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Project Information

Property Address: 1319 NE 65th St, Seattle

Owner: Roosevelt Development Group LLC

Developer: Roosevelt Development Group LLC
 Nick Miller (Contact)
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Architect: Weinstein A+U LLC
 Adrienne Watkins (Contact)
 T (206) 443-8606

ZONING DATA

PRELIMINARY SEATTLE ZONING CODE ANALYSIS

PARCEL NO	1797500090 (north parcel) ; 1797500095 (south parcel)	
LOT AREA	4,144 SF (north parcel) ; 4,480 SF (south parcel) = 8,624 SF Total	
ZONING	NC1P-40 (1.2) (north parcel) ; NC1P-40 (0.75) (south parcel) , Roosevelt Residential Urban Village, Frequent Transit Corridor, Roosevelt Station Overlay District	
PERMITTED USES	<ul style="list-style-type: none"> • Restaurant Uses permitted up to 10,000 SF • Retail sales and services, general use, permitted up to 10,000 SF • Residential uses permitted outright 	23.47A.004, Table A
STREET LEVEL STANDARDS	<ul style="list-style-type: none"> • Non-residential street level frontage to comply with transparency & blank façade provisions. • Required average depth of street level nonresidential spaces 30' (15' min) • Street level nonresidential required to have 13-ft floor to floor height. • Residential uses may occupy, in the aggregate, no more than 20% of the street-level street-facing façade within a NC1 zone. • Along designated principal pedestrian streets, one or more of the following is required along 80% of street-facing façade: general sales, retail, eating and drinking establishments. 	23.47A.008 23.47A.005.C 23.47A.005.D1 (NE 65th is a principal pedestrian street)
STRUCTURE HEIGHT	<ul style="list-style-type: none"> • 40' as zoned. • Height of a structure may exceed the otherwise applicable limit by up to 4' provided a floor-to-floor height of 13' or more is provided for nonresidential uses at street level. • Open railings, planters, parapets, etc permitted up to 4; above height limit. • Solar collectors, mechanical equipment, stair & elevator penthouses allowed to extend up to 15' above height limit, provided the combined total coverage of all features gaining additional height does not exceed 20% of the roof area, or 25% of the roof area if the total includes stair and elevator penthouses or screened mechanical equipment. • Stair and elevator penthouses may extend above the applicable height limit up to 16'. 	23.47A.012
FAR	<ul style="list-style-type: none"> • 4 for lots with 40-ft height limit with a mixed-use building. Both lots have incentive suffixes: <ul style="list-style-type: none"> • north parcel: (1.2 incentive), 4 Max = 4 x 4,144-SF lot area • south parcel: (0.75 incentive), 4 Max = 4 x 4,480-SF lot area • Combined maximum allowable area: 34,496-SF, including all incentive provisions 	23.47A.013
SETBACKS	<ul style="list-style-type: none"> • A setback is required where a lot abuts the intersection of a side lot line and a front lot line of a lot in a residential zone. The required setback forms a triangular area 15' on a side • For a structure containing a residential use, a setback is required along any side or rear lot line that abuts a lot in a residential zone, as follows: 15' for portions of structures above 13' in height to a maximum of 40' - for each portion above 40' in height additional setback at the rate of 2' of setback for every 10' by which the height of such portion exceeds 40' • No entrance, window, or other opening is permitted closer than 5' to an abutting residentially-zoned lot 	23.47A.014
PARKING	<ul style="list-style-type: none"> • None required within urban village / station area overlay district 	23.47A.032
AMENITY AREA	<ul style="list-style-type: none"> • Amenity spaces equivalent to 5% of residential gross floor area required for residential uses <ul style="list-style-type: none"> • Amenity Spaces shall not be enclosed. • Common amenity area: min area 250-sf; 10' min horizontal dimension • Private balconies: min area 60-sf; 6' min horizontal dimension 	23.47A.024
PARKING ACCESS	<ul style="list-style-type: none"> • Access to parking shall be from the alley if the lot abuts an alley improved to the standards of Section 23.53.030.C (paved and min. 12' width) 	23.47A.032 (Lot abuts an alley that meets the min. width but is not paved)

PROPOSAL & SITE CONTEXT



NE 68TH ST

Location
 The proposed project is in Seattle's Roosevelt Residential Urban Village on the SW corner of the intersection of NE 65th St and 14th Ave NE. The site is within a frequent transit corridor and the Roosevelt Station Overlay District for the incoming Light Rail Station (overlay shown outlined in red).

Existing Uses
 The project site is composed of two parcels. Two wood-framed single-family structures, each with an uncovered parking stall off the alley, currently occupy the site.

NE 65TH ST

NE 63RD ST

- Future Link Light Rail Station
- 1. Roosevelt High School
- 2. Rising Sun Produce
- 3. Pies and Pints
- 4. Whole Foods
- 5. Pizza Hut
- 6. AutoPlus
- 7. Transit Oriented Development Sites
- 8. Cowen Park
- 9. Ravenna Park
- 10. Dwell Condominiums
- 11. Bartell Drugs, Wells Fargo, UPS Store
- 12. Tornado
- 13. Health Mutt
- 14. All-Star Cleaners
- 15. Qwest Communications
- 16. Future Development parcels
- 17. Future 7-Story Mixed-Use Project (MUP approval)



EXISTING SITE PLAN

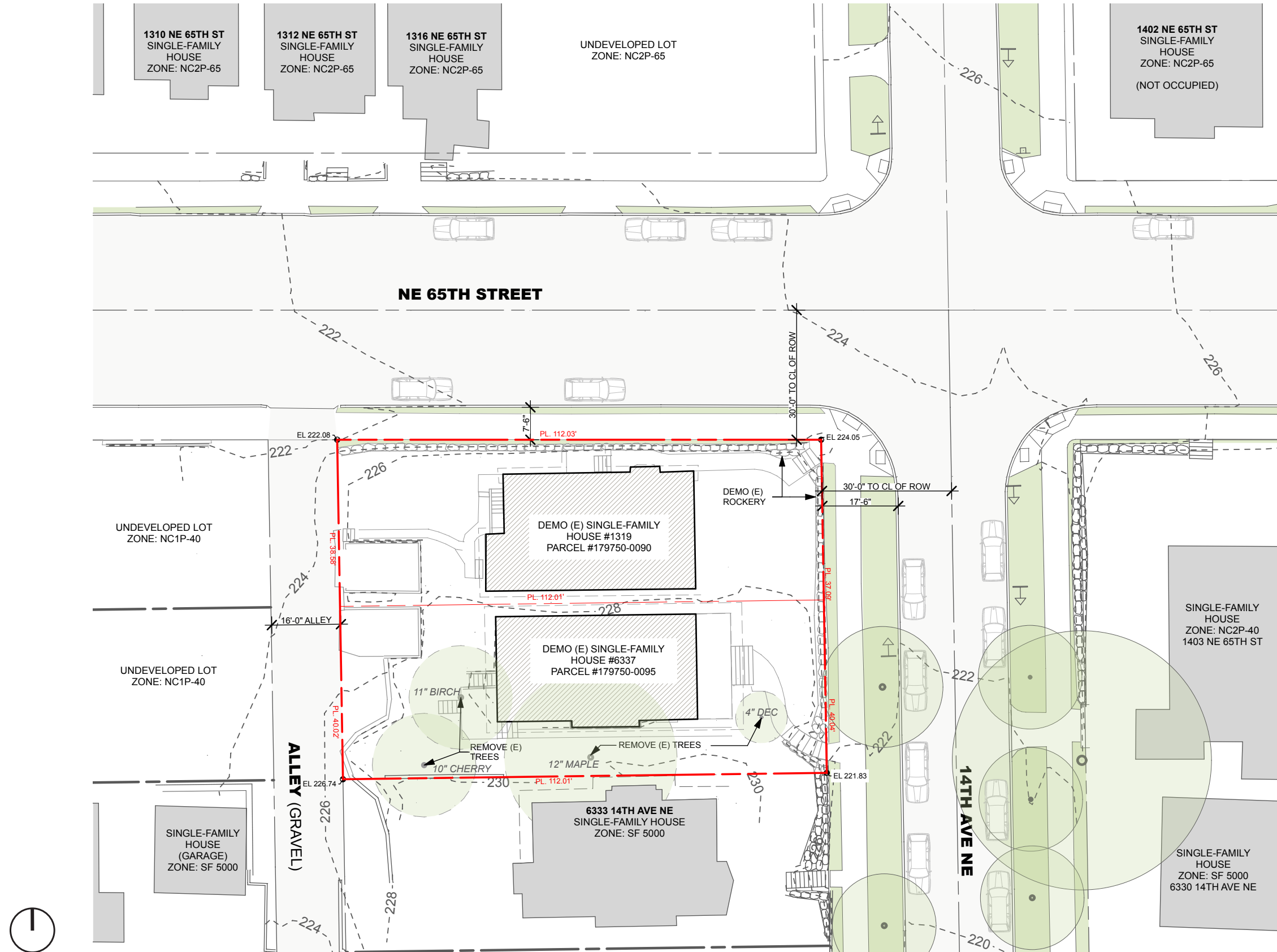
The proposed project site's two parcels total 8,624 SF. The site fronts NE 65th St at the north property line, 14th Ave NE at the east property line, an unimproved (gravel) alley at the west, and the side lot line of a single-family property to the south.

The NE 65th St frontage is 112' in length. The pedestrian rights-of-way width is +7'-6" from back of curb, inclusive of a narrow 1'-6" planter. There are no street trees along this portion of NE 65th, and overhead high-voltage power lines are located on the opposite side of the street. Sidewalk curb ramps are present at the corner, but no painted crosswalks are provided across 14th Ave NE or NE 65th St.

The 14th Ave NE frontage is 77' in length. The pedestrian right-of-way width is +17'-6" from back of curb, and characterized by a wide planter and street trees on the street side, and a narrow planter on the property side. Parking access to properties is provided from the alley, limiting curb cuts down 14th.

Rockeries at the property lines on both street fronts raise the grade of the site within the property boundaries to 4-7 feet above the right-of-way. The highest raised grade is at the southern property line. The sidewalks at the rights-of-way have relatively gentle slopes, rising towards the NE corner of the property. Along NE 65th St the grade slopes gradually from el. +222.08' at the alley entrance to el. +224.05' at the corner of NE 65th St and 14th Ave NE (1.76% slope). Along 14th Ave NE the grade change is shallow as well (2.81%), dropping from the intersection with 65th down to the south to reach el. +221.83 at the SE corner of the site.

The two existing structures on the site located on the rocky berms are accessed by stairs from street level, a condition that continues at the single-family homes south of the property on 14th Ave NE. There are four existing deciduous trees on the southern parcel, three of which are of greater than 6" caliper. There are also a number of larger hedges on the site, including a significant hedge along the alley at the NW corner of the site.





- Project Site Under Current Zoning
- Neighboring Developments with permits or in construction
- Potential Neighboring Development Parcels

* Zoning Heights Shown As Approximation / Not to Scale






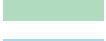

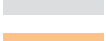




SITE CONTEXT: LAND USE

The site is bounded by public rights-of-way to the north (NE 65th St), east (14th Ave NE), and west (alley). The south property line abuts a single-family home's side-lot line.

The neighborhood directly south is a single-family residential zone. The surrounding neighborhood to the north, east, and west of the site is a mix of single-family homes, low-rise density retail with surface parking, and undeveloped lots. The Roosevelt Neighborhood's Core Commercial Area begins a block west of the site, and is characterized by higher density multi-family housing and retail. The new Roosevelt Light Rail Station will be located a block and half west of the site on NE 65th St.

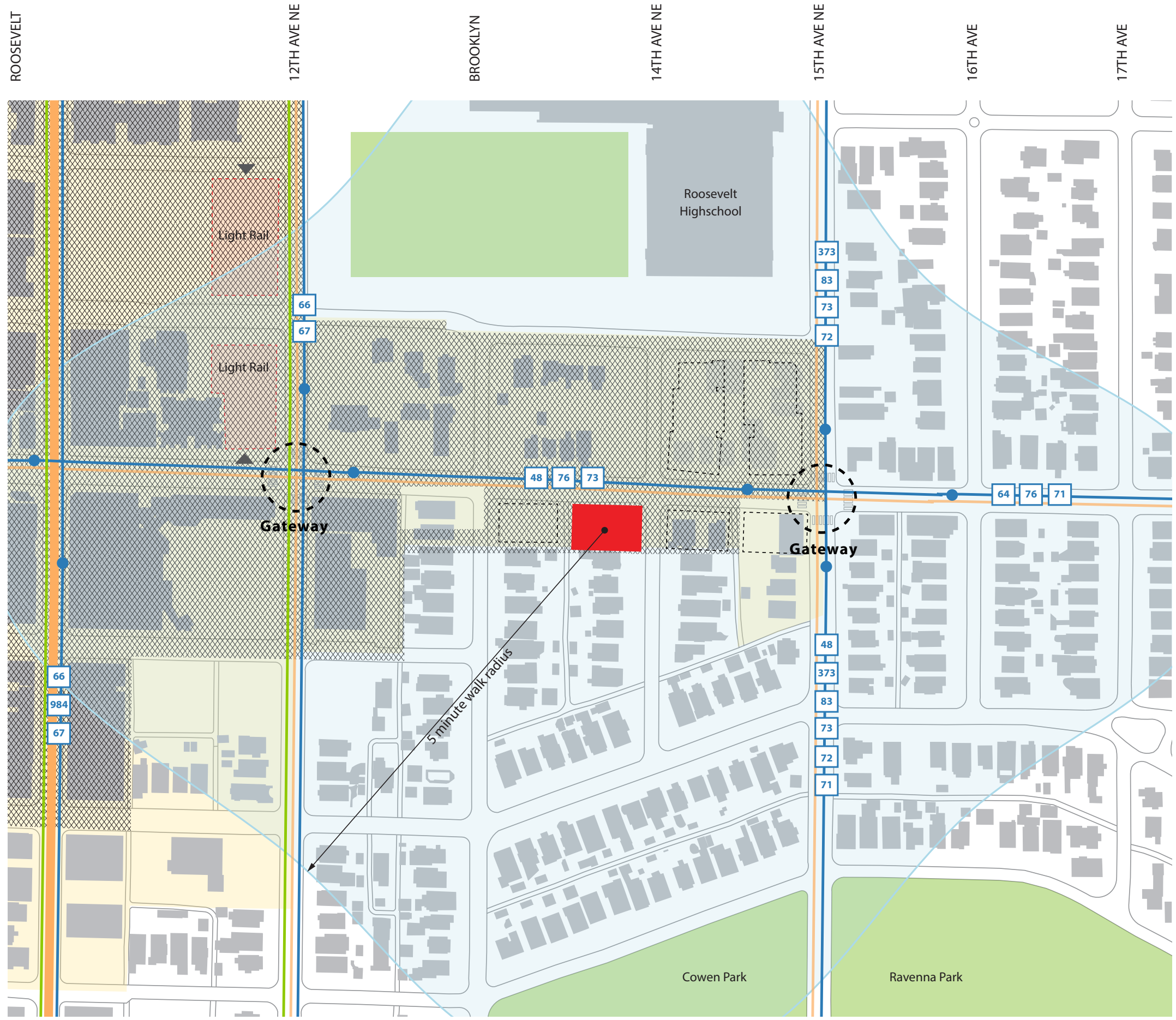
Two blocks north of the site is Roosevelt High School, and two blocks south of the Site are Cowen and Ravenna Parks.

Legend

- Future Light Rail Station (2021) 
- Future Development 
- Single Family Residential 
- Multi Family Residential 
- Mixed Use 
- Retail 
- Office 
- Surface Parking 
- School / Institutional 
- Utility 
- Parks/Open Space 
- Roosevelt Commercial Core 



SITE CONTEXT: TRANSIT MODES



NE 68TH ST

NE 65TH ST

NE 63RD ST

The project site is well served by several bus lines located within a 5-minute walk radius. King County Metro bus routes 48, 73 and 76 run past the site along NE 65th Street and provide connections to Ballard and Capitol Hill, the Central Area and Mount Baker (route 48); to Jackson Park, the University of Washington and the Downtown retail core (route 73); and to Wedgwood, Hawthorne Hills and additional connection to the Downtown retail core (route 76). Additional connections to the Downtown retail core, Northgate, the University of Washington and a host of other neighborhoods are within a short walk of the site. Travel times to the University, Capitol Hill and Downtown will greatly improve with the opening of Sound Transit light rail service to Roosevelt Station located 4 short blocks west of the site.

The public transportation options in the vicinity of the site also speak its pedestrian environment. The relative flatness of NE 65th Street provides an easy walk to the future light rail station and Roosevelt's commercial core located within a five-minute walk to the west of the site. Cowen Park and Ravenna Park are also located within a five-minute walk to the south of the site, while Roosevelt High School is a block to the north.

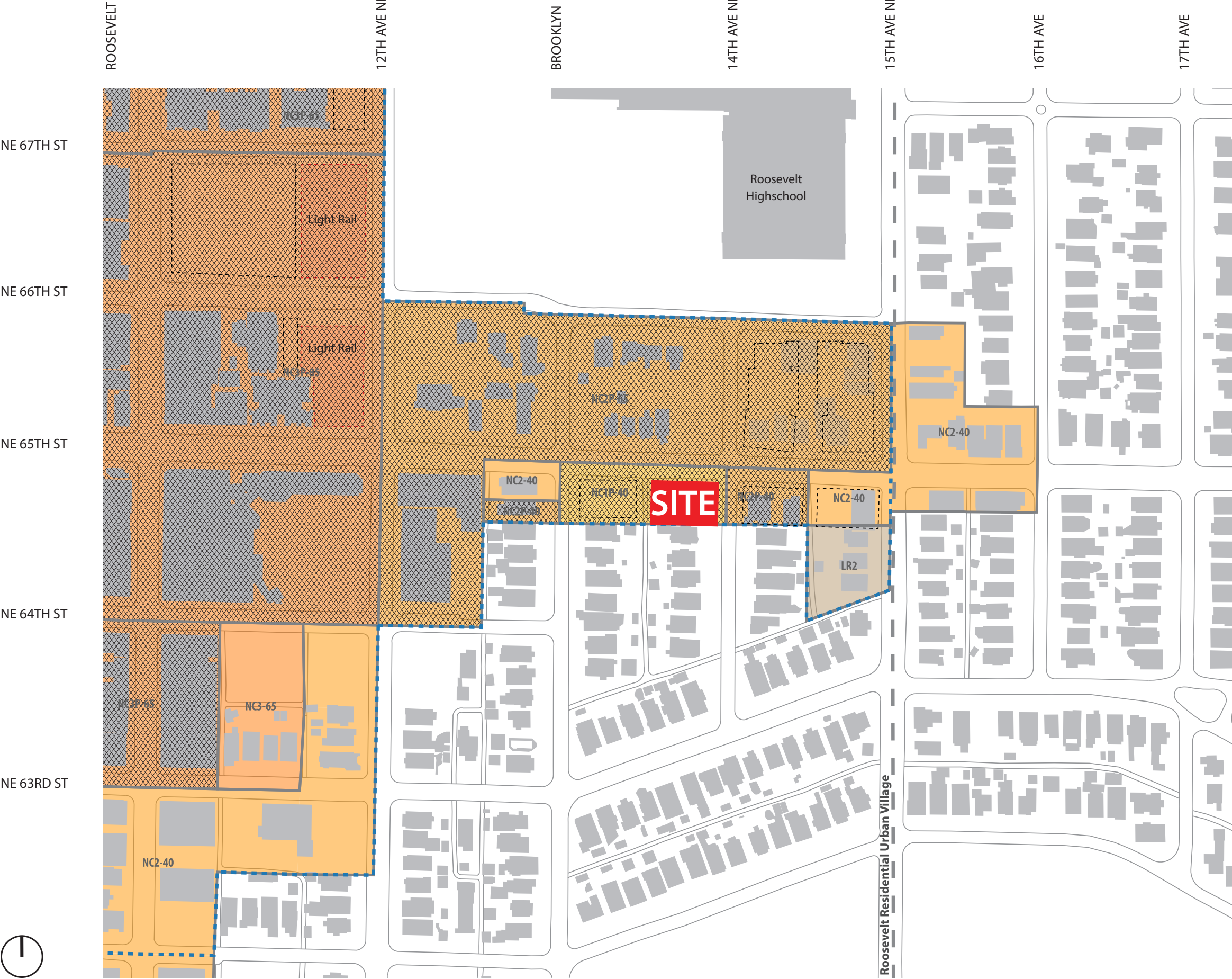
Legend

- Main Car Arterials:**
 - Principal Arterial / Regional Connector
 - Minor Arterial / Commercial Connector
- Future Light Rail Station (2021)
- Identified Gateway Intersection per Roosevelt Neighborhood Guideline
- Frequent Transit Corridors
- Pedestrian Overlay
- 5 Minute Pedestrian Walkshed
- Bus Routes
- Bus Lines 2
- Current Bicycle Routes



CONTEXT: EXISTING ZONING

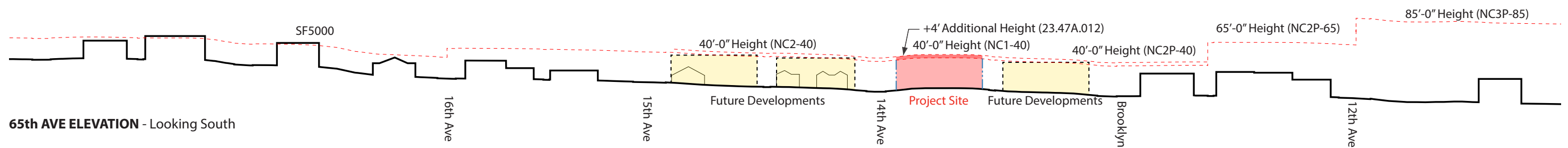
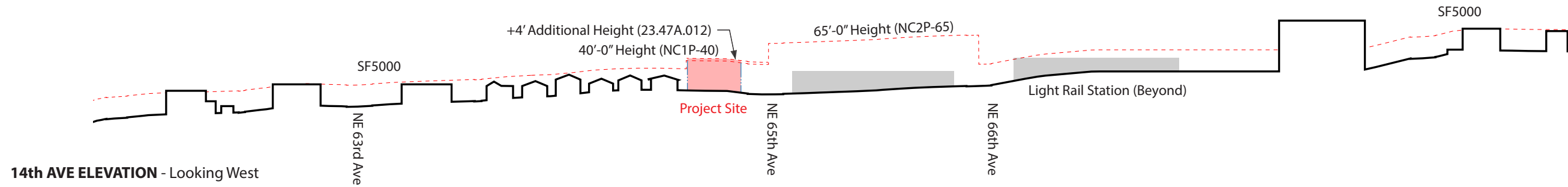
The site is zoned NC1P-40. The site is bounded by public rights-of-way to the north (NE 65th St), east (14th Ave NE), and west (alley). Properties beyond the rights-of-way in these three directions are NC1P and NC2P zones. The south property line abuts the side lot line of a SF 5000 zone.



Existing Zoning Legend

- NC1
- NC2
- NC3
- LR1 & LR2
- SF
- Roosevelt Station Area Overlay
- Roosevelt Residential Urban Village
- Zone Boundaries
- Pedestrian Overlay

CONTEXT: ZONING ENVELOPE



CONTEXT: NE 65TH AVE

The northern edge of the property is bounded by NE 65th Street. The topography along the property line has a minimal slope (under 2%) up towards the east. The sidewalk along this stretch of 65th is narrow (roughly 7 1/2-feet), inclusive of a thin, planted strip at the curb. There are no street trees on either side within the neighboring blocks. Both sides of the 65th are zoned for NC, with a 65-ft height limit at the north side, and a 40-ft height limit on the south, though current property uses do not reflect this potential. Currently, 65th is a mix of single-family houses, undeveloped lots, and low-intensity commercial, often with surface parking.

The Roosevelt Commercial Core boundary edge is a block west, and this area of higher density retail and mixed-use is visible from the site.

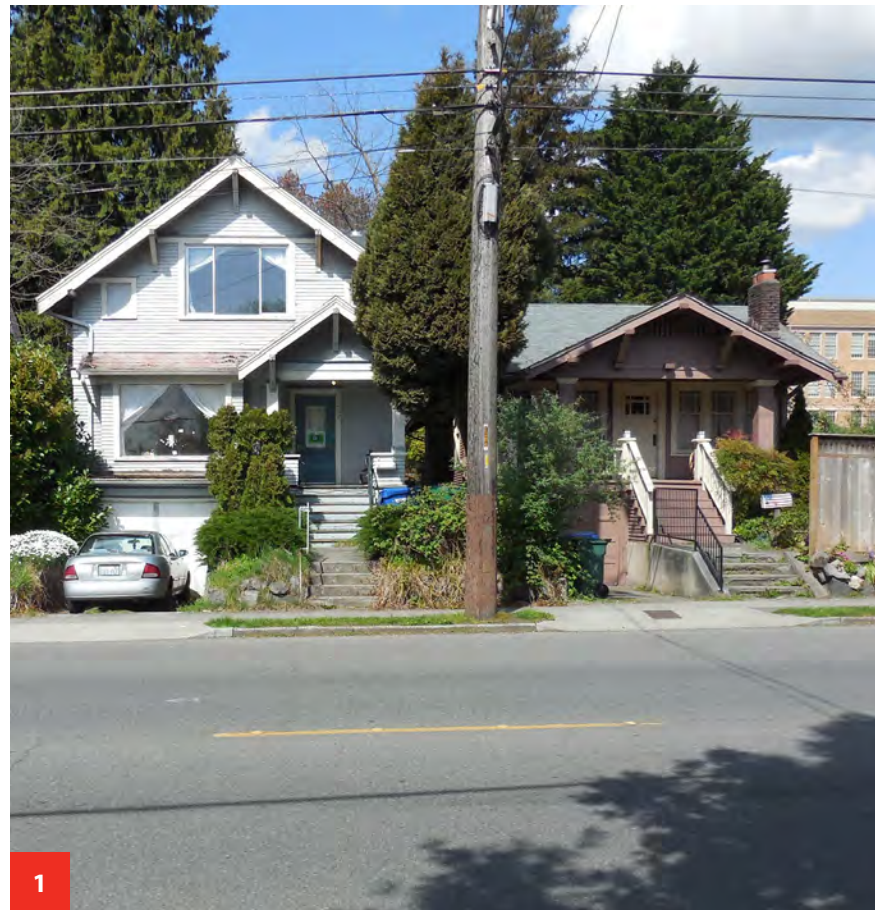
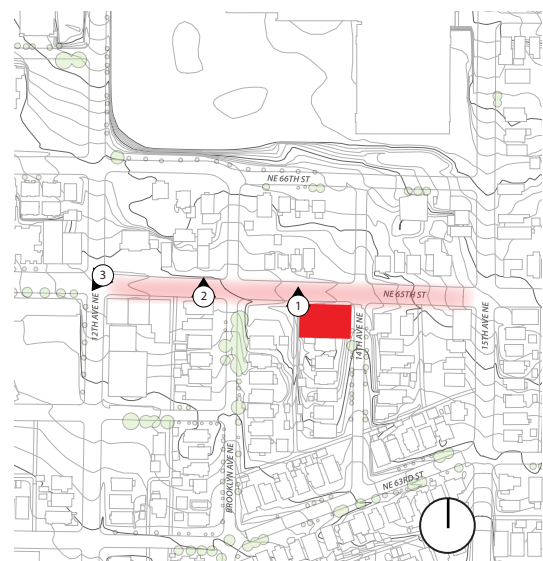
The future Roosevelt Link Light Rail Station, set to open in 2012, is a block and half west of the site on the opposite side of the street.



NE 65th - Looking South



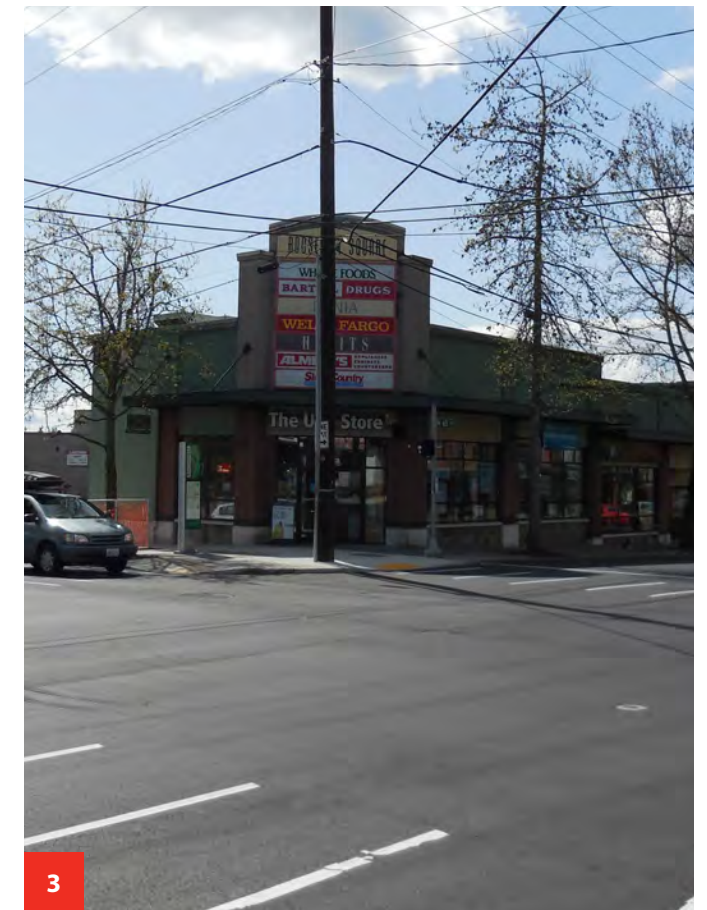
NE 65th - Looking North



1



2



3

CONTEXT: 14TH AVE NE

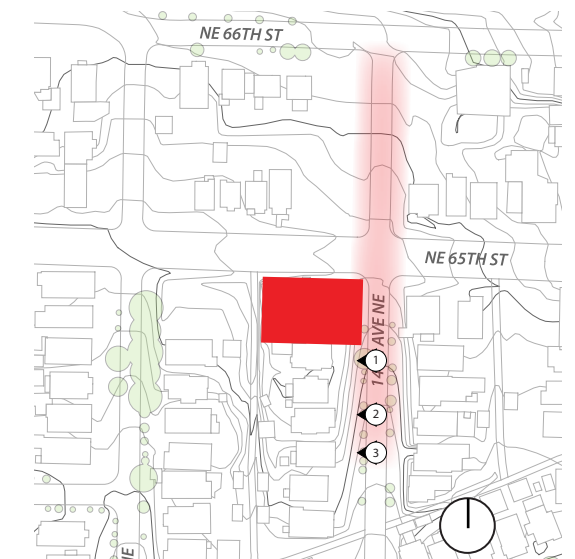
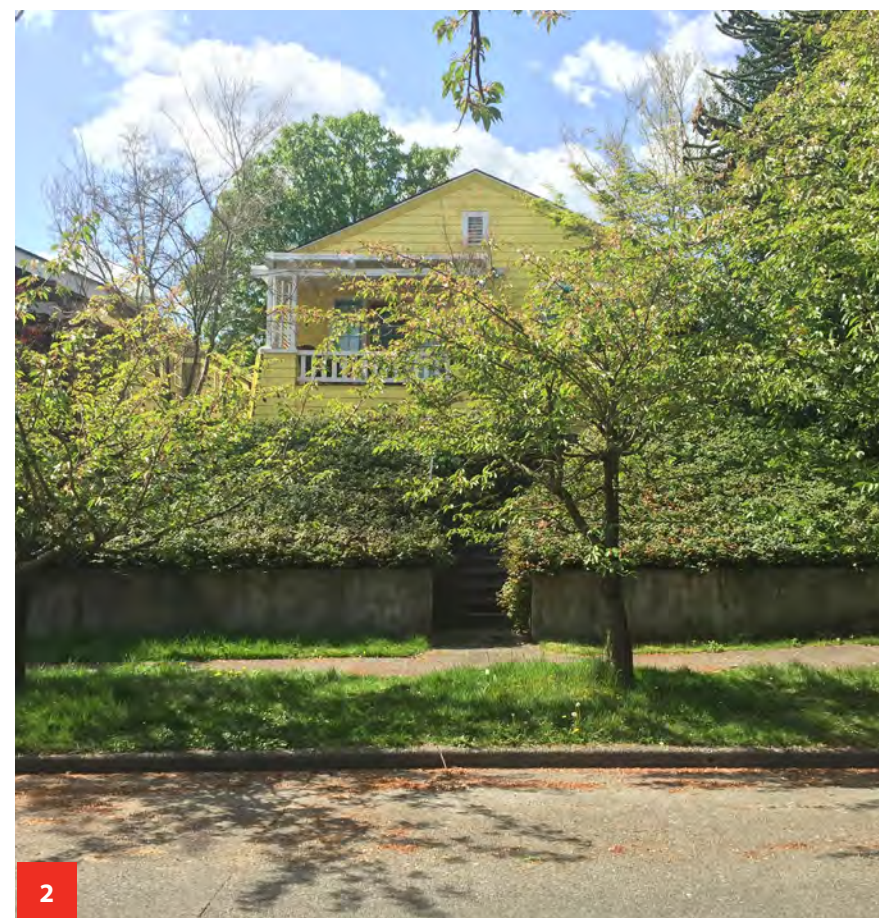


14th AVE NE - Looking West



14th AVE NE - Looking East

The eastern edge of the property is bounded by 14th Ave NE. The topography along the property line has a gentle slope (under 3%) towards the north. 14th is a quiet street characterized by single-family residential houses, typically set back from the property line and raised above the street level by landscaped rockeries or retaining walls. Street trees are well established and set within wide planting beds. Resident-only parking is provided on both sides of the street. One block north of the site, across NE 65th, both sides of the street are awaiting new development (empty lots and boarded houses), and at the end of that block 14th dead-ends into the Roosevelt High School campus.



CONTEXT: SURROUNDING NEIGHBORHOOD

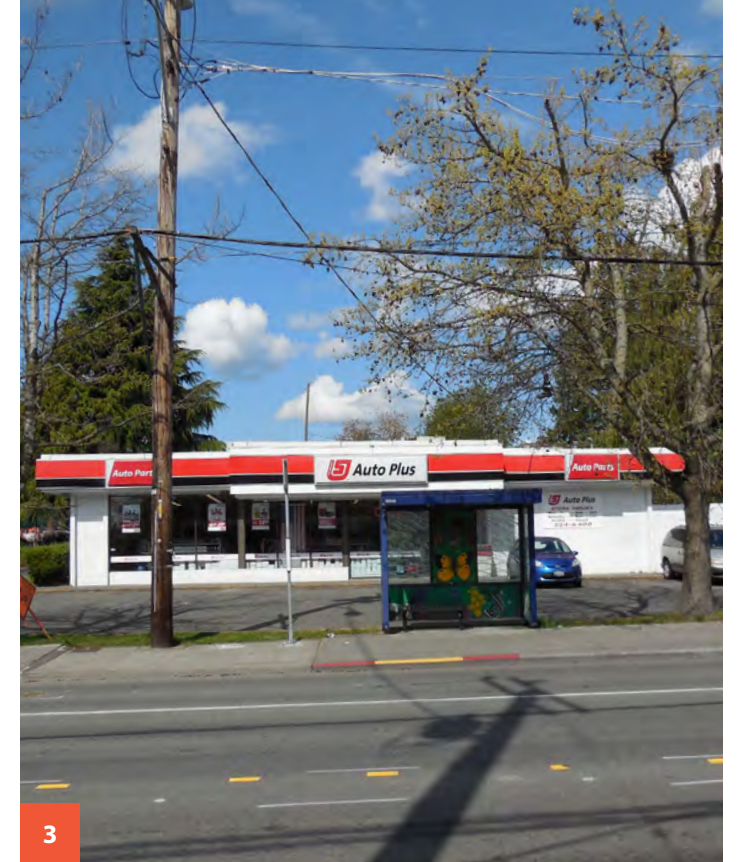
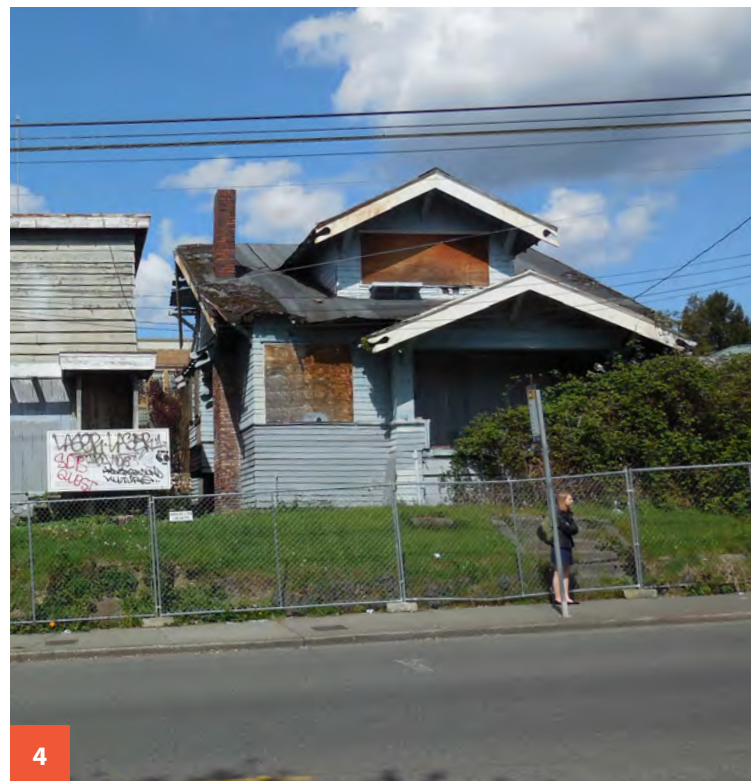
The neighborhood surrounding the site is in transition, due in large part to the Link light rail station that will soon open at the corner of NE 65th and 12th Ave NE. As noted above, NE 65th Street is zoned for a higher density and height than its current uses. Multiple lots are boarded up or empty, awaiting redevelopment, and a MUP has already been approved for a new mixed-use high-rise project diagonally across the street from the proposed site. The existing commercial streetscape lacks street wall continuity, as building setbacks and building siting on the lots vary, with some commercial structures separated from the street by surface parking.

The eastern edge of the Roosevelt Neighborhood Commercial Core is a block west of the site, where retail and mixed-use properties increase, pedestrian amenities such as street trees and crosswalks are more prevalent, and building heights increase. With the incoming light rail and current zoning allowances, the blocks surrounding the site could develop as an extension of the commercial core. The Roosevelt Design Guidelines acknowledge this extension, with the identification of two neighborhood “gateway” intersections on 65th: one at the 12th Ave NE intersection and one at 15th Ave. Further development on the blocks surrounding the site would enhance the pedestrian connection between these two gateways.

In contrast to the rapidly morphing environment of 65th, the residential areas of the Roosevelt neighborhood, such as south of the proposed project site, are well-established single-family blocks. Houses and yards are maintained, and the building style is cohesive with pitched roofs with eaves, and raised porches.

Roosevelt High School, the largest high school in the city, is located a block north of the site. Two parks, Cowen and Ravenna, are located a few blocks south.

1 Link Light Rail Roosevelt Station. Expected opening in 2021, located 1-1/2 blocks from the site. // **2 Retail at eastern edge of Roosevelt Commercial Core.** // **3 Retail at eastern edge of Roosevelt Commercial Core.** // **4 Properties on NE 65th St Awaiting Redevelopment.** // **5 Typical Residential.** South of the site is a well-established single-family residential zone. // **6 Roosevelt High School.** The school is located a block north of the site

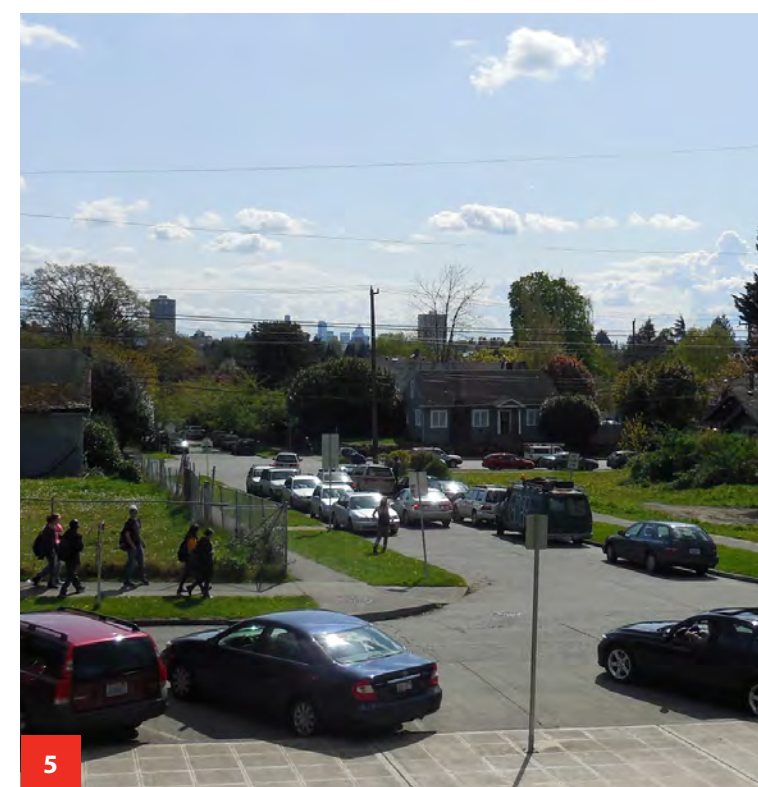


CONTEXT: EXISTING SITE

The site is currently occupied by two small single-family structures, raised approximately 4-7' above the street level grades by a continuous rockery at the property lines facing NE 65th St and 14th Ave NE. No curb cuts exist on either street; each single-family house has a single uncovered parking stall accessed off the unimproved alley. Power lines for all the single-family houses on the block run overhead in the alley, crossing over NE 65th at the site's alley property line. The proposed project will consider undergrounding the power under 65th and across the length of the new building as part of construction. There are four trees on the site, all on the southern parcel, and a number of larger hedges.

The sidewalk on NE 65th St, as mentioned previously, is narrow and lacks street trees. This width, coupled with the heavy vehicular use on NE 65th leads to an unsafe pedestrian condition.

By contrast, the sidewalk on 14th Ave NE is spacious and inclusive of a wide planting strip at the curb. Street trees line both sides of 14th.



1 View of Site from Northeast Corner. Existing single-family is raised on a rockery that edges the property line. // **2 View of Site from Northwest Corner.** A gravel alley backs the property to the west, providing access to residential parking. // **3 Sidewalk at 14th Ave NE.** A generous planting strip and sidewalk fronts the east side of the property. // **4 Sidewalk at NE 65th St.** A narrow sidewalk and minimal planting strip without street trees fronts the north side of the property. // **5 View of Site from Roosevelt High School (North of Site).** The site is directly north of a single-family zone, which provides protected views for upper levels to the south.

SITE ANALYSIS

Topography

- Very gentle slope along NE 65th St
 - 2' gain in elevation from west to east
- Gentle slope along 14th Ave NE
 - 2.25' gain in elevation from south to north

Neighboring Buildings

- Undeveloped NC2P-65 zoned lot to north (across NE 65th St)
- Single-family residence to east (across 14th Ave NE)
- Single-family residence to south
- Undeveloped NC1P-40 zoned lot to west (across alley)

Solar Access

- Excellent solar access due to low-height single-family zone to the south.
- Some overshadowing or shading possible on lower levels in the morning due to abundance of street trees on 14th

Views

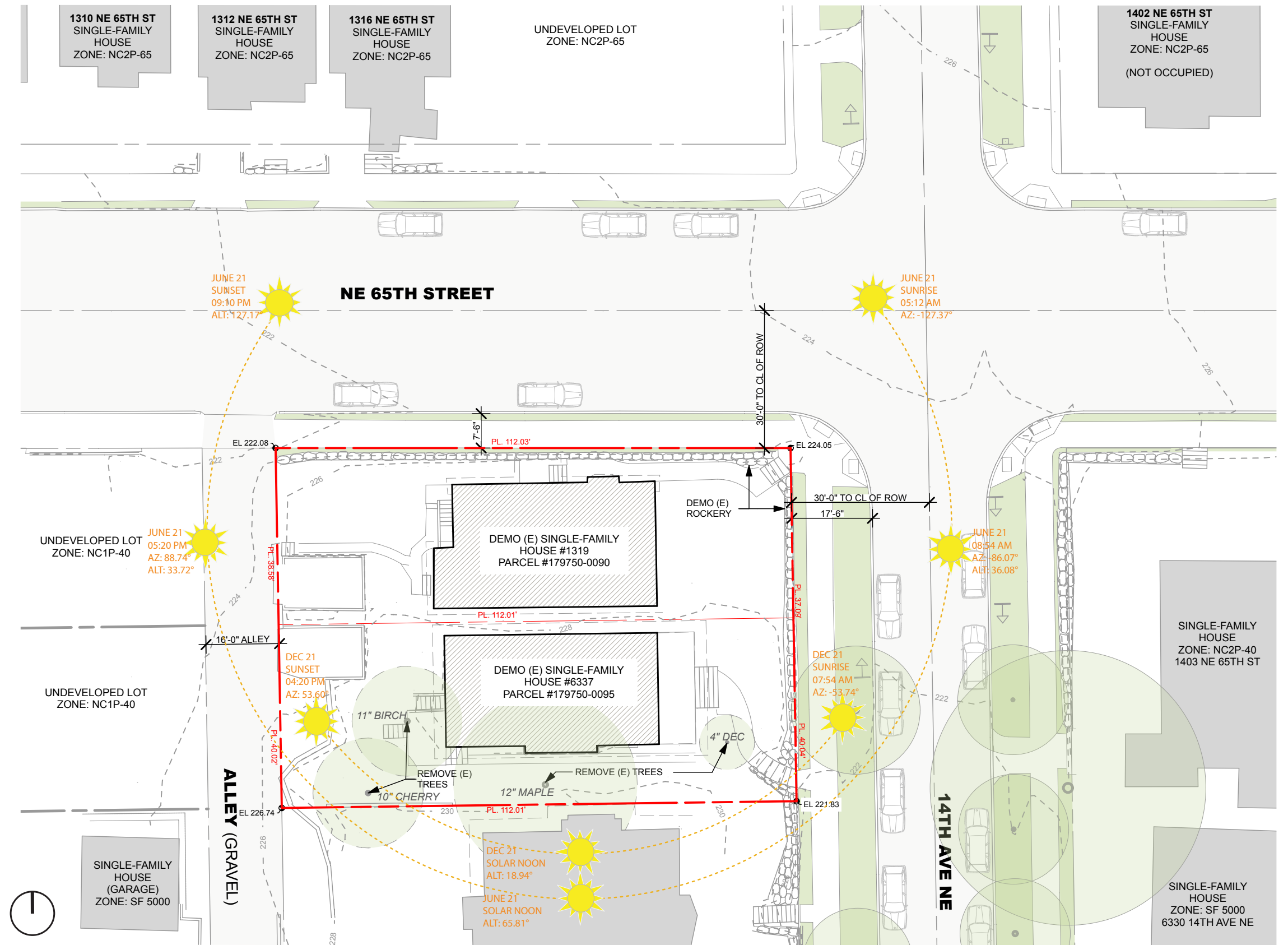
- Currently, territorial views are available in all directions above the first floor. Views to the south are protected due to adjacent single-family zoning.

Structure Height

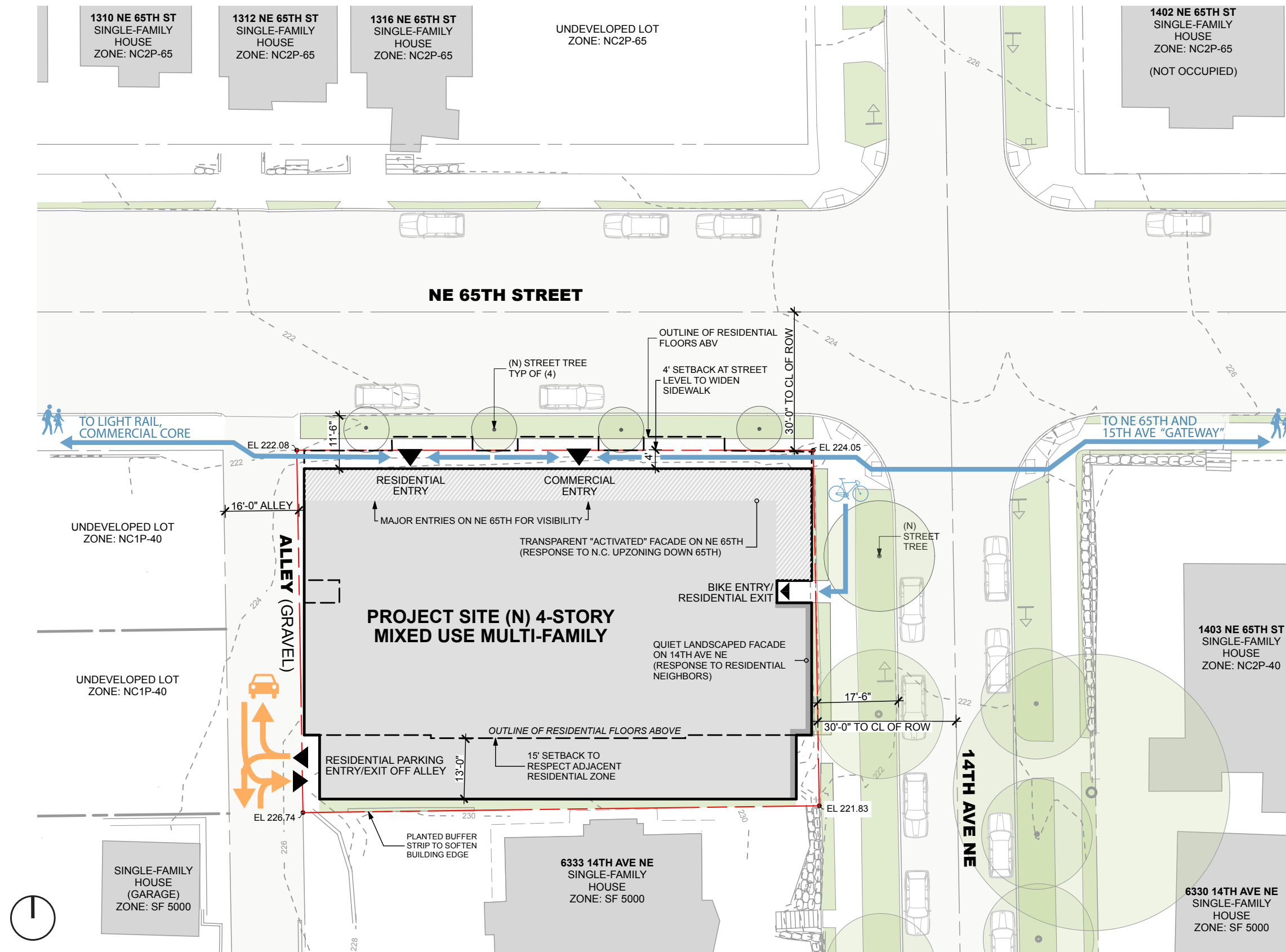
- Zoned as NC1P-40 with a base height limit of 40-ft.
- Located within 40-ft mapped height limit zone, which allows a 4-ft height increase when a floor-to-floor height of 13' or more is provided for nonresidential uses at street level.
- Total maximum allowable building height: 44-ft

Allowable Building Area

- The maximum FAR and site areas for the building site's two parcels are:
 - north parcel:
 - 1.2 Base FAR (4 Max FAR*)
 - 4,144-sf lot area
 - south parcel:
 - 0.75 Base FAR (4 Max FAR*)
 - 4,480-sf lot area
- Maximum allowable area: 34,496-sf*
 - * including incentive provisions



PRELIMINARY SITE PLAN



Setback Requirements

- A 15-ft setback for portions of structures above 13-ft in height to a max of 40-ft, and for each portion above 40-ft, an additional setback at the rate of 2-ft of setback for every 10-ft, is required where a lot abuts a side lot in a residential zone. This setback is from the south property line.
- A setback forming a triangular area 15-ft on a side is required where a lot abuts the intersection of a side lot line and front lot line of a lot in a residential zone. This setback is from the southeast corner of the property.
- High-voltage overhead power lines that cross NE 65th at the northwest corner of the property and extend down the alley along the western property line would require a setback, but the project proposes to underground the lines.

Traffic and Circulation

- NE 65th is a minor arterial/comm. connector street with time-limited parking to the south, and unrestricted parking to the north.
- NE 65th serves a number of bus lines
- NE 65th is a principal pedestrian street.
- 14th Ave NE is a residential with minimal traffic and resident parking on both sides of the street.
- There are no curb cuts on the site, and a mid-block alley off NE 65th can serve vehicular access needs.

Streetscape

- NE 65th St has a narrow sidewalk (+ 7'-6") and lacks street trees on either side of the street. There are infrequent pedestrian crosswalks (and closest crosswalks are at 12th Ave NE and 15th Ave NE).
- 4th Ave NE has wide tree-lined sidewalks (+ 17'-6") with generous planting strips at the curb edge. Street trees are well established.

Neighborhood Influences

- NE 65th St is zoned to become commercial. Locate major building entries, and commercial programs on this street.
- 14th Ave NE is residential and quiet. Avoid adding activity to this street to preserve privacy.

Building Footprint	
Street Parking Area	
Pedestrian Entry/Exit	
Parking Entry/Exit	

DESIGN PROPOSAL: ALTERNATE 1 (L-SHAPE)

The street level of Design Alternate 1 identifies NE 65th St as the commercial frontage, and 14th Ave NE as the residential frontage. The multifamily residential lobby is located halfway down 14th Ave, and is flanked by residential amenity spaces. The corner is occupied by a residential amenity space that could be converted to a new commercial space or an expansion of the commercial space to the west. The NE 65th street-level façade and the northern half of the 14th Ave NE street-level façade is set back 4' from the property line to expand the pedestrian realm and provide weather protection. Access to the residential parking garage is off the alley.

The three residential floors above are an L-shape configuration in response to the corner lot location, and to maximize residential area. The L-shaped floor plan would require departures from both residential setbacks at the south property line.

A small basement houses building services and residential storage units.

Primary Disadvantages/Concerns

- The scheme lacks a response to the southern residential zone transition; there are no provisions to respond to the 15' setback triangle, or the 15' setback above 13' in height.
- The location of the multifamily entry on 14th Ave NE will draw more noise and activity down the quiet residential street.

Summary

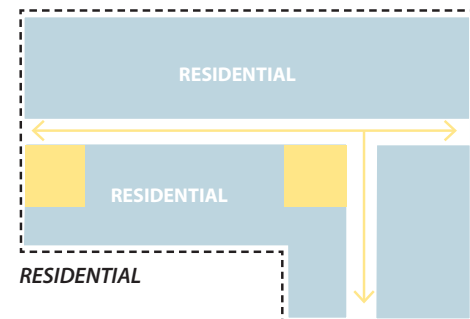
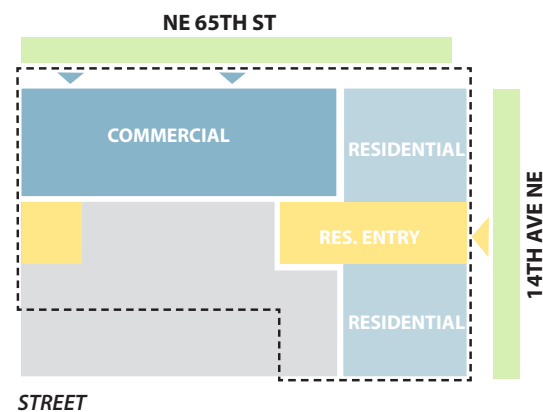
Stories: 4 (3-over-1 + 1 below grade)
 Unit Count: 41 (6,160 unit SF/ Floor)
 Floor Area: 24,430-SF Residential
 1,850-SF Commercial
 2,581-SF Parking
 30,834-SF Total
 Parking: 7 stalls (residential)

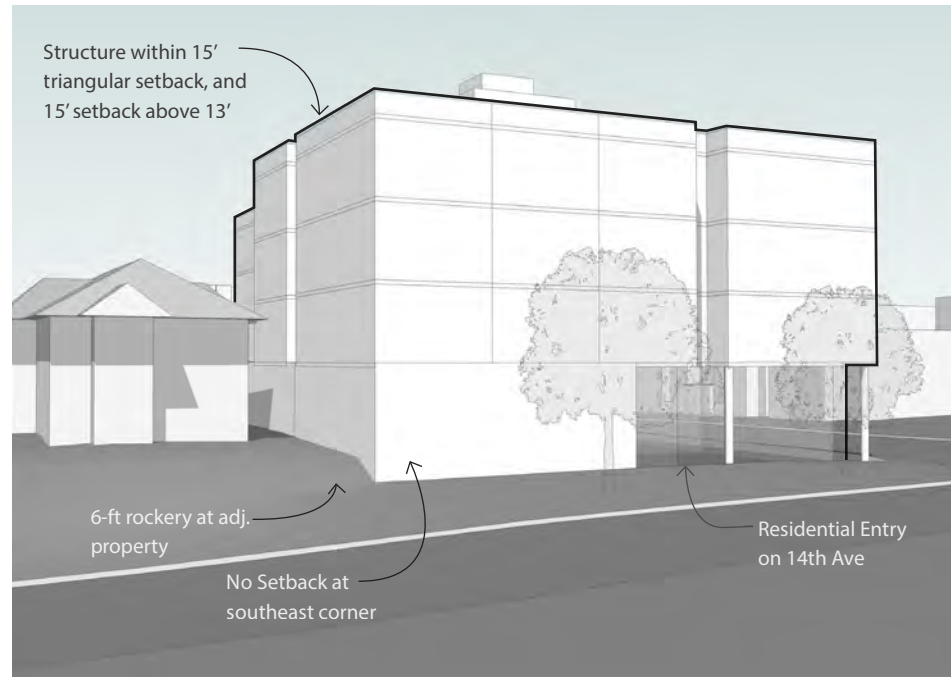
Ground Floor Uses:

- NE 65th St: Commercial / Residential Amenity
- 14th Ave NE: Residential Lobby & Amenity
- Alley: Parking Access

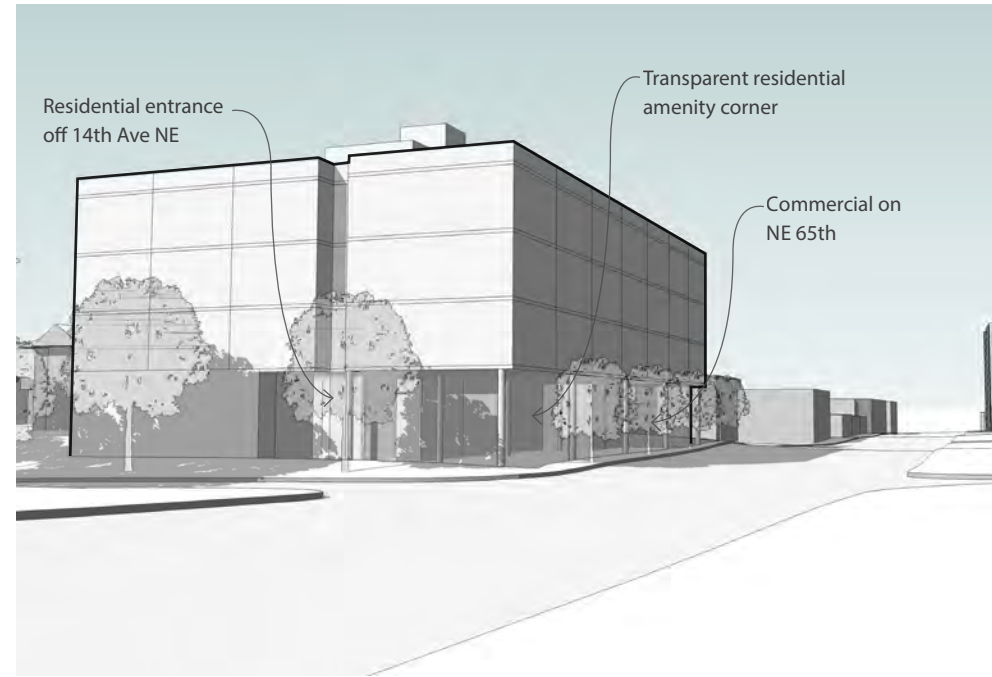
Potential Departures

- Increased percentage of residential use at street level in NC Zones (NE 65th & 14th Ave NE)
- Reduced width of overhead weather protection on principal pedestrian streets (< 6-FT)
- Reduced non-residential use depth at street level
- Structure in triangular setback abutting a residential lot
- Structure in south PL setback above 13-ft
- Reduced parking lot aisle width

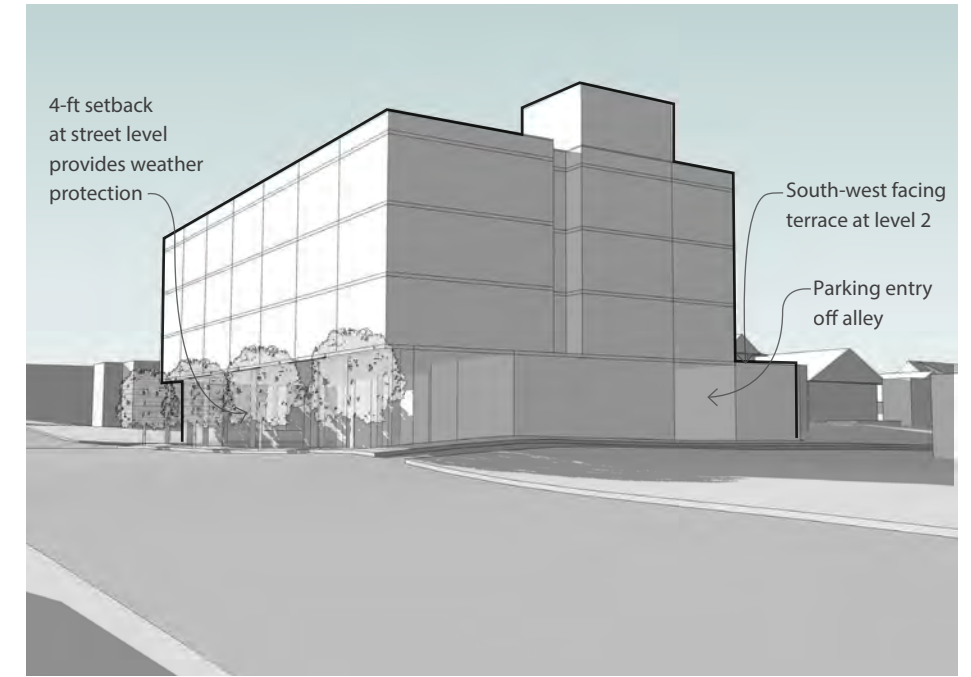




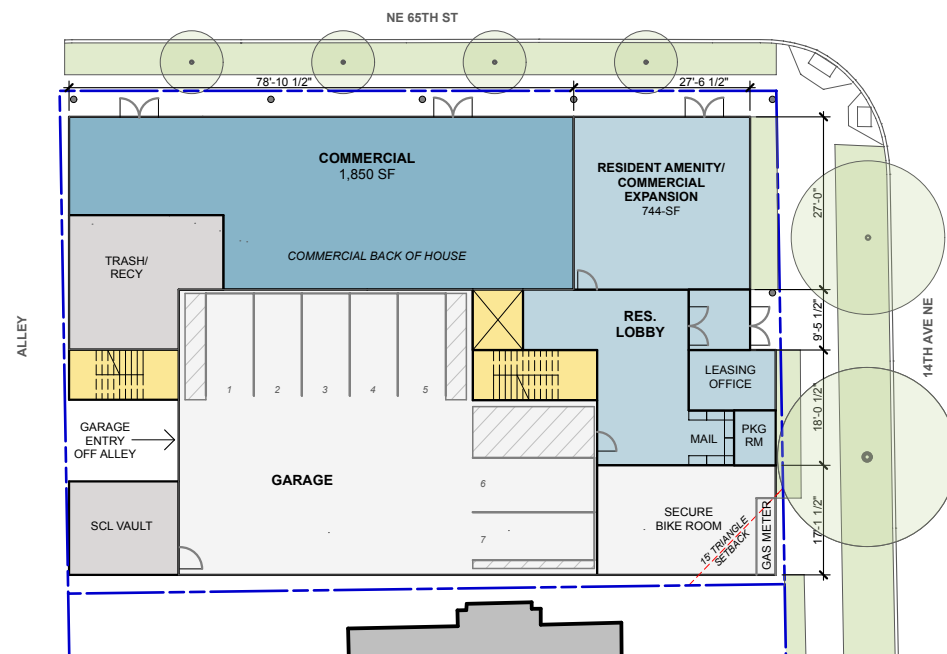
Southeast Corner (14th Ave NE)



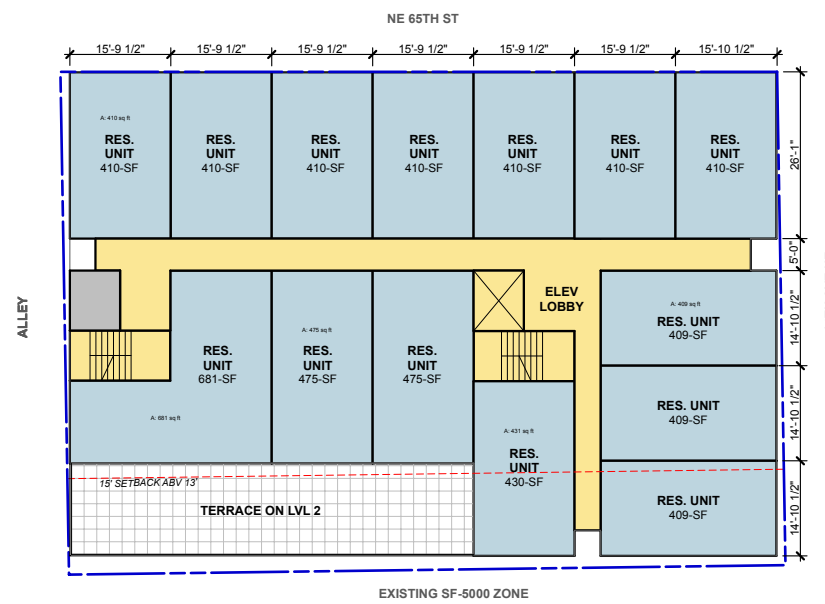
Northeast Corner (Corner of NE 65th St and 14th Ave NE)



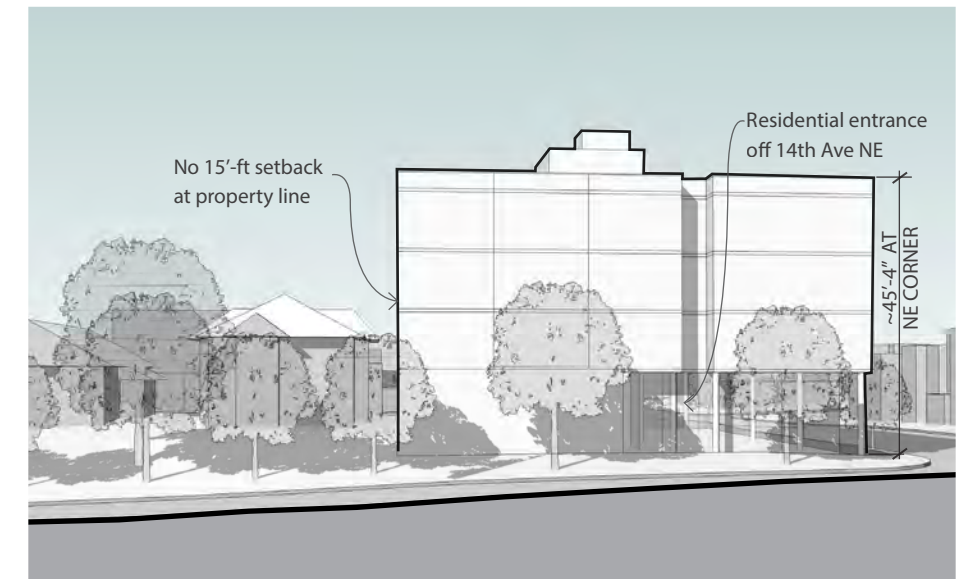
Northwest Corner (NE 65th St)



Street Level Plan (Level 1)



Typical Residential Plan (Level 2)



East Elevation (14th Avenue NE)

DESIGN PROPOSAL: ALTERNATE 2 (NO DEPARTURES)

The street level of Design Alternate 2 is heavily weighted towards commercial use, in compliance with the land use codes. A large commercial space is located on NE 65th, and four live-work units front 14th Ave NE. The multifamily entry is minimal and located halfway down NE 65th. The allowable 20% of the facade length for residential use is occupied by a vestibule and leasing office, with no formal lobby. Instead, residential amenity spaces are stacked above the multifamily entry on the residential floors (borrowing space from rentable units). Residential Parking is accessed off the alley.

A small basement houses building services and residential storage units.

The three residential floors above are organized in a bar around a double-loaded corridor. Behind the bar the building steps down to a second floor garden terrace, bringing the building mass down to the scale of the southern residential houses.

Primary Disadvantages/Concerns

- To achieve minimum required depths in the commercial space, and required parking aisle dimensions behind, the building must build to the northern lot line. Consequently, the scheme cannot widen the pedestrian right-of-way at NE 65th, which is currently 7'-6" from back of curb with a very minimal plant strip.
- The building preserves the 15-ft triangular setback abutting the residential lot at the southeast corner of the property, but this missing corner will be a dark, hidden inlet with low visibility to passersby.
- Locating live-work units that will attract more activity and noise on 14th Ave NE may not be to the benefit of the quiet residential neighbors.

Summary

Stories: 4 (3-over-1 + 1 below grade)
 Unit Count: 41 (5,401 unit SF/ Floor)
 Floor Area: 20,889-SF Residential
 4,261-SF Commercial (includes Live-Work)
 3,052-SF Parking
 29,389-SFTotal

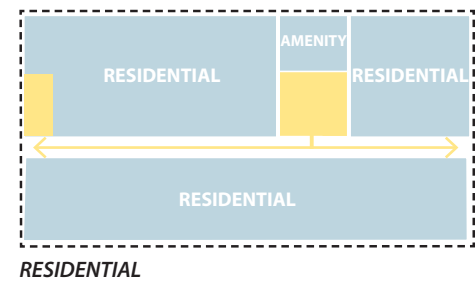
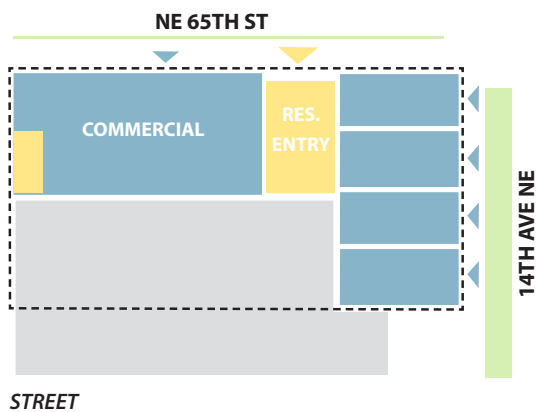
Parking: 8 stalls (residential)

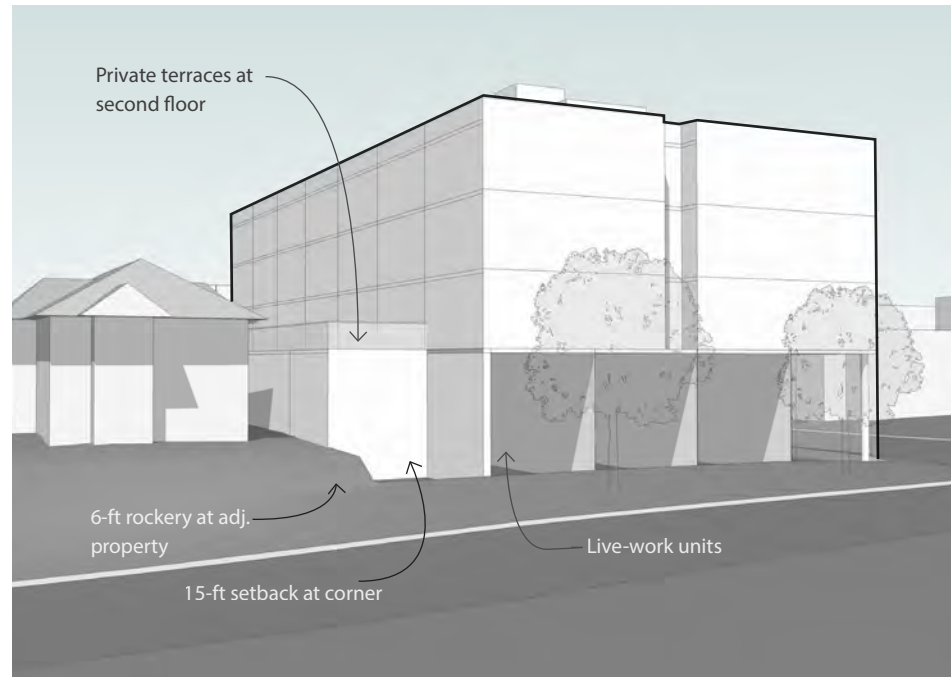
Ground Floor Uses:

- NE 65th St: Commercial / Residential Lobby
- 14th Ave NE: Live-Work Units
- Alley: Parking Access

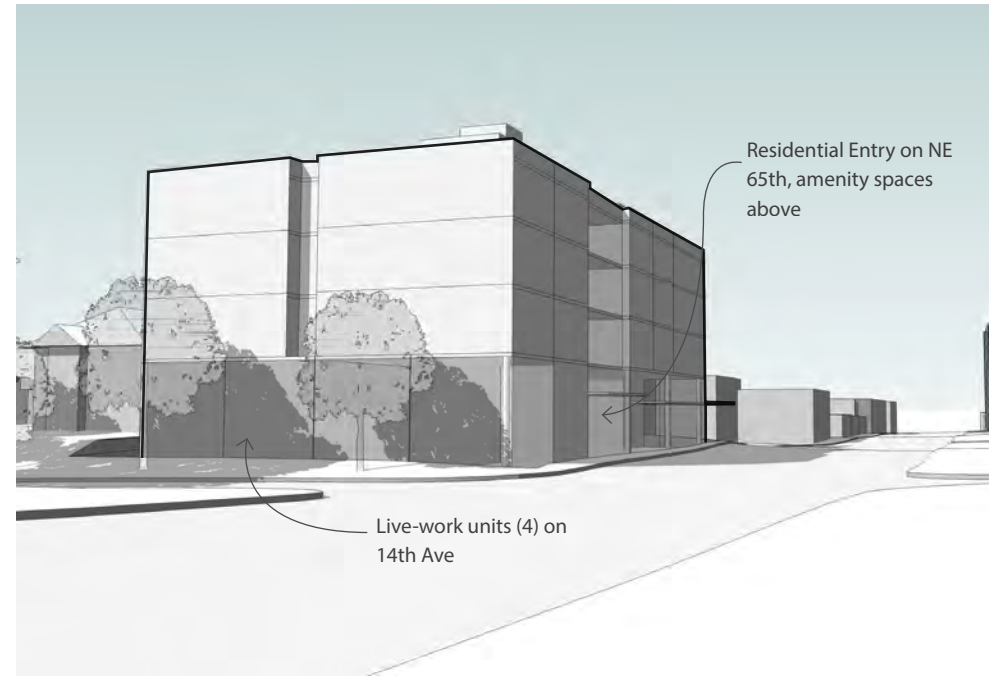
Potential Departures

None

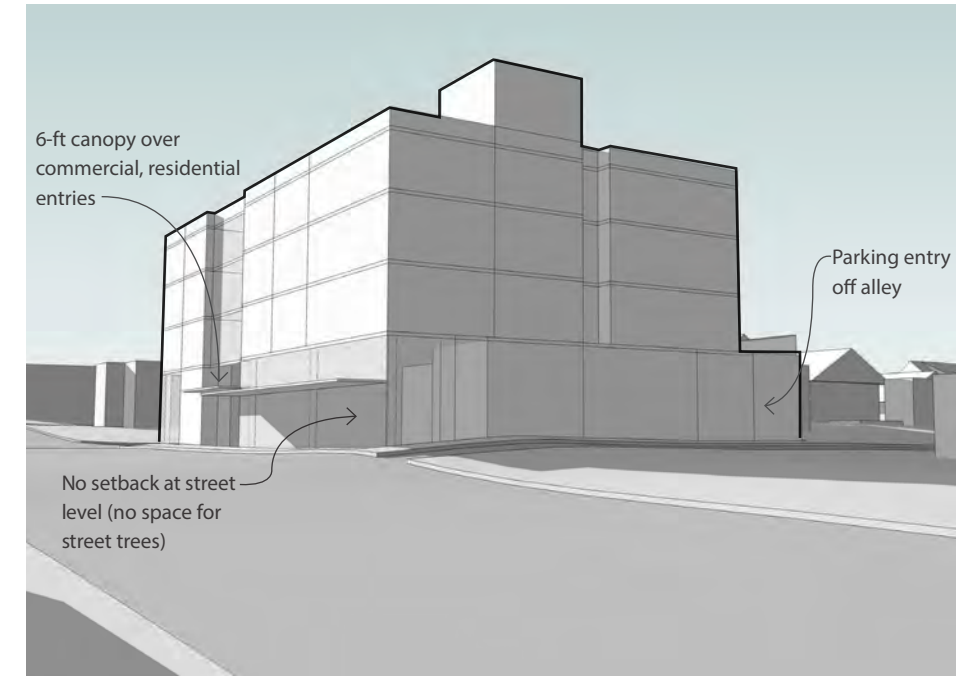




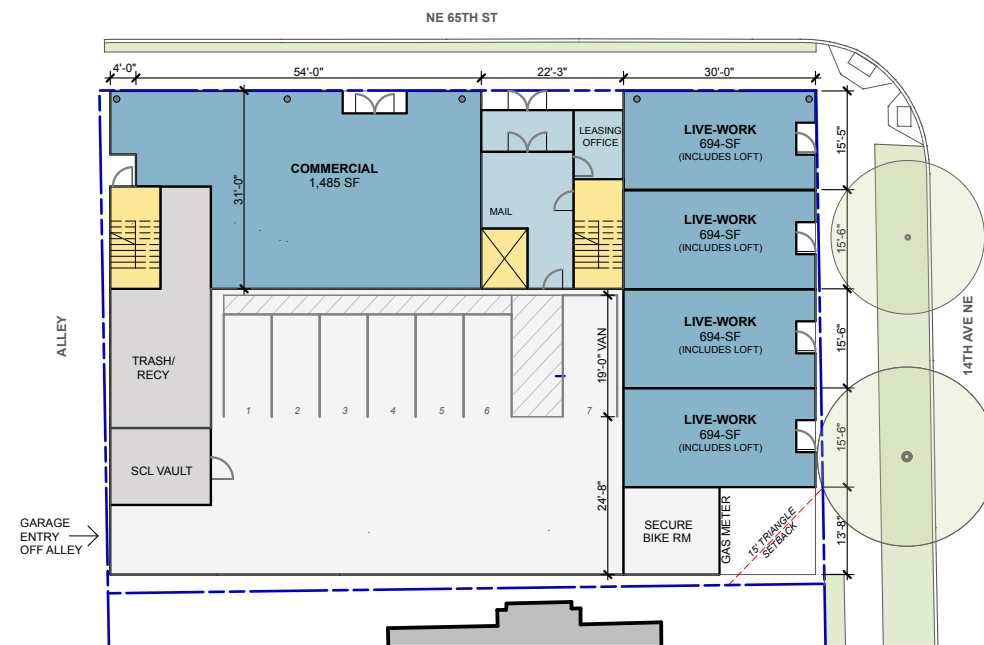
Southeast Corner (14th Ave NE)



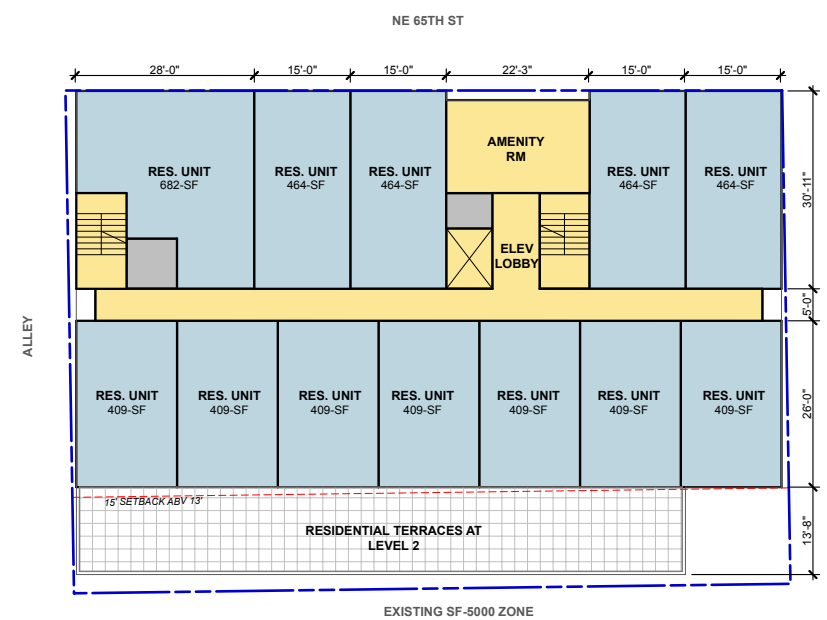
Northeast Corner (Corner of NE 65th St and 14th Ave NE)



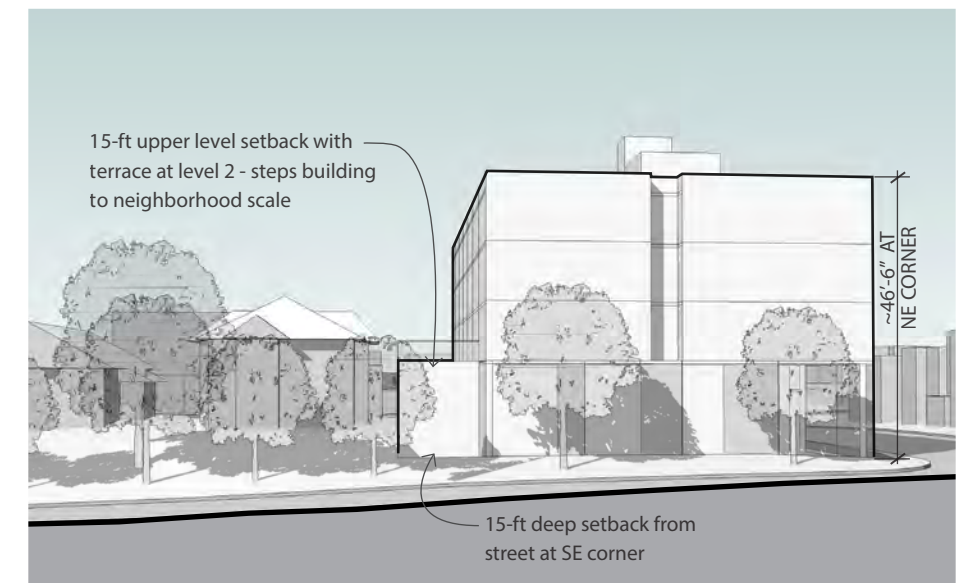
Northwest Corner (NE 65th St)



Street Level Plan (Level 1)



Typical Residential Plan (Level 2)



East Elevation (14th Avenue NE)

DESIGN PROPOSAL: ALTERNATE 3 (PREFERRED)

The street level of Design Alternate 3 is oriented towards NE 65th St, the primary pedestrian street and commercial spine. A commercial space occupies the eastern half of the facade, and wraps the corner at 14th Ave. The multifamily entry is located on the western corner of the facade for commuter proximity to the Light Rail station. The entry is flanked by an open lobby and residential amenity space (potential business center), both of which are glazed with commercial storefront and will provide eyes on the street. The street level facade on NE 65th is set back 4-ft from the property line to widen the pedestrian ROW. The street level on 14th Ave NE is responsive to the NC to SF zone transition: an activated northern commercial corner transitions south to a quiet landscaped facade consistent with the planted rockeries and retaining walls of the neighboring lots. Residential parking access is off the alley.

As in previous alternates, a small basement houses building services and residential storage units.

The three residential floors above are organized around a double-loaded corridor and set back 15-ft from the southern property line. This setback at the podium creates private terraces for the residential units, and the stepped massing responds to the neighboring southern residential zone.

Primary Advantages/Benefits

- A 4-ft setback at street level on NE 65th enhances the pedestrian experience: providing continuous weather protection, a wider sidewalk, and street trees in a curbside planter.
- The massing (setbacks on the south) responds to the zone edge condition from NC to SF.
- Street level programming reflects the zone edge, with activated uses focused on NE 65th to preserve the privacy of 14th Ave NE neighbors.

Summary

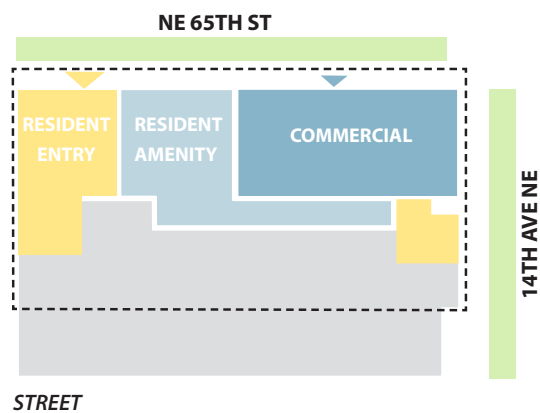
Stories: 4 (3-over-1 + 1 below grade)
 Unit Count: 41 (5,826 unit SF/ Floor)
 Parking: 8 stalls (residential)
 Floor Area: 20,301-SF Residential
 1,440-SF Commercial
 3,093-SF Parking
 29,627-SF Total

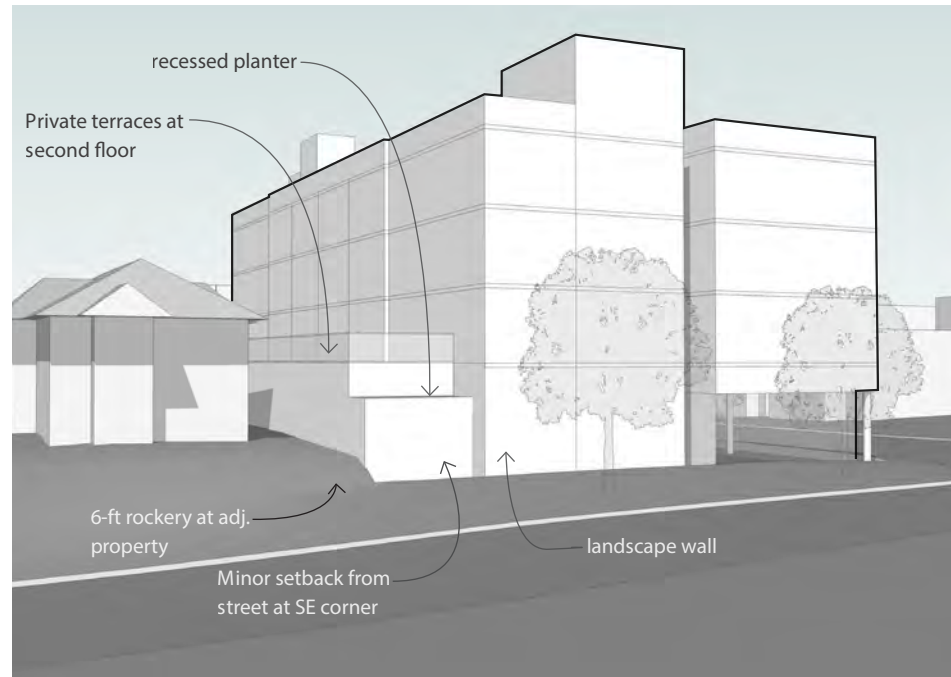
Ground Floor Uses:

- NE 65th St: Commercial / Residential Lobby
- 14th Ave NE: Commercial (at corner) / secondary residential bike entry
- Alley: Parking Access

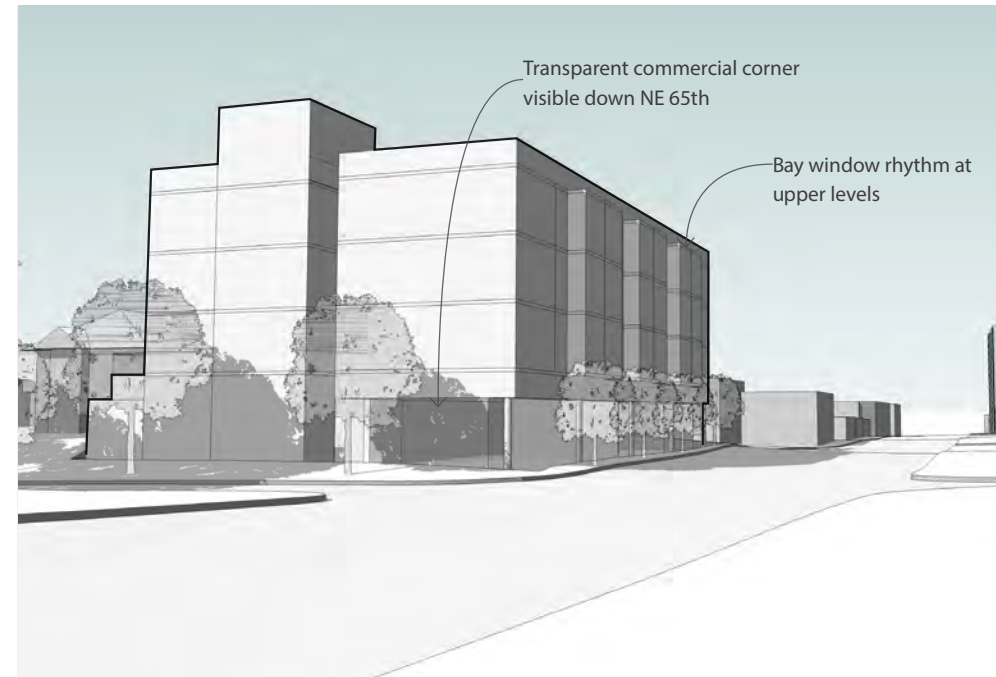
Potential Departures

- Increased percentage of residential use at street level in NC Zones (NE 65th)
- Reduced street level transparency on 14th Ave NE
- Reduced width of overhead weather protection on principal pedestrian streets (< 6-ft)
- Reduced non-residential use depth at street level
- Structure in triangular setback abutting a residential lot
- Garage door opening closer than 5-ft to a residential lot
- Reduced parking lot aisle width





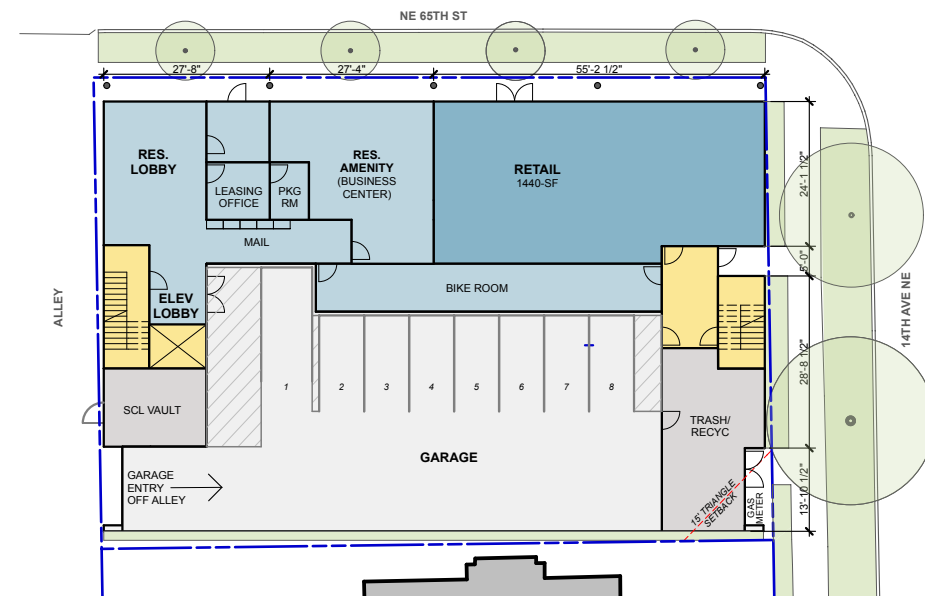
Southeast Corner (14th Ave NE)



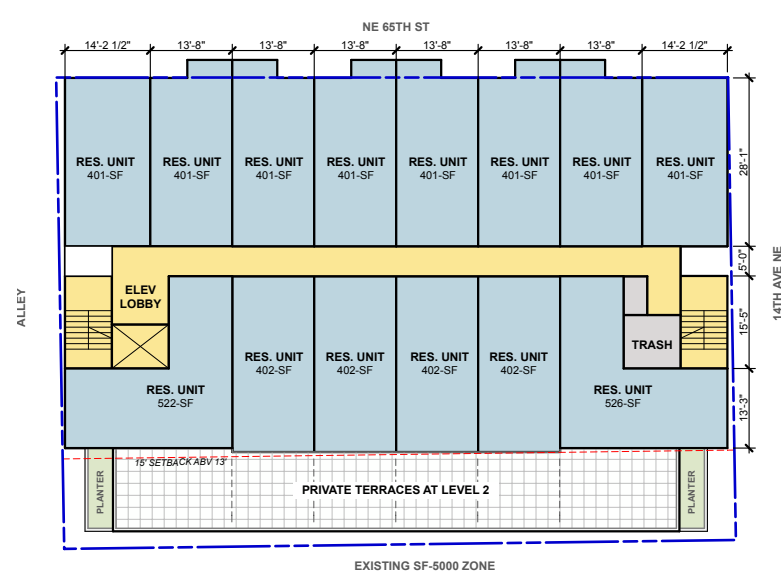
Northeast Corner (Corner of NE 65th St and 14th Ave NE)



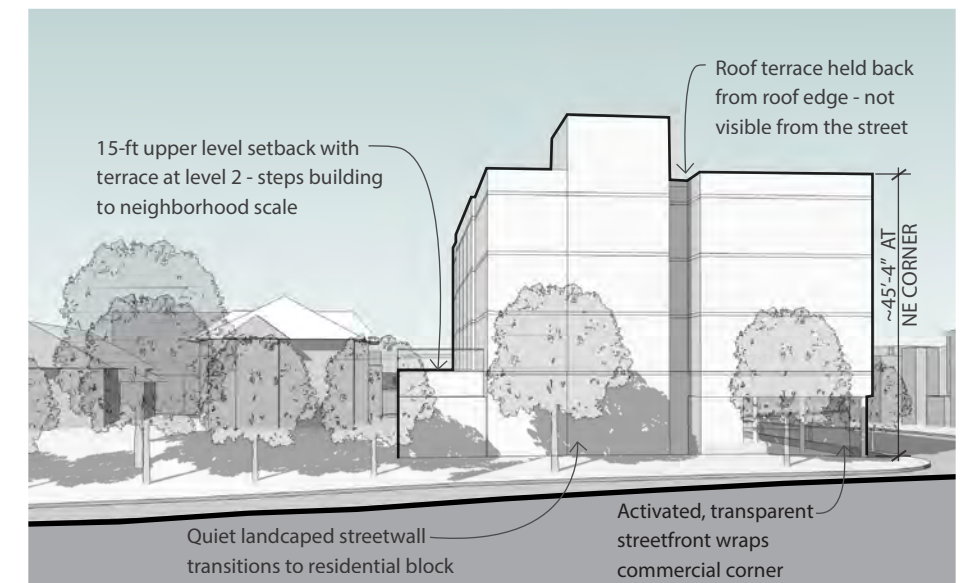
Northwest Corner (NE 65th St)



Street Level Plan (Level 1)



Typical Residential Plan (Level 2)



East Elevation (14th Avenue NE)

PREFERRED ALTERNATIVE SHADOW STUDY

10:00 AM



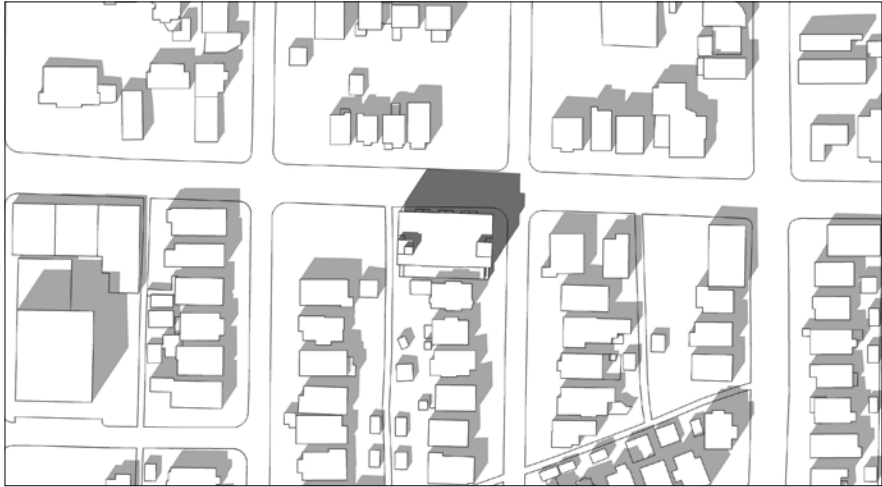
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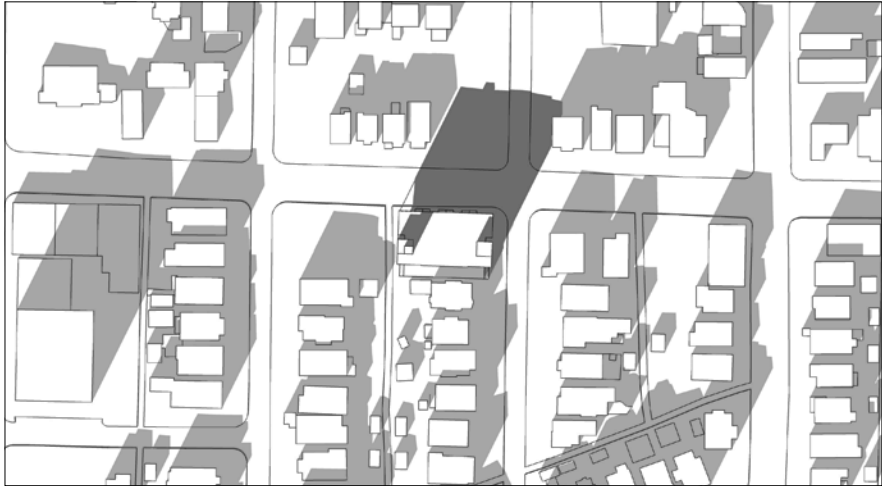
2:00 PM



March/ September 21



December 21





ALTERNATE 1 (L-SHAPE)

Summary

Stories: 4 (3-over-1 + 1 below grade)
 Unit Count: 41 (6,160 unit SF/ Floor)
 Floor Area: 30,834-SF Total
 Parking: 7 stalls (residential)

Ground Floor Uses:

- NE 65th St: Commercial / Residential Amenity
- 14th Ave NE: Residential Lobby & Amenity
- Alley: Parking Access

Potential Departures:

- Increased percentage of residential use at street level in NC Zones (NE 65th & 14th Ave NE)
- Reduced width of overhead weather protection on principal pedestrian streets (< 6-ft)
- Reduced non-residential use depth at street level
- Structure in triangular setback abutting a residential lot
- Structure in south PL setback above 13-ft
- Reduced parking lot aisle width

Pros

- Setback façade at street level widens pedestrian zone on NE 65th st.
- No curb cuts required - parking entry is provided off alley, separating cars and pedestrians.
- L-shape configuration at upper levels maximizes unit sizes.

Cons

- Lack of setback or step-down of massing at south property line imposes a greater visual impact on neighboring residential property.
- The 2nd floor terrace residential amenity will be overshadowed in the morning by north-south tower
- Location of multifamily entrance on 14th will draw more activity and noise down 14th, which may not be desired by residents of the quiet street.
- L-shape Configuration will require a number of Departures.



ALTERNATE 2 (NO DEPARTURES)

Summary

Stories: 4 (3-over-1 + 1 below grade)
 Unit Count: 40 studios (5,401 unit SF/ Floor)
 Floor Area: 29, 389-SF Total
 Parking: 7 stalls (residential)

Ground Floor Uses:

- NE 65th St: Commercial / Residential Lobby
- 14th Ave NE: Live-Work Units
- Alley: Parking Access

Potential Departures:

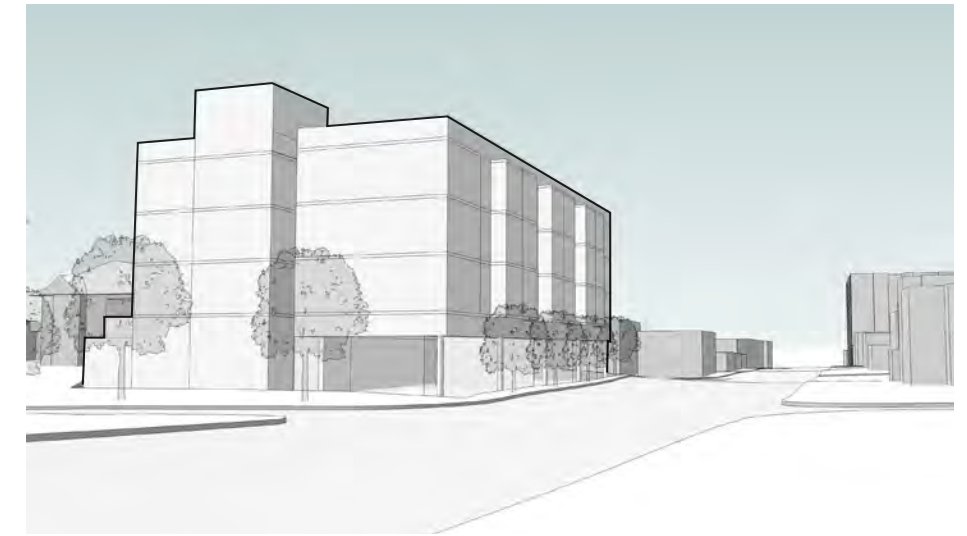
None

Pros

- No curb cuts required - parking entry is provided off alley, separating cars and pedestrians.
- Upper level setbacks at south property line create a gradual transition to the adjacent low-height residential zone.
- No Departures required.

Cons

- Without a setback façade at street level, the narrow 7'-6" pedestrian right-of-way cannot be widened, and there is no space to add street trees in the narrow curb planting strip.
- 14th Ave NE is a quiet single-family residential street. Live-work units that draw noise and activity to the street may not be a good fit for the neighbors.
- Respect of the 15-ft triangular side yard setback creates a dark corner at the back of the site, with little solar access due to neighboring property's raised rockery, and little visibility to passersby.



ALTERNATE 3 (PREFERRED)

Summary

Stories: 4 (3-over-1 + 1 below grade)
 Unit Count: 41 studios (5,864 unit SF/ Floor)
 Floor Area: 29,627 SF Total
 Parking: 8 stalls (residential)

Ground Floor Uses:

- NE 65th St: Commercial / Residential Lobby
- 14th Ave NE: Commercial (at corner) / secondary residential bike entry
- Alley: Parking Access

Potential Departures:

- Increased percentage of residential use at street level in NC Zones (NE 65th)
- Reduced street level transparency on 14th Ave NE
- Reduced width of overhead weather protection on principal pedestrian streets (< 6-ft)
- Reduced non-residential use depth at street level
- Structure in triangular setback abutting a residential lot
- Garage door opening closer than 5-ft to a residential lot
- Reduced parking lot aisle width

Pros

- Street level setback on NE 65th widens pedestrian zone and creates space for street trees
- No curb cuts required - parking entry is provided off alley, separating cars and pedestrians.
- Location of residential entry at the west end of NE 65th minimize distance to the new Light Rail Station for commuter residents.
- Location of all major building entrances on NE 65th increases their visibility to the public, and preserves 14th Ave NE as a quiet, private residential street.
- Upper level setbacks at south property line create a gradual transition to the adjacent low-height residential zone.
- A shallow setback in plan at the south-east corner respects the intent of the Code's 15-ft triangular setback to respond to residential neighbors, but does not create a deep enough space to invite unwanted activity, as Alternate 2 might.

Cons

- Departures required at street level for setbacks, street level uses, transparency, and parking

POTENTIAL DEVELOPMENT DEPARTURES

The proposed project will be a four-story, apartment building approximately 44-feet in height and approximately 29,627-sf, including the below grade level. The building will contain 41 residential apartment units, resident lobby and amenity spaces, an outdoor landscaped terrace and roof deck, on-site leasing office, street level commercial space, street-level structured resident parking for 8 vehicles, and a below-grade level for building services and resident storage units.

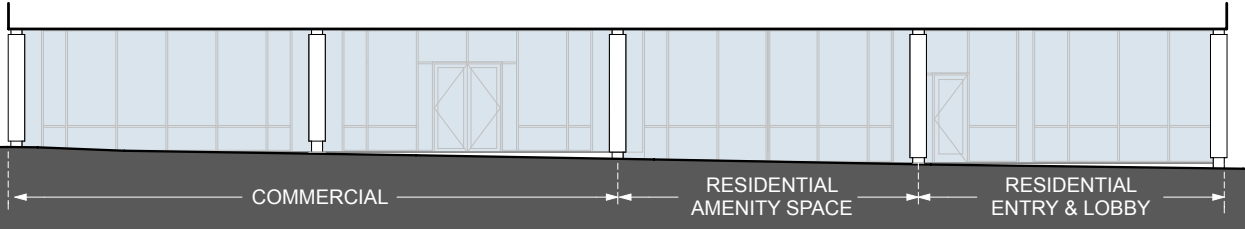
The project development objectives are as follows:

- Provide a high quality living environment for residents convenient to work, leisure and recreation
- Provide a lively environment for urban residents, including accommodations for retail and/or restaurants
- Set precedence as the first new development along the south side of NE 65th by setting up the pedestrian-oriented design decisions to benefit the neighborhood:
 - Widen sidewalks
 - Strengthen the street edge with an appropriately scaled facade
 - Create vibrant and transparent street level facades
 - Plant street trees to buffer traffic
 - Offer overhead weather protection
- Be sensitive to the site's location in a transition zone from neighborhood commercial to single-family residential. This will inform building massing, scale, street level programming and building access.
- Be a good neighbor. This will inform the project in terms of:
 - Commercial level streetscape design
 - Landscape design & material selection
 - Exterior lighting design
 - Parking access
 - Trash & recycling storage

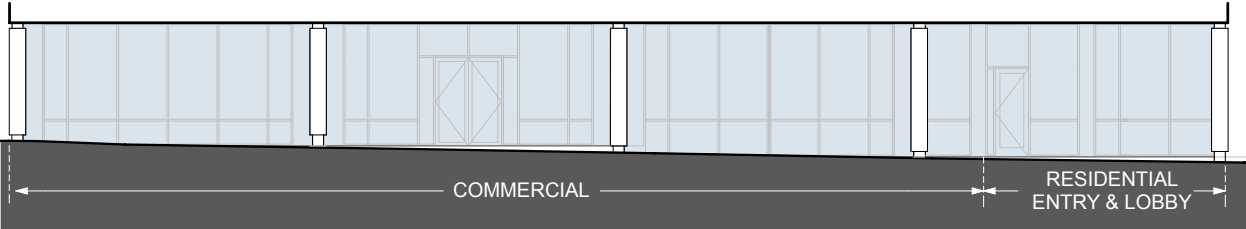
The following Development Departures have been identified as potentially necessary to achieve the preferred design alternate. If the board indicates their willingness to consider the departures, additional study and refinement of the design will be conducted in advance of the Design Recommendation meeting.

DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
<p>1a 23.47A.005.C: Residential Uses at Street Level Residential uses may occupy, in the aggregate, no more than 20% of the street-level street-facing façade in the following circumstances or locations: e) Within NC1 zones.</p> <p>b 23.47A.005.D.1: Uses at Principal Pedestrian Streets Along designated principal pedestrian streets, one or more of the following is required along 80% of street-facing façade: general sales, retail, eating and drinking establishments.</p> <p>c 23.47A.008.C.1: Street Level Uses on Principal Pedestrian Streets In pedestrian-designated zones, a minimum of 80% of the width of a structure's street-level street-facing façade that faces a principal pedestrian street shall be occupied by uses listed in subsection 23.47A.005.D.1. The remaining 20% of the street frontage may contain other permitted uses and/or pedestrian entrances.</p>	<p>Departure Request #1 is for the allowance of a larger percentage of residential uses (and consequently smaller percentage of the required uses) at the NE 65th St façade. This percentage requirement is referred to in the code in 3 separate sections, which are referred to here as Departure Requests 1, 1a, and 1b. On the 110-ft long façade, the request is for 50% of residential use.</p>	<p><i>Neighborhood Conditions: As the first new construction project on this length of NE 65th, drawing the quantity or size of retailer to fill the code-prescribed 60% of the NE 65th facade will be difficult. If 60% of the facade is dedicated to commercial there is risk of the storefront remaining un-rented and empty.</i></p> <p><i>-The street-facing façade percentage requirements are intended to enhance and activate the pedestrian environment. While the proposed design does not meet the required percentages, the uses are intended to compliment the streetscape. 50% of the façade will be a commercial space, with large windows and high transparency, while the remaining 50% will be the residential lobby and business center resident amenity. With small residential units, the public spaces like the lobby and business center are expected to be highly-used gathering spaces. From the exterior, these residential amenity spaces will have large windows consistent with the commercial frontage.</i></p>
<p>2 23.47A.008.B.2a: Street Level Transparency 60% of the street-facing façade between 2-ft and 8-ft above the sidewalk shall be transparent.</p>	<p>Allow landscaping in lieu of transparency on the 14th Ave façade.</p>	<p><i>Neighborhood Conditions: 14th Ave NE is a quiet residential street. An activated, transparent facade across the length of the building would not be an appropriate fit for the community.</i></p> <p><i>- To respect the existing condition, and due to the constraints of fitting the building program and services on a small site, the preferred alternative located quieter building elements on the back end of the 14th Ave facade. Consequently these uses are not conducive to transparency and would be better served by landscaping than windows. This landscape approach is consistent with qualities of 14th Ave NE, which is characterized by heavy vegetation along the sidewalk.</i></p>
<p>3 23.47A.008.B.3: Non-Residential Use Depth Non-residential uses shall extend an average depth of at least 30-ft and a minimum depth of 15-ft from the street-level street-facing façade.</p>	<p>Allow a reduced depth of 26.5-ft for the commercial space.</p>	<p><i>Neighborhood Conditions: NE 65th St has a very narrow pedestrian ROW adjacent to a busy arterial. In its current state there is no space available for street trees.</i></p> <p><i>- A project goal is to enhance the pedestrian environment on NE 65th. To provide space for improvements, the streetfront on NE 65th is set back from the property line. Due to the building's narrow site, the depth of this setback is taken out of the commercial depth. The 4' setback creates a wider sidewalk, with space for street trees and planting to increase separation between pedestrians and traffic.</i></p>
<p>4 23.47A.008.C.4: Overhead Weather Protection Continuous overhead weather protection is required along at least 60% of the street frontage of a structure on a principal pedestrian street. The covered area shall have a min. width of 6-ft. The lower edge of the overhead weather protection shall be a minimum of 8-ft and a max of 12-ft above the sidewalk for projections extending a maximum of 6-ft.</p>	<p>Allow 4' deep weather protection between 10'-13' above grade along 60% of the NE 65th façade, in lieu of a 6' deep weather protection canopy between 8'-12' above grade along 60% of the NE 65th façade .</p>	<p><i>Neighborhood Conditions: NE 65th St has a very narrow pedestrian ROW adjacent to a busy arterial. In its current state there is no space available for street trees.</i></p> <p><i>- A project goal is to enhance the pedestrian environment on NE 65th. To provide space for improvements, the streetfront on NE 65th St is set back from the property line. The 4' setback creates weather protection for pedestrians across the entire length of the building, without interfering with the space required for street trees.</i></p>
<p>5 23.47A.014.B.1: 15' Triangular Setback at Residential Lots A setback is required where a lot abuts the intersection of a side lot line and a front lot line of a lot in a residential zone. The required setback forms a triangular area 15-ft on a side.</p>	<p>Allow structure within the 15-ft triangular setback.</p>	<p><i>Neighborhood Conditions: The site's south property line abuts the side lot line of a residential zone. The neighboring residential property is raised on a 6-ft rockery.</i></p> <p><i>-We understand the intent of the 15-ft triangular setback is to mediate the NC zone transition to a residential scale. An undeveloped edge of the size in compliance with the setback will create a deep corner with little visibility to passersby, and may invite undesired activity. The neighboring southern property's 6-ft rockery would overshadow this corner and exacerbate this condition. The proposal suggests minimal construction (single-story) within the setback zone, with a minor plan setback to respect the neighboring single-family, be considered as an alternative to the full setback depth and height. Change in topography at the southern property line limits the vertical encroachment into the setback to 9.5-ft above the adjacent property grade.</i></p>
<p>6 23.47A.014.B.5: Openings closer than 5' to Residential Lot no opening is permitted closer than 5-ft to an abutting residentially-zoned lot.</p>	<p>Allow a garage door opening 4-ft off the property line at the alley.</p>	<p><i>While not required to allow the preferred scheme to work, a wider garage door opening will increase the view angle of drivers exiting the garage, increasing safety for pedestrians in the alley.</i></p>
<p>7 23.54.030.E: Parking Aisle Width Parking Aisles: two way-traffic requires an aisle width of 24-ft or greater. (based on van stall)</p>	<p>Allow a narrower drive aisle (20-feet) to serve the aisle containing the ADA van space.</p>	<p><i>The ADA van space is located to provide convenient access to the building lobby. It is located nearest the garage entry of the street level garage, and has a compliant 24-ft backup depth behind the space. Beyond this stall the aisle width is narrowed to 20-ft, which is the compliant dimension for the remaining 7 small depth parking stalls in garage. The small site constrains the garage from providing a consistent aisle width across the parking aisle. If a consistent aisle width was required, the secure bike parking room would be removed, a feature of the building encouraging bicyclists and aligned with Design Guideline PL4 (Active Transportation).</i></p>

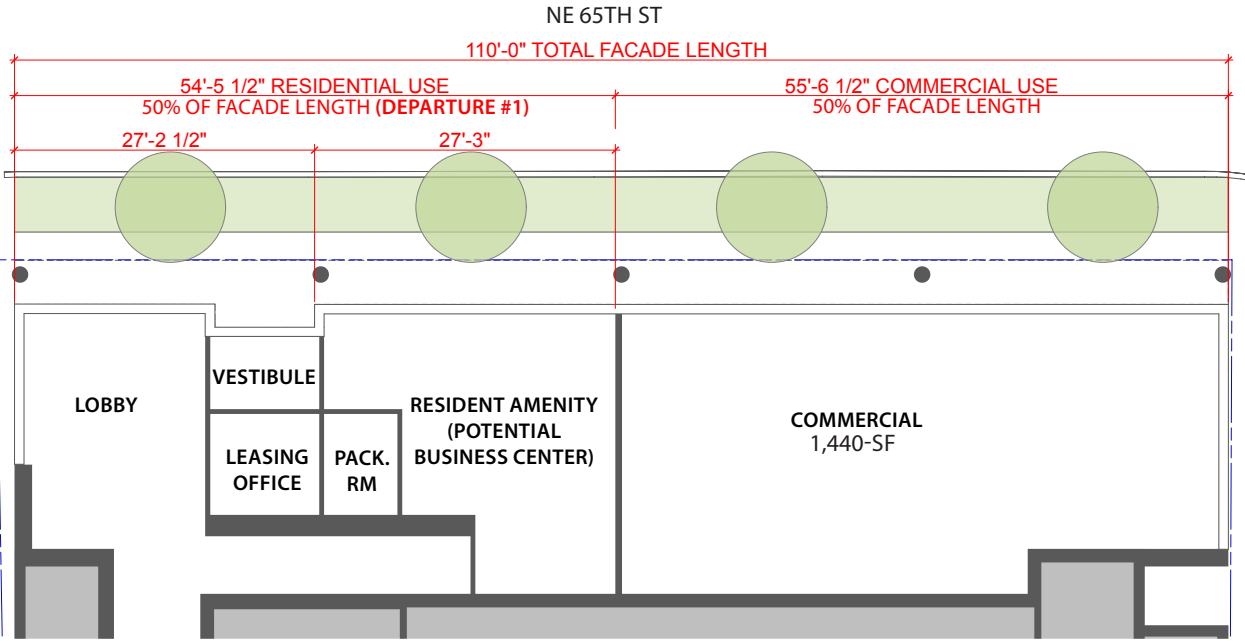
DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE	DESIGN GUIDELINES
<p>1a 23.47A.005.C: Residential Uses at Street Level Residential uses may occupy, in the aggregate, no more than 20% of the street-level street-facing façade in the following circumstances or locations: e) Within NC1 zones.</p> <p>b 23.47A.005.D.1: Uses at Principal Pedestrian Streets Along designated principal pedestrian streets, one or more of the following is required along 80% of street-facing façade: general sales, retail, eating and drinking establishments.</p> <p>c 23.47A.008.C.1: Street Level Uses on Principal Pedestrian Streets In pedestrian-designated zones, a minimum of 80% of the width of a structure's street-level street-facing façade that faces a principal pedestrian street shall be occupied by uses listed in subsection 23.47A.005.D.1. The remaining 20% of the street frontage may contain other permitted uses and/or pedestrian entrances.</p>	<p>Departure Request #1 is for the allowance of a larger percentage of residential uses (and consequently smaller percentage of the required uses) at the NE 65th St façade. This percentage requirement is referred to in the code in 3 separate sections, which are referred to here as Departure Requests 1, 1a, and 1b. On the 110-ft long façade, the request is for 50% of residential use.</p>	<p><i>Neighborhood Conditions: As the first new construction project on this length of NE 65th, drawing the quantity or size of retailer to fill the code-prescribed 60% of the NE 65th facade will be difficult. If 60% of the facade is dedicated to commercial there is risk of the storefront remaining un-rented and empty.</i></p> <p><i>-The street-facing façade percentage requirements are intended to enhance and activate the pedestrian environment. While the proposed design does not meet the required percentages, the uses are intended to compliment the streetscape. 50% of the façade will be a commercial space, with large windows and high transparency, while the remaining 50% will be the residential lobby and business center resident amenity. With small residential units, the public spaces like the lobby and business center are expected to be highly-used gathering spaces. From the exterior, these residential amenity spaces will have large windows consistent with the commercial frontage.</i></p>	<p>PL2 WALKABILITY (Safety and Security: Eyes on the Street, Street Level Transparency) ...“create a safe environment by providing lines of sight and encouraging natural surveillance”...“ensure transparency of street-level uses (for uses such as nonresidential uses or residential lobbies”</p> <p>PL3 STREET-LEVEL INTERACTION (Retail Edges: Visibility) ...“maximize visibility into building interiors”</p>



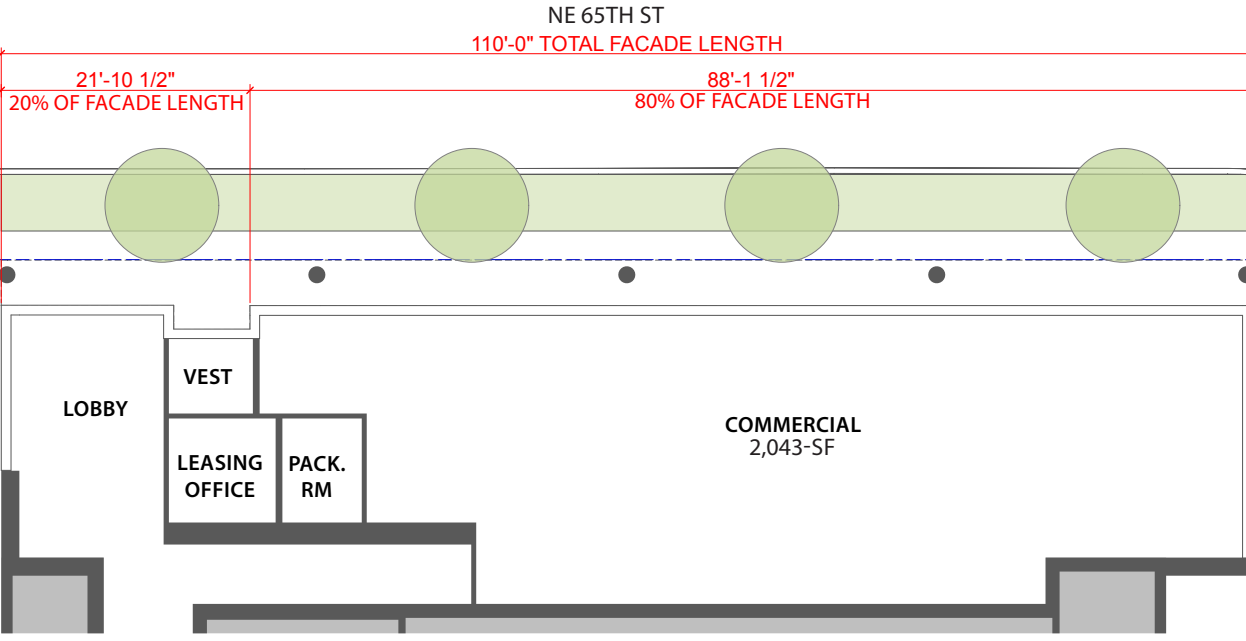
PREFERRED FIRST FLOOR NE 65TH ST ELEVATION



COMPLIANT FIRST FLOOR NE 65TH ST ELEVATION



PREFERRED FIRST FLOOR PLAN



COMPLIANT FIRST FLOOR PLAN

DEPARTURE REQUEST DIAGRAMS

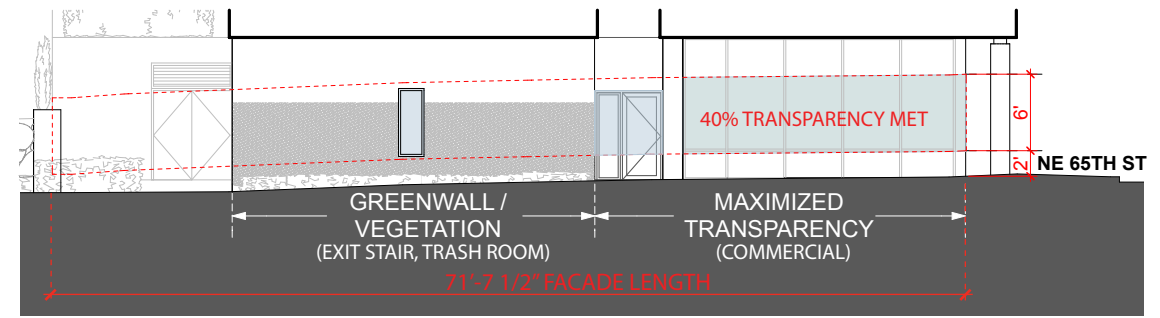
DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE	DESIGN GUIDELINES
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2 23.47A.008.B.2a: Street Level Transparency
60% of the street-facing façade between 2-ft and 8-ft above the sidewalk shall be transparent.

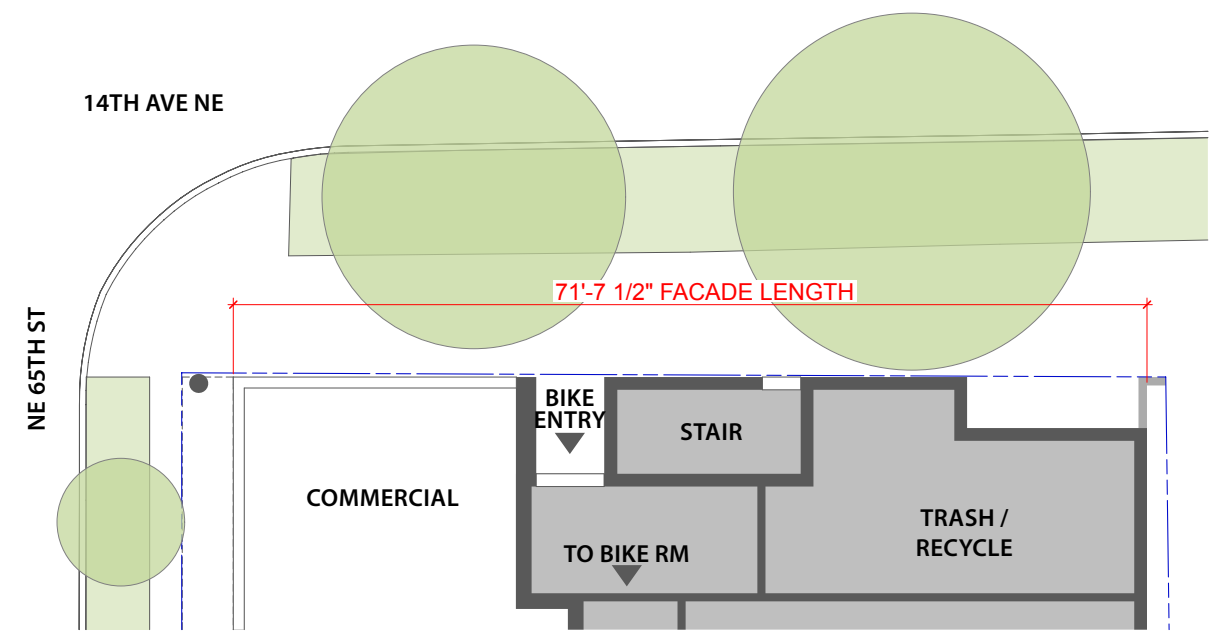
Allow landscaping in lieu of transparency on the 14th Ave façade.

Neighborhood Conditions: 14th Ave NE is a quiet residential street. An activated, transparent facade across the length of the building would not be an appropriate fit for the community. - To respect the existing condition, and due to the constraints of fitting the building program and services on a small site, the preferred alternative located quieter building elements on the back end of the 14th Ave facade. Consequently these uses are not conducive to transparency and would be better served by landscaping than windows. This landscape approach is consistent with qualities of 14th Ave NE, which is characterized by heavy vegetation along the sidewalk.

DC2 ARCHITECTURAL CONCEPT (Secondary Architectural Features: Fit with Neighboring Buildings, Architectural and Facade Composition: Blank Walls) ...“avoid large blank walls along visible facades where possible, include uses at street level that are designed for pedestrians... green walls, landscaped areas or raised planters...” “use design elements to achieve a successful fit between a building and its neighbors...” “using trees and or landscaping to enhance the building design and fit with the surrounding context”

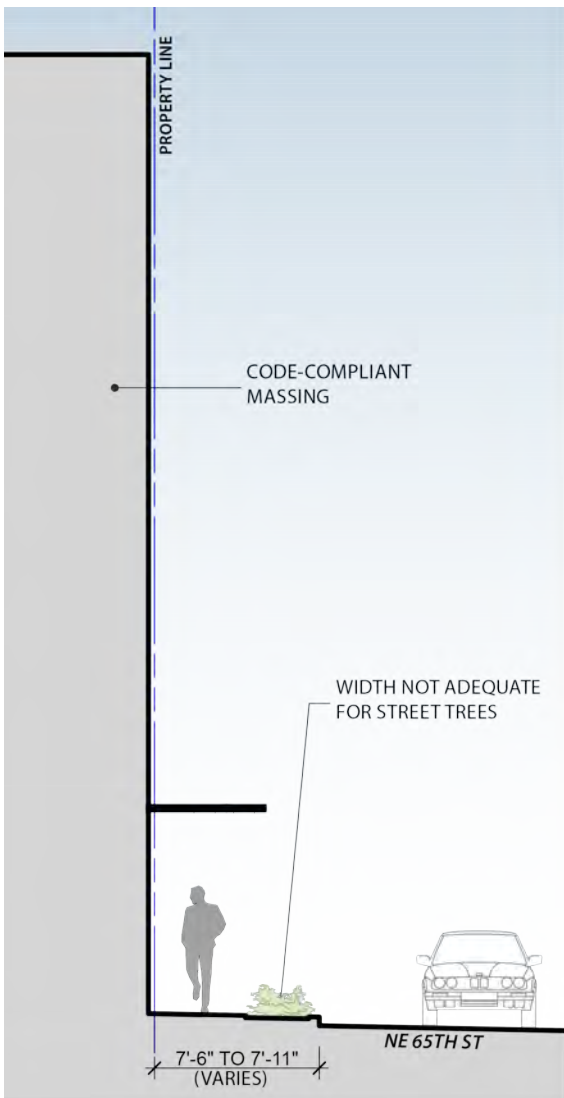


PREFERRED FIRST FLOOR 14TH AVE NE ELEVATION

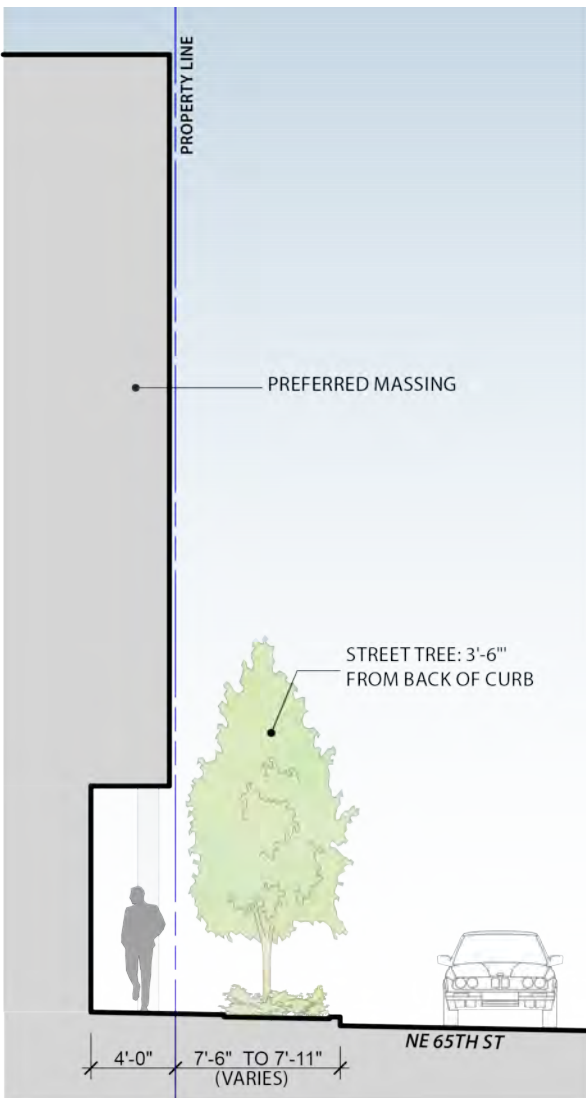


PREFERRED FIRST FLOOR PLAN

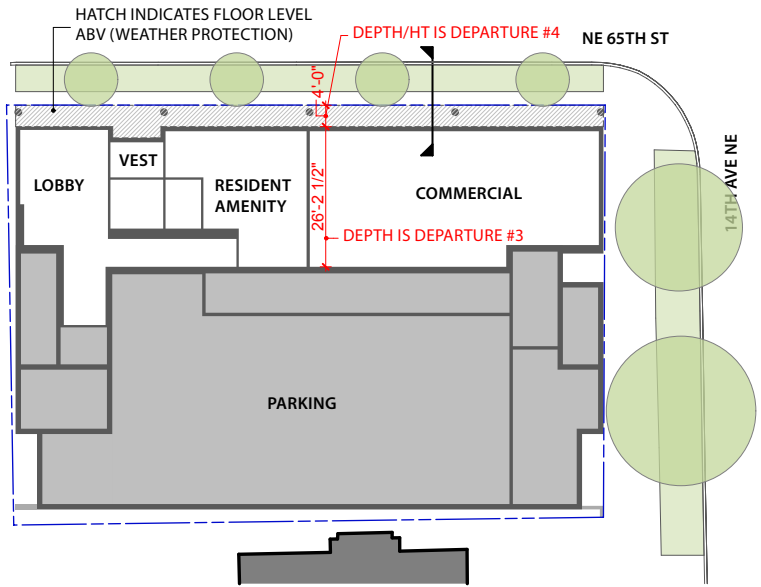
DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE	DESIGN GUIDELINES
<p>3 23.47A.008.B.3: Non-Residential Use Depth Non-residential uses shall extend an average depth of at least 30-ft and a minimum depth of 15-ft from the street-level street-facing façade.</p>	<p>Allow a reduced depth of 26.5-ft for the commercial space.</p>	<p><i>Neighborhood Conditions: NE 65th St has a very narrow pedestrian ROW adjacent to a busy arterial. In its current state there is no space available for street trees.</i> - A project goal is to enhance the pedestrian environment on NE 65th. To provide space for improvements, the streetfront on NE 65th is set back from the property line. Due to the building's narrow site, the depth of this setback is taken out of the commercial depth. The 4' setback creates a wider sidewalk, with space for street trees and planting to increase separation between pedestrians and traffic.</p>	<p>PL1 CONNECTIVITY (Walkways and Connections: Pedestrian Volumes) ...“provide ample space for pedestrian flow and circulation”... PL2 WALKABILITY (Weather Protection: Locations and Coverage) ...“overhead weather protection encouraged... provide continuous coverage the length of the building, where possible” Roosevelt I Supplemental Guidance (Human Activity along sidewalks) ...“encourage pedestrian activity along sidewalks... applicants encouraged to increase ground level setback in order to accomodate pedestrian traffic”</p>
<p>4 23.47A.008.C.4: Overhead Weather Protection Continuous overhead weather protection is required along at least 60% of the street frontage of a structure on a principal pedestrian street. The covered area shall have a min. width of 6-ft. The lower edge of the overhead weather protection shall be a minimum of 8-ft and a max of 12-ft above the sidewalk for projections extending a maximum of 6-ft.</p>	<p>Allow 4' deep weather protection between 10'-13' above grade along 60% of the NE 65th façade, in lieu of a 6' deep weather protection canopy between 8'-12' above grade along 60% of the NE 65th façade .</p>	<p><i>Neighborhood Conditions: NE 65th St has a very narrow pedestrian ROW adjacent to a busy arterial. In its current state there is no space available for street trees.</i> - A project goal is to enhance the pedestrian environment on NE 65th. To provide space for improvements, the streetfront on NE 65th St is set back from the property line. The 4' setback creates weather protection for pedestrians across the entire length of the building, without interfering with the space required for street trees.</p>	



COMPLIANT STREETWALL SECTION
 - 30' deep commercial space with construction to property line
 - Intermitent 6' deep weather protection overhangs at 60% of facade length



PREFERRED STREETWALL SECTION
 - 4' setback of street level facade from property line
 - Continuous 4' deep weather protection at 100% of facade length



PREFERRED FIRST FLOOR PLAN
 (showing setback depth subtracted from commercial depth)

DEPARTURE REQUEST DIAGRAMS

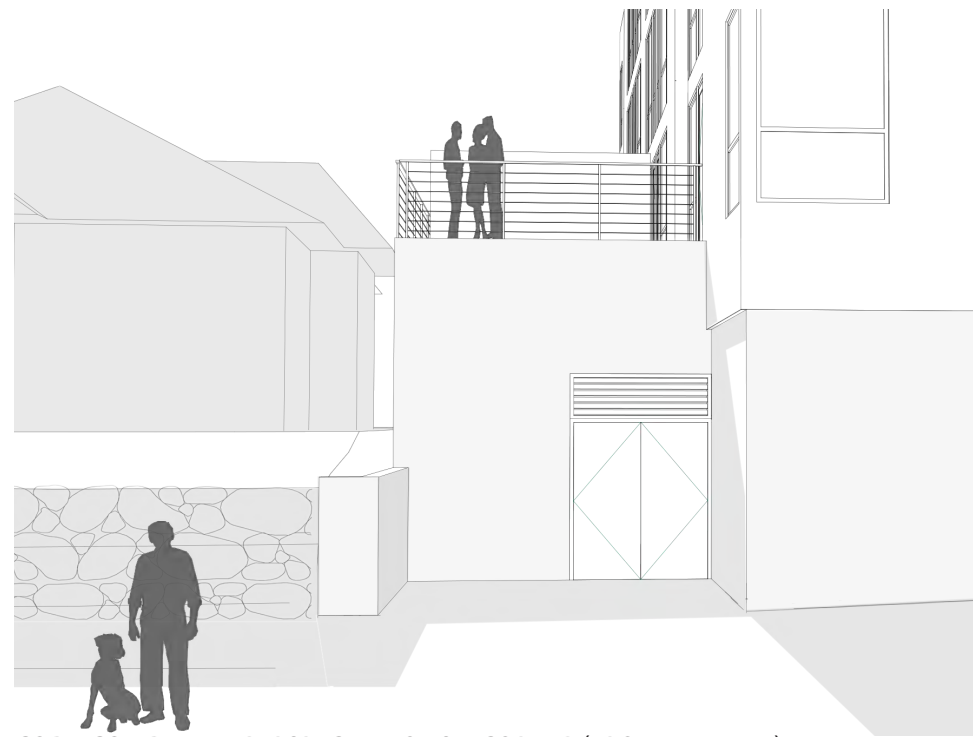
DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE	DESIGN GUIDELINES
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5 23.47A.014.B.1: 15' Triangular Setback at Residential Lots
 A setback is required where a lot abuts the intersection of a side lot line and a front lot line of a lot in a residential zone. The required setback forms a triangular area 15-ft on a side.

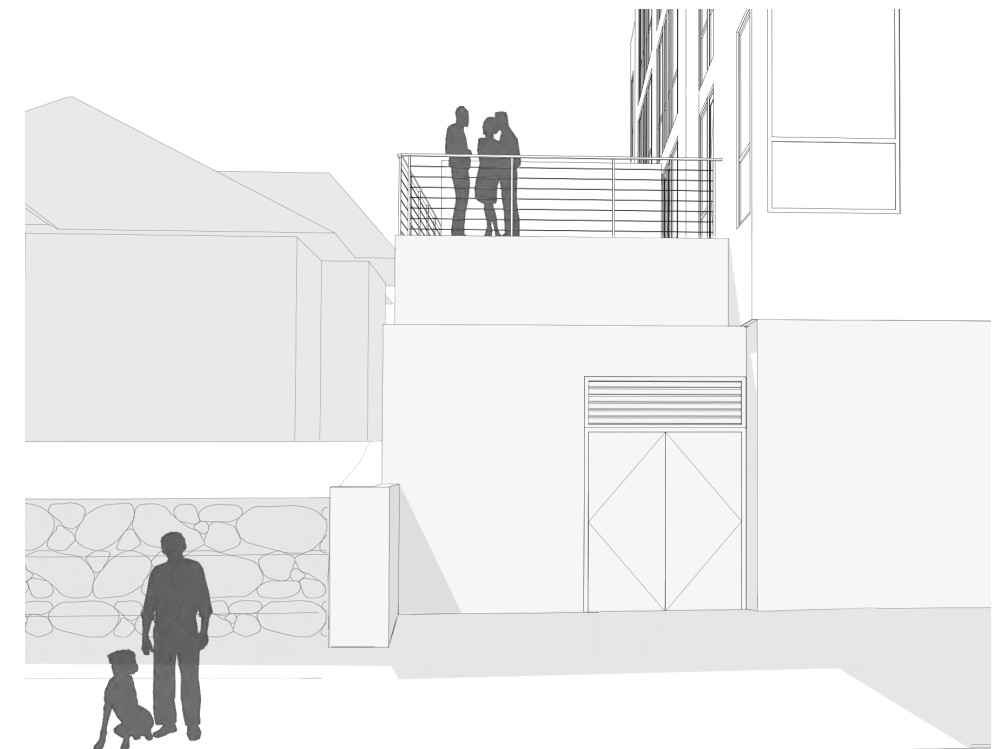
Allow structure within the 15-ft triangular setback.

Neighborhood Conditions: The site's south property line abuts the side lot line of a residential zone. The neighboring residential property is raised on a 6-ft rockery. -We understand the intent of the 15-ft triangular setback is to mediate the NC zone transition to a residential scale. An undeveloped edge of the size in compliance with the setback will create a deep corner with little visibility to passersby, and may invite undesired activity. The neighboring southern property's 6-ft rockery would overshadow this corner and exacerbate this condition. The proposal suggests minimal construction (single-story) within the setback zone, with a minor plan setback to respect the neighboring single-family, be considered as an alternative to the full setback depth and height. Change in topography at the southern property line limits the vertical encroachment into the setback to 9.5-ft above the adjacent property grade.

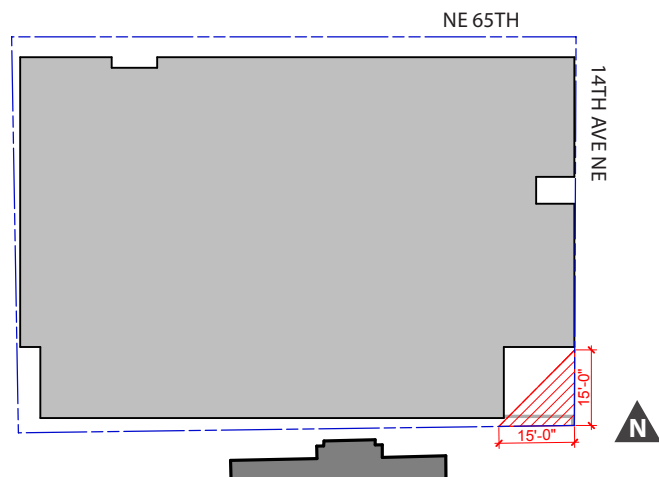
CS2 URBAN PATTERN AND FORM (Height Bulk and Scale, Respect for Adjacent Sites) ...“review the height, bulk and scale of neighboring buildings... scale of development anticipated.. determine an appropriate complement/transition”...“respect adjacent properties to minimize disturbing the privacy of adjacent buildings”...
Roosevelt III Supplemental Guidance (Height, Bulk and Scale: Zone Edge Condition One: rear lot line of commercial abuts side of residential zone) ...“step back the upper floors”



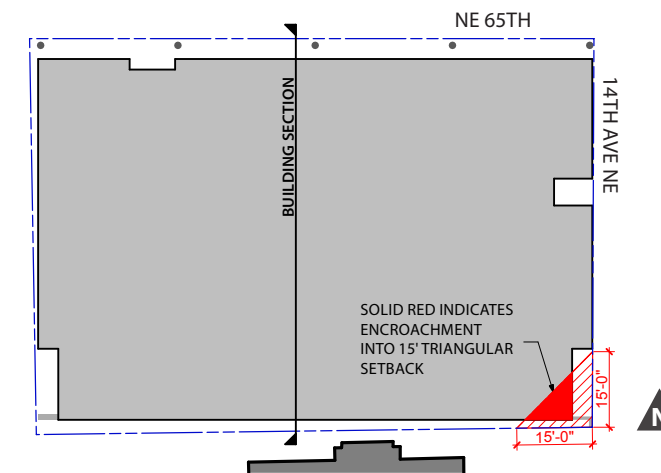
CODE COMPLIANT PERSPECTIVE OF S-E CORNER (FROM 14TH AVE)



PREFERRED PERSPECTIVE OF S-E CORNER (FROM 14TH AVE)



CODE COMPLIANT FIRST FLOOR PLAN

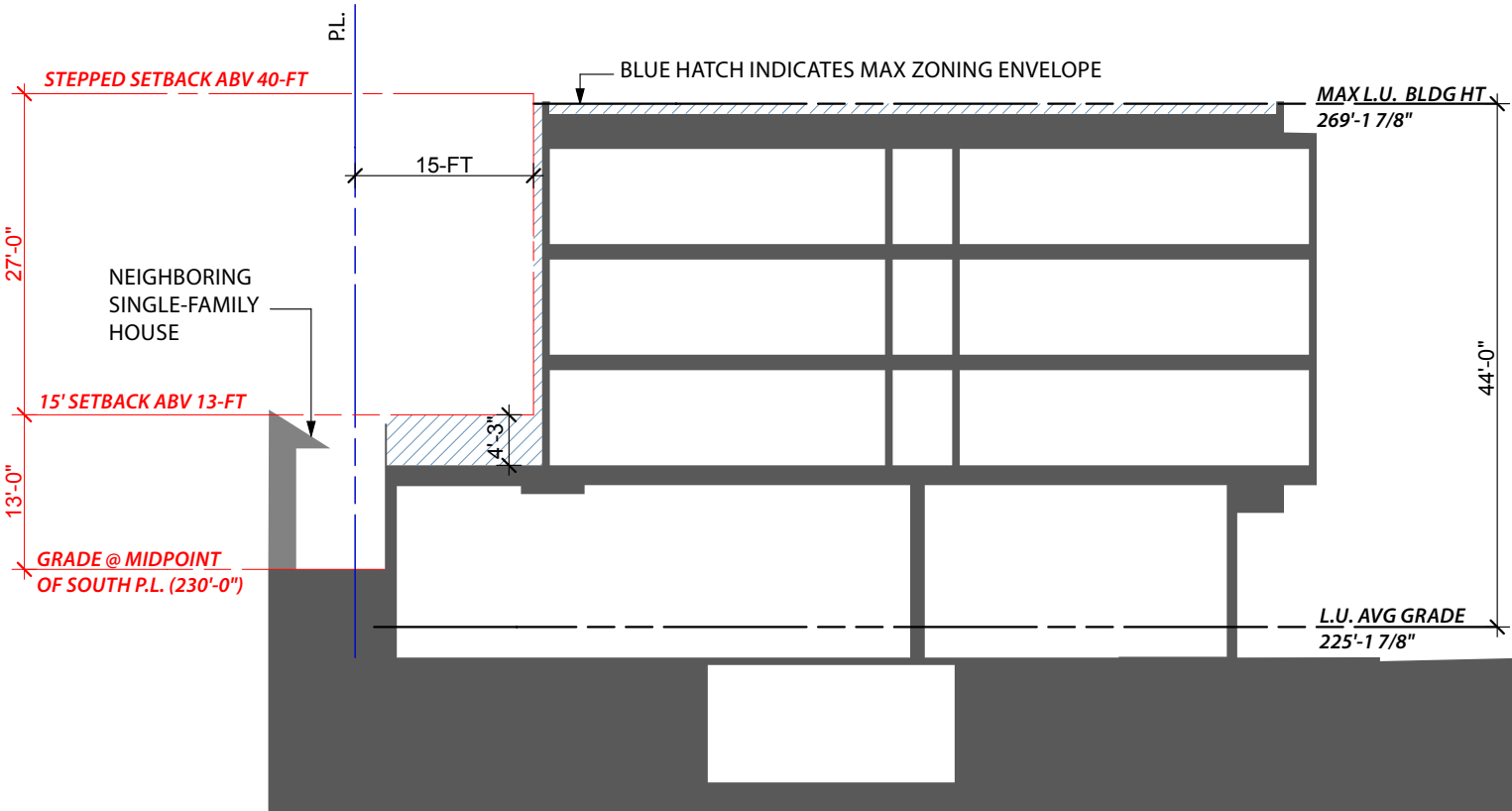


PREFERRED FIRST FLOOR PLAN

DEPARTURE REQUEST DIAGRAMS



PREFERRED PERSPECTIVE OF S-E CORNER (FROM 14TH AVE) SHOWING LANDSCAPING

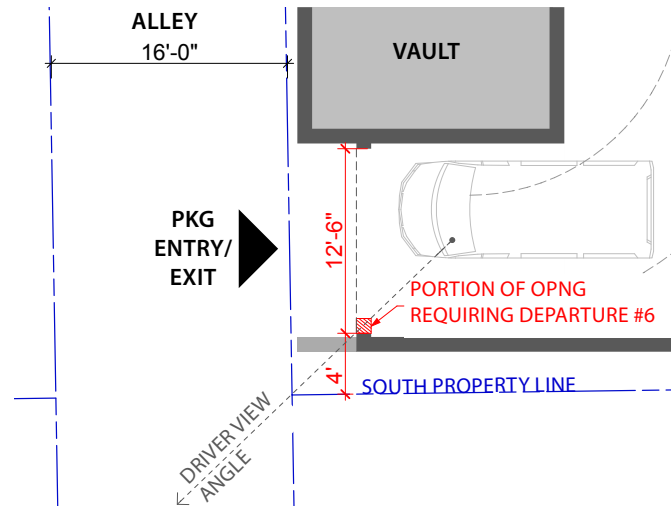


PREFERRED: BUILDING SECTION SHOWING MAX ZONING ENVELOPE

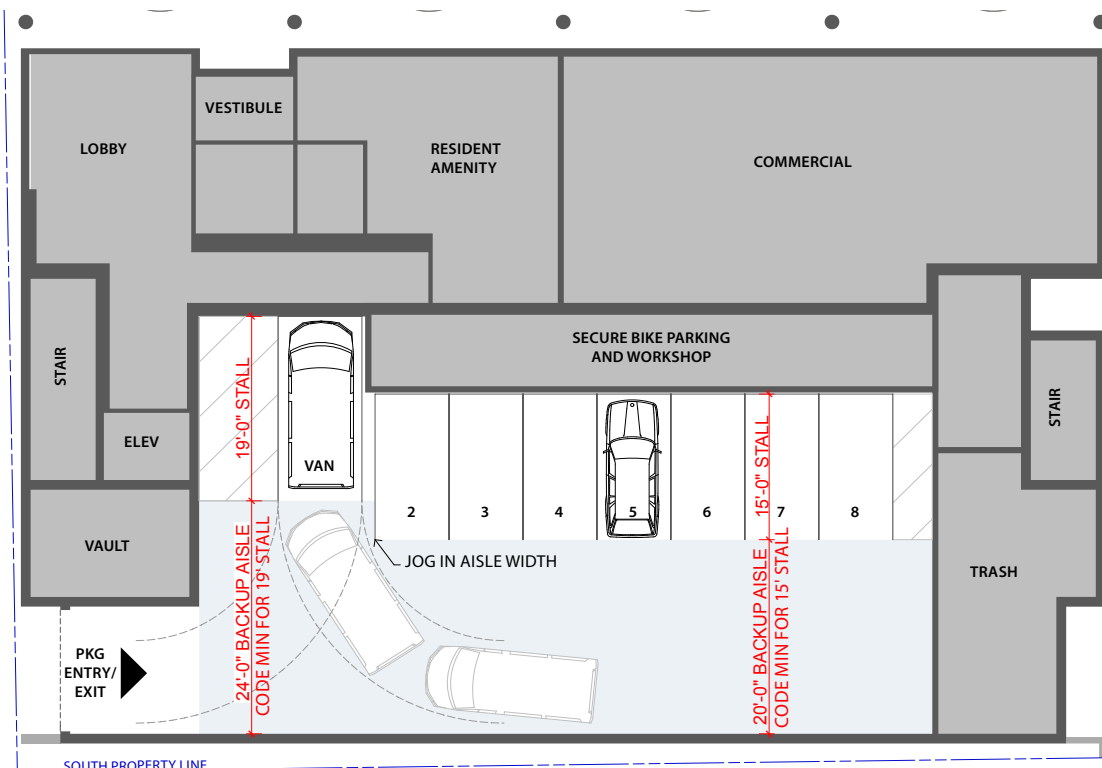
DEPARTURE REQUEST #5: SECTION 23.47A.014.B.1: REQUEST TO BUILD IN 15' TRIANGULAR SETBACK AT SF-5000 ZONE EDGE

DEPARTURE REQUEST DIAGRAMS

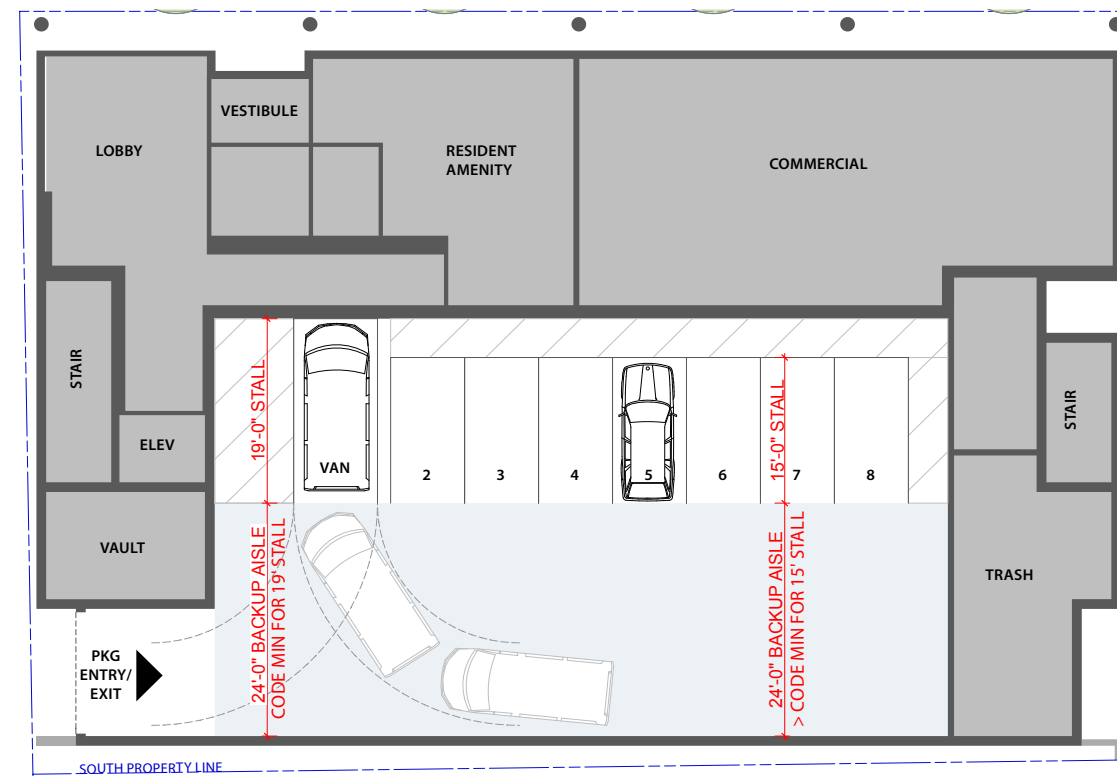
DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE
<p>6 23.47A.014.B.5: Openings closer than 5' to Residential Lot no opening is permitted closer than 5-ft to an abutting residentially-zoned lot.</p>	<p>Allow a garage door opening 4-ft off the property line at the alley.</p>	<p>While not required to allow the preferred scheme to work, a wider garage door opening will increase the view angle of drivers exiting the garage, increasing safety for pedestrians in the alley.</p>
<p>7 23.54.030.E: Parking Aisle Width Parking Aisles: two way-traffic requires an aisle width of 24-ft or greater. (based on van stall)</p>	<p>Allow a narrower drive aisle (20-feet) to serve the aisle containing the ADA van space.</p>	<p>The ADA van space is located to provide convenient access to the building lobby. It is located nearest the garage entry of the street level garage, and has a compliant 24-ft backup depth behind the space. Beyond this stall the aisle width is narrowed to 20-ft, which is the compliant dimension for the remaining 7 small depth parking stalls in garage. The small site constrains the garage from providing a consistent aisle width across the parking aisle. If a consistent aisle width was required, the secure bike parking room would be removed, a feature of the building encouraging bicyclists and aligned with Design Guideline PL4 (Active Transportation).</p>



PARKING ENTRY/EXIT DEPARTURE #6



PARKING AISLE WIDTH: DEPARTURE #7 : PREFERRED PLAN

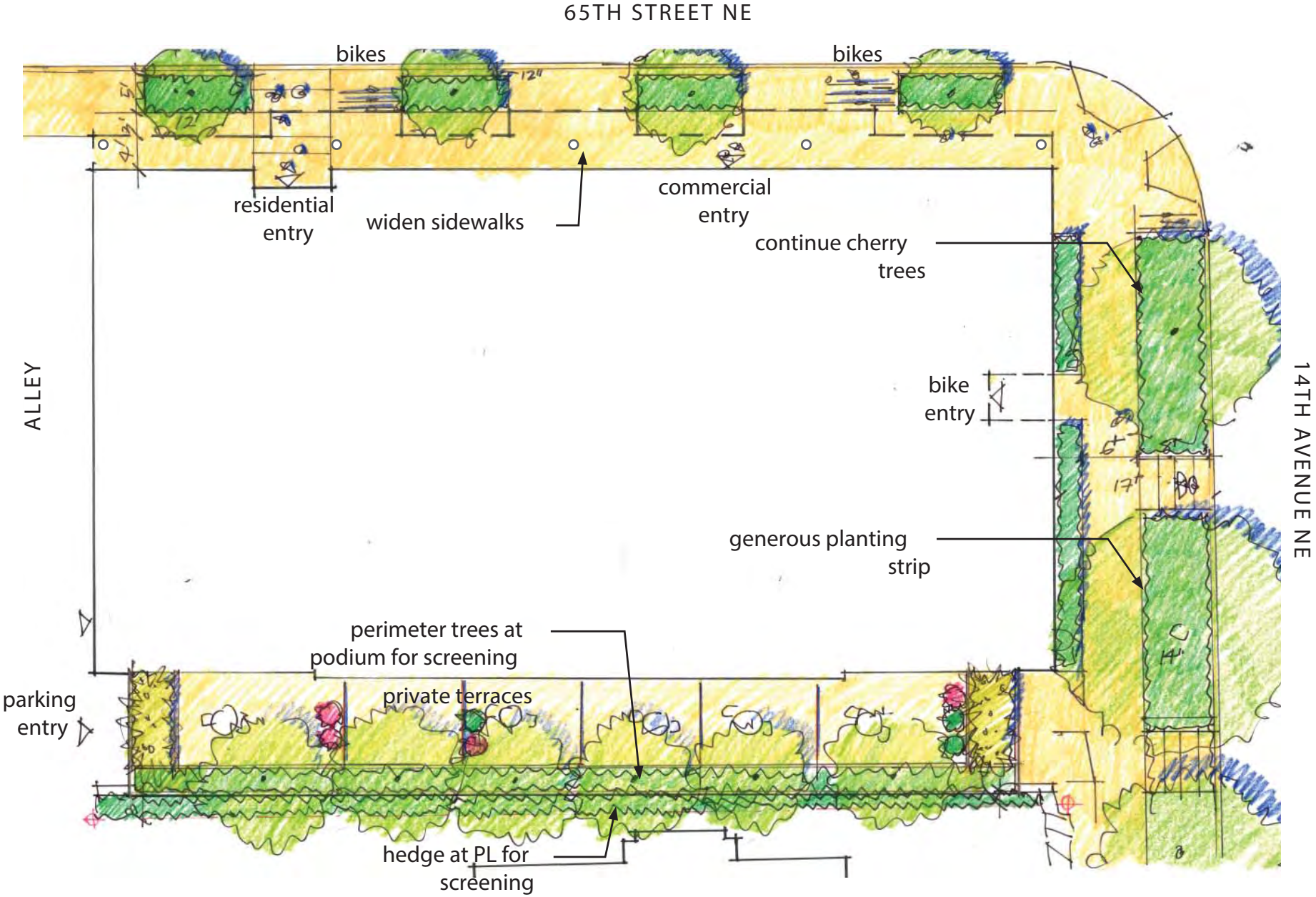


PARKING AISLE WIDTH: DEPARTURE #7 : COMPLIANT PLAN (NO BIKE ROOM)



VIEW LOOKING NORTH-WEST AT CORNER OF NE 65TH ST AND 14TH AVE NE

CONCEPTUAL LANDSCAPE PLAN (STREETSCAPE AND PODIUM)





WIDEN SIDEWALKS FOR SPILLOVER



GENEROUS PLANTING STRIP



PRIVATE TERRACES WITH PERIMETER TREES FOR SCREENING

POTENTIAL DESIGN GUIDELINES

SEATTLE DESIGN GUIDELINES	DESIGN RESPONSE
<p>CS1 Natural Systems and Site Features <i>Use natural systems and features of the site and its surroundings as a starting point for project design.</i></p> <p>B2 Sunlight & Natural Ventilation, Daylight and Shading Maximize daylight for interior and exterior spaces and minimize shading on adjacent sites through the placement and/or design of structures on the site</p> <p>Roosevelt I Supplemental Guidance Responding to Site Characteristics Minimizing shadow impacts along Roosevelt Way and NE 65th St is especially important in the Roosevelt neighborhood. The design of a structure and its massing on the site can enhance solar exposure for the project and minimize shadow impacts onto adjacent public areas between March 21st and September 21st. In addition to solar orientation and building siting, two other methods that can help minimize shadow impacts on public sidewalks include:</p> <ol style="list-style-type: none"> i. Upper level building setbacks ii. Setbacks along the building base 	<p>In all Design Alternatives the structure height is the max allowable (44-feet), but roof features are reduced or set back to reduce overshadowing across NE 65th St. The parapet is a minimum dimension needed for waterproofing. The stair and elevator cores are pulled back from the roof edge to reduce overshadowing.</p> <p>In the Preferred Alternative the building base is set back 4-ft from the property line.</p>
<p>CS2 Urban Pattern and Form <i>Strengthen the most desirable forms, characteristics, and patterns of the streets, block faces, and open spaces in the surrounding area.</i></p> <p>B2 Location in the City and Neighborhood, Connection to the Street Identify opportunities for the project to make a strong connection to the street and carefully consider how the building will interact with the public realm. Consider the qualities and character of the streetscape— its physical features (sidewalk, parking, landscape strip, street trees, travel lanes, and other amenities) and its function (major retail street or quieter residential street)—in siting and designing the building</p>	<p>The site is located at the intersection of two very different streetfronts. NE 65th St is a public, commercial and transit-oriented street that is in the transition period of becoming more densely developed with larger buildings. To respond to this street the Preferred Alternative's street level facade is highly glazed, and set back from the property line to widen the sidewalk, provide weather protection, and create planting buffers between car and pedestrian. In contrast, 14th Ave NE is a quiet, tree-lined residential street. To preserve the private qualities of this street, the Preferred Alternative's street level is quiet, with no major building entries to reduce noise and disruption to neighboring residential, and a landscaped facade to relate to the garden environment of the street.</p>
<p>C1 Relationship to the Block, Corner Sites Corner sites can serve as gateways or focal points; both require careful detailing at the first three floors due to their high visibility from two or more streets and long distances. Consider using a corner to provide extra space for pedestrians and a generous entry, or build out to the corner to provide a strong urban edge to the block</p>	<p>The proposed project sits on a corner site, and in all Design Alternatives a strong glassy corner is presented at the intersection to increase visibility of the commercial space or multifamily residential lobby from blocks away.</p>
<p>D1 Height Bulk and Scale, Existing Development & Zoning Review the height, bulk, and scale of neighboring buildings as well as the scale of development anticipated by zoning for the area to determine an appropriate complement and/or transition. Note that existing buildings may or may not reflect the density allowed by zoning or anticipated by applicable policies</p>	<p>Neighboring sites on NE 65th are zoned for NC1-40 and NC2-65, but are not developed to that potential. Properties are characterized by small, low-rise commercial and single-family houses. The neighboring sites south on 14th Ave NE are zoned for SF-5000 and occupied by single-family houses. All three Design Alternatives respond to the dual characteristics of the site, providing the higher mass and strong street wall to NE 65th, and stepping down to respond at the south end to respond to the neighboring residential.</p>
<p>D3 Height Bulk and Scale, Zone Transitions For projects located at the edge of different zones, provide an appropriate transition or complement to the adjacent zone(s). Projects should create a step in perceived height, bulk and scale between the anticipated development potential of the adjacent zone and the proposed development</p>	<p>see response to Roosevelt Supplemental Guidance III below.</p>
<p>D5 Height Bulk and Scale, Respect for Adjacent Sites Respect adjacent properties with design and site planning to minimize disrupting the privacy and outdoor activities of residents in adjacent buildings</p>	<p>In the Preferred Alternative a quieter facade is presented to the residential street of 14th Ave NE. Facade interest relies less on glazing and more on vegetation to fit with the tree-lined and heavily landscaped character of the street. With the major building entries located around the corner on NE 65th, the privacy 14th enjoys as a residential street is maintained.</p>

<p>Roosevelt I Supplemental Guidance</p>	<p>Streetscape Compatibility, Commercial and Mixed-Use Developments: Continuity of the Street Wall Along Sidewalks Where building setbacks vary along the street due to required street dedications, new developments are encouraged to introduce elements that can help preserve the continuity of adjacent street-facing building walls, especially within the Core Commercial Area. Any element within the public right-of-way such as awnings, planters, etc., will require SDOT (Seattle Department of Transportation) approval. The following design solutions could provide design continuity of the building wall at the pedestrian level where buildings are set back: a. Visually reinforce the existing street wall by placing horizontal or vertical elements in a line corresponding with the setbacks of adjacent building fronts. These could include trees, columns, planters, benches, overhead weather protection features or other building features. b. Visually reinforce the existing street wall by using paving materials that differentiate the setback area from the sidewalk. c. Consider using decorative paving within the public right-of-way with SDOT approval. d. Make use of the building setback to create a public space.</p>	<p>A strong streetwall is created on NE 65th, a primary pedestrian-oriented street, and a connector block between two neighborhood "gateway" intersections on NE 65th (65th and Roosevelt Way NE, and 65th and 15th) identified by the Roosevelt Design Guidelines. The street level is set back 4-ft from the property line, providing weather protection and widening the sidewalk to 11'-6". This dimension provides the width needed for Seattle's standard street section of a 5-ft planting and tree buffer between the sidewalk and street, to create a safer and more inviting pedestrian environment.</p>
<p>Roosevelt III Supplemental Guidance</p>	<p>Height, Bulk and Scale, Zone Edge Condition One Where a rear lot line of a commercially zoned lot (height limit of 30, 40, or 65 feet) abuts a side or rear of a residentially zoned lot (height limit of 25-35 feet). Examples of recommended design methods follow in order of preference: a. For commercial uses, place surface parking and access behind commercial buildings; b. Increase building setbacks along zone edges; c. Step back the upper floors or modify the roofline to reduce the overall building height.</p>	<p>The project site has a Zone Edge Condition One, where the rear lot line of the property abuts a residential zone. Design Alternative 2 and the Preferred Alternative respond to this condition by stepping back the upper floors at this lot line, and reducing the overall building's apparent height by reducing the parapet height to a minimum.</p>
<p>CS3 Architectural Context and Character <i>Contribute to the architectural character of the neighborhood</i></p>		
<p>A4 Emphasizing Positive Neighborhood Attributes, Evolving Neighborhoods In neighborhoods where architectural character is evolving or otherwise in transition, explore ways for new development to establish a positive and desirable context for others to build upon in the future</p>		
<p>The neighborhood surrounding the project site is in transition, with many adjacent sites under design, undeveloped, or not developed to their potential, particularly along NE 65th Street. With the incoming Light Rail Station a block and a half to the west, NE 65th in particular will become a more pedestrian-travelled street. The street-level design of the Preferred Alternative anticipates this change with modifications to the street section for plants and trees, and with transparent facades for street activation.</p>		
<p>PL1 Connectivity <i>Complement and contribute to the network of open spaces around the site and the connections among them</i></p>		
<p>B2 Walkways and Connections, Pedestrian Volumes Provide ample space for pedestrian flow and circulation, particularly in areas where there is already heavy pedestrian traffic or where the project is expected to add or attract pedestrians to the area.</p>		
<p>The Preferred Alternative widens the street section on NE 65th in anticipation of a higher volume of pedestrian activity resulting from neighborhood redevelopment and the incoming Light Rail Station. The existing sidewalk on NE 65th is a narrow 7'-6", with a minimal planting separation between pedestrians and the street, and no street trees. The wider sidewalk will accommodate more pedestrians and provide planted separation between pedestrians and vehicles.</p>		
<p>B3 Walkways and Connections, Pedestrian Amenities Opportunities for creating lively, pedestrian oriented open spaces to enliven the area and attract interest and interaction with the site and building should be considered. Visible access to the building's entry should be provided. Examples of pedestrian amenities include seating, other street furniture, lighting, year-round landscaping, seasonal plantings, pedestrian scale signage, site furniture, art work, awnings, large storefront windows, and engaging retail displays and/ or kiosks</p>		
<p>PL2 Walkability <i>Create a safe and comfortable walking environment that is easy to navigate and well-connected to existing pedestrian walkways and features</i></p>		

POTENTIAL DESIGN GUIDELINES

<p>B1 Safety and Security, Eyes on the Street Create a safe environment by providing lines of sight and encouraging natural surveillance through strategic placement of doors, windows, balconies and street-level uses</p>	<p>In all Design Alternatives the street level, particularly at NE 65th, is designed for transparency to provide eyes on the street from the commercial spaces, and residential amenity spaces.</p>
<p>B3 Safety and Security, Street-level Transparency Ensure transparency of street-level uses (for uses such as nonresidential uses or residential lobbies), where appropriate, by keeping views open into spaces behind walls or plantings, at corners, or along narrow passageways. Choose semi-transparent rather than opaque screening</p>	<p>In the Preferred Alternative the project builds to the rear lot line on 14th Ave NE. This is a departure from the Land Use Code due to the rear lot line shared with a side lot line of a residential zone, but by doing so the project avoids creating a dark corner off the back side of the building where visibility is low and safety becomes a concern.</p>
<p>C1 Weather Protection, Locations and Coverage Overhead weather protection is encouraged and should be located at or near uses that generate pedestrian activity such as entries, retail uses, and transit stops. Address changes in topography as needed to provide continuous coverage the full length of the building, where possible</p>	<p>Weather protection is built into the massing of Design Alternative 1 and the Preferred Alternative along NE 65th due to a street-level setback. The setback will cover entries to the multifamily residential lobby and commercial, and provide protection to pedestrians crossing in front to the site to reach transit stops.</p>
<p>PL3 Street-Level Interaction <i>Encourage human interaction and activity at the street-level with clear connections to building entries and edges</i></p>	
<p>A1 Entries, Design Objectives Design primary entries to be obvious, identifiable, and distinctive with clear lines of sight and lobbies visually connected to the street. Scale and detail them to function well for their anticipated use and also to fit with the building of which they are a part, differentiating residential and commercial entries with design features and amenities specific to each.</p>	<p>In the Preferred Alternative the commercial and multi-family entries are located on the pedestrian-oriented NE 65th Street, where they are weather-protected with a streetlevel setback, and highly glazed for visual connection between interior and exterior.</p>
<p>C2 Retail Edges, Visibility Maximize visibility into the building interior and merchandise displays. Consider fully operational glazed wall-sized doors that can be completely opened to the street, increased height in lobbies, and/ or special lighting for displays</p>	<p>In all Design Alternatives the NE 65th St street level façade is highly transparent to optimize opportunities for retail visibility and connections.</p>
<p>Roosevelt I Supplemental Guidance Human Activity Roosevelt is looking for opportunities to encourage pedestrian activity along sidewalks within the Commercial Core. This is especially important because sidewalks along Roosevelt and 65th are considered too narrow. If not required with new development, applicants are encouraged to increase the ground level setback in order to accommodate pedestrian traffic and amenity features.</p>	<p><i>*note, while the project site is not within the defined Roosevelt Commercial Core, the project still strives to increase pedestrian activity along NE 65th. It is expected that with the addition of the new Light Rail station, pedestrian activity along NE 65th further east of the Commercial Core is likely, thus the project designs for this potential. To this end, the above pedestrian realm improvements are proposed.</i></p>
<p>PL4 Active Transportation <i>Incorporate design features that facilitate active forms of transportation such as walking, bicycling and use of transit</i></p>	
<p>A2 Entry Locations and Relationships, Connection to All Modes Site the primary entry in a location that logically relates to building uses and clearly connects all major points of access</p>	<p>The Preferred Alternative locates the residential multi-family entry at the north-west corner of the site on NE 65th, which is the closes point to the future Light Rail stop a block and a half away. All main building entries are located on NE 65th, which is the major public transit access street for the site.</p>
<p>B2 Planning Ahead for Bicyclists, Bike Facilities Facilities such as bike racks and storage, bike share stations, shower facilities and lockers for bicyclists should be located to maximize convenience, security, and safety.</p>	<p>Limited parking is provided in all Design Alternatives so public transportation and bicycle use is encouraged. To encourage bicyclists, a secure bike storage room and bike workshop is proposed within the building.</p>
<p>C1 Planning Ahead for Transit, Influence on Project Design Identify how a transit stop (planned or built) adjacent to or near the site may influence project design, provide opportunities for place making, and/or suggest logical locations for building entries, retail uses, open space, or landscaping. Take advantage of the presence of transit patrons to support retail uses in the building.</p>	<p>The future Roosevelt Light Rail is a major driver in the project design. In the Preferred Alternative, beyond the influence on building entry locations and reduced parking provisions, the NE 65th street front is designed for pedestrian walkability (overhead protection, widened sidewalk, plantings) to connect pedestrians across the site to the transit stops along the street.</p>
<p>DC1 Project Uses and Activities <i>Optimize the arrangement of uses and activities on site</i></p>	

A4 Arrangement of Interior Uses, View and Connections

Locate interior uses and activities to take advantage of views and physical connections to exterior spaces and uses, particularly activities along sidewalks, parks or other public spaces

Common-use residential amenity space and commercial retail or potentially a restaurant are located on 65th, the more pedestrian-oriented street front.

B1 Vehicular Access and Circulation, Access Location and Design

Choose locations for vehicular access, service uses, and delivery areas that minimize conflict between vehicles and non-motorists wherever possible. Emphasize use of the sidewalk for pedestrians, and create safe and attractive conditions for pedestrians, bicyclists, and drivers

No curb cuts are proposed for the project, with all vehicular entrances off the existing alley.

Roosevelt I Supplemental Guidance Parking and Vehicle Access

Minimize the number of curb cuts and width of driveways and curb cuts along Roosevelt Way NE and NE 65th Street by locating vehicle access onto alleys and/or side streets when feasible.

DC2 Architectural Concept

Develop an architectural concept that will result in a unified & functional design that fits well on the site & within its surroundings

A1 Massing, Site Characteristics and Uses

Arrange the mass of the building taking into consideration the characteristics of the site and the proposed uses of the building and its open space. In addition, special situations such as very large sites, unusually shaped sites, or sites with varied topography may require particular attention to where and how building massing is arranged as they can accentuate mass and height

Alternative 2 and the Preferred Alternative's massing's step down at the south side of the site to a single-story height. The single-story level creates an opportunity for a landscaped terrace on the second level for residents. The higher mass to the north avoids overshadowing the terrace, and blocks the street noise from the NE 65th.

B2 Architectural and Facade Composition, Blank Walls

Avoid large blank walls along visible façades wherever possible. Where expanses of blank walls, retaining walls, or garage facades are unavoidable, include uses or design treatments at the street level that have human scale and are designed for pedestrians. These may include:
 a. newsstands, ticket booths and flower shops (even if small or narrow);
 b. green walls, landscaped areas or raised planters;
 c. wall setbacks or other indentations;
 d. display windows; trellises or other secondary elements;
 e. art as appropriate to area zoning and uses; and/or
 f. terraces and landscaping where retaining walls above eye level are unavoidable

The Preferred Alternative has program elements that are not conducive to transparency along the south end of 14th Ave NE. Facade interest is proposed via landscaping and climbing vines. This strategy is consistent with the quiet landscaped quality of 14th Ave NE.

C3 Secondary Architectural Features, Fit with Neighboring Buildings

Use design elements to achieve a successful fit between a building and its neighbors, such as:
 a. consider aspects of neighboring buildings through architectural style, roof line, datum line detailing, fenestration, color or materials,
 b. using trees and landscaping to enhance the building design and fit with the surrounding context, and/or
 c. creating a well-proportioned base, middle and top to the building in locations where this might be appropriate. Consider how surrounding buildings have addressed base, middle, and top, and whether those solutions-- or similar ones-- might be a good fit for the project and its context.

Alternative 2 and the Preferred Alternative step down at the south side of the site to a landscaped terrace provided to units on the second floor. The height of this terrace is similar to the neighboring single-family house, and is fitting in the raised landscapes along 14th Ave NE. The neighboring houses are raised on landscaped rockeries or retaining walls.

DC3 Open Space Concept

Integrate open space design with the design of the building so that each complements the other

B4 Open Space Uses and Activities, Multifamily Open Space

Design common and private open spaces in multi-family projects for use by all residents to encourage physical activity and social interaction. Some examples include areas for gardening, children's play (covered and uncovered), barbecues, resident meetings, and crafts or hobbies

A range of residential amenity spaces are proposed for the project: a roof patio with barbecues within a green roof garden, a gathering space on the second floor with a spill-out terrace, and a business center off the lobby with windows overlooking the street.

POTENTIAL DESIGN GUIDELINES

C2 Design, Amenities and Features

Create attractive outdoor spaces well-suited to the uses envisioned for the project. Use a combination of hardscape and plantings to shape these spaces and to screen less attractive areas as needed. Use a variety of features, such as planters, green roofs and decks, groves of trees, and vertical green trellises along with more traditional foundation plantings, street trees, and seasonal displays

Roosevelt I Supplemental Guidance

Residential Open Space

The Roosevelt Neighborhood values places for residents to gather. For mixed use developments, provision of ground-related common open space areas in exchange for departures especially to the maximum residential coverage limit is encouraged, in addition to other allowable departures. Open space areas can also be achieved in a variety of ways including:

- i. Terraces on sloping land to create level yard space
- ii. Courtyards
- iii. Front and/or rear yards
- iv. Roof tops

DC4 Exterior Elements and Finishes

Use appropriate and high-quality elements and finishes for the building and its open spaces

A1 Building Materials, Exterior Finish Materials

Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged

As the project is developed further these guidelines will influence façade finishes and landscape choices.

D4 Trees Landscape and Hardscape Materials, Place Making

Create a landscape design that helps define spaces with significant elements such as trees

Roosevelt I Supplemental Guidance

Exterior Finish Materials, Signs

Developments should accommodate places for signage that are in keeping with the building's architecture and overall sign program.

Preferred sign types include:

- i. Small signs incorporated into the building's architecture, along a sign band, on awnings or marquees, located in windows, or hung perpendicular to the building facade are preferred within the Commercial Core Area.
- ii. Neon signs are also encouraged, while large illuminated box signs are discouraged.
- iii. Blade signs hung from beneath awnings or marquees are especially favored in the Commercial Core Area.

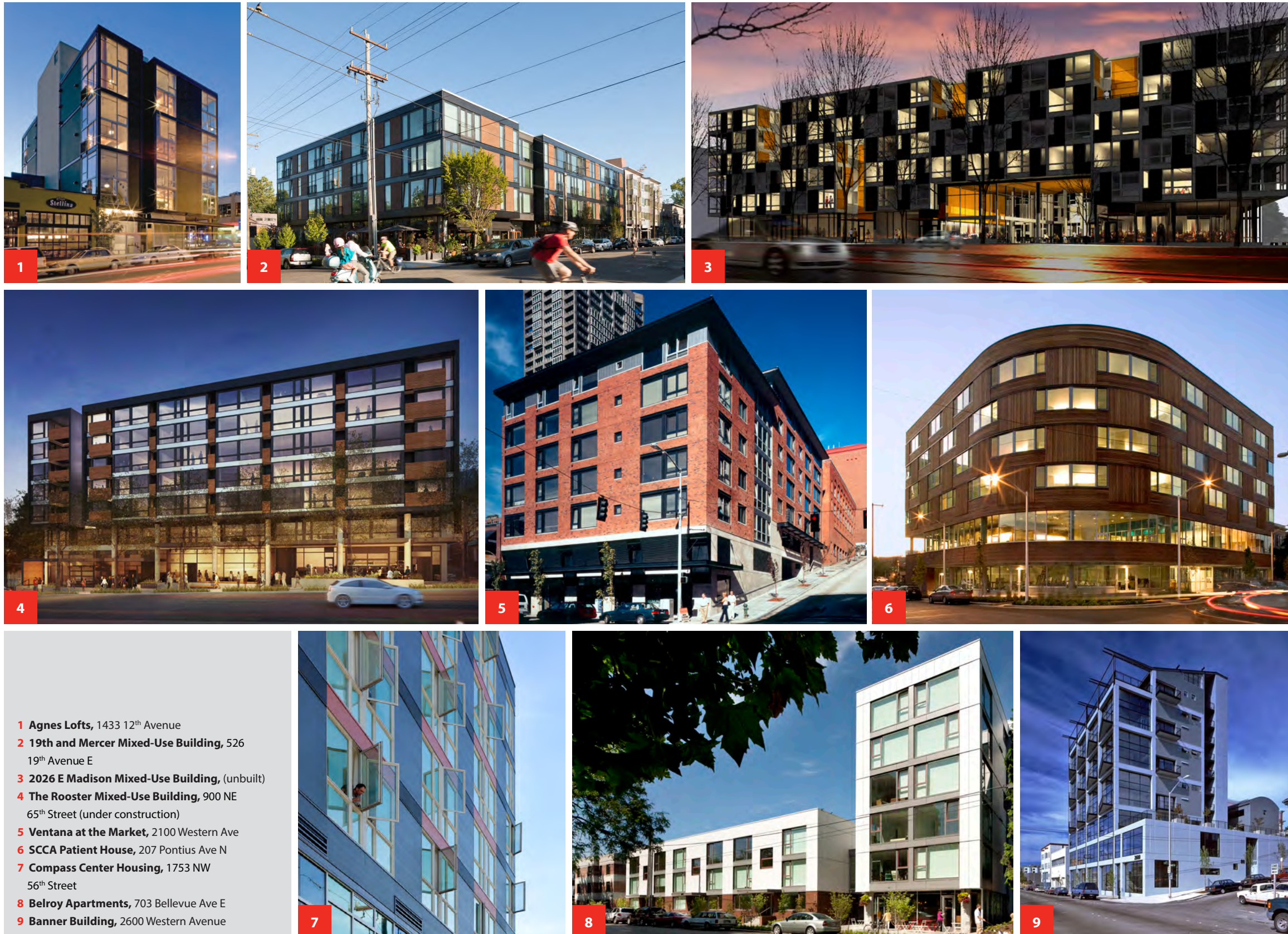
Large box signs, large-scale super graphics and back-lit awnings or canopies are less desirable, especially within the Commercial Core. Where awnings are illuminated, the light source should be screened to minimize glare impacts to pedestrians and vehicles.

REPRESENTATIVE PROJECTS

Weinstein A+U is recognized as one of the Northwest's leading design firms and has continually demonstrated design excellence on a broad array of projects for State, City, Federal, private, and not-for-profit clients. We are passionate about our city and the shaping of its urban neighborhoods through the integration of architecture and urban design is central to our practice.

Well-designed and thoughtful urban housing is a special concern of ours, and we have worked aggressively to advance the expectations of mixed-use projects in Seattle, both technically and aesthetically. While each project presents very specific challenges, a number of recurring themes inform much of our work and form the basis of our approach to housing design:

- All of our buildings are situational and are inseparable from their sites. They sit comfortably amongst their established neighbors, drawing from established precedents while looking to the future
- Well-designed unit plans are essential to the success of a housing project. While the functionality of each unit type is important, the organization of units across a floor plate and their influence on building elevations is equally important
- Appropriately located and proportioned open space is a significant design determinant for most mixed-use and urban housing projects
- We avoid arbitrary façade embellishment. Instead we utilize the organization of individual units and their aggregation to establish the pattern and rhythm of multi-family facades that is furthered informed by site organization and orientation. Plans correlate to elevations and variation occurs within an established system
- The constrained budgets for typical mixed-use projects demand careful consideration of a project's primary orientation and configuration to provide cost effective sustainable design strategies
- The scale and proportion of new mixed-use buildings must address, but need not directly reflect, those of adjacent structures. Plan, section, and elevation strategies should be integrated to provide a comprehensible "read" of the building's composition and organization



1 Agnes Lofts, 1433 12th Avenue
2 19th and Mercer Mixed-Use Building, 526 19th Avenue E
3 2026 E Madison Mixed-Use Building, (unbuilt)
4 The Rooster Mixed-Use Building, 900 NE 65th Street (under construction)
5 Ventana at the Market, 2100 Western Ave
6 SCCA Patient House, 207 Pontius Ave N
7 Compass Center Housing, 1753 NW 56th Street
8 Belroy Apartments, 703 Bellevue Ave E
9 Banner Building, 2600 Western Avenue