



4516 Union Bay Place NE

DESIGN REVIEW BOARD MEETING - FEBRUARY 22, 2016

(DPD #3019495)

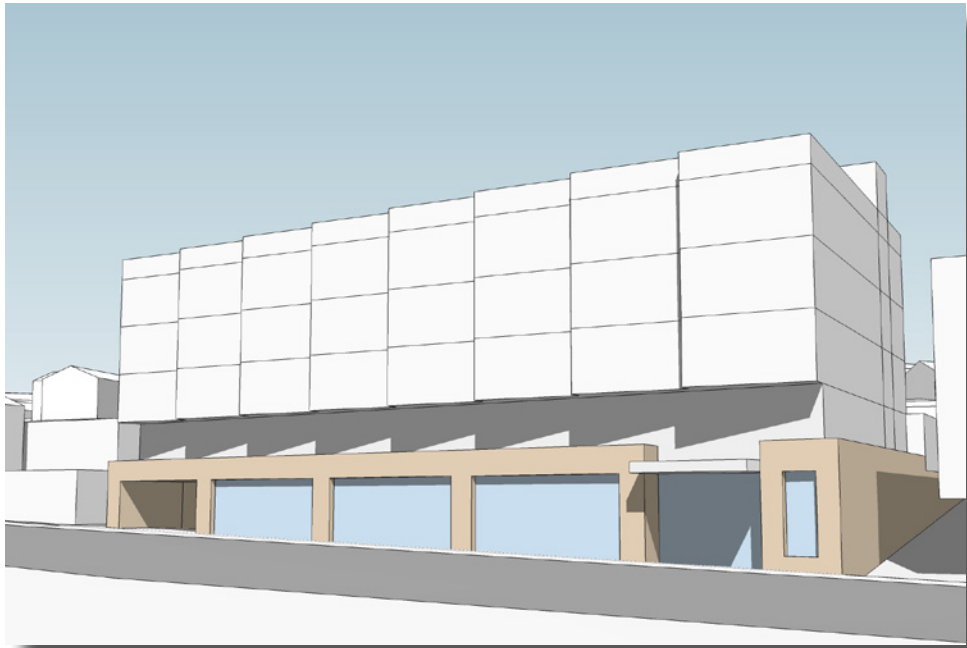
ICP CAPITAL

ENCORE architects

GHA LANDSCAPE ARCHITECTS

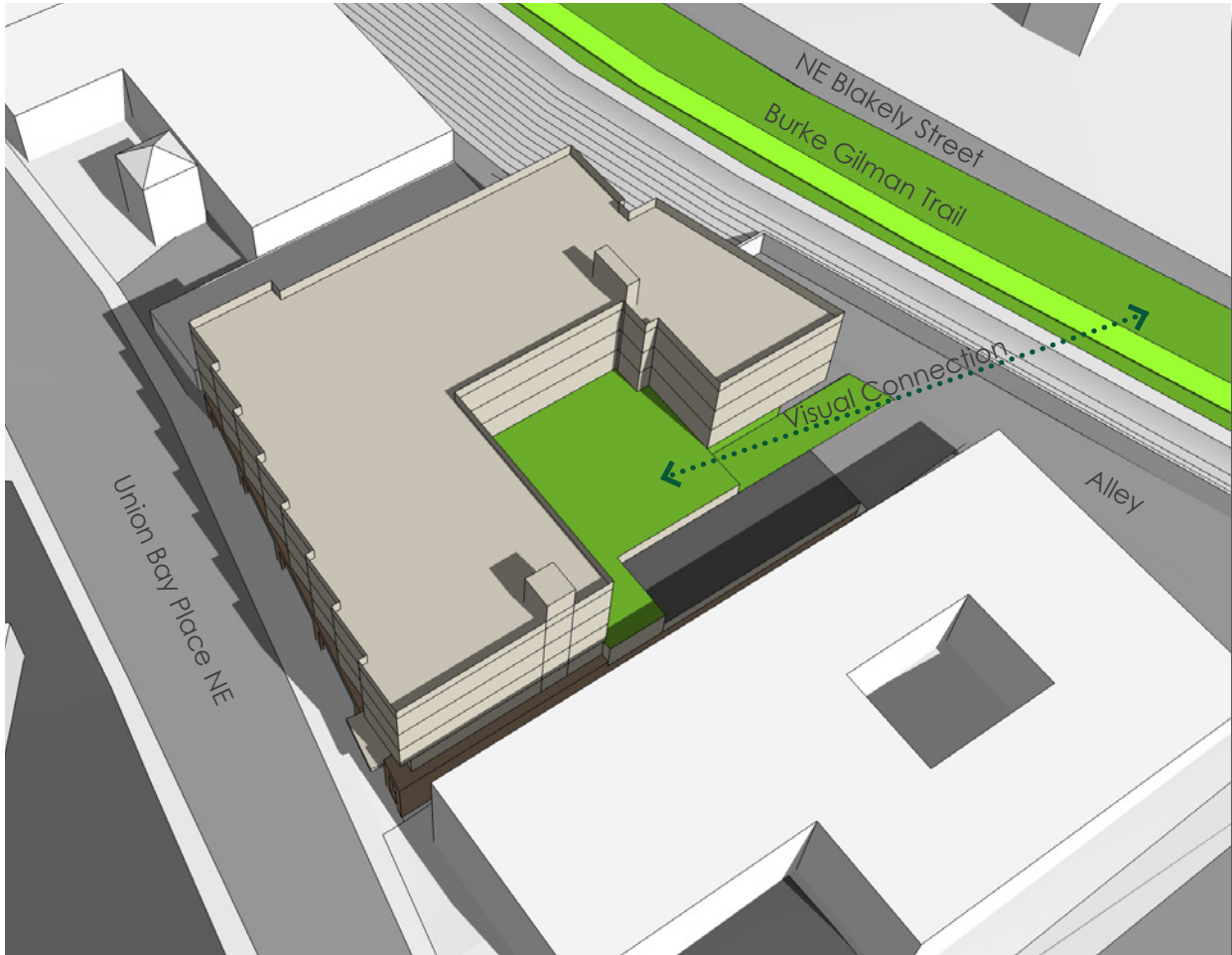


EXISTING PARKING STRUCTURE



BOARD SUPPORTED THE MASSING PRESENTED

- CLEAR ARTICULATION OF BASE, MIDDLE AND TOP



BOARD SUPPORTED THE LOCATION OF THE COURTYARD

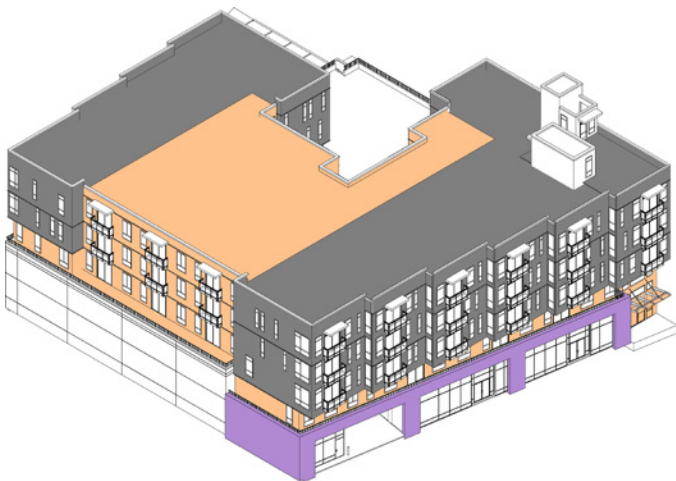
- SUN ACCESS
- VISUAL CONNECTION TO THE BG TRAIL
- BLDG RETAINS A STRONG STREET EDGE

Approved Massing
CONCEPT 3
"THE ONE, ONE, THREE"

1

Massing and Context Response:

- a. The Board supported the massing presented in Option 3 as an appropriate response to site characteristics and context, and encouraged the applicant to further **explore how the programming of the building can be expressed in the massing.** (CS1-C, C2-B, CS2-D, DC2-A)
- b. The Board preferred the location of the upper level courtyard and massing of Option 3, as it allows for sun access, provides a visual connection to the Burke Gilman Trail corridor, and retains a strong Street edge along Union Bay Place NE. (CS1-B, PL1-C, DC3-A, DC3-B)
- c. The Board requested **more information regarding the design of the courtyard.** (DC3-A, DC3-B, DC3-C, DC4-D)
- d. A pedestrian connection from Blakely would be supported, but the Board recognized the difficulty of achieving this due to the location of the right-of-way and the significant elevation change. If this is pursued, the Board suggested locating this connection towards the west side of the site. (PL1-A, PL1-B)
- e. The design of the **north façade should respond to potential development** that could occur on the adjacent parcel. The Board expressed concern that locating patio spaces on this façade could create privacy concerns. (CS2-B, CS2-D)



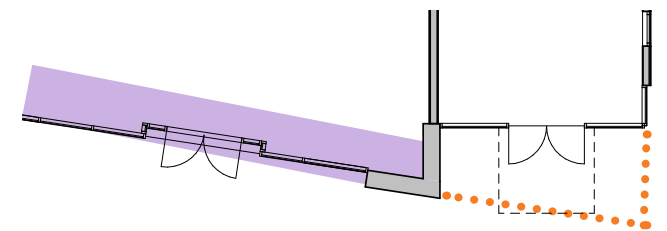
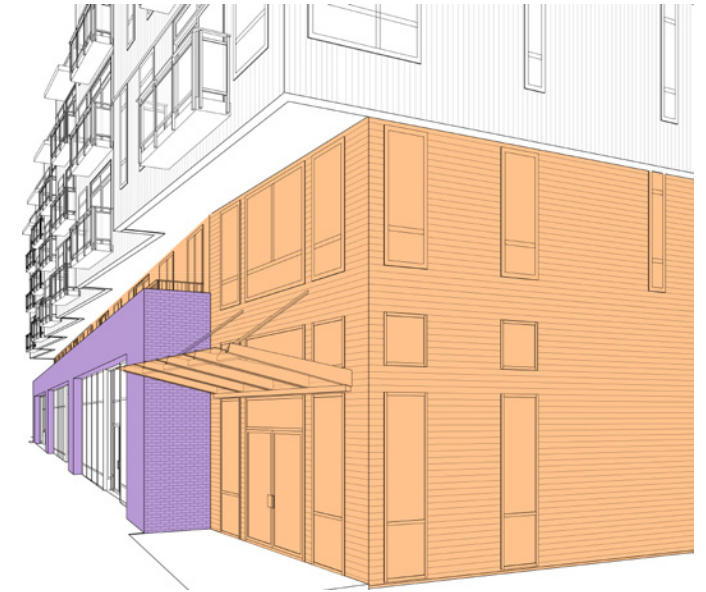
2 Architectural Composition:

- a. The Board supported the design concept of a one-story Base and floating three-story mass above, and the clear articulation of a base, middle, and top.
(DC2-A, DC2-B)
- b. **Demonstrate how the materials respond to the design Concept, for each façade.** (DC2-B)
- c. **The northeast façade will be visible the Burke-Gilman Trail and NE Blakely Street. Provide more information,** including sections and perspectives from these locations, and design the façade appropriately.
(CS2-B, DC2-B)
- d. Explore incorporating continuous overhead weather protection, and consider how this relates to the overall design concept. (PL2-C)
- e. Demonstrate how the **units relate to the massing and architectural composition.** (DC2-A, DC2-B)
- f. **Provide a conceptual signage plan,** especially in regards to the parking entry. (DC4-B)



3 Entry:

The Board supported the proposed location of the lobby entry of the preferred alternative; however, they expressed that the **entry should be clearly articulated and reinforced through the overall massing and architectural composition.** (PL3-A, PL4-A)

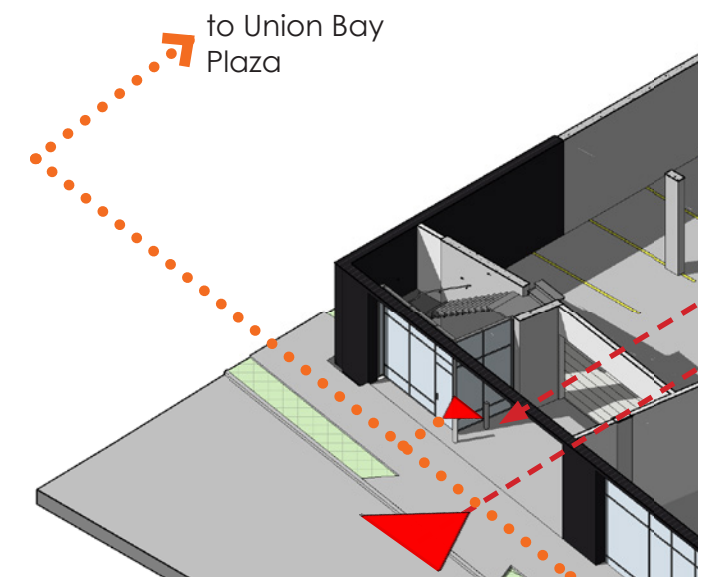
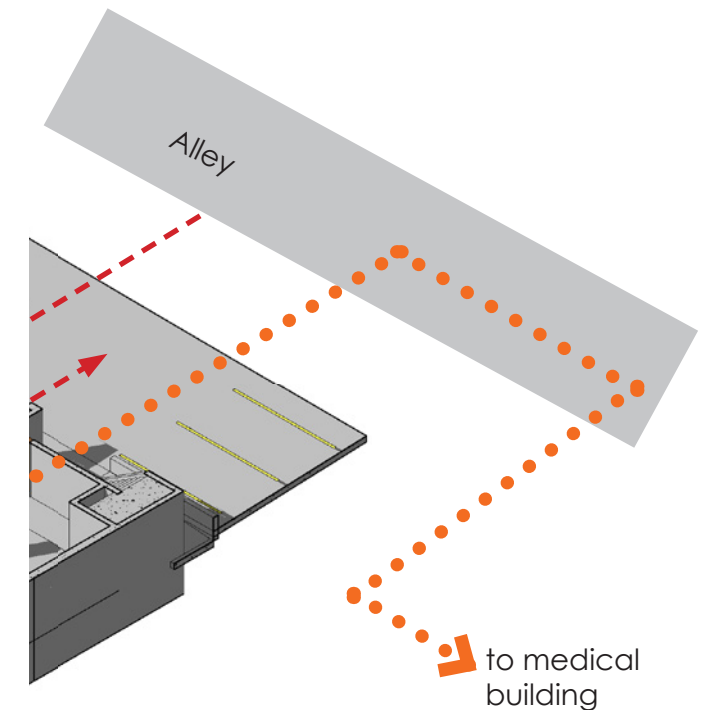


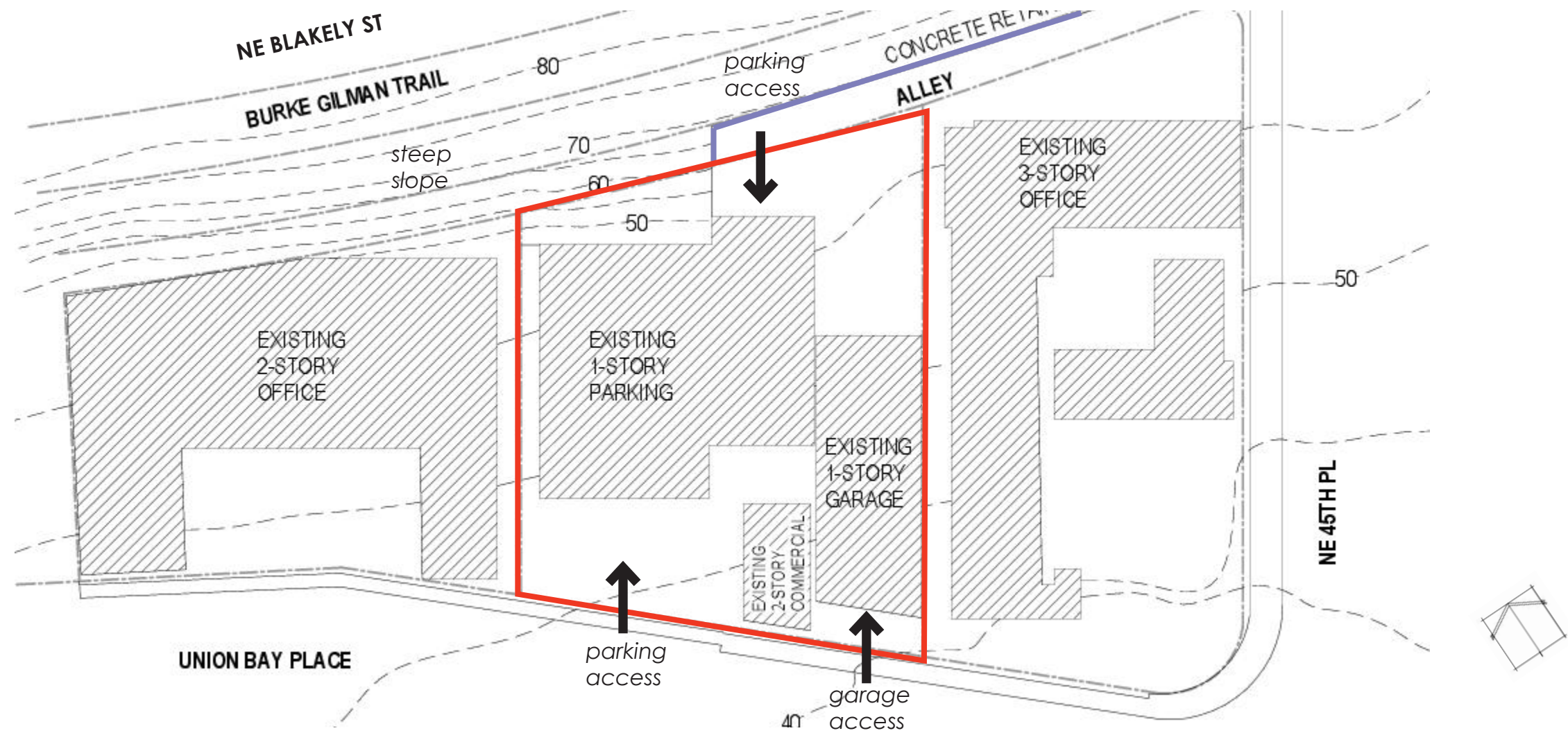
4 Access to Parking (23.47A.032.A.3):

The Code requires structures in Commercial zones with residential uses to meet the requirements of parking access for NC zones. SMC 247A.032A.1.a requires that access to parking in NC zones shall be from the alley if the lot abuts an alley. The applicant proposes taking access from the alley and from a curb cut on Union Bay Place NE.

The Board acknowledged that the lack of visibility of the alley may present a wayfinding challenge to direct parking for the adjacent medical office buildings towards the alley. After discussing the potential impacts on the design of the streetscape and pedestrian experience of the proposed departure, the Board indicated that they would be open to considering the departure if more information is provided by the applicant. At the Recommendation Meeting, the applicant should **provide studies and graphics demonstrating how the departure would impact the streetscape and pedestrian experience; how retail users get from the parking to the retail uses, lobby, and adjacent structures; and how the parking entry interfaces with the adjacent property to the north.**

(PL2-B, PL4-A, DC1-B, DC1-C)





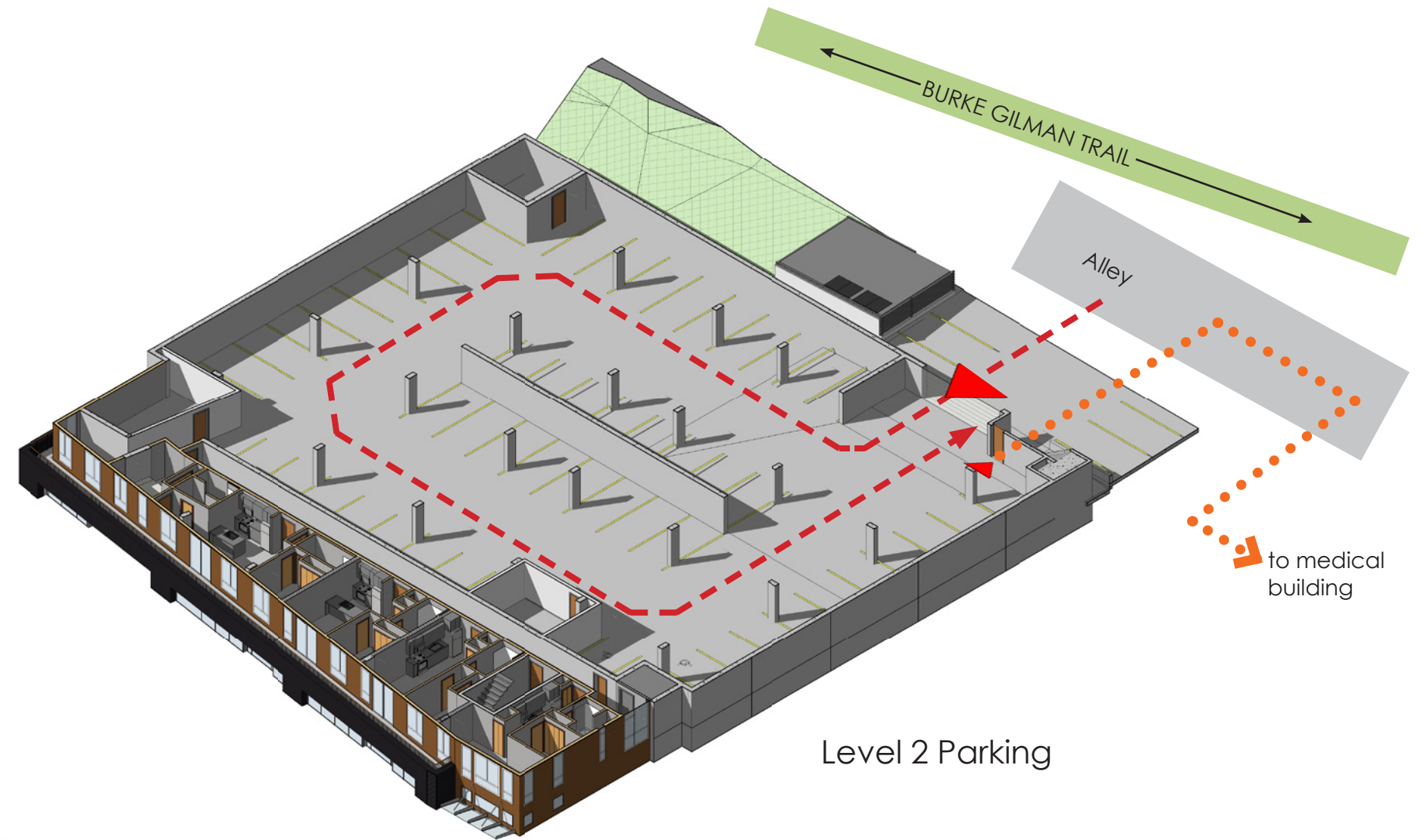
UNION BAY PLAZA BUILDING



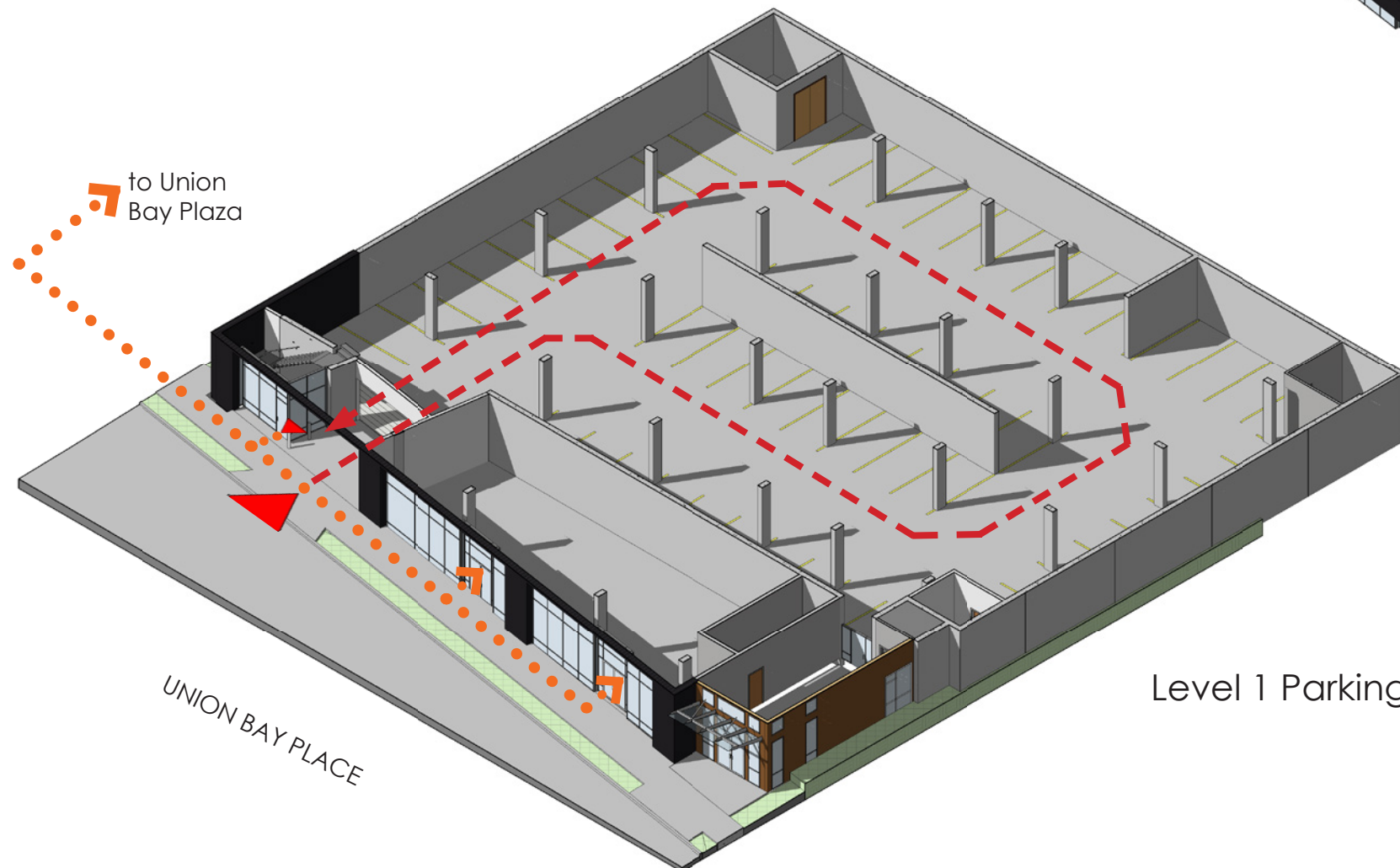
EXISTING PARKING STRUCTURE







LAKE VIEW MEDICAL DENTAL BUILDING



Level 2 Parking

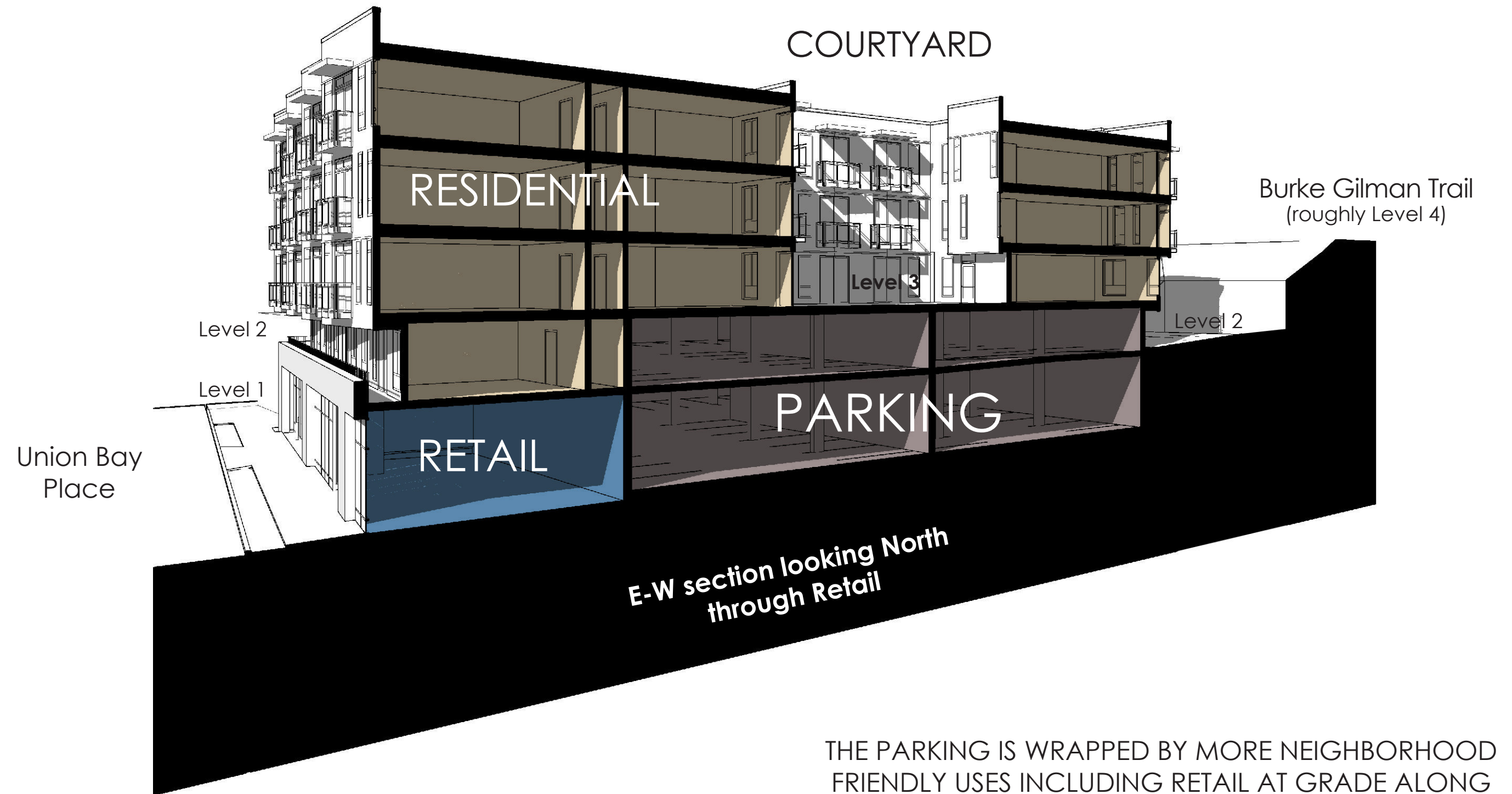


Level 1 Parking

-  Pedestrian Path
-  Vehicular Circulation
-  Vehicular Access
-  Pedestrian Access

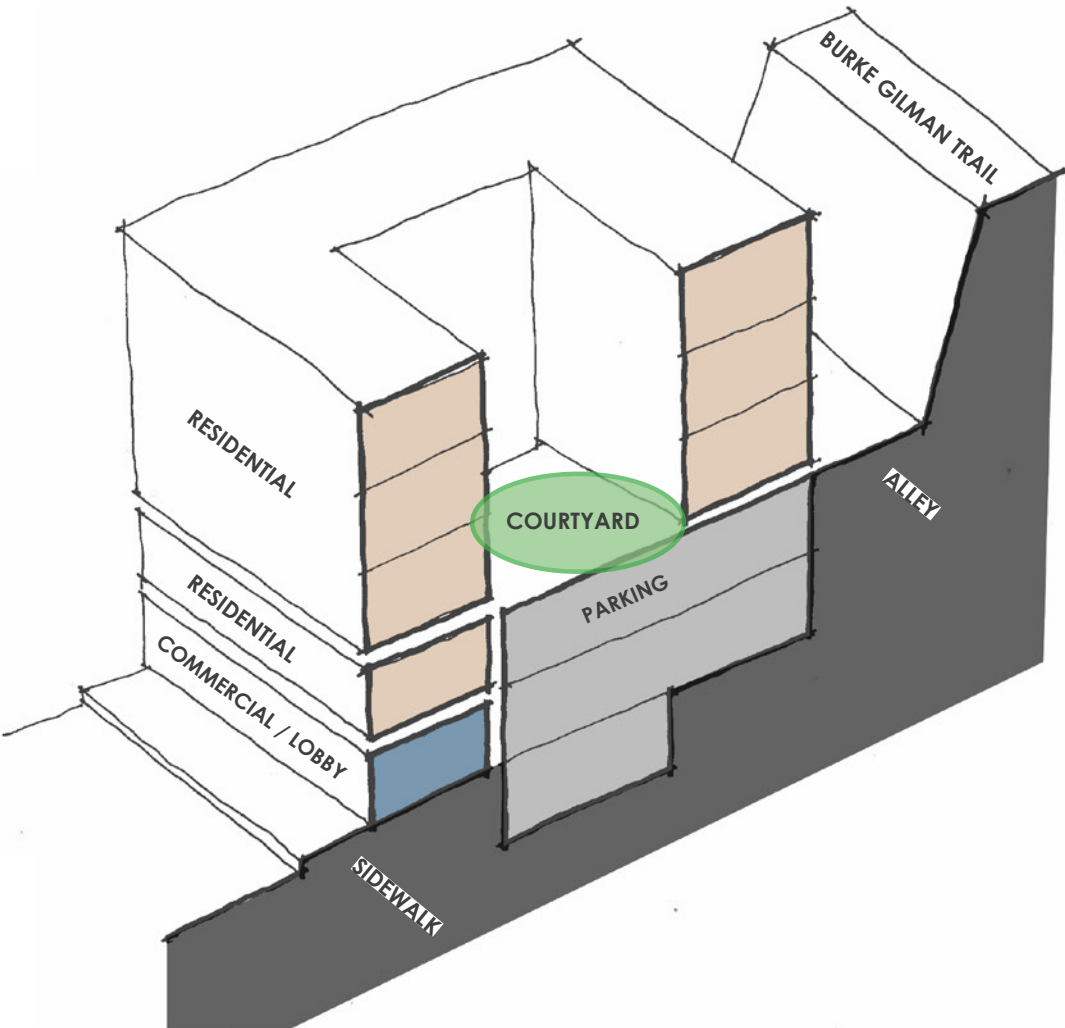
4 Access to Parking (23.47A.032.A.3):

...how retail users get from the parking to the retail uses, lobby, and adjacent structures; and how the parking entry interfaces with the adjacent property to the north. (PL2-B, PL4-A, DC1-B, DC1-C)

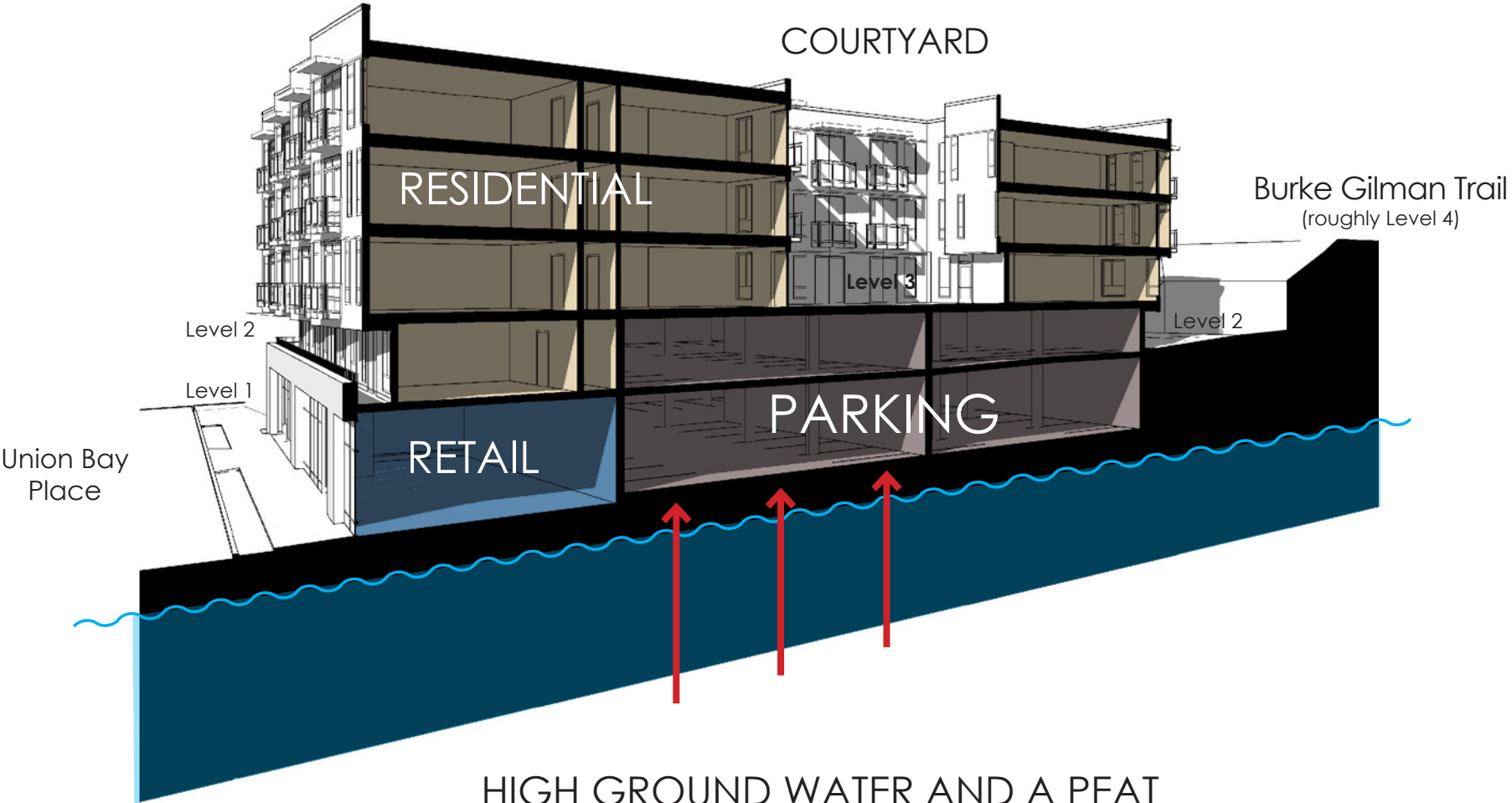


THE PARKING IS WRAPPED BY MORE NEIGHBORHOOD FRIENDLY USES INCLUDING RETAIL AT GRADE ALONG UNION BAY PLACE AND RESIDENTIAL UNITS ABOVE AND ON LEVEL 2.

LEVEL 2 MEETS THE ALLEY AT GRADE AND THE BURKE GILMAN TRAIL IS UPHILL ABOUT 2 STORIES HIGHER.



SECTION FROM EDG SHOWING
2 1/2 STORIES OF PARKING IN A
CONTINUOUS SPIRAL



HIGH GROUND WATER AND A PEAT
SETTLEMENT ENVIRONMENTAL CRITICAL
AREA REQUIRE THE PARKING TO BE
PUSHED UP OUT OF THE GROUND

TO RESPOND TO PEAT SETTLEMENT
ECA PARKING IS NOW TWO SEPARATE
LEVELS FOR EFFICIENCY AND REQUIRES
SEPARATE ENTRIES TO FUNCTION

4 Access to Parking (23.47A.032.A.3)

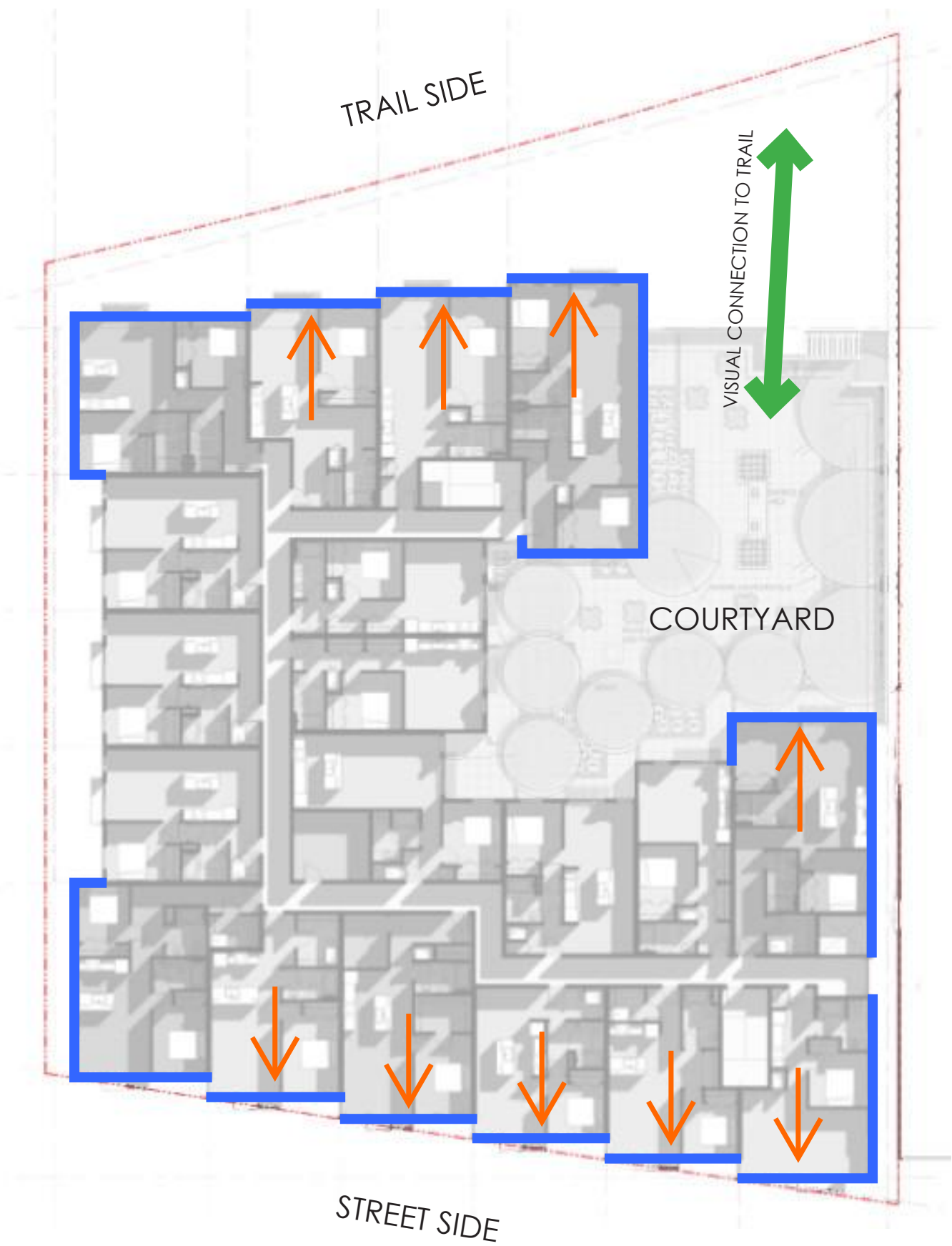


UNION BAY PLACE
approved massing from EDG
showing 1-1-3 scheme

- 3 RESIDENTIAL BARS
- 1 "MIDDLE ZONE"
- 1 PUBLIC RETAIL

1 Massing and Context Response:

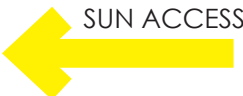
a. ...explore how the programming of the building can be expressed in the massing. (CS1-C, C2-B, CS2-D, DC2-A)



BAYS ARE PUSHED OUT TO FOLLOW THE SPLAYED PROPERTY LINES

EACH BAY CORRESPONDS TO ONE UNIT

COURTYARD IS OPEN FOR SUN ACCESS AND A VISUAL CONNECTION TO THE TRAIL



Elevation along Union Bay Place

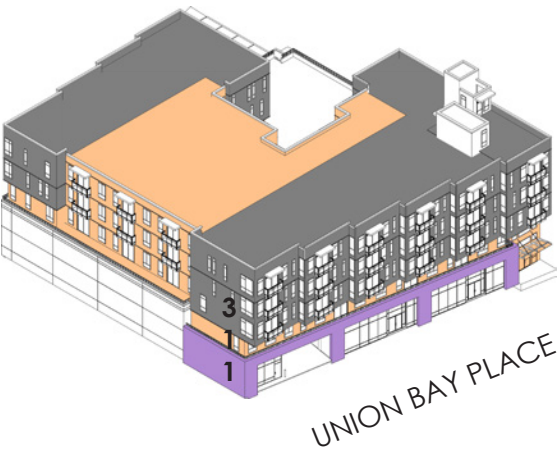
2 Architectural Composition:

b. Demonstrate how the materials respond to the design Concept, for each façade. (DC2-B)

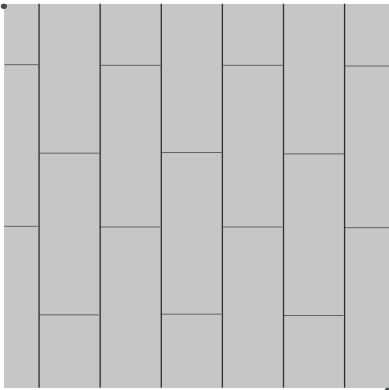
e. Demonstrate how the units relate to the massing and architectural composition. (DC2-A, DC2-B)



- 2
- Architectural Composition:**
- b. Demonstrate how the materials respond to the design Concept, for each façade. (DC2-B)
 - e. Demonstrate how the **units relate to the massing and architectural composition.** (DC2-A, DC2-B)



- 3 RESIDENTIAL BARS
- 1 "MIDDLE ZONE"
- 1 PUBLIC RETAIL



Metal Panel at the Residential Masses
Metal Standing Seam Shingle Siding
Color = Light Grey



**Wood-tone Stained Manufactured Lap Siding
(Secondary Residential Massing)**



Durable Brick at the Retail Bar
Color = Coal Creek



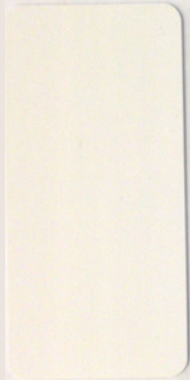
Cast-in-place Concrete

2 Architectural Composition:

b. Demonstrate how the materials respond to the design Concept, for each façade. (DC2-B)



Metal standing seam shingle siding



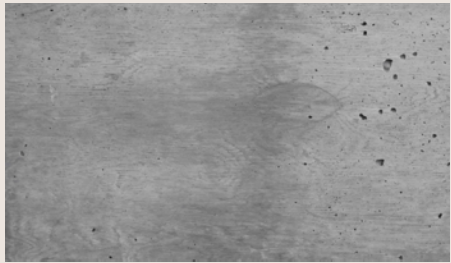
Vinyl window frames



Powder coated metal balcony and railings



Wood-tone stained manufactured lap siding



Cast-in-place concrete



Durable brick at the retail bar



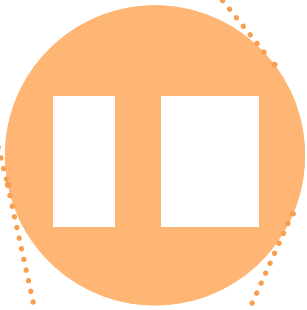
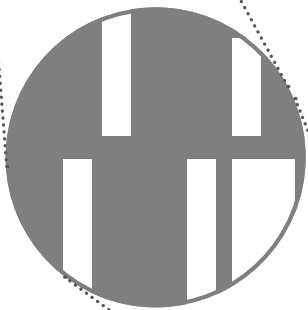
East Elevation - Burke Gilman Trail



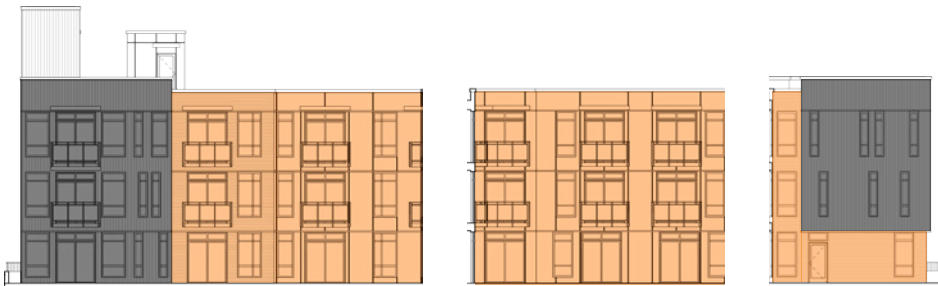
North Elevation - Faces Union Bay Plaza



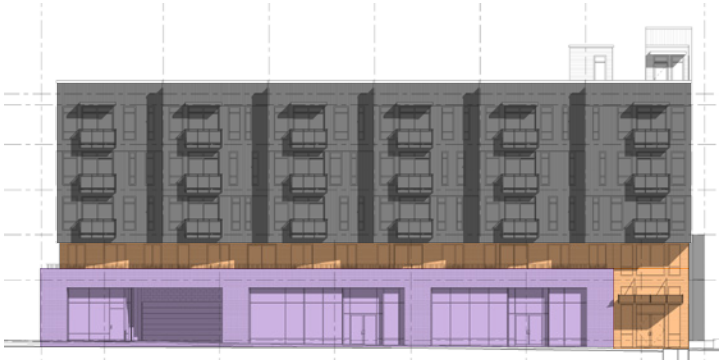
West Elevation - Union Bay Place NE



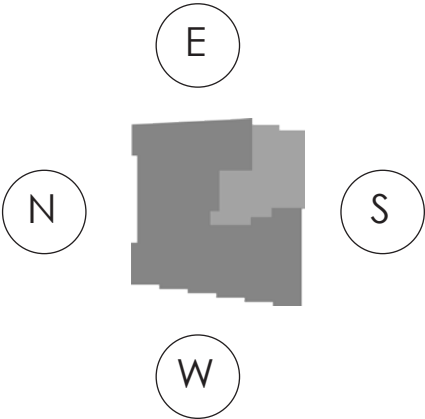
South Elevation - Faces Medical Buidling



Courtyard Elevations - continuing the concept



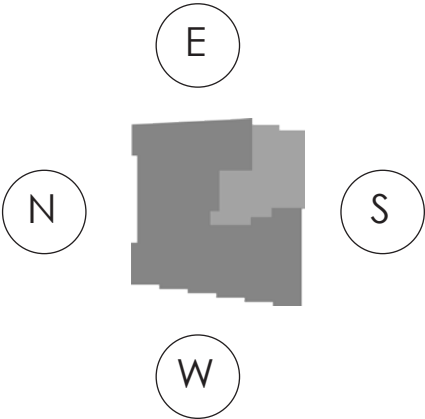
Elevation along Union Bay Place

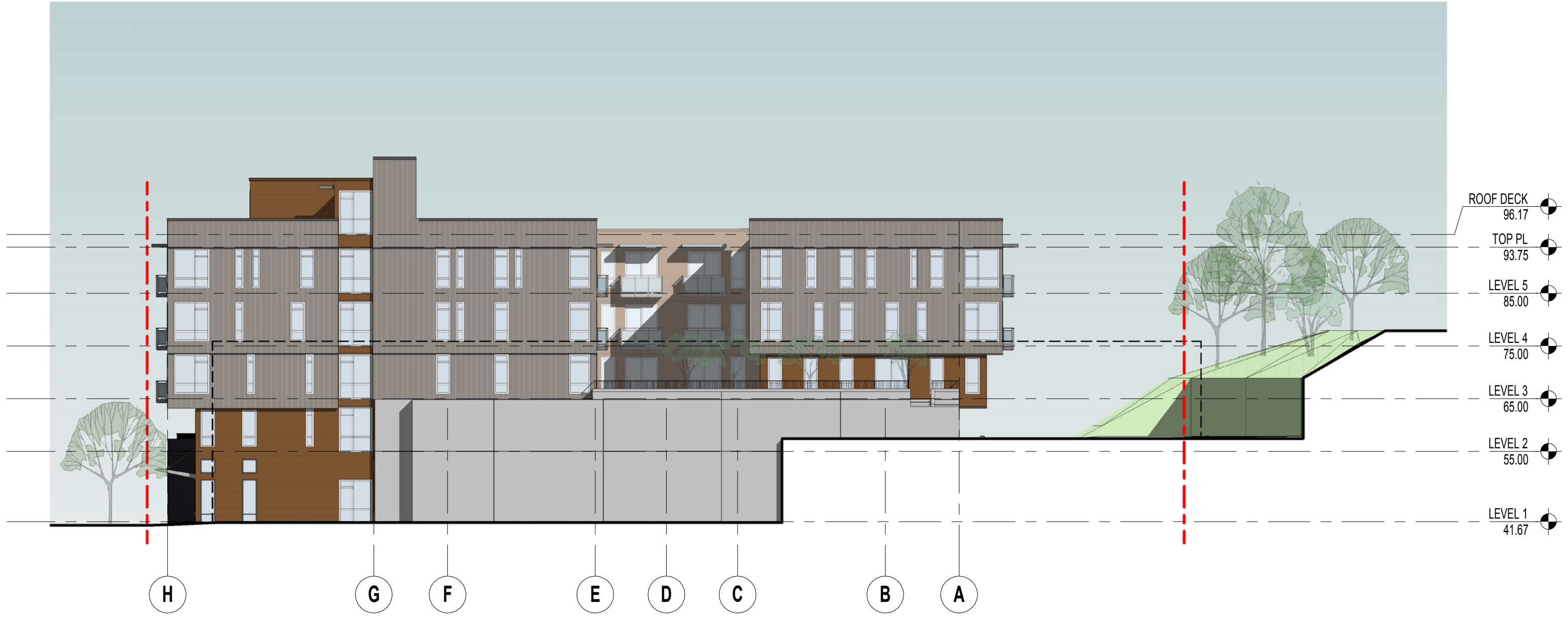




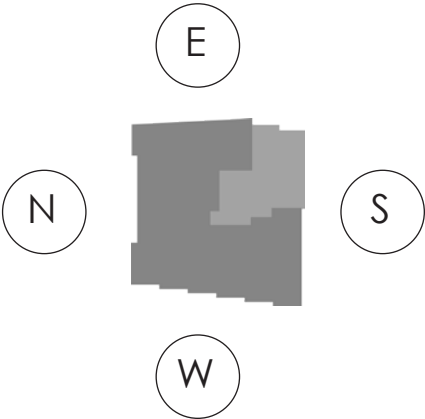


Elevation facing the Burke Gilman Trail





Elevation facing Lakeview Building

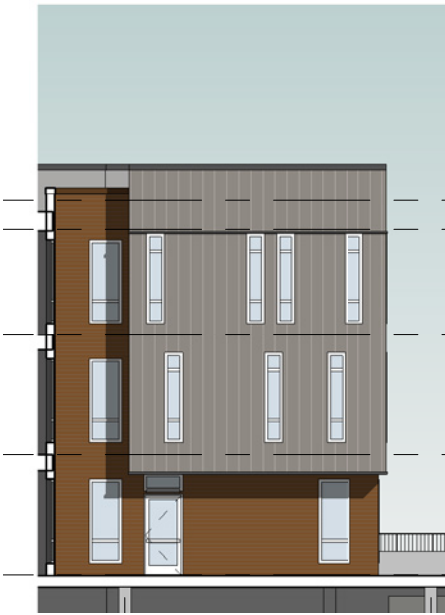




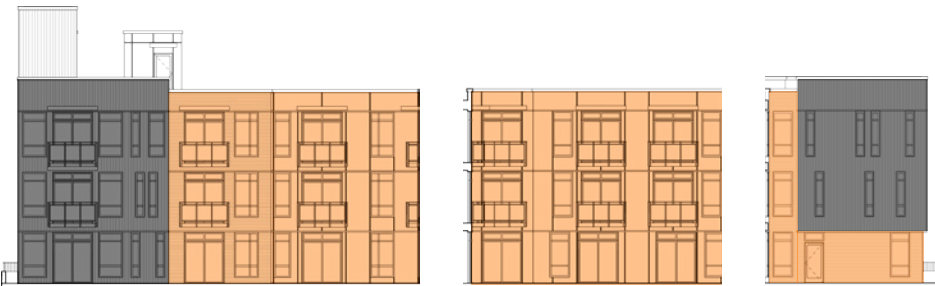
W Courtyard



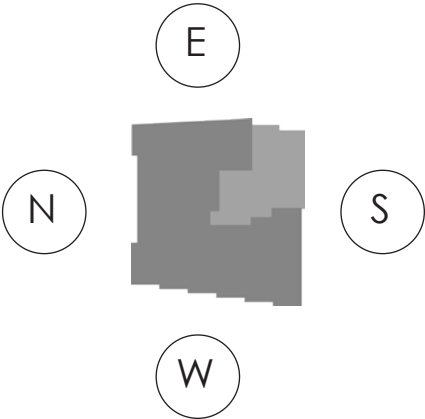
N Courtyard

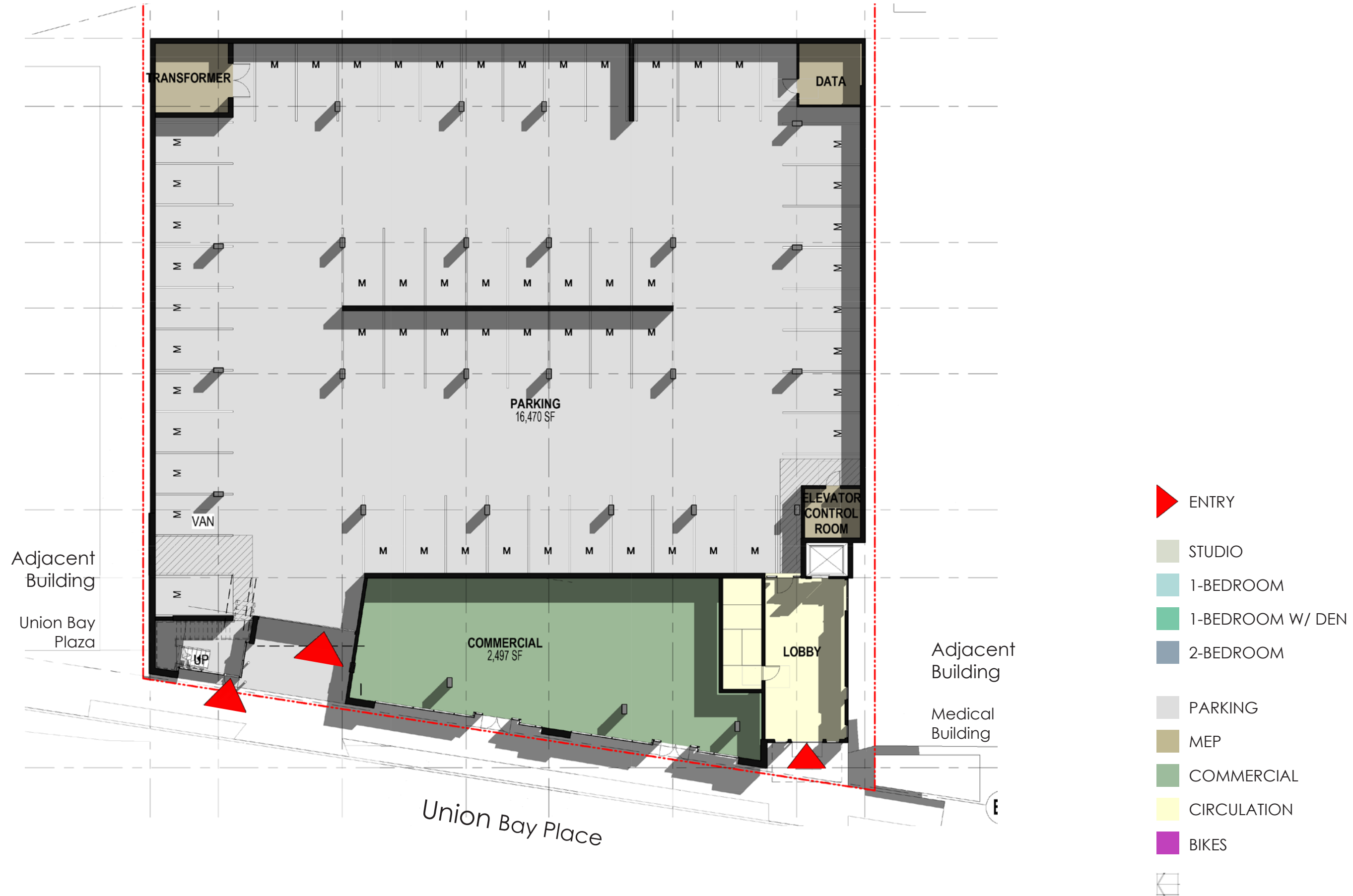


E Courtyard



Courtyard Elevations



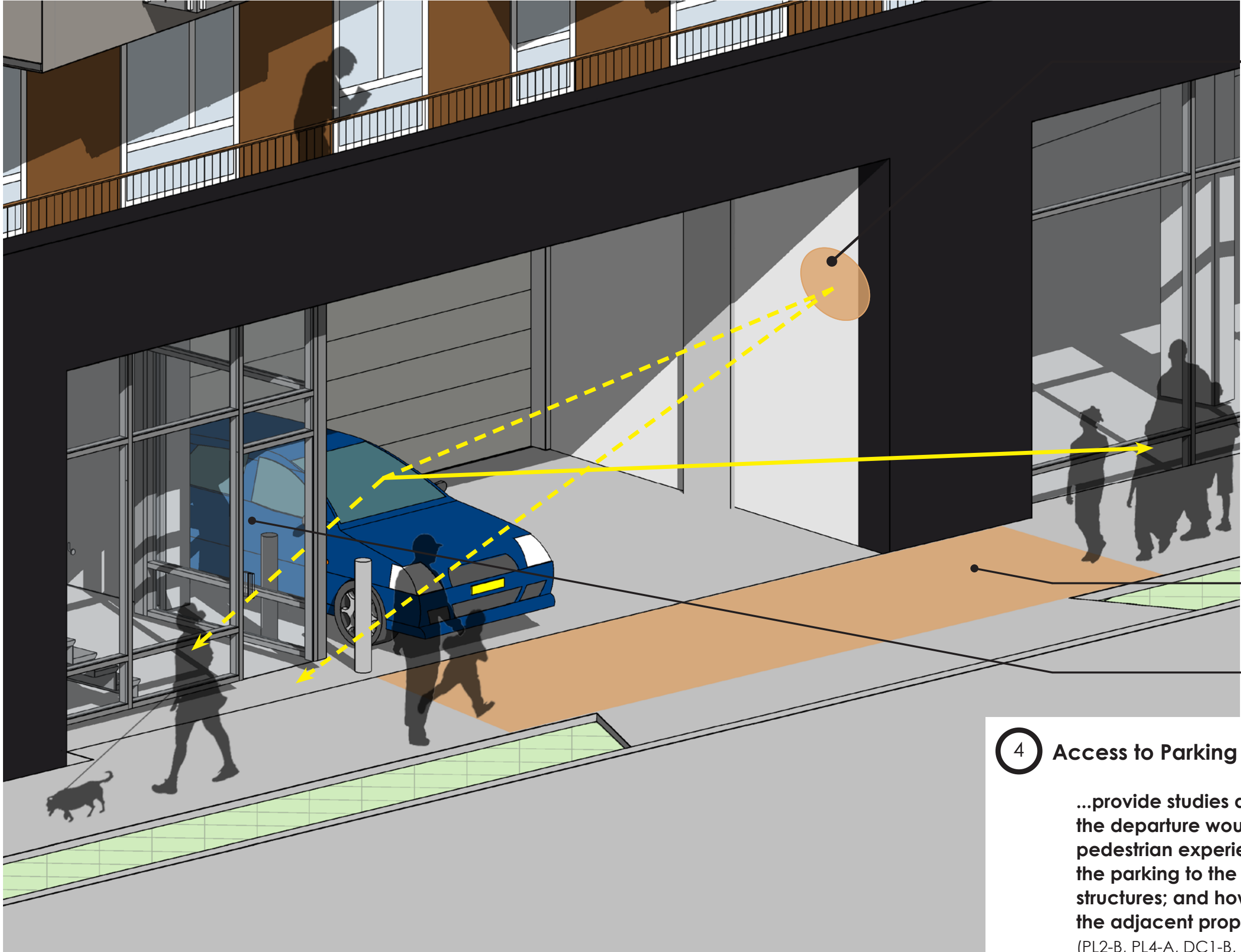




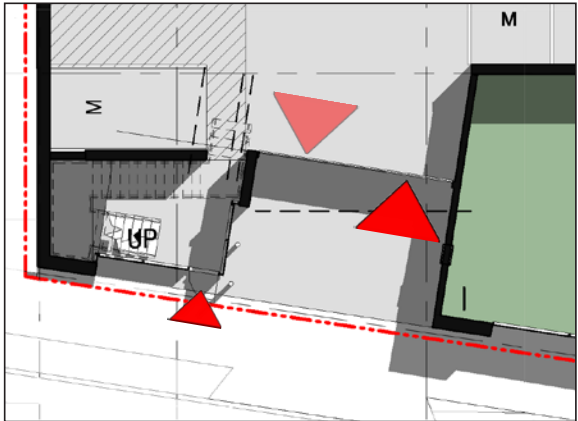
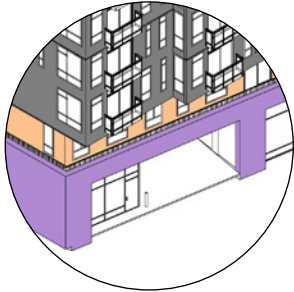
3

Entry:

...entry should be clearly articulated and reinforced through the overall massing and architectural composition. (PL3-A, PL4-A)



A concave mirror on this location expands what a driver exiting the garage can see.



Bumpy pavement treatment in this area gets the attention of both pedestrians and drivers.

Transparent corner also increases visibility for drivers exiting the garage and for pedestrians coming and going.

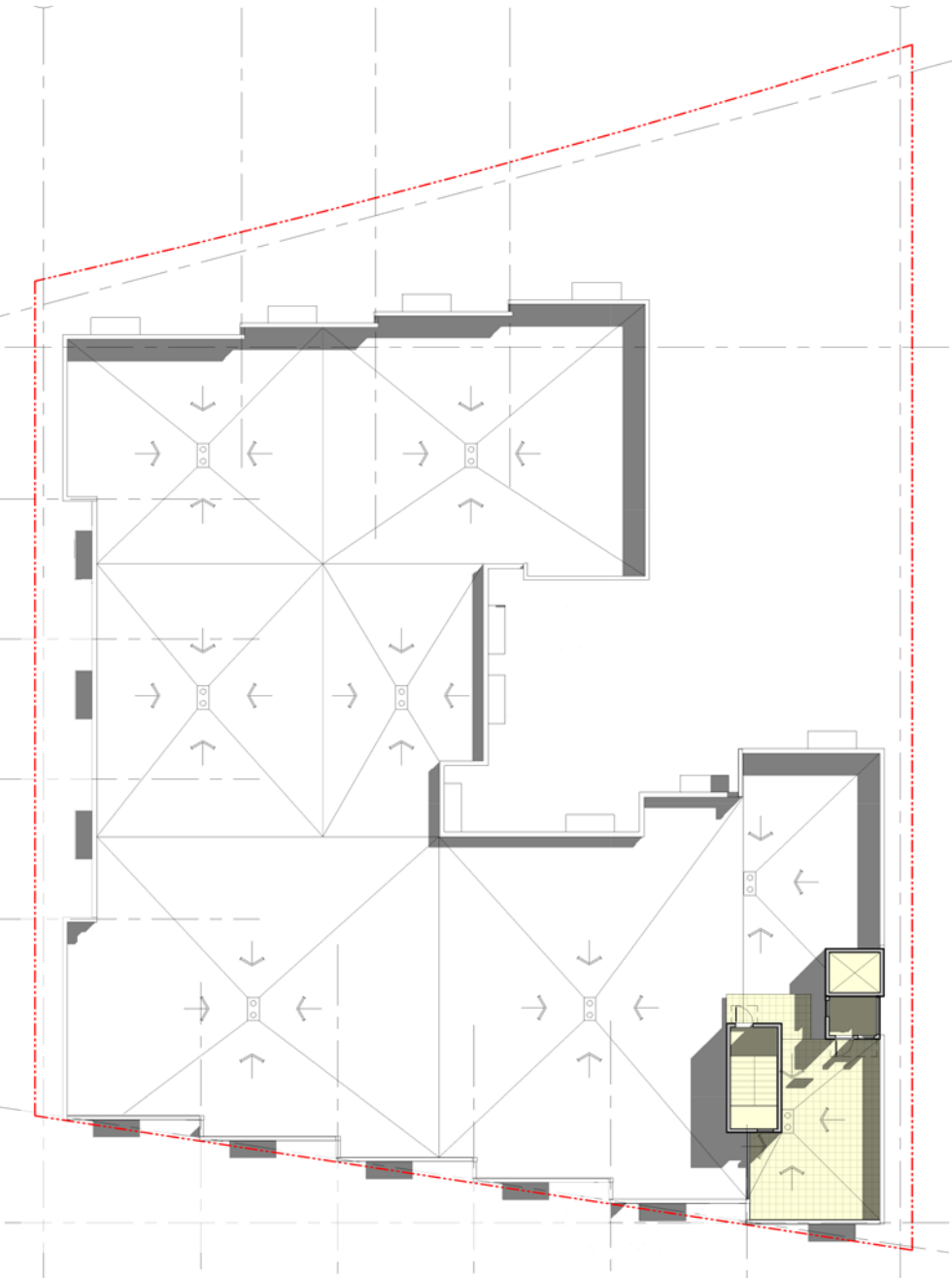
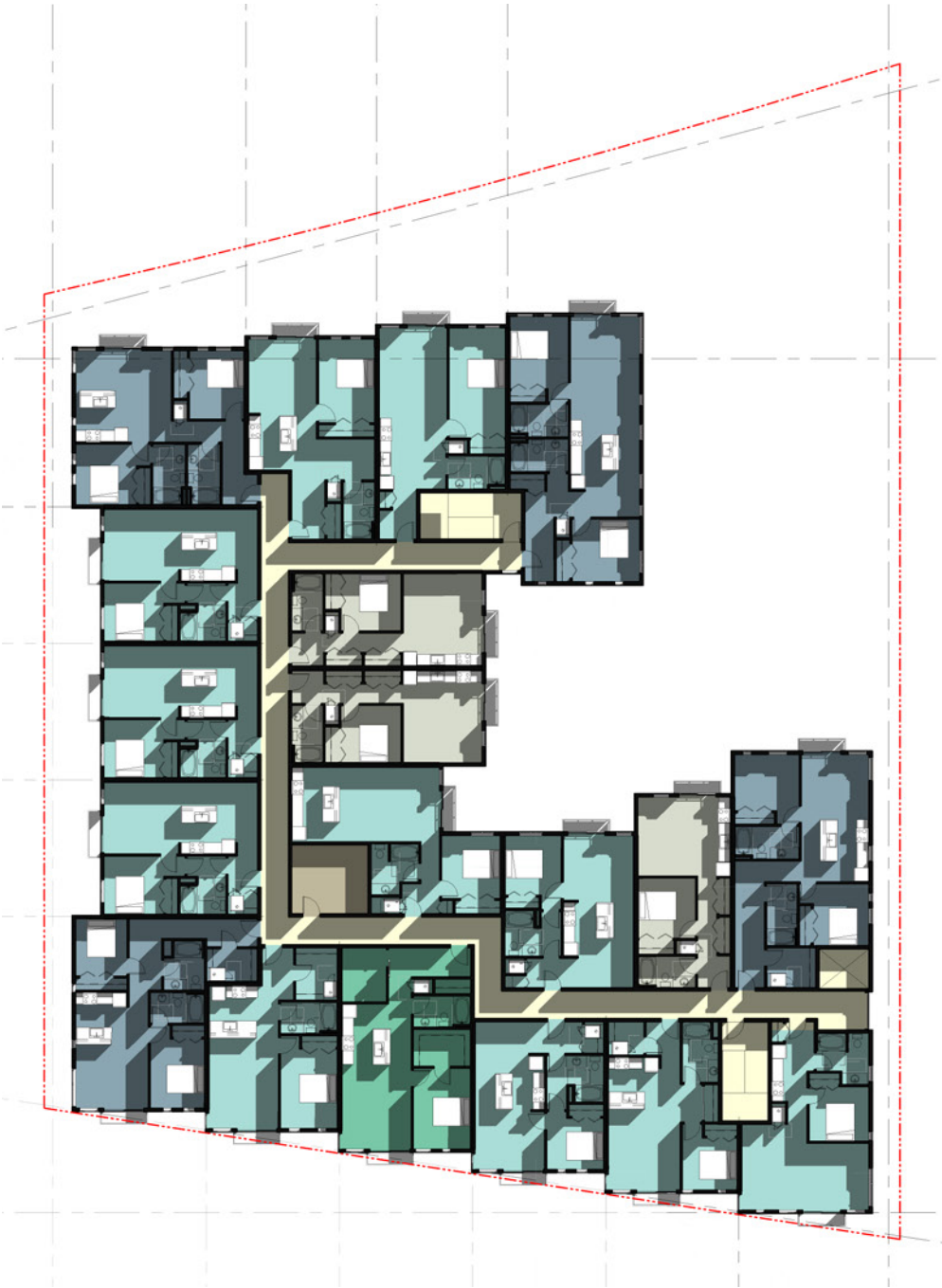
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...provide studies and graphics demonstrating how the departure would impact the streetscape and pedestrian experience; how retail users get from the parking to the retail uses, lobby, and adjacent structures; and how the parking entry interfaces with the adjacent property to the north.

(PL2-B, PL4-A, DC1-B, DC1-C)

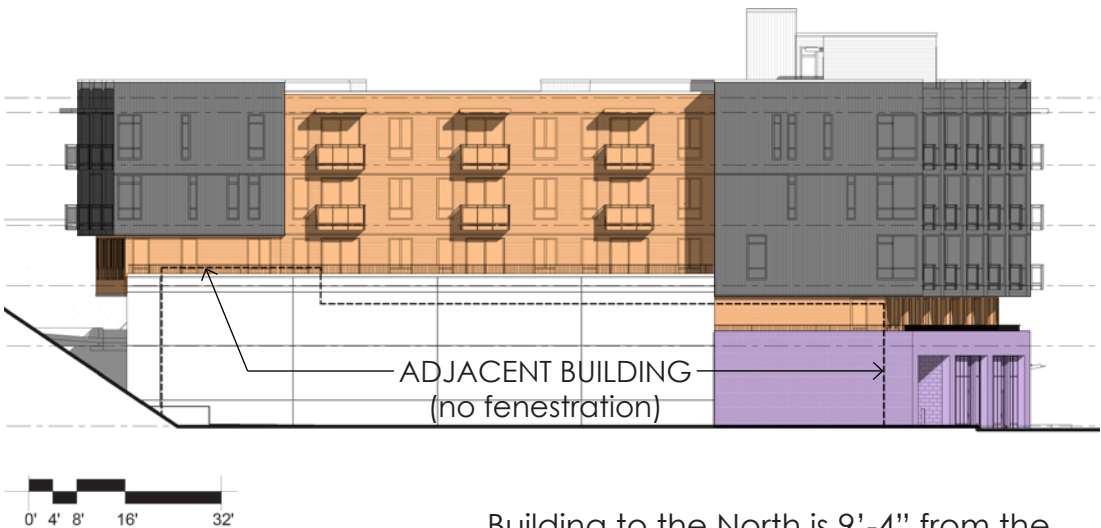






- ENTRY
- STUDIO
- 1-BEDROOM
- 1-BEDROOM W/ DEN
- 2-BEDROOM
- PARKING
- MEP
- COMMERCIAL
- CIRCULATION
- BIKES





Building to the North is 9'-4" from the PL, minimum. The closest unit on Level 3 is 5'-6" from the PL, leaving 14'-10" minimum between the two buildings.

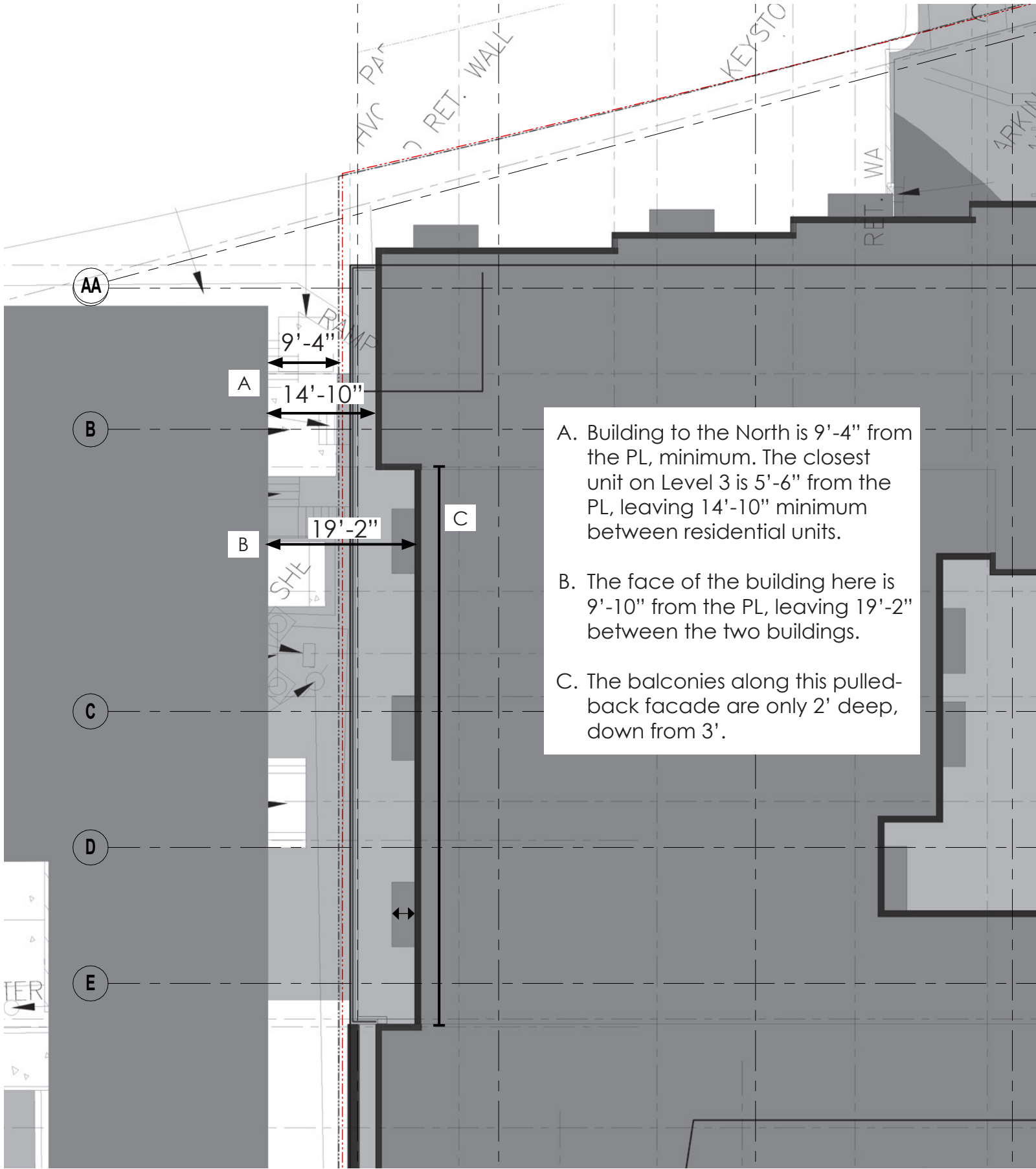
The face of the building here is 9'-10" from the PL, leaving 19'-2" between the two buildings.

The balconies along this pulled-back facade are only 2' deep, down from 3'.

3

PRIVACY ALONG NORTH FACADE

- BUILDING PULLS BACK FOR PRIVACY
- BALCONIES PULL BACK FOR PRIVACY





QUERCUS BICOLOR 'LONG'



EUONYMUS 'GREEN SPIRE'



ILEX CRENATA SKY PENCIL



MT VERNON LAUREL



LIRIOPE SILVERY SUNPROOF



ARP ROSEMARY



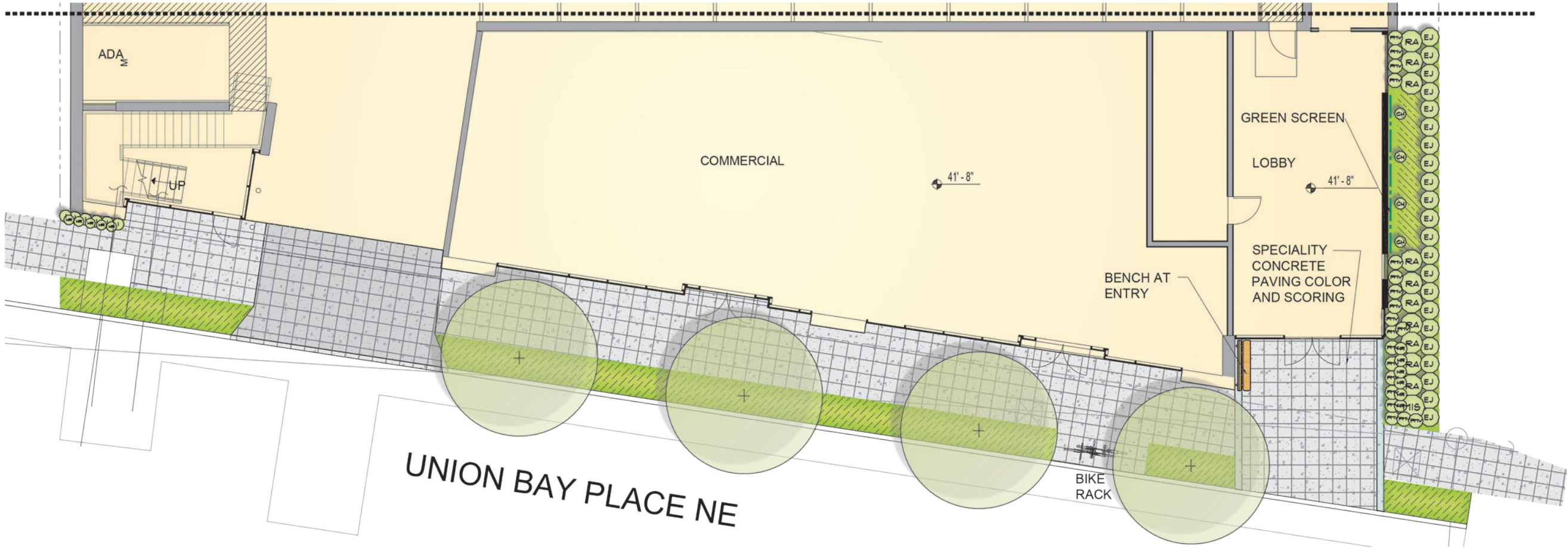
FIVE LEAF AKEBIA



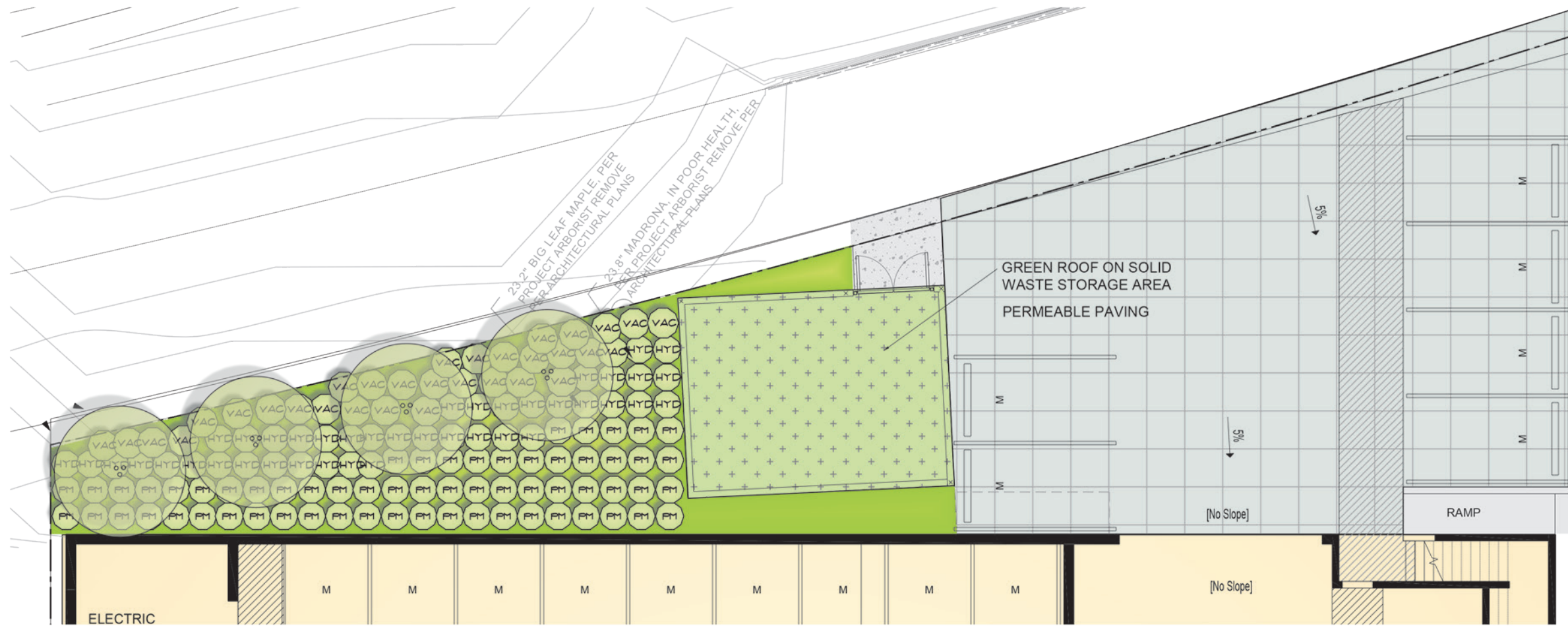
WHITE FLOWERING CLEMATIS



KINNIKINNICK



1 LEVEL 1 LANDSCAPE PLAN
NOT TO SCALE



1

NOT TO SCALE



VINE MAPLE MULTI-STEM



SWORD FERN



FIVE LEAF AKEBIA



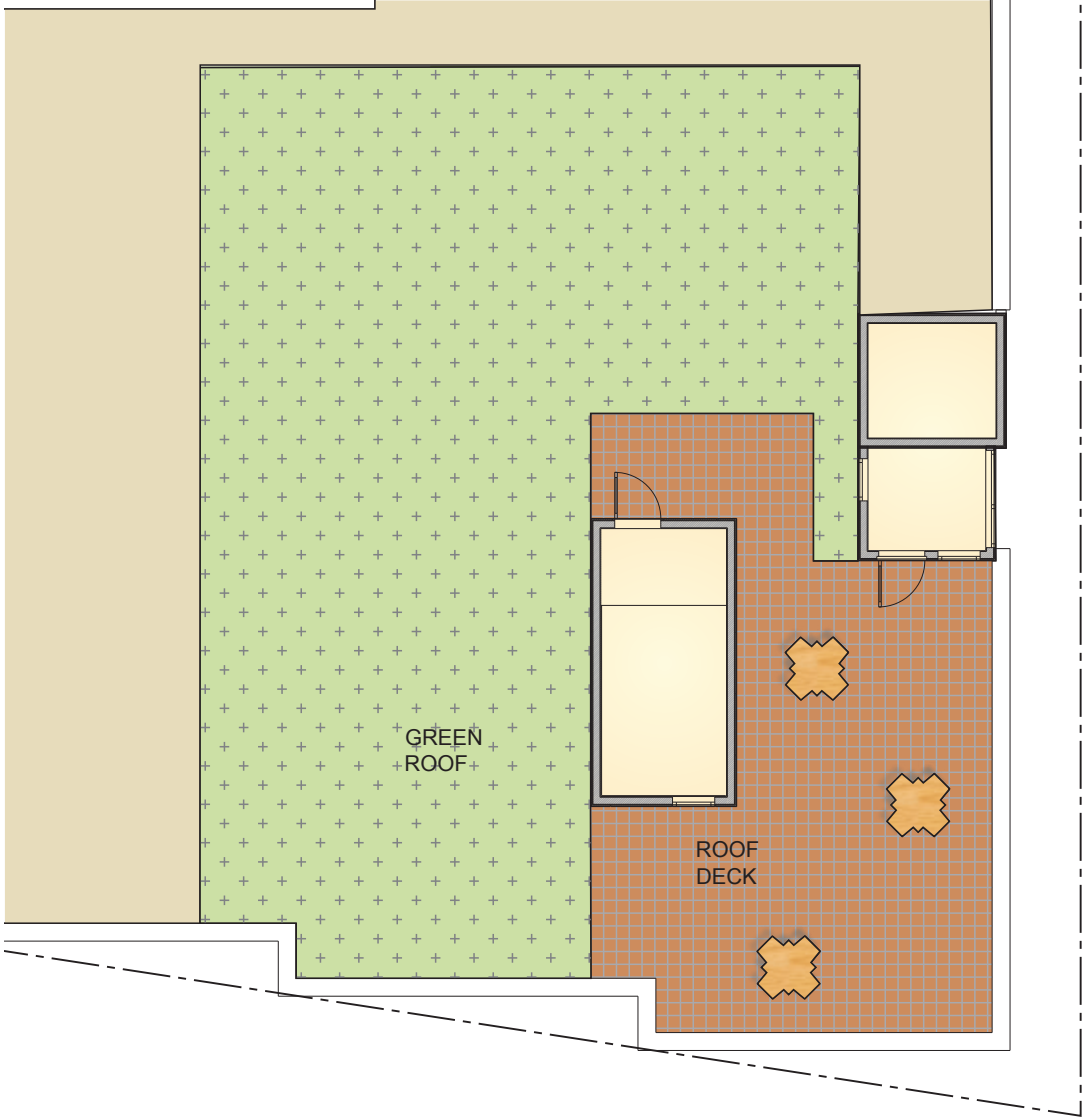
DWF. OAKLEAF HYDRANGEA



EVERGREEN HUCKLEBERRY



GREEN ROOF TRAY SYSTEM



GREEN ROOF TRAY SYSTEM

1 ROOF LEVEL LANDSCAPE PLAN



Courtyard Fire Pit



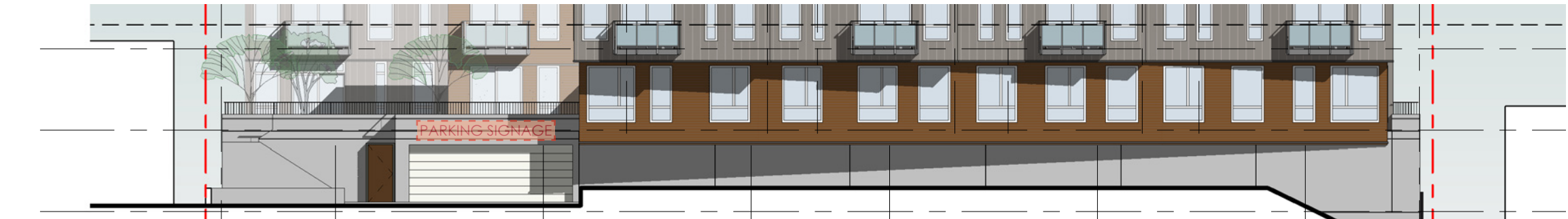
Union Bay Place Entry Bench



Courtyard Tables and Chairs



Level 1 - Union Bay Place



Level 2 - Union Bay Place



Flood Lights



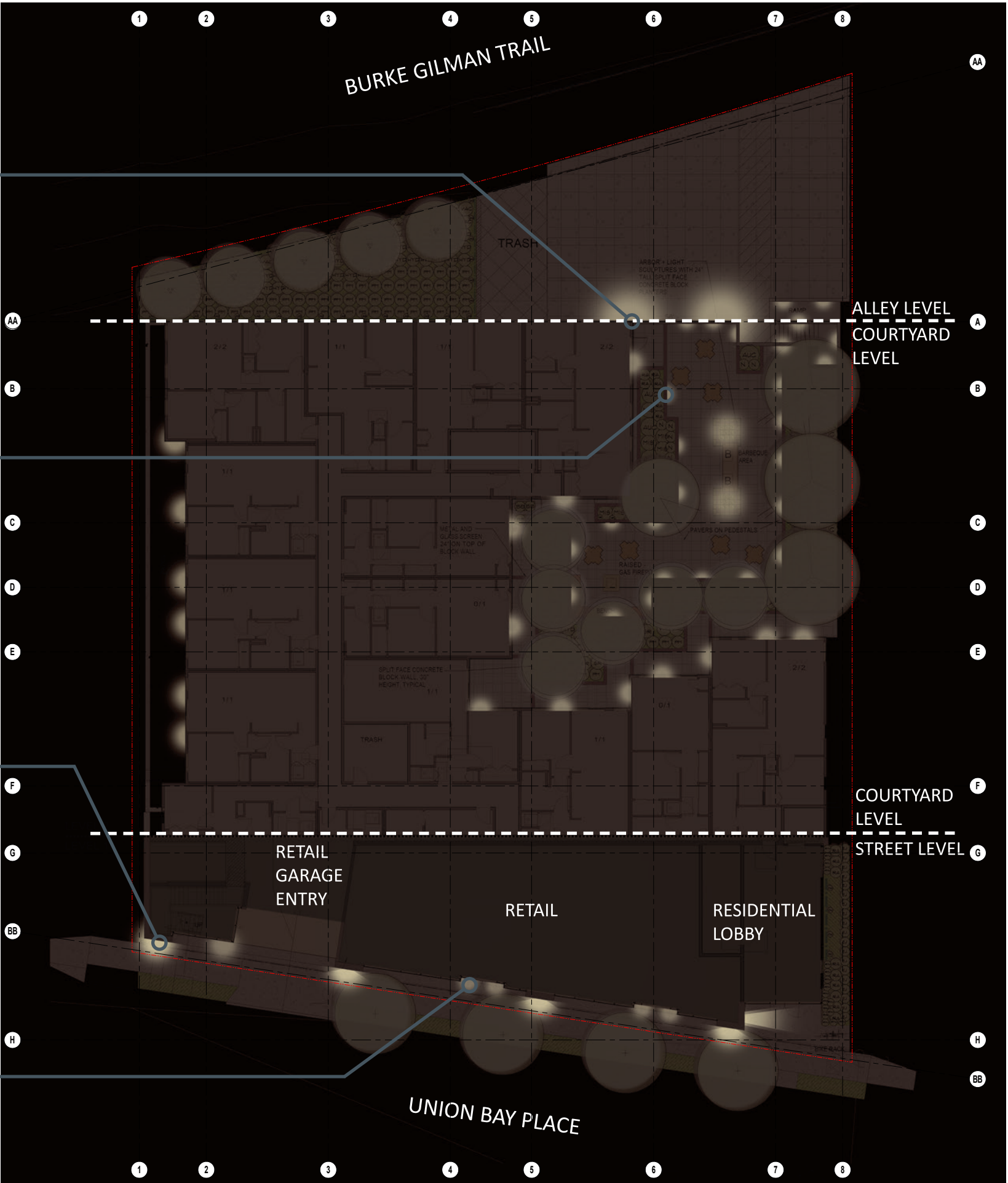
Step Lights



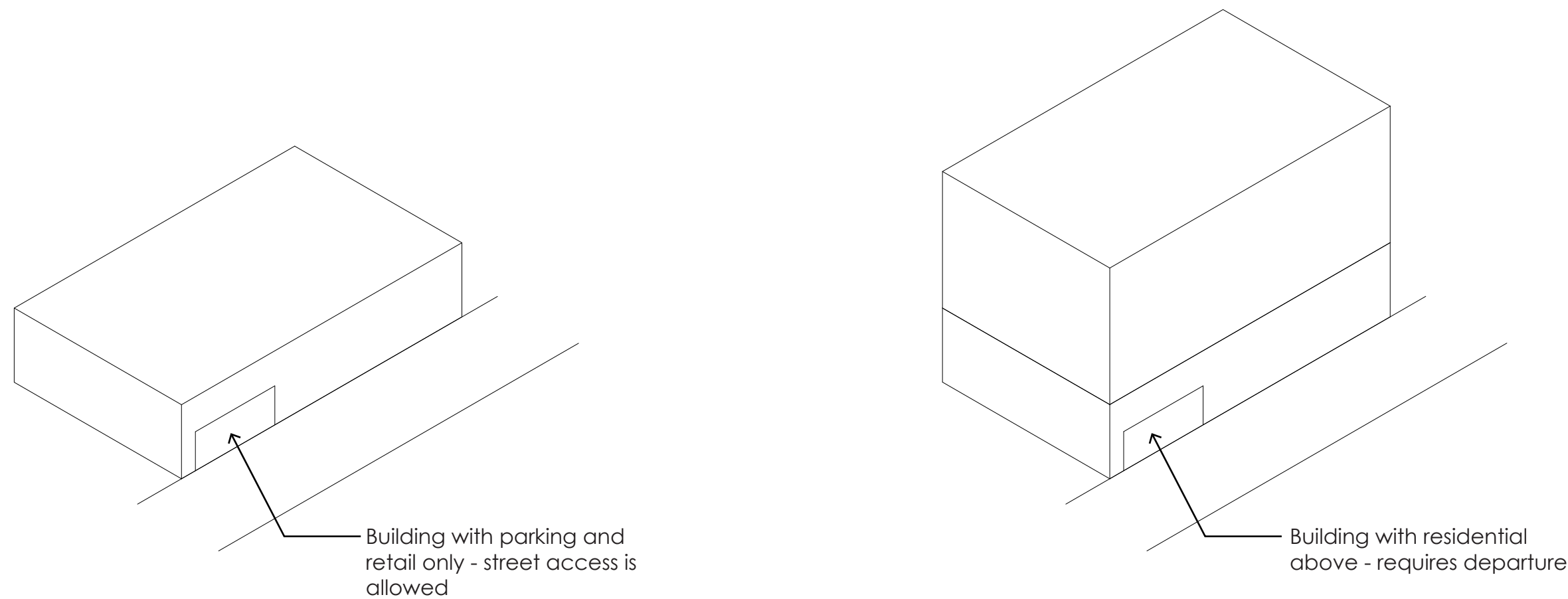
Wall-Mounted Brick Pier Lights

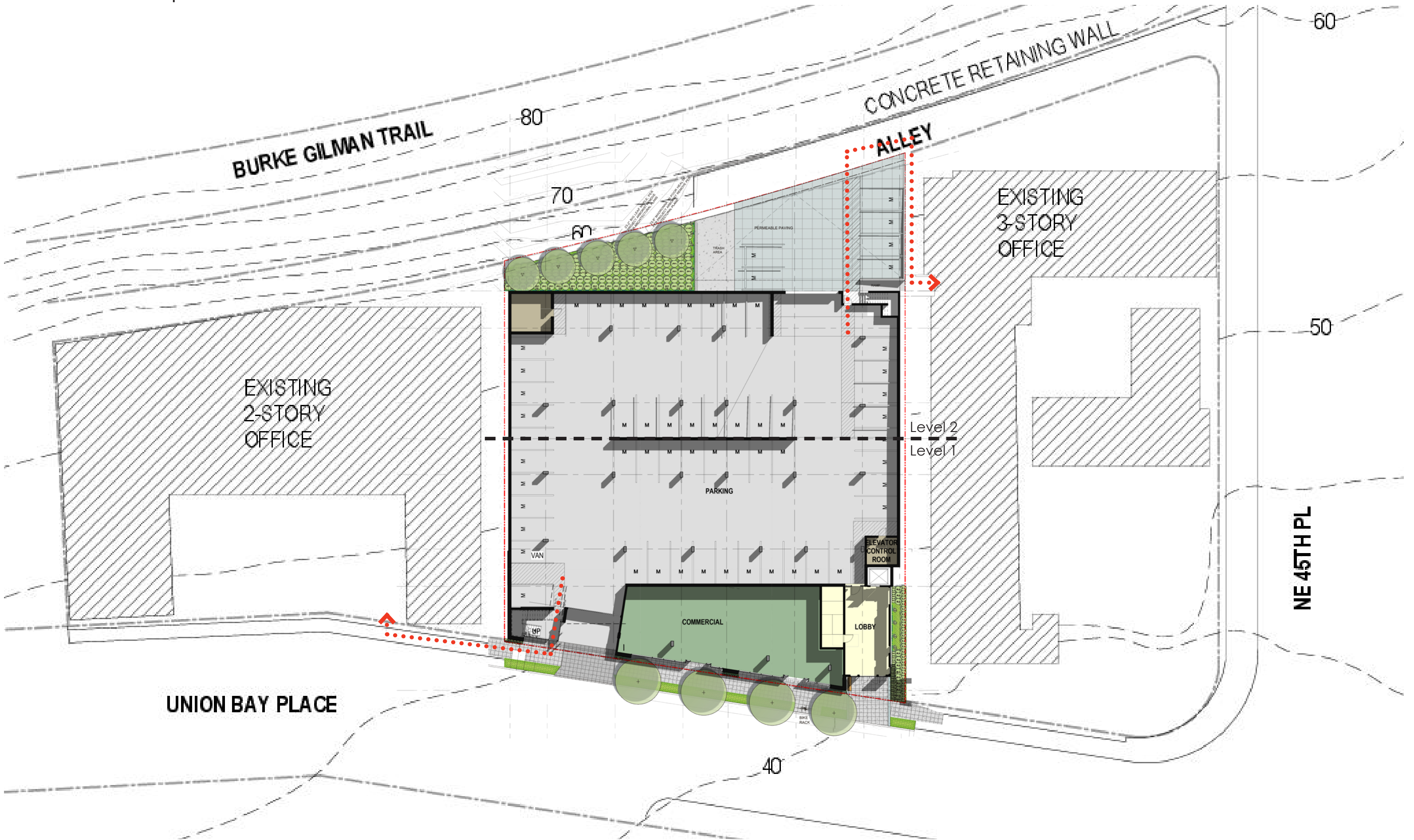


Recessed Soffit Lights



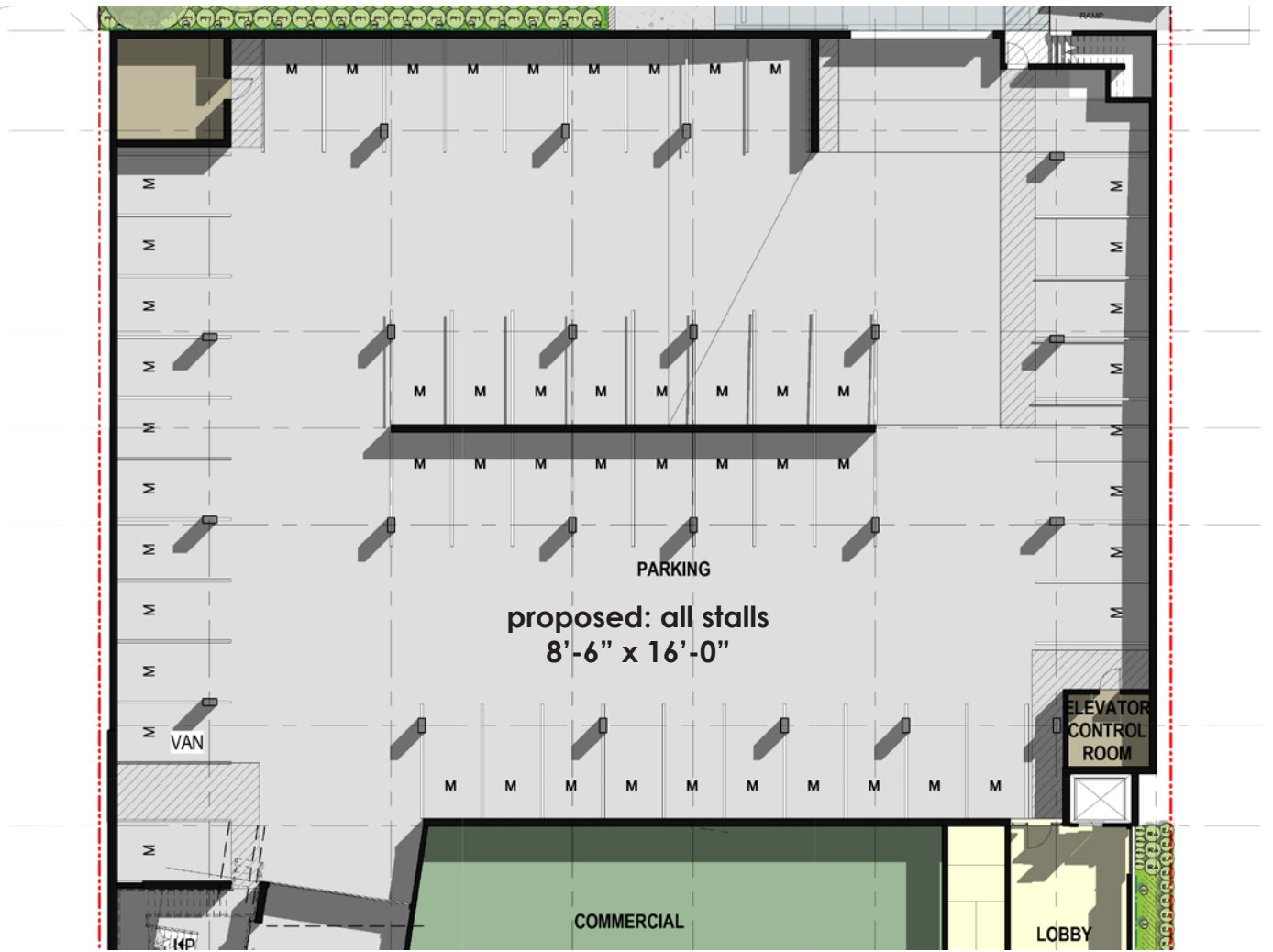
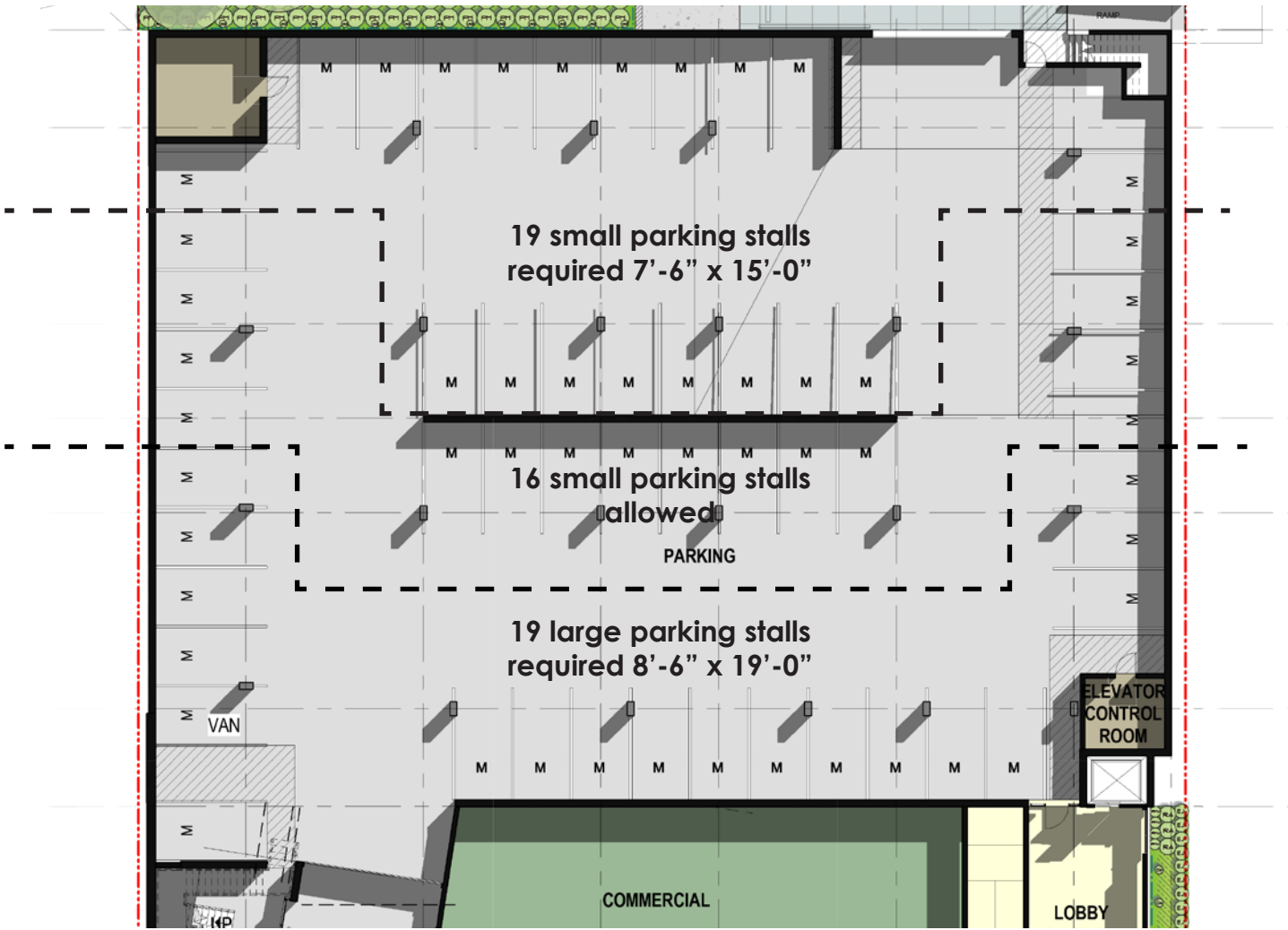
REQUIREMENT	JUSTIFICATION	RELATED DESIGN GUIDELINES
Structures in C zones with Residential uses shall meet requirements for NC zones, which requires alley access.	<p>The project is replacing existing parking for the adjacent medical office building to the North and the Lakeview building to the South.</p> <p>The parking has been divided into two disconnected floors to maximize efficiency in response to the peat settlement ECA. Two separate entries are required for the parking to function. Furthermore this cuts in half the amount of traffic and pedestrian conflicts on the streetside.</p> <p>Maintaining the access off of the street is critical for the project goals. It would be very confusing to existing and new visitors to the medical offices if the parking access was moved to an alley that is down the street, through the large busy intersection onto NE 45th Pl, not to be confused with NE 45th St which merges at the same intersection and has almost the same name, then go down the alley, which can also easily be confused with the adjacent street, to get to the parking garage. Instead the design team would prefer to keep the parking access where it currently is adjacent to the building which it will be serving. In this way the building configuration helps to solve the wayfinding problem and improve the visitor's experience. There are currently complaints from the community that visitors park up by the single family homes. Locating the garage entry off of Union Bay Place is an important measure to mitigate this problem.</p> <p>Locating the garage access off of Union Bay Place on the Western side of the site while the lobby is to the East will minimize pedestrian conflicts and maintain a contiguous commercial space in between.</p>	<p>PL2.D1 - Design as Wayfinding <i>"Use design features as a means of wayfinding wherever possible, and provide clear directional signage where needed."</i></p> <p>CS2 D.1 - Height, Bulk and Scale <i>"Existing Development and Zoning: Review the height, bulk, and scale of neighboring buildings as well as the scale of development anticipated by zoning for the area to determine an appropriate complement and/or transition. Note that existing buildings may or may not reflect the density allowed by zoning or anticipated by applicable policies."</i></p>







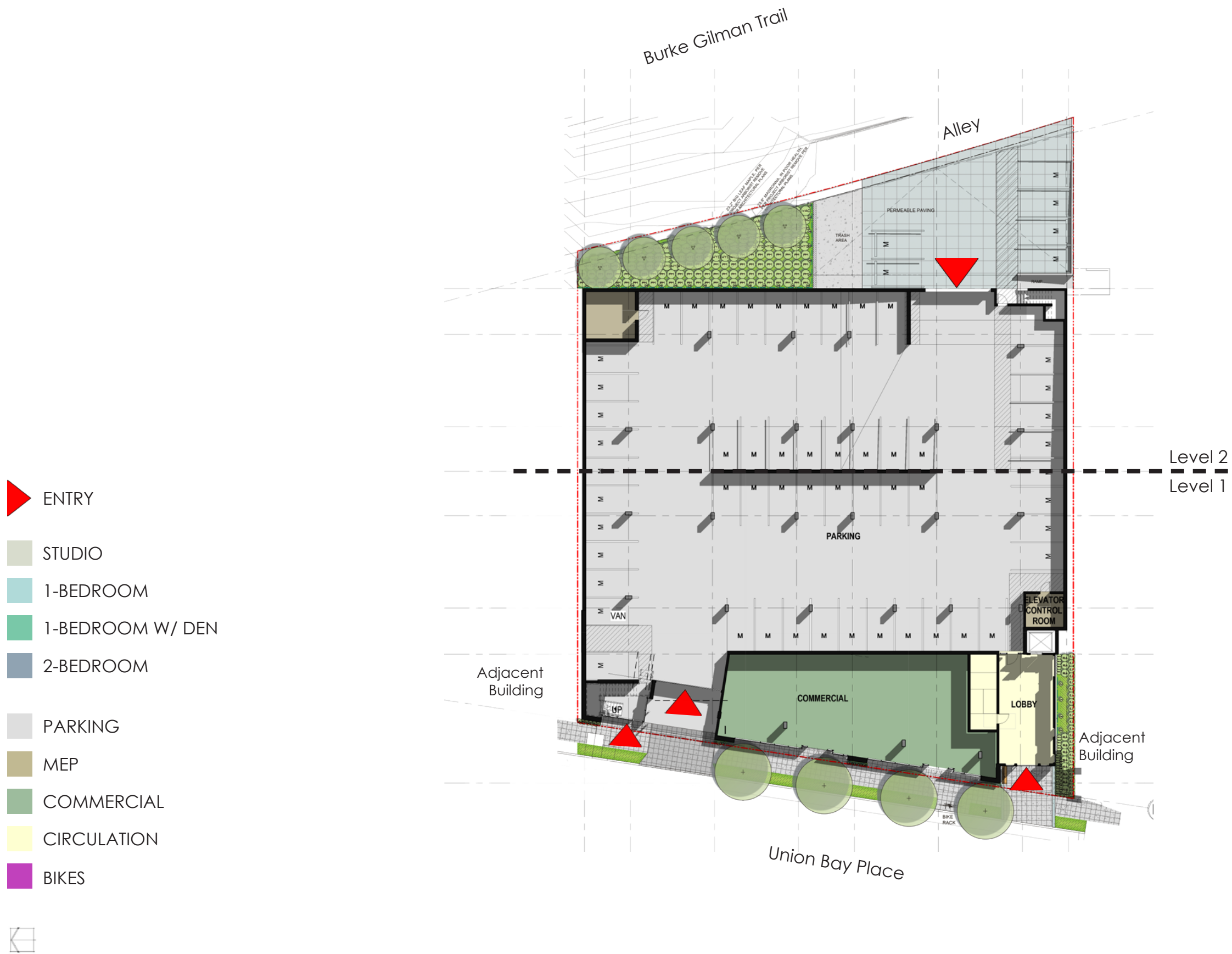
REQUIREMENT	JUSTIFICATION	RELATED DESIGN GUIDELINES
<p>SMC 23.54.030 B:</p> <p>Minimum of 35% small, max 65% small, minimum 35% large parking stalls.</p>	<p>We're proposing that all parking stalls be 8'6" wide by 16'-0" deep instead of a mix of small, medium and large stalls.</p> <p>Having all the stalls be a little larger than medium stalls and all the same size makes for more flexible parking especially for shared parking arrangements.</p>	<p>DC1 A.3:</p> <p><i>"Flexibility: Build in flexibility so the building can adapt over time to evolving needs..."</i></p>



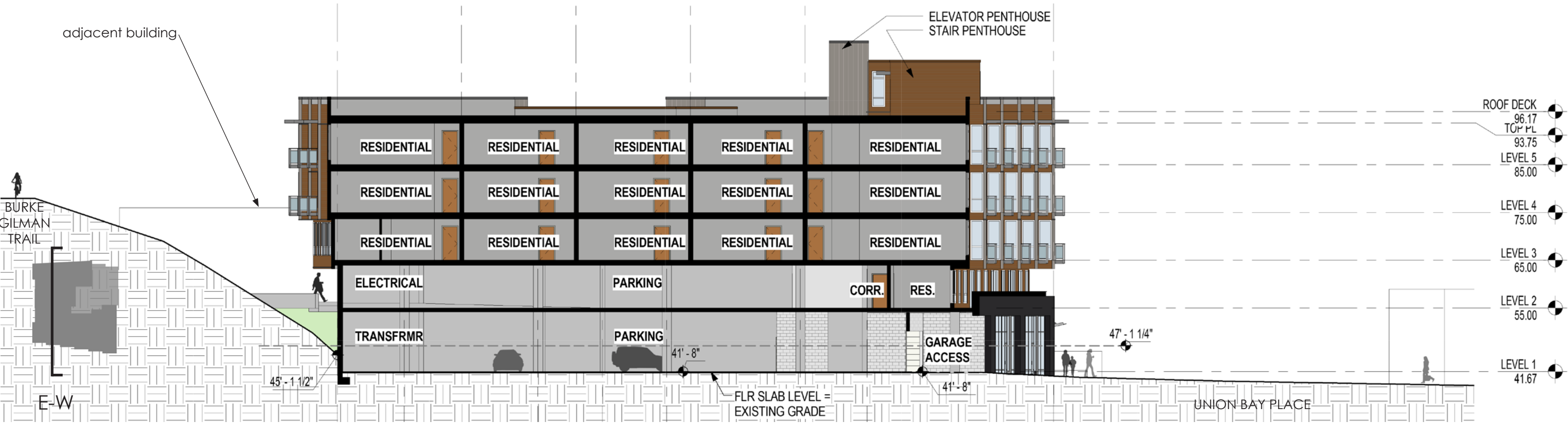
REQUIREMENT	JUSTIFICATION	RELATED DESIGN GUIDELINES
<p>SMC 23.54.030 D2:</p> <p>The minimum width of driveways for two way traffic shall be 22 feet and the maximum width shall be 25 feet.</p>	<p>Reducing the width of the driveway slows down vehicles moving across the sidewalk and reduces the aperture that the pedestrians need to walk past.</p> <p>This is intended to minimize conflicts between vehicles and pedestrians.</p>	<p>DC1 B.1.b:</p> <p>b. where driveways and cutb cuts are unavoidable, minimize the number and width as much as possible, and/or</p>

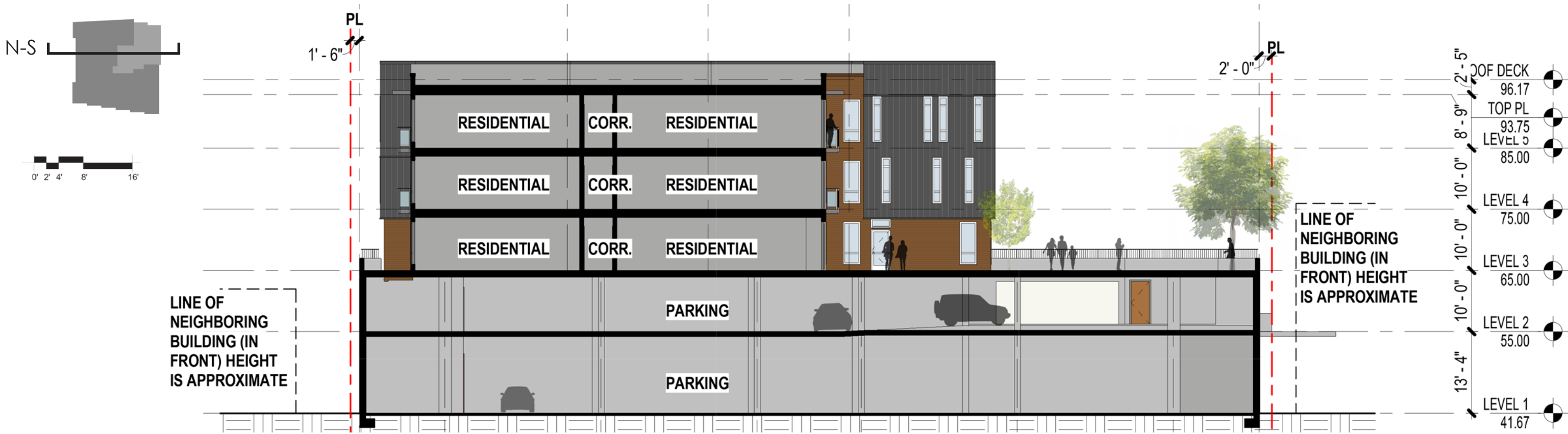


THANKS!



SECTIONS | E-W SECTION LOOKING SOUTH THROUGH GARAGE ENTRY





building across the street



DATE: January 6, 2015

TO: Robert McIntosh
Department of Planning & Development
700 5th Avenue, Suite 2000
PO Box 34019
Seattle, WA 98124-4019

FROM: Thomas Hemba, AIA
Encore Architects
1402 3rd Avenue, Suite 1000
Seattle, WA 98101

RE: DPD Project #3019495
Project Address: 4516 Union Bay Place NE
Response to MUP Cycle 1 Corrections – ECA SLIDE AND ECA PEAT

Dear Robert McIntosh;

We have reviewed your Correction Notice #1 dated November 4, 2015. We thank you for your thorough review and feedback on our design for the project and its ability to meet the city's environmental requirements. We have prepared the following responses shown here in [blue](#):

Corrections

1 SMC 25.09.330 C. The submitted geotechnical engineering report discusses the subject property in a preliminary fashion. The report regarding dewatering is not stamped by a civil engineer P.E. and raises multiple issues associated with dewatering the site. More information is needed to review this submittal relative to offsite impacts due to dewatering. Please submit a geotechnical report, in accordance with Director's Rule 18-2011, that at a minimum addresses dewatering and shoring issues for this application. Correct the plans accordingly to indicate conceptually the means of shoring and dewatering that will be required to construct this project.

[An updated geotechnical report is included in this document set.](#)

2 SMC 25.09.110. Based on GIS and the plans, a portion of the site is in a peat settlement-prone ECA. Provide a geotechnical report addendum with an evaluation of the peat settlement hazard and recommendations to mitigate the hazard. Revise the plans to demonstrate that the project has been designed to avoid impacts to the subject property and adjacent properties from peat settlement.

[See attached updated geotechnical report. Also see the conceptual layout and details of the foundation drainage system on sheet C1 "ME-01."](#)



DATE: January 6, 2015

TO: Louis Webster, AICP
Real Property Agent
Property and Acquisition Services
Planning and Development Division
Seattle Department of Parks and Recreation
800 Maynard Avenue South
Seattle, WA 98134

FROM: Thomas Hemba, AIA
Encore Architects
1402 3rd Avenue, Suite 1000
Seattle, WA 98101

RE: DPD Project #3022244
Project Address: 4516 Union Bay Place NE
Response to MUP Cycle 1 Corrections – PARKS AND RECREATION

Dear Louis Webster;

We have reviewed your Correction Notice #1 dated October 15, 2015. We thank you for your thorough review and feedback on our design for the project and its ability to meet the city's land use vision. We have prepared the following responses shown here highlighted in [blue](#):

Corrections

I am the reviewer for parks for the above referenced project. Park's concern is the trail and I see no impacts to the trail on the plans. I do see the trail and the alley lumped together as ROW. The alley is ROW and the trail is not. Please note this on the plans. Any impact to the trail will require a permit from Parks and Recreation. Thank you for your attention to this small matter.

[Your assessment that the proposed project will not impact the Burke Gilman Trail is correct. The significant change in grade \(in excess of 20' vertically\) precludes the building from physically affecting the trail area. Plans are updated to reflect different designations for the Burke Gilman Trail, shown as a park, and the alley, shown as Right of Way \(ROW\).](#)

DATE: January 6, 2015

TO: John Shaw
Department of Planning & Development
700 5th Avenue, Suite 2000
PO Box 34019
Seattle, WA 98124-4019

FROM: Thomas Hemba, AIA
Encore Architects
1402 3rd Avenue, Suite 1000
Seattle, WA 98101

RE: DPD Project #3019495
Project Address: 4516 Union Bay Place NE
Response to MUP Cycle 1 Corrections – TRANSPORTATION

Dear John Shaw;

We have reviewed your Correction Notice #1 dated December 17, 2015. We thank you for your thorough review and feedback on our design for the project and its ability to meet the city's land use requirements. We have prepared the following responses shown here in [blue](#):

Corrections

1 Page 3, Section 3.1: Is the reference to transit stops on University Way and NE 50th Street meant to refer to transit stops on NE 45th Street?

[See transportation report.](#)

2 Please provide a sight distance analysis at the intersection of the alley and NE 45th Place, particularly indicating the extent to which the Burke Gilman Trail overpass might limit visibility.

[See transportation report.](#)

DATE: January 6, 2015

TO: Katy Haima
Department of Planning & Development
700 5th Avenue, Suite 2000
PO Box 34019
Seattle, WA 98124-4019

FROM: Thomas Hemba, AIA
Encore Architects
1402 3rd Avenue, Suite 1000
Seattle, WA 98101

RE: DPD Project #3019495
Project Address: 4516 Union Bay Place NE
Response to MUP Cycle 1 Corrections – LAND USE

Dear Katy Haima;

We have reviewed your Correction Notice #1 dated December 15, 2015. We thank you for your thorough review and feedback on our design for the project and its ability to meet the city's land use vision. We have prepared the following responses shown here highlighted in blue:

Corrections

1 Massing and Context Response. At EDG, the Board supported the massing presented in Option 3 as an appropriate response to site characteristics and context, and encouraged the applicant to further explore how the programming of the building can be expressed in the massing composition.

The design response indicates that the modulation has been changed to wider bays to fit the module of the units; however, the site plan and elevations depict the unit sizes as not corresponding to the modulation, which results in an awkward pattern of balconies and fenestration. Revise the internal program and/or modules to resolve the overall composition of the façade in relation to the programming and the relationship to the base.

[Façade modulation and units have been updated to match bays. See Elevations on the A3 series sheets and Units on sheet A2.03.](#)

2 Massing, design concept. At EDG, the Board supported the design concept of a one-story base and floating three-story mass above, and the clear articulation of a base, middle and top.

The concept of floating box has been diminished since EDG, and the east elevation no longer reads as 3 separate pieces. Continue to explore how each of the pieces be refined to reinforce the cohesive architectural concept.

The East elevation has been changed so that the two public sides, the street side and the trail side, both share a similar language of large bays which correspond to individual unit modules which step out with the splayed property lines. Filling in the spaces in between at the 2nd story along the street side and the 3rd story on the trail side as well as the middle zone of the building is another layer which makes up the secondary residential massing.

The gasket at the residential level was successful at EDG due to the proportion and rhythm of glazing. Increase the glazing at this level to further distinguish it from the mass above and below, and to enhance the appearance of the upper massing "floating" above the base.

At EDG, the upper mass was shown as white, further reinforcing the concept of the floating box. The dark beige metal siding does not appear to achieve the design intent, as it makes the mass appear heavy and changes the relationship with the base. Consider how the detailing and articulation of the materials for the upper mass can strengthen the overall composition by distinguishing the design languages used at the base and the top. The top mass should appear light and airy. To this extent, consider removing or revising the cornice and eyebrows above the residential units, and increasing the amount of glazing.

The goal of the façade diagram is to articulate a clear one-story retail base and the stepped bay massing above. The diagram has evolved now to include the whole residential building so that there is a single in-between zone or gasket layer that works both vertically and horizontally. This then carries a series of blocky stepped bays that mark the public facades. We feel that it is more consistent for the whole building diagram to treat the "upper mass" as bays that have stepped out rather than a "floating mass."

The second floor is pulled back and the siding is a simple horizontal lap siding. The windows are spaced regularly and the size is based on the width of a door which is what was shown in the EDG. The intent is to make a quiet joint-like layer that doesn't draw attention. In this way the concept is achieved without overdoing the use of windows in these small, shallow residential units. The energy code allows us a finite quota of windows which need to be used at the living spaces throughout the building.

The upper mass is to read as a blocky solid that has some substance to it. The forms are to be clearly and consistently expressed to keep this relatively small building from becoming visually complicated. There are no cornices planned. The line on top is a typical minimalist metal flashing coping. The canopies over the windows are required for weather protection over the sliding doors and work well to top off the stacks of decks.

3 Design Review, Base & Entry. At EDG, the Board supported the location of the lobby entry, and expressed that the entry should be clearly articulated and reinforced through the overall massing and architectural composition.

Please demonstrate how the proposal achieves this guidance. The entry should create a break in the massing and/or design language at street level, as shown at EDG. The response noted that the proportions have changed, however, the change in module size from the commercial bays and residential entry is not substantial enough to read as an intentional design move. The

proportions should be expressed in manner that the contrast is easily discernible to reinforce the prominence of the residential entry.

The retail bar has been pulled back and the entry has been connected to the level 2 residential "gasket" plane so that the lobby is part of a larger form that occupies the corner. A canopy is located over the door and on center with the decks and windows above to visually link them together. Being the only break in the retail bar clearly separates it as something of special importance and being recessed provides some extra space in this narrow sidewalk to make a small gathering spot for residents to chat with their neighbors.

In addition, it is unclear why the entry has been recessed. The residential entry should be prominent, and express a different character than the retail entry for wayfinding purposes. The interruption should appear intentional, and relate to the overall massing and architectural expression. Centering the entry module on the corner is beginning to strengthen this relationship. Continue to refine the massing and materials to create a prominent entry that appears conceptually related to the residential units above.

Consider revising the corner massing to pull the bay all the way to the ground level, removing the brick storefront, and instead utilize a more residential expression that anchors the corner and ties in to the residential uses above.

We would like to maintain the clarity of the serrated 3-story mass, but feel that we have found another similar strategy that anchors the corner and expresses the entry. Also see response above.

Alternatively, bring the entrance forward to the street, and employ elements from the design language from the middle of upper mass down to highlight the entry and visually connect it to the residential above.

See above response.

4 Materials and Composition. At EDG, the Board requested a demonstration of how the materials respond to the design concept for each façade. Please include graphics, including sketches and diagrams, that explain the intent of the material application, and how it responds to the architectural forms. Currently, the materials appear to be used as wallpaper, and do not demonstrate a judicious application which reinforces to the architectural concept and massing moves. The use of color and materials should reinforce the underlying composition.

Consider highlighting the form and depth of each jog by wrapping each corner with transparency, or employing a unique material application.

There are three materials and fenestration/pattern strategies:

1. Base = brick / dark brown: Solid, textural, earthy material that relates well to the ground plane and pedestrians. At the columns and around the corner it will return so that it is not a one-dimensional veneer and to help give it some mass. Fenestration: Arranged in three equal bays with equal size columns.

2. Upper mass = Vertical Metal Panel / dark grey: These are meant to read as chunky blocks. Fenestration: Larger openings are provided at the living spaces and capture the corners. Decks are stacked and capped off by a canopy which is required for weather protection (by the envelope consultant and good practice) over the sliding doors. Smaller more varied windows are distributed in between in a more playful, random pattern.
3. Joint layer / middle zone = stained manufactured lap siding / wood tone stain: This is a background material. The horizontal lines work with the orientation of the associated forms. Fenestration: Regular repetition of a single window module that is based on the door width. The minimal variation is to create a consistent band or field that is meant to be uniform.

5 Design Review, Secondary features. AT EDG, the Board provided the following guidance:

2.a. Demonstrate how the materials respond to the design concept, for each façade. (DC2-B)

2.e Demonstrate how the units relate to the massing and architectural composition. (DC2-A, DC2-B)

Each bay is exactly one unit. This is true for the stepped façade along both the street side and the trail side.

The fenestration pattern on the east façade should be refined to demonstrate an organizing principle. Even if the pattern is "random", it should appear intentional, and have a hierarchy to the organization.

The east façade has been redesigned to create a stepped façade to give a consistent treatment the trail side and the street side taking advantage of the splayed property lines and associated context. See notes on fenestration in #4 above and see A3 series sheets.

Consider using the large windows consistently across the façade, and then adding variation/play with the arrangement of smaller windows.

Agreed. See #4 above.

Please include a diagram that explains the design intent of the fenestration pattern. How does it relate to the massing moves? How do the "eyebrows" above the windows add to the façade composition?

See attached diagram. See #4 above for discussion of the fenestration strategy.

6 Design Review, Location of Vehicular Entry, and Base Element. It appears that information regarding the design of the garage entry door is missing.

At EDG, the garage entry was shown as the northernmost "module" at street level. It appears that the design of the street-level facade has been revised, and is now dictated by the arrangement of parking and garage entry. The consistent rhythm of bays from EDG has been

lost--but without a rationale of improving wayfinding or, architectural composition-which does not appear to better meet the intent of the design guidelines.

The consistent rhythm of the bays has been restored so that we now have three equal modules.

It is unclear as to why the north end of the façade at street-level is inconsistent with the retail bar in regards to proportion and transparency, and why the stairs are located along the streetscape.

Pull the stair door closer to the street, and expand the transparency at the stairs to match the commercial expression at the stair entry.

The stair has been moved closer to the street and the language of the storefront will be in keeping with the retail. The transparency has been maximized. The pedestrian garage entry and stair is located on the sidewalk to provide effective wayfinding as this garage will be used by the patrons for the medical offices next door and for the retail spaces and they can't travel through the residential lobby due to security concerns.

Consider minimizing the impact of the garage by including a one-way garage access on Union Bay Place, thus reducing the width of the garage entrance.

The garage used to be two and a half levels that spiraled down and required excavation. Due to a high artesian water table in this location and a peat settlement area ECA we are not able to excavate. The garage is now reduced to two disconnected levels that are arranged efficiently enough where enough parking is maintained to meet the project development requirements. The parking will need two-way access to be viable. That said, we are proposing to reduce the width of the driveway from the required 22' down to 20' and are asking for a departure for this.

7 Design Review, Design concept. The regular rhythm of the jog on the upper massing is the strongest component of the design concept and architectural composition, but is lost though the material application and expression of each portion of the massing.

Consider rearranging the units to locate balconies consistently at each corner to emphasize the bays and modulation. Alternatively, consider removing the decks from this façade and strengthening the expression of the vertical modulation with the fenestration pattern.

Demonstrate how the design concept is being reinforced through material application and secondary features.

The balconies have been updated to a consistent pattern centered on each serration. The regular pattern of the balconies reinforces both the residential nature of the upper mass and the serrated pattern.

8 North façade, balconies and patio. At EDG, the Board expressed concern over the location of the balconies as related to potential future privacy concerns. Please include dimensions from the building and balconies to the property line, and demonstrate how privacy will be addressed.

Dimensions have been included. The depth and size of the balconies along the north wall have been reduced so there is now more separation to the property line. See sheet A2.04

9 Design Review, Departures. A reminder, the rationale for departures needs to demonstrate how the resulting massing/design is better meeting the intent of the design guidelines. This will include how the proposed design is interacting with the public realm, and the impacts on the pedestrian experience. Please see guidance under item 6.

Acknowledged.

10 Administrative Conditional Use, Residential uses in C2 zones. SMC 23.47A.006.A.3 lists the criteria for permitting residential uses in C2 zones. Additional information is necessary to determine if criteria 2 is being met.

Please provide a brief inventory of the nearby uses and businesses, including any potential sources of noise or odor.

Along Union Bay Place:

GR Home: store open M-F, 9 am – 5 pm

Seattle Languages International: school appears to be open M-F, 9 am – 6 pm

Dentist office: open M-F, 10 am – 4 pm, plus early open at 8 on Wednesdays, and late closing at 7 pm on Thursdays.

CrossFit Deliverance: gym open M-F 6 am – 8:30 pm, and Sat 9 – 11 am.

FedEx: store open M-F 7 am – 11 pm, Sat & Sun 9 am – 9 pm.

Tully's coffee: café open M-F 5:30 am – 7 pm. Sat & Sun 6 am – 7 pm.

Burgermaster: restaurant open Mon-Sat 6:30 am – 10 pm, Sun 7:30 – 10 pm.

Blink Tattoo Removal: Medical office open M-F 7:30 am – 5 pm.

None of these uses appear to cause any negative noise or odor to the site.

Please demonstrate that the outdoor recreation uses across 45th Street (which are prohibited in NC3 zones) are not impacting the project. This includes lighting and potential noise sources. You may want to include perspectives from the site of the field, times the lights are on, etc.

The online UW schedule for the fields shows that intramural games will be occurring starting between 4 and 5 pm. They appear to be year-round. It is unlikely that games will extend past 10 p.m.

11 FYI MUP Conditions, CMP. The MUP decision will likely include a condition requiring a Construction Management Plan (CMP), approved by Seattle Department of Transportation (prior to SDOTPermits@seattle.gov any construction permits). You may send the CMP to for

review and approval prior to issuance of this permit. For the CMP Standard Element Guide see <http://www.seattle.gov/transportation/CMP.htm>

Acknowledged.

12 Design Review, Recommendation Packets. When the Recommendation meeting is scheduled, please include the following information in the packets:

- Pedestrian level perspectives showing all areas of the building facades at the streets and alley.
- Include a view from the north of the building.
- Lighting plan showing light fixture locations and light fixture designs.
- Conceptual signage plan.
- Photo of the colors and materials board, including any decorative metal or screen materials.
- Shadow studies as shown in the MUP plan set. Include adjacent structures and rights-of-way.
- Colored and shadowed elevations with a clearly readable materials key.
- Landscape plan including location, size, and species, as well as proposed hardscape locations and materials.
- Sections showing the building, adjacent sites, and sites across the alley and streets.
- Graphics and narrative clearly demonstrating the proposed design concept.
- Dimensioned floor plans showing the property line, as well as the outline of adjacent buildings across the street/alley. The floor plans should dimension the depth of modulation/articulation.
- Chart and diagrams demonstrating proposed departures (code section, code requirement, proposed departure dimensions/specifics, proposed rationale for how departure better meets intent of Design Review Guidelines).

Acknowledged.

DATE: January 6, 2015

TO: Maria Cruz
Department of Planning & Development
700 5th Avenue, Suite 2000
PO Box 34019
Seattle, WA 98124-4019

FROM: Thomas Hemba, AIA
Encore Architects
1402 3rd Avenue, Suite 1000
Seattle, WA 98101

RE: DPD Project #3019495
Project Address: 4516 Union Bay Place NE
Response to MUP Cycle 1 Corrections – ZONING

Dear Maria Cruz;

We have reviewed your Correction Notice #1 dated December 14, 2015. We thank you for your thorough review and feedback on our design for the project and its ability to meet the city's zoning requirements. We have prepared the following responses shown here in [blue](#):

Corrections

1 General Plan Set Corrections.

Floor Plan of Proposed Residential Units. Please provide a floor plan of each residential unit complying with the configuration of dwelling units in 23.42.048.

[The plans on sheet A2.02, A2.03 and A2.04 have been updated to include unit plans that comply with the code requirements in SMC 23.42.048.](#)

Permitted Use. Please specify and indicate on sheet G0.00 and sheet A2.01 which use in Table A 23.47A.004 is being proposed in the commercial spaces on Level 1.

[Sheets G0.00 and A2.01 have been updated to reflect the proposed use in the commercial space as an eating and drinking establishment. This use is permitted according to SMC 23.47A.004 Table A.C.2.](#)

Site Plan. Please clarify the location of structures, walls and fences on the site plan. It may be helpful to remove the interior floor plans from the site plan to help distinguish between interior and exterior spaces.

[The site plan on sheet A1.01 has been updated to clarify the location of the structures, walls and fences.](#)

South Elevation. Please clarify the feature/shading shown on the right side of the South Elevation. If a structure is located in this location, it appears to cross into the public right of way.

[This shading depicted in the South Elevation showed the existing grade at the hillside beyond our property. It has been removed from the black and white view for clarity. It is was not a structure.](#)

2 Street-Level Development Standards.

Blank Facade. Please provide dimensioned graphics and calculations demonstrating that project complies with blank facade standards per SMC 23.47A.008.A.2 and as measured in 23.86.028. This code section states that blank segments of the street-facing facade between 2 and 8 feet above the sidewalk may not exceed 20 feet in width. It also states that the total of all blank facade segments may not exceed 40% of the width of the facade of the structure along the street. The length of a blank facade located within this area shall be measured between the closest points of adjacent transparent areas, at 5 feet above the elevation of the lot line at the sidewalk.

[Sheet G0.02 has been included in this drawing submission with calculations and a dimensioned diagram elevation of the façade facing Union Bay Place NE. The percentage of blank façade is 17.6%.](#)

Transparency. Please provide dimensioned graphics and calculations demonstrating that this project complies with transparency standards per SMC 23.47A.008.B2. This code section states that 60% of the street-facing facade between 2 and 8 feet above the sidewalk shall be transparent. For purposes of calculating the 60% of a structure's street-facing facade, the width of a driveway at street level, not to exceed 22 feet, may be subtracted from the width of the street-facing facade if the access cannot be provided from an alley. A departure from transparency may be an option or comply with this requirement.

[Sheet G0.02 has been included in this drawing submission with calculations and a dimensioned diagram elevation of the façade facing Union Bay Place NE. The façade is 68.5% transparent.](#)

Prominent Entry. The street-level street-facing facade containing a residential use shall have a visually prominent pedestrian entry per 23.47A.008.D1. Please comply with this requirement distinguishing between the prominent entry of the residential use and the prominent entry of the commercial use on sheet A3.01DR, West Elevation.

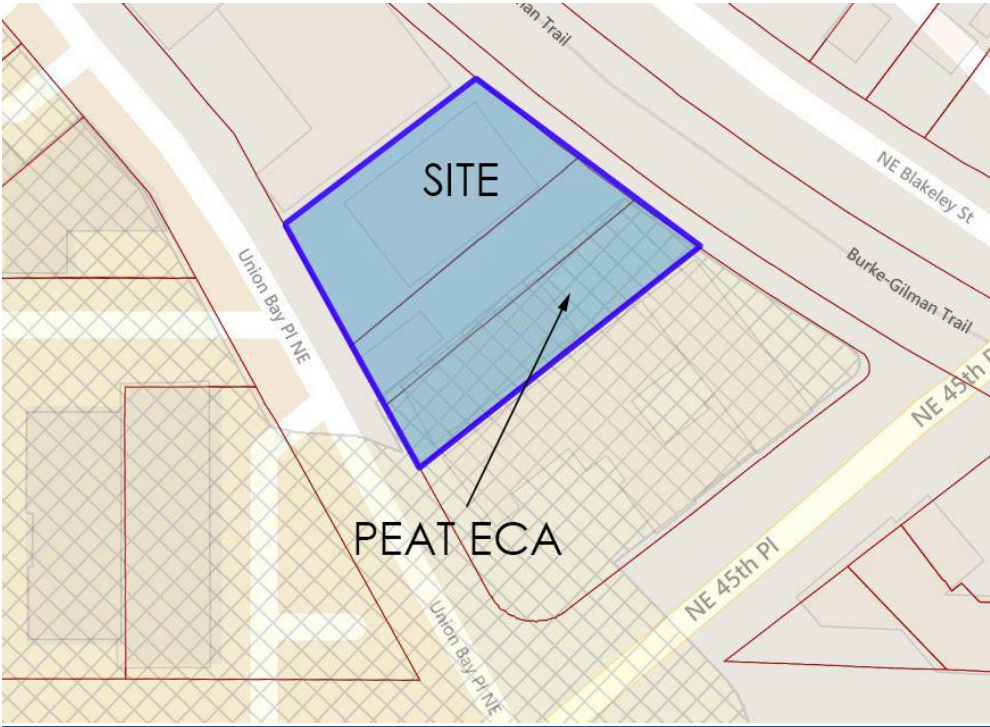
[The entry has been revised so that the residential entry is clearly distinguished from the retail entries. The retail bar has been peeled back and the residential entry has been linked to the 2nd floor residential mas and reads as a two story form to be more visually prominent. See A3.01.](#)

3 Structure Height.

Height Limit. Please revise the height limit noted on the elevations to remove the 3' allowance over the height limit as this lot is not in a peat settlement-prone environmentally critical area

per 23.47A.012.A4. Please dimension all features that extend above the height limit on the elevations. Also, provide rooftop coverage for all features exceeding the height limit per 23.47.012.C.

The proposed project is located in a peat settlement-prone ECA. See below:



Height of rooftop elements such as stair and elevator penthouses has been noted on the drawings. See A3.01. Per SMC 23.47.012.C stair and elevator penthouses may extend 16' above the zoned height limit.

See G0.02 for the penthouse coverage diagram.

Average Grade/Enclosing Rectangle. Under Formula 2 of Director's Rule 4-2012, the average grade level is calculated by first drawing the smallest rectangle that encloses the entire structure, including all occupied floor area. Please see enclosed diagram showing the smallest drawn rectangle. Determine the elevation beyond the property line, and use this elevation at the midpoints of "Plane B". Provide dimensioned graphics and calculation using the formula per Director's Rule and show on sheet G0.01.

Updated Average Grade calculations are shown on sheet G0.02. The calculations are based on the "Option to the General Rule" provided in the Director's Rule 4-2012.

4 Floor Area Ratio (FAR).

Parking. Except as provided in subsection 23.47A.013.D.7, parking that is within or covered by a structure or portion of a structure and that is within a story that is not underground shall be

included in gross floor area calculations. Please provide a diagram/section showing existing or finished grade, whichever is lower as it relates to the portion of the story containing parking.

Updated FAR calculations are shown on sheet G0.01. The entire garage has been included in the gross square footage.

5 Landscaping & Screening Standards.

Street Tree Requirements. Street trees are required along Union Bay Pl NE in the C2-40 zone per SMC 23.47A.016.B and must be planted according to the Seattle Department of Transportation (SDOT) Tree Planting Standards. Please contact Bill Ames with SDOT at 206-684-5693 to determine species of tree and standards of planting. Once you have contacted Mr. Ames please provide written confirmation from SDOT that the proposed trees are acceptable in the planting strip and add a note to the plans showing size, location and species of tree to be planted. The Landscape Plan and Green Factor Work Sheet and Score Sheet should be updated accordingly.

The See landscape sheets. Street trees are now shown in the drawings.

Dumpster Screening. Garbage dumpsters associated with structures containing a residential use in the C2-40 zone are required to have a minimum screening of 6 foot in height per Table D for 23.47A.016. Please provide the required screening on sheet A2.02 & all Landscape sheets.

See landscape sheets. The required 6 foot high screening is called out on sheet A2.02.

Tree Replacement. Each tree over two (2) feet in diameter that is removed in association with development in all zones shall be replaced by one or more new trees; the tree replacement required shall be designed to result, upon maturity, in a canopy cover that is at least equal to the canopy cover prior to tree removal per 25.11.090.A. Please provide a report from a certified land professional identifying existing canopy to be removed (24" Maple) and the proposed/replacement canopy to confirm compliance with this code.

See landscape sheets. Per the code Madrona trees 6" caliper and over should be considered exceptional. The arborist's report lists the tree as not exceptional per code, I think based on its declining health, FYI. If it were exceptional and/or over 24" caliper we would need replacement tree canopy.

6 Street Improvement.

Alley Setback. An exception was granted for the required 2.5' dedication, therefore a setback would not be required. Please remove all notation of a 2.5' setback from alley.

Notation referencing the 2.5' setback has been removed from the drawings. See A1.01 and the A2 series.

No Protest Agreement (Alley). SMC 23.53.030.F requires a No Protest Agreement for site with underdeveloped or reduced street improvements. (A No Protest Agreement waives your opposition to the formation of a Local Improvement District for future street improvements in the city right-of-way, and is a common condition of construction permits when the right-of-way is not

fully improved per SMC 23.53.015). Please complete the No Protest Agreement, record at King County, and submit a copy of the recorded document with your corrected plans. Blank No Protest Agreement forms are available online at http://www.seattle.gov/dpd/Publications/Forms/Land_Use/default.asp.

A "No Protest Agreement" was signed by the owner and is provided with this resubmittal.

7 Structural Building Overhangs.

Please clarify if any features of the building will project over the Right-of-Way. Building overhangs are regulated by 23.53.035.B. An SDOT annual permit is required of building overhangs projecting in the Right-of-Way.

Structural overhangs that comply with 23.53.035.B are called out and dimensioned in detail 2/G0.02.

8 Amenity Areas.

Amenity areas are required in an amount equal to 5% of the total gross floor area in residential use per 23.47A.024.A. Please include in the gross floor area calculations for residential use to include all parking areas allocated for residential use. Please update the amenity area diagram and calculations.

Calculations of required amenity area are located on sheet G0.00. 5% of gross residential square footage = 2,510 SF required. We are providing 4,097 SF of common amenity space and additional private amenity space.

9 Parking, Location & Access.

Access to Parking (Departure). Access to parking shall be from the alley if the lot abuts an alley per 23.47A.032. Please work with the assigned land use reviewer on this departure or show compliance with this code.

Per 23.47A.032.A.1.a access to parking is required from the alley if it is improved to the standards of subsection 23.53.030.C. The alley does not meet these standards; it is less than 16 feet wide so it is not considered to be improved, therefor we understand that we are allowed to have access to the parking from the street. That said, we are asking for a departure to allow additional access from the alley just for the upper level of parking which is separate from the lower level of parking.

We will continue to work with the land use reviewer to achieve this departure.

Parking Space Requirements (Departure). The required size of parking spaces is determined by whether the parking is for a residential or non-residential use. In structures containing residential uses and also containing non-residential uses, parking that is clearly set aside and reserved for residential use shall meet the standards of subsection 23.54.030.B.1; parking for all other uses within the structure shall meet the standards of subsection 23.54.030.B.2. Please work with the assigned land use reviewer on this departure or show compliance with this code.

We will continue to work with the land use reviewer on this departure.

Curb Cut. Please provide and dimension a curb cut along Union Bay PI NE complying with the standards in 23.54.030.F.

The curb cut for the access off of Union Bay Place NE is shown to be 20 feet wide within the revised Street Improvement Plan sheet 2 of 3.

Driveway Slope. No portion of a driveway, whether located on a lot or on a right-of-way, shall exceed a slope of 15 percent, except as provided in this subsection 23.54.030.D.3. The maximum 15 percent slope shall apply in relation to both the current grade of the right-of-way to which the driveway connects, and to the proposed finished grade of the right-of-way if it is different from the current grade per 23.54.030.D.3. Please show a calculation complying with driveway slope on sheet A1.00.

All driveways are sloping in accordance with SMC 23.54.030.D3. Refer to spot-slope annotations on sheets A2.01 and A2.02.

Driveway slope as it relates to the ROW: The proposed vehicular access points will be less than 15%. See the revise Street Improvement Plan sheet 2 of 3.

Driveway. The minimum width of driveways for two way traffic is 22 feet with a maximum width of 25 feet. Please clearly show the entrance and exit lanes per 23.54.030.D2 & 23.54.030.G2. Please dimension driveway on sheet A1.00 and sheet A2.01 off alley & Union Bay PL NE.

The driveway entrance width is planned at 20'-4 1/2" We are asking for a departure to reduce the driveway width to reduce the scale of the opening to reduce the impact on the pedestrian scale. See departure request on G0.00 and see sheet A2.01.

Aisle Slope. Aisle slope shall not exceed 17% per 23.54.030.E4. Please provide calculation complying with this requirement.

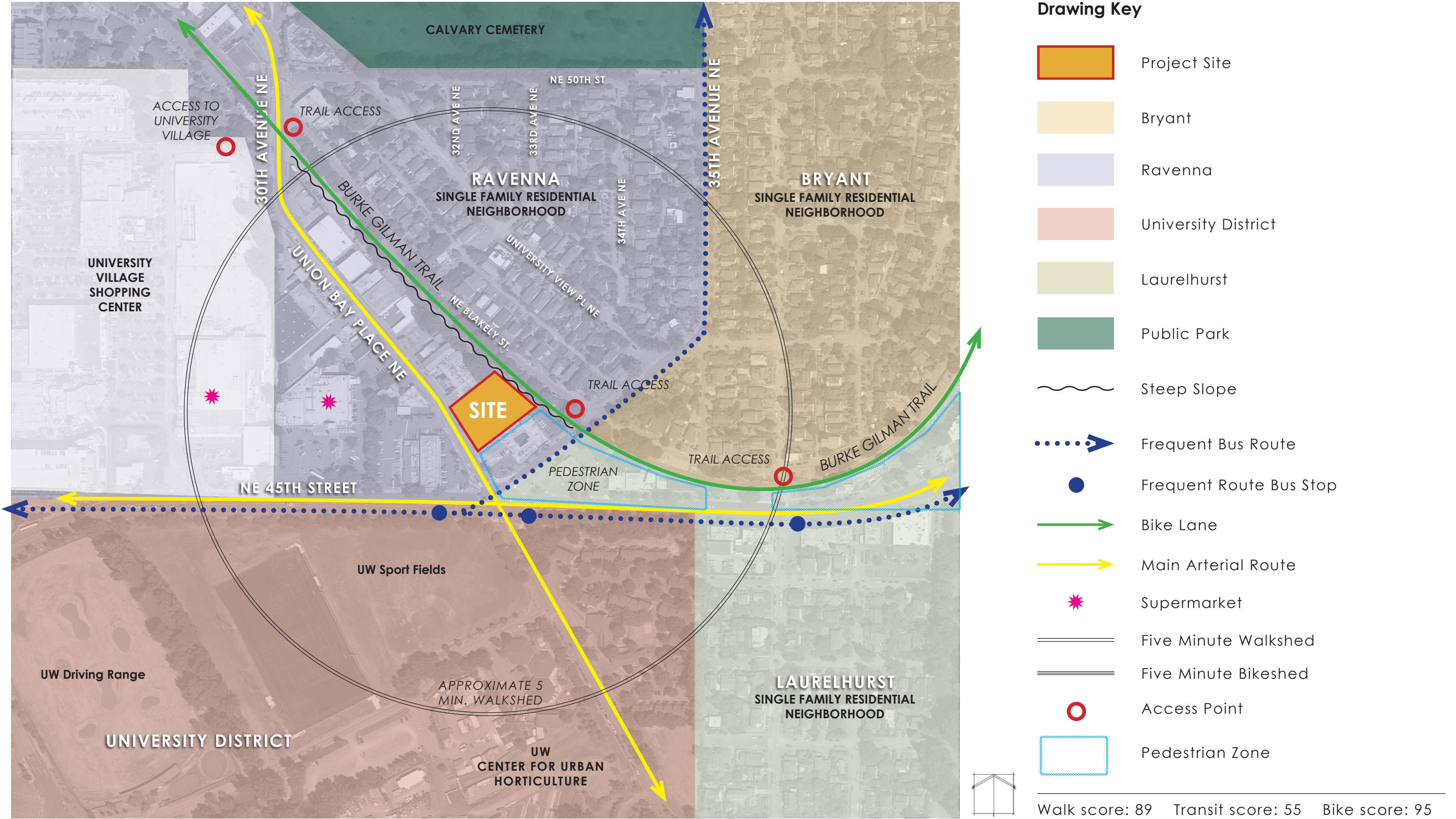
Similar as "Driveway Slope" response: All aisles are sloping in accordance with SMC 23.54.030.E4. Refer to spot-slope annotations on sheets A2.01 and A2.02.

Ravenna/University District/Laurelhurst

The project is located in Ravenna but also near the intersection of the University District to the West and the mostly residential community of Laurelhurst to the East. The topography of the surrounding area slopes steeply down to the South but is relatively flat where the University of Washington conducts environmental study and research.

The Burke Gilman Trail is a buffer to Ravenna's mostly residential neighborhood at the North of the site. The University Village shopping center is within a 5 min walking distance and both Ravenna Park and the Burke Gilman Playground are within 1 mile. The site is near the intersection of a principal arterial (NE 45th Street), a minor arterial (Union Bay Place NE) and a collector arterial (NE 45th Place)







Nine Block Context

Union Bay Place is a mixed-use street that acts as a transition between the University Village and the single family residential zone to the North and East. There is a newly enacted pedestrian zone starting adjacent to the site to the Southeast and continuing along NE 45th St. This change along with other future development will significantly change the scale and character of the immediate surroundings.

Currently there are, for the most part, low scale one- and two-story buildings and expansive parking lots. The sidewalks are not developed on most of the street. There is an opportunity for this project to make a strong presence on the street pointing the way for a more pedestrian related future.

To the Northeast the Burke Gilman Trail and the steep slope leading up to it create a quiet buffer zone.



Commercial Residential Site



VIEW OF SITE FROM UNION BAY PLACE
Neighboring building has few windows on the street.



TULLY'S AND FEDEX OFFICE
One story retail buildings. View of parking lot in foreground.



PLAY FIELDS SOUTH OF NE 45TH STREET
Street facing units will have an oblique view of this.



VIEW FROM BURKE GILMAN TRAIL
Trail is about 2 1/2 stories up. Project will be quite visible.



VIEW OF SITE FROM UNION BAY PLACE
Neighbor is one-story retail that comes to the sidewalk.



SAFeway GROCERY STORE
Parking lots and low buildings are still dominant in the area.

View of site from Union Bay Place NE:



View opposite site on Union Bay Place NE:



Project Site:

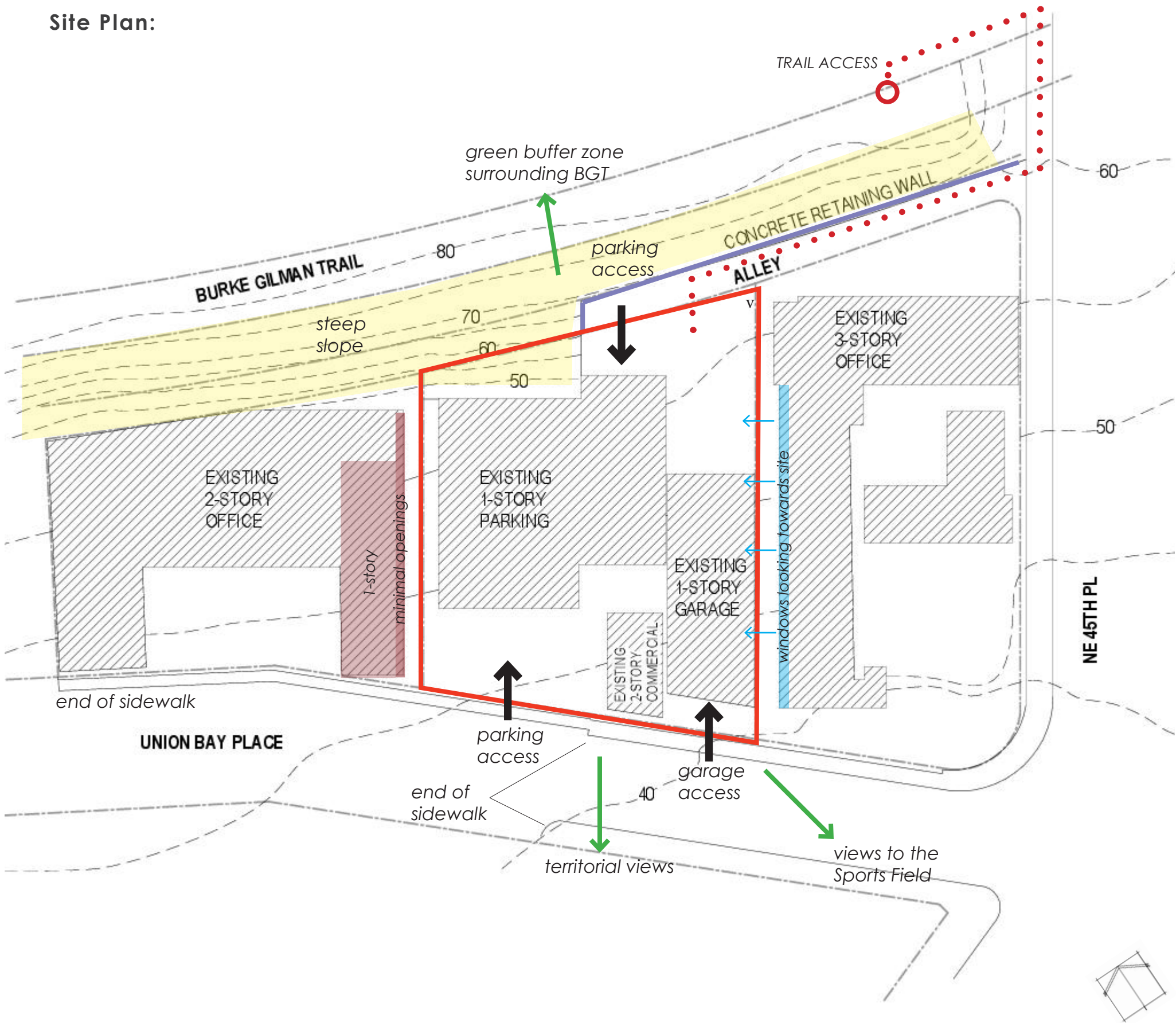
The site is zoned C2-40 (Commercial) and is located in the middle of a city block with the Burke Gilman Trail bounding the site to the North. There is a 156'-6" frontage on Union Bay Place NE.

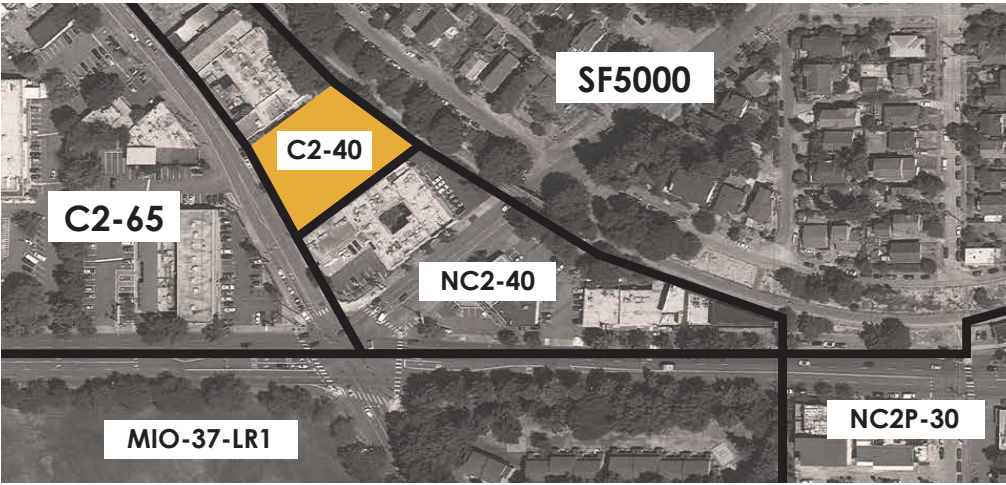
The site has a significant slope from 60' at the East corner where a retaining wall supports the Burke Gilman Trail above down to 40' at the South corner on Union Bay Place. This represents a 20' total difference. Currently only half of the alley on the North side has been constructed and there is a very steep slope that will make improvement impractical.

There is an existing two story (split level) parking structure that is accessible from both Union Bay Place and the alley. On Union Bay Place there is also two other existing buildings: a two story office building and a one story automotive garage building. There are several curb cuts along this street frontage.

There are territorial views to the Southwest with parking lots and low commercial buildings in the foreground. To the South there is an oblique view of the IMA Sports Field with the Union Bay Natural Area beyond. Looking back towards the Northeast the Burke Gilman Trail is surrounded by trees and shrubs creating a dense green buffer zone. This green zone has the advantage that it won't ever be obstructed by future construction.

Site Plan:





Zoning Code: City of Seattle Zoning Code
Zone: C2-40
Lot Area: 26,790SF

23.47A.004/006 PERMITTED AND PROHIBITED USES

- Residential use is a conditional use in C2 zones. Residential uses permitted in C2 zones are subject to the following:
 - i. The distance between the lot in question and major transportation systems and potential nuisances
 - ii. The presence of physical buffers between the lot in question and major transportation systems and potential nuisance uses
 - iii. The potential cumulative impacts of residential uses on the availability for nonresidential uses of land near major transportation systems; and
 - iv. The number, size and cumulative impacts of potential nuisances on the proposed residential uses.

23.47A.008 STREET LEVEL DEVELOPMENT STANDARDS

- Blank facade and Transparency Requirements:
- Blank segments of the street-facing facade between 2 feet and 8 feet above the sidewalk may not exceed 20 feet in width.
 - The total of all blank facade segments may not exceed 40 percent of the width of the facade of the structure along the street.
 - Street-level street-facing facades shall be located within 10 feet of the street lot line, unless wider sidewalks, plazas, or other approved landscaped or open spaces are provided.
 - Sixty percent of the street-facing facade between 2 feet and 8 feet above the sidewalk shall be transparent.
 - Transparent areas of facades shall be designed and maintained to allow unobstructed views from the outside into the structure or, in the case of live-work units, into display windows that have a minimum 30 inch depth.

23.47A.013 FLOOR AREA RATIO

- Base FAR is 3.25 on lots with a mix of uses

23.47A.014 SETBACK REQUIREMENTS

For a structure containing a residential use, a setback is required along any side or rear lot line that abuts a lot in a residential zone or that is across an alley from a lot in a residential zone, as follows:

- Fifteen feet for portions of structures above 13 feet in height to a maximum of 40 feet; and
- For each portion of a structure above 40 feet in height, additional setback at the rate of 2 feet of setback for every 10 feet by which the height of such portion exceeds 40 feet
- One-half of the width of an abutting alley may be counted as part of the required setback. For the purpose of this Section 23.47A.014, the alley width and the location of the rear lot line shall be determined prior to any dedication that may be required for alley improvement purposes.
- No entrance, window, or other opening is permitted closer than 5 feet to an abutting residentially-zoned lot.
- Setback requirements do not limit underground structures.

23.47A.016 LANDSCAPING STANDARDS

Landscaping that achieves a Green Factor score of 0.3 or greater, pursuant to Section 23.86.019, is required for any lot with:

- development containing more than four new dwelling units or a congregate residence; or
- development, either a new structure or an addition to an existing structure, containing more than 4,000 new square feet of non-residential uses; or
- any parking lot containing more than 20 new parking spaces for automobiles.

23.47A.024 AMENITY AREA

- Amenity areas are required in an amount equal to 5 percent of the total gross floor area in residential use, except as otherwise specifically provided in this Chapter 23.47A. Gross floor area, for the purposes of this subsection, excludes areas used for mechanical equipment and accessory parking.
- Required amenity areas shall meet the following standards, as applicable:
 - a. All residents shall have access to at least one common or private amenity area;

- b. Amenity areas shall not be enclosed;
- c. Common amenity areas shall have a minimum horizontal dimension of 10 feet, and no common amenity area shall be less than 250 square feet in size;
- d. Private balconies and decks shall have a minimum area of 60 square feet, and no horizontal dimension shall be less than 6 feet.

23.47A.032 PARKING LOCATION AND ACCESS

- Access to off-street parking may be from a street, alley, or both when the lot abuts an alley. However, structures in C zones with residential uses and structures in C zones across the street from residential zones shall meet the requirements for parking access for NC zones as provided in subsection 23.47A.032.A.1
- Access to parking shall be from the alley if the lot abuts an alley improved to the standards of Section 23.53.030.C, or if the Director determines that alley access is feasible and desirable to mitigate parking access impacts.
- If access is not provided from an alley and the lot abuts only one street, access is permitted from the street, and limited to one two-way curb cut.
- Access to off-street parking may be from a street if, due to the relationship of an alley to the street system, use of the alley for parking access would create a significant safety hazard as determined by the Director.

23.54.015/016/020 PARKING

1. 1 space per dwelling unit for dwelling units with fewer than two bedrooms; plus 1.5 spaces per dwelling units with 2 or more bedrooms
2. Live/Work : Zero spaces for units with 1,500sf or less; one space for each unit greater than 1,500sf
3. Sales and service space : one space for each 500sf
4. In multifamily and commercial zones, the minimum parking requirement for all uses is reduced by 50 percent if the use is located within 1,320 feet of a street with frequent transit service. This distance will be the walking distance measured from the nearest transit stop to the lot line of the lot containing the use.

23.76.006 MASTER USE PERMITS REQUIRED

- MUP required for all projects requiring one or more of the following:
1. Establishment or change of use
 2. Lot boundary adjustments