



HOLLAND PARTNER GROUP

# 970 DENNY

WEST DRB RECOMMENDATION MEETING

08.19.2015 | 14-062 | DPD #3018935



WEBER THOMPSON





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# PROJECT INFORMATION

## PROJECT DESCRIPTION & VISION

The 970 Denny project is located at the southern edge of the South Lake Union and north edge of the Denny Triangle neighborhoods, divided by Denny Way. Within close proximity of the downtown office core, major employers in South Lake Union, and easy access to transit linking employers to afar, 970 Denny is positioned to enhance the walkability and transit orientation of Seattle's downtown.

970 Denny is being developed as rental apartments, offering a mix of unit sizes and configurations that meet potential resident needs. Amenities will be located at three levels, the 1st floor, 7th floor and the rooftop, providing spaces for residents to relax at the immediate neighborhood scale and in the larger context of the city and its surroundings, capturing the spectacular regional views surrounding the site. Retail locations have been chosen to enhance Terry Avenue N. as the emerging, pedestrian friendly, retail / restaurant / and nightlife corridor to Lake Union, and be visible from the already vibrant intersection of Westlake and Denny Way.

Our careful study of the existing building stock in the neighborhood, including proposed projects under construction, shows examples of many different architectural styles and a wide variety of materials. Generally, many of the buildings exemplify the prevalent character and styles of their time. We propose to continue that established pattern; 970 Denny will be detailed as a unique, contemporary expression of a high-rise residential building, while maintaining some of the character and scale reminiscent of the pre-Amazon, industrial South Lake Union neighborhood. As indicated in the following pages, the design has taken cues from the existing context to provide guidance to the massing of the building, materiality, and contextual response.

## PROJECT STATISTICS

(ALL APPROXIMATE)

PROGRAM	FLOORS	AREA (APPROXIMATE)
BELOW GRADE PARKING	P2-P6	144,055 SF
MECH, VERT TRANS, BOH	P1-L2	85,427 SF
RESIDENTIAL	L2-L41	353,788 SF
AMENITY & ROOF DECK	L1, L7, L42	19,057 SF

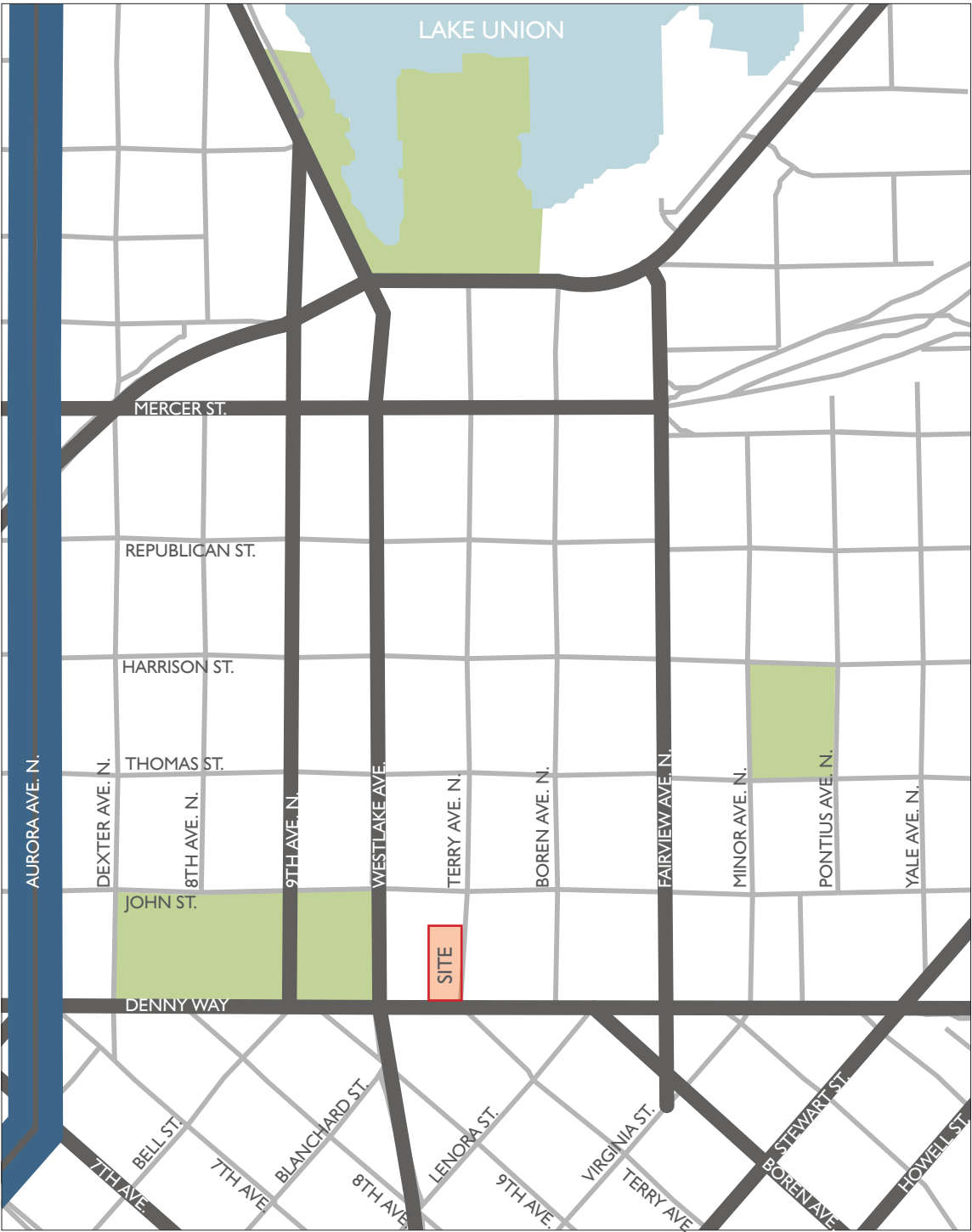
RESIDENTIAL UNITS  
461 UNITS

RETAIL  
15,098 SF

PARKING STALLS  
374 STALLS

BIKE PARKING  
188 STALLS

HEIGHT  
440 FT.



SITE LOCATION



# SURROUNDING PROJECTS



## BOXCAR APARTMENTS

7 Story Residential Apartments  
Built in 2013  
145 Residential Units  
80 Feet Tall

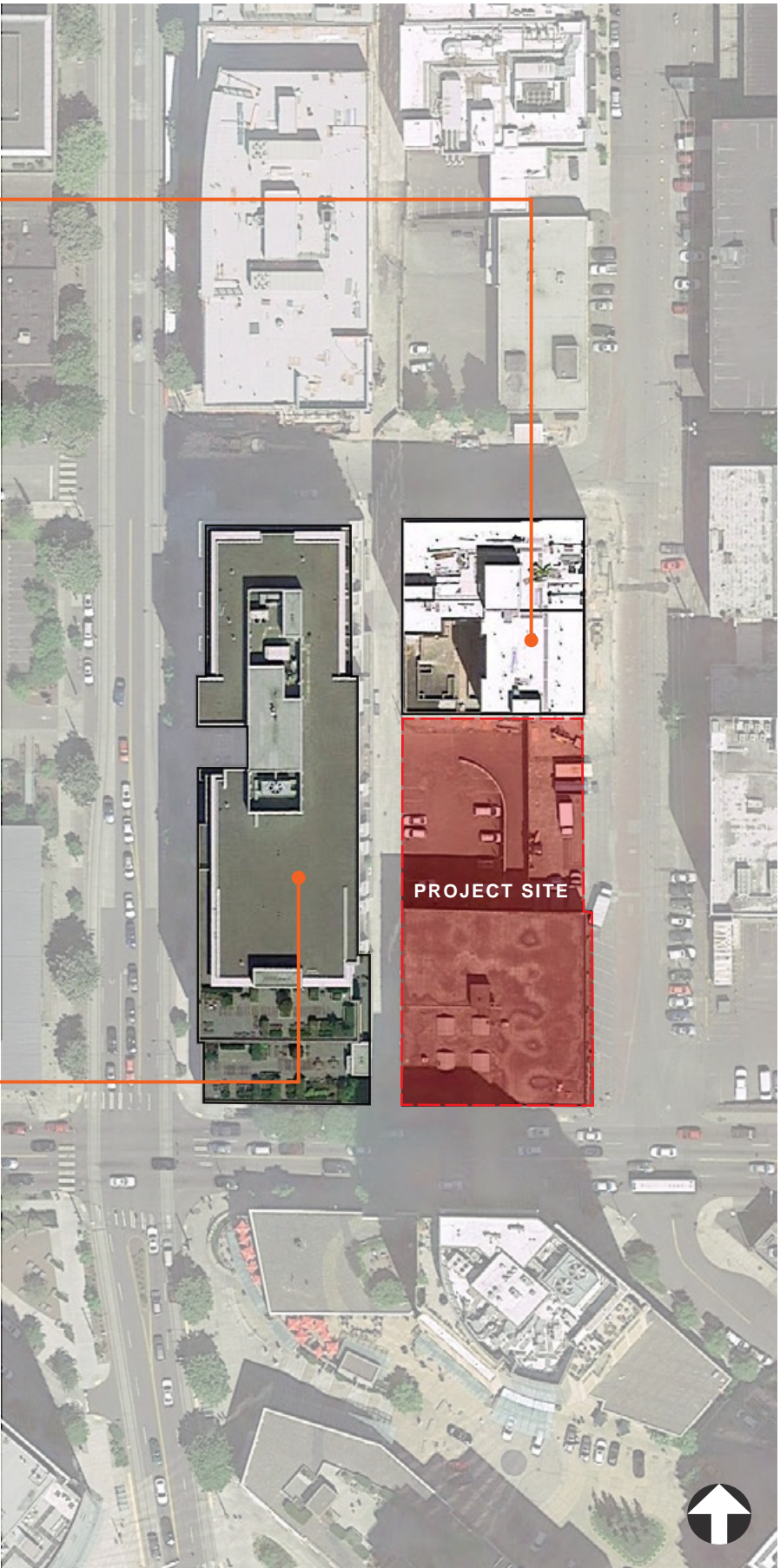
Originally envisioned as student housing, this seven story apartment building houses 145 studio and one bedroom apartments ranging from 319 to 667 sq. ft. They are small scale apartments targeted to “those who spend less time on the couch and more time taking in all the City and Sound have to offer.” The “L” shaped building opens up at the southwest corner of the site, directly to the north of our site.



## ROLLIN STREET FLATS

11 Story Residential Apartments  
Built in 2012  
Street Front Retail  
Modernism | Brick | Wood Veneer | Metal

Completed in 2012, the Rollin Street Flats are modern high end apartments ranging from 670 to over 2000 sq. ft., many featuring decks or terraces. The building’s exterior is clad in brick, wood, and aluminum for a modern industrial look. The building also features retail storefronts along Westlake Ave. and a rooftop terrace along Denny.





# SURROUNDING PROJECTS



## SEATTLE TIMES BUILDING

8 Story Office  
Built in 1929  
Steel-Concrete Structure

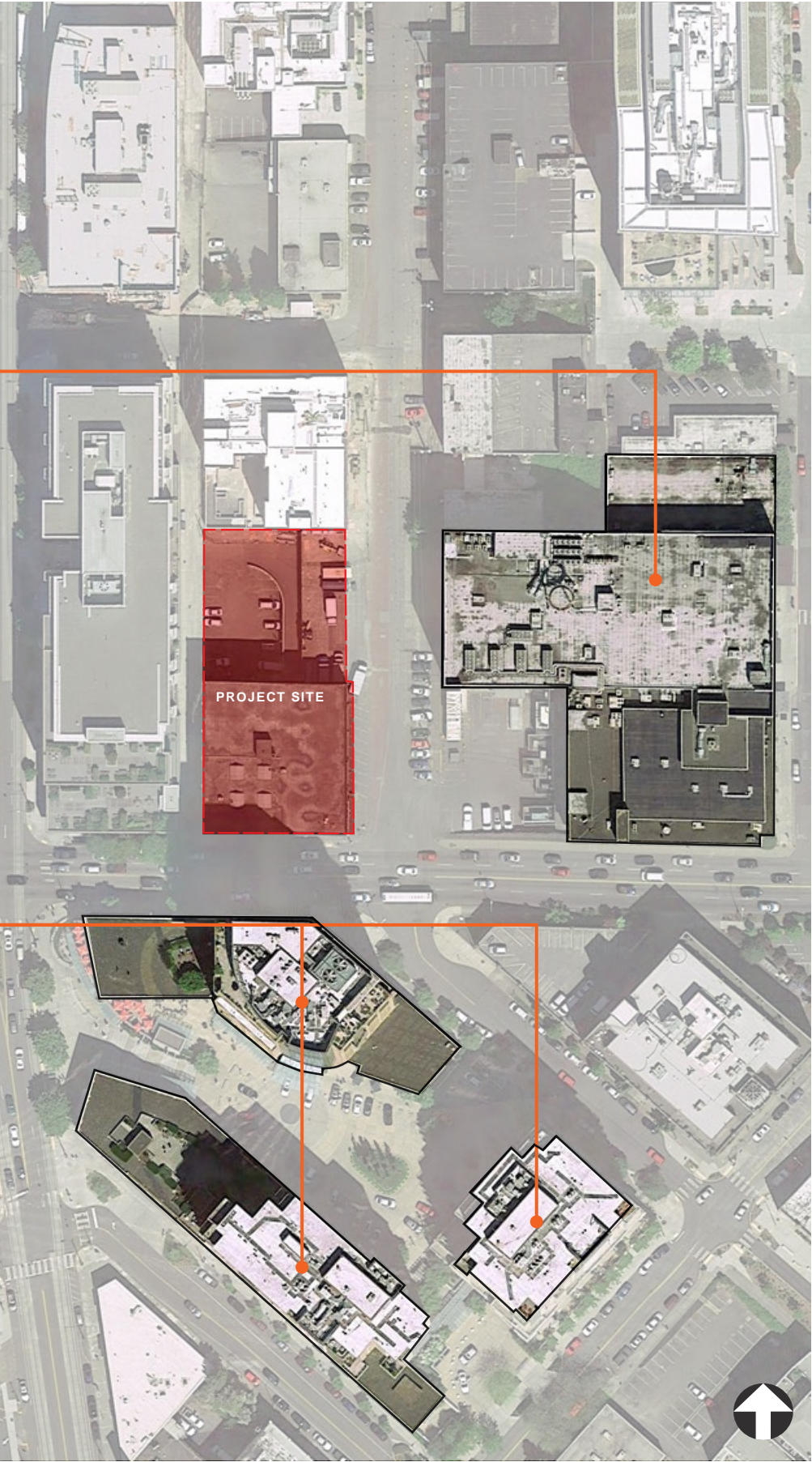
Built in 1929 and serving as the headquarters for the Seattle Times for 81 years, the eight story concrete building was sold in 2011 and was seismically reinforced with interior and exterior steel bracing. It now serves as a mixed-use property, it contains communications, storage, and restaurant tenants.



## 2200 WESTLAKE

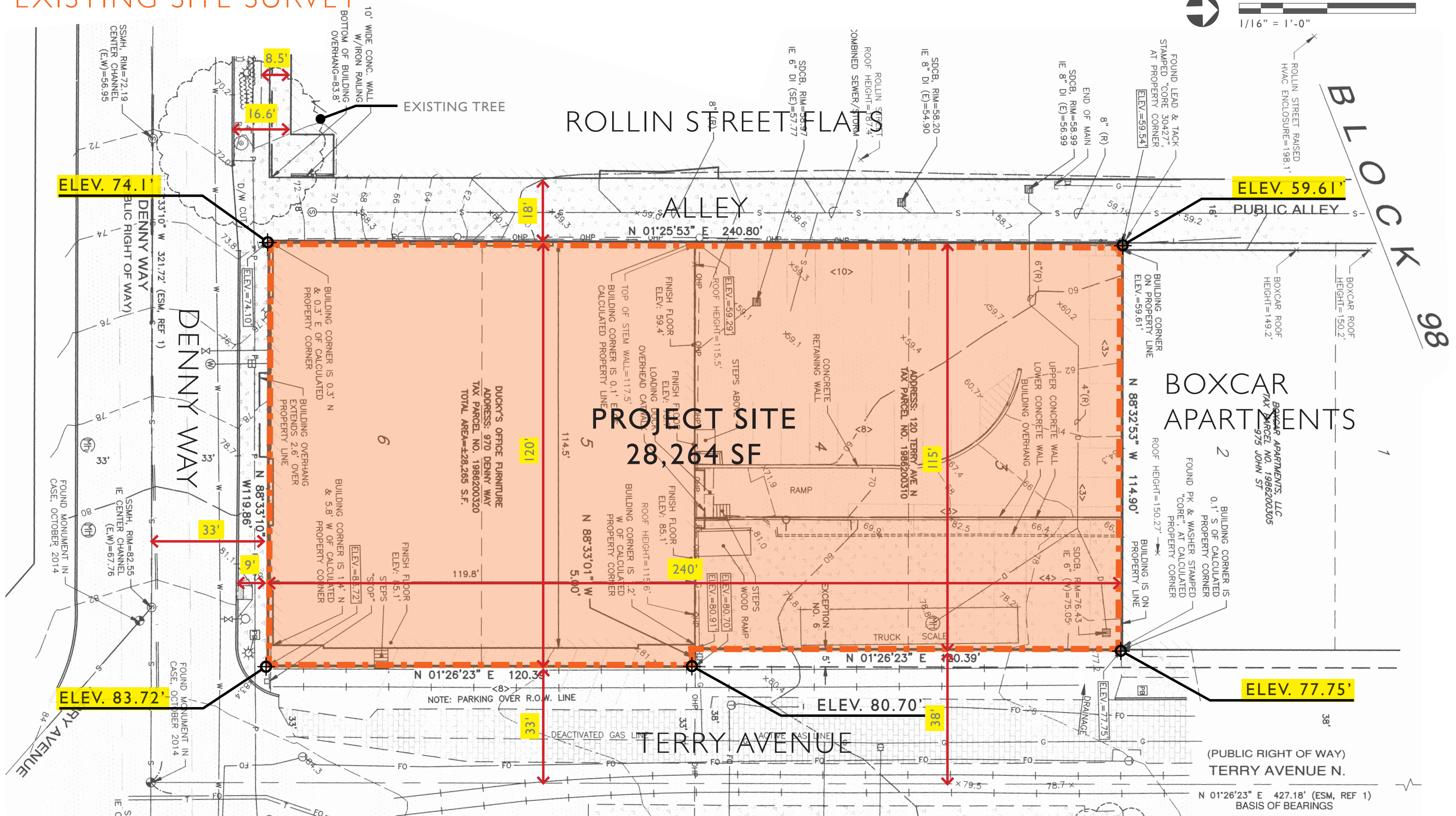
3 Story Retail | Residential | Hotel  
Built in 2007  
Street Front Retail  
Modern | Brick | Curtain wall

Completed in 2007, this one-million sf mixed-use project served as a catalyst for the redevelopment of the South Lake Union neighborhood and an iconic gateway to the Denny Triangle and downtown Seattle. Program includes 93,000 sf retail, 260 residential units, and a 160-room Pan Pacific Hotel. Currently Whole Foods anchors the development's large tenant space at the corner of Westlake and Denny, and many restaurants such as Tutta Bella fill the upper retail floors.



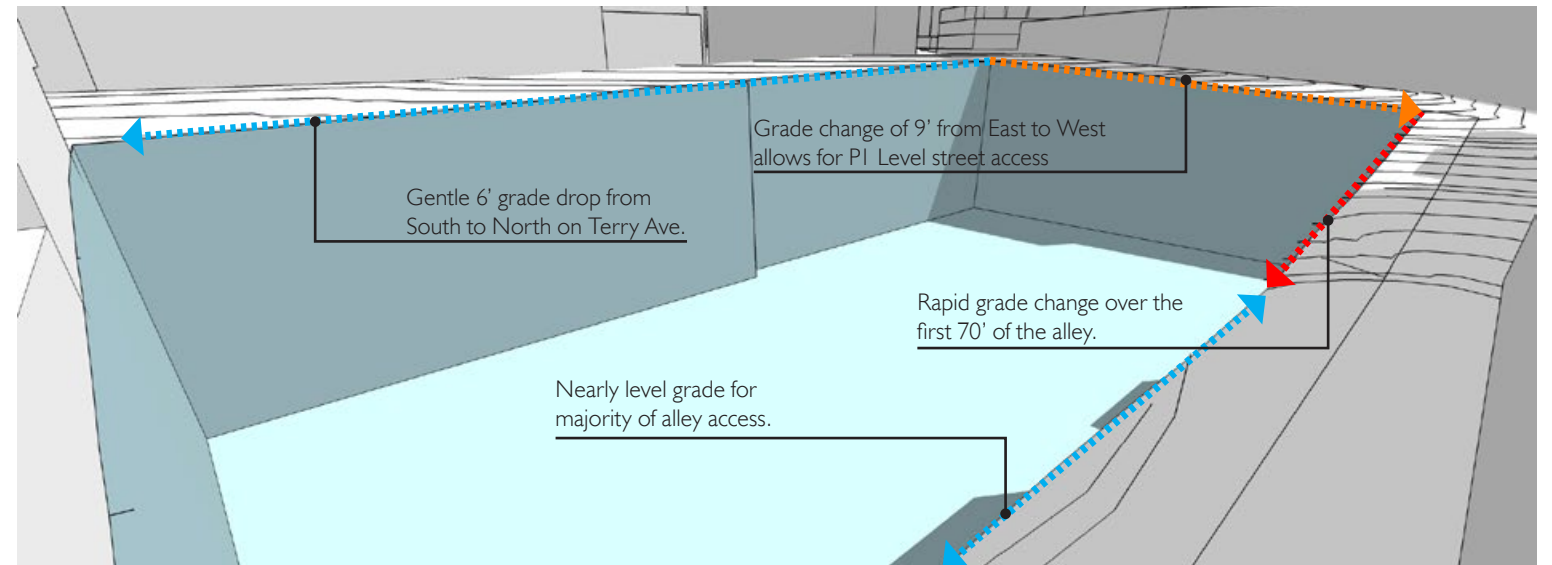
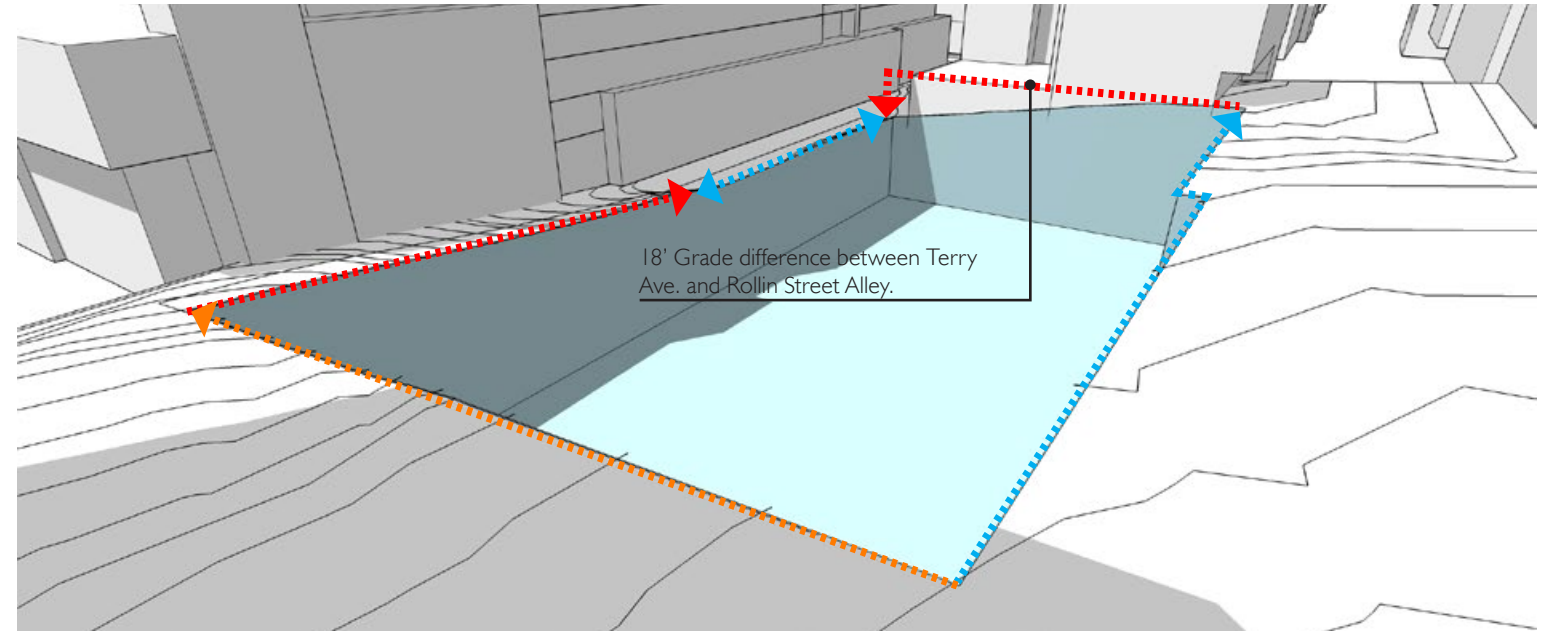
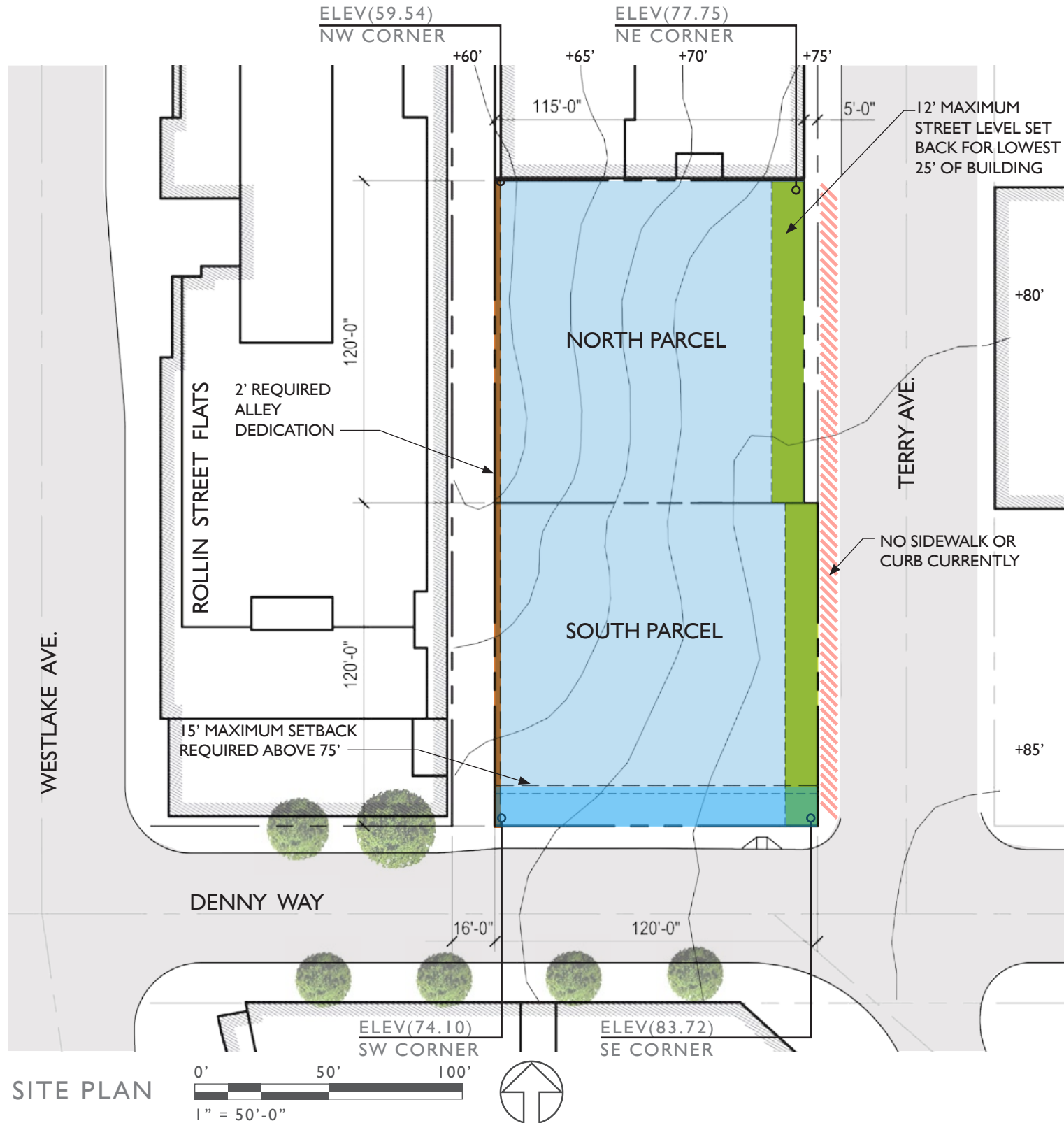


# EXISTING SITE SURVEY





## SITE DETAILS



Terry Avenue

- Two-way street with light traffic.
- Two-lane street with parking along the east side.
- Designated festival street with major pedestrian presence.
- Site slopes downward approximately six feet from the southeast corner of the site to the northeast corner

## DENNY AVENUE

- Two-way heavy traffic street.
- Four-lane street with turn lanes at key intersections.
- Class two pedestrian
- Site slopes downward approximately ten feet from the southeast to southwest corner of the site.

# MASSING OPTIONS AS PRESENTED AT EDG



## MASSING OPTION 1

Code Compliant scheme with industrial proportions on the podium and a rotated rectilinear tower.



## MASSING OPTION 2

Angled Tower and podium with mass shifting in and out vertically to breakdown mass.



## MASSING OPTION 3 – PREFERRED

Curved tower scheme with a rectilinear base reflecting the industrial history of South Lake Union.

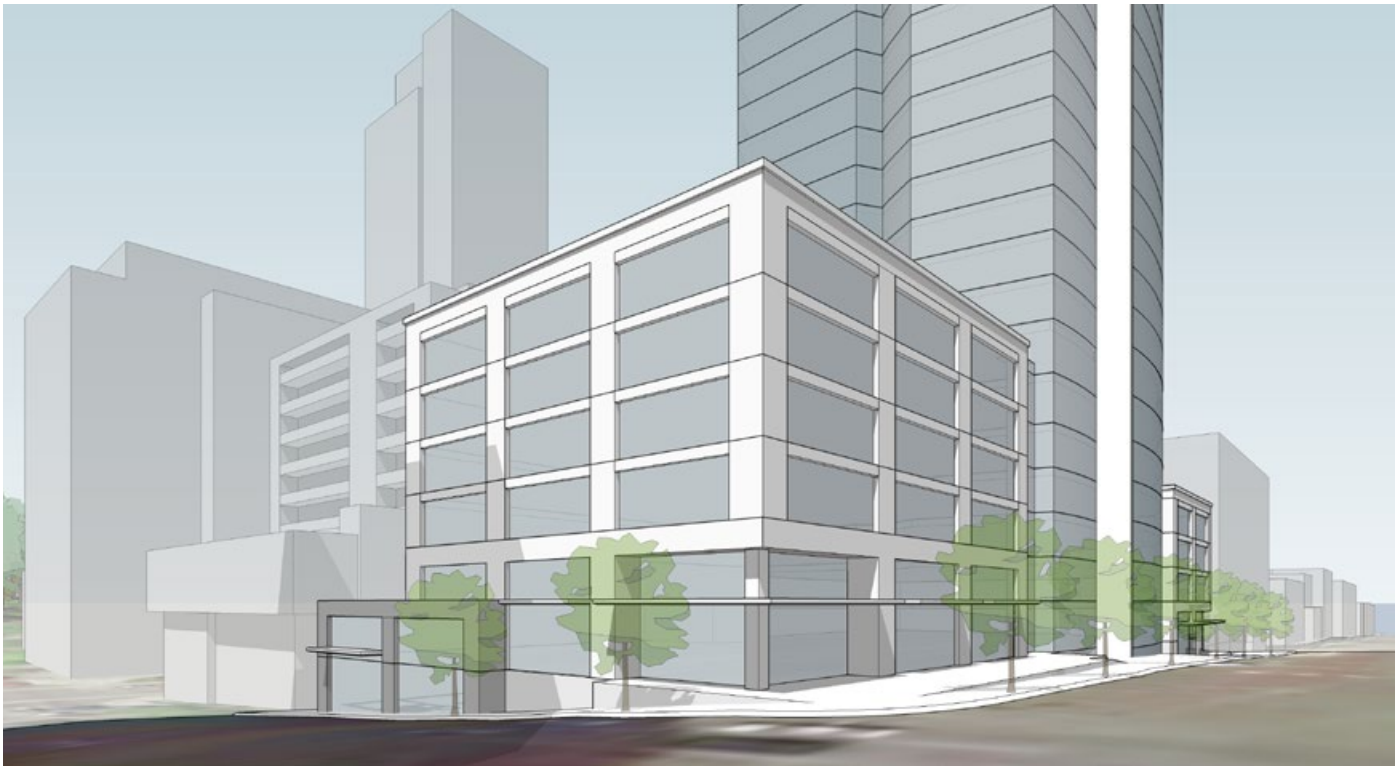


# PREFERRED MASSING AT EDG

“The Board strongly endorsed the north tower placement for this prominent and tall form, as it creates a south roof terrace and better scale along Denny (EDG booklet pages 23/24). The Board also endorsed the preferred massing option 3, with some aspects at the lower levels along Terry Avenue of option 2 being desirable.” – #3018935 EDG Report 2.18.15



BIRD'S EYE FROM SOUTHEAST



PODIUM FROM SOUTHEAST





# BOARD GUIDANCE AT EDG

## SUMMARY OF BOARD GUIDANCE

### TOWER COMMENTS

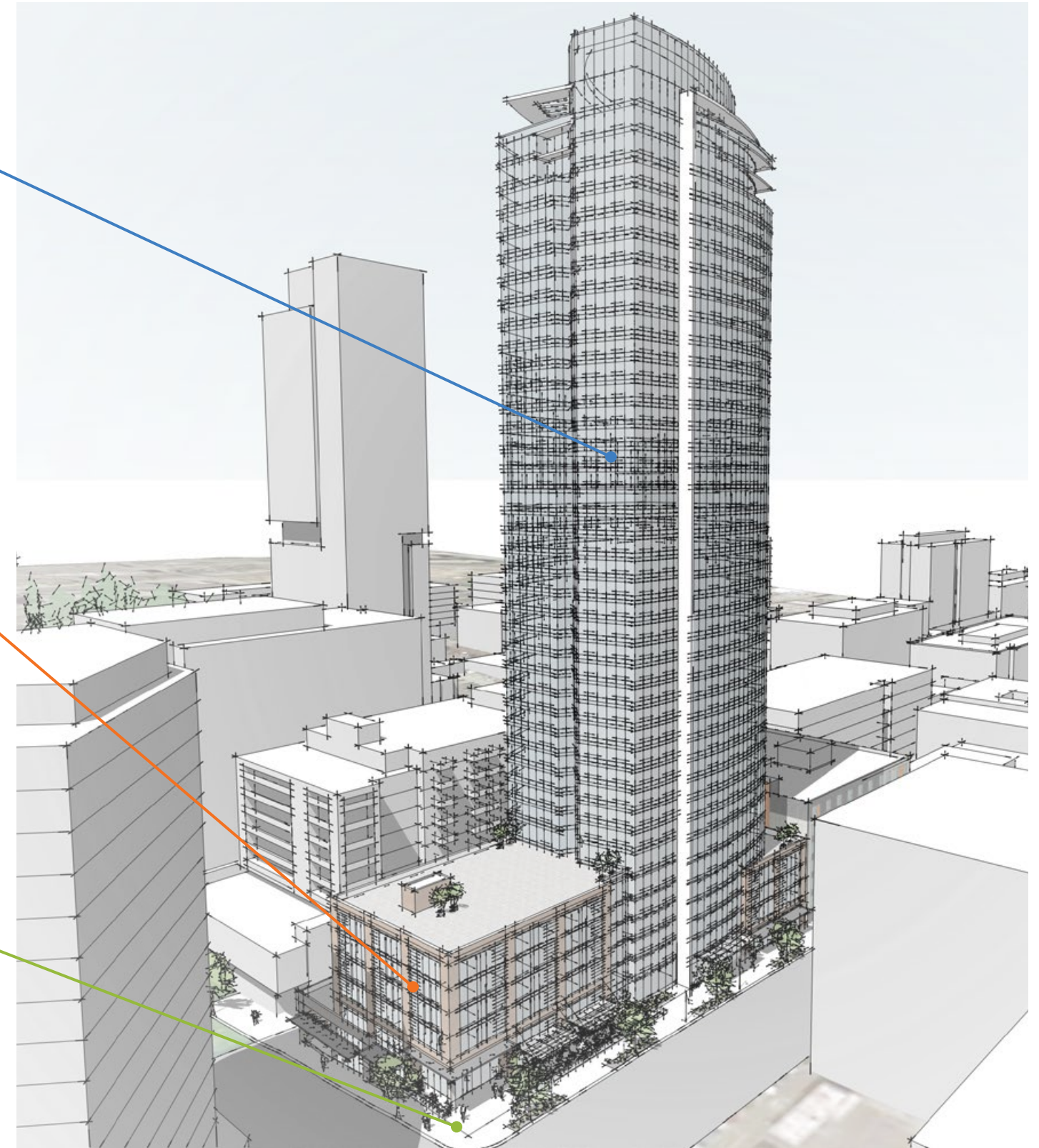
- Questioned how the tower expression meets the street and interacts with the 'industrial' base along Terry.
- Board agreed that the tower lacked intermediate scale and plane shifts at the southeast base of the tower to delineate entry.
- Board encouraged four elevations to be carefully composed as they will be visible from a distance, and not equal on each side, responding to different factors for each orientation.
- Complete explanation of sustainable strategies

### PODIUM COMMENTS

- Agreed the north 'industrial' mass should extend further south to better balance the street facade.
- Board encouraged to not simply mimic historic architecture, but to interpret and distill key material and composition for a modern building.
- Board encouraged further exploration of the corner and upper podium for elements that acknowledge the pivotal Terry hinge at Denny.
- Board endorsed high activation of the south podium rooftop.

### STREETSCAPE COMMENTS

- Dock on Southeast corner encroached too far onto pedestrian sidewalk.
- Denny Way streetscape plan suggests a 6ft setback inside the property line, with paved area inside this having canopies for pedestrian protection.
- The board agreed the pedestrian volumes along Denny warrant the 10ft paved zone.
- Encouraged to work with SDOT on a crosswalk across Denny in the SIP plans.



Design proposal at EDG



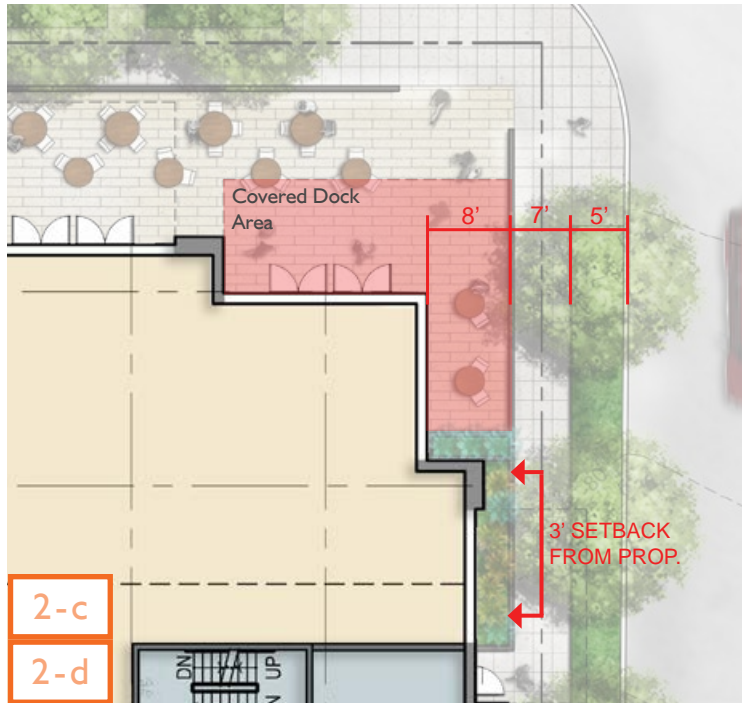
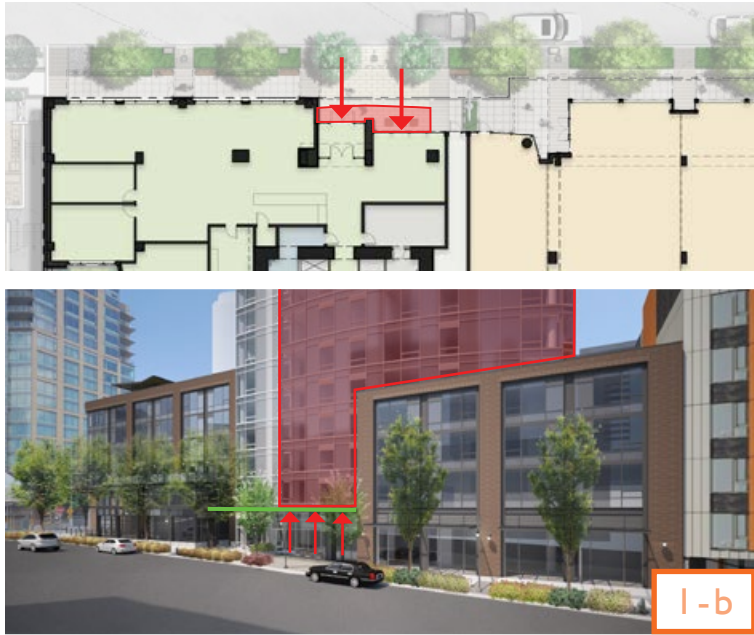
TITLE	DRB GUIDANCE AT EDG	RESPONSE
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<b>1</b> MASSING & CONTEXT RESPONSE	<p><b>a)</b> The Board strongly endorsed the north tower placement for this prominent and tall form, as it creates a south roof terrace and better scale along Denny (EDG booklet pages 23/24). The Board also endorsed the preferred massing option 3, with some aspects at the lower levels along Terry Avenue of option 2 being desirable. See comments under 3b below. (CS2, DC2-A)</p> <p><b>b)</b> The Board agreed the curving tower contrasted with the rectilinear ‘industrial base’ is a sound approach, but had comments about how the tower expression meets the street and interacts with the ‘industrial base’ along Terry (3c), and other refinements to the upper tower (4a &amp; b below). (CS3-II, DC2-I)</p>
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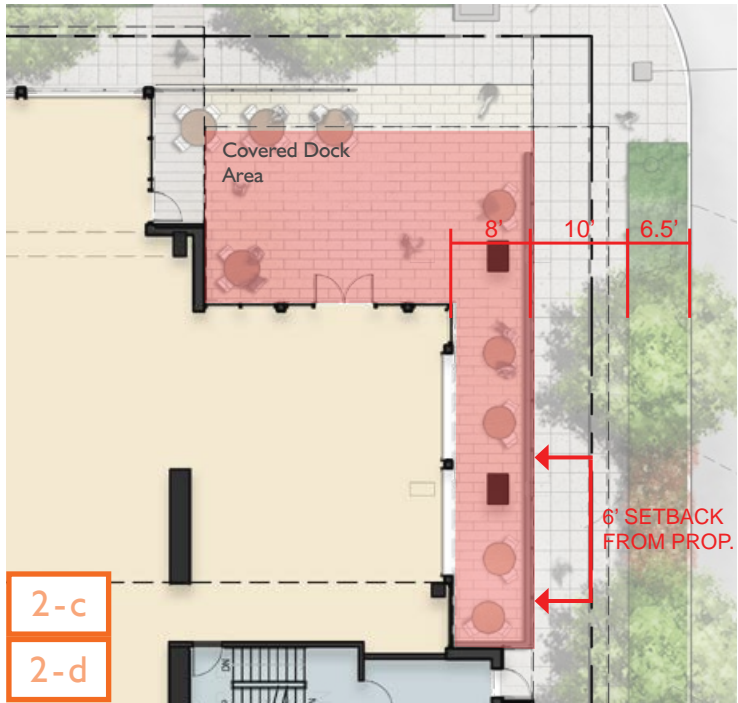
<p><b>1a.</b> The tower remains to the north portion of the site, in fact it was moved slightly more to the north in order to improve the ratio of tower to industrial base podiums at the street level (reducing the tower, increasing the industrial base).</p> <p><b>1b.</b> The tower has been moved west, and part of the tower massing has been held up from the street level in order to become more subordinate to the “industrial” base shoulders, and emphasize the residential entry.</p>
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<b>2</b> GROUND LEVEL USES & STREETSCAPE	<p><b>a)</b> The Board strongly endorsed the large quantity and full-depth retail shown on page 37, and especially endorsed the 2-story retail entry activating the alley corner, shown on page 47. (Guidelines DC1-A, PL3-C)</p> <p><b>b)</b> The Board supported the option 3 voluntary setbacks along Terry (page 39, option 3), and the use of these setbacks (referred to as docks’) as a café zone, as long as the public sidewalk remains 12 ft minimum wide along Terry, the docks stay as low to grade as possible, and the cafe ‘fence’ stays low and fully transparent as shown on page 46. The Board required color drawings at the next meeting that show the proposed Terry Avenue street design fully in the context of existing and proposed streetscapes 1 block to the south and 3 blocks to the north. (Guidelines PL2-I, PL3)</p> <p><b>c)</b> The Board supported a ‘dock’ wrapping and activating the southeast corner, but as shown on pages 45 and 61, it encroaches too much on the sidewalk. It should be recessed onto the site and not overly encroach on the public, pedestrian realm. It appears the deeply recessed corner storefront could shift and accommodate this dock, plus maintain a generous level spot at the corner and the recommended sidewalk width of the Denny Way Streetscape Concept Plan. (Guidelines PLI-III, PL3)</p> <p><b>d)</b> While not presented at the EDG meeting, the City Council adopted Denny Way Streetscape Concept Plan (DWSCP) has bearing on this site. The recommended typical street section on this portion of Denny (DWSCP page 13) shows the sidewalk on the north side of Denny Way (adjacent to this project) “should total 18 ft in width”, with about 8 ft of landscaped buffer element along the busy Denny curb, and about 10 ft net paving. This means a “6 ft setback” inside the property line, and the paved setback should have a canopy for pedestrian protection.</p> <p>The proposed landscape plan on page 61 shows about 6ft paving. The Board concurred that pedestrian volumes along busy Denny warrant the 10 ft paved zone. Subsequent site plans and landscape plans should show the recommended setback and canopies, while retaining the corner dock element inside the setback. (Guidelines CS2-B, PLI-B, PL2)</p>
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<p><b>2a.</b> The design team has also studied various subdivision options between 1 and 4 retail venues to make sure that site grades and door locations / accessibility is feasible.</p> <p><b>2b.</b> The enclosed retail space has been expanded to enclose a portion of the outdoor retail porch, which after substantial sun studies and market analysis was deemed a less desirable solution for the retail in this location. The enclosed “porch” area has operable walls in the form of industrial garage doors, fitting in with the traditionally less refined architecture of the light industrial lands of SLU. These operable walls retain the indoor / outdoor patron / pedestrian connection of the original porch proposal.</p> <p><b>2c.</b> This dock has been pulled back six feet from the property line to create a more generous sidewalk. The “dock” or “sun porch” has also been widened, and the retail utilizes similar garage door operable walls to create vibrant inside / outside spaces. The wider porch also provides more generous outdoor seating, under cover, but also with southern exposure.</p> <p><b>2d.</b> Per the Denny Way Streetscape Concept Plan the proposal is providing a 6’ setback from our property line which allows for 10’ of pedestrian walkway and a 6.5’ planted buffer. Additional width to achieve the 18’ desired width would have to be accommodated in the Denny ROW which SDOT is reluctant to provide.</p>
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Massing at EDG

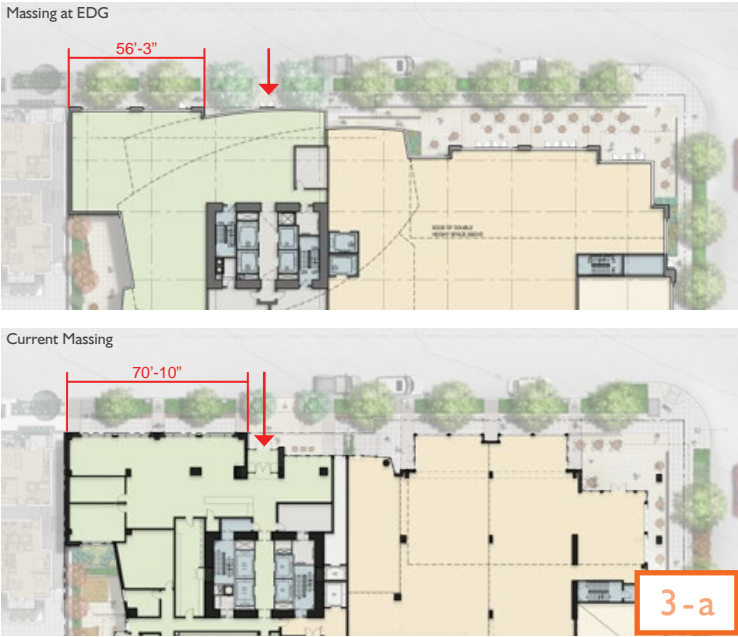


Current Massing



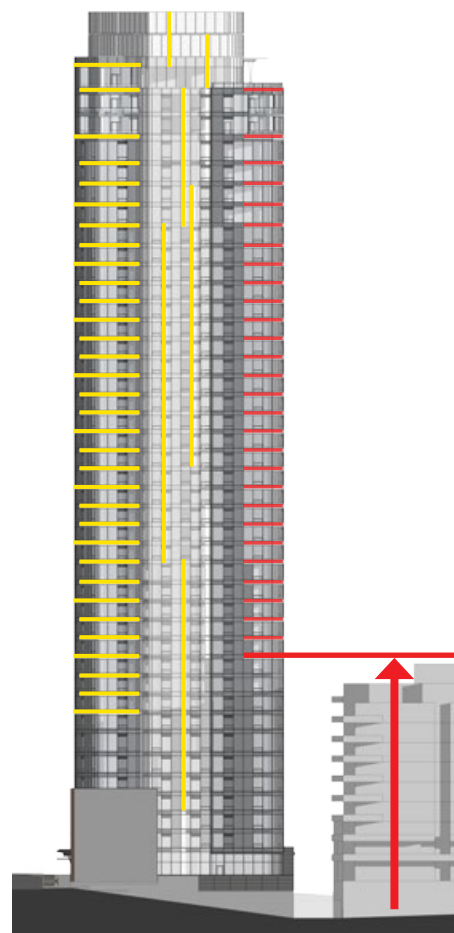
TITLE	DRB GUIDANCE AT EDG	RESPONSE
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2 GROUND LEVEL USES & STREETSCAPE	<p>e) While outside the Design Review Board purview, the Board strongly supported implementation of a pedestrian crosswalk of Denny, so pedestrians can access bus stops on Denny and the Terry Avenue Green Street continuity from downtown to South Lake Union is walkable. Terry Avenue is a designated “Neighborhood Heart” in the SLU Design Guidelines (see CS2-I-iv, for specific Terry Avenue Streetscape recommendations), and the Board considers Terry and Denny to be a minor Gateway. The applicants are encouraged to work with SDOT for this item in their SIP plans. (Guidelines CS2-B, CS2-I-iv, PLI-B, PL4)</p>	<p>2e. We have discussed this possibility with SDOT during the SIPs process, and it is not a possibility at this time.</p>
3 LOWER LEVEL & PODIUM EXPRESSION	<p>a) The Board endorsed the three-part massing for the Terry Avenue Façade as shown on page 48, but agreed the north ‘industrial’ mass should extend further south to better balance that street façade. The Board supported a portion of the southeast corner of the residential tower reaching grade along Terry. The Board suggested the glazed southeast corner, south of the vertical fin depicted on page 44, is logical and legible as the primary residential lobby entrance, and the ground floor program should adjust. (Guidelines CS2-C-3, CS3, PL2, DC2)</p> <p>b) The Board agreed the southeast corner of the option 3 tower lacked intermediate scale and plane shifts, such as those shown on option 2, page 25. The lower floors at this location should be refined to integrate overhangs, the adjacent fin, and/or cues of entry. (Guidelines DC2-C)</p> <p>c) The Board endorsed the cubic massing and the implied quality of masonry materials for the two ‘industrial’ blocks, and encouraged the applicants to not simply mimic historic architecture but rather interpret and distill key material and compositional principles for buildings of their own time. (Guidelines DC2, DC4-A)</p> <p>d) The Board agreed the southeast corner of the podium acts as a hinge for the Terry Avenue street kink, and that corner will be a ‘beacon’ for north-bound pedestrians and users. The Board supported the special corner column and extra tall, transparent storefront entry, as depicted on page 45, and encouraged further exploration of the corner and the upper podium for elements which acknowledge this pivotal location in the urban context. (Guidelines CS2-A, CS2-C-I, DC2)</p> <p>e) The Board endorsed high activation of the south podium rooftop, with diverse uses and a sophisticated and lush landscape palette. A complete landscape design for all roof terraces is required at the next meeting (no design was provided, however, the Board noted the precedent images on page 67 were a promising response to the guidelines). (Guidelines DC2-I-i, DC3-B-4, DC4-D)</p>	<p>3a. The tower was moved slightly further to the north in order to improve the ratio of tower to industrial base podiums at the street level (reducing the tower, increasing the industrial base). Similarly, the northern industrial base was widened nearly 15' from the EDG proposal to further improve this ratio. The location of the residential entry proposed by the board did not make for good interior planning. As you can see in the plans presented in this booklet, the residential entry is located on axis with the elevator lobby, which allows for a small lobby sitting area to one side. This entry must serve the leasing office as well, which is located further north within the main building lobby.</p> <p>3b. The tower massing has been revised so the eastern massing does not come all the way to grade. Instead it hovers above grade, integrated with the entry portal and canopy to signify the building entry. The vertical fin was eliminated in order to simplify the tower.</p> <p>3c. The proposal seeks to utilize modern building materials and technology to reflect but reinterpret the historic character of the neighborhood.</p> <p>3d. The design has further refined this corner to continue to reinforce its prominent location.</p> <p>3e. We have provided further plans and images defining the landscape for the L7 terrace as well as other terraces and streetscape.</p>

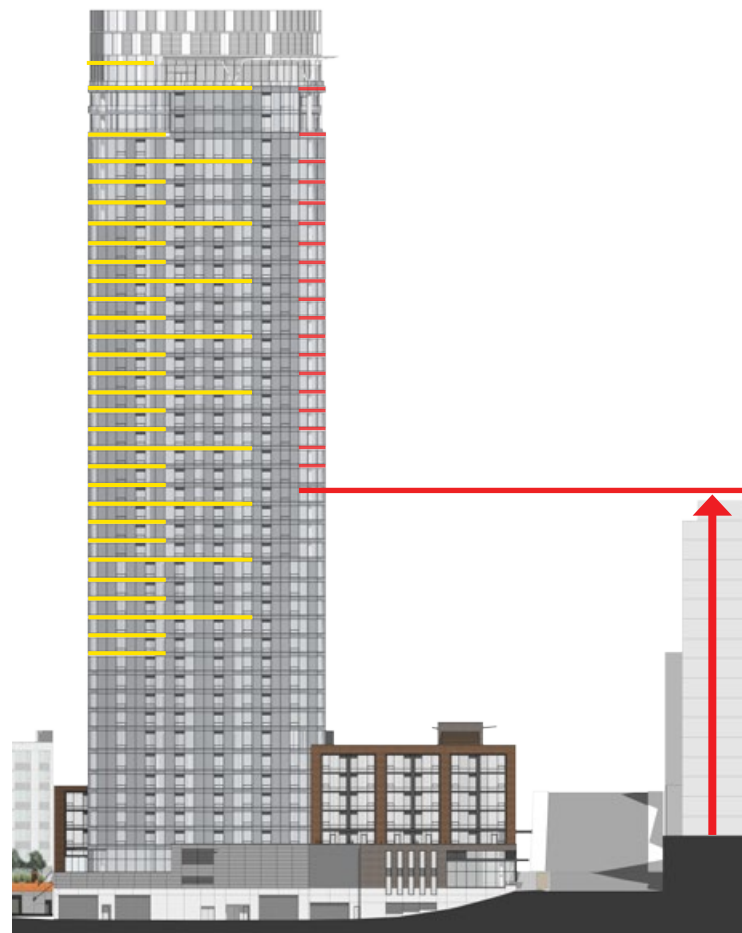




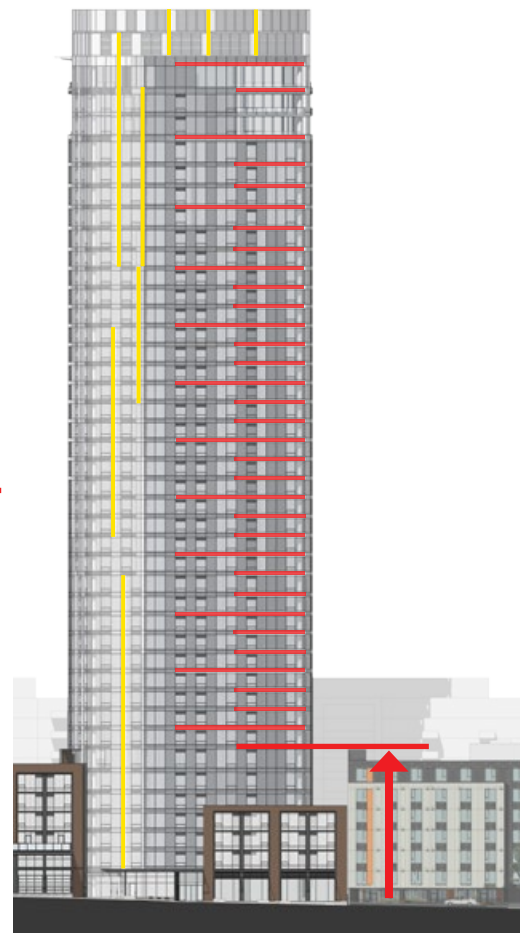
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<p><b>4</b> TOWER MATERIALITY &amp; DESIGN</p>	<p><b>a)</b> The Board supported the curving east and west facades of the option 3 tower, and the interlocking ovoid form that breaks up the north and south elevations (as shown on pages 37 and 49). The Board encouraged all four tower elevations to be carefully composed as they will be highly visible from a distance and proximate views, in a largely lower height, built-out context. (Guidelines CS2-A, DC2)</p> <p><b>b)</b> The Board encouraged the tower elevations to not be equal on all sides, and to respond to the distinct environmental factors and context cues that inform each different orientation. Consistent with SLU Design Guidelines, the Board emphasized environmental and sustainable factors for elevational design, as the applicant's stated response to this guideline currently appears to rely on specifications and internal systems. The Board requires a complete explanation of sustainable strategies and response to Guideline CSI-I at the next meeting.(Guidelines CSI, DC2-B, DC4-A)</p>	<p>4a. All four tower facades have been composed so that the massing forms are subtly but intentionally different.</p> <p>4b. The tower facades are different on each side with a mix of glass color, mullion color, and a change in accent spandrel direction. The project's sustainable strategies are outlined in this booklet.</p>



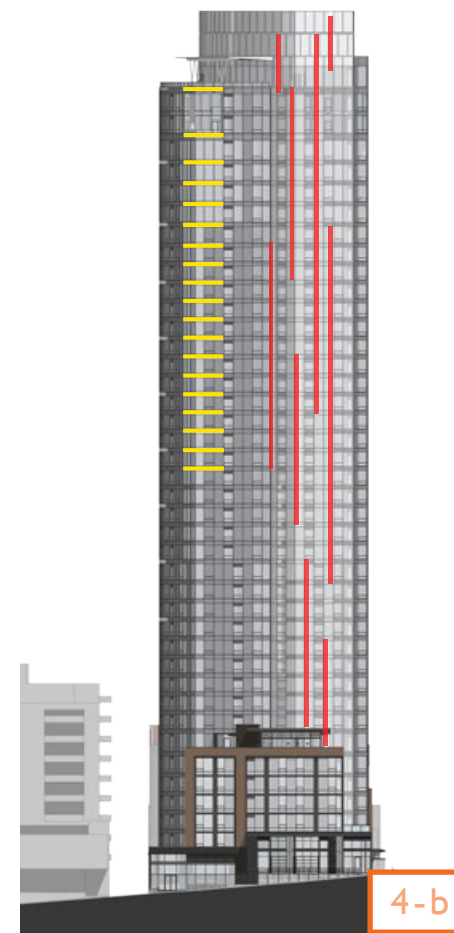
HORIZONTAL BANDING ON THE NORTHWEST FACADE RELATE TO THE HEIGHT OF ROLLIN STREET FLATS.



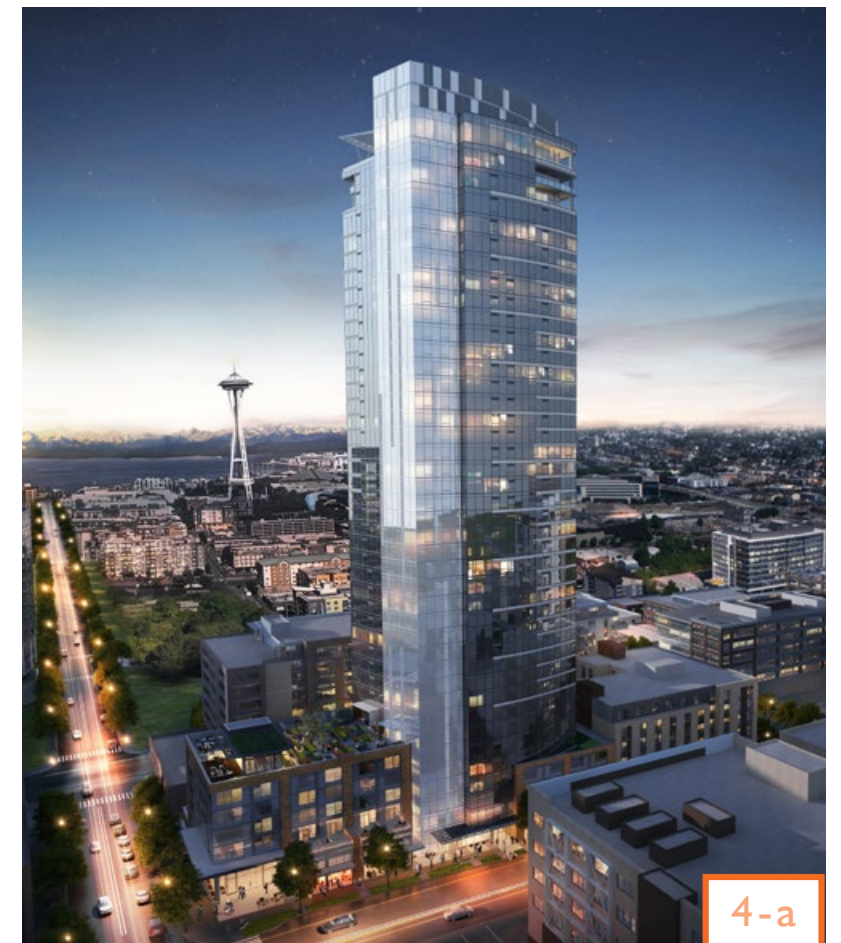
HORIZONTAL BANDING ON THE SOUTHWEST FACADE RELATE TO THE PAN PACIFIC TO THE SOUTH.



HORIZONTAL BANDING ON THE EAST FACADE IS TIED TO THE MASSING OF BOXCAR APARTMENTS



VERTICAL BANDS ON THE SOUTH FACADE CREATE A UNIQUE EXPRESSION ORIENTED DOWN TERRY AND DOWNTOWN.



4-a

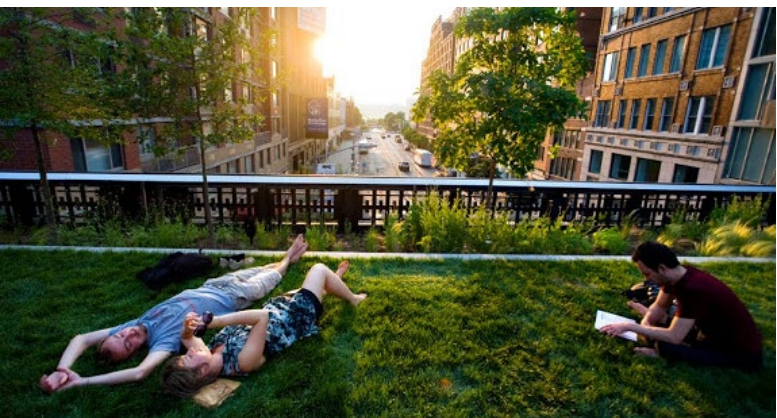
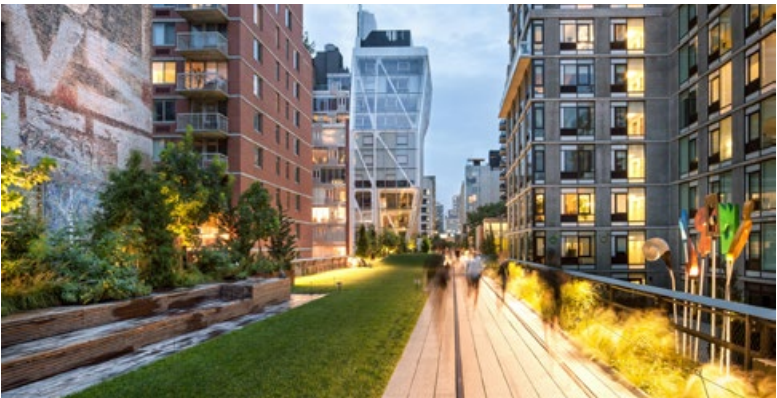
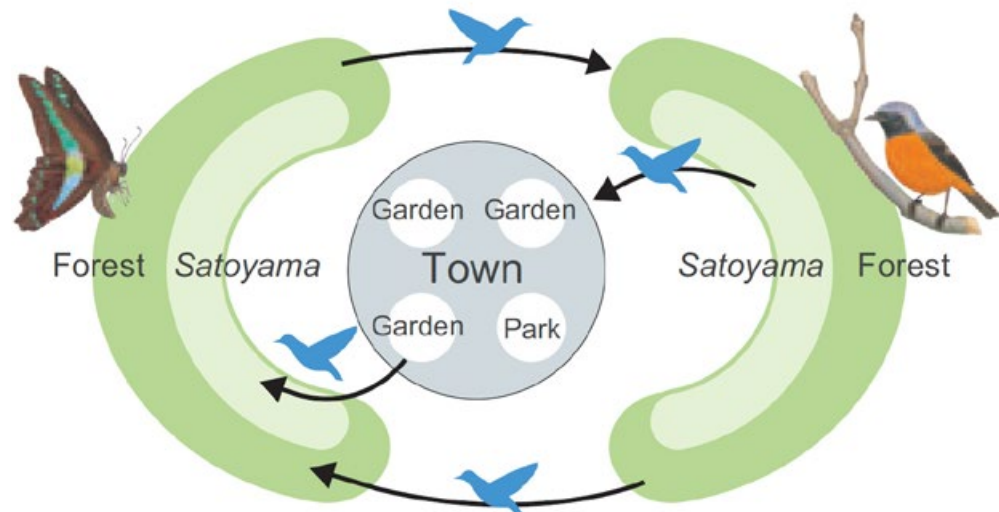
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# SUSTAINABILITY VALUES

Holland Partner Group LLC has partnered with NASH (North America Sekisui House) one of Japan’s largest developers of housing, to bring an ethos for sustainability rooted in restoring harmony between the built and natural environments. This philosophy is called Gohan no ki, or simply “five trees,” which can be applied to both landscape and buildings.

Satoyama is a Japanese term applied to the border zone or area between mountain foothills and arable flat land. It is the relationship of these areas with the built environment that Gohan no ki Landscaping hopes to restore. A core value of Gohan no ki Landscaping is selecting plants and tree species best suited to the local climate under the principle of “three trees for birds and two for butterflies.”. Utilizing local species sustains and restores regional biodiversity by linking urban landscapes with more natural landscapes outside of developed areas.



## ADDITIONAL PROGRAMS UTILIZED



The project team is exploring the opportunity to meet the water reduction goal of 50% through potable water reduction and management of storm water. The team is also researching the feasibility of meeting the energy reduction goal through energy modeling. Emissions from transportation are expected to be far below the district average due to the project location and access to multiple modes of transportation.



A requirement for the project, the building has been designed to meet the standards of the LEED gold certification and beyond.



# ADAPTING GOHAN NO KI TO THE BUILT ENVIRONMENT



## 1) Harmony with the streetscape

The entire project is designed in manner that harmonizes with the surrounding environment so that it contributes to the attractiveness of the streetscape, utilizing...

- Indoor / Outdoor spaces that can open seasonally to respond to favorable weather conditions
- Outdoor seating areas
- Emphasis on Human scale
- Quality Materials
- Seamless integration of building edge, hardscape and landscape.

## 2) A design that brings greater safety and security

The project is provided with multiple common spaces for tenants to enjoy social interactions as well as an open space conducive to fostering friendly ties with residents in the neighborhood. These spaces are effective in building unity among community members.

- Eyes on the Street: A combination of retail space that promotes inside / outside interaction, outdoor seating areas, tenant terraces overlooking the primary streets, units immediately above the street level the entire length of Terry, and substantially on Denny provide eyes on the street.

## 3) A design that brings greater comfort

A green environment is an important factor for tenants to live comfortable lives. The development team is committed to creating a green environment that grows more attractive with the passing of time by providing greenery in more than 30% of the site area.

- Indoor Air Quality: The living environment should be as free from toxins as possible. Holland has an established history of using low VOC paints, adhesives and flooring materials. To further improve the indoor air quality, the team is also researching the following strategies:
  - No added formaldehyde composite wood products.
  - Green cleaning program for all building common areas and education of tenants on green cleaning practices for their dwelling units.
  - Integrated pest management program for all interior and exterior areas of the building, to reduce potential exposure to toxic chemicals.

## 4) Reduction of environmental impacts

The project team has looked beyond LEED to other sustainability programs to incorporate features that are more experiential. Following are some of the imperatives the project team is considering:

- Build on a brownfield to preserve greenfields and sensitive areas from development.
- Urban Agriculture: Integrate opportunities for plants that provide sustenance for pollinators.
- Habitat exchange: Through a land trust organization, set aside a habitat area of at least one acre to be maintained in perpetuity.
- Human Powered Living: Provide secure, weather protected storage for bikes for 15% of occupants; Provide electric vehicle charging stations.
- Human Scale and Humane Places: Provide provisions for people to gather and connect; Provide elements along the project edge to support human scale (i.e. art, displays, seating).
- Beauty and Spirit: Provide design features intended solely for human delight, celebration of culture, spirit and place through incorporation of public art.
- Inspiration and Education: Provide an ongoing, public educational plan and share information about the building operation and performance.

Other Strategies:

- Target highest possible water use reduction in flow and flush fixtures within dwelling units, amenity spaces, and future retail space.
- Landscape and irrigation will be designed to use at least 50% less potable water through selection of drought tolerant plants and high efficiency irrigation systems and watering practices.
- Green roofs and bio-retention are provided in addition to detention to further reduce peak stormwater discharge.
- Selection of Energy Star dishwashers and clothes washer – which will also reduce energy use.
- Optimize a range of building envelope characteristics, including amount of glazing, thermal performance and day lighting levels.
- Optimize the building envelope to reduce heating and cooling loads sufficiently to allow the use of smaller / fewer air conditioning units of higher efficiency.
- Analyze available natural light along the perimeter; lighting systems in public spaces can be dimmed during periods of sufficient daylight, which further reduces cooling loads in the building.
- Utilize conventional energy efficiency technologies, including LED lighting, Energy Star appliances and high efficiency water source heat pumps.

## 5) Preservation and restoration of the natural environment

The project site has contaminated soils which the development team is committed to removing.

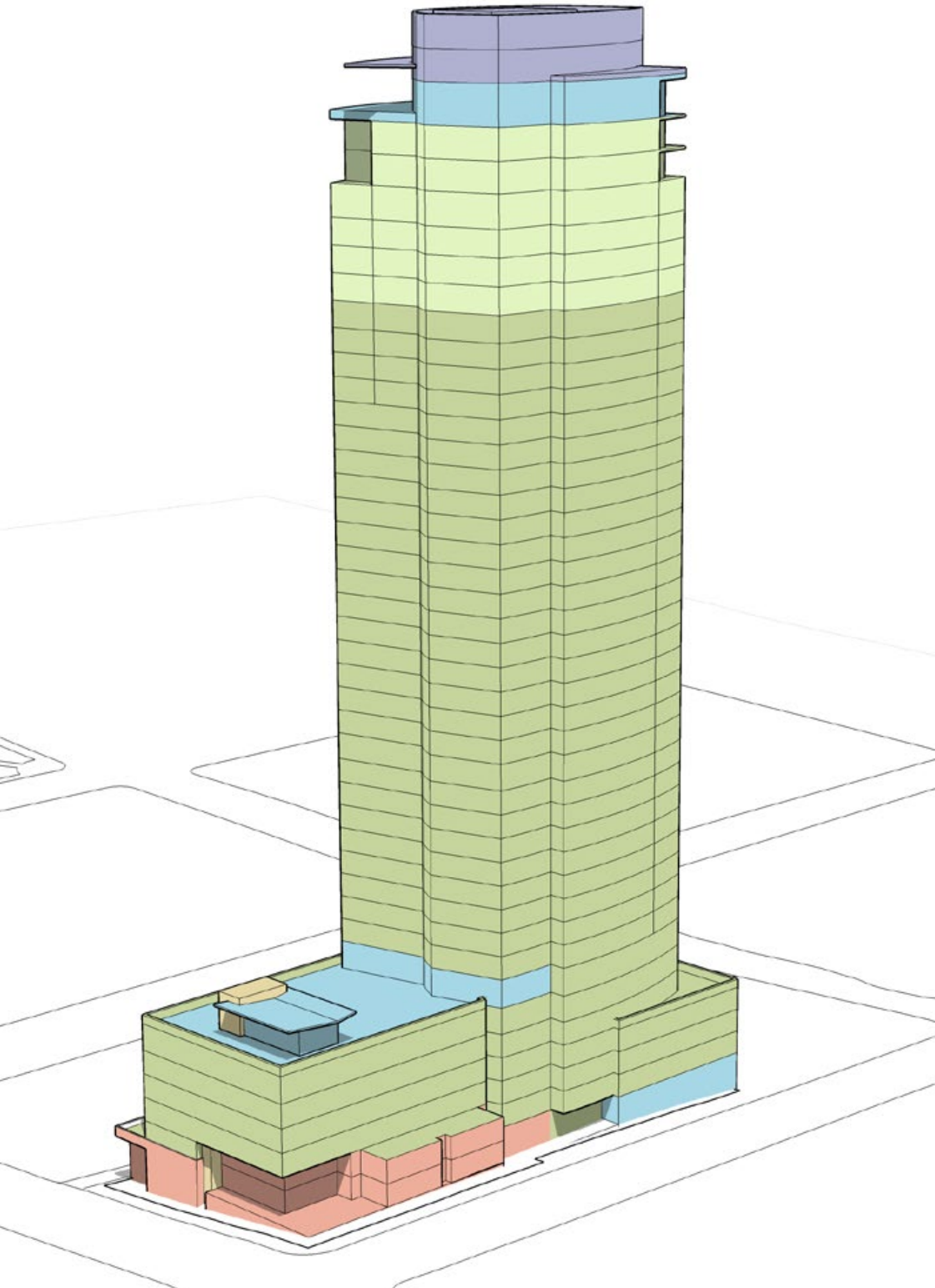
Additionally, the project team will be remediating contaminated soils outside of their site area, going above and beyond what is required.

Rural / Forest Land Preservation: As part of the height bonus incentive program the Project will purchase Regional Development Credits in order to achieve its bonus floor area. Purchase of these Regional Development Credits benefits the residents of Seattle and King County because rural and resource lands will be permanently preserved as such. This preservation focuses density in urban areas, and helps Seattle and the County become more sustainable by promoting more efficient development patterns.

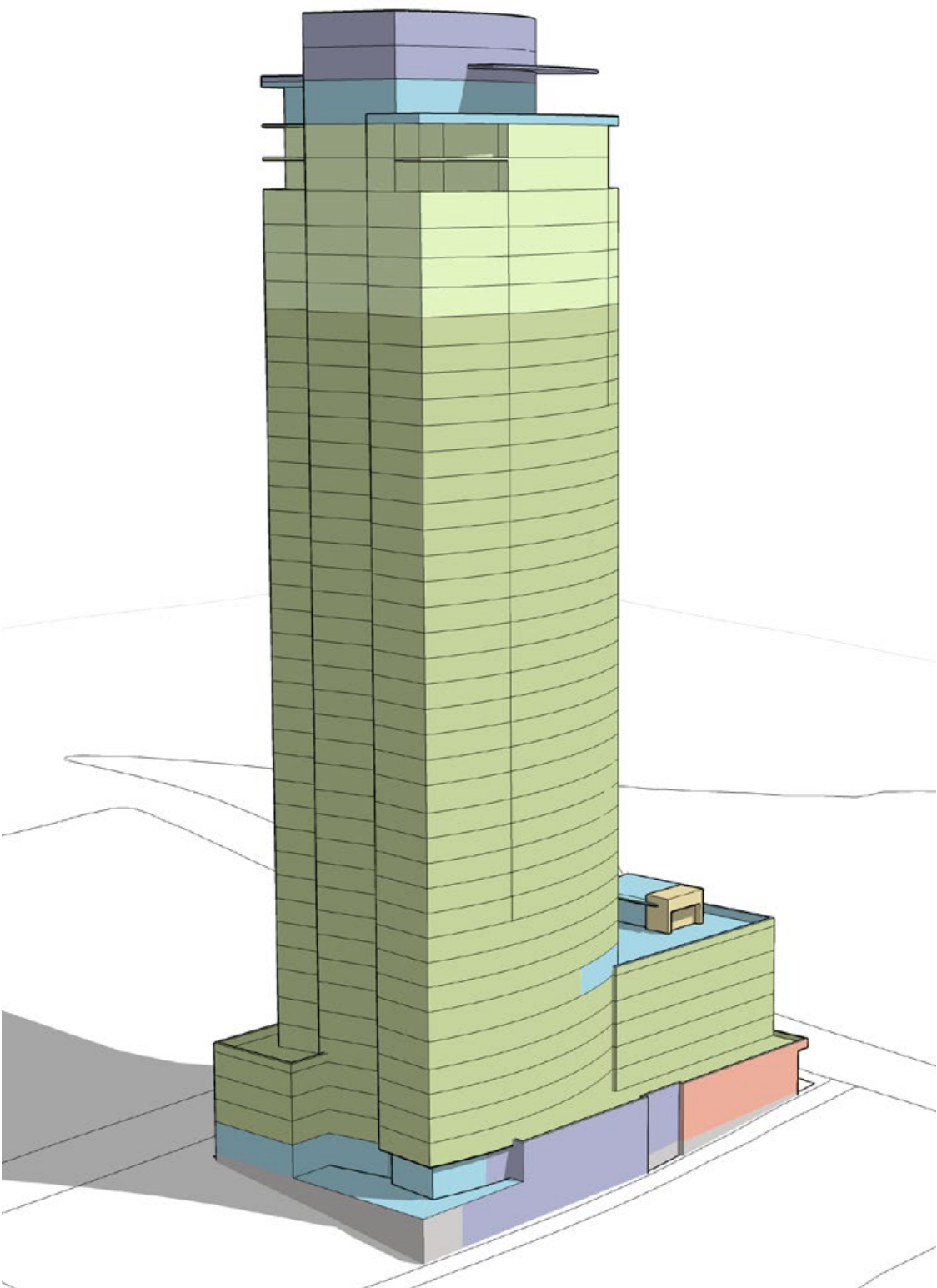
Alternative Transportation Emphasis: The site of this project is well served by transit which connects directly to regional transit options, the building will accommodate well beyond the required number of bicycles per zoning code, most tenant needs (grocery, exercise, job base, entertainment, connection to regional transit) are within easy walking distance.



# PROGRAM MASSING AND SECTION



PROGRAM MASSING DIAGRAM SOUTHEAST



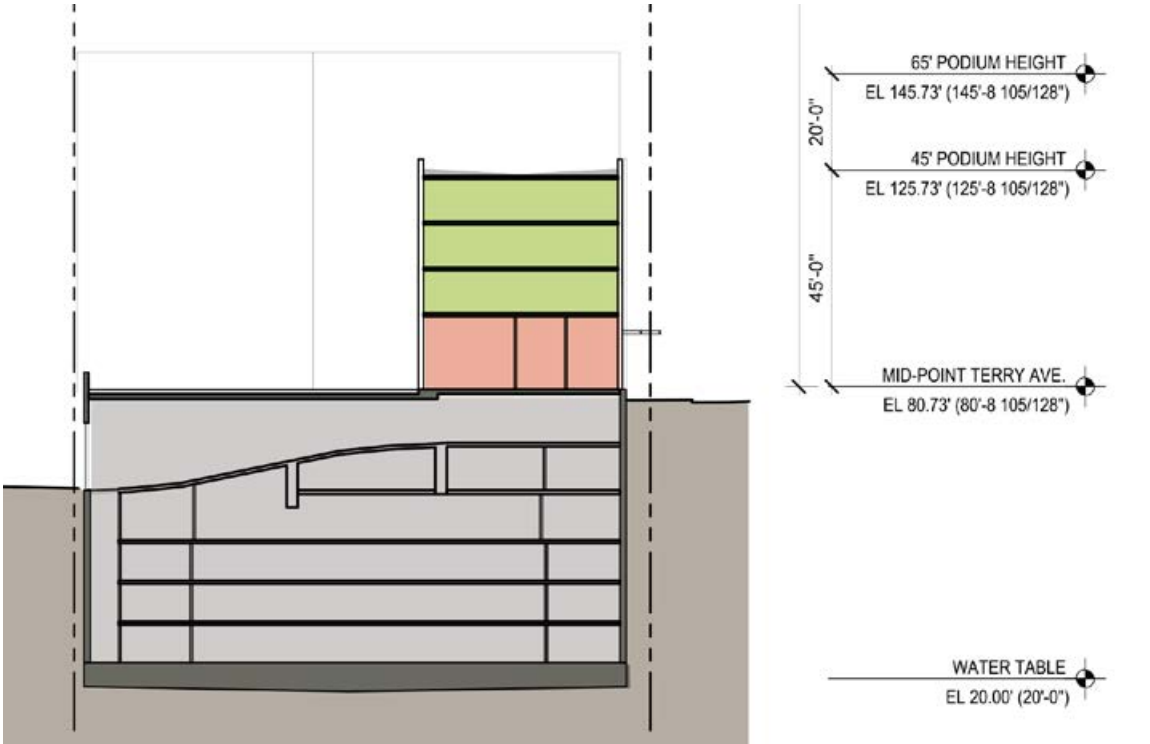
PROGRAM MASSING DIAGRAM NORTHWEST

## SPACE FUNCTION KEY

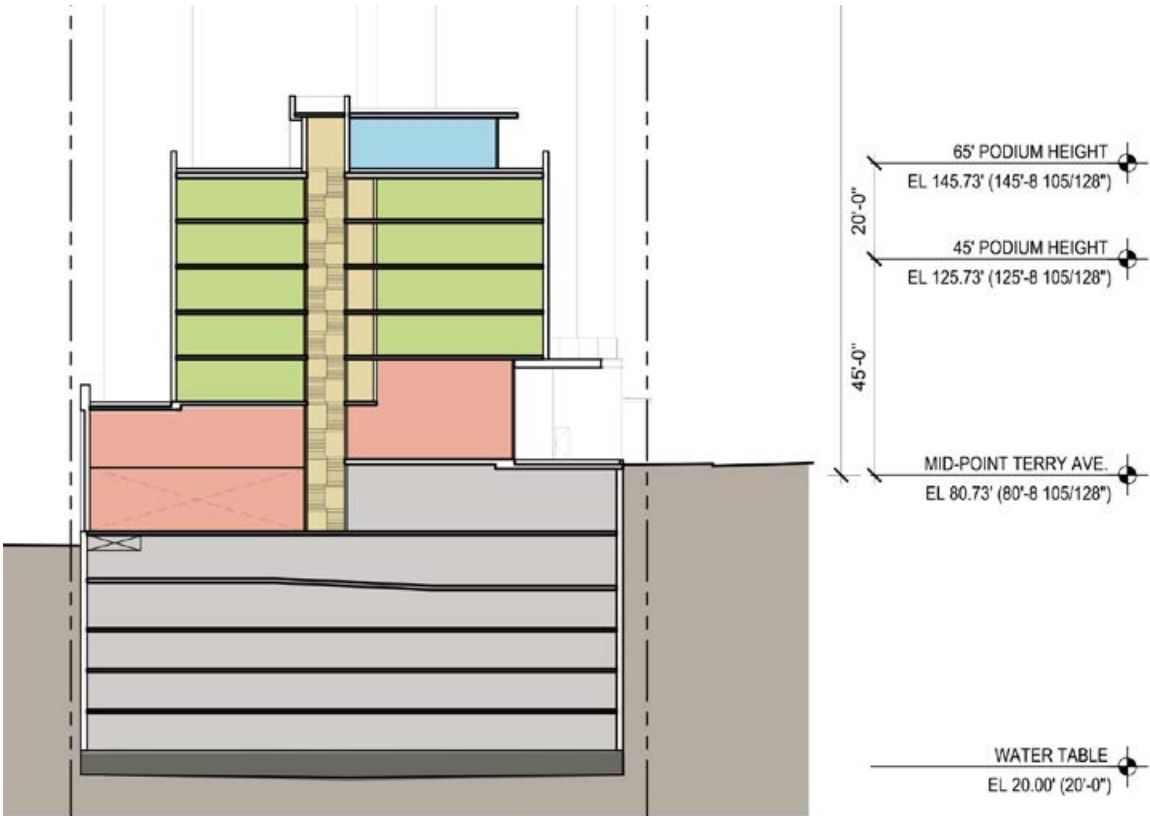
- RESIDENTIAL
- RESIDENTIAL PENTHOUSE
- STAIR / ELEVATOR CORE
- RETAIL
- PARKING
- AMENITIES
- BOH / LOADING / MECH



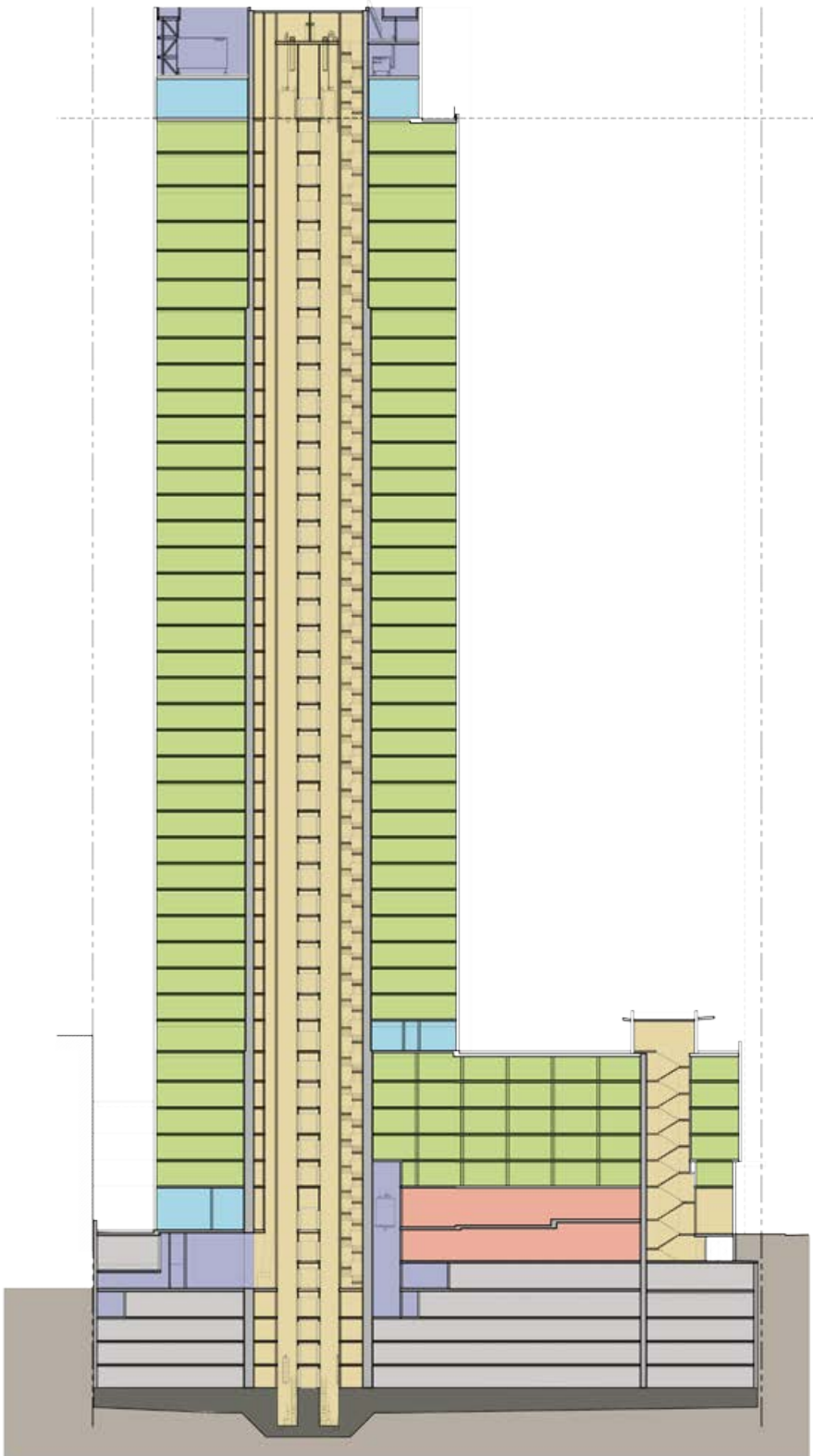
# ENLARGED BUILDING SECTIONS



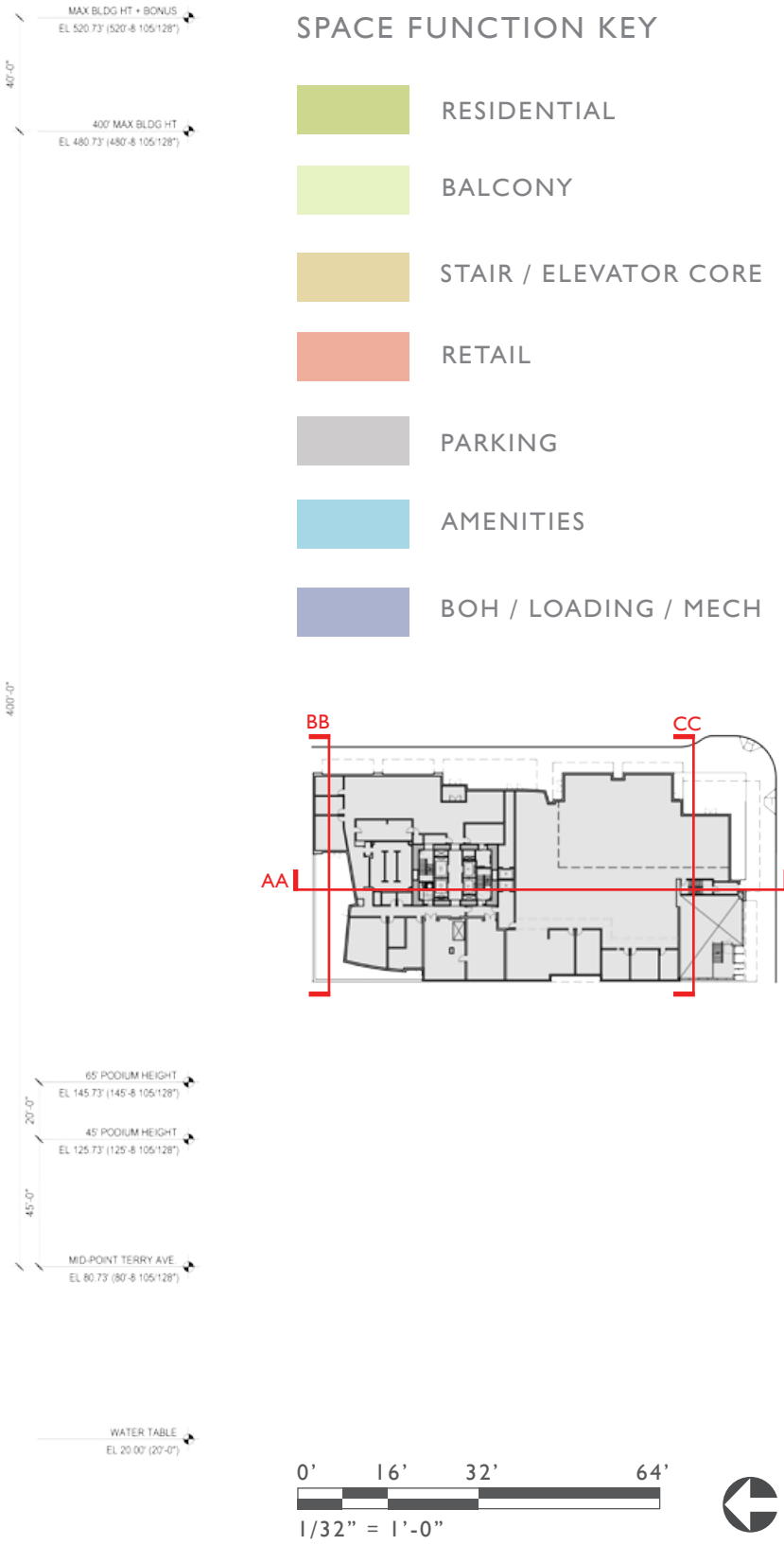
SECTION BB – EAST/WEST THROUGH PODIUM



SECTION CC – EAST/WEST THROUGH PODIUM



SECTION AA – NORTH/SOUTH THROUGH PROJECT - 1/64" = 1'-0"





# SITE AND CONTEXT ENTRY DIAGRAM





# DENNY AND TERRY STREETSCAPE PLANS ADOPTIONS

**Denny Way Streetscape Concept Plan**  
Plan created in July 2019. Revisited in October 2019.

**Contributing Stakeholders**

John Currey	Urban Alliance
Ben Carter	Seattle Community
Dan Harris	Seattle Senior Center
Mark Harris	Seattle College of Arts
Lyn Krasch	City Properties Inc.
100 Street Design Team	Seattle Properties Inc.
100 Street Design Team	Seattle Properties Inc.
100 Street Design Team	Seattle Properties Inc.

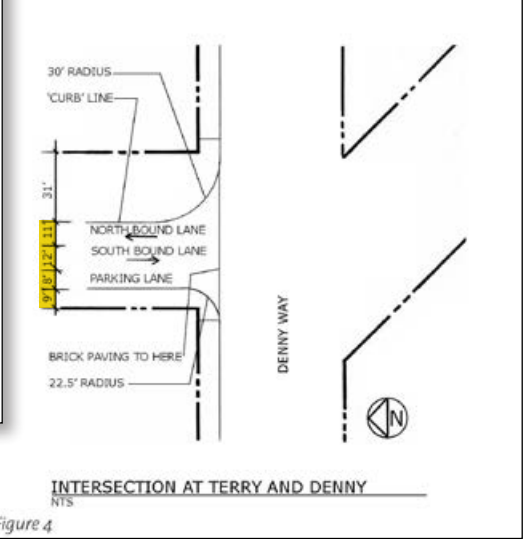
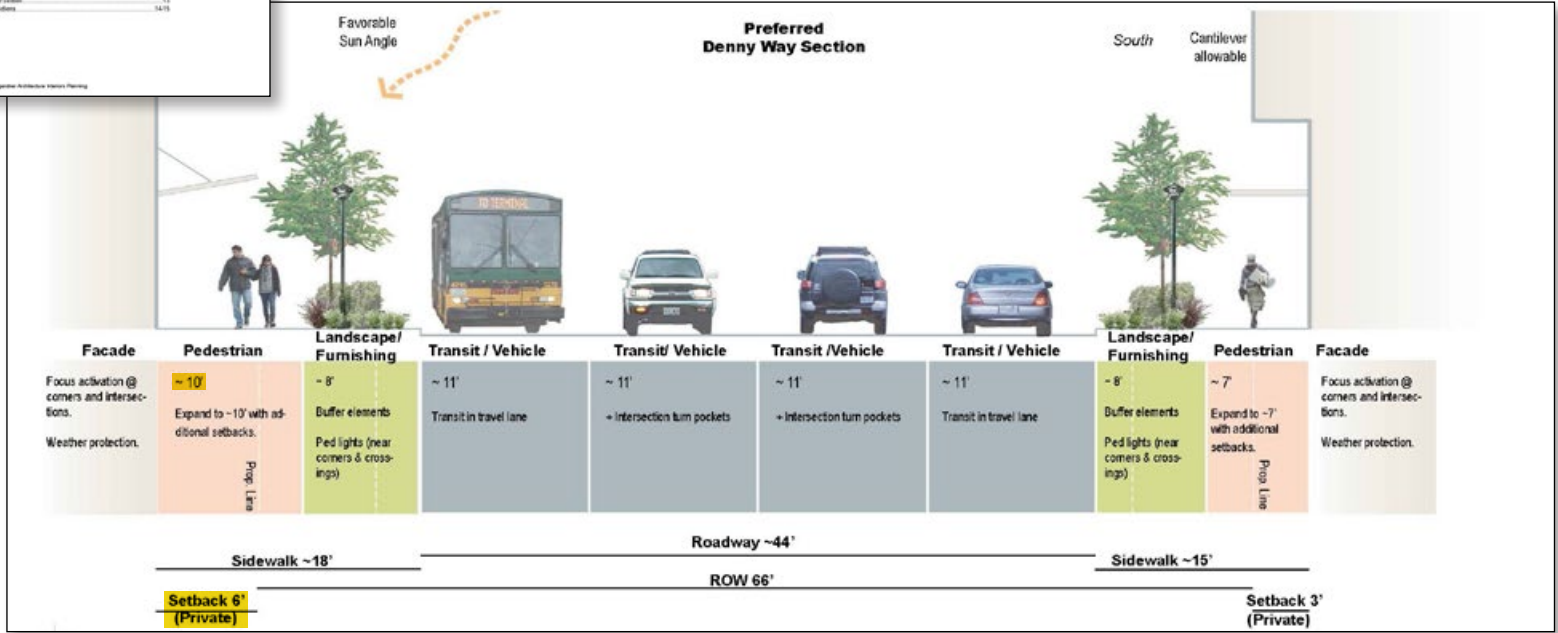
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Project Overview	3
Background & Context	4-5
Denny Way Vision	6
Project Objectives	7
Proposed Actions	8

**Streetscape Concept Plan**

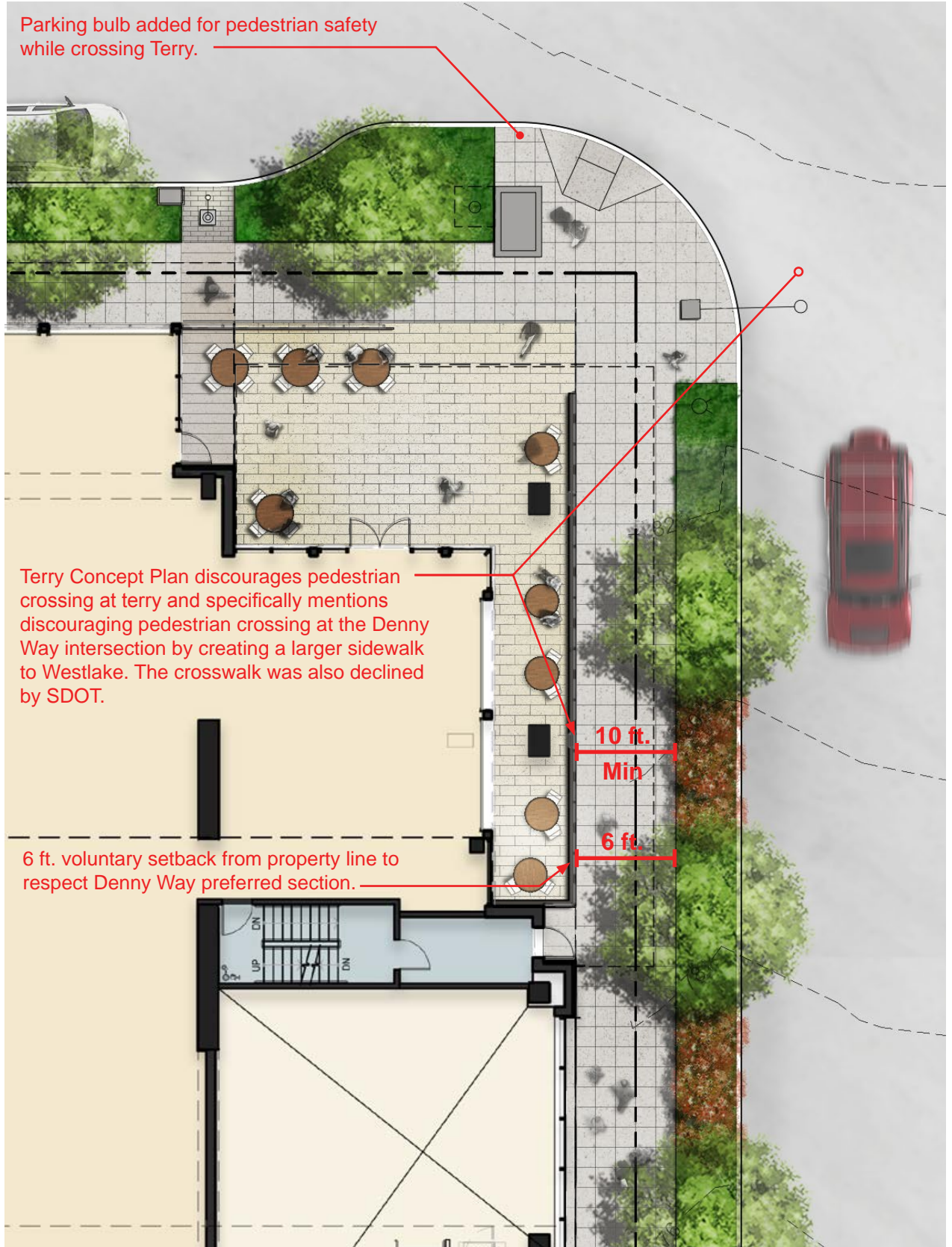
Streetscape Concept Plan	9-10
Concept Color Palette	11
Concept Color Palette	12
Concept Color Palette	13
Proposed Street Section	14-15

Both the Terry Avenue North Street Design Guidelines and Denny Way Streetscape Concept Plan were carefully considered in design of the streetscapes around our site. Both make reference to encouraging pedestrian traffic along the north side of Denny, which we have done by setting our building back an additional 6' from the property line (also defined by the section below) as well as creating an enlarged pedestrian sidewalk 10' wide. This not only creates a large buffered path for the pedestrian, but encourages people to travel west to the major pedestrian crossing at Westlake and not try to cross Denny where unsafe.



**Denny** The intersection at Denny Way and Terry Avenue North would seem to be a location for a gateway on to Terry from downtown. However, topography and vehicle speeds make the pedestrian crossing across Denny difficult, and crossing at Denny should be discouraged. In order to encourage pedestrians to continue on Denny to the crossing at Westlake, wider sidewalks on the north side of Denny should be considered as part of new development. See Figure 4.

**John** John Street runs east from Westlake Avenue to Terry and has low vehicle use. The topography and large grade change between Terry and Boren do not allow for a street. The former City right-of-way between Terry and Boren is now private property, divided between the two adjacent landowners. A portion remains undeveloped and is one of the few green places along the street.





GRADE LEVEL PLAN – LI

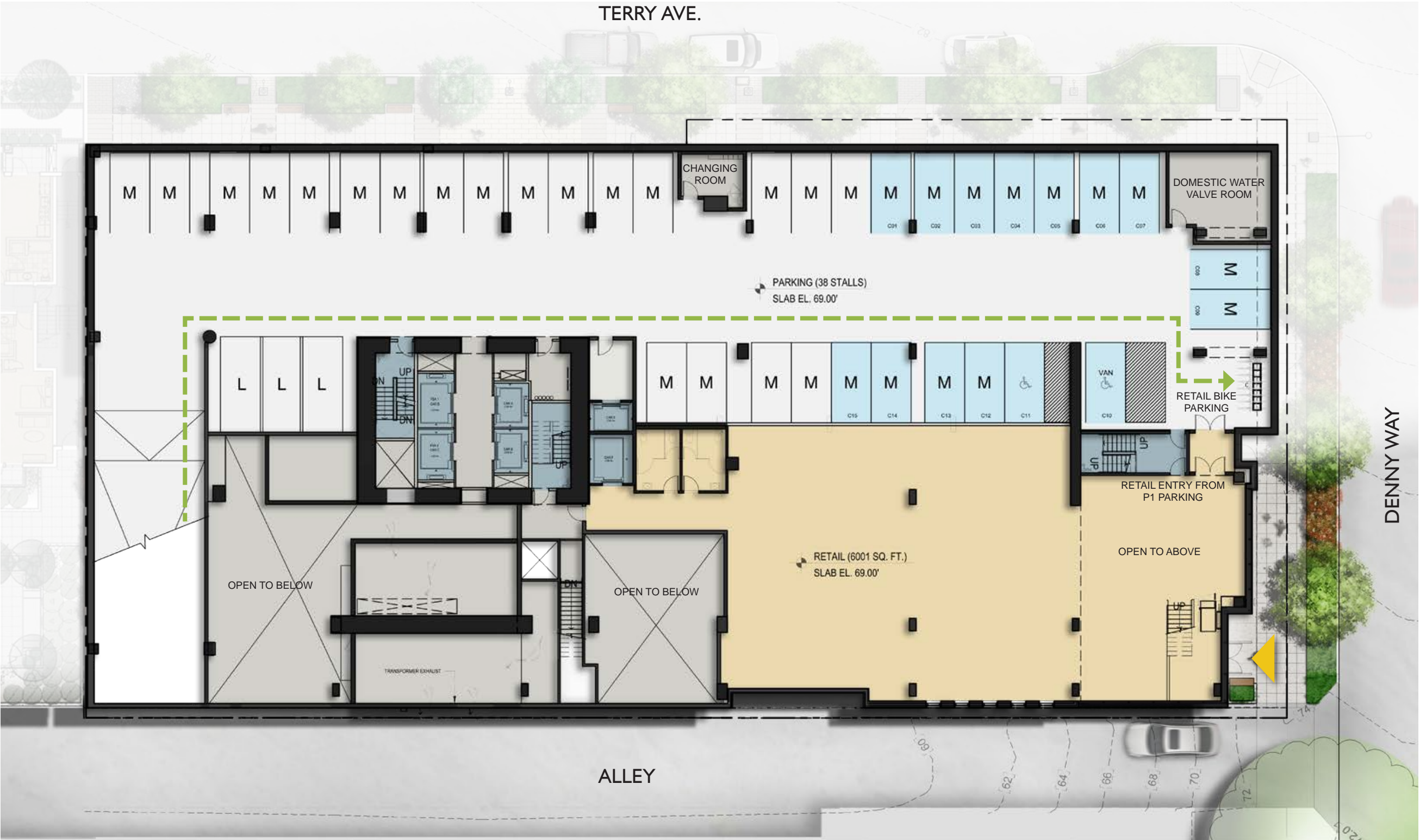


SPACE FUNCTION KEY

- RESIDENTIAL
- BALCONY
- COMMON SPACE
- STAIR / ELEVATOR CORE
- RETAIL
- PARKING
- AMENITIES
- BOH / LOADING / MECH
- MAJOR RESIDENTIAL OR OFFICE ENTRY
- RETAIL ENTRY
- AUTOMOTIVE ENTRY
- BIKE ENTRY

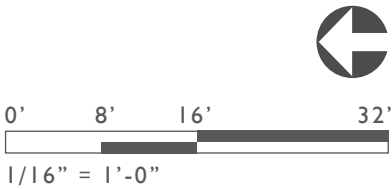


GRADE LEVEL PLAN – P1



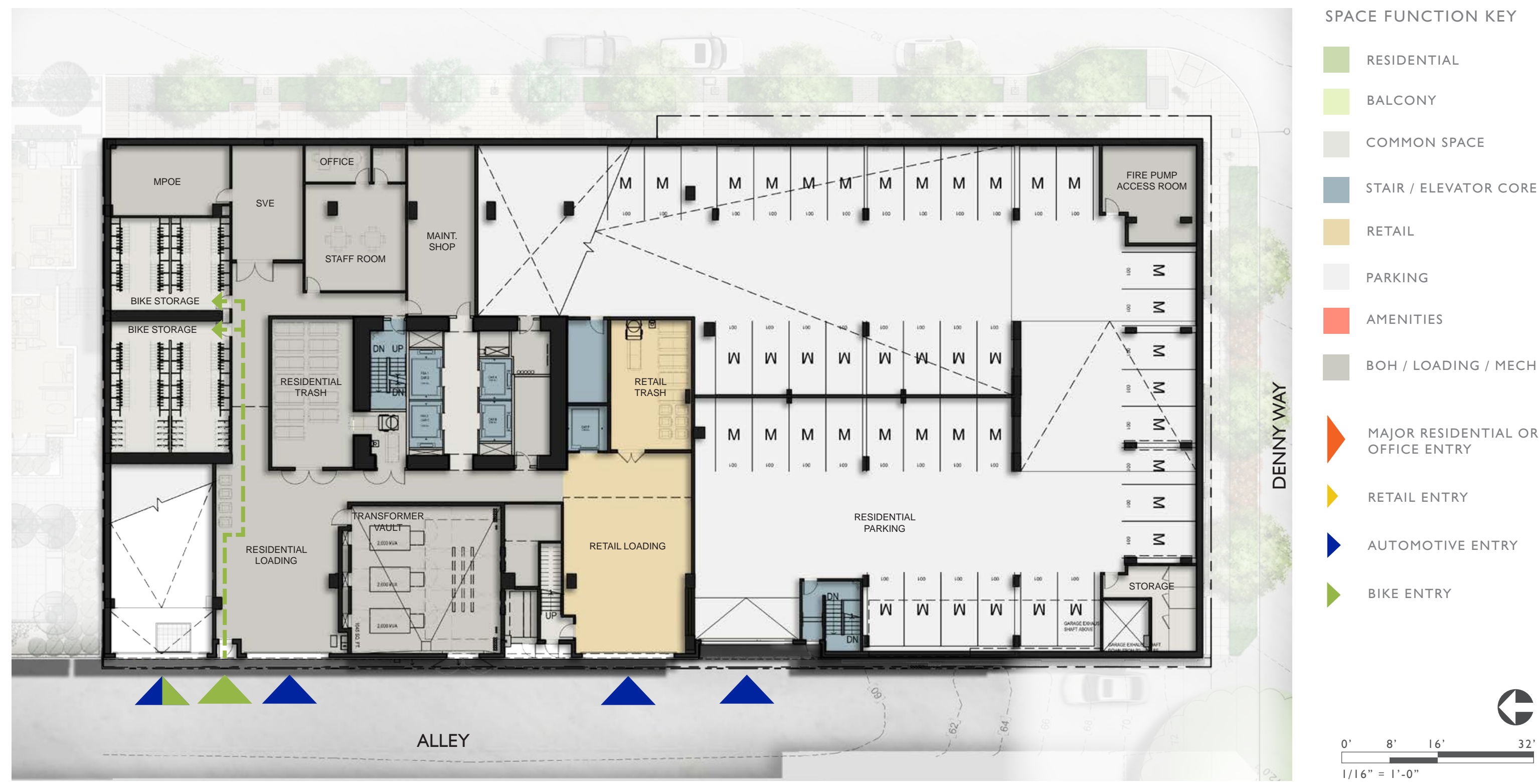
SPACE FUNCTION KEY

- RESIDENTIAL
- BALCONY
- COMMON SPACE
- STAIR / ELEVATOR CORE
- RETAIL
- PARKING
- COMMERCIAL PARKING
- AMENITIES
- BOH / LOADING / MECH
- MAJOR RESIDENTIAL OR OFFICE ENTRY
- RETAIL ENTRY
- AUTOMOTIVE ENTRY
- BIKE ENTRY





GRADE LEVEL PLAN – P2















PODIUM DESIGN



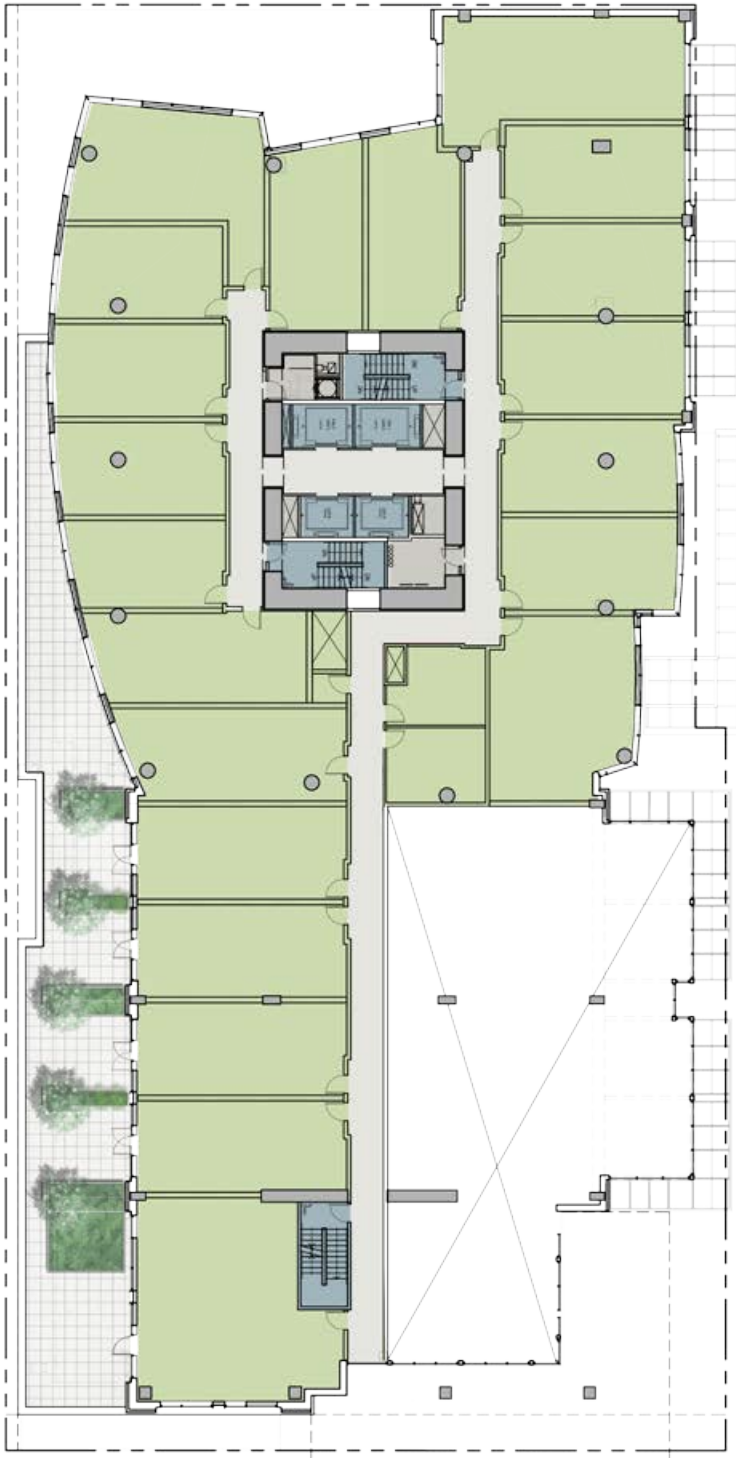




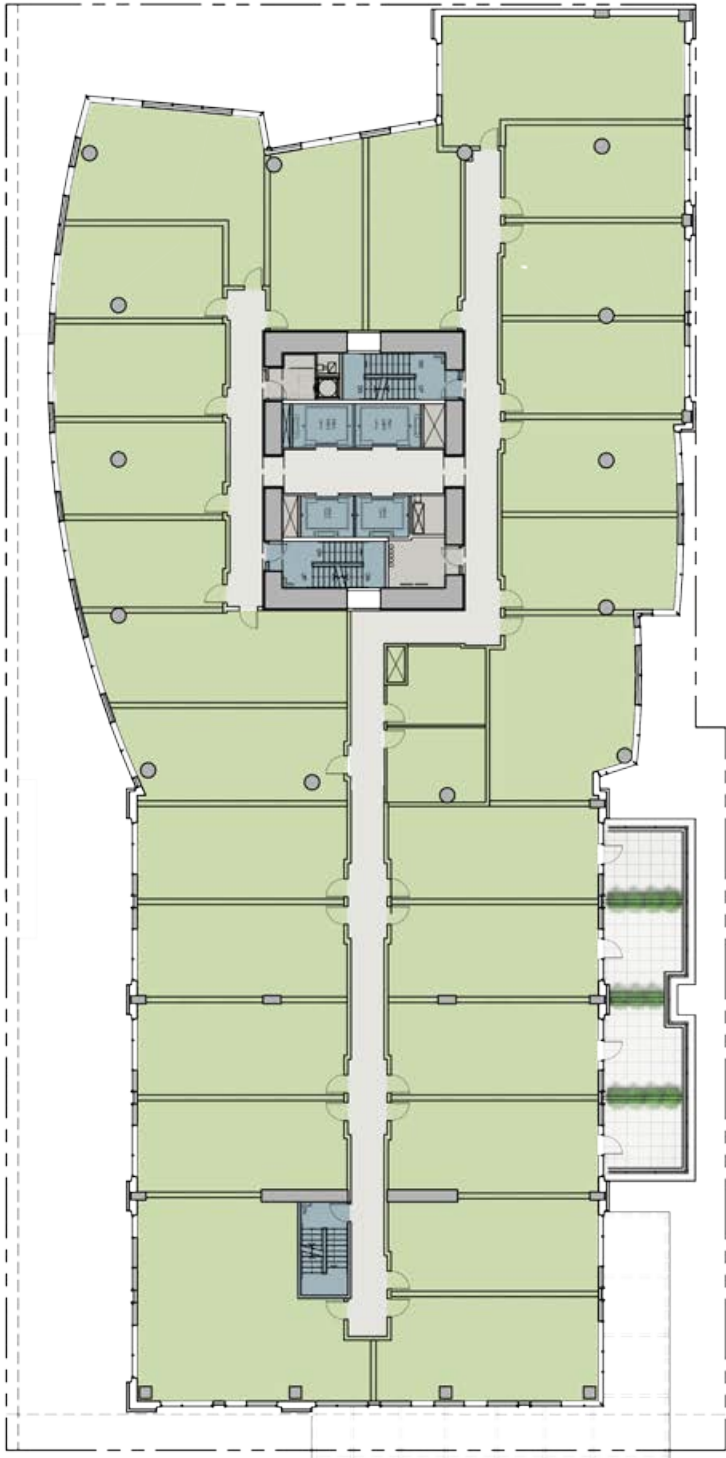




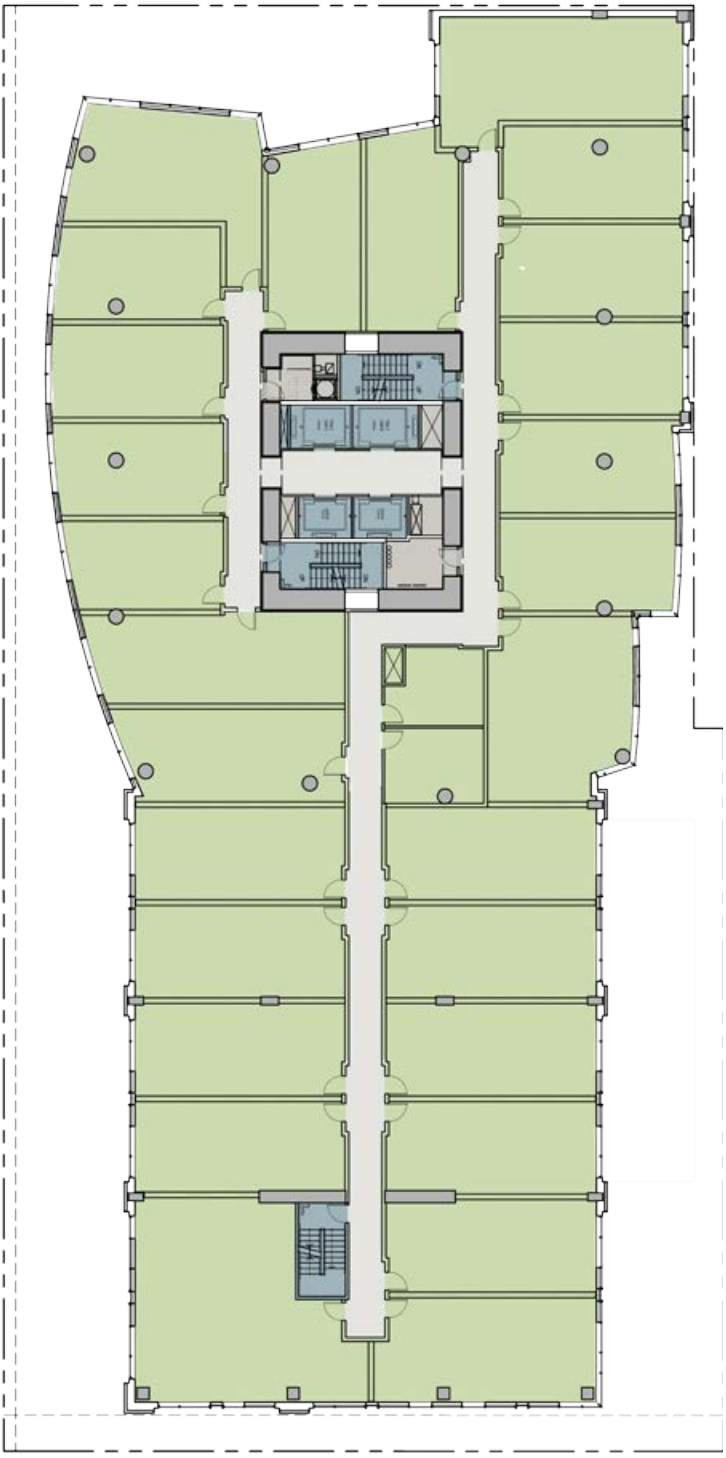
PODIUM LEVEL PLANS



L2 FLOOR PLAN  
1/32" = 1'-0"



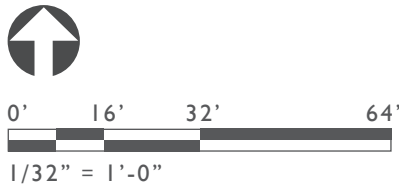
L3 FLOOR PLAN  
1/32" = 1'-0"



L4 FLOOR PLAN  
1/32" = 1'-0"

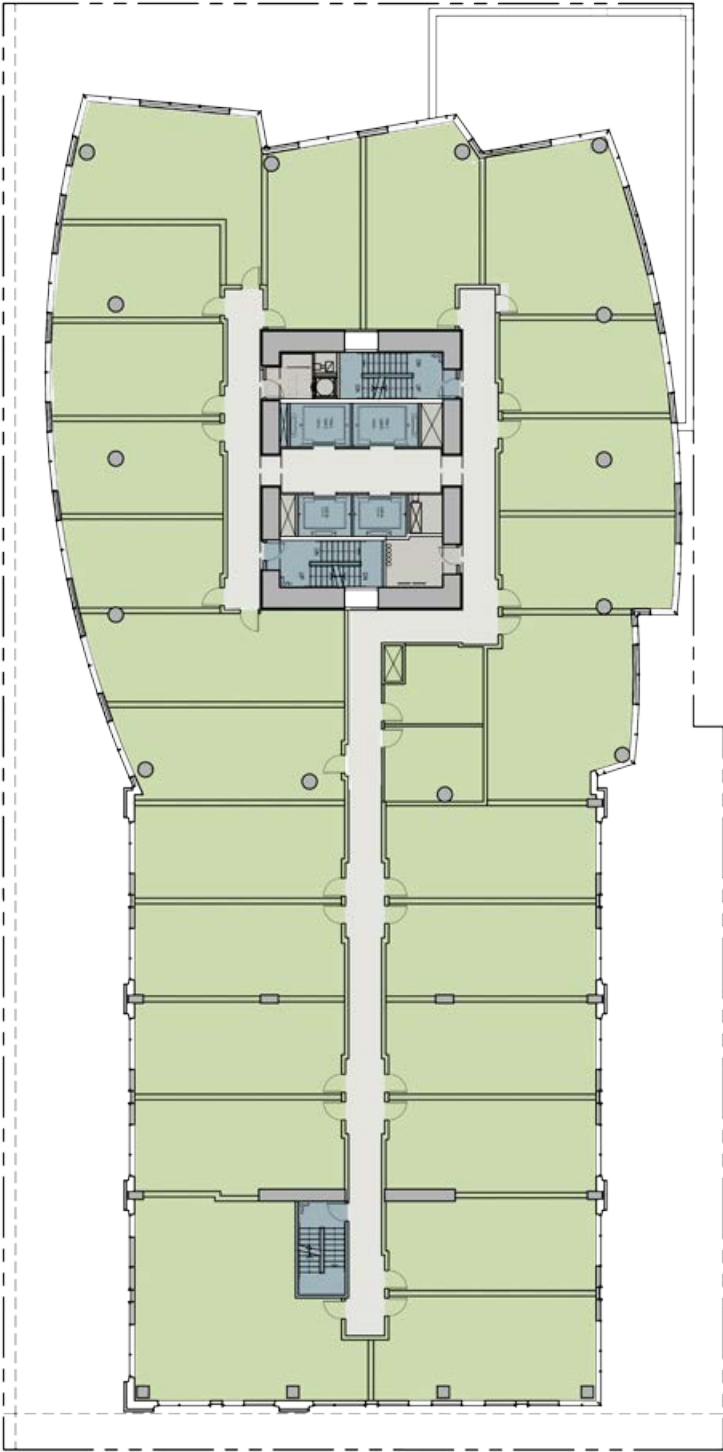
SPACE FUNCTION KEY

- RESIDENTIAL
- COMMON SPACE
- STAIR / ELEVATOR CORE
- RETAIL
- PARKING
- AMENITIES
- BOH / LOADING / MECH





# PODIUM LEVEL PLANS



L5-6 FLOOR PLAN  
1/32" = 1'-0"



L7 FLOOR PLAN  
1/32" = 1'-0"

### SPACE FUNCTION KEY

- RESIDENTIAL
- COMMON SPACE
- STAIR / ELEVATOR CORE
- RETAIL
- PARKING
- AMENITIES
- BOH / LOADING / MECH





# GROUND LEVEL AT EDG VS CURRENT PLAN

## PREVIOUS

The design presented at EDG created a large covered porch that allowed for retailers to spill out into an exterior space during summer months. After analyzing solar exposure and basic restaurant operations, we concluded that this space will be hard to use during the prime hours of business for these types of tenants. Dinner and afternoon crowds would always be in the shade which will make the space less desirable to a tenant, which means it may not attract the uses that would activate such a porch. An alternate approach was needed to make this a vibrant and desired space.



## CURRENT

Our new design encloses two bays of the outdoor porch with a light steel structure with operable doors and windows along the entire street level. This not only creates a space usable 365 days a year, but helps break down the massing and emphasize the corner. We also enlarged the retail porch along the south facade where sunlight is more available.







ENCLOSED RETAIL PORCH



# EAST PODIUM PERSPECTIVE



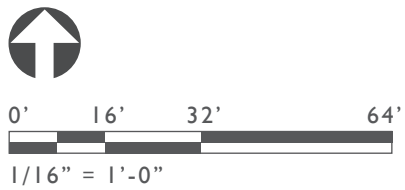
EASTERN FACADE AND STREETScape



# EAST PODIUM ELEVATION + LI PLAN



(MATERIAL PICTURES ON PAGE 47)





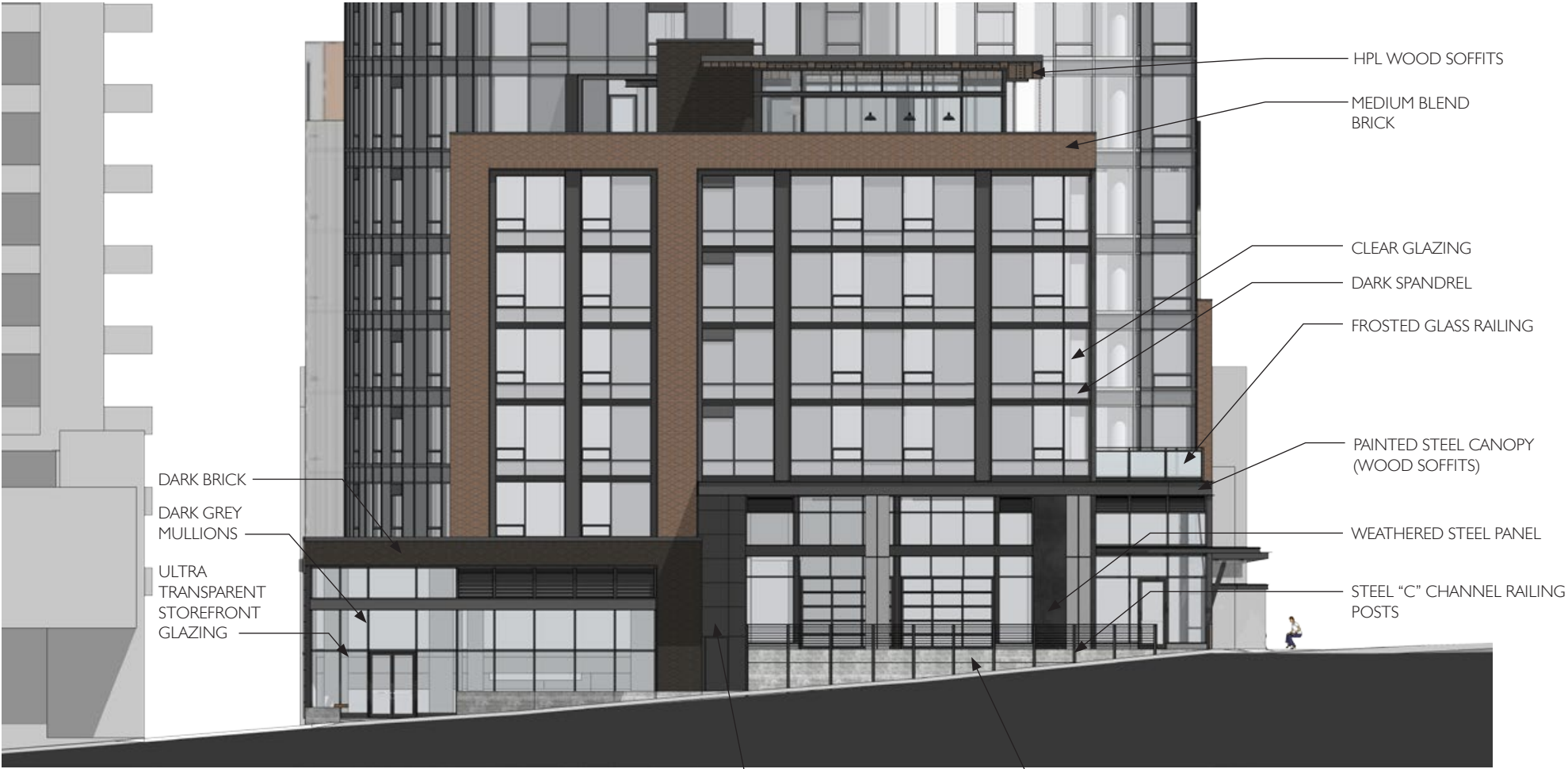
SOUTH PODIUM PERSPECTIVE



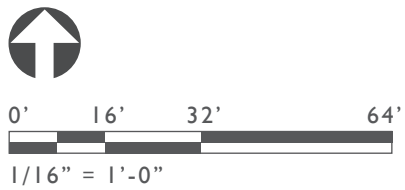
PROJECT VIEW FROM SOUTH



# SOUTH PODIUM ELEVATION + LI PLAN



(MATERIAL PICTURES ON PAGE 47)





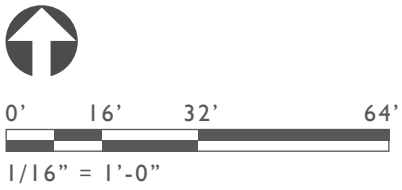
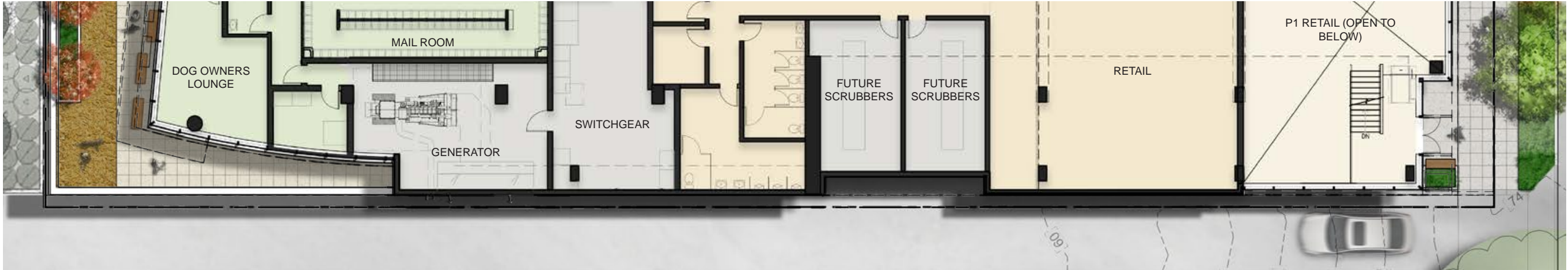
SOUTHWEST PODIUM PERSPECTIVE (AT ALLEY)



SOUTHEASTERN CORNER AND ALLEY



WEST PODIUM ELEVATION + LI PLAN





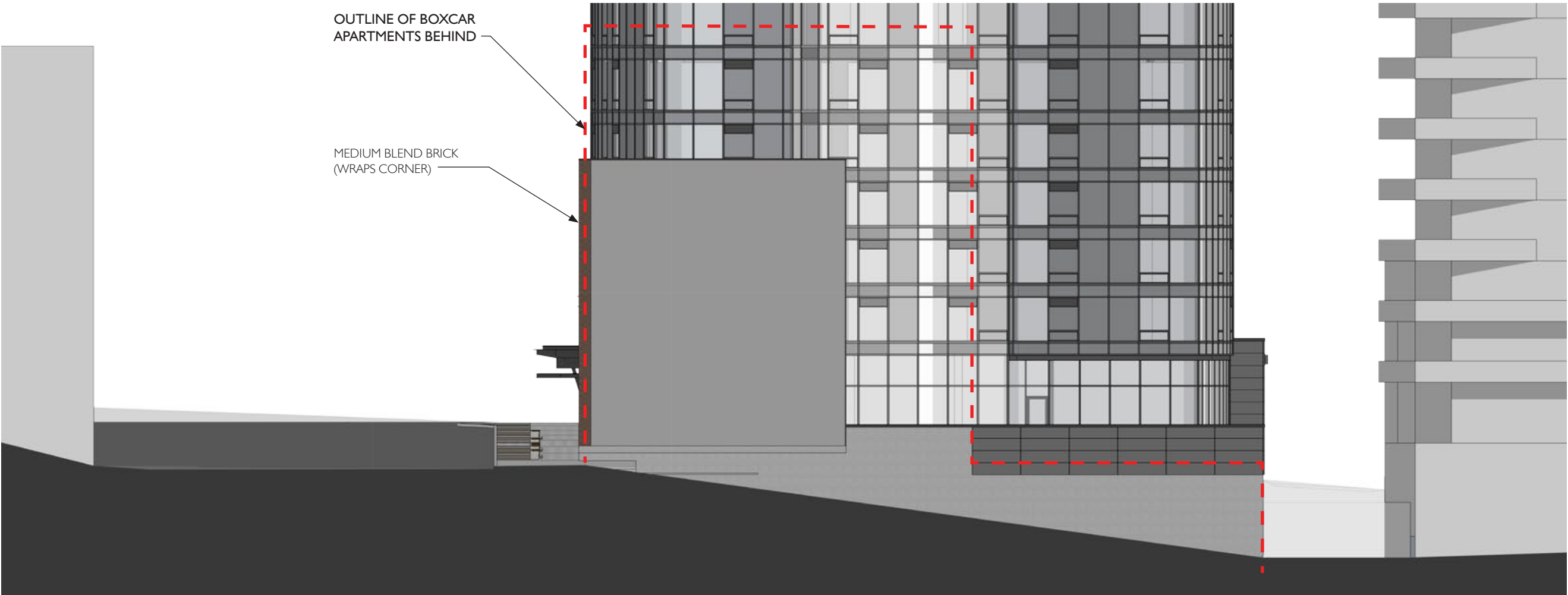
NORTH PODIUM PERSPECTIVE



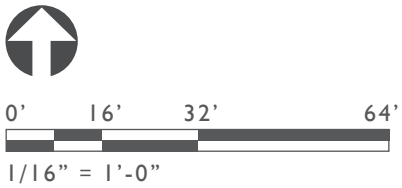
NORTHERN PODIUM AND DOG RUN (BOXCAR FADED IN FOREGROUND)



# NORTH PODIUM ELEVATION + LI PLAN

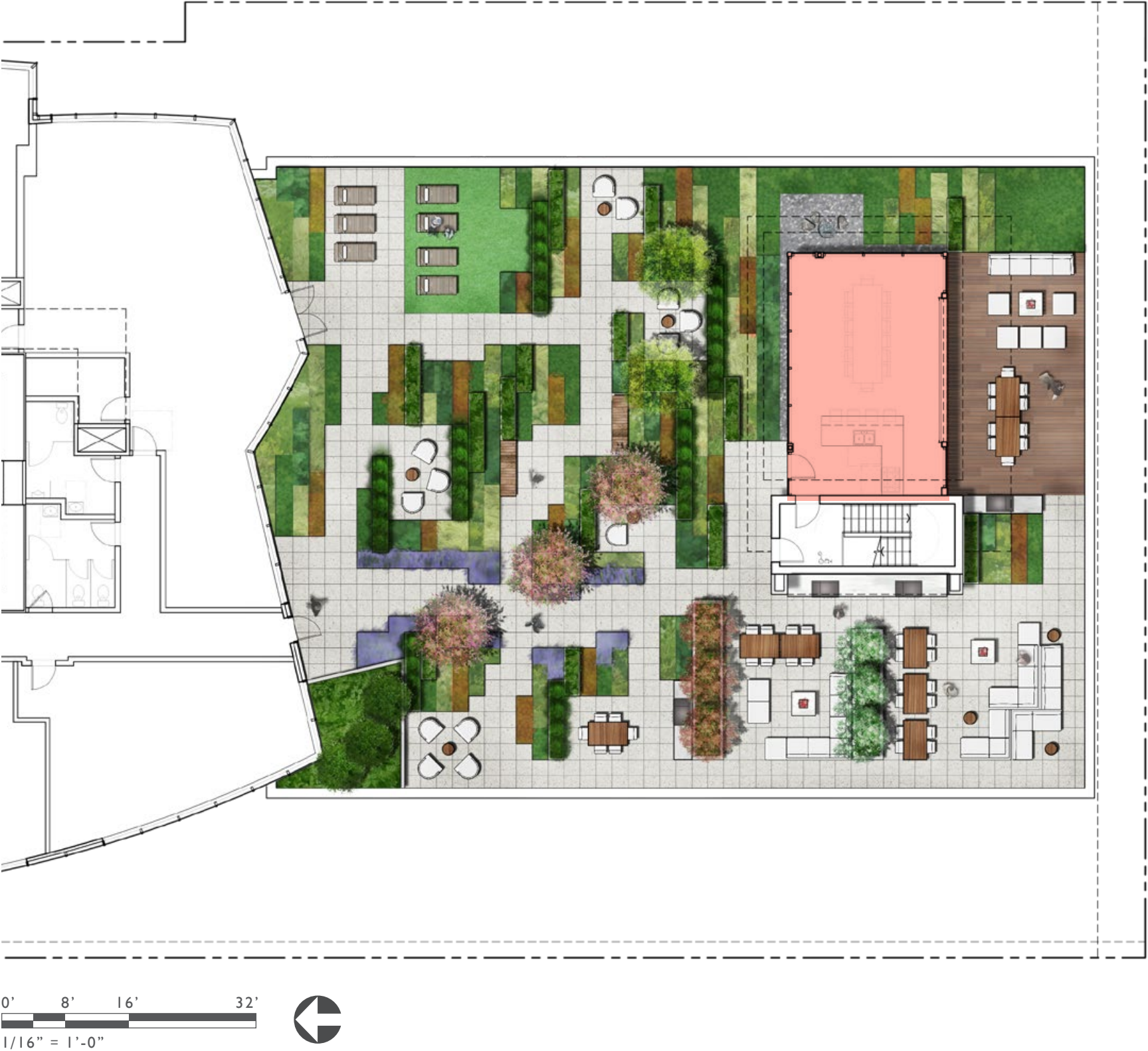


(MATERIAL PICTURES ON PAGE 47)





L7 AMENITIES





L7 PAVILION



L7 ROOFTOP PAVILION



PAVILION APPROACH FROM TOWER



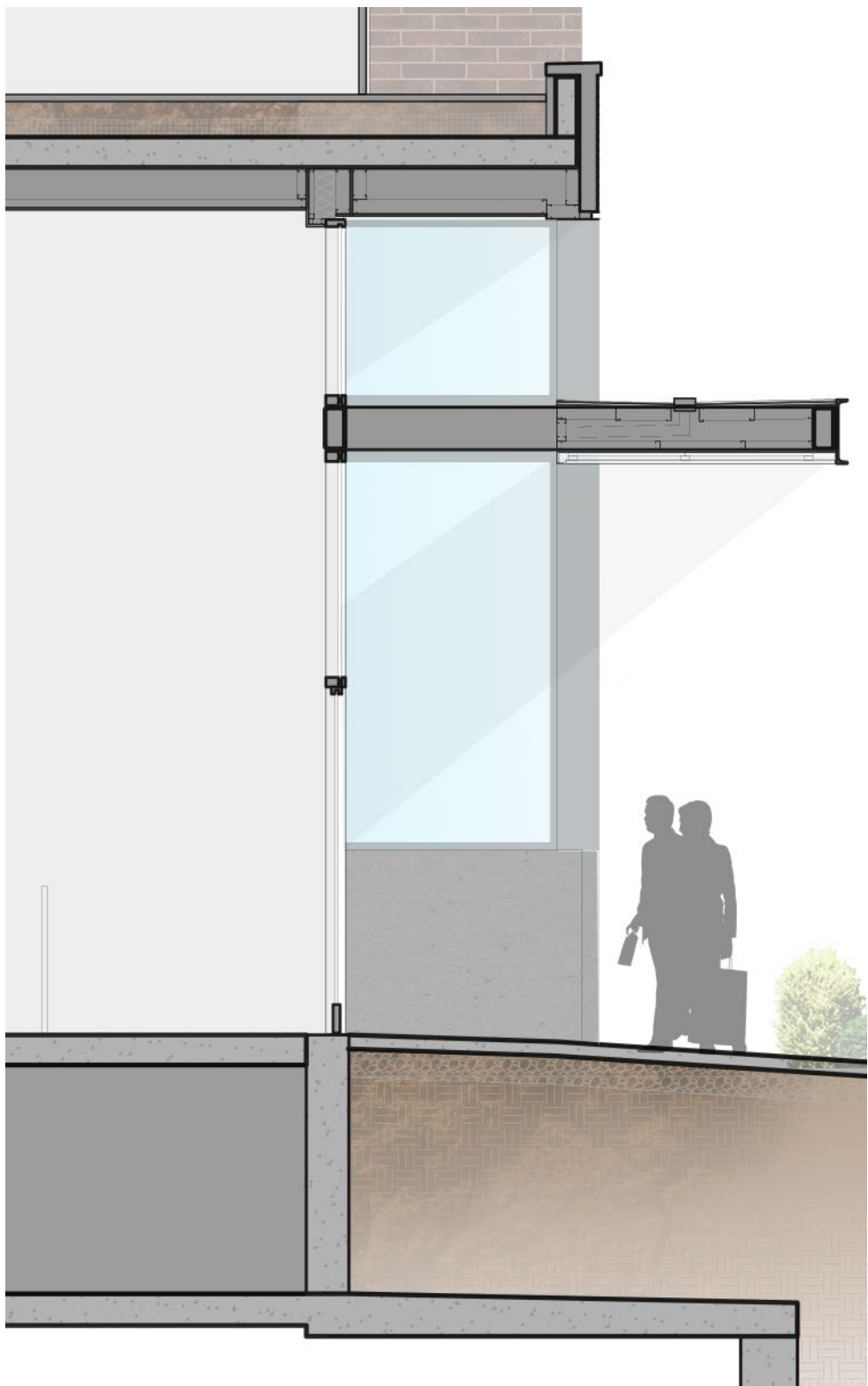
L7 ROOFTOP PAVILION INTERIOR CONCEPT



DENNY RETAIL ENTRY DETAIL



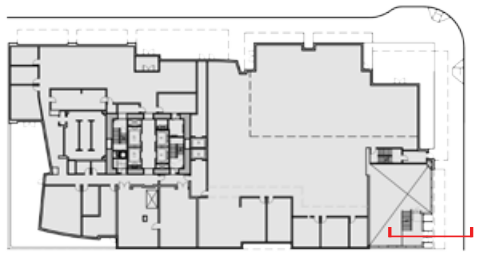
DENNY RETAIL ENTRY



SECTION AT ENTRY



ELEVATION - 1/16" = 1'-0"





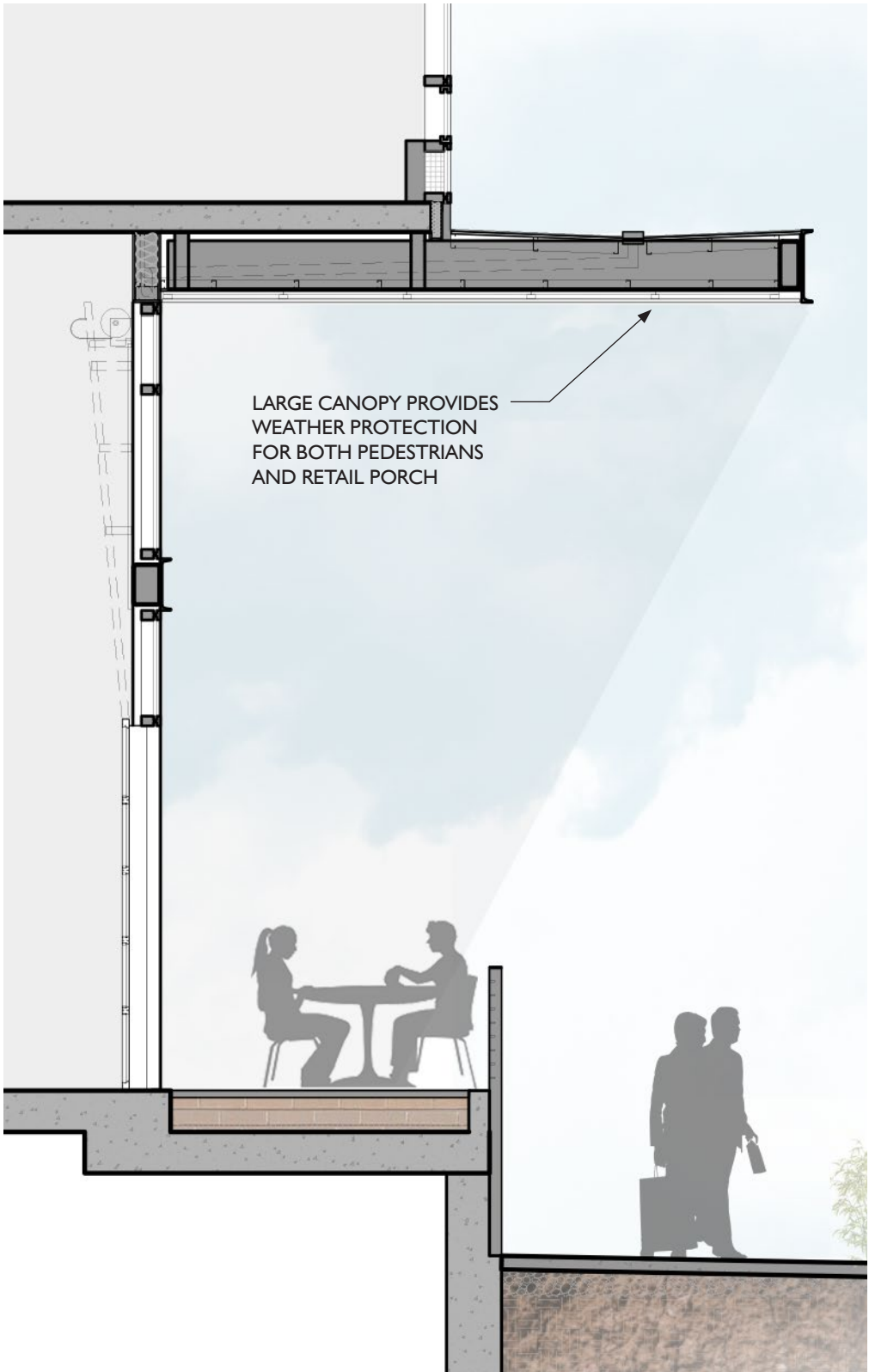
# DENNY STREET LEVEL DETAIL



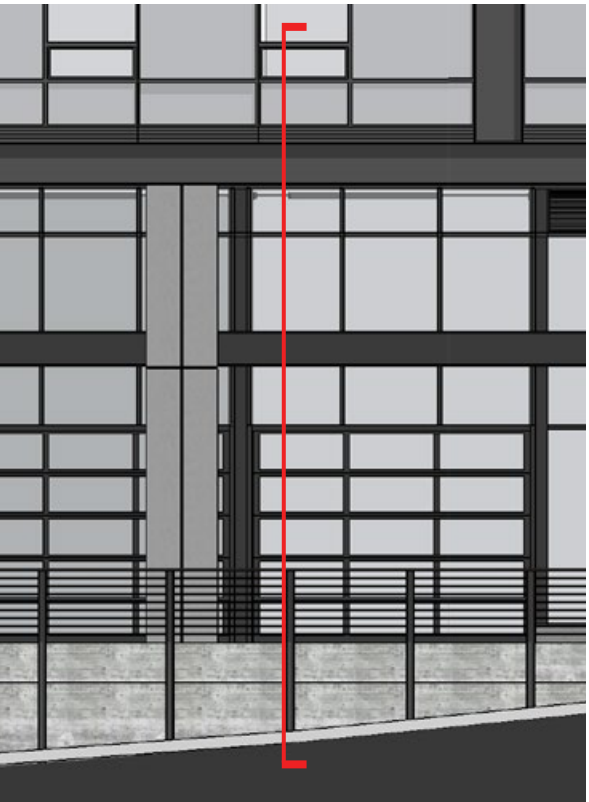
SOUTHERN RETAIL PORCH



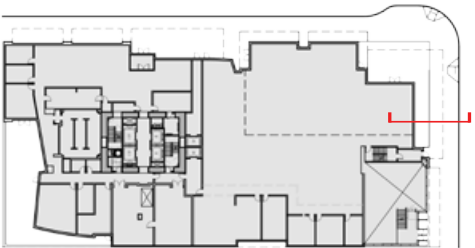
SOUTHERN RETAIL PORCH



SECTION AT RETAIL PORCH



ELEVATION - 1/16" = 1'-0"





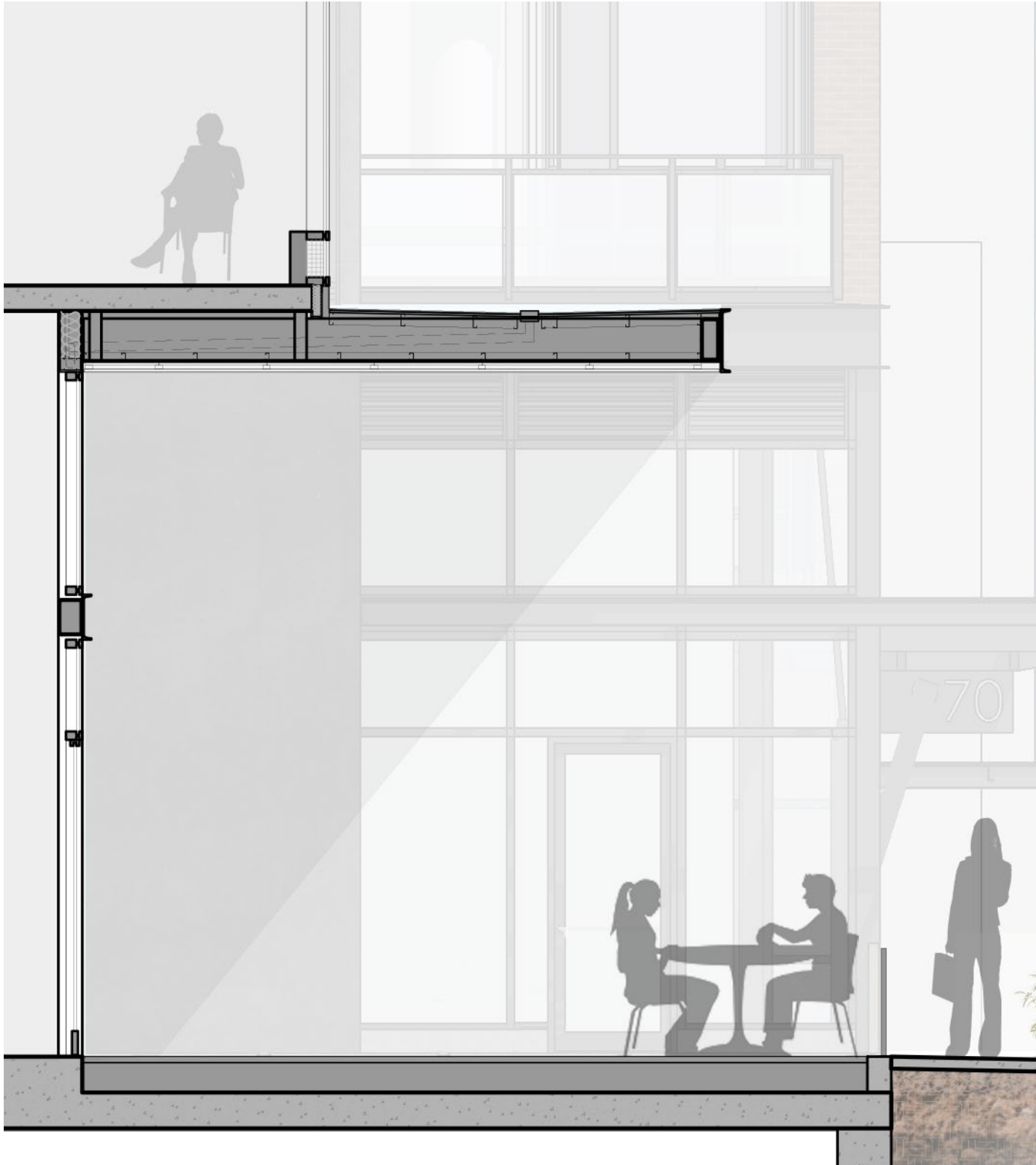
MAIN CORNER STREET LEVEL DETAIL



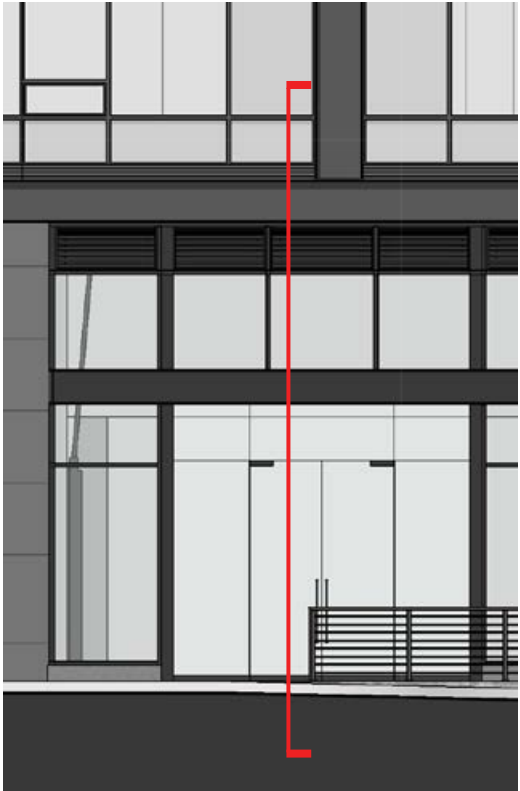
RETAIL ENTRY PEDESTRIAN VIEW



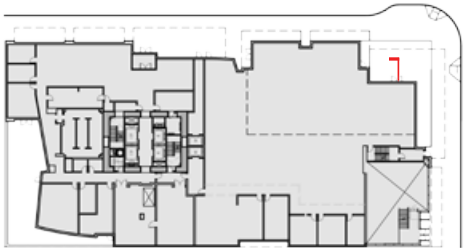
RETAIL CORNER ENTRY



SECTION AT ENTRY



ELEVATION - 1/16" = 1'-0"





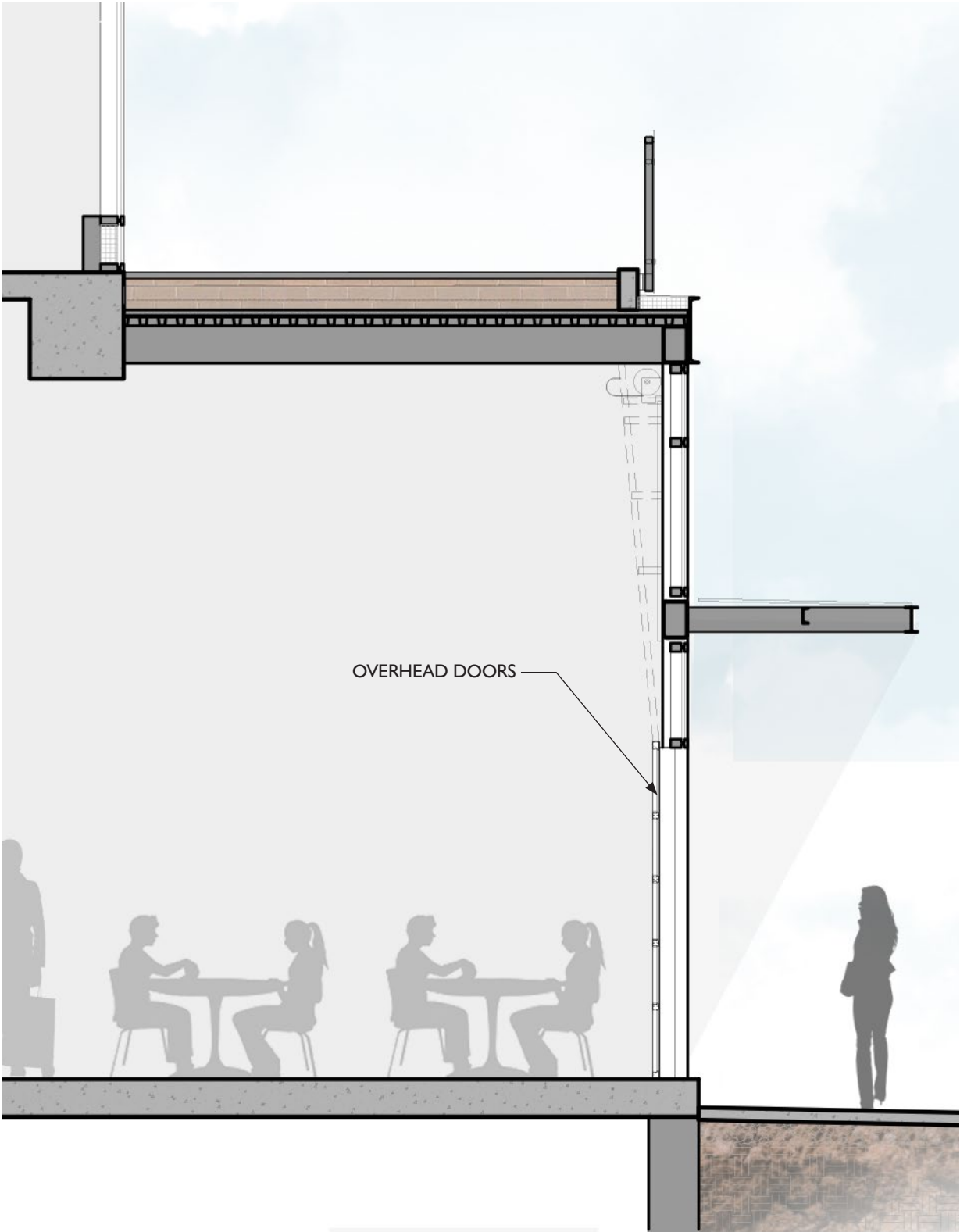
TERRY RETAIL STREET LEVEL DETAIL



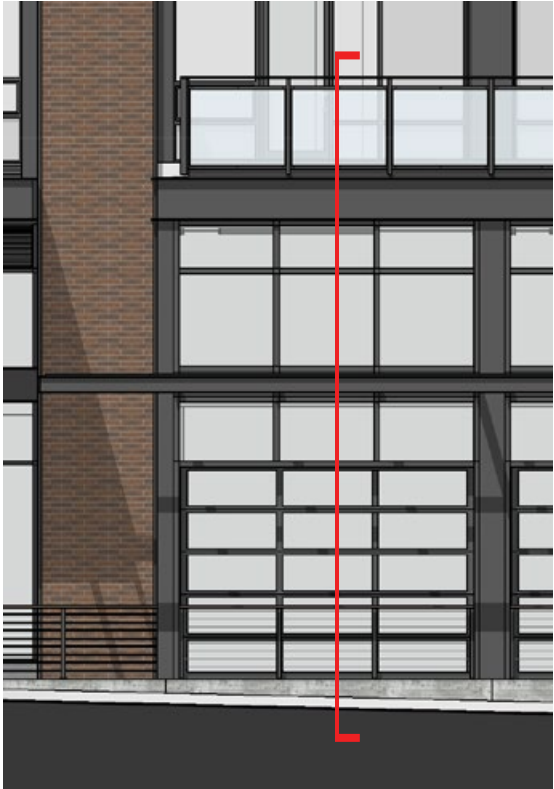
TERRY RETAIL PEDESTRIAN VIEW



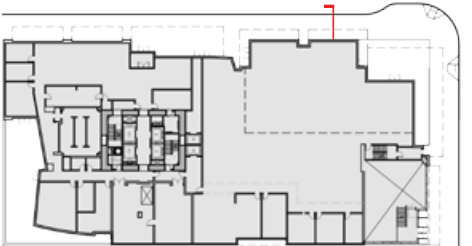
TERRY AVE RETAIL PORCH ENCLOSURE



SECTION AT RETAIL PORCH



ELEVATION - 1/16" = 1'-0"





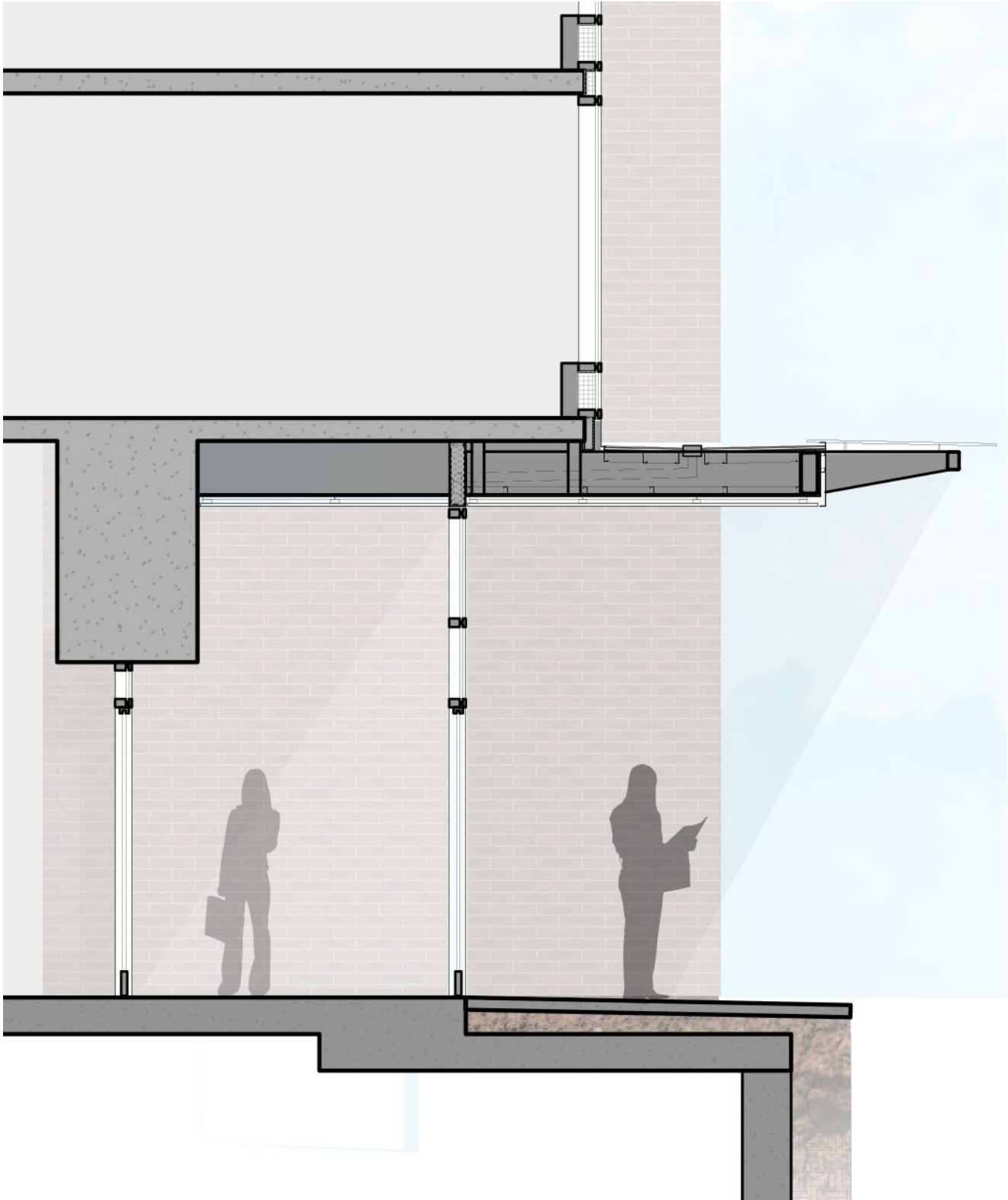
# RESIDENTIAL ENTRY STREET LEVEL DETAIL



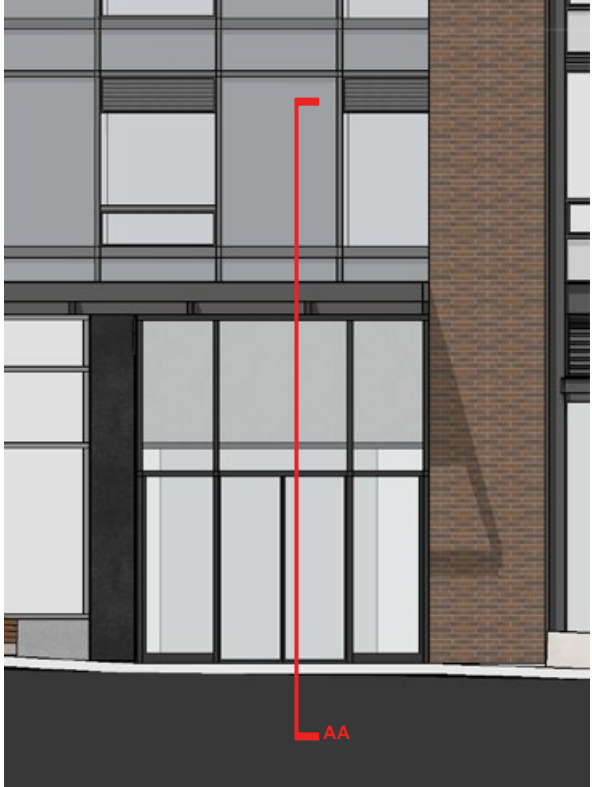
TERRY AVE SIDEWALK AT RESIDENTIAL ENTRY



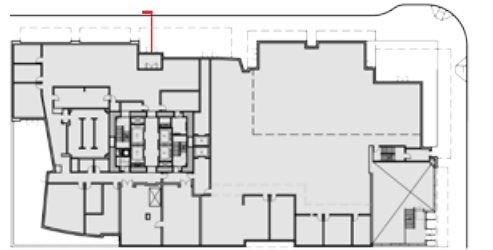
TERRY AVE RESIDENTIAL ENTRY



SECTION AT RESIDENTIAL ENTRY

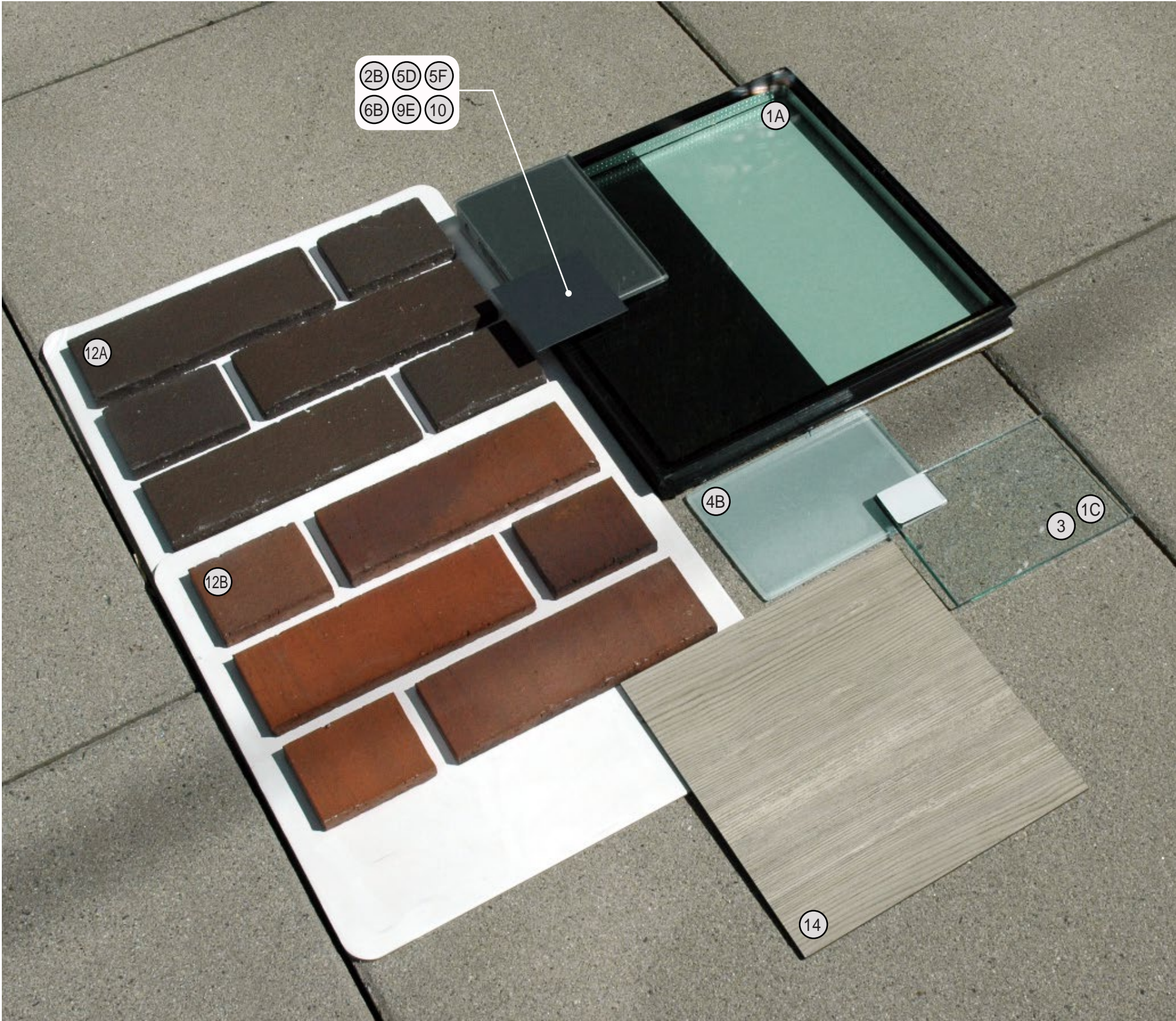
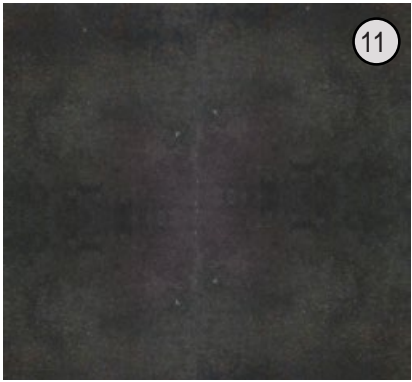
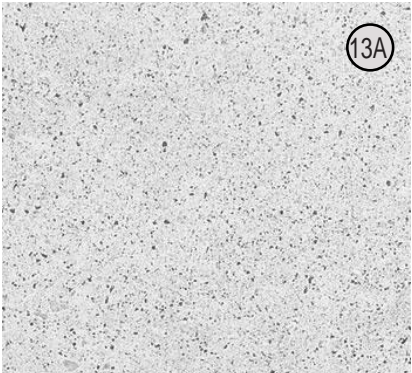
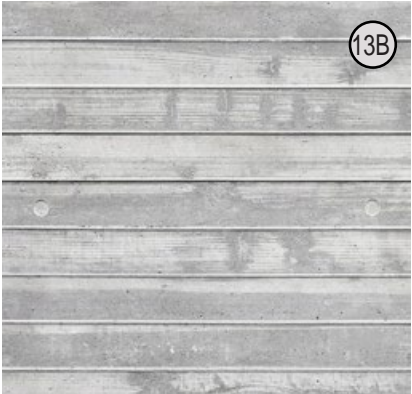


ELEVATION - 1/16" = 1'-0"





# PROJECT MATERIALS – PODIUM



## MATERIAL KEY

- 1A VISION GLASS  
COLOR: CLEAR
- 1C VISION GLASS  
COLOR: ULTRA TRANSPARENT
- 2B SPANDREL GLASS  
COLOR: DARK GRAY
- 3 CANOPY GLASS  
COLOR: CLEAR
- 4B RAILING GLASS  
COLOR: FROSTED
- 5D MULLION (WINDOW WALL)  
COLOR: DARK GREY
- 5F MULLION (STORE FRONT)  
COLOR: DARK GREY
- 6B METAL PANEL (WINDOW WALL)  
COLOR: DARK GREY
- 7A METAL PANEL (FRAMED WALL)  
COLOR: DARK GREY
- 8 PROFILE SHEET METAL PANEL  
COLOR: DARK GREY
- 9E LOUVER (STOREFRONT)  
COLOR: DARK GREY
- 10 MISC METALS  
COLOR: DARK GREY
- 11 WEATHERED STEEL PANELS  
COLOR: DARK GREY
- 12A BRICK  
COLOR: DARK BROWN
- 12B BRICK  
COLOR: MEDIUM BLEND
- 13A CONCRETE: ARCHITECTURAL FINISH
- 13B CONCRETE: BOARD FORM
- 14 HPL WOOD PANEL  
COLOR: AGED WOOD









TOWER DESIGN



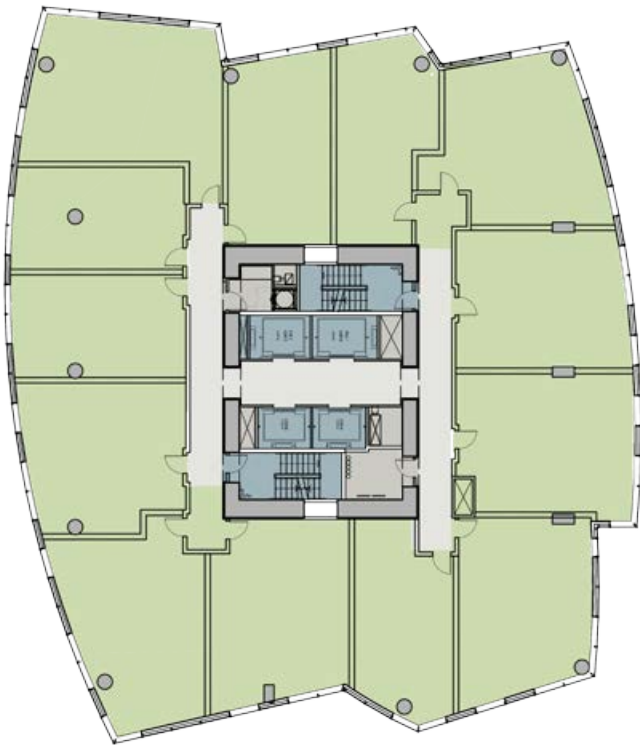




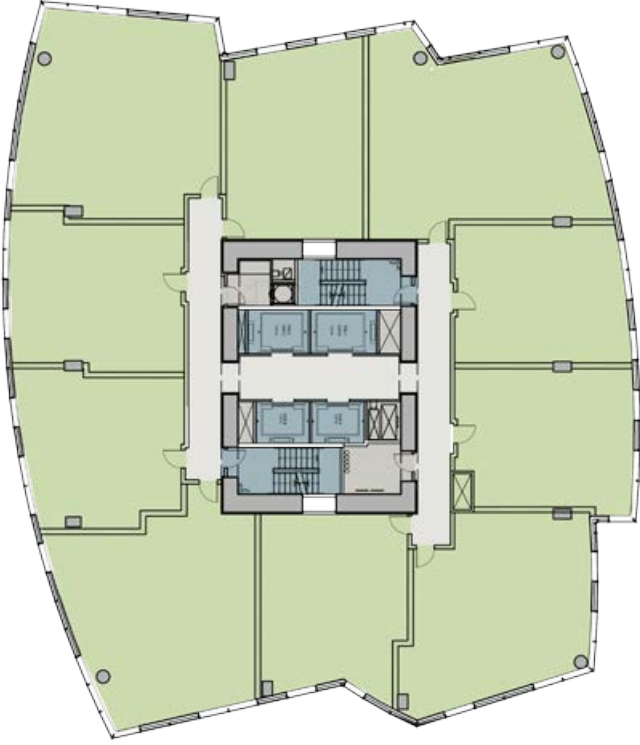




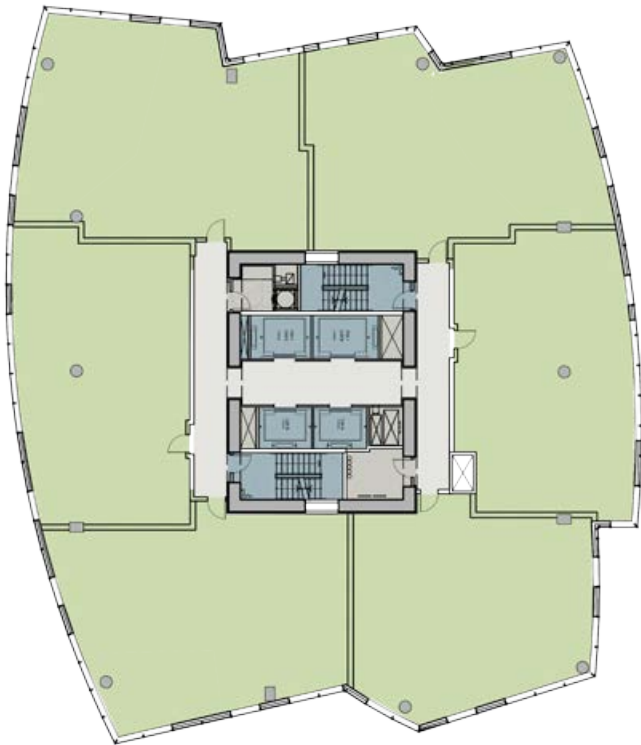
TOWER PLANS



L8-L16 FLOOR PLAN  
1/32" = 1'-0"



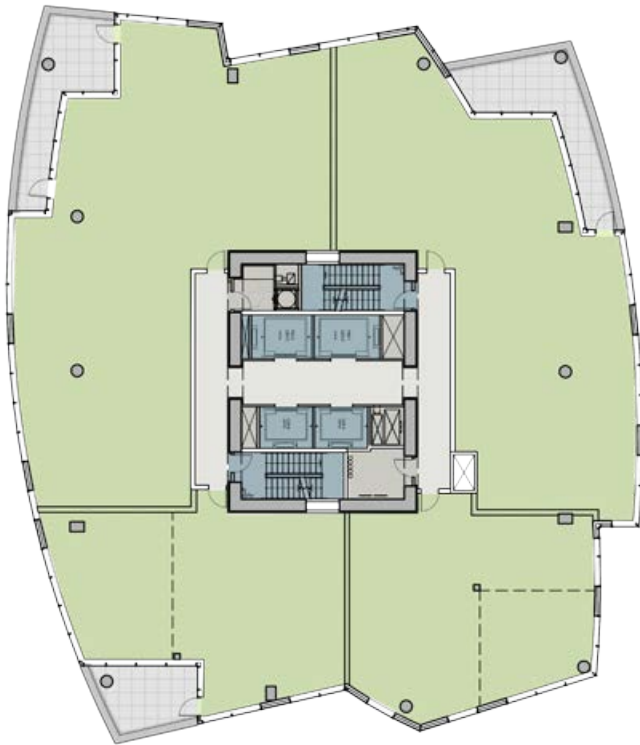
L17-L34 FLOOR PLAN  
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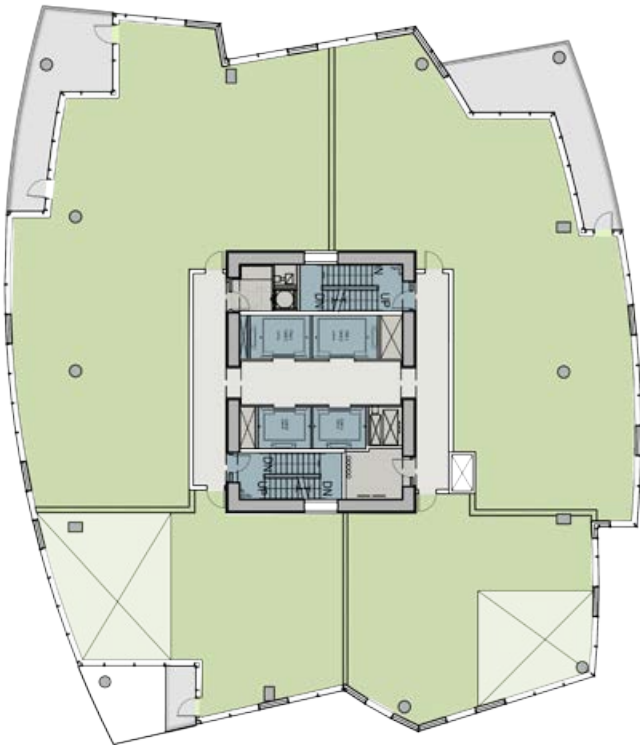
L35-L38 FLOOR PLAN  
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SPACE FUNCTION KEY

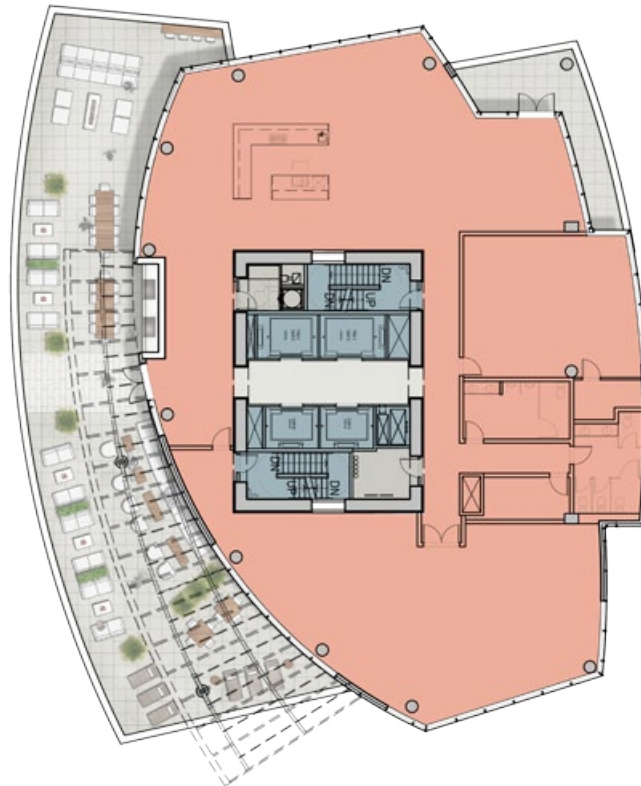
- RESIDENTIAL
- COMMON SPACE
- STAIR / ELEVATOR CORE
- RETAIL
- PARKING
- AMENITIES
- BOH / LOADING / MECH



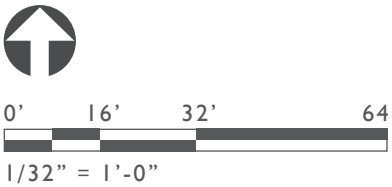
L39 FLOOR PLAN  
1/32" = 1'-0"



L40 FLOOR PLAN  
1/32" = 1'-0"

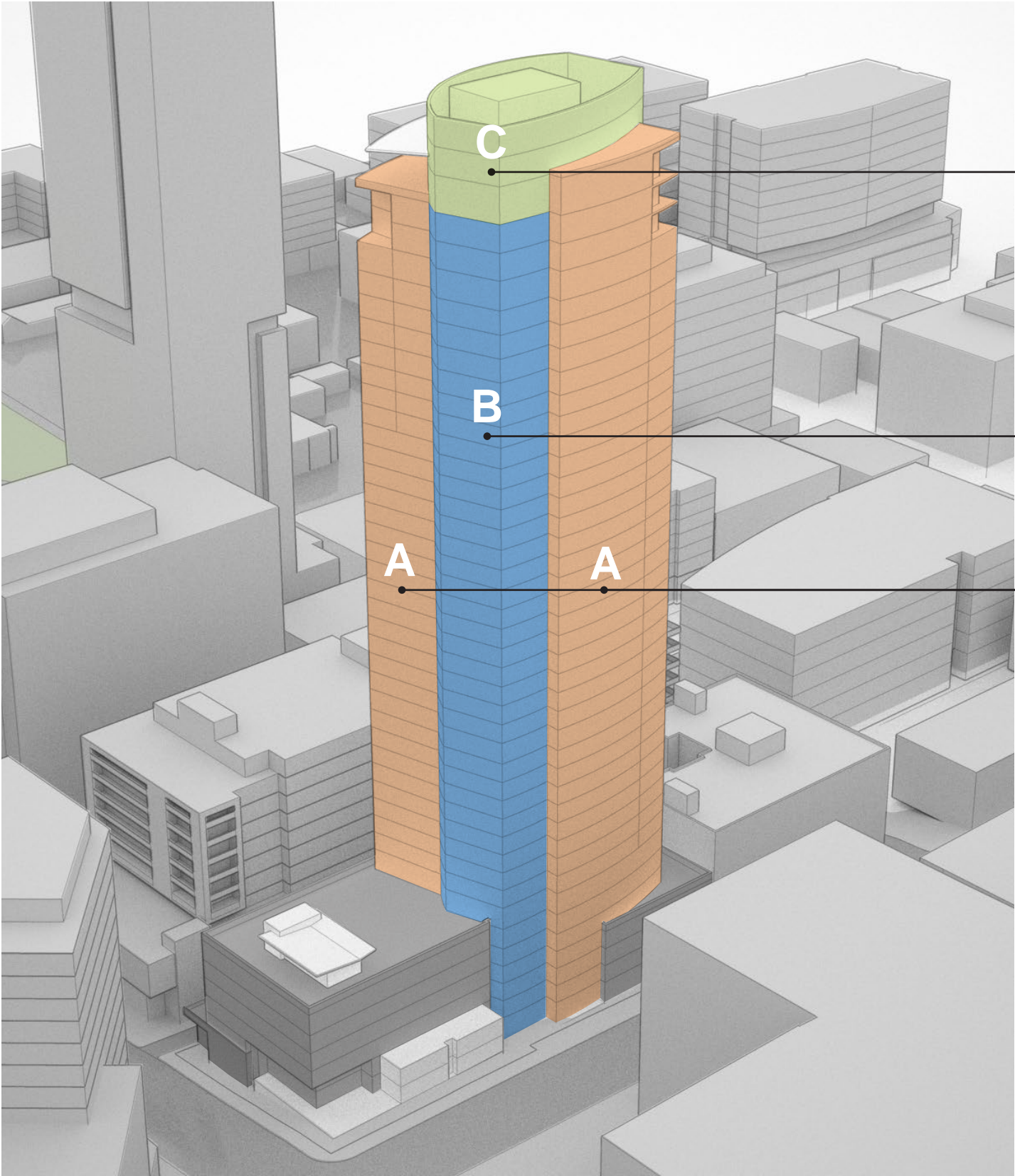


RI - ROOF PLAN  
1/32" = 1'-0"

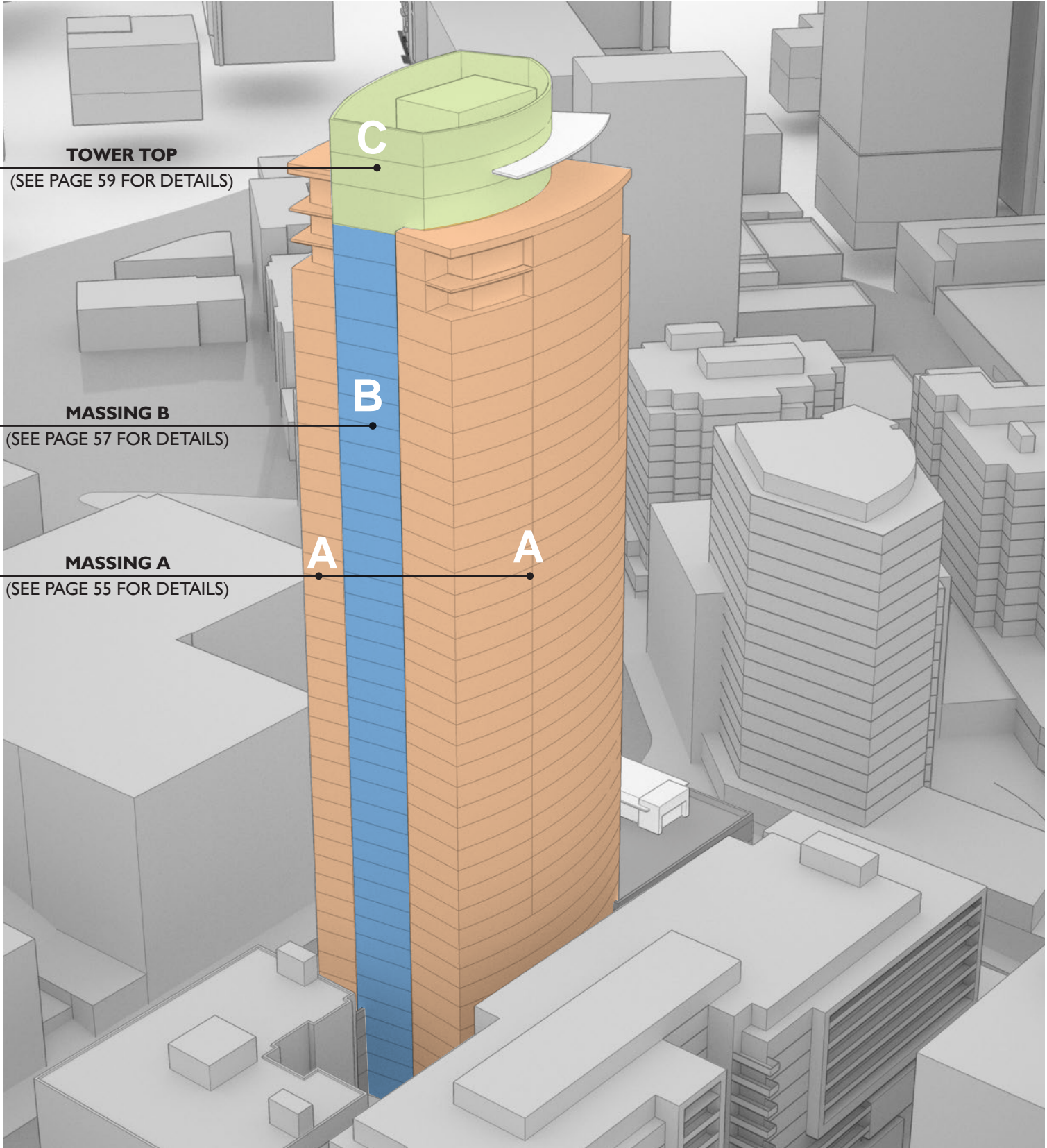




TOWER MASSING DIAGRAMS



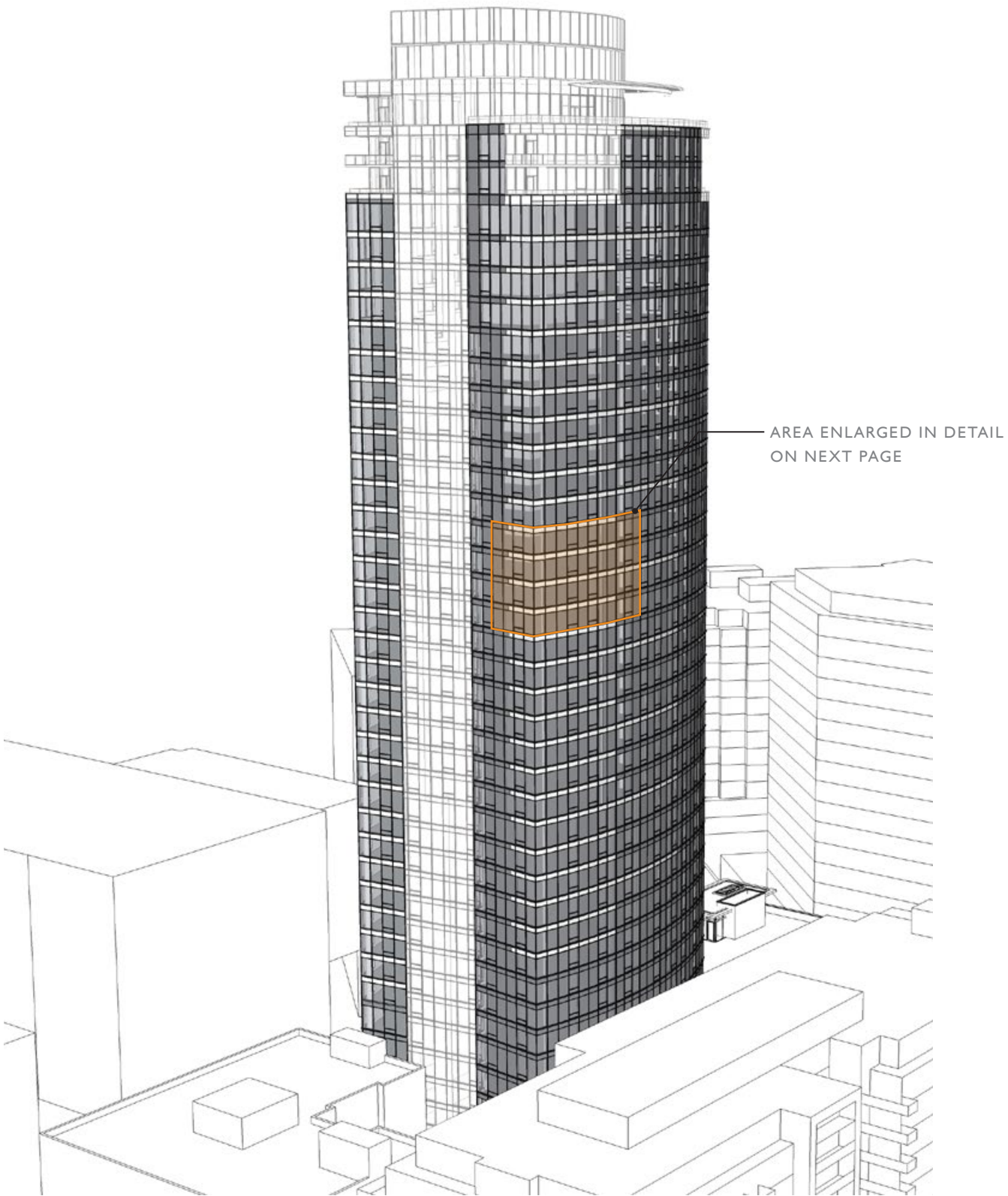
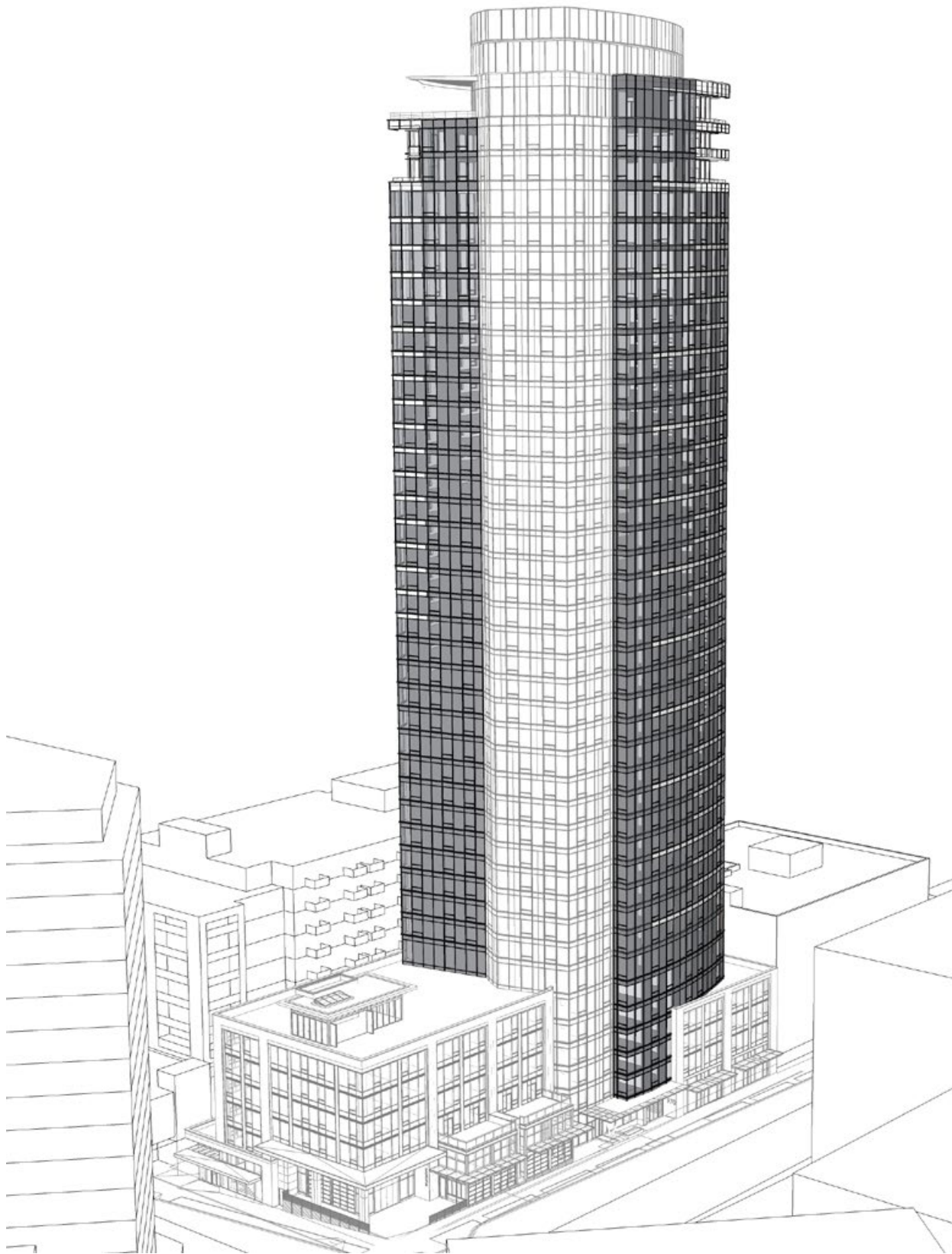
MASSING DIAGRAM (FROM THE SOUTHEAST)



MASSING DIAGRAM (FROM THE NORTHWEST)

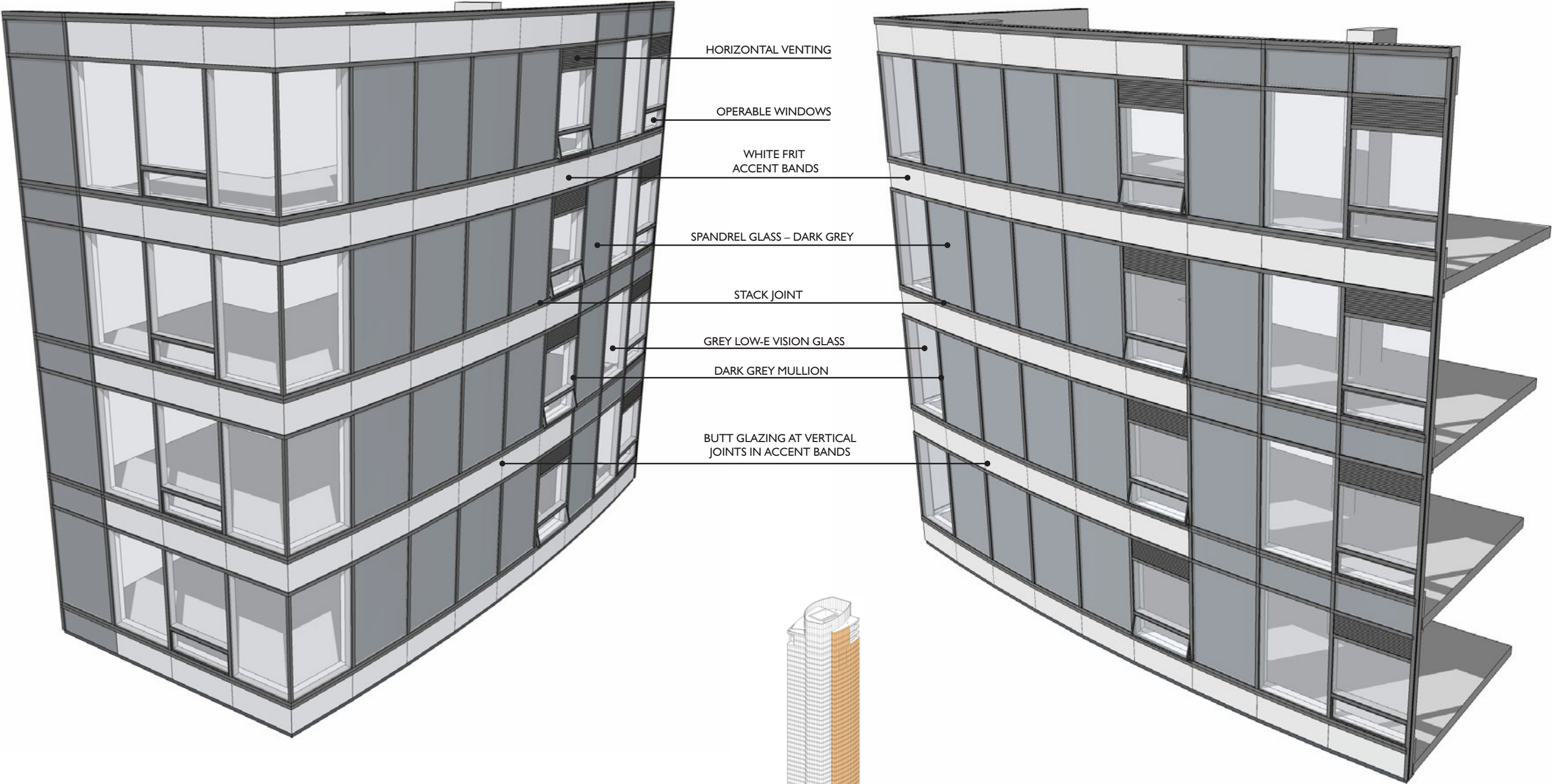


DESIGN ELEMENTS – MASS A





DESIGN ELEMENTS – MASS A

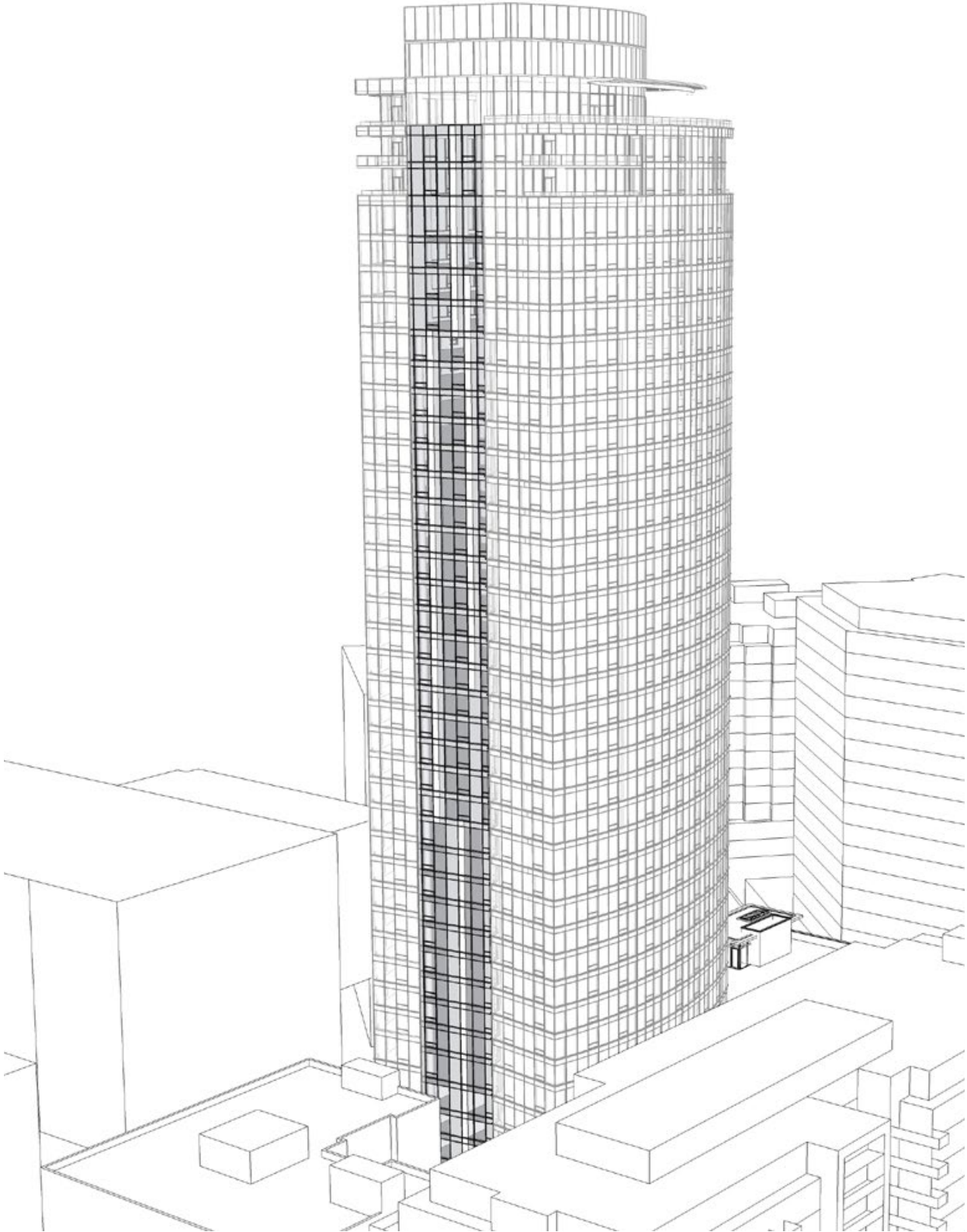
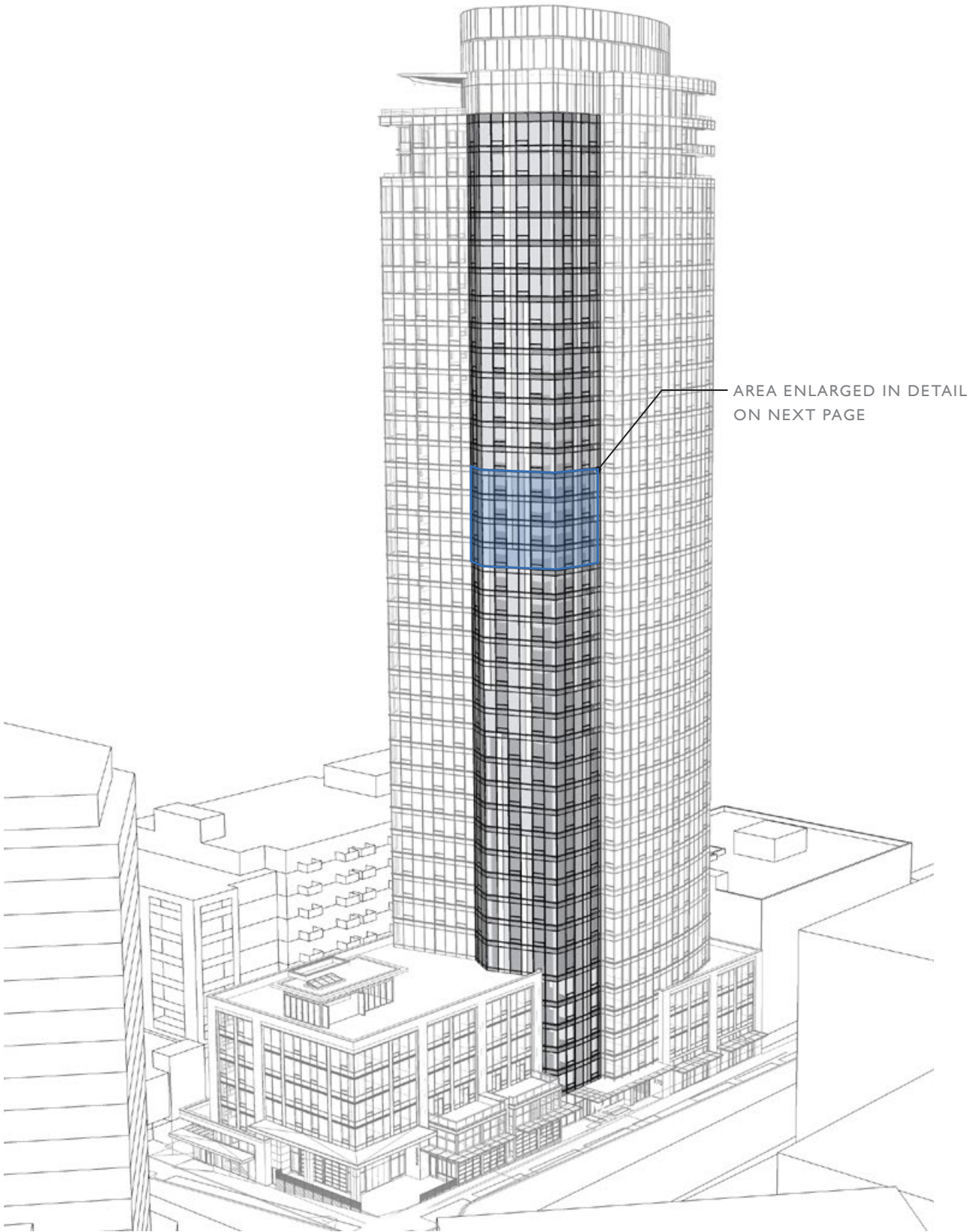


NORTH MASS "A" DETAIL

WEST MASS "A" DETAIL

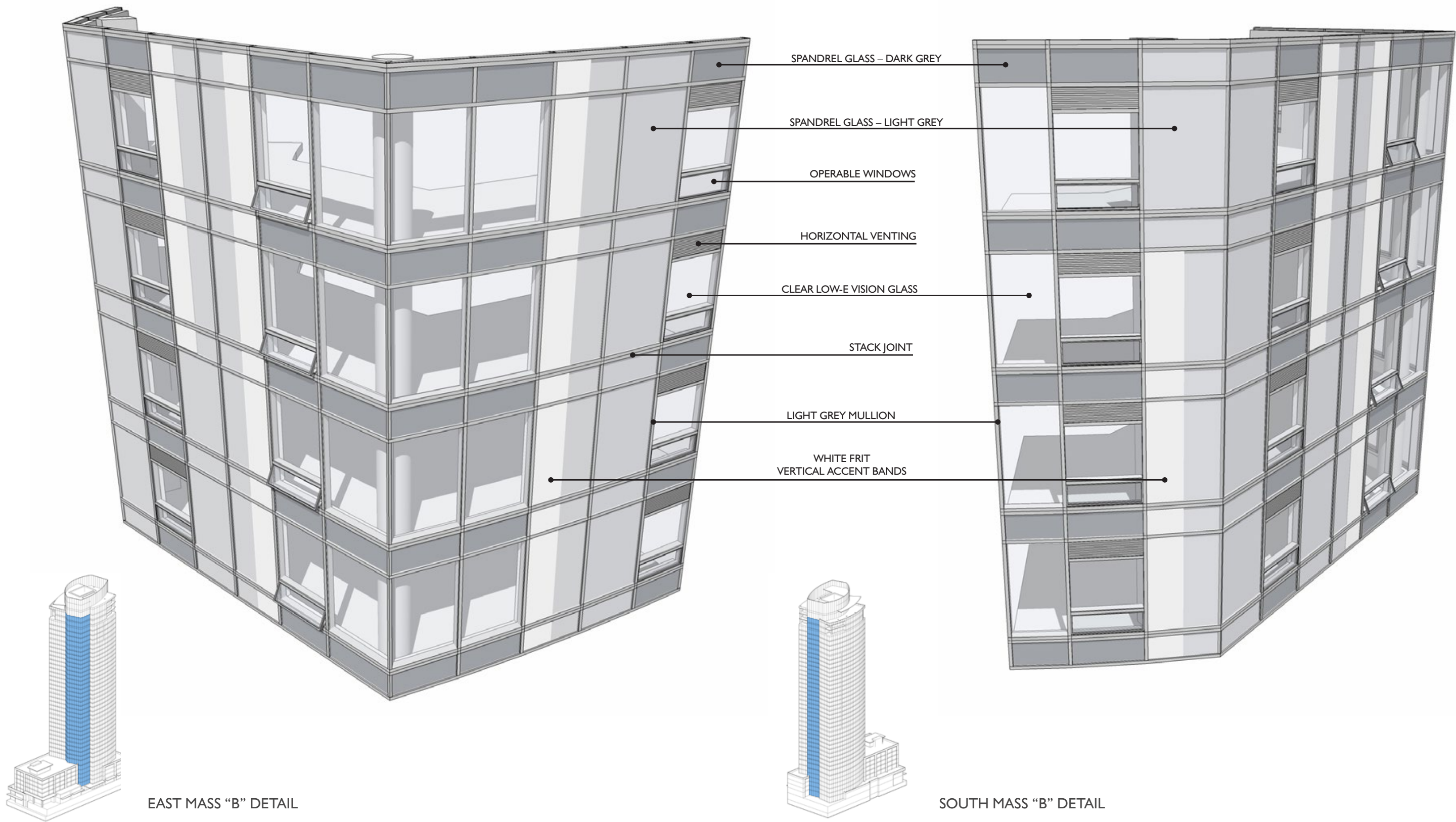


DESIGN ELEMENTS – MASS B



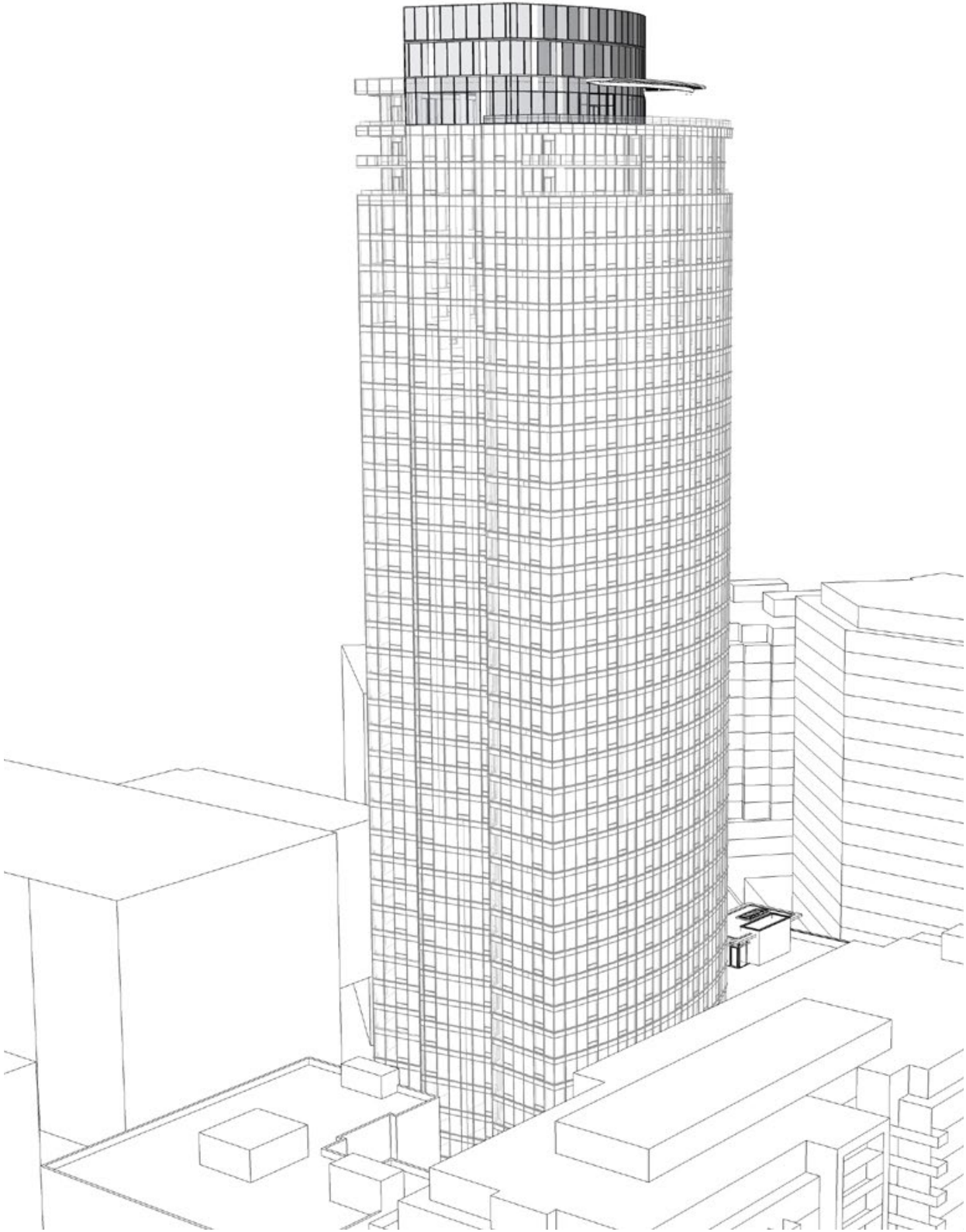
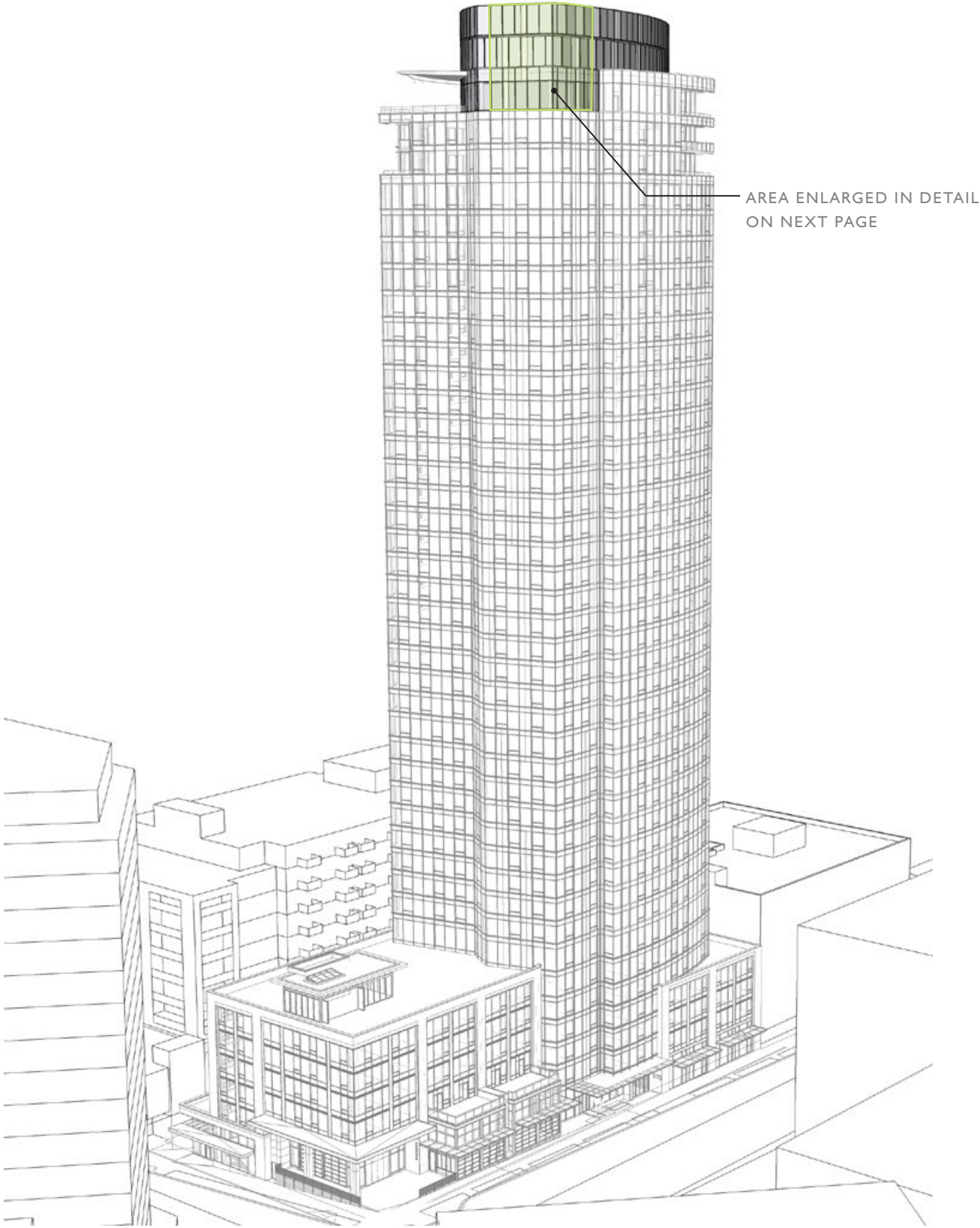


DESIGN ELEMENTS – MASS B



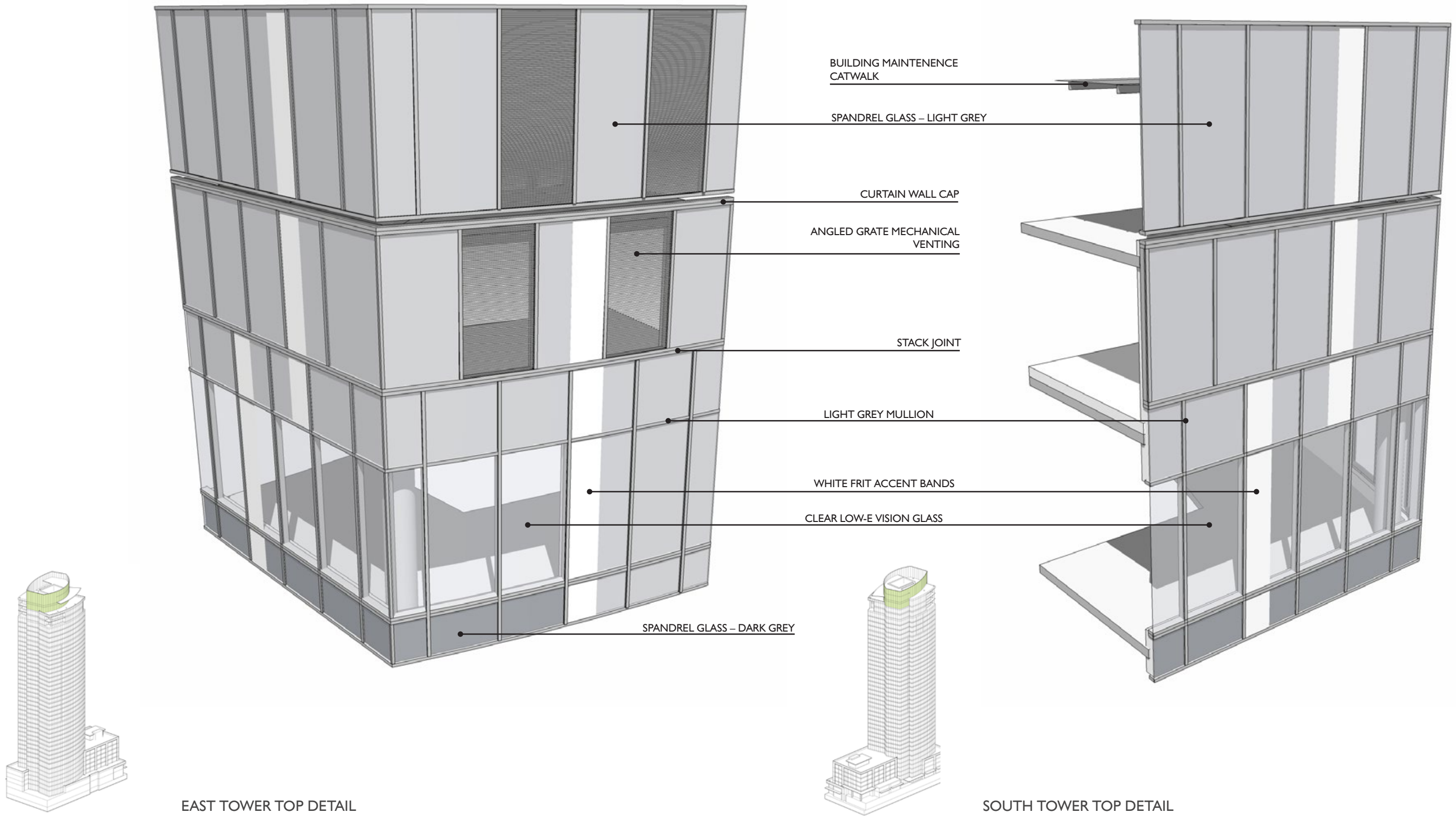


DESIGN ELEMENTS – TOWER TOP



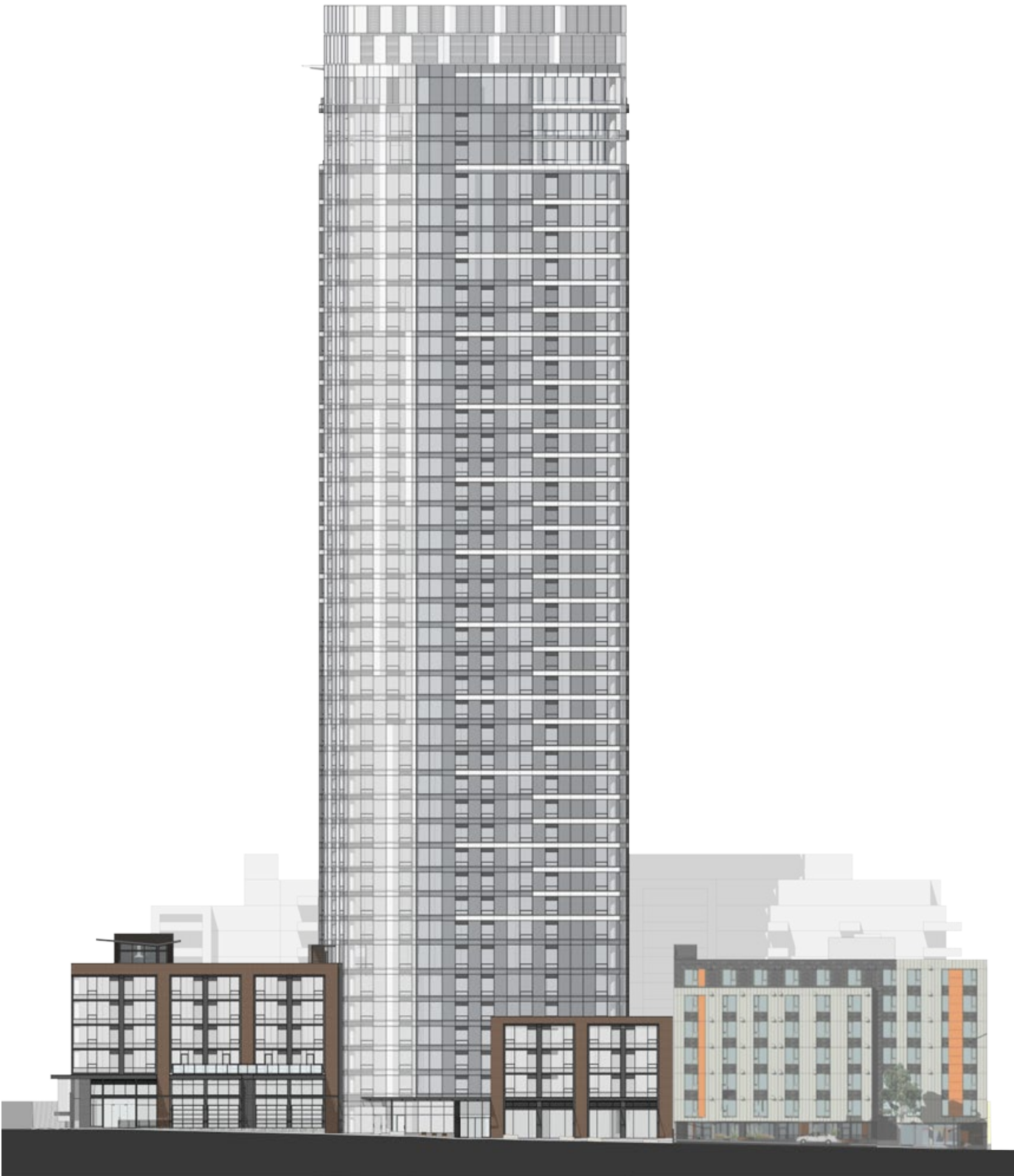


# DESIGN ELEMENTS – TOWER TOP

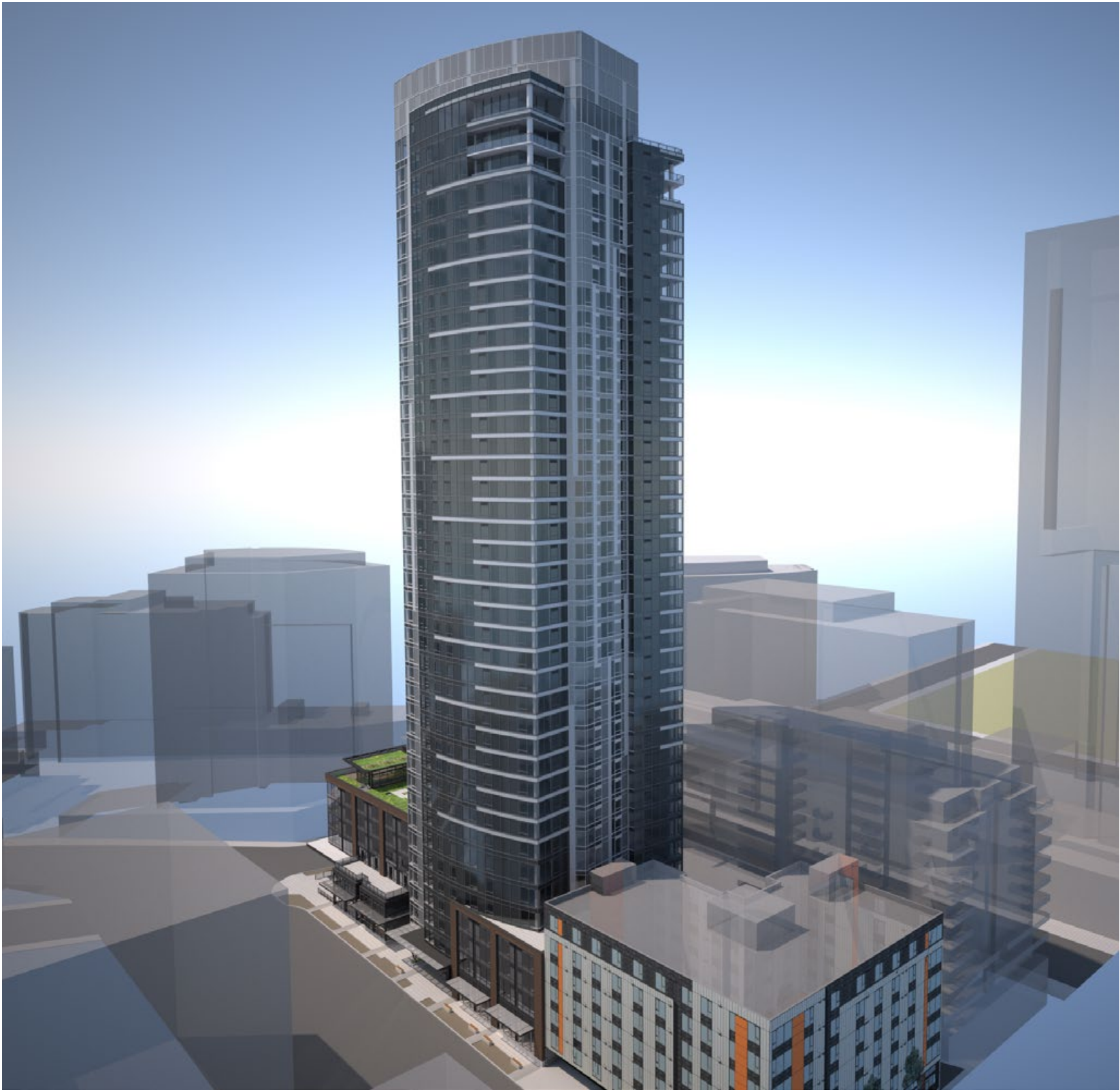




EAST FACADE ELEVATION



EAST ELEVATION





SOUTH FACADE ELEVATION



SOUTH ELEVATION





WEST FACADE ELEVATION

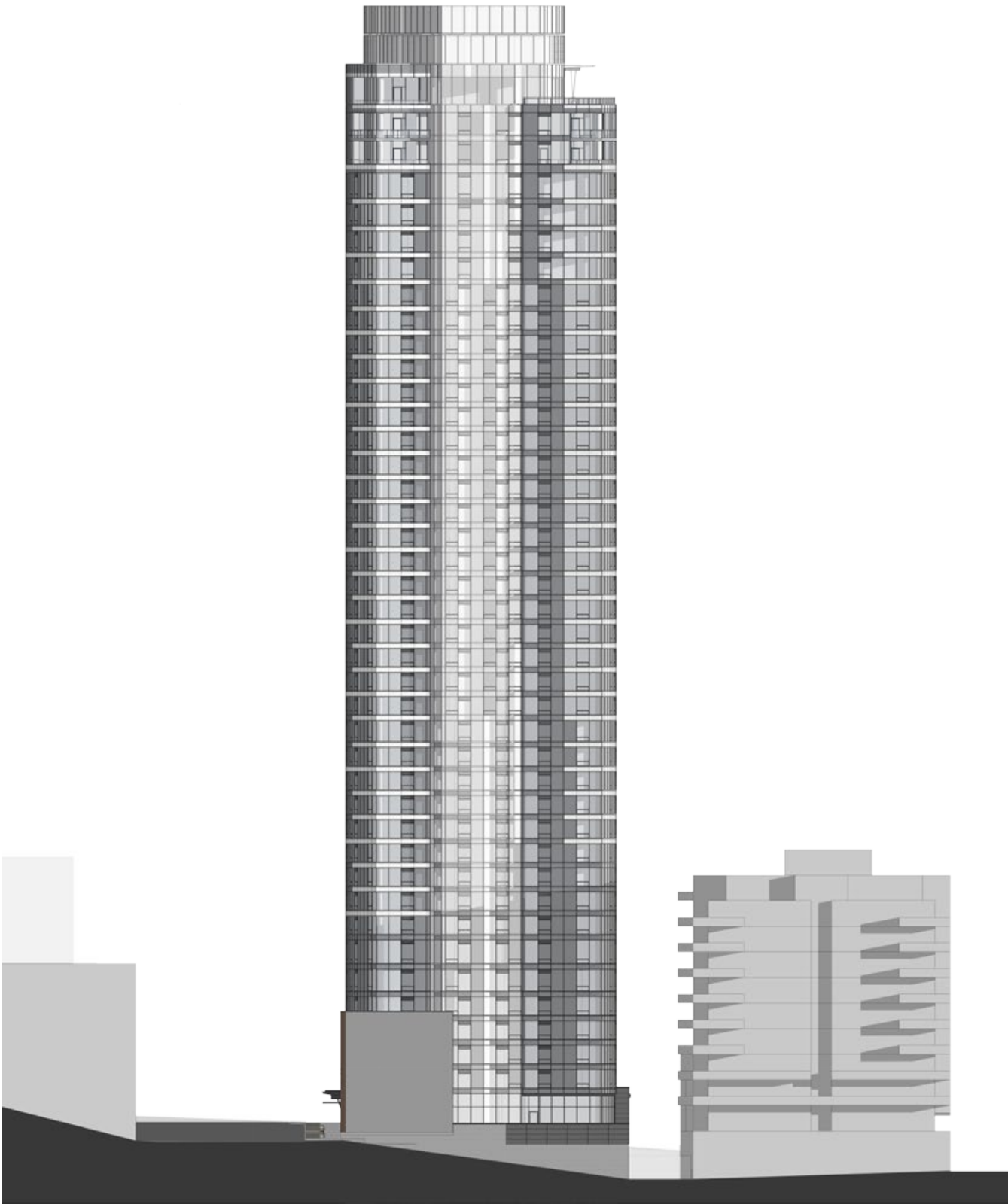


WEST ELEVATION





NORTH FACADE ELEVATION

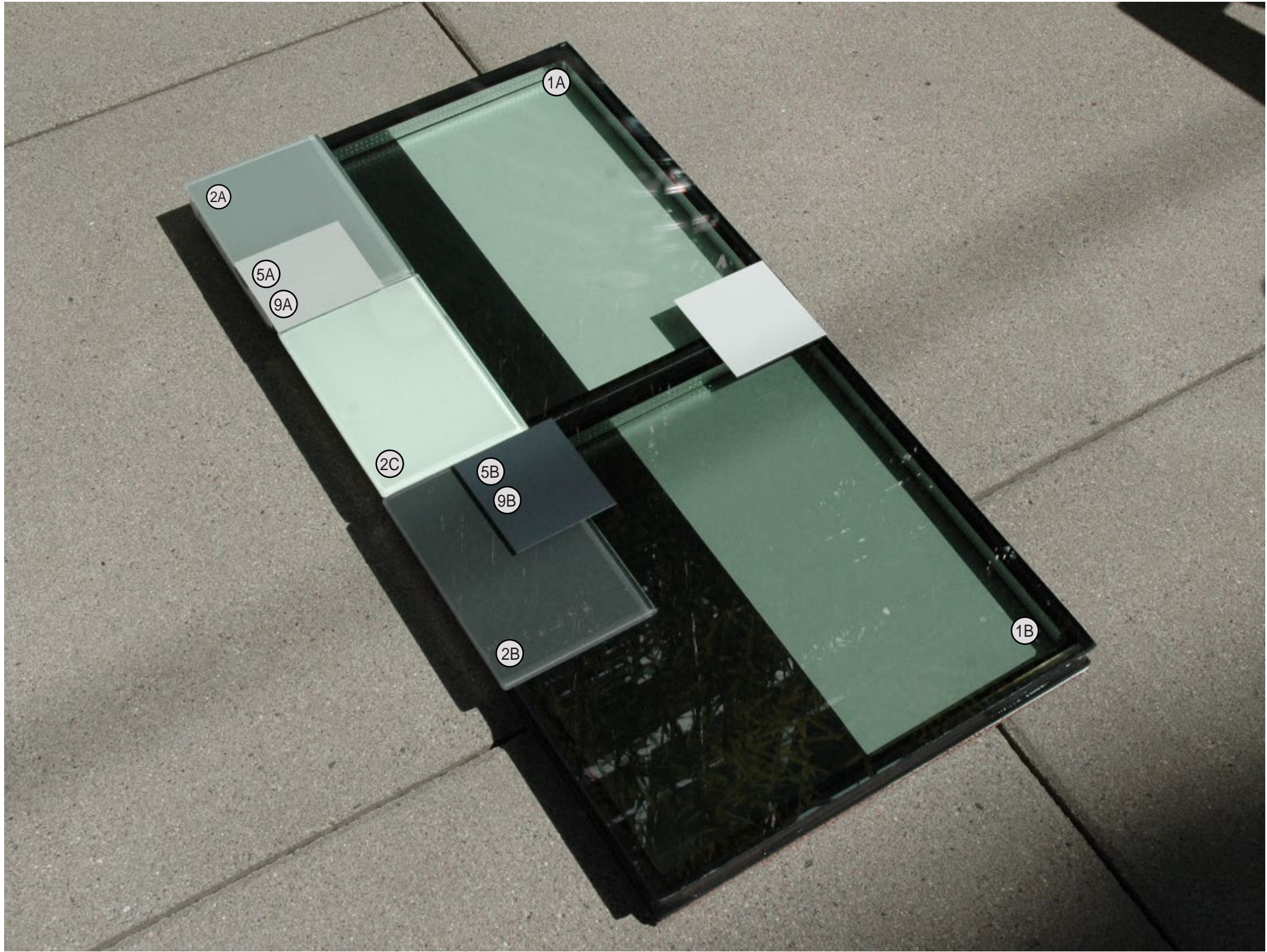


NORTH ELEVATION





# PROJECT MATERIALS – TOWER

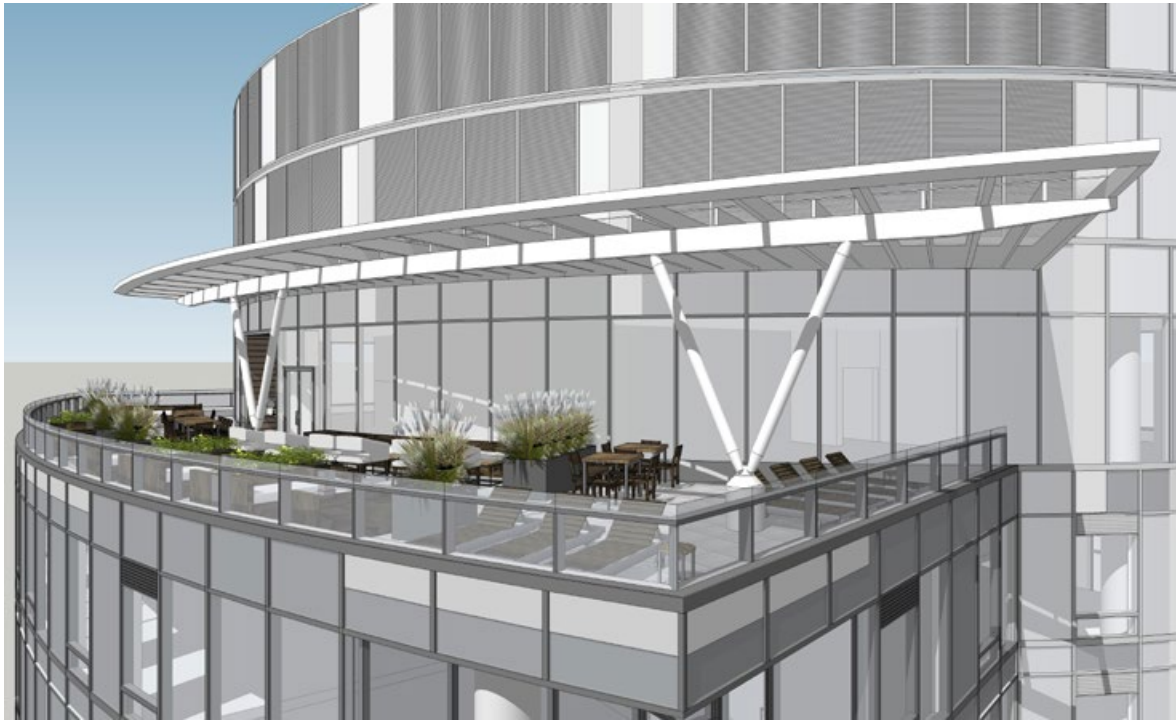
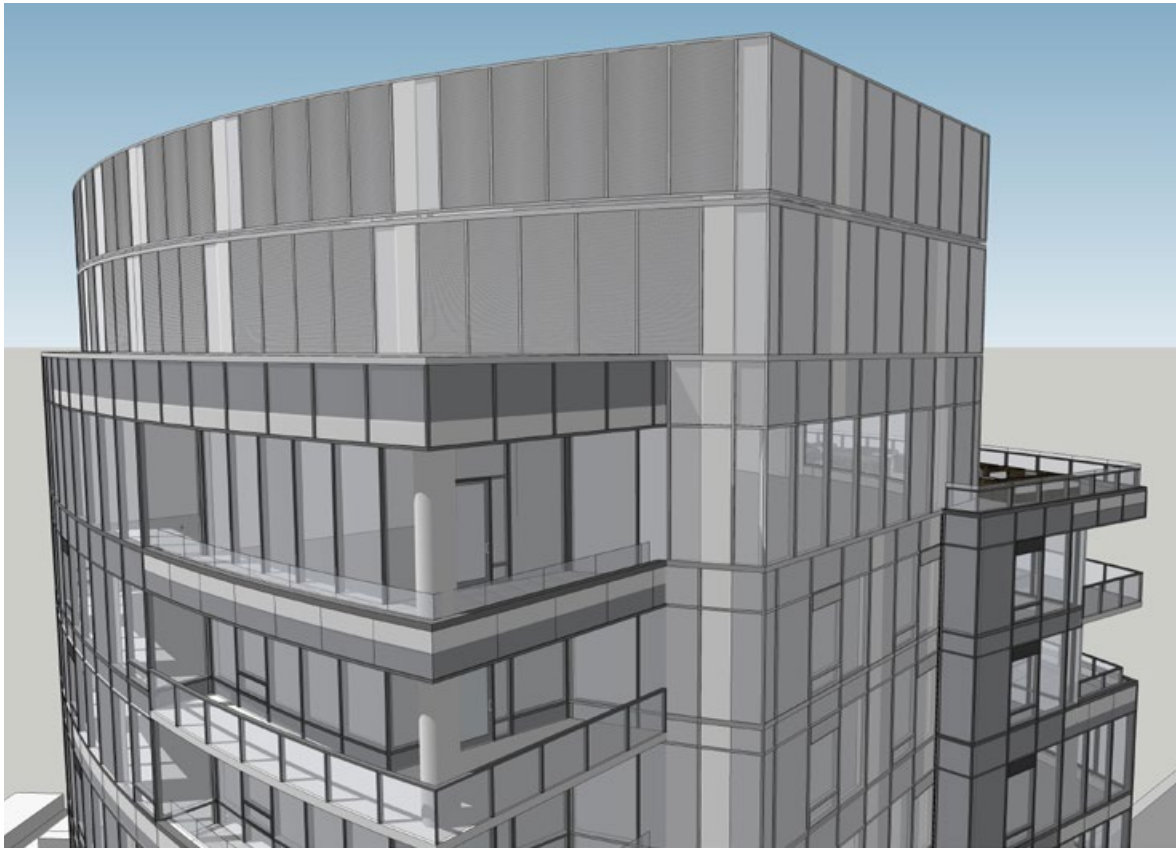


## MATERIAL KEY

- 1A VISION GLASS  
COLOR: CLEAR
- 1B VISION GLASS  
COLOR: MEDIUM GREY
- 2A SPANDREL GLASS  
COLOR: WARM GRAY
- 2B SPANDREL GLASS  
COLOR: DARK GRAY
- 2C ACCENT SPANDREL GLASS  
COLOR: WHITE
- 2A RAILING GLASS  
COLOR: CLEAR (NOT PICTURED)
- 5A MULLION (CURTAINWALL)  
COLOR: LIGHT GREY
- 5B MULLION (CURTAINWALL)  
COLOR: DARK GREY
- 9A LOUVER (CURTAINWALL)  
COLOR: LIGHT GREY
- 9B LOUVER (CURTAINWALL)  
COLOR: DARK GREY



ROOFTOP AMENITY AND SCREENING









DEPARTURES

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REQUESTED DEPARTURE #1

ROOF TOP COVERAGE

CODE SECTION	CODE REQUIREMENT	DEPARTURE REQUEST	DIFFERENCE	RATIONALE FOR REQUEST
SMC 23.48.010.H.4 ROOF TOP COVERAGE	<p>The following rooftop features may extend up to 15 feet above the maximum height limit, so long as the combined total coverage of all features listed in this subsection 23.48.010.H.4 does not exceed 20 percent of the roof area, or 25 percent of the roof area if the total includes stair or elevator penthouses or screened mechanical equipment:</p> <ul style="list-style-type: none"><li>a. Solar collectors;</li><li>b. Stair penthouses;</li><li>c. Mechanical equipment;</li><li>d. Atriums, greenhouses, and solariums;</li><li>e. Play equipment and open-mesh fencing that encloses it, as long as the fencing is at least 15 feet from the roof edge;</li><li>f. Minor communication utilities and accessory communication devices, except that height is regulated according to the provisions of Section 23.57.012; and</li><li>g. Covered or enclosed common amenity area for structures exceeding a height of 125 feet</li></ul>	Allow for rooftop coverage greater than 25 percent.	The project proposes 84% coverage (includes fully enclosed space, and the canopy / trellis element); an increase of 59% from SMC 23.48.010.H.4 allowance of 25% coverage. However, per SMC 23.48.010.H.7 at the applicant's discretion the 25% can be raised to 65% if certain provisions are met.	Departures 1,2,3 intertwine as three facets of a design solution for how the tower meets the sky. Because we seek a departure from both the 65% coverage and the requirements that allow for 65% coverage under SMC 23.48.010.H.7, DPD has advised that as a technical matter, we are required to seek a departure from SMC 23.48.010.H.4 as well. We believe that this departure fosters a much stronger architectural terminus to the tower - one that will better contribute to a more graceful skyline. Please see the rationale for departures #2 and #3 as they apply to departure #1 as well.



TOWER TOP VIEW SOUTH



TOWER TOP VIEW WEST

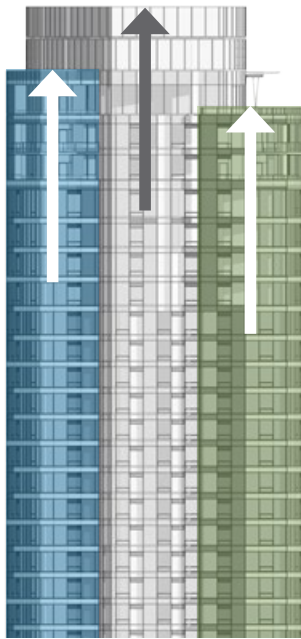


TOWER TOP VIEW EAST



TOWER TOP VIEW NORTH

“ Departures 1,2,3 intertwine as three facets of a design solution for how the tower meets the sky. ”



VARIATION IN TOWER MASS BREAKS UP TOP OF TOWER AND PROVIDES A MORE GRACEFUL TERMINUS.

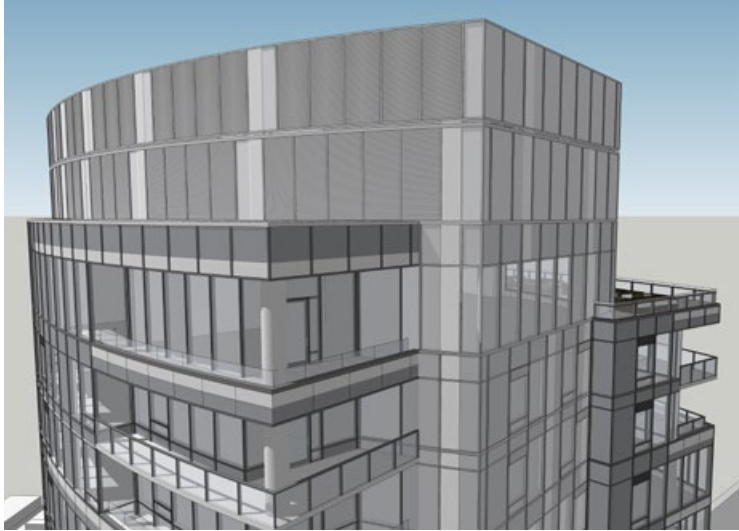
TOP OF TOWER MASSING



# REQUESTED DEPARTURE #2

## ROOF TOP COVERAGE

CODE SECTION	CODE REQUIREMENT	DEPARTURE REQUEST	DIFFERENCE	RATIONALE FOR REQUEST
SMC 23.48.010.H.7 ROOF TOP COVERAGE	At the applicant's option, the combined total coverage of all features listed in subsections 23.48.010.H.4 and 23.48.010.H.5 above may be increased to 65 percent of the roof area, provided that all of the following are satisfied: a. All mechanical equipment is screened; and b. No rooftop features are located closer than 10 feet to the roof edge.	Allow for rooftop coverage greater than 65 percent.	The project proposes 84% rooftop coverage: an increase of 19% from the SMC 23.48.010.H.7 requirement. The enclosed portion of the rooftop features cover 69% of the roof (only a 4% increase from the 65% limit), the other 11% coverage comes from the canopy / trellis element which counts as covered area because it provides weather protection to the common amenity terrace. Finally, the last 4% percent come at a covered exterior area in the NE corner.	Due to the abundance of outdoor amenity area on lower levels, the rooftop becomes a prime location favoring indoor amenity space. The additional fully enclosed massing (4% coverage over what is allowed by SMC 23.48.010.H.7) helps create a 3 tiered "terraced" rooftop massing scheme. Additionally, the canopy/trellis (additional 11% coverage) and the terrace at the NE corner (4% coverage) provide vital protection from the elements (wind, rain, and sun) to allow for extended use into more inclement times of year, and provides for a variety of climate responses for a wider range of comfort. This element is also highly visible along the northern axis of Westlake, to make that view of the building more dynamic and interesting. This departure provides for a more sculpted top, with better proportions and massing then allow by prescriptive code. These design elements add interest to the overall design and specifically how the tower meets the sky. This rationale also supports the priority design guidelines; CS3-I-i, CS3-I-ii, DC2-B-I



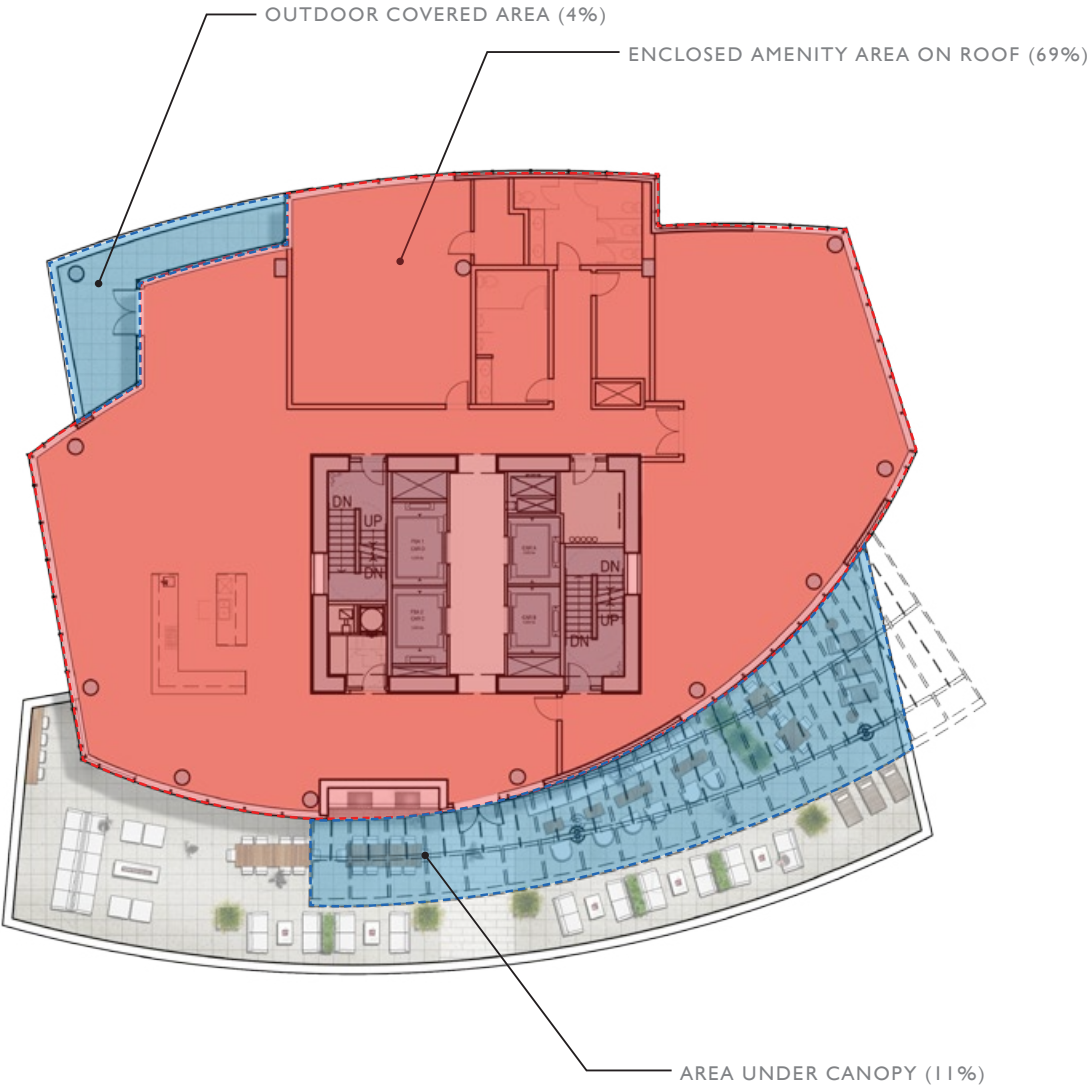
TERRACED MASSING FROM NORTHEAST



ROOFTOP AMENITY SPACE



VIEW FROM GRADE ALONG TERRY



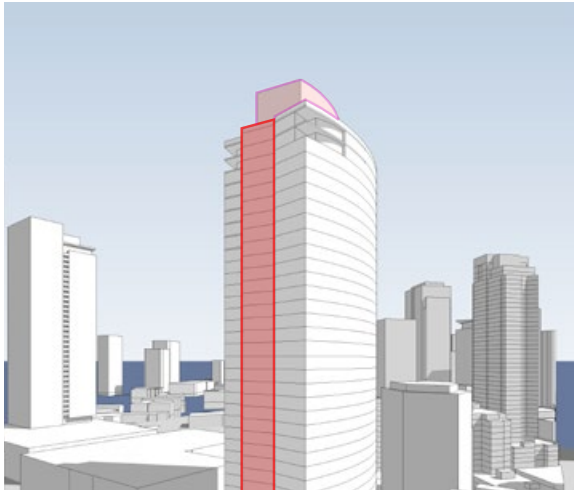
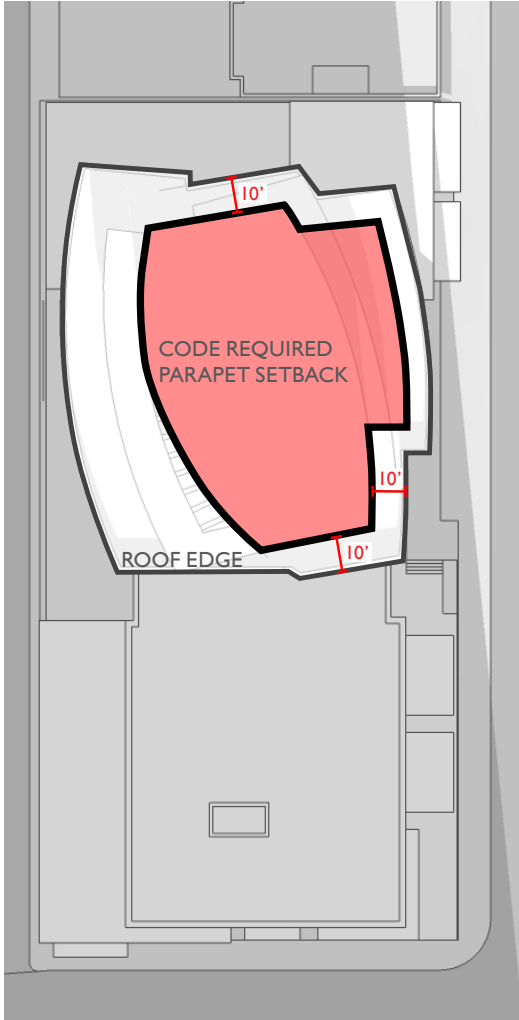
RI ROOFTOP PLAN



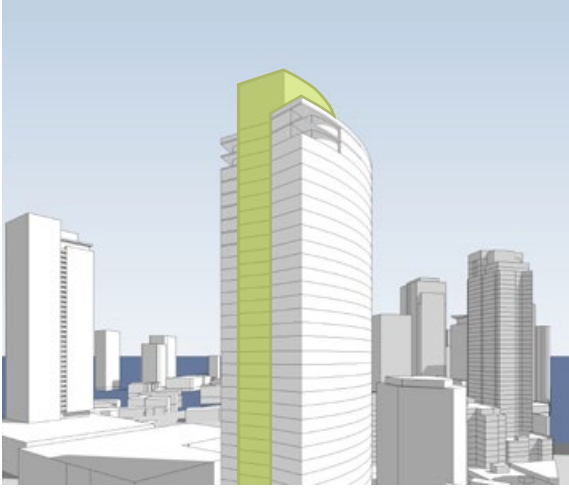
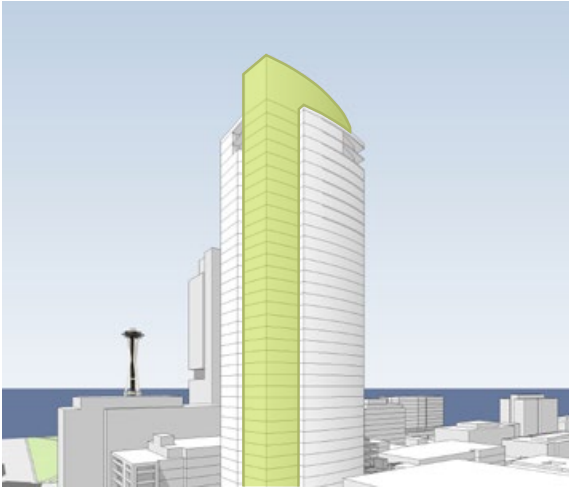
# REQUESTED DEPARTURE #3

## ROOF TOP COVERAGE

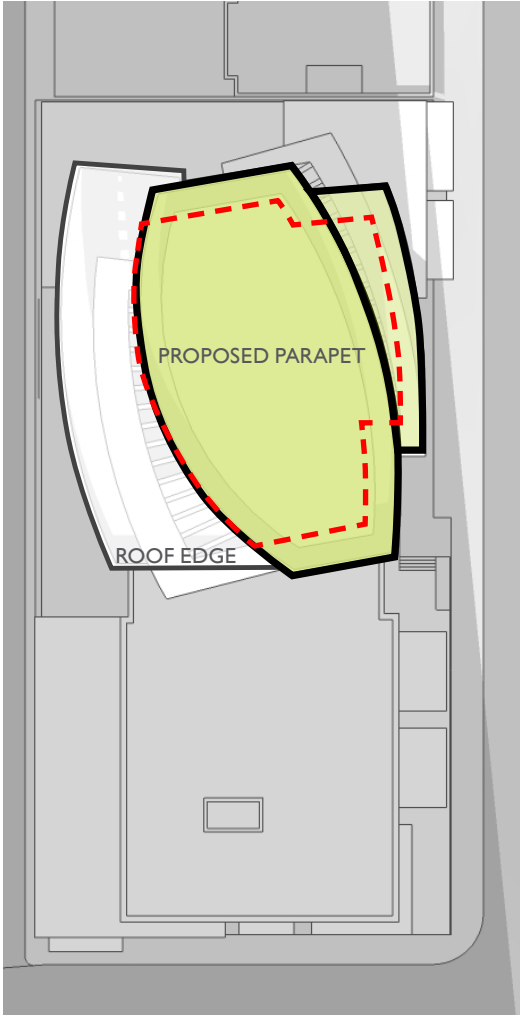
CODE SECTION	CODE REQUIREMENT	DEPARTURE REQUEST	DIFFERENCE	RATIONALE FOR REQUEST
SMC 23.48.010.H.7 ROOF TOP COVERAGE	<p>At the applicant's option, the combined total coverage of all features listed in subsections 23.48.010.H.4 and 23.48.010.H.5 above may be increased to 65 percent of the roof area, provided that all of the following are satisfied:</p> <p>a. All mechanical equipment is screened; and</p> <p>b. No rooftop features are located closer than 10 feet to the roof edge.</p>	<p>Allow enclosed rooftop features (amenity space, mechanical screen, trellis / canopy) to be flush to façade below without ten foot setback.</p>	<p>The proposed enclosed rooftop space is set back less than ten feet from the roof edge, on the north, east, and south. Those facades are co-planar with face of tower below.</p>	<p>Architecturally, the project team feels that having the additional height of the amenity space coplanar with the tower facade simplifies the massing, further slenderizes the tower, and creates a more elegant vertical resolution between the tower and sky. The design team has created vertical spandrel motifs that further emphasize this continuous plane, a 10' offset would disrupt these measures. The long vertical lines also emphasize the retail and residential lobby located at grade and increases the project's identity potential. This rationale also supports the priority design guidelines; CS3-I-i, CS3-I-ii, DC2-B-I</p>



MASSING WITH CODE REQUIRED 10' SETBACK AT ROOF EDGE



PROPOSED TOWER MASSING WITHOUT 10' SETBACK

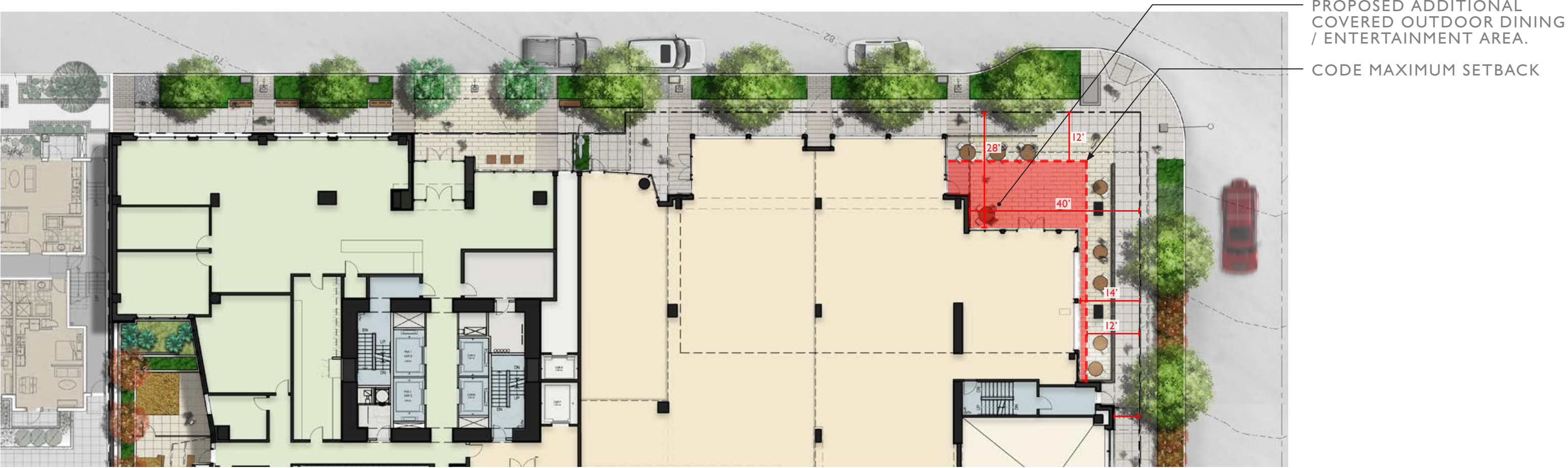




REQUESTED DEPARTURE #4

MAXIMUM SETBACKS

CODE SECTION	CODE REQUIREMENT	DEPARTURE REQUEST	DIFFERENCE	RATIONALE FOR REQUEST
SMC 23.48.014.A.3.b MAXIMUM SETBACKS	<p>Except on Class I Pedestrian Streets, as shown on Map A for 23.48.014, and as specified in subsection 23.48.014.B.1, the street-facing façade of a structure may be set back up to 12 feet from the street lot line subject to the following (Exhibit B for 23.48.014):</p> <p>1) The setback area shall be landscaped according to the provisions of Section 23.48.024;</p> <p>2) Additional setbacks are permitted for up to 30 percent of the length of portions of the street façade that are set back from the street lot line, provided that the additional setback is located 20 feet or more from any street corner; and</p> <p>3) Any required outdoor amenity area, or other required open space, or usable open space provided in accordance with subsections 23.49.013.B.4.c, 23.48.014.F or 23.48.014.G is not considered part of the setback area and may extend beyond the limit on setbacks from the street lot line that would otherwise apply under subsections 23.48.014.A.3.b or 23.49.014.A.3.b.2.</p>	<p>Along Terry Avenue; allow for a setback greater than twelve (12) feet but not to exceed twenty-eight (28) feet for a hardscaped retail entry plaza that does not meet the provisions of SMC 23.48.014.A.3.b. Along Denny Way; allow for a setback greater than twelve (12) feet but not to exceed forty-five (45) feet for a hardscaped retail entry plaza that does not meet the provisions of SMC 23.48.014.A.3.b. It is important to note that the project is voluntarily setting back from both the Terry Avenue (5') and Denny Way (6') property lines to best adhere to specific street design guidelines and align with existing sidewalks, so the setbacks are effectively 23' and 39' from our "new" setback.</p>	<p>The project proposes a setback for an additional sixteen (16) feet at the corner facing Terry Avenue, and thirty-three (33) feet facing Denny Way, for the retail entry plaza</p>	<p>Terry Avenue is one of the preeminent emerging retail / dining / entertainment corridors in Seattle. Meanwhile the Denny Way Design Guidelines envision this auto-oriented cross town connector as a more pedestrian oriented street. The gateway to this district needs to showcase and support these goals, which our proposal does by celebrating the entry and corner condition in a large outdoor entry and "retail porch." Along Terry this porch provides a large outdoor retail area to compliment the activity along Terry. On Denny, the setback provides an "eddy" of pedestrian interest and respite along the otherwise fast moving thoroughfare lined with facades that do not generally support the pedestrian scale envisioned. It also provides for a more appropriate urban response and a "place maker" solution at this gateway corner. This rationale also supports the priority design guidelines; CSI-C, CSI-I-i, CS2-A-I, CS2-B-I, CS2-B-2, CS2-B-3, CS2-C-I, CS2-I-iii, CS2-I-iv, CS3-A-4, CS3-I-i, PL1-B-I, PL1-B-3, PL2-A-I, PL1-I-iii, PL2-I-i, PL3-C-I, PL3-C-3, PL3-II, PL3-II-ii, PL3-II-iii, PL3-II-iv, DC2-C-I, DC2-C-2.</p>

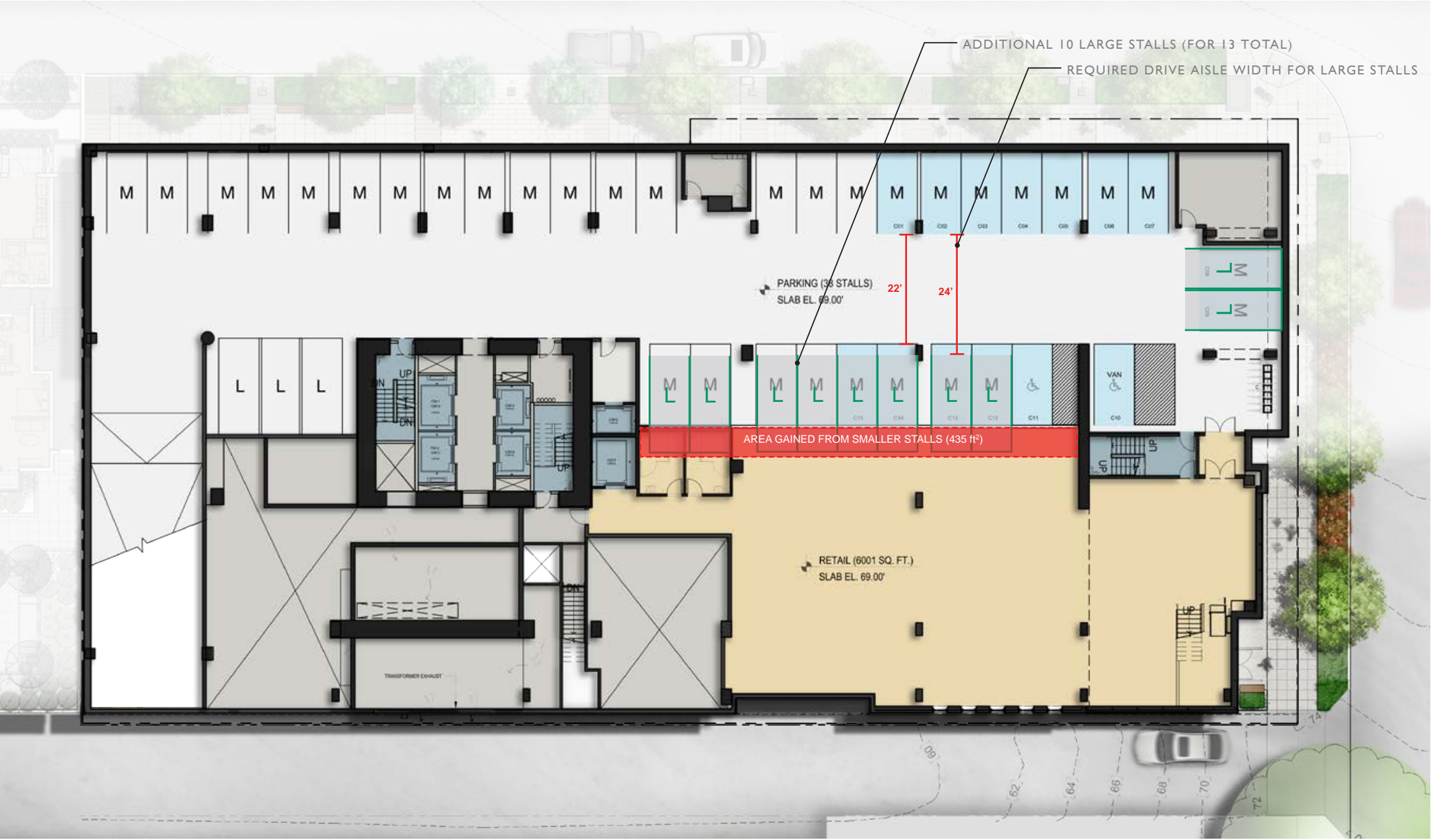




# REQUESTED DEPARTURE #5

## PARKING SPACE REQUIREMENTS

CODE SECTION	CODE REQUIREMENT	DEPARTURE REQUEST	DIFFERENCE	RATIONALE FOR REQUEST
SMC 23.54.030.B.2.b <b>PARKING SPACE REQUIREMENTS</b>	Nonresidential uses and live-work units. When between 11 and 19 parking spaces are provided, a minimum of 25 percent of the parking spaces shall be striped for small vehicles. The minimum required size for these small parking spaces shall also be the maximum size. A maximum of 65 percent of the parking spaces may be striped for small vehicles. A minimum of 35 percent of the spaces shall be striped for large vehicles.	Allow for nonresidential use parking with no large spaces.	The project proposes fifteen (15) parking spaces for nonresidential use, with none striped as large spaces. Large spaces are required to a minimum of 35 percent of total spaces, a difference of six (6) spaces.	Not providing large stalls reduces both the size of the drive aisle to access those stalls and the parking stall depth itself. This in turn, provides for a larger retail floorplate for the SW corner retail. The additional sf will make the space more attractive to tenants, which will make the space more marketable, and thus have a better chance at being leased. Leasability in a competitive commercial market should provide for more consistently active retail which will benefit the streetscape and help draw pedestrians up the hill, and around Terry. This rationale also supports the priority design guidelines; DCI-A-3 (This larger retail area will help to support activation of this location, and the 22' drive isle is perfectly viable.)



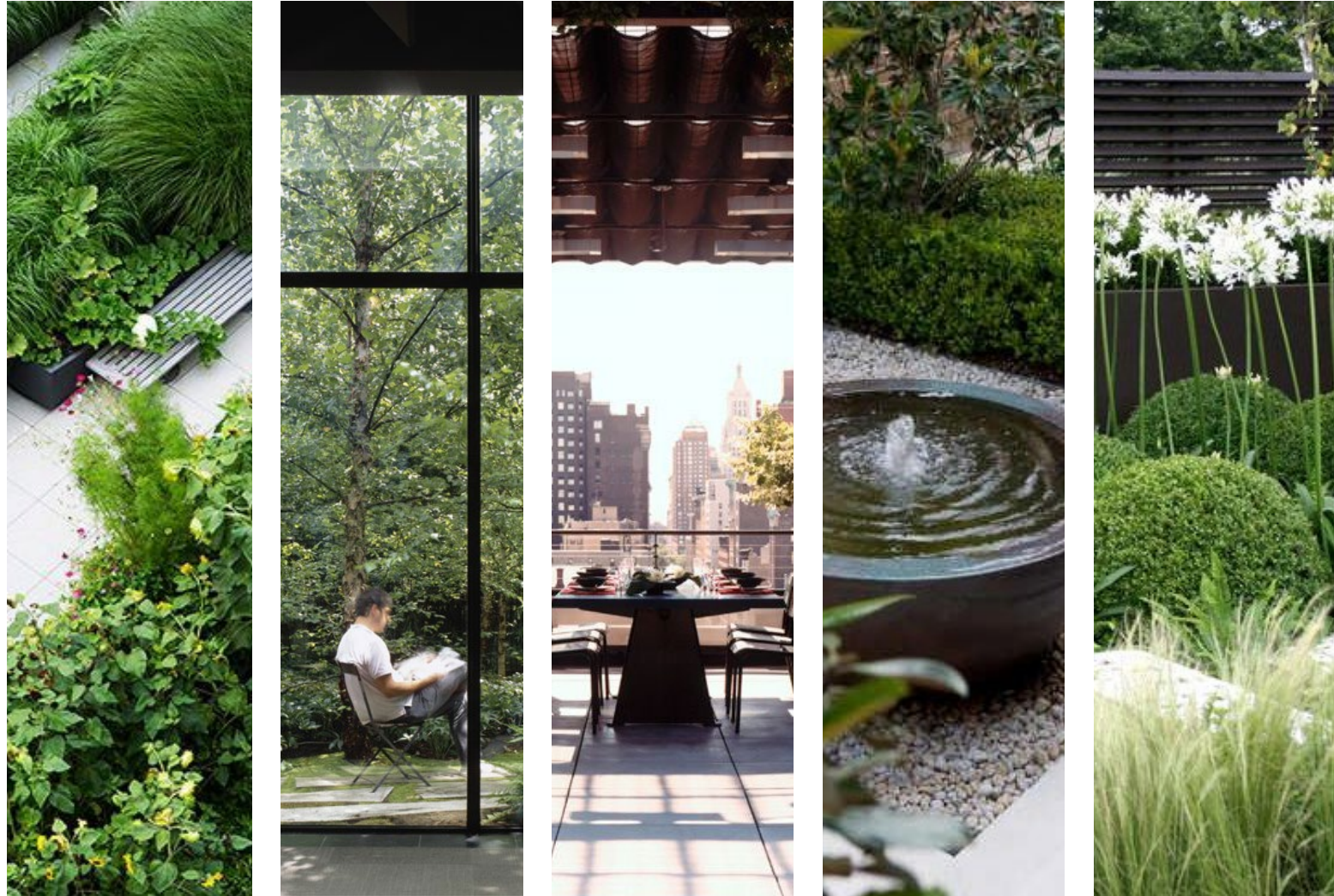








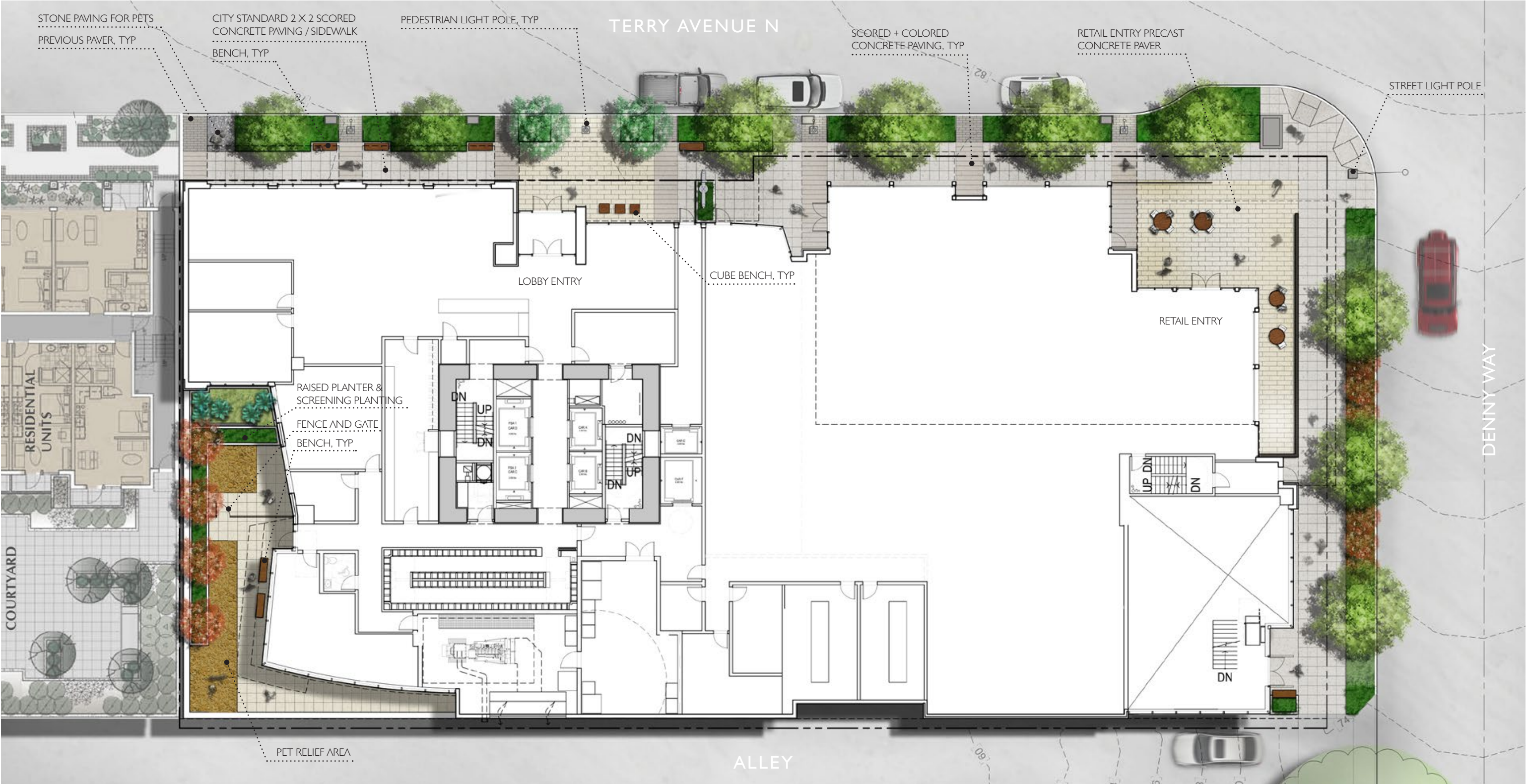




# LANDSCAPE



# LI STREETSCAPE LANDSCAPE PLAN





LI STREETScape VIEWS – TERRY AVENUE



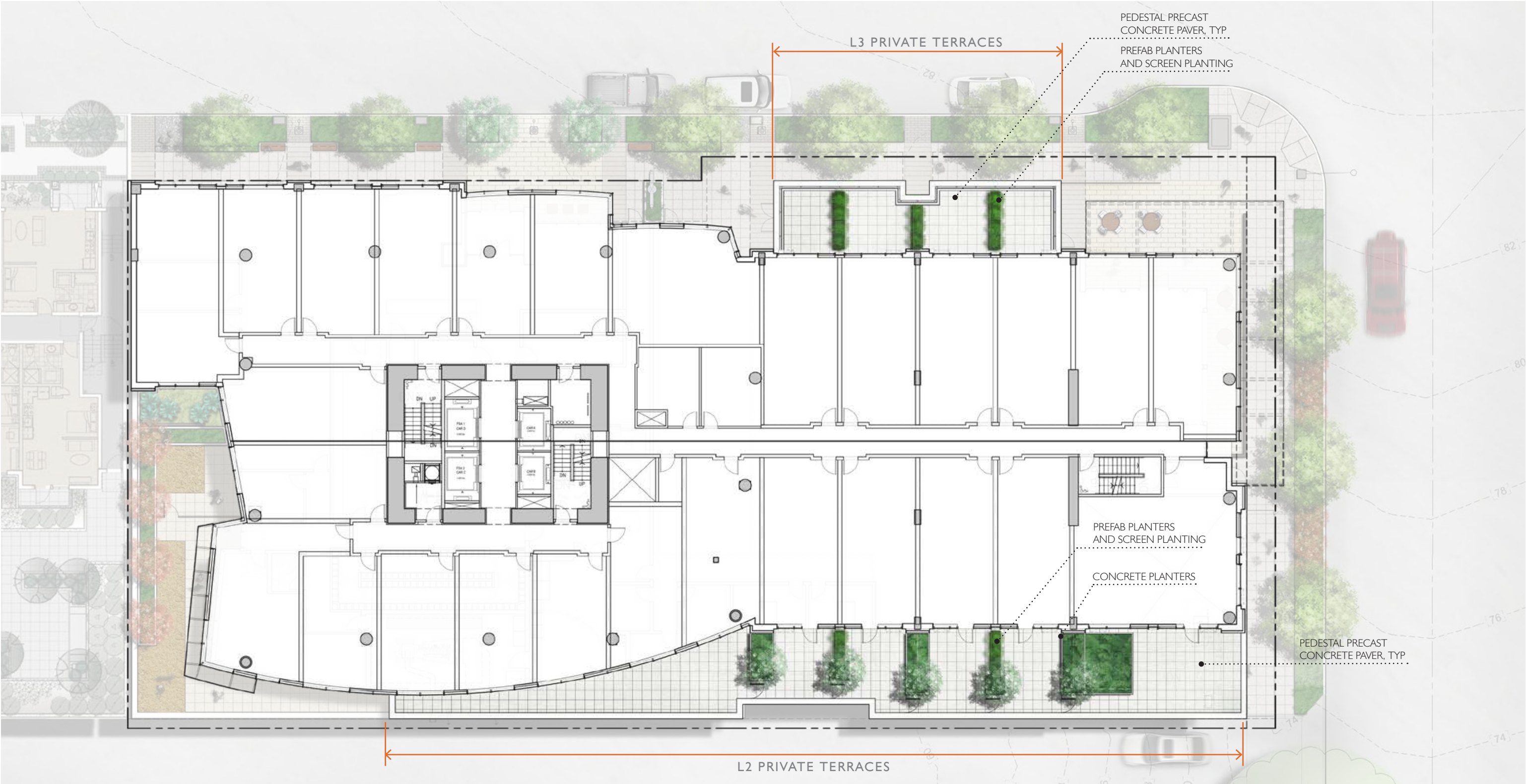


LI STREETSCAPE VIEWS – DENNY WAY



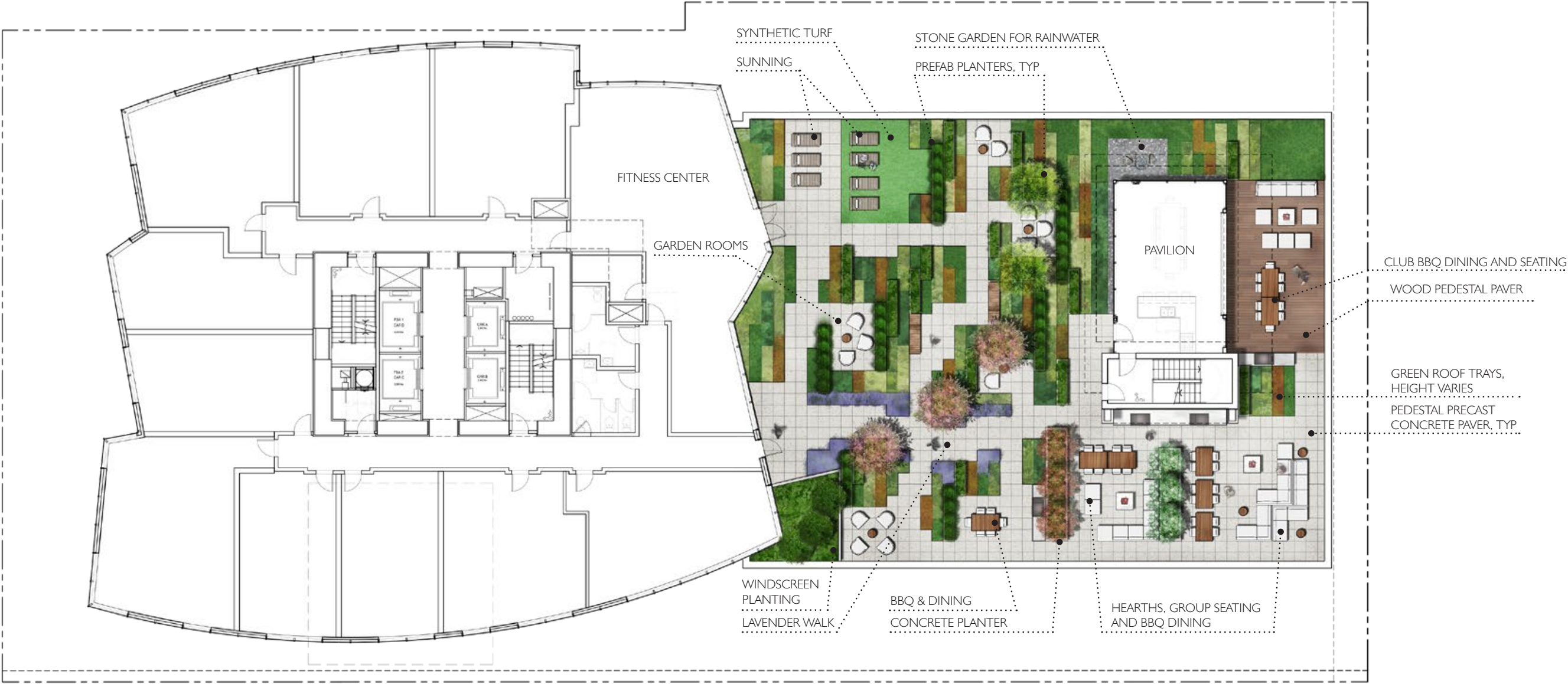


L2 + L3 PRIVATE TERRACE LANDSCAPE PLAN





# L7 AMENITY ROOF TERRACE LANDSCAPE PLAN



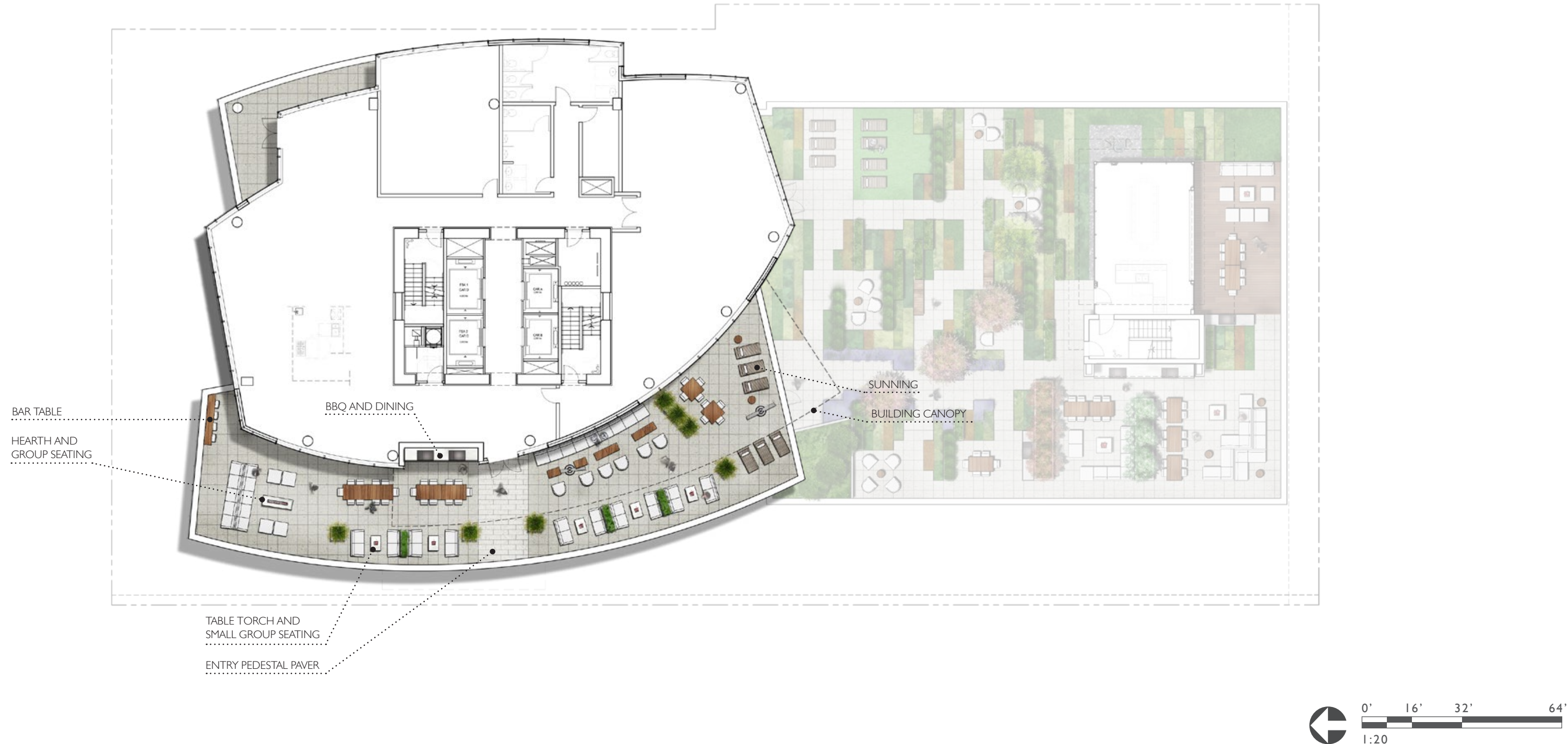


L7 AMENITY ROOF TERRACE LANDSCAPE VIEWS



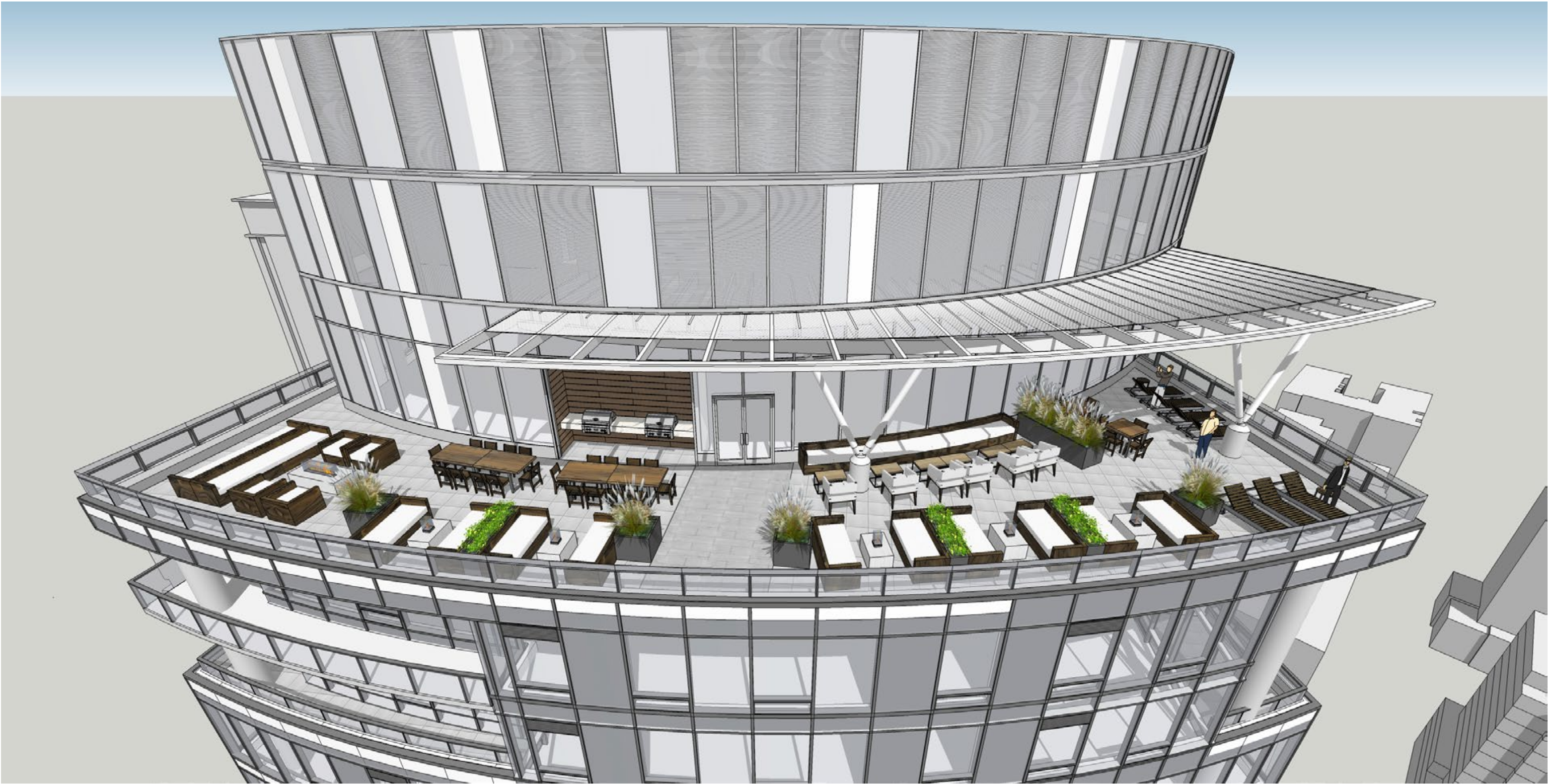


# RI AMENITY ROOF TERRACE LANDSCAPE PLAN





RI AMENITY ROOF TERRACE LANDSCAPE VIEWS





# PLANT PALETTES

## TERRY AVENUE STREETScape



Green Vase Zelkova



Silver Queen Euonymus



Firepower Nandina



Rose Creek Abelia



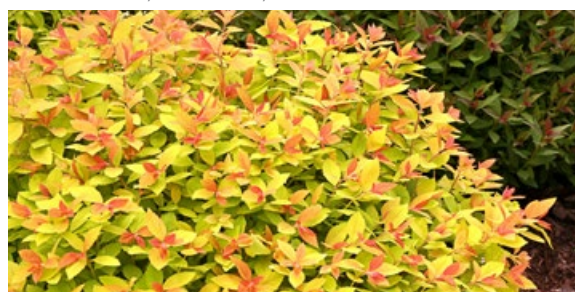
Musashino Zelkova



Spire Cherry



Golden Ruby Barberry



Double Play Gold Spirea

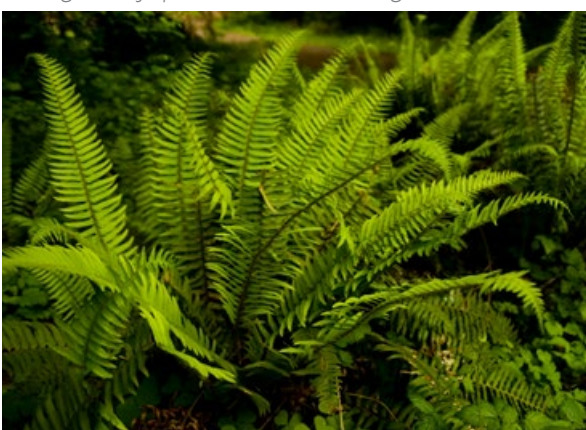
## DENNY WAY STREETScape AND BUILDING FRONTAGE



Tonto Dwarf Crape Myrtle



Variegated Japanese Mock Orange



Western Sword Fern



Bell Tower Maple

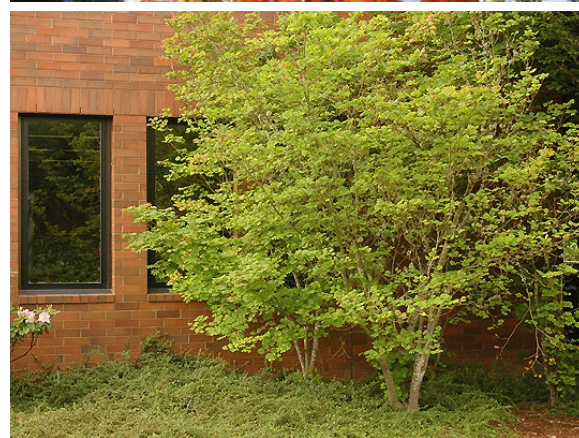


Dark Green Spreader Yew



Variegated Bluebeard

## LI DOG RUN



Pacific Fire Vine Maple



Japanese Forest Grass



Mariessi Variegated Lacecap Hydrangea



# PLANT AND MATERIAL PALETTES

## L2 AND L3 PRIVATE TERRACES



Doublefile Viburnum



Weeping Boxwood



Biloxi Crape Myrtle

## L7 AMENITY TERRACE: 5 TREE CONCEPT (BIRDS 60%, BUTTERFLY 40%)



Natchez Crape Myrtle



Hearts of Gold Redbud



Calistoga California Fuchsia



Coppertina Ninebark



Sargent Columnar Cherry



Lucifer Crocosmia



Hummingbird Summersweet



Lavender



Blue Mist Caryopteris

## HARDSCAPE MATERIALS



Wood Pedestal Paver



Wood Pedestal & Concrete Unit Paver



Precast Concrete Unit Paver



Pervious Unit Paver




# STREET LEVEL LIGHTING PLAN

1

### LED WALL SCONCE


LED wall sconce lights provide regular lighting along the building facade as well as providing safety light in dimly lit areas.



2

### LED CYLINDER LIGHT


LED cylinder lights sit inside glass canopies and illuminate the ground below.



3

### RECESSED LIGHTING

LED adjustable down-lighting sits in soffits on both the pavilion and wind screens, providing a lit area for both wayfinding and nighttime functions.








# ROOF LEVEL LIGHTING PLAN

1

### LED PATH LIGHT


These LED stick lights help illuminate paths, planting beds, and seating areas with a light that washes the ground and does not create glare.



2

### LED SPOT LIGHT


LED spotlight mounted in planting areas to illuminate major trees.



3

### LED STEP LIGHT


LED lighting along the perimeter of the parapet illuminate pathways and egress as well as providing ambient light for seating areas.

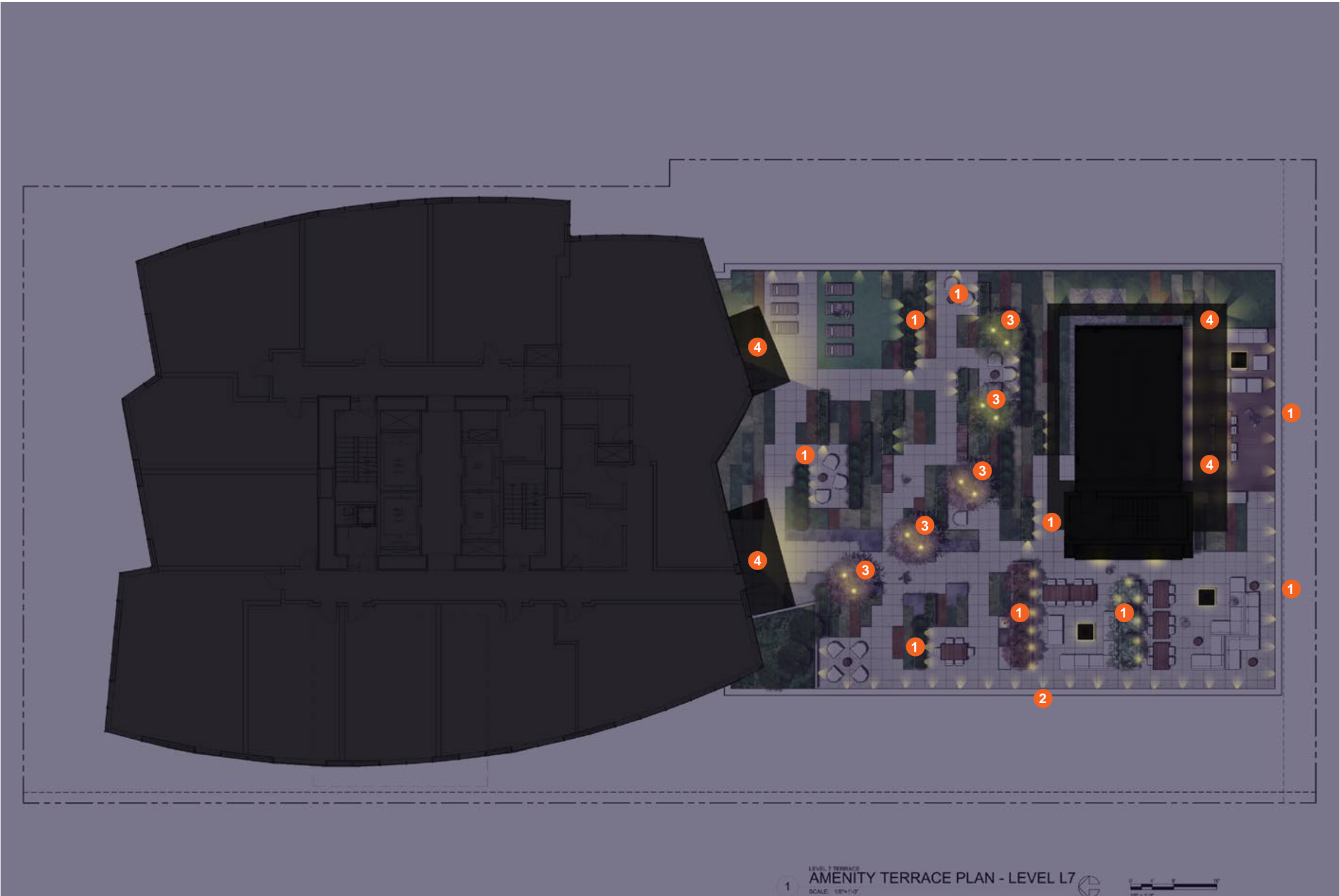


4

### RECESSED LIGHTING

LED adjustable down-lighting sits in soffits on both the pavilion and wind screens, providing a lit area for both wayfinding and nighttime functions.







# POTENTIAL SIGNAGE LOCATIONS



## TYPES OF SIGNAGE

- SIGNAGE AT STOREFRONT GLAZING
- 3D LETTERS MOUNTED TO TOP OF CANOPY
- BLADE SIGN MOUNTED TO UNDERSIDE OF CANOPY









## APPENDIX



# AERIAL CONTEXT



## BUILDING USE COLOR KEY

- OFFICE / COMMERCIAL
- RESIDENTIAL / HOSPITALITY
- MEDICAL / RESEARCH
- PROJECT SITE
- UNDER CONSTRUCTION
- PLANNED PROJECTS



# LAND USE CODE SUMMARY

## SEATTLE MUNICIPAL CODE TITLE 23

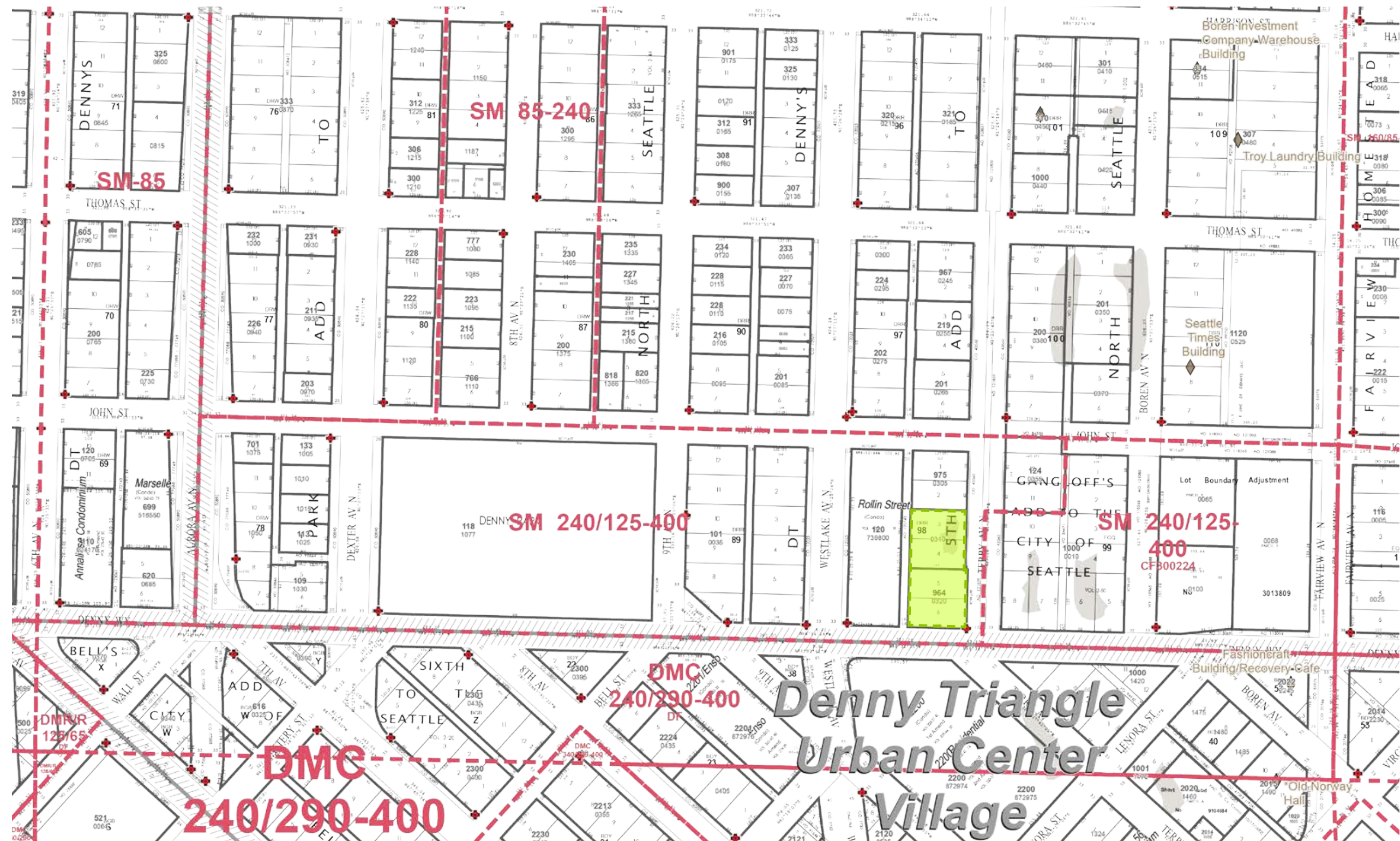
The following is an abbreviated summary and general overview of the existing zoning for the site.

PARCEL NUMBER(S):	1986200310, 1986200320
ADDRESS:	970 Denny Avenue
SITE AREA:	28,264 SF (240.75' x 120', .65 acres)
ZONING:	SM 240/I25-400
OVERLAY(S):	South Lake Union Urban Center South Lake Union Archaeological Buffer
ALLOWABLE BUILDING HEIGHT:	Structure height measured at midpoint of Terry Ave. lot line (80.73' elevation) 400'-0" Residential max height limit with additional 35' for vertical transportation.  Note: Pending code update before City Council would allow for 45' of vertical transportation overrun instead of 35'.
PODIUM HEIGHT:	[23.48.013 Map A] Southern Parcel (1986200301) = 65' Northern Parcel (1986200320) = 45'
PODIUM LOT COVERAGE:	[23.48.013.B.4.b] The average footprint of all stories below the podium height = 75% of the site area
FACADE MODULATION:	[23.48.013.D.1] No limit to Podium Height Podium Height to 125' = 150' 126-400' = 120'
FACADE SETBACK LIMITS:	Setbacks up to 12 feet allowed at grade if you meet provision of 23.48.14.A.3.b.1,2,3 [23.48.014.A.3.b] Denny Way 1' Setback for ever 2' over 75', to a maximum of 15' setback.
ROOFTOP FEATURES COVERAGE:	Enclosed Common recreation / Mechanical = 65% Maximum with 10' rooftop setback and all mechanical equipment is screened. [23.48.010.H.8]





# ZONING / PARCEL MAP



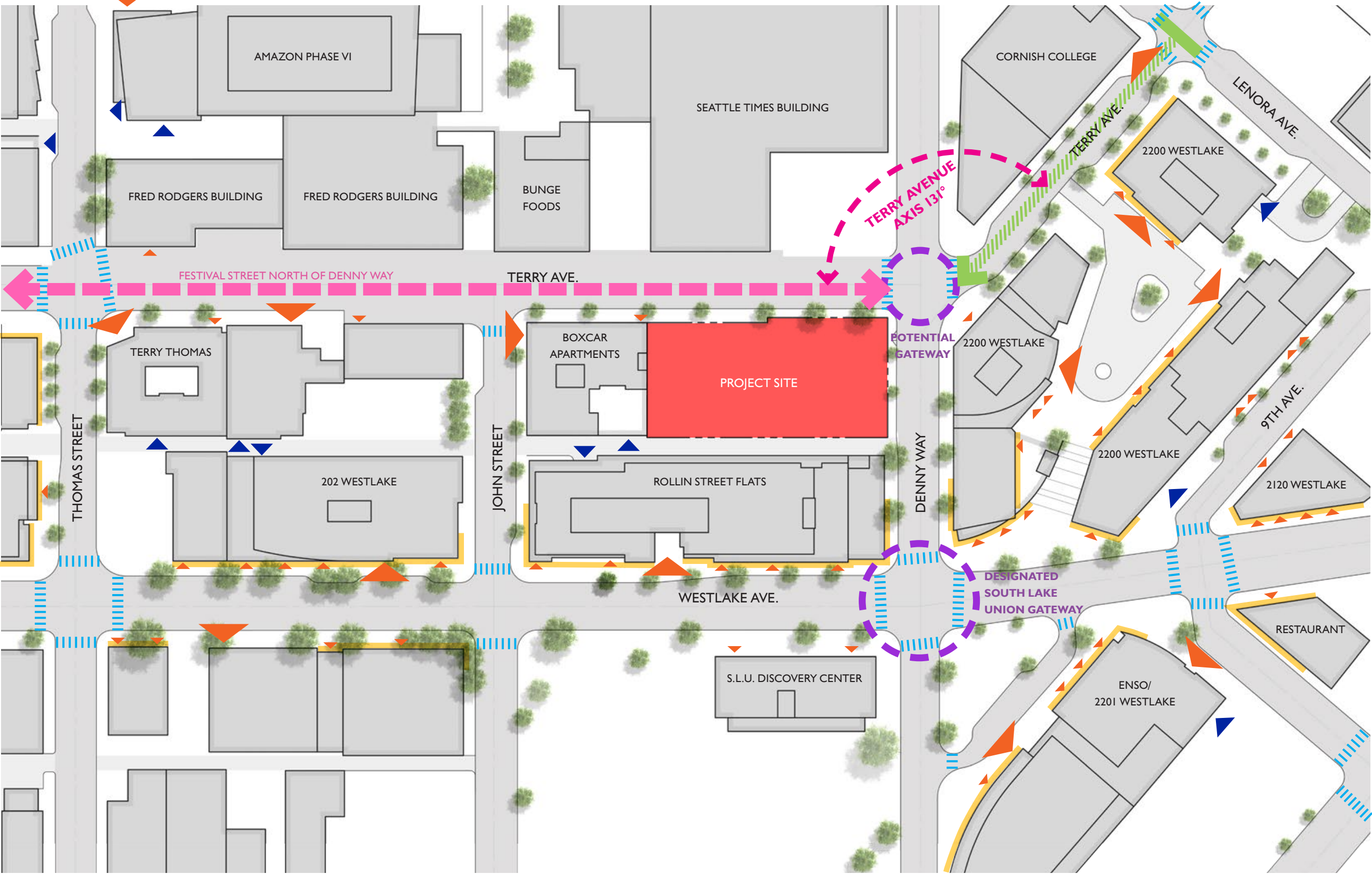


# TRANSPORTATION AND LOCAL INFRASTRUCTURE



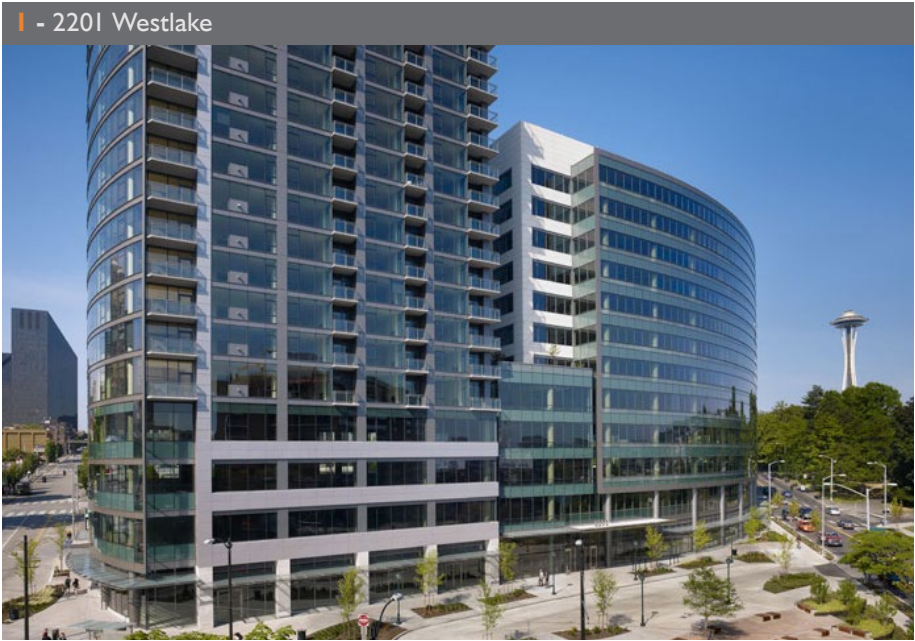


# PROJECT VICINITY OBSERVATIONS AND CONTEXT





# NEIGHBORHOOD CONTEXT





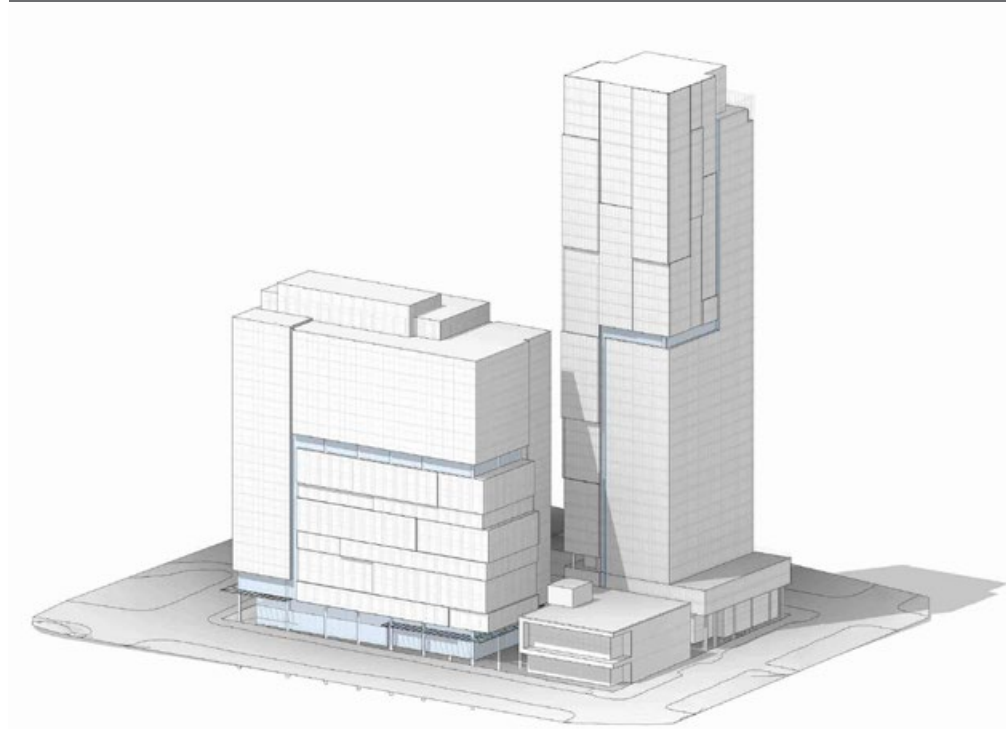
3 - 9th and Lenora



4 - Seattle Times Project



5 - South Lake Union Block 48



6 - Amazon Phase IV



7 - The Terry Thomas



8 - 202 Westlake



9 - Group Health





# DESIGN INSPIRATION – PODIUM



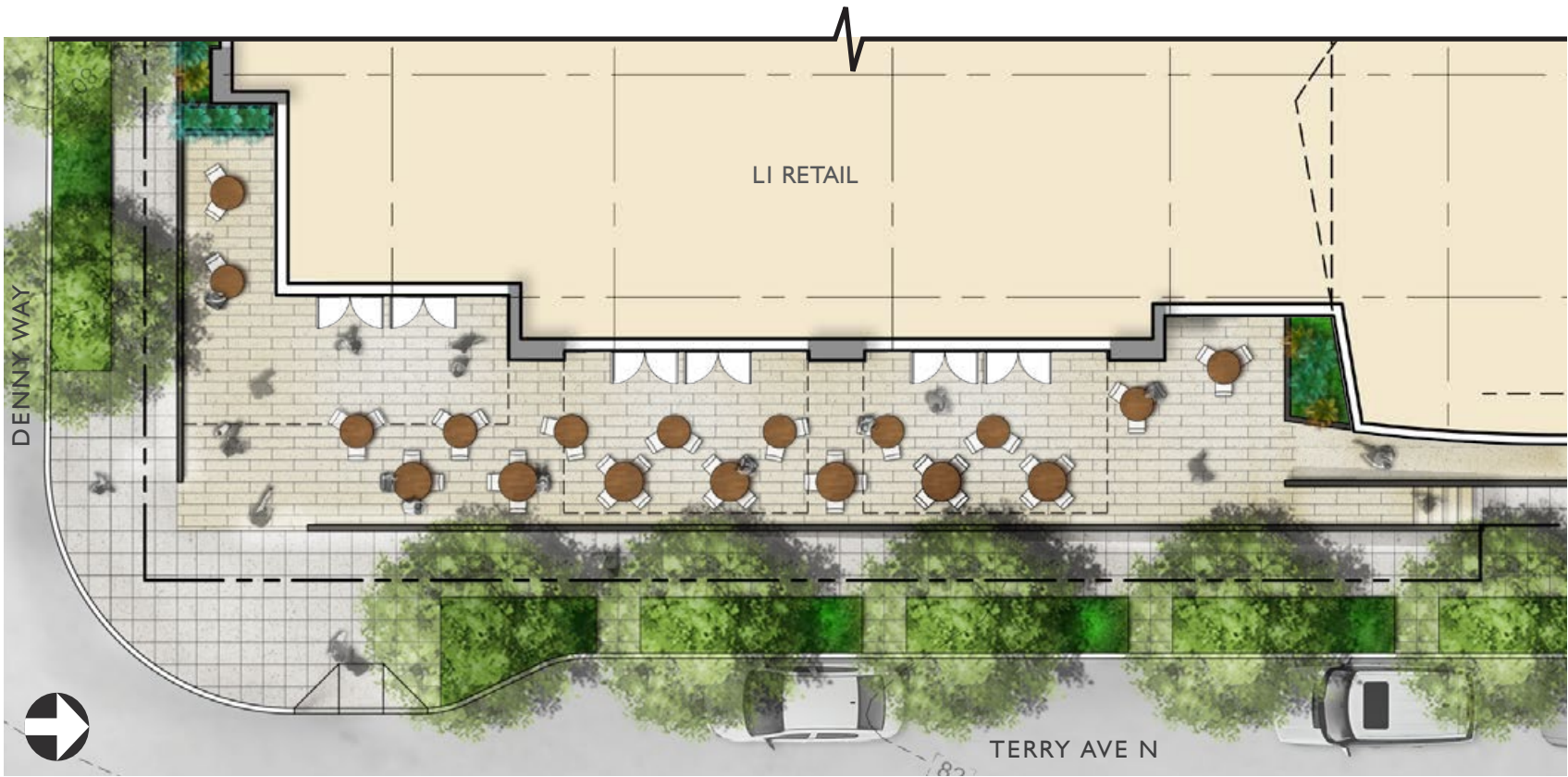
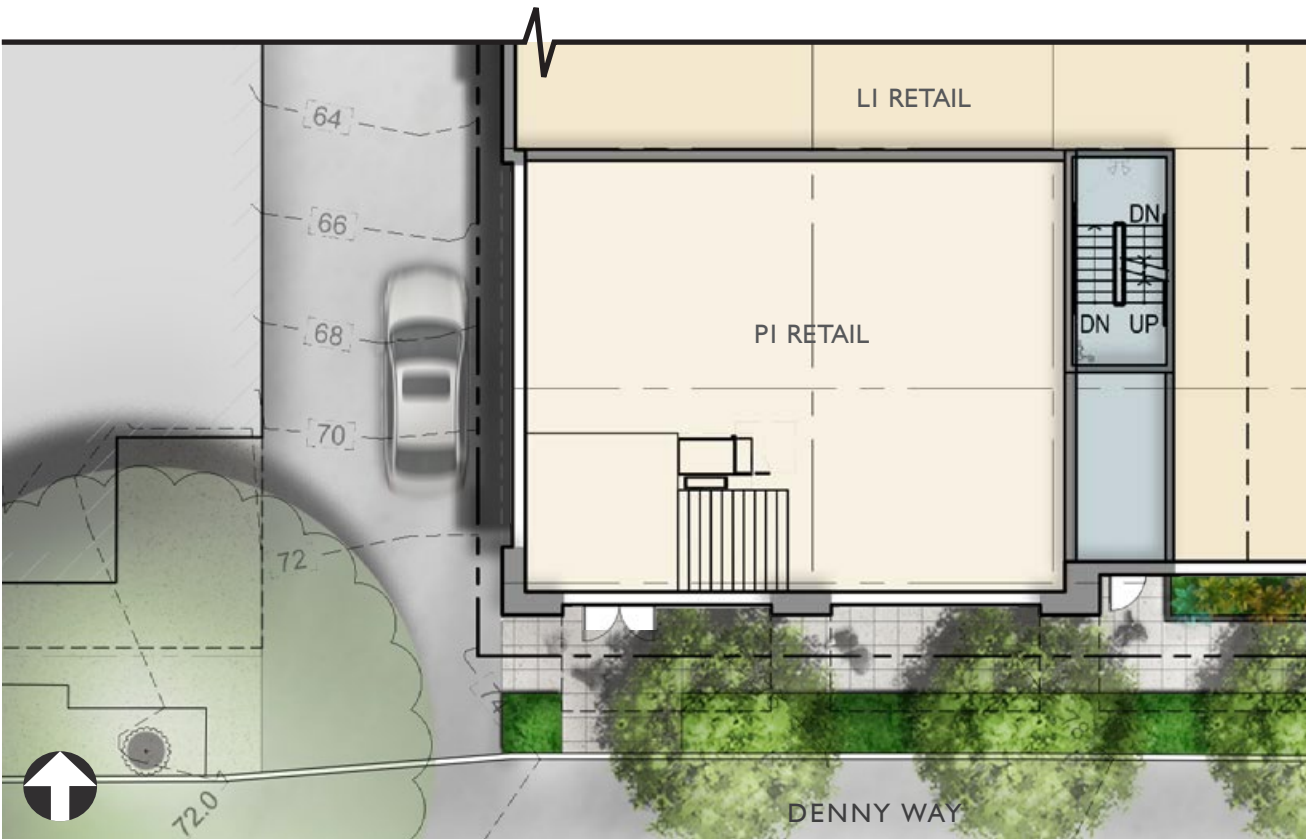


# DESIGN INSPIRATION – TOWER



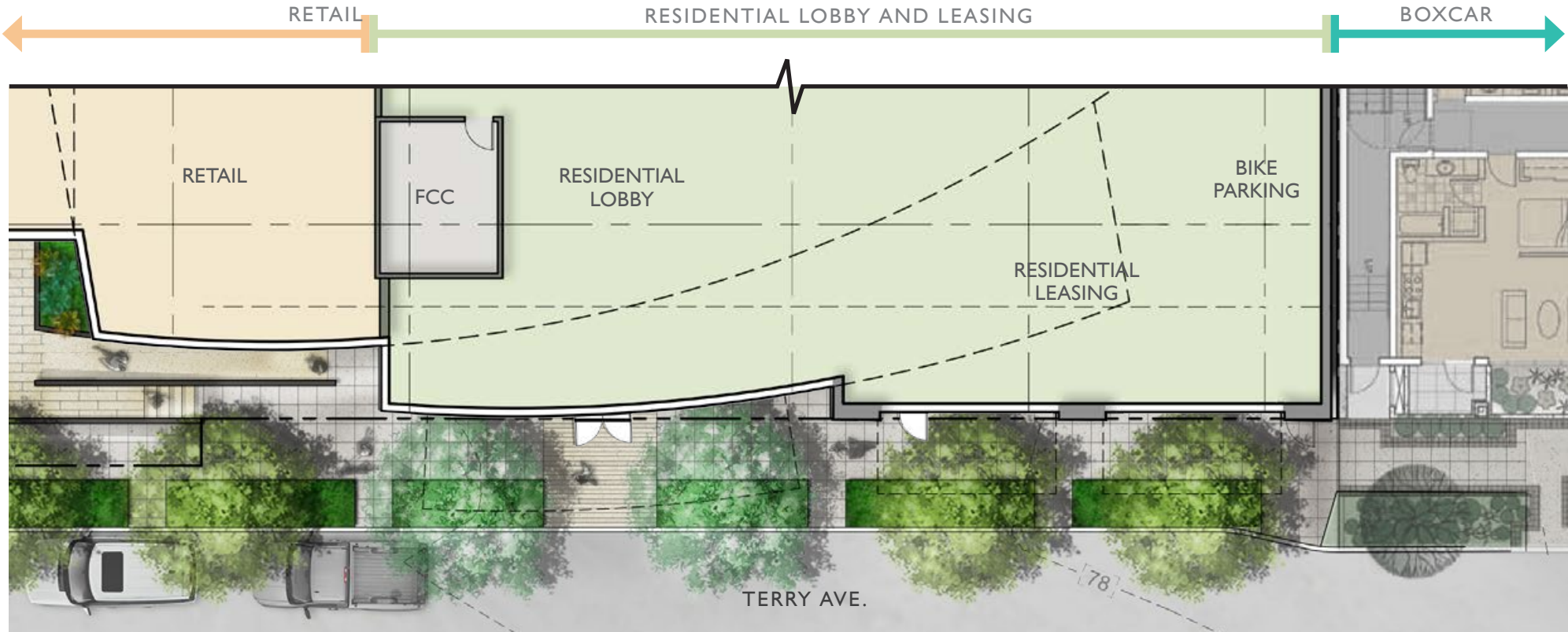


# STREET CORNER PLANS AT EDG



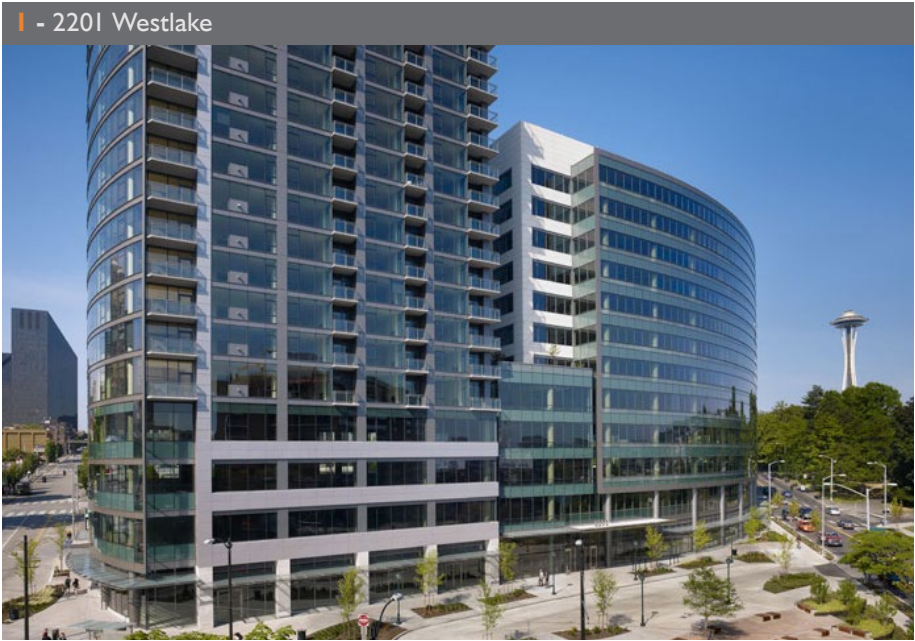


# STREET PLANS AT EDG





# NEIGHBORHOOD CONTEXT





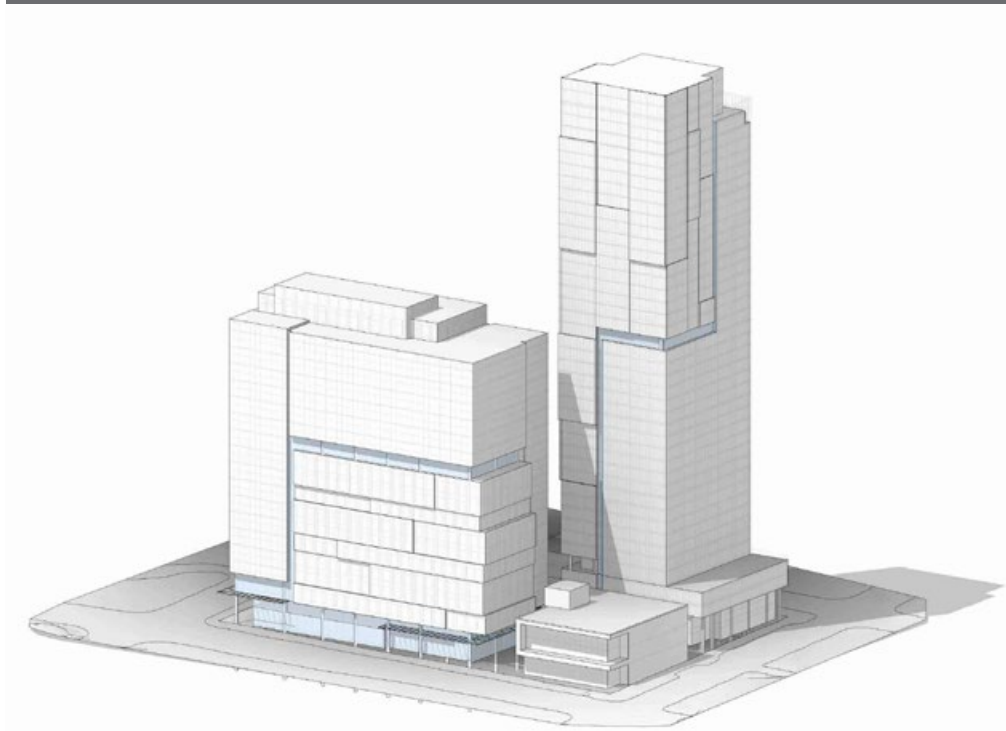
3 - 9th and Lenora



4 - Seattle Times Project



5 - South Lake Union Block 48



6 - Amazon Phase IV



7 - The Terry Thomas



8 - 202 Westlake

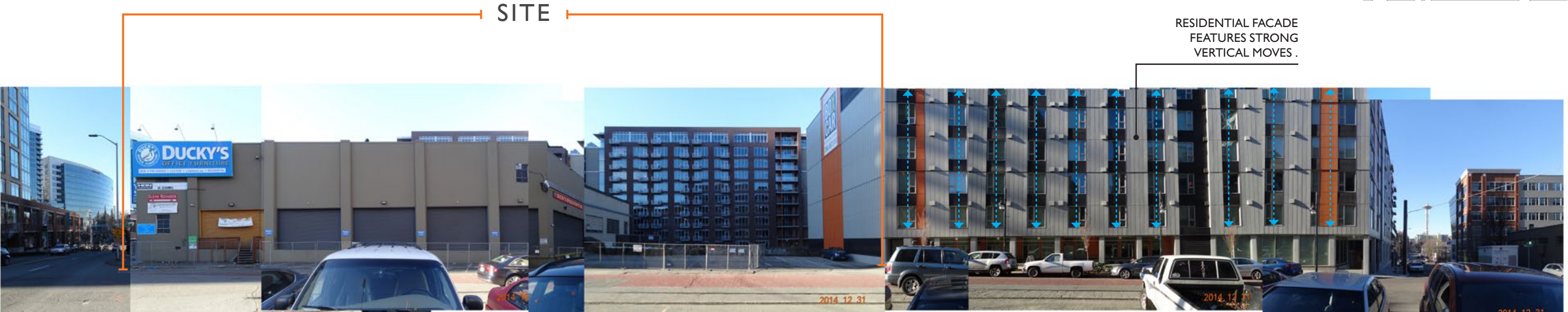
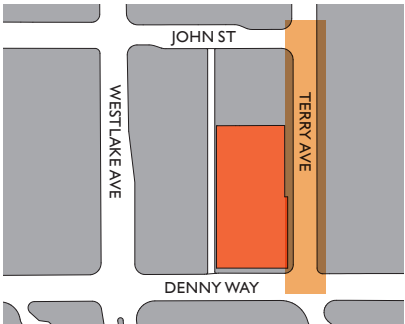


9 - Group Health





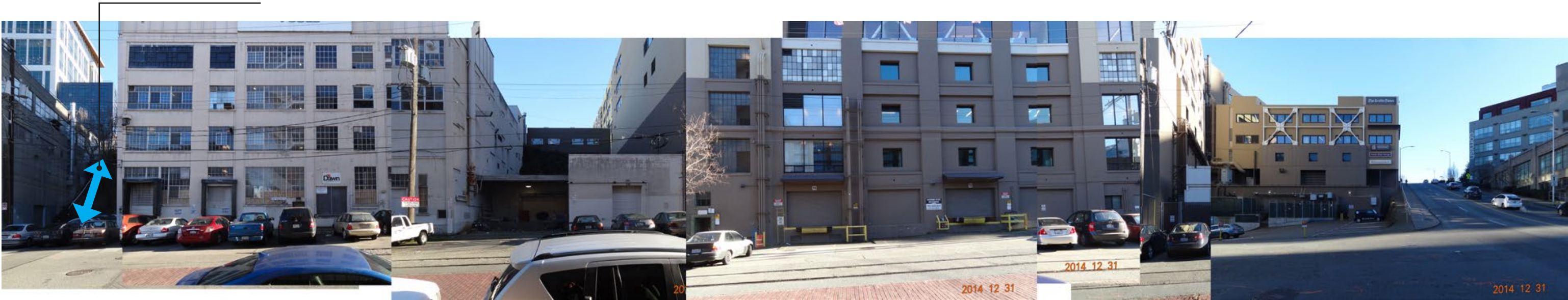
# ELEVATIONS – TERRY AVENUE



RESIDENTIAL FACADE  
FEATURES STRONG  
VERTICAL MOVES.

WEST SIDE OF TERRY AVENUE

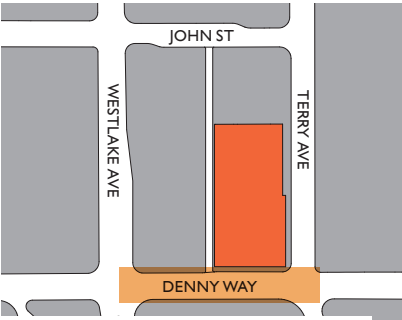
POTENTIAL HILL CLIMB  
/ STEPS CONNECTING  
NEIGHBORHOODS



EAST SIDE OF TERRY AVENUE



# ELEVATIONS – DENNY WAY



SOUTH SIDE OF DENNY WAY

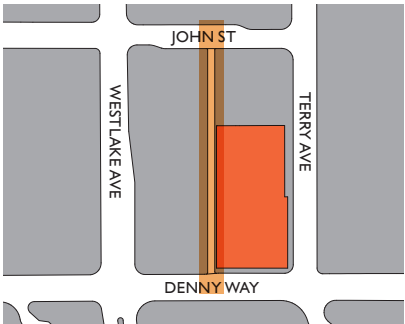


NORTH SIDE OF DENNY WAY

MAIN ENTRY LOBBY



# ELEVATIONS – ROLLIN STREET



WEST SIDE OF ALLEY



EAST SIDE OF ALLEY

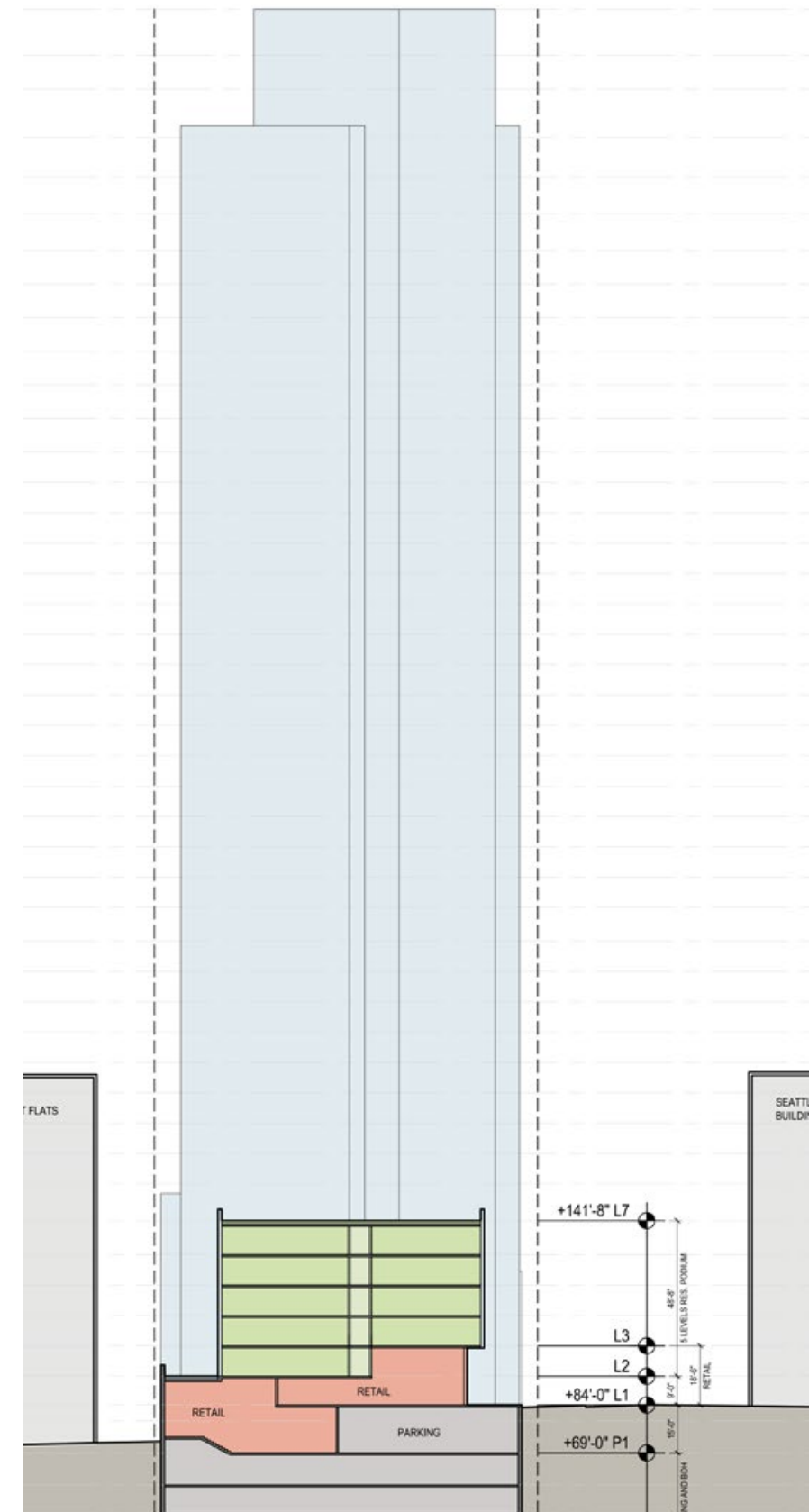
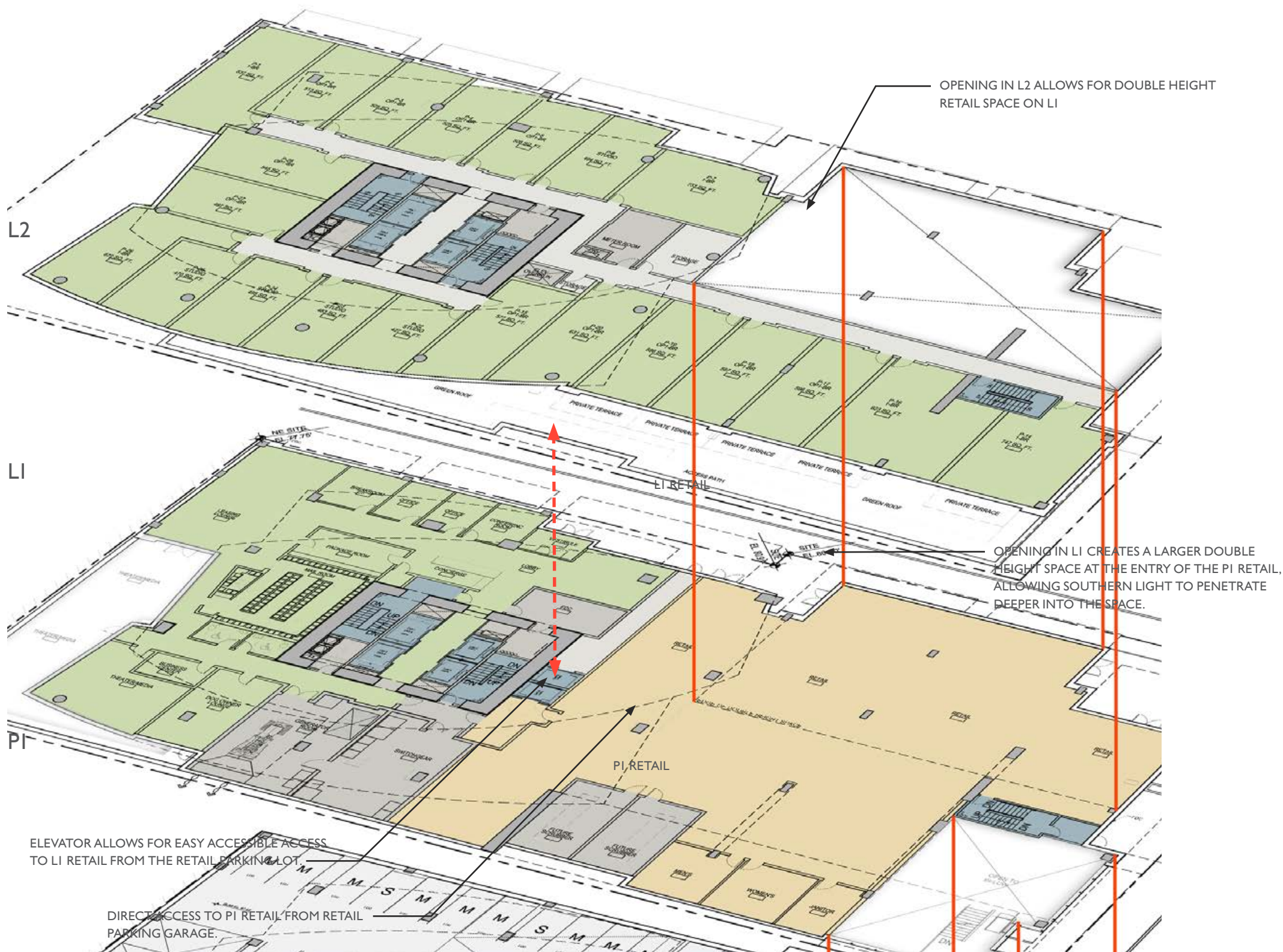


# SITE CONDITIONS



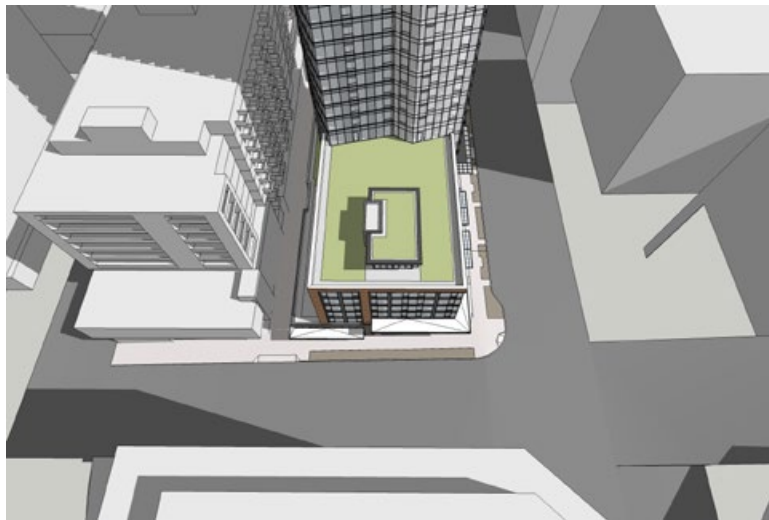


# RETAIL LEVELS

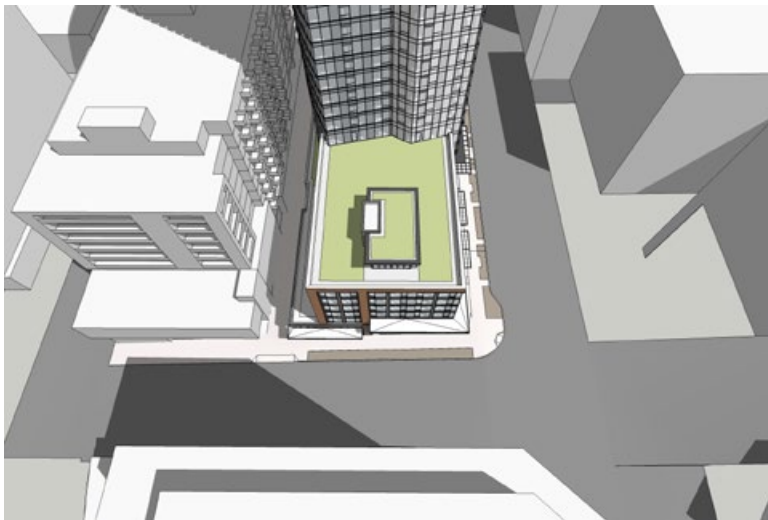




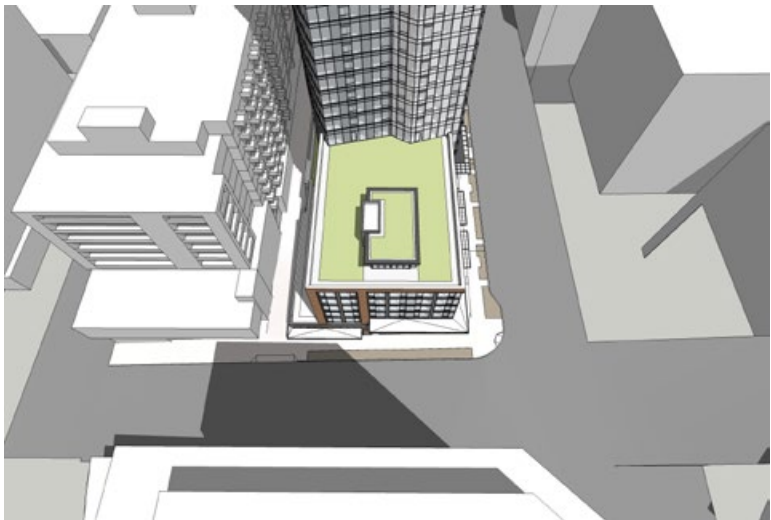
# SUN SHADOW STUDIES OF PEDESTRIAN ENVIRONMENTS – JUNE 21ST



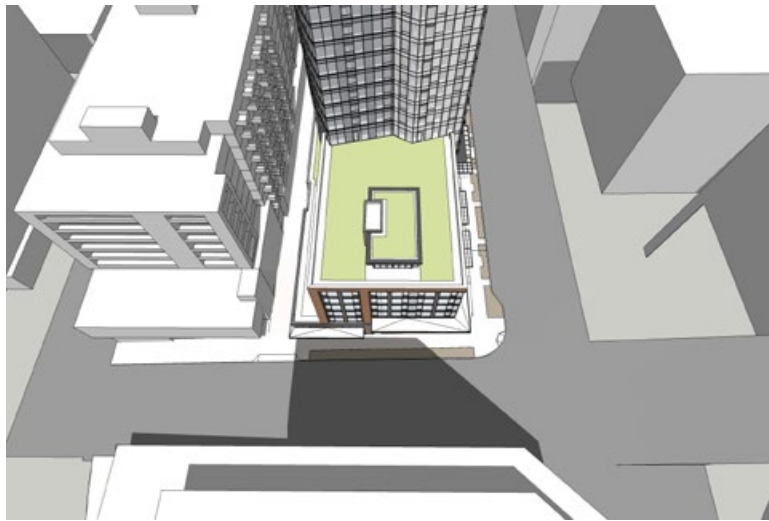
9 am



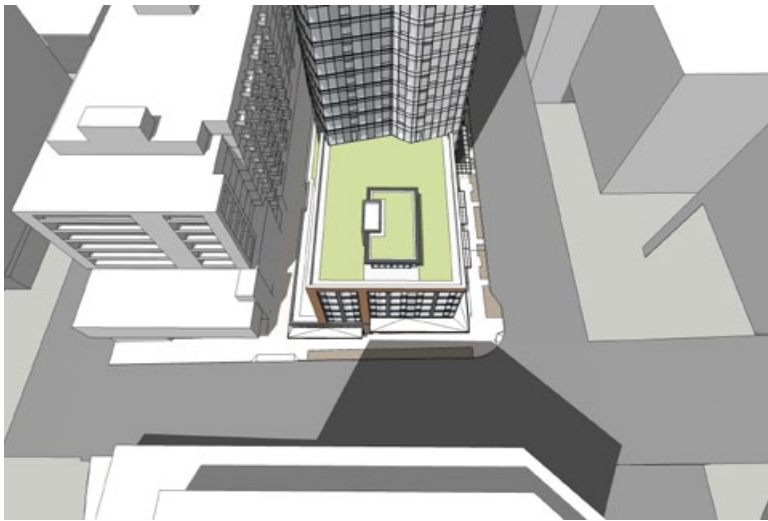
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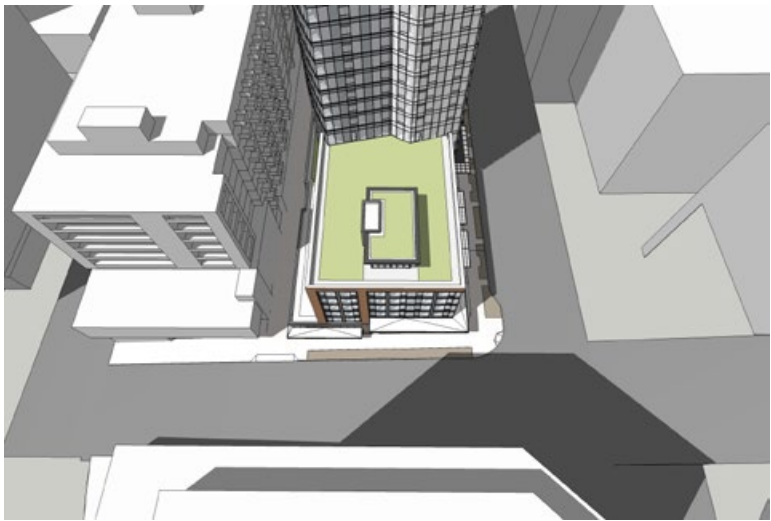
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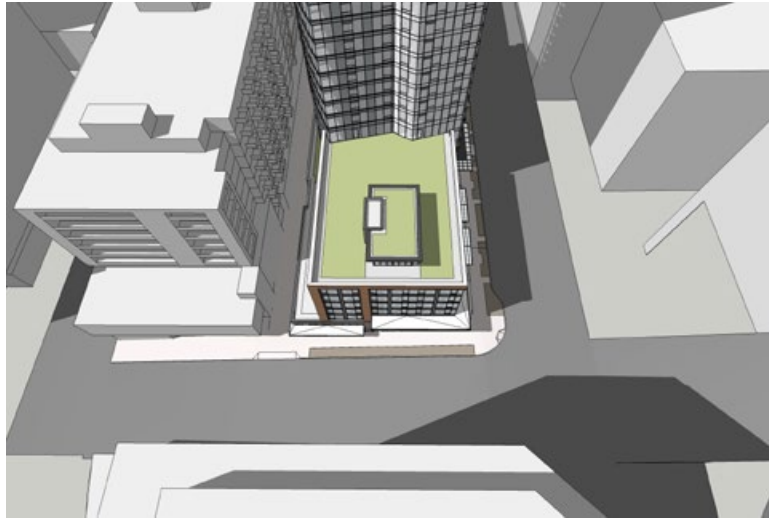
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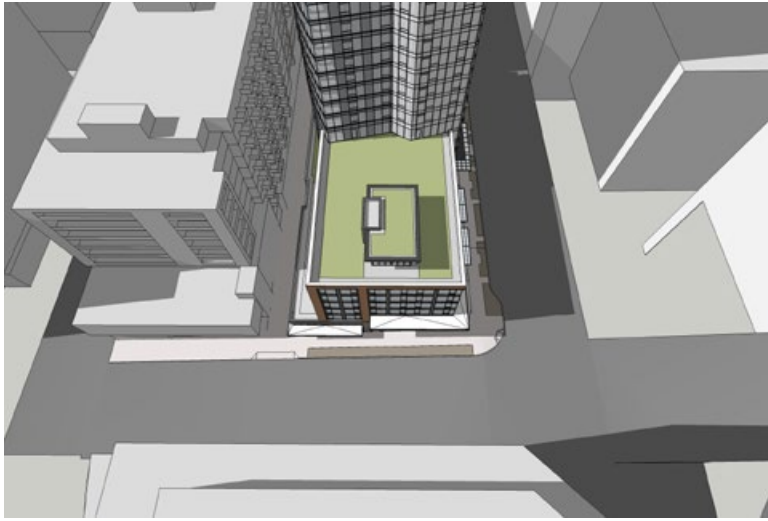
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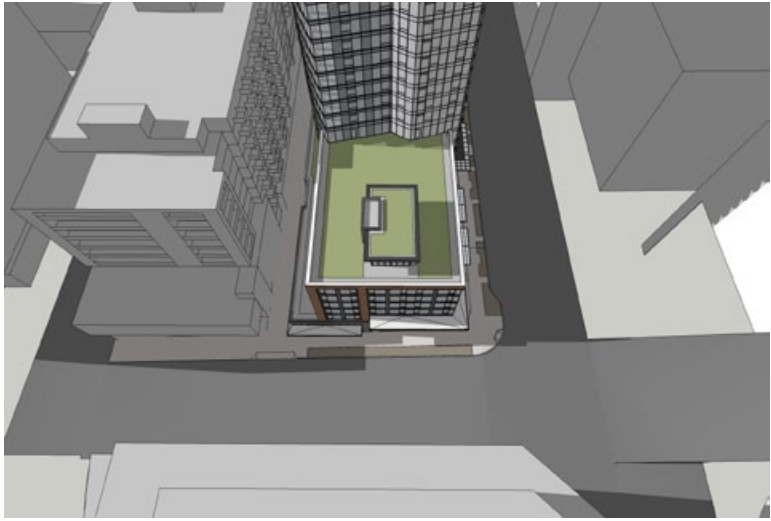
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3 pm



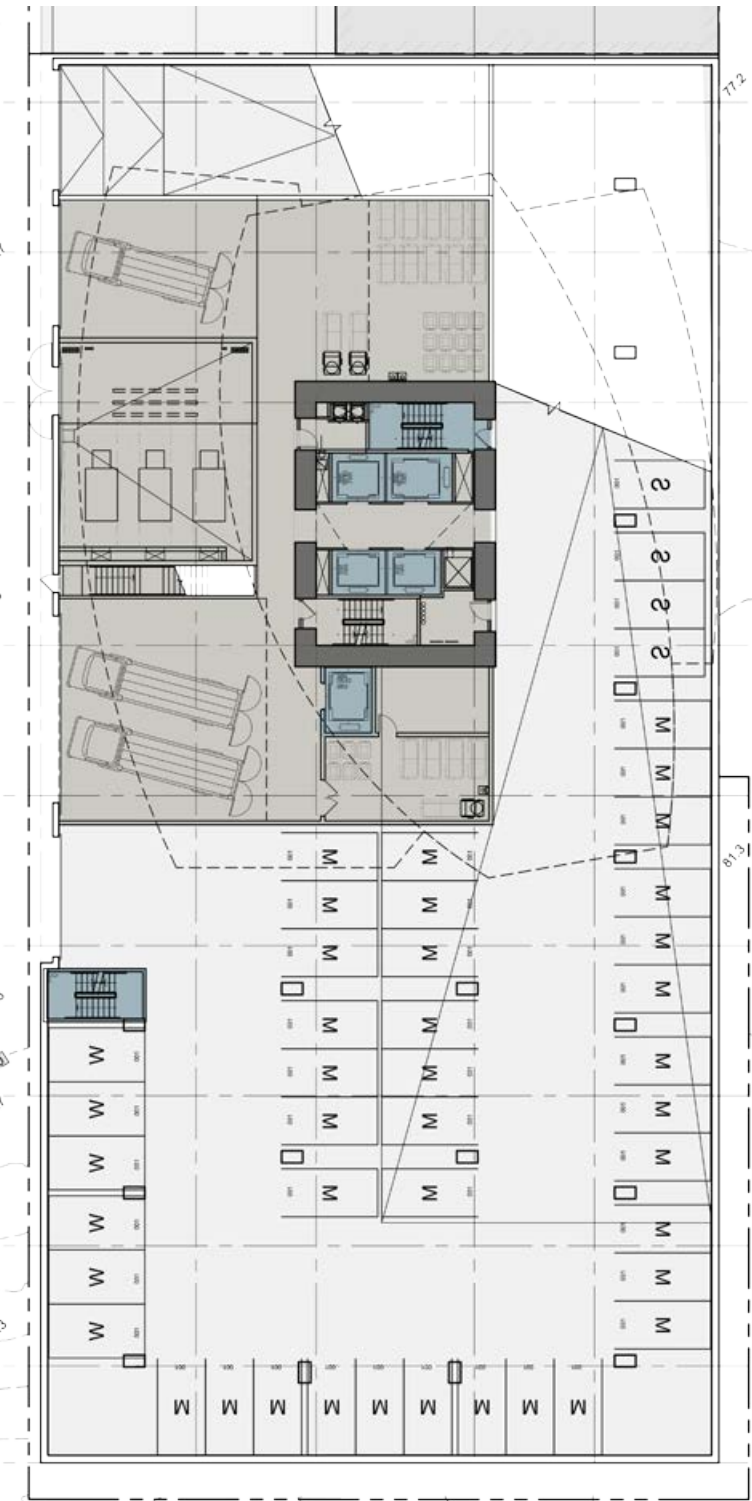
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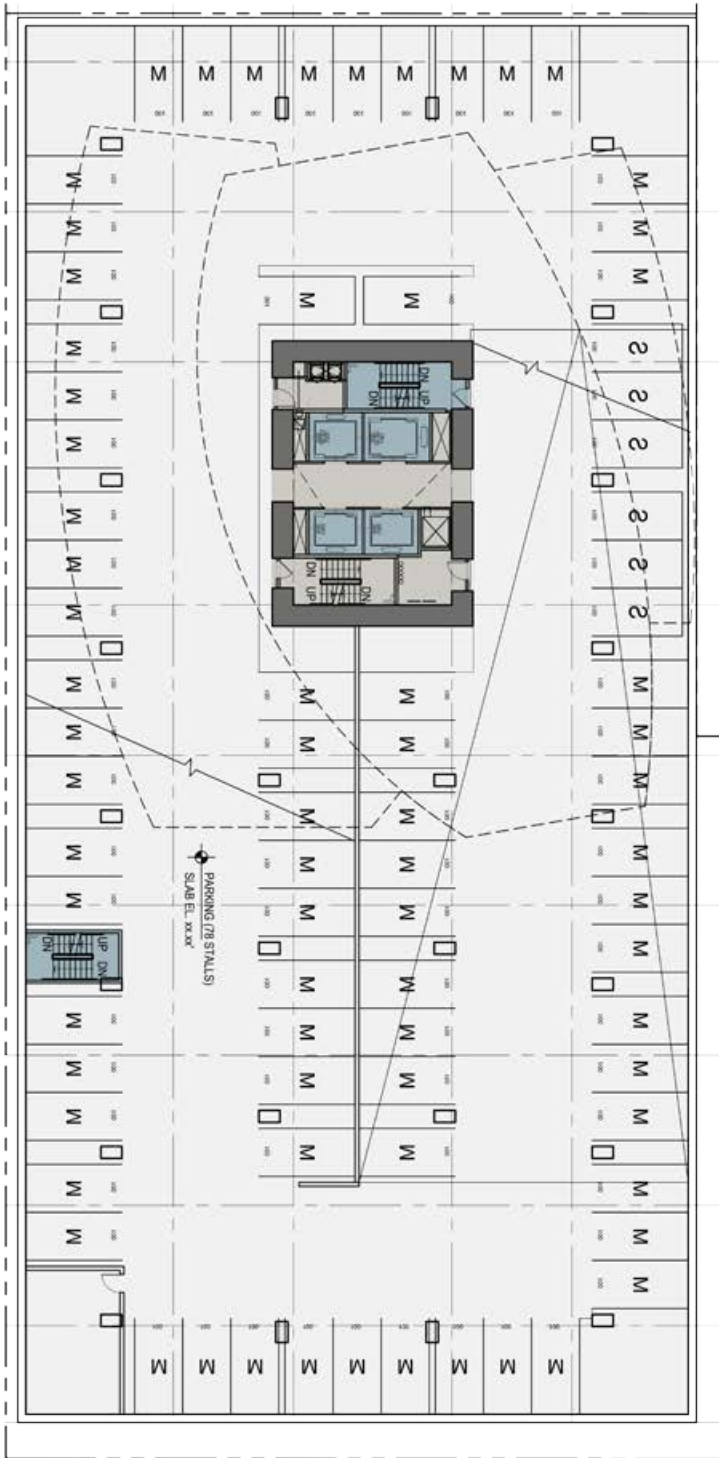
5 pm



# PARKING LEVEL PLANS



P2 FLOOR PLAN  
1/32" = 1'-0"



P3-P5 FLOOR PLAN  
1/32" = 1'-0"



P6 FLOOR PLAN  
1/32" = 1'-0"

## SPACE FUNCTION KEY

- RESIDENTIAL
- BALCONY
- COMMON SPACE
- STAIR / ELEVATOR CORE
- RETAIL
- PARKING
- AMENITIES
- BOH / LOADING / MECH

