

1319 NE 65TH STREET: MIXED-USE BUILDING
DESIGN REVIEW BOARD MEETING • MARCH 28, 2016
1319 NE 65th Street • PROJECT NO. 3019953



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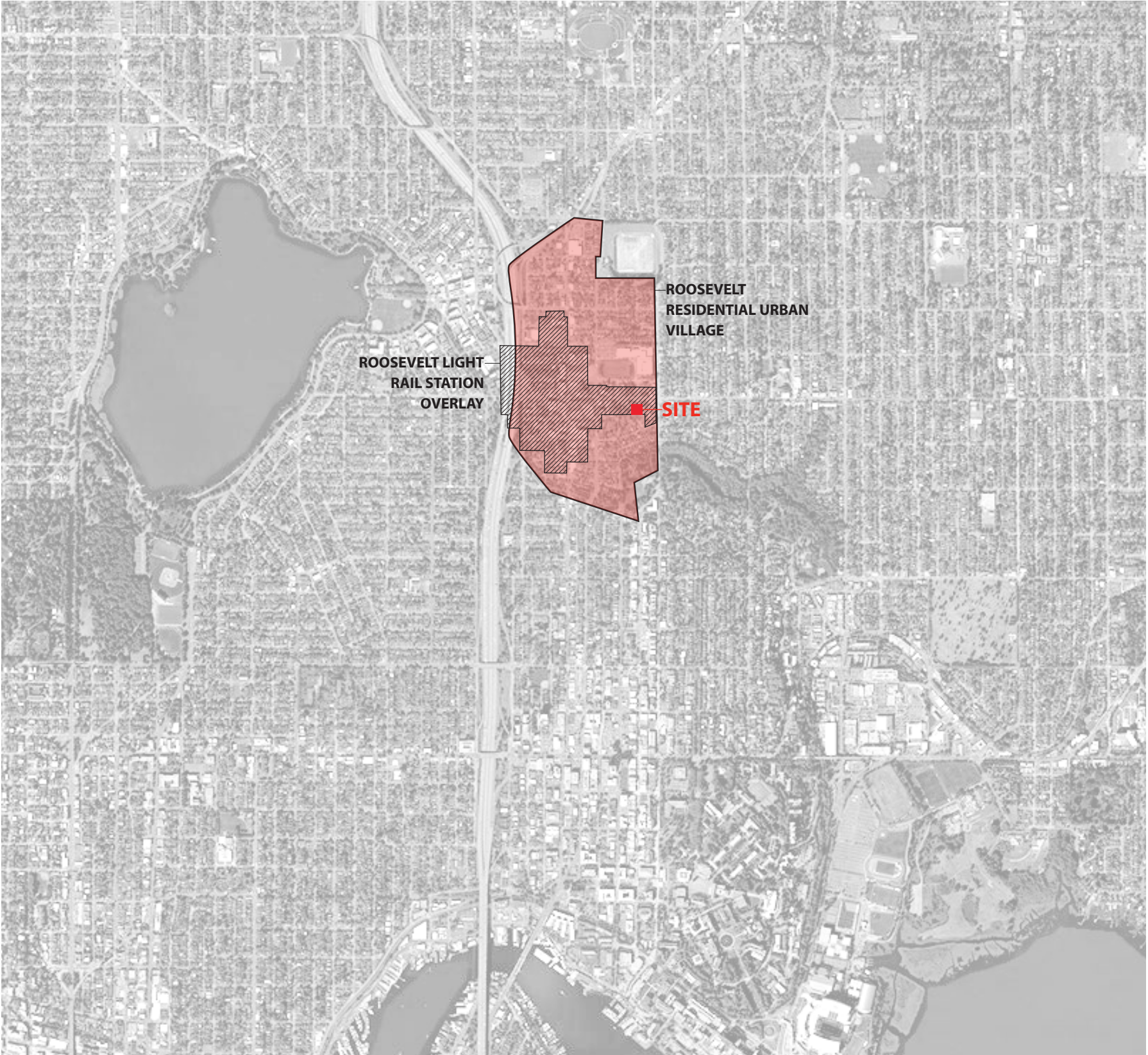
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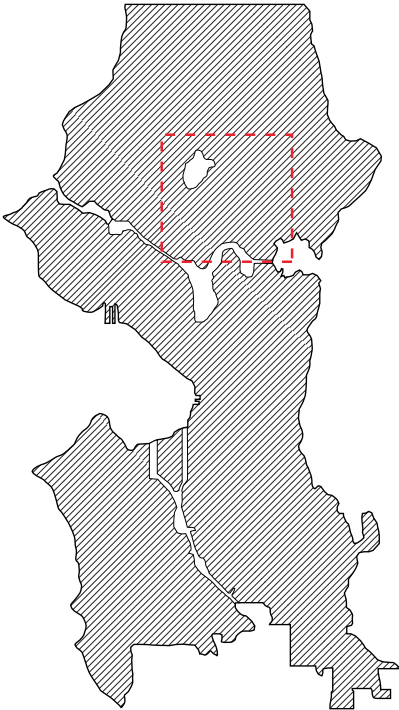
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Project Information

Property Address: 1319 NE 65th St, Seattle	
Owner:	Roosevelt Development Group LLC
Developer:	Roosevelt Development Group LLC Nick Miller (Contact) T (206) 812-8126
Architect:	Weinstein A+U LLC Adrienne Watkins (Contact) T (206) 443-8606



Site Context



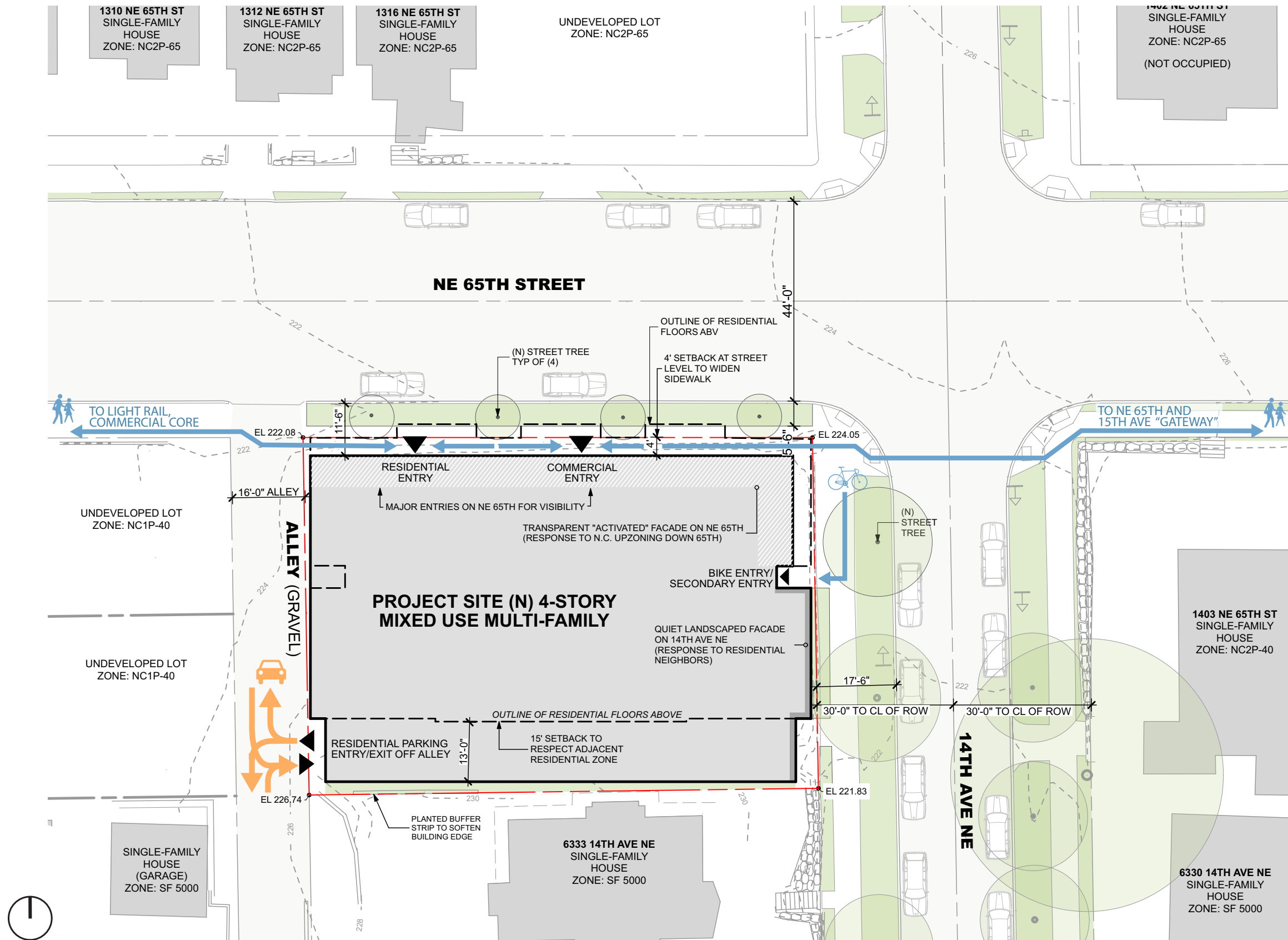
PROPOSAL & SITE CONTEXT

Location
The proposed project is in Seattle's Roosevelt Residential Urban Village on the SW corner of the intersection of NE 65th St and 14th Ave NE. The site is within a frequent transit corridor and the Roosevelt Station Overlay District for the incoming Light Rail Station (overlay shown outlined in red).

Existing Uses
The project site is composed of two parcels. Two wood-framed single-family structures, each with an uncovered parking stall off the alley, currently occupy the site.

- Future Link Light Rail Station
- 1. Roosevelt High School
- 2. Rising Sun Produce
- 3. Pies and Pints
- 4. Whole Foods
- 5. Pizza Hut
- 6. AutoPlus
- 7. Transit Oriented Development Sites
- 8. Cowen Park
- 9. Ravenna Park
- 10. Dwell Condominiums
- 11. Bartell Drugs, Wells Fargo, UPS Store
- 12. Tornado
- 13. Health Mutt
- 14. All-Star Cleaners
- 15. Qwest Communications
- 16. Future Development parcels
- 17. Future 7-Story Mixed-Use Project (MUP approval)
- 18. Future 7-Story Mixed-Use Project (MUP approval)





Setback Requirements

- A 15-ft setback for portions of structures above 13-ft in height to a max of 40-ft, and for each portion above 40-ft, an additional setback at the rate of 2-ft of setback for every 10-ft, is required where a lot abuts a side lot in a residential zone. This setback is from the south property line.
- A setback forming a triangular area 15-ft on a side is required where a lot abuts the intersection of a side lot line and front lot line of a lot in a residential zone. This setback is from the southeast corner of the property.
- High-voltage overhead power lines that cross NE 65th at the northwest corner of the property and extend down the alley along the western property line would require a setback, but the project proposes to underground the lines.

Traffic and Circulation

- NE 65th is a minor arterial/comm. connector street with time-limited parking to the south, and unrestricted parking to the north.
- NE 65th serves a number of bus lines
- NE 65th is a principal pedestrian street.
- 14th Ave NE is a residential with minimal traffic and resident parking on both sides of the street.
- There are no curb cuts on the site, and a mid-block alley off NE 65th can serve vehicular access needs.

Streetscape

- NE 65th St has a narrow sidewalk (+ 7'-6") and lacks street trees on either side of the street. There are infrequent pedestrian crosswalks (and closest crosswalks are at 12th Ave NE and 15th Ave NE).
- 4th Ave NE has wide tree-lined sidewalks (+ 17'-6") with generous planting strips at the curb edge. Street trees are well established.

Neighborhood Influences

- NE 65th St is zoned to become commercial. Locate major building entries, and commercial programs on this street.
- 14th Ave NE is residential and quiet. Avoid adding activity to this street to preserve privacy.

Building Footprint	
Street Parking Area	
Pedestrian Entry/Exit	
Parking Entry/Exit	

DESIGN ALTERNATE SUMMARY



ALTERNATE 1 (L-SHAPE)

Summary
Stories: 4 (3-over-1 + 1 below grade)
Unit Count: 41 (6,160 unit SF/ Floor)
Floor Area: 30,834-SF Total
Parking: 7 stalls (residential)
Ground Floor Uses:
• NE 65th St: Commercial / Residential Amenity
• 14th Ave NE: Residential Lobby & Amenity
• Alley: Parking Access

Potential Departures:
• Increased percentage of residential use at street level in NC Zones (NE 65th & 14th Ave NE)
• Reduced width of overhead weather protection on principal pedestrian streets (< 6-ft)
• Reduced non-residential use depth at street level
• Structure in triangular setback abutting a residential lot
• Structure in south PL setback above 13-ft
• Reduced parking lot aisle width

Pros
• Setback façade at street level widens pedestrian zone on NE 65th st.
• No curb cuts required - parking entry is provided off alley, separating cars and pedestrians.
• L-shape configuration at upper levels maximizes unit sizes.

Cons
• Lack of setback or step-down of massing at south property line imposes a greater visual impact on neighboring residential property.
• The 2nd floor terrace residential amenity will be overshadowed in the morning by north-south tower
• Location of multifamily entrance on 14th will draw more activity and noise down 14th, which may not be desired by residents of the quiet street.
• L-shape Configuration will require a number of Departures.



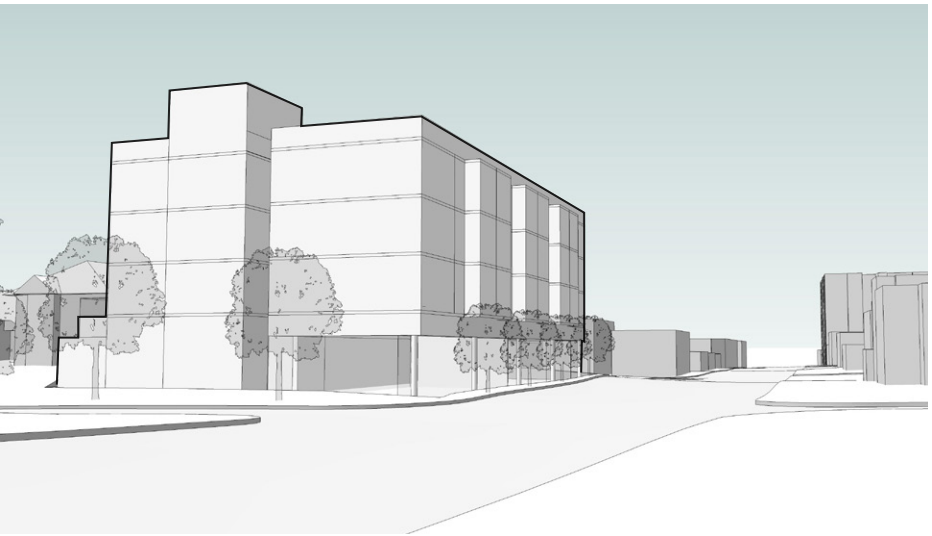
ALTERNATE 2 (NO DEPARTURES)

Summary
Stories: 4 (3-over-1 + 1 below grade)
Unit Count: 40 studios (5,401 unit SF/ Floor)
Floor Area: 29, 389-SF Total
Parking: 7 stalls (residential)
Ground Floor Uses:
• NE 65th St: Commercial / Residential Lobby
• 14th Ave NE: Live-Work Units
• Alley: Parking Access

Potential Departures:
None

Pros
• No curb cuts required - parking entry is provided off alley, separating cars and pedestrians.
• Upper level setbacks at south property line create a gradual transition to the adjacent low-height residential zone.
• No Departures required.

Cons
• Without a setback façade at street level, the narrow 7'-6" pedestrian right-of-way cannot be widened, and there is no space to add street trees in the narrow curb planting strip.
• 14th Ave NE is a quiet single-family residential street. Live-work units that draw noise and activity to the street may not be a good fit for the neighbors.
• Respect of the 15-ft triangular side yard setback creates a dark corner at the back of the site, with little solar access due to neighboring property's raised rockery, and little visibility to passersby.



ALTERNATE 3 (PREFERRED)

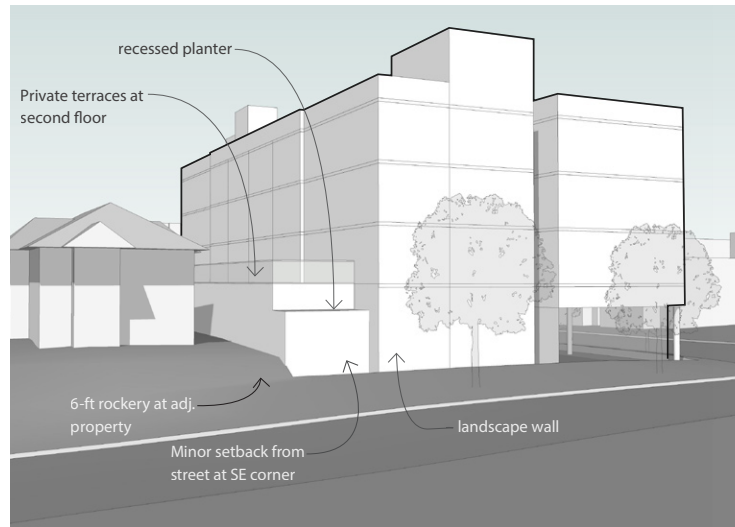
Summary
Stories: 4 (3-over-1 + 1 below grade)
Unit Count: 41 studios (5,864 unit SF/ Floor)
Floor Area: 29,627 SF Total
Parking: 8 stalls (residential)
Ground Floor Uses:
• NE 65th St: Commercial / Residential Lobby
• 14th Ave NE: Commercial (at corner) / secondary residential bike entry
• Alley: Parking Access

Potential Departures:
• Increased percentage of residential use at street level in NC Zones (NE 65th)
• Reduced street level transparency on 14th Ave NE
• Reduced width of overhead weather protection on principal pedestrian streets (< 6-ft)
• Reduced non-residential use depth at street level
• Structure in triangular setback abutting a residential lot
• Garage door opening closer than 5-ft to a residential lot
• Reduced parking lot aisle width

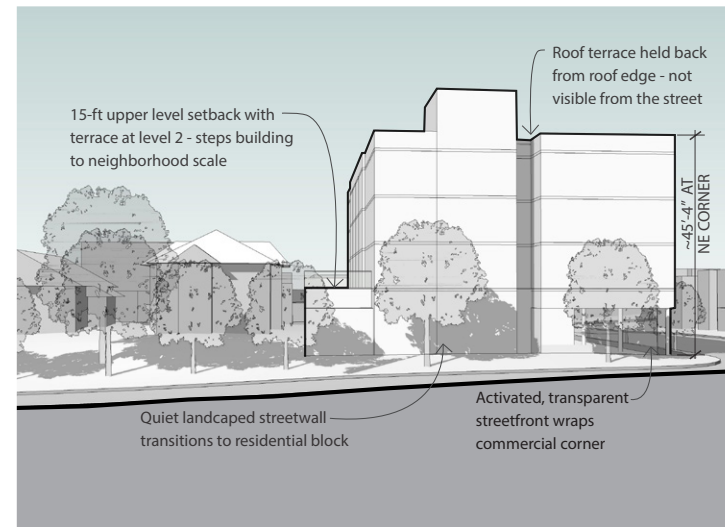
Pros
• Street level setback on NE 65th widens pedestrian zone and creates space for street trees
• No curb cuts required - parking entry is provided off alley, separating cars and pedestrians.
• Location of residential entry at the west end of NE 65th minimize distance to the new Light Rail Station for commuter residents.
• Location of all major building entrances on NE 65th increases their visibility to the public, and preserves 14th Ave NE as a quiet, private residential street.
• Upper level setbacks at south property line create a gradual transition to the adjacent low-height residential zone.
• A shallow setback in plan at the south-east corner respects the intent of the Code's 15-ft triangular setback to respond to residential neighbors, but does not create a deep enough space to invite unwanted activity, as Alternate 2 might.

Cons
• Departures required at street level for setbacks, street level uses, transparency, and parking

EDG APPROVED CONCEPT



Southeast Corner (14th Ave NE)



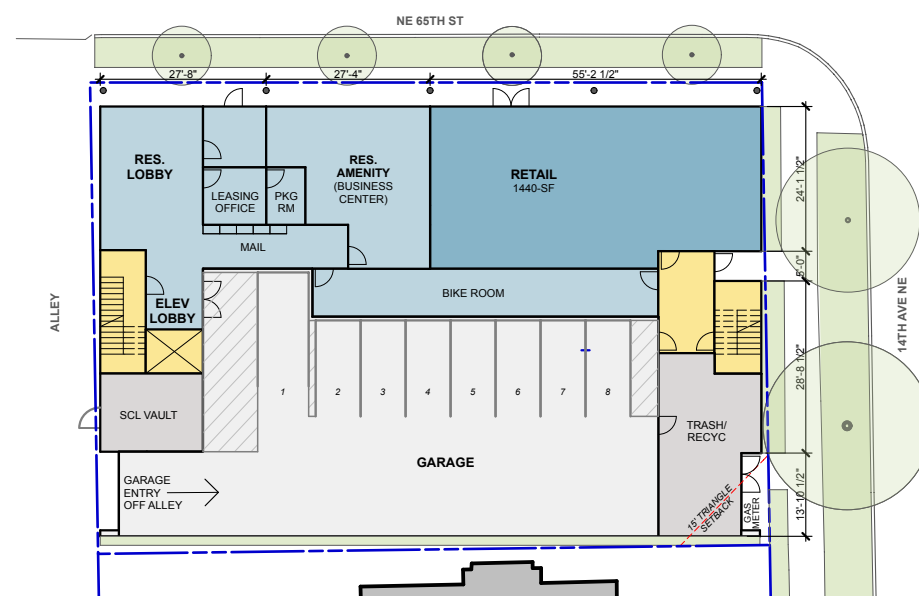
East Elevation (14th Avenue NE)



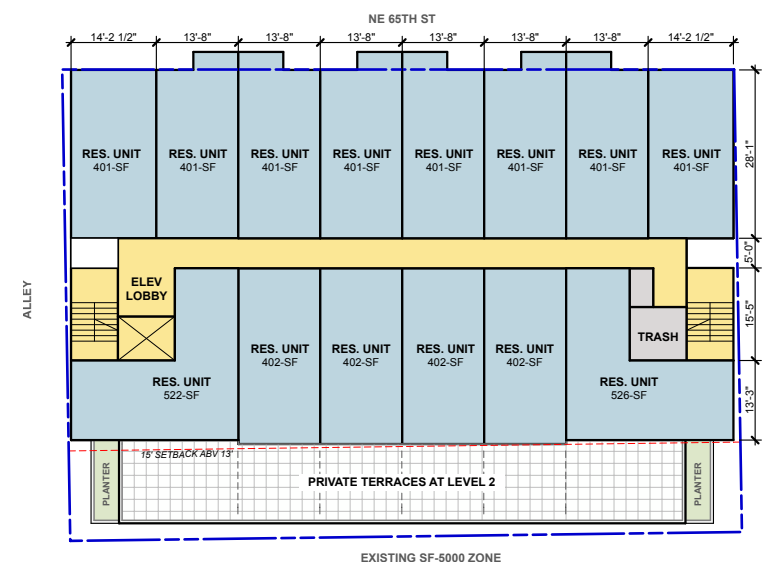
Northwest Corner (NE 65th St)

Summary

- Open commercial space on NE corner of site activates 65th Street and 14th Ave NE
- Residential lobby anchors NW corner, oriented towards future light rail station
- Alternating projecting bays and street trees on 65th Street provide massing modulation and rhythm
- Parking access off of alley



Street Level Plan (Level 1)



Typical Residential Plan (Level 2)

CURRENT MASSING & SUMMARY OF EDG GUIDANCE



The bullets points to the right summarize the guidance provided to the design team by the Board at the Early Design Guidance meeting on July 13, 2015. The guidance has been grouped into categories that appear on the right side of the following two-page spreads with a brief description of how the design has been developed in response to the Board’s guidance.

- 1

Zone Transition and Perceived Mass / Presence
 - Unanimous support for Preferred Option, a bar building massing fronting NE 65th St with a step down to a second level terrace to the south
 - Board felt double sided corridor with gaskets at each end providing natural light was successful and should be maintained
 - Board noted constraints due to the size of the site zone transition and acknowledged that the second level terraces to the south provide some setback and scale transition to neighboring single-family.
 - Board expressed concern with perceived scale of the building adjacent to single-family and directed the applicant to explore strategies to minimize perceived mass and screen views.
- 2

Privacy / Landscape screening: 2nd Level Terrace
 - Board supported the depth of the terraces as a buffer to the single family neighbor to the south.
 - Board requested cross sections with sightlines, perspectives, and renderings showing what the spaces will look like from the existing private garden to the south.
 - Board requested applicant demonstrate how the proposed landscape will remain successful over time, including details on the landscaping including installation and a long-term maintenance plan.

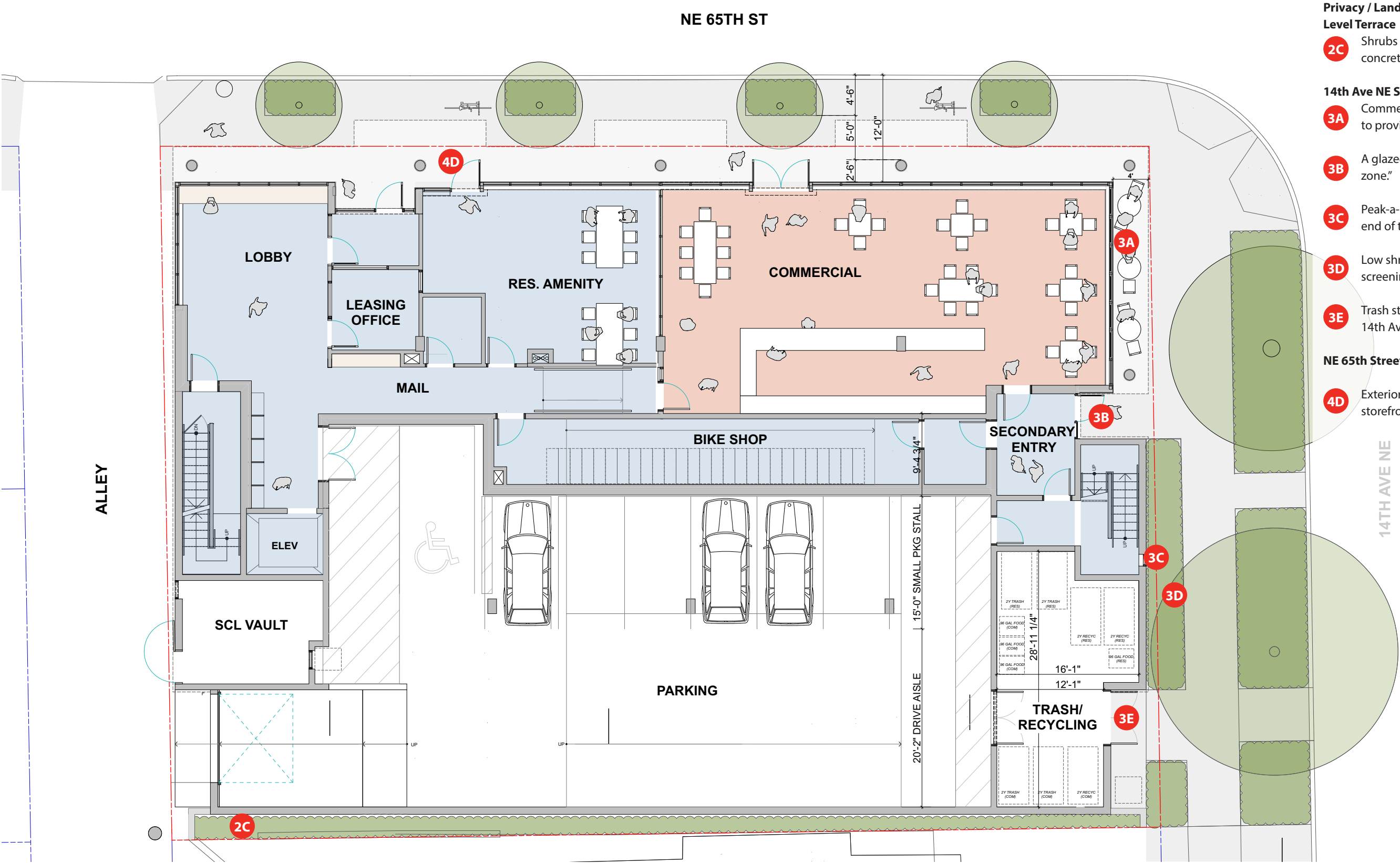


- 3

14th Ave NE Streetscape
 - Board generally supported the requested structure within a required 15-ft triangular setback at residential lots, noting it was a better design solution for safety, security and streetscape conditions.
 - Board recongnized the potential for successful transition between the proposed commercial space and the single family residential zone.
 - Board noted that some activity is needed at this location to mitigate the negative impacts of a blank facade at ground level.
 - Board directed applicant to explore a secondary lobby or entry at this location.
 - Board requested additional information on the rationale for garbage at the 14th Ave NE location as opposed to the alley.
- 4

NE 65th Street Streetscape and Alley
 - Board supported the requested overhead weather protection departure, supporting the 4-ft setback at street level along NE 65th St
 - Board preferred street trees over additional weather protection.
 - Board strongly supported the bays along NE 65th St, stating they would provide additional weather protection.
 - Board requested additional information on the siting of the residential entry, solid waste storage area and alley.
 - Board indicated preliminary support for the requested use percentage departures on NE 65th St,
 - Board indicated preliminary support for the requested non-residential use depth departure but requested the appplicant explore increasing the depth, though not at the expense of the 4-ft sidewalk setback.

STREET LEVEL PLAN



Privacy / Landscape screening: 2nd Level Terrace

2C Shrubs and vines screen the concrete parking garage wall.

14th Ave NE Streetscape

3A Commercial space is set back 4-ft to provide potential cafe spill-out.

3B A glazed entry door to "mixing zone."

3C Peak-a-boo slot window at the end of the residential stair.

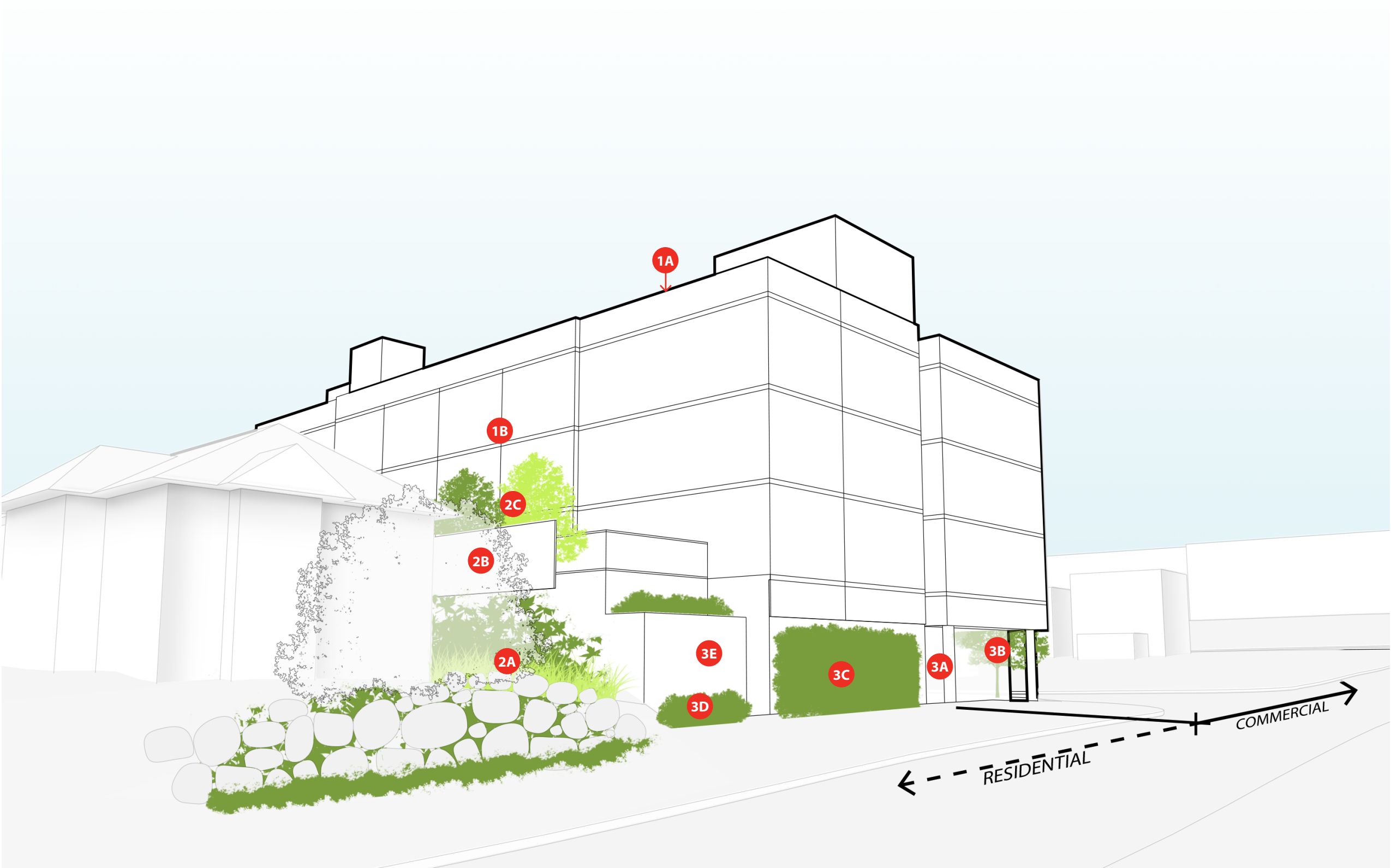
3D Low shrubs and taller wall-screening bamboo

3E Trash storage room as required on 14th Ave NE by SPU.

NE 65th Street Streetscape and Alley

4D Exterior door activates NE 65th St storefront.

ZONE EDGE



ZONE EDGE

Zone Transition and Perceived Mass / Presence

- 1A Lowered parapet reduces perceived mass.
- 1B Gaskets at corridor provides natural light.
- 1C Roof terrace is centrally located, to avoid “overlook” vantages.
- 1D Penthouses are minimized to the extent possible

Privacy / Landscape screening: 2nd Level Terrace

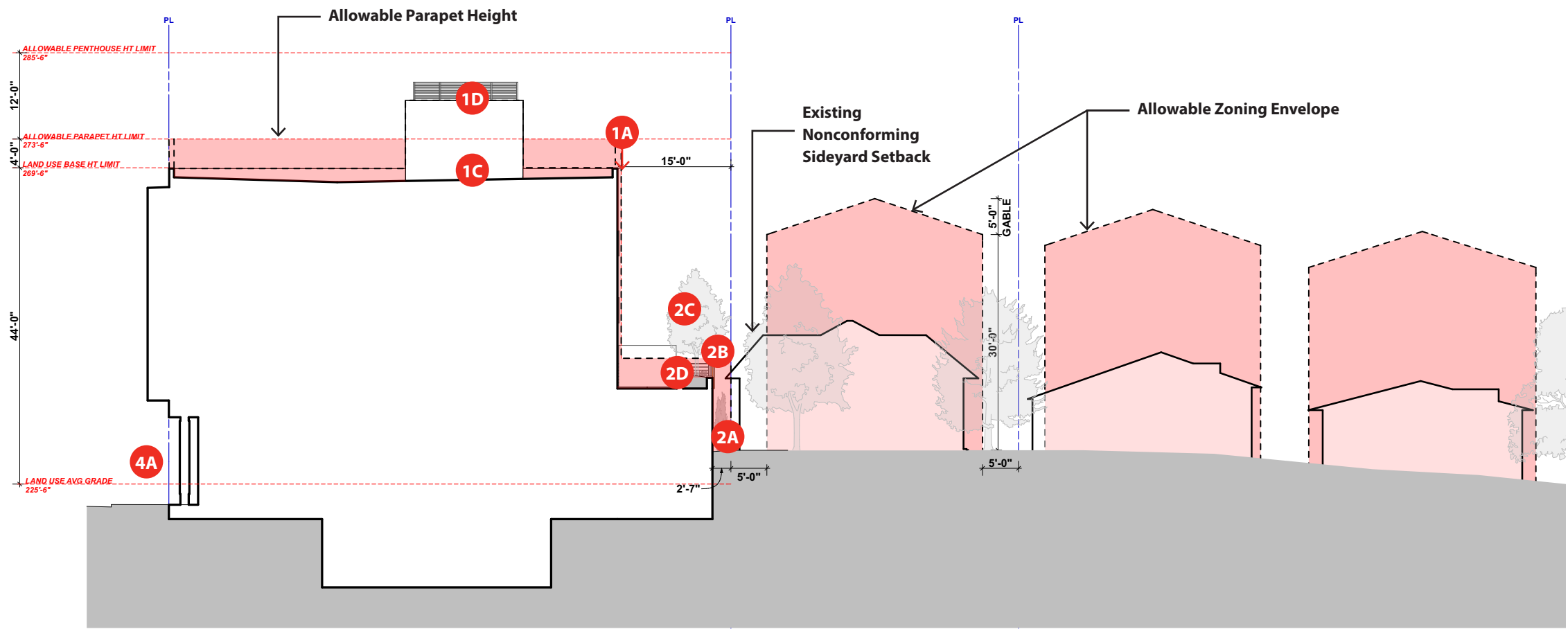
- 2A Trees at terraces provide buffer to south neighbor
- 2B Wood fences ensures year-round visual separation.
- 2C Shrubs and vines screen the concrete parking garage wall.

14th Ave NE Streetscape

- 3A Commercial space is set back 4-ft to provide potential cafe spill-out.
- 3B A glazed entry door to entry “mixing zone.”
- 3C Peak-a-boo slot window at the end of the residential stair.
- 3D Low shrubs and taller wall-screening bamboo
- 3E Trash storage room as required on 14th Ave NE by SPU.

NE 65th Street Streetscape and Alley

- 4A 4-ft setback at street level

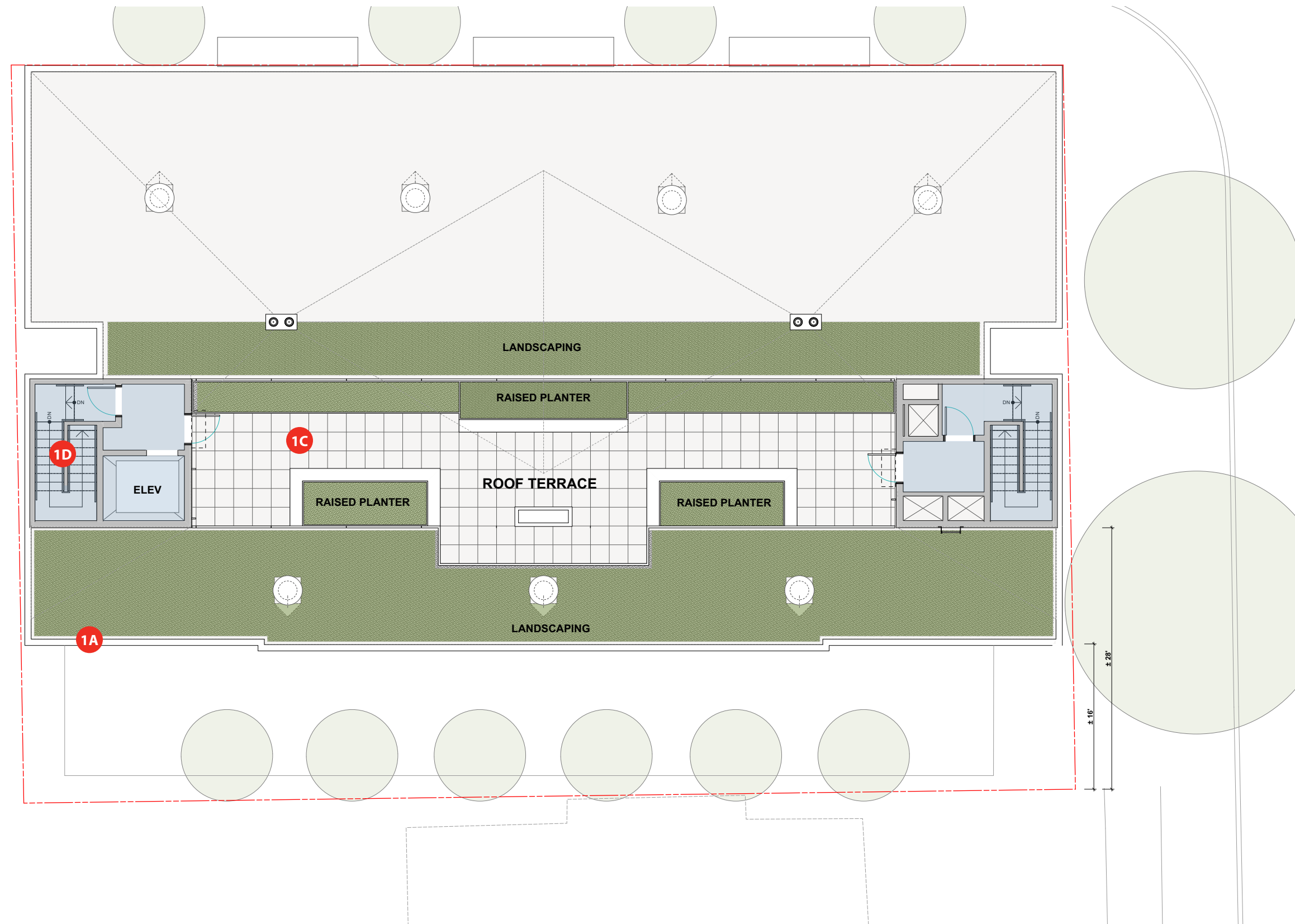


North-South Section Cut Indicating Allowable Massing Envelope versus Proposed Massing

TYP RESIDENTIAL PLAN



ROOF PLAN



Zone Transition and Perceived Mass / Presence

- 1A** Lowered parapet reduces perceived mass.
- 1B** Gaskets at corridor provides natural light.
- 1C** Roof terrace is centrally located, to avoid "overlook" vantages.
- 1D** Penthouses are minimized to the extent possible

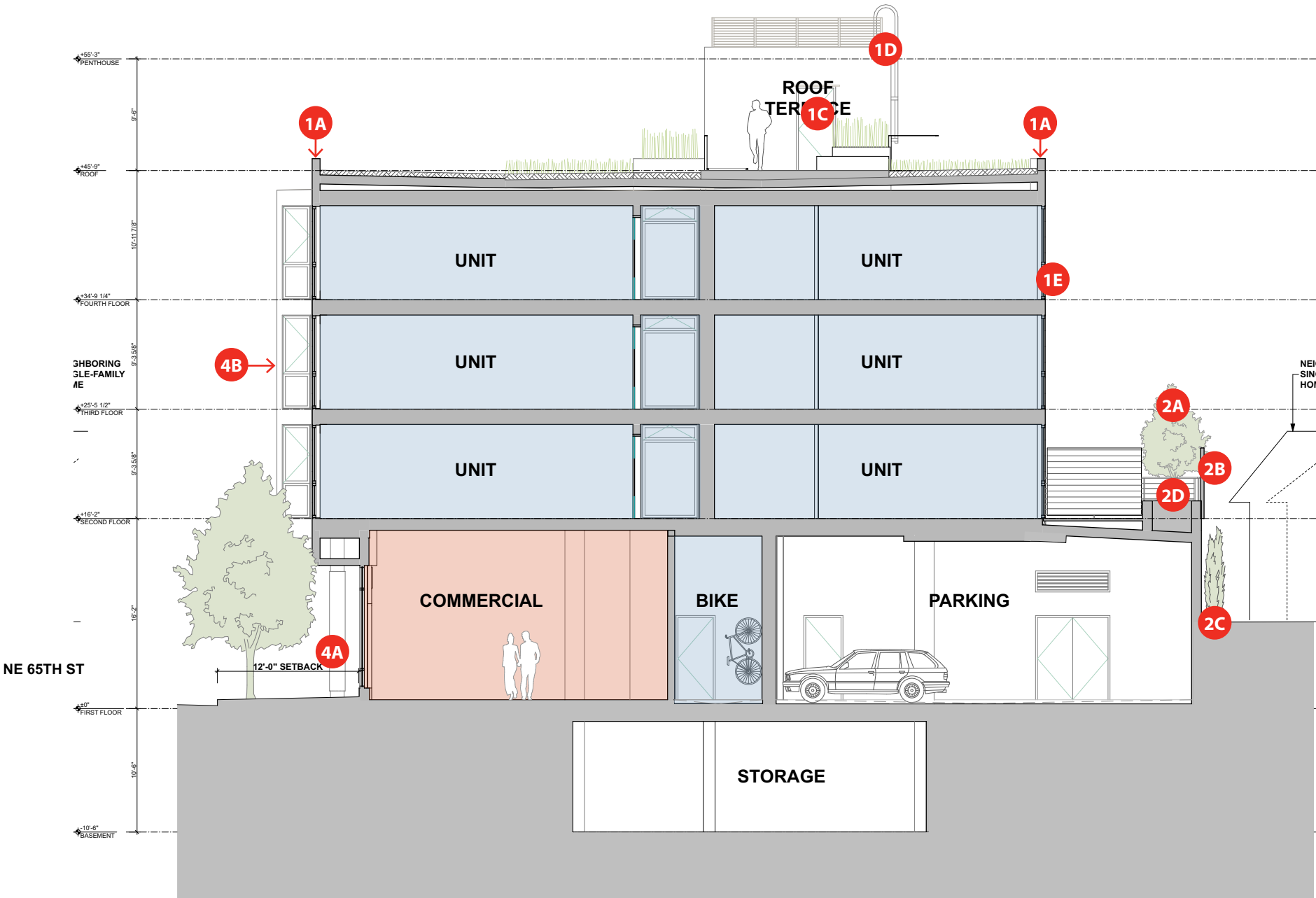
Privacy / Landscape screening: 2nd Level Terrace

- 2A** Trees at terraces provide buffer to south neighbor
- 2B** Wood fences ensures year-round visual separation.
- 2D** Raised planters keep residents 5-ft back from the terrace edge
- 2E** Seatwall at planter directs user views back towards the building

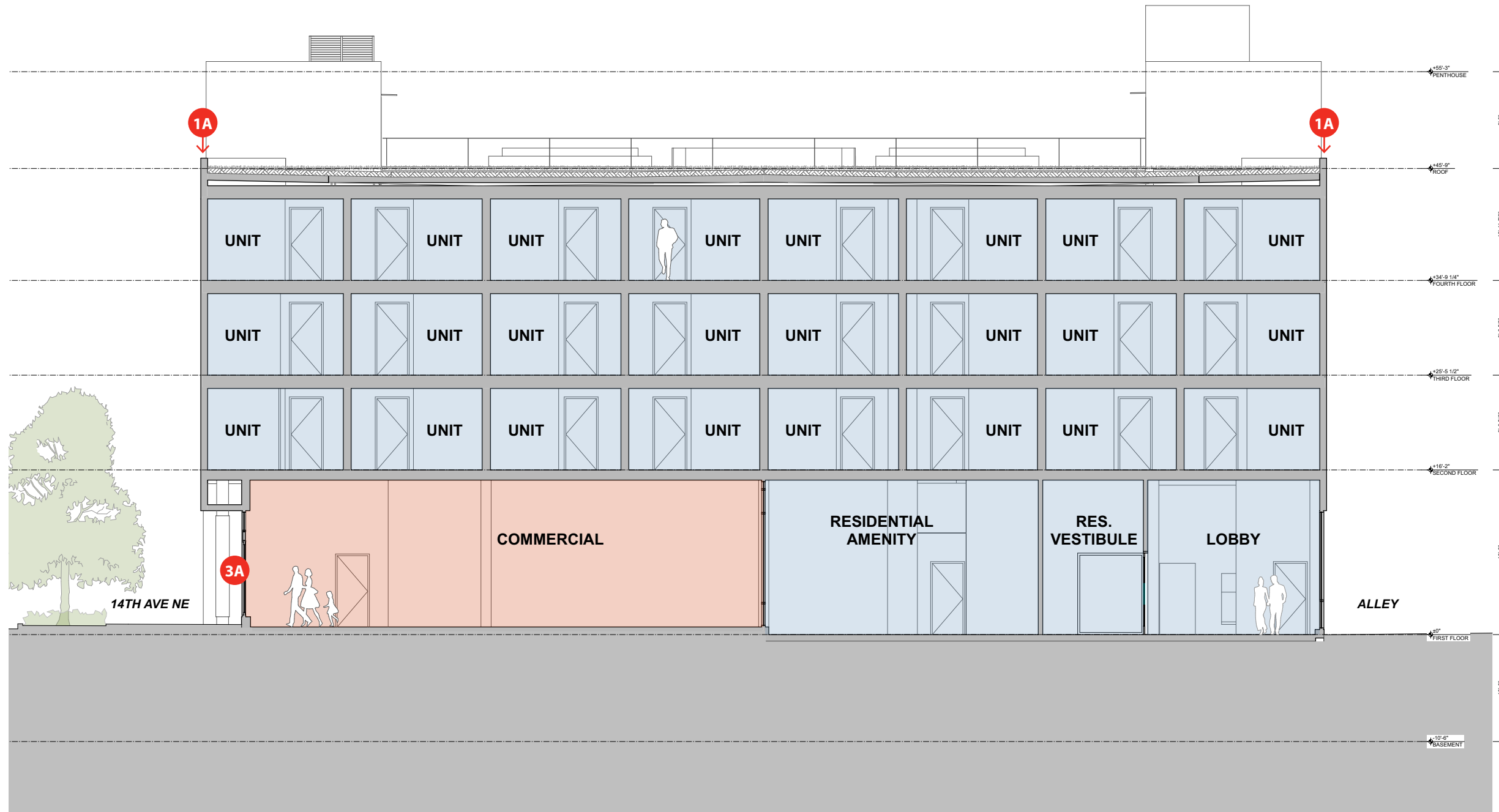
NE 65th Street Streetscape and Alley

- 4B** Projecting bays provide modulation and visual interest.

N/S SECTION



E/W SECTION



Zone Transition and Perceived Mass / Presence

- 1A** Lowered parapet reduces perceived mass.
- 1D** Penthouses are minimized to the extent possible
- 1E** Juliet balconies removed.

Privacy / Landscape screening: 2nd Level Terrace

- 2A** Trees at terraces provide buffer to south neighbor
- 2B** Wood fences ensures year-round visual separation.
- 2C** Shrubs and vines screen the concrete parking garage wall.
- 2D** Raised planters keep residents 5-ft back from the terrace edge

14th Ave NE Streetscape

- 3A** Commercial space is set back 4-ft to provide potential cafe spill-out.

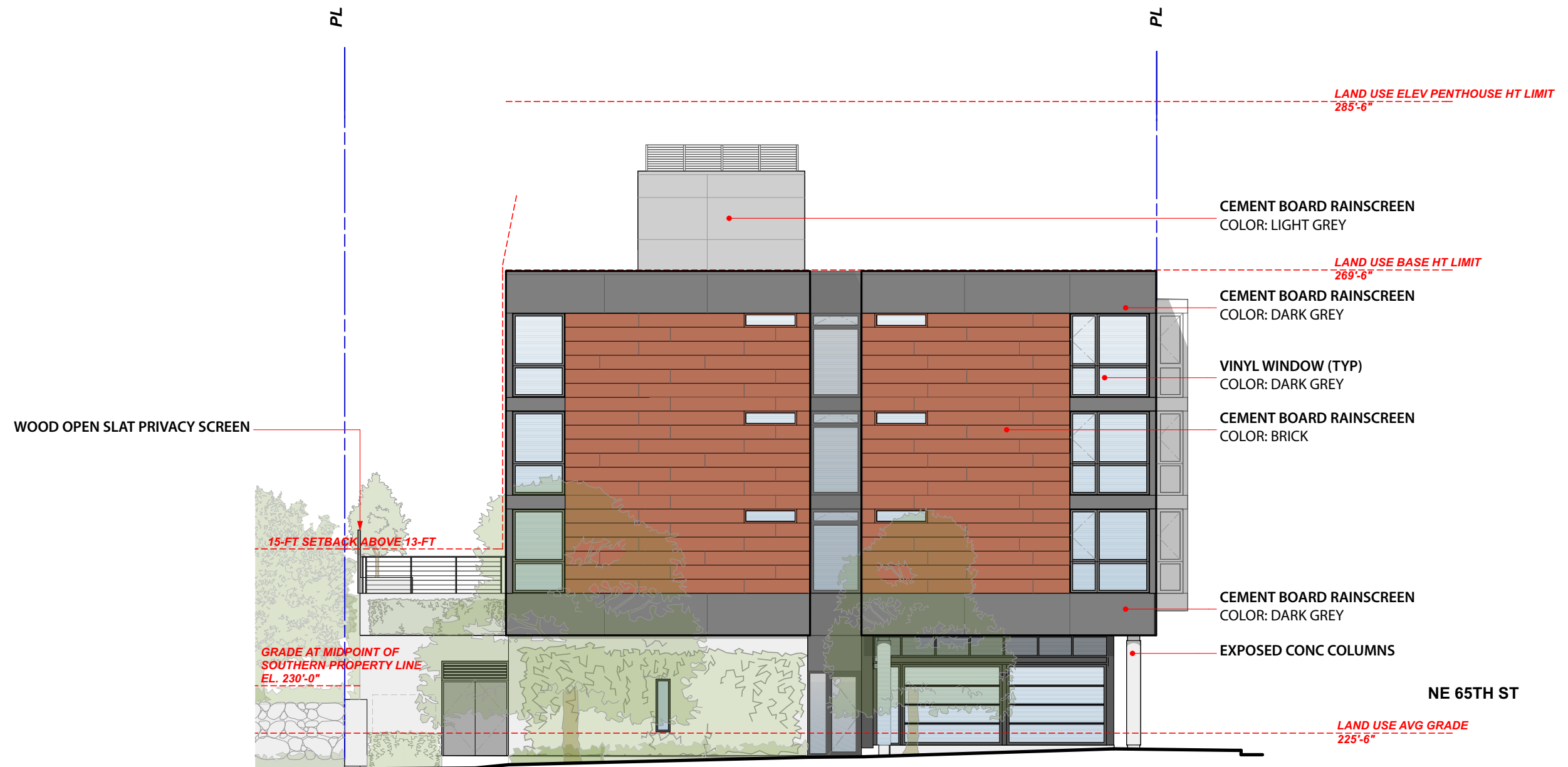
NE 65th Street Streetscape and Alley

- 4A** 4-ft setback at street level
- 4B** Projecting bays provide modulation and visual interest.

NORTH ELEVATION (NE 65TH ST)



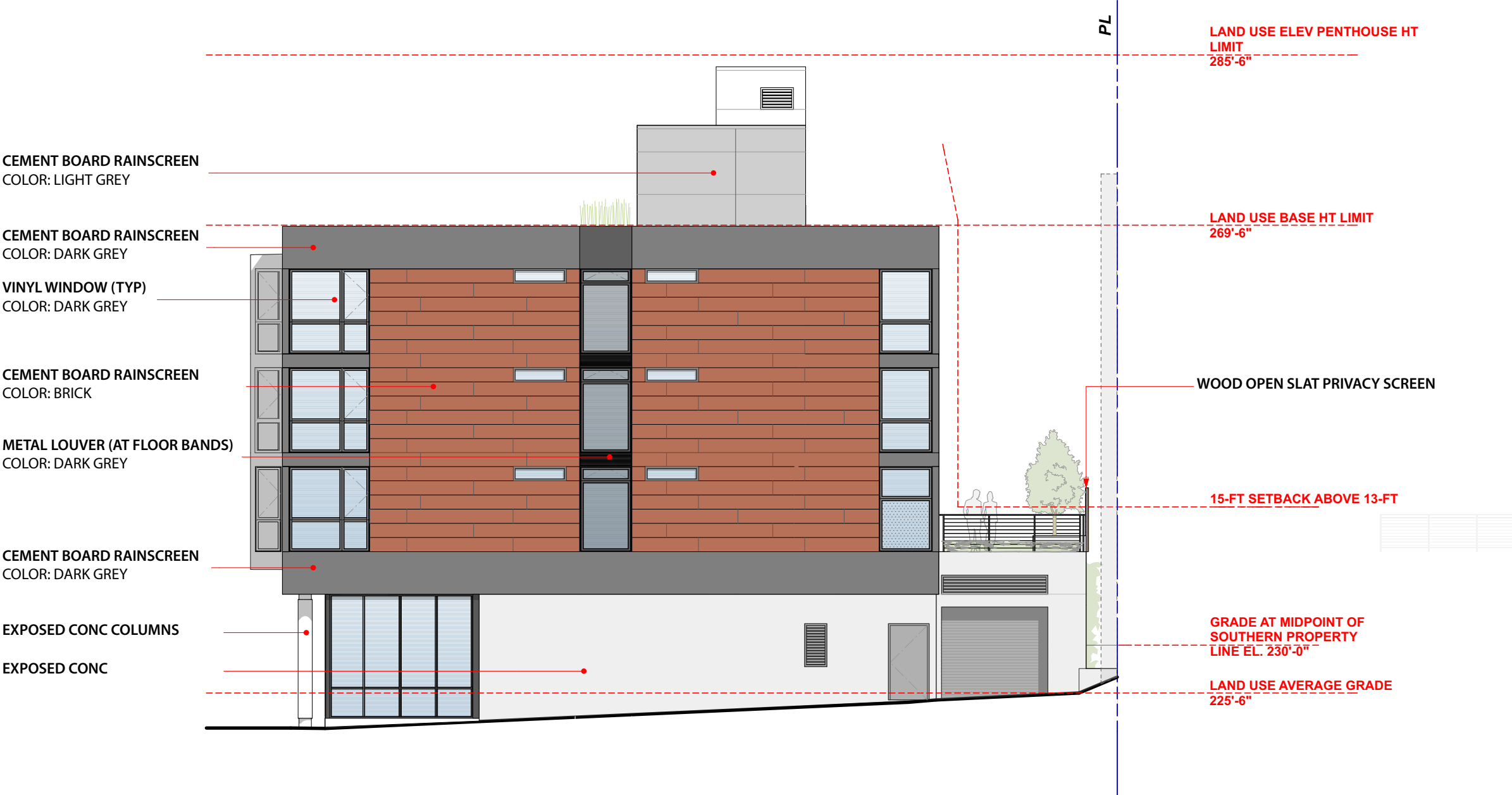
EAST ELEVATION (14TH AVE NE)



SOUTH ELEVATION



WEST ELEVATION (ALLEY)



DESIGN PROPOSAL: MATERIAL PALETTE



Partial North Elevation (65th Street)

CEMENT BOARD RAINSCREEN (PENTHOUSE)
COLOR: LIGHT GREY

CEMENT BOARD RAINSCREEN
COLOR: DARK GREY

CEMENT BOARD RAINSCREEN:
COLOR: BRICK

VINYL WDW (TYP)
COLOR: DARK GREY

METAL LOUVER (AT FLOOR BANDS)
COLOR: DARK GREY

BRAKE METAL TRIM (AT BAY WDWS)
COLOR: BLACK (TO MATCH VINYL WDW)

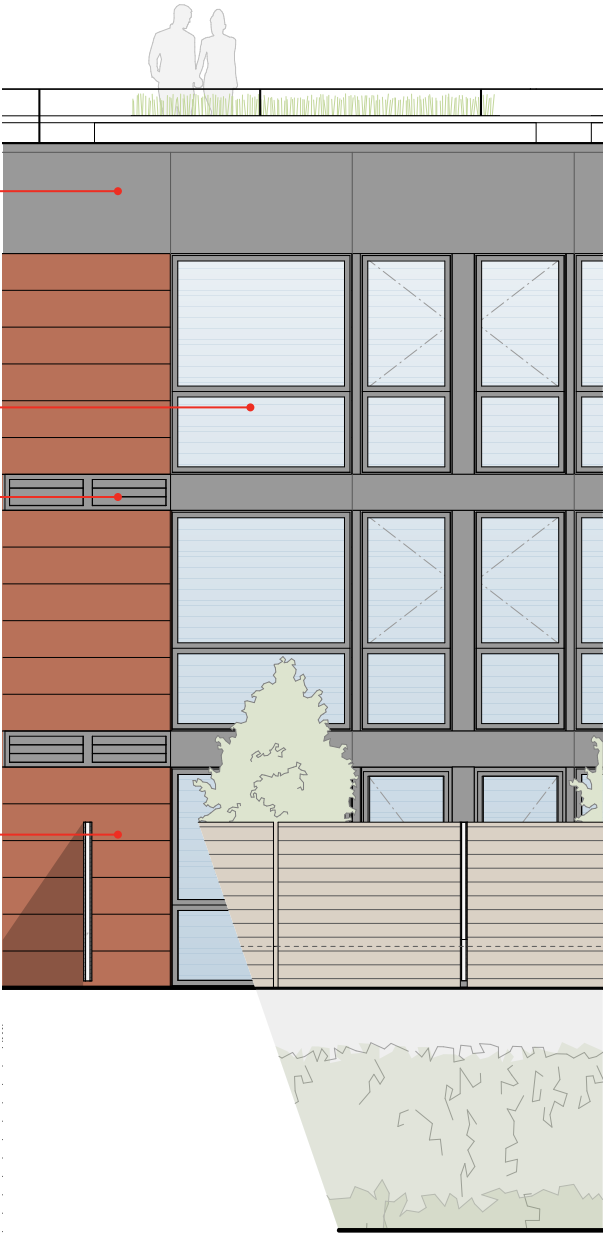
**WOOD OPEN SLAT TERRACE
PRIVACY FENCE AND SEPARATION
SCREEN**

VINYL WDW (AT BAY WDWS)
COLOR: BLACK

**ALUMINUM LOUVER TO MATCH
STOREFRONT**

ALUMINUM STOREFRONT
COLOR: DARK BRONZE

EXPOSED CONC COLUMN



Partial South Elevation



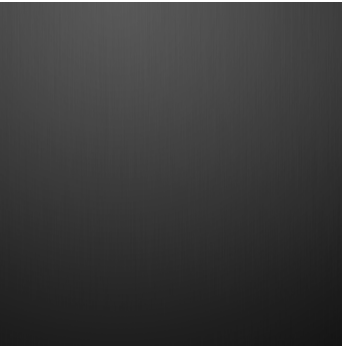
CEMENT BOARD RAINSCREEN:
COLOR: BRICK



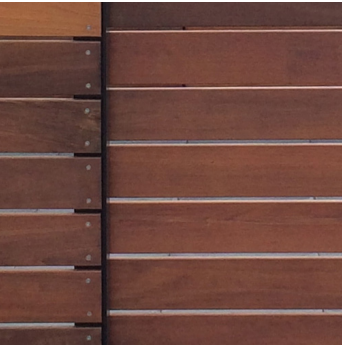
CEMENT BOARD RAINSCREEN:
COLOR: GREY



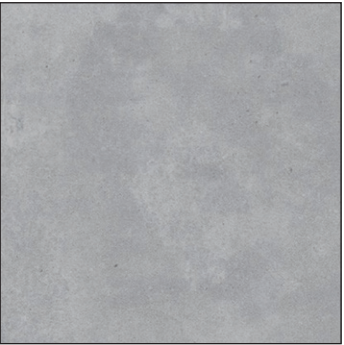
CEMENT BOARD RAINSCREEN:
COLOR: DARK GREY



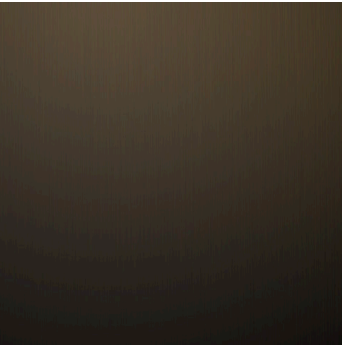
BRAKE METAL TRIM (AT BAY WDWS)
COLOR: BLACK (TO MATCH VINYL WDW)



WOOD OPEN SLAT TERRACE SCREEN



CAST CONCRETE



ALUMINUM STOREFRONT
COLOR: DARK BRONZE

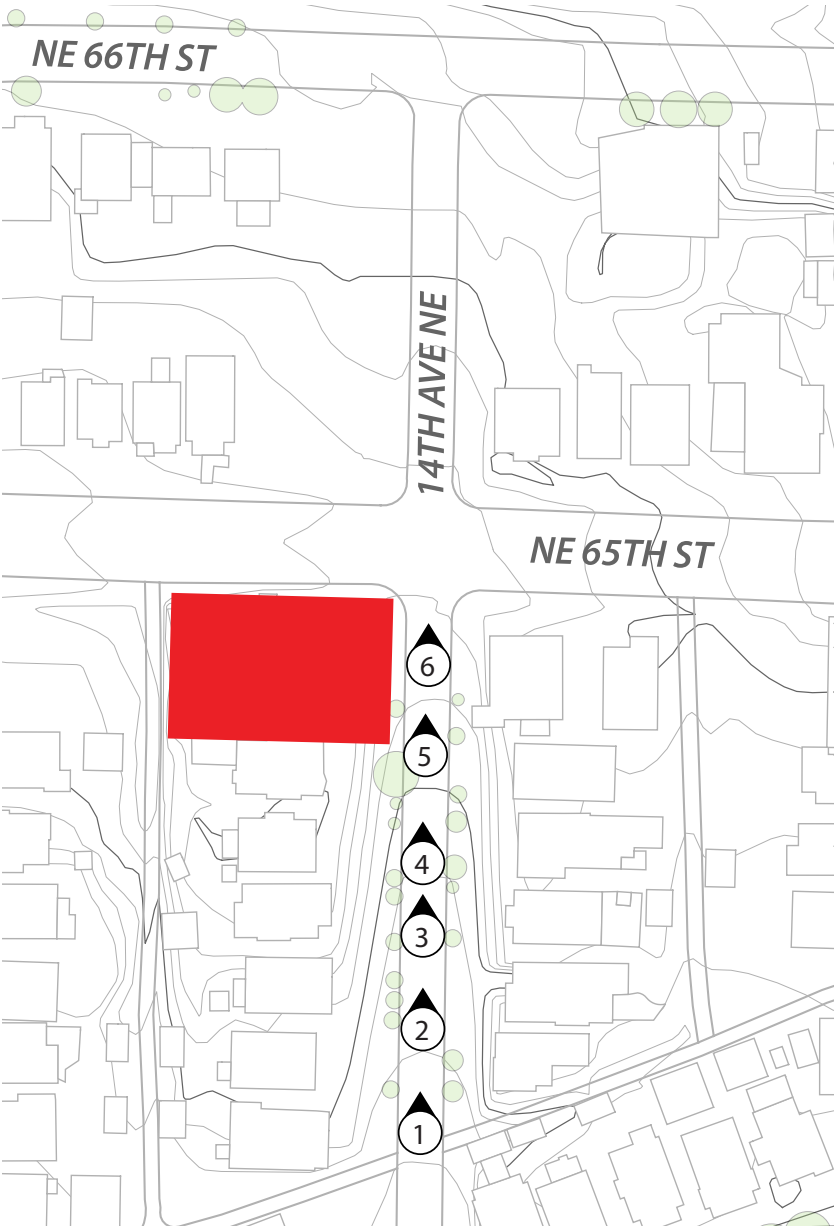


1F *Materials/Modulation:* The building cladding design has a horizontal pattern which is emphasized by changes in color. A strong grey horizontal band wraps the top and base of the upper levels, and the main field cladding panels are thin horizontal bands that further emphasize the landscape nature of the building.

4B *Bay Windows / First Floor Setback:* The bay windows and 4-ft setback supported by the Board is maintained, and the 4-ft setback was extended to wrap the corner on 14th Ave NE.

ZONE TRANSITION & PERCEIVED MASS / PRESENCE

14th Ave NE Visual Impact Study
Incremental views down 14th Ave NE were studied to understand the impact of the 4-story structure on the residential street. As seen in these images, the tree canopies greatly reduce the impact of the new building. The mass is obscured until the observer is nearly adjacent to the property.



Design Response

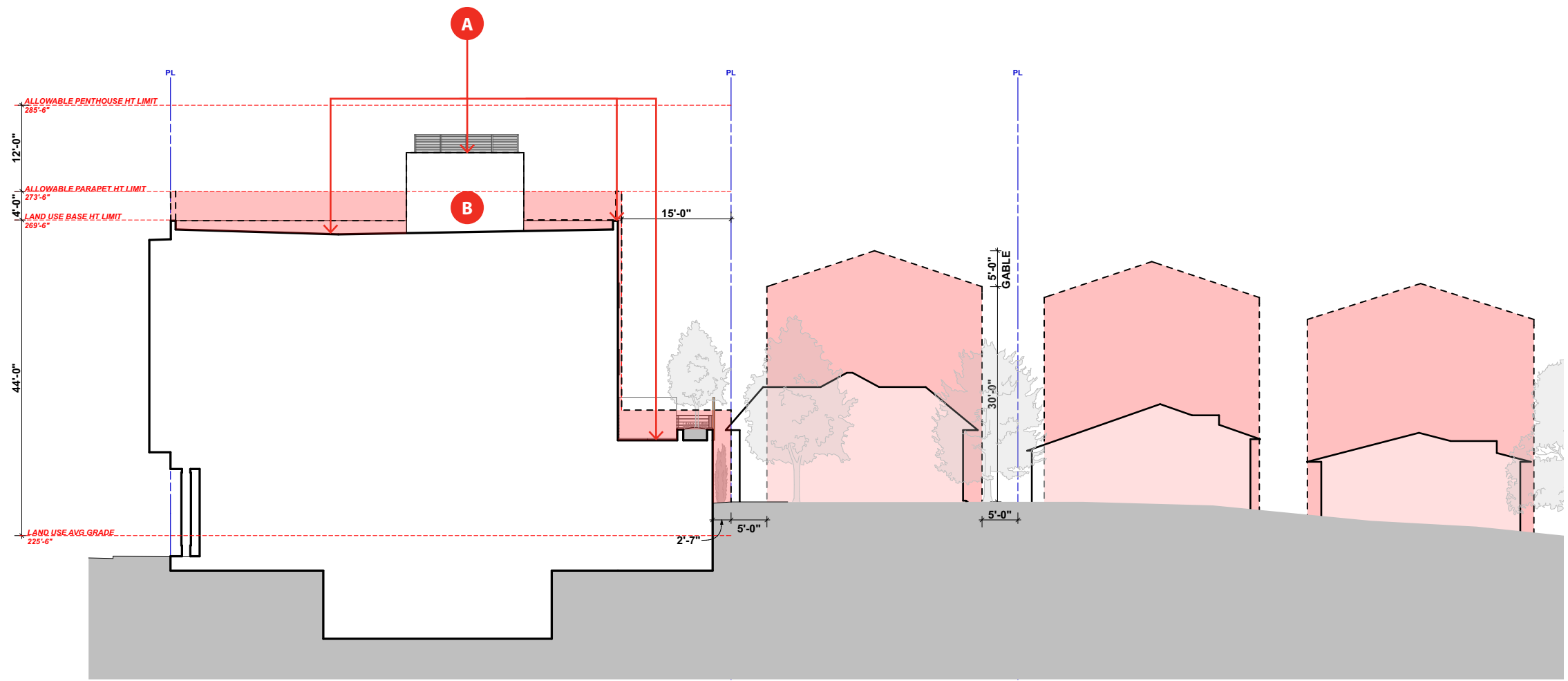
After hearing the boards' comments at EDG, the design team incorporated features to minimize the perceived height of the building from the street and neighboring vantages.

A *Massing*

The building does not maximize the zoning envelope: the elevator overrun is minimal, the parapet is lowered to the minimum needed for waterproofing, and the southern terrace is significantly lower than the setback allowance. These features minimize the perceived height of the building from the street and neighboring vantages.

B *Presence/Programming*

The occupiable roof terrace is centrally located to avoid overlooks from the roof edge into neighboring yards.



North-South Section Cut Indicating Allowable Massing Envelope versus Proposed Massing

PRIVACY/LANDSCAPE SCREENING/RESPECT FOR ADJACENT SITES

Design Response

Architectural Elements

- Planter
- Linear, cast-in-place raised planter with a seat edge bounds the south edge of the terrace
 - Planter keeps terrace residents 5-ft back from the terrace edge (7.25-ft from the shared lot line)
 - Directs resident views back towards the building.
- Wood Privacy Screen
- 6-ft slatted wood screen blocks views to south neighbor from the south terrace
 - Ensures year round privacy

Screening

A layered landscape and screening approach is proposed for the southern edge of the 2nd level terrace, described in more detail further in this document.

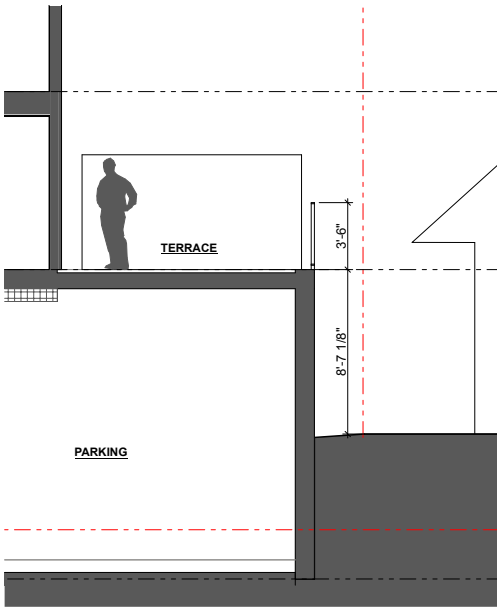
Trees

- Japanese maples are located along the south property line at each terrace planter
- Helps to screen views from upper levels of the building into the neighboring yard, and from the neighboring yard up to the building

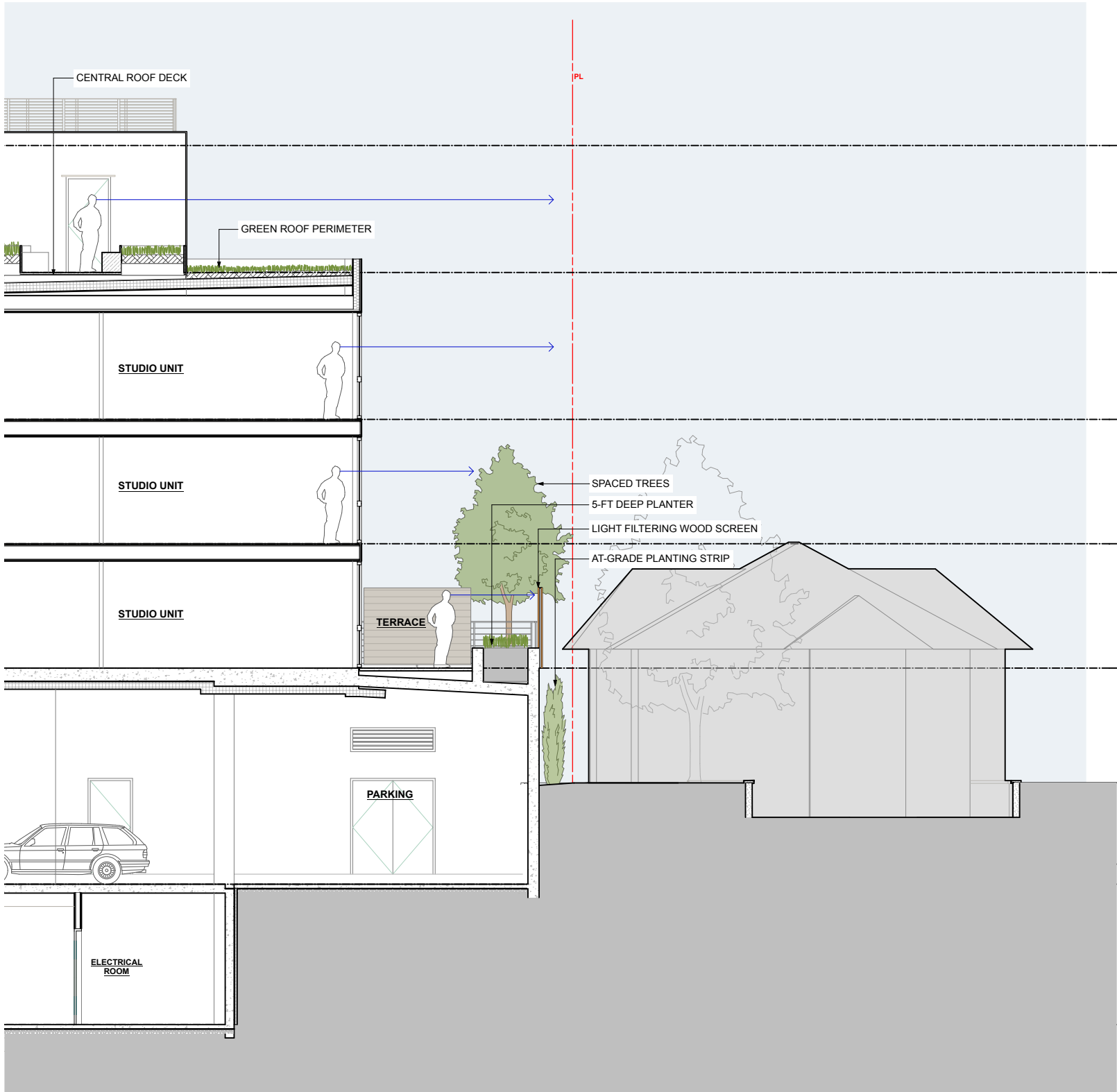
Ground level planting

- Landscape, including low shrubs and bamboo or climbing vines, will be planted in the strip of soil between the building edge and the shared property line
- Screens and softens the concrete wall of the building
- Enlarges the feel of the neighbor's yard by roughly 1 ½ - 2 feet.
- Neighbor has been involved in the selection of the specific plants

***Maintenance:** Plant choices have been selected for their suitability for the intended design, attractive screening between the two properties, and for the local climate conditions. Larger specimens are being installed to increase coverage at the time of planting. While all living plant material can benefit from some amount of maintenance, the plant selections along the south property line are intended to require minimal or occasional maintenance. All proposed planting areas are to be watered with an in-ground, automatic irrigation system. Irrigation for the at-grade planting along the south property line will be on a separate valve, to ensure successful plant establishment. The terrace landscape will be maintained to ensure the viability and long-term health of the plant material.



Terrace at EDG





Current View from South Neighbor's Backyard



Proposed View from South Neighbor's Backyard

PRIVACY/LANDSCAPE SCREENING/RESPECT FOR ADJACENT SITES

February 10th 2016

Re: Project #3019953, 1319 NE 65th ST

To whom it may concern,

We live at 6333 14th Ave NE, which is adjacent to the proposed development and therefore have a keen interest in the design of the building that is being proposed for 1319 NE 65th St.

On October 23rd 2015 we met with Adrienne Watkins of Weinstein A+U, Karen Kiest Landscape Architect, and Ed Hewson of Roosevelt Development Group. We were given elevations that showed our property next to the proposed project, and renderings of the project from our back garden. The site lines from the terrace and other parts of the project were explained to us. Karen Kiest went over the proposed planting for the site and listened to our feedback.

Certainly the new project will completely alter the environment around our home, but we were reassured that the developer is making a best effort to mitigate our concerns about privacy.

Yours sincerely,





Andrew Glass & Zequn Ma

Letter from South Neighbor



Current View from South Neighbor’s Backyard



Proposed View from South Neighbor's Backyard

NE 65TH STREET STREETSCAPE & ALLEY

Design Response

Increased Activation of Residential Spaces

- Direct access to the residential amenity space off NE 65th St is added
- Increases permeability to the street and further activate the façade
- Enhances retail storefront appearance of residential amenity space

Bay Windows / First Floor Setback

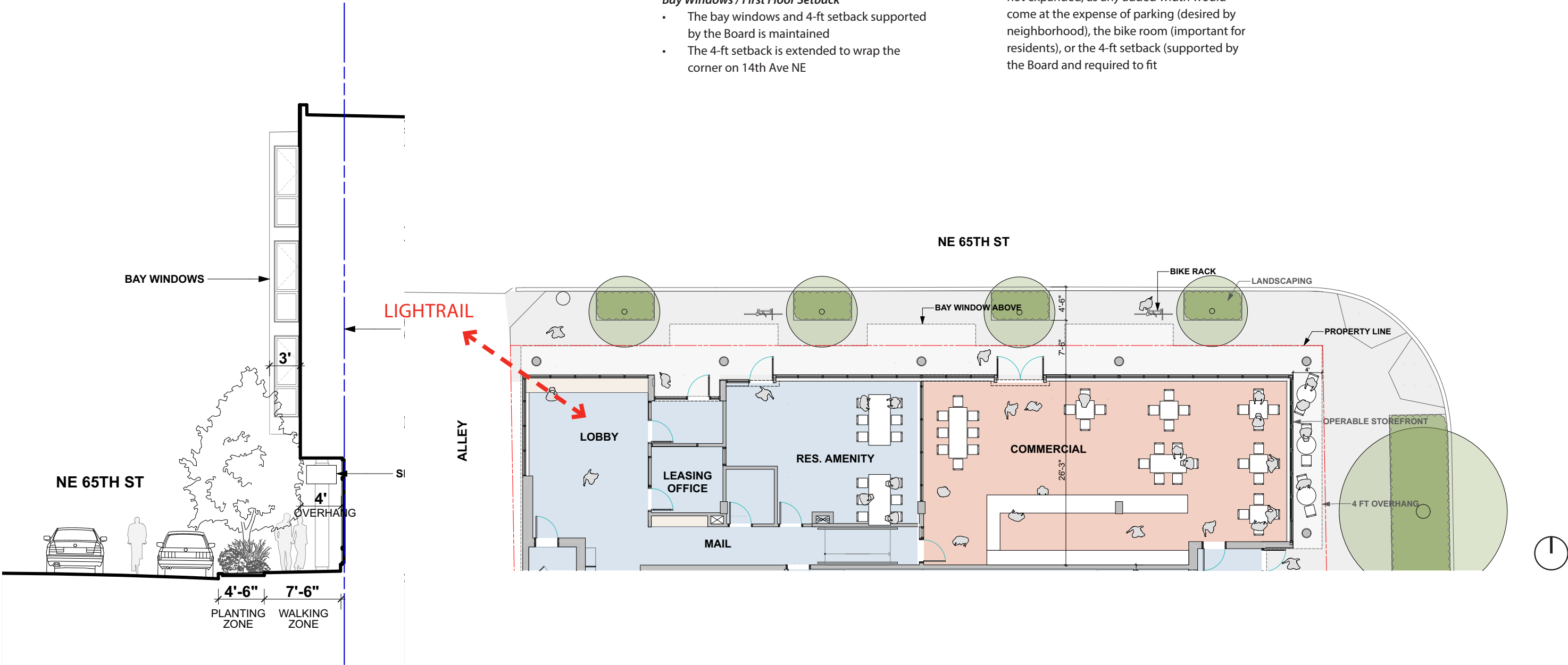
- The bay windows and 4-ft setback supported by the Board is maintained
- The 4-ft setback is extended to wrap the corner on 14th Ave NE

Signage

- Signage for the residential entry and commercial at entries and under soffit
- Enhances the visibility of the residential and commercial entries on NE 65th St.

Commercial Depth

- The commercial depth was explored but not expanded, as any added width would come at the expense of parking (desired by neighborhood), the bike room (important for residents), or the 4-ft setback (supported by the Board and required to fit





14TH AVENUE NE STREETScape

Design Response

Facade setback / Commercial Spill-out

- The 14th Ave facade is recessed by 4-ft
- Increases sidewalk activity related to the commercial space
- An operable wall opens this facade to the street.

Secondary Entry/Exit

- A glazed entry door to entry “mixing zone.”
- Café spill-out space is served by the second entry on 14th, increasing use of this secondary entry/exit.
- The recessed corner increases the entry’s visibility, extending transparency to the stair core.
- The entry serves the bike room and residents living on the east half of the building

Transition to Residential

- Entry and niche above provides a transition point to residential character to the south

Increased Transparency

- A slot window in the stairway offers provides eyes on the street.

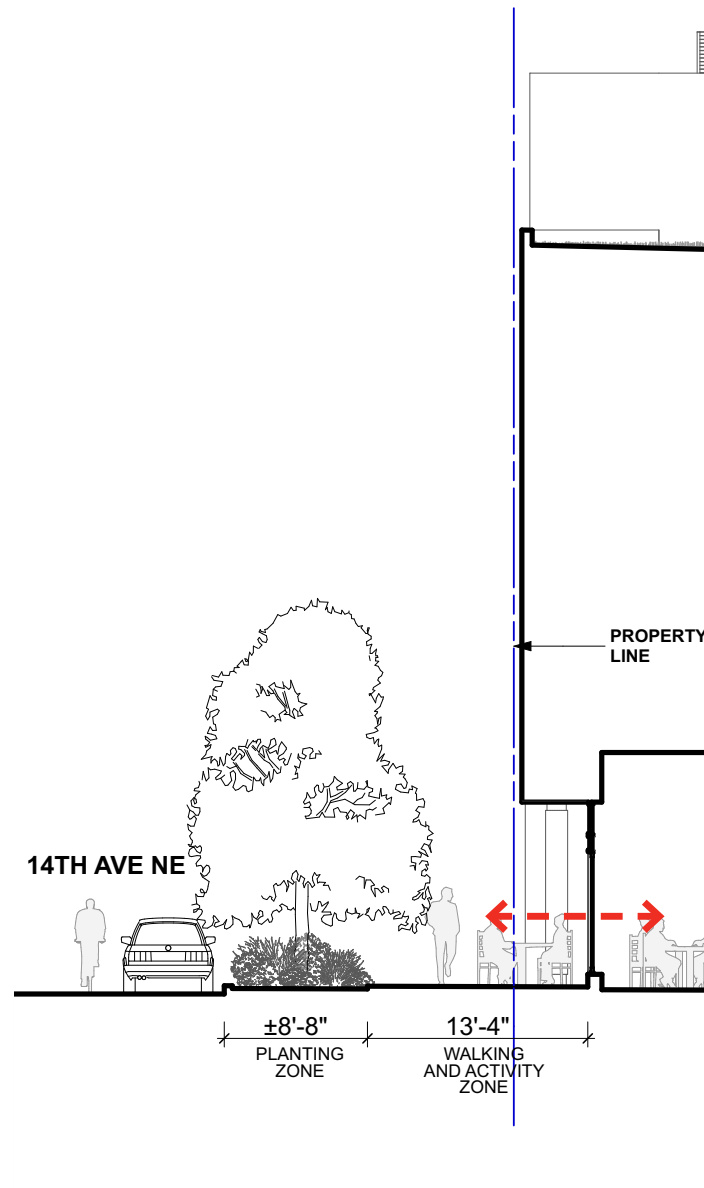
Landscape Screening

- Landscape screening incorporates low groundcover and taller hedges to increase interest and coverage
- Blends into the sidewalk character of 14th Ave NE- planted rockeries and retaining walls of a similar height and scale.

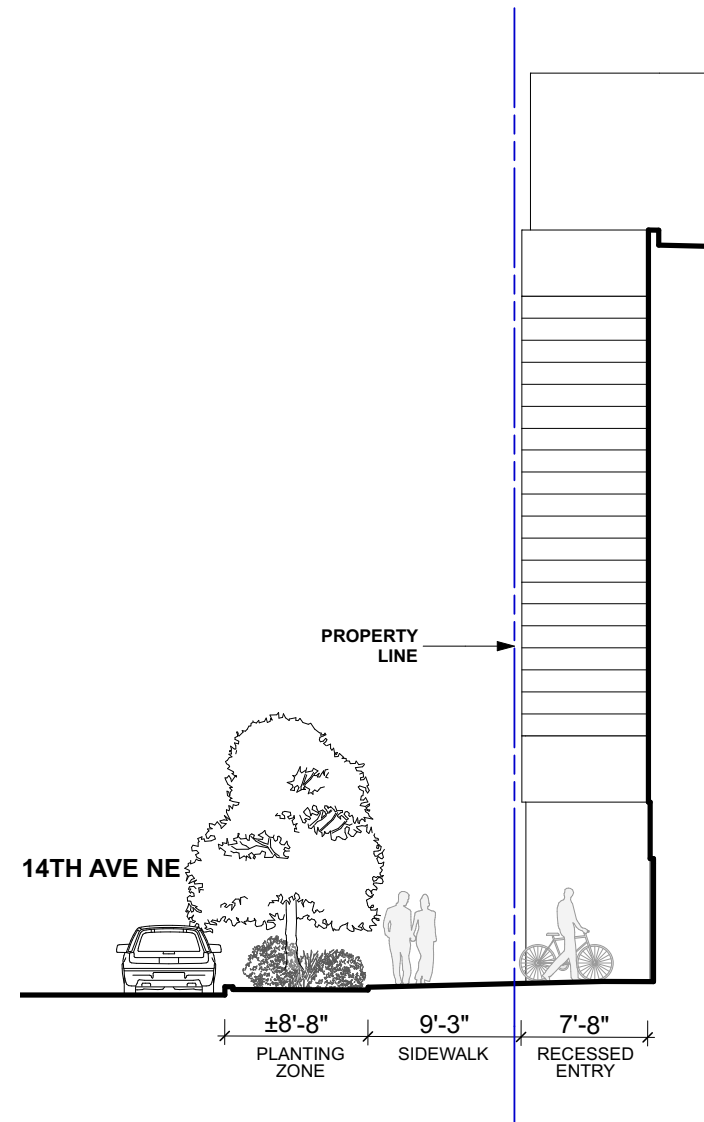
Interior Trash Storage

- Trash storage with access off 14th allows the trash contractor to pick-up directly from the storage room
- Eliminates dumpster staging on the street.

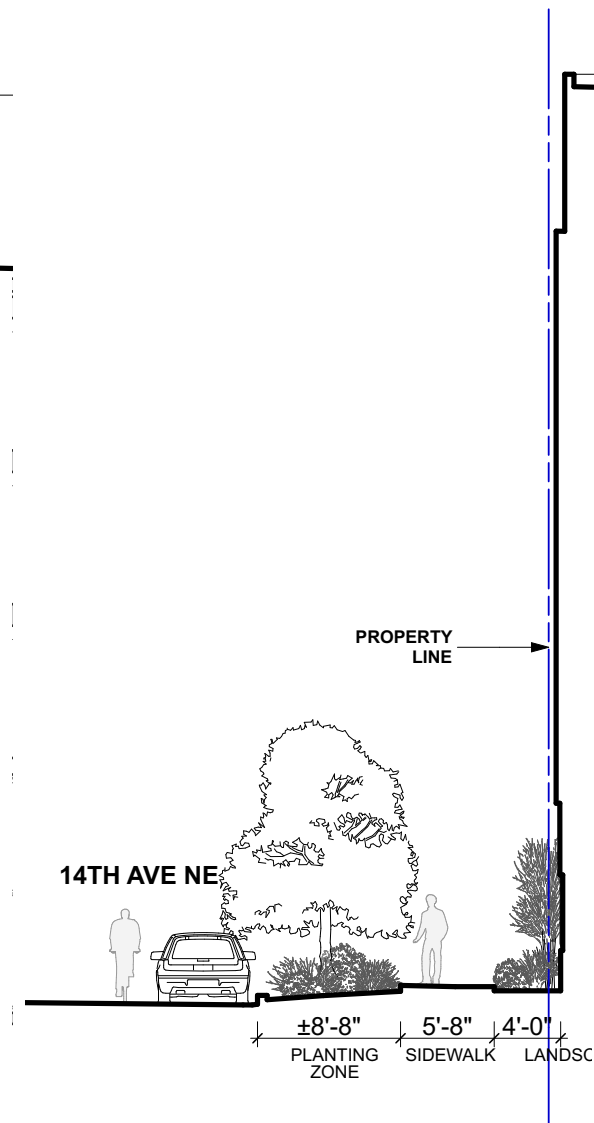




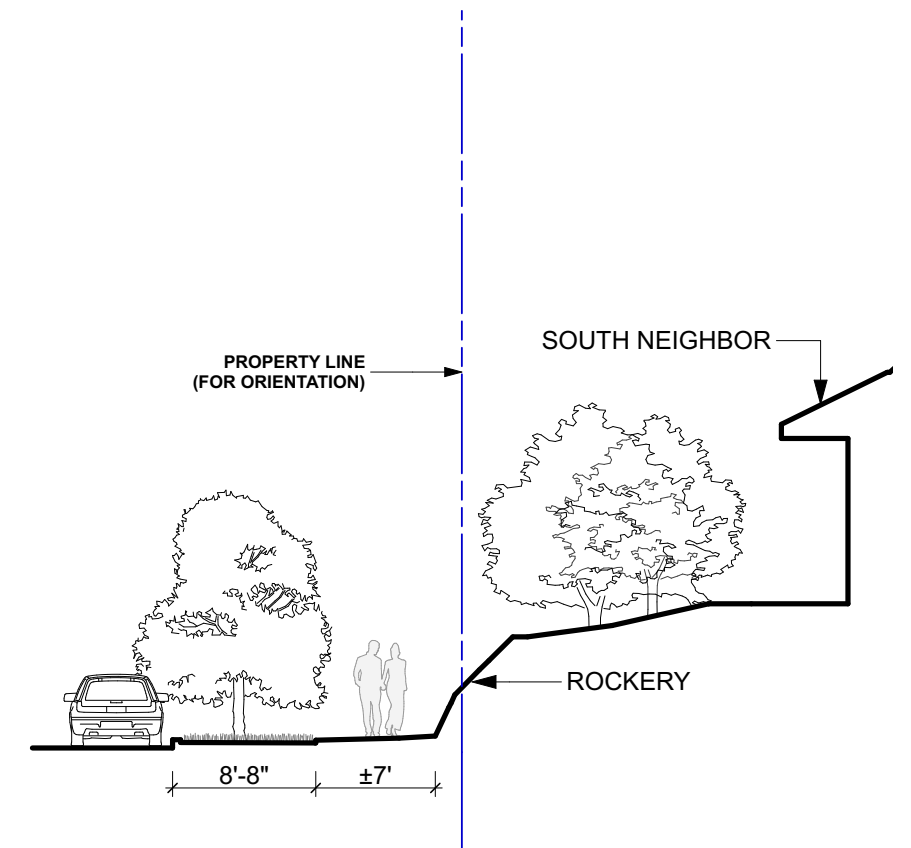
Street Section at Retail



Street Section at Secondary Entry




Street Section at E Stair



Street Section at S Neighbor

RESPONSE TO EARLY DESIGN GUIDANCE - 14TH AVENUE NE STREETSCAPE



City of Seattle
Seattle Public Utilities

November 16, 2015

Liz Kain, SW Contract Administration
Seattle Public Utilities
Utility Systems Management
P.O. Box 34018
Seattle, WA. 98124-4018

This Project #3019953at address 1319 NE 65th St has been reviewed and the space has been found adequate for storage of residential and commercial garbage and recycling containers and is approved by SPU/Solid Waste Management. The alley is not used by the contractor for collection. The contractor will bring the containers from the trash room out to 14th Ave NW for collection. There will be a 5- foot curb cut to allow the containers to be moved to the street for collection.

Liz Kain

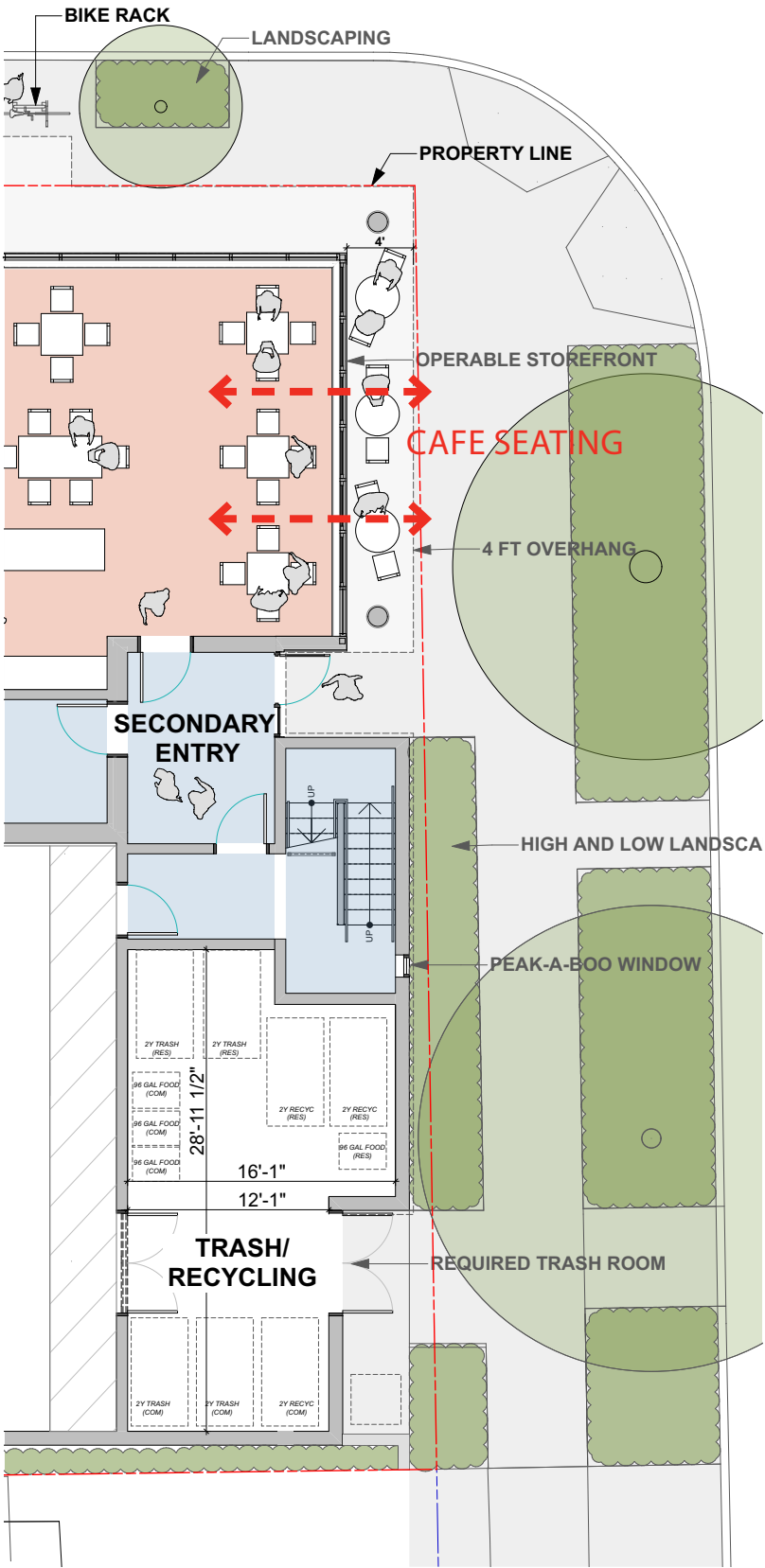
Date: 11/16/15

Office (206) 684-4166
Fax (206) 684-0206
liz.kain@seattle.gov

Ray Hoffman, Director
Seattle Public Utilities
PO Box 34018
Seattle, WA 98124-4018

Tel (206) 684-5851
Fax (206) 684-4631
TDD (206) 233-7241
<http://www.seattle.gov/util>

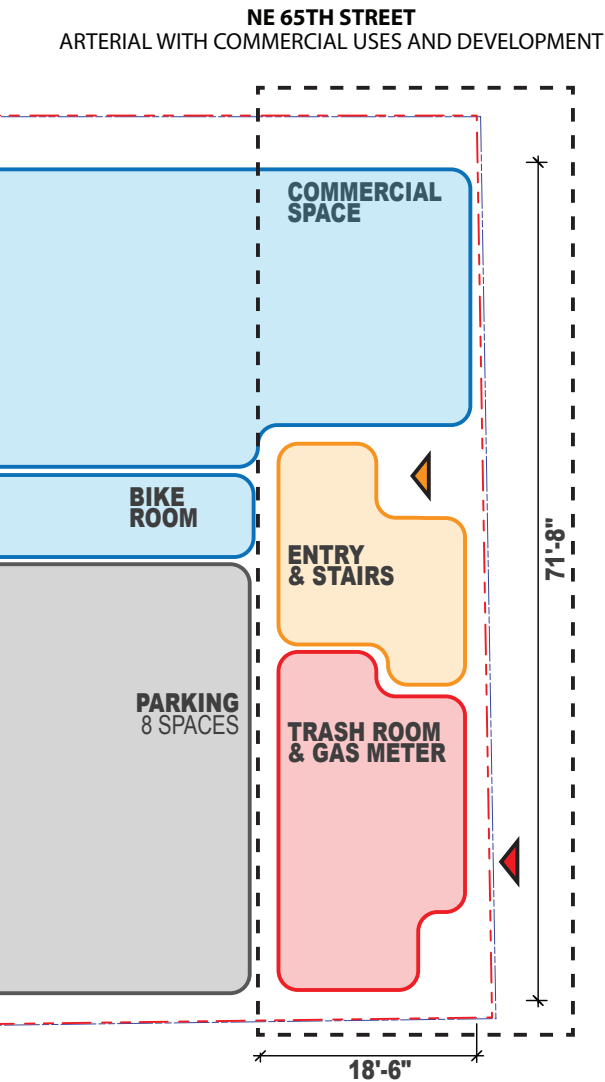
Letter from Liz Kain Regarding Trash Pickup on 14th Ave NE



RESPONSE TO EARLY DESIGN GUIDANCE - 14TH AVENUE NE STREETSCAPE

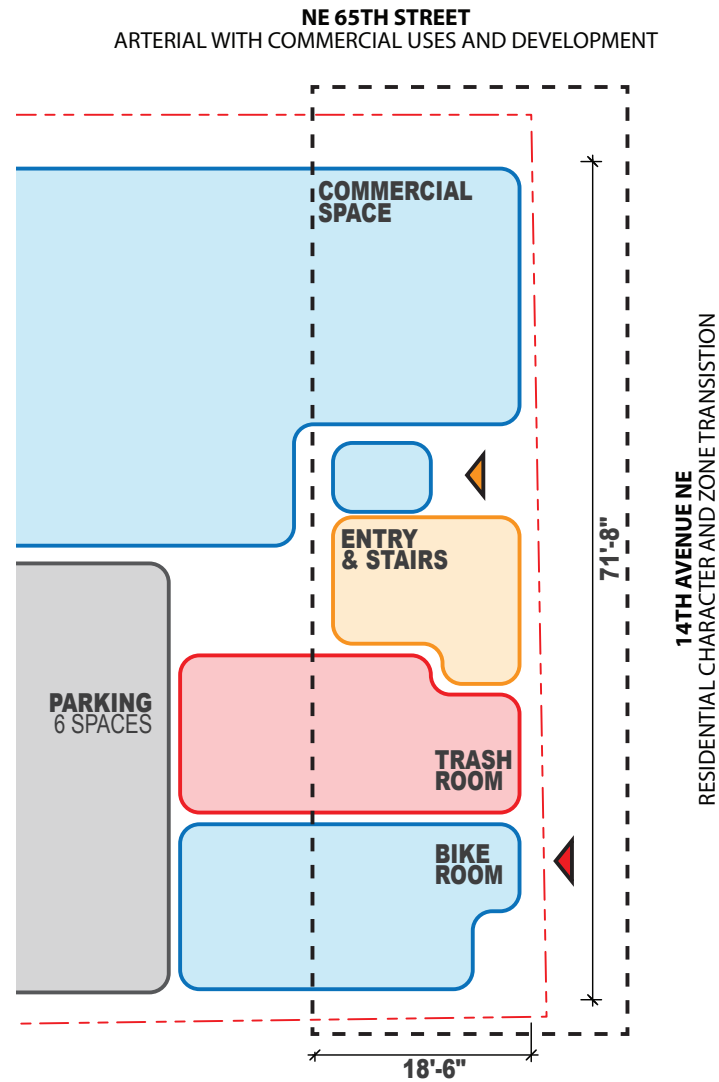
Post EDG, the following alternatives were studied for 14th Ave NE. The goal was to increase activation on 14th and smoothly transition to the residential zone at the southern property line.

- DESIGN CONSTRAINTS:**
- Trash pickup remains on 14th, as required by SPU
 - Maximize parking per community comments
 - Minimize stair/core layout impacts on tight residential floors above
 - Maximize activity and visibility of bike room/commercial spaces and entires.



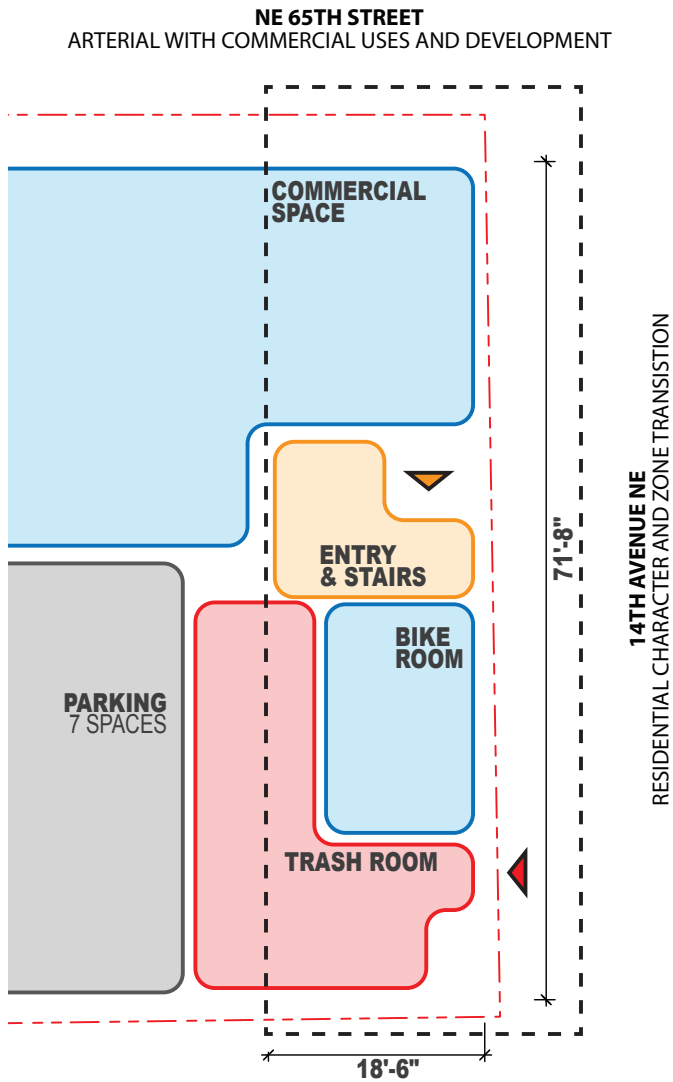
EDG

- Pros**
- 14th Street facade provides natural transition to residential street beyond
 - Trash Room located correctly per SPU direction and eliminates need for sidewalk staging area
- Cons**
- Secondary entry underused, effectively stopping activity from traveling down 14th Ave NE.



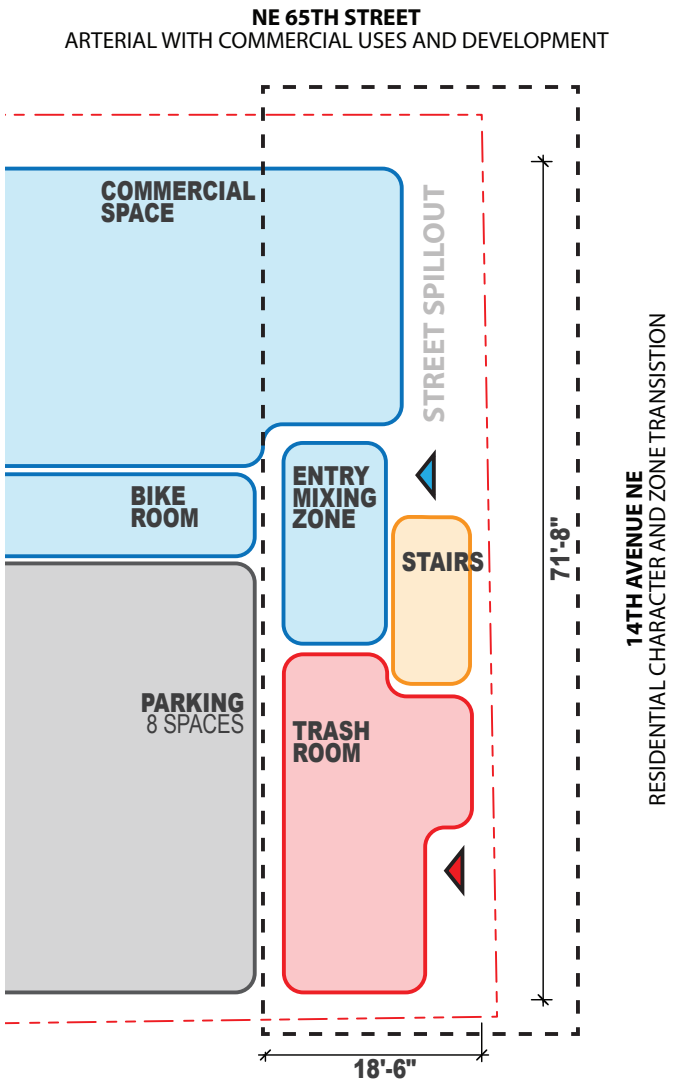
BIKE ROOM ON 14TH

- Pros**
- Transparency of Bike Room helps activate street
 - Deepened commercial space
 - Trash Room located correctly per SPU direction and eliminates need for sidewalk staging area
- Cons**
- Loss of 2 parking stalls
 - Bike room is disconnected from residential lobby
 - Added transparency is adjacent to residential zone and does not enhance transition strategy.



COMPRESSED BIKE ROOM AND ENTRY

- Pros**
- Transparency of Bike Room helps activate street
 - Trash Room located correctly per SPU direction and eliminates need for sidewalk staging area
- Cons**
- Trash Room is inefficiently proportioned
 - Loss of 1 parking stall
 - Stairs orientation results in loss of one unit per floor
 - Bike room isolated



PREFERRED

- Pros**
- Set back commercial space creates sidewalk cafe
 - Shared entry helps to activate 14th Street and pulls more activity down 14th Ave NE
 - Stair location is natural transistoin to quieter, residential zone
 - Trash Room located correctly per SPU direction and eliminates need for sidewalk staging area
- Cons**
- Trash Room requires vegetation screening

DESIGN PROPOSAL: STREETSCAPE (NE 65TH)





A Residential Entry Signage



A Cafe Signage



A Secondary Entry Cafe Signage

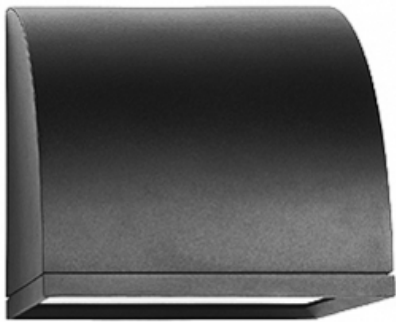
DESIGN PROPOSAL: LIGHTING STREET LEVEL PLAN



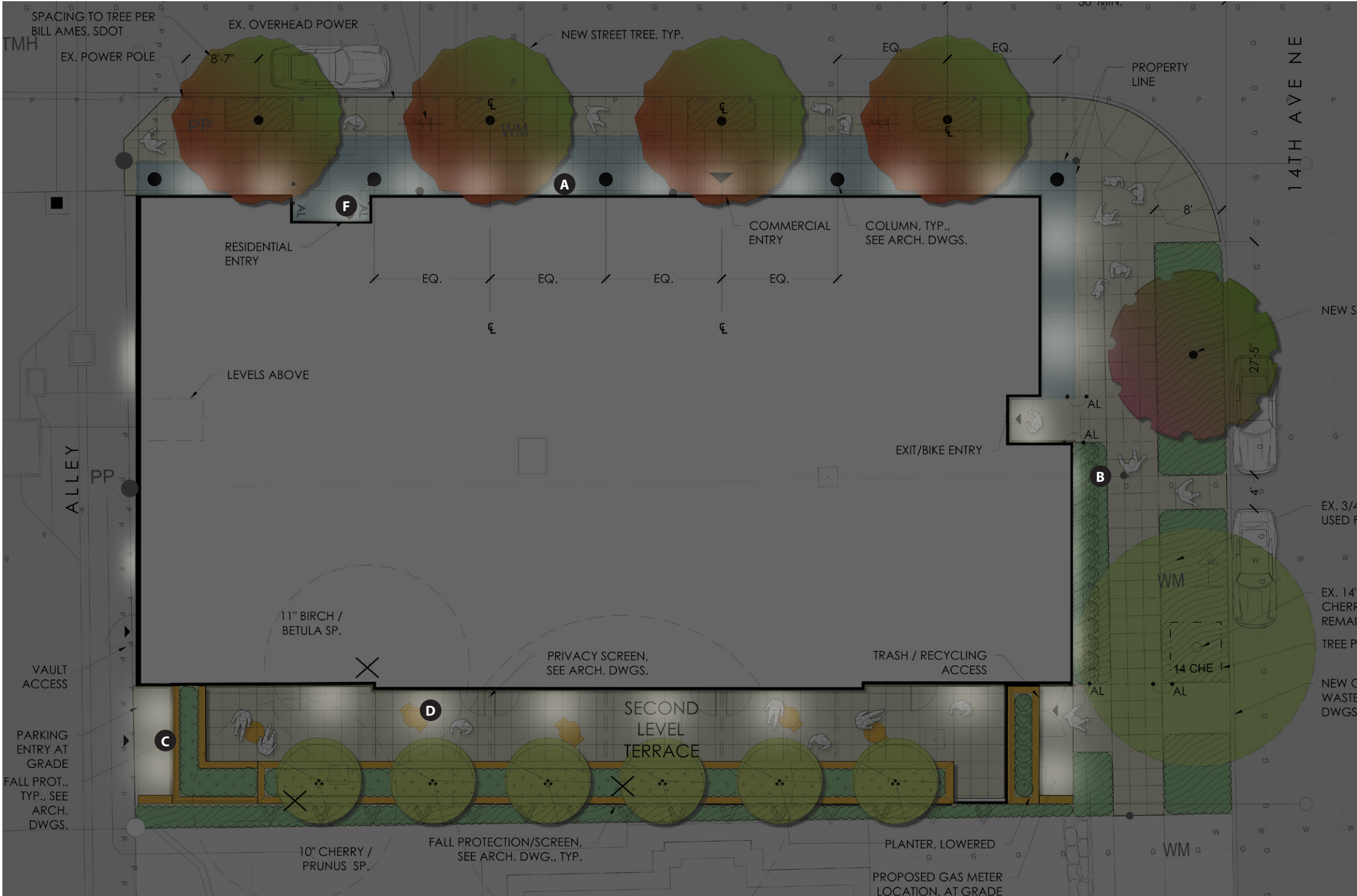
A Canopy downlight



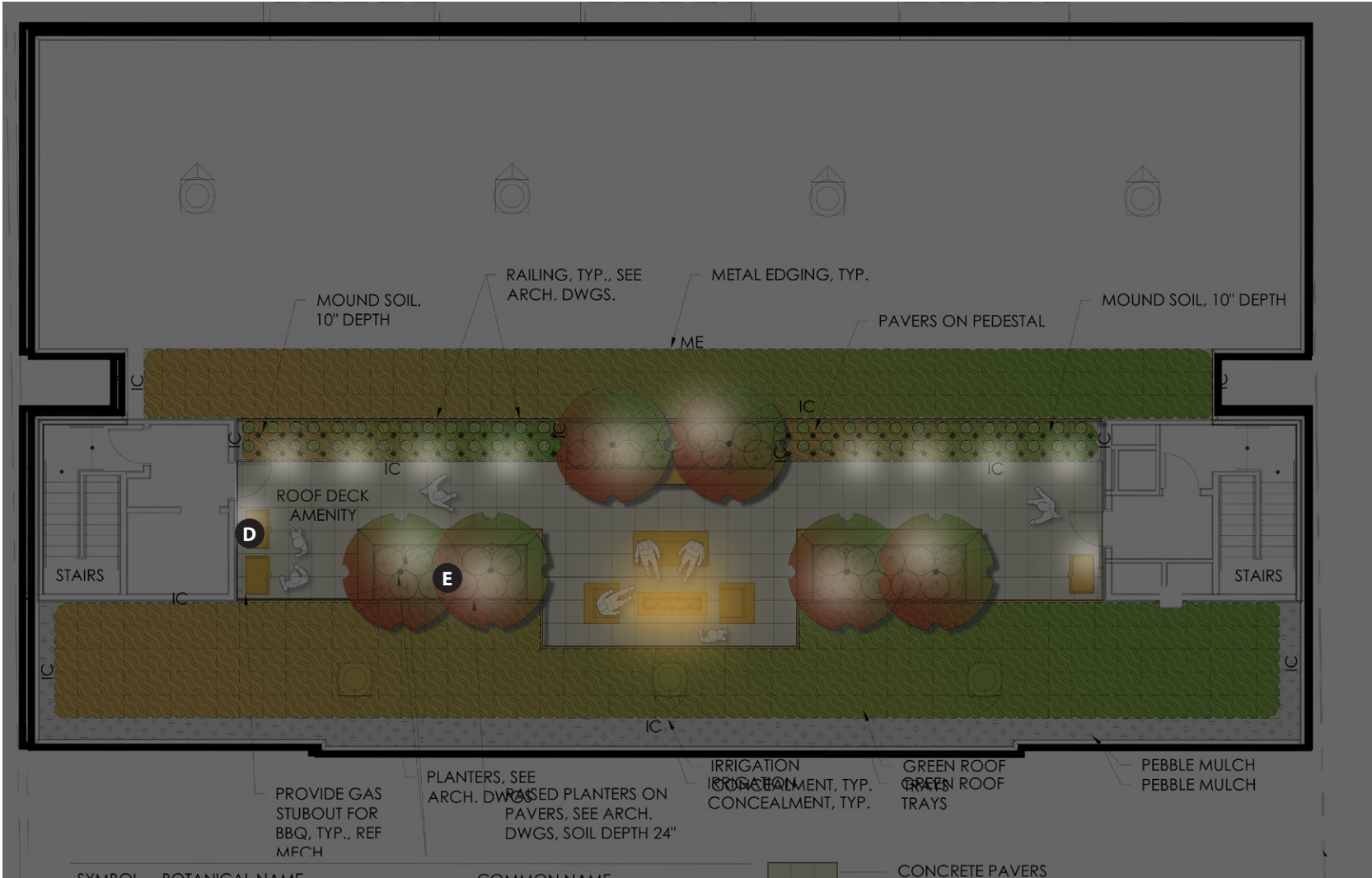
B Landscape accent light



C Garage entry light



DESIGN PROPOSAL: LIGHTING ROOF PLAN



D Residential entry light

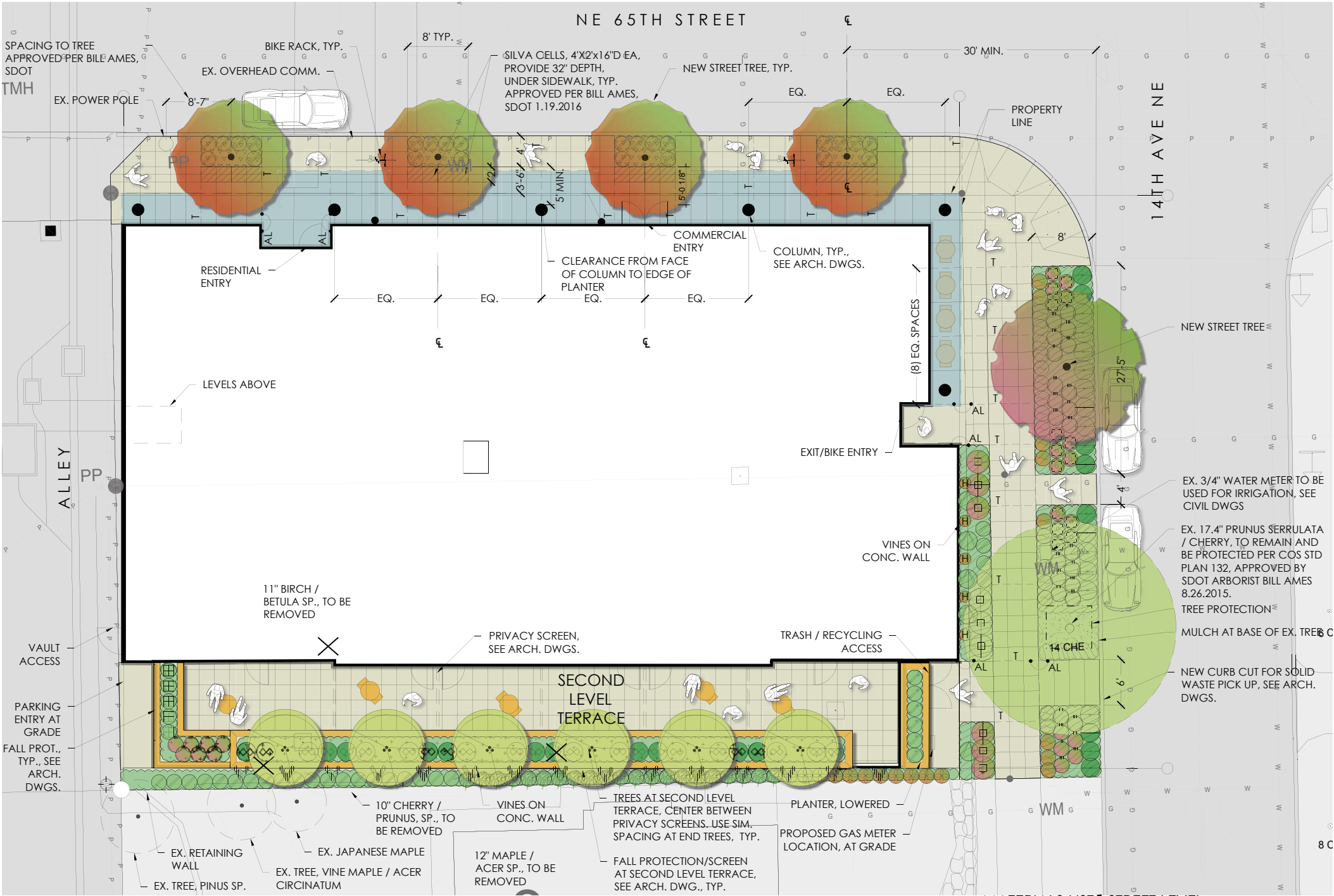


E Landscape Downlight



F Directional signage light

DESIGN PROPOSAL: LANDSCAPE PLAN STREET LEVEL



MATERIALS LIST - STREET LEVEL

- CONCRETE PAVING PER COS STD. PLAN 420 W/ THE FOLLOWING EXCEPTIONS:
 - SAND COATED EXPANSION JOINTS
 - SAW CUT CONTROL JOINTS
- THROUGH JOINT
- BIKE RACK "INVERTED U" RACK PER CITY OF SEATTLE DETAIL, INSTALL PER MFG. RECOMMENDATIONS
- TREE PROTECTION PER COS STANDARD DETAILS 132 & 133



DESIGN PROPOSAL: LANDSCAPE PLANT LIST

PLANT LIST - STREET LEVEL		
SYMBOL	BOTANICAL NAME	COMMON NAME
STREET TREES		
EXISTING TREE		
	CERCIDYPHYLLUM JAPONICA 'ROTFUCHS' (APPROVED BY SDOT ARBORIST BILL AMES, 01/19/2016)	RED FOX KATSURA
	PRUNUS SERRULATA (APPROVED BY SDOT ARBORIST BILL AMES, 08/26/2015)	DOUBLE CHINESE CHERRY
TREES - ON STRUCTURE		
	ACER PALMATUM (GREEN)	JAPANESE MAPLE
SHRUBS		
	CORNUS STOLONIFERA 'KELSEY' *	'KELSEY' RED TWIG DOGWOOD
	EUPHORBIA AMYGDALOIDES ROBBIAE **	EUPHORBIA
	ILEX CRENATA 'CONVEXA' **	JAPANESE HOLLY
	LONICERA PILEATA **	PRIVET HONEYSUCKLE
	NANDINA DOMESTICA **	HEAVENLY BAMBOO
	PHYLLOSTACHYS AUREA **	GOLDEN BAMBOO
	PIERIS JAPONICA 'CAVATINE' **	'CAVATINE' JAPANESE PIERIS
	ROSA 'AMBER FLOWER CARPET'	'AMBER FLOWER CARPET' ROSE
	RHODODENDRON 'KEN JANECK'	HYBRID RHODODENDRON
	VIBURNUM DAVIDII	DAVID'S VIBURNUM
	VIBURNUM BODNANTENSE 'DAWN'	DAWN VIBURNUM
GROUNDCOVER MIXES		
	50% EPIMEDIUM ALPINUM **	50% EPIMEDIUM
	25% ASTILBE X ARENDsii 'PEACH BLOSSOM'	25% 'PEACH BLOSSOM' ASTILBE
	25% POLYSTICHUM MUNITUM **	25% SWORD FERN
	OPHIOPOGON P. 'NIGRESCENS' **	BLACK MONDO GRASS
	LIRIOPE SPICATA	CREeping LILYTurf
	MULCH AT EX. TREES, 4" DEPTH	
VINES		
	HYDRANGEA ANOMALA 'PETIOLARIS'	CLIMBING HYDRANGEA
	PARTHENOCISSUS QUINQUEFOLIA *	VIRGINIA CREEPER

STREET



Cercidiphyllum 'Rotfuchs'
Red Fox Katsura



Cornus Stolonifera 'Kelsey'
'Kelsey' Red Twig Dogwood



Pieris japonica 'Cavatine'
'Cavatine' Japanese Pieris



Viburnum bodnantense 'Dawn'
'Dawn' Viburnum



Prunus serrulata
Japanese Cherry (to match ex.)



Nandina 'Moon Bay'
'Moon Bay' Nandina



Rosa 'Amber Flower Carpet'
'Amber Flower Carpet' Rose



Hydrangea andromeda 'Petiolaris'
Climbing Hydrangea

STRUCTURE



Acer palmatum
Japanese Maple



Ilex crenata 'Convexa'
Compact Japanese Holly



Lonicera Pileata
Privet Honeysuckle



Ophiopogon 'Nigrescens'
Black Mondo Grass

ROOF



Acer griseum
Paperbark Maple



Shibatea kumasaca
Shibatea



Echinacea purpurea
Purple Cone Flower



Sedum 'Autumn Joy'
'Autumn Joy' Sedum

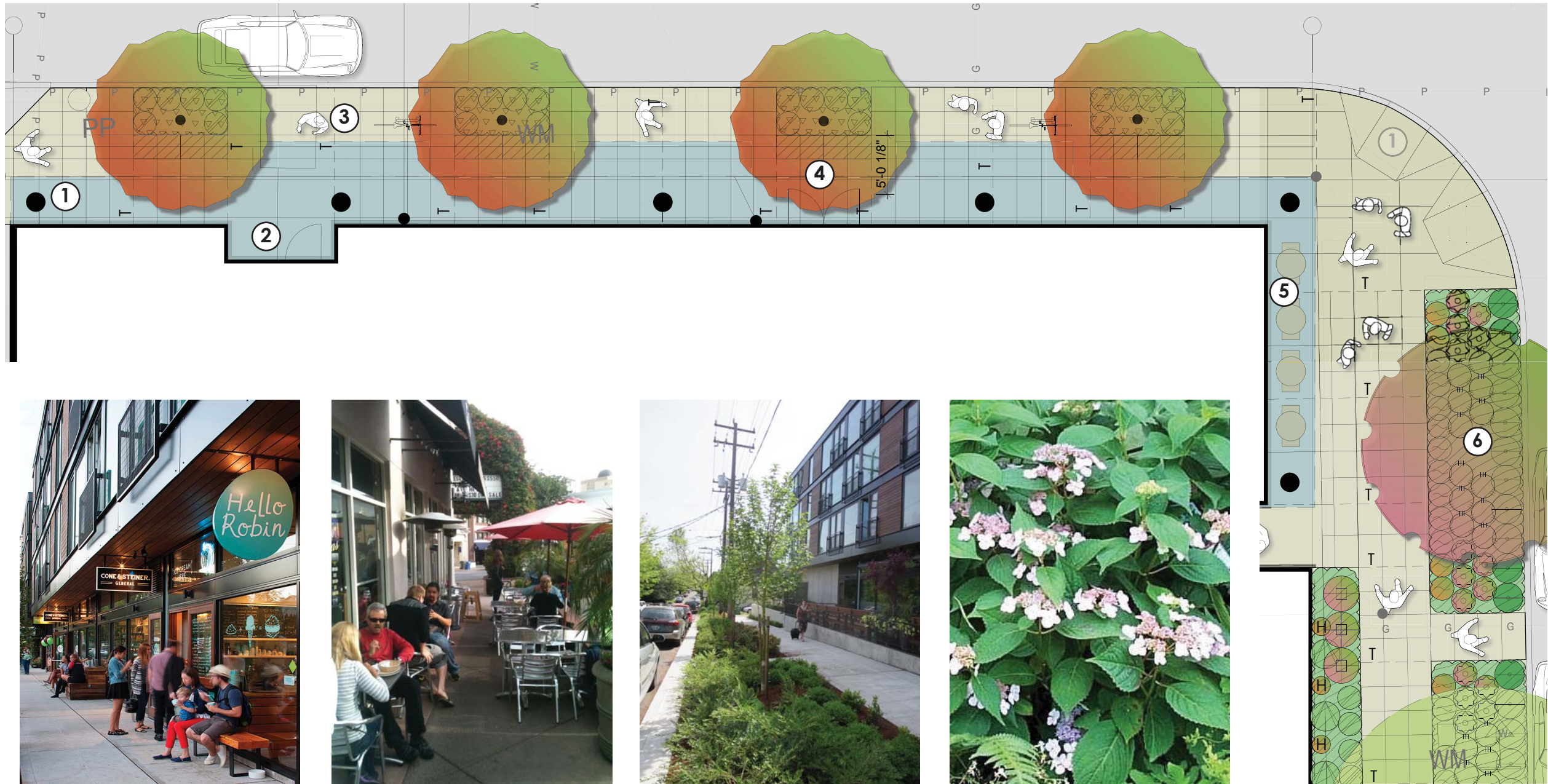
DESIGN PROPOSAL: LANDSCAPE PLAN STREET LEVEL

NE 65th Street

- NE 65th Street**
- 1 Colonnade
 - 2 Residential Entry
 - 3 Bikes
 - 4 Retail Entry

- 14th Avenue NE**
- 5 Outdoor Dining
 - 6 Generous Planter Strip with New Cherry Tree
 - 7 Hydrangea vine, with Dawn Viburnum at Building

- Podium Level Terraces**
- 7 Japanese Maples at Perimeter for Screening
 - 8 Parking Entry
 - 9 Landscape Buffer at Property Line



4 Retail entry



5 Outdoor Dining

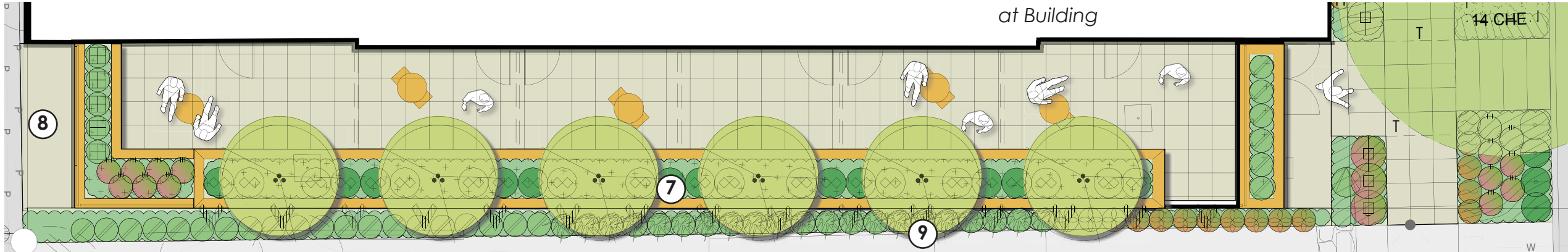


6 Generous Planter Strip at 14th Avenue NE

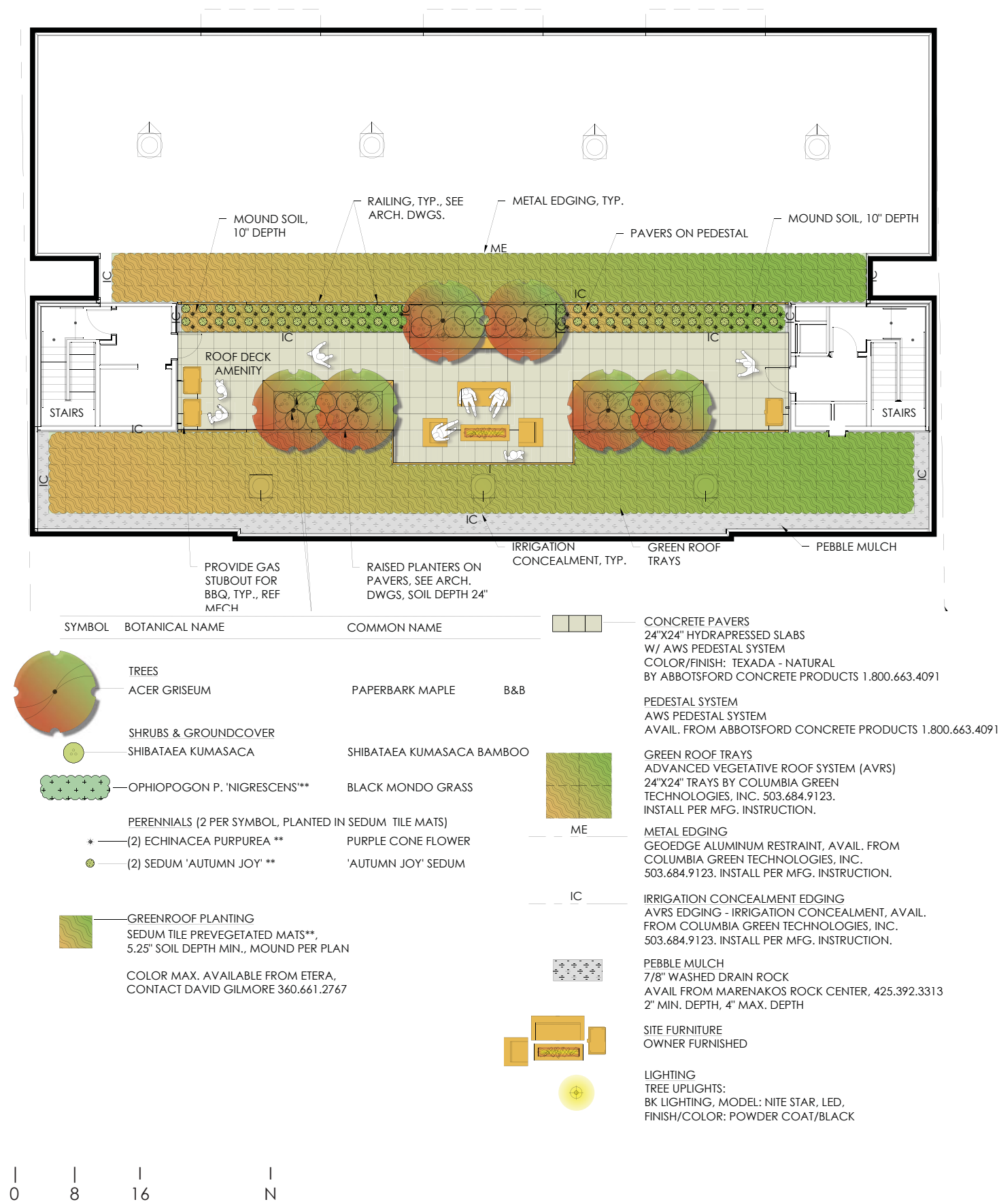


7 Hydrangea vine, with Dawn Viburnum at Building

Podium Level Terraces



DESIGN PROPOSAL: LANDSCAPE PLAN ROOF LEVEL



Shallow Sedum Mats



Sedums with Grasses



Mounded Soil with Flowers



Narrow but Generous Terrace



Seating with Raised Planters



Barbecue and Seating

ROOF TERRACE



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DEVELOPMENT DEPARTURES

The proposed project will be a four-story, apartment building approximately 44-feet in height and approximately 29,627-sf, including the below grade level. The building will contain 41 residential apartment units, resident lobby and amenity spaces, an outdoor landscaped terrace and roof deck, on-site leasing office, street level commercial space, street-level structured resident parking for 8 vehicles, and a below-grade level for building services and resident storage units.

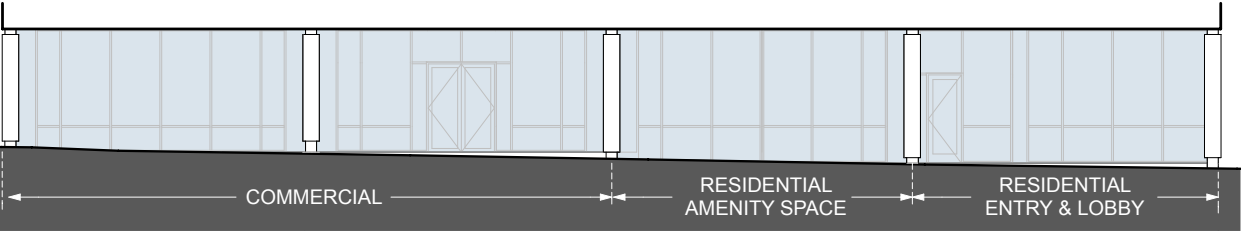
The project development objectives are as follows:

- Provide a high quality living environment for residents convenient to work, leisure and recreation
- Provide a lively environment for urban residents, including accommodations for retail and/or restaurants
- Set precedence as the first new development along the south side of NE 65th by setting up the pedestrian-oriented design decisions to benefit the neighborhood:
 - Widen sidewalks
 - Strengthen the street edge with an appropriately scaled facade
 - Create vibrant and transparent street level façades
 - Plant street trees to buffer traffic
 - Offer overhead weather protection
- Be sensitive to the site's location in a transition zone from neighborhood commercial to single-family residential. This will inform building massing, scale, street level programming and building access.
- Be a good neighbor. This will inform the project in terms of:
 - Commercial level streetscape design
 - Landscape design & material selection
 - Exterior lighting design
 - Parking access
 - Trash & recycling storage

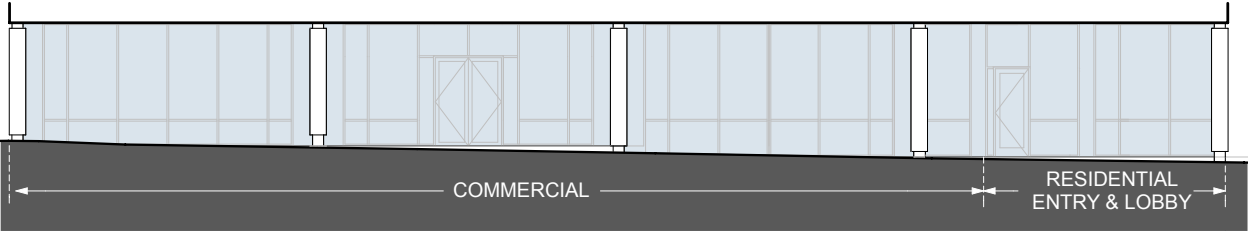
The following Development Departures have been identified as potentially necessary to achieve the preferred design alternate. If the board indicates their willingness to consider the departures, additional study and refinement of the design will be conducted in advance of the Design Recommendation meeting.

DEVELOPMENT STANDARD		DEPARTURE	RATIONALE FOR REQUESTED DEPARTURE
1a	23.47A.005.C: RESIDENTIAL USES AT STREET LEVEL Residential uses may occupy, in the aggregate, no more than 20% of the street-level street-facing façade in the following circumstances or locations: e) within nc1 zones.	Allow a larger percentage (50%) of residential uses along NE 65th st.	<i>The rationale for Departures 1, 2a and 2b are connected, as they all relate to the ne 65th st street level frontage.</i> <i>As the first new construction project on this stretch of NE 65th, drawing the quantity or size of retailer to fill the code-prescribed 80% of the NE 65th facade will be difficult. If 80% of the facade is dedicated to commercial there is risk of the storefront remaining un-rented and empty. The street-facing façade percentage requirements are intended to enhance and activate the pedestrian environment. While the proposed design does not meet the required percentages, the uses are intended to complement the streetscape. 50% of the façade will be a commercial space, with large windows and high transparency, while the remaining 50% will be the residential lobby and business center residential amenity with similar transparency. With small residential units, the public spaces like the lobby and business center are expected to be heavy-use gathering spaces. From the exterior, these residential amenity spaces will have large windows consistent with the commercial frontage. (DESIGN GUIDELINES PL2-B-1 EYES ON THE STREET; PL3-C-3 RETAIL EDGES: VISIBILITY)</i>
	2a 23.47A.005.D.1: USES AT PRINCIPAL PEDESTRIAN STREETS Along designated principal pedestrian streets, one or more of the following is required along 80% of street-facing façade: general sales, retail, eating and drinking establishments.	Allow a smaller percentage (50%) of the required uses along NE65th St. This percentage requirement is referred to in both sections listed here as Departures 2a and 2b.	
2b	23.47A.008.C.1: STREET LEVEL USES ON PRINCIPAL PEDESTRIAN STREETS In pedestrian-designated zones, a min of 80% of the width of a structure's street-level street-facing façade that faces a principal pedestrian street shall be occupied by uses listed in subsection 23.47a.005.d.1. Remaining 20% of the street frontage may contain other permitted uses and/or pedestrian entrances.		
3	23.47A.008.B.2A: STREET LEVEL TRANSPARENCY 60% of the street-facing façade between 2-ft and 8-ft above the sidewalk shall be transparent.	Allow landscaping in lieu of transparency on the 14th Ave façade.	<i>14th Ave NE is a quiet residential street south of the project site. An activated, transparent facade across the length of the building would not be an appropriate fit for the neighbors. A highly activated and transparent commercial façade is located on the northern half of the 14th Ave façade. To respect the existing single-family condition south of the site, and due to the constraints of fitting the building program and services on a small site, quieter building elements are located on the south end of the 14th Ave facade. These uses are not conducive to transparency and would be better served by intentional landscaping than windows. This landscape approach is consistent with qualities of 14th Ave NE, which is characterized by heavy vegetation along the sidewalk. (DESIGN GUIDELINES CS3-I-II ARCHITECTURAL FEATURES: TRANSPARENCY, PL3-C RETAIL EDGES)</i>
4	23.47A.008.B.3: NON-RESIDENTIAL USE DEPTH Non-residential uses shall extend an average depth of at least 30-ft and a minimum depth of 15-ft from the street-level street-facing façade.	Allow a reduced depth an average of 24.8-ft for the commercial space.	<i>NE 65th St has a very narrow pedestrian row adjacent to a busy arterial. In its current state there is no space available for street trees. A project goal is to enhance the pedestrian environment on NE 65th. To provide space for improvements, the street level on NE 65th is set back 4-ft from the property line. Due to the building's narrow site, the depth of this setback is taken out of the commercial depth. This setback creates a wider sidewalk, with space for street trees and planting to increase separation between pedestrians and traffic. (DESIGN GUIDELINES PL1-B-2 WALKWAYS AND CONNECTIONS: PEDESTRIAN VOLUMES)</i>
5	23.47A.008.C.4: OVERHEAD WEATHER PROTECTION Continuous overhead weather protection is required along at least 60% of the street frontage of a structure on a principal pedestrian street. The covered area shall have a min. Width of 6-ft.	Allow 4' deep weather protection instead of 6' deep weather protection.	<i>NE 65th St has a very narrow pedestrian row adjacent to a busy arterial. In its current state there is no space available for street trees. As described in departure request #4, the street level on NE 65th St is set back 4-ft from the property line to create space to add street trees. This setback also creates built-in 4-ft deep weather protection for pedestrians across the entire length of the building (greater coverage than the required 60% coverage per code), without interfering with the space required for street trees. Additionally, three 15-ft long bay window projections above street level provide additional deeper (7-ft) weather protection at intervals across the NE 65th façade. (DESIGN GUIDELINES PL2-C-1 WEATHER PROTECTION LOCATIONS AND COVERAGE; PL3-I-I PEDESTRIAN AMENITY/SETBACK, PL3-C-3 ANCILLARY ACTIVITIES; CS3-I-II ARCHITECTURAL FEATURES)</i>
6	23.47A.014.B.1: 15' TRIANGULAR SETBACK AT RESIDENTIAL LOTS A setback is required where a lot abuts the intersection of a side lot line and a front lot line of a lot in a residential zone. The required setback forms a triangular area 15-ft on a side.	Allow structure within the 15-ft triangular setback.	<i>The site's south property line abuts the side lot line of a residential zone. The neighboring residential property is raised on a 6-ft rockery. The intent of the 15-ft triangular setback is to mediate the nc zone transition to a residential scale. However, an undeveloped edge of the size in compliance with the setback will create a deep corner with little visibility to passersby, and may invite undesired activity. The neighboring southern property's 6-ft rockery would overshadow this corner and exacerbate this condition. A departure is requested to allow minimal construction (single-story) within the setback zone, with a minor plan setback to respect the neighboring single-family, as an alternative to the full setback depth and height. Change in topography at the southern property line limits the vertical encroachment into the setback to 9.5-ft above the adjacent property grade. (DESIGN GUIDELINES CS2-D-5 RESPECT FOR ADJACENT SITES; CS2-D-4 MASSING CHOICES, DC2-A-1 SITE CHARACTERISTICS AND USES; CS2-III-II ZONE EDGE CONDITION ONE)</i>
7	23.54.030.E: PARKING AISLE WIDTH Parking aisles: two-way traffic requires an aisle width of 24-ft or greater. (based on van stall)	Allow a narrower drive aisle (20-feet) to serve the aisle containing the ada van space.	<i>The ADA van space is located to provide convenient access to the building lobby. It is located nearest the garage entry of the street level garage, and has a compliant 24-ft backup depth behind the space. Beyond this stall the aisle width is narrowed to 20-ft, which is the compliant dimension for the remaining 7 small depth parking stalls in garage. The small site constrains the garage from providing a consistent aisle width across the parking aisle. If a consistent aisle width was required, the secure bike parking room would be removed, a feature of the building encouraging bicyclists and aligned with design guideline p14 active transportation. (DESIGN GUIDELINES PL3-I-I PEDESTRIAN AMENITY/SETBACK AND PL3-C-3 ANCILLARY ACTIVITIES)</i>
8	23.47A.014.E.1. STRUCTURES IN SETBACKS: DECKS WITH OPEN RAILINGS Decks with open railings may extend into the required setback, but are not permitted within five (5) feet of a lot in a residential zone.	Allow the railings of a deck to extend within 5-ft of a lot line in a residential zone.	<i>The site's south property line abuts the side lot line of a residential zone. The 2nd floor terrace is located lower than the required setback above 13' in height and therefore in compliance with the code and does not require a departure. However, due to the terraces proximity to the southern property line, and the neighboring house's proximity to the property line, a taller railing is desired on the southern edge of the terrace for privacy screening. The screening rail will be an open slat screen to reduce views from the terraces and upper levels of the new building into the southern residential neighbor's garage, and reduce view angles up from the neighbor's yard to the new building. This screen would impede into the 13' in height setback by 1'-9" in height, and be located less than 5' from the residential lot line. (DESIGN GUIDELINES: CS2-III-I ZONE EDGE CONDITION ONE, CS2-D-5 RESPECT FOR ADJACENT SITES)</i>

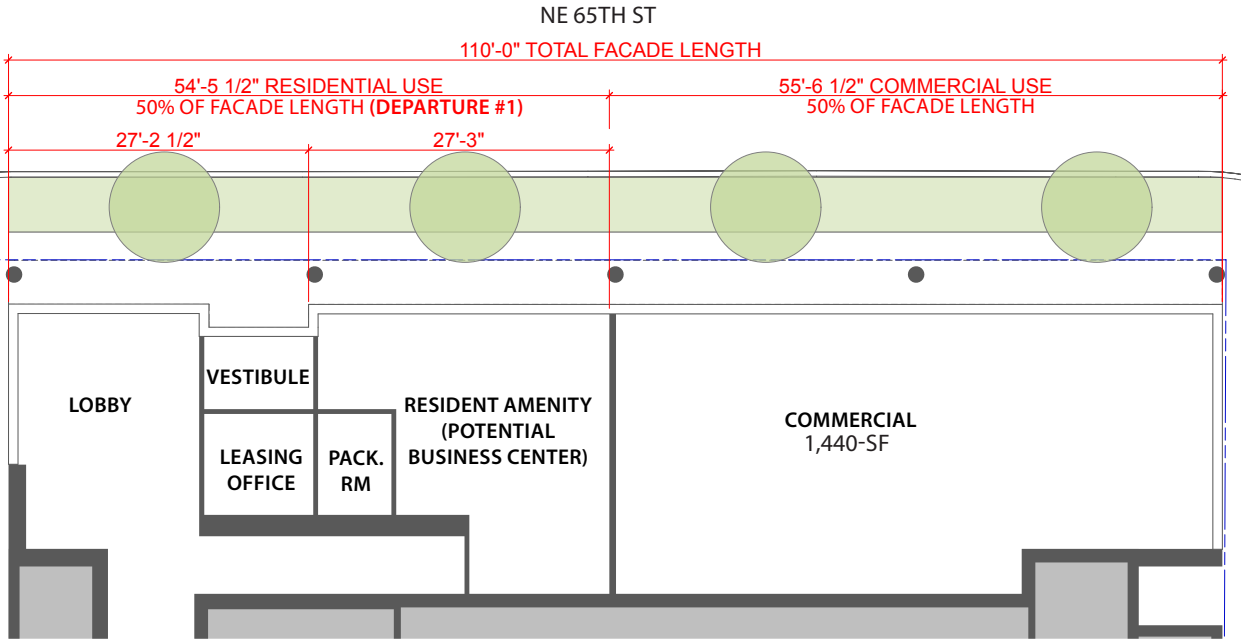
DEVELOPMENT STANDARD	DEPARTURE	RATIONALE FOR REQUESTED DEPARTURE	DESIGN GUIDELINES
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2a 23.47A.005.D.1: USES AT PRINCIPAL PEDESTRIAN STREETS Along designated principal pedestrian streets, one or more of the following is required along 80% of street-facing façade: general sales, retail, eating and drinking establishments.	Allow a smaller percentage (50%) of the required uses along NE65th St. This percentage requirement is referred to in both sections listed here as Departures 2a and 2b.		
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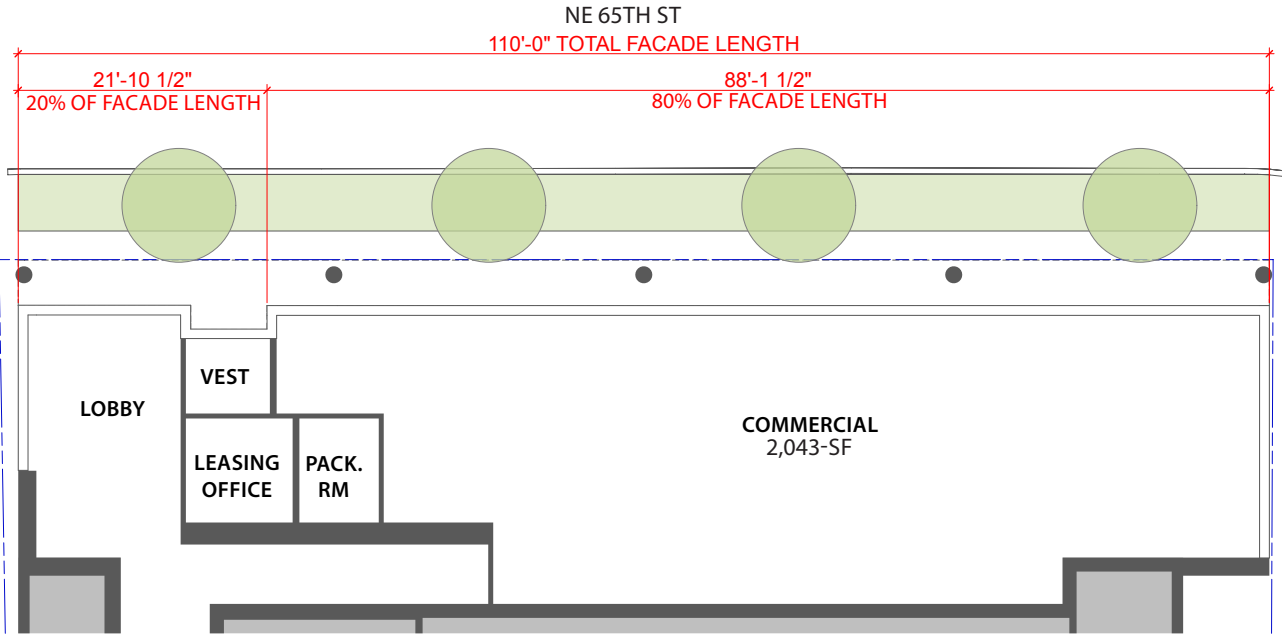
PROPOSED FIRST FLOOR NE 65TH ST ELEVATION



COMPLIANT/FUTURE FIRST FLOOR NE 65TH ST ELEVATION



PROPOSED FIRST FLOOR PLAN

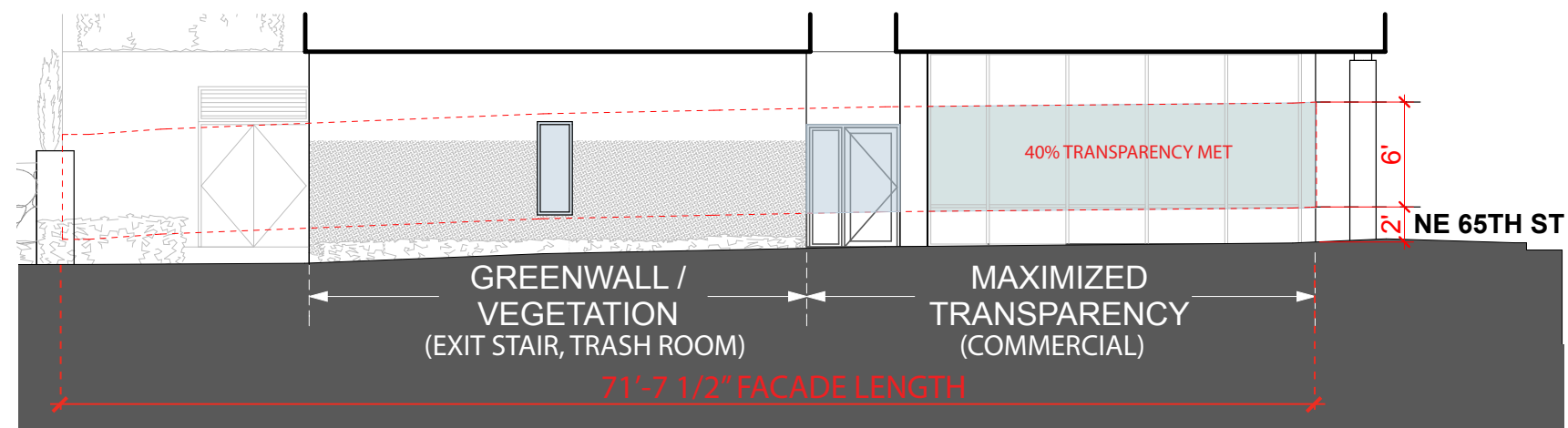


COMPLIANT/FUTURE FIRST FLOOR PLAN

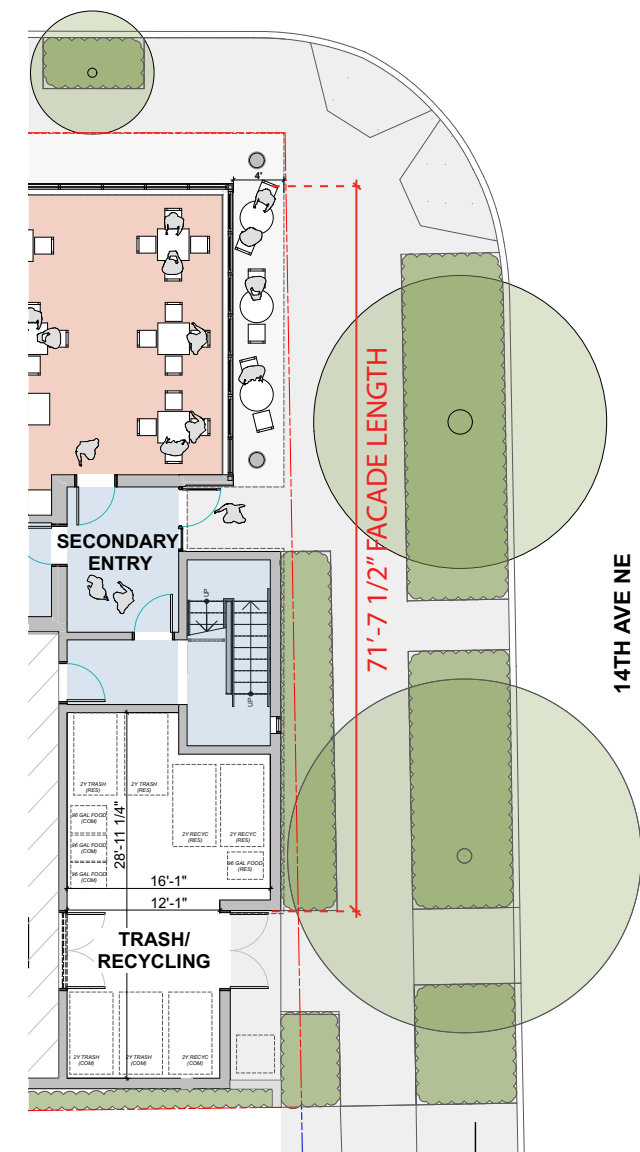


DEPARTURE REQUEST DIAGRAMS

DEVELOPMENT STANDARD	DEPARTURE	RATIONALE FOR REQUESTED DEPARTURE	DESIGN GUIDELINES
<p>3 23.47A.008.B.2A: STREET LEVEL TRANSPARENCY 60% of the street-facing façade between 2-ft and 8-ft above the sidewalk shall be transparent.</p>	<p>Allow landscaping in lieu of transparency on the 14th Ave façade.</p>	<p><i>14th Ave NE is a quiet residential street south of the project site. An activated, transparent facade across the length of the building would not be an appropriate fit for the neighbors. A highly activated and transparent commercial façade is located on the northern half of the 14th Ave façade. To respect the existing single-family condition south of the site, and due to the constraints of fitting the building program and services on a small site, quieter building elements are located on the south end of the 14th Ave facade. These uses are not conducive to transparency and would be better served by intentional landscaping than windows. This landscape approach is consistent with qualities of 14th Ave NE, which is characterized by heavy vegetation along the sidewalk. (DESIGN GUIDELINES CS3-I-II ARCHITECTURAL FEATURES: TRANSPARENCY, PL3-C RETAIL EDGES)</i></p>	<p>DC2 ARCHITECTURAL CONCEPT (Secondary Architectural Features: Fit with Neighboring Buildings, Architectural and Facade Composition: Blank Walls) ...”avoid large blank walls along visible facades where possible, include uses at street level that are designed for pedestrians... green walls, landscaped areas or raised planters”... “use design elements to achieve a successful fit between a building and its neighbors...”using trees and or landscaping to enhance the building design and fit with the surrounding context”</p>



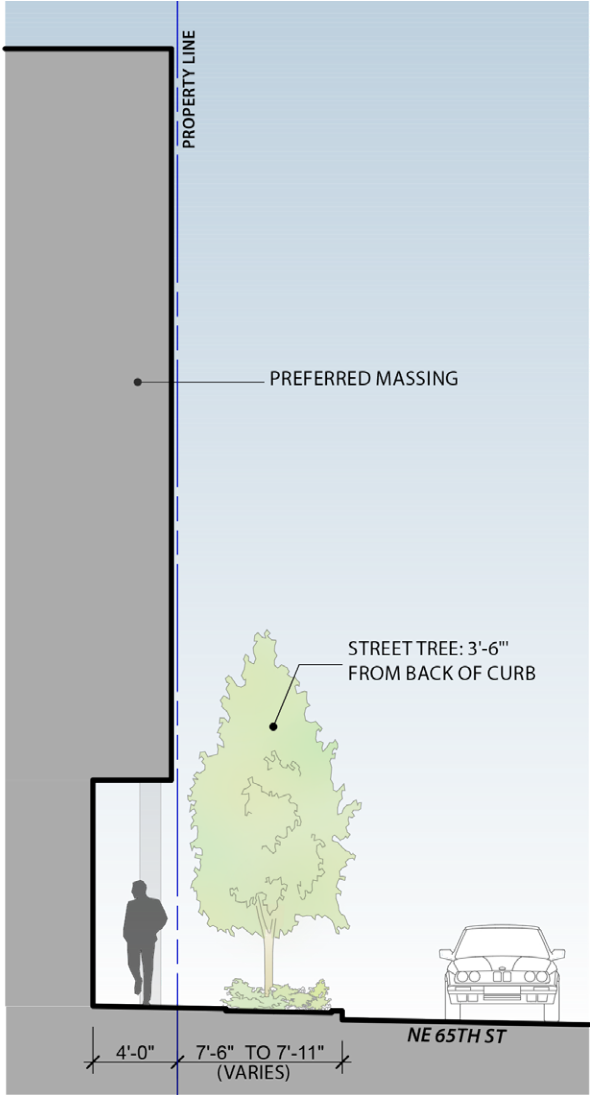
PROPOSED FIRST FLOOR 14TH AVE NE ELEVATION



PROPOSED FIRST FLOOR PLAN



DEVELOPMENT STANDARD	DEPARTURE	RATIONALE FOR REQUESTED DEPARTURE	DESIGN GUIDELINES
<p>4 23.47A.008.B.3: NON-RESIDENTIAL USE DEPTH</p> <p>Non-residential uses shall extend an average depth of at least 30-ft and a minimum depth of 15-ft from the street-level street-facing façade.</p>	<p>Allow a reduced depth an average of 24.8-ft for the commercial space.</p>	<p><i>NE 65th St has a very narrow pedestrian row adjacent to a busy arterial. In its current state there is no space available for street trees. A project goal is to enhance the pedestrian environment on NE 65th. To provide space for improvements, the street level on NE 65th is set back 4-ft from the property line. Due to the building's narrow site, the depth of this setback is taken out of the commercial depth. This setback creates a wider sidewalk, with space for street trees and planting to increase separation between pedestrians and traffic. (DESIGN GUIDELINES PL1-B-2 WALKWAYS AND CONNECTIONS: PEDESTRIAN VOLUMES)</i></p>	<p>PL1 CONNECTIVITY (Walkways and Connections: Pedestrian Volumes) ...“provide ample space for pedestrian flow and circulation”... “</p> <p>PL2 WALKABILITY (Weather Protection: Locations and Coverage) ...“overhead weather protection encouraged.... provide continuous coverage the length of the building, where possible”</p> <p>Roosevelt I Supplemental Guidance (Human Activity along sidewalks) ...“encourage pedestrian activity along sidewalks... applicants encouraged to increase ground level setback in order to accomodate pedestrian traffic”</p>



PROPOSED STREETWALL SECTION

- 4' setback of street level facade from property line
- Continuous 4' deep weather protection at 100% of facade length

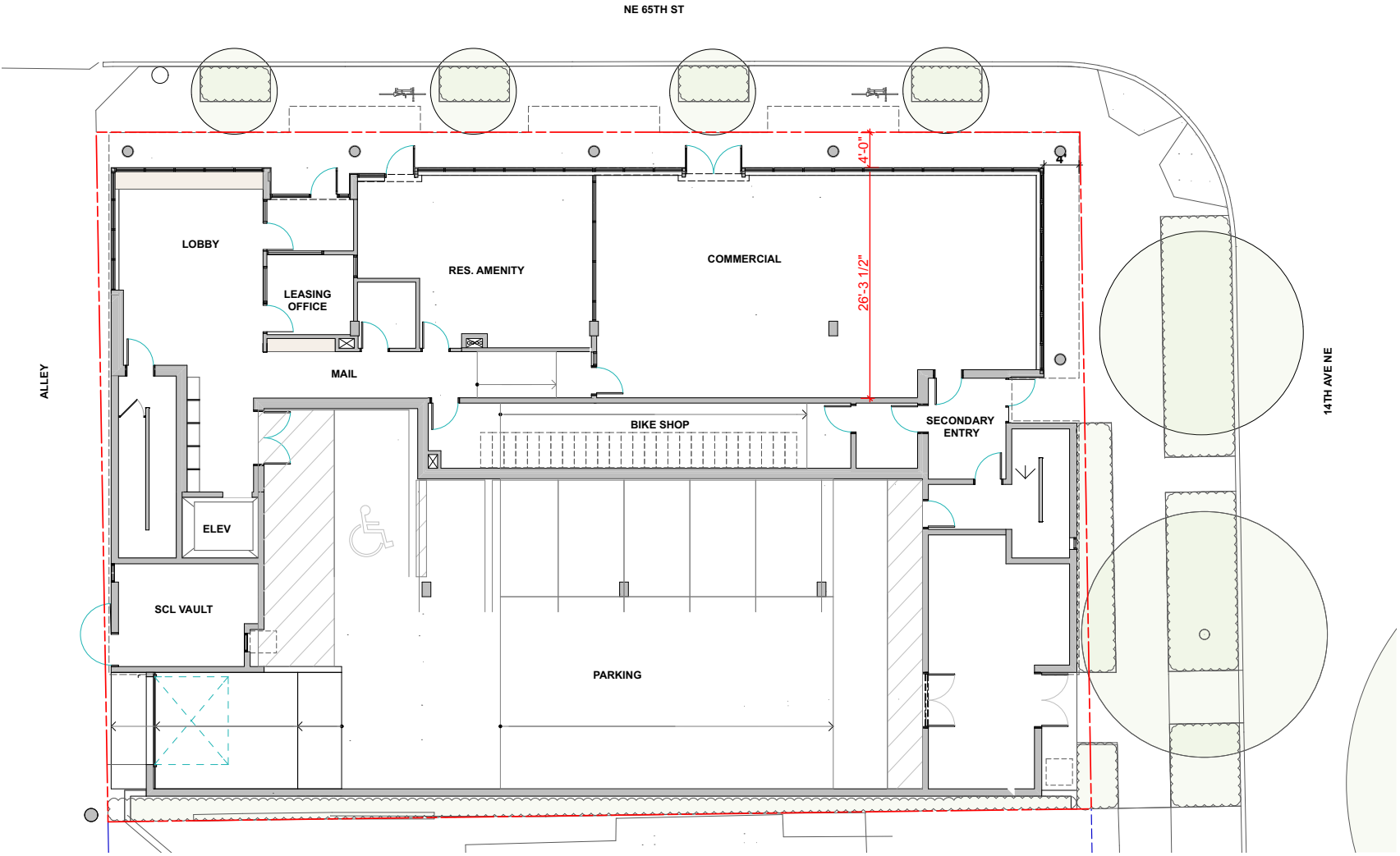


PROPOSED VIEW FROM NE CORNER

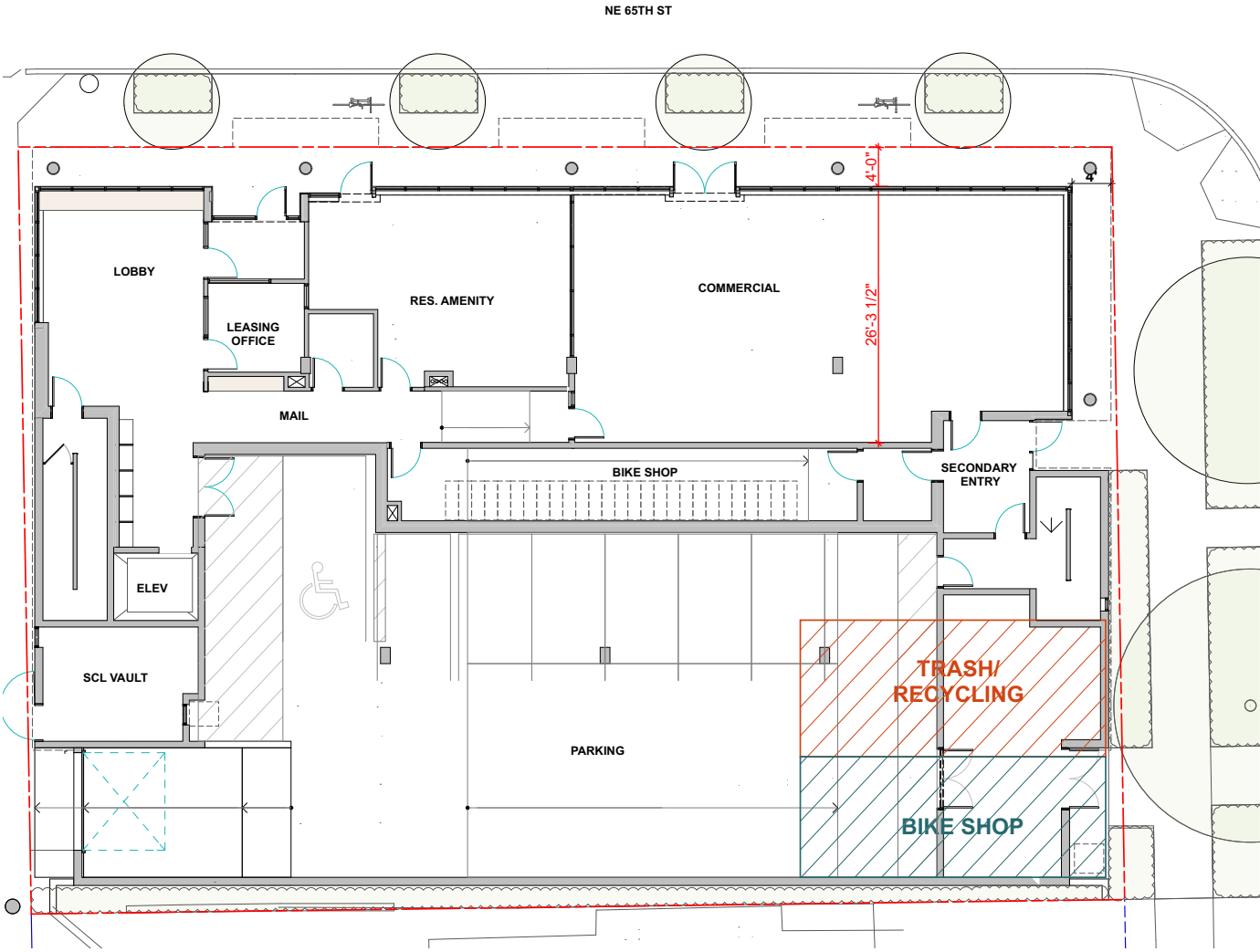


PROPOSED VIEW ALONG NE 65TH

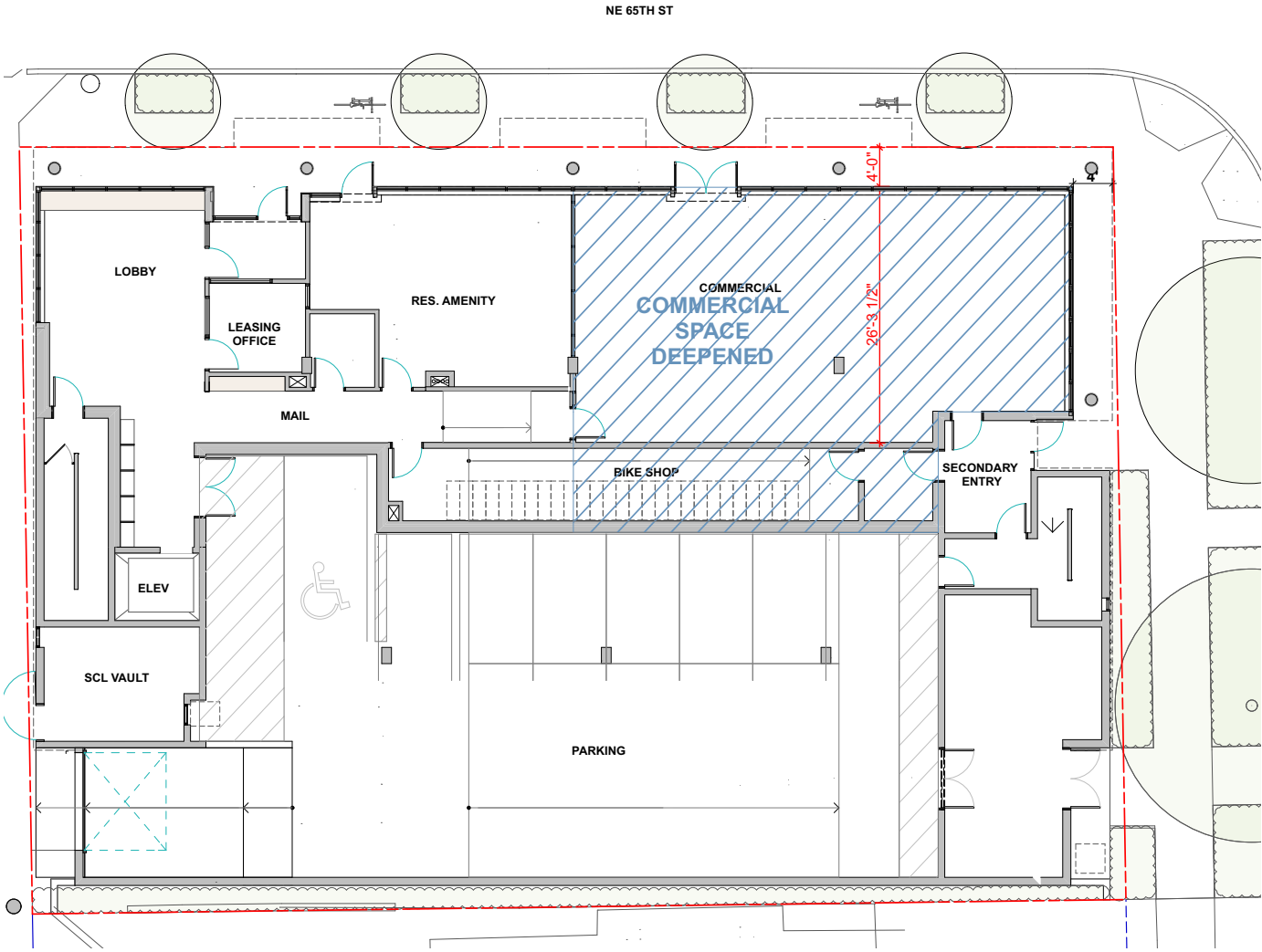
DEVELOPMENT STANDARD	DEPARTURE	RATIONALE FOR REQUESTED DEPARTURE	DESIGN GUIDELINES
5 23.47A.008.C.4: OVERHEAD WEATHER PROTECTION Continuous overhead weather protection is required along at least 60% of the street frontage of a structure on a principal pedestrian street. The covered area shall have a min. Width of 6-ft.	Allow 4' deep weather protection instead of 6' deep weather protection.	<i>NE 65th St has a very narrow pedestrian row adjacent to a busy arterial. In its current state there is no space available for street trees. As described in departure request #4, the street level on NE 65th St is set back 4-ft from the property line to create space to add street trees. This setback also creates built-in 4-ft deep weather protection for pedestrians across the entire length of the building (greater coverage than the required 60% coverage per code), without interfering with the space required for street trees. Additionally, three 15-ft long bay window projections above street level provide additional deeper (7-ft) weather protection at intervals across the NE 65th façade. (DESIGN GUIDELINES PL2-C-1 WEATHER PROTECTION LOCATIONS AND COVERAGE; PL3-I-I PEDESTRIAN AMENITY/SETBACK, PL3-C-3 ANCILLARY ACTIVITIES; CS3-I-II ARCHITECTURAL FEATURES)</i>	PL1 CONNECTIVITY (Walkways and Connections: Pedestrian Volumes) ...“provide ample space for pedestrian flow and circulation”... PL2 WALKABILITY (Weather Protection: Locations and Coverage) ...“overhead weather protection encouraged.... provide continuous coverage the length of the building, where possible” Roosevelt I Supplemental Guidance (Human Activity along sidewalks) ...“encourage pedestrian activity along sidewalks... applicants encouraged to increase ground level setback in order to accomodate pedestrian traffic”



PROPOSED FIRST FLOOR PLAN
(showing setback depth subtracted from commercial depth)



ALTERNATE FIRST FLOOR PLAN (PARKING COMPROMISED)



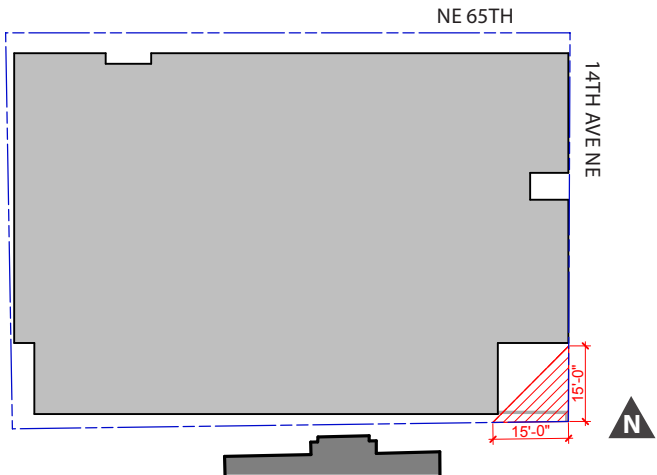
ALTERNATE FIRST FLOOR PLAN (BIKE ROOM COMPROMISED)

DEPARTURE REQUEST DIAGRAMS

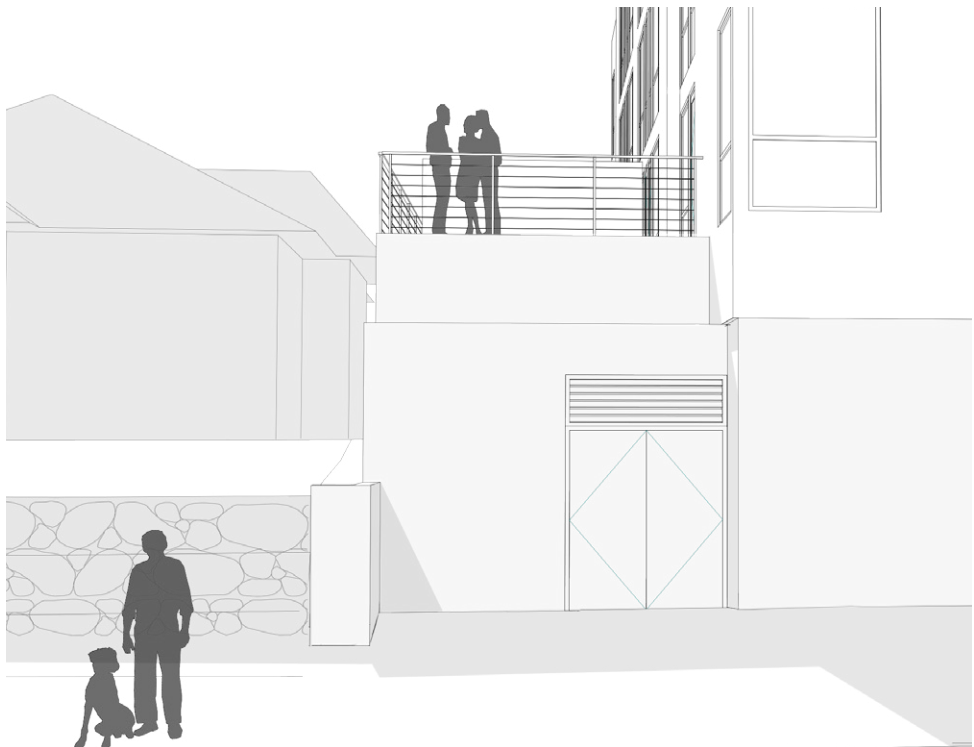
DEVELOPMENT STANDARD	DEPARTURE	RATIONALE FOR REQUESTED DEPARTURE	DESIGN GUIDELINES
<p>6 23.47A.014.B.1: 15' TRIANGULAR SETBACK AT RESIDENTIAL LOTS</p> <p>A setback is required where a lot abuts the intersection of a side lot line and a front lot line of a lot in a residential zone. The required setback forms a triangular area 15-ft on a side.</p>	<p>Allow structure within the 15-ft triangular setback.</p>	<p><i>The site's south property line abuts the side lot line of a residential zone. The neighboring residential property is raised on a 6-ft rockery. The intent of the 15-ft triangular setback is to mediate the nc zone transition to a residential scale. However, an undeveloped edge of the size in compliance with the setback will create a deep corner with little visibility to passersby, and may invite undesired activity. The neighboring southern property's 6-ft rockery would overshadow this corner and exacerbate this condition. A departure is requested to allow minimal construction (single-story) within the setback zone, with a minor plan setback to respect the neighboring single-family, as an alternative to the full setback depth and height. Change in topography at the southern property line limits the vertical encroachment into the setback to 9.5-ft above the adjacent property grade. (DESIGN GUIDELINES CS2-D-5 RESPECT FOR ADJACENT SITES; CS2-D-4 MASSING CHOICES, DC2-A-1 SITE CHARACTERISTICS AND USES; CS2-III-II ZONE EDGE CONDITION ONE)</i></p>	<p>CS2 URBAN PATTERN AND FORM (Height Bulk and Scale, Respect for Adjacent Sites) ...“review the height, bulk and scale of neighboring buildings... scale of development anticipated.. determine an appropriate complement/transition”...“respect adjacent properties to minimize disturbing the privacy of adjacent buildings”... Roosevelt III Supplemental Guidance (Height, Bulk and Scale: Zone Edge Condition One: rear lot line of commercial abuts side of residential zone) ...“step back the upper floors”</p>



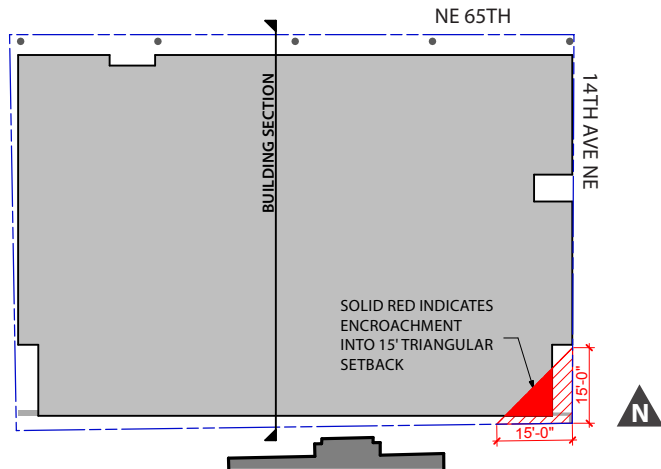
CODE COMPLIANT PERSPECTIVE OF S-E CORNER (FROM 14TH AVE)



CODE COMPLIANT FIRST FLOOR PLAN



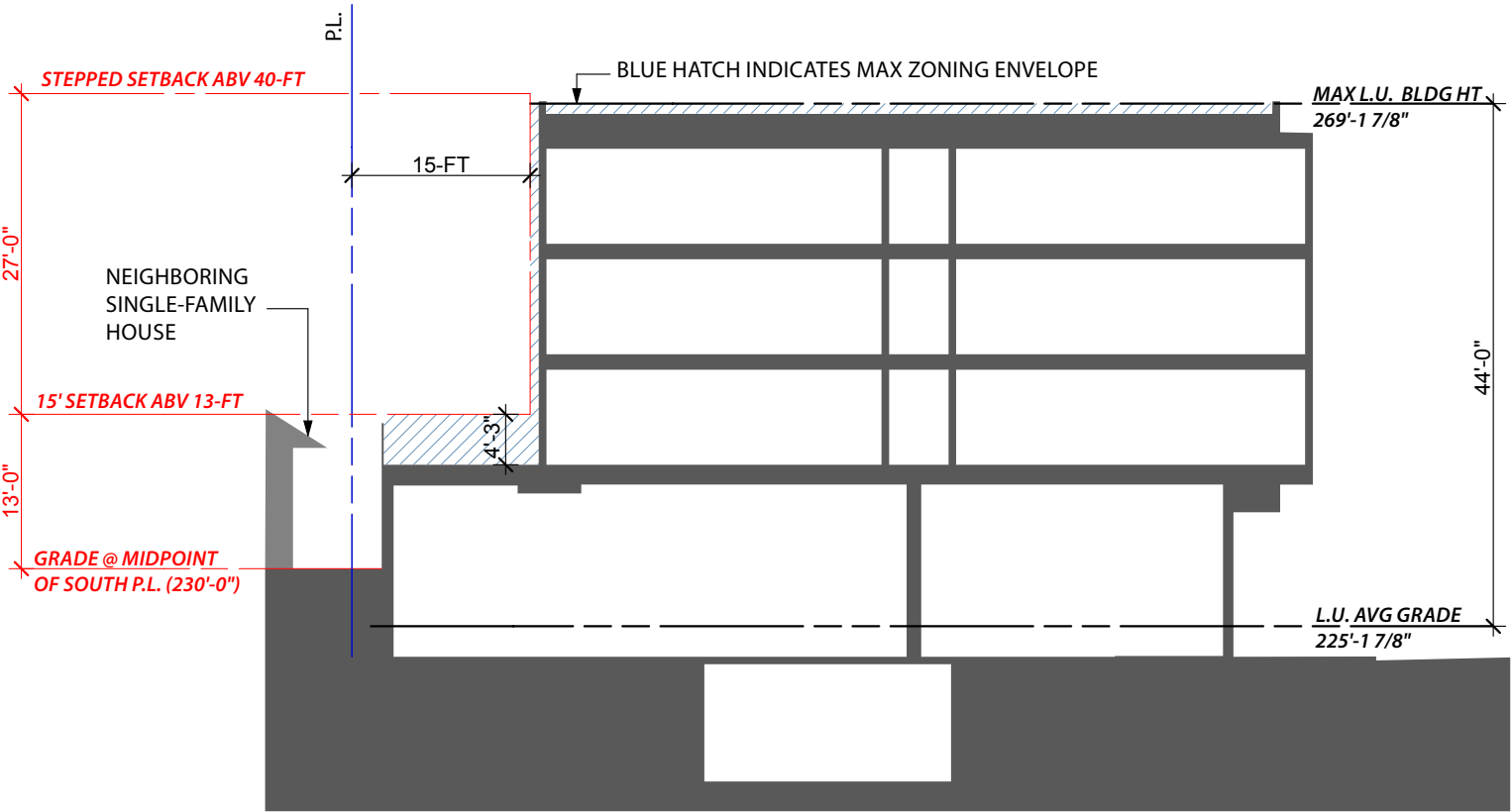
PROPOSED PERSPECTIVE OF S-E CORNER (FROM 14TH AVE)



PROPOSED FIRST FLOOR PLAN



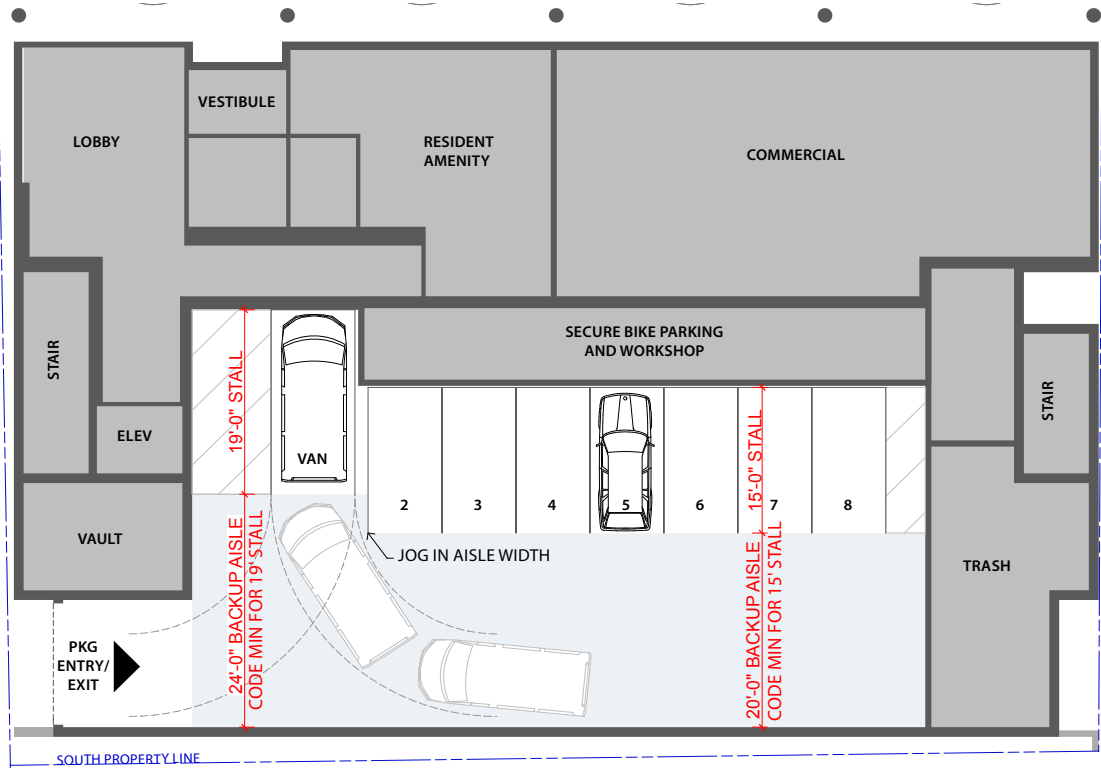
PROPOSED PERSPECTIVE OF S-E CORNER (FROM 14TH AVE) SHOWING LANDSCAPING



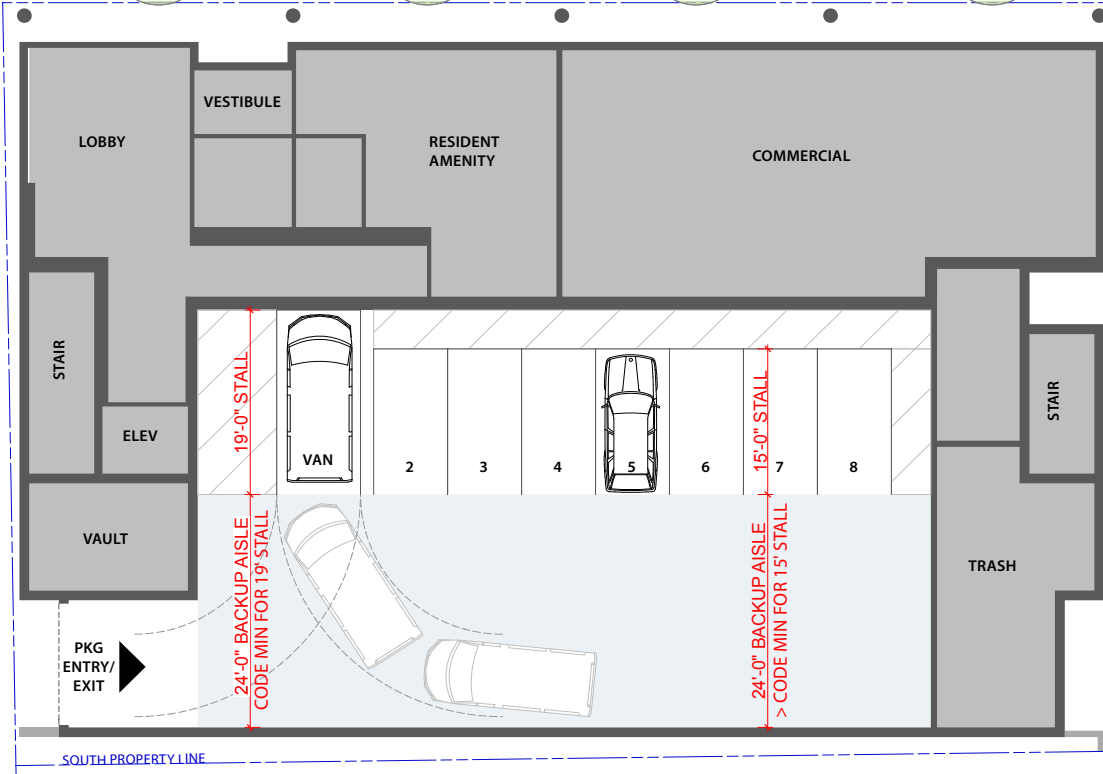
PROPOSED: BUILDING SECTION SHOWING MAX ZONING ENVELOPE

DEPARTURE REQUEST DIAGRAMS

DEVELOPMENT STANDARD	DEPARTURE	RATIONALE FOR REQUESTED DEPARTURE
7 23.54.030.E: PARKING AISLE WIDTH Parking aisles: two-way traffic requires an aisle width of 24-ft or greater. (based on van stall)	Allow a narrower drive aisle (20-feet) to serve the aisle containing the ada van space.	<i>The ADA van space is located to provide convenient access to the building lobby. It is located nearest the garage entry of the street level garage, and has a compliant 24-ft backup depth behind the space. Beyond this stall the aisle width is narrowed to 20-ft, which is the compliant dimension for the remaining 7 small depth parking stalls in garage. The small site constrains the garage from providing a consistent aisle width across the parking aisle. If a consistent aisle width was required, the secure bike parking room would be removed, a feature of the building encouraging bicyclists and aligned with design guideline pl4 active transportation. (DESIGN GUIDELINES PL3-I-I PEDESTRIAN AMENITY/SETBACK AND PL3-C-3 ANCILLARY ACTIVITIES)</i>



PARKING AISLE WIDTH: DEPARTURE #7 : PROPOSED PLAN

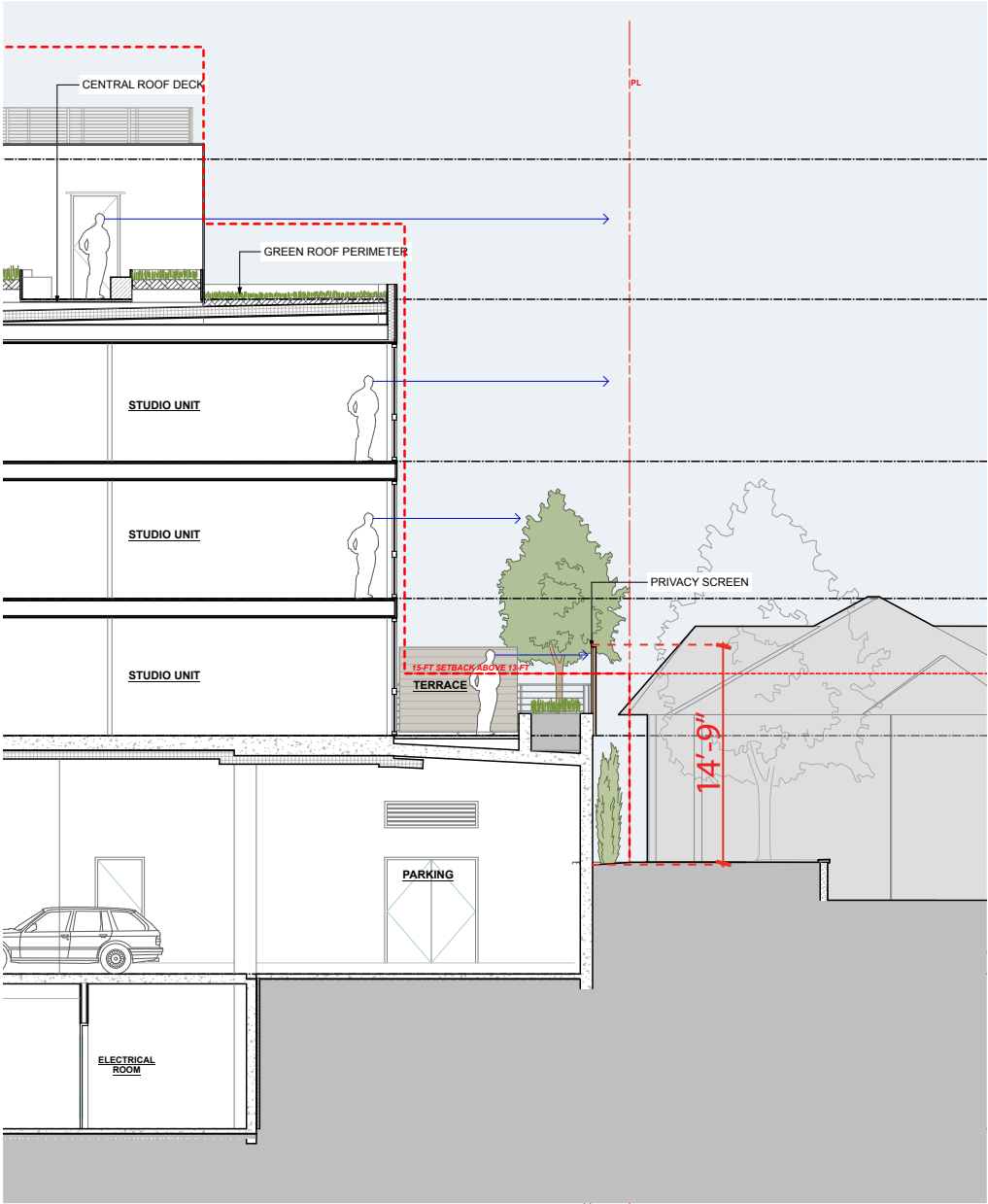


PARKING AISLE WIDTH: DEPARTURE #7 : COMPLIANT PLAN (NO BIKE ROOM)

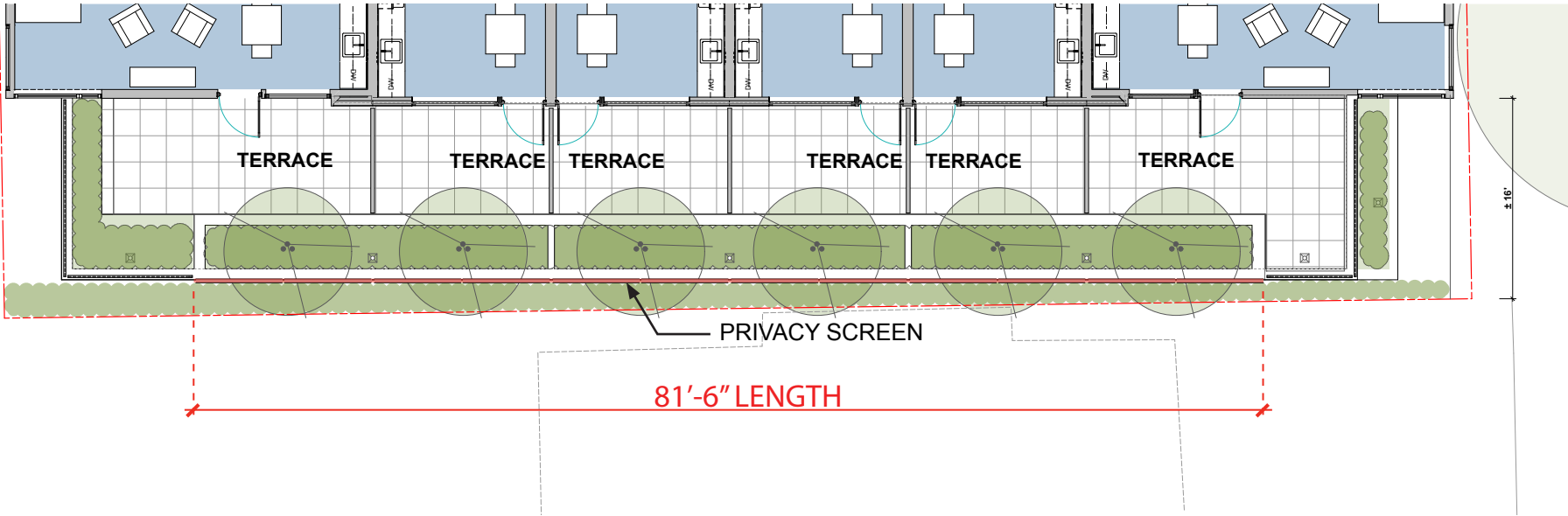
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DEPARTURE REQUEST DIAGRAMS

DEVELOPMENT STANDARD	DEPARTURE	RATIONALE FOR REQUESTED DEPARTURE
<p>8 23.47A.014.B.3.a Setback requirements for lots abutting residential zones 15-foot setback above 13-feet in height to a maximum of 40-feet</p> <p>23.47A.014.E.5.a Structures in Required Setbacks Fences 6-feet or less in height above existing or finished grade are permitted in required setbacks</p>	<p>Allow a privacy screen (fence) greater than 6-feet above grade and within the required side setback above 13-feet.</p>	<p>The site's south property line abuts the side lot line of a residential zone. The 2nd floor terrace is located lower than the required setback above 13' in height and therefore in compliance with the code and does not require a departure. However, due to the terraces proximity to the southern property line, and the neighboring house's proximity to the property line, a taller privacy screen is desired on the southern edge of the terrace. The privacy screen will be an open slat screen to reduce views from the terraces and upper levels of the new building into the southern residential neighbor's garage, and reduce view angles up from the neighbor's yard to the new building. This screen would impede into the 13' in height setback by 1'-9" in height, and be located less than 5' from the residential lot line. (DESIGN GUIDELINES: CS2-III-I ZONE EDGE CONDITION ONE, CS2-D-5 RESPECT FOR ADJACENT SITES)</p>



PRIVACY SCREEN: DEPARTURE #8: PROPOSED SECTION



PRIVACY SCREEN: DEPARTURE #8: PROPOSED PLAN



PRIVACY SCREEN: DEPARTURE #8: PROPOSED VIEW FROM NEIGHBOR'S YARD

SEATTLE DESIGN GUIDELINES	DESIGN RESPONSE
CS2 Urban Pattern and Form <i>Strengthen the most desirable forms, characteristics, and patterns of the streets, block faces, and open spaces in the surrounding area.</i>	
C1 Relationship to the Block, Corner Sites Corner sites can serve as gateways or focal points; both require careful detailing at the first three floors due to their high visibility from two or more streets and long distances. Consider using a corner to provide extra space for pedestrians and a generous entry, or build out to the corner to provide a strong urban edge to the block	The building’s corner condition at the intersection of NE 65th St and 14th Ave NE was well considered in the building program layout and façade design. At street level, the prominent corner is held by a commercial/restaurant space for visibility at greater distances and increased likelihood of success of the future tenant. The street level façade is set back 4-ft from the property line on NE 65th St, and this setback wraps around the corner of the commercial/restaurant to highlight the corner condition and widen the sidewalk for activity or potential seating. The façade at the corner is highly transparent, and the 14th Ave NE side may include an operable storefront for increased permeability at the corner. Spill-out space in the setback on 14th Ave NE for a commercial or restaurant space will further accent the corner, and be visible from blocks east on NE 65th St, a designated arterial street and primary pedestrian street. Above street level, large corner windows in the residential units provide scaled modulation at the upper levels.
D1 Height Bulk and Scale, Existing Development & Zoning Review the height, bulk, and scale of neighboring buildings as well as the scale of development anticipated by zoning for the area to determine an appropriate complement and/or transition. Note that existing buildings may or may not reflect the density allowed by zoning or anticipated by applicable policies	<p>The project site is within a Neighborhood Commercial 1 zone with a Pedestrian Overlay and 40-ft height limit (NC1P-40), and eligible for an additional 4-ft of height per SMC 23.47A.012.A.1.c. East and west of the project site the adjacent properties are also zoned NC1P-40. Across NE 65th St to the north, properties are zoned Neighborhood Commercial 2 with a Pedestrian Overlay and a height limit of 65-ft. The project site has a zone transition at the southern property line to single-family (SF-5000). Currently the neighboring sites, including the sites zoned Neighborhood Commercial, are empty or occupied by single-family.</p> <p>The scale and bulk of the building is designed to respect the neighboring single-family, but also fit into a future streetscape on NE 65th St of taller structures. The primary mass of the building is a 4-story linear bar aligned with and set on the north property line at NE 65th St, to fit into that future streetscape. The bar is a double-loaded corridor with glazed “gasket” insets at the east and west elevations, to break the façade’s 61.5-ft bulk width into two smaller portions. The height of the bar is roughly 43-ft above average grade plane, and parapet of the building is minimal to reduce the overall perceived height of the building.</p> <p>Behind the 4-story bar the building steps down to a single story structure to transition to a scale appropriate for the neighboring single-family zone. See the response below to CS2-D-3: Zone Transitions for further detail on the designed transition at the southern property line.</p>
D3 Height Bulk and Scale, Zone Transitions For projects located at the edge of different zones, provide an appropriate transition or complement to the adjacent zone(s). Projects should create a step in perceived height, bulk and scale between the anticipated development potential of the adjacent zone and the proposed development	As described above in CS-2-D-1, the mass of the building steps down to a single-story height roughly 15-ft north of the southern property line in respect to the neighboring single-family zone. Due to the southern neighboring site’s raised grade above the rights-of-way, the height of this single-story structure at the southern property line is approximately 8.75-ft in height, significantly lower than the code-allowed 13-ft structure height limit. The single-height structure is used as a residential terrace for units at the second floor of the proposed building. Further ancillary detailing of the edge of this terrace for screening purposes and added privacy for the southern neighbors is described in response to DC2-C-3: Fit with Neighboring Buildings below.
D4 Urban Pattern and Form, Height, Bulk and Scale: Massing Choices Strive for a successful transition between zones where a project abuts a less intense zone.	As described above, the project site abuts a single-family zone. In response, the tallest mass of the building is weighted towards the northern side of the property, adjacent to other Neighborhood Commercial zones with height limits of 40-ft and 65-ft, and a smaller massing height is adjacent to the single-family zone. For greater detail regarding massing, see responses to CS2-D-1: Existing Development and Zoning, and CS2-D-3: Zone Transitions.

	D5 Height Bulk and Scale, Respect for Adjacent Sites Respect adjacent properties with design and site planning to minimize disrupting the privacy and outdoor activities of residents in adjacent buildings	Three sides of the project site front public rights-of-way: to the north is NE 65th St, to the east is 14th Ave NE, and to the west is a mid-block alley. Privacy issues in these directions are less critical than at the fourth property line (south), which is shared with the side lot-line of a single-family home. The one-story structure on that site is located close to the shared lot line at 5.5-ft typically, with a bump-out located closer at 2.4-ft. To respect the southern neighbor's privacy, the building is setback 15-ft above the first floor. Consequently all residential units are located 15-ft from this shared property line to minimize views from windows in the new building into the neighbor's yard, and from the neighbors yard back up to the new building. Residential terraces are located on the lid of the single-story setback, but they are designed with a built-in planter at the southern edge of the terrace that keeps people back 5-ft from the terrace edge, and 7.25-ft back from the shared property line. Additional privacy screening is provided with built-in railing screens and vegetation. Further detail on the terrace edge is provided in DC2-C-3: Fit with Neighboring Buildings.
Roosevelt i Supplemental Guidance CS2-II	Corner Lots: Gateways Gateway features could include a variety of design elements that enhance these prominent neighborhood intersections identified below. See guidelines for gateways locations. The following design elements are encouraged: a. special paving or surface treatments; b. art; c. water features; d. landscaping;; e. seating; f. kiosks, etc.	See the response to CS2-C-1: Relationship to the Block: Corner Sites above.
Roosevelt i Supplemental Guidance CS2-III	Height, Bulk and Scale Compatability, Commercia/Residential Zone Edges Map Careful siting, building design and building massing at the upper levels should be used to achieve a sensitive transition between multifamily and commercial zones as well as mitigating height, bulk and scale impacts. Some of the techniques already identified in the citywide design guidelines are preferred in Roosevelt. These techniques include: a. increasing building setbacks from the zone edge at ground level; b. reducing the bulk of the building's upper floors; c. reducing the height of the structure; d. use of landscaping or other screening (such as a 5-foot landscape buffer). e. Departures to development standards are encouraged in Roosevelt in order to create a positive transition along zone edges.	See the responses to CS2-D-3: Zone Transitions and CS2-D-5: Respect for Adjacent Sites above.
Roosevelt ii Supplemental Guidance CS2-III	Height, Bulk and Scale Compatability, Zone Edge Condition One Where a rear lot line of a commercially zoned lot (height limit of 30, 40 or 65 feet) abuts a side or rear of a residentially zoned lot (height limit of 25-35 feet). Examples of recommended design methods follow in order of preference: a. For commercial uses, place surface parking and access behind commercial buildings; b. Increase building setbacks along zone edges; c. Step back the upper floors or modify the roofline to reduce the overall building height.	See the responses to CS2-D-1: Existing Development and Zoning, CS2-D-3: Zone Transitions, and CS2-D-5: Respect for Adjacent Sites above.

CS3 Architectural Context and Character		
Contribute to the architectural character of the neighborhood		
Roosevelt ii Supplemental Guidance CS3-I	Architectural Context, Architectural Features Features preferred in Roosevelt include the following: a. Building base emphasizing materials and/or texture that is different from the material(s) and texture(s) of the main body of the building b. Kickplate c. Ground floor storefront transparent windows that allow pedestrians to see activity within the building d. Ground floor display windows (where product displays are changed frequently to create interest along the street) e. Recessed entries on the street level and building modulation on the upper levels f. Transom windows g. Upper level windows that are interrupted by solid façade area h. Parapet cap or cornice i. Beltcourse j. Marquee or awning: marquees or retractable awnings are generally preferred k. Arcades l. Change in materials m. Variety in color and/or texture n. Building overhangs (where upper levels are brought closer to a front property line) o. Courtyards	The proposed 4-story building has a distinct base and top. The first floor of the building, in alignment with the Roosevelt Guidelines, is differentiated from the residential stories above. On NE 65th St and the northern portion of 14th Ave NE, the street-level is set back 4-ft from the property line to widen the sidewalk, and is highly glazed with storefront. Programmatic elements on these facades include commercial space, the residential lobby, and residential amenities spaces, all of which benefit from high transparency and pedestrian interaction. The entrance to the residential lobby is further recessed from the façade to create a sense of arrival for visitors. Above the street level the upper three floors extend back out to the property lines, supported by a line of exterior columns below. This overhang creates built-in weather protection at the street level. A series of three bay window projections march along NE 65th St and provide a secondary pattern to the structure. The materials at the upper levels are primarily painted fiber cement board and large floor-to-ceiling windows, providing modulation to the façade.
PL2 Walkability		
Create a safe and comfortable walking environment that is easy to navigate and well-connected to existing pedestrian walkways and features		
	C1 Weather Protection, Locations and Coverage Overhead weather protection is encouraged and should be located at or near uses that generate pedestrian activity such as entries, retail uses, and transit stops. Address changes in topography as needed to provide continuous coverage the full length of the building, where possible	The site is located two blocks east of the new Roosevelt Light Rail Station on NE 65th St, a designated Primary Pedestrian Street well served by bus lines. Overhead weather protection is provided continuously on NE 65th St, and wraps the corner to 14th Ave NE. The weather protection is provided by a 4-ft setback of the first floor of the building.
	C2 Weather Protection, Design Integration Integrate weather protection, gutters and downspouts into the design of the structure as a whole, and ensure that it also relates well to neighboring buildings in design, coverage, or other features.	As described above in CS3-I-i: Architectural Features, and PL2-C-1: Weather Protection Locations and Coverage, weather protection is provided inherently in the massing of the building due to the continuous setback of the street-level façade on NE 65th St and the corner of 14th Ave NE. Rain collection from the roof is by rain leaders internal to the building so as not to disrupt the simple planes of the façade design.
	C3 Weather Protection, People-Friendly Spaces Create an artful and people-friendly space beneath the building.	As described above in CS3-I-l: Architectural Features, the street level façade on NE 65th St and 14th Ave NE is set back from the property line by 4-ft, to create a wider pedestrian realm. This added dimension to the streetscape provides enough space for street trees and new planting medians at the curb, bike racks, and potentially café-seating underneath the building soffit at the building edge.
PL3 Street-Level Interaction		
Encourage human interaction and activity at the street-level with clear connections to building entries and edges		
	C1 Retail Edges, Porous Edge Engage passersby with opportunities to interact visually with the building interior using glazing and transparency. Create multiple entries where possible and make a physical and visual connection between people on the sidewalk and retail activities in the building.	The street level on NE 65th St is heavily glazed for transparency for views into and out of the new building. Due to the gradual but consistent grade change on NE 65th street, a single entry is provided into the residential lobby, and a single entry is provided into the commercial space, where ADA compliant grades can be accommodated. However, as the building turns the corner onto 14th Ave NE, the design team is exploring the potential of an operable storefront to increase the permeability of the commercial space.

Roosevelt i Supplemental Guidance I	C2 Retail Edges, Visibility Maximize visibility into the building interior and merchandise displays. Consider fully operational glazed wall-sized doors that can be completely opened to the street, increased height in lobbies, and/ or special lighting for displays	As described above in PL3-C-1: Porous Edge, the street level is highly transparent on NE 65th St and the corner of 14th Ave NE, with potential for operable storefront on 14th. The street level is also tall, with a floor-to-floor height between 16-ft and 15-ft to the second floor level.
	C3 Retail Edges, Ancillary Activities Allow space for activities such as sidewalk vending, seating, and restaurant dining to occur. Consider setting structures back from the street or incorporating space in the project design into which retail uses can extend.	See the responses to CS2-C-1: Corner Sites and PL2-C-3: People-Friendly Spaces, for further detail on the 4-ft setback at street-level on NE 65th St and the corner of 14th Ave NE, and the potential integration of café seating into the setback on 14th Ave NE.
	Human Activity, Pedestrian Amenity / Setback Roosevelt is looking for opportunities to encourage pedestrian activity along sidewalks within the Commercial Core. This is especially important because sidewalks along Roosevelt and 65th are considered too narrow. If not required with new development, applicants are encouraged to increase the ground level setback in order to accommodate pedestrian traffic and amenity features.	See the responses to CS2-C-1: Corner Sites and PL2-C-3: People-Friendly Spaces, for further detail on the 4-ft setback at street-level on NE 65th St and the corner of 14th Ave NE.
PL4 Active Transportation <i>Incorporate design features that facilitate active forms of transportation such as walking, bicycling and use of transit</i>		
	C1 Planning Ahead for Transit, Influence on Project Design Identify how a transit stop (planned or built) adjacent to or near the site may influence project design, provide opportunities for place making, and/or suggest logical locations for building entries, retail uses, open space, or landscaping. Take advantage of the presence of transit patrons to support retail uses in the building.	The location of the Roosevelt Light Rail Station (currently in construction), two blocks west of the project site on NE 65th St was a driver in the building program layout and street-level design. The location of the main residential entry on the west end of the NE 65th St was selected for proximity to the new station for commuting residents and visibility to pedestrians walking from the new station. The 4-ft street level setback on NE 65th is intended to increase the walkability of the street in front of the building, with wider sidewalks for increased pedestrian loads driven by the new station opening, overhead weather protection, and space for new street trees to buffer the arterial traffic in the roadway.
DC1 Project Uses and Activities <i>Optimize the arrangement of uses and activities on site</i>		
	C4 Project Uses and Activities: Parking and Service Uses, Service Uses Locate and design service entries, loading docks, and trash receptacles away from pedestrian areas or to a less visible portion of the site to reduce possible impacts of these facilities on building aesthetics and pedestrian circulation.	The existing alley west of the property will be paved as part of this project (from the southern property line of the site north to NE 65th St) and will provide access to the small residential parking garage. The trash storage room is located internally, with an access door for pickup located near the southeastern corner of the site on 14th Ave NE façade. The pickup of trash on 14th Ave NE is the preferred location per Seattle Public Utilities, as the existing alley narrows and becomes impassable for garbage trucks south of the property. All trash pickup for this block is done on 14th Ave NE. The visual impact of the trash pickup on 14th is minimized as it is stored inside the building, and the exterior face of the trash room (concrete wall) will be planted and screened with vegetation.
DC2 Architectural Concept <i>Develop an architectural concept that will result in a unified & functional design that fits well on the site & within its surroundings</i>		
	A1 Massing, Site Characteristics and Uses Arrange the mass of the building taking into consideration the characteristics of the site and the proposed uses of the building and its open space. In addition, special situations such as very large sites, unusually shaped sites, or sites with varied topography may require particular attention to where and how building massing is arranged as they can accentuate mass and height	The building massing is a result of the site conditions, zoning, and uses. To respect the zone transition at the southern property line to single-family, the main mass of the building is located towards the north half of the site. This locates the tallest structure height away from the one-story neighboring residential house, and provides the greatest separation between the multifamily residential units and single-family home for privacy. The location of the taller mass to the south also provides the opportunity for a lower-level terrace on the south end of the site. With the taller mass to the north, the terrace will not be overshadowed. For further detail on the rationale behind the massing strategy, see response to CS2-D-1: Height, Bulk and Scale: Existing Development and Zoning.

A2 Massing, Reducing Perceived Mass Use secondary architectural elements to reduce the perceived mass of larger projects.	See response to CS2-D-1: Height, Bulk and Scale: Existing Development and Zoning for details one how “gasket” recesses at the ends of the corridors of the main massing bar, minimal parapets, and upper level setbacks are proposed to reduce the perceived mass of the building.
B1 Architectural and Façade Composition, Façade Composition Design all building facades – including alleys and visible roofs – considering the composition and architectural expression of the building as a whole. Ensure that all facades are attractive and well proportioned.	The proposed building is designed to fit into the Roosevelt neighborhood as a modern fabric building. The building has three facades highly visible to the public; two sides of the proposed building front streets (NE 65th St and 14th Ave NE), and a third fronts an existing alley with an empty lot opposite. The fourth facade faces a single-family side-lot, but is significantly taller than the neighboring lots, and will consequently be visible from greater distances as well. All four of these elevations are designed as primary elevations, with attention paid to the material choice (painted fiber cement panels), large windows, and overall pattern and proportion. (Note, see DC4-A-1: Exterior Finish Materials for more detail on the facade.) The roof, while not visible from the street level or any existing surrounding buildings, is also considered. If in the future structures across NE 65th St are built to their zoning potential (65-ft) there may be some views onto the roof. There is a residential-use pedestal paver roof deck, integrated planters, and green roof.
B2 Architectural and Facade Composition, Blank Walls Avoid large blank walls along visible façades wherever possible. Where expanses of blank walls, retaining walls, or garage facades are unavoidable, include uses or design treatments at the street level that have human scale and are designed for pedestrians. These may include: a. newsstands, ticket booths and flower shops (even if small or narrow); b. green walls, landscaped areas or raised planters; c. wall setbacks or other indentations; d. display windows; trellises or other secondary elements; e. art as appropriate to area zoning and uses; and/or f. terraces and landscaping where retaining walls above eye level are unavoidable	Due to the constraints of a small site and the necessity to locate certain building functions at grade level, there is a length of concrete wall on the southern half of the 14th Ave NE facade at street level. The concrete wall is the face of the internal trash/recycling storage room, which has been kept inside the building to reduce its impact on the streetscape. The concrete wall will be screened with intentional landscaping. A 3-ft wide planter located in front of the concrete wall will provide adequate depth for both low and high plant screening. This landscape screening will fit in with the streetscape further down 14th Ave NE, which is characterized by vegetated rockeries and retaining walls, and heavily landscaped yards.
C1 Secondary Architectural Features, Visual Depth and Interest Where possible, include features in open spaces for activities beyond daylight hours and throughout the seasons of the year, especially in neighborhood centers where active open space will contribute vibrancy, economic health, and public safety.	Two exterior amenity spaces are provided for residential use: private and shared terraces on the second floor of the building, and a shared roof deck on the main building roof. The shared terrace on the second floor is a spill-out space from an interior amenity room, which may be used for evening activities. Exterior lighting on the building will be provided on the building at the shared and private terraces, for resident uses beyond daylight hours. The main roof deck will have barbeque zones and spaces for group gatherings, which may also be used for evening events. Lighting will be provided off the penthouse structures. Due to the location of the occupied roof deck at the center of the roof, light spill from evening activities is not anticipated.
C2 Secondary Architectural Features, Dual Purpose Elements Consider architectural features that can be dual purpose – adding depth, texture, and scale as well as serving other project functions.	The street level facades of NE 65th St and the corner of 14th Ave NE are set back 4-ft from the property line. This setback is an architectural feature, but also provides benefits for pedestrians at street level. Formally, the setback breaks the vertical plane of the 4-story building into a clear base and top, adding scale to the structure. At the ground plane the setback provides built-in weather protection for pedestrians, widens the sidewalk for greater pedestrian activity and new street trees.

C3 Secondary Architectural Features, Fit with Neighboring Buildings

Use design elements to achieve a successful fit between a building and its neighbors.

The southern property line is a shared lot line with a single-family zone. The house on the neighboring property is set proximate to the shared lot line. The massing of the proposed structure steps down on the south side to a single-story structure in deference to this zone transition. The lid of the single-story structure is used for residential terraces off the first floor (5 private unit terraces and 1 shared amenity terrace). The terrace extends to within feet of the shared lot line. To preserve the privacy of the single-family lot, a series of secondary design elements are used at the south edge of the terrace. The first is a linear, cast-in-place raised planter bounding the south edge of the terrace, which keeps terrace residents 5-ft back from the terrace edge (7.25-ft from the shared lot line). Secondly, a line of trees is planted in the planter to further screen views between the two lots both at the terrace level, and from the upper two residential levels with south-facing windows down into the neighboring yard. Finally, a railing at the south edge of the terrace functions as a screen wall between the two properties, providing privacy and light screening between the properties.

DC3 Open Space Concept

Integrate open space design with the design of the building so that each complements the other

B1 Open Space Uses and Activities, Meeting User Needs

Plan the size, uses, activities, and features of each open space to meet the needs of expected users, ensuring each space has a purpose and function.

Two exterior communal amenity spaces are provided for residents of the proposed project. A landscaped roof deck is provided on the main roof. The deck location is central, to keep residents back from the edges of the building to preserve the privacy of single-family residential homes adjacent. The roof deck will have barbeque hookups and gathering spaces for multiple groups. Preserved views oriented south will be available from the deck due to the transition to single-family residential south of the site. A second smaller communal exterior terrace is provided as a spill-out space from the second floor amenity room. This space is likely to be used for smaller gatherings in conjunction with the use of the interior amenity. Private terraces are also provided for the south-facing units on the second floor.

B4 Open Space Uses and Activities, Multifamily Open Space

Design common and private open spaces in multi-family projects for use by all residents to encourage physical activity and social interaction. Some examples include areas for gardening, children's play (covered and uncovered), barbeques, resident meetings, and crafts or hobbies

See response above the DC3-B-1 Open Space Uses and Activities, Meeting User Needs.

DC4 Exterior Elements and Finishes

Use appropriate and high-quality elements and finishes for the building and its open spaces

A1 Building Materials, Exterior Finish Materials

Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged

Materials at the street level of the building will primarily be storefront and architectural exposed concrete. At the residential levels the main expanse of the exterior façade will be composed of large floor-to-ceiling windows and painted fiber cement board. The large windows will be subdivided into smaller human-scale operable and fixed windows to provide modulation, and the expanses of fiber cement board will have a finer-scale vertical breakup for added building texture. The three bay windows on NE 65th St will be highly glazed, with painted metal flashings and accessories accenting the windows at corners, intersections and soffits.

A2 Exterior Elements and Finishes, Climate Appropriateness

Select durable and attractive materials that will age well in Seattle's climate, taking special care to detail corners, edges and transitions.

The materials selected for the project are well documented, commonly used materials in the Pacific Northwest climate. The detail of corners, edges and transitions will be well considered. A key design feature of the building will be the bay window projections on NE 65th St, which are considered a secondary pattern to the building. The detailing of these bay projects is intended to read as an expansion of the window system out from the building, with thinly detailed corners, soffits and edges in painted metal to match the window mullions.

D1 Trees, Landscape and Hardscape Materials, Choice of Plant Materials Reinforce the overall architectural and open space design concepts through the selection of landscape materials.	Landscape design is a key feature of the proposed project. At the street level, street trees and landscape on NE 65th are designed to enhance the pedestrian experience and provide a buffer from the arterial roadway. On 14th Ave NE landscape is used to blend the building into the existing heavily planted single-family street south of the property. Vertical landscape elements will provide quiet visual interest as the building transitions from an activated commercial corner to back-of-house elements further south. Landscape planters on the building edge in the southeastern corner will help soften the transition to neighboring single-family structures. On the building structure, landscape accentuates the exterior amenity areas, defining edges at the roof terrace, and providing visual and physical buffers at the second level terrace between residents of the proposed project and the southern single-family house.
D2 Trees, Landscape and Hardscape Materials, Hardscape Materials Use exterior courtyards, plazas, and other hard surfaced areas as an opportunity to add color, texture, and/or pattern and enliven public areas through the use of distinctive and durable paving materials. Use permeable materials wherever possible.	The roof deck and second floor terrace will have pedestal pavers for a durable hard surface. The roof deck paver layout is heavily complimented by a surrounding green roof, and integrated raised planters providing a variety of color and texture. The second floor terrace hardscape areas are buffered by a raised planter of ground vegetation and small trees, and bookended by a series of light-filtering terrace separation screens, providing an added color and material to the pallet.
D3 Trees, Landscape and Hardscape Materials, Long-Range Planning Select plants that upon maturity will be of appropriate size, scale and shape to contribute to the site as intended.	All plants have been reviewed for initial as well as long-term appropriateness to not overwhelm the site. Small street trees were selected for the NE 65th St façade, partly due to the 4-ft wide tree-wells standard for substandard width streets, as well as to ensure their canopies fit between the bay window projections. A cherry tree was selected for 14th Ave NE to blend with the existing cherry trees on 14th Ave NE, and because it is a quick-growing tree that will quickly provide a big impact. Small trees are selected for the south-facing second-level terrace to provide screening between the new building and the southern residential neighbor's back yard, but so as not to overwhelm the scale of the terrace and block upper level unit views out.
D4 Trees Landscape and Hardscape Materials, Place Making Create a landscape design that helps define spaces with significant elements such as trees	At the street level, street trees, as well as ground vegetation, create a landscape buffer between the street and sidewalk. On the building structure, small trees are used on the second level terrace to define the southern edge of the structure, and provide a physical and visual buffer between residents of the proposed project and the neighboring single-family home.



VIEW LOOKING NORTH-WEST AT CORNER OF NE 65TH ST AND 14TH AVE NE

REPRESENTATIVE PROJECTS

Weinstein A+U is recognized as one of the Northwest’s leading design firms and has continually demonstrated design excellence on a broad array of projects for State, City, Federal, private, and not-for-profit clients. We are passionate about our city and the shaping of its urban neighborhoods through the integration of architecture and urban design is central to our practice.

Well-designed and thoughtful urban housing is a special concern of ours, and we have worked aggressively to advance the expectations of mixed-use projects in Seattle, both technically and aesthetically. While each project presents very specific challenges, a number of recurring themes inform much of our work and form the basis of our approach to housing design:

- All of our buildings are situational and are inseparable from their sites. They sit comfortably amongst their established neighbors, drawing from established precedents while looking to the future
- Well-designed unit plans are essential to the success of a housing project. While the functionality of each unit type is important, the organization of units across a floor plate and their influence on building elevations is equally important
- Appropriately located and proportioned open space is a significant design determinant for most mixed-use and urban housing projects
- We avoid arbitrary façade embellishment. Instead we utilize the organization of individual units and their aggregation to establish the pattern and rhythm of multi-family facades that is furthered informed by site organization and orientation. Plans correlate to elevations and variation occurs within an established system
- The constrained budgets for typical mixed-use projects demand careful consideration of a project’s primary orientation and configuration to provide cost effective sustainable design strategies
- The scale and proportion of new mixed-use buildings must address, but need not directly reflect, those of adjacent structures. Plan, section, and elevation strategies should be integrated to provide a comprehensible “read” of the building’s composition and organization



- 1 **Agnes Lofts**, 1433 12th Avenue
- 2 **19th and Mercer Mixed-Use Building**, 526 19th Avenue E
- 3 **2026 E Madison Mixed-Use Building**, (unbuilt)
- 4 **The Rooster Mixed-Use Building**, 900 NE 65th Street (under construction)
- 5 **Ventana at the Market**, 2100 Western Ave
- 6 **SCCA Patient House**, 207 Pontius Ave N
- 7 **Compass Center Housing**, 1753 NW 56th Street
- 8 **Belroy Apartments**, 703 Bellevue Ave E
- 9 **Banner Building**, 2600 Western Avenue



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ZONING DATA

PRELIMINARY SEATTLE ZONING CODE ANALYSIS

PARCEL NO	1797500090 (north parcel) ; 1797500095 (south parcel)	
LOT AREA	4,144 SF (north parcel) ; 4,480 SF (south parcel) = 8,624 SF Total	
ZONING	NC1P-40 (1.2) (north parcel) ; NC1P-40 (0.75) (south parcel) , Roosevelt Residential Urban Village, Frequent Transit Corridor, Roosevelt Station Overlay District	
PERMITTED USES	<ul style="list-style-type: none">Restaurant Uses permitted up to 10,000 SFRetail sales and services, general use, permitted up to 10,000 SFResidential uses permitted outright	23.47A.004, Table A
STREET LEVEL STANDARDS	<ul style="list-style-type: none">Non-residential street level frontage to comply with transparency & blank façade provisions.Required average depth of street level nonresidential spaces 30' (15' min)Street level nonresidential required to have 13-ft floor to floor height.	23.47A.008
	<ul style="list-style-type: none">Residential uses may occupy, in the aggregate, no more than 20% of the street-level street-facing façade within a NC1 zone.Along designated principal pedestrian streets, one or more of the following is required along 80% of street-facing façade: general sales, retail, eating and drinking establishments.	23.47A.005.C 23.47A.005.D1 (NE 65th is a principal pedestrian street)
STRUCTURE HEIGHT	<ul style="list-style-type: none">40' as zoned.Height of a structure may exceed the otherwise applicable limit by up to 4' provided a floor-to-floor height of 13' or more is provided for nonresidential uses at street level.Open railings, planters, parapets, etc permitted up to 4; above height limit.Solar collectors, mechanical equipment, stair & elevator penthouses allowed to extend up to 15' above height limit, provided the combined total coverage of all features gaining additional height does not exceed 20% of the roof area, or 25% of the roof area if the total includes stair and elevator penthouses or screened mechanical equipment.Stair and elevator penthouses may extend above the applicable height limit up to 16'.	23.47A.012
FAR	<ul style="list-style-type: none">4 for lots with 40-ft height limit with a mixed-use building. Both lots have incentive suffixes:<ul style="list-style-type: none">north parcel: (1.2 incentive), 4 Max = 4 x 4,144-SF lot areasouth parcel: (0.75 incentive), 4 Max = 4 x 4,480-SF lot areaCombined maximum allowable area: 34,496-SF, including all incentive provisions	23.47A.013
SETBACKS	<ul style="list-style-type: none">A setback is required where a lot abuts the intersection of a side lot line and a front lot line of a lot in a residential zone. The required setback forms a triangular area 15' on a sideFor a structure containing a residential use, a setback is required along any side or rear lot line that abuts a lot in a residential zone, as follows: 15' for portions of structures above 13' in height to a maximum of 40' - for each portion above 40' in height additional setback at the rate of 2' of setback for every 10' by which the height of such portion exceeds 40'No entrance, window, or other opening is permitted closer than 5' to an abutting residentially-zoned lot	23.47A.014
PARKING	<ul style="list-style-type: none">None required within urban village / station area overlay district	23.47A.032
AMENITY AREA	<ul style="list-style-type: none">Amenity spaces equivalent to 5% of residential gross floor area required for residential uses<ul style="list-style-type: none">Amenity Spaces shall not be enclosed.Common amenity area: min area 250-sf; 10' min horizontal dimensionPrivate balconies: min area 60-sf; 6' min horizontal dimension	23.47A.024
PARKING ACCESS	<ul style="list-style-type: none">Access to parking shall be from the alley if the lot abuts an alley improved to the standards of Section 23.53.030.C (paved and min. 12' width)	23.47A.032 (Lot abuts an alley that meets the min. width but is not paved)

SITE ANALYSIS

Topography

- Very gentle slope along NE 65th St
 - 2' gain in elevation from west to east
- Gentle slope along 14th Ave NE
 - 2.25' gain in elevation from south to north

Neighboring Buildings

- Undeveloped NC2P-65 zoned lot to north
(across NE 65th St)
- Single-family residence to east
(across 14th Ave NE)
- Single-family residence to south
- Undeveloped NC1P-40 zoned lot to west
(across alley)

Solar Access

- Excellent solar access due to low-height single-family zone to the south.
- Some overshadowing or shading possible on lower levels in the morning due to abundance of street trees on 14th

Views

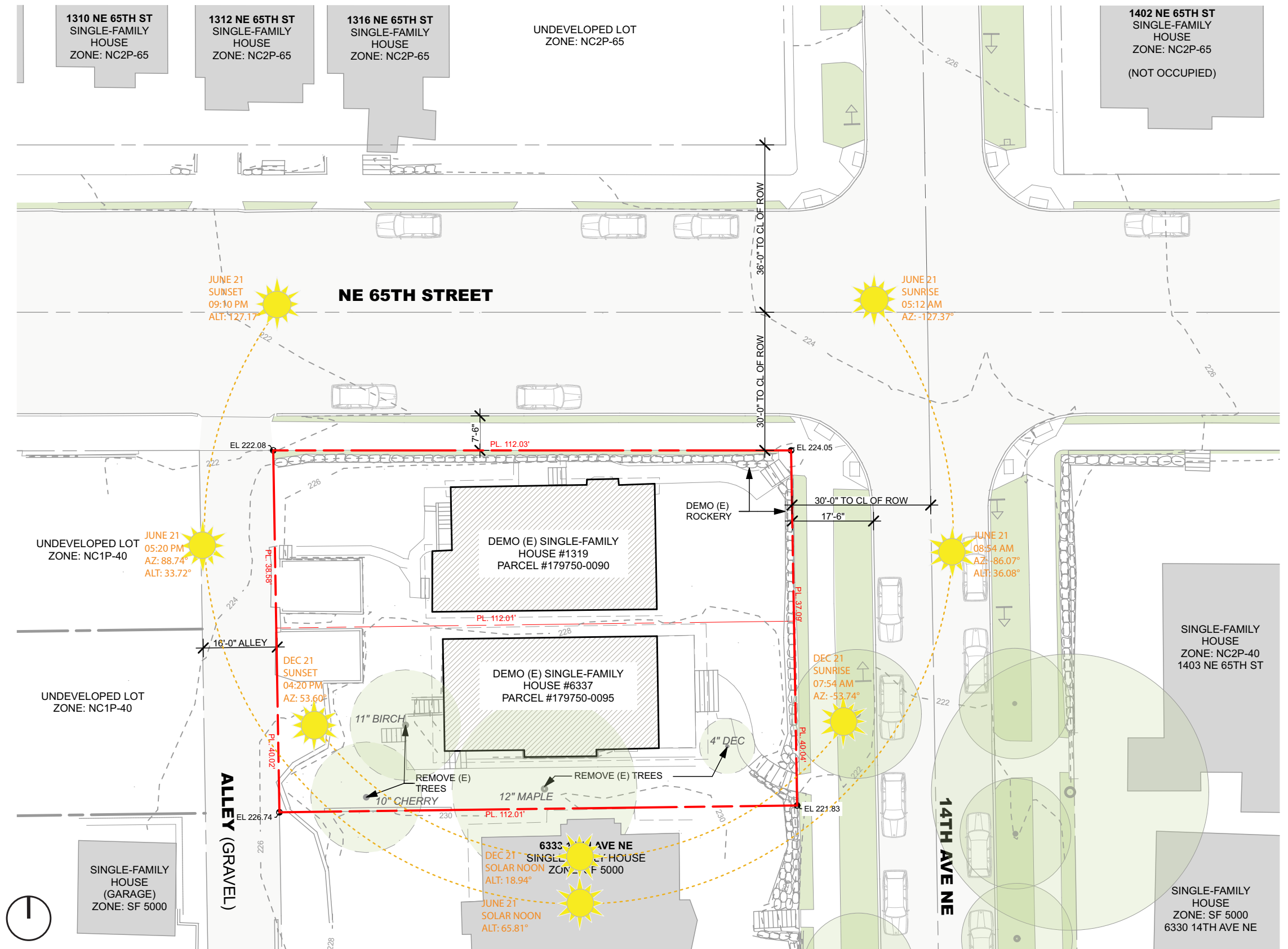
- Currently, territorial views are available in all directions above the first floor. Views to the south are protected due to adjacent single-family zoning.

Structure Height

- Zoned as NC1P-40 with a base height limit of 40-ft.
- Located within 40-ft mapped height limit zone, which allows a 4-ft height increase when a floor-to-floor height of 13' or more is provided for nonresidential uses at street level.
- Total maximum allowable building height: 44-ft

Allowable Building Area

- The maximum FAR and site areas for the building site's two parcels are:
 - north parcel:
 - 1.2 Base FAR (4 Max FAR*)
 - 4,144-sf lot area
 - south parcel:
 - 0.75 Base FAR (4 Max FAR*)
 - 4,480-sf lot area
 - Maximum allowable area: 34,496-sf*
- * including incentive provisions



EXISTING SITE PLAN

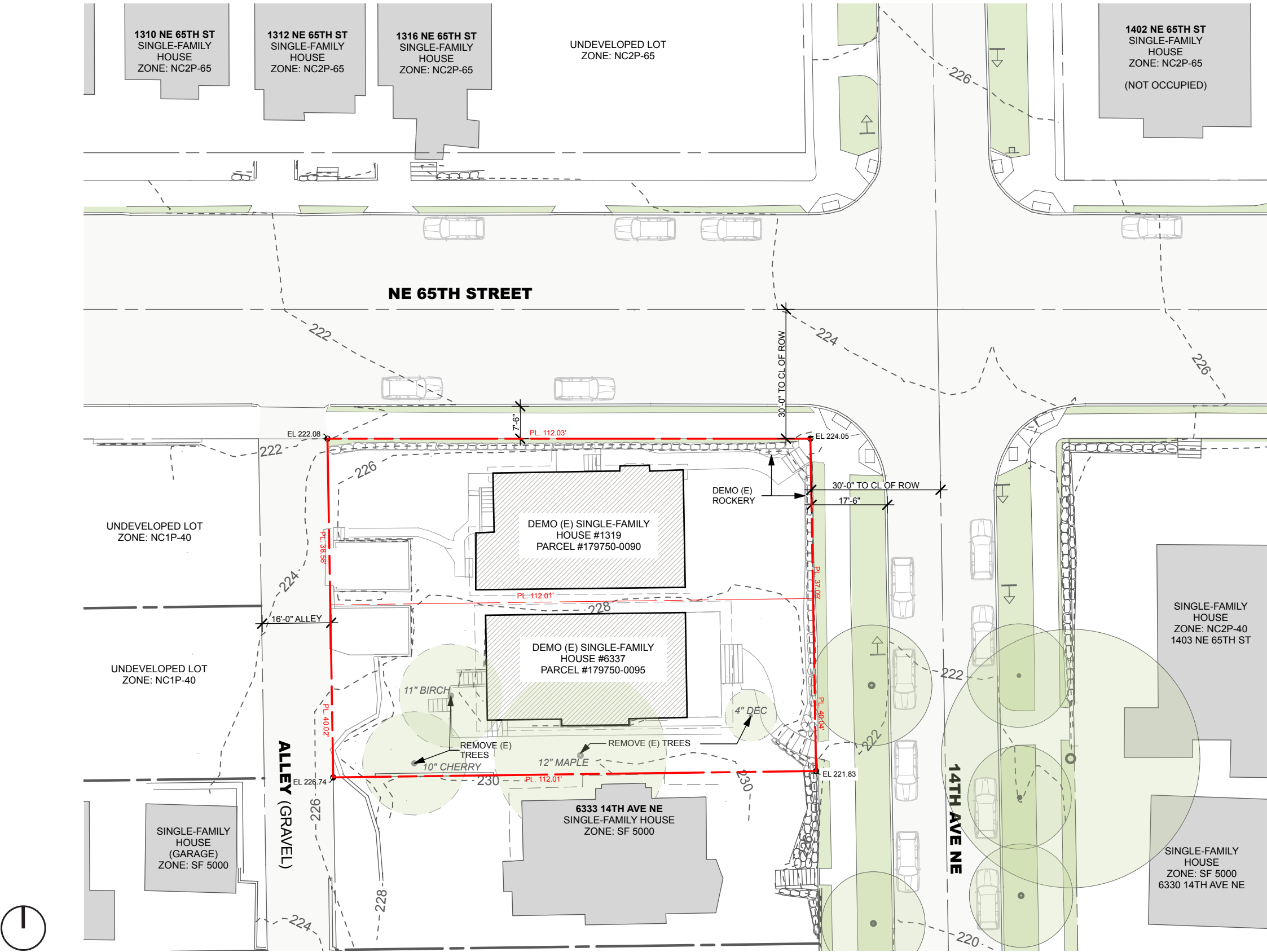
The proposed project site's two parcels total 8,624 SF. The site fronts NE 65th St at the north property line, 14th Ave NE at the east property line, an unimproved (gravel) alley at the west, and the side lot line of a single-family property to the south.

The NE 65th St frontage is 112' in length. The pedestrian rights-of-way width is +7'-6" from back of curb, inclusive of a narrow 1'-6" planter. There are no street trees along this portion of NE 65th, and overhead high-voltage power lines are located on the opposite side of the street. Sidewalk curb ramps are present at the corner, but no painted crosswalks are provided across 14th Ave NE or NE 65th St.

The 14th Ave NE frontage is 77' in length. The pedestrian right-of-way width is +17'-6" from back of curb, and characterized by a wide planter and street trees on the street side, and a narrow planter on the property side. Parking access to properties is provided from the alley, limiting curb cuts down 14th.

Rockeries at the property lines on both street fronts raise the grade of the site within the property boundaries to 4-7 feet above the right-of-way. The highest raised grade is at the southern property line. The sidewalks at the rights-of-way have relatively gentle slopes, rising towards the NE corner of the property. Along NE 65th St the grade slopes gradually from el. +222.08' at the alley entrance to el. +224.05' at the corner of NE 65th St and 14th Ave NE (1.76% slope). Along 14th Ave NE the grade change is shallow as well (2.81%), dropping from the intersection with 65th down to the south to reach el. +221.83 at the SE corner of the site.

The two existing structures on the site located on the rockery berms are accessed by stairs from street level, a condition that continues at the single-family homes south of the property on 14th Ave NE. There are four existing deciduous trees on the southern parcel, three of which are of greater than 6" caliper. There are also a number of larger hedges on the site, including a significant hedge along the alley at the NW corner of the site.





SITE CONTEXT: LAND USE

The site is bounded by public rights-of-way to the north (NE 65th St), east (14th Ave NE), and west (alley). The south property line abuts a single-family home's side-lot line.

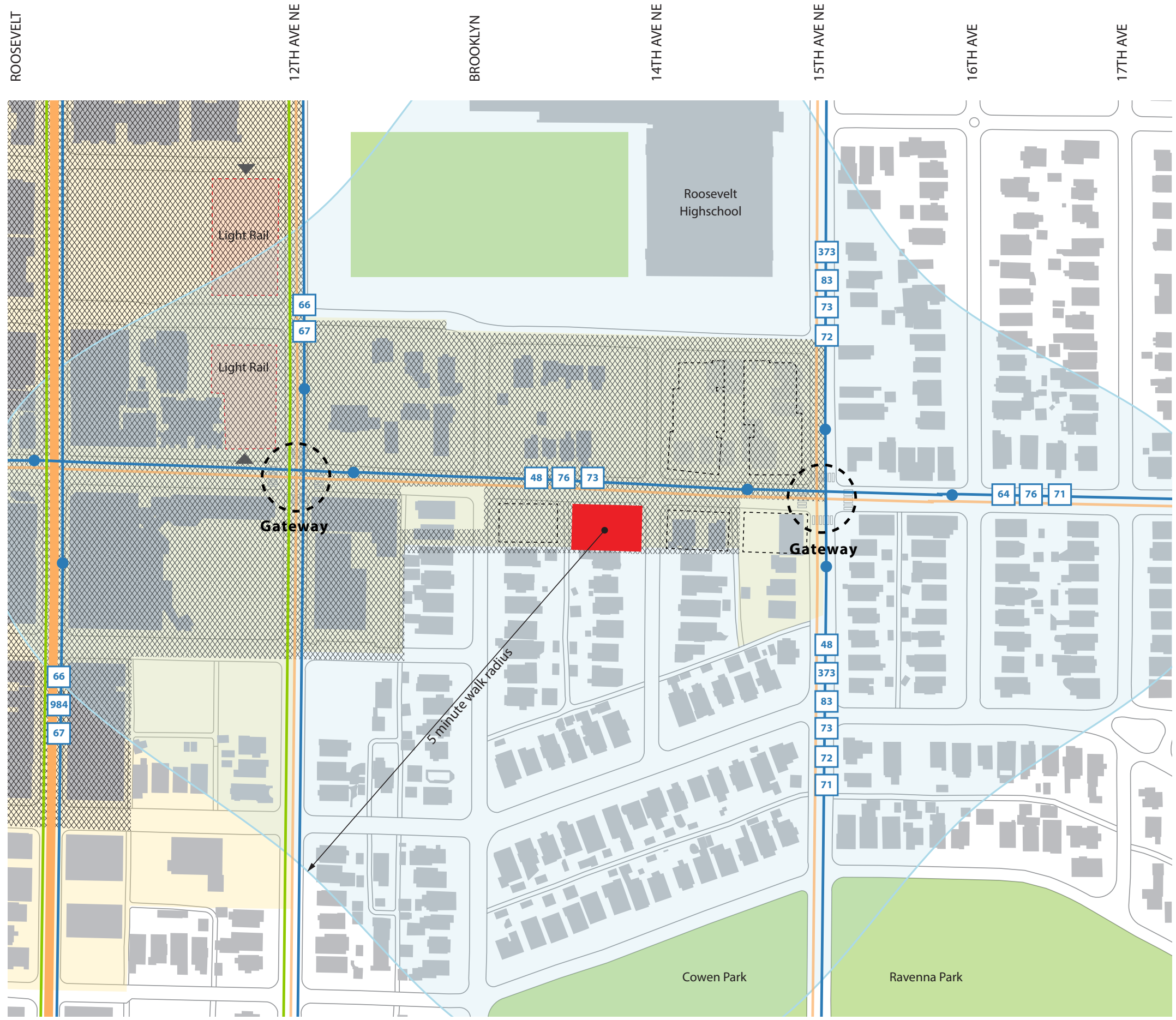
The neighborhood directly south is a single-family residential zone. The surrounding neighborhood to the north, east, and west of the site is a mix of single-family homes, low-rise density retail with surface parking, and undeveloped lots. The Roosevelt Neighborhood's Core Commercial Area begins a block west of the site, and is characterized by higher density multi-family housing and retail. The new Roosevelt Light Rail Station will be located a block and half west of the site on NE 65th St.

Two blocks north of the site is Roosevelt High School, and two blocks south of the Site are Cowen and Ravenna Parks.

Legend

- Future Light Rail Station (2021)
- Future Development
- Single Family Residential
- Multi Family Residential
- Mixed Use
- Retail
- Office
- Surface Parking
- School / Institutional
- Utility
- Parks/Open Space
- Roosevelt Commercial Core





SITE CONTEXT: TRANSIT MODES

The project site is well served by several bus lines located within a 5-minute walk radius. King County Metro bus routes 48, 73 and 76 run past the site along NE 65th Street and provide connections to Ballard and Capitol Hill, the Central Area and Mount Baker (route 48); to Jackson Park, the University of Washington and the Downtown retail core (route 73); and to Wedgwood, Hawthorne Hills and additional connection to the Downtown retail core (route 76). Additional connections to the Downtown retail core, Northgate, the University of Washington and a host of other neighborhoods are within a short walk of the site. Travel times to the University, Capitol Hill and Downtown will greatly improve with the opening of Sound Transit light rail service to Roosevelt Station located 4 short blocks west of the site.

The public transportation options in the vicinity of the site also speak its pedestrian environment. The relative flatness of NE 65th Street provides an easy walk to the future light rail station and Roosevelt's commercial core located within a five-minute walk to the west of the site. Cowen Park and Ravenna Park are also located within a five-minute walk to the south of the site, while Roosevelt High School is a block to the north.

Legend

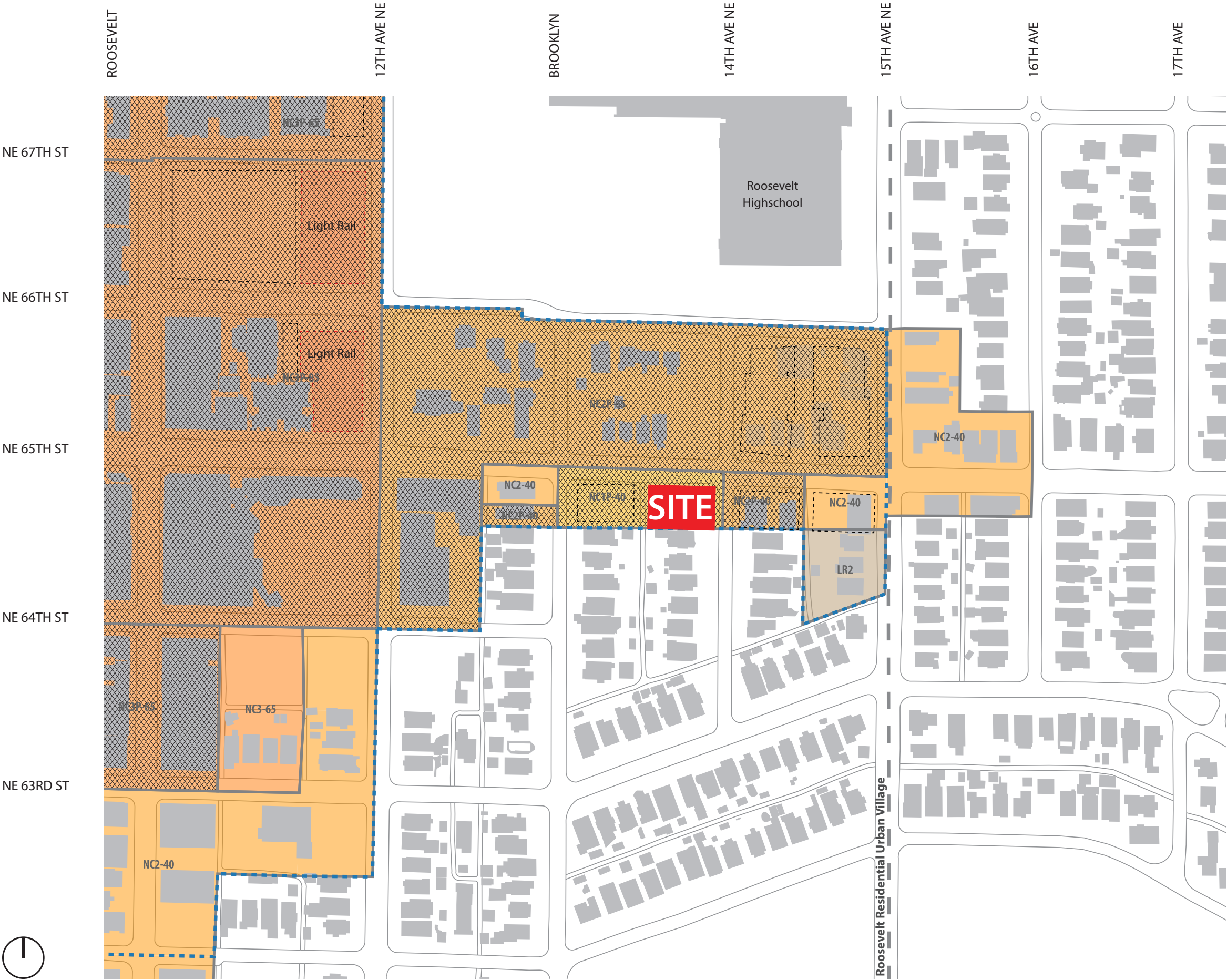
- Main Car Arterials:**
 - Principal Arterial / Regional Connector
 - Minor Arterial / Commercial Connector
- Future Light Rail Station (2021)
- Identified Gateway Intersection per Roosevelt Neighborhood Guideline
- Frequent Transit Corridors
- Pedestrian Overlay
- 5 Minute Pedestrian Walkshed
- Bus Routes
- Bus Lines
- Current Bicycle Routes

CONTEXT: EXISTING ZONING

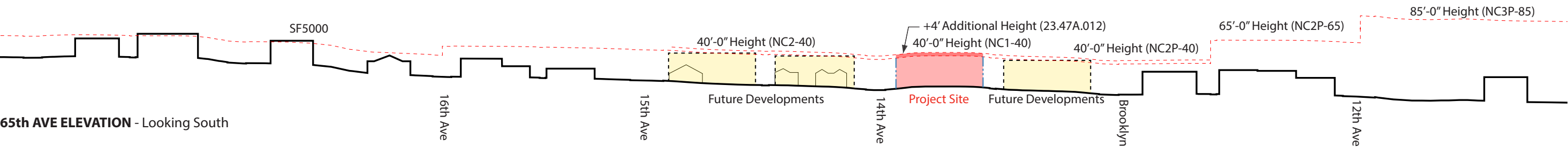
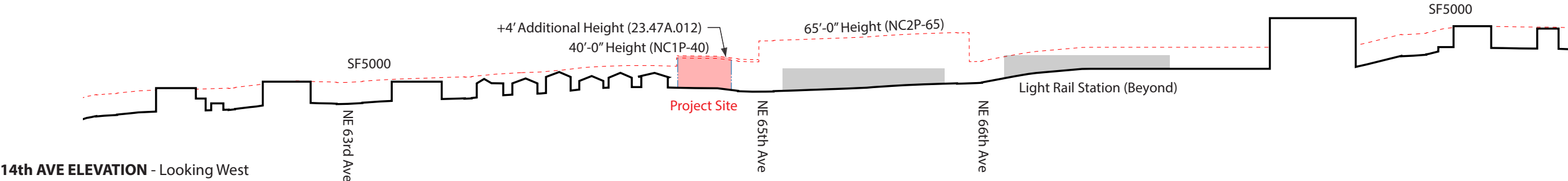
The site is zoned NC1P-40. The site is bounded by public rights-of-way to the north (NE 65th St), east (14th Ave NE), and west (alley). Properties beyond the rights-of-way in these three directions are NC1P and NC2P zones. The south property line abuts the side lot line of a SF 5000 zone.

Existing Zoning Legend

- NC1
- NC2
- NC3
- LR1 & LR2
- SF
- Roosevelt Station Area Overlay
- Roosevelt Residential Urban Village
- Zone Boundaries
- Pedestrian Overlay



CONTEXT: ZONING ENVELOPE



CONTEXT: NE 65TH AVE

The northern edge of the property is bounded by NE 65th Street. The topography along the property line has a minimal slope (under 2%) up towards the east. The sidewalk along this stretch of 65th is narrow (roughly 7 1/2-feet), inclusive of a thin, planted strip at the curb. There are no street trees on either side within the neighboring blocks. Both sides of the 65th are zoned for NC, with a 65-ft height limit at the north side, and a 40-ft height limit on the south, though current property uses do not reflect this potential. Currently, 65th is a mix of single-family houses, undeveloped lots, and low-intensity commercial, often with surface parking.

The Roosevelt Commercial Core boundary edge is a block west, and this area of higher density retail and mixed-use is visible from the site.

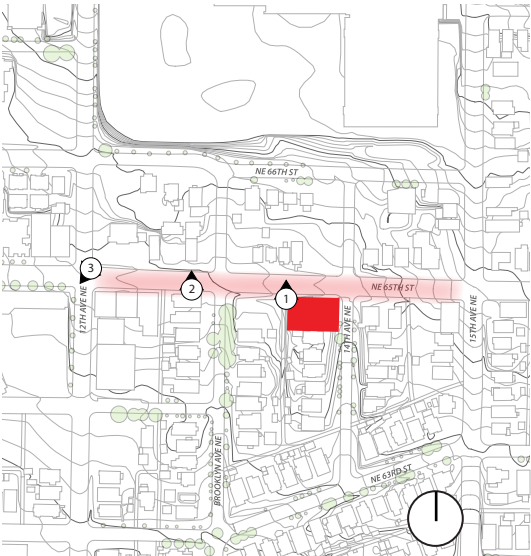
The future Roosevelt Link Light Rail Station, set to open in 2012, is a block and half west of the site on the opposite side of the street.



NE 65th - Looking South



NE 65th - Looking North



CONTEXT: 14TH AVE NE



14th AVE NE - Looking West



14th AVE NE - Looking East

The eastern edge of the property is bounded by 14th Ave NE. The topography along the property line has a gentle slope (under 3%) towards the north. 14th is a quiet street characterized by single-family residential houses, typically set back from the property line and raised above the street level by landscaped rockeries or retaining walls. Street trees are well established and set within wide planting beds. Resident-only parking is provided on both sides of the street. One block north of the site, across NE 65th, both sides of the street are awaiting new development (empty lots and boarded houses), and at the end of that block 14th dead-ends into the Roosevelt High School campus.



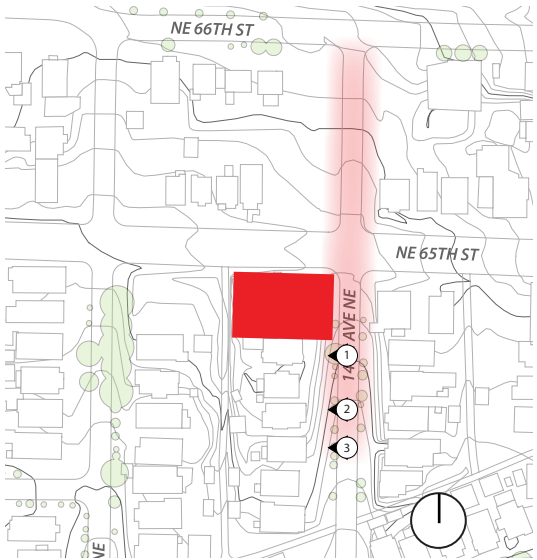
1



2



3



CONTEXT: SURROUNDING NEIGHBORHOOD

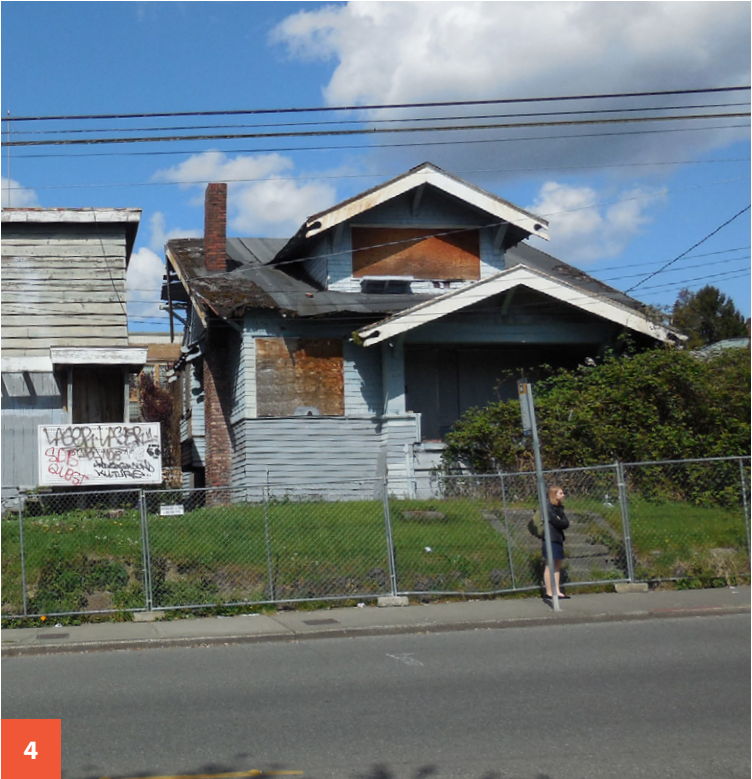
The neighborhood surrounding the site is in transition, due in large part to the Link light rail station that will soon open at the corner of NE 65th and 12th Ave NE. As noted above, NE 65th Street is zoned for a higher density and height than its current uses. Multiple lots are boarded up or empty, awaiting redevelopment, and a MUP has already been approved for a new mixed-use high-rise project diagonally across the street from the proposed site. The existing commercial streetscape lacks street wall continuity, as building setbacks and building siting on the lots vary, with some commercial structures separated from the street by surface parking.

The eastern edge of the Roosevelt Neighborhood Commercial Core is a block west of the site, where retail and mixed-use properties increase, pedestrian amenities such as street trees and crosswalks are more prevalent, and building heights increase. With the incoming light rail and current zoning allowances, the blocks surrounding the site could develop as an extension of the commercial core. The Roosevelt Design Guidelines acknowledge this extension, with the identification of two neighborhood “gateway” intersections on 65th: one at the 12th Ave NE intersection and one at 15th Ave. Further development on the blocks surrounding the site would enhance the pedestrian connection between these two gateways.

In contrast to the rapidly morphing environment of 65th, the residential areas of the Roosevelt neighborhood, such as south of the proposed project site, are well-established single-family blocks. Houses and yards are maintained, and the building style is cohesive with pitched roofs with eaves, and raised porches.

Roosevelt High School, the largest high school in the city, is located a block north of the site. Two parks, Cowen and Ravenna, are located a few blocks south.

1 Link Light Rail Roosevelt Station. Expected opening in 2021, located 1-1/2 blocks from the site. // **2 Retail at eastern edge of Roosevelt Commercial Core.** // **3 Retail at eastern edge of Roosevelt Commercial Core.** // **4 Properties on NE 65th St Awaiting Redevelopment.** // **5 Typical Residential.** South of the site is a well-established single-family residential zone. // **6 Roosevelt High School.** The school is located a block north of the site



CONTEXT: EXISTING SITE

The site is currently occupied by two small single-family structures, raised approximately 4-7' above the street level grades by a continuous rockery at the property lines facing NE 65th St and 14th Ave NE. No curb cuts exist on either street; each single-family has a single uncovered parking stall accessed off the unimproved alley. Power lines for all the single-family houses on the block run overhead in the alley, crossing over NE 65th at the site's alley property line. The proposed project will consider undergrounding the power under 65th and across the length of the new building as part of construction. There are four trees on the site, all on the southern parcel, and a number of larger hedges.

The sidewalk on NE 65th St, as mentioned previously, is narrow and lacks street trees. This width, coupled with the heavy vehicular use on NE 65th leads to an unsafe pedestrian condition.

By contrast, the sidewalk on 14th Ave NE is spacious and inclusive of a wide planting strip at the curb. Street trees line both sides of 14th.



1 View of Site from Northeast Corner. Existing single-family is raised on a rockery that edges the property line. // **2 View of Site from Northwest Corner.** A gravel alley backs the property to the west, providing access to residential parking. // **3 Sidewalk at 14th Ave NE.** A generous planting strip and sidewalk fronts the east side of the property. // **4 Sidewalk at NE 65th St.** A narrow sidewalk and minimal planting strip without street trees fronts the north side of the property. // **5 View of Site from Roosevelt High School (North of Site).** The site is directly north of a single-family zone, which provides protected views for upper levels to the south.

DESIGN PROPOSAL: ALTERNATE 1 (L-SHAPE)

The street level of Design Alternate 1 identifies NE 65th St as the commercial frontage, and 14th Ave NE as the residential frontage. The multifamily residential lobby is located halfway down 14th Ave, and is flanked by residential amenity spaces. The corner is occupied by a residential amenity space that could be converted to a new commercial space or an expansion of the commercial space to the west. The NE 65th street-level façade and the northern half of the 14th Ave NE street-level façade is set back 4' from the property line to expand the pedestrian realm and provide weather protection. Access to the residential parking garage is off the alley.

The three residential floors above are an L-shape configuration in response to the corner lot location, and to maximize residential area. The L-shaped floor plan would require departures from both residential setbacks at the south property line.

A small basement houses building services and residential storage units.

Primary Disadvantages/Concerns

- The scheme lacks a response to the southern residential zone transition; there are no provisions to respond to the 15' setback triangle, or the 15' setback above 13' in height.
- The location of the multifamily entry on 14th Ave NE will draw more noise and activity down the quiet residential street.

Summary

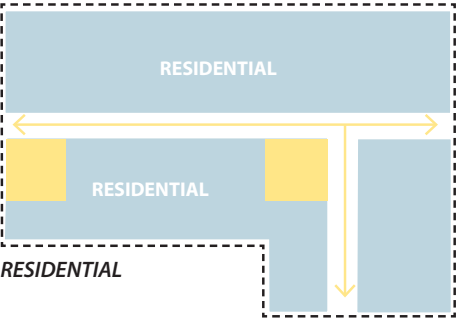
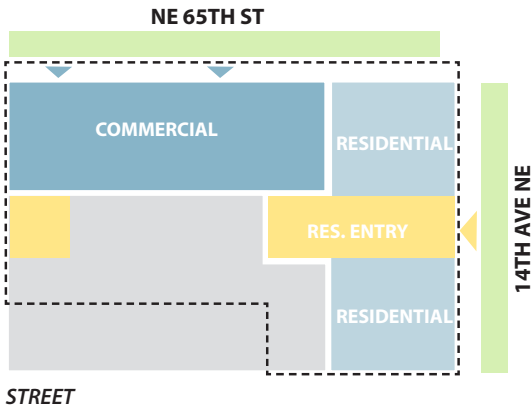
Stories:	4 (3-over-1 + 1 below grade)
Unit Count:	41 (6,160 unit SF/ Floor)
Floor Area:	24,430-SF Residential
	1,850-SF Commercial
	2,581-SF Parking
	30,834-SF Total
Parking:	7 stalls (residential)

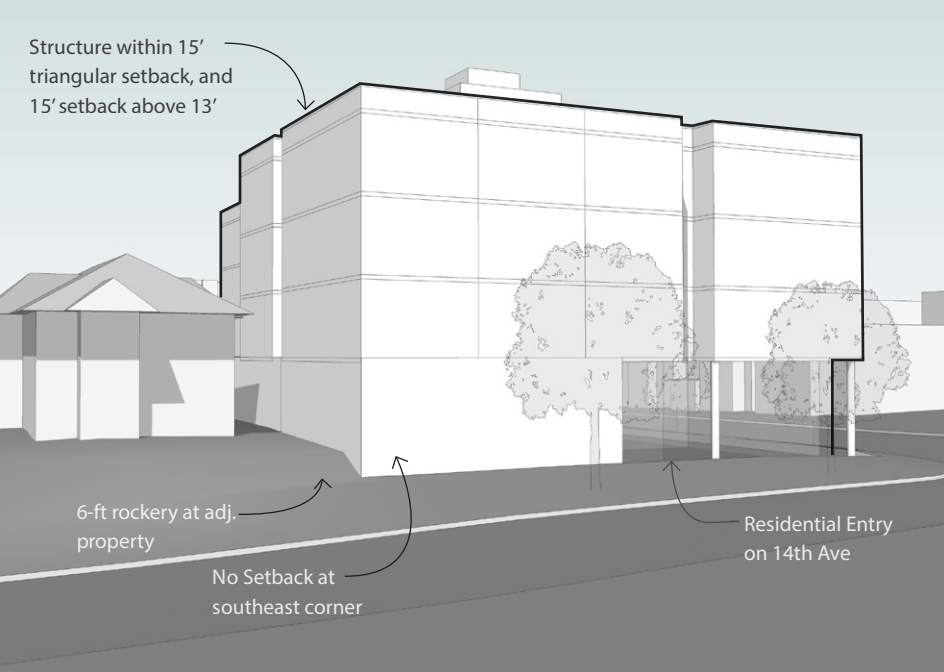
Ground Floor Uses:

- NE 65th St: Commercial / Residential Amenity
- 14th Ave NE: Residential Lobby & Amenity
- Alley: Parking Access

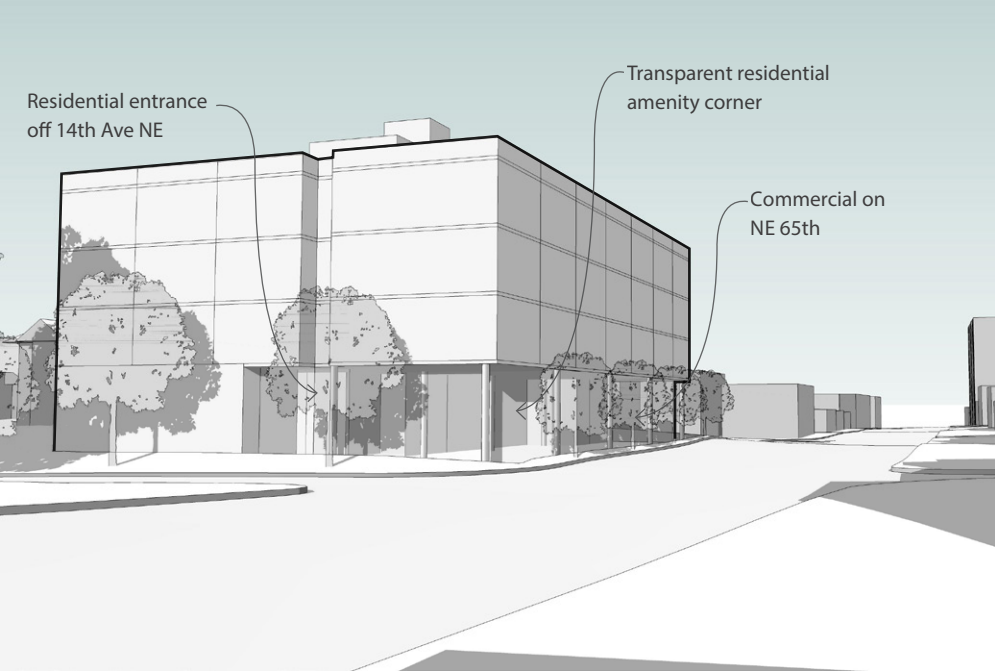
Potential Departures

- Increased percentage of residential use at street level in NC Zones (NE 65th & 14th Ave NE)
- Reduced width of overhead weather protection on principal pedestrian streets (< 6-FT)
- Reduced non-residential use depth at street level
- Structure in triangular setback abutting a residential lot
- Structure in south PL setback above 13-ft
- Reduced parking lot aisle width

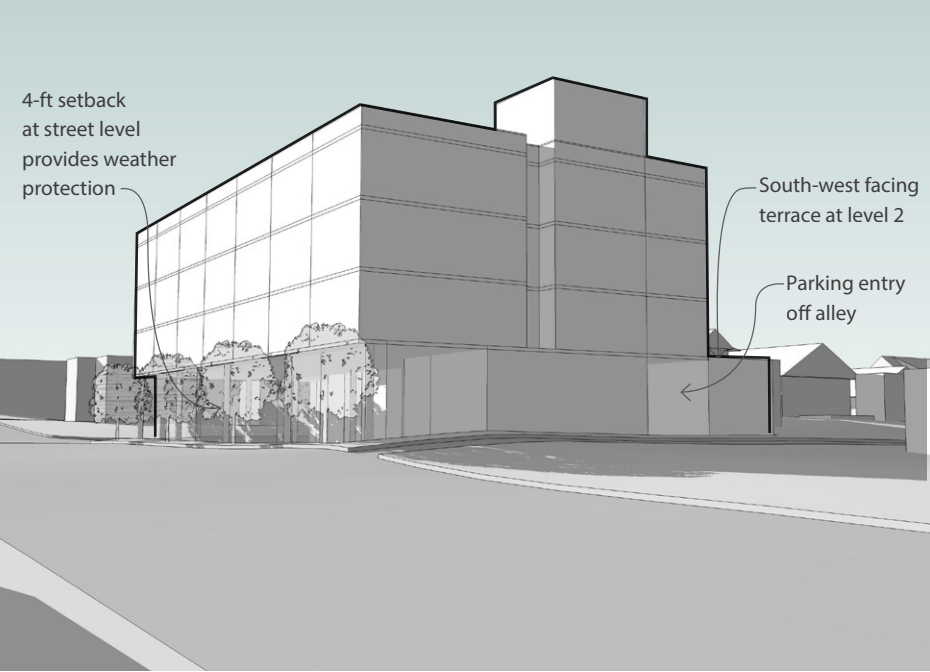




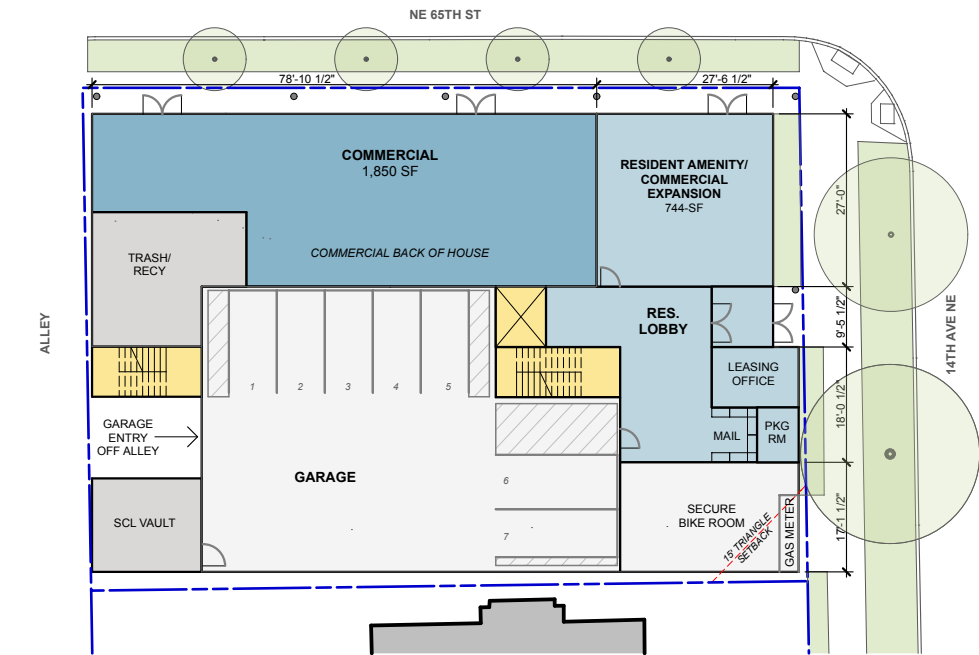
Southeast Corner (14th Ave NE)



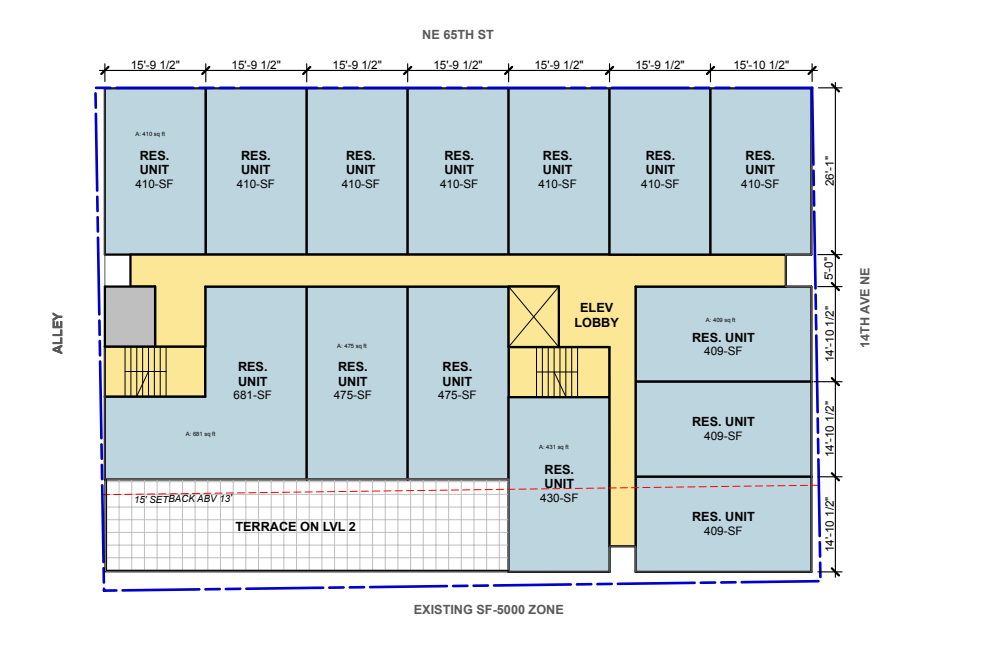
Northeast Corner (Corner of NE 65th St and 14th Ave NE)



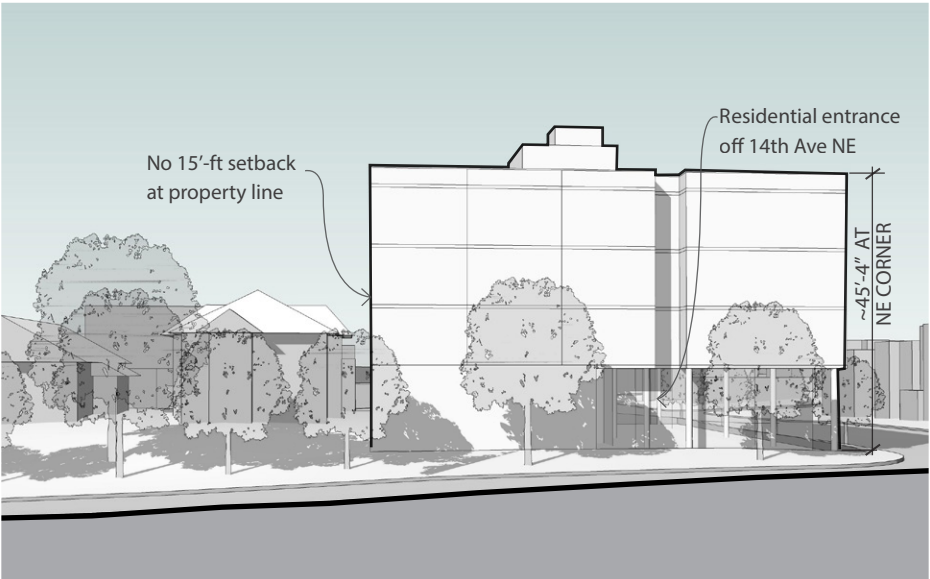
Northwest Corner (NE 65th St)



Street Level Plan (Level 1)



Typical Residential Plan (Level 2)



East Elevation (14th Avenue NE)

DESIGN PROPOSAL: ALTERNATE 2 (NO DEPARTURES)

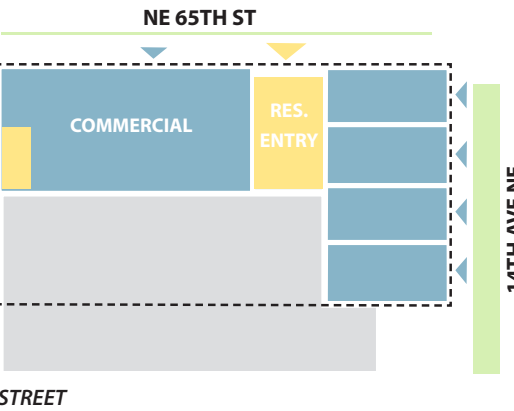
The street level of Design Alternate 2 is heavily weighted towards commercial use, in compliance with the land use codes. A large commercial space is located on NE 65th, and four live-work units front 14th Ave NE. The multifamily entry is minimal and located halfway down NE 65th. The allowable 20% of the facade length for residential use is occupied by a vestibule and leasing office, with no formal lobby. Instead, residential amenity spaces are stacked above the multifamily entry on the residential floors (borrowing space from rentable units). Residential Parking is accessed off the alley.

A small basement houses building services and residential storage units.

The three residential floors above are organized in a bar around a double-loaded corridor. Behind the bar the building steps down to a second floor garden terrace, bringing the building mass down to the scale of the southern residential houses.

Primary Disadvantages/Concerns

- To achieve minimum required depths in the commercial space, and required parking aisle dimensions behind, the building must build to the northern lot line. Consequently, the scheme cannot widen the pedestrian right-of-way at NE 65th, which is currently 7'-6" from back of curb with a very minimal plant strip.
- The building preserves the 15-ft triangular setback abutting the residential lot at the southeast corner of the property, but this missing corner will be a dark, hidden inlet with low visibility to passersby.
- Locating live-work units that will attract more activity and noise on 14th Ave NE may not be to the benefit of the quiet residential neighbors.



Summary

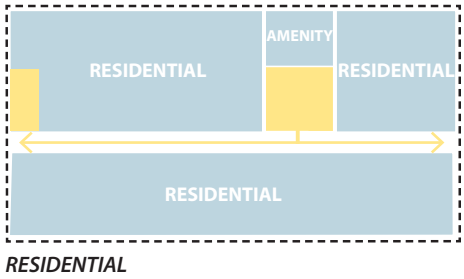
Stories: 4 (3-over-1 + 1 below grade)
Unit Count: 41 (5,401 unit SF/ Floor)
Floor Area: 20,889-SF Residential
4,261-SF Commercial (includes Live-Work)
3,052-SF Parking
29,389-SFTotal
Parking: 8 stalls (residential)

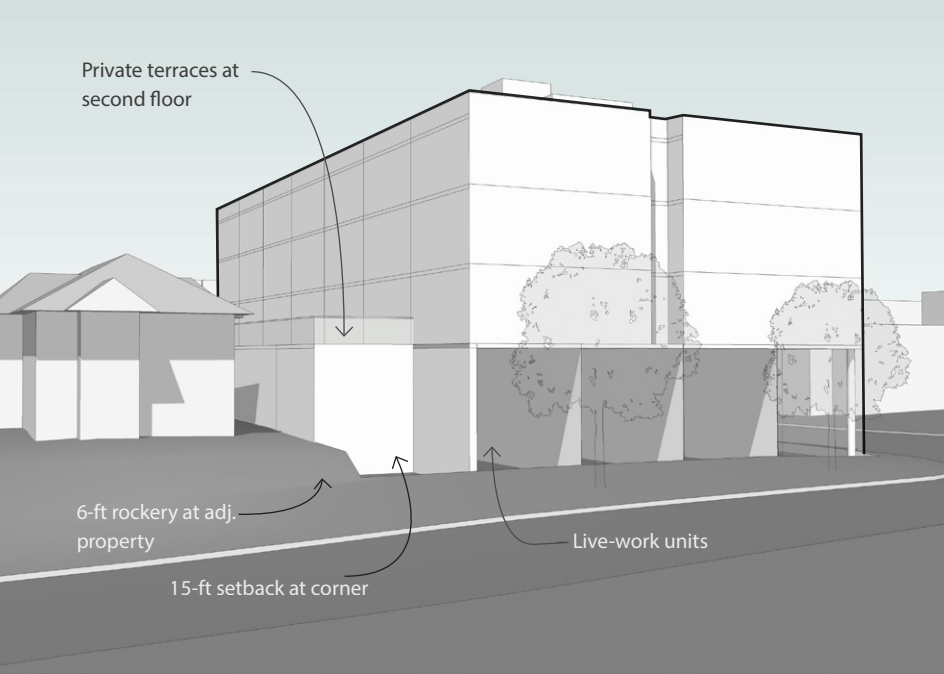
Ground Floor Uses:

- NE 65th St: Commercial / Residential Lobby
- 14th Ave NE: Live-Work Units
- Alley: Parking Access

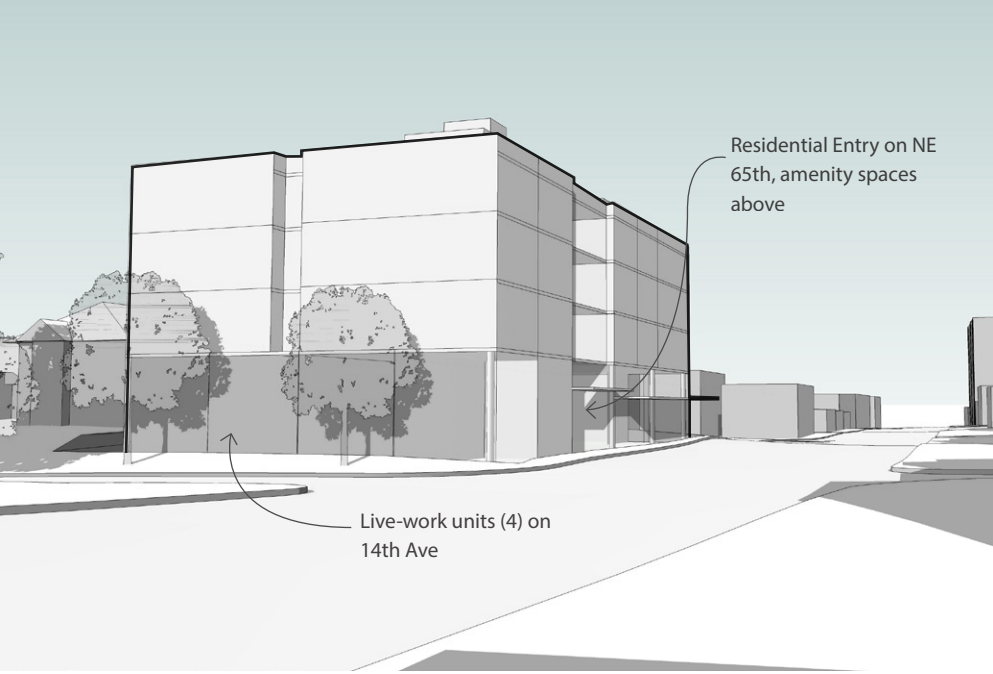
Potential Departures

None

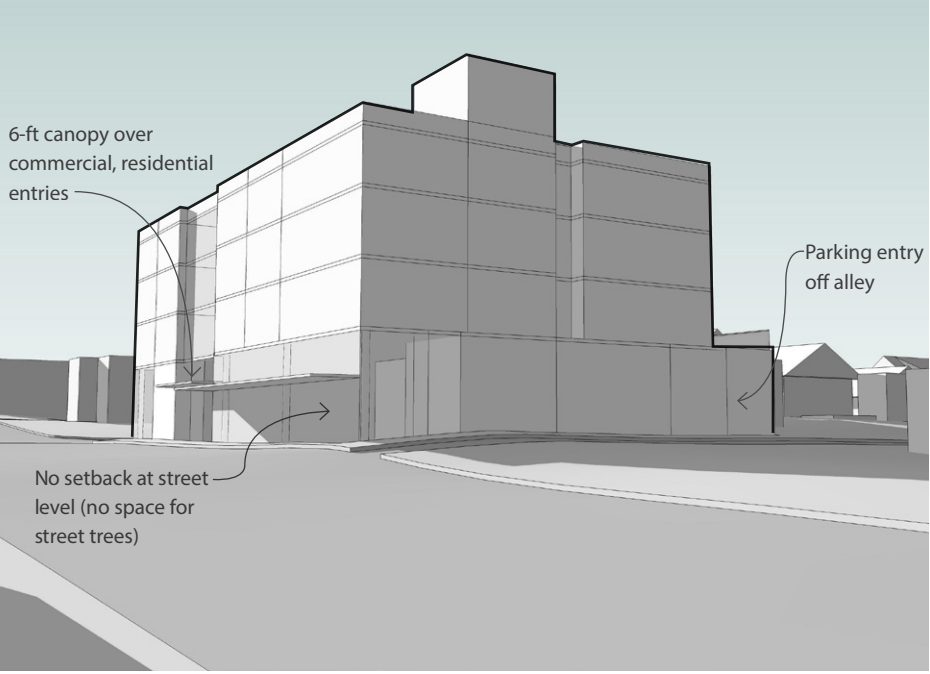




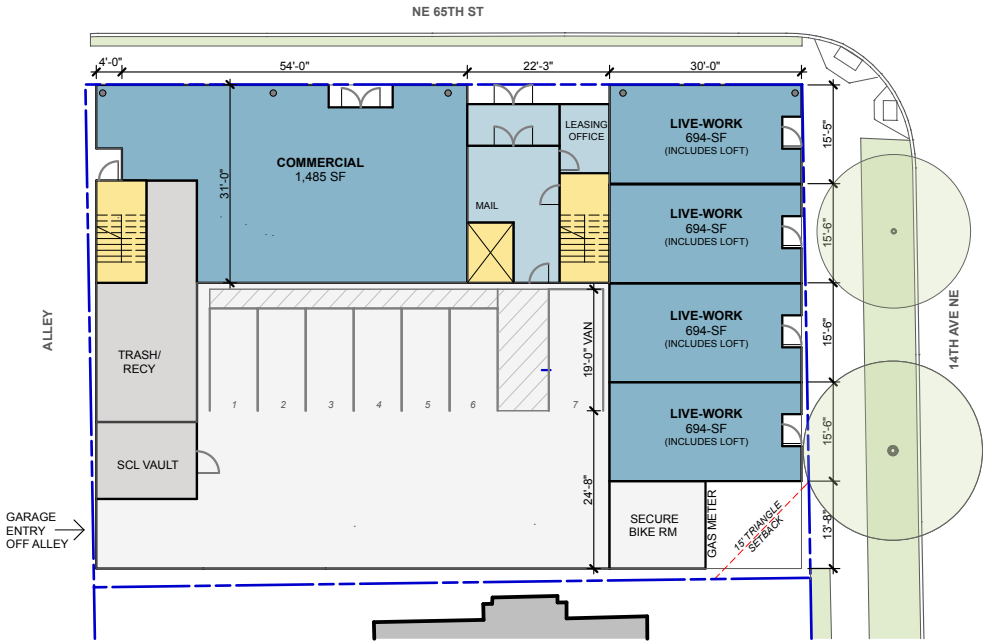
Southeast Corner (14th Ave NE)



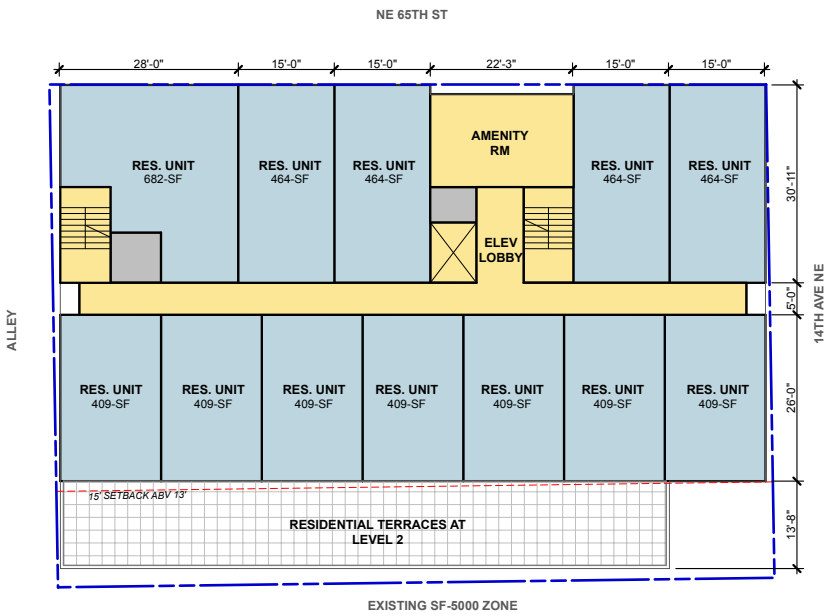
Northeast Corner (Corner of NE 65th St and 14th Ave NE)



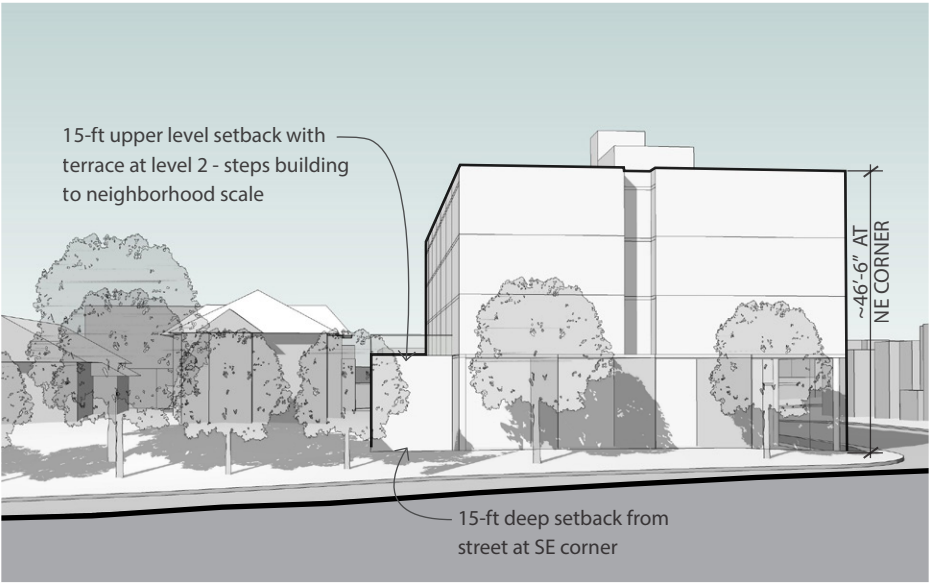
Northwest Corner (NE 65th St)



Street Level Plan (Level 1)



Typical Residential Plan (Level 2)



East Elevation (14th Avenue NE)

DESIGN PROPOSAL: ALTERNATE 3 (PREFERRED)

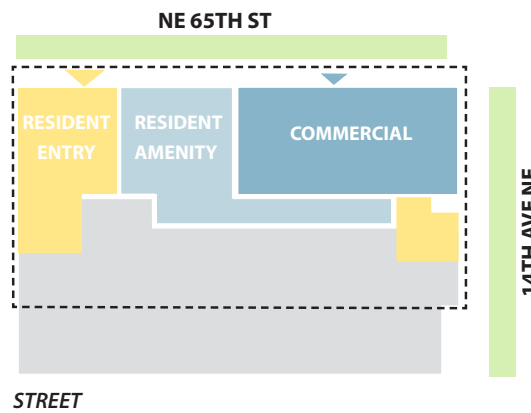
The street level of Design Alternate 3 is oriented towards NE 65th St, the primary pedestrian street and commercial spine. A commercial space occupies the eastern half of the facade, and wraps the corner at 14th Ave. The multifamily entry is located on the western corner of the facade for commuter proximity to the Light Rail station. The entry is flanked by an open lobby and residential amenity space (potential business center), both of which are glazed with commercial storefront and will provide eyes on the street. The street level facade on NE 65th is set back 4-ft from the property line to widen the pedestrian ROW. The street level on 14th Ave NE is responsive to the NC to SF zone transition: an activated northern commercial corner transitions south to a quiet landscaped facade consistent with the planted rockeries and retaining walls of the neighboring lots. Residential parking access is off the alley.

As in previous alternates, a small basement houses building services and residential storage units.

The three residential floors above are organized around a double-loaded corridor and set back 15-ft from the southern property line. This setback at the podium creates private terraces for the residential units, and the stepped massing responds to the neighboring southern residential zone.

Primary Advantages/Benefits

- A 4-ft setback at street level on NE 65th enhances the pedestrian experience: providing continuous weather protection, a wider sidewalk, and street trees in a curbside planter.
- The massing (setbacks on the south) responds to the zone edge condition from NC to SF.
- Street level programming reflects the zone edge, with activated uses focused on NE 65th to preserve the privacy of 14th Ave NE neighbors.



Summary

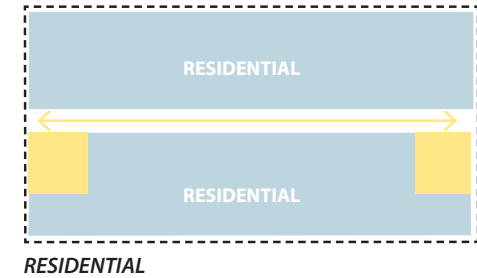
Stories:	4 (3-over-1 + 1 below grade)
Unit Count:	41 (5,826 unit SF/ Floor)
Parking:	8 stalls (residential)
Floor Area:	20,301-SF Residential
	1,440-SF Commercial
	3,093-SF Parking
	29,627-SF Total

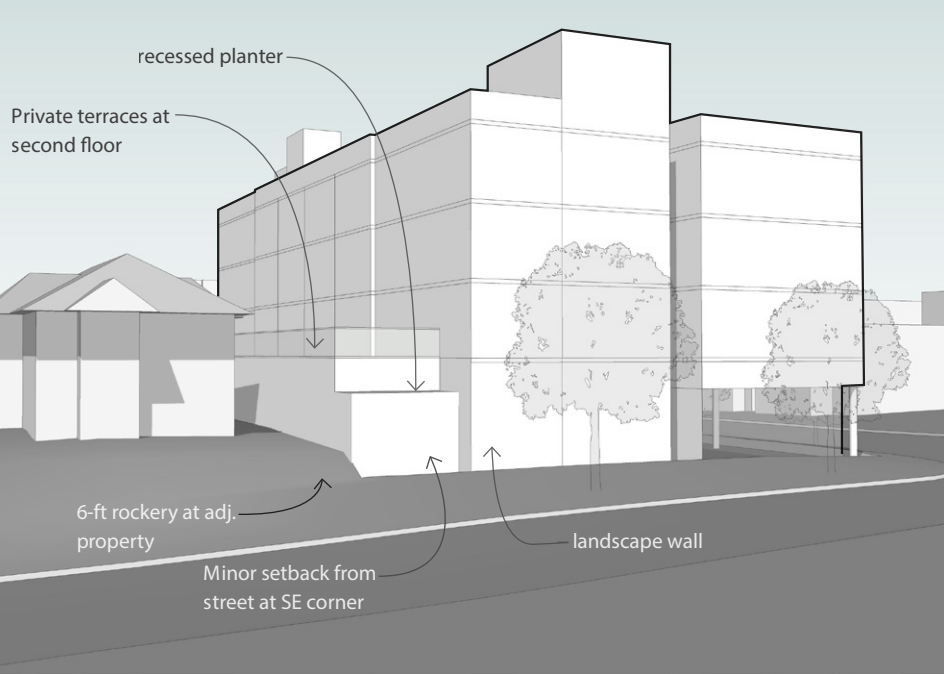
Ground Floor Uses:

- NE 65th St: Commercial / Residential Lobby
- 14th Ave NE: Commercial (at corner) / secondary residential bike entry
- Alley: Parking Access

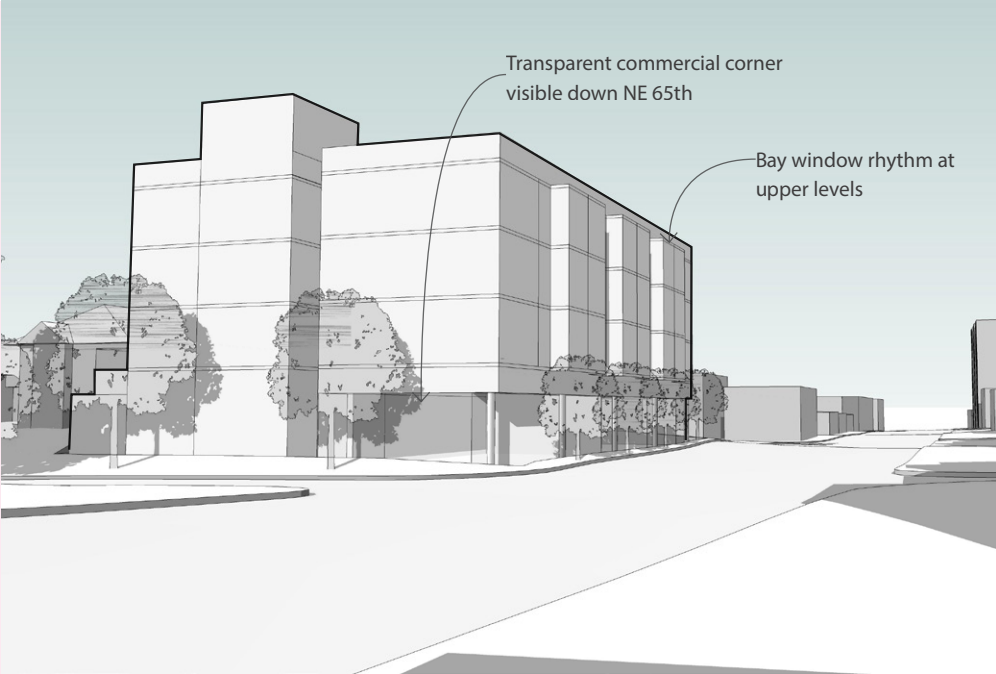
Potential Departures

- Increased percentage of residential use at street level in NC Zones (NE 65th)
- Reduced street level transparency on 14th Ave NE
- Reduced width of overhead weather protection on principal pedestrian streets (< 6-ft)
- Reduced non-residential use depth at street level
- Structure in triangular setback abutting a residential lot
- Garage door opening closer than 5-ft to a residential lot
- Reduced parking lot aisle width





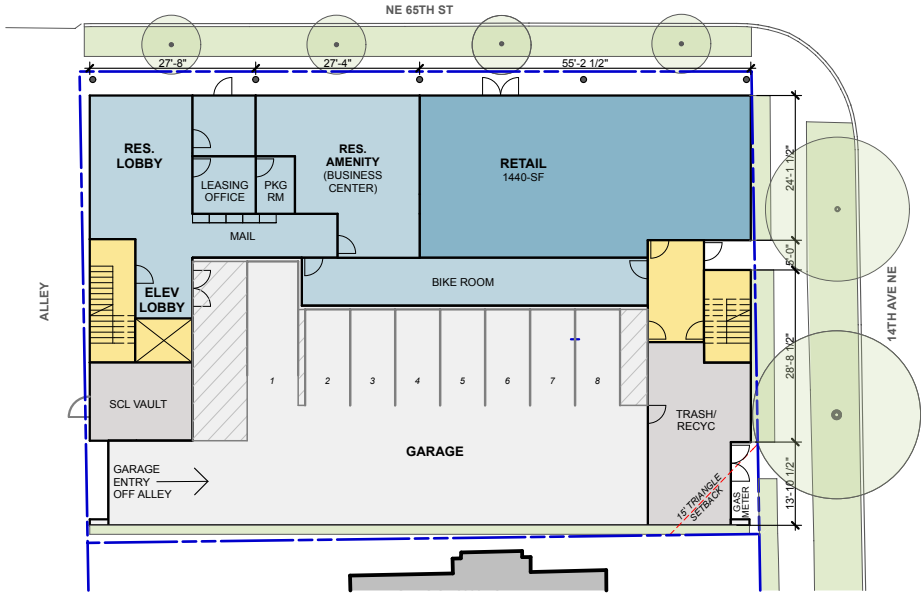
Southeast Corner (14th Ave NE)



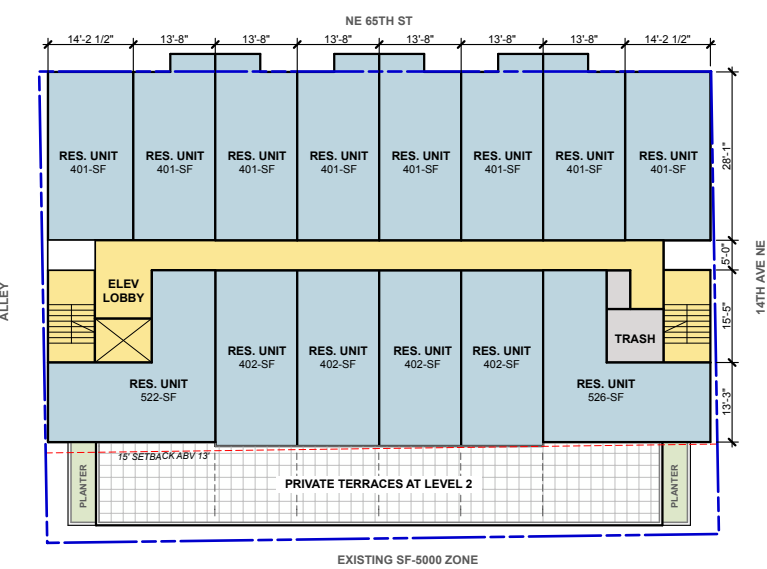
Northeast Corner (Corner of NE 65th St and 14th Ave NE)



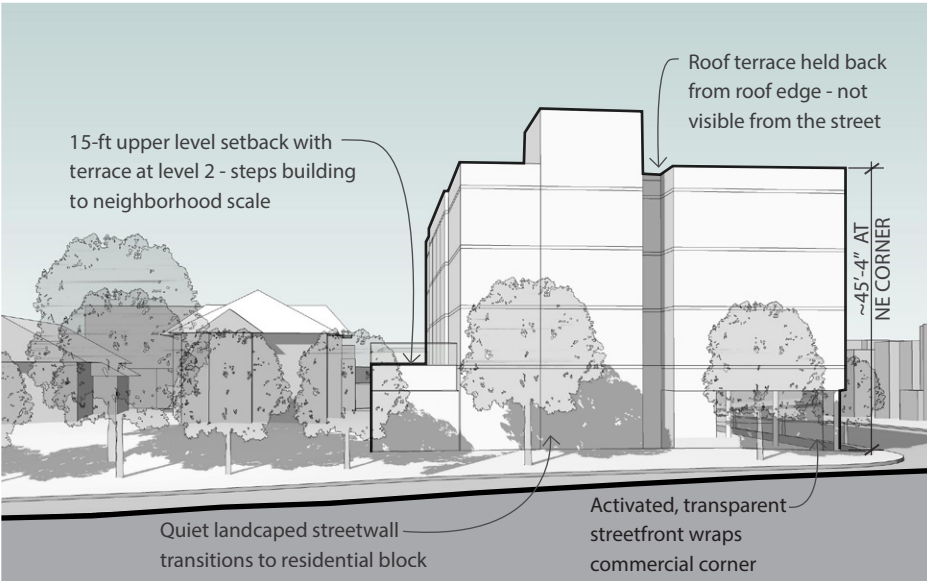
Northwest Corner (NE 65th St)



Street Level Plan (Level 1)

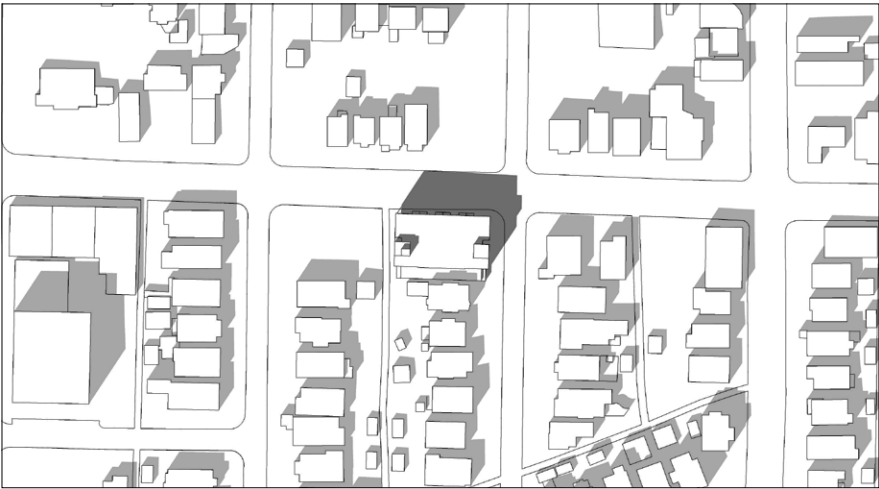


Typical Residential Plan (Level 2)



East Elevation (14th Avenue NE)

PREFFERED ALTERNATIVE SHADOW STUDY





ALTERNATE 1 (L-SHAPE)

Summary
Stories: 4 (3-over-1 + 1 below grade)
Unit Count: 41 (6,160 unit SF/ Floor)
Floor Area: 30,834-SF Total
Parking: 7 stalls (residential)
Ground Floor Uses:
• NE 65th St: Commercial / Residential Amenity
• 14th Ave NE: Residential Lobby & Amenity
• Alley: Parking Access

Potential Departures:
• Increased percentage of residential use at street level in NC Zones (NE 65th & 14th Ave NE)
• Reduced width of overhead weather protection on principal pedestrian streets (< 6-ft)
• Reduced non-residential use depth at street level
• Structure in triangular setback abutting a residential lot
• Structure in south PL setback above 13-ft
• Reduced parking lot aisle width

Pros
• Setback façade at street level widens pedestrian zone on NE 65th st.
• No curb cuts required - parking entry is provided off alley, separating cars and pedestrians.
• L-shape configuration at upper levels maximizes unit sizes.

Cons
• Lack of setback or step-down of massing at south property line imposes a greater visual impact on neighboring residential property.
• The 2nd floor terrace residential amenity will be overshadowed in the morning by north-south tower
• Location of multifamily entrance on 14th will draw more activity and noise down 14th, which may not be desired by residents of the quiet street.
• L-shape Configuration will require a number of Departures.



ALTERNATE 2 (NO DEPARTURES)

Summary
Stories: 4 (3-over-1 + 1 below grade)
Unit Count: 40 studios (5,401 unit SF/ Floor)
Floor Area: 29, 389-SF Total
Parking: 7 stalls (residential)
Ground Floor Uses:
• NE 65th St: Commercial / Residential Lobby
• 14th Ave NE: Live-Work Units
• Alley: Parking Access

Potential Departures:
None

Pros
• No curb cuts required - parking entry is provided off alley, separating cars and pedestrians.
• Upper level setbacks at south property line create a gradual transition to the adjacent low-height residential zone.
• No Departures required.

Cons
• Without a setback façade at street level, the narrow 7'-6" pedestrian right-of-way cannot be widened, and there is no space to add street trees in the narrow curb planting strip.
• 14th Ave NE is a quiet single-family residential street. Live-work units that draw noise and activity to the street may not be a good fit for the neighbors.
• Respect of the 15-ft triangular side yard setback creates a dark corner at the back of the site, with little solar access due to neighboring property's raised rockery, and little visibility to passersby.



ALTERNATE 3 (PREFERRED)

Summary
Stories: 4 (3-over-1 + 1 below grade)
Unit Count: 41 studios (5,864 unit SF/ Floor)
Floor Area: 29,627 SF Total
Parking: 8 stalls (residential)
Ground Floor Uses:
• NE 65th St: Commercial / Residential Lobby
• 14th Ave NE: Commercial (at corner) / secondary residential bike entry
• Alley: Parking Access

Potential Departures:
• Increased percentage of residential use at street level in NC Zones (NE 65th)
• Reduced street level transparency on 14th Ave NE
• Reduced width of overhead weather protection on principal pedestrian streets (< 6-ft)
• Reduced non-residential use depth at street level
• Structure in triangular setback abutting a residential lot
• Garage door opening closer than 5-ft to a residential lot
• Reduced parking lot aisle width

Pros
• Street level setback on NE 65th widens pedestrian zone and creates space for street trees
• No curb cuts required - parking entry is provided off alley, separating cars and pedestrians.
• Location of residential entry at the west end of NE 65th minimize distance to the new Light Rail Station for commuter residents.
• Location of all major building entrances on NE 65th increases their visibility to the public, and preserves 14th Ave NE as a quiet, private residential street.
• Upper level setbacks at south property line create a gradual transition to the adjacent low-height residential zone.
• A shallow setback in plan at the south-east corner respects the intent of the Code's 15-ft triangular setback to respond to residential neighbors, but does not create a deep enough space to invite unwanted activity, as Alternate 2 might.

Cons
• Departures required at street level for setbacks, street level uses, transparency, and parking

PREFERRED MASSING (FROM EARLY DESIGN GUIDANCE)

