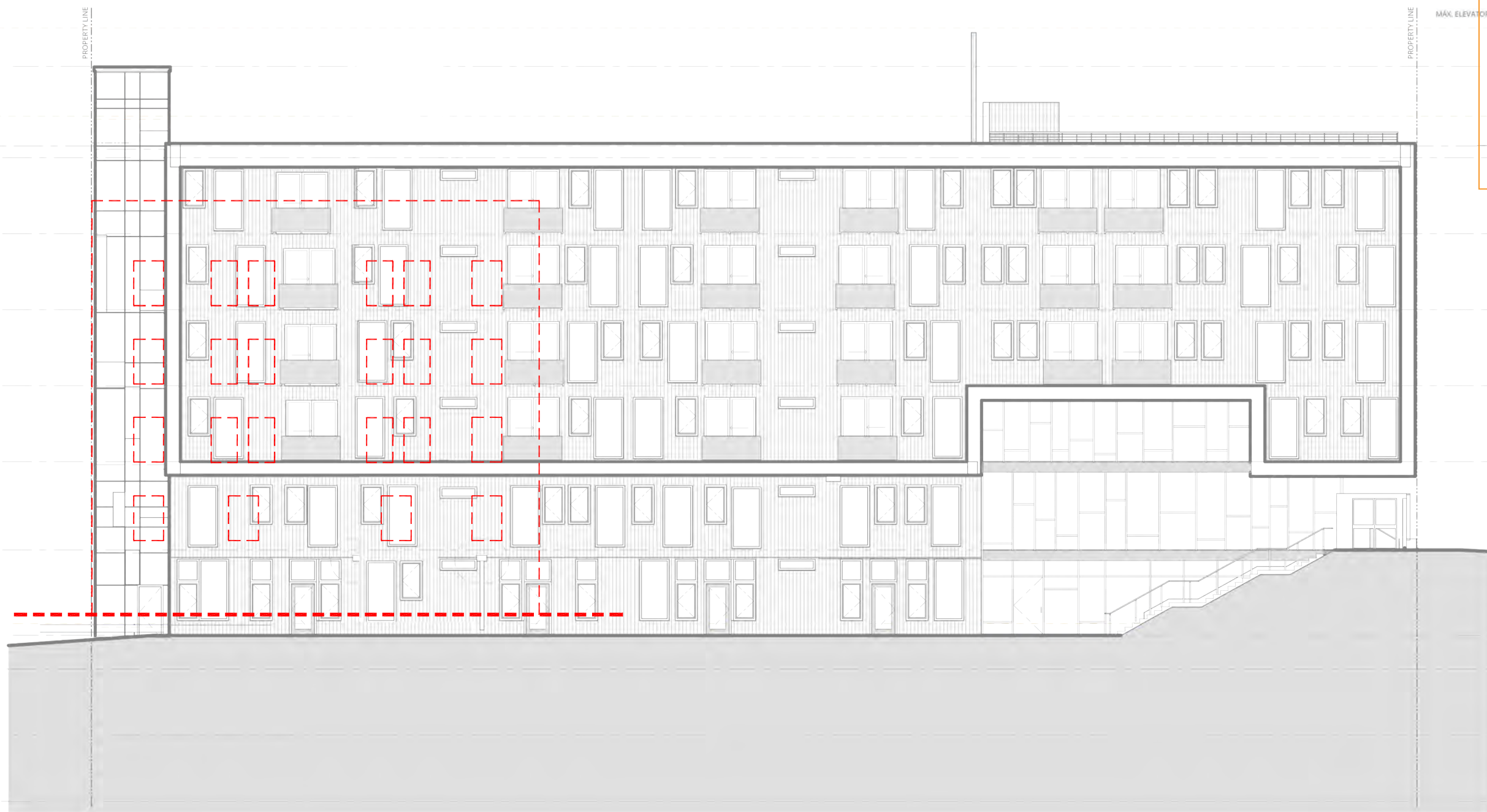
The mapping study shows that this stair location also:

- provides the greatest amount of privacy for the residents of the Qualman Apartments
- reduces bulk and massing at the SE corner of the building, allowing more light and air to reach neighboring buildings.



EAST ELEVATION WINDOW MAPPING STUDY WITH QUALMAN APARTMENTS IN FOREGROUND

CODE-COMPLIANT
COURTYARD STAIR
RELOCATION &
WINDOW MAPPING/
PRIVACY STUDY



PLAN DIAGRAM WITH
CODE-COMPLIANT STAIR

EAST ELEVATION COURTYARD STAIR RELOCATION WINDOW MAPPING STUDY WITH QUALMAN APARTMENTS IN FOREGROUND

LOBBY / LEASING

The lobby/leasing area has been developed to provide activity and support street interaction. Leasing offices have been pulled back from the street frontage to allow for more active uses to occur along Madison and Pike Streets.

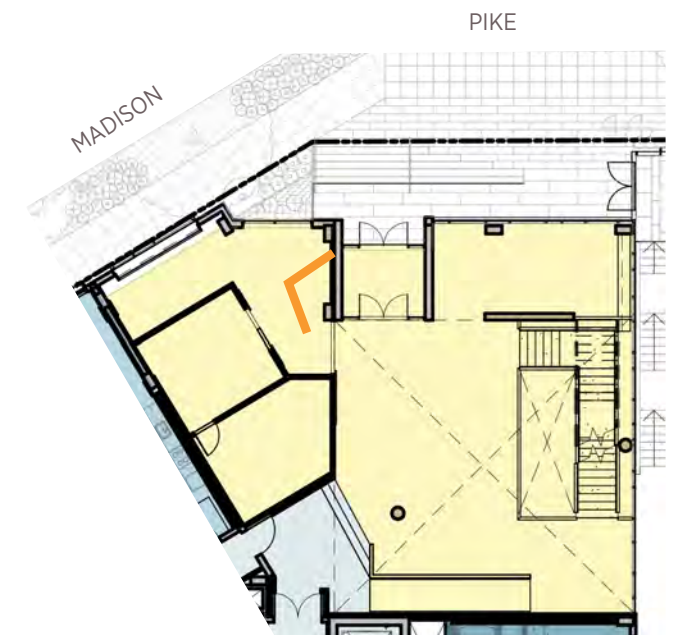


CONCEPT IMAGERY: OPEN, INVITING PLACE FOR RESIDENTS TO WORK AND RELAX, ACTIVATING THE STREET FRONTAGE

LOBBY / LEASING



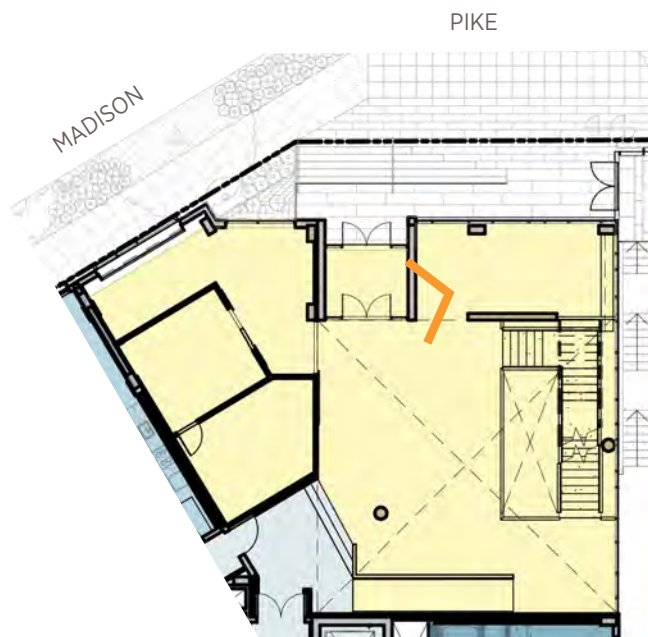
LOBBY "PERCH" VIEW TOWARDS MADISON



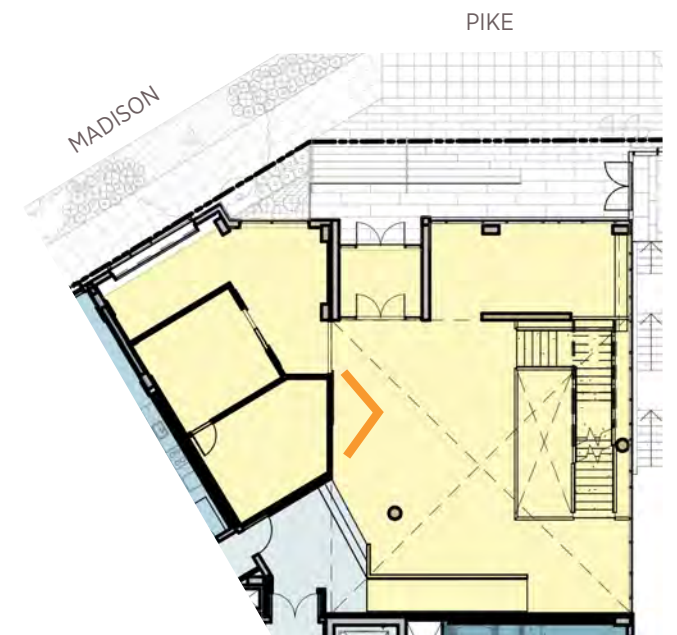
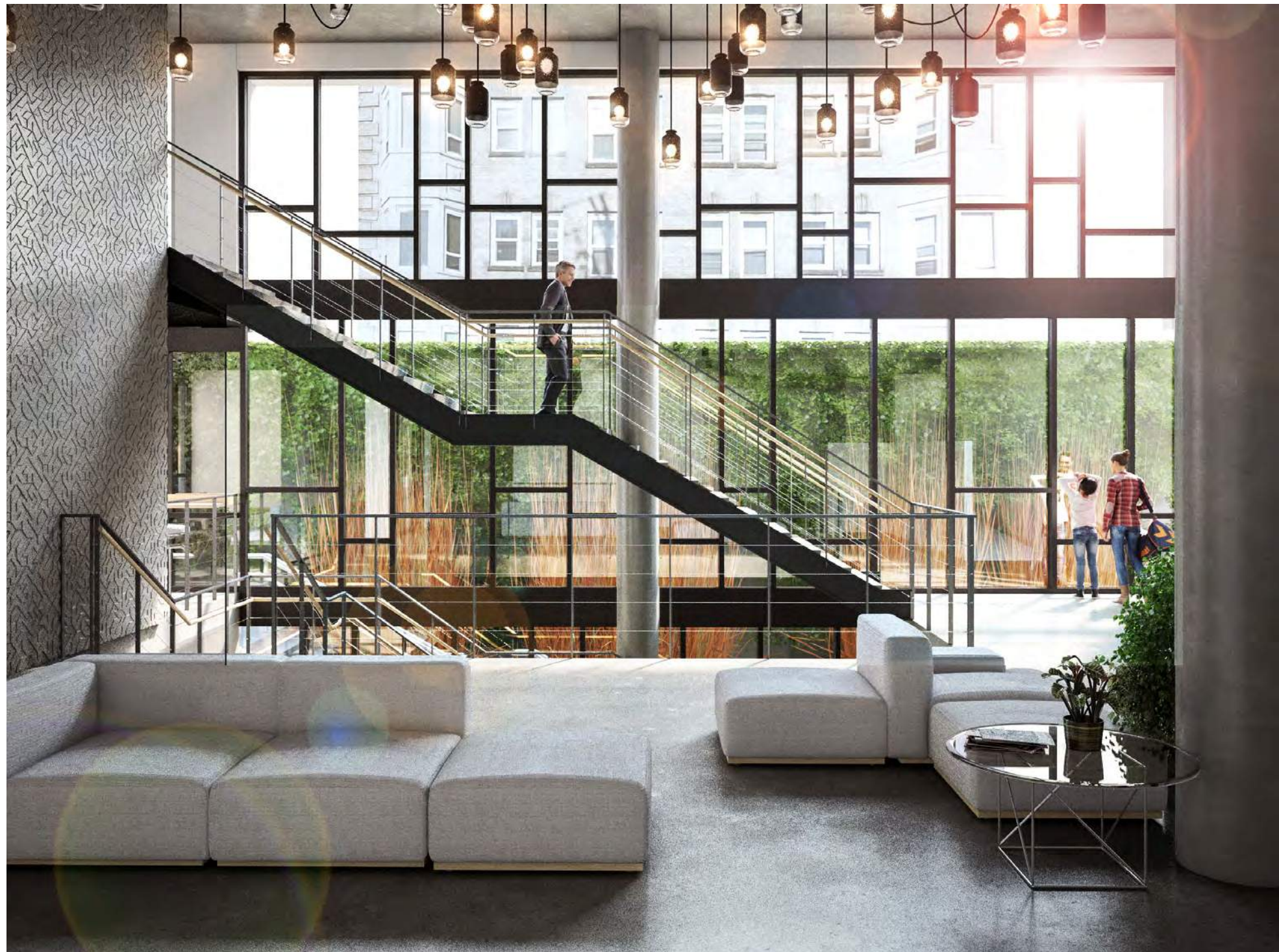
LOBBY / LEASING



LOBBY "LOUNGE" VIEW TOWARDS PRIVATE MEWS AT PIKE



LOBBY / LEASING

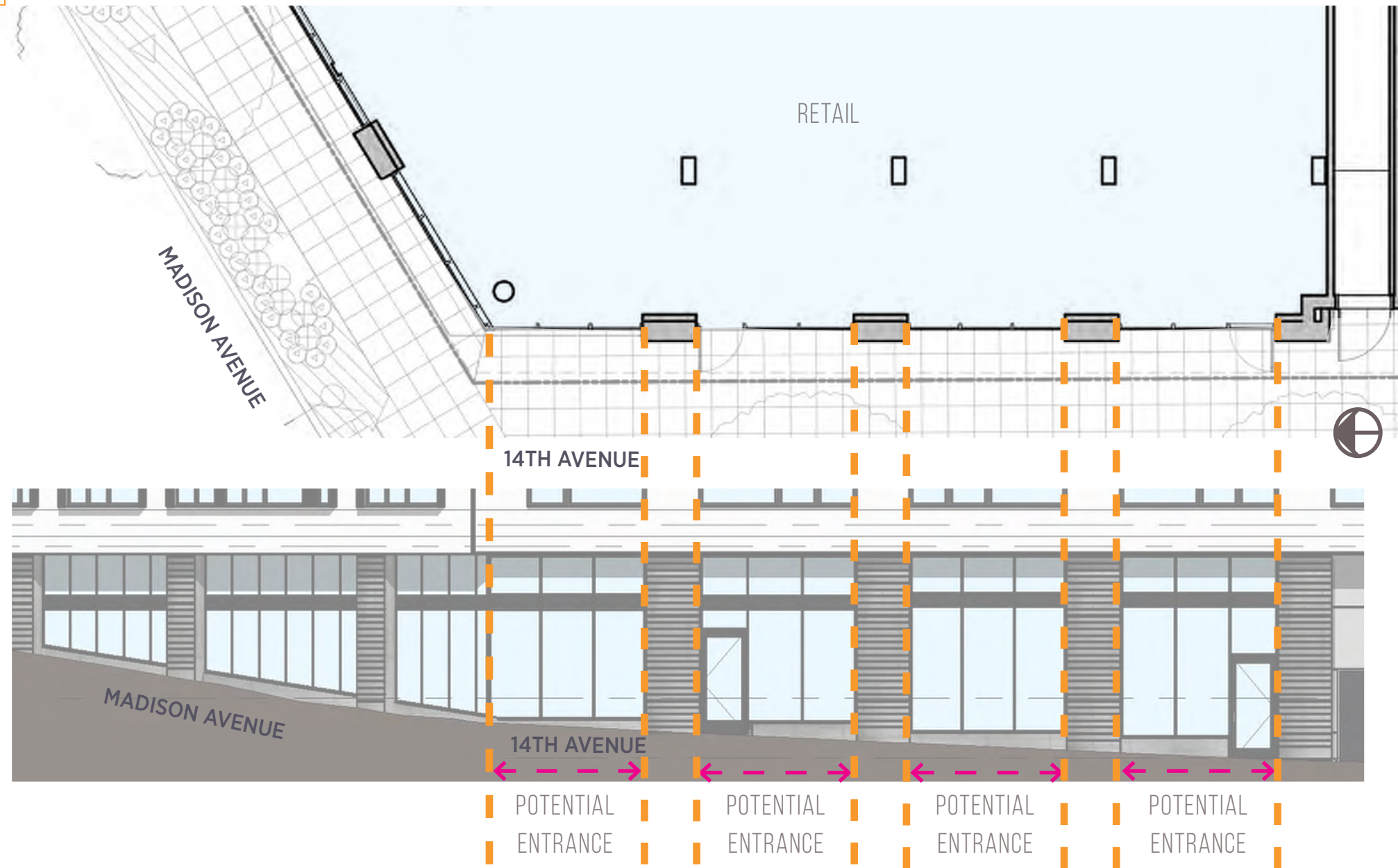


LOBBY ATRIUM VIEW TOWARDS PRIVATE MEWS

STOREFRONT OPTIONS

A. STRATEGIES FOR FLEXIBILITY

- STOREFRONT DIVIDED INTO SEVERAL "BAYS" TO ENCOURAGE A DIVERSITY OF TENANTS



STOREFRONT
OPTIONS



NANA WALL OPTION



GARAGE DOOR

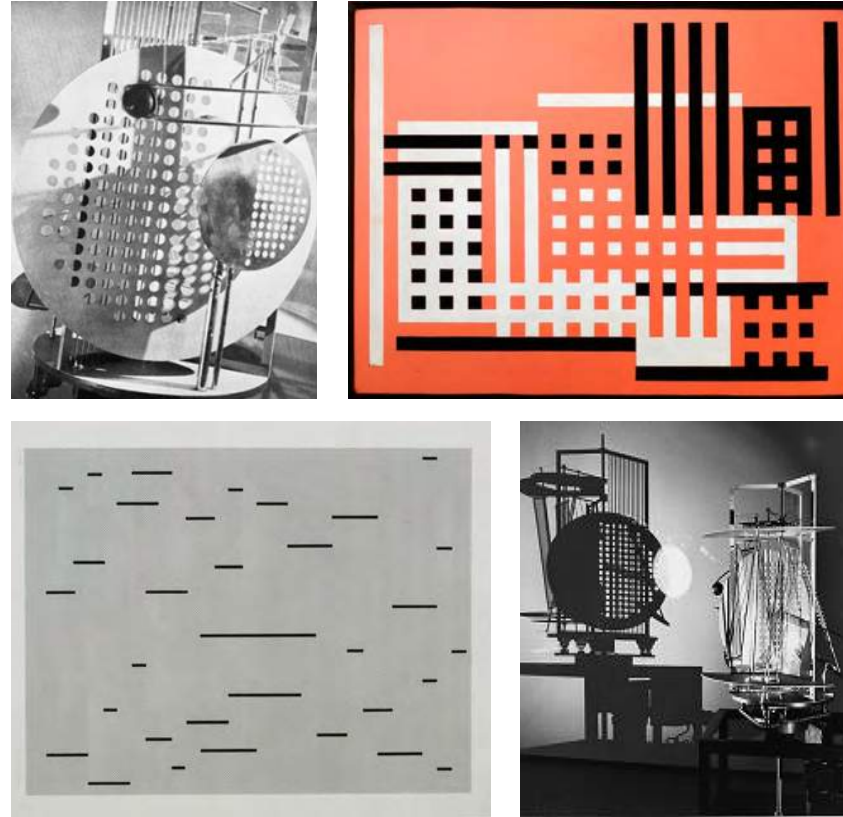


WOOD OPTION

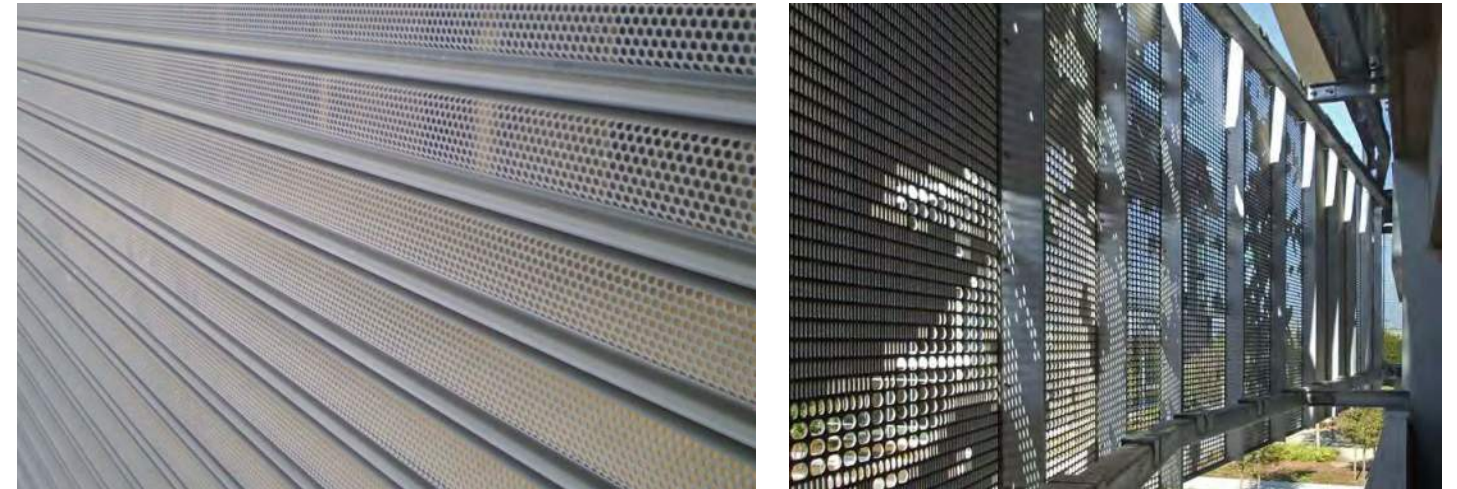


METAL OPTION

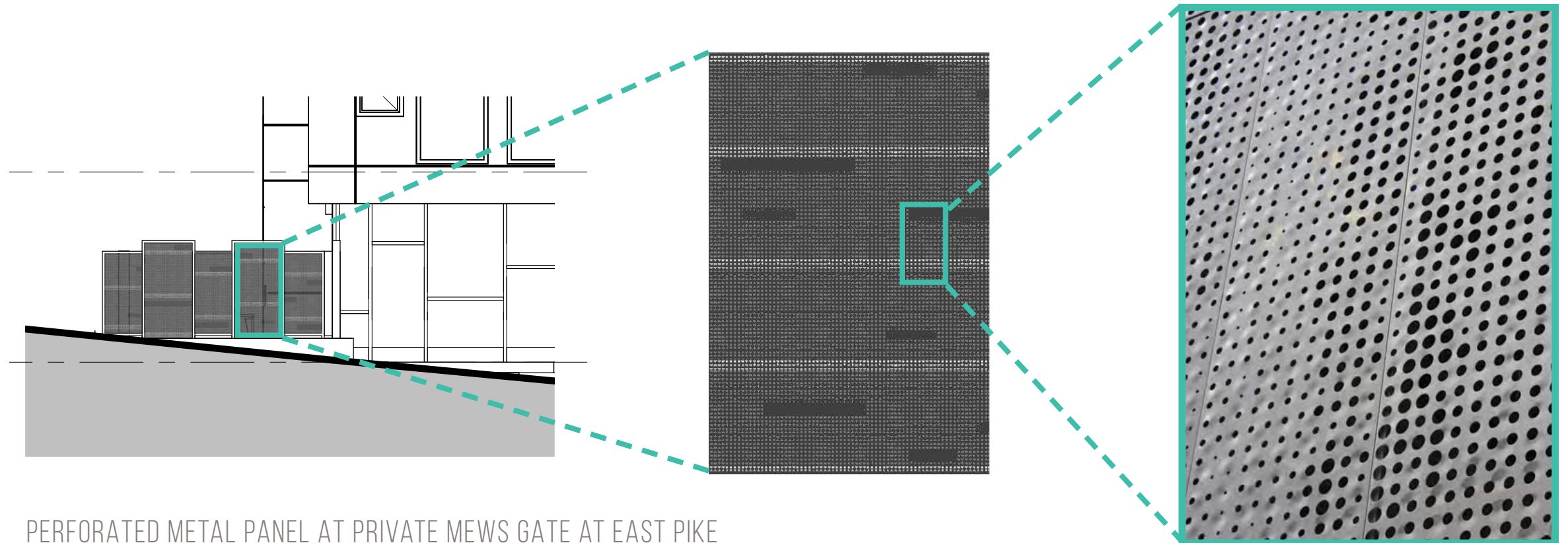
ART ELEMENTS



BAUHAUS DESIGN INSPIRATION

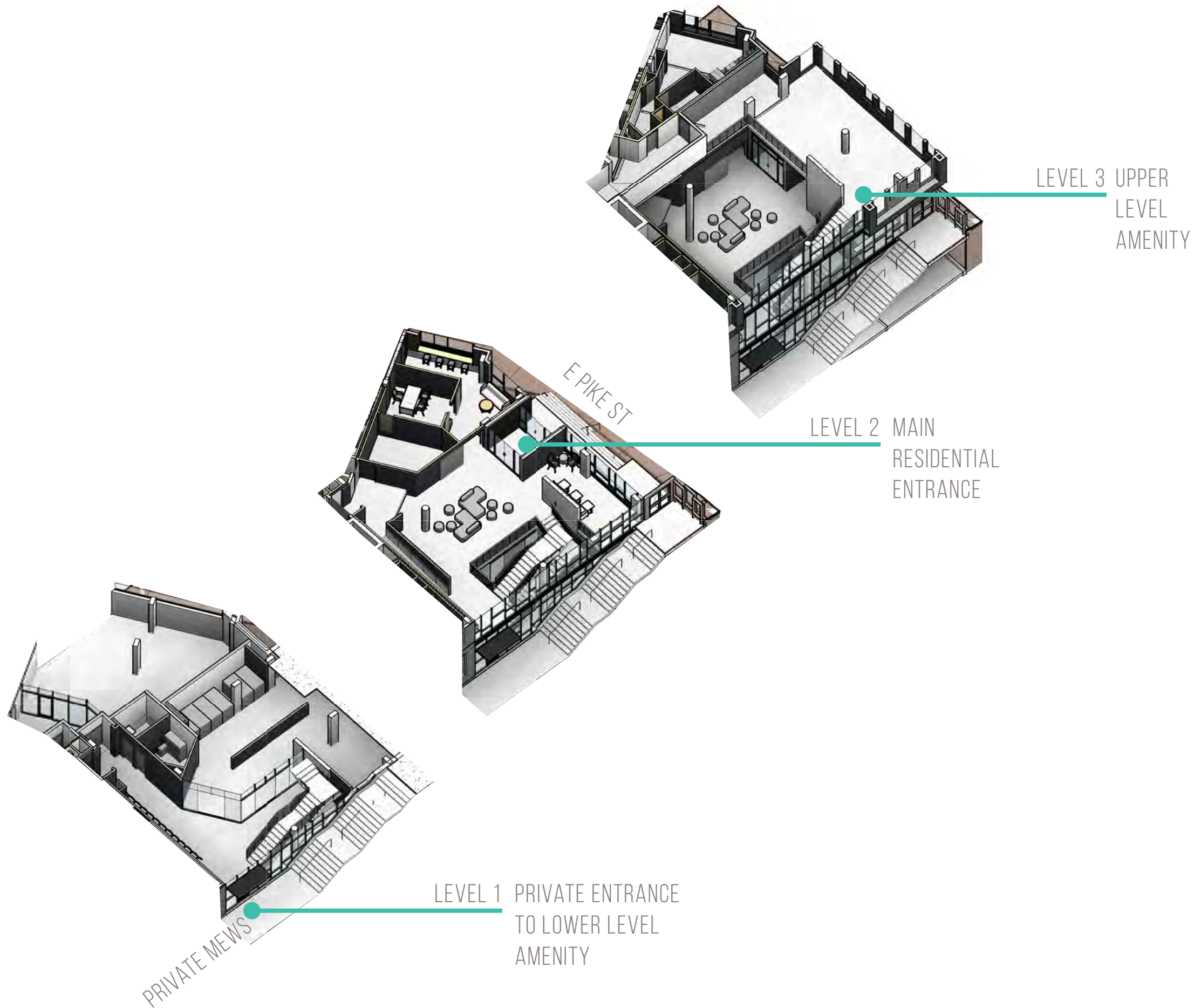


PERFORATED METAL PATTERN CONCEPTS

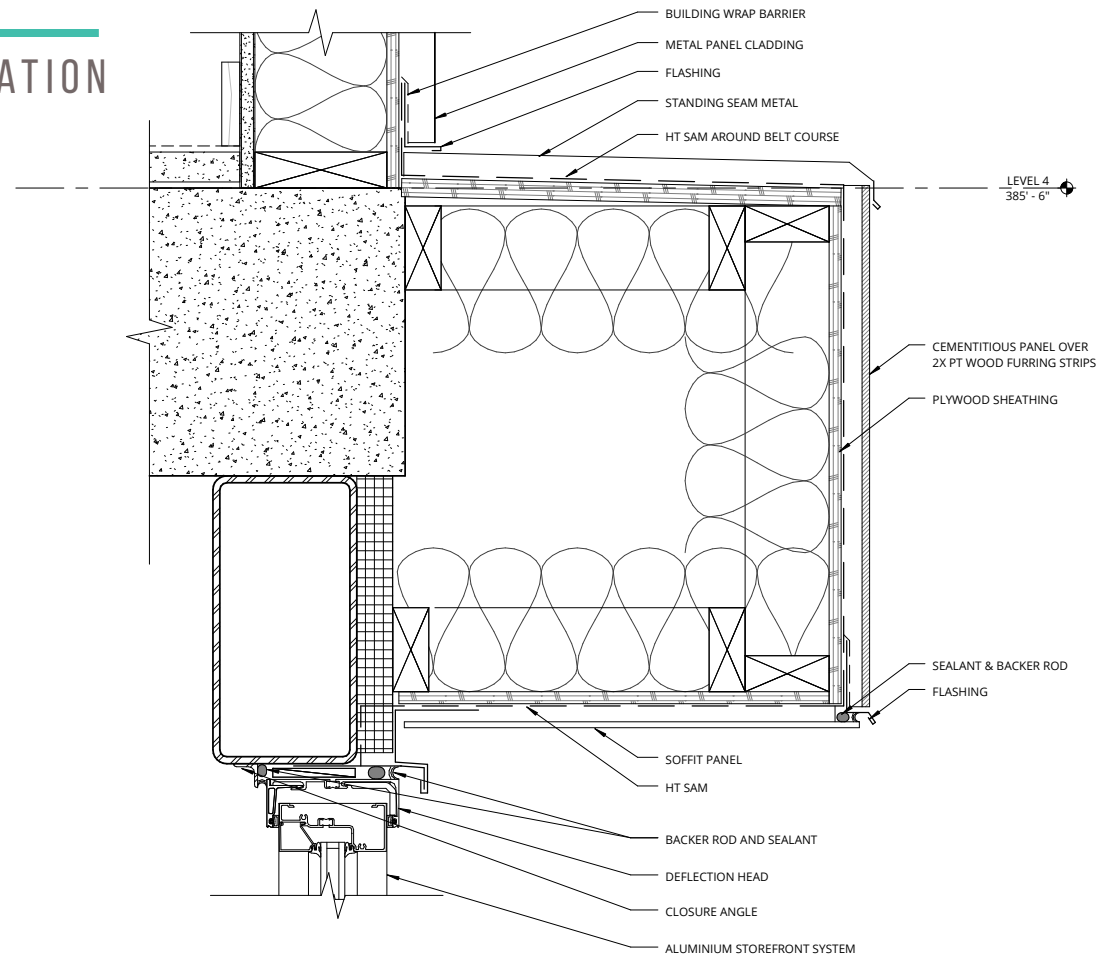


PERFORATED METAL PANEL AT PRIVATE MEWS GATE AT EAST PIKE

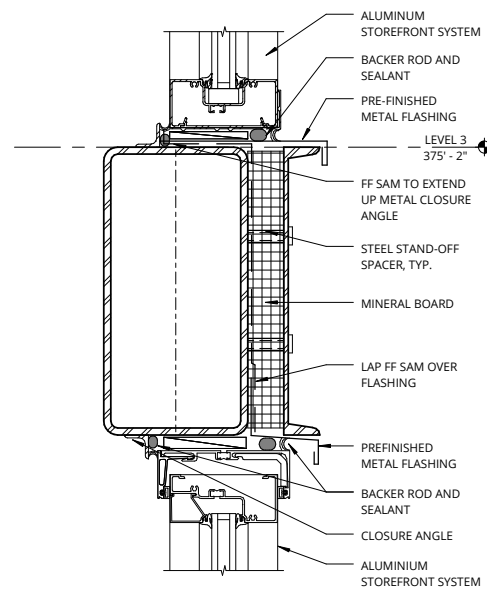
CIRCULATION
DIAGRAM



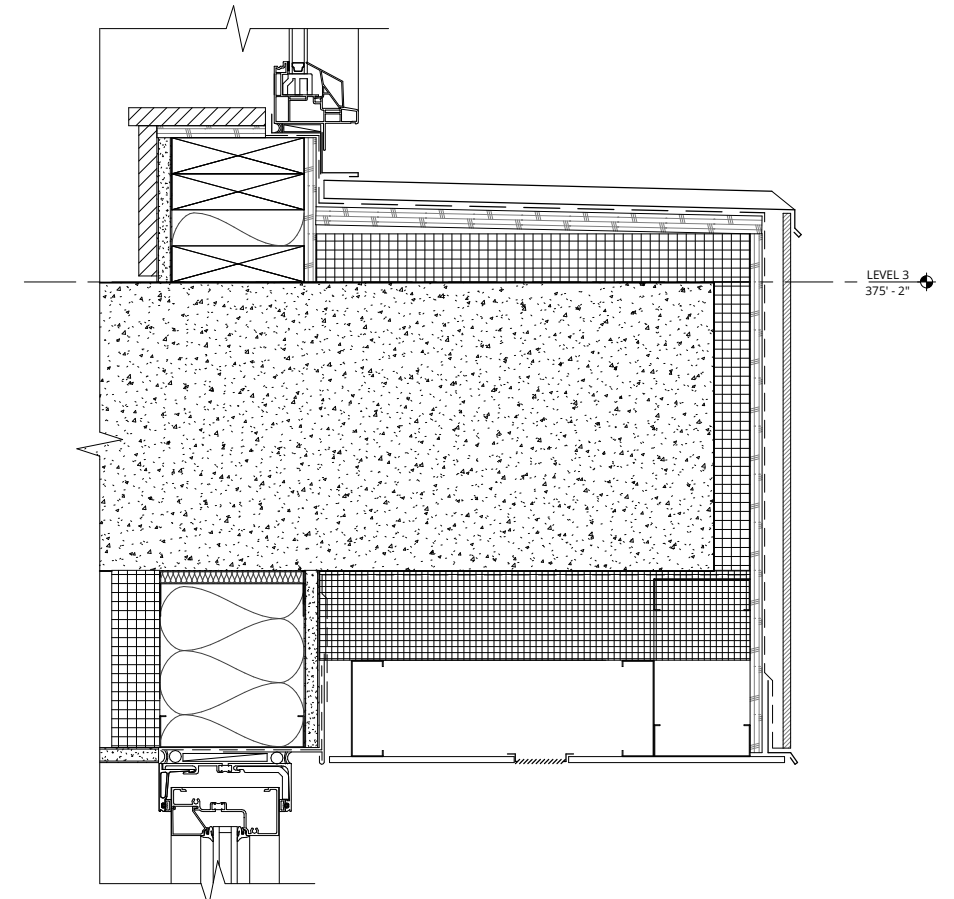
18.0 SUPPLEMENTAL INFORMATION



5 - Belt Course @ L4 - NTS

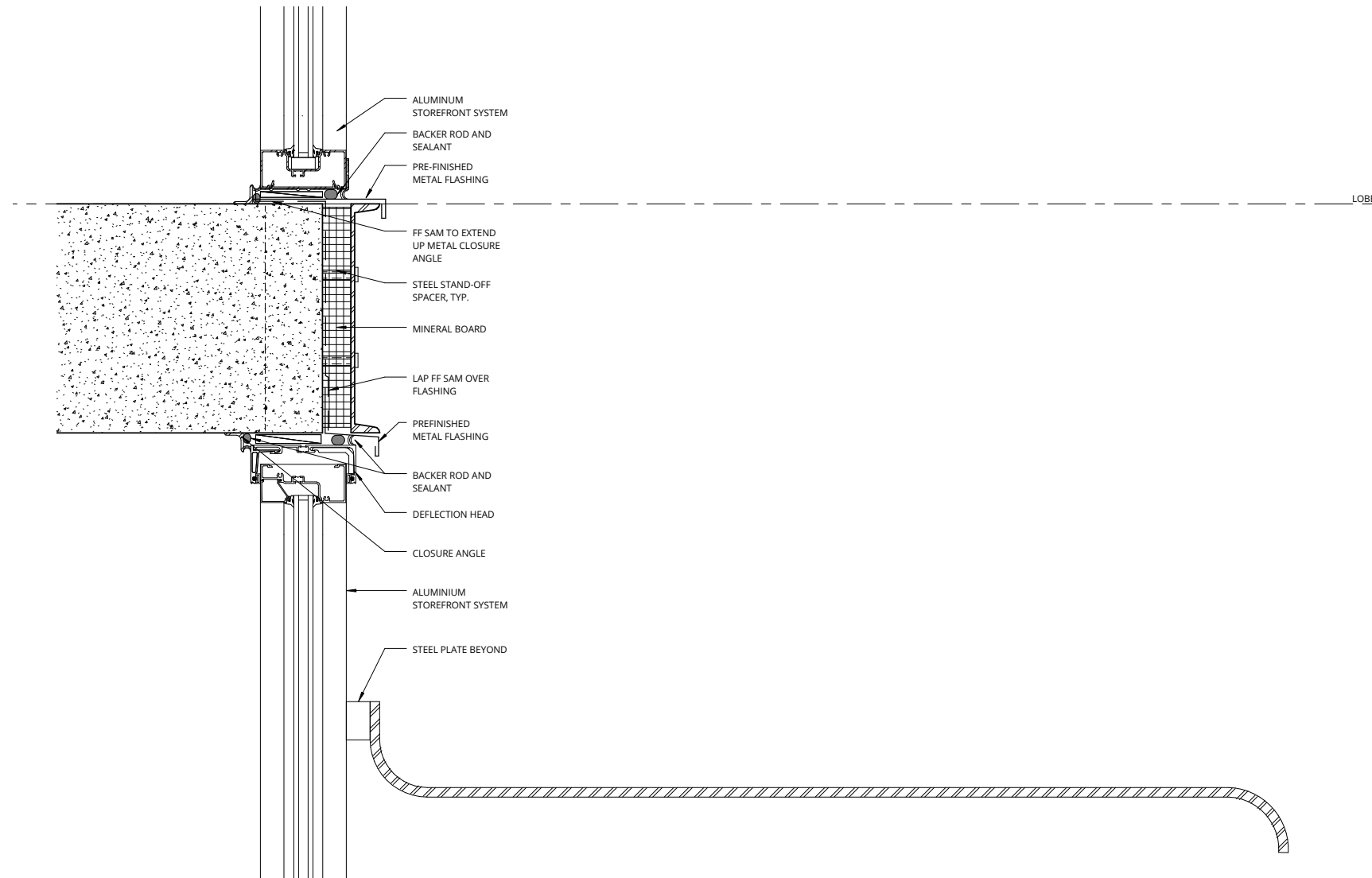


4 - Steel Storefront Band @ L3 Mews - NTS

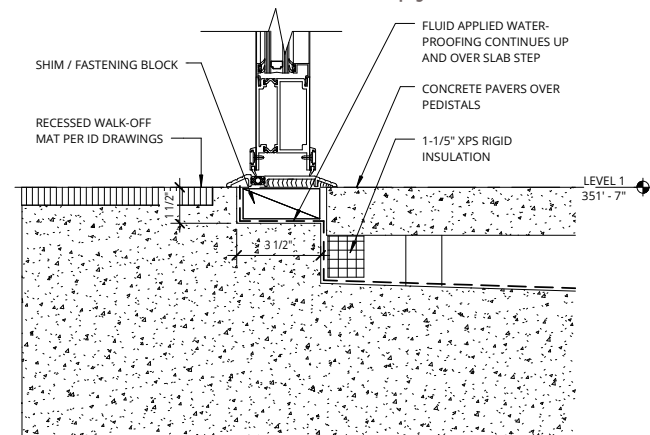


3 - Belt Course @ L3 - NTS

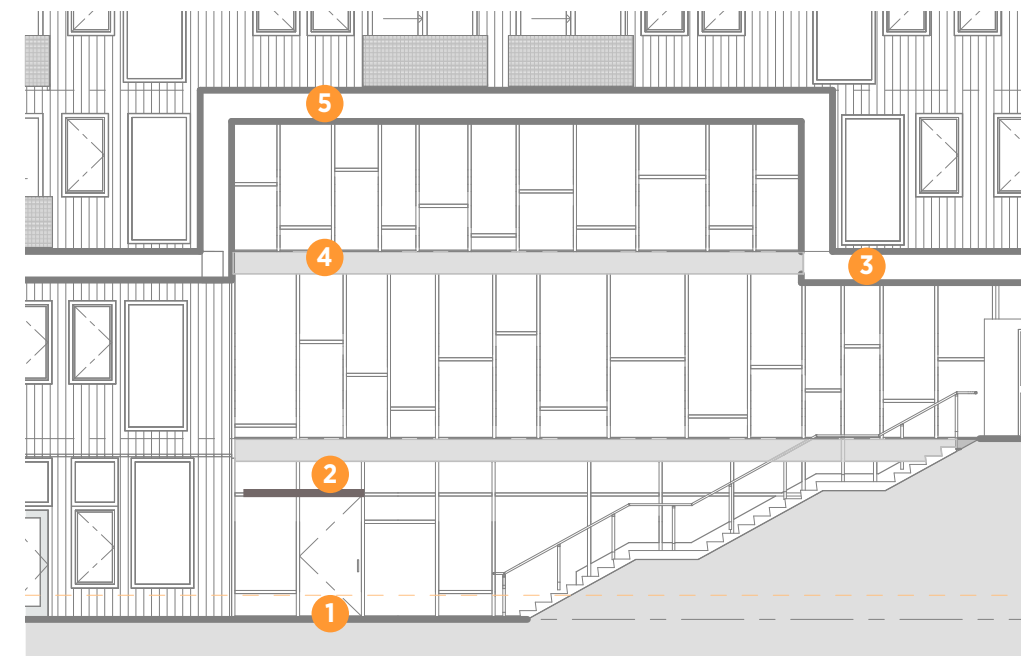
EXTERIOR DETAILING



2 - Steel Storefront Band and Canopy at Mews - NTS



1 - Threshold @ Mews Entry - NTS

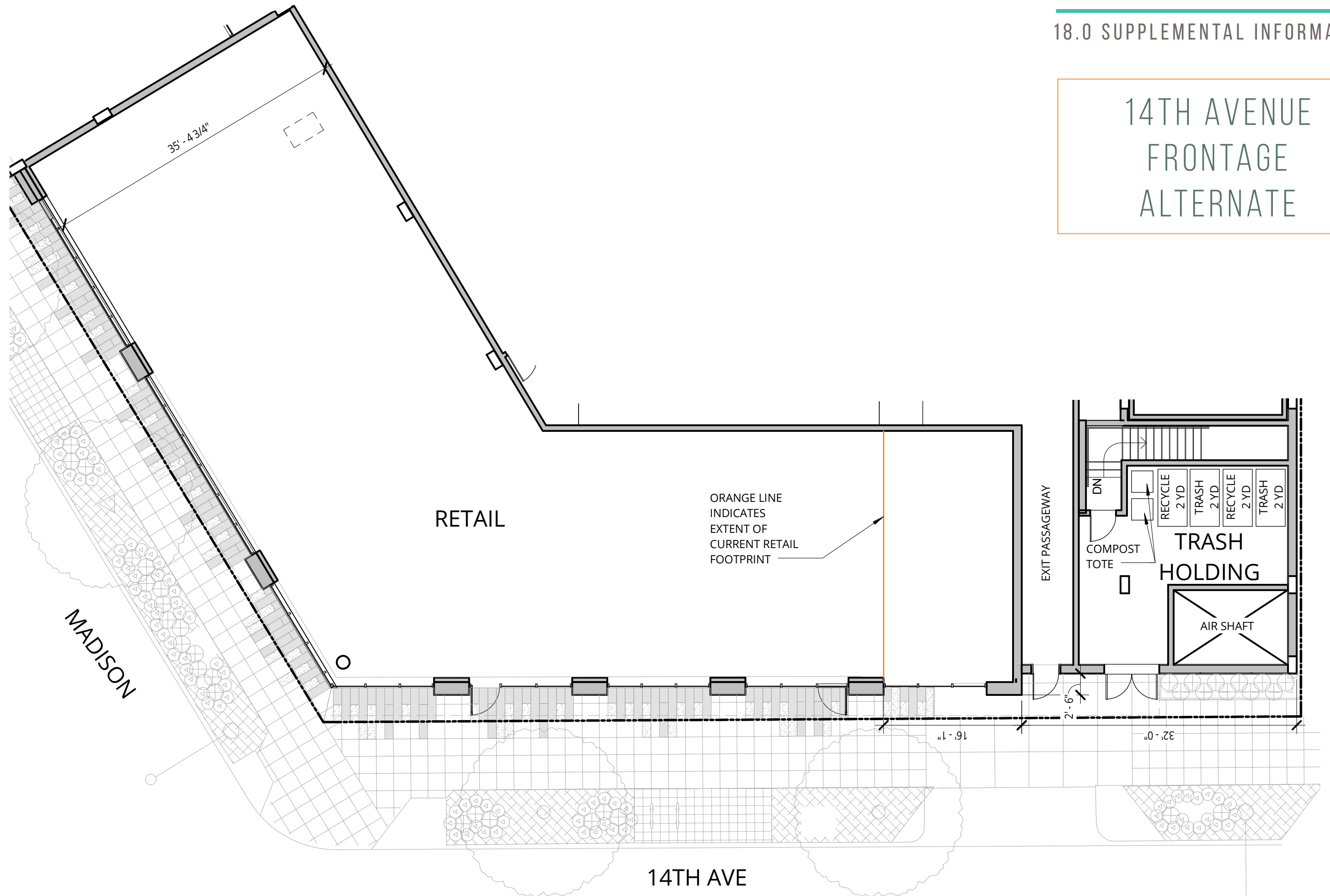


PRIVATE MEWS DETAIL LOCATION DIAGRAM

14TH AVENUE
FRONTAGE
ALTERNATE



14TH AVENUE
FRONTAGE
ALTERNATE



PORTLAND	SEATTLE
OFFICE	OFFICE
6720 SW MACADAM	1505 5TH AVE
SUITE 100	SUITE 300
PORTLAND, OR	SEATTLE, WA
9 7 2 1 9	9 8 1 0 1
503.245.7100	206.576.1600
ANKROMMOISAN.COM	