



BLOCK 21 Full Alley Vacation Proposal

DRB Recommendation
September 8, 2015

2200 7th Ave
DPD #3018578



PROJECT INFO

PROPERTY ADDRESS & DPD PROJECT NUMBER

2200 7th Avenue, DPD # 3018578

OWNER

Acorn Development

ARCHITECT

Graphite Design Group

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Seattle, WA 98101

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DEVELOPMENT OBJECTIVES

Develop a commercial project with approximately 834,430 gsf of office space and approximately 25,990 gsf of street level retail in two buildings. Parking below grade will be provided for approximately 877 Cars.

TABLE OF CONTENTS

DEVELOPMENT OBJECTIVES

A-4 Development Objectives

URBAN DESIGN ANALYSIS

A-6 Urban Context Plan
A-7 Urban Context Aerial
A-8 Zoning Designation

EDG #2 RESPONSE

A-10-1 EDG 2 Guidance
A-12-13 Plan Comparison
A-14 Proposed Level G Plan
A-15 Proposed Level 1 Plan
A-16-17 Facade Development
A-18-27 Tower & Massing
A-28-31 Treatment Along Bell Street
A-32-43 Relationship to the Street
A-44-53 Open Space

GENERAL INFO

A-56-61 Floor Plans
A-62-65 Elevations
A-66-67 Sections
A-68-73 Enlarged Elevations
A-74 Material Palette
A-76-79 Tower Curtain Wall
A-80-81 Signage
A-82 Canopy Menu
A-83-85 Soffit Element

LANDSCAPE

A-87 Building Entries
A-88 Paving Diagram
A-89 Seating Diagram
A-90 Planting Diagram
A-91 Planting Diagram
A-92-93 Bell Street Concept Plan

LIGHTING

A-96-97 Lighting Concept
A-98-99 Lighting Site Plan
A-100-101 Primary Pathways (Hardscape)
A-102 Secondary Pathways (Landscape)
A-103 Central Open Space
A-104 Canopies & Entries
A-105 Level 8 Terrace
A-106 Soffit Element

DEPARTURES

A-108-114 Departures
A-115-117 Departures Summary

APPENDIX

A-119 Existing Site Plan
A-120-121 Urban Design Analysis
A-122-123 Code Analysis
A-124 Zoning Envelope
A-125 Existing Site
A-126 Shadow Analysis

DEVELOPMENT OBJECTIVES



DEVELOPMENT OBJECTIVES



The applicant proposes to design and construct a development on the full block bisected by a public alleyway and bounded by 7th Avenue on the west, 8th Avenue on the east, Blanchard Street on the south and Bell Street on the north. Bell and Blanchard Streets are city-designated Green Streets. The site is zoned DMC 340/290-400, with a site area of approximately 77,700 square feet plus a public alleyway of approximately 5,700 square feet. The site has a base FAR of 5 with a maximum of 10.

This application is for a commercial project with approximately 834,430 gsf of office space and approximately 25,990 gsf of street level retail in two buildings. The north building is 24 levels, the south building 8 levels, with an aerial connection at levels 3 & 4. An open space and through block connection are proposed connecting 7th and 8th Avenues. Approximately 877 parking stalls will be provided below grade in 4 parking levels. All building services will be located below grade, with primary access from 8th Avenue and a secondary parking exit from Bell Street. This proposal assumes a full alley vacation.

PROJECT GOALS

- Create rich, diverse pedestrian environment with a variety of scales, active uses and character of open spaces
- Connect to and enhance existing neighborhood pedestrian, vehicular, transit and cycling circulation patterns
- Create transitional opportunity between the Denny Triangle and South Lake Union
- Respond and contribute to the established urban density pattern in a thoughtful manner
- Create flexible, active open space and retail opportunities that add vitality to the project site as well as surrounding community
- Extend northward enhancements of 7th Avenue landscaping and cycle track in pattern established by Rufus 2.0
- Maximize utilization of Green Streets at Blanchard and Bell Streets
- Locate all parking and services below grade
- Develop project utilizing sustainable design methodologies and connection to existing community sustainability initiatives such as District Energy
- Maximize development potential

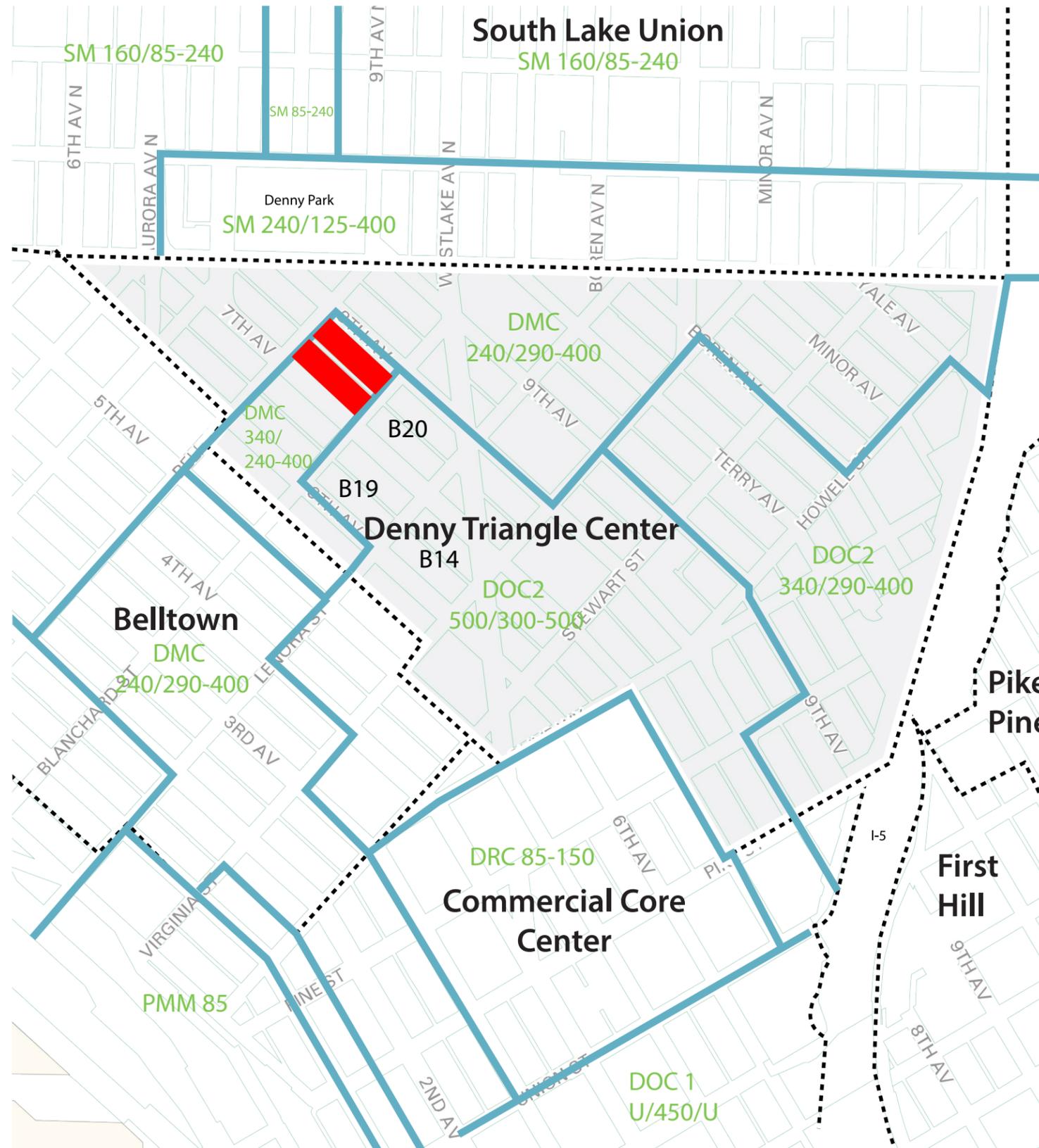
URBAN DESIGN ANALYSIS





 Under Construction
 Future Development

URBAN CONTEXT - ZONING DESIGNATION



 Project Site

EDG #2 RESPONSE





A-10 | Rendering looking North East

Block 21 - Full Alley Vacation

DPD # 3018578

DRB Recommendation

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1. Tower & Massing (B1.1, B4.1,B4.2)

- Design & scale back the lower level zone to respect the Green Streets and public space

2. Treatment Along Bell Street (C1, C3, D1.1, D2.1.I, E2.1.I, E2.2)

- Design street level facades, open space & landscaping @ Bell St to the same level as the through-block
- Curb cut on Bell St should be exit only
- Design Bell St to minimize vehicle use

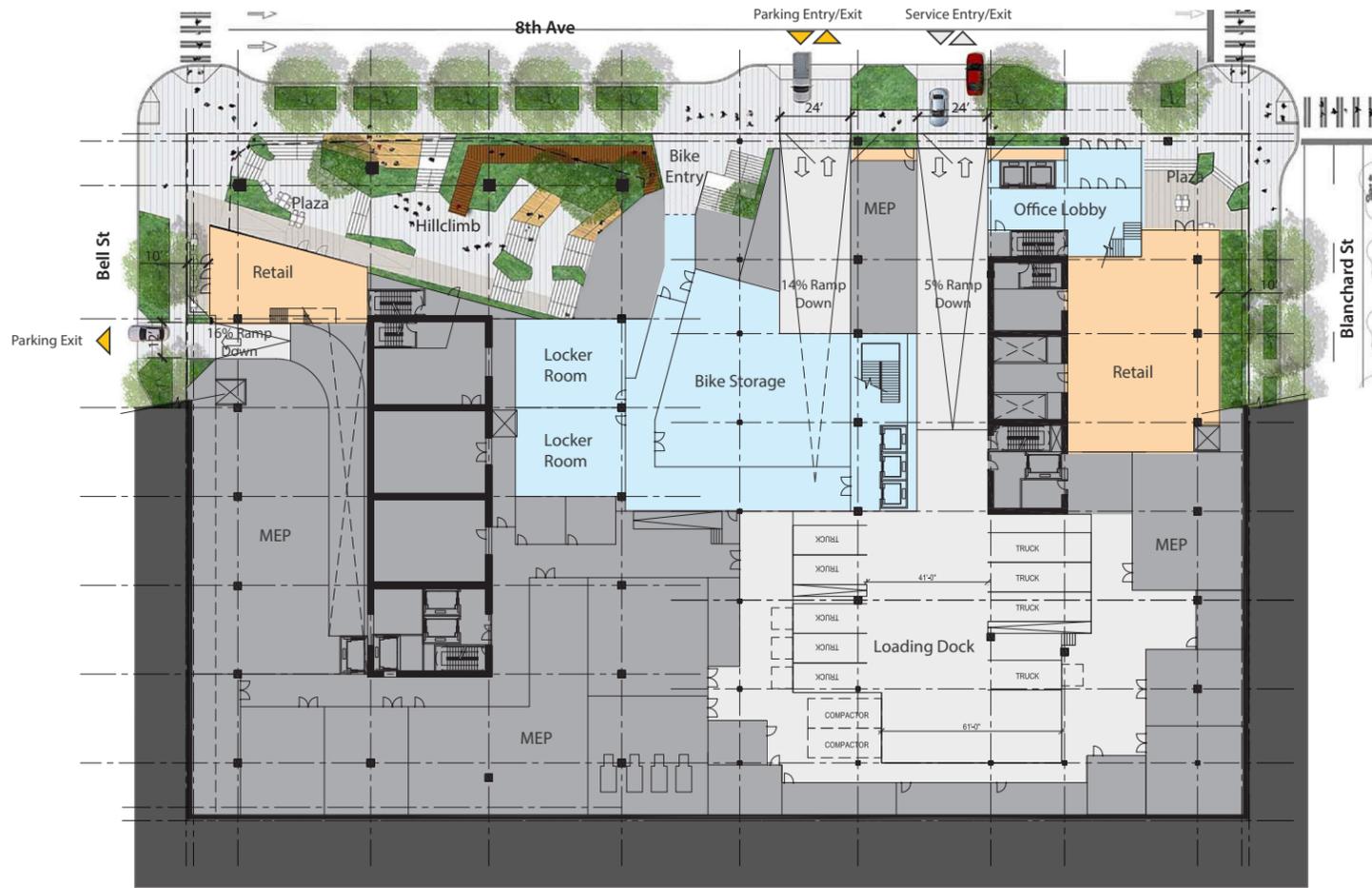
3. Relationship to the Street (C1, C3, D1)

- Design of public edge and central open space needs to be well executed
- Design street level facades, open space & landscaping @ green streets to the same level as the through-block
- Design all site corners with same level of attention and detail
- Provide more info about how ADA access works at 7th & Blanchard
- Design all open space & the edge of the right of way to be attractive to public

4. Open Space (C1, D1)

- Design of public edge and central open space needs to be well executed
- Design & program the open space along 7th & Blanchard for public use
- The open space at 8th & Bell needs to be programmed and designed to be considerate of public use and space. Scale back the lower zone of the building to respect this public space

Ground Floor Plan Comparison - 8th Ave

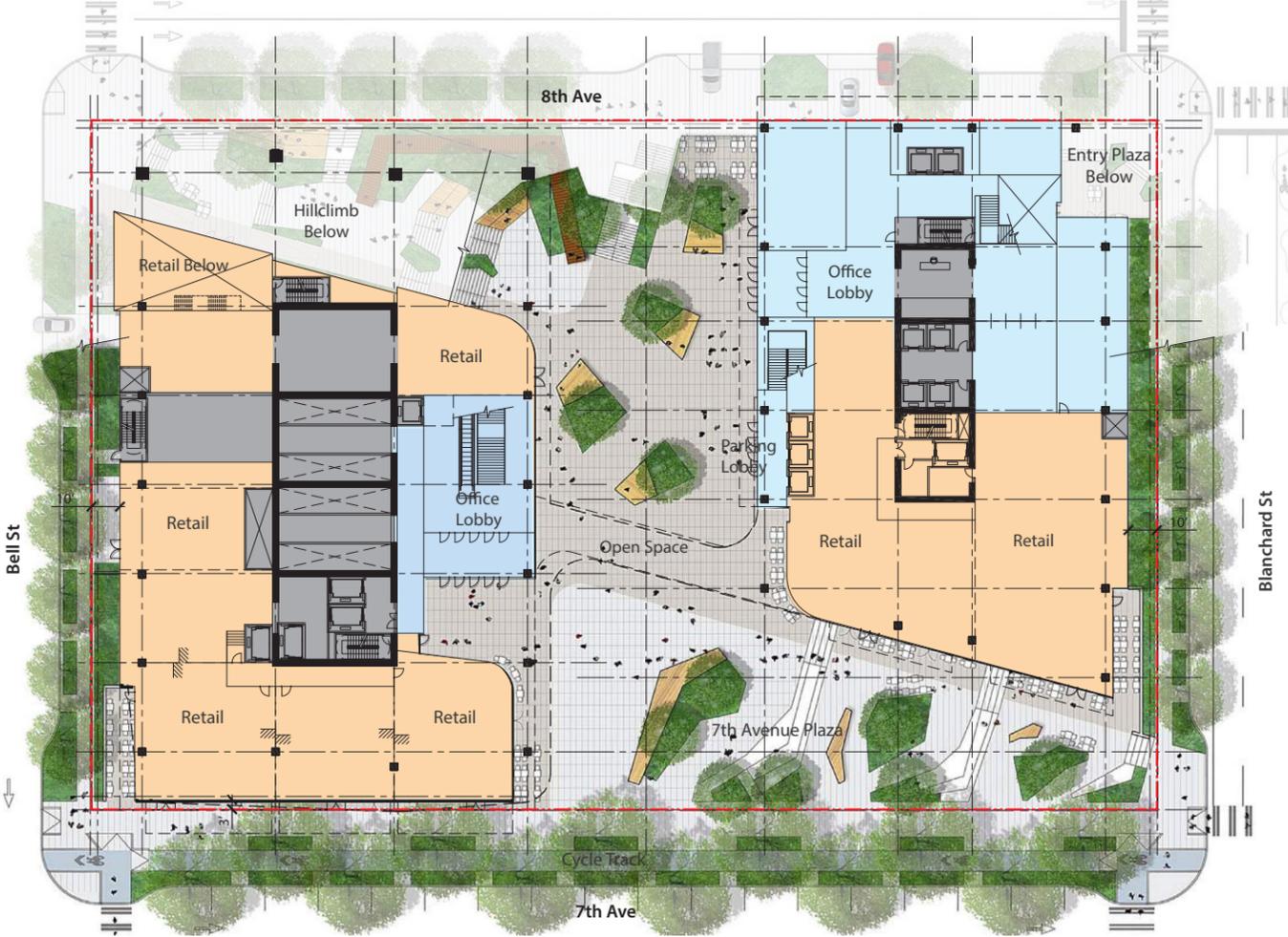


Previous Proposal (EDG#2)



Current Proposal

Ground Floor Plan Comparison - 7th Ave

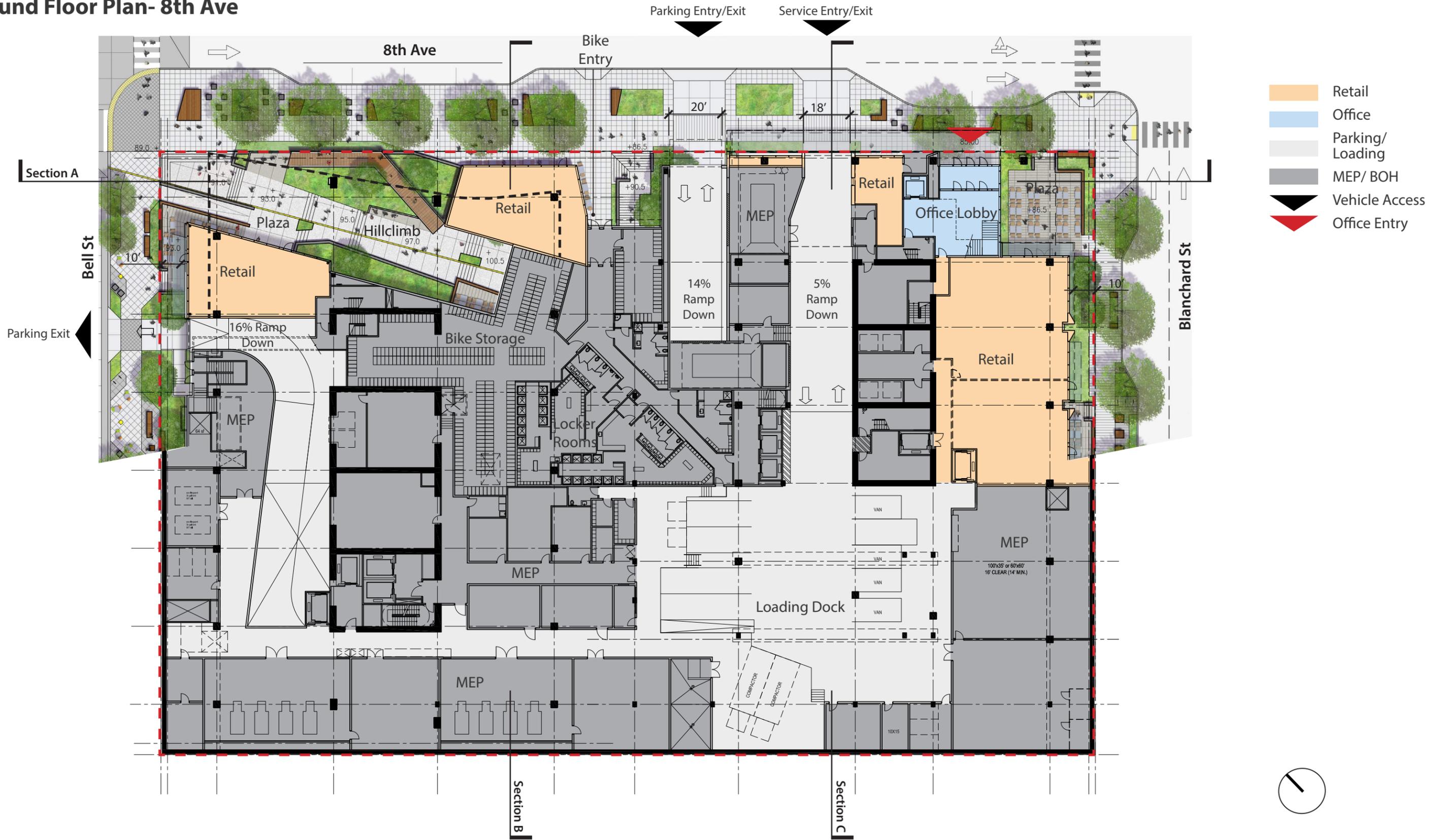


Previous Proposal (EDG#2)



Current Proposal

Ground Floor Plan- 8th Ave



Ground Floor Plan- 7th Ave



- Retail
- Office
- MEP/ BOH
- Office Entry

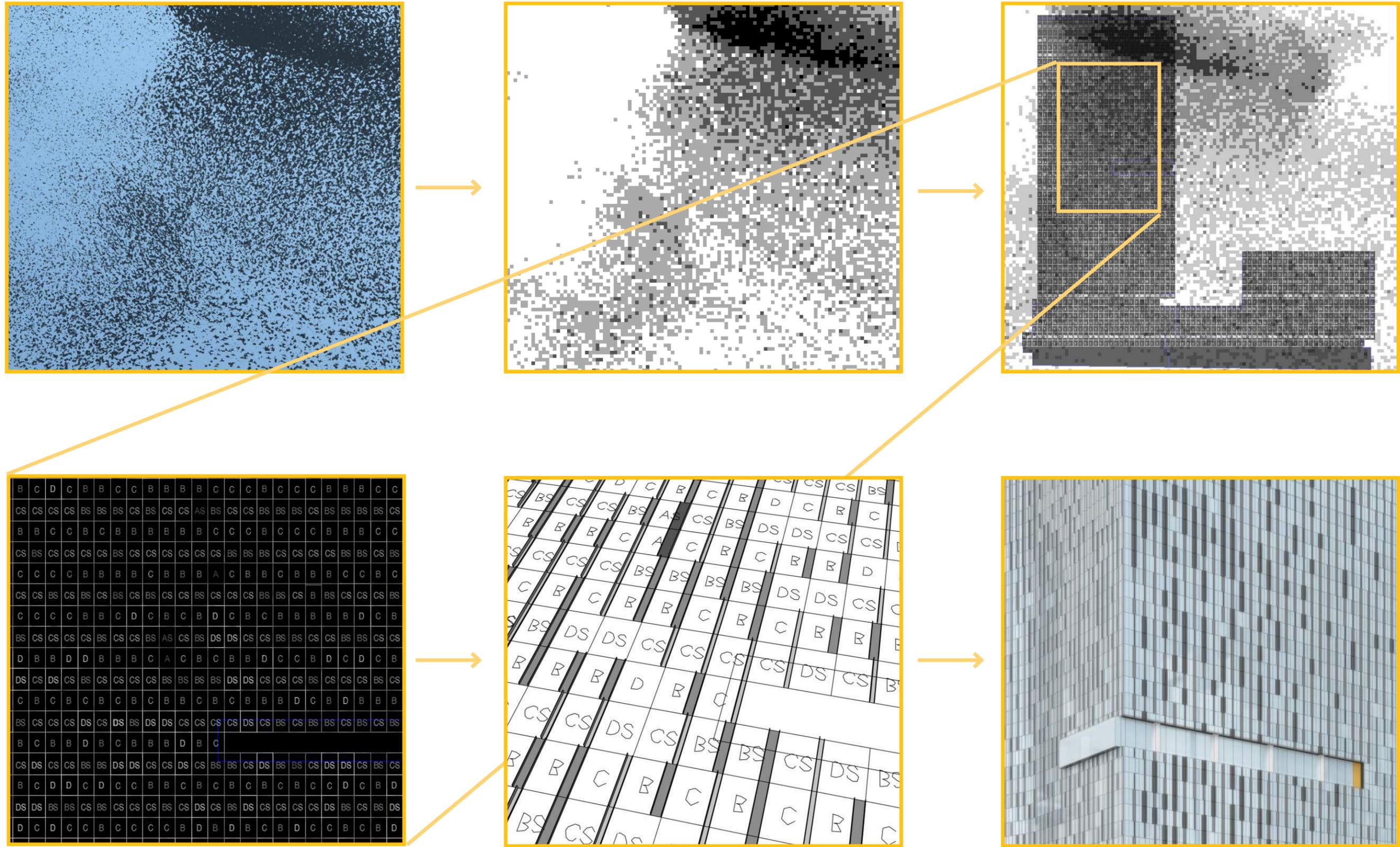


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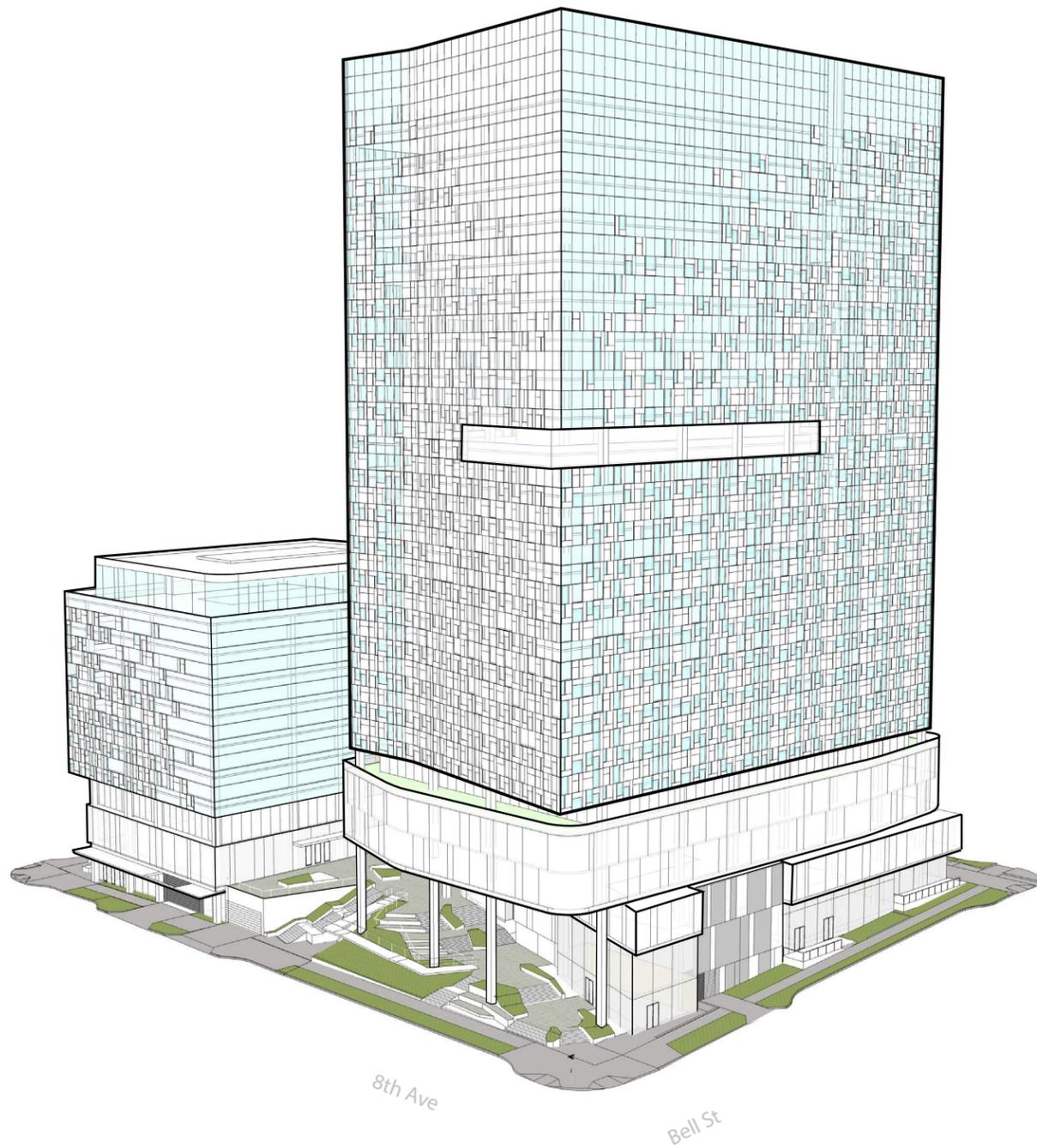
/ˌmɜrməˈrɑːʃən/

noun literary

1. the action of murmuring.
"the murmuration of a flock of warblers"
2. *rare*
a flock of starlings.



1. Tower & Massing

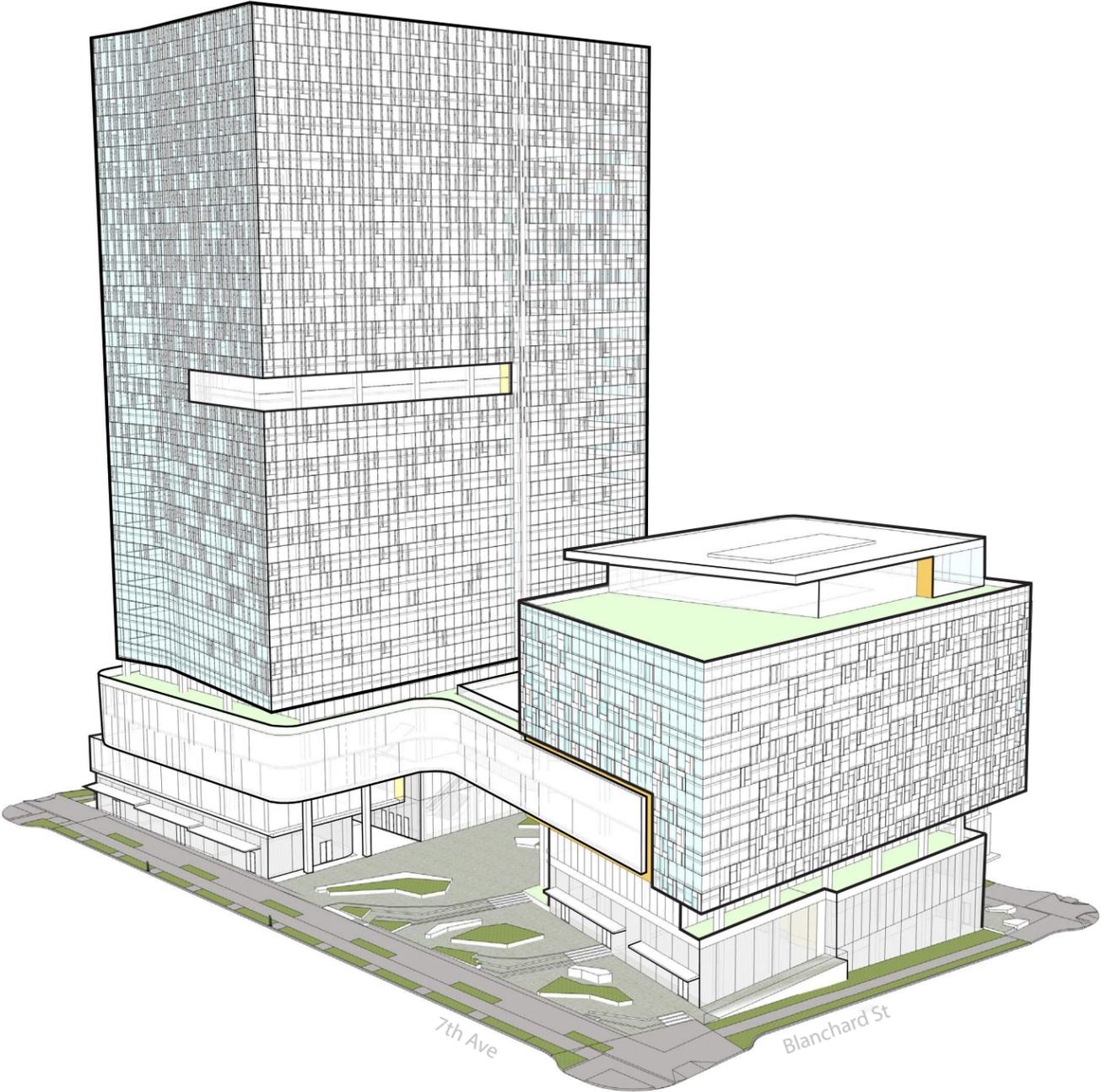


Previous Proposal (EDG#2)

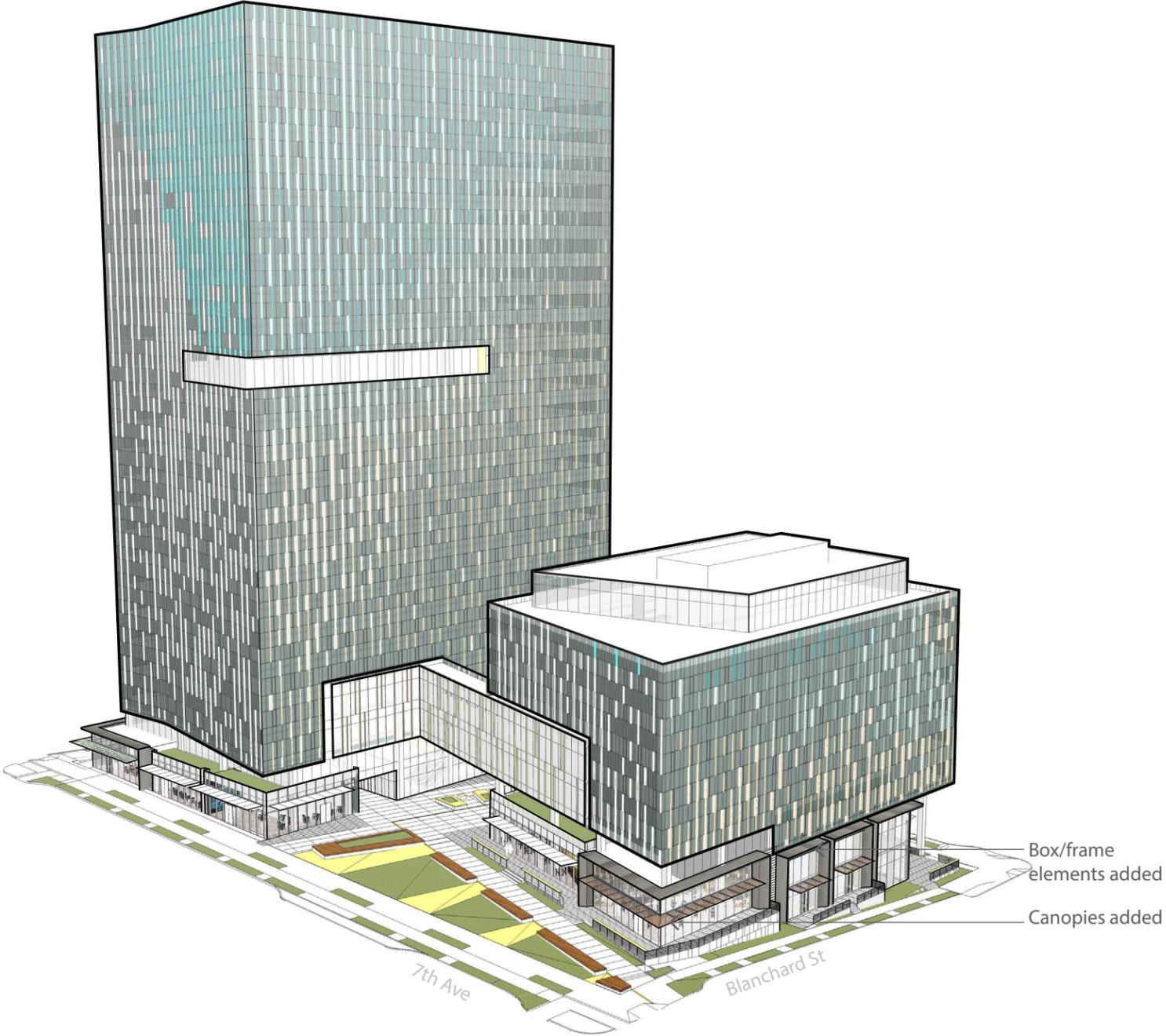


Current Proposal

- Removed protruding ribbon element
- Seating platforms added
- Canopies added
- Box/frame elements added
- Relocated Column
- Added Retail Space



Previous Proposal (EDG#2)



Current Proposal

1. Tower & Massing

The overall design has evolved in response to EDG feedback, Design Commission review, and public comment. Notable changes along the south and west facades:

- The south building has been reduced from 9 stories to 8.
- The 7th Avenue plaza has been redesigned to enhance accessibility, eliminating stairs and introducing programmable lawn and hardscape areas accessible from the ROW
- The link at levels 3 and 4 has been revised to decrease building overhangs at the plaza, provide accessible terraces at the podium, and improve the overall vertical proportions of the north tower massing.
- The retail frontage at Blanchard Street has been modulated to respond to the surrounding neighborhood context and provide pedestrian scale. Additional retail entries have been introduced mid-block with outdoor seating and landscape areas.



Previous Proposal (EDG#2)

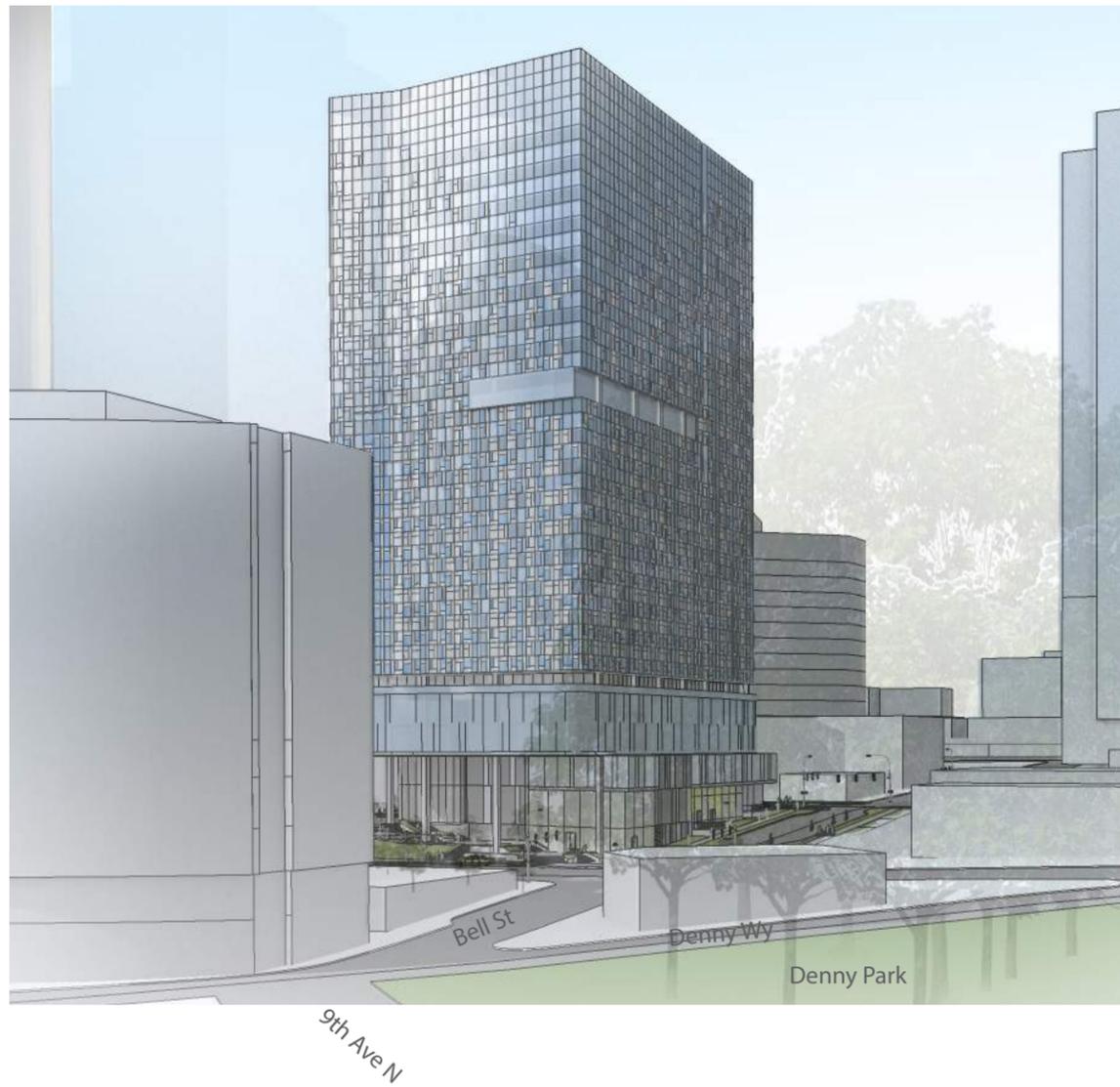


Current Proposal

1. Tower & Massing

The overall design has evolved in response to EDG feedback and public comment. Notable changes along the north and east facades:

- The north building has been revised from 23 to 24 levels, although the overall height is unchanged.
- 8th Avenue activation has been enhanced by the addition of a mid-block retail space adjacent to the cycle entry and hill climb stair.
- The 8th Avenue garage portals have been reduced and further separated, with a larger area of refuge in between.
- The former cantilevered podium element has been eliminated above the hill climb at 8th Avenue, allowing more daylight access to the pedestrian passage and improving the overall vertical proportions of the north tower massing.
- The retail frontage at Bell Street has been modulated to respond to the surrounding neighborhood context and provide pedestrian scale along the Green Street. Cantilevered overhead elements have also been removed and accessible outdoor terraces introduced at the NE corner of level 2.

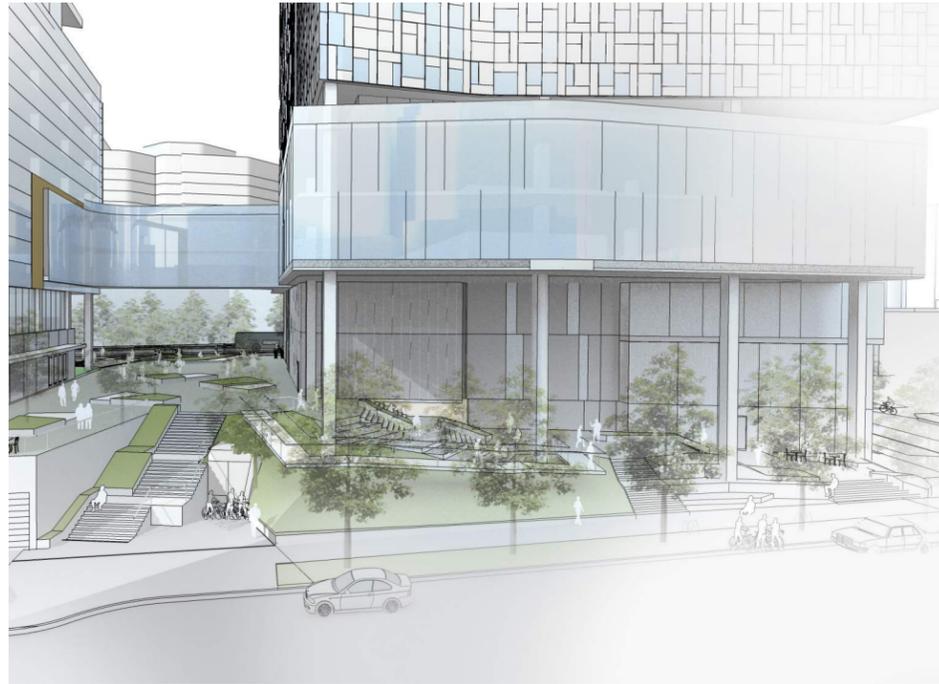


Previous Proposal (EDG#2)

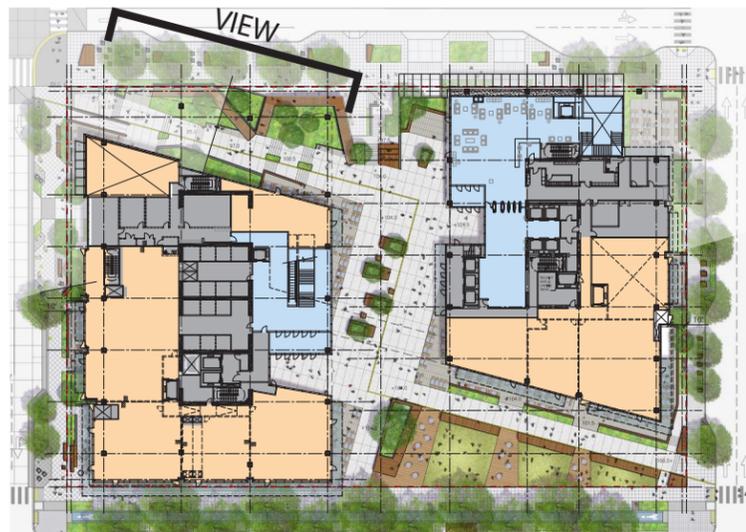


Current Proposal

1. Tower & Massing



Previous Proposal (EDG#2)



Current Proposal



Previous Proposal (EDG#2)



Current Proposal



1. Tower & Massing



Previous Proposal (EDG#2)



Current Proposal

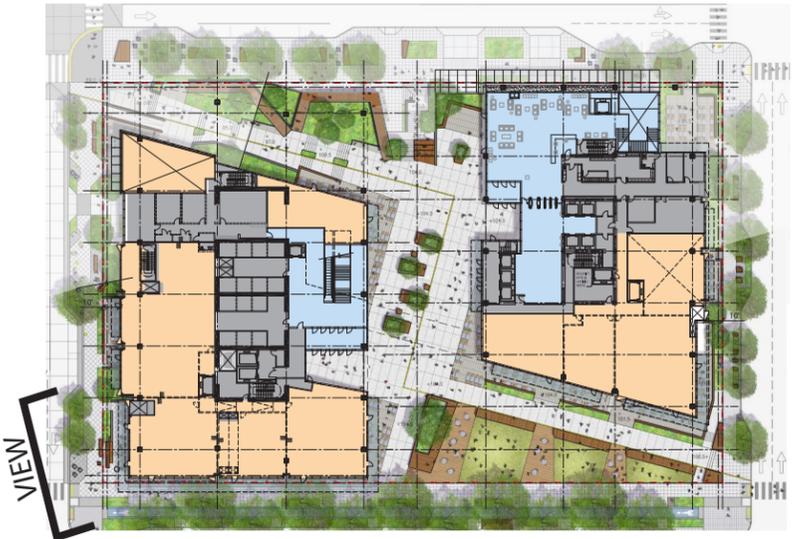




Previous Proposal (EDG#2)



Current Proposal



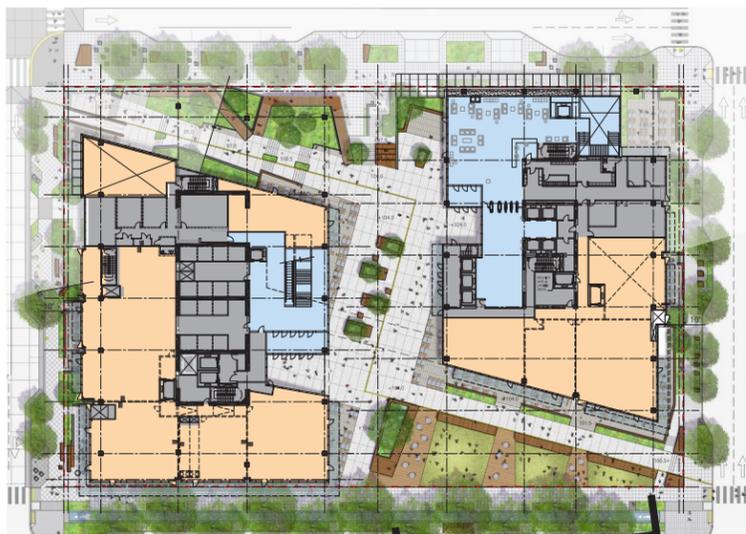
1. Tower & Massing



Previous Proposal (EDG#2)



Current Proposal



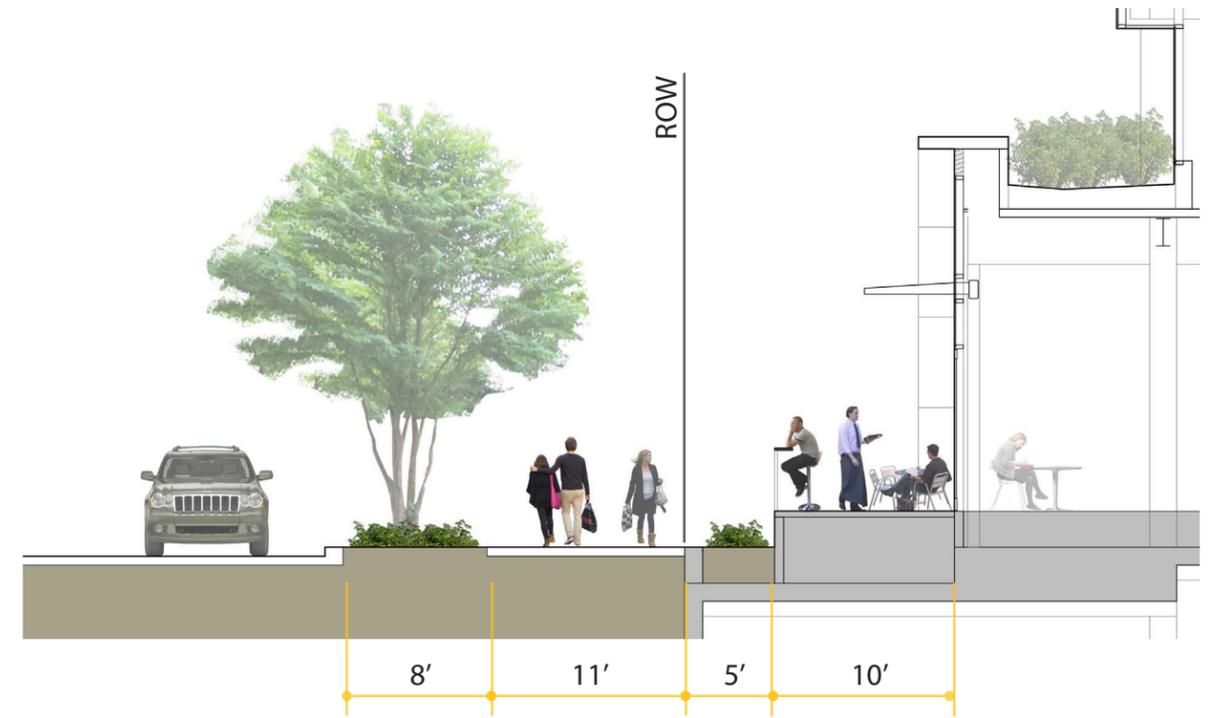
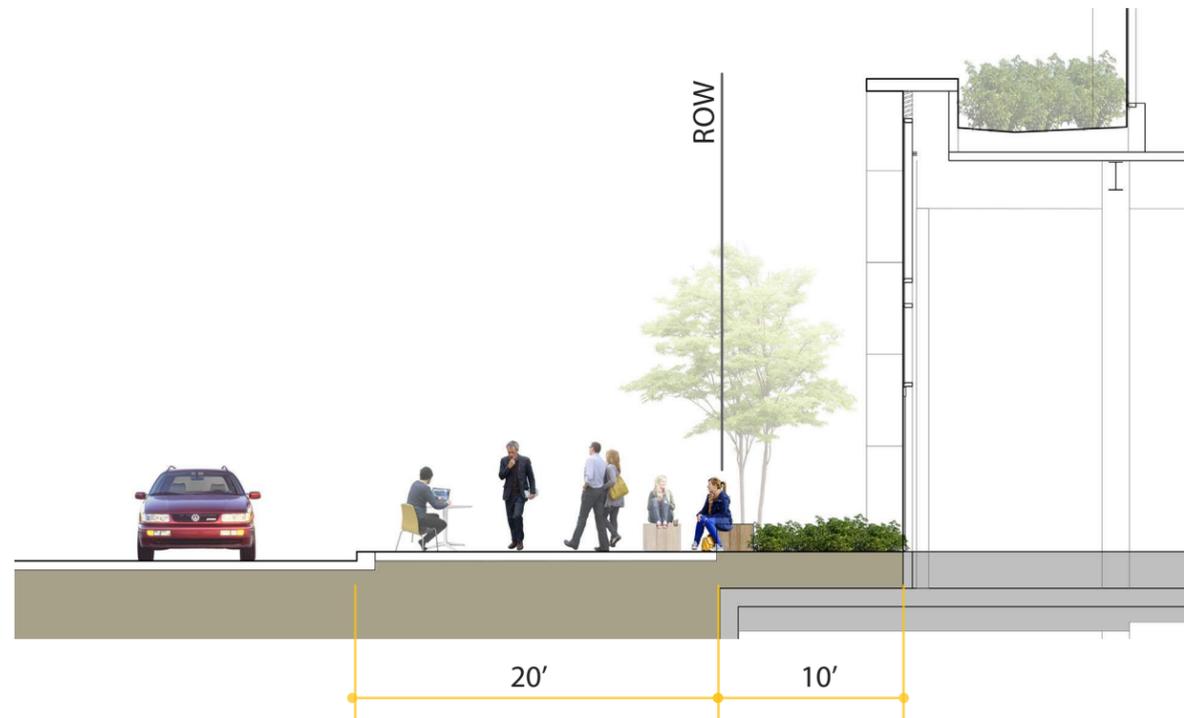
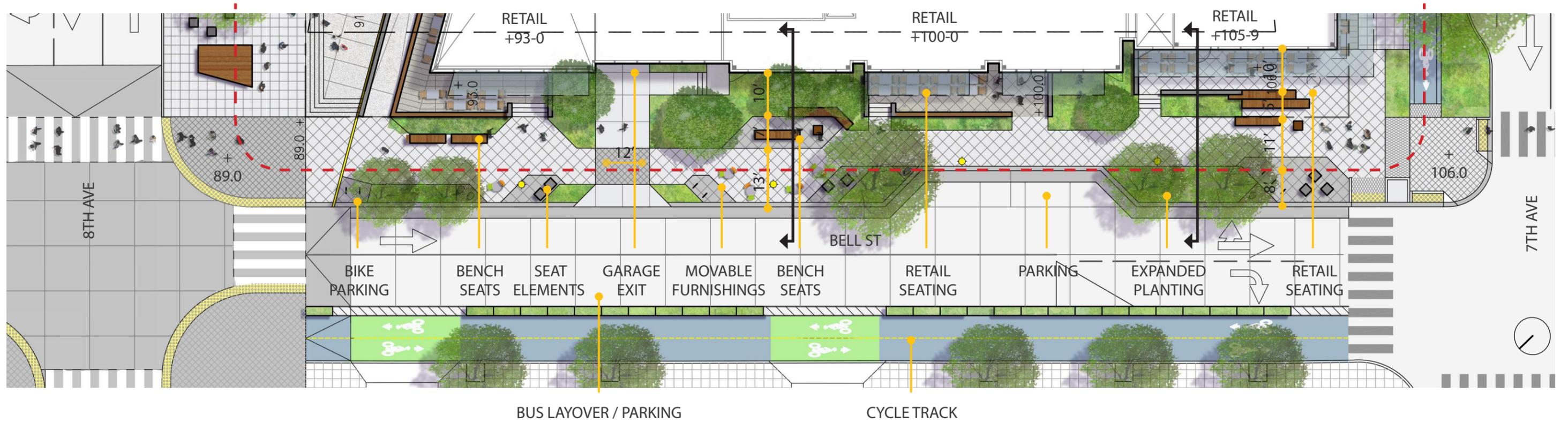


Previous Proposal (EDG#2)



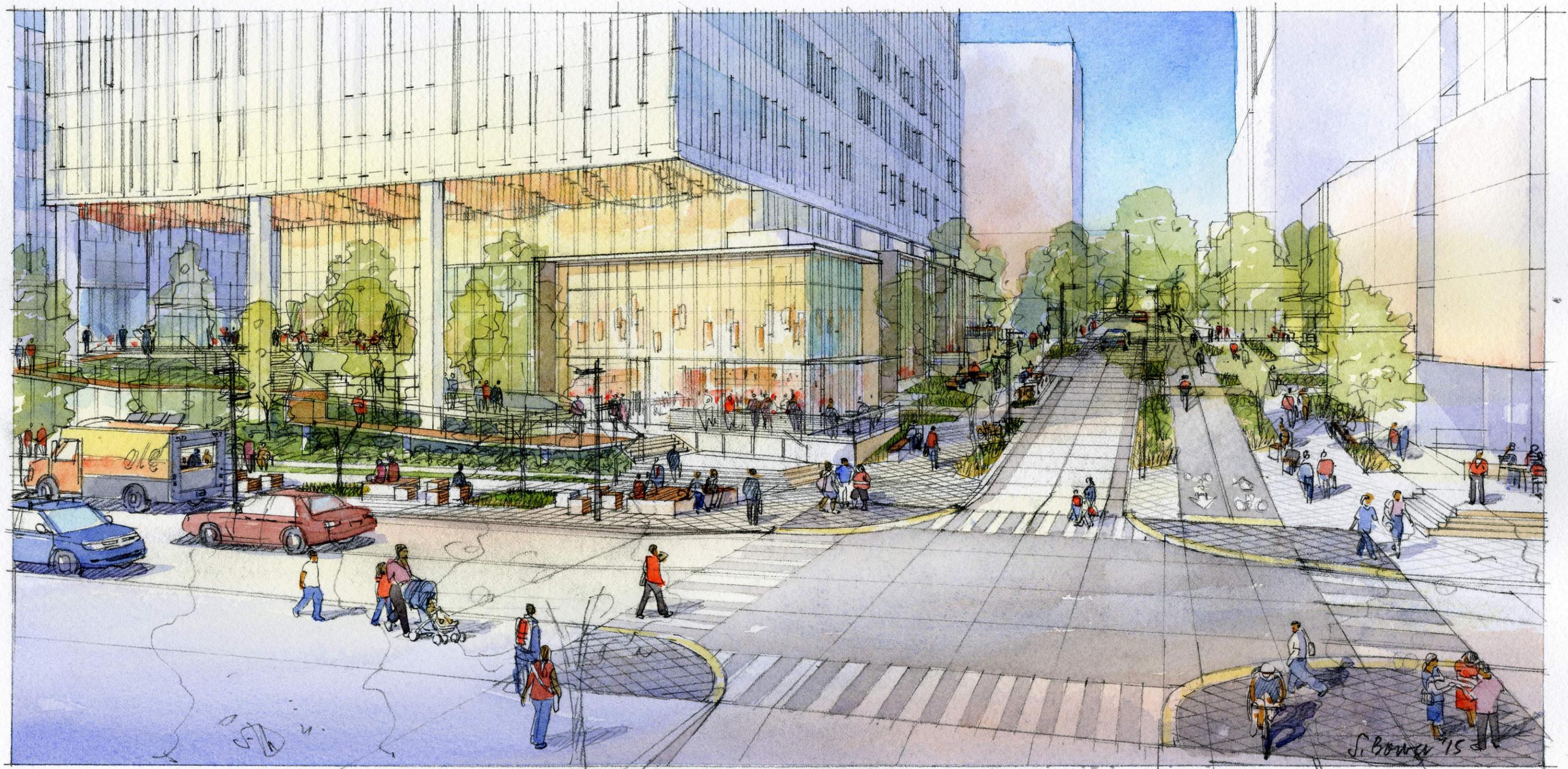
Current Proposal

2. Treatment Along Bell Street



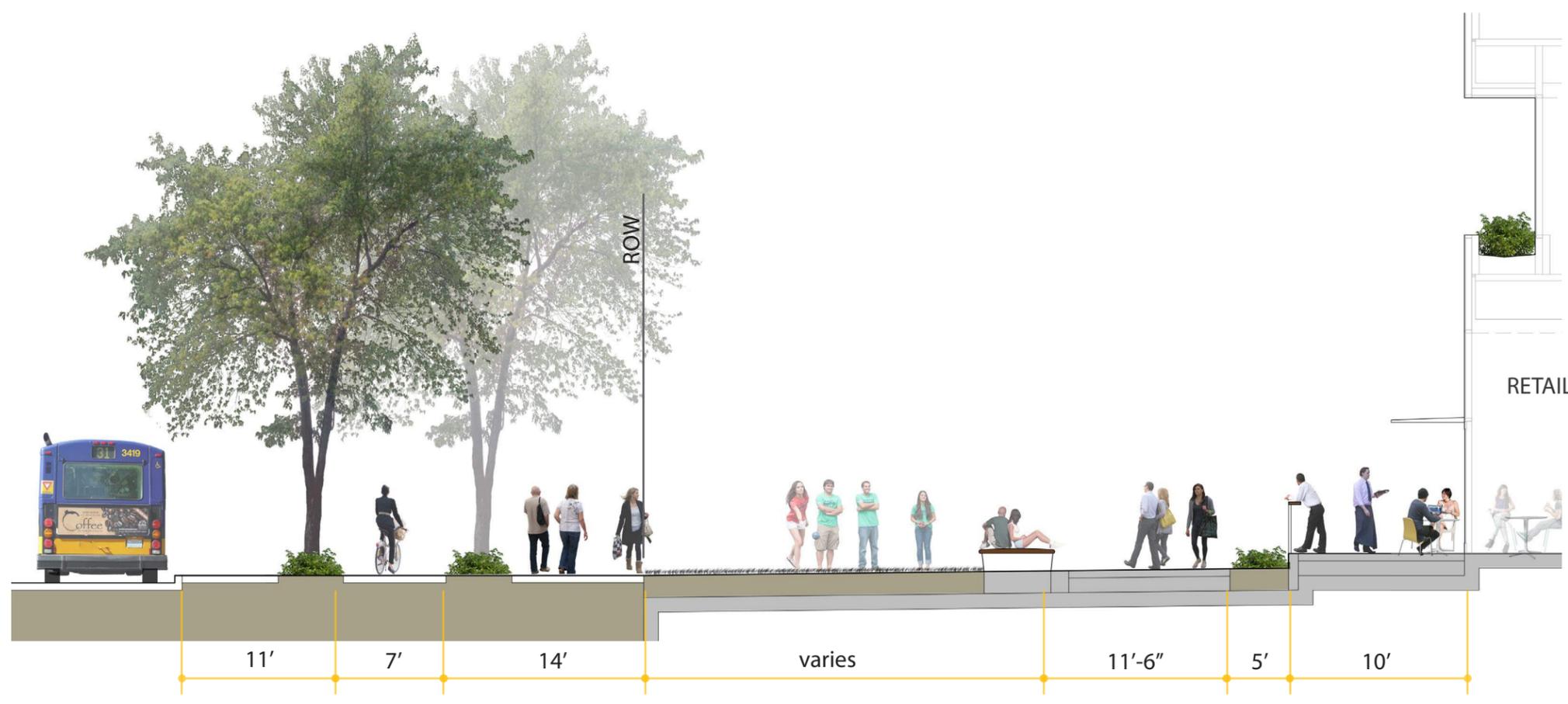
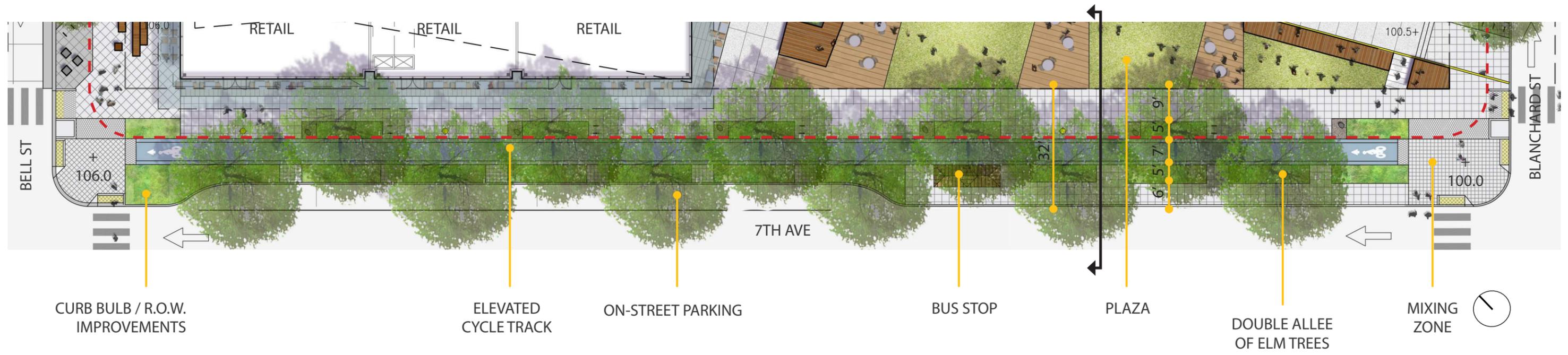


2. Treatment Along Bell Street





3. Relationship to the Street

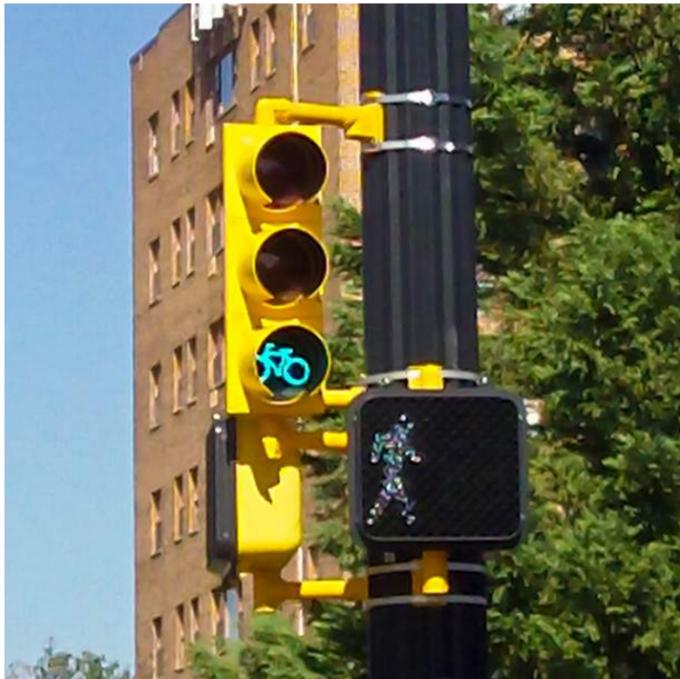
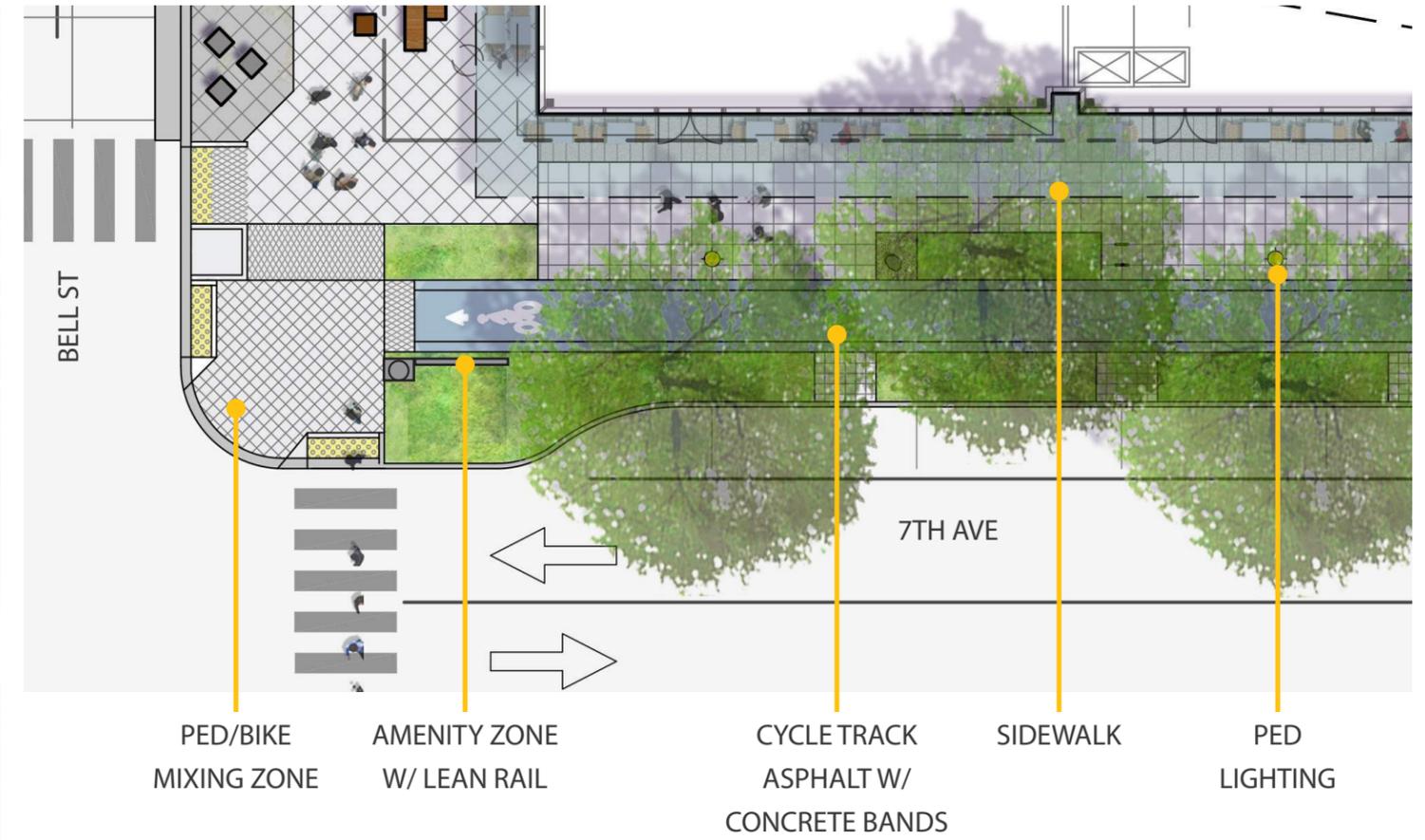




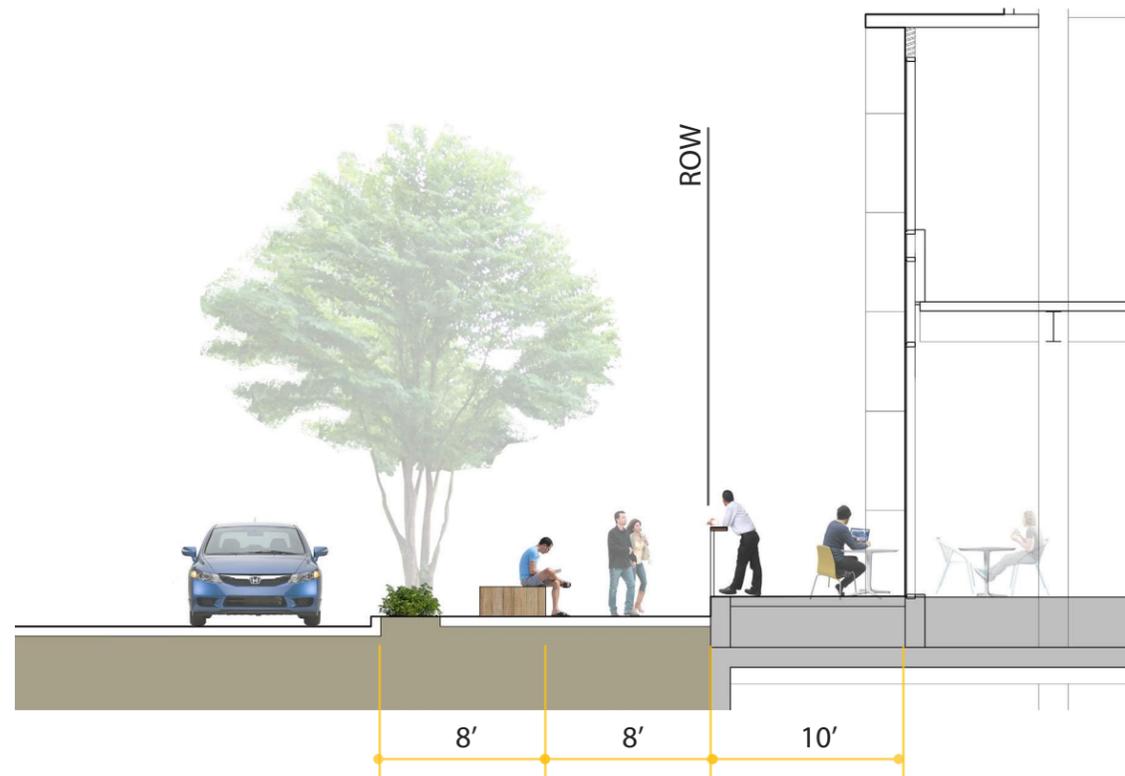
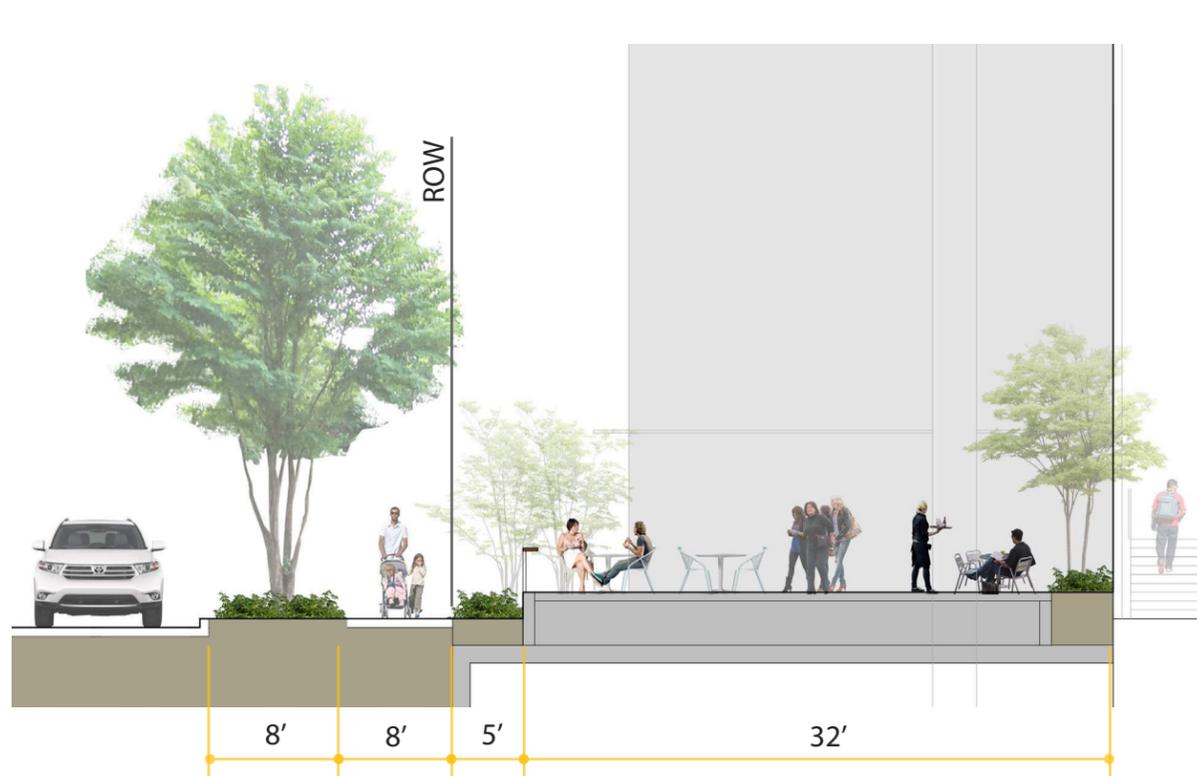
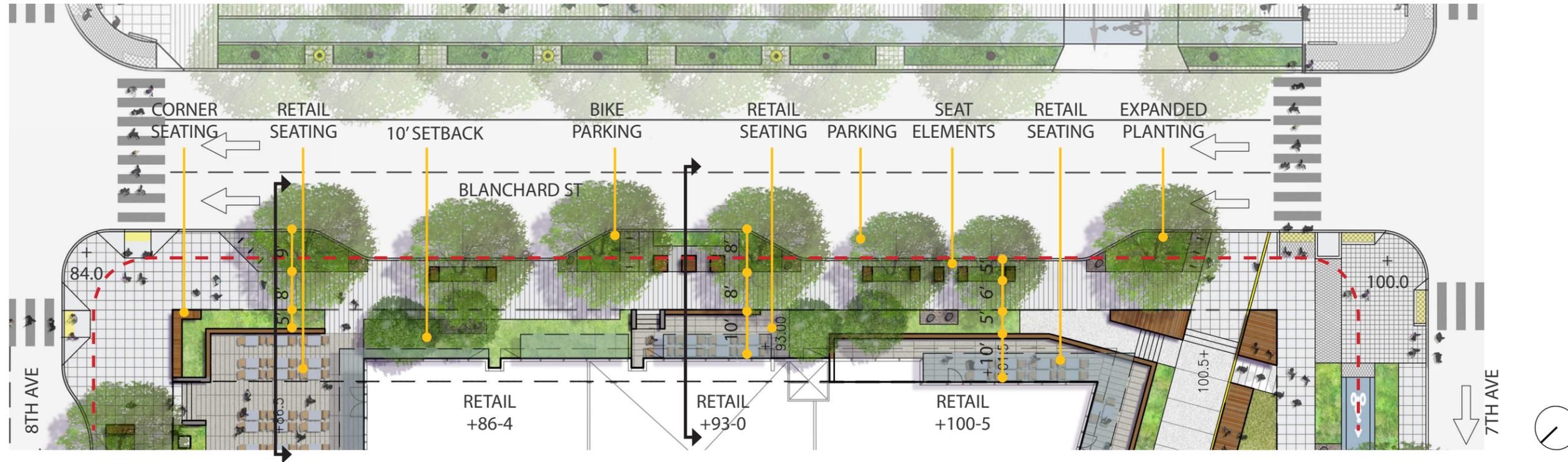
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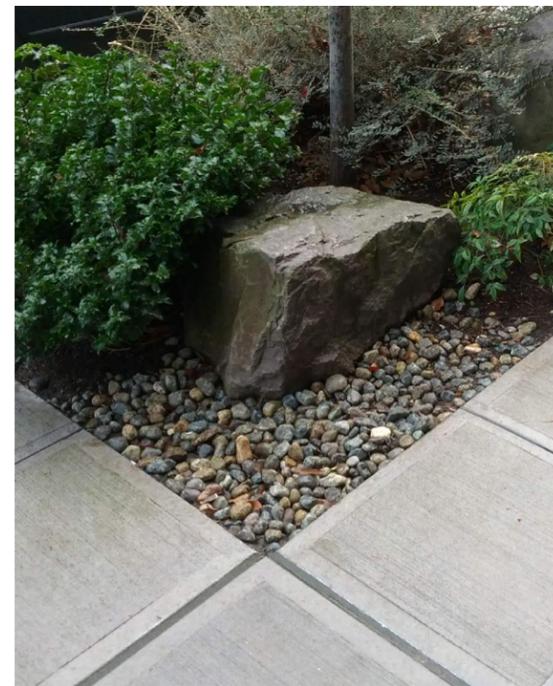
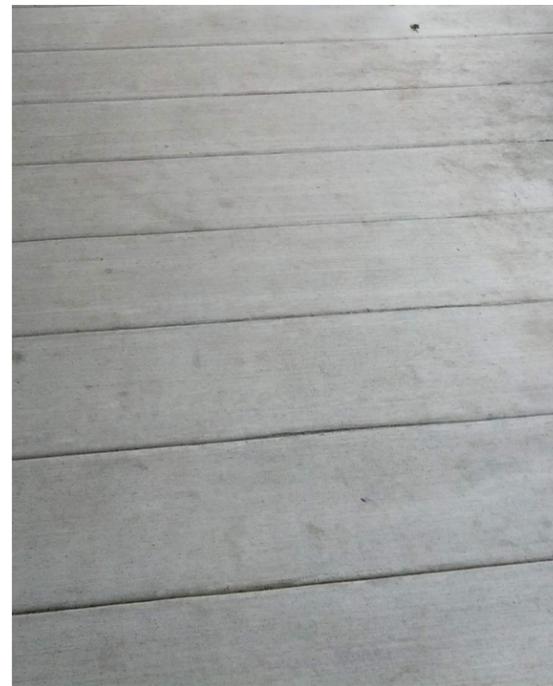


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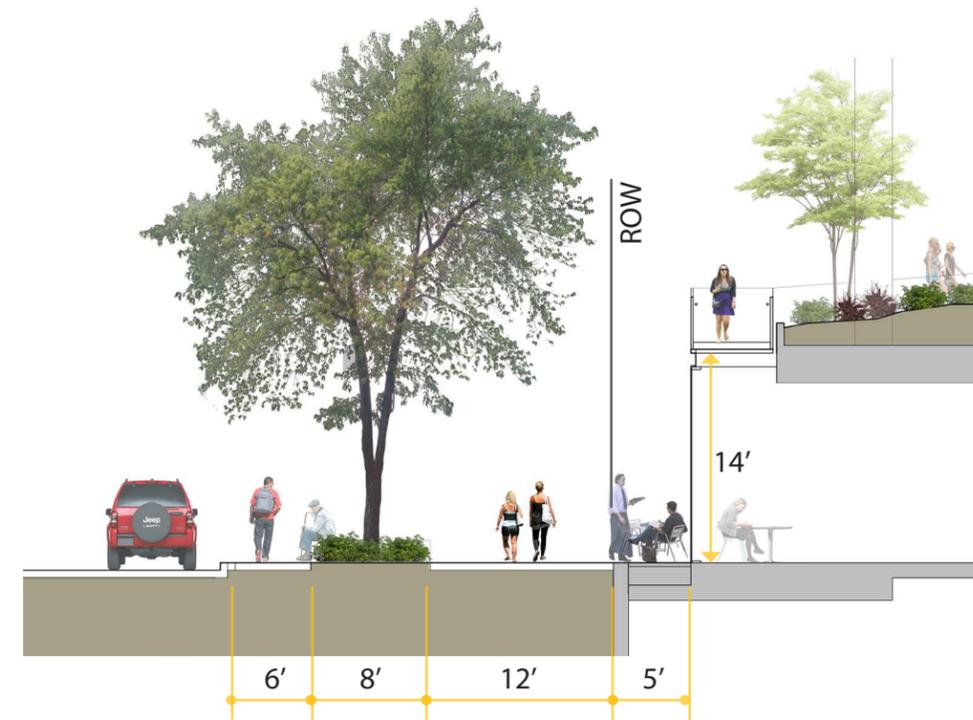
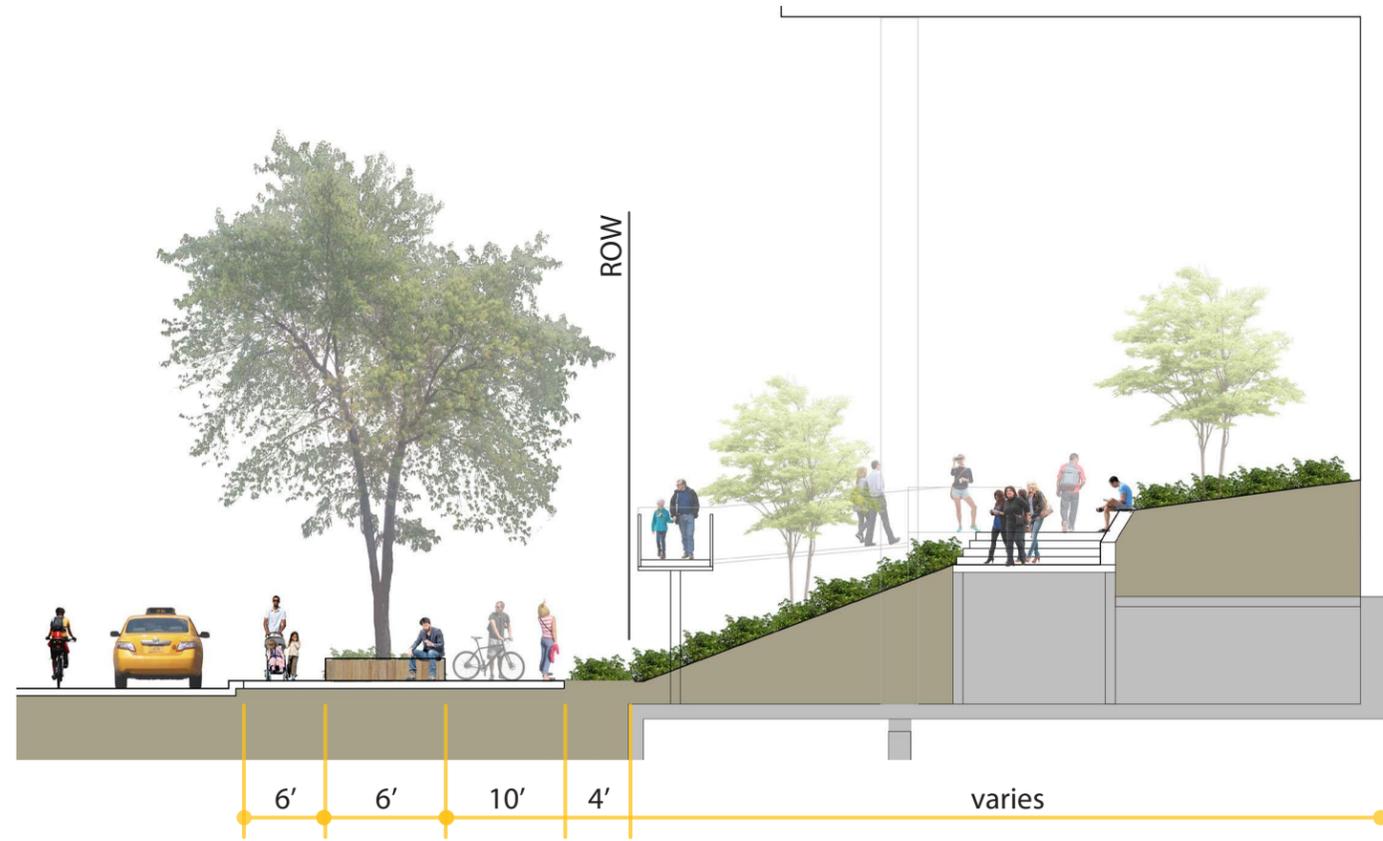
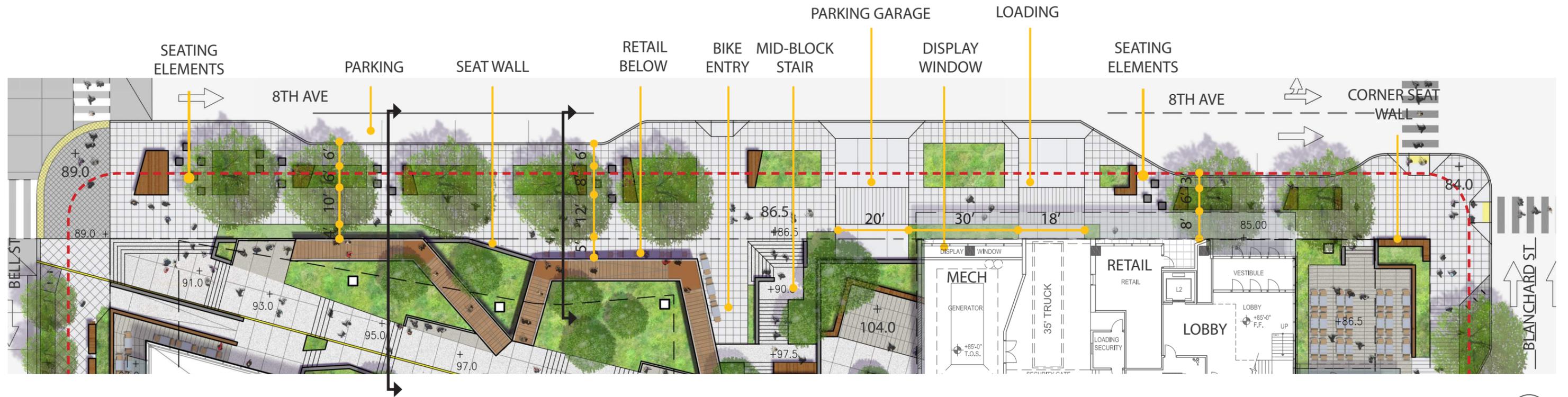




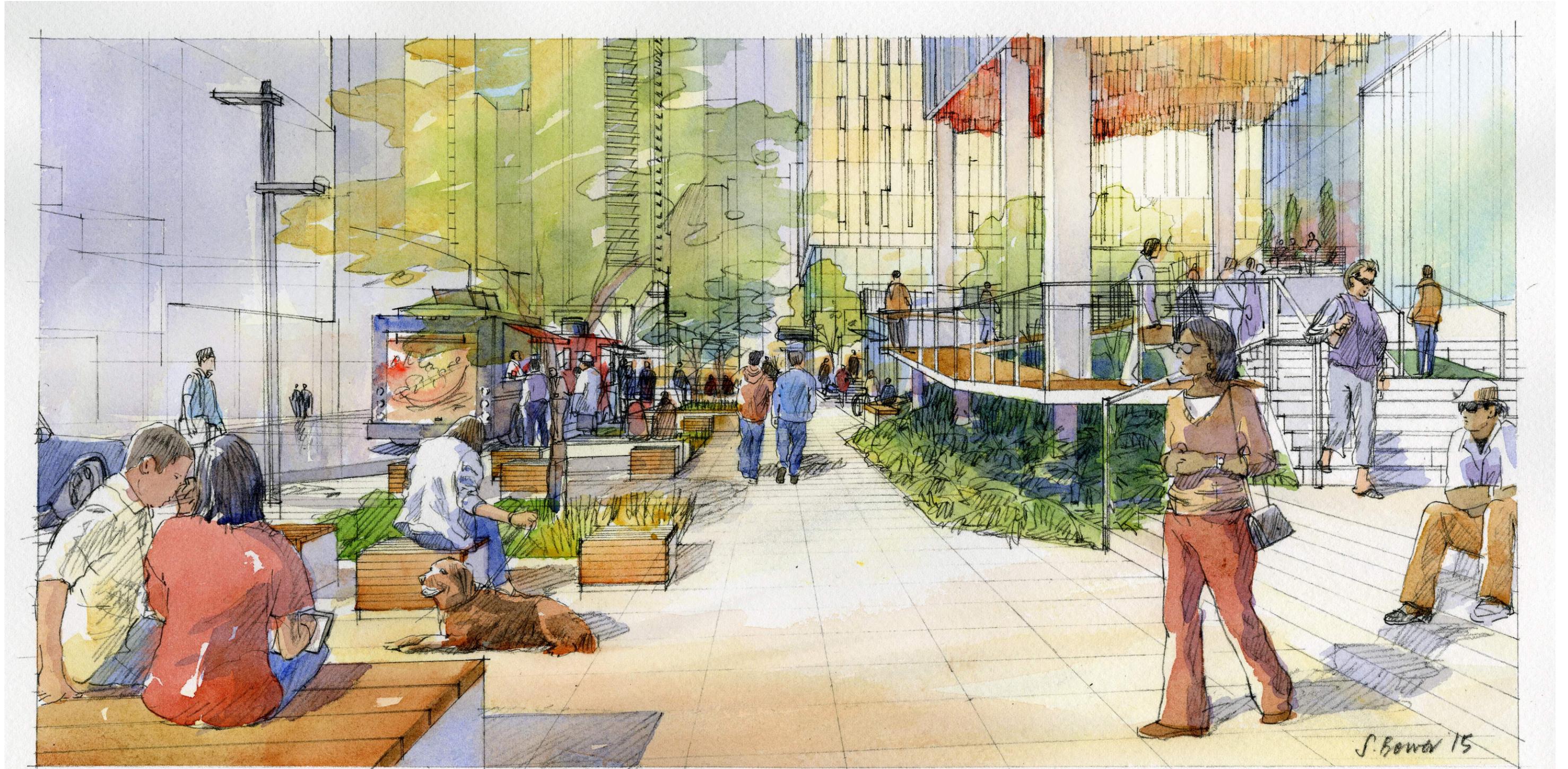
3. Relationship to the Street



3. Relationship to the Street



3. Relationship to the Street





OFFICE LOBBY RETAIL STOREFRONT (34') LOADING (18') DISPLAY WINDOW (32') PARKING (20') PLAZA STAIR / BIKE ENTRY

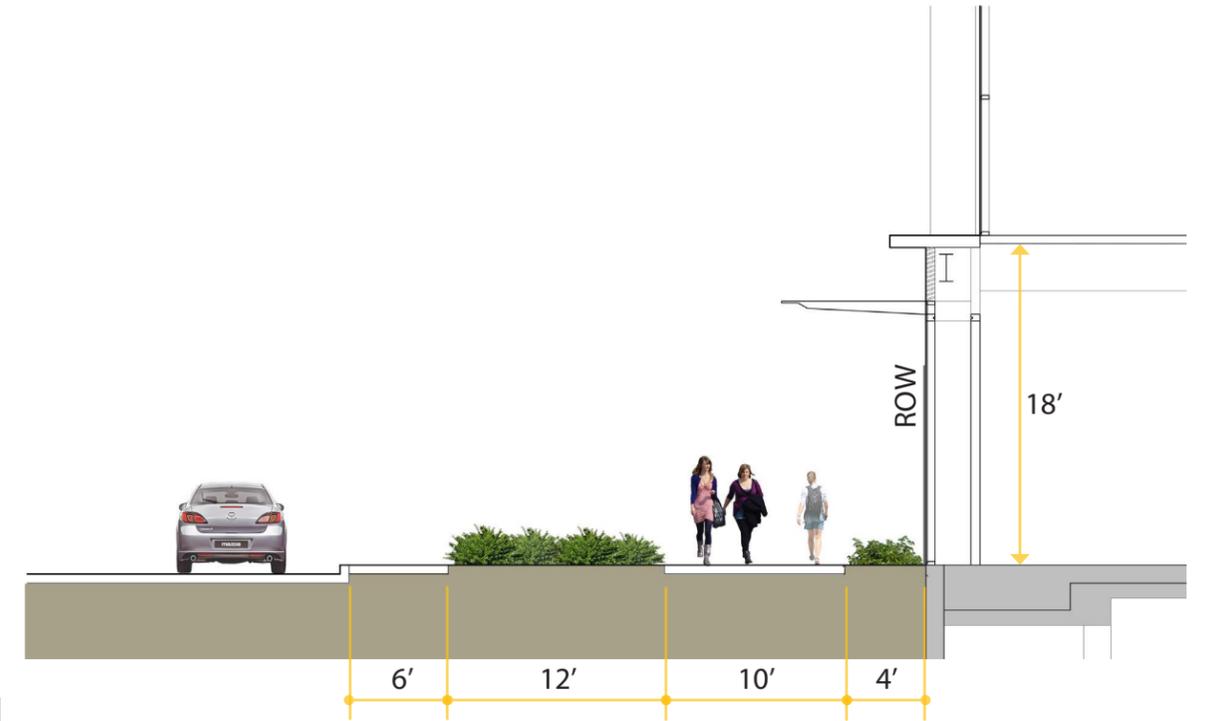
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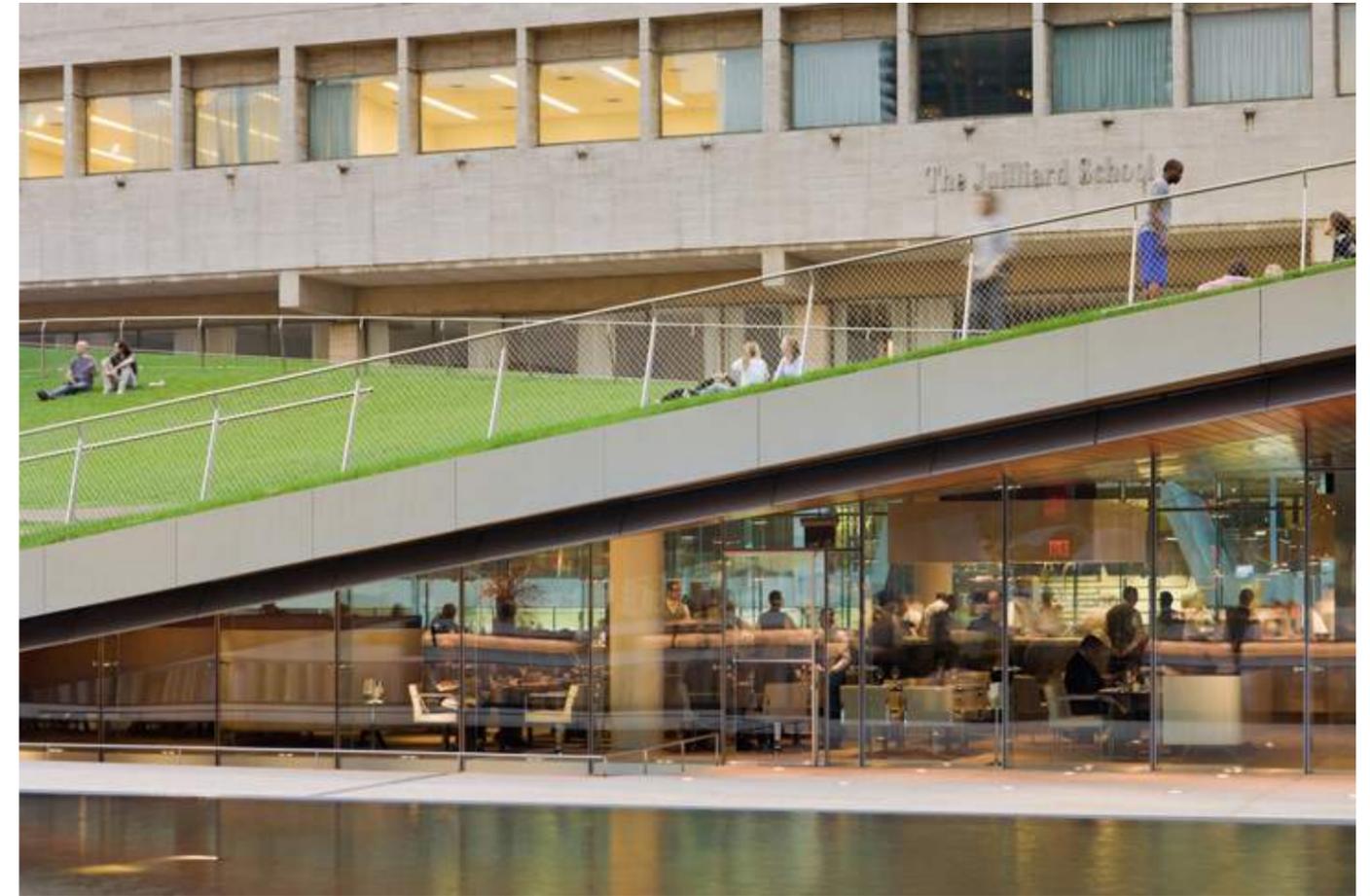


8th AVE - Looking South



8th AVE - Looking North





4. Open Space





4. Open Space





4. Open Space



4. Open Space



4. Open Space





4. Open Space

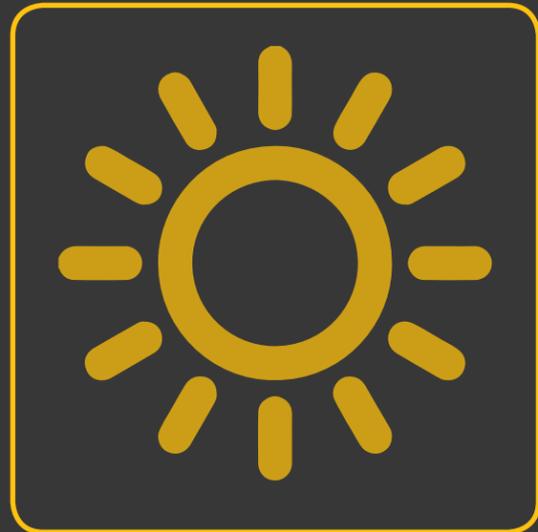


4. Open Space



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GENERAL INFORMATION



Plans and Elevations

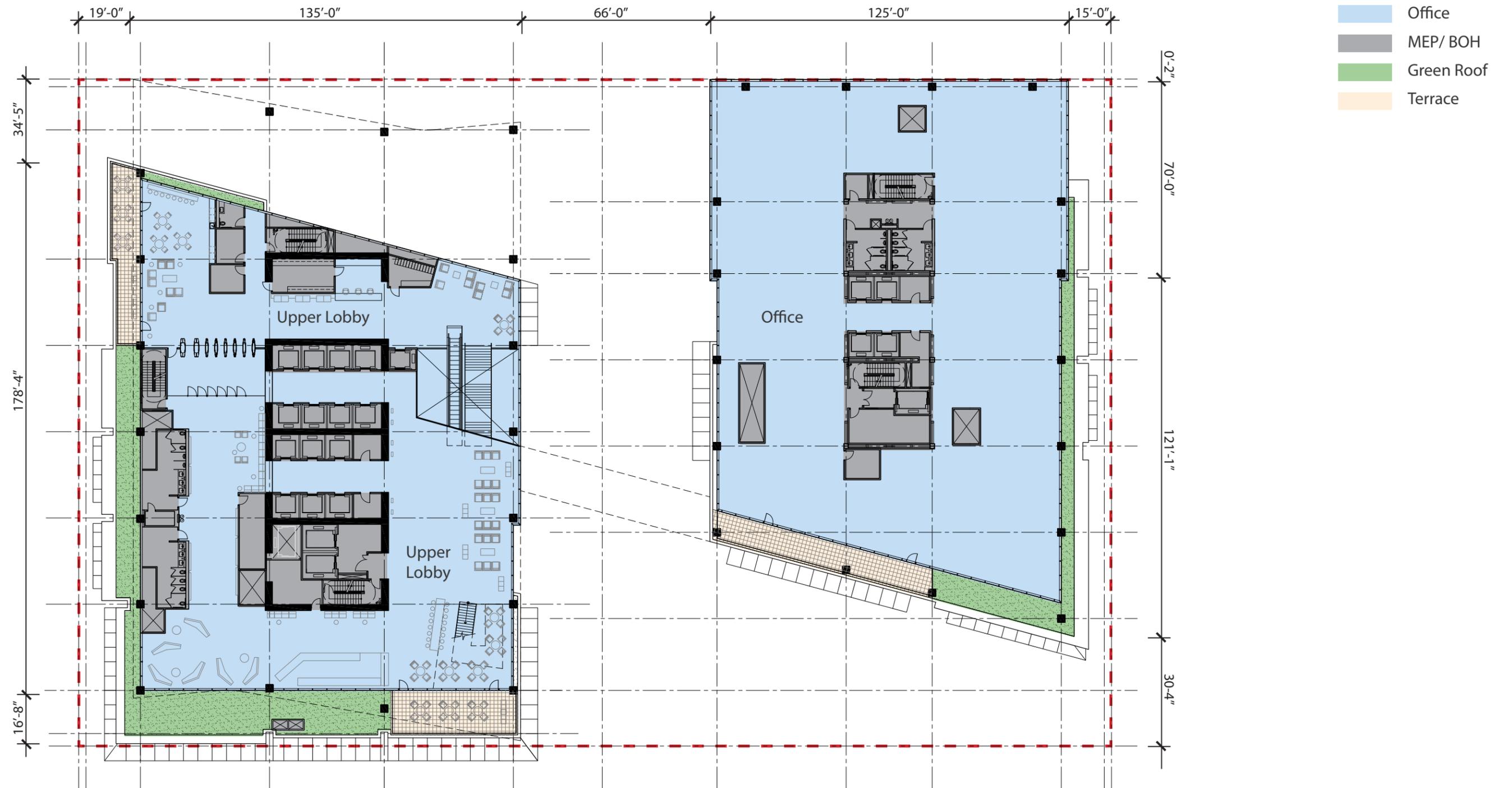


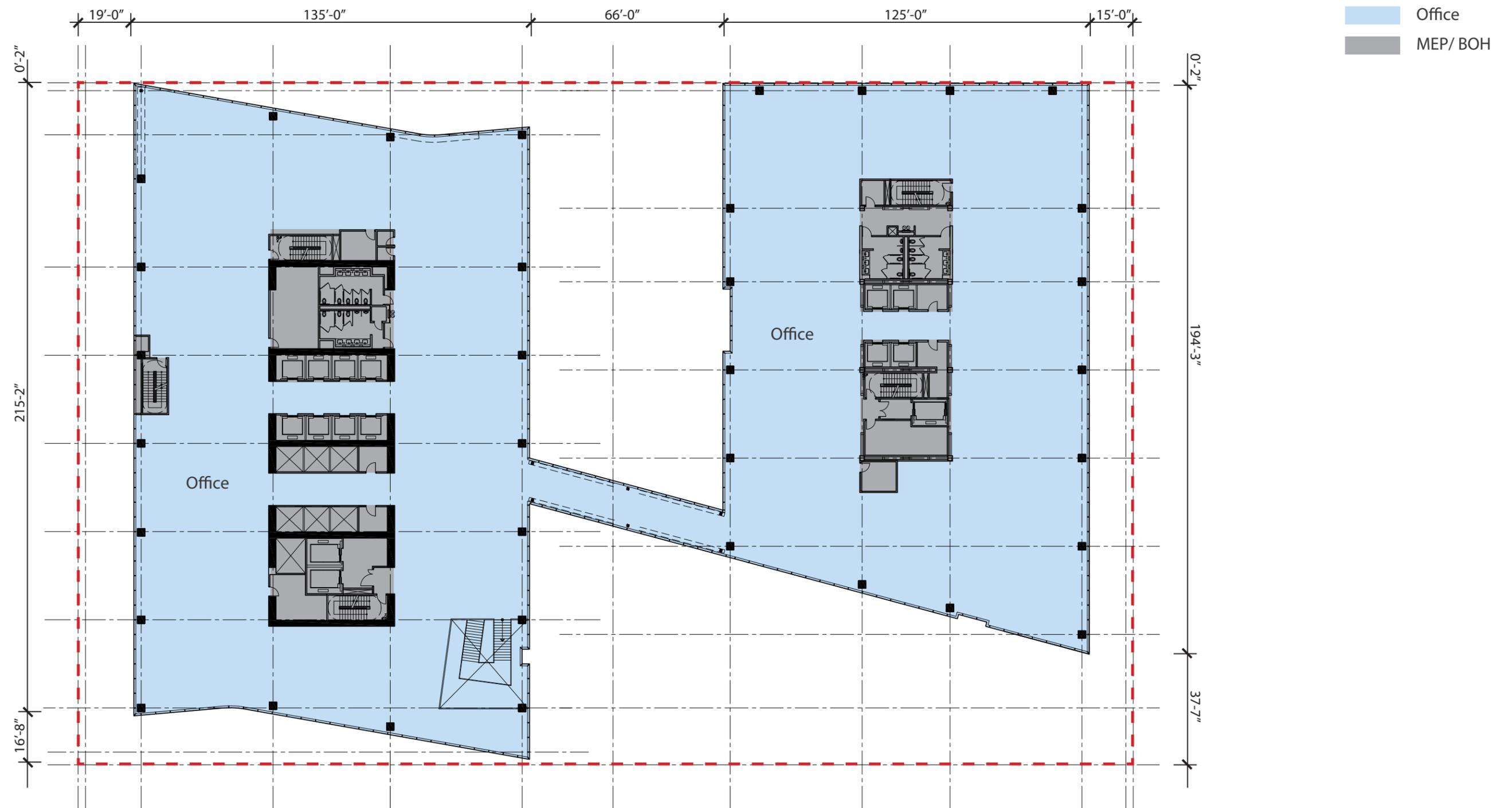
- Retail
- Office
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- Office Entry

Plans and Elevations

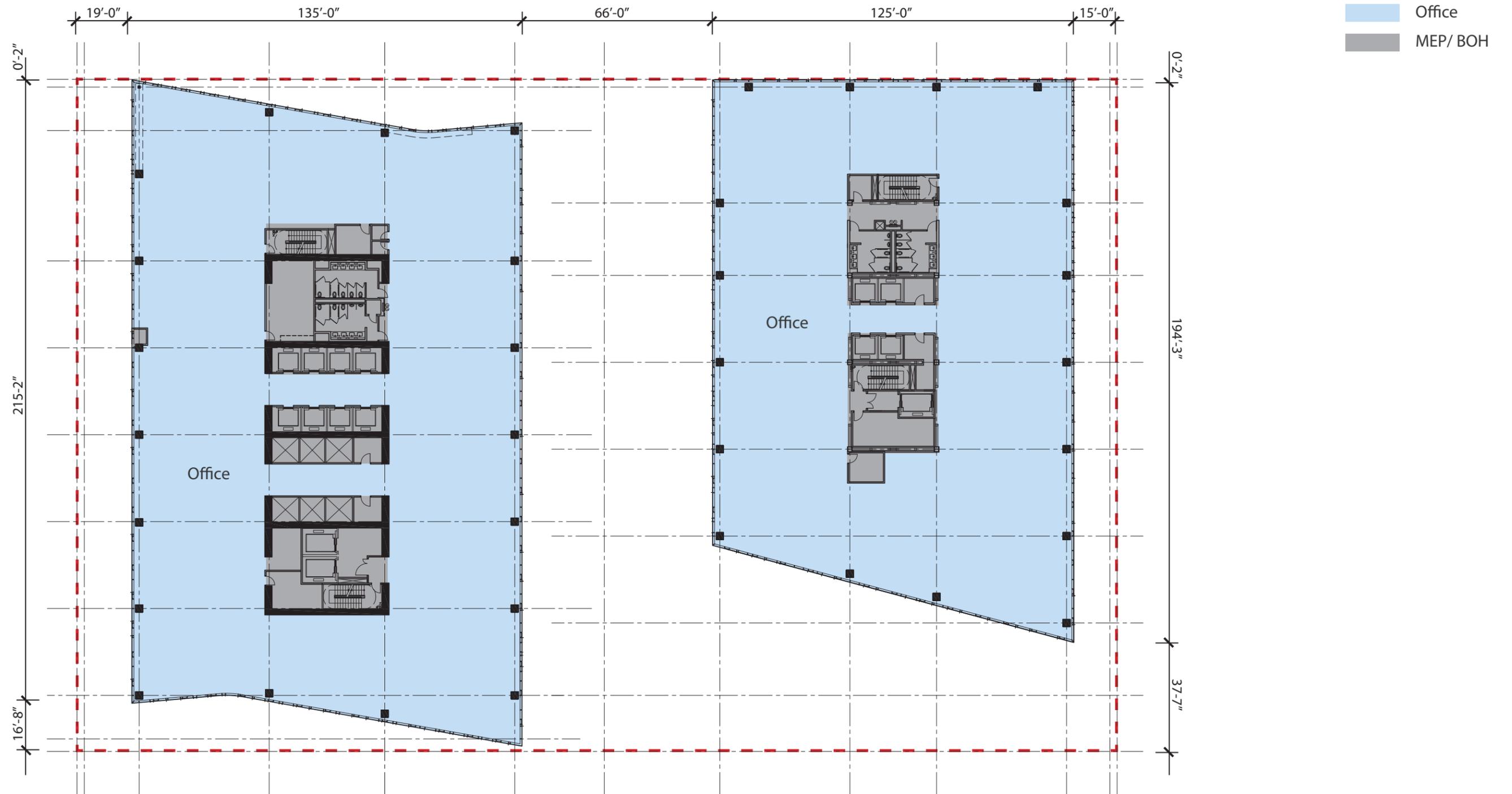


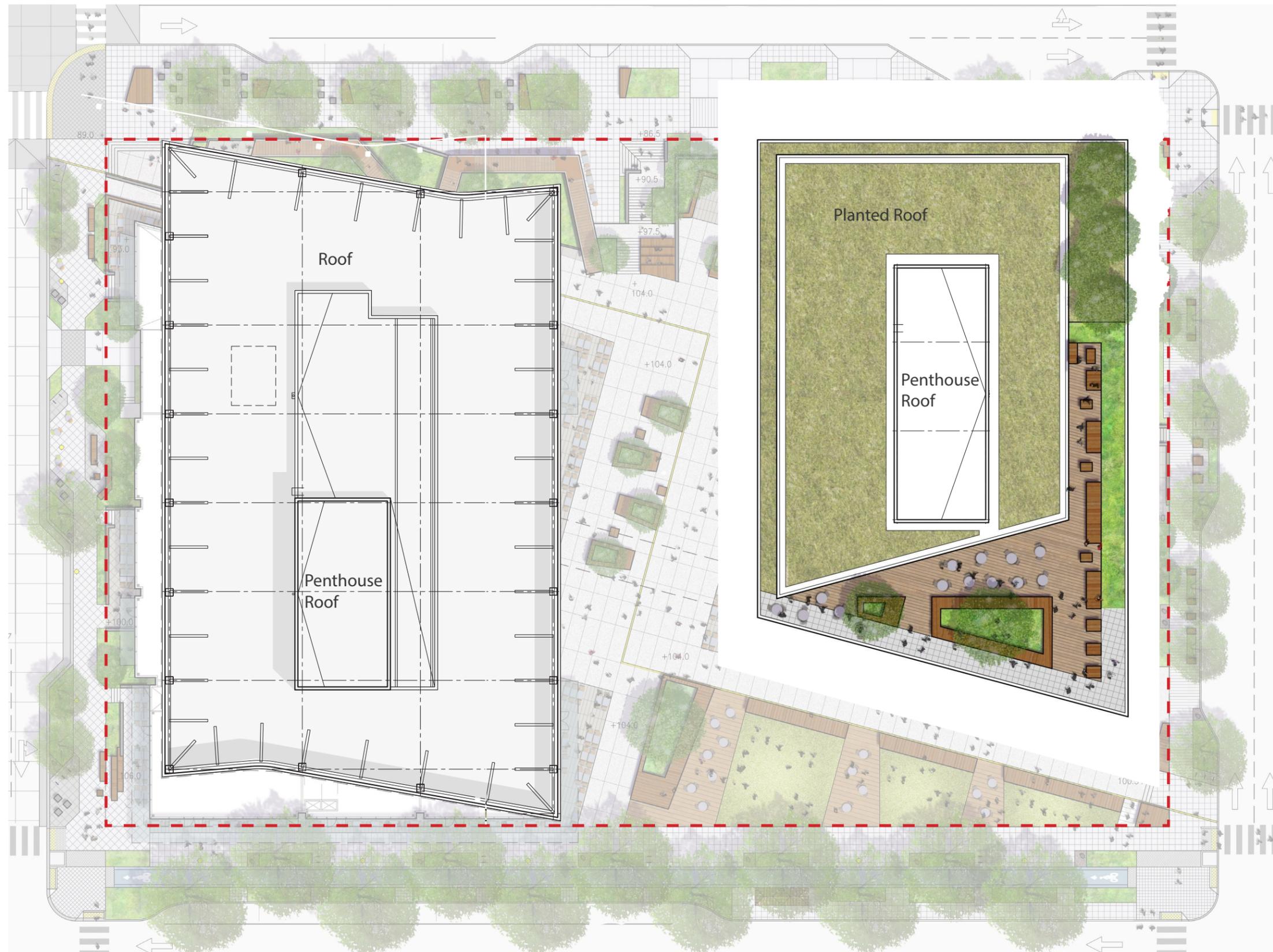
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Plans and Elevations





Plans and Elevations

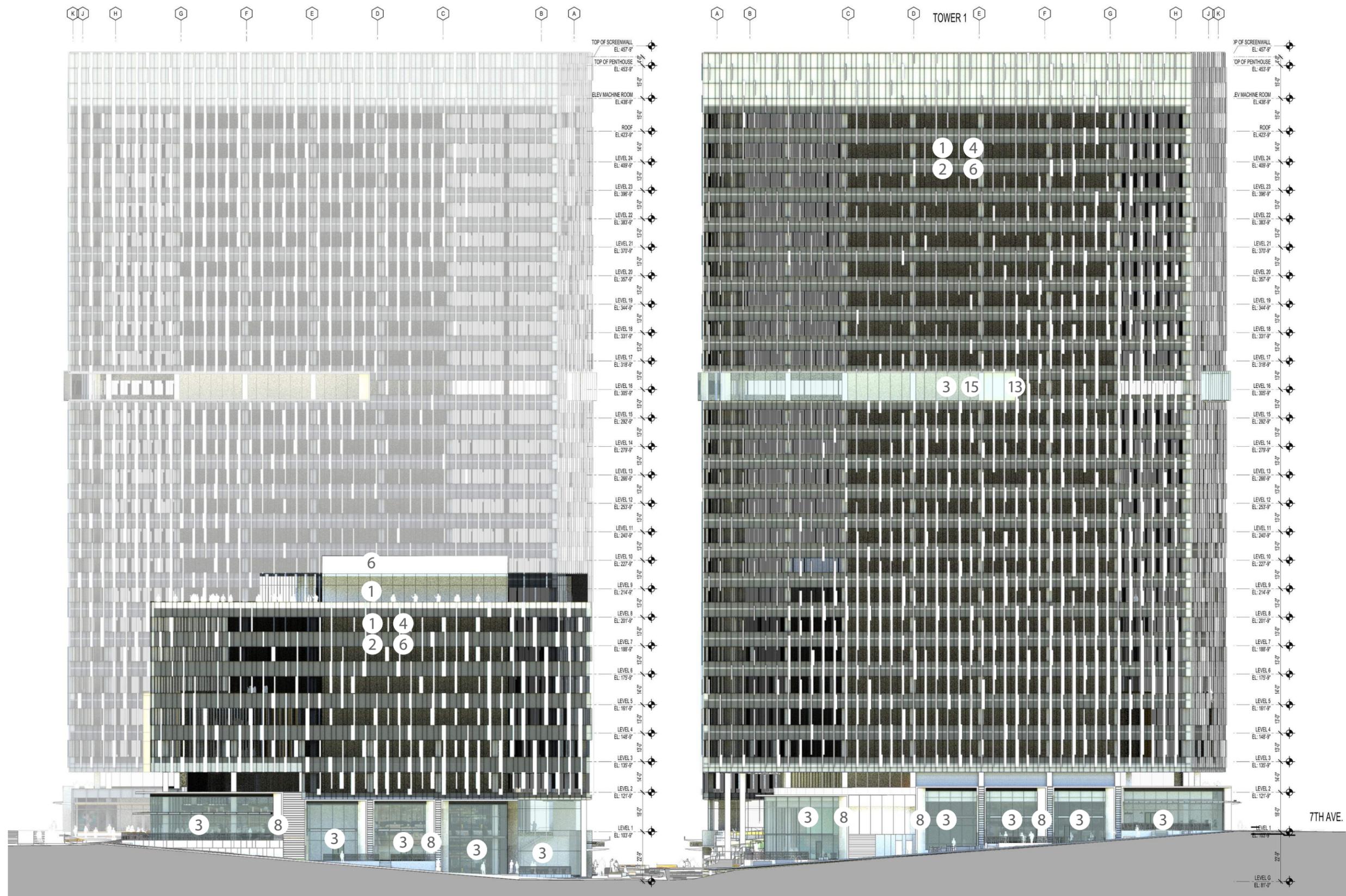


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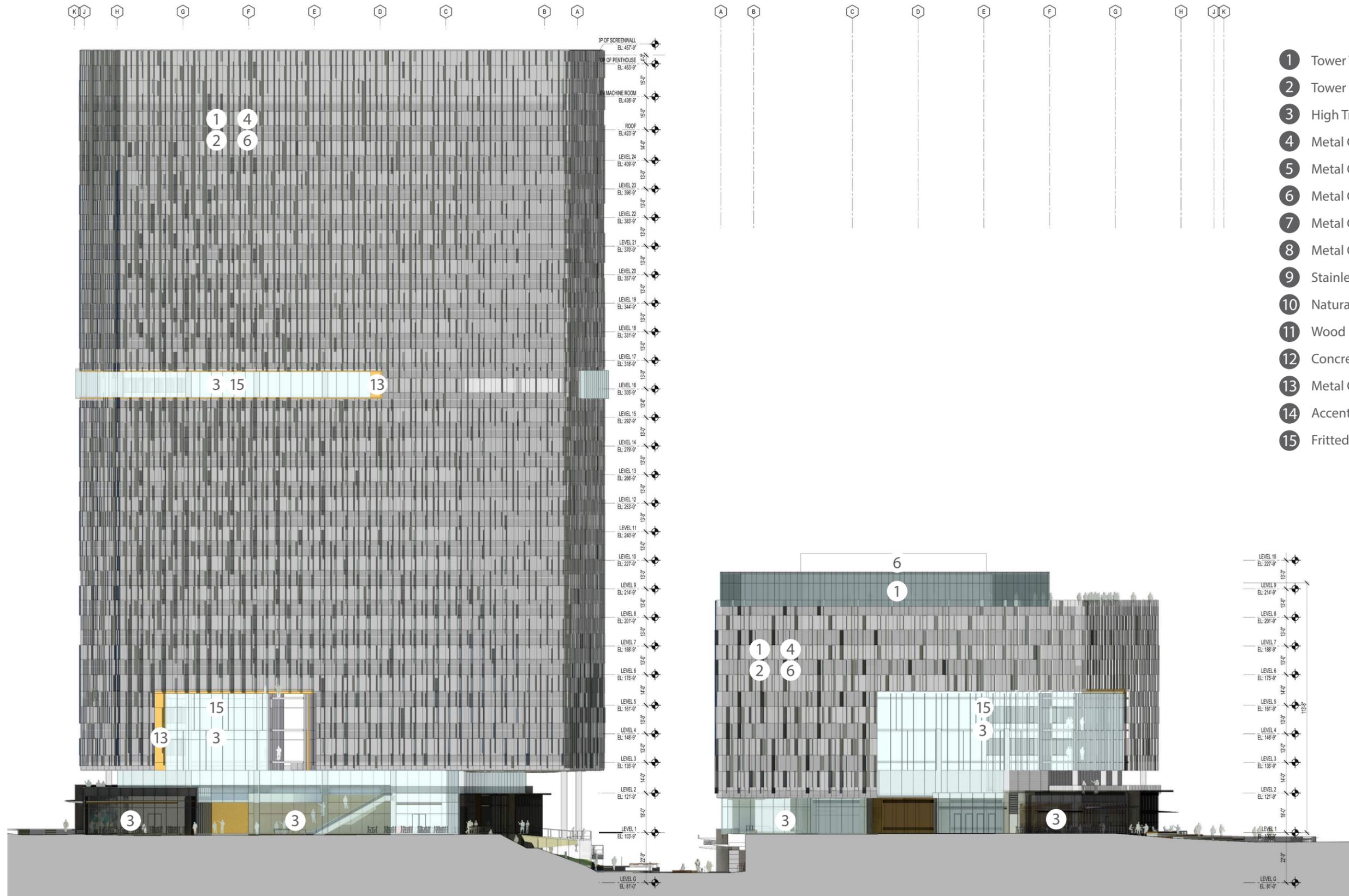
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- 3 High Transparency Glass
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- 5 Metal Color 2
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- 7 Metal Color 4
- 8 Metal Color 5
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- 12 Concrete
- 13 Metal Color 6
- 14 Accent Color (Gradient)
- 15 Fritted Glass

Plans and Elevations

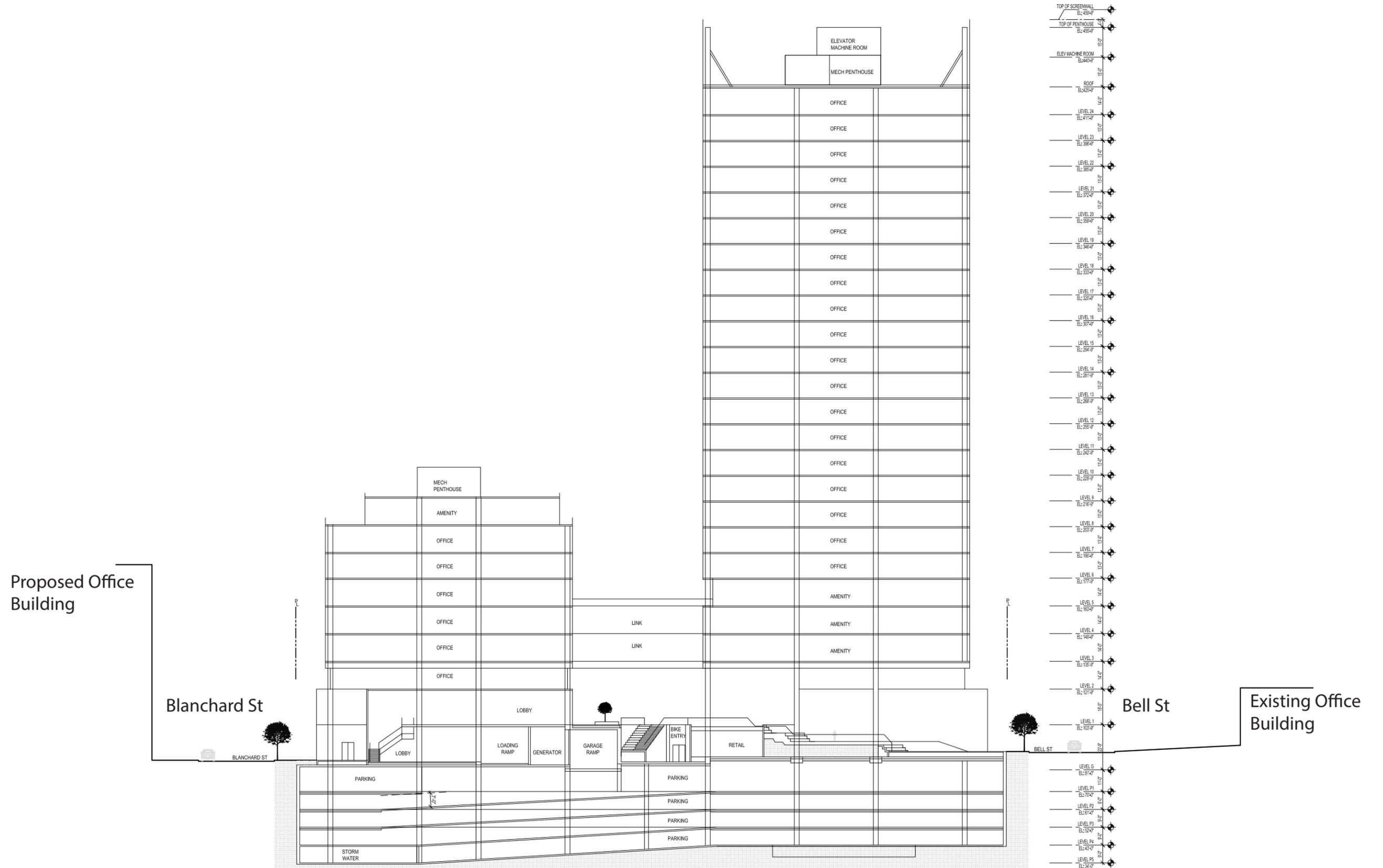


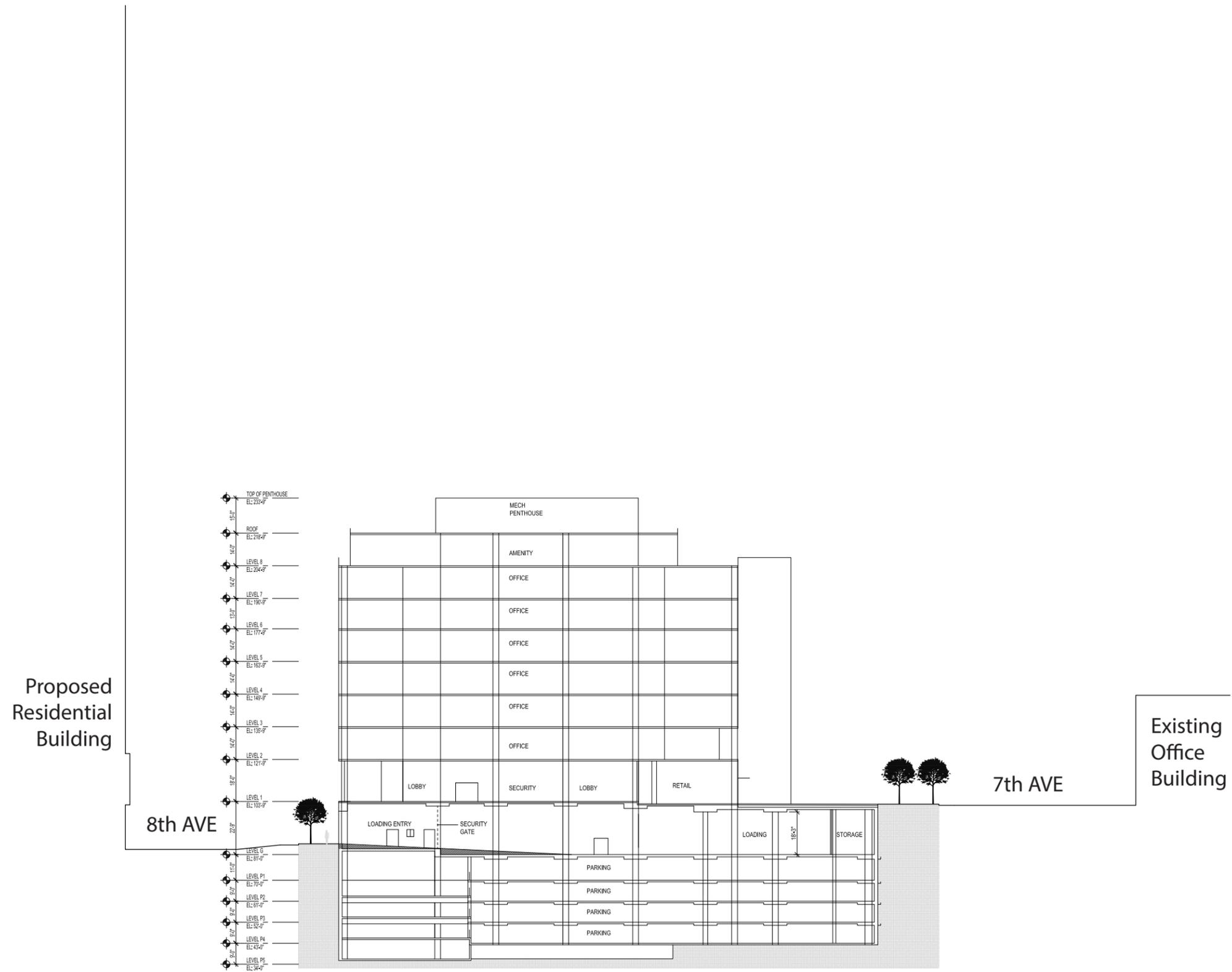
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Plans and Elevations



Plans and Elevations





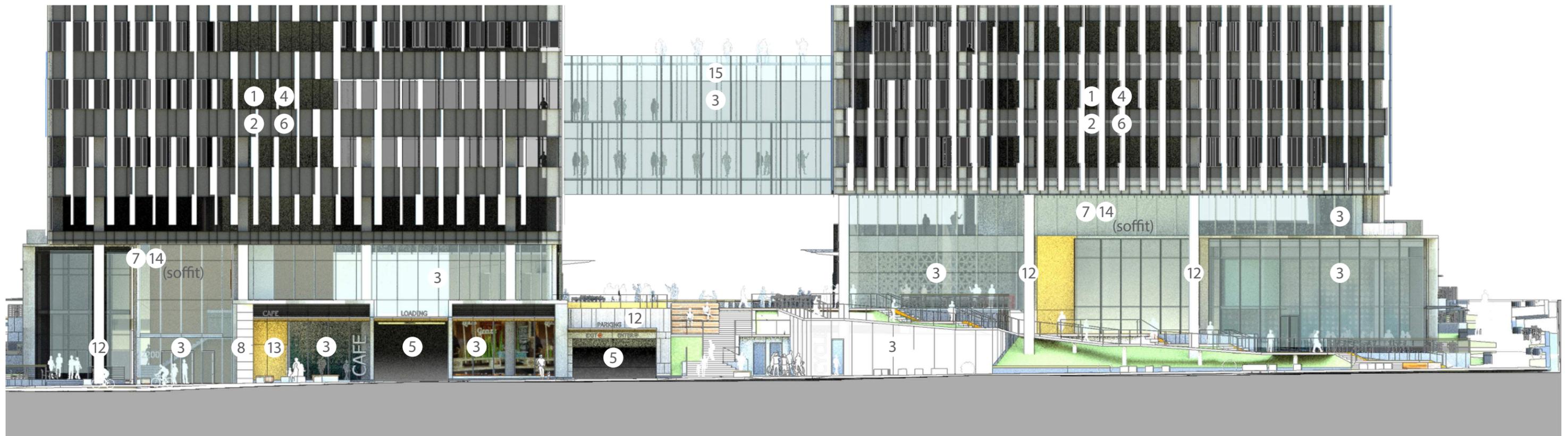
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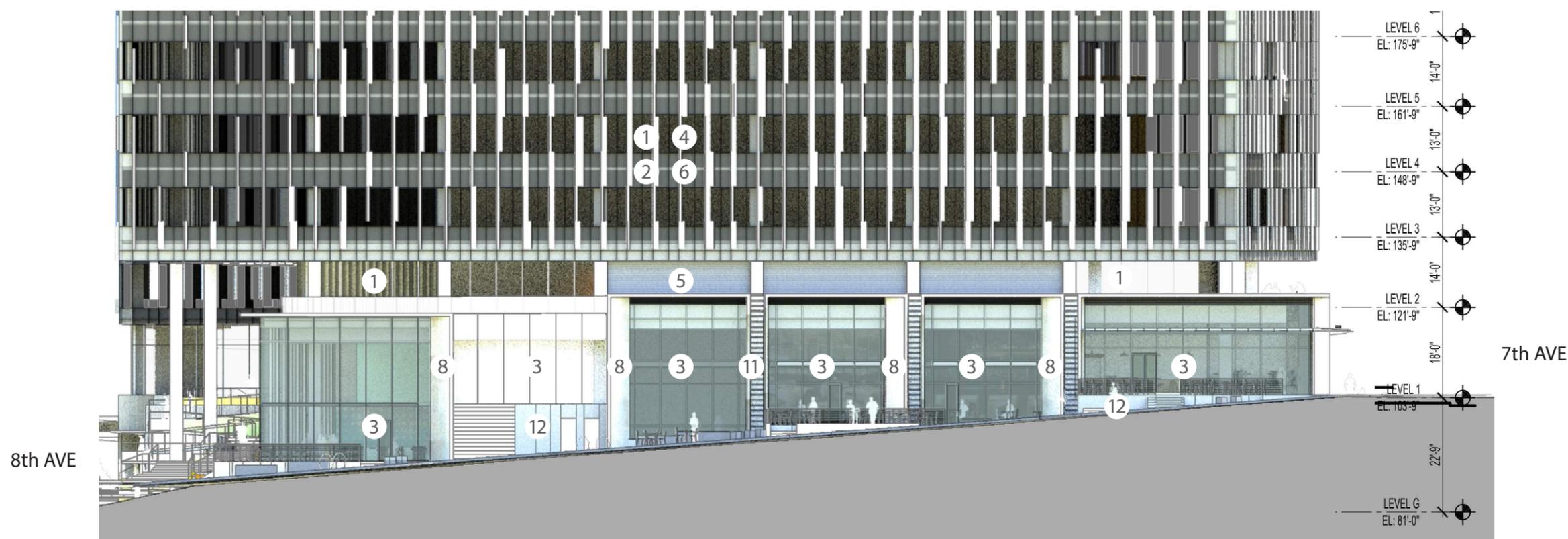
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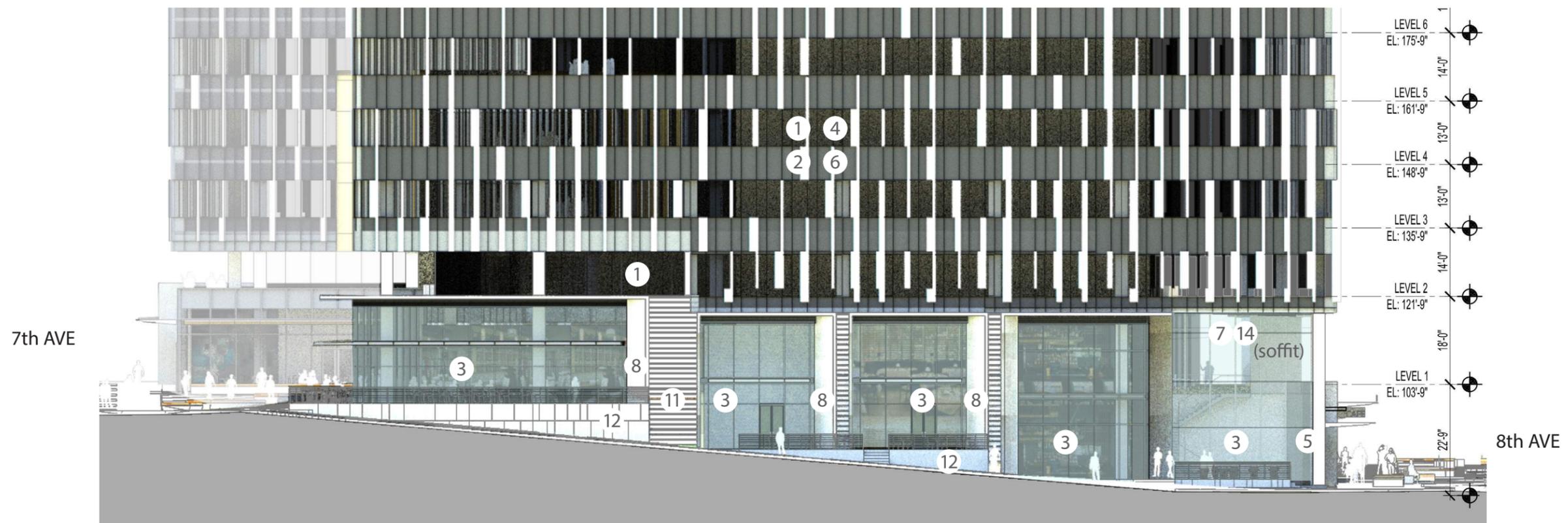
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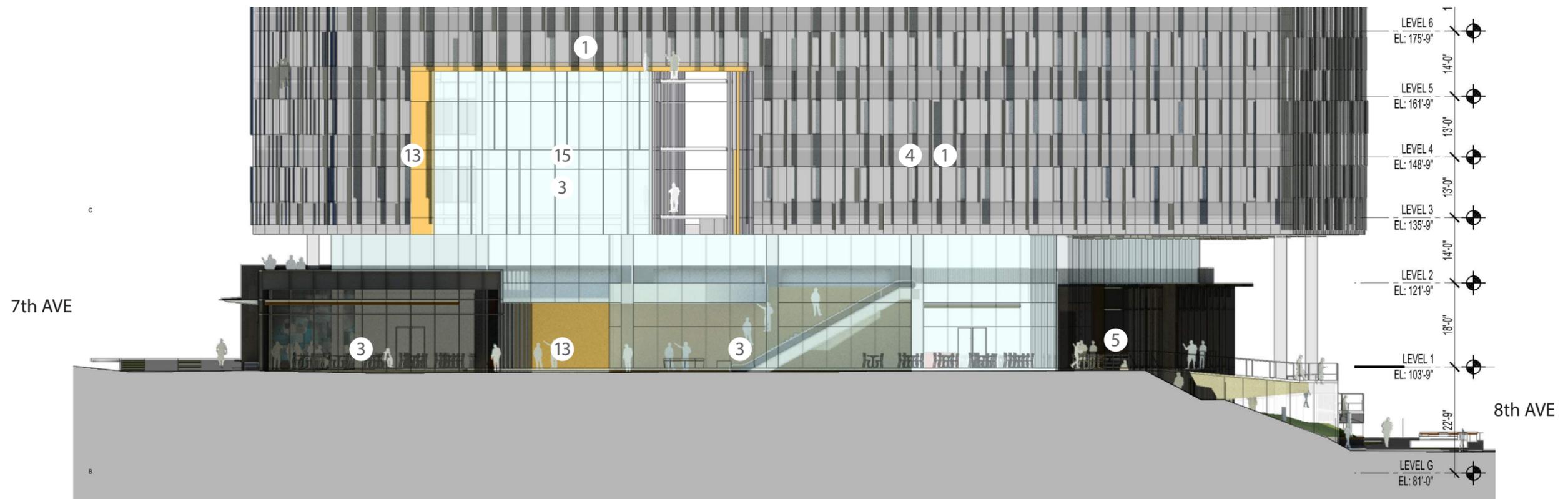
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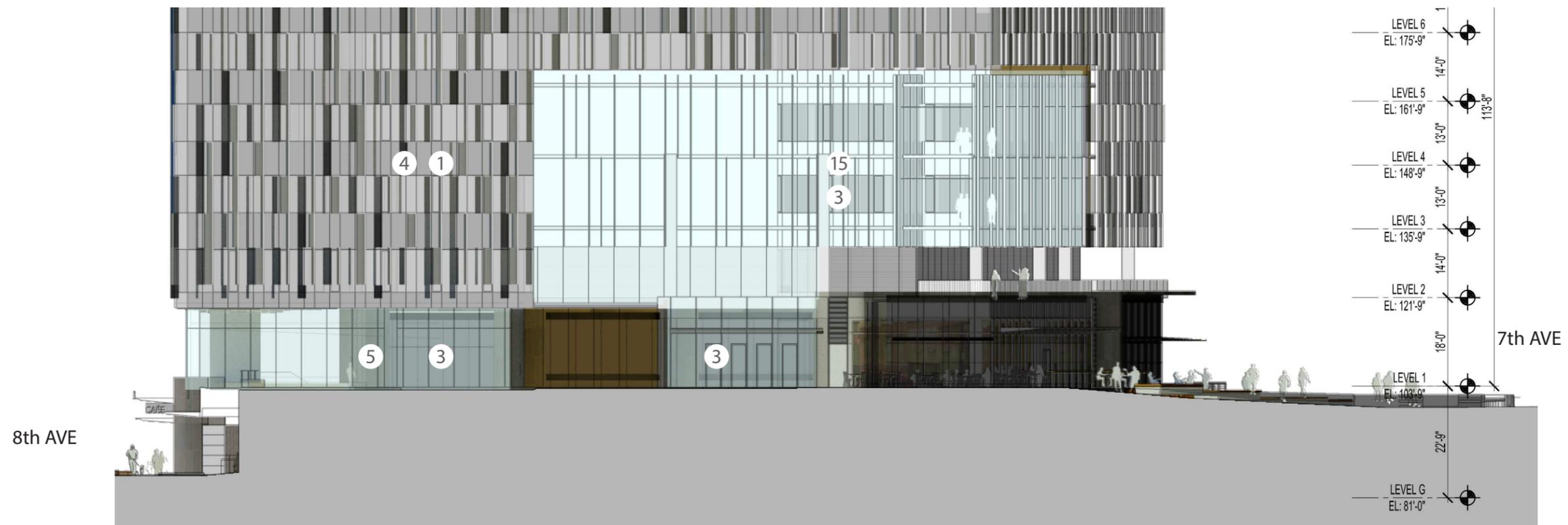
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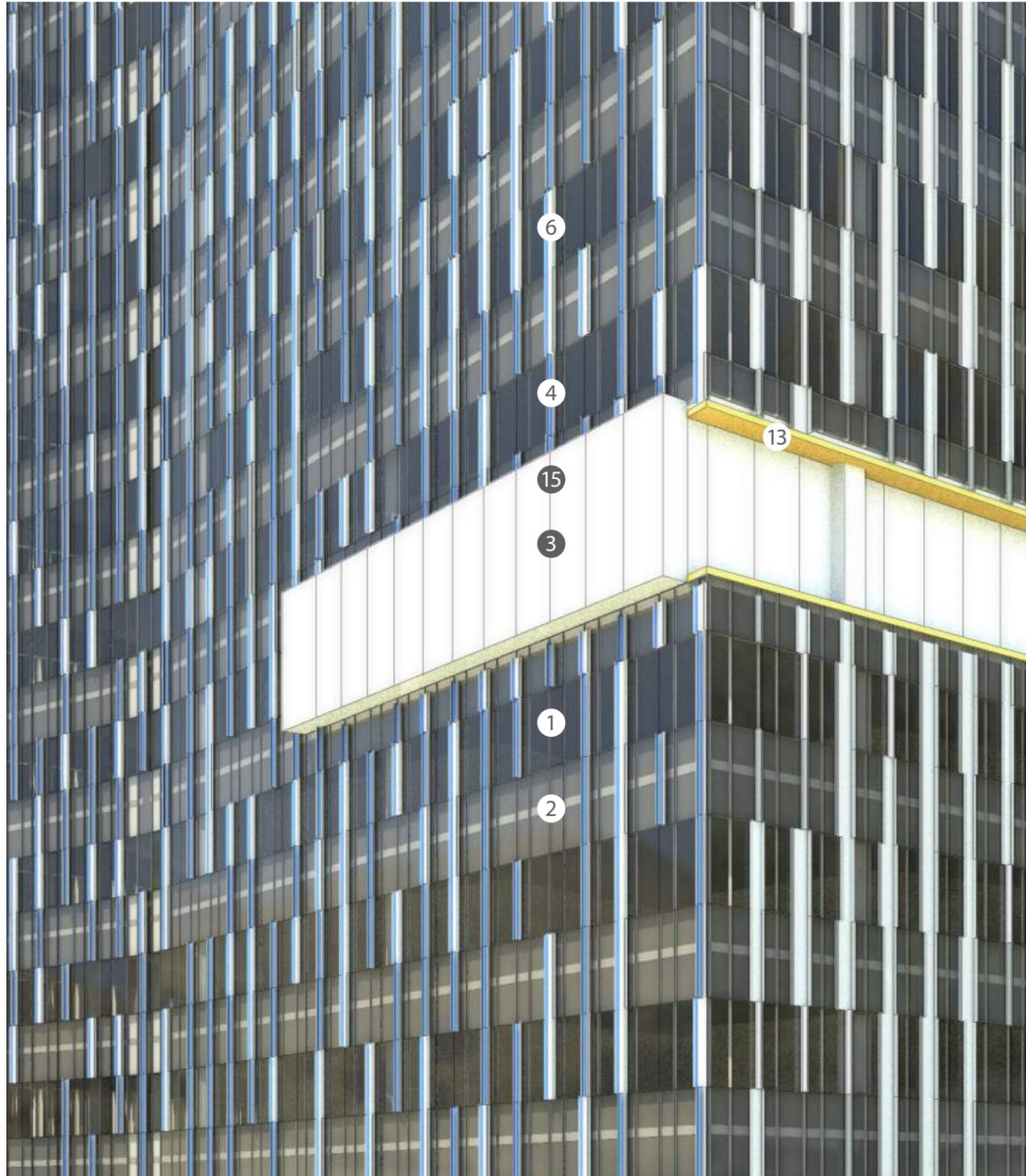
Systems and Details



- ① Tower Vision Glass
- ② Tower Spandrel Glass
- ③ High Transparency Glass
Retail
Lobbies
Tower Guardrails
- ④ Metal Color 1
Tower Fins
- ⑤ Metal Color 2
Mullions
Canopies
Louvers
Column Covers
- ⑥ Metal Color 3
Curtain Wall Panels
Penthouses
Miscellaneous
- ⑦ Satin Stainless Steel
Soffits
- ⑧ Metal Color 5
Retail Surrounds
- ⑨ Brushed Stainless Steel
Lobby Entry Doors
- ⑩ Natural Steel
Site Guardrails
- ⑪ Wood
Deck
Site Top Rails
Canopy Option
- ⑫ Concrete
Site Walls
- ⑬ Metal Color 6
Accent Panels
- ⑭ Accent Color (Gradient)
8th Avenue Soffit
- ⑮ Fritted Glass
Link
L16 Projection

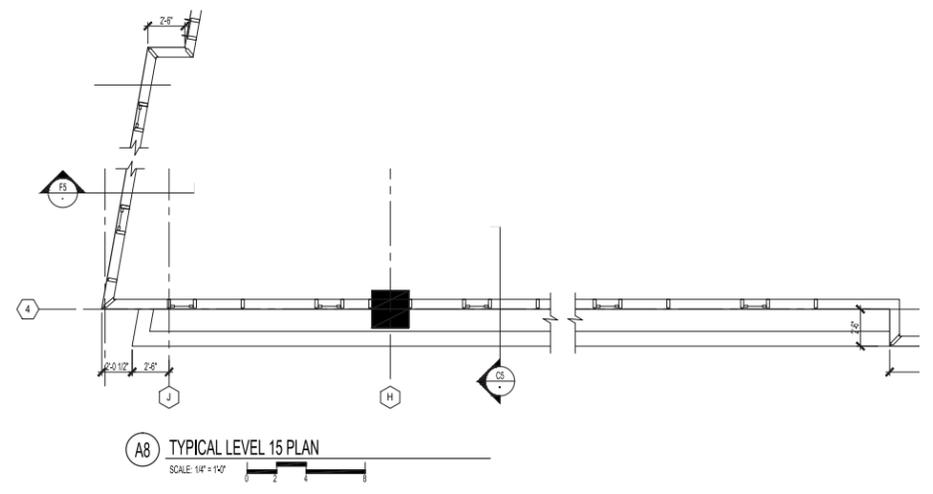
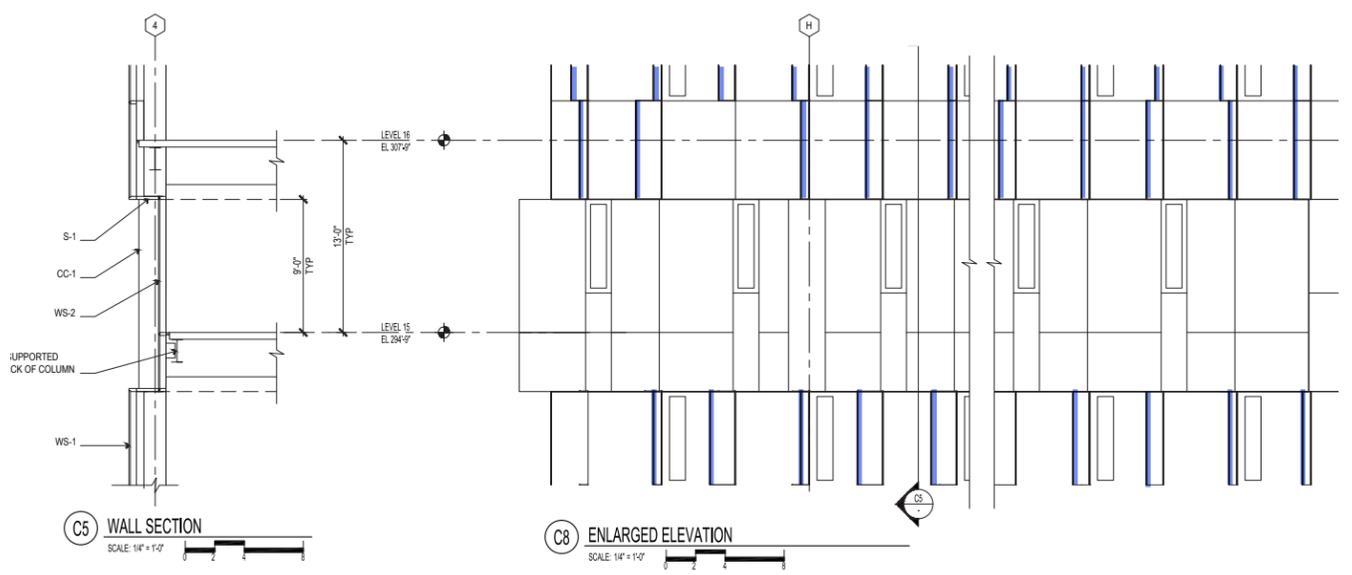
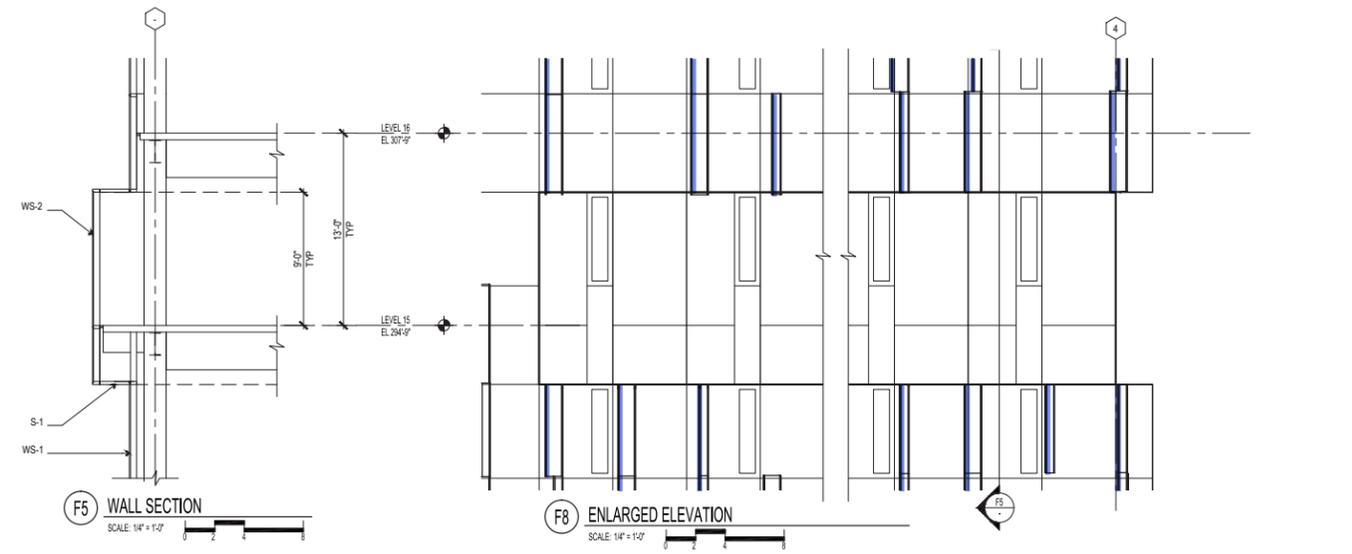
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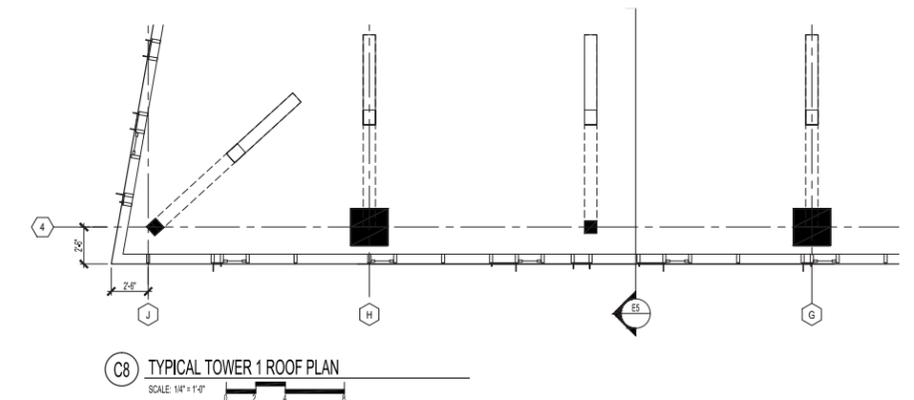
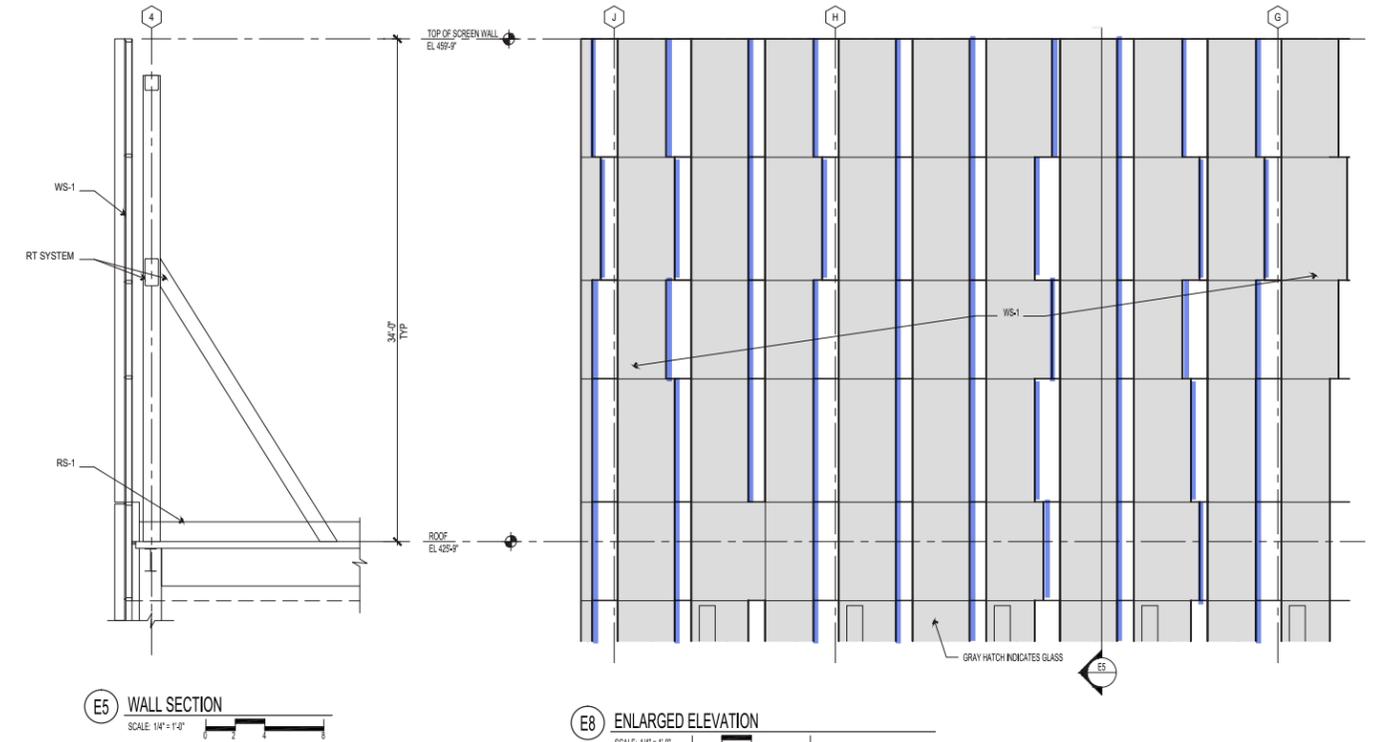
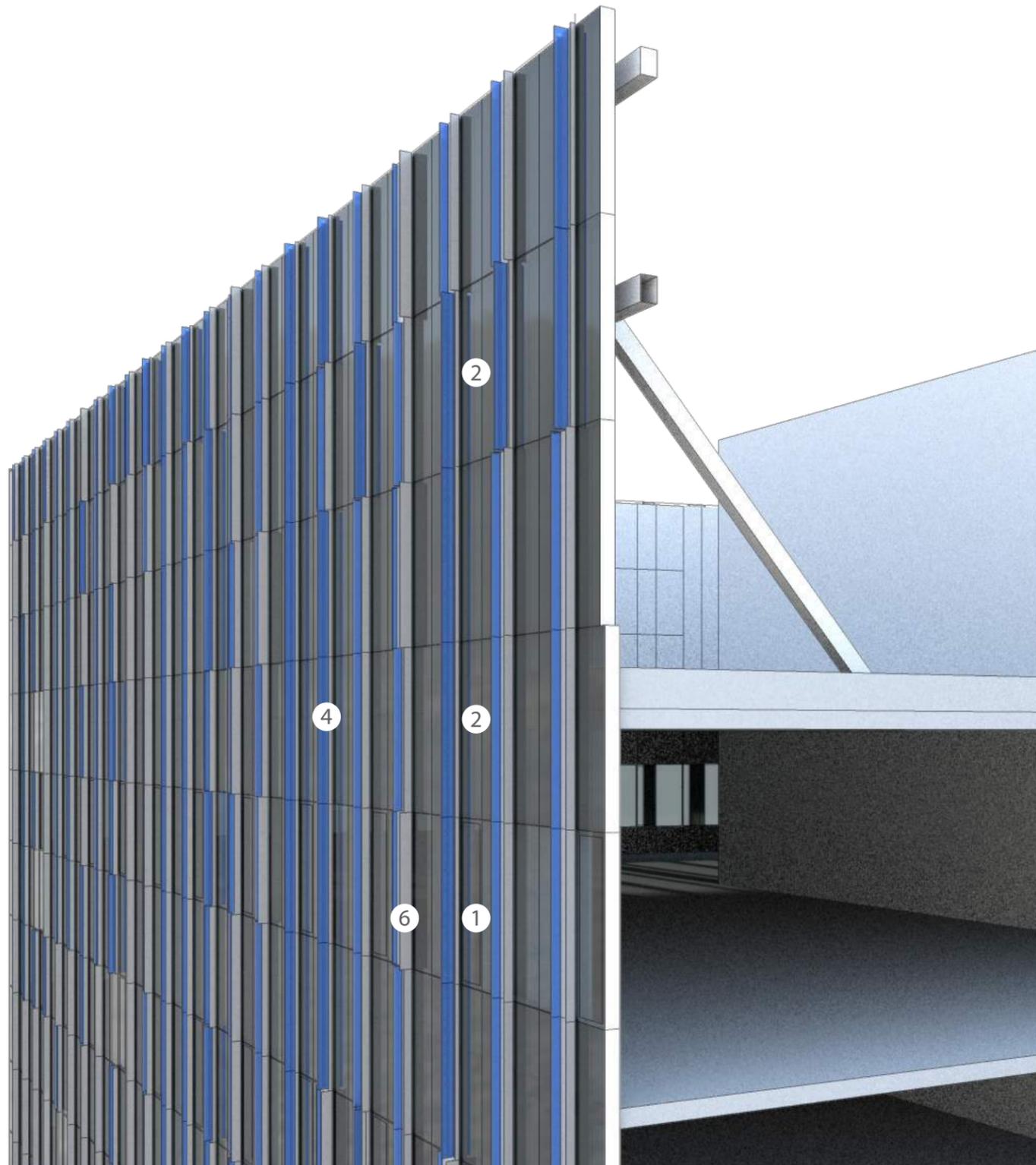
Systems and Details



- 1 Tower Vision Glass
- 2 Tower Spandrel Glass
- 3 High Transparency Glass
- 4 Metal Color 1
- 5 Metal Color 2
- 6 Metal Color 3
- 13 Metal Color 6
- 15 Fritted Glass

See Sheet A-74 for Color Palette

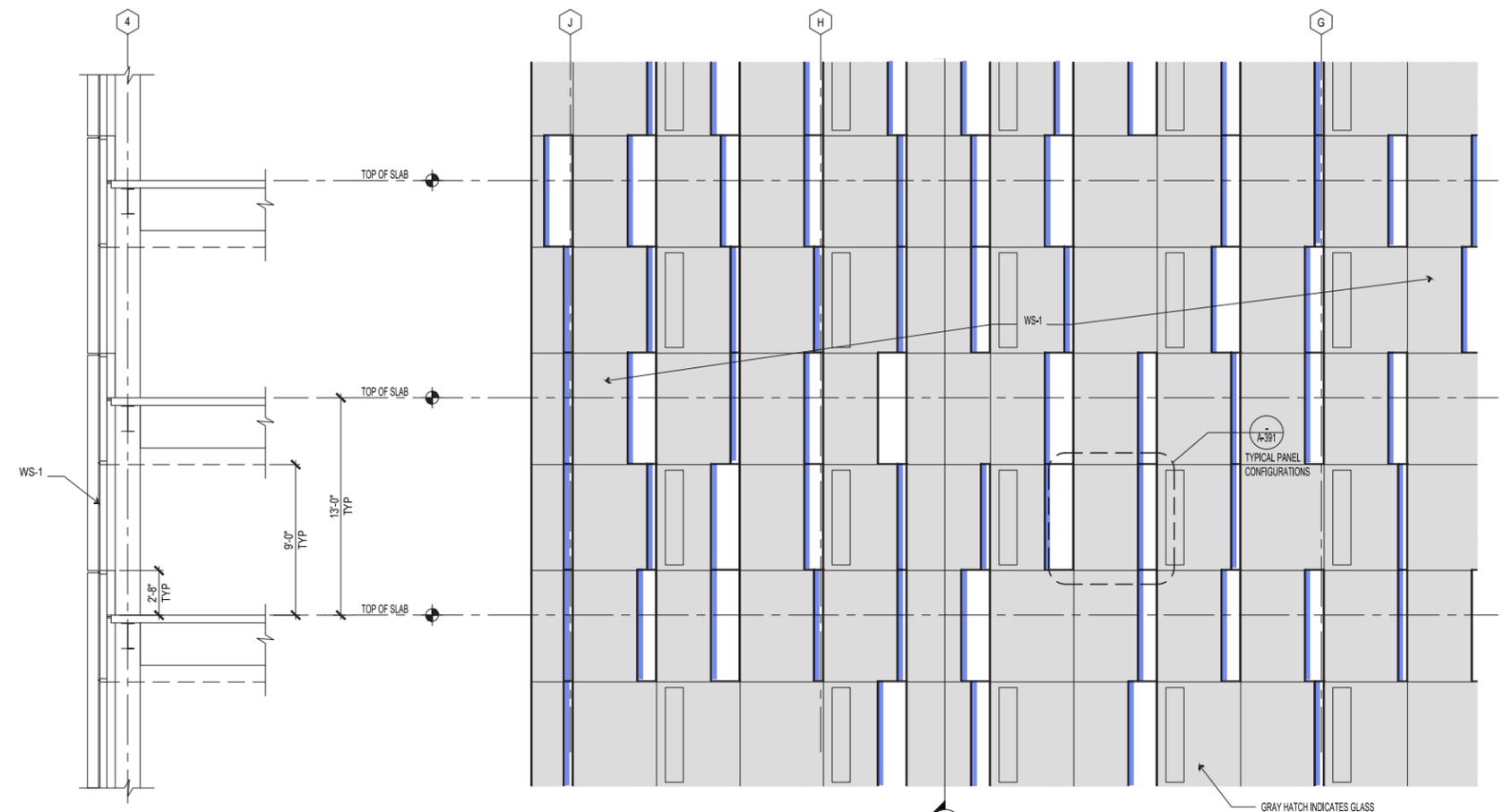
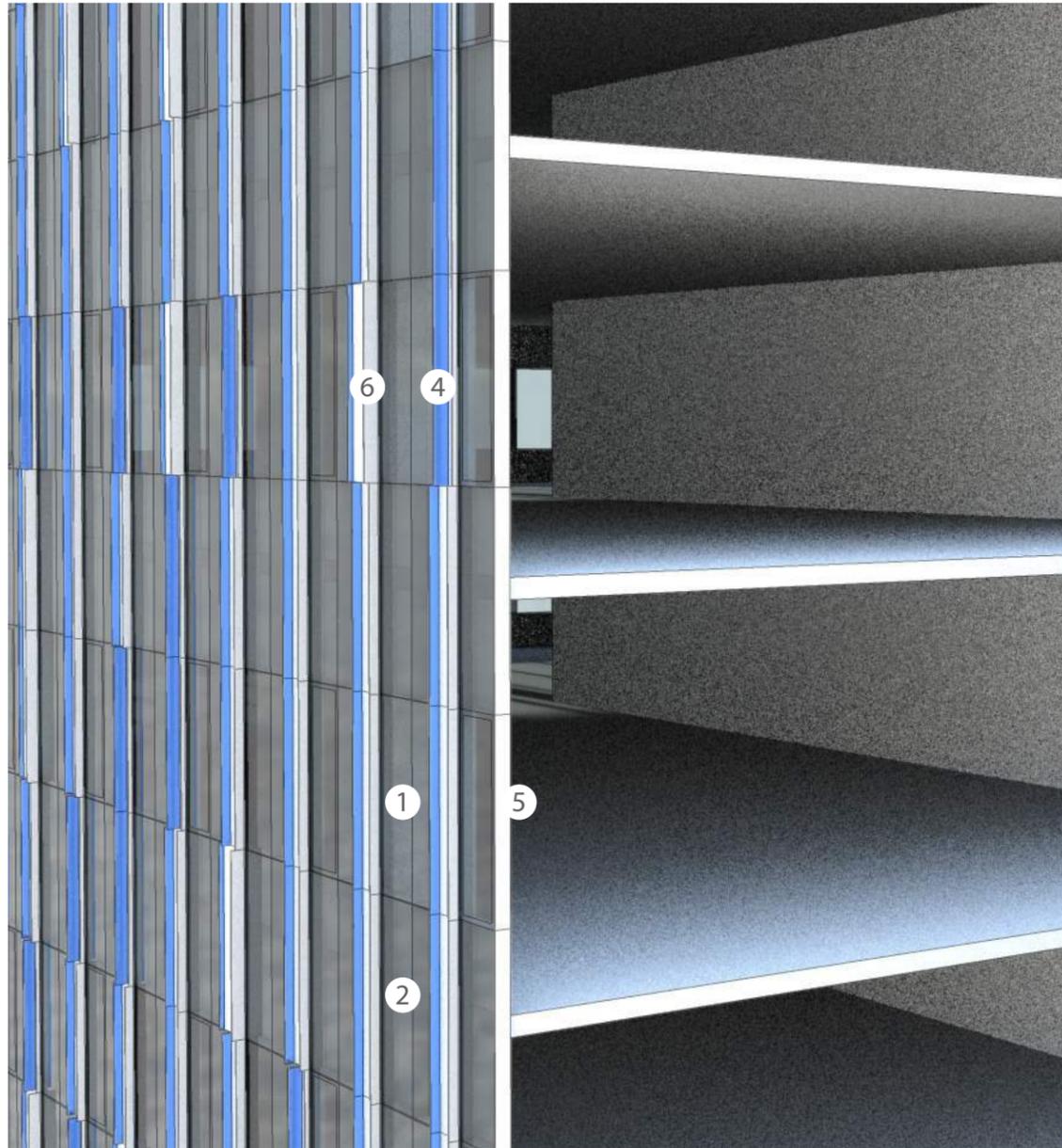




- 1 Tower Vision Glass
- 2 Tower Spandrel Glass
- 3 High Transparency Glass
- 4 Metal Color 1
- 5 Metal Color 2
- 6 Metal Color 3
- 13 Metal Color 6
- 15 Fritted Glass

See Sheet A-74 for Color Palette

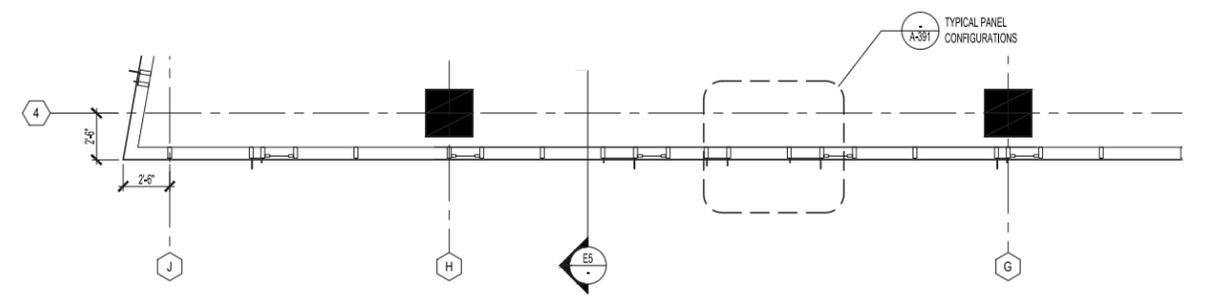
Systems and Details



E5 WALL SECTION
SCALE: 1/4" = 1'-0"

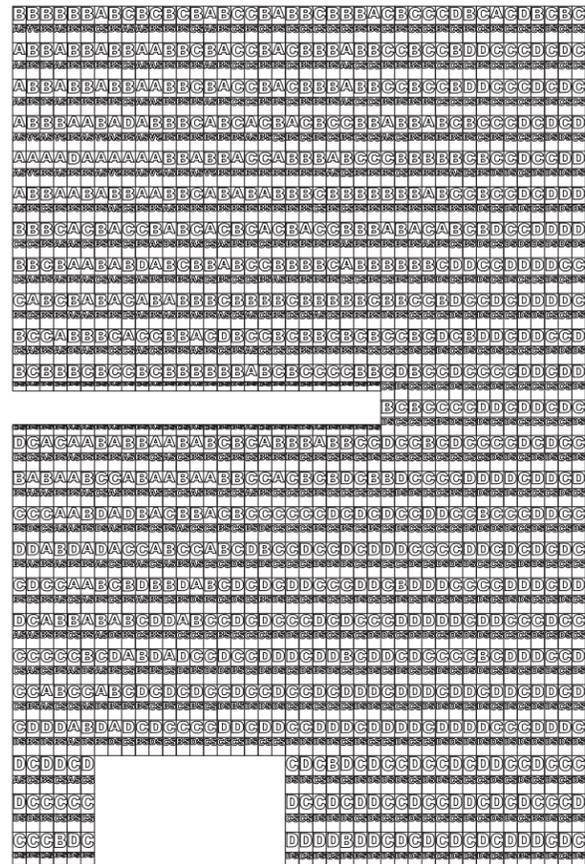
E8 ENLARGED ELEVATION
SCALE: 1/4" = 1'-0"

- 1 Tower Vision Glass
- 2 Tower Spandrel Glass
- 3 High Transparency Glass
- 4 Metal Color 1
- 5 Metal Color 2
- 6 Metal Color 3
- 13 Metal Color 6
- 15 Fritted Glass

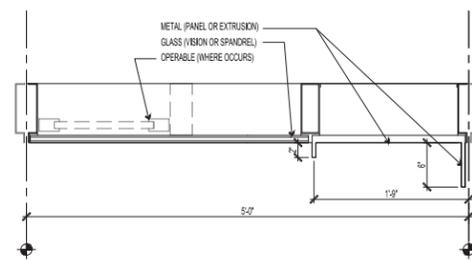
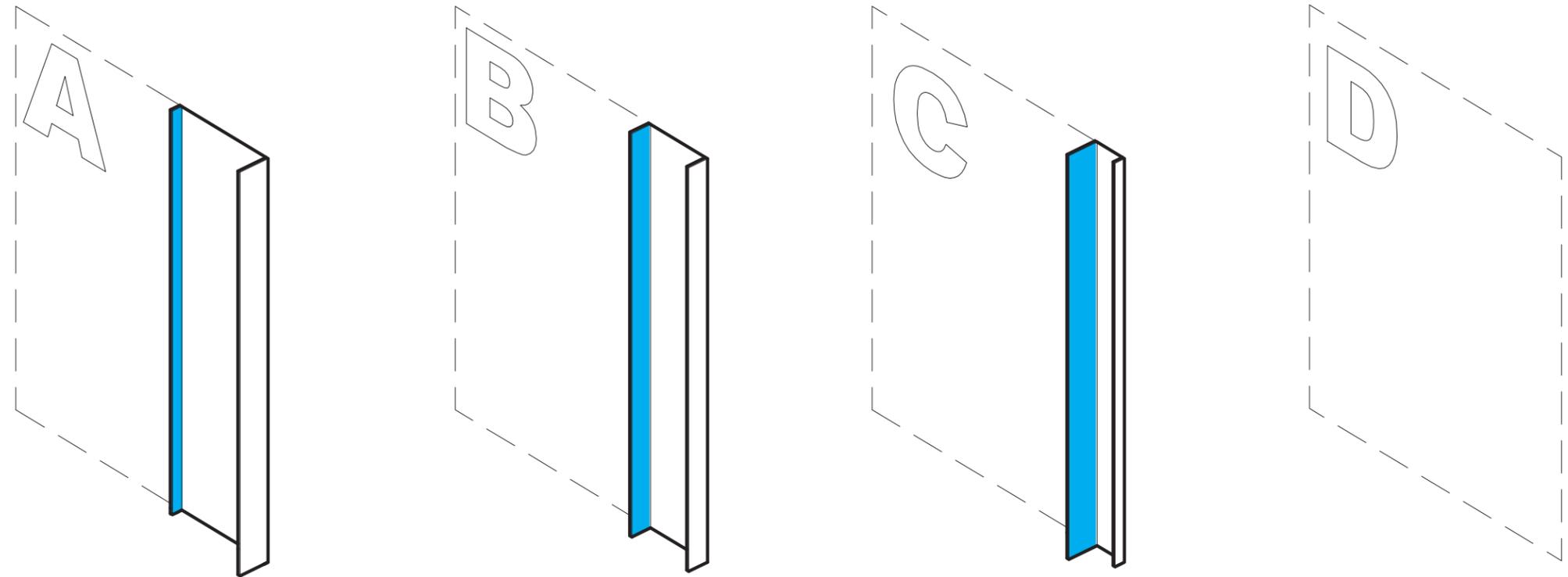


C7 TYPICAL TOWER PLAN
SCALE: 1/4" = 1'-0"

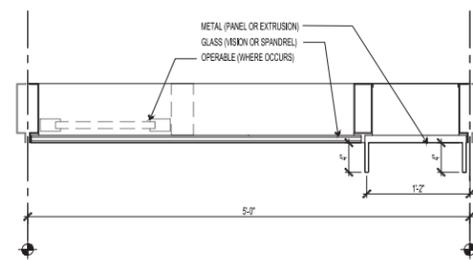
See Sheet A-74 for Color Palette



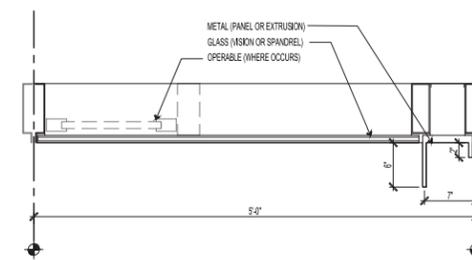
Representative elevation (south facade, north tower shown) illustrating the “addressable” curtain wall system matrix comprised of four panel types



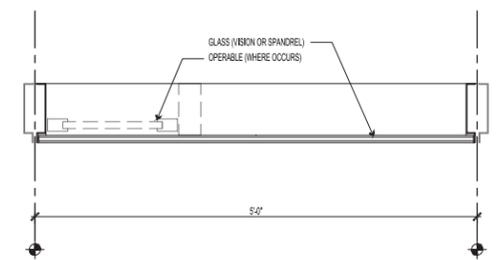
B1 PANEL A - PLAN



B4 PANEL B - PLAN

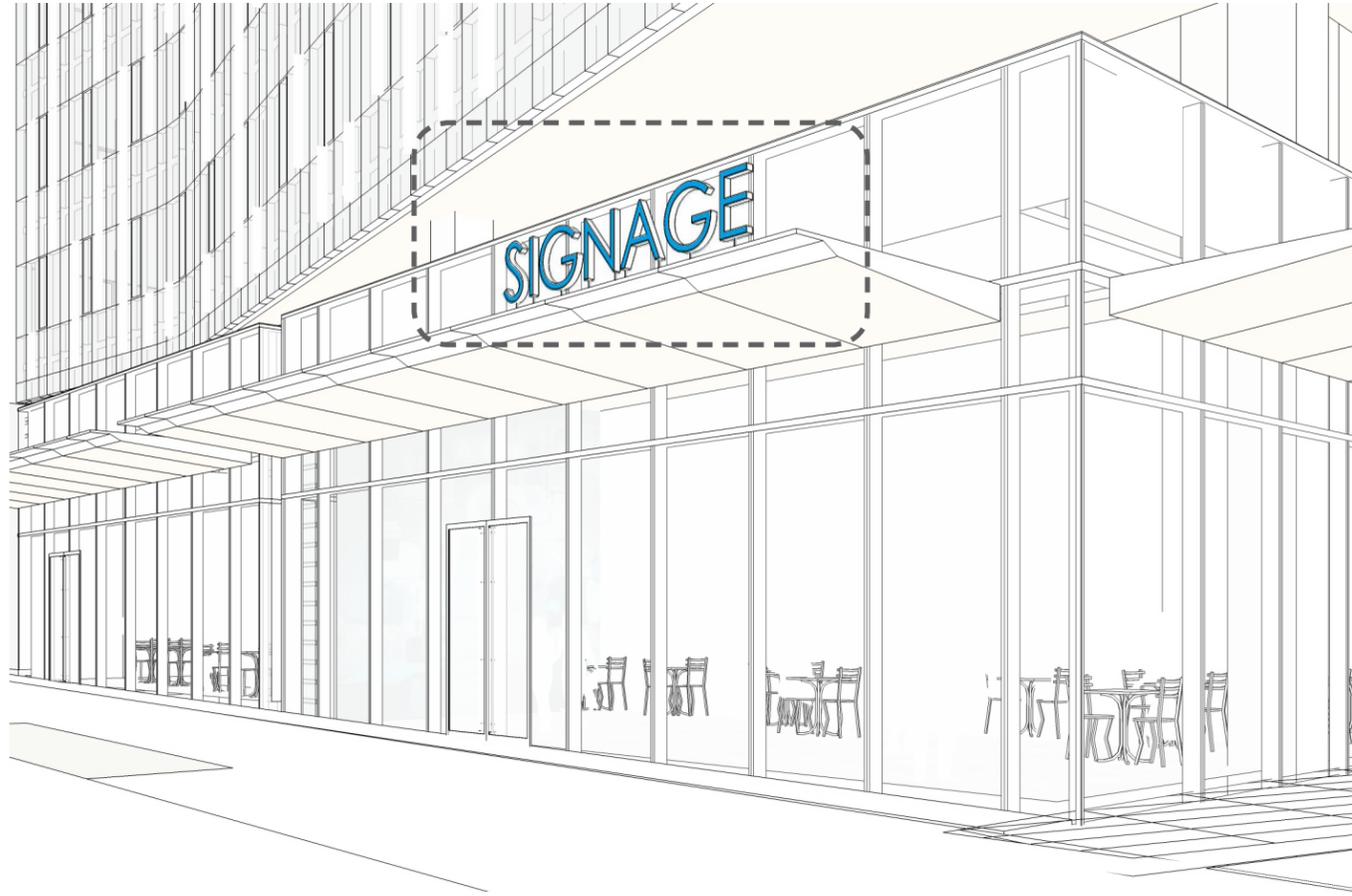


B7 PANEL C - PLAN

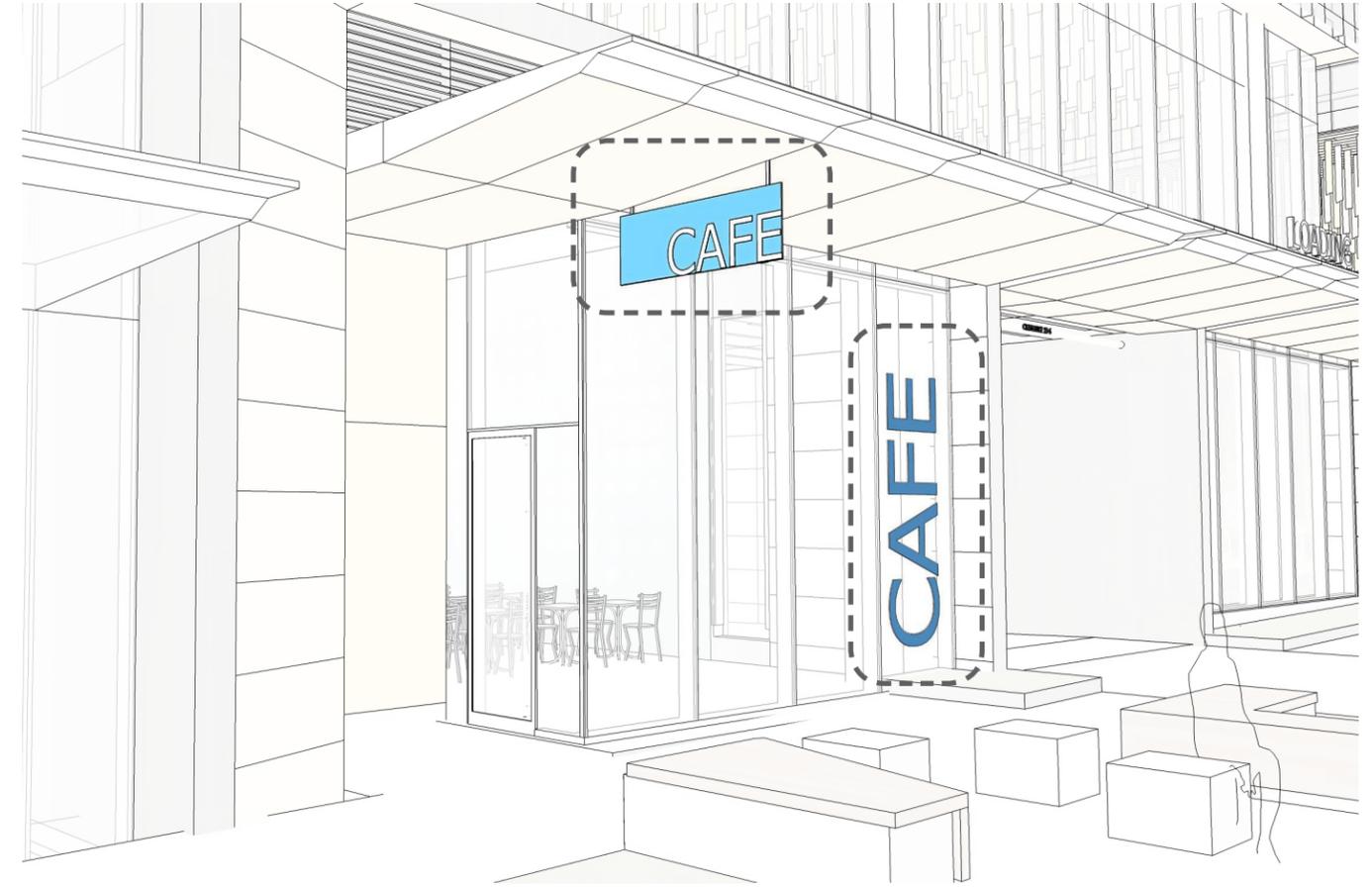


B10 PANEL D - PLAN

Systems and Details

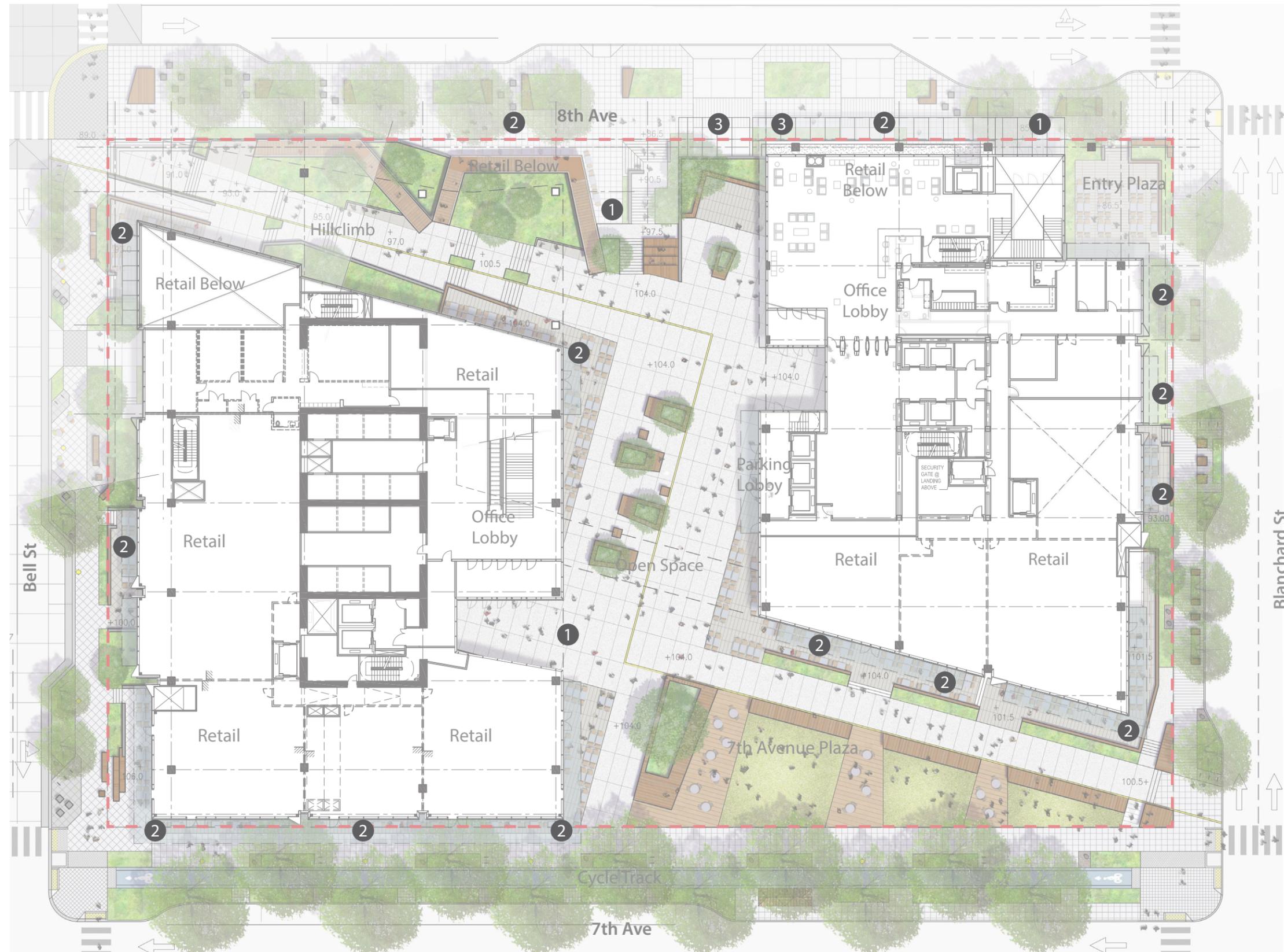


Description: Pin-mounted to canopy. Three-dimensional relief letters in stainless steel, powder-coated metal or illuminated channel letters. (Note: final retail signage is subject to retailer's requirements and will be reviewed by owner for consistency with building design).



Description: Hung from canopy or wall adhered. Three-dimensional relief letters in stainless steel or powder-coated metal. (Note: final retail signage is subject to retailer's requirements and will be reviewed by owner for consistency with building design).





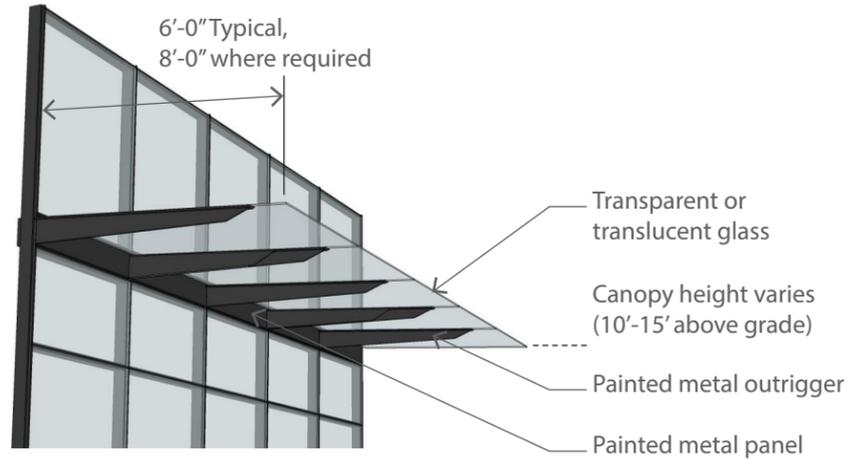
- 1 Entry/ Building Identity Sign
- 2 Retail Signage
- 3 Parking/ Loading Signage



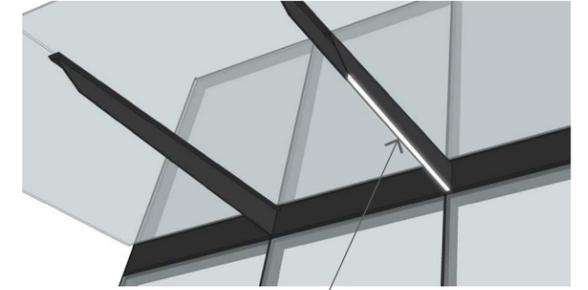
Systems and Details



All Glass Canopy



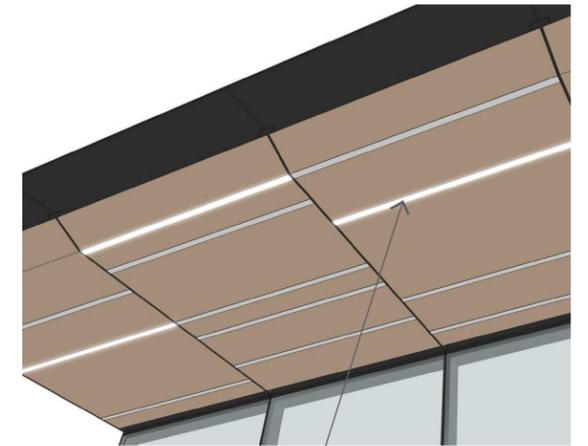
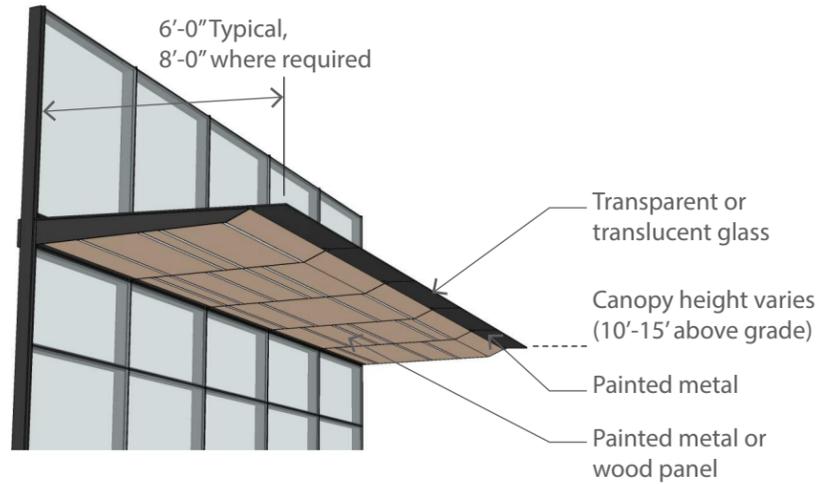
Note: Final *retail* canopy design is subject to retailer's requirements and will be reviewed by owner for consistency with building design.



Integrated LED fixture



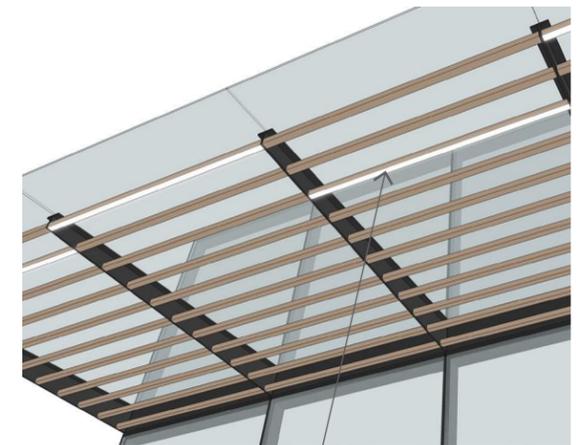
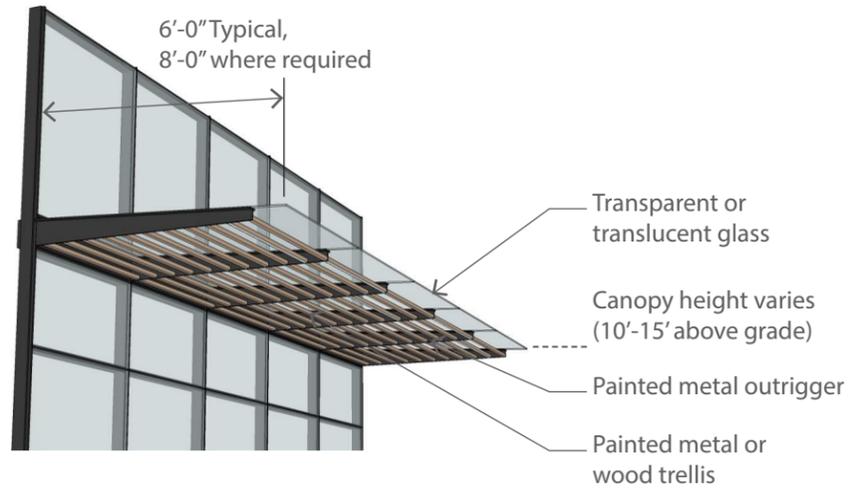
Opaque Canopy



Integrated LED fixture



Trellis Canopy



Integrated LED fixture

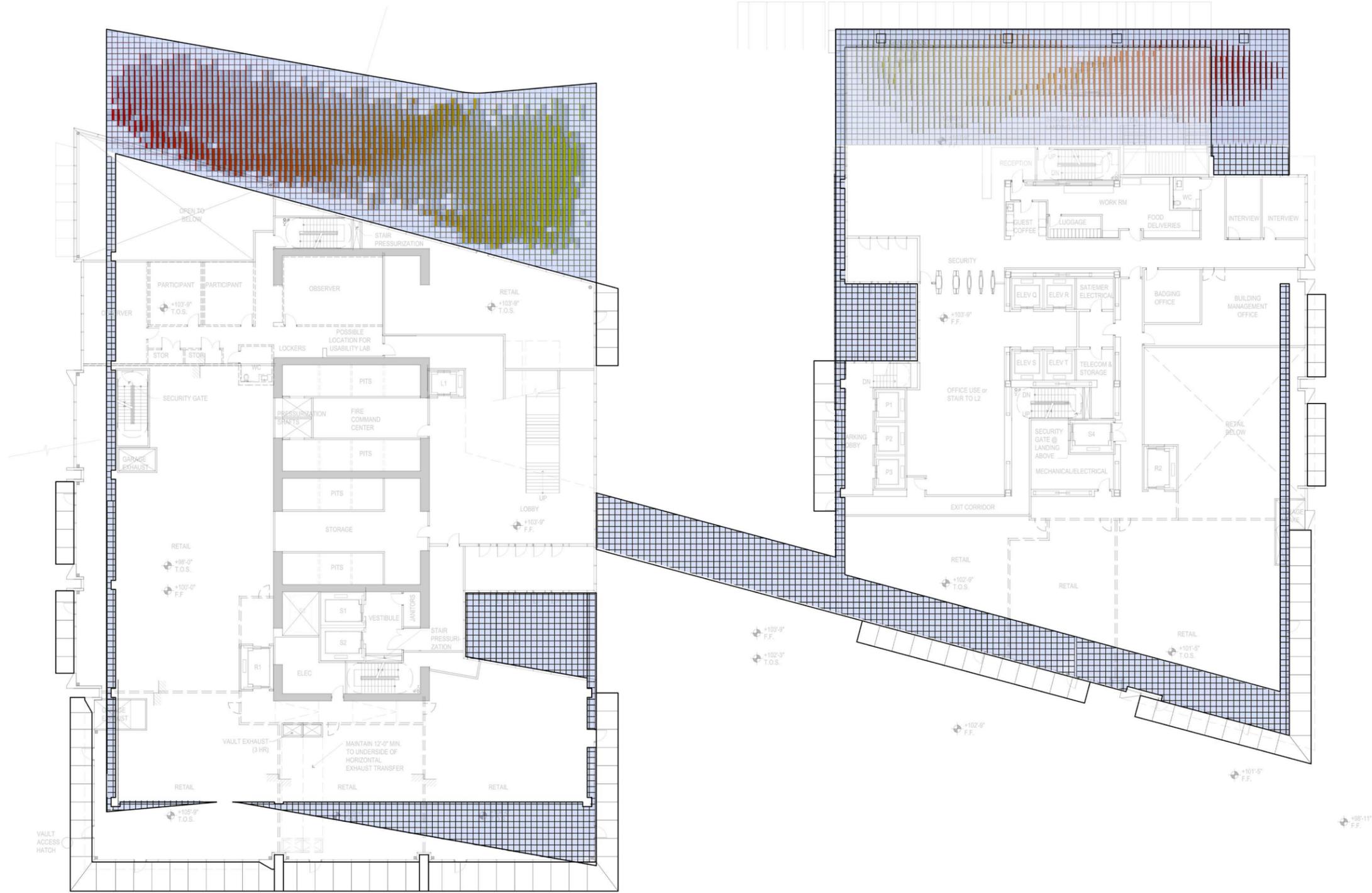


The overhead soffit at the 8th Avenue hill climb provides an opportunity for an engaging, dynamic experience for those ascending the pedestrian path along the through-block connection or passing the building along 8th Avenue and Bell Streets. Echoing the same dynamic, shifting character of the curtain wall, the soffit features a matrix of stainless steel panels “folded” in sequence to reveal a gradient of color beneath. The soffit also provides visual continuity along 8th Avenue, as the hill climb expression is repeated at the entry plaza at 8th and Blanchard.



8th Ave

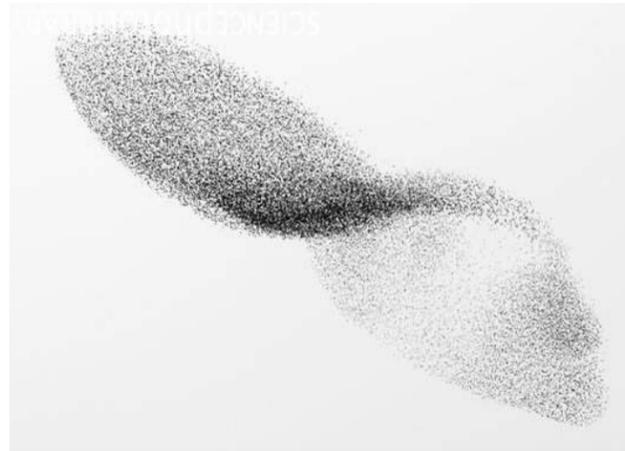
Bell St



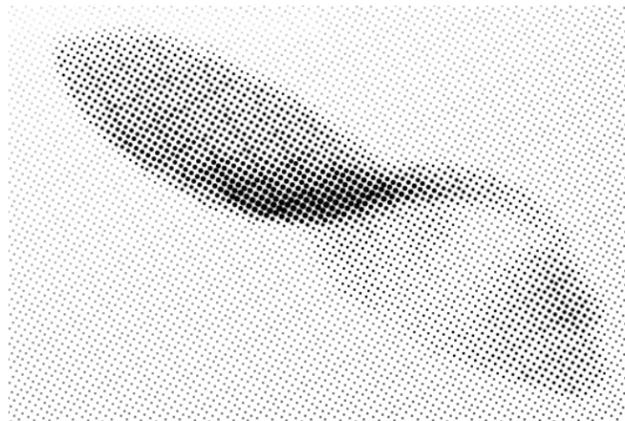
Blanchard St

7th Ave

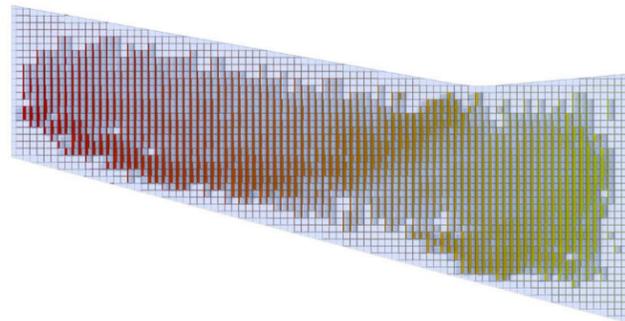




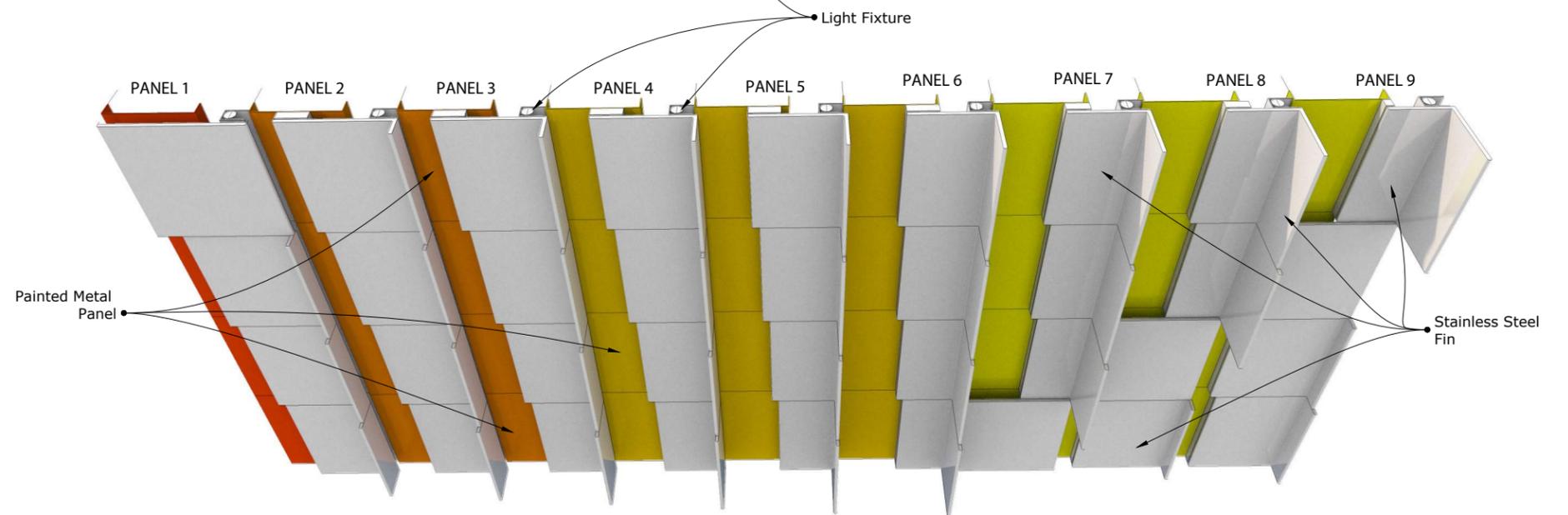
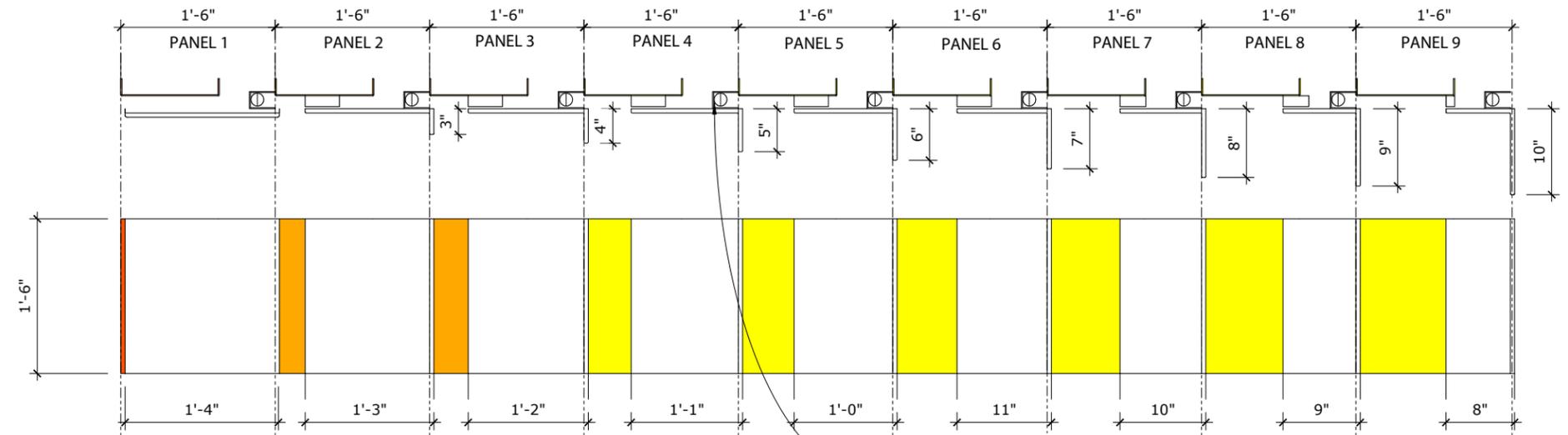
Murmuration



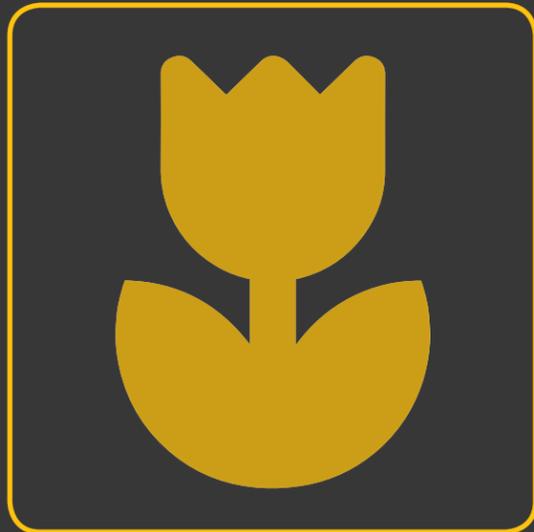
Matrix

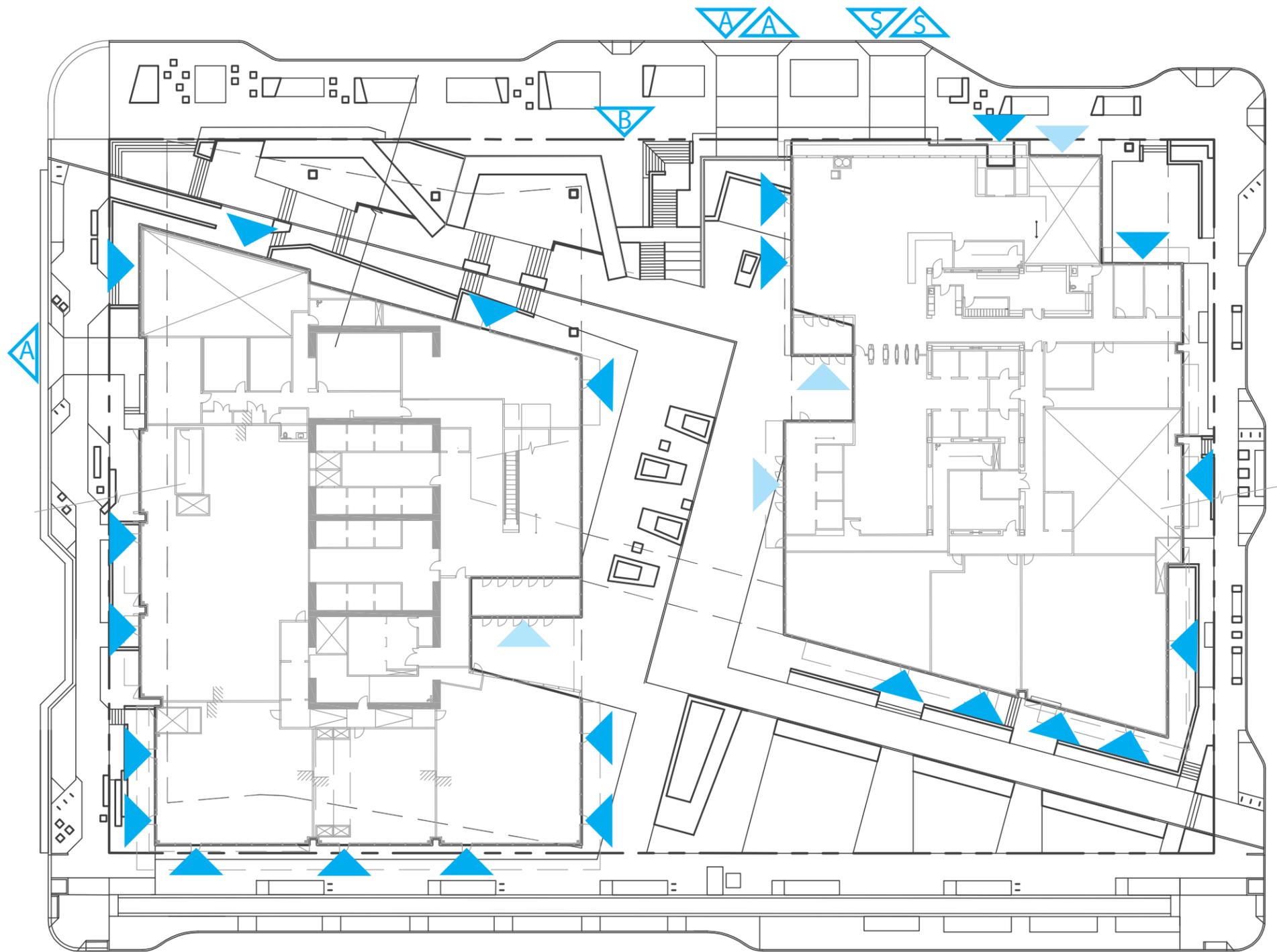


Execution



LANDSCAPE DESIGN

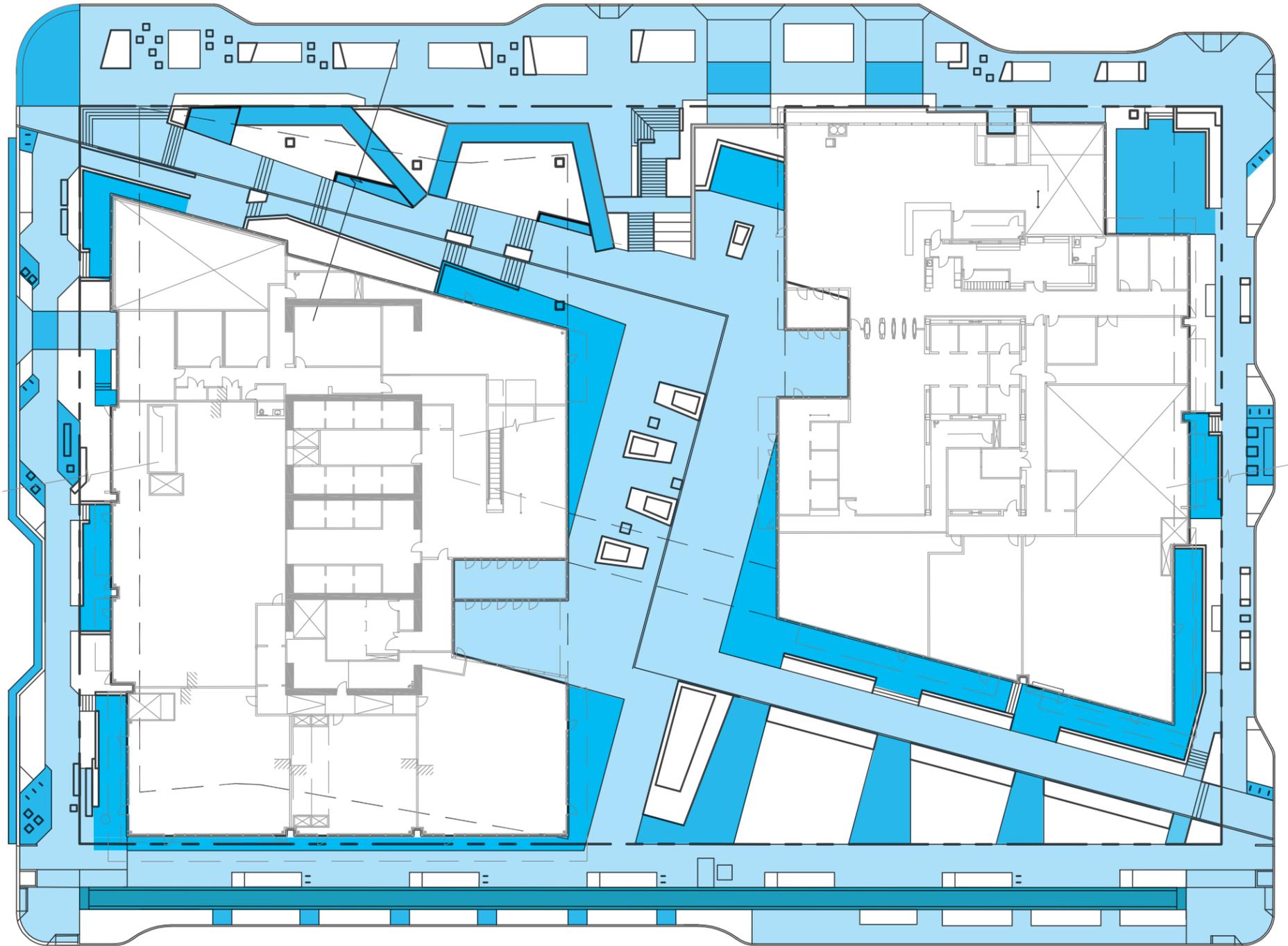




Legend

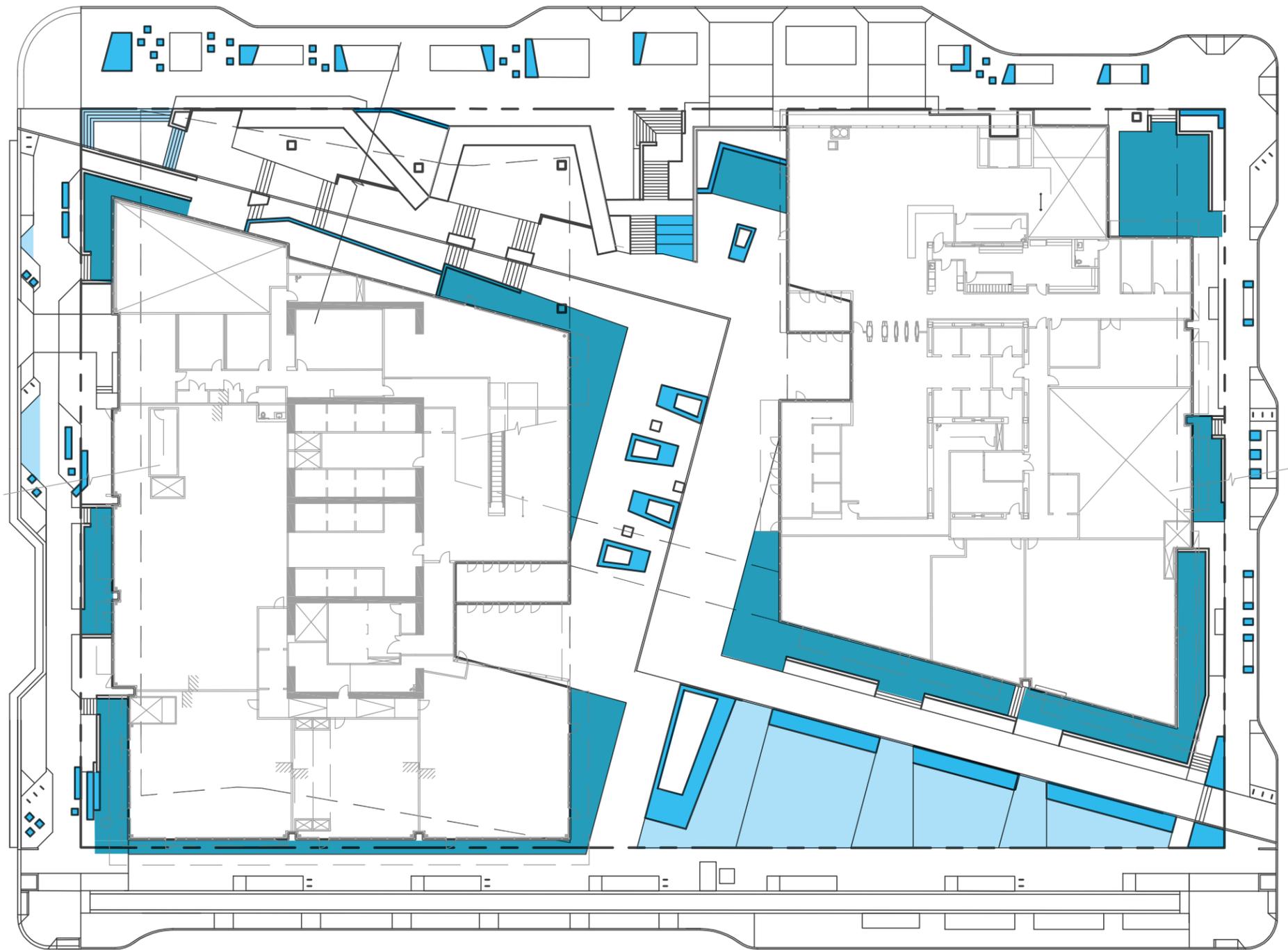
-  Retail
-  Lobby
-  Auto
-  Bike
-  Service

Landscape/Hardscape



Legend

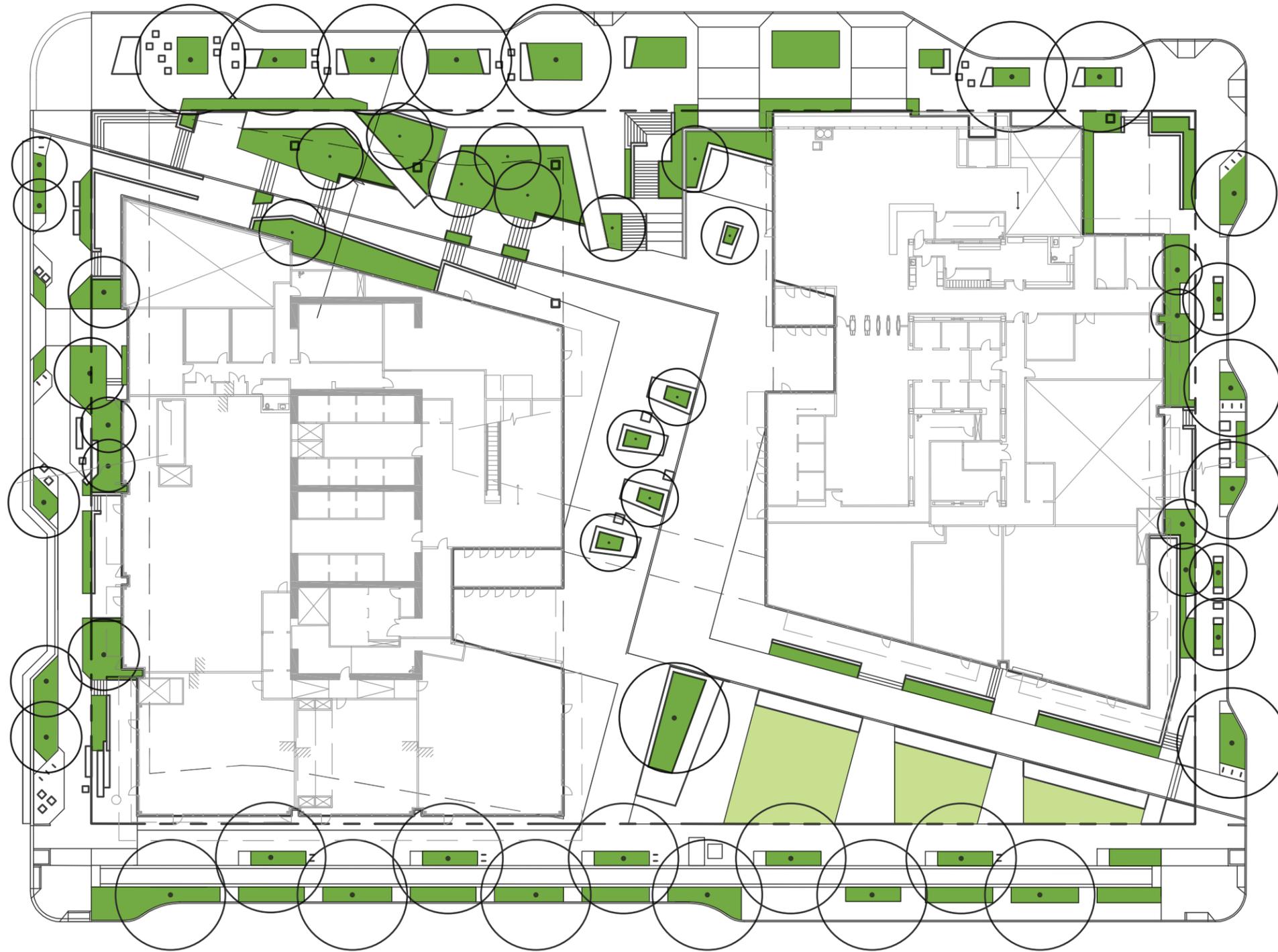
-  Concrete Paving
-  Accent Paving
-  Cycle Track



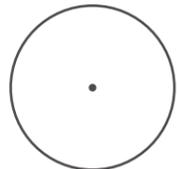
Legend

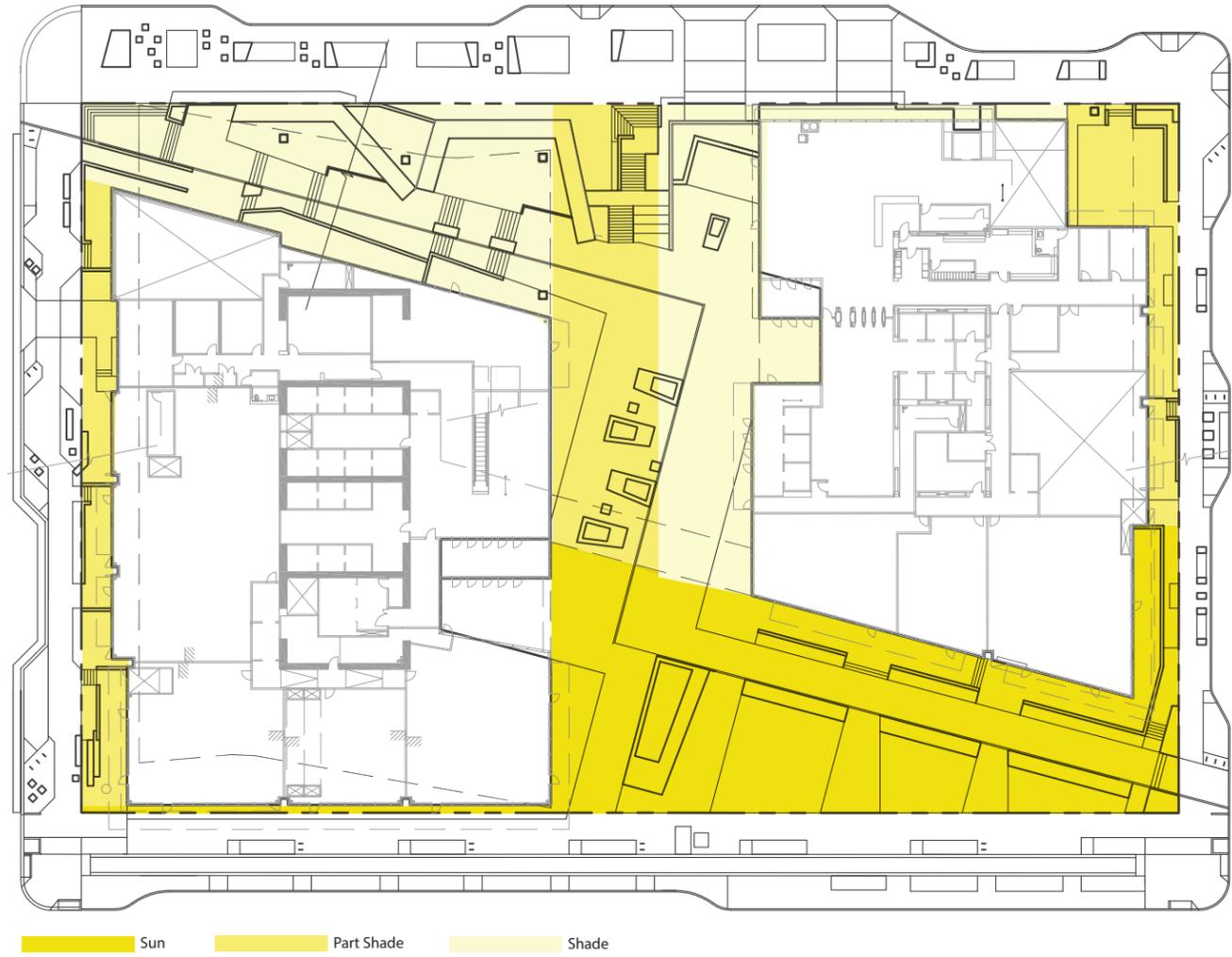
-  Flexible Seating Zones / Lawn
-  Seating Elements
-  Retail Seating

Landscape/Hardscape



Legend

-  Planting
-  Lawn
-  Trees



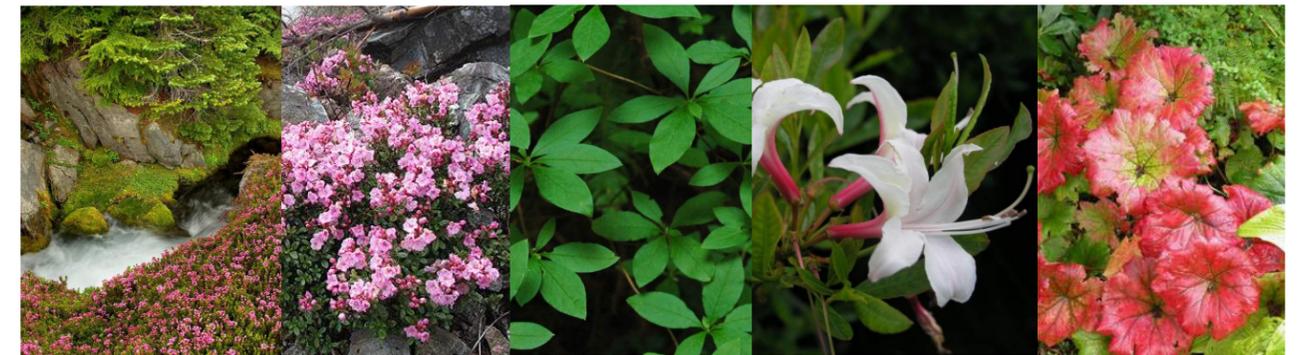
Quercus sadleriana

Eriogonum nudum

Quercus kelloggii

Calochortus tolmiei

Sun



Kalmiopsis leachiana

Menziesia ferruginea

Rhododendron occidentale

Darmera peltata

Part Shade



Ulmus propinqua 'JFS-Bieberich'

Cercidiphyllum japonicum

Ulmus 'Morton' Accolade

Stewartia pseudocamillia

Carpinus caroliniana

Street Trees



Cardamine californica

Thalictrum occidentale

Erythronium oregonum

Asarum marmoratum

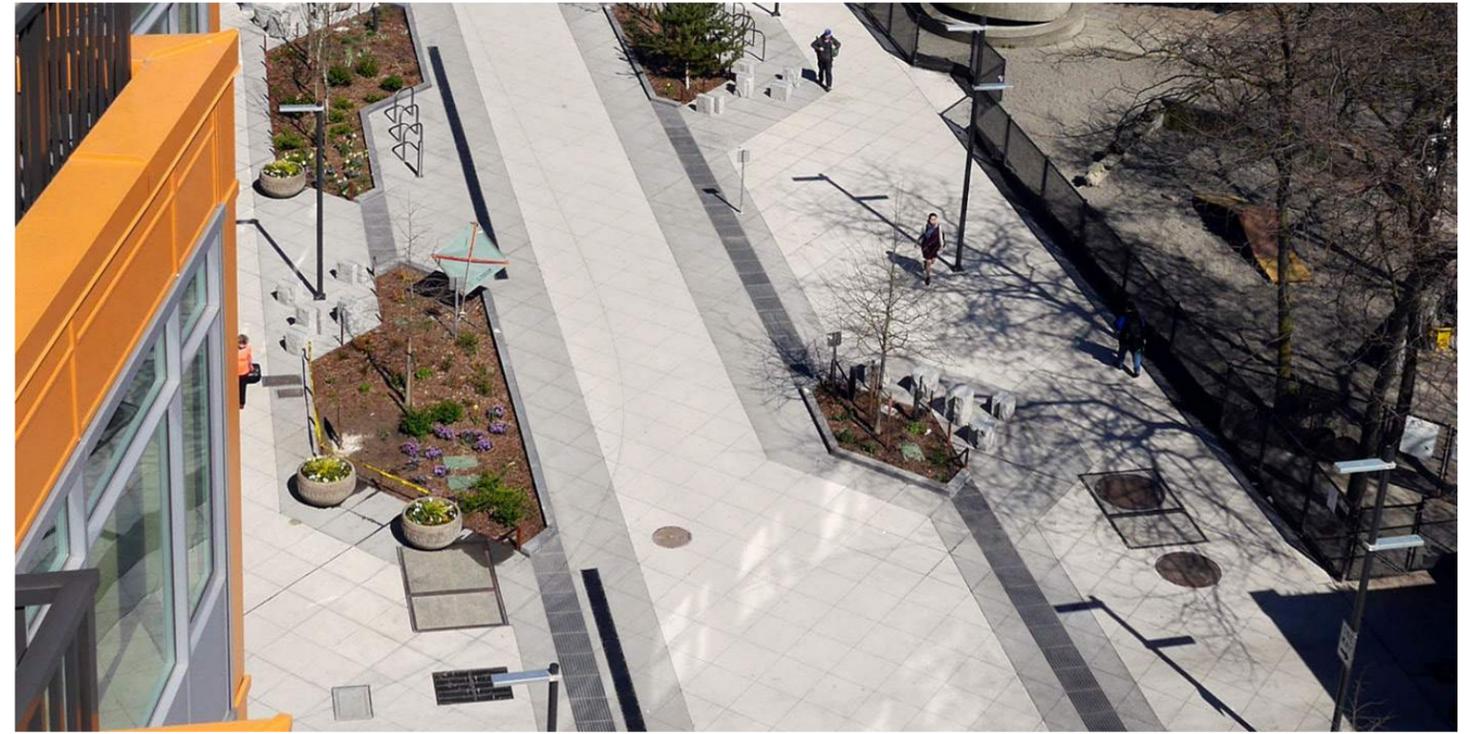
Woodwardia fimbriata

Shade

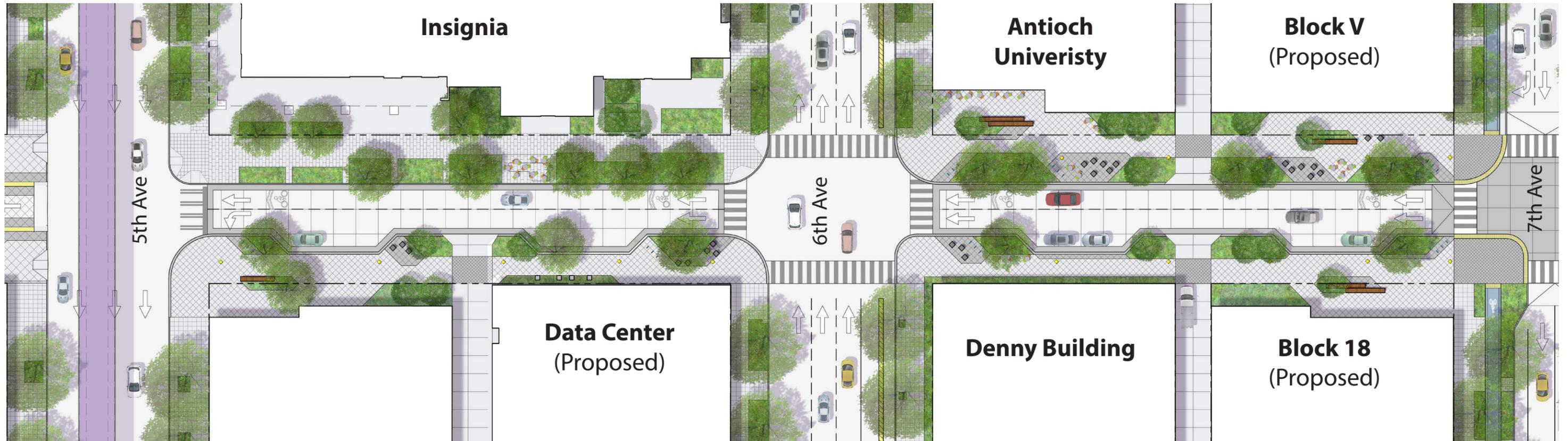
Landscape/Hardscape



Bell Street Park between 4th Ave. and 5th Ave. (looking NW).

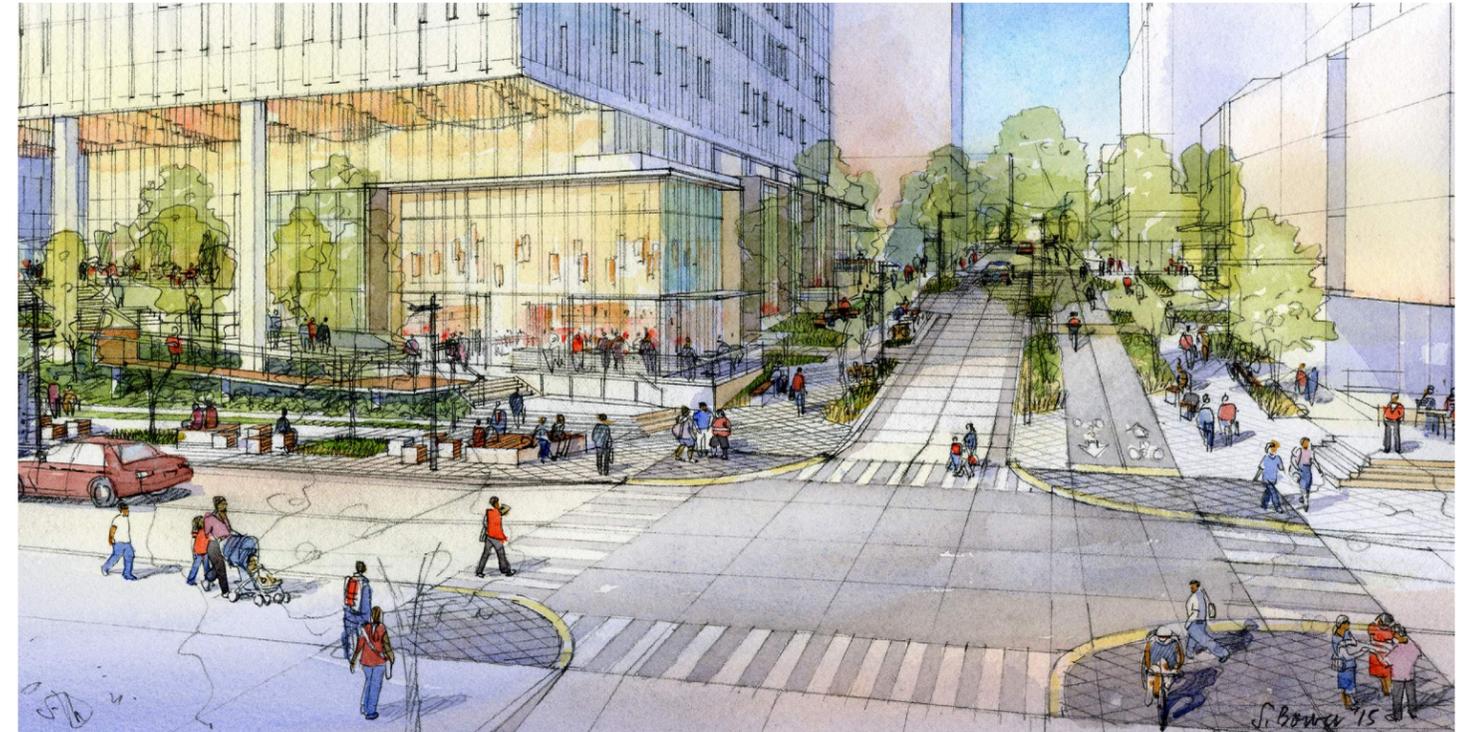


Bell Street Park

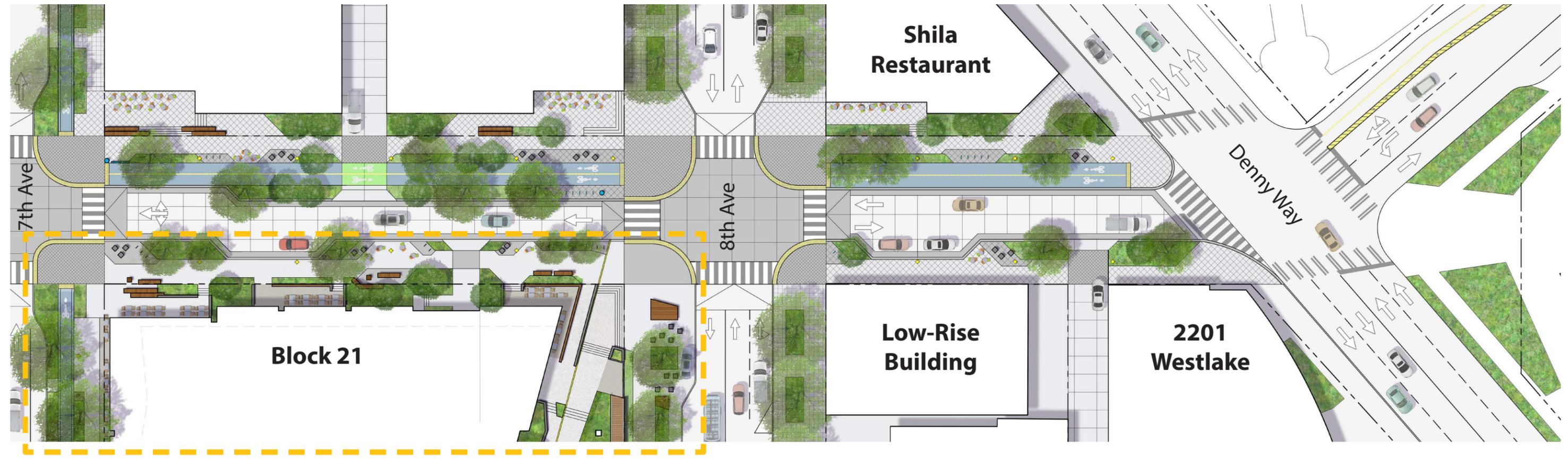




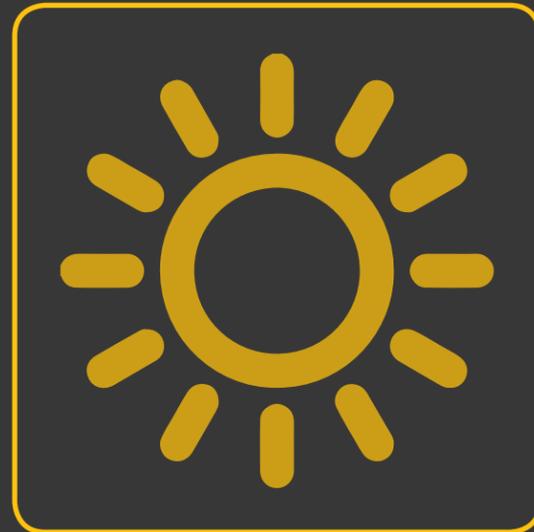
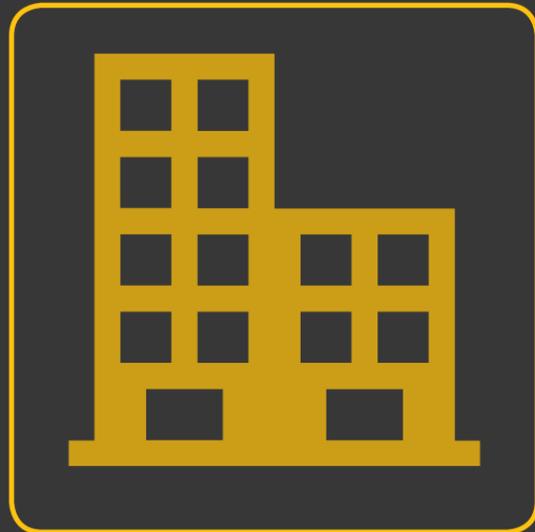
Proposed Bell street streetscape (looking SW towards 7th Ave.)

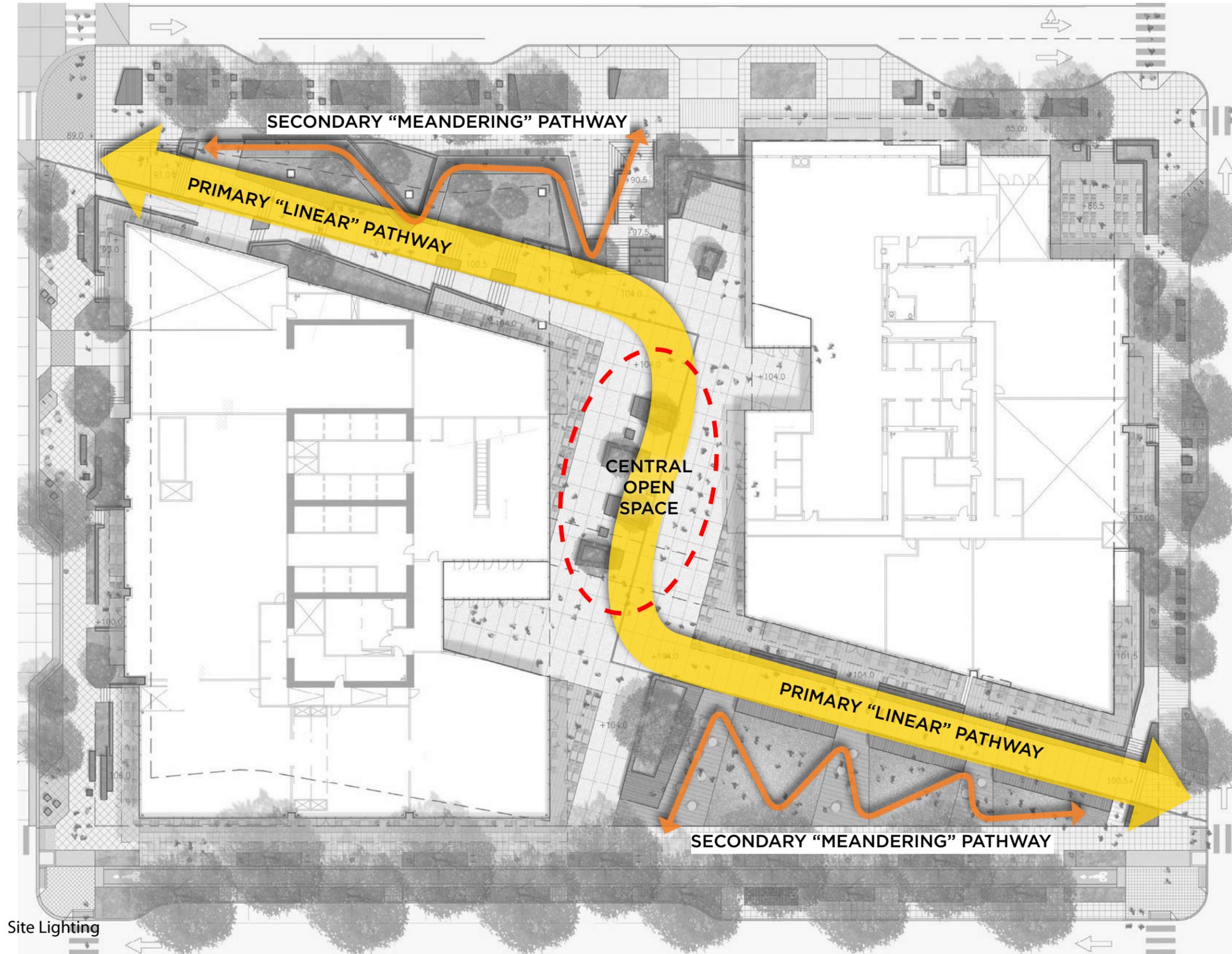


Proposed raised intersection at Bell St. and 8th Ave (looking SW towards 7th Ave.)



LIGHTING DESIGN



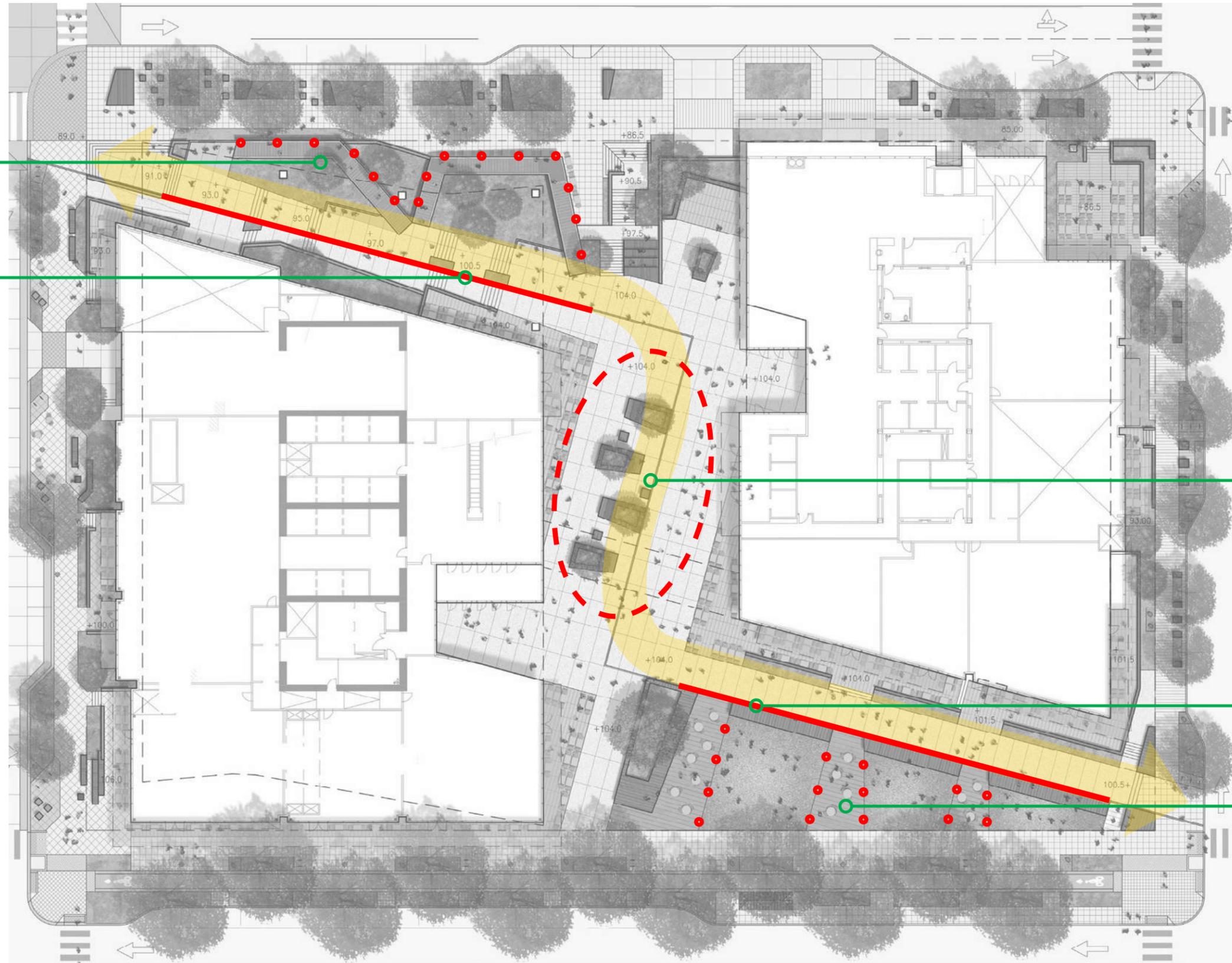


Site Lighting

Site Diagram

“MEANDERING” ELEMENTS THROUGHOUT BOARDWALKS AND SECONDARY PATHWAYS

“LINEAR” ELEMENT TO LEAD ALONG PRIMARY PATHWAY



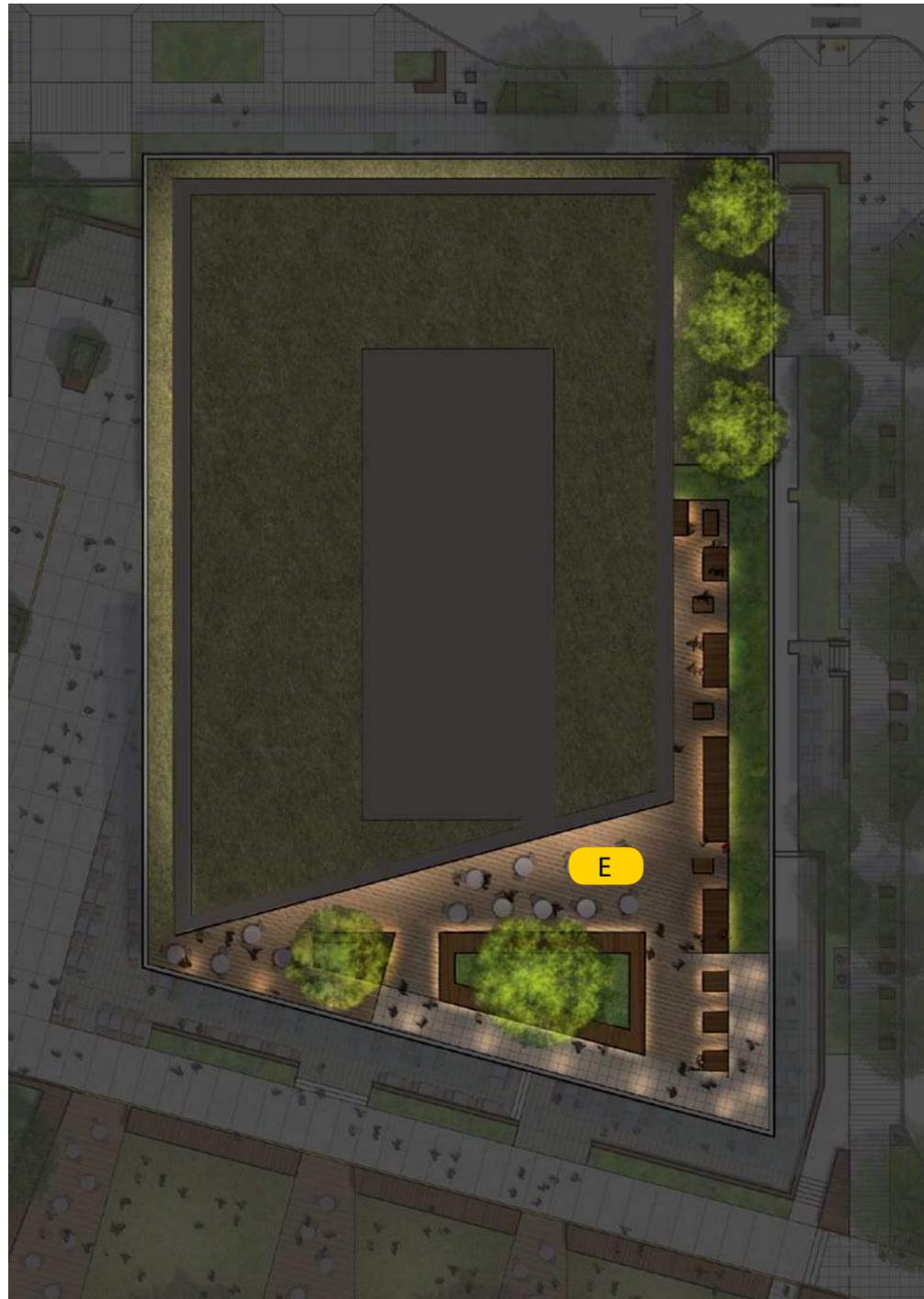
TALLER AMBIENT LIGHT FIXTURES AND UPLIT TREES AS “BEACON” IN CENTRAL OPEN SPACE

“LINEAR” ELEMENT TO LEAD ALONG PRIMARY PATHWAY

“MEANDERING” ELEMENTS THROUGHOUT PLAZA AND SECONDARY PATHWAYS



Site Lighting



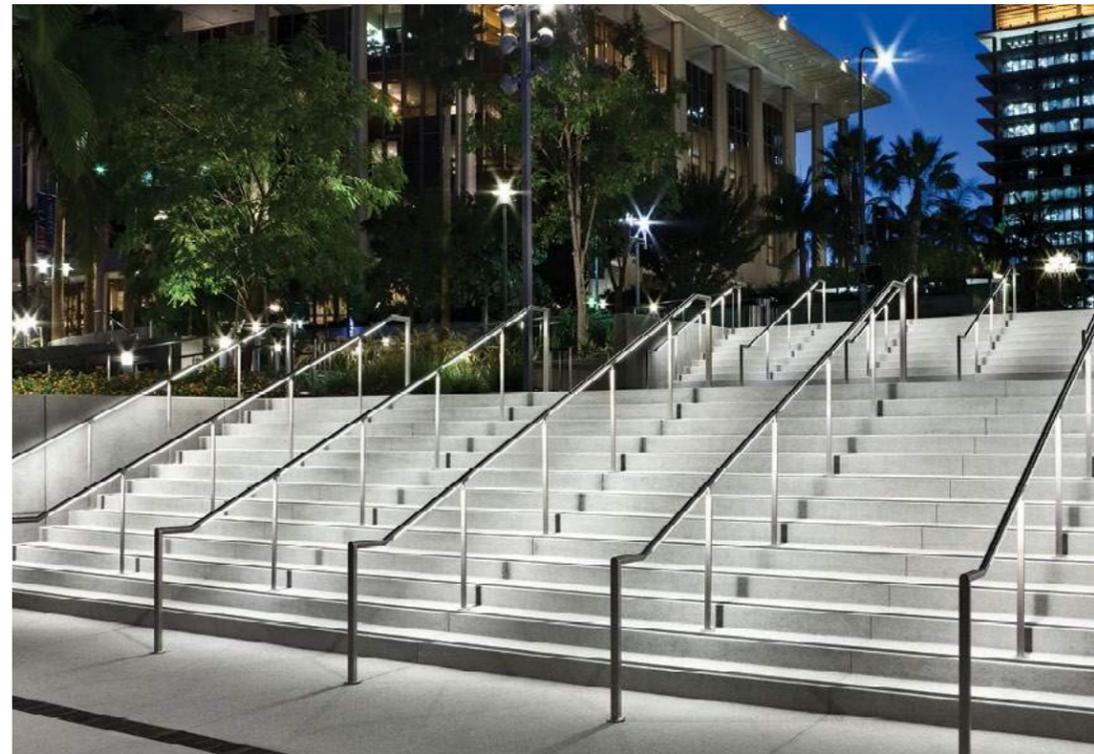
Level 8 Terrace Lighting

- A Primary Pathways (Hardscape)**
 Linear elements integrated in handrails, planters and benches direct the pathway towards the Central Open Space
- B Secondary Pathways (Landscape)**
 Low-level bollards provide illumination at the ground level boardwalks and plazas. Selective trees to be uplit for warm nighttime presence.
- C Central Open Space**
 Pedestrian-scale decorative post-tops both provide ambient light and their visual presence adds to the “gathering space” atmosphere. LED lighting integrated in planters and benches, as well as tree uplighting, provide a soft warm glow throughout
- D Canopies and Entries**
 Linear downlights recessed in the canopies draw focus to the entry points
- E Level 8 Terrace**
 Taking its cues from the ground level, the Level 8 terrace lighting will have consistent visual elements and light levels

Lighting Design

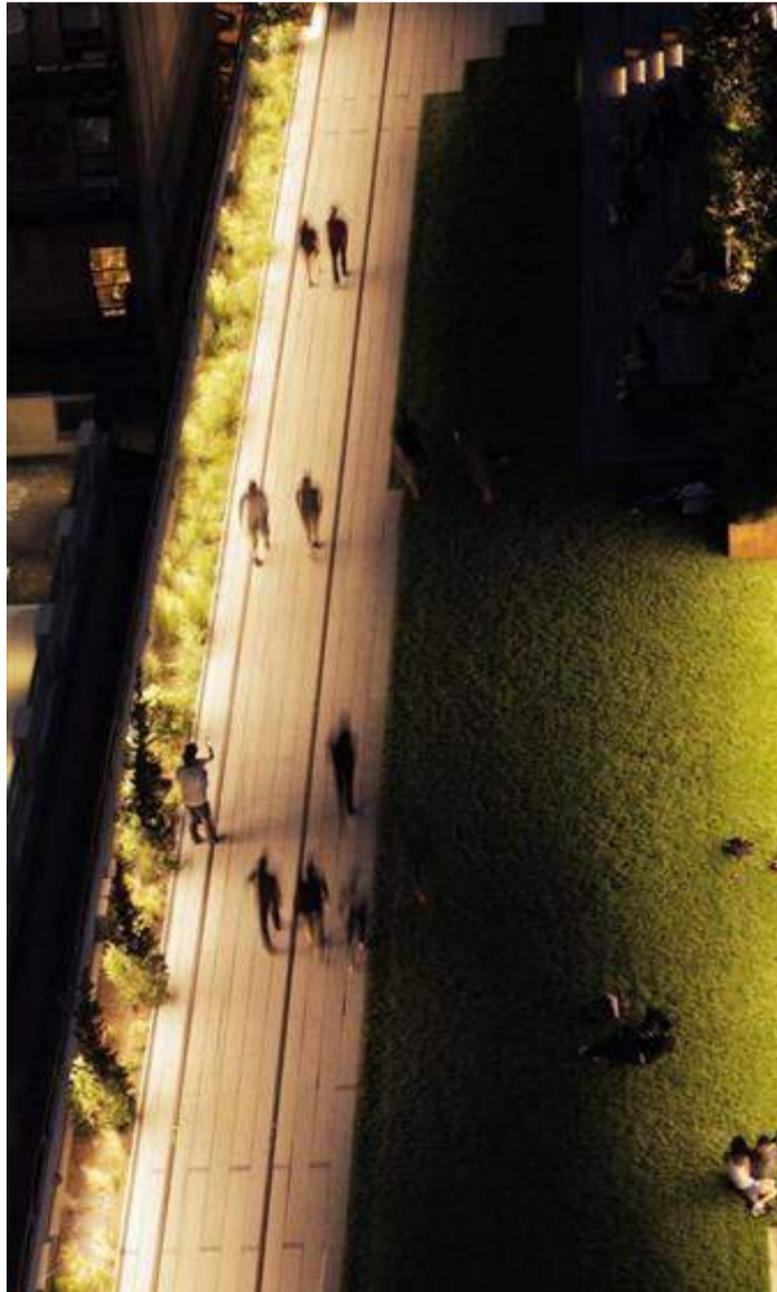
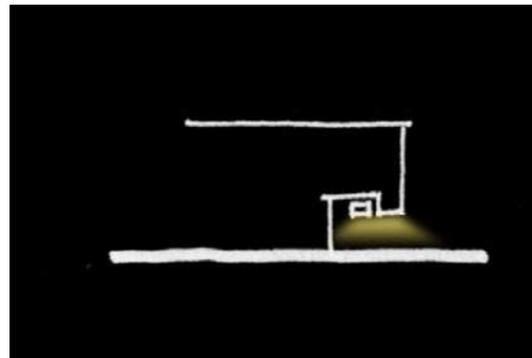


A LED lit handrails provide ambient light at the stairs and create a visual line along the 8th Street Hillside Climb towards the Central Open Space

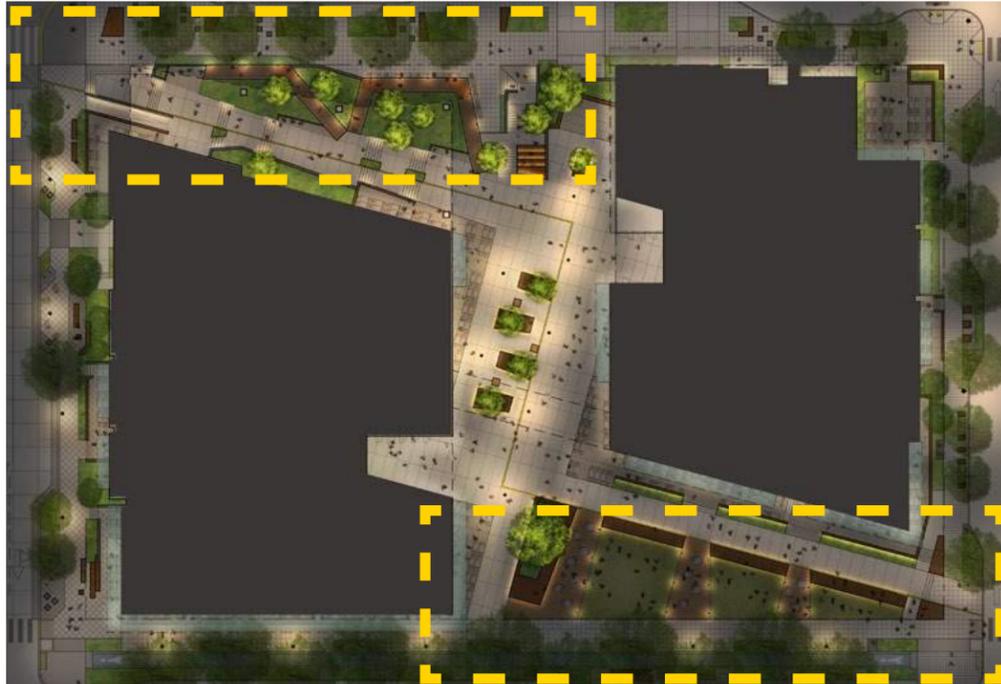




A LED striplight concealed underneath planters and benches at 7th Street Plaza also create a visual line along the path towards the Central Open Space

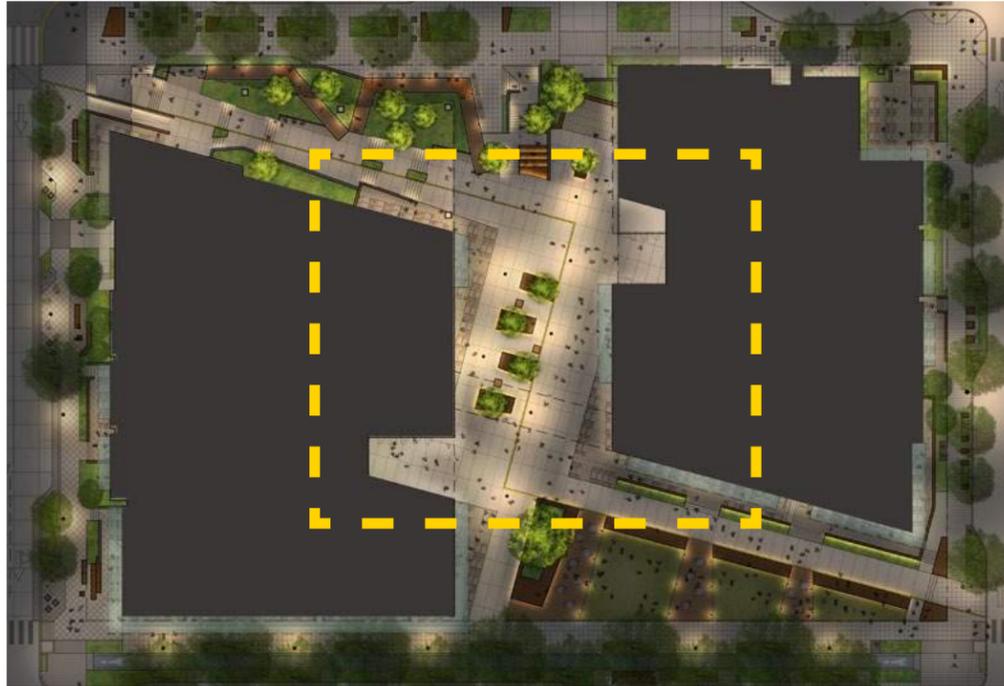


Lighting Design

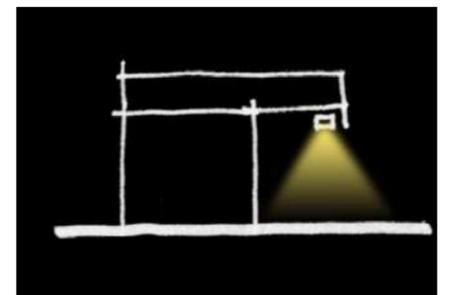
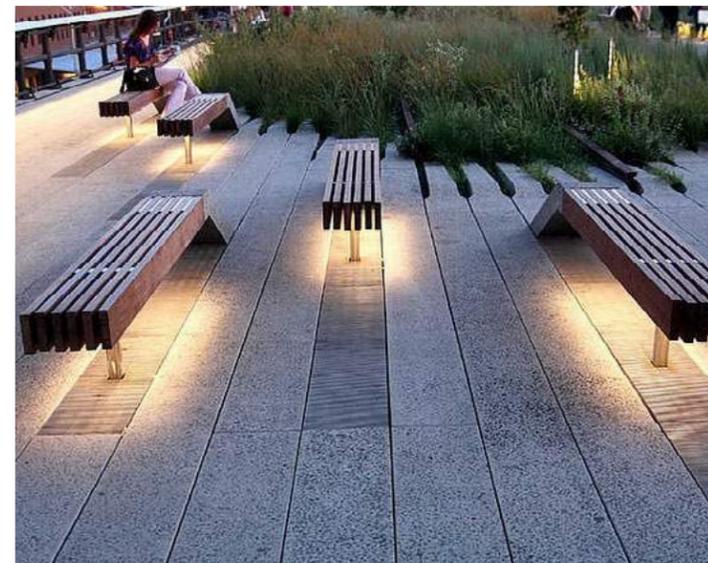


B Low-level bollards and steplights integrated in railing posts provide illumination at the ground level boardwalks and plazas

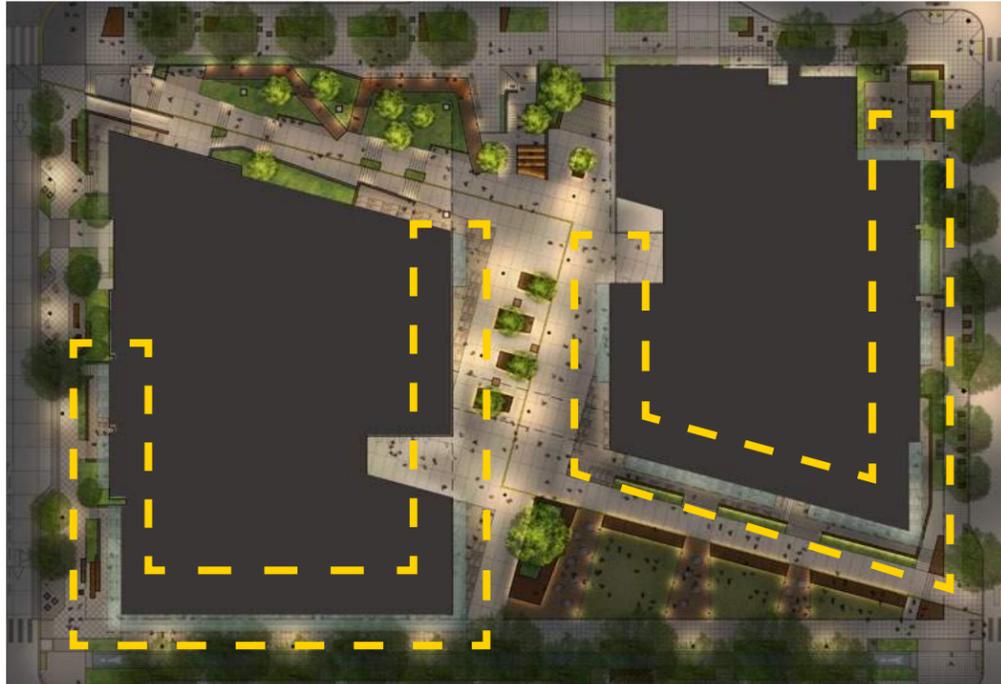




C Pedestrian-scale decorative post-tops both provide ambient light and their visual presence gives the Central Open Space an inviting atmosphere. LED lighting underneath bench seating and tree uplights also add to the “gathering space” ambiance.



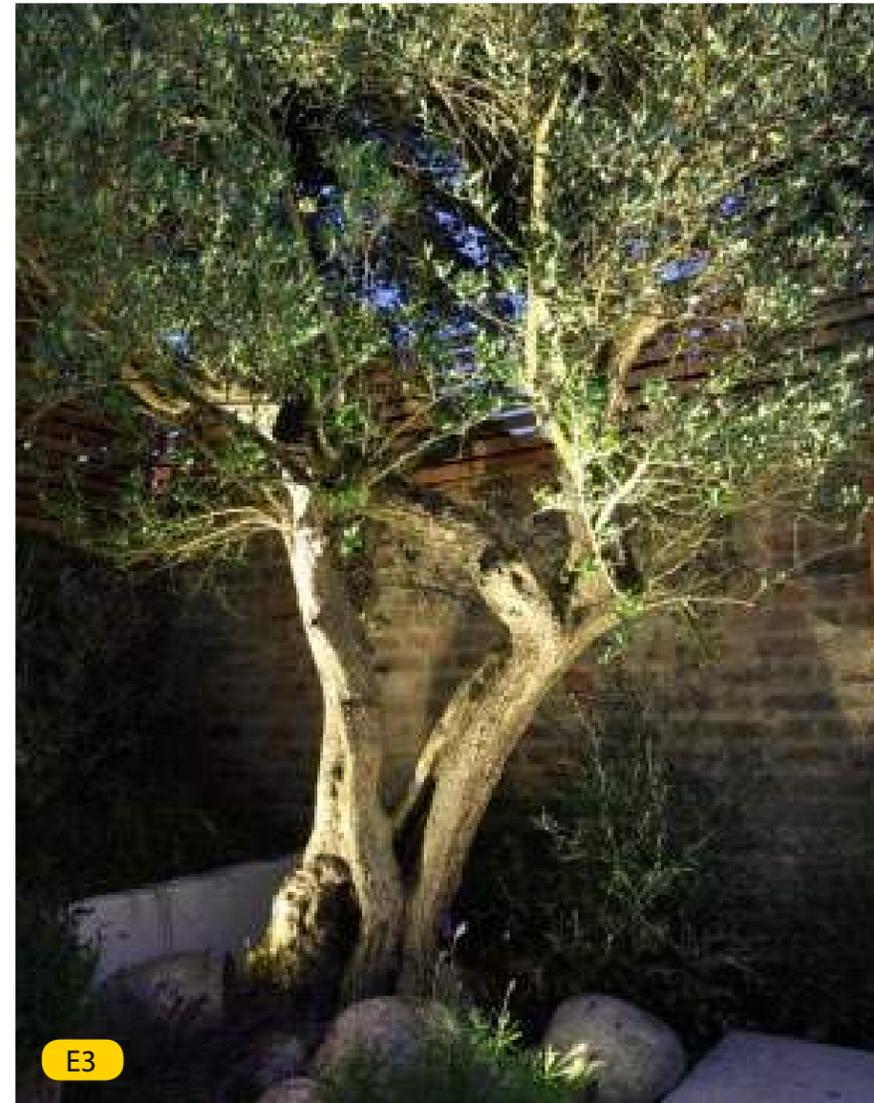
Lighting Design



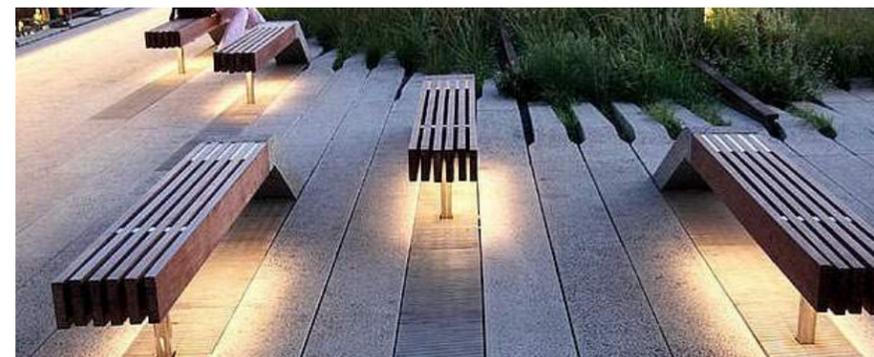
D

Linear recessed downlights integrated in the canopies highlight the architectural structure of the canopies and draw focus to the entry points

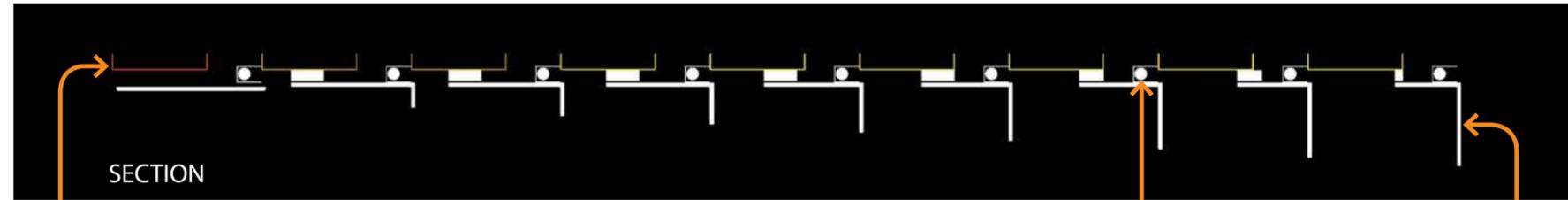




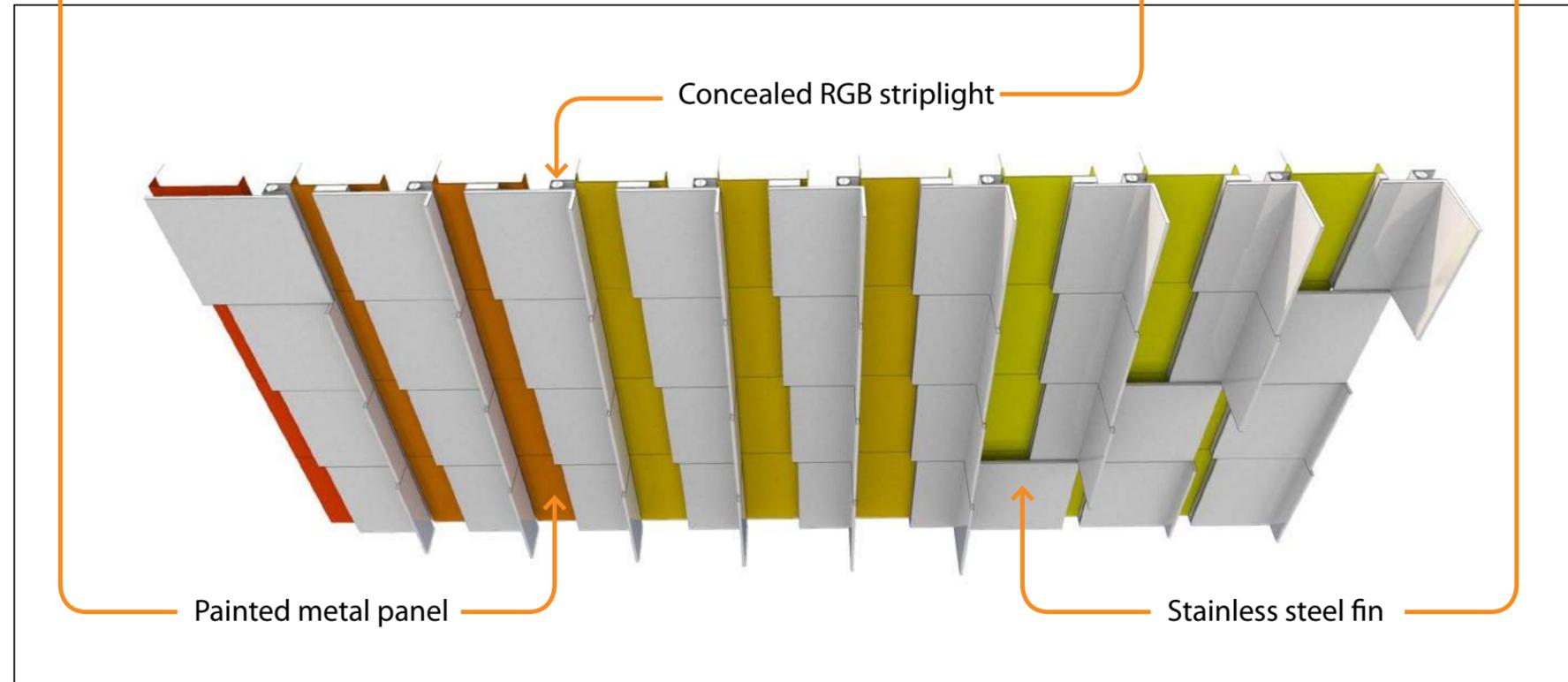
- E1** LED lighting underneath bench seating and tree uplights add to the “gathering space” ambiance.
- E2** Low-level bollards or steplights integrated in railing posts provide illumination at the perimeter
- E3** Uplights at the trees give the landscape a warm glow and soft nighttime presence



Lighting Design

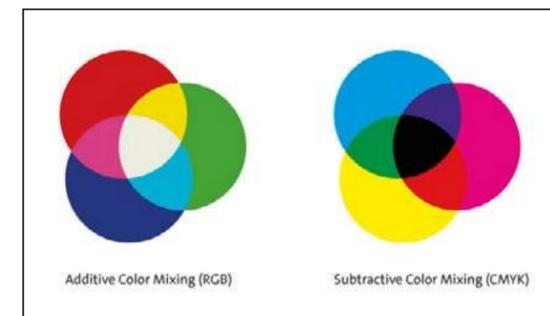


Section View

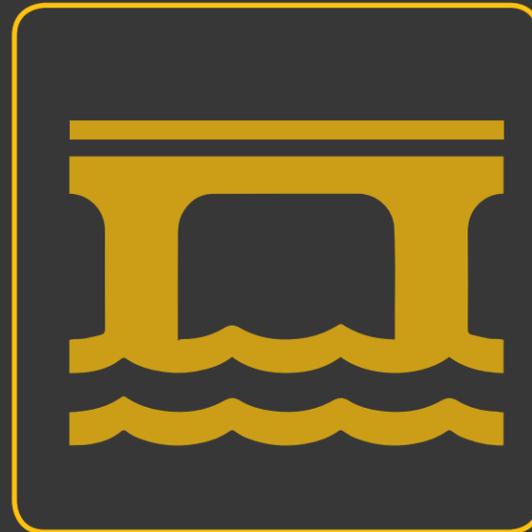
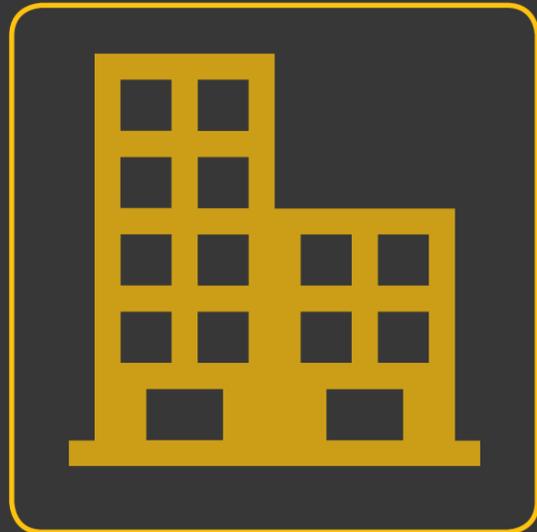


Perspective View

COLOR OF LIGHT	ON A CYAN SURFACE (- R) (absorbs and "negates" Red)	ON A MAGENTA SURFACE (- G) (absorbs and "negates" Green)	ON A YELLOW SURFACE (- B) (absorbs and "negates" Blue)
WHITE (R + G + B)			
CYAN (G + B)			
MAGENTA (R + B)			
YELLOW (R + G)			



DEVELOPMENT DEPARTURES



DEPARTURE REQUEST #2

DEVELOPMENT STANDARD

23.49.056 Street Facade, Landscaping, and Street Setbacks

REQUIREMENT

B. Facade Setback Limits

2. General Setback Limits. The following setback limits apply on streets not requiring property line facades, as shown on Map 1H:
 - b. The maximum area of all setbacks between the street lot line & facade along each street frontage of a lot shall not exceed the area derived by multiplying the averaging factor by the width of the street frontage of the structure along that street (see Exhibit D for 23.49.056). The averaging factor is five on Class I pedestrian streets and ten on Class II pedestrian streets and designated green streets.

DEPARTURE AMOUNT REQUIRED

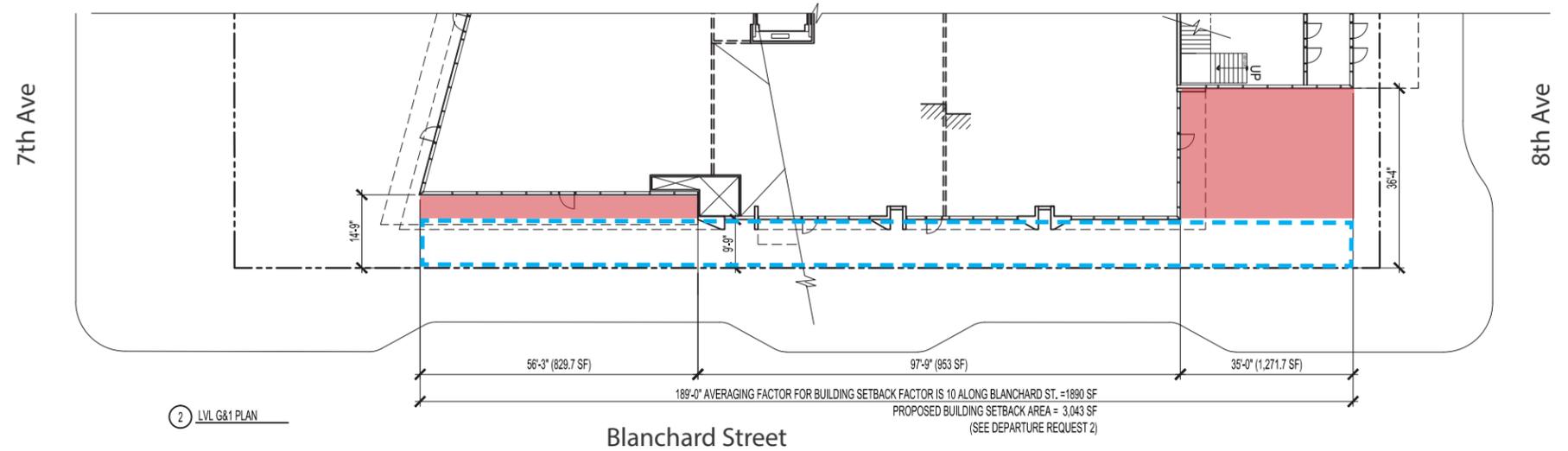
See adjacent illustrative diagrams

2. The façade setback parallel to Bell Street varies from 10' to 36'-4" for a distance of 189'-0" feet. Maximum setback area is 1,890 sf; Proposed is 3,043 sf.

RATIONALE

2. The corner of Blanchard Street and 8th Avenue, where the greatest building setback from Blanchard Street is proposed, is enhanced and supported by a more gracious zone between the public right-of-way and the building. The setback also provides for corner entries into the office lobby and retail, enhancing street level active uses.

APPLICABLE DESIGN GUIDELINES:
CS2-C.1, CS3-A.4, DC2-B.1, DC2-D.1, DC3-B.3, DC3-C.1



DEPARTURE REQUEST #2

- Allowable setback area
- Departure area

DEPARTURE REQUEST #3

DEVELOPMENT STANDARD

23.49.056 Street Facade, Landscaping, and Street Setbacks

REQUIREMENT

B. Facade Setback Limits

2. General Setback Limits. The following setback limits apply on streets not requiring property line facades, as shown on Map 1H:
 - c. The maximum width...of any setback area exceeding a depth of 15' from the street lot line shall not exceed 80', or 30% of the lot frontage on that street, whatever is less.

DEPARTURE AMOUNT REQUIRED

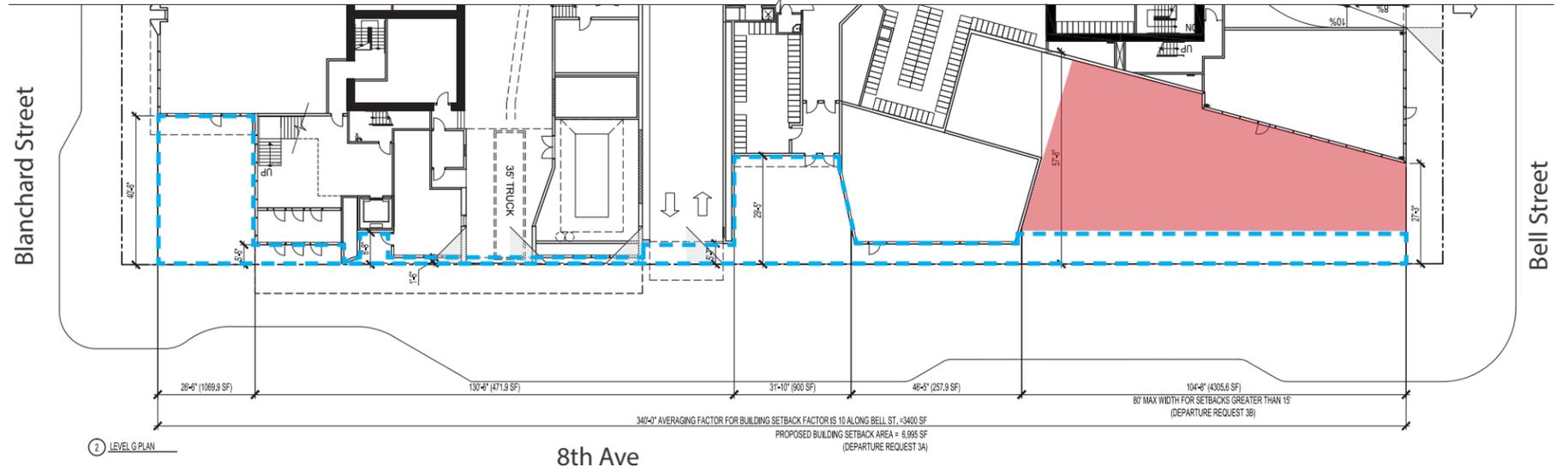
See adjacent illustrative diagrams

- 3A. Maximum setback area along 8th AVE is 3,060 SF; Proposed is 6,995 SF
- 3B. Allow for a section of facade 104'-5" in width that is that is setback more than 15' & is greater than the 80' max width.

RATIONALE

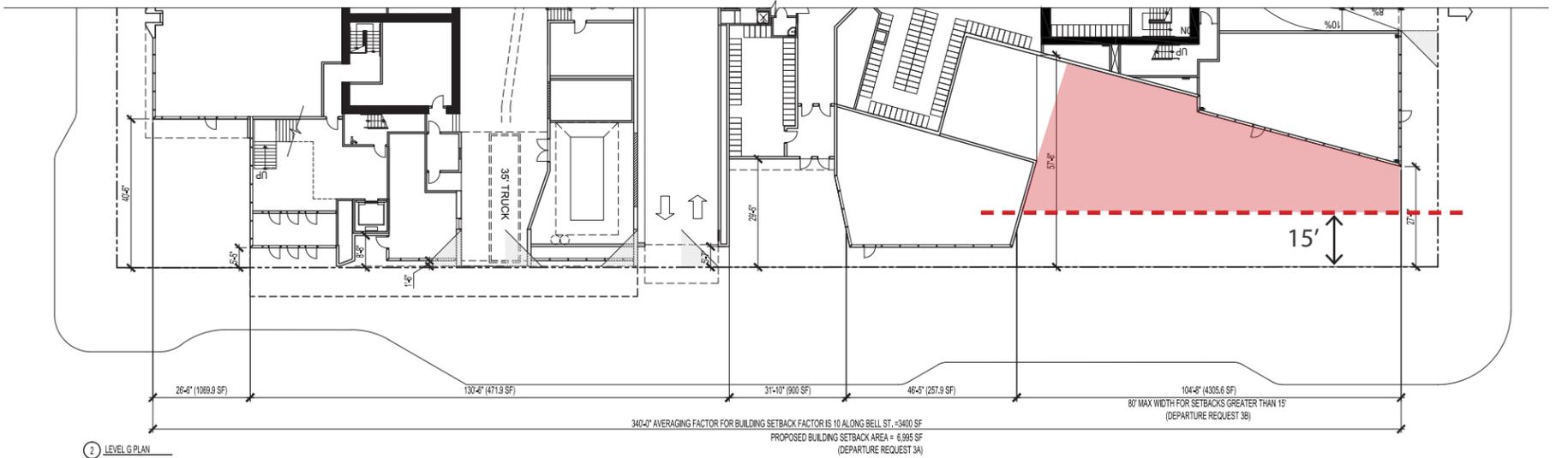
- 3A. The corner of 8th Avenue and Bell Street, where the greatest building setback from 8th Avenue is proposed, is enhanced and supported by a more gracious zone between the public right-of-way and the building. The setback also provides for a sidewalk-level transition to the through-block connection, enhancing pedestrian circulation.
- 3B. The corner of 8th Avenue and Bell Street, where the greatest building setback from 8th Avenue is proposed, is enhanced and supported by a more gracious zone between the public right-of-way and the building. The setback also provides for a sidewalk-level transition to the through-block connection, enhancing pedestrian circulation.

APPLICABLE DESIGN GUIDELINES:
CS1-D.1, CS2-C.1, CS2-B.2, PL1-A.1, DC3-A.1, DC3-D.4



DEPARTURE REQUEST #3A

- Allowable setback area
- Departure area



DEPARTURE REQUEST #3B

DEPARTURE REQUEST #4, 5 & 6

DEVELOPMENT STANDARD

23.49.056 Street Facade, Landscaping, and Street Setbacks

REQUIREMENT

B. Facade Setback Limits

2. General Setback Limits

- d. The maximum setback of the facade from the street lot lines at intersection is 10'. The minimum distance the facade must conform to this limit is 20' along each street.

DEPARTURE AMOUNT REQUIRED

See adjacent illustrative diagrams

- 4. The façade parallel to Blanchard Street sets back 26'-3" from the property line. The façade parallel to 8th Avenue sets back 40' from the property line.
- 5. The façade parallel to Bell Street sets back 10'-0" from the property line. The façade parallel to 8th Avenue sets back 27'-6" from the property line.
- 6. The façade parallel to Bell Street sets back 15'-0" from the property line.

RATIONALE

- 4. The Green Street at this corner is enhanced and supported by a more gracious zone between the public right-of-way and the building. The setback also provides for corner entries into the office lobby and retail, enhancing street level active uses.
- 5. The Green Street at this corner is enhanced and supported by a more gracious zone between the public right-of-way and the building. The setback also provides for a sidewalk-level transition to the through-block connection, enhancing pedestrian circulation.
- 6. The Green Street at this corner is enhanced and supported by a more gracious zone between the public right-of-way and the building. The corner setback provides for an outdoor seating terrace adjacent to retail, enhancing active street level uses.

APPLICABLE DESIGN GUIDELINES:
CS2-C.1, CS3-A.4



DEPARTURE REQUEST #4, 5 & 6

DEPARTURE REQUEST #7

DEVELOPMENT STANDARD

23.49.056 Street Facade, Landscaping, and Street Setbacks

REQUIREMENT

D. Blank Facade Limits

2. Blank facade limits for Class I Pedestrian & designated Green Streets

- a. Blank facades shall be no more than 15' wide except segments with garage doors may exceed a width of 15' and may be as wide as the driveway plus 5'.
- c. The total width of all blank facade segments, including garage doors, shall not exceed 40% of the street-facing facade of the structure on each street frontage.

DEPARTURE AMOUNT REQUIRED

See adjacent illustrative diagrams

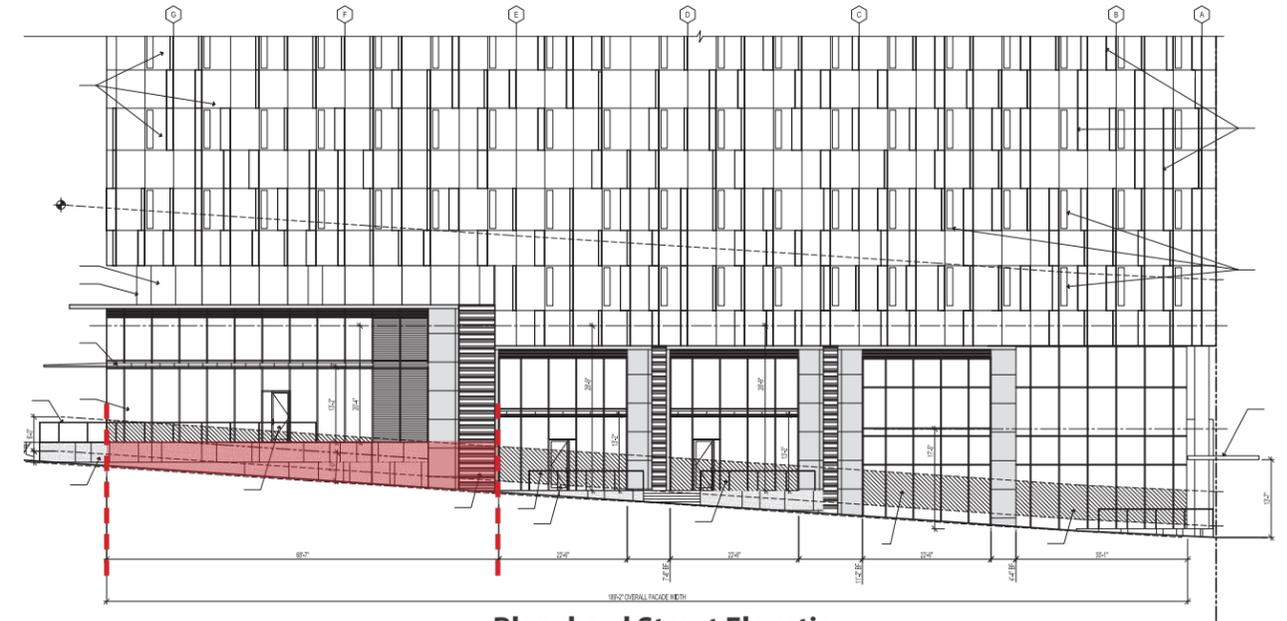
- 7A. Allow for a blank façade of 68'-7" on Blanchard Street
- 7B. Allow for a non-transparent facade area of 40.9% on Blanchard Street

RATIONALE

7A. The extent of blank façade proposed occurs at a grade transition that facilitates an outdoor seating zone adjacent to retail, activating the streetscape along the Green Street. Retail finish floor elevation is set to provide unobstructed ADA access to the public plaza at the corner of 7th & Blanchard.

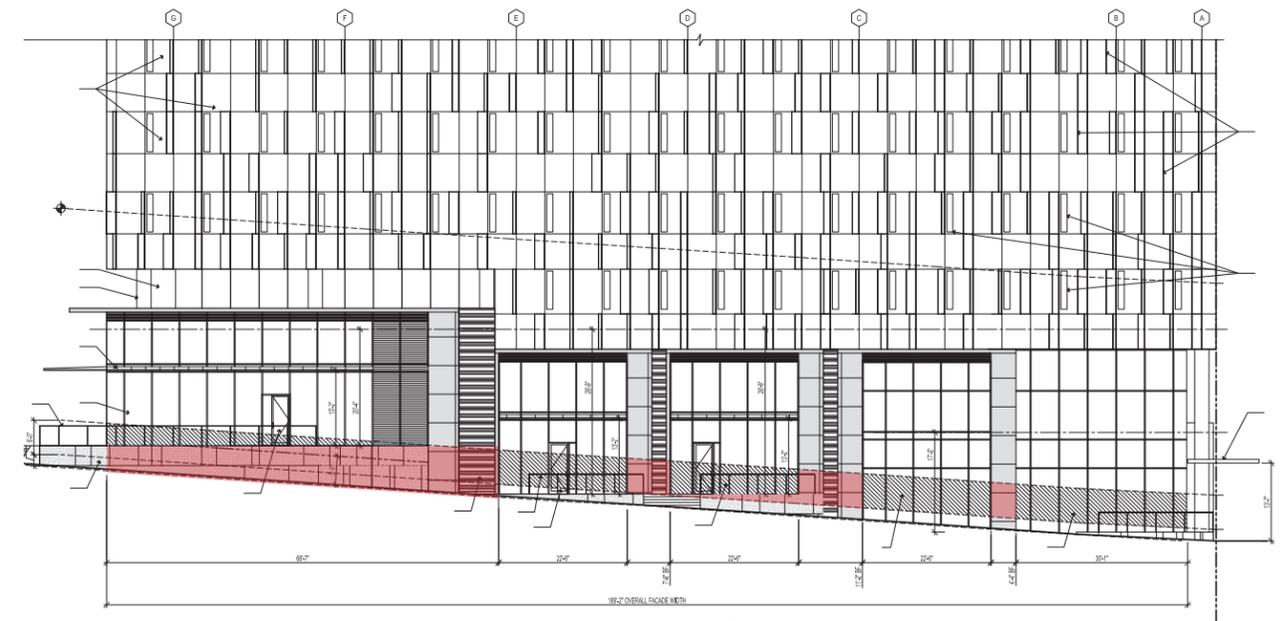
7B. A majority of the non-transparent façade occurs at a retail terrace which is adjacent to the 7th Ave plaza, requiring the finish floor elevation to match the plaza for public access.

APPLICABLE DESIGN GUIDELINES:
CS2-B.2, PL1-B.3, PL1-C.1



Blanchard Street Elevation

DEPARTURE REQUEST #7a



Blanchard Street Elevation

DEPARTURE REQUEST #7b

DEPARTURE REQUEST #8

DEVELOPMENT STANDARD

23.49.056 Street Facade, Landscaping, and Street Setbacks

REQUIREMENT

D. Blank Facade Limits

3. Blank facade limits for Class II Pedestrian Streets

- a. Blank facade segments shall be no more than 30' wide, except for garage doors, which may exceed 30'.

DEPARTURE AMOUNT REQUIRED

See adjacent illustrative diagrams

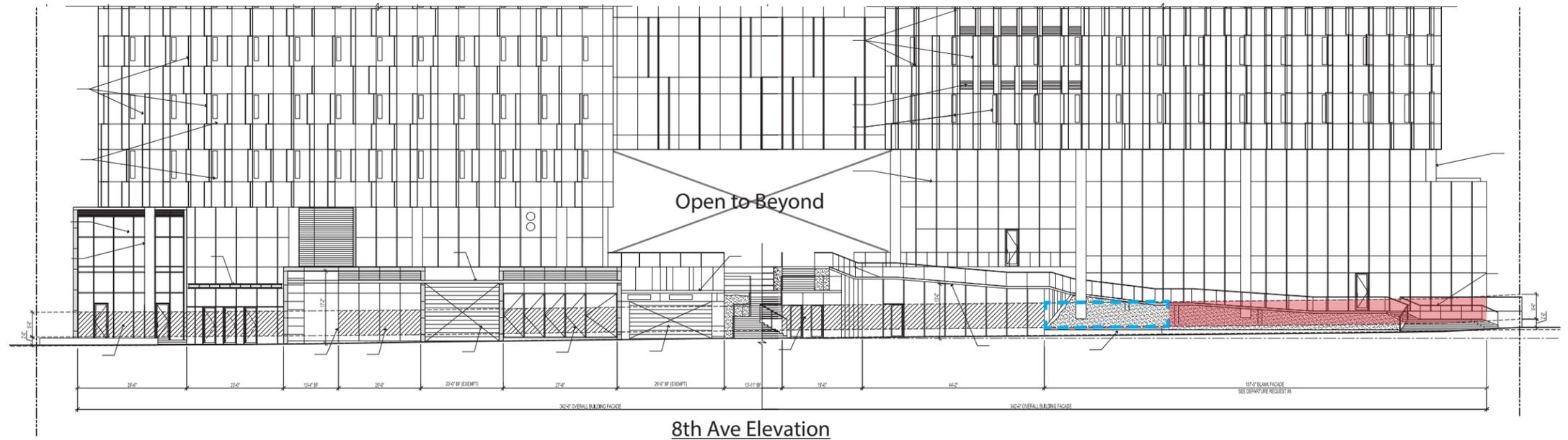
- 8. Allow for a blank façade segment of 107'-5" on 8th Ave (77'-5" greater than the allowable)

RATIONALE

- 8. The extent of the blank façade segment proposed occurs at a grade transition that facilitates a hill climb through-block connection and provides a variety of landscape features, an ADA accessible route to the elevated mid-block plaza, and pedestrian seating.

APPLICABLE DESIGN GUIDELINES:
CS2-B.2, PL1-B.3, PL1-C.1, DC4-D.4

 Allowable blank facade width
 Departure area



DEPARTURE REQUEST #8

DEPARTURE REQUEST #9

DEVELOPMENT STANDARD

23.54.035 Loading berth requirements and space standards

REQUIREMENT

C. Standards for Loading Berths

1. Width and Clearance. Each loading berth shall be not less than ten (10) feet in width and shall provide not less than fourteen (14) feet vertical clearance.
2. Length
 - b. Low- and Medium-demand Uses. Each loading berth for low- and medium-demand uses, except those uses identified in subsection C2d, shall be a minimum of thirty-five (35) feet in length unless reduced by determination of the Director as provided at subsection C2c.

DEPARTURE AMOUNT REQUIRED

See adjacent illustrative diagrams

9. Allow that of the 9 required loading berths, 7 be permitted to be reduced in size. We request that a Design Departure be granted to allow for (4) 10' x 20' van berths. We will separately request a Type 1 Director's Decision per 23.54.35.C.2.c.ii to allow for (3) 10' x 25' berths

RATIONALE

9. A high percentage of the deliveries made to Amazon buildings are done by carriers which utilize smaller vans that do not require standard loading berths sized at 10' x 35'. This will enable the loading dock to be sized more efficiently to meet the true loading demands of the building user. A more compact loading dock will also permit more space for uses such as retail at the ground level.

APPLICABLE DESIGN GUIDELINES:
DC1-C.4



DEPARTURE REQUEST #9

Departures Summary

Departure Request 1A: See Sheet G-001C

Standard	Request	Proposed	Rationale	Applicable Design Guidelines	Reference
23.49.056 Street Facade, Landscaping, and Street Setbacks B. Facade Setback Limits 2. General Setback Limits b. The maximum area of all setbacks between the street lot line and facade along each street frontage of a lot shall not exceed the area derived by multiplying the averaging factor by the width of the street frontage of the structure along that street. The averaging factor is...ten on Class II pedestrian streets and designated green streets.	Allow for a setback area greater than allowed using the averaging factor of ten on Bell Street.	The façade setback parallel to Bell Street varies from 10' to 15' for a distance of 201'-9" feet. Maximum setback area is 2,018 SF; Proposed is 2,313 SF	The building massing and facade scale transition is improved by a massing volume lower than 25' as it turns the corner from 7th Avenue to Bell Street. This scale is consistent with the lower scale of retail facades along Bell Street and emphasizes the clear distinction between tower and podium volumes.	A-1 Respond to physical environment B-3 Reinforce the positive urban form & architectural attributes of the immediate area B-4 Design a well proportioned & unified building	See Diagram C1/G-001C

Departure Request 1B: See Sheet G-001C

Standard	Request	Proposed	Rationale	Applicable Design Guidelines	Reference
23.49.056 Street Facade, Landscaping, and Street Setbacks A. Minimum Façade Heights a.1 Per Table A, minimum façade heights for Green Streets (Bell and Blanchard Streets) = 25'-0"	Allow for a façade height below the 25' minimum as measured from Bell Street.	Allow for a section of facade 16'-4" in width that is below the 25' min height.	The building massing and facade scale transition is improved by a massing volume lower than 25' as it turns the corner from 7th Avenue to Bell Street. This scale is consistent with the lower scale of retail facades along Bell Street and emphasizes the clear distinction between tower and podium volumes.	A-1 Respond to physical environment B-3 Reinforce the positive urban form & architectural attributes of the immediate area B-4 Design a well proportioned & unified building	See Diagram C1/G-001C

Departure Request 2: See Sheet G-001C

Standard	Request	Proposed	Rationale	Applicable Design Guidelines	Reference
23.49.056 Street Facade, Landscaping, and Street Setbacks Same as Departure Request #1B Above	Allow for a setback area greater than allowed using the averaging factor of ten on Blanchard Street.	Maximum Façade setback along Blanchard Street is 1,890 SF. Proposed setback is 3,043 SF.	The corner of Blanchard Street and 8th Avenue, where the greatest building setback from Blanchard Street is proposed, is enhanced and supported by a more gracious zone between the public right-of-way and the building. The setback also provides for corner entries into the office lobby and retail, enhancing street level active uses.	B-3 Reinforce the positive urban form & architectural attributes of the immediate area C-1 Promote pedestrian interaction C-4 Reinforce building entries	See Diagram A1/G-001C

Departure Request 3A: See Sheet G-001D

Standard	Request	Proposed	Rationale	Applicable Design Guidelines	Reference
23.49.056 Street Facade, Landscaping, and Street Setbacks Same as Departure Request #1B Above	Allow for a setback area greater than allowed using the averaging factor of ten on 8th Avenue.	Maximum Façade setback along 8th Avenue is 3,060 SF. Proposed setback is 6,995 SF.	The corner of 8th Avenue and Bell Street, where the greatest building setback from 8th Avenue is proposed, is enhanced and supported by a more gracious zone between the public right-of-way and the building. The setback also provides for a sidewalk-level transition to the through-block connection, enhancing pedestrian circulation.	B-3 Reinforce the positive urban form & architectural attributes of the immediate area C-1 Promote pedestrian interaction D-1 Provide inviting & usable open space	See Diagram D1/G-001D

Departure Request 3B: See Sheet G-001D

Standard	Request	Proposed	Rationale	Applicable Design Guidelines	Reference
23.49.056 Street Facade, Landscaping, and Street Setbacks B. Facade Setback Limits 2. General Setback Limits c. The maximum width...of any setback area exceeding a depth of 15' from the street lot line shall not exceed 80 feet, or 30% of the lot frontage on that street, whatever is less.	Allow for a setback greater than 15' to be longer than the 80' maximum prescribed.	Proposed setback on 8th AVE at the hillclimb is 104.5'	The corner of 8th Avenue and Bell Street, where the greatest building setback from 8th Avenue is proposed, is enhanced and supported by a more gracious zone between the public right-of-way and the building. The setback also provides for a sidewalk-level transition to the through-block connection, enhancing pedestrian circulation.	B-3 Reinforce the positive urban form & architectural attributes of the immediate area C-1 Promote pedestrian interaction D-1 Provide inviting & usable open space	See Diagram D1/G-001D

Departure Request 4: See Sheet G-001B

Standard	Request	Proposed	Rationale	Applicable Design Guidelines	Reference
23.49.056 Street Facade, Landscaping, and Street Setbacks B. Facade Setback Limits 2. General Setback Limits. d. The maximum setback of the facade from the street lot lines at intersections is 10 feet. The minimum distance the facade must conform to this limit is 20 feet along each street.	Set back the building corner closest to the property line corner at the intersection of Blanchard Street and 8th Avenue farther than the maximum prescribed standards.	The façade parallel to Blanchard Street sets back 26'-3" from the property line. The façade parallel to 8th Avenue sets back 40' from the property line.	The Green Street at this corner is enhanced and supported by a more gracious zone between the public right-of-way and the building. The setback also provides for corner entries into the office lobby and retail, enhancing street level active uses.	B-3 Reinforce the positive urban form & architectural attributes of the immediate area C-1 Promote pedestrian interaction C-4 Reinforce building entries	See Diagram F7/G-001B

Departure Request 5: See Sheet G-001B

Standard	Request	Proposed	Rationale	Applicable Design Guidelines	Reference
23.49.056 Street Facade, Landscaping, and Street Setbacks Same as Departure Request #4 Above	Set back the building corner closest to the property line corner at the intersection of Bell Street and 8th Avenue farther than the maximum prescribed standards.	The façade parallel to Bell Street sets back 10'-0" from the property line. The façade parallel to 8th Avenue sets back 27'-6" from the property line.	The Green Street at this corner is enhanced and supported by a more gracious zone between the public right-of-way and the building. The setback also provides for a sidewalk-level transition to the through-block connection, enhancing pedestrian circulation.	B-3 Reinforce the positive urban form & architectural attributes of the immediate area C-1 Promote pedestrian interaction D-1 Provide inviting & usable open space	See Diagram F7/G-001B

Departure Request 6: See Sheet G-001B

Standard	Request	Proposed	Rationale	Applicable Design Guidelines	Reference
23.49.056 Street Facade, Landscaping, and Street Setbacks Same as Departure Request #4 Above	Set back the building corner closest to the property line corner at the intersection of Bell Street and 7th Avenue farther than the maximum prescribed standards.	The façade parallel to Bell Street sets back 15'-0" from the property line.	The Green Street at this corner is enhanced and supported by a more gracious zone between the public right-of-way and the building. The corner setback provides for an outdoor seating terrace adjacent to retail, enhancing active street level uses.	B-3 Reinforce the positive urban form & architectural attributes of the immediate area C-1 Promote pedestrian interaction C-4 Reinforce building entries	See Diagram F7/G-001B

Departure Request 7A: See Sheet A-206

Standard	Request	Proposed	Rationale	Applicable Design Guidelines	Reference
23.49.056 Street Facade, Landscaping, and Street Setbacks D. Blank Façade Limits 2. Blank Facade Limits for Class I Pedestrian Streets and Designated Green Streets. a. Blank facades shall be no more than 15 feet wide except segments with garage doors may exceed a width of 15 feet and may be as wide as the driveway plus 5 feet.	Allow for a blank façade greater than 15 feet on Blanchard Street (a designated Green Street).	Blank Façade length of 68'-7"	The extent of blank façade proposed occurs at a grade transition that facilitates an outdoor seating zone adjacent to retail, activating the streetscape along the Green Street. Retail finish floor elevation is set to provide unobstructed ADA access to the public plaza at the corner of 7th & Blanchard.	A-1 Respond to physical environment C-1 Promote pedestrian interaction D-1 Provide inviting & usable open space	See A1/A-206

Departure Request 7B: See Sheet A-206

Standard	Request	Proposed	Rationale	Applicable Design Guidelines	Reference
23.49.056 Street Facade, Landscaping, and Street Setbacks D. Blank Façade Limits 2. Blank Facade Limits for Class I Pedestrian Streets and Designated Green Streets. c. The total width of all blank facade segments, including garage doors, shall not exceed 40 percent of the street-facing facade of the structure on each street frontage	Allow for an area of non-transparent façade greater than 40% on Blanchard Street (a designated Green Street).	Non-Transparent façade area of 40.9 %	A majority of the non-transparent façade occurs at a retail terrace which is adjacent to the 7th Ave plaza, requiring the finish floor elevation to match the plaza for public access.	A-1 Respond to physical environment C-1 Promote pedestrian interaction D-1 Provide inviting & usable open space	See A1/A-206

Departure Request 7A: See Sheet A-206

Standard	Request	Proposed	Rationale	Applicable Design Guidelines	Reference
23.49.056 Street Facade, Landscaping, and Street Setbacks D. Blank Façade Limits 2. Blank Façade Limits for Class I Pedestrian Streets and Designated Green Streets. a. Blank facades shall be no more than 15 feet wide except segments with garage doors may exceed a width of 15 feet and may be as wide as the driveway plus 5 feet.	Allow for a blank façade greater than 15 feet on Blanchard Street (a designated Green Street).	Blank Façade length of 68'-7"	The extent of blank façade proposed occurs at a grade transition that facilitates an outdoor seating zone adjacent to retail, activating the streetscape along the Green Street. Retail finish floor elevation is set to provide unobstructed ADA access to the public plaza at the corner of 7th & Blanchard.	A-1 Respond to physical environment C-1 Promote pedestrian interaction D-1 Provide inviting & usable open space	See A1/A-206

Departure Request 7B: See Sheet A-206

Standard	Request	Proposed	Rationale	Applicable Design Guidelines	Reference
23.49.056 Street Facade, Landscaping, and Street Setbacks D. Blank Façade Limits 2. Blank Façade Limits for Class I Pedestrian Streets and Designated Green Streets. c. The total width of all blank facade segments, including garage doors, shall not exceed 40 percent of the street-facing facade of the structure on each street frontage	Allow for an area of non-transparent façade greater than 40% on Blanchard Street (a designated Green Street).	Non-Transparent façade area of 40.9 %	A majority of the non-transparent façade occurs at a retail terrace which is adjacent to the 7th Ave plaza, requiring the finish floor elevation to match the plaza for public access.	A-1 Respond to physical environment C-1 Promote pedestrian interaction D-1 Provide inviting & usable open space	See A1/A-206

Departure Request 8: See Sheet A-207

Standard	Request	Proposed	Rationale	Applicable Design Guidelines	Reference
23.49.056 Street Facade, Landscaping, and Street Setbacks D. Blank Façade Limits 3. Blank Façade Limits for Class II Pedestrian Streets. a. Blank façade segments shall be no more than 30 feet wide, except for garage doors, which may exceed 30 feet.	Allow for a blank façade segment greater than 30 feet on 8th Avenue (Type II Pedestrian Street).	Blank Façade segment length of 107'-5"	The extent of the blank façade segment proposed occurs at a grade transition that facilitates a hill climb through-block connection and provides a variety of landscape features, an ADA accessible route to the elevated mid-block plaza, and pedestrian seating.	C-1 Promote pedestrian interaction D-1 Provide inviting & usable open space D-2 Enhance the building with landscaping	See D1/A-207

Departure Request 9: See Sheet G-001B

Standard	Request	Proposed	Rationale	Applicable Design Guidelines	Reference
23.54.035.C2 Loading Berth Requirements and Space Standards The standards length of a loading berth shall be 10 feet by 35 feet.	Allow that of the 9 required loading berths, 7 be permitted to be reduced in size. We request that a Design Departure be granted to allow for (4) 10' x 20' van berths. We will separately request a Type 1 Director's Decision per 23.54.35.C.2.c.ii to allow for (3) 10' x 25' berths	9 Total loading berths are required; proposed: (2) large truck berths @ 10' x 35'; (3) medium truck berths @ 10' x 25'; (4) van berths @ 10' x 20'	A high percentage of the deliveries made to Amazon buildings are done by carriers which utilize smaller vans that do not require standard loading berths sized at 10' x 35'. This will enable the loading dock to be sized more efficiently to meet the true loading demands of the building user. A more compact loading dock will also permit more space for uses such as retail at the ground level.	E-3 Minimize the presence of service areas	See Diagram B7/G-001B

Departure Request 10: See Sheet G-001B

Standard	Request	Proposed	Rationale	Applicable Design Guidelines	Reference
23.54.040 Solid waste and recyclable materials storage and access F. Access for service providers to the storage space from the collection location shall meet the following requirements 2. For containers larger than 2 cubic yards and all compacted refuse containers d. If accessed directly by a collection vehicle, whether into a structure or otherwise, a 21 foot overhead clearance shall be provided.	Allow a clear height of less than 21'.	Clear height of 18'-3" at structure in loading dock/trash area. Loading dock entry on 8th Ave to be 14' clear minimum.	Loading dock clear height is limited by street-level public access to the plaza on 7th AVE (above) and maximum ramp slope for truck access to the loading dock off of 8th AVE.	A-1 Respond to physical environment Minimize the presence of service areas	E-3 See Diagram A1/G-001B

APPENDIX



Site Area:

77,700 square feet plus a public alleyway of approximately 5,700 square feet with approximately 360 feet of frontage on both 7th and 8th Avenues, 232 feet of frontage on both Bell and Blanchard Streets.

Topography:

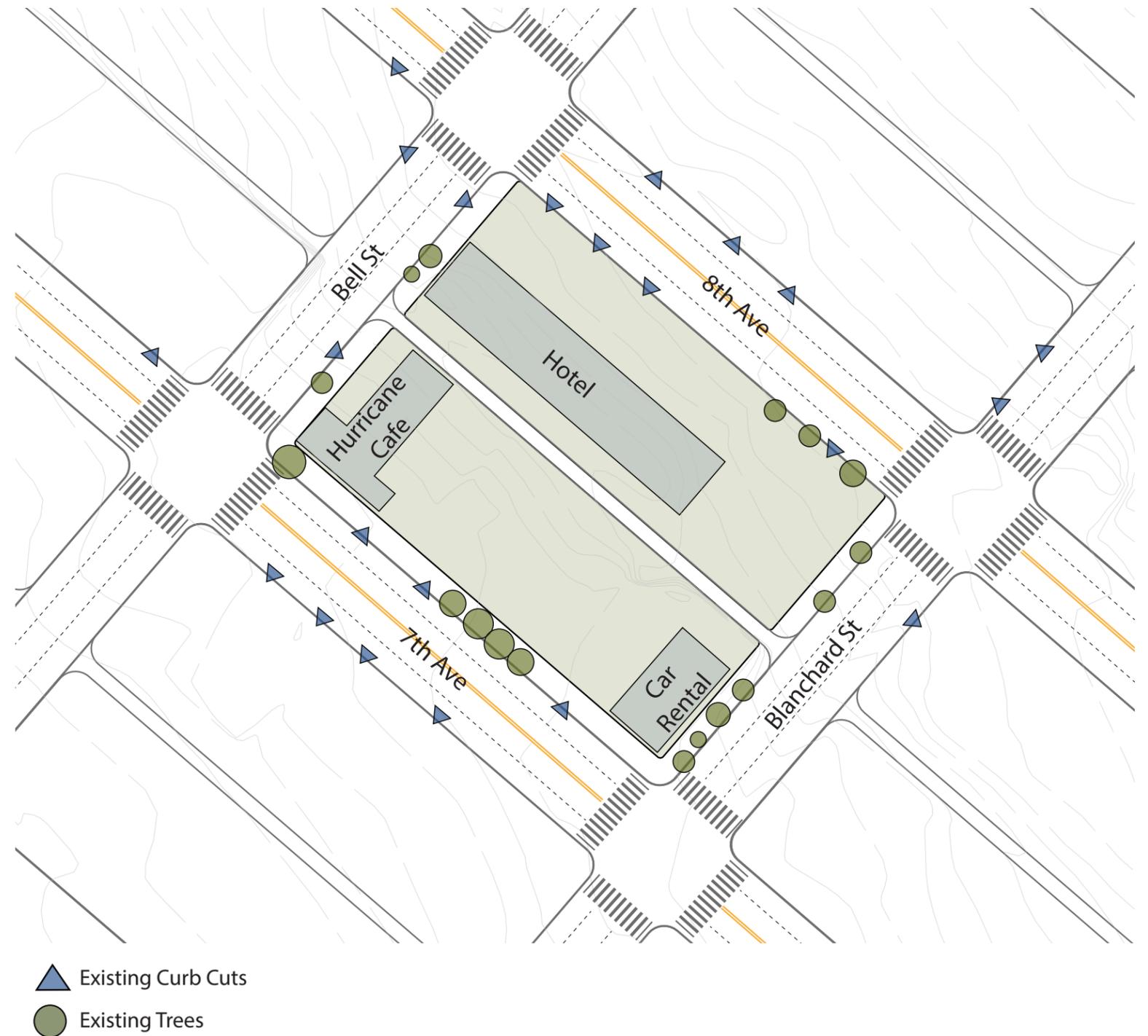
The site slopes from elevation 104' 3" in the northwest corner down to 83' 9" in the south-east corner.

Tree Survey:

There are no significant trees on the site. Trees are located within the sidewalk right-of-way. Five trees are located along 7th Avenue, three along 8th Avenue, six along Blanchard Street and three along Bell Street.

Existing Buildings:

The site has a hotel, restaurant, rental car facility and surface parking lot.



Urban Design Analysis



C. The Streetscape

C-1 Promote pedestrian interaction

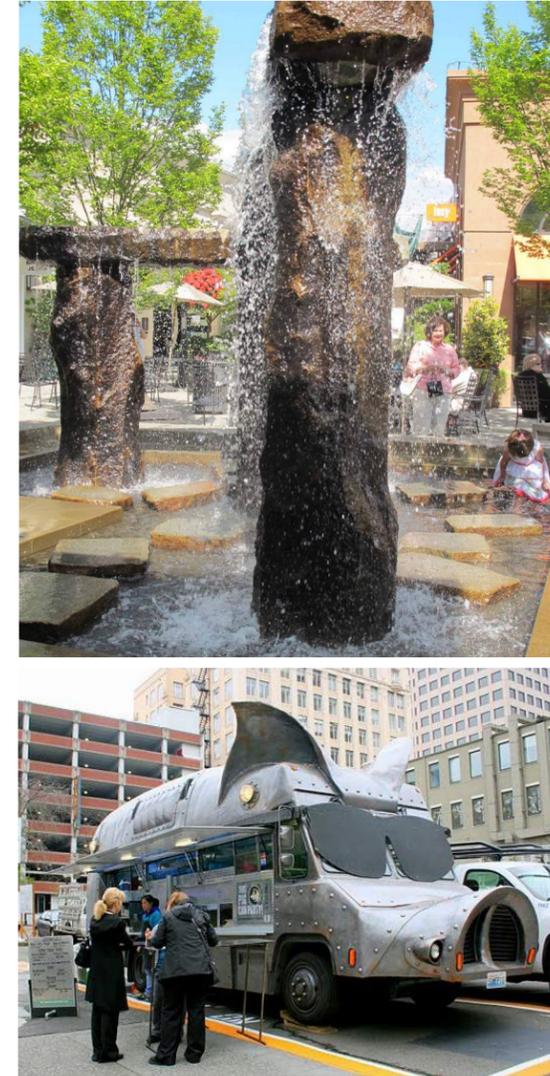
Spaces for street level uses should be designed to engage pedestrians with the activities occurring within them. Sidewalk-related spaces should be open to the general public and appear safe and welcoming.

C-6 Develop the alley facade

To increase pedestrian safety, comfort, and interest, develop portions of the alley facade in response to the unique conditions of the site or project.

The proposal has been designed to reinforce and enhance existing pedestrian patterns and capitalize on the sites transitional location between the Denny Triangle and South Lake Union.

The proposal is organized to provide a through-block connection and linked public plazas that engage pedestrians. The perimeter street frontage gives priority to active uses such as retail and building entries.



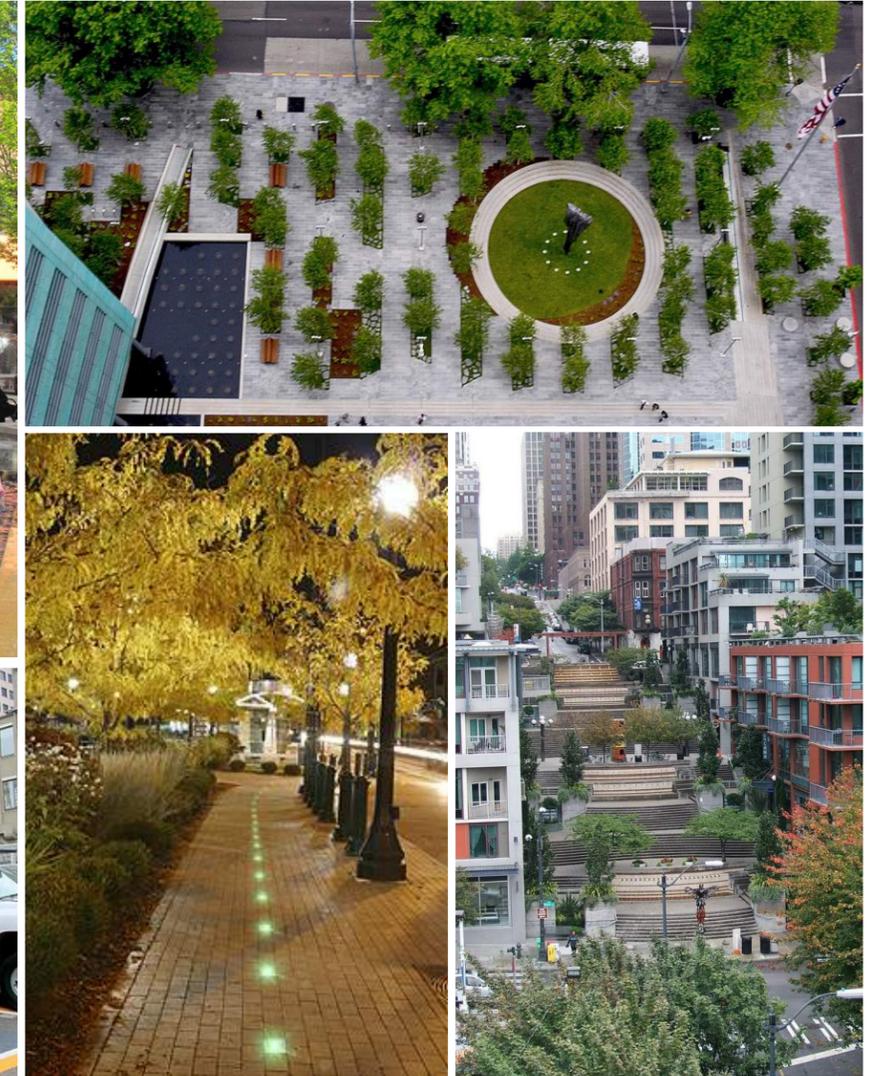
D. Public amenities

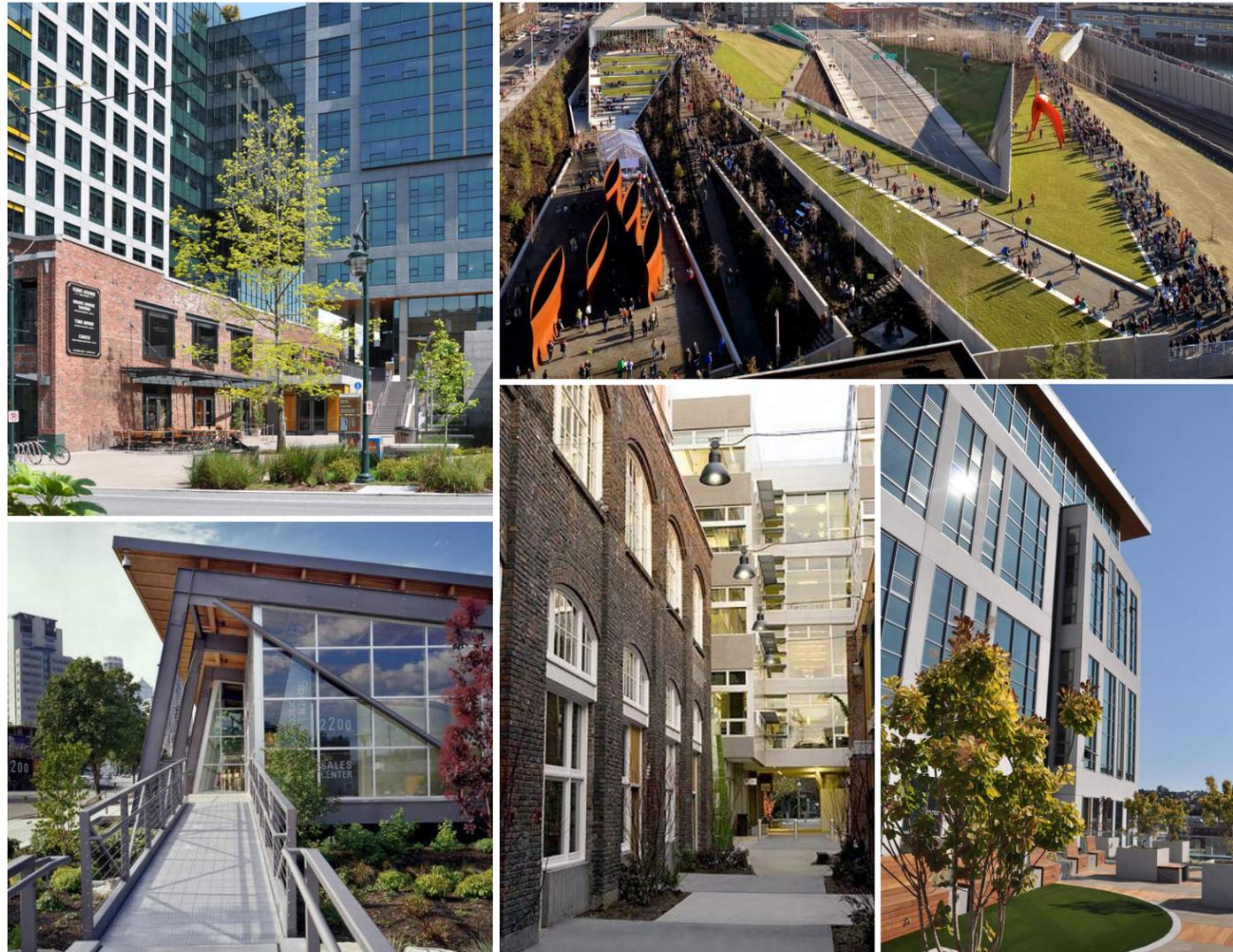
D-1 Provide inviting and usable open space

Design public opens spaces to promote a visually pleasing, safe and active environment for workers, residents, and visitors. Views and solar access from the principal area of the open space should be especially emphasized.

The design gives emphasis to high quality open spaces that knit into the existing urban fabric and have been situated to maximize solar exposure.

An open plaza is located on the southwest corner of the block to invite pedestrians into the site and encourage through-block circulation. A complimentary open space on the northeast corner of the site engages pedestrian movement from the north.





A. Site Planning and Massing

A-2 Respond to the physical environment

Develop an architectural concept and compose the building's massing in response to geographic conditions and patterns of urban form found beyond the immediate context of the building site.

A-2 Enhance the Skyline

Design the upper portion of the building to promote visual interest and variety in the downtown skyline

The proposed design responds to the allowable zoning envelope and resultant presence on the skyline by employing distinctive massing solutions that respond to the surrounding context and significant site slope. The proposal continues the established pattern of urban density of open space. Sculpted building forms and expressive tops will distinguish the proposal from the city skyline and respond to the prominent views of the project from the adjacent South Lake Union neighborhood to the north and Downtown to the south.



B. Architectural Expression

B-2 Create a transition in bulk and scale

Compose a massing for the building to create a transition to the height, bulk and scale of development in neighboring or nearby less-intensive zones.

B-3 Reinforce the positive urban form & architectural attributes of the immediate area

Consider the predominant attributes of the immediate neighborhood and reinforce desirable siting patterns, massing arrangements, and streetscape characteristics of nearby development.

B-4 Design a well-proportioned & unified building

Compose the massing and organize the publicly accessible interior and exterior spaces to create a well-proportioned building that exhibits a coherent architectural concept. Design the architectural elements and finish details to create a unified building, so that all components appear integral to the whole.

The proposal distinguishes the high-rise portion of the tower from the lower zones (aka "podium") through scale, massing and material delineation. The building podium acknowledges the surrounding low- and mid-rise structures by providing setbacks that relate to adjacent structures. The site planning of the buildings follows the existing pattern of giving priority to the southern exposure to public open space and extending the boulevard character of 7th Avenue. As a multi-building proposal, the project unifies the architectural vocabulary of the block by employing complementary materials and detailing that are shared by all structures on the block.

Appendix

Zone: DMC 340/290-400

Denny Triangle Urban Center Village

23.49.042 Permitted Uses

Standard
All uses are permitted outright except those prohibited by SMC 23.49.046, and parking, which shall be regulated by 23.49.045.

23.49.008 Structure Height

Standard
Nonresidential Height Maximum: 340'
Rooftop Features allowed above height limit: <ul style="list-style-type: none"> Railings, planters, skylights, clerestories, greenhouses and parapets may extend up to 4' above height limit. Solar collectors may extend up to 7' above height limit. Mechanical equipment, stair penthouses, etc... may extend up to 15' above the height limit.
Rooftop features may cover up to a combined coverage limit of 35%.
Elevator penthouses may extend up to 23' above the height limit (8' cab) or 25' above the limit (9' cab) plus an additional 10' if elevator provides access to usable rooftop open space.
The amount of rooftop area enclosed by screening may exceed the maximum percentage of the combined coverage of all rooftop features.
Measures may be taken to screen rooftop features from public view through the design review process. Rooftop screening may exceed ten percent of the applicable height limit or 15 feet, whichever is greater.

23.49.009 Street-level Use Requirements

Standard
None required on 7th Avenue, 8th Avenue, Bell Street, or Blanchard Street.

23.49.011 Floor Area Ratio

Standard
Base FAR: 5 Maximum FAR: 10 Additional chargeable floor area above the base FAR may be obtained as outlined in section 23.49.011 and may include generally the following: <ul style="list-style-type: none"> Amenity Bonuses Transfer Development Rights Rural Development Credit Housing and Child Care
<ul style="list-style-type: none"> A minimum of 5% of floor area above base FAR must be obtained through Landmark TDRs to the extent they are available. FAR gained through housing and child care bonuses (23.49.012) together with housing (23.49.015) and landmark TDRs shall equal 75% of the area by which the total chargeable area permitted on the lot exceeds the base FAR. At least 1/2 of the balance of the 25% shall be gained from a sending lot with a major performing arts center if available. The balance of the 25% shall be gained through bonus floor area for amenities (23.49.013)
<ul style="list-style-type: none"> The first increment above base FAR must be provided through regional development credits, pursuant to SMC 23.58.A.044 (SMC 23.49.011.A.2.a).
Areas Exempt from FAR: <ul style="list-style-type: none"> Street level use (retail) that has a minimum flr-flr of 13', horizontal depth of 15', and overhead weather protection is provided. <ul style="list-style-type: none"> Child Care Human Services Residential use and live-work units Museums and museum expansion spaces Performing art theaters Floor area below grade Public restrooms Shower facilities for bicycle commuters Certain area in Landmark structures An allowance of 3.5% of GFA for mechanical equipment after all other deductions have been taken
Rooftop mechanical equipment, whether enclosed or not, shall be counted as part of the GFA of the structure except for those structures existing prior to June 1, 1989 or replacement mechanical equipment.

23.49.013 Bonus Floor Area for Amenities

Standard
<ol style="list-style-type: none"> Open Space and Green Street Improvements Hill Climb Assists (N/A) Human Services Per 23.49.013 A3 Public Restrooms Rehabilitation and Preservation of Landmark Structure Transit Station Access (N/A)
Amenity Ratios and Limits per 23.49.13 B3

23.49.014 Transfer of Development Rights

Standard
<ol style="list-style-type: none"> Housing TDR DMC Housing TDR Landmark Housing TDR Landmark TDR Open Space TDR; and South Downtown Historic TDR
Note Table A 23.49.014

23.49.016 Open Space

Standard
Private Open Space - Office Use Requirements: <ul style="list-style-type: none"> 20 SF for every 1000 GSF of Office Use Only applies to office use greater than 85,000 GSF; Office use less than 85,000 GSF is exempt. Must be open to the sky, meet Downtown Amenity Standards and be accessible to all tenants.
On-site public open space <ul style="list-style-type: none"> Available for amenity feature bonus per section 23.49.013
Off-site public open space <ul style="list-style-type: none"> Available for amenity feature bonus per section 23.49.013 Must be in a downtown zone within 1/4 mile of the project site. Must be open to the public without charge. Minimum of 5,000 SF of contiguous area.
Payment in lieu <ul style="list-style-type: none"> Payment in lieu of open space development is permitted if the Director determines that such payment will contribute to the improvement of a green street or there is public open space abutting the lot or in the vicinity.

23.49.018 Overhead weather protection and lighting

Standard
Continuous weather protection is required along entire street frontage
Exceptions: <ul style="list-style-type: none"> • If set back farther than 5' from property line • Abuts a bonused open space or amenity feature • If separated from the street property line by a landscaped area at least 2' in width • Driveways and loading docks
Dimensions: <ul style="list-style-type: none"> • Min. 8' from building wall or must extend to a line 2' from curb line, whichever is less • Lower edge minimum height of 10' and a maximum of 15' above the sidewalk • Pedestrian lighting to be provided

23.49.019 Parking quantity, location and access requirements

Standard
No parking, either long-term or short-term, is required on lots in Downtown zones
<ul style="list-style-type: none"> • On Blanchard Street and Bell Street (green street), parking is permitted at street level only if separated from the street by other uses • On 7th and 8th Avenue (class II pedestrian streets), parking is permitted at street level if it meets the standards of 23.49.019B, including: <ul style="list-style-type: none"> • At least 30% of the street frontage (excluding garage doors) is separated from the street by other uses; • The façade of the separating uses meets the transparency and blank wall standards for class I ped. streets; • The portion of parking not separated by other uses is screened, and; • The street façade is enhanced by detailing, artwork, landscaping, etc... • Parking not at street level within structures must be located below street level or separated from street level by other uses • Up to four levels of above grade parking may be permitted if it meets the standards of 23.49.019B
Maximum parking limit for nonresidential uses <ul style="list-style-type: none"> • Parking for nonresidential uses is limited to one parking space per every 1,000 square feet of gross floor area in nonresidential use. • Parking for nonresidential uses may be permitted to exceed the maximum standard as a special exception as granted by the Director. • Access to parking and loading shall be from the alley when the lot abuts an improved alley, unless the Director approves an alternate access route.
Bicycle Parking (Minimums): <ul style="list-style-type: none"> • Office: 1 space per 5,000 SF • Hotel: .05 spaces per hotel room • Retail use over 10,000 SF: 1 space per 10,000 SF • Residential: 1 space for every 2 dwelling units
After the first 50 spaces are provided additional spaces are required at 1/2 the ratio noted
Structures containing more than 250,000 SF of office space shall include shower facilities
Off-street loading spaces shall be provided per 23.54.030

23.49.022 Minimum sidewalk and alley width

Standard
Minimum sidewalk width on Blanchard Street, Bell Street, 7th Avenue and 8th Avenue: 12'.
Minimum alley width: 20', achievable through setback or dedication if required.

23.49.045 Parking

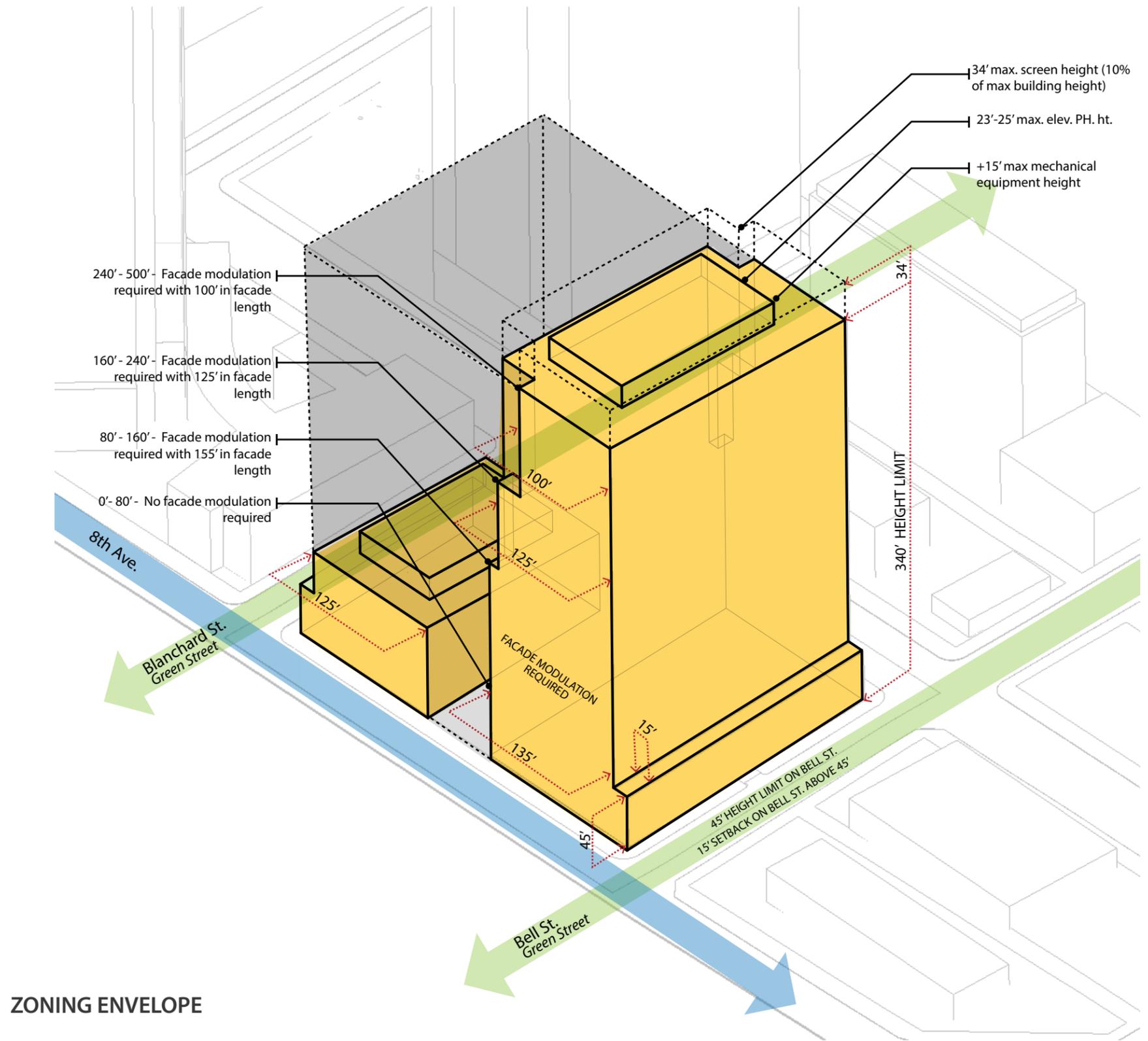
Standard
Principal use parking garages for short-term parking may be permitted as conditional use.
In DMC zones, principal use long-term and short-term surface parking may be permitted as administrative conditional use.
Accessory parking garages for both long-term and short-term parking are permitted outright up to the maximum parking limit established by 23.49.019

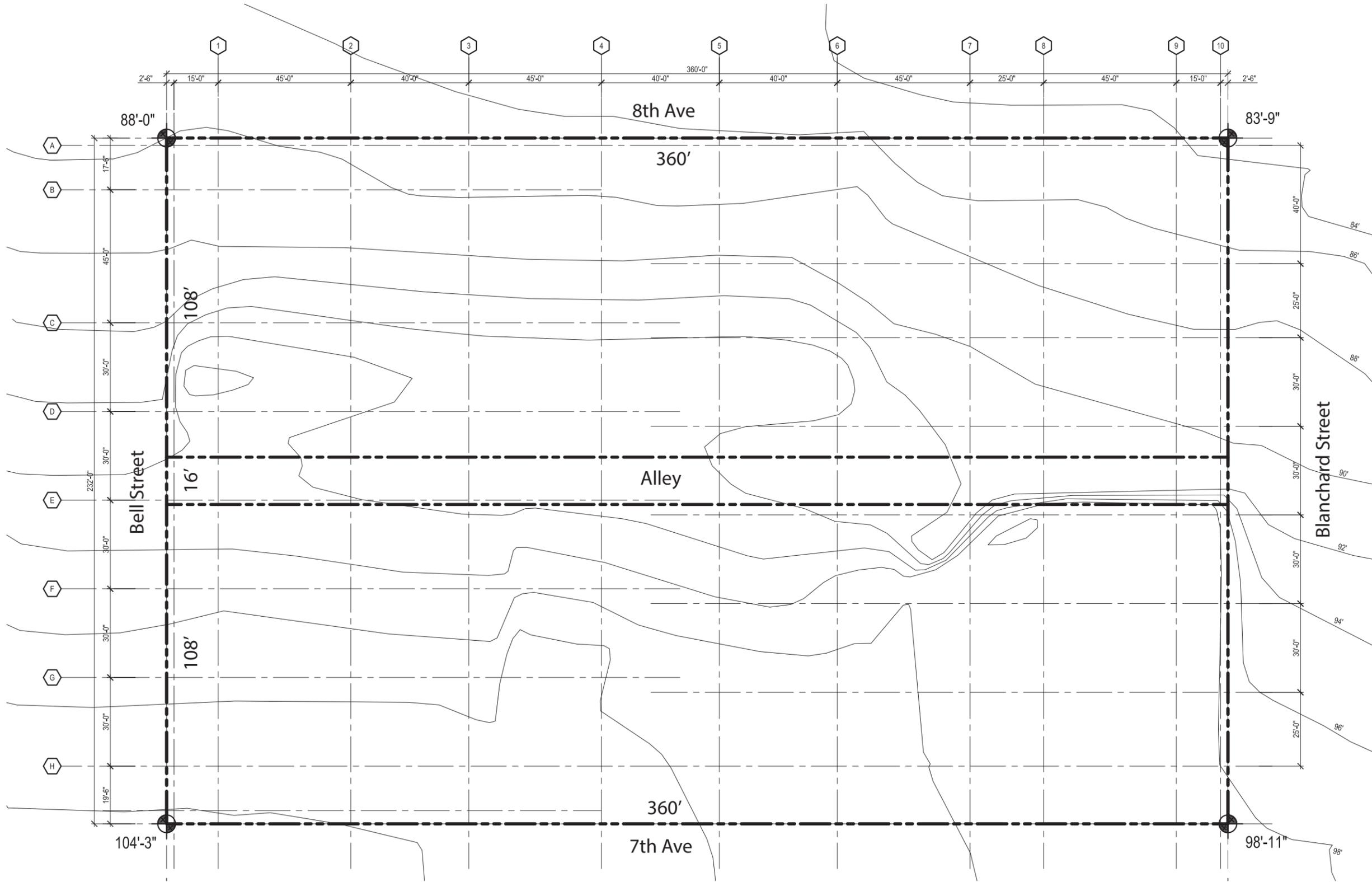
23.49.056 street façade, landscaping and street setback requirements

Standard
Minimum façade heights: <ul style="list-style-type: none"> • 7th and 8th Avenues (class II pedestrian streets): 15' • Blanchard Street and Bell Street (green streets): 25'
Setbacks <ul style="list-style-type: none"> • The max. area of all setbacks shall not exceed the area derived by multiplying the averaging factor by the width of the street frontage. The averaging factor is... ten on Class II pedestrian streets and designated green streets. • The maximum setback of the facade from the street lot lines at intersections is 10 feet. Minimum conforming distance is 20 feet along each street. • Any exterior open space that meets amenity standards is not considered part of the setback area. • If a sidewalk is widened into the lot as a condition of the development setback shall be measured from the line of the new sidewalk.
Transparency and blank façade requirements: <ul style="list-style-type: none"> • Along 7th and 8th Avenues (class II ped. street) 30% of street façade to be transparent between 2' and 8' above sidewalk level. • Along Bell and Blanchard Streets (green streets) 60% of street façade to be transparent between 2' and 8' above sidewalk level. • On 7th and 8th Avenues blank façades limited to segments 30' except for garage doors which may be wider than 30'. • On 7th and 8th Avenues the total of all blank façade segments shall not exceed 70% of the street façade. • On Blanchard and Bell Streets blank façades limited to segments 15' except for garage doors which may be wider than 30'. • On Blanchard and Bell Streets the total of all blank façade segments shall not exceed 40% of the street façade. • Blank façade sections shall be separated by transparent area at least 2' wide
Street Trees are required on all streets.
Landscaping in the Denny Triangle Urban Village <ul style="list-style-type: none"> • All areas abutting a street lot line that are not covered by a structure, have a depth of 10 feet or more, and are larger than 300 SF shall be landscaped. • Setbacks required to meet minimum sidewalk widths shall be exempt from landscape requirements.

23.49.058 Upper-Level Development Standards

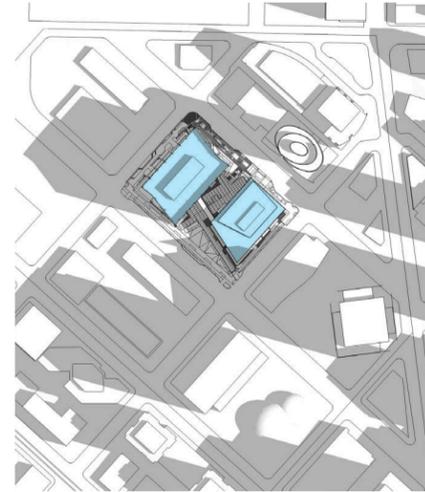
Standard
"Tower" Definition <ul style="list-style-type: none"> • Any structure where a portion is above a height of 85 feet in a structure that has any nonresidential use above 65 feet or does not have residential use above a height of 160 feet.
Façade modulation and upper-level width limits apply to: <ul style="list-style-type: none"> • Structures 160' in height or less in which any story above 85' exceeds 15,000 SF • Portions of structures in non-residential use above a height of 160' in which any story above an elevation of 85' exceeds 15,000 SF.
Façade Modulation (non-residential) <ul style="list-style-type: none"> • Required of street facing facades within 15' of street above 85'. • Maximum façade length without modulation within 15' of street lot line: <ul style="list-style-type: none"> • 155' façade length from elevation 86 to 160 feet. • 125' façade length from elevation 161 to 240 feet. • 100' façade length from elevation 241 to 500 feet. • 80' façade length for elevations above 500 feet. • Modulation defined as at least 15' deep step back from property line at least 60' long.
Upper Level Width Limit <ul style="list-style-type: none"> • On lots where the width and depth of the lot each exceed 200 feet, the maximum facade width for any portion of a building above 240 feet shall be 145 feet along the general north/south axis of a site.
Tower Separation <ul style="list-style-type: none"> • On DMC sites zoned with a maximum height limit of more than 160' located in the Denny Triangle Urban Village, if any part of a tower exceeds 160' then all portions of the tower that are above 125' must be separated by a minimum of 60' from any portion of any other existing tower above 125' in height. From a structure allowed pursuant to the Land Use Code in effect prior to the effective date of March 20th 2006 Ordinance 122054.
Upper level setbacks <ul style="list-style-type: none"> • When a lot in a DMC Zone is located on a designated green street, a continuous upper-level setback of 15' shall be provided on the street frontage abutting the green street at a height of 45 feet.





Appendix

Summer- June 21st



9:00 am



12:00 pm



3:00 pm

Spring & Fall- March & Sept. 20th



9:00 am



12:00 pm

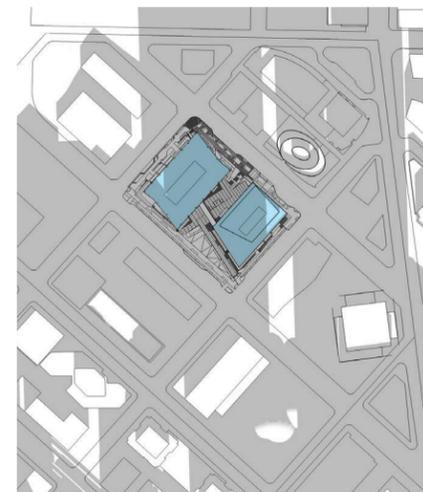


3:00 pm

Winter- December 21st



9:00 am



12:00 pm



3:00 pm

