



BLOCK 21 Full Alley Vacation Proposal

Early Design Guidance Draft Package
November 18, 2014

2200 7th Ave, DPD #3018578



PROJECT INFO

PROPERTY ADDRESSES & DPD PROJECT NUMBERS

2200 7th Avenue, DPD # 3018578

OWNER

Acorn Development

ARCHITECT

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DEVELOPMENT OBJECTIVES

Develop a commercial project with approximately 835,200 gsf of office space and approximately 35,000 gsf of street level retail in three buildings.. Parking below grade will be provided for approximately 835 Cars.

TABLE OF CONTENTS

DEVELOPMENT OBJECTIVES

- A-103 Development Objectives
- A-104 Project Goals
- A-105-07 Zoning & Code Analysis

URBAN DESIGN ANALYSIS

- A-109 Zoning Designation
- A-110 Seattle Section
- A-111 Adjacent Building Heights
- A-112 Projected Site Aerial
- A-113 Street Classification
- A-114 Surrounding Buildings
- A-115 View Analysis
- A-116 Aerial Photographs
- A-117-18 Urban Density Patterns
- A-119 Open Space Connections

DESIGN GUIDELINES

- A-121-22 Design Guidelines

SITE ANALYSIS

- A-124 Site Plan
- A-125-28 Street Views
- A-129 Inter - Office Pedestrian Routes
- A-130 Vehicle Trips - Existing Site
- A-131 Travel Distances
- A-132 Local Amenities
- A-133 Climate Data
- A-134 Site Photos

ARCHITECTURAL CONCEPTS

- A-136 Proposal Summary
- A-137-38 Option 1
- A-139-40 Option 2
- A-141-42 Option 3 - Preferred
- A-143 Circulation Diagrams
- A-144 Site Diagrams
- A-145-47 Shadow Studies

DESIGN CONCEPT

- A-149-50 Design Concept

LANDSCAPE CONCEPT

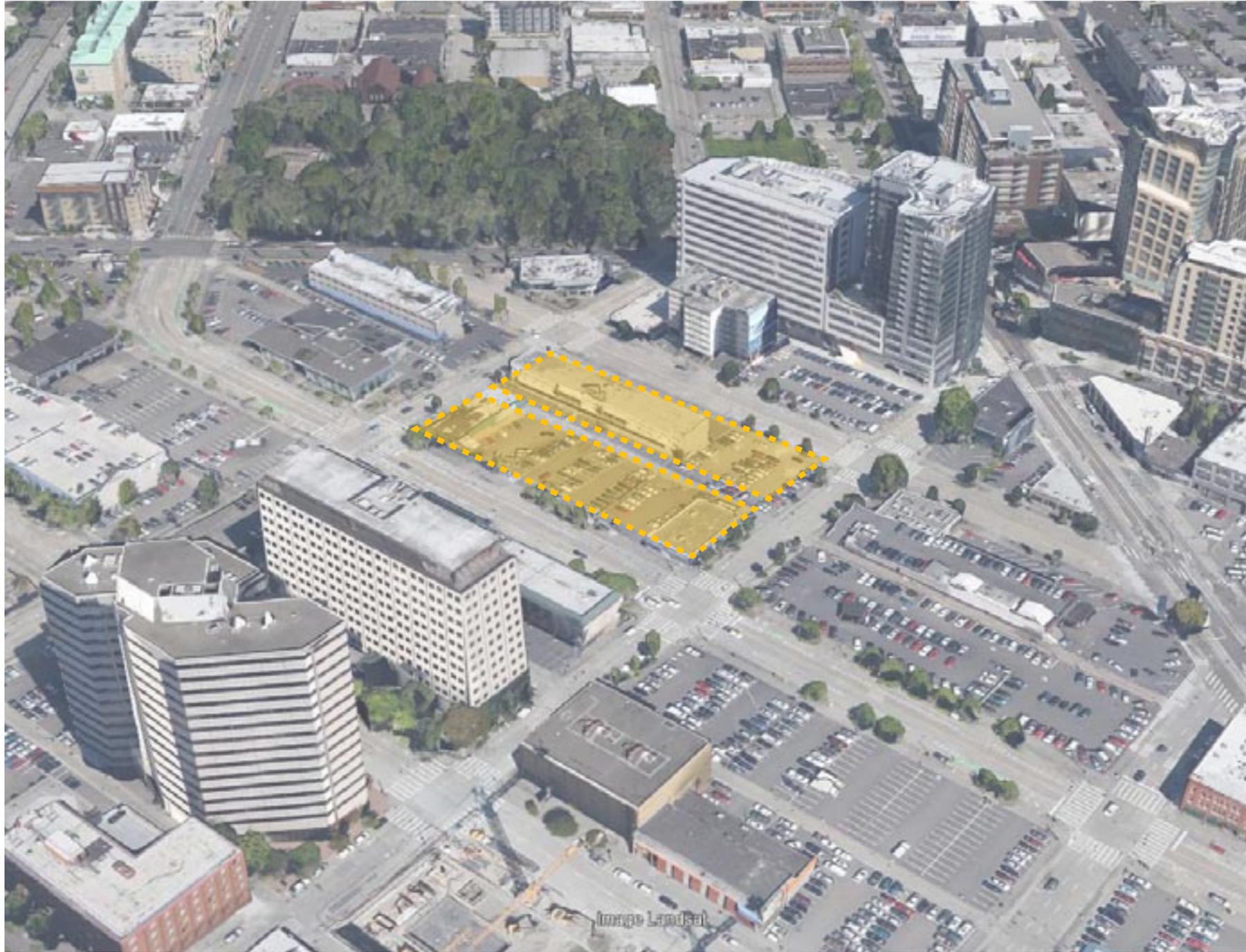
- A-152 Projects in Development/Construction
- A-153 Connections
- A-154 Open Space Map
- A-155 Open Space
- A-156 Landscape Concept Plan
- A-157-58 Section Studies
- A-159 8th & Blanchard Landscape Plan

DEVELOPMENT DEPARTURES

- A-161-62 Development Departures Option 2
- A-163-64 Development Departures Option 3

DEVELOPMENT OBJECTIVES





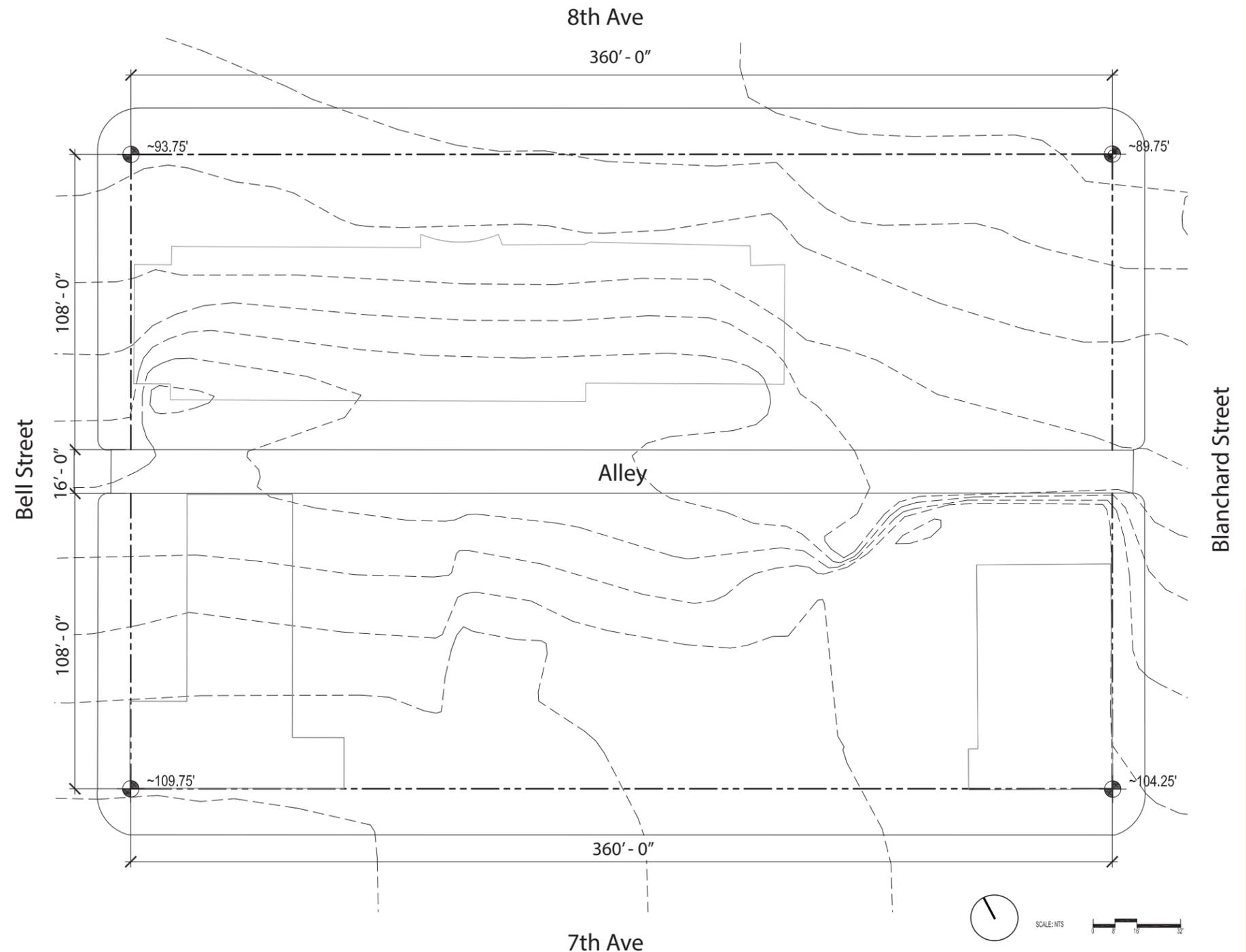
STATEMENT OF DEVELOPMENT OBJECTIVES

The applicant proposes to design and construct a development on the full block bisected by a public alleyway and bounded by 7th Avenue on the east, 8th Avenue on the west, Blanchard Street on the south and Bell Street on the north. The site is zoned DMC 340/125-400, with a site area of approximately 77,700 square feet plus a public alleyway of approximately 5,700 square feet. The site has a base FAR of 5 with a maximum of 10.

This application is for a commercial project with approximately 835,200 gsf of office space and approximately 35,000 gsf of street level retail in three buildings. An open space and through block connection are proposed connecting 7th and 8th Avenues. Approximately 835 parking stalls will be provided below grade. All building services will be located below grade, with primary access from 8th Avenue and a secondary parking access from Bell Street. This proposal assumes a full alley vacation.

PROJECT GOALS

- Create rich, diverse pedestrian environment with a variety of scales, active uses and character of open spaces
- Connect to and enhance existing neighborhood pedestrian, vehicular, transit and cycling circulation patterns
- Create transitional opportunity between the Denny Triangle and South Lake Union
- Respond and contribute to the established urban density pattern in a thoughtful manner
- Create flexible, active open space and retail opportunities that add vitality to the project site as well as surrounding community
- Extend northward enhancements of 7th Avenue landscaping and cycle track in pattern established by Rufus 2.0
- Maximize utilization of Green Streets at Blanchard and Bell Streets
- Locate all parking and services below grade
- Develop project utilizing sustainable design methodologies and connection to existing community sustainability initiatives such as District Energy
- Maximize development potential



ZONING & CODE ANALYSIS

Zone: DMC 340/290-400

Denny Triangle Urban Center Village

23.49.042 Permitted Uses

Standard
All uses are permitted outright except those prohibited by SMC 23.49.046, and parking, which shall be regulated by 23.49.045.

23.49.008 Structure Height

Standard
Nonresidential Height Maximum: 340'
Rooftop Features allowed above height limit: <ul style="list-style-type: none"> Railings, planters, skylights, clerestories, greenhouses and parapets may extend up to 4' above height limit. Solar collectors may extend up to 7' above height limit. Mechanical equipment, stair penthouses, etc... may extend up to 15' above the height limit.
Some rooftop features may extend up to 50' above the maximum height through administrative conditional use per 23.49.008-D-1-c
Rooftop features may cover up to a combined coverage limit of 35%.
Elevator penthouses may extend up to 23' above the height limit (8' cab) or 25' above the limit (9' cab) plus an additional 10' if elevator provides access to usable rooftop open space.
The amount of rooftop area enclosed by screening may exceed the maximum percentage of the combined coverage of all rooftop features.
Measures may be taken to screen rooftop features from public view through the design review process. Rooftop screening may exceed ten percent of the applicable height limit or 15 feet, whichever is greater.

23.49.009 Street-level Use Requirements

Standard
None required on 7th Avenue, 8th Avenue, Bell Street, or Blanchard Street.

23.49.011 Floor Area Ratio

Standard
Base FAR: 5 Maximum FAR: 10 Additional chargeable floor area above the base FAR may be obtained as outlined in section 23.49.011 and may include generally the following: <ul style="list-style-type: none"> Amenity Bonuses Transfer Development Rights Rural Development Credit Housing and Child Care
<ul style="list-style-type: none"> A minimum of 5% of floor area above base FAR must be obtained through Landmark TDRs to the extent they are available. FAR gained through housing and child care bonuses (23.49.012) together with housing (23.49.015) and landmark TDRs shall equal 75% of the area by which the total chargeable area permitted on the lot exceeds the base FAR. At least 1/2 of the balance of the 25% shall be gained from a sending lot with a major performing arts center if available. The balance of the 25% shall be gained through bonus floor area for amenities (23.49.013)
<ul style="list-style-type: none"> The first increment above base FAR must be provided through regional development credits, pursuant to SMC 23.58.A.044 (SMC 23.49.011.A.2.a).
Areas Exempt from FAR: <ul style="list-style-type: none"> Street level use (retail) that has a minimum flr-flr of 13', horizontal depth of 15', and overhead weather protection is provided. <ul style="list-style-type: none"> Child Care Human Services Residential use and live-work units Museums and museum expansion spaces Performing art theaters Floor area below grade Public restrooms Shower facilities for bicycle commuters Certain area in Landmark structures An allowance of 3.5% of GFA for mechanical equipment after all other deductions have been taken
Rooftop mechanical equipment, whether enclosed or not, shall be counted as part of the GFA of the structure except for those structures existing prior to June 1, 1989 or replacement mechanical equipment.

23.49.013 Bonus Floor Area for Amenities

Standard
<ol style="list-style-type: none"> Open Space and Green Street Improvements Hill Climb Assists (N/A) Human Services Per 23.49.013 A3 Public Restrooms Rehabilitation and Preservation of Landmark Structure Transit Station Access (N/A)
Amenity Ratios and Limits per 23.49.13 B3

23.49.014 Transfer of Development Rights

Standard
<ol style="list-style-type: none"> Housing TDR DMC Housing TDR Landmark Housing TDR Landmark TDR Open Space TDR; and South Downtown Historic TDR
Note Table A 23.49.014

23.49.016 Open Space

Standard
Private Open Space - Office Use Requirements: <ul style="list-style-type: none"> 20 SF for every 1000 GSF of Office Use Only applies to office use greater than 85,000 GSF; Office use less than 85,000 GSF is exempt. Must be open to the sky, meet Downtown Amenity Standards and be accessible to all tenants.
On-site public open space <ul style="list-style-type: none"> Available for amenity feature bonus per section 23.49.013
Off-site public open space <ul style="list-style-type: none"> Available for amenity feature bonus per section 23.49.013 Must be in a downtown zone within 1/4 mile of the project site. Must be open to the public without charge. Minimum of 5,000 SF of contiguous area.
Payment in lieu <ul style="list-style-type: none"> Payment in lieu of open space development is permitted if the Director determines that such payment will contribute to the improvement of a green street or there is public open space abutting the lot or in the vicinity.

23.49.018 Overhead weather protection and lighting

Standard
Continuous weather protection is required along entire street frontage
Exceptions: <ul style="list-style-type: none"> • If set back farther than 5' from property line • Abuts a bonused open space or amenity feature • If separated from the street property line by a landscaped area at least 2' in width • Driveways and loading docks
Dimensions: <ul style="list-style-type: none"> • Min. 8' from building wall or must extend to a line 2' from curb line, whichever is less • Lower edge minimum height of 10' and a maximum of 15' above the sidewalk • Pedestrian lighting to be provided

23.49.019 Parking quantity, location and access requirements

Standard
No parking, either long-term or short-term, is required on lots in Downtown zones
<ul style="list-style-type: none"> • On Blanchard Street and Bell Street (green street), parking is permitted at street level only if separated from the street by other uses • On 7th and 8th Avenue (class II pedestrian streets), parking is permitted at street level if it meets the standards of 23.49.019B, including: <ul style="list-style-type: none"> • At least 30% of the street frontage (excluding garage doors) is separated from the street by other uses; • The façade of the separating uses meets the transparency and blank wall standards for class I ped. streets; • The portion of parking not separated by other uses is screened, and; • The street façade is enhanced by detailing, artwork, landscaping, etc... • Parking not at street level within structures must be located below street level or separated from street level by other uses • Up to four levels of above grade parking may be permitted if it meets the standards of 23.49.019B
Maximum parking limit for nonresidential uses <ul style="list-style-type: none"> • Parking for nonresidential uses is limited to one parking space per every 1,000 square feet of gross floor area in nonresidential use. • Parking for nonresidential uses may be permitted to exceed the maximum standard as a special exception as granted by the Director. • Access to parking and loading shall be from the alley when the lot abuts an improved alley, unless the Director approves an alternate access route.
Bicycle Parking (Minimums): <ul style="list-style-type: none"> • Office: 1 space per 5,000 SF • Hotel: .05 spaces per hotel room • Retail use over 10,000 SF: 1 space per 10,000 SF • Residential: 1 space for every 2 dwelling units
After the first 50 spaces are provided additional spaces are required at 1/2 the ratio noted
Structures containing more than 250,000 SF of office space shall include shower facilities
Off-street loading spaces shall be provided per 23.54.030

23.49.022 Minimum sidewalk and alley width

Standard
Minimum sidewalk width on Blanchard Street, Bell Street, 7th Avenue and 8th Avenue: 12'. Minimum alley width: 20', achievable through setback or dedication if required.

23.49.041 Combined lot development

Standard
When authorized by the Director pursuant to this section, lots located on the same block in DOC1 or DOC2 zones, or in DMC zones with a maximum FAR of ten (10), or lots zoned DOC1 and DMC on the same block, may be combined, whether contiguous or not, solely for the purpose of allowing some or all of the capacity for chargeable floor area on one such lot under this chapter to be used on one (1) or more other lots, according to the following provisions outlined in 23.49.041-A through D
The Director shall allow combined lot development only to the extent that the Director determines, in a Type I land use decision, that permitting more chargeable floor area than would otherwise be allowed on a lot shall result in a significant public benefit. In addition to features for which floor area bonuses are granted, the Director may also consider the following as public benefits that could satisfy this condition when provided for as a result of the lot combination: <ul style="list-style-type: none"> • preservation of a landmark structure located on the block or adjacent blocks; • uses serving the downtown residential community; • public facilities serving the Downtown population; • transportation facilities promoting pedestrian circulation and transit use; • Short-term parking on blocks within convenient walking distance of the retail core or other Downtown business areas ; • a significant amount of housing serving households with a range of income levels; • improved massing of development on the block that achieves a better relationship with surrounding conditions; • public view protection within an area; and/or • arts and cultural facilities, including a museum or museum expansion space.
See SMC 23.49.041-D for full conditions.

23.49.045 Parking

Standard
Principal use parking garages for short-term parking may be permitted as conditional use.
In DMC zones, principal use long-term and short-term surface parking may be permitted as administrative conditional use.
Accessory parking garages for both long-term and short-term parking are permitted outright up to the maximum parking limit established by 23.49.019

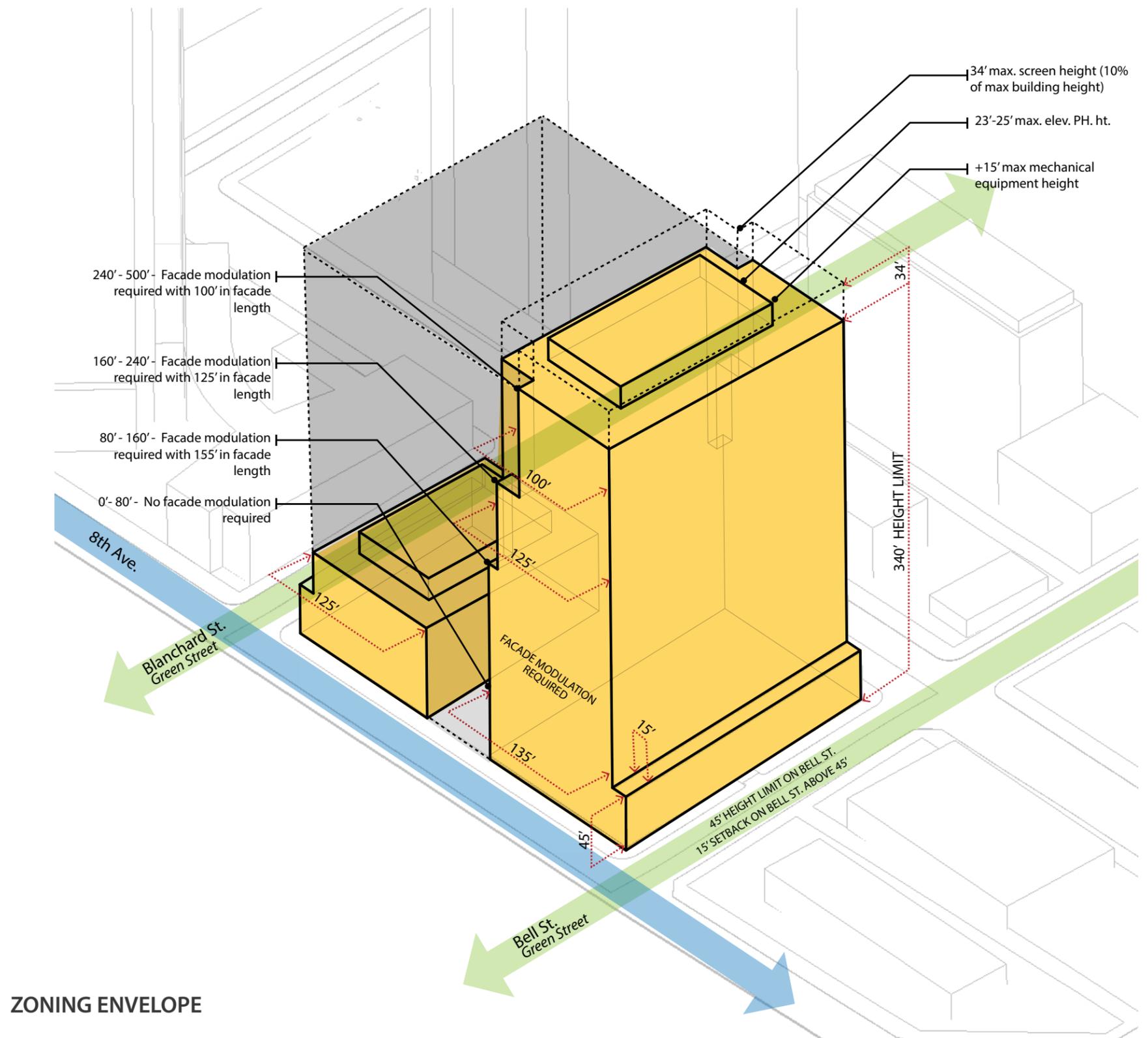
23.49.056 street façade, landscaping and street setback requirements

Standard
Minimum façade heights: <ul style="list-style-type: none"> • 7th and 8th Avenues (class II pedestrian streets): 15' • Blanchard Street and Bell Street (green streets): 25'
Setbacks <ul style="list-style-type: none"> • The max. area of all setbacks shall not exceed the area derived by multiplying the averaging factor by the width of the street frontage. The averaging factor is... ten on Class II pedestrian streets and designated green streets. • The maximum setback of the facade from the street lot lines at intersections is 10 feet. Minimum conforming distance is 20 feet along each street. • Any exterior open space that meets amenity standards is not considered part of the setback area. • If a sidewalk is widened into the lot as a condition of the development setback shall be measured from the line of the new sidewalk.
Transparency and blank façade requirements: <ul style="list-style-type: none"> • Along 7th and 8th Avenues (class II ped. street) 30% of street façade to be transparent between 2' and 8' above sidewalk level. • Along Bell and Blanchard Streets (green streets) 60% of street façade to be transparent between 2' and 8' above sidewalk level. • On 7th and 8th Avenues blank façades limited to segments 30' except for garage doors which may be wider than 30'. • On 7th and 8th Avenues the total of all blank façade segments shall not exceed 70% of the street façade. • On Blanchard and Bell Streets blank façades limited to segments 15' except for garage doors which may be wider than 30'. • On Blanchard and Bell Streets the total of all blank façade segments shall not exceed 40% of the street façade. • Blank façade sections shall be separated by transparent area at least 2' wide
Street Trees are required on all streets.
Landscaping in the Denny Triangle Urban Village <ul style="list-style-type: none"> • All areas abutting a street lot line that are not covered by a structure, have a depth of 10 feet or more, and are larger than 300 SF shall be landscaped. • Setbacks required to meet minimum sidewalk widths shall be exempt from landscape requirements.

ZONING & CODE ANALYSIS + ZONING ENVELOPE

23.49.058 Upper-Level Development Standards

Standard
<p>"Tower" Definition</p> <ul style="list-style-type: none"> Any structure where a portion is above a height of 85 feet in a structure that has any nonresidential use above 65 feet or does not have residential use above a height of 160 feet.
<p>Façade modulation and upper-level width limits apply to:</p> <ul style="list-style-type: none"> Structures 160' in height or less in which any story above 85' exceeds 15,000 SF Portions of structures in non-residential use above a height of 160' in which any story above an elevation of 85' exceeds 15,000 SF.
<p>Façade Modulation (non-residential)</p> <ul style="list-style-type: none"> Required of street facing facades within 15' of street above 85'. Maximum façade length without modulation within 15' of street lot line: <ul style="list-style-type: none"> 155' façade length from elevation 86 to 160 feet. 125' façade length from elevation 161 to 240 feet. 100' façade length from elevation 241 to 500 feet. 80' façade length for elevations above 500 feet. Modulation defined as at least 15' deep step back from property line at least 60' long.
<p>Upper Level Width Limit</p> <ul style="list-style-type: none"> On lots where the width and depth of the lot each exceed 200 feet, the maximum façade width for any portion of a building above 240 feet shall be 145 feet along the general north/south axis of a site.
<p>Tower Separation</p> <ul style="list-style-type: none"> On DMC sites zoned with a maximum height limit of more than 160' located in the Denny Triangle Urban Village, if any part of a tower exceeds 160' then all portions of the tower that are above 125' must be separated by a minimum of 60' from any portion of any other existing tower above 125' in height. From a structure allowed pursuant to the Land Use Code in effect prior to the effective date of March 20th 2006 Ordinance 122054.
<p>Upper level setbacks</p> <ul style="list-style-type: none"> When a lot in a DMC Zone is located on a designated green street, a continuous upper-level setback of 15' shall be provided on the street frontage abutting the green street at a height of 45 feet.

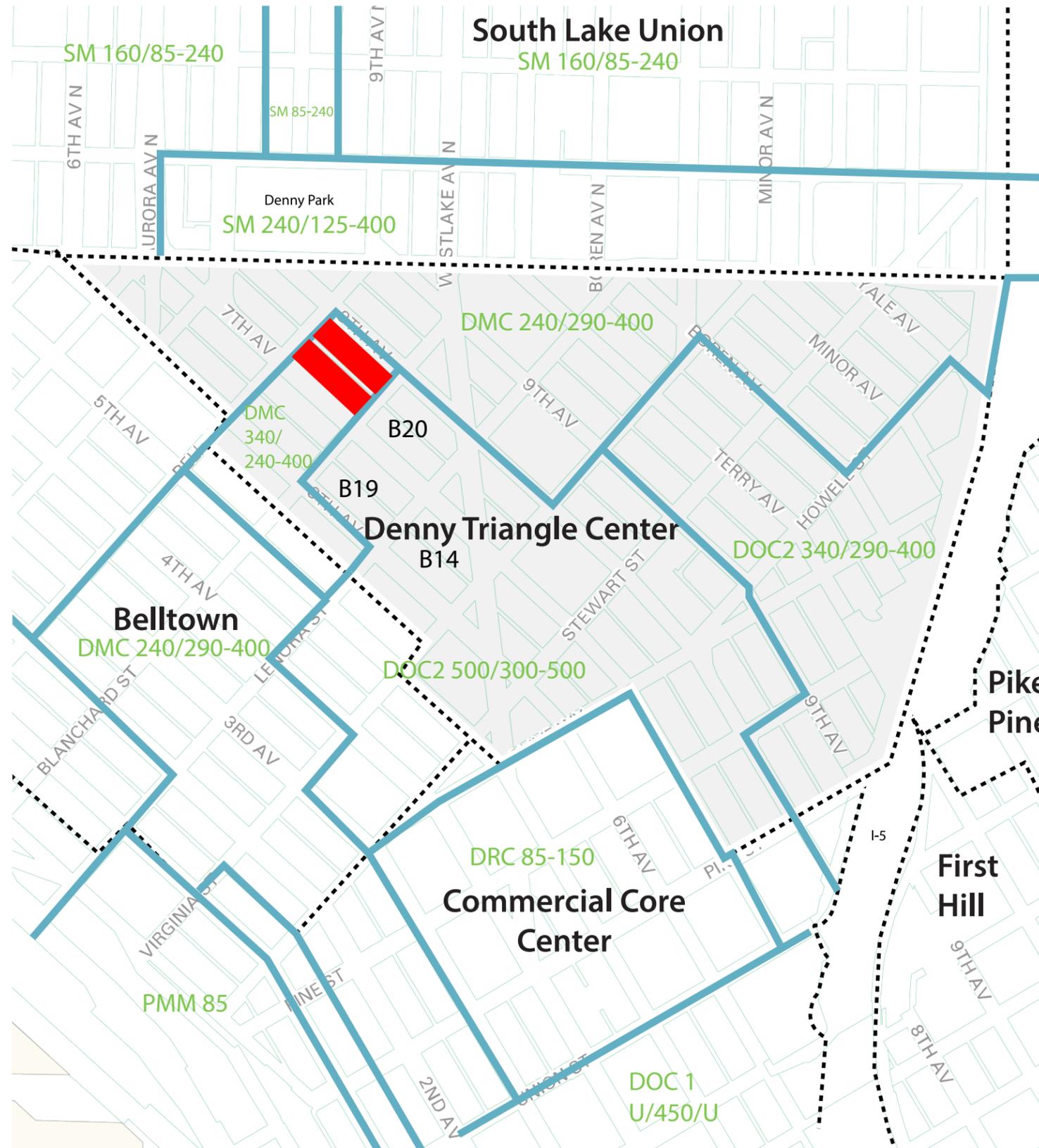


ZONING ENVELOPE

URBAN DESIGN ANALYSIS



URBAN CONTEXT - ZONING DESIGNATION

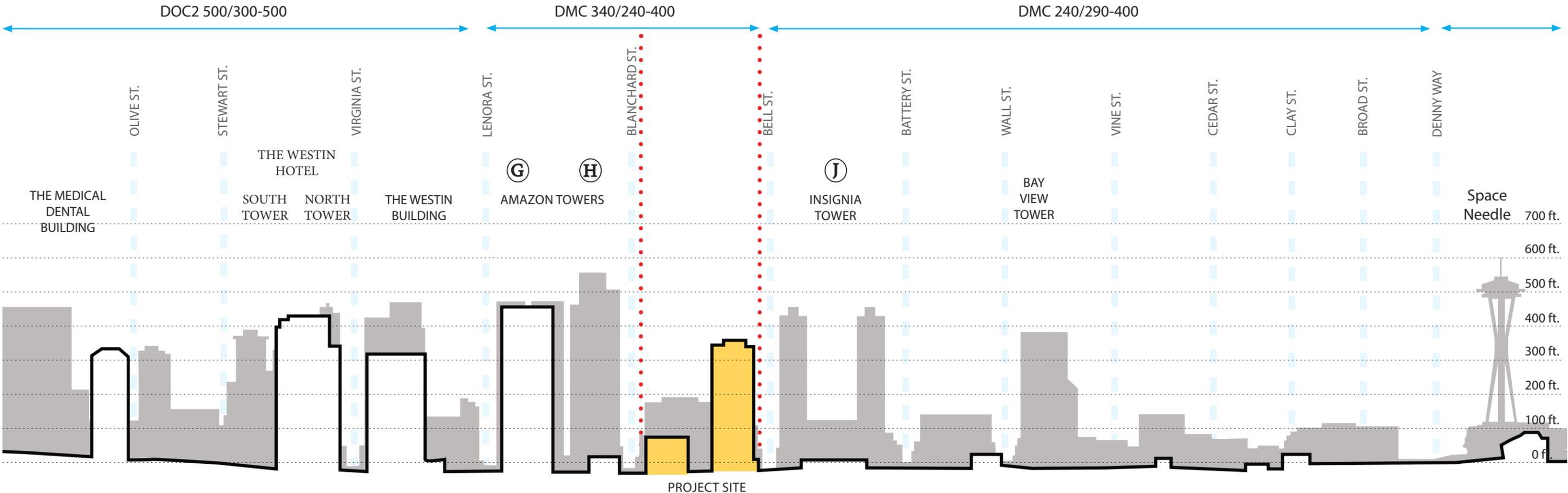


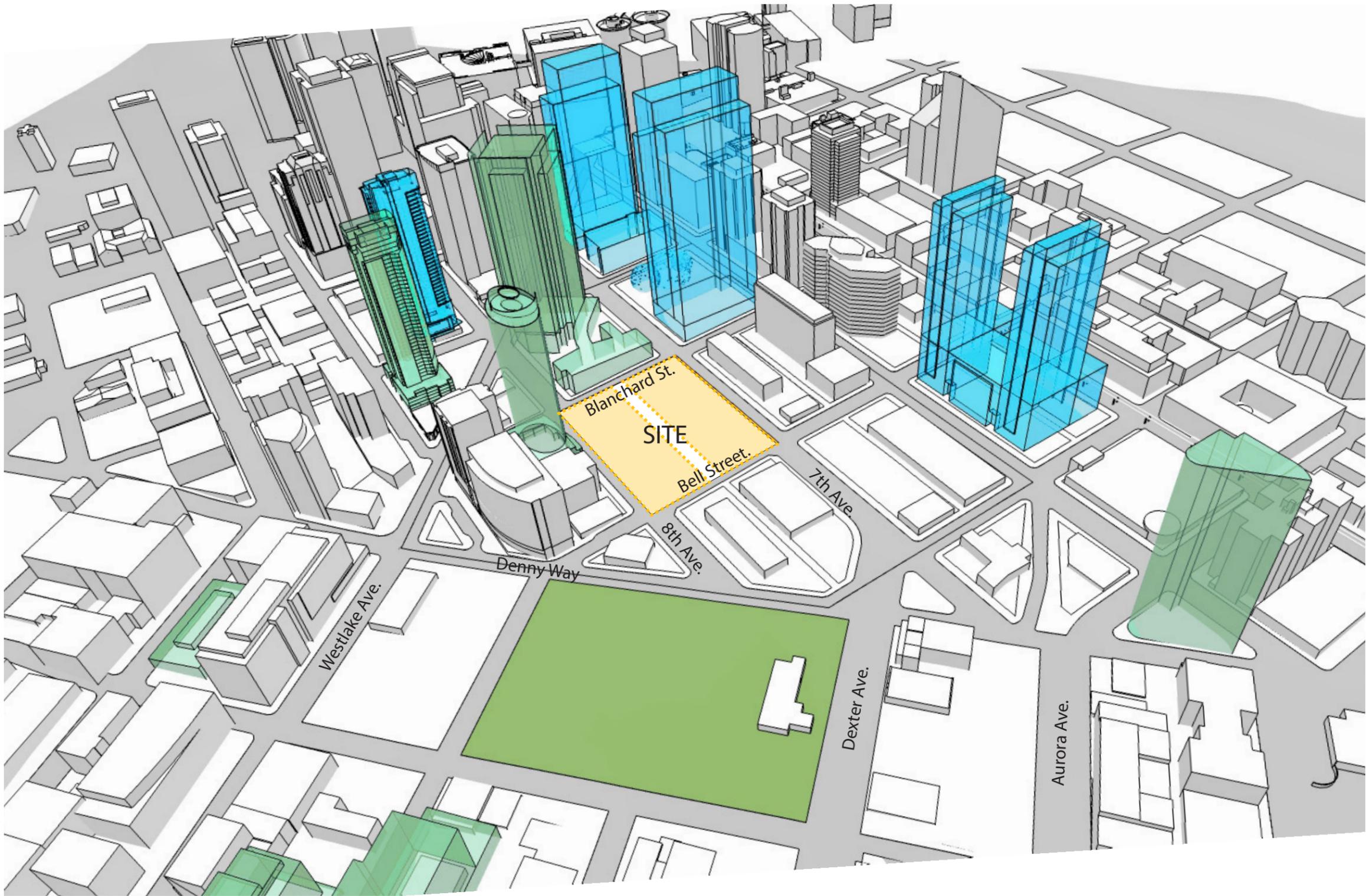
 Project Site

URBAN CONTEXT- SEATTLE SECTION



The site section taken along Eighth Avenue shows the site relative to adjacent zones and their respective height and density limits. Generally allowable heights increase as one transitions south from South Lake Union to the downtown CBD.





- Under Construction
- Future Development



URBAN CONTEXT - STREET CLASSIFICATION

Street Classification DENNY TRIANGLE

The site is convenient for multiple modes of public transportation and is easily accessed by autos, cyclists and pedestrians. The nearby street car stop is at the intersection of Blanchard and Westlake, which is only a ½ block walking distance. Metro bus service is provided on Denny, Dexter and 7th Avenue. Seventh Avenue will be the main access and egress thoroughfare for bicycle traffic with the addition of a cycle track and numerous cross street bike lanes.

-  Monorail Route
-  Street Car Route
-  Class I Pedestrian Street
-  Class II Pedestrian Street
-  Bicycle Lanes
-  Main Bicycle Thoroughfare
-  Green Street
-  Bus Stop
-  Street Car Stop
-  Bicycle Street



URBAN CONTEXT - SURROUNDING BUILDINGS



A. La Quinta Hotel



B. Proposed Apartment Tower



C. 2201 Westlake/Enso



D. 2200 Westlake/ Pan Pacific Hotel



E. Proposed Apartment Tower



F. Apartment Tower Under Construction



G. Office Tower Under Construction



H. Office Tower Under Construction



I. Apartment Towers

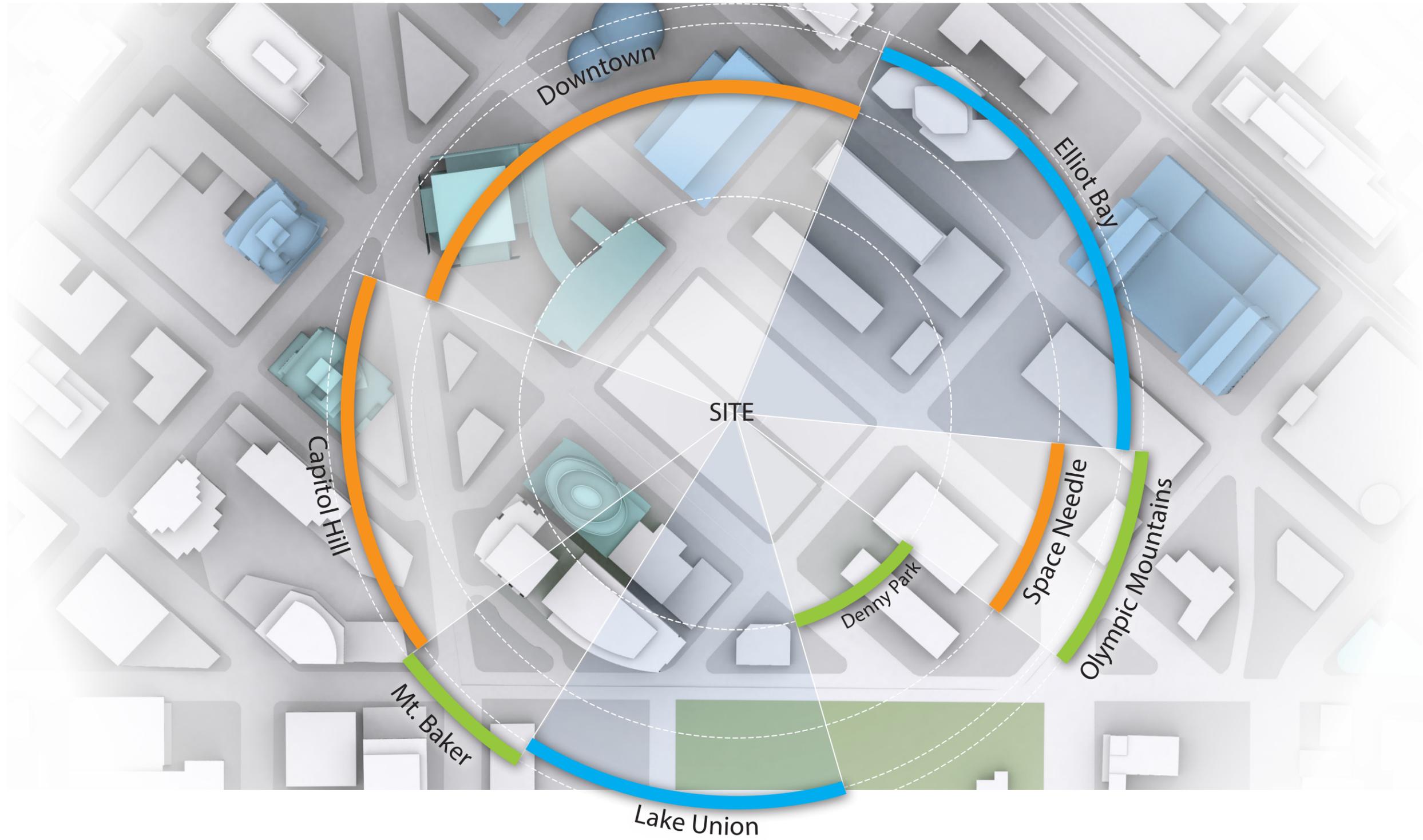


J. Condo Towers Under Construction



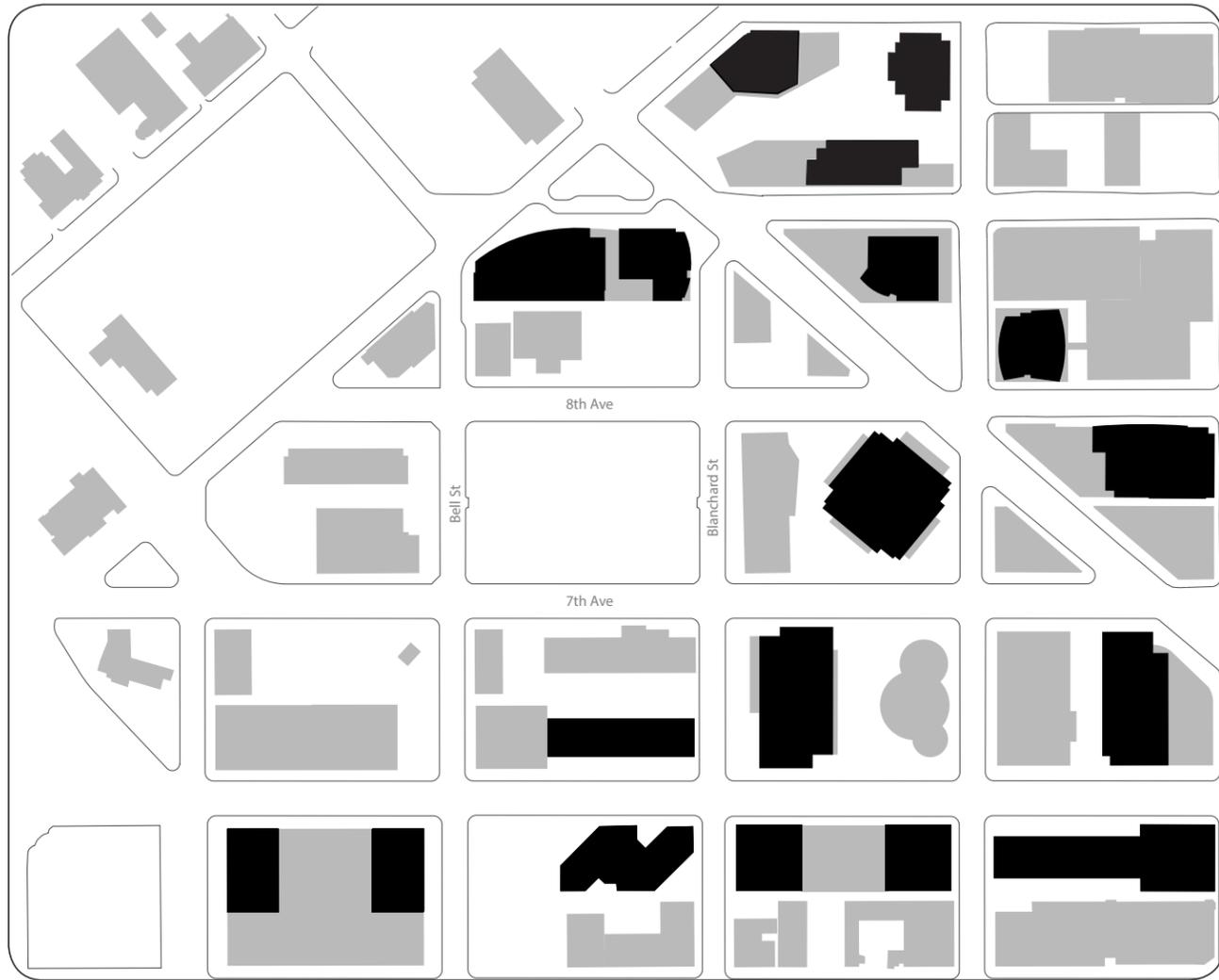
K. Office Tower Under Construction

URBAN CONTEXT - CONTEXT ANALYSIS

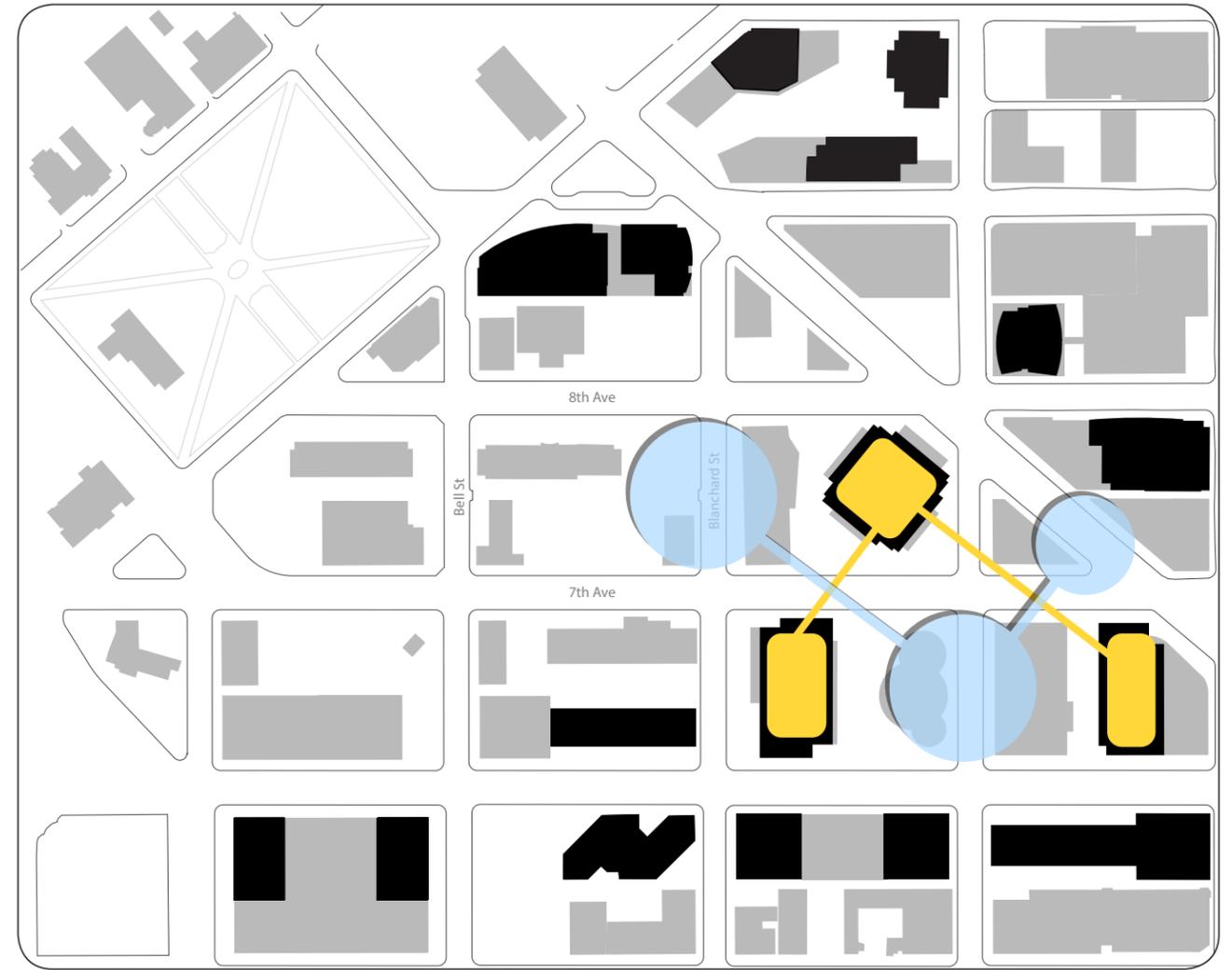




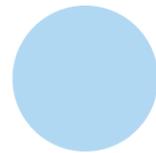
URBAN DENSITY PATTERNS - PRESENT & FUTURE



1. Current



2. Rufus 2.0

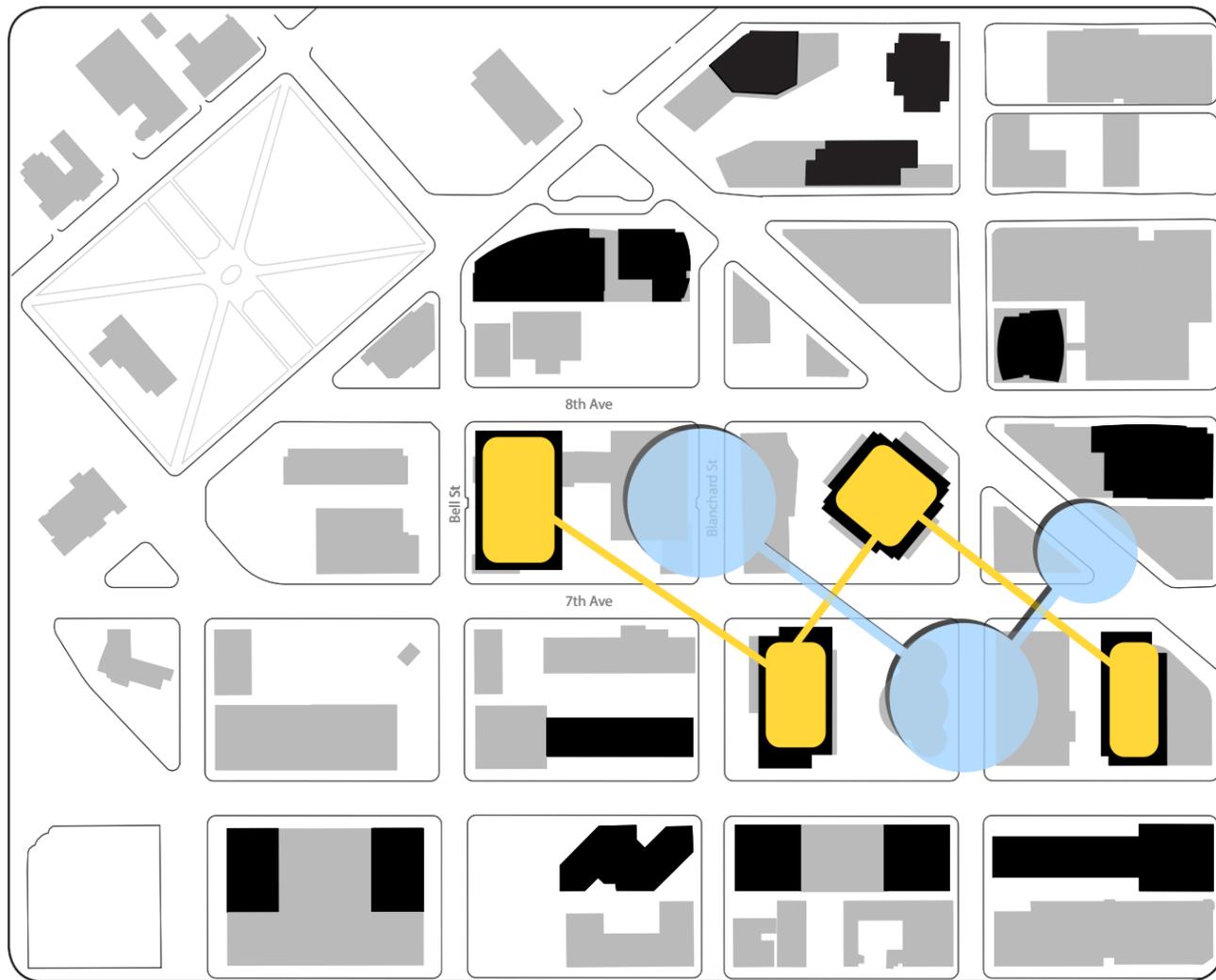


Low-rise / Open Space

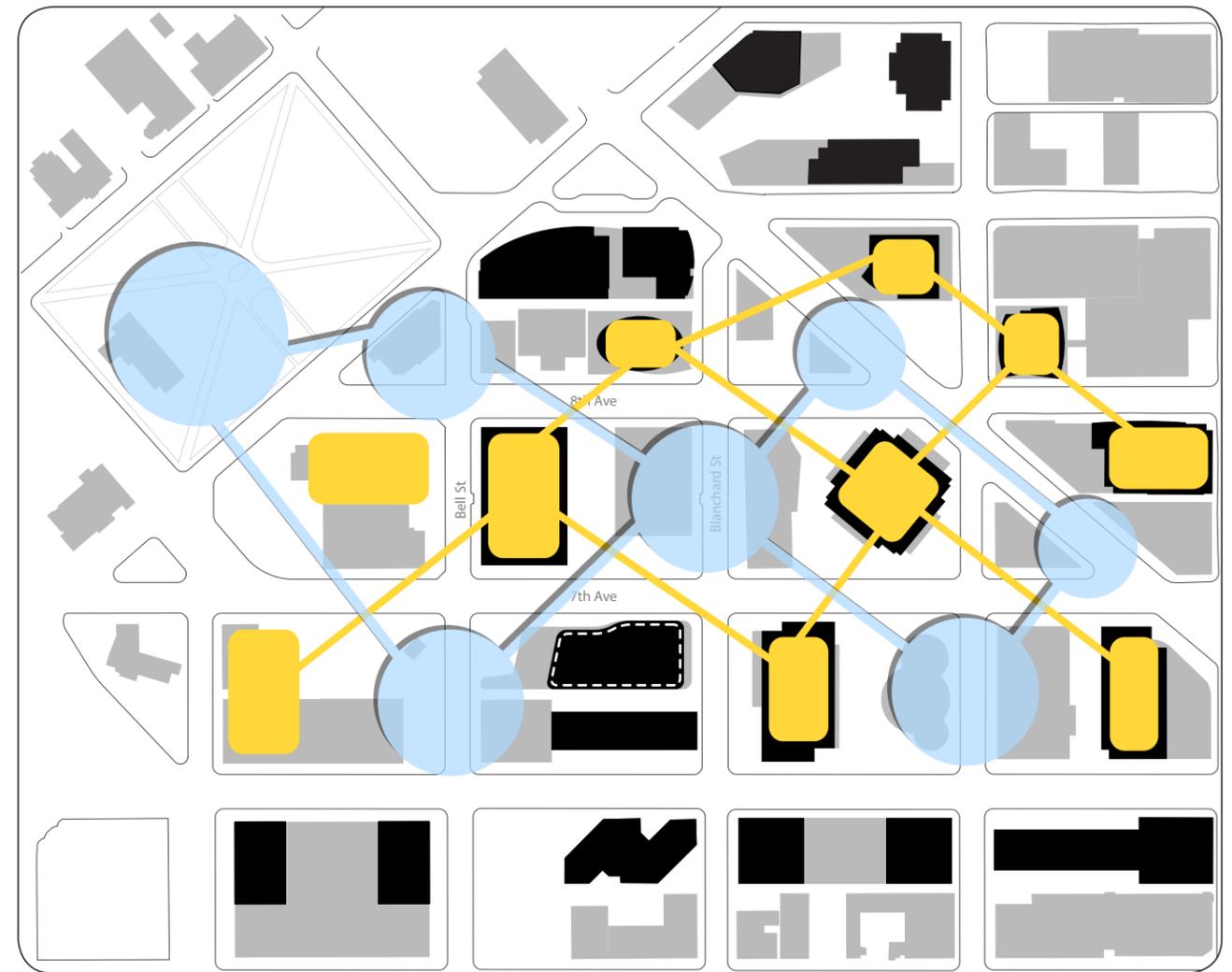


High-Rise Nodes

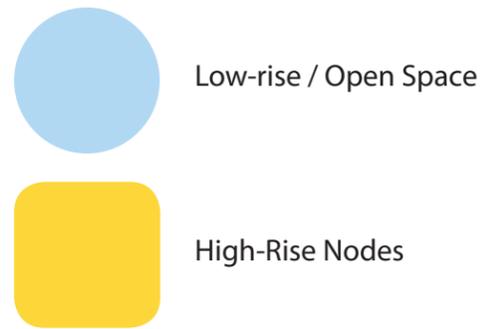




3. Rufus 2.0 + Block 21



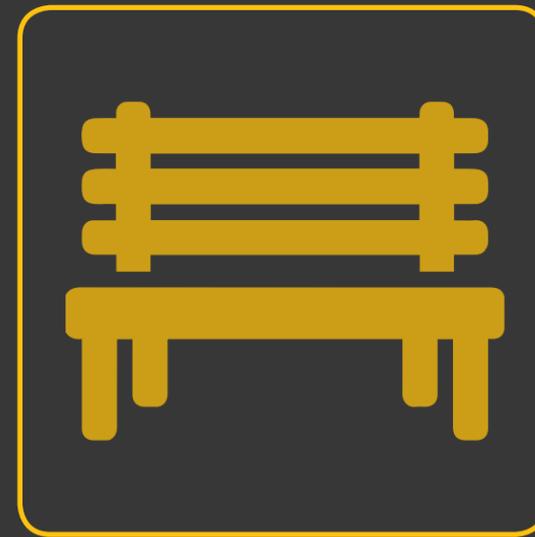
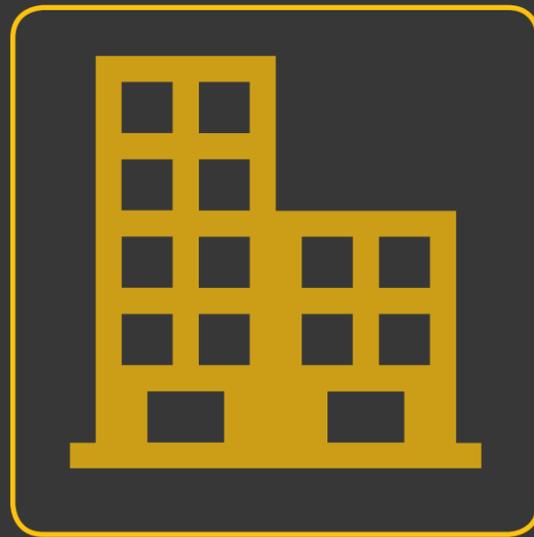
4. Potential Future Development

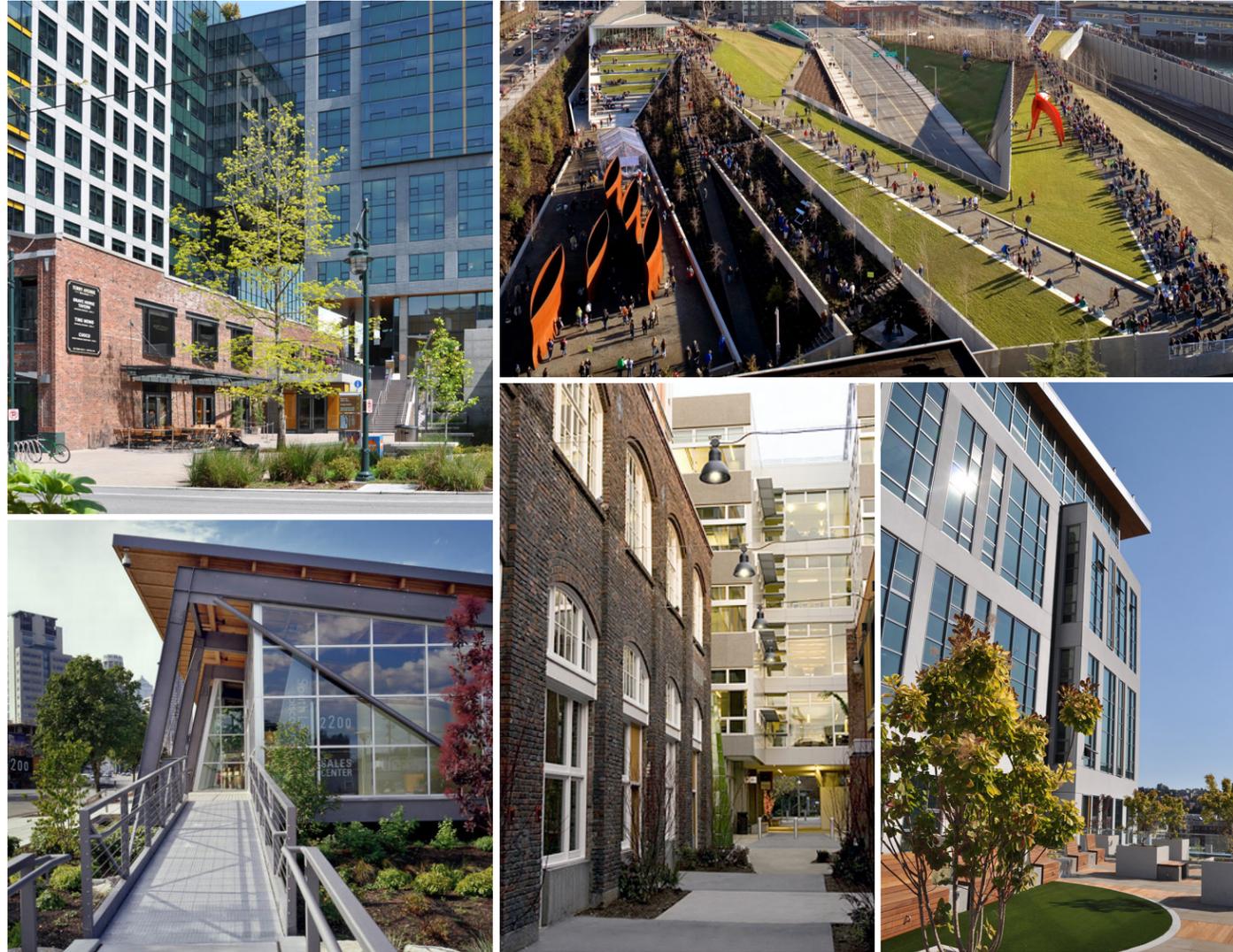


OPEN SPACE CONNECTIONS



DESIGN GUIDELINES





A. Site Planning and Massing

A-2 Respond to the physical environment

Develop an architectural concept and compose the building's massing in response to geographic conditions and patterns of urban form found beyond the immediate context of the building site.

A-2 Enhance the Skyline

Design the upper portion of the building to promote visual interest and variety in the downtown skyline

The proposed design responds to the allowable zoning envelope and resultant presence on the skyline by employing distinctive massing solutions that respond to the surrounding context and significant site slope. The proposal continues the established pattern of urban density of open space. Sculpted building forms and expressive tops will distinguish the proposal from the city skyline and respond to the prominent views of the project from the adjacent South Lake Union neighborhood to the north and Downtown to the south.



B. Architectural Expression

B-2 Create a transition in bulk and scale

Compose a massing for the building to create a transition to the height, bulk and scale of development in neighboring or nearby less-intensive zones.

B-3 Reinforce the positive urban form & architectural attributes of the immediate area

Consider the predominant attributes of the immediate neighborhood and reinforce desirable siting patterns, massing arrangements, and streetscape characteristics of nearby development.

B-4 Design a well-proportioned & unified building

Compose the massing and organize the publicly accessible interior and exterior spaces to create a well-proportioned building that exhibits a coherent architectural concept. Design the architectural elements and finish details to create a unified building, so that all components appear integral to the whole.

The proposal distinguishes the high-rise portion of the tower from the lower zones (aka "podium") through scale, massing and material delineation. The building podium acknowledges the surrounding low- and mid-rise structures by providing setbacks that relate to adjacent structures. The site planning of the buildings follows the existing pattern of giving priority to the southern exposure to public open space and extending the boulevard character of 7th Avenue. As a multi-building proposal, the project unifies the architectural vocabulary of the block by employing complementary materials and detailing that are shared by all structures on the block.



C. The Streetscape

C-1 Promote pedestrian interaction

Spaces for street level uses should be designed to engage pedestrians with the activities occurring within them. Sidewalk-related spaces should be open to the general public and appear safe and welcoming.

C-6 Develop the alley facade

To increase pedestrian safety, comfort, and interest, develop portions of the alley facade in response to the unique conditions of the site or project.

The proposal has been designed to reinforce and enhance existing pedestrian patterns and capitalize on the sites transitional location between the Denny Triangle and South Lake Union.

The proposal is organized to provide a through-block connection and linked public plazas that engage pedestrians. The perimeter street frontage gives priority to active uses such as retail and building entries.



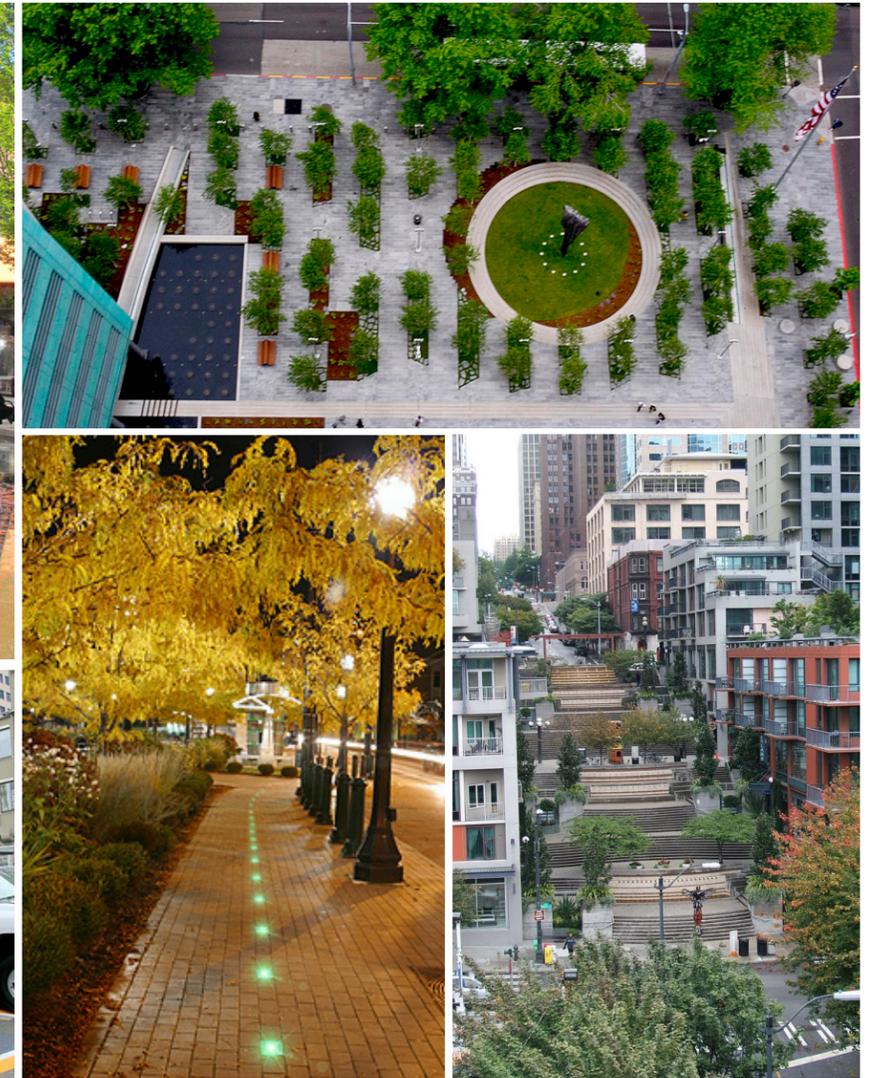
D. Public amenities

D-1 Provide inviting and usable open space

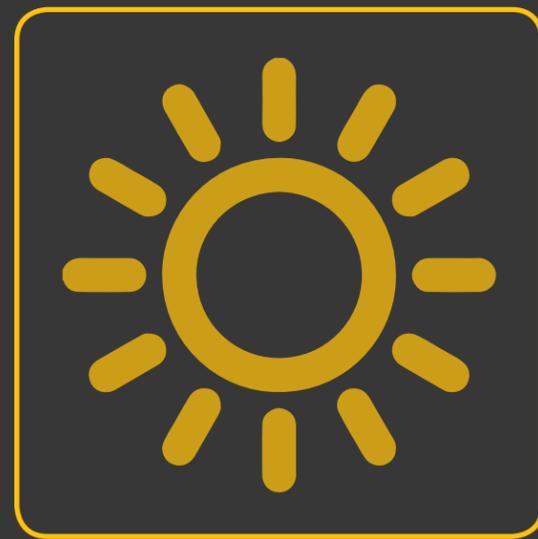
Design public opens spaces to promote a visually pleasing, safe and active environment for workers, residents, and visitors. Views and solar access from the principal area of the open space should be especially emphasized.

The design gives emphasis to high quality open spaces that knit into the existing urban fabric and have been situated to maximize solar exposure.

An open plaza is located on the southwest corner of the block to invite pedestrians into the site and encourage through-block circulation. A complimentary open space on the northeast corner of the site engages pedestrian movement from the north.



SITE ANALYSIS



Site Area:

77,700 square feet plus a public alleyway of approximately 5,700 square feet with approximately 360 FT of frontage on both 7th and 8th avenues, 232 FT of frontage on both Bell and Blanchard streets.

Topography:

The site slopes from elevation 104' 3" in the northwest corner down to 83' 9" in the south-east corner.

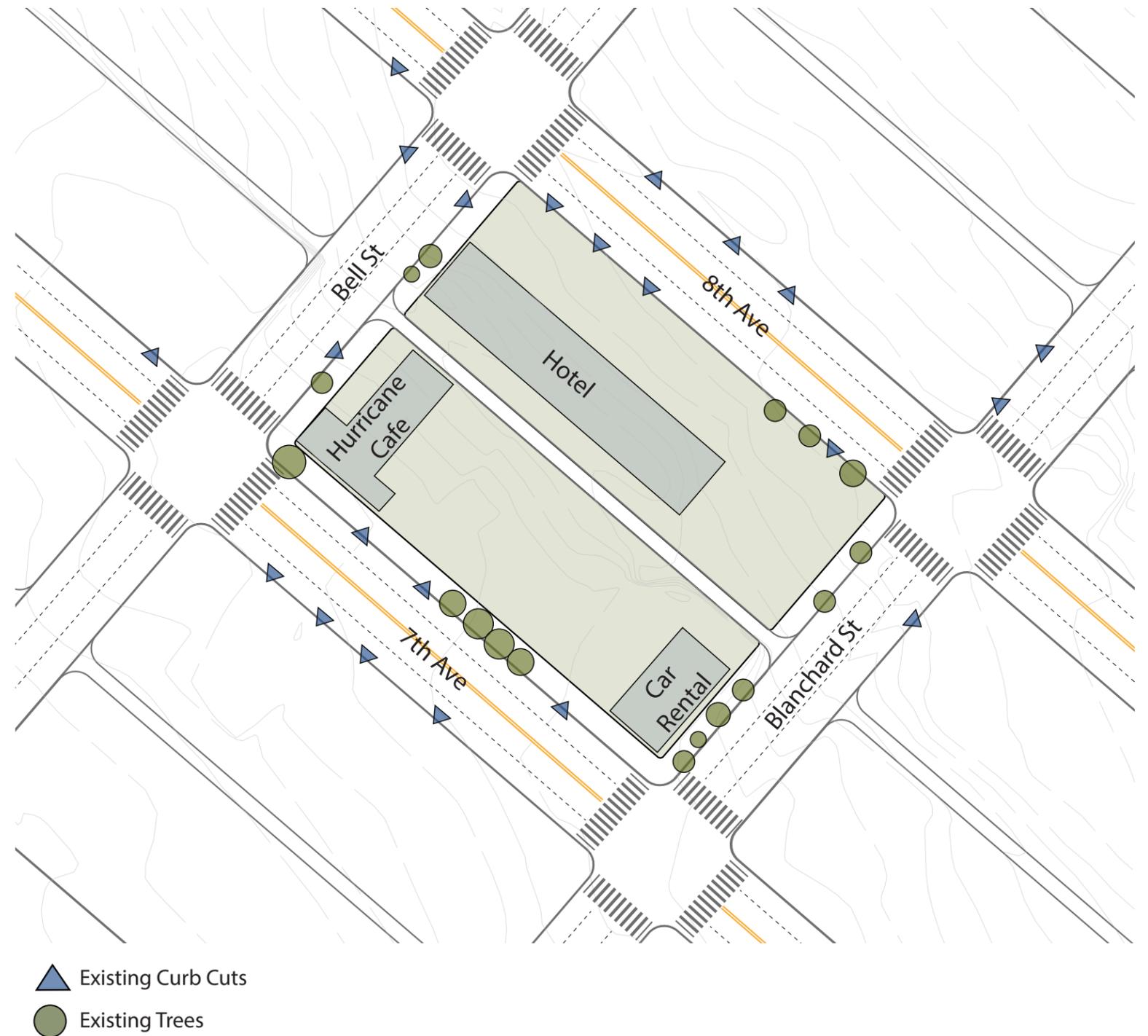
Tree Survey:

There are no significant trees on the site. Trees are located within the sidewalk Right-of-way. Five trees are located along 7th ave, three along 8th ave, six along Blanchard street and three along Bell street.

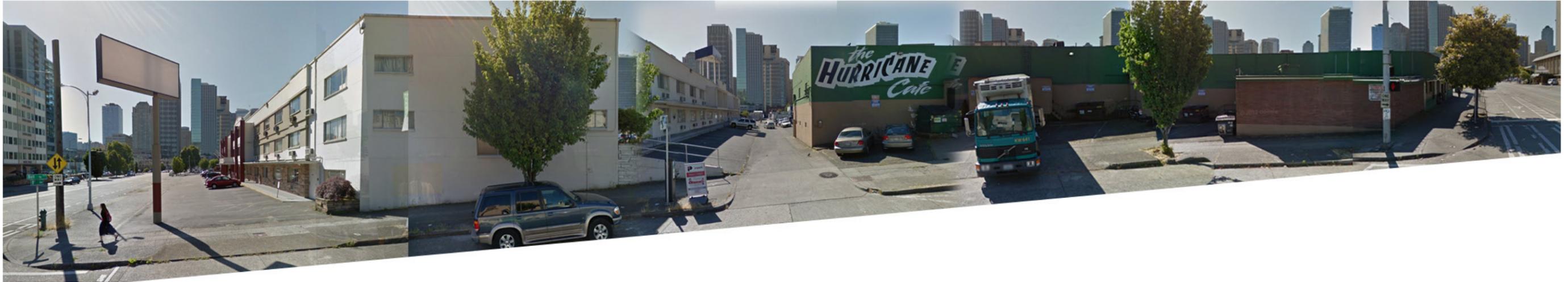
Existing Buildings:

The site has a hotel, restaurant, rental car facility and surface parking lot.

Combined Lot Development Provision
SMC 23.49.041



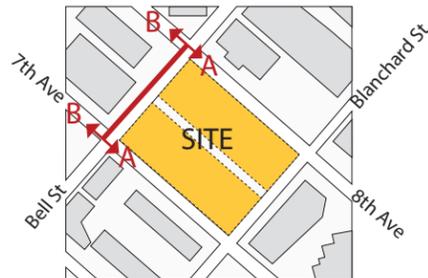
SITE ANALYSIS - PLAN



A. Bell Street Looking South



B. Bell Street Looking North





A. 7th Avenue Looking East



B. 7th Avenue Looking West



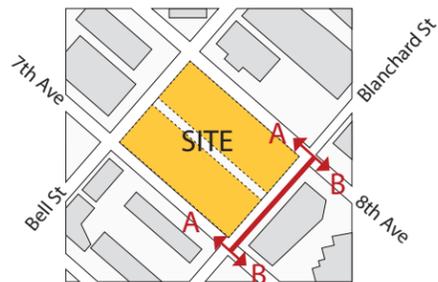
SITE ANALYSIS - PLAN



A. Blanchard Street Looking North



B. Blanchard Street Looking South





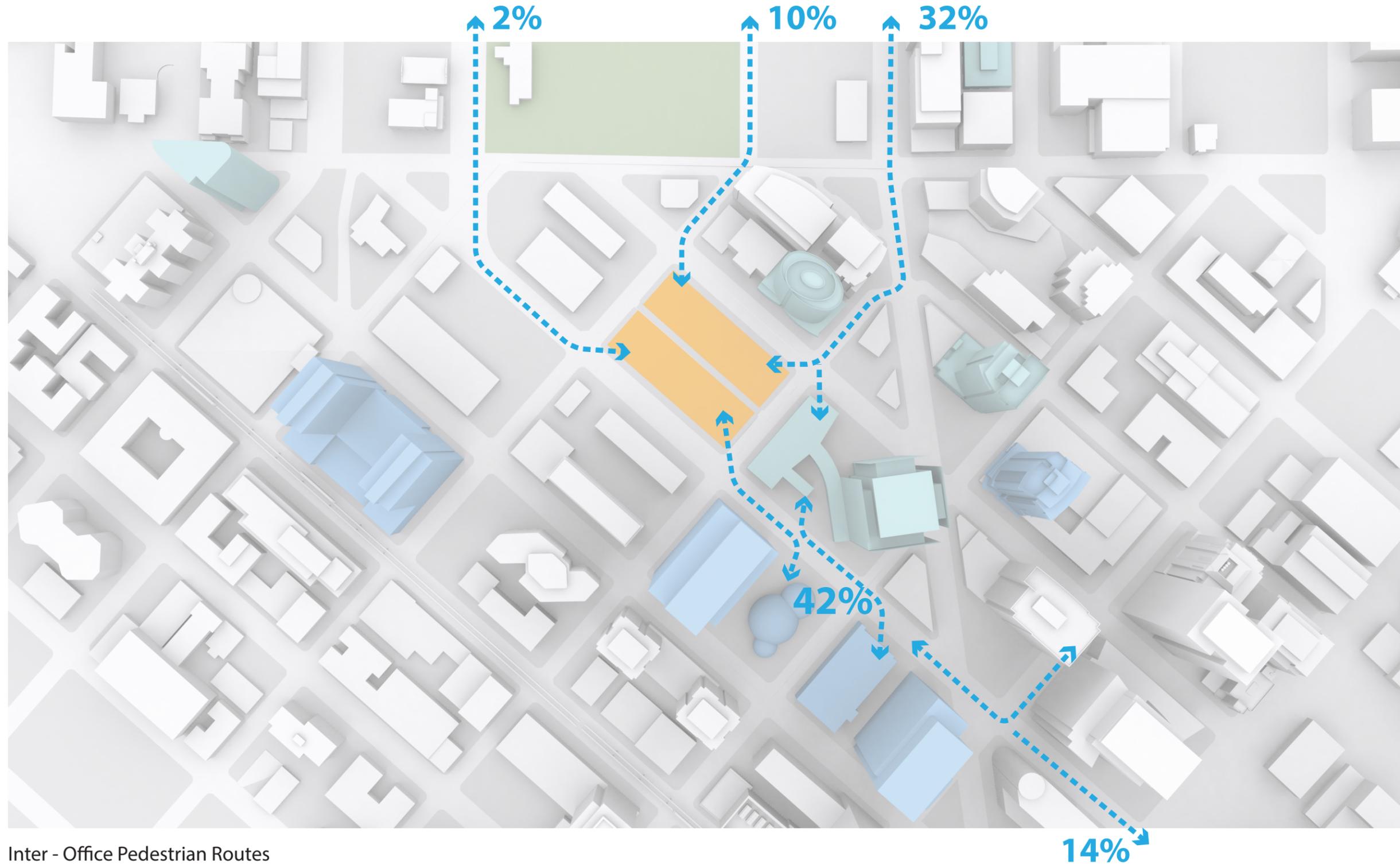
A. 8th Avenue Looking East



B. 8th Avenue Looking West

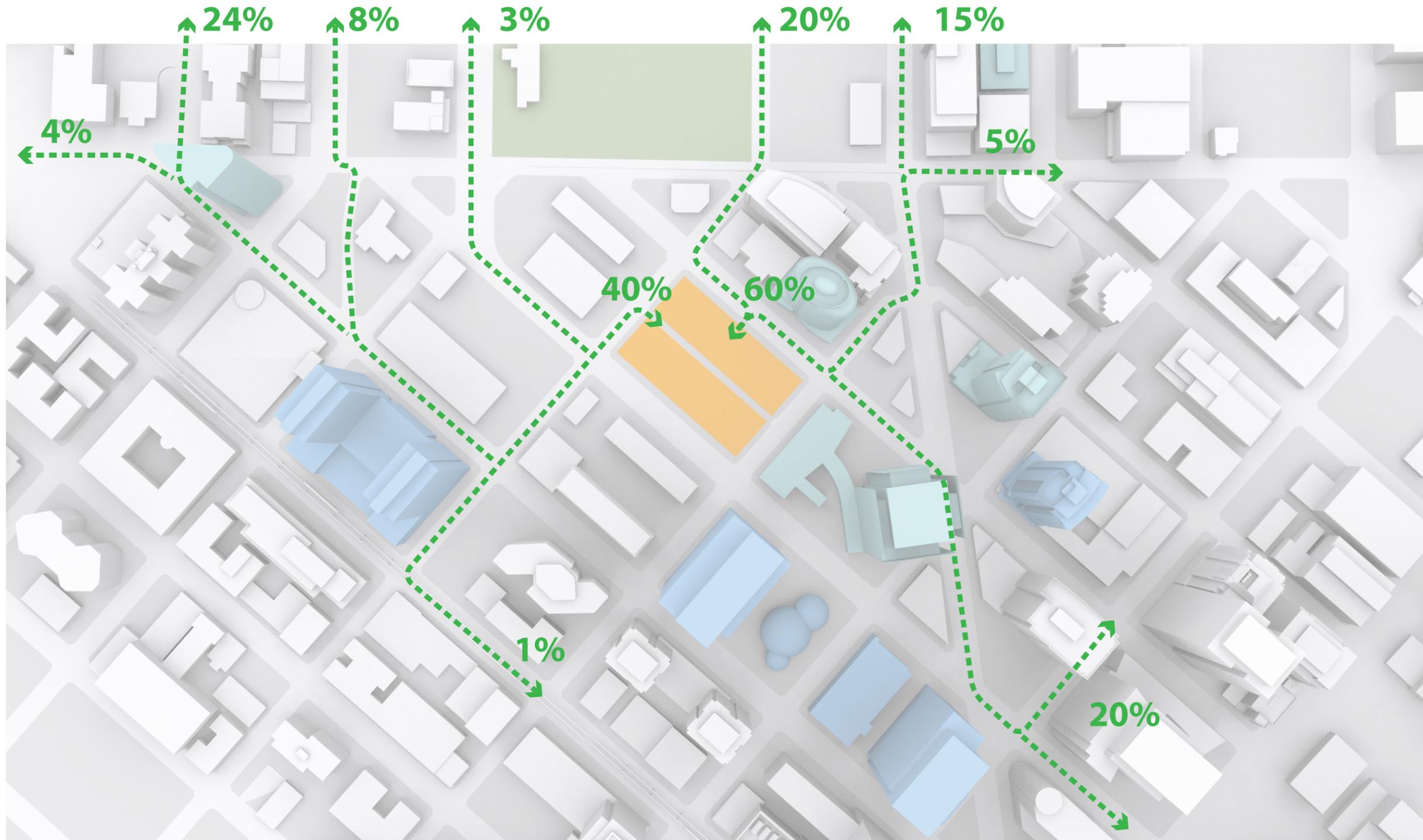


INTER - OFFICE PEDESTRIAN ROUTES



Inter - Office Pedestrian Routes

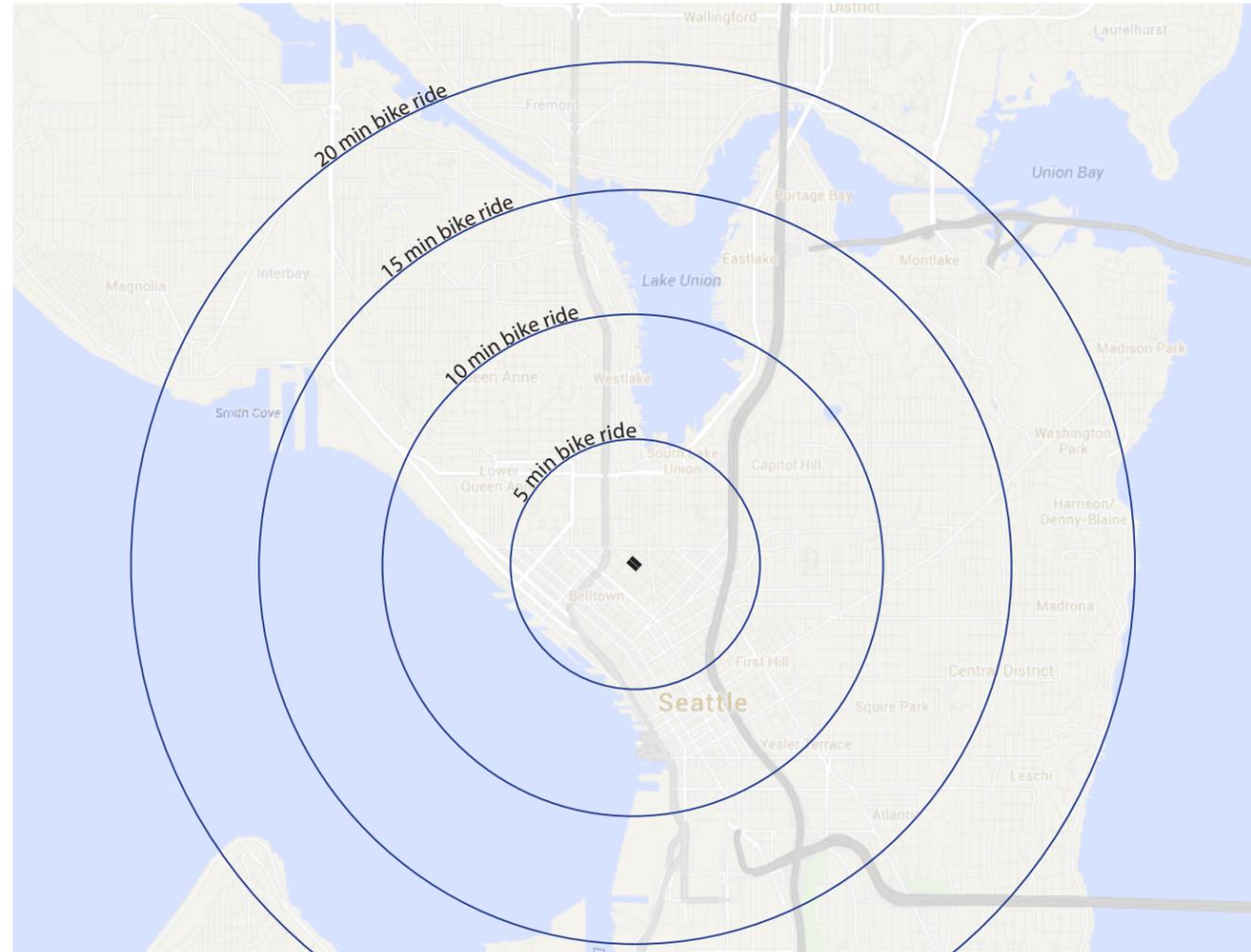




Vehicle Trips - Existing Site



TRAVEL DISTANCES - WALK AND BIKE



Bicycle Travel Time

Bike Score: 86

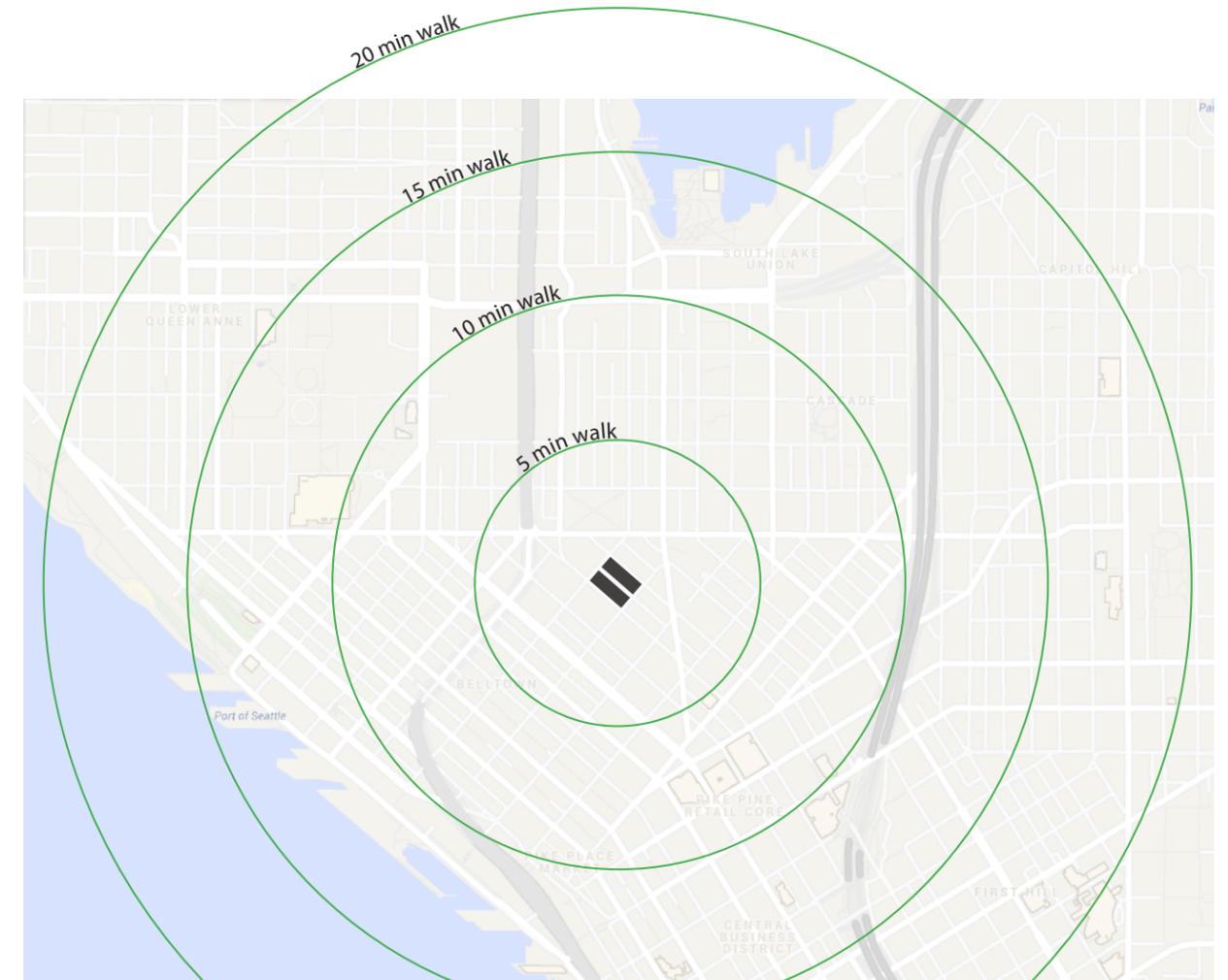
Very Bikeable, flat with excellent bike lanes

Restaurants within .25 miles:

- One Red Pillar Cafe
- Shilia Restaurant
- Far eats
- Bebas Deli market
- Dos Amigos
- Eggs and Plants
- Tio Taco
- Snout and Co. Food Truck
- Tutta Bella Neapolitan Pizzeria
- TanakaSan
- Seastar Restaurant & Raw Bar
- Mio Sushi Westlake

Coffee Shops within .25 miles:

- Cafe Two
- Denny Cafe
- Baristas Coffee Co.
- Starbucks x2
- Dailyz
- Wheelhouse Coffee
- Midtown Coffee
- Top Pot Doughnuts
- Assembly Hall Juice & Coffee
- Yellow Leaf Cupcake Co.
- Cafe Suisse
- Artisan Cafe



Walking Travel Time

Walk Score: 98

Walker's Paradise, daily errands do not require a car

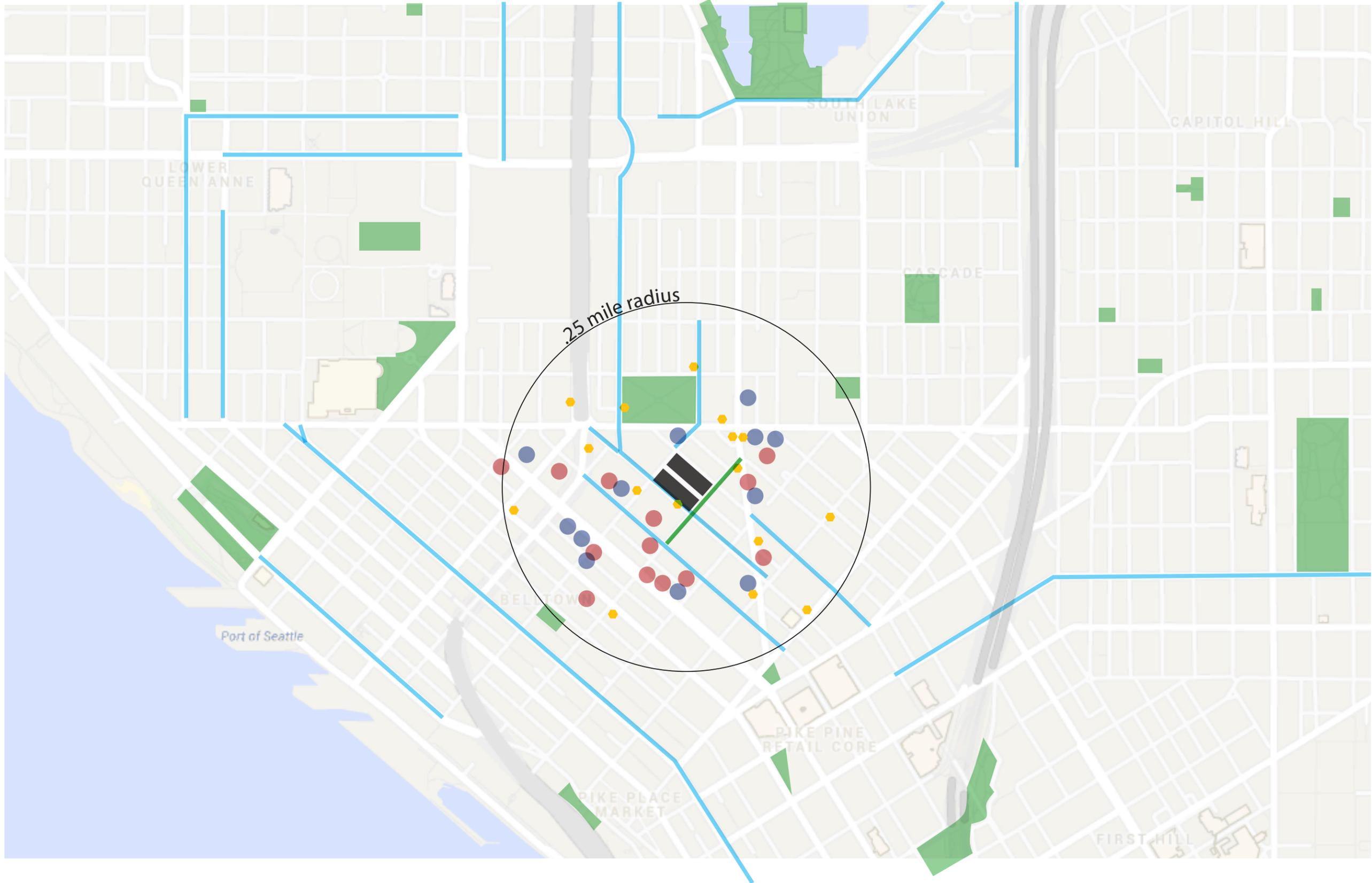
Bus & Rail Lines near by:

- 98
- LINK
- 118
- 7
- 26
- 28
- 62
- 111
- 55
- 114
- 119
- 143

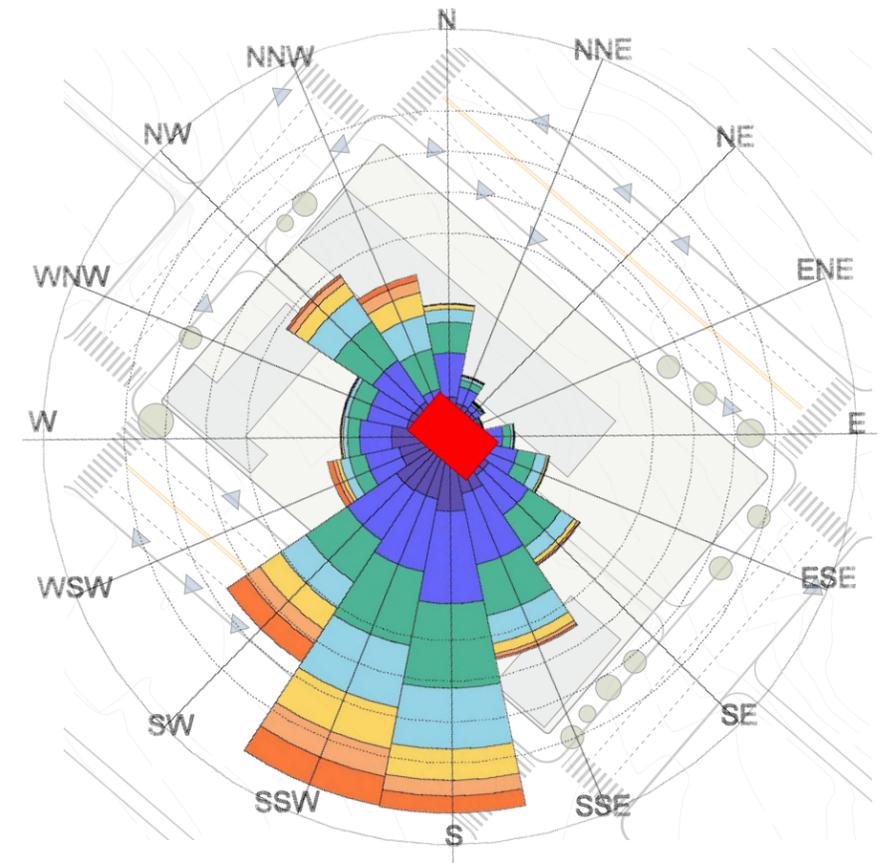
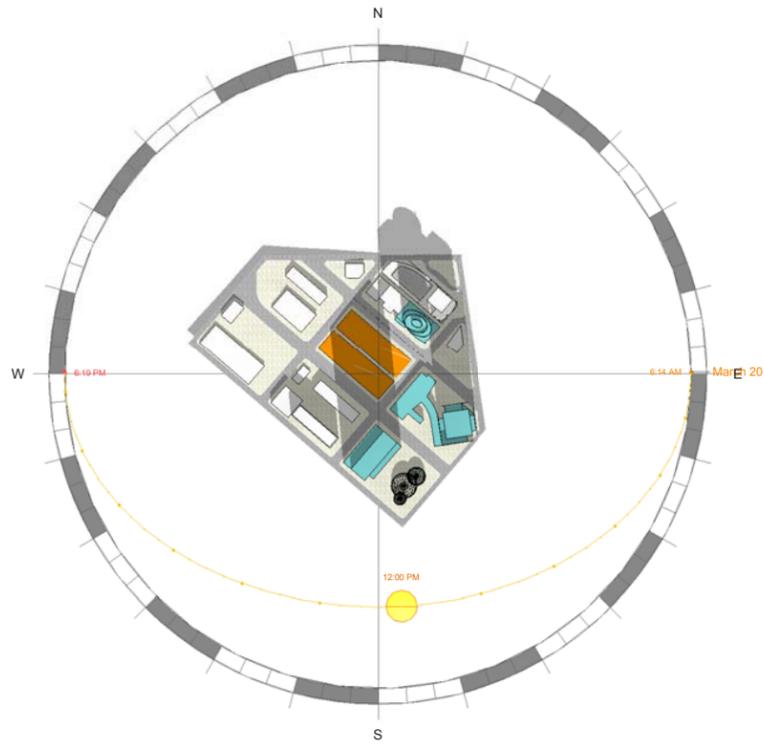
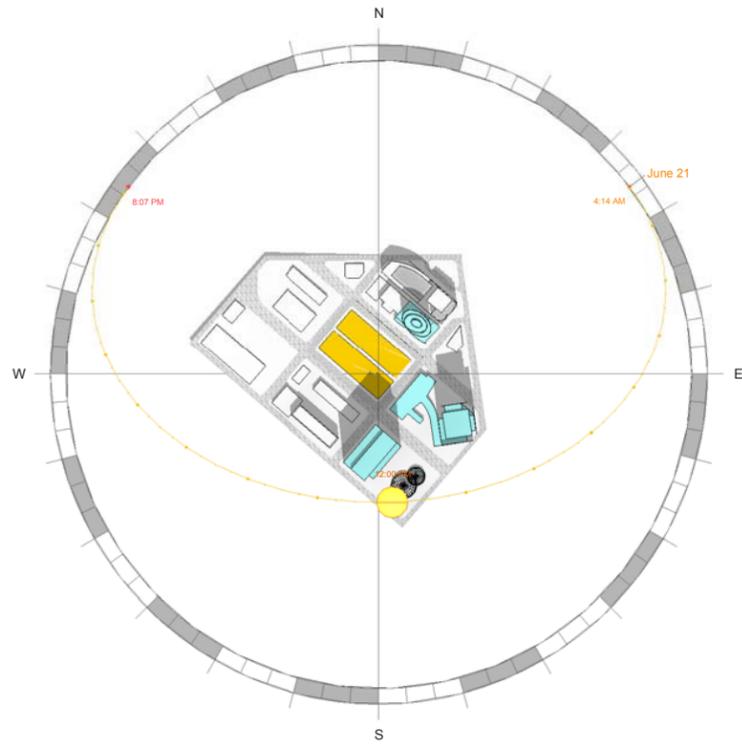
Parks within .6 miles:

1. Denny Park
2. Cascade P-Patch
3. Westlake Park
4. Cascade Playground
5. Victor Steinbrueck Park
6. South Fountain Lawn
7. Olympic Sculpture Park
8. Plymouth Pillars Park
9. Piers 62-63 Park





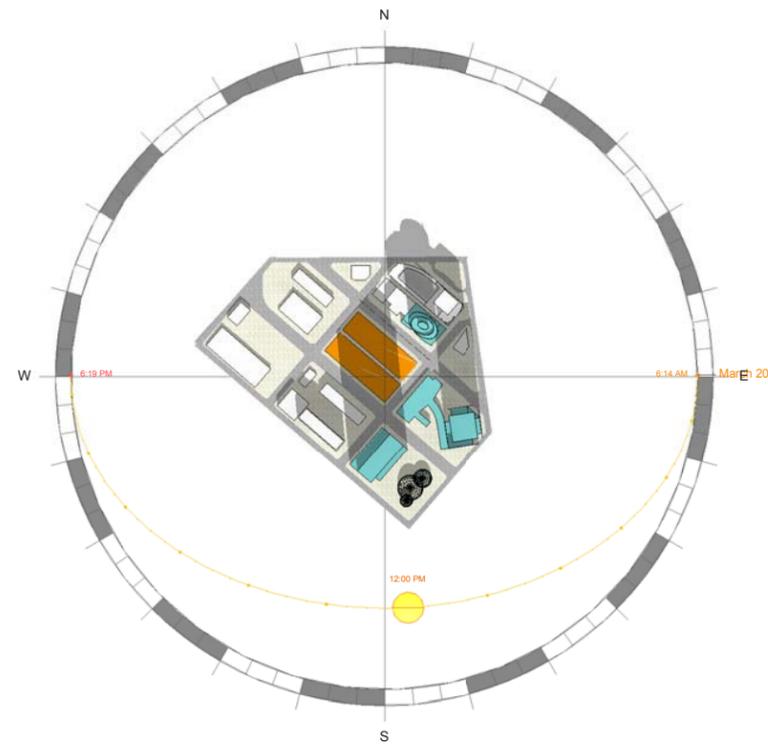
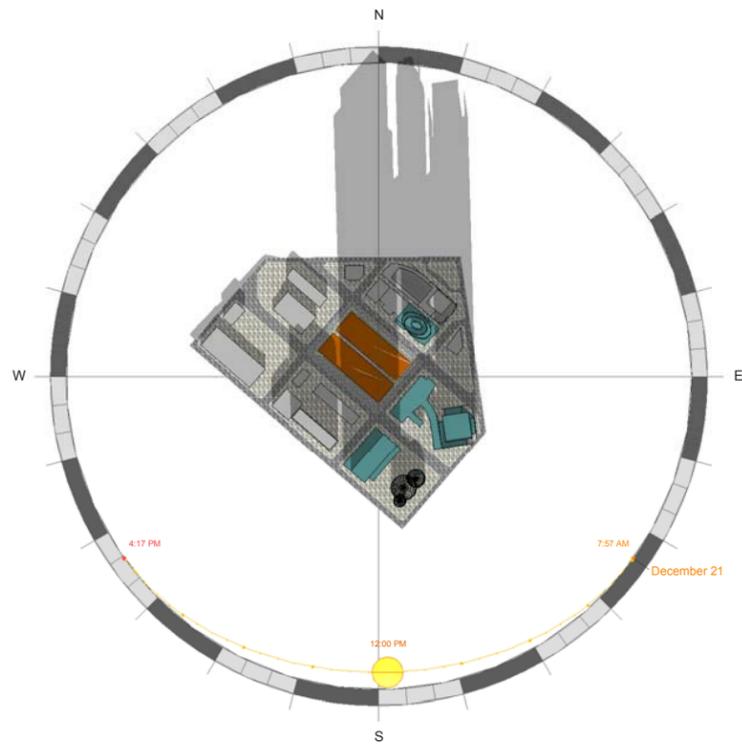
CLIMATE ANALYSIS



① Solar Study - Summer Solstice

② Solar Study - Fall Equinox

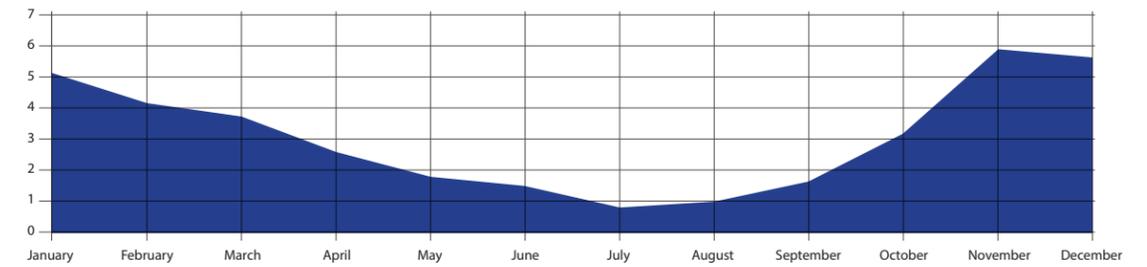
Wind Rose



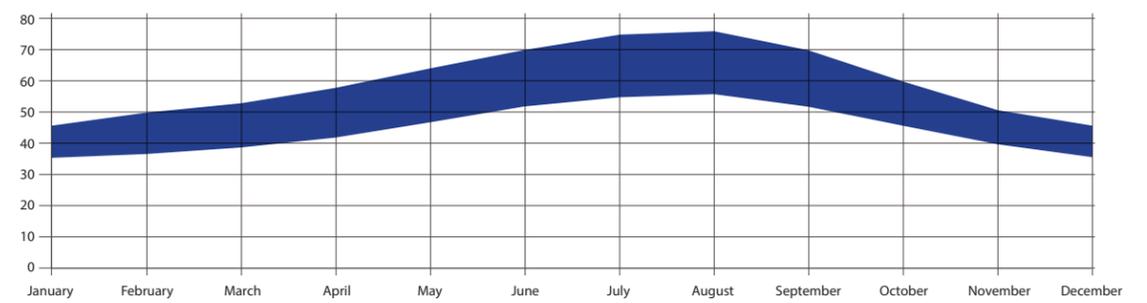
③ Solar Study - Winter Solstice

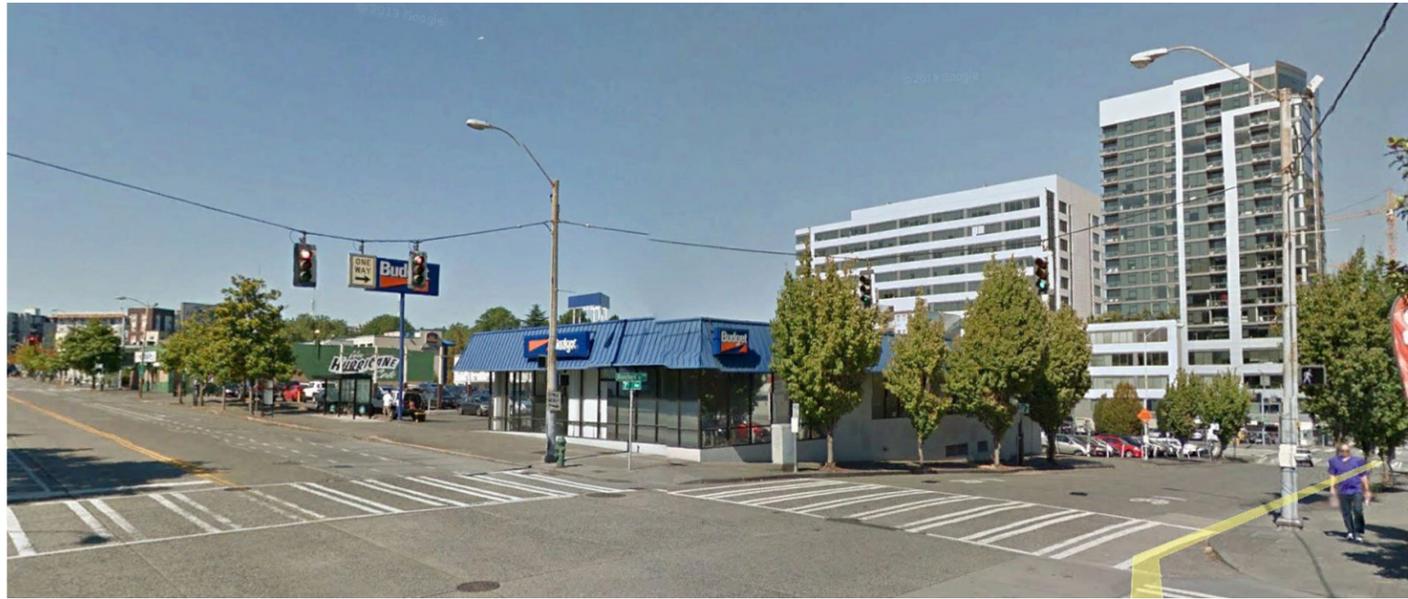
④ Solar Study - Spring Equinox

Average Annual Precipitation



Average Annual Temperature





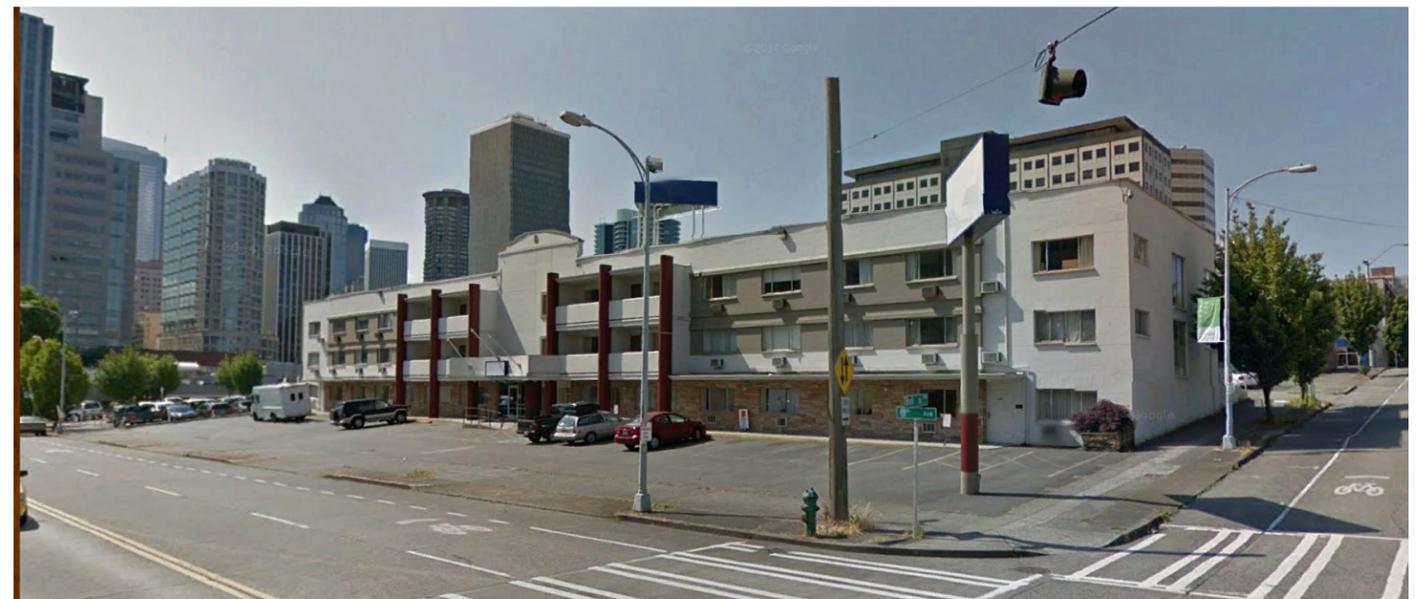
A



B



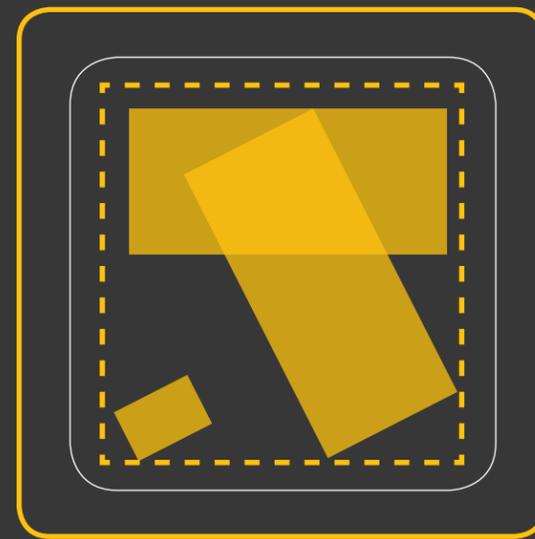
C



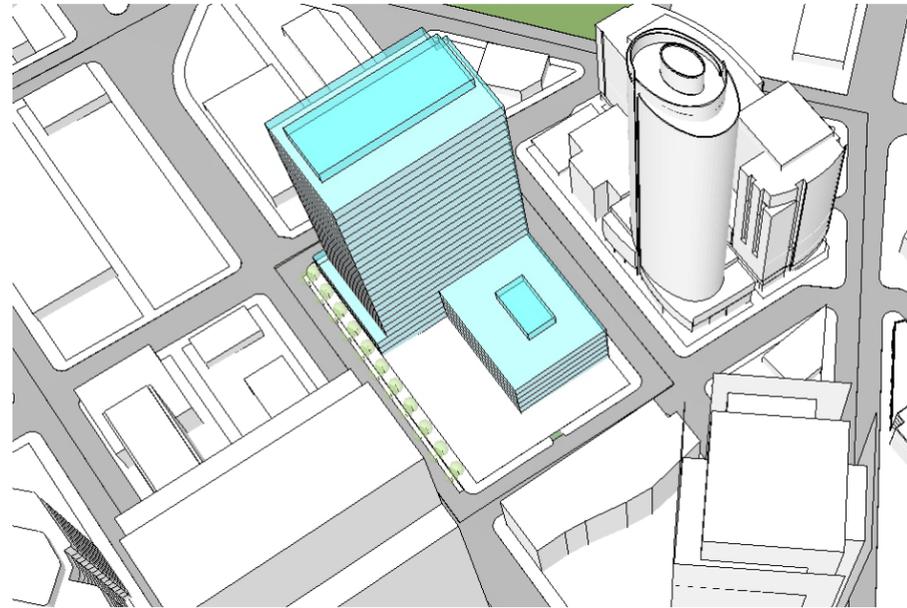
D



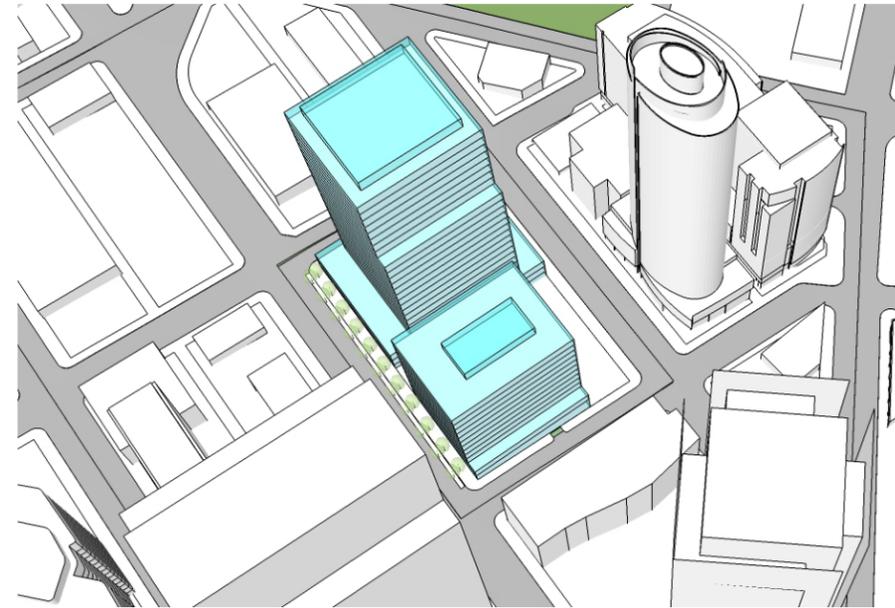
ARCHITECTURAL CONCEPTS



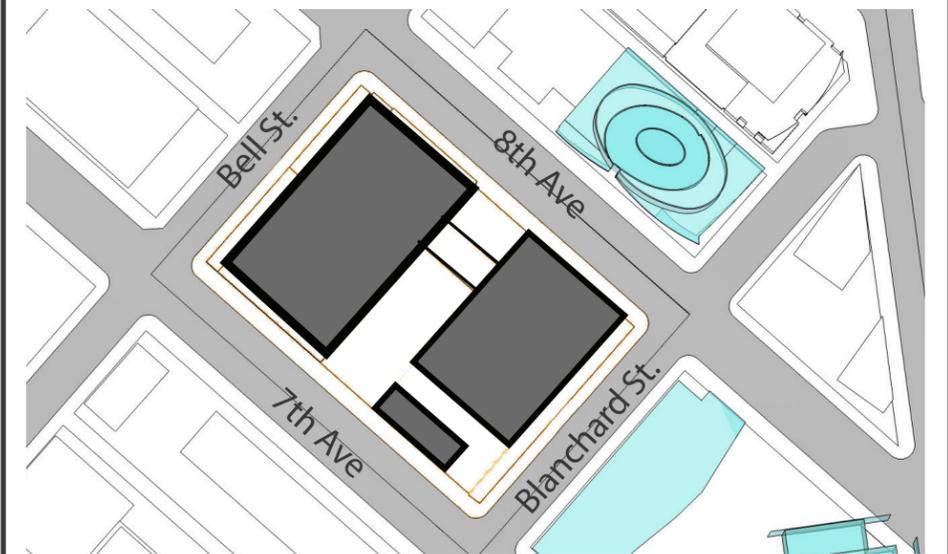
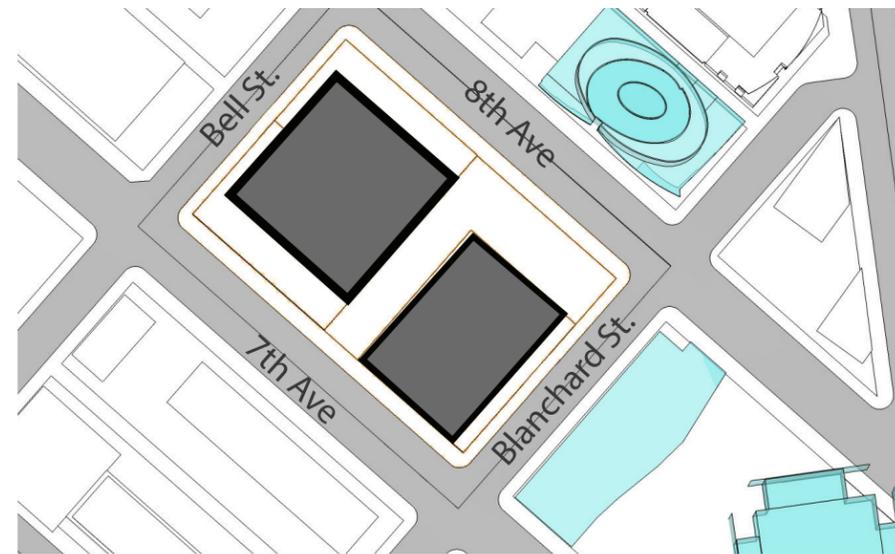
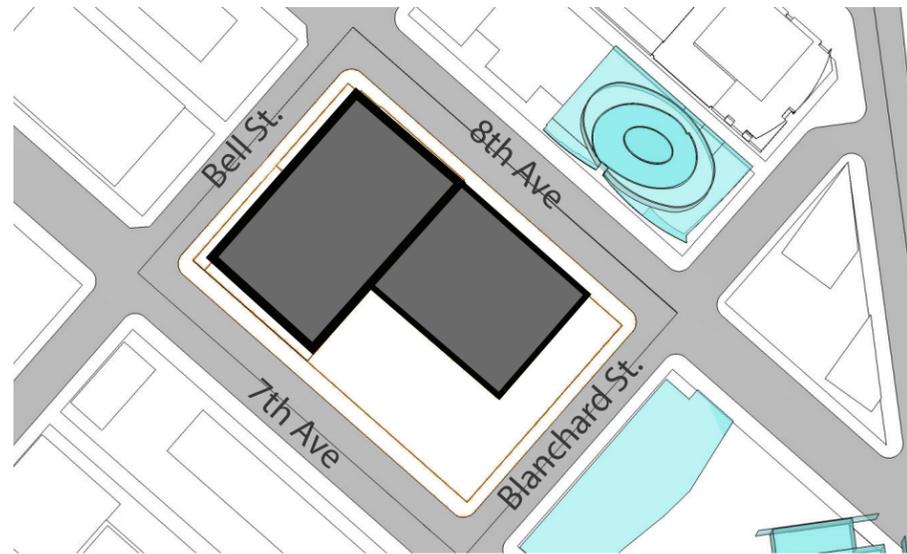
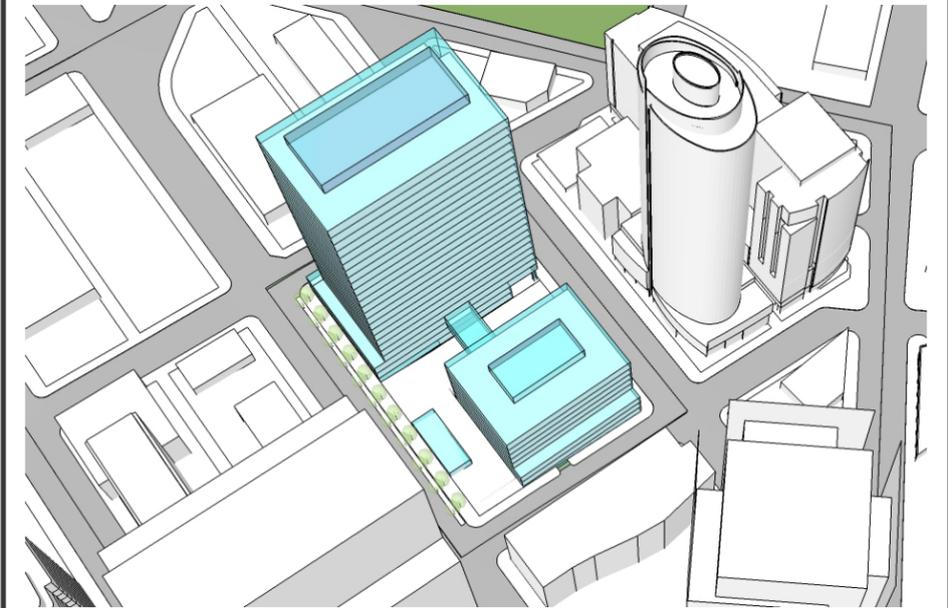
OPTION 1



OPTION 2



OPTION 3 - PREFERRED



Introduction

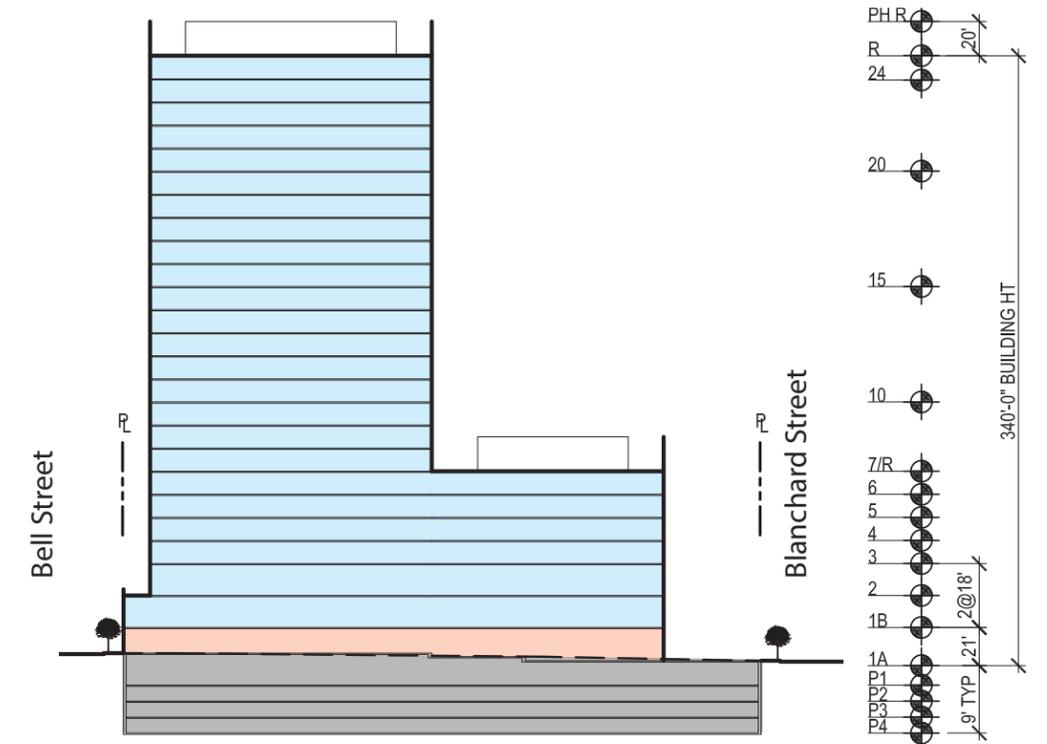
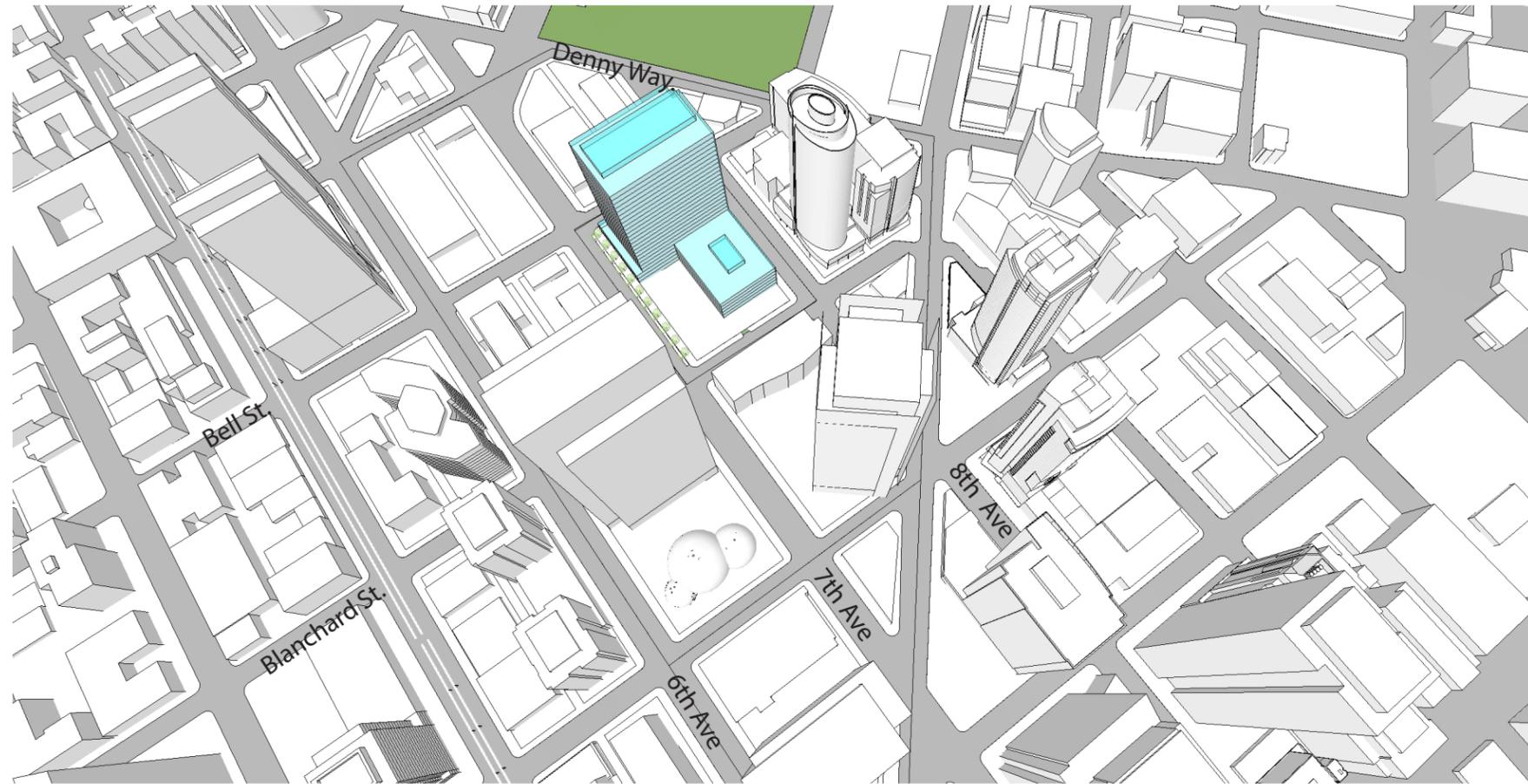
The applicant proposes to design and construct a development on the full block bisected by a public alleyway and bounded by 7th Avenue on the east, 8th Avenue on the west, Blanchard Street on the south and Bell Street on the north. The site is zoned DMC 340/125-400, with a site area of approximately 77,700 square feet plus a public alleyway of approximately 5,700 square feet. The site has a base FAR of 5 with a maximum of 10.

Description

This application is for a commercial project with approximately 835,200 gsf of office space and approximately 35,000 gsf of street level retail in two buildings. An open space and thru block connection are proposed connecting 7th and 8th Avenues. Approximately 835 parking stalls will be provided below grade. All building services will be located below grade, with primary access from 8th Avenue and a secondary parking access from Bell Street. This proposal assumes a full alley vacation.



FULL VACATION - OPTION 1



Summary:

- 4 Parking Levels
- 1-24 Story Building with 6 level podium
- Open Space at Grade
- Loading/Parking Entries on 8th Avenue and Bell Street
- 835,200 SF of FAR
- 30,000 SF Retail

+ (6) Pros

1. Access to daylight and views
2. Grade level open space
3. Enhanced Blanchard green street
4. Enhanced neighborhood urban fabric
5. Opportunity to enhance 7th Ave cycle track
6. Open space in optimal solar location

- (7) Cons

1. Shadow impact on streets
2. Curb cuts on 8th Ave and possible curb cut on Bell Street
3. No through block connection
4. Large floor plates create planning challenges
5. Massing rhythm dissimilar to adjacent blocks
6. Erosion of urban street edge at 7th Avenue
7. Singular building lacks porosity, scale variety

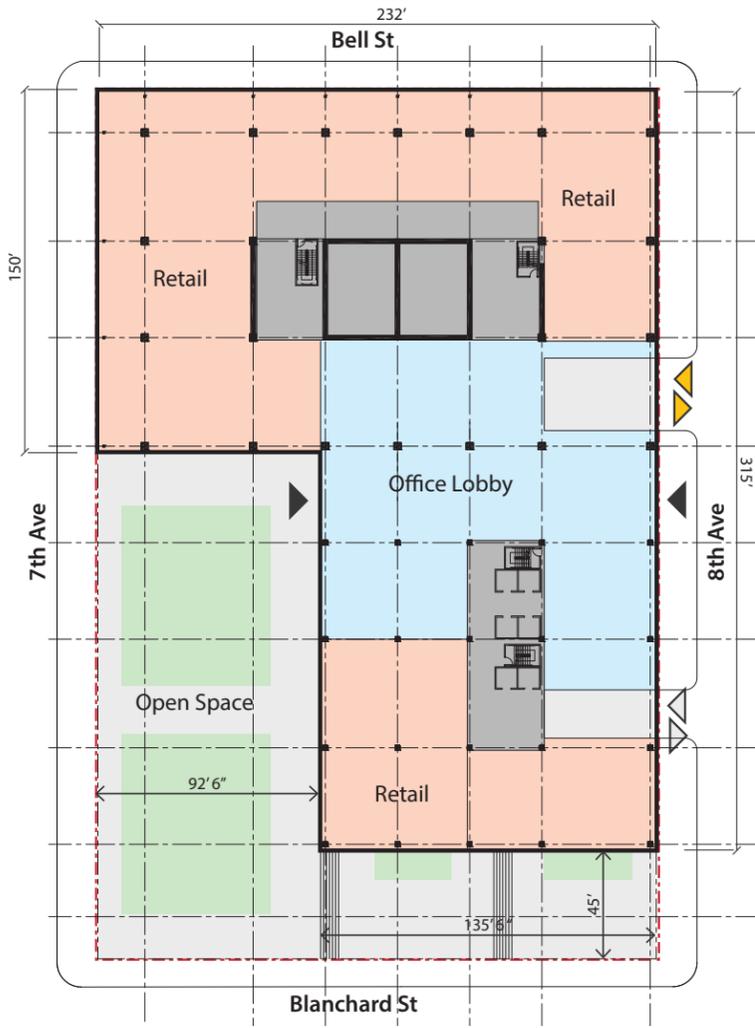
Note: This option requires a favorable Type 1 Director's Decision to permit vehicular curb cuts at 8th Avenue rather than utilizing the alley for access as required by SMC 23.49.019-H-1-a.

Aerial

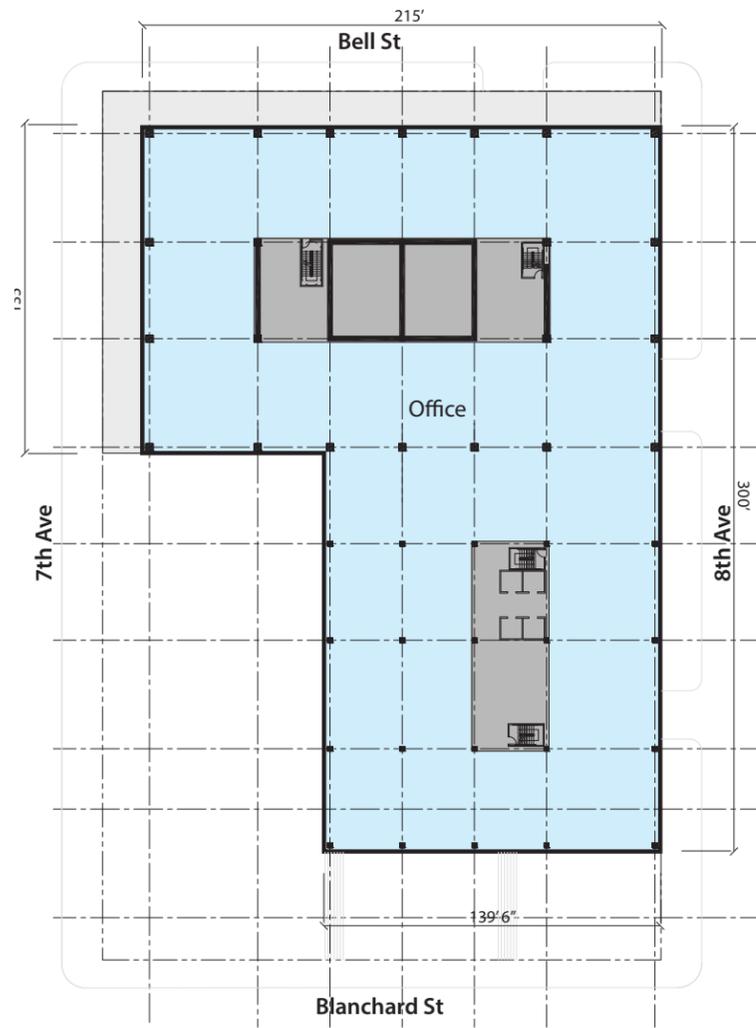
Building Section



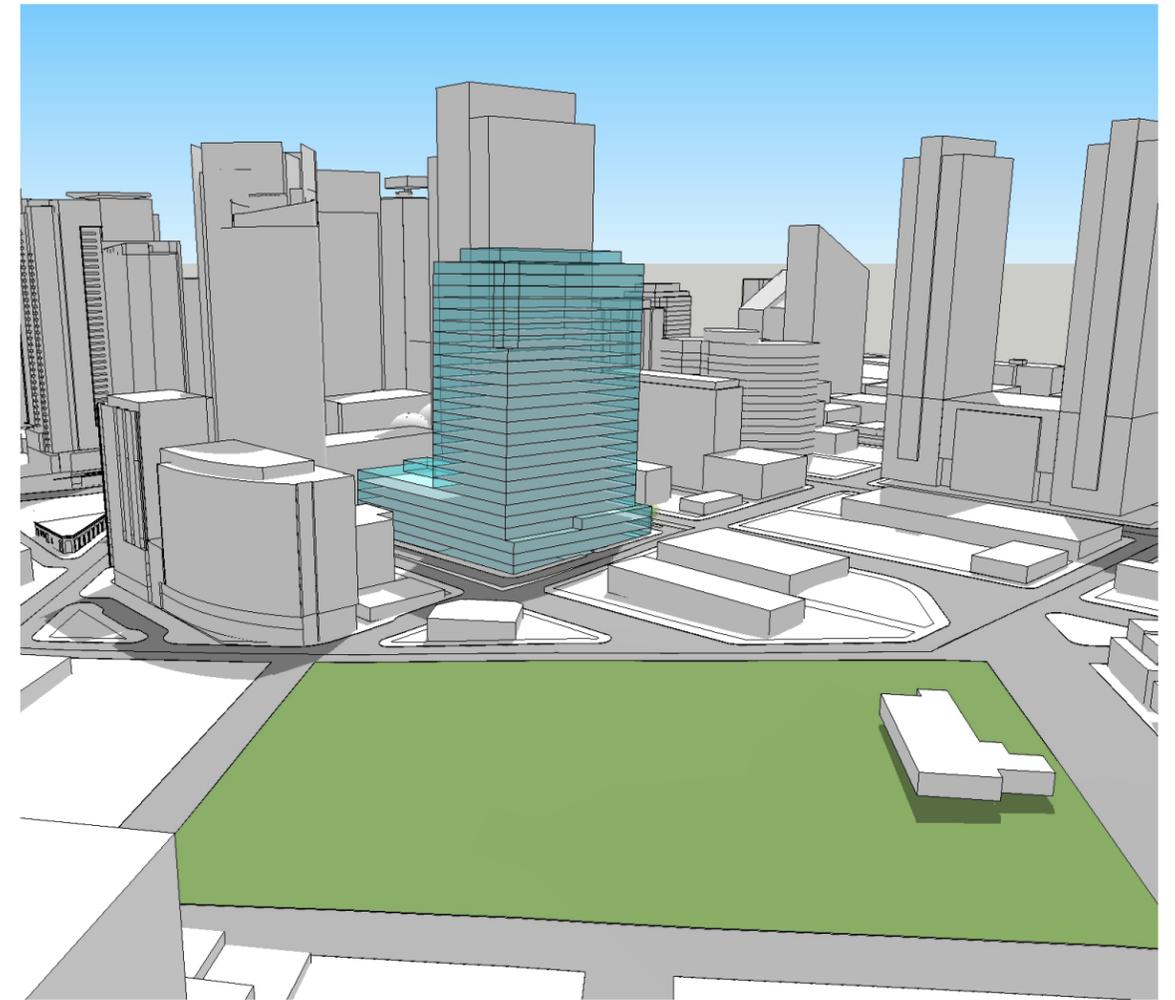
Vicinity Plan



Level 1 Plan



Typical Upper Plan



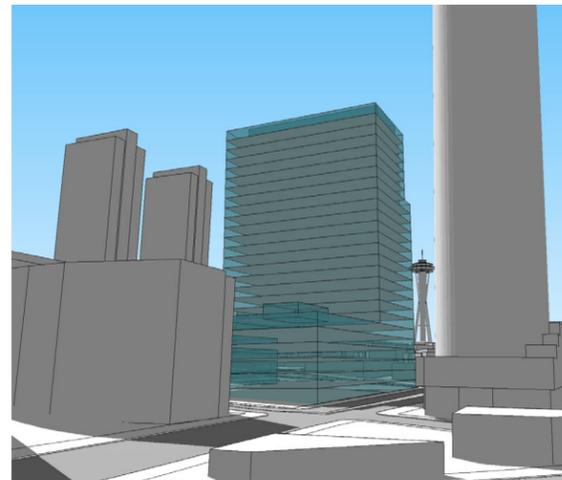
NE Perspective



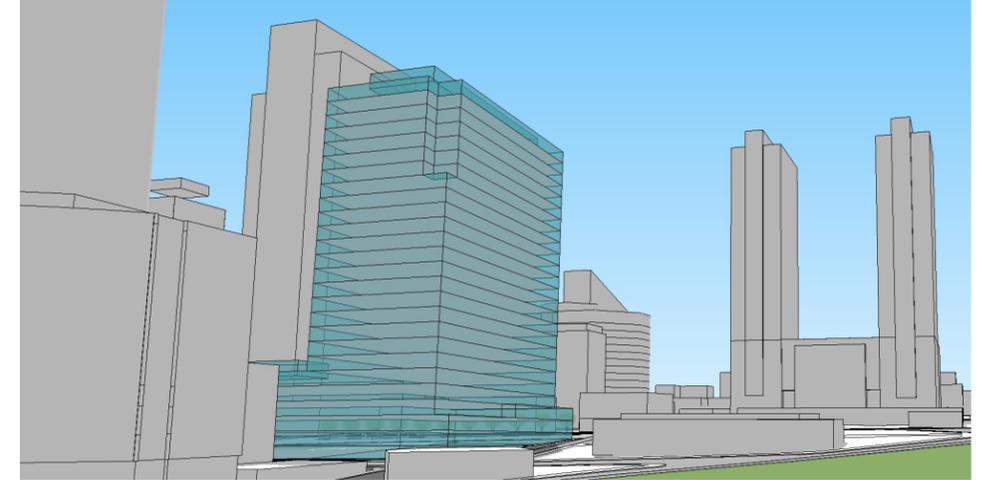
SW Perspective



7th Ave View

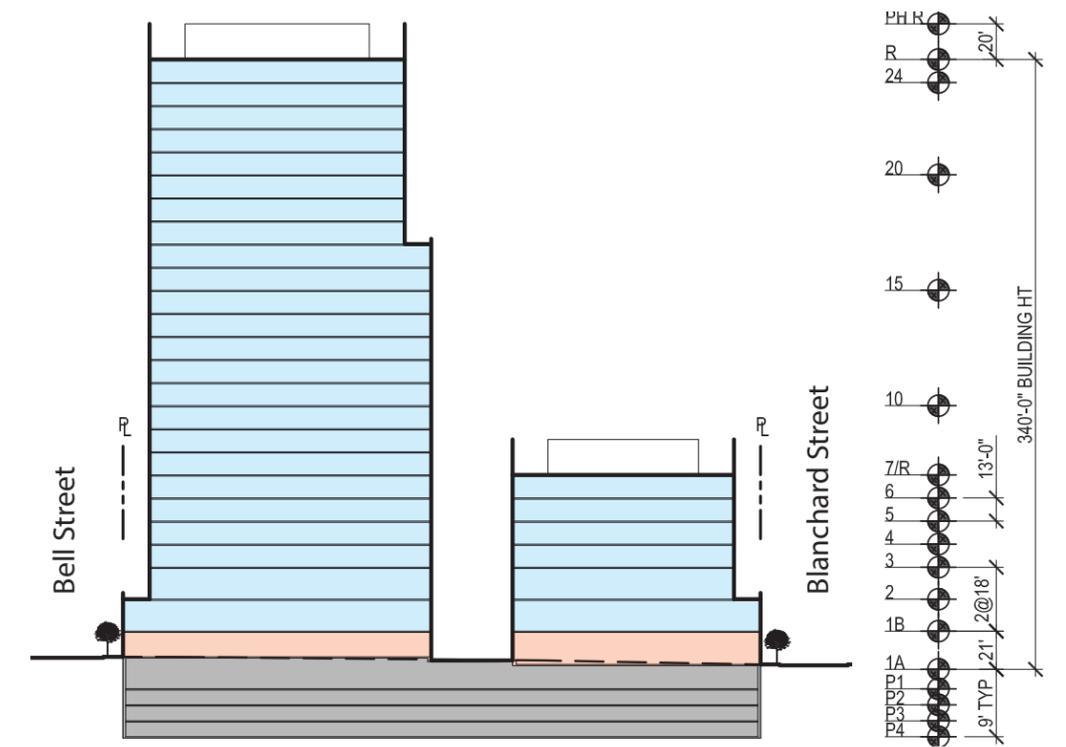
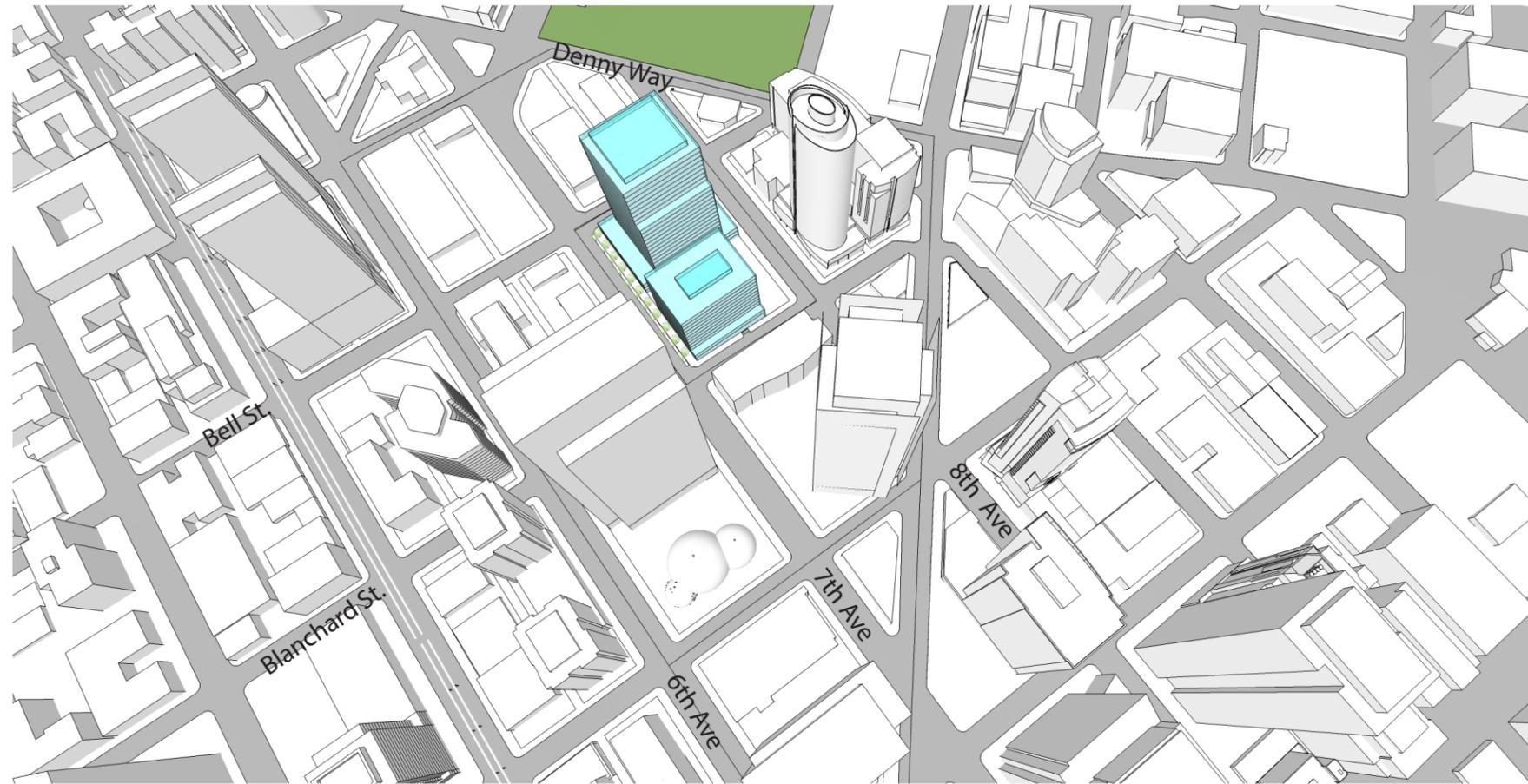


8th Ave View



View From Denny Park

FULL VACATION - OPTION 2



Summary:

- 4 Parking Levels
- 1- 7 Story Building, 1-24 Story Building
- Loading/Parking Entries on Bell street and 8th Ave
- 835,200 SF of FAR
- 30,000 SF Retail

+ (5) Pros

1. Efficient high-rise tower footprint
2. Podium setbacks on three sides of tower
3. Opportunity to enhance 7th Ave cycle track
4. Massing rhythm similar to adjacent blocks
5. Through block connection

- (8) Cons

1. Narrow open space between structures
2. Open space is distributed rather than consolidated
3. Static building massing
4. Tower does not "land" on any street facade
5. Does not respond to 7th Avenue boulevard
6. Curb cuts on 8th Ave and possible curb cut on Bell Street
7. Open space not in optimal solar location
8. Requires facade width departure

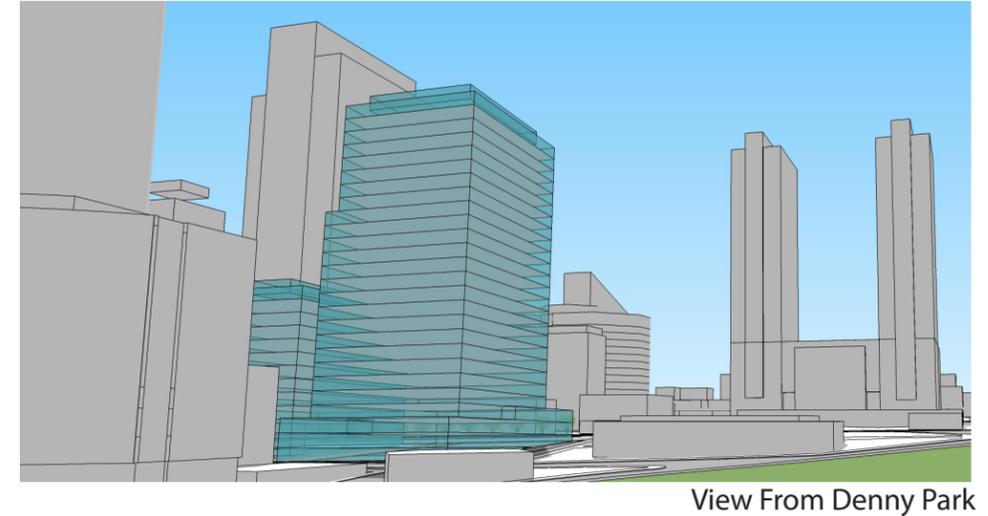
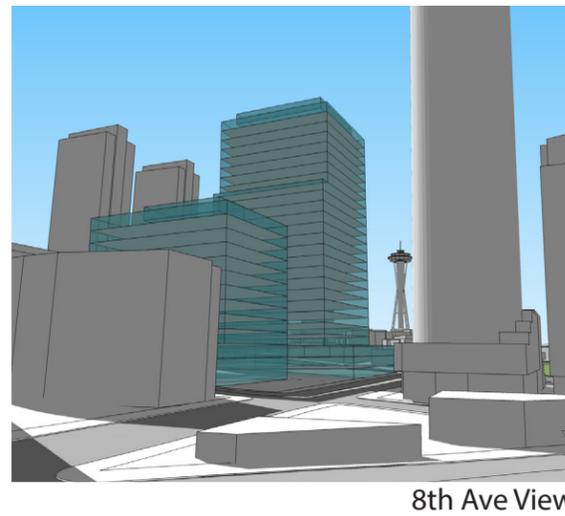
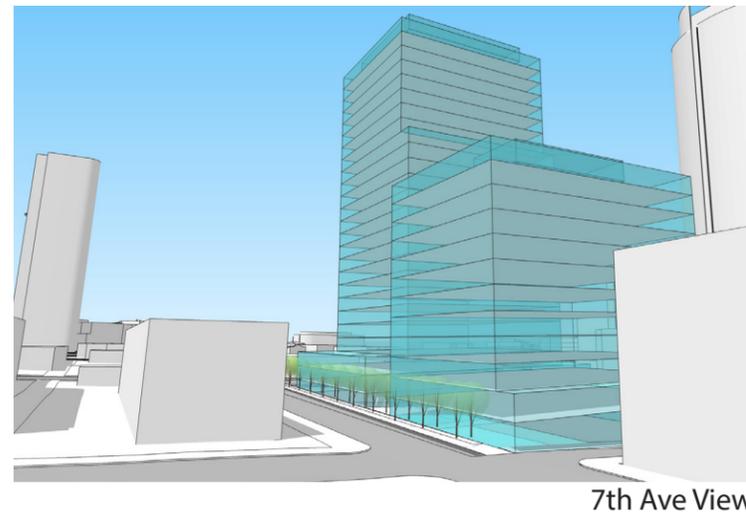
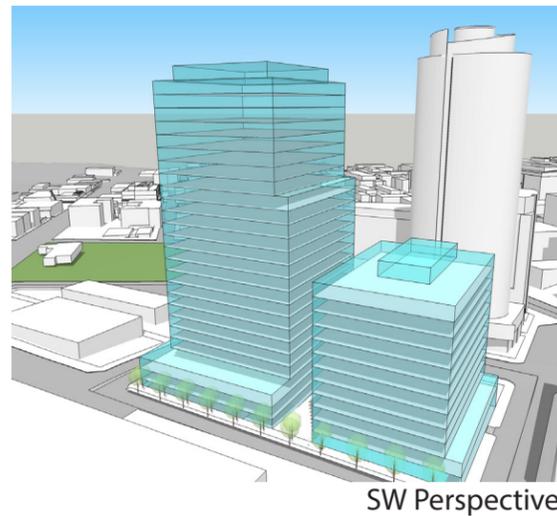
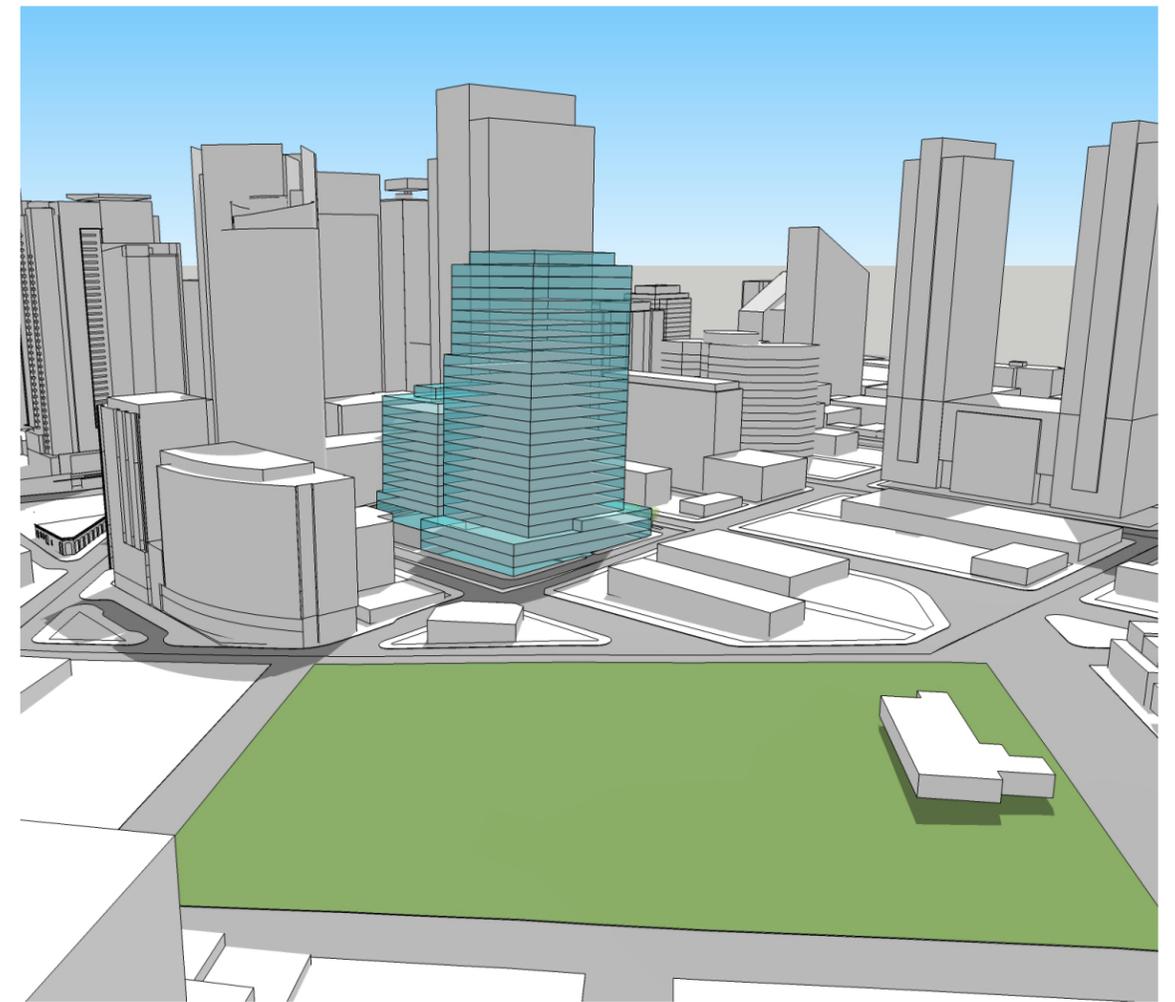
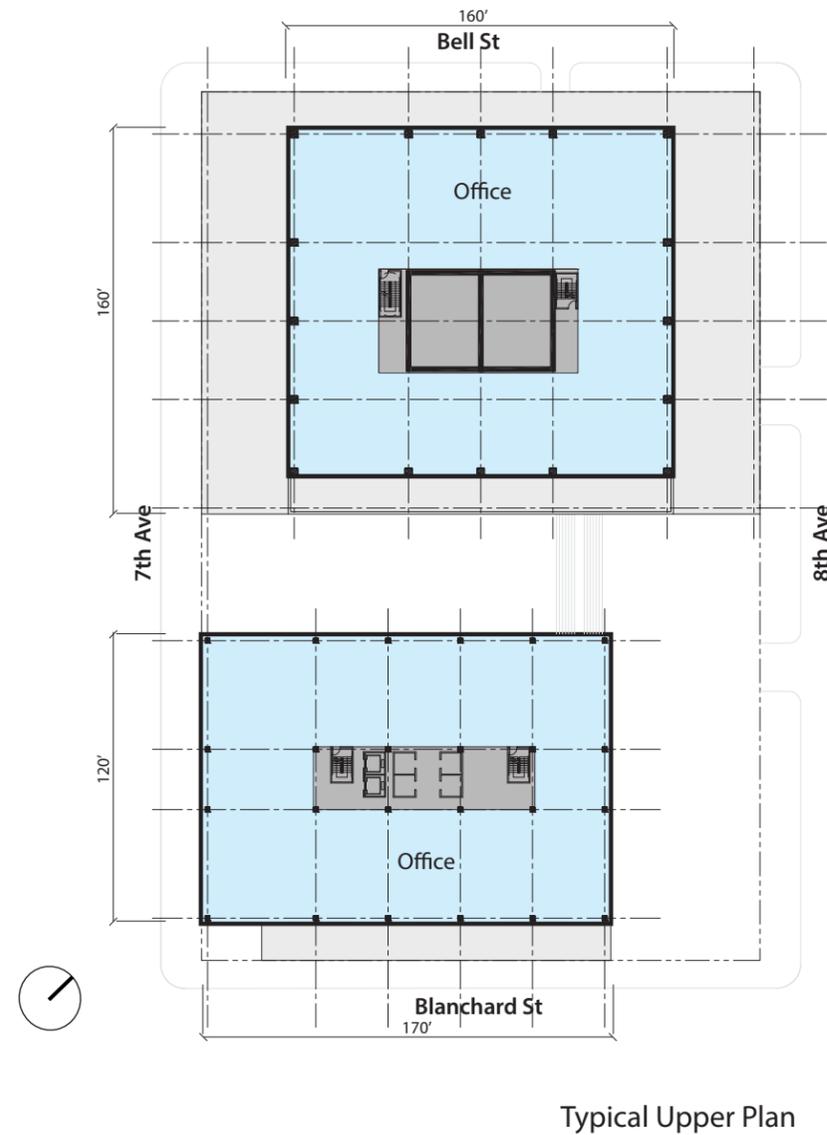
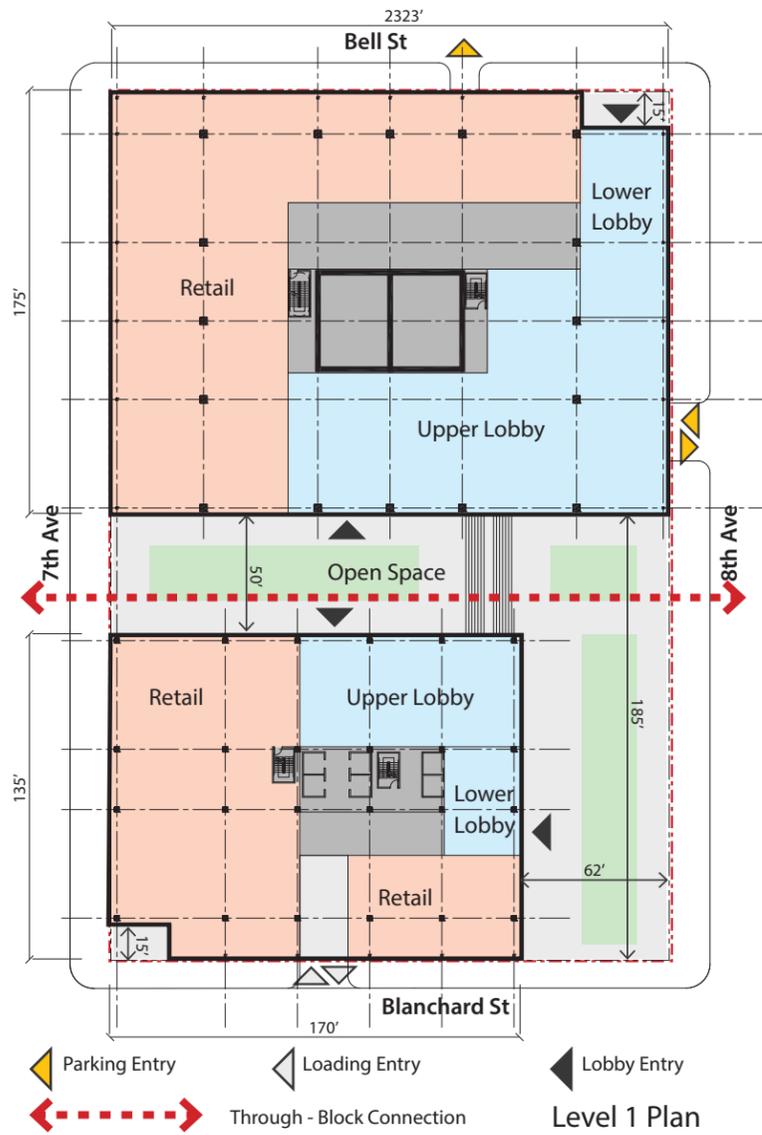
Aerial

Building Section

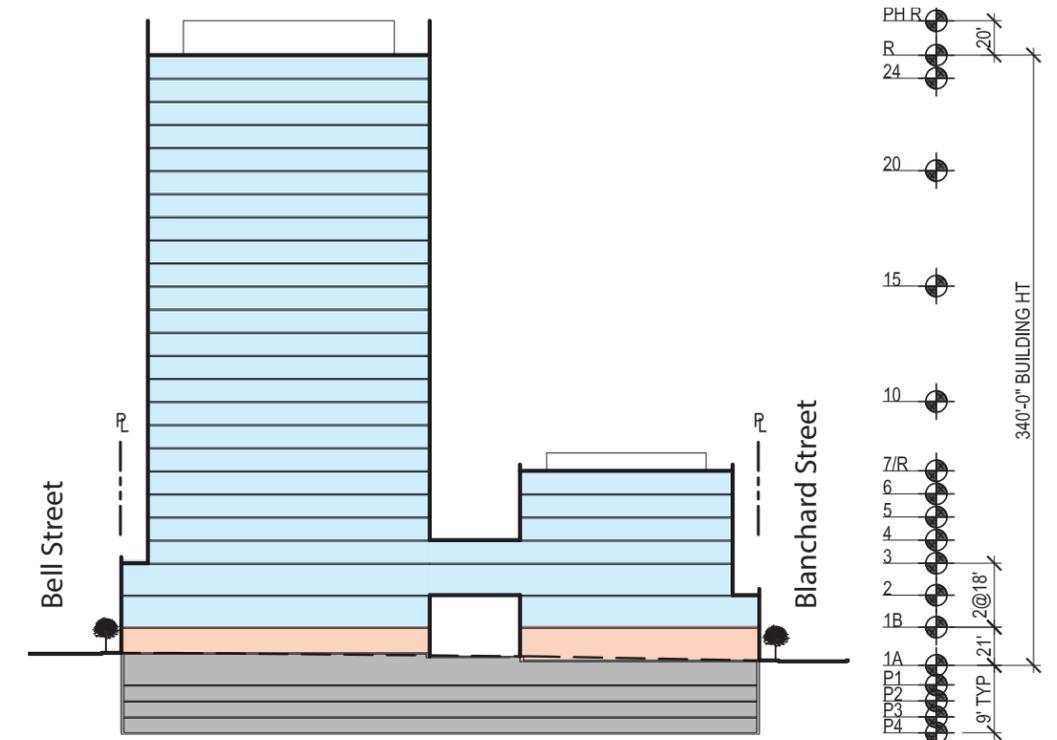
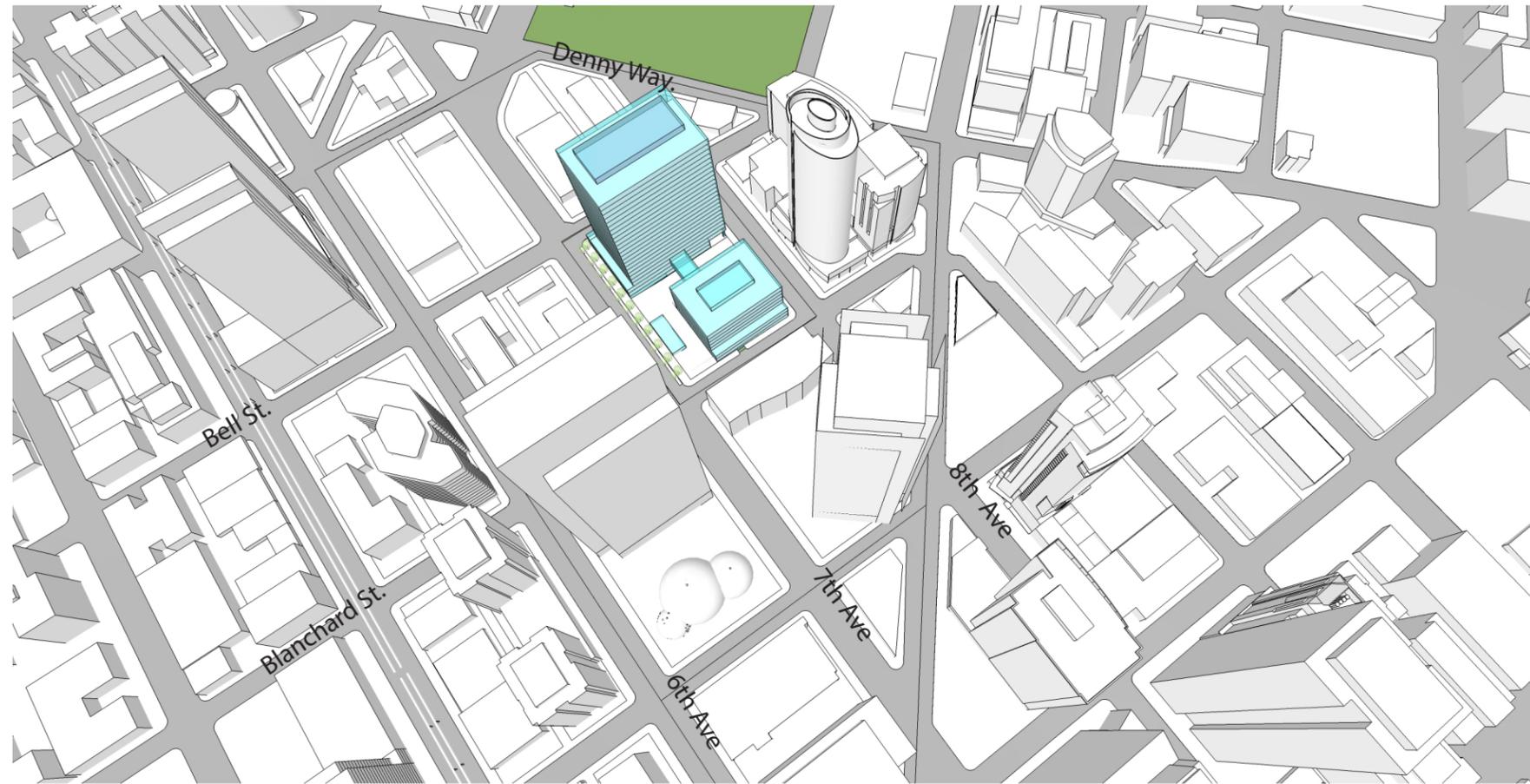
Note: This option requires a favorable Type 1 Director's Decision to permit vehicular curb cuts at 8th Avenue and Bell Street rather than utilizing the alley for access as required by SMC 23.49.019-H-1-a.



Vicinity Plan



FULL VACATION - OPTION 3 (PREFERRED)



Summary:

- 4 Parking Levels
- 1- 7 Story Building, 1-24 Story Building, 1 - 1 Story Building
- Open Space at Grade
- Loading/Parking Entries on Bell Street and 8th Ave
- 835,200 SF of FAR
- 30,000 SF Retail

+ (9) Pros

1. Access to daylight and views
2. Grade level open space
3. Enhanced Blanchard green street
4. Enhanced neighborhood urban fabric
5. Massing rhythm similar to adjacent blocks
6. Through block connection
7. Opportunity to enhance 7th Ave cycle track
8. Open space in optimal solar location
9. Aerial connection enhances long term flexibility

- (3) Cons

1. Shadow impact on streets
2. View impact on adjacent buildings
3. Curb cuts on 8th Ave and possible curb cut on Bell Street

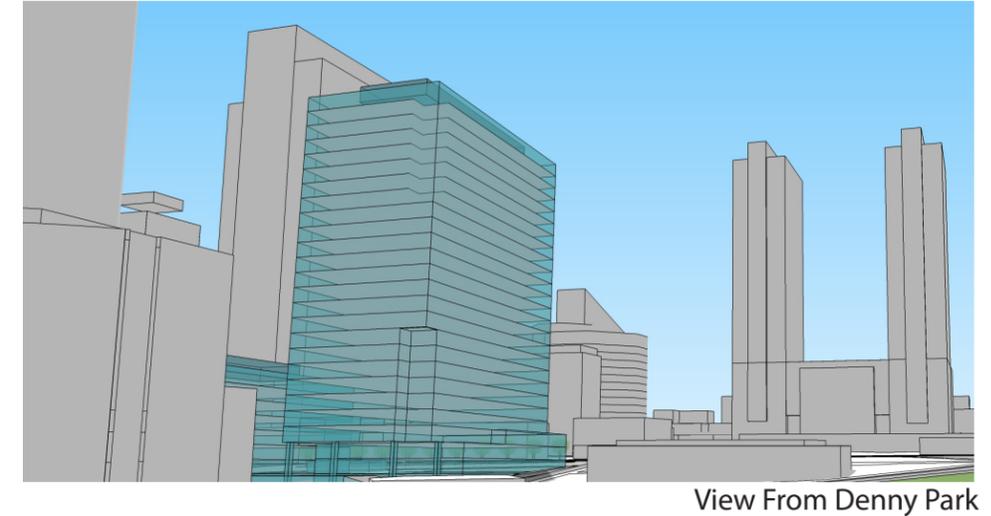
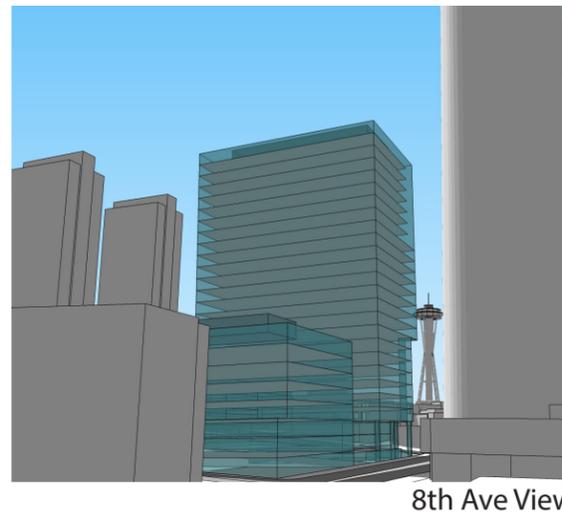
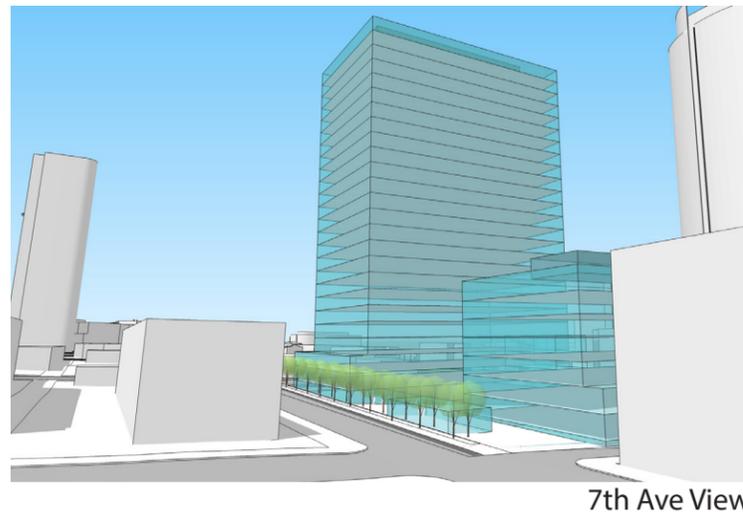
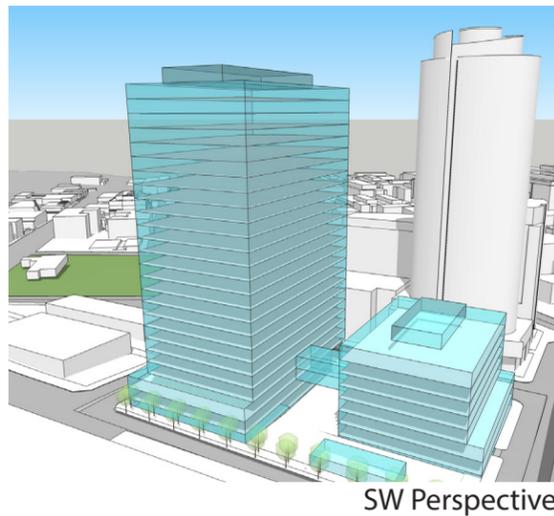
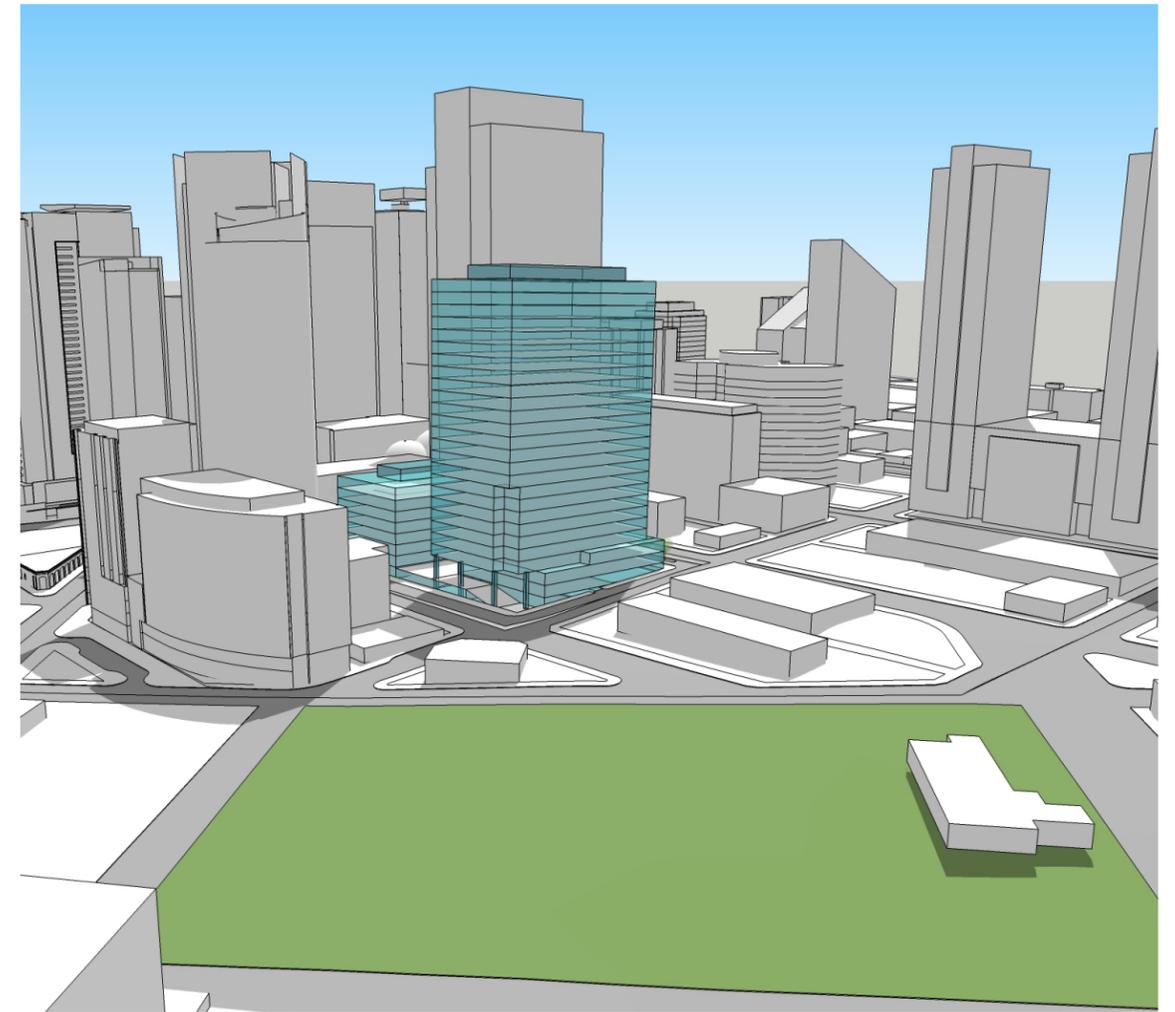
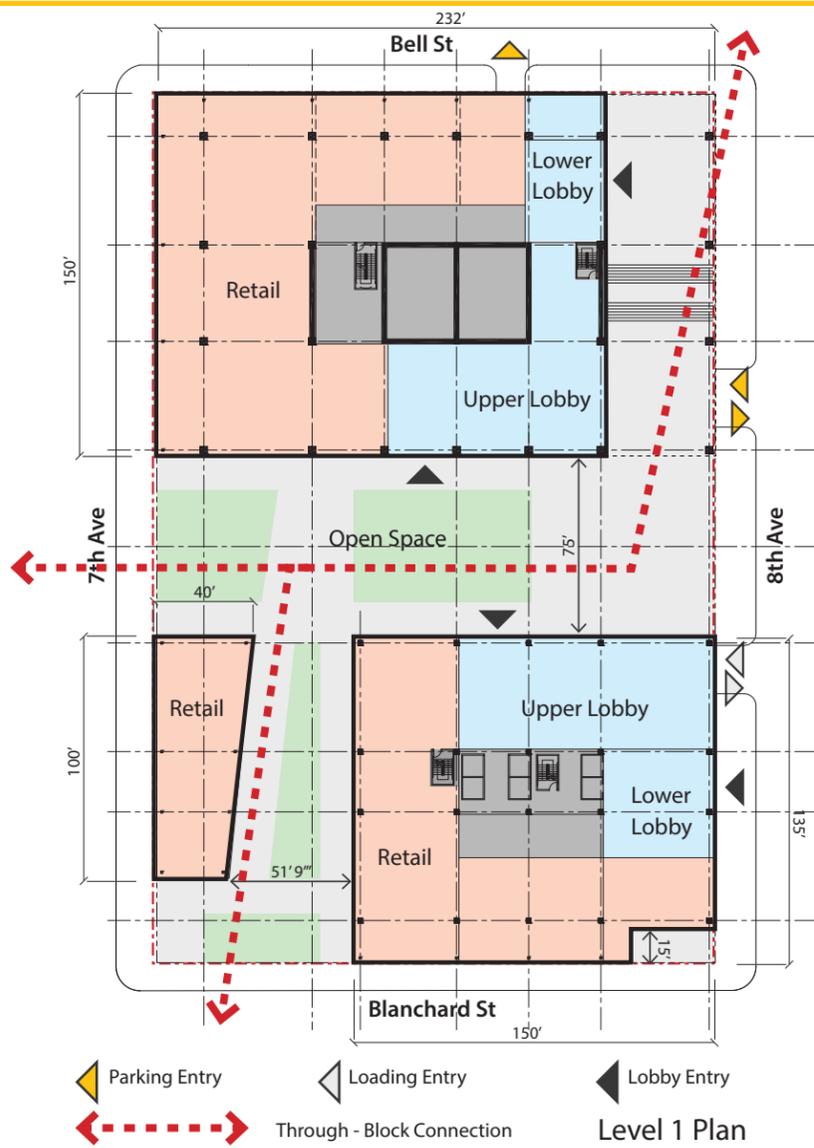
Aerial

Building Section

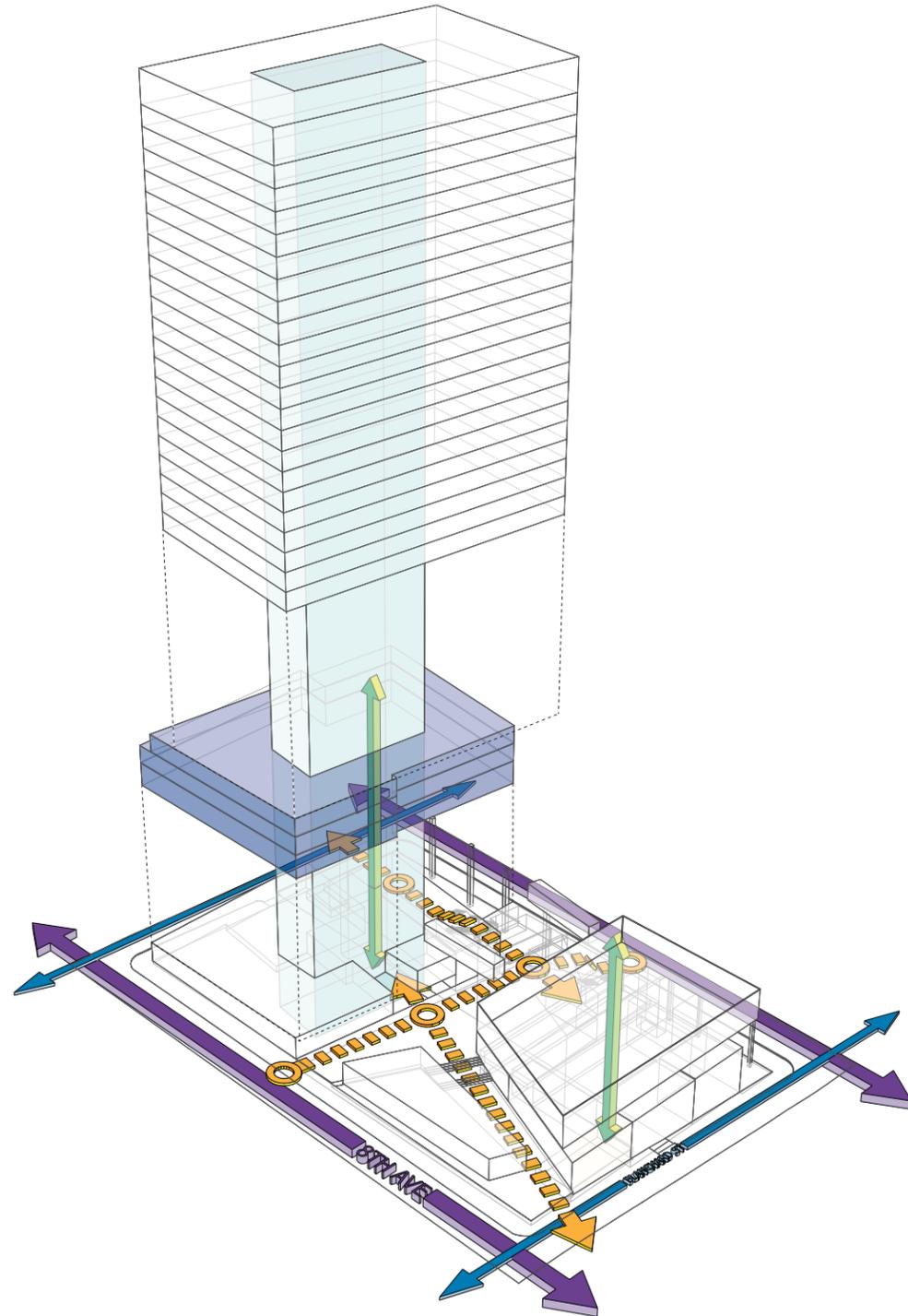


Vicinity Plan

Note: This option requires a favorable Type 1 Director's Decision to permit vehicular curb cuts at 8th Avenue and Bell Street rather than utilizing the alley for access as required by SMC 23.49.019-H-1-a.



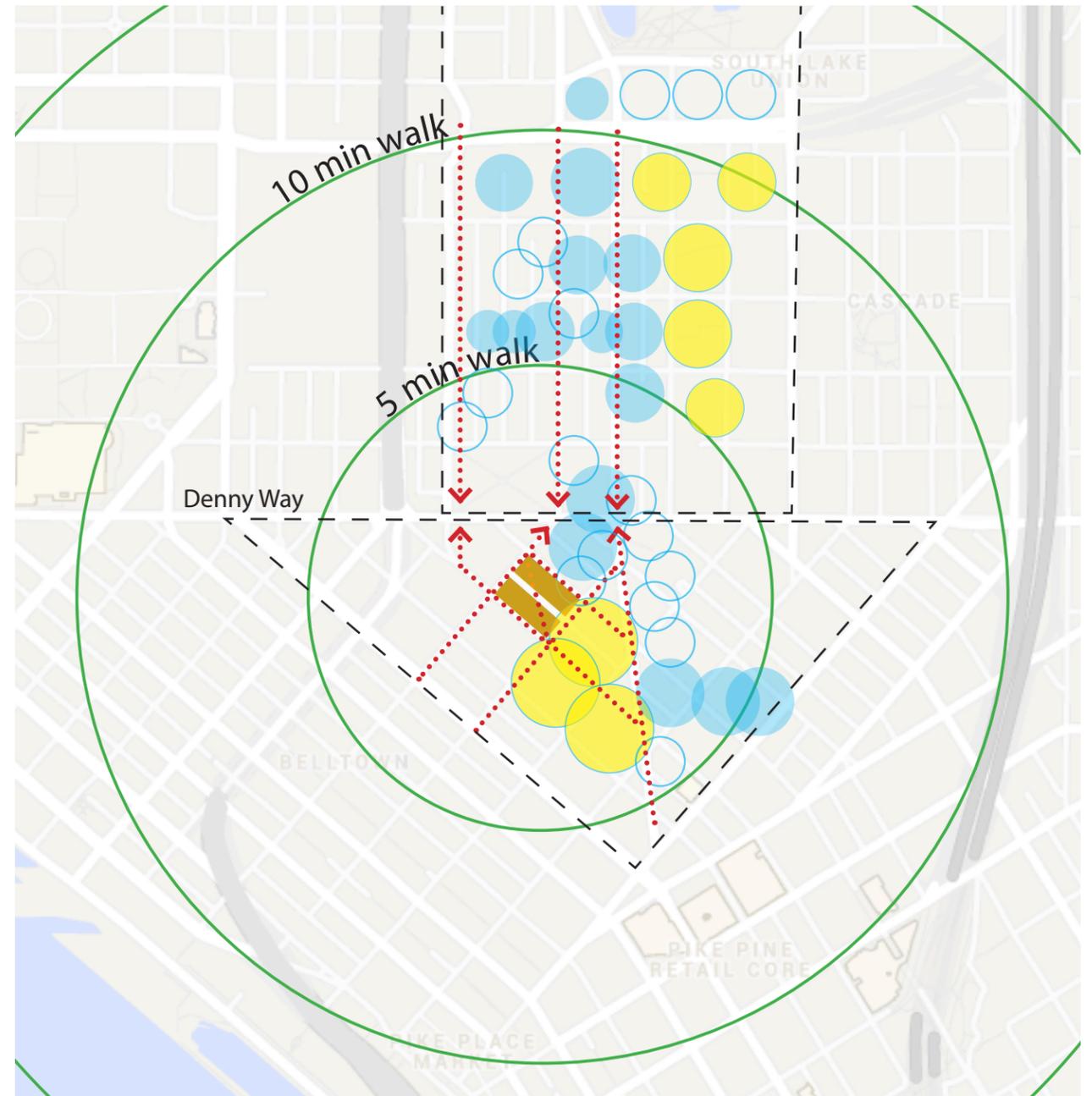
CIRCULATION DIAGRAM (PREFERRED SCHEME)



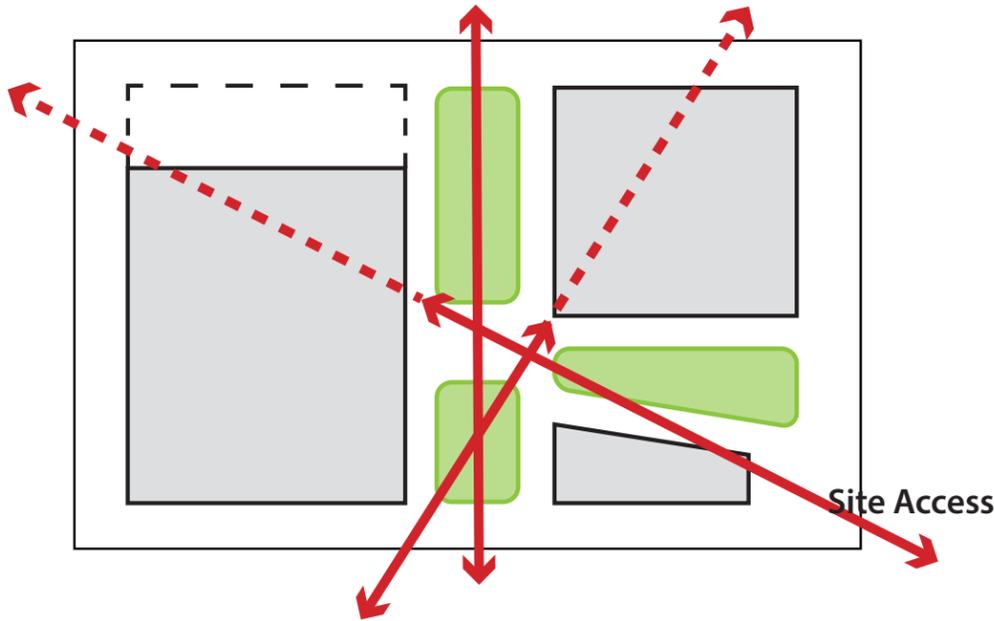
Linkages

Site organization and resulting building massing in the preferred scheme knit into the existing and anticipated pedestrian circulation patterns that connect the commercial and residential population densities of South Lake Union and the Denny Triangle, enhancing north-south ties between these two emerging neighborhoods.

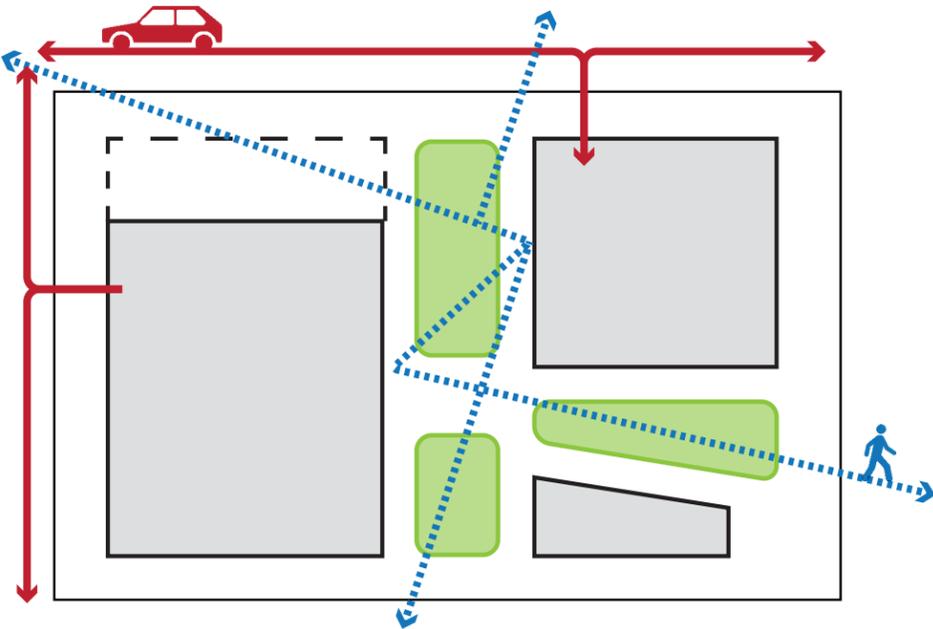
DENNY TRIANGLE / SOUTH LAKE UNION CONNECTIONS



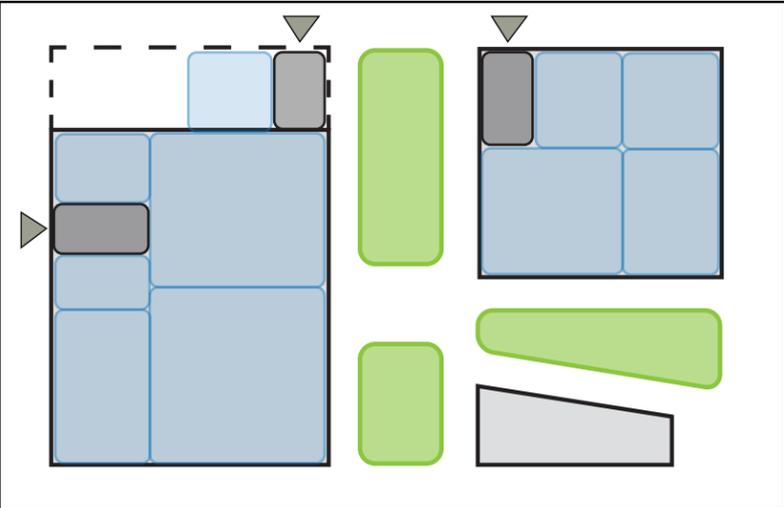
- Owner-occupied office 200-900K SF
- Other office/lab 200-500K SF
- Residential >100 Units



PEDESTRIAN TRAJECTORIES



CIRCULATION PATTERNS



VEHICULAR ACCESS PORTALS

Site Access

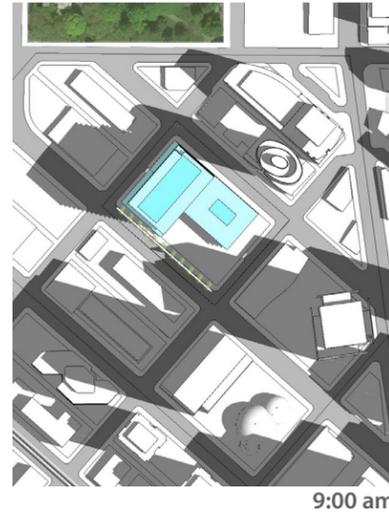
Vehicular site access is proposed to be predominantly from the east on 8th Avenue with a secondary egress-only portal to the north on Bell Street. This perimeter distribution is intended to encourage pedestrian flow through and around the site, particularly at the Green Streets and enhanced 7th Avenue. It has the additional benefit of locating building services below grade where they are fully screened from pedestrians.

Pedestrian Circulation

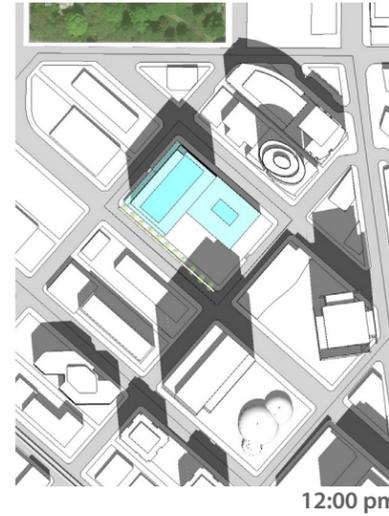
Established neighborhood pedestrian circulation patterns are acknowledged and enhanced in the preferred scheme through a responsive site plan arrangement. As the Denny Triangle and South Lake Union neighborhoods densify, the predominant pedestrian flow will be north-south, creating opportunities to move around and through the Block 21 site. Overlapping open spaces and nodal points draw one around and through the block, encouraging movement and engagement with active uses at the ground level.

SHADOW STUDIES (OPTION 1)

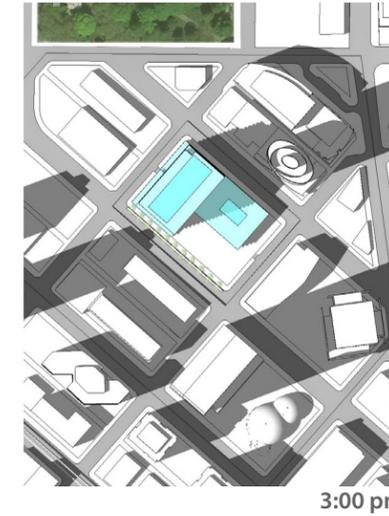
Summer- June 21st



9:00 am

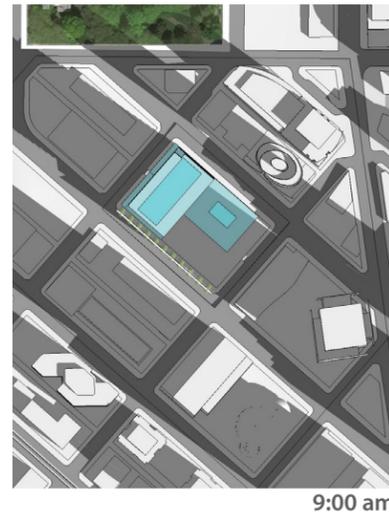


12:00 pm

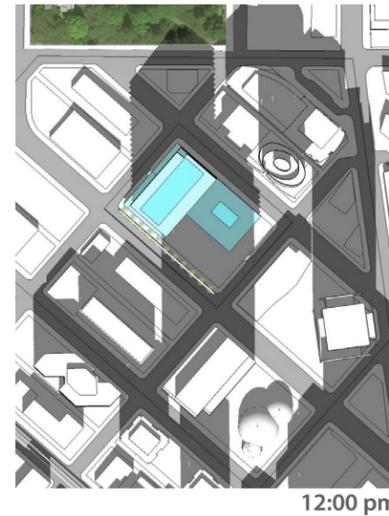


3:00 pm

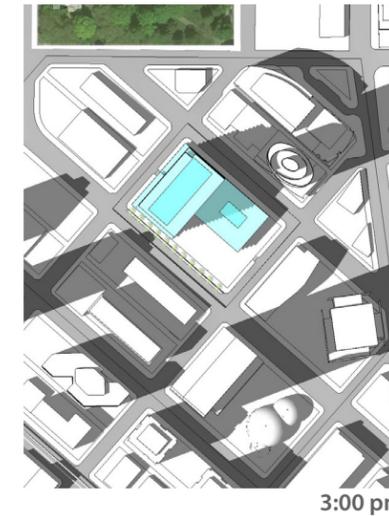
Spring & Fall- March & Sept. 20th



9:00 am



12:00 pm

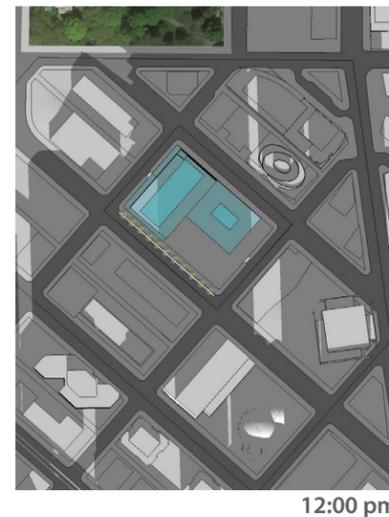


3:00 pm

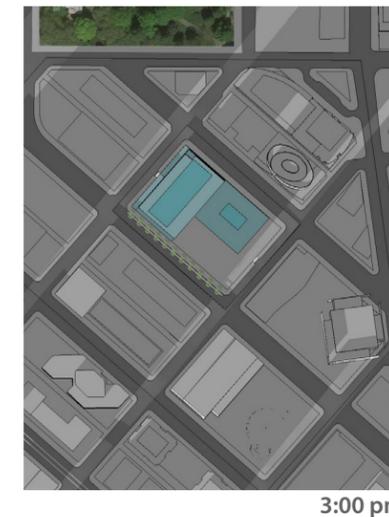
Winter- December 21st



9:00 am



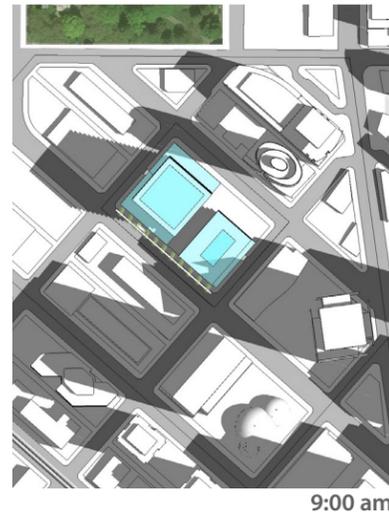
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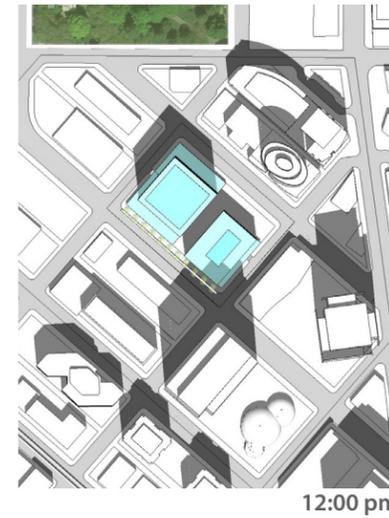
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SHADOW STUDIES (OPTION 2)

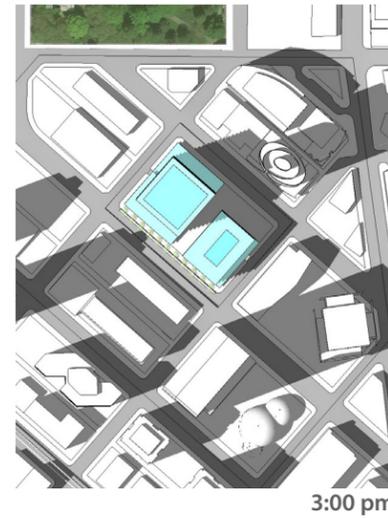
Summer- June 21st



9:00 am

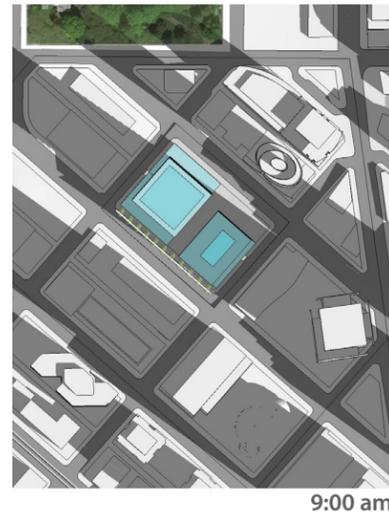


12:00 pm

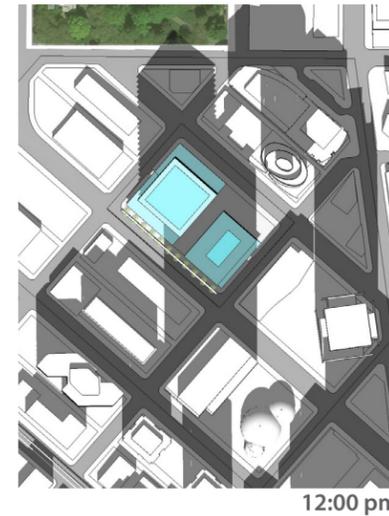


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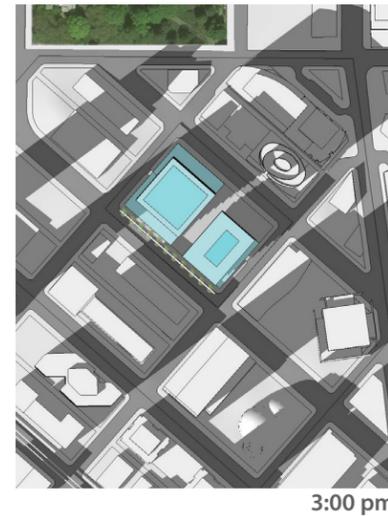
Spring & Fall- March & Sept. 20th



9:00 am

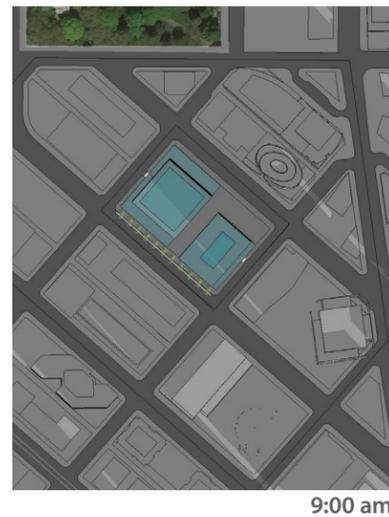


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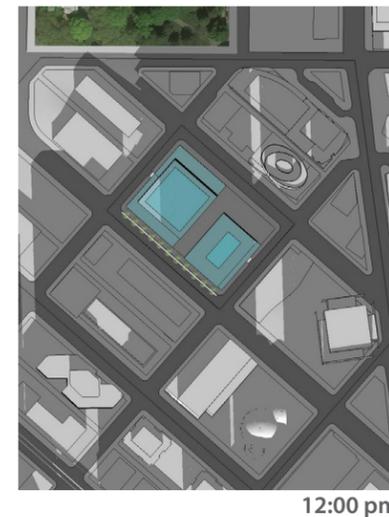


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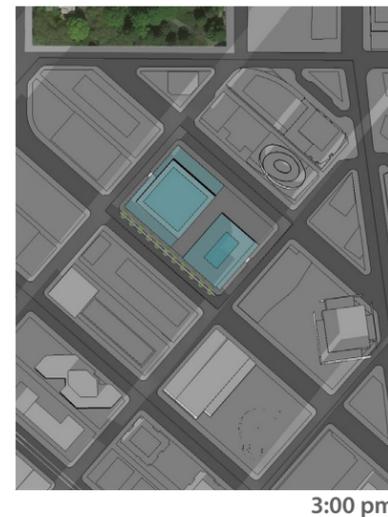
Winter- December 21st



9:00 am



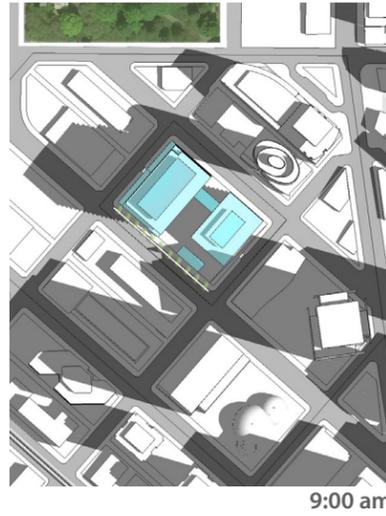
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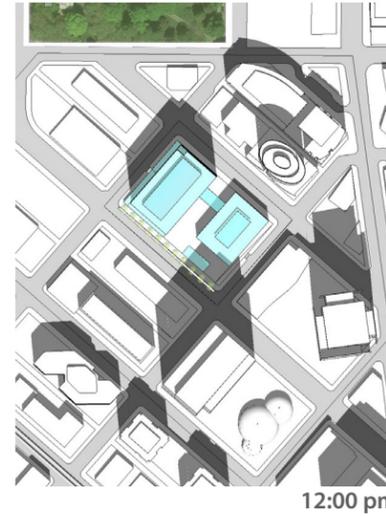
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SHADOW STUDIES (OPTION 3 PREFERRED)

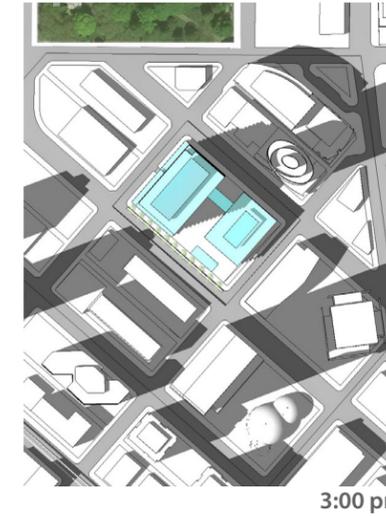
Summer- June 21st



9:00 am

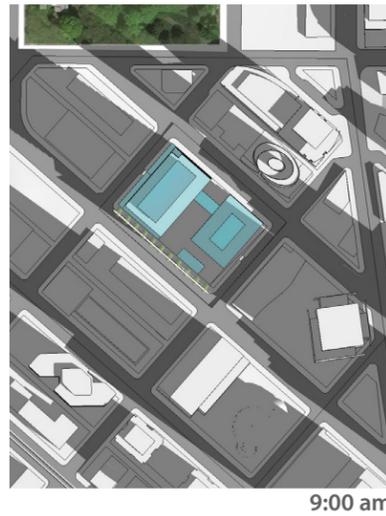


12:00 pm

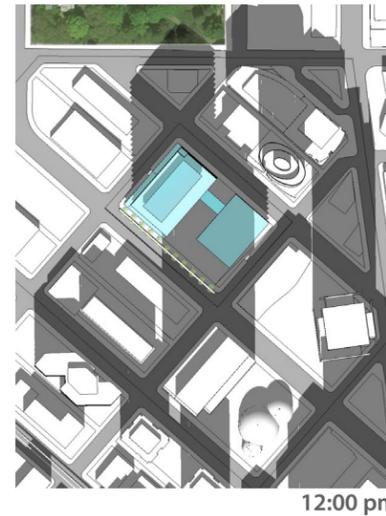


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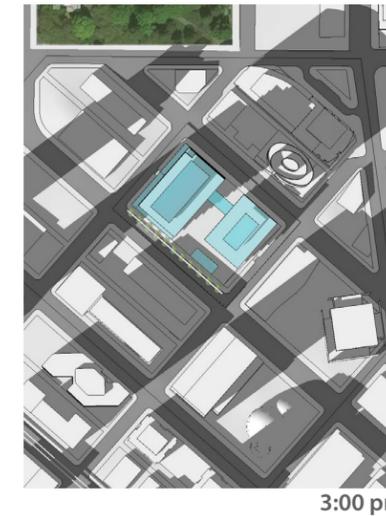
Spring & Fall- March & Sept. 20th



9:00 am

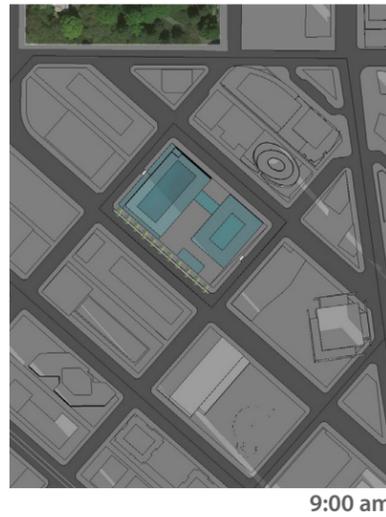


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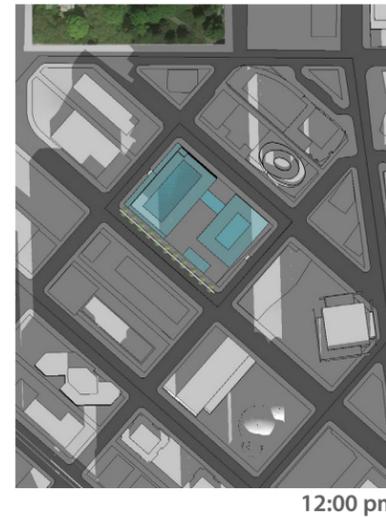


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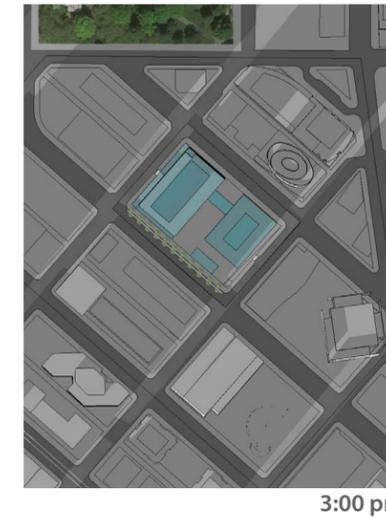
Winter- December 21st



9:00 am



12:00 pm

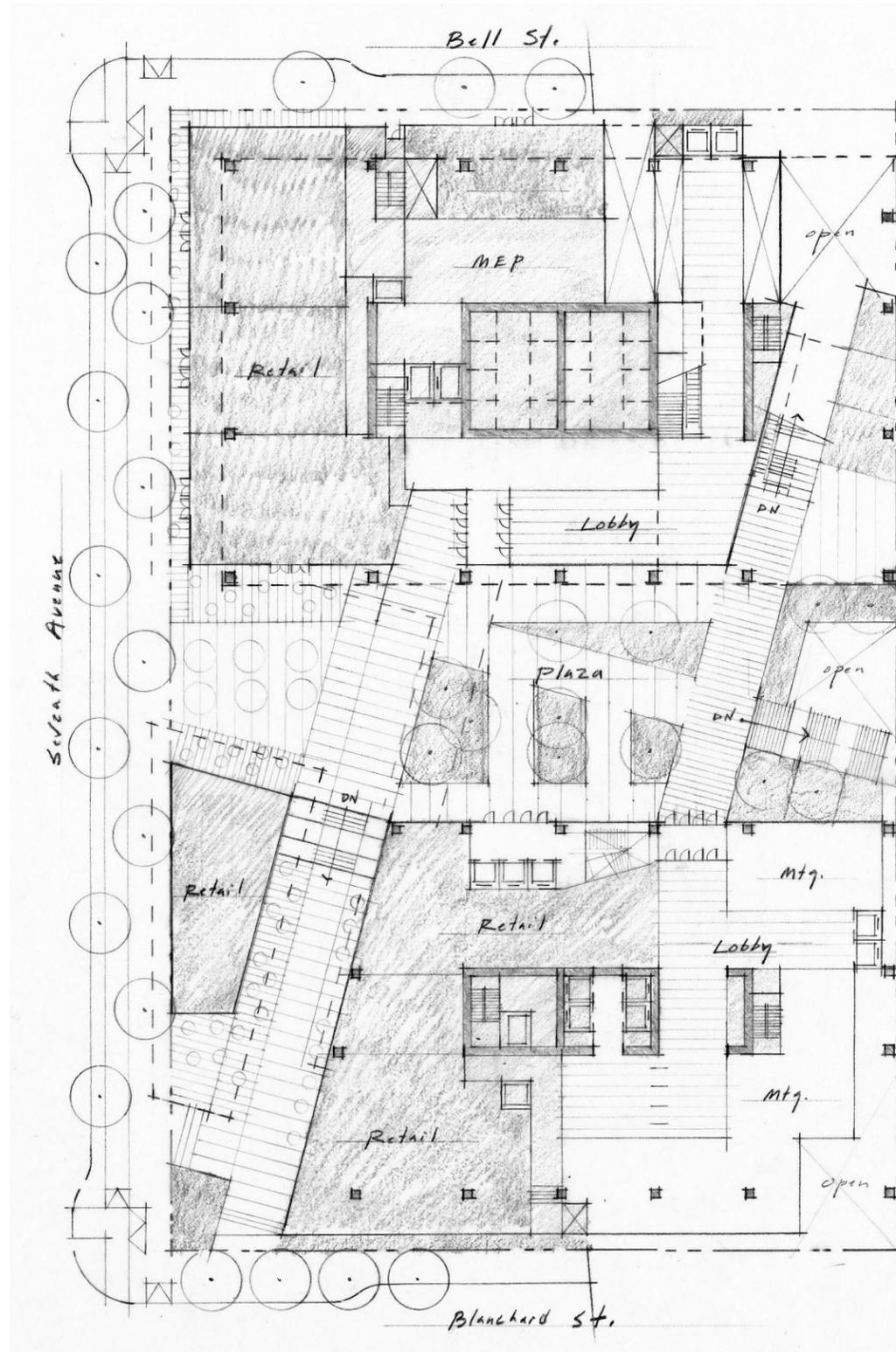


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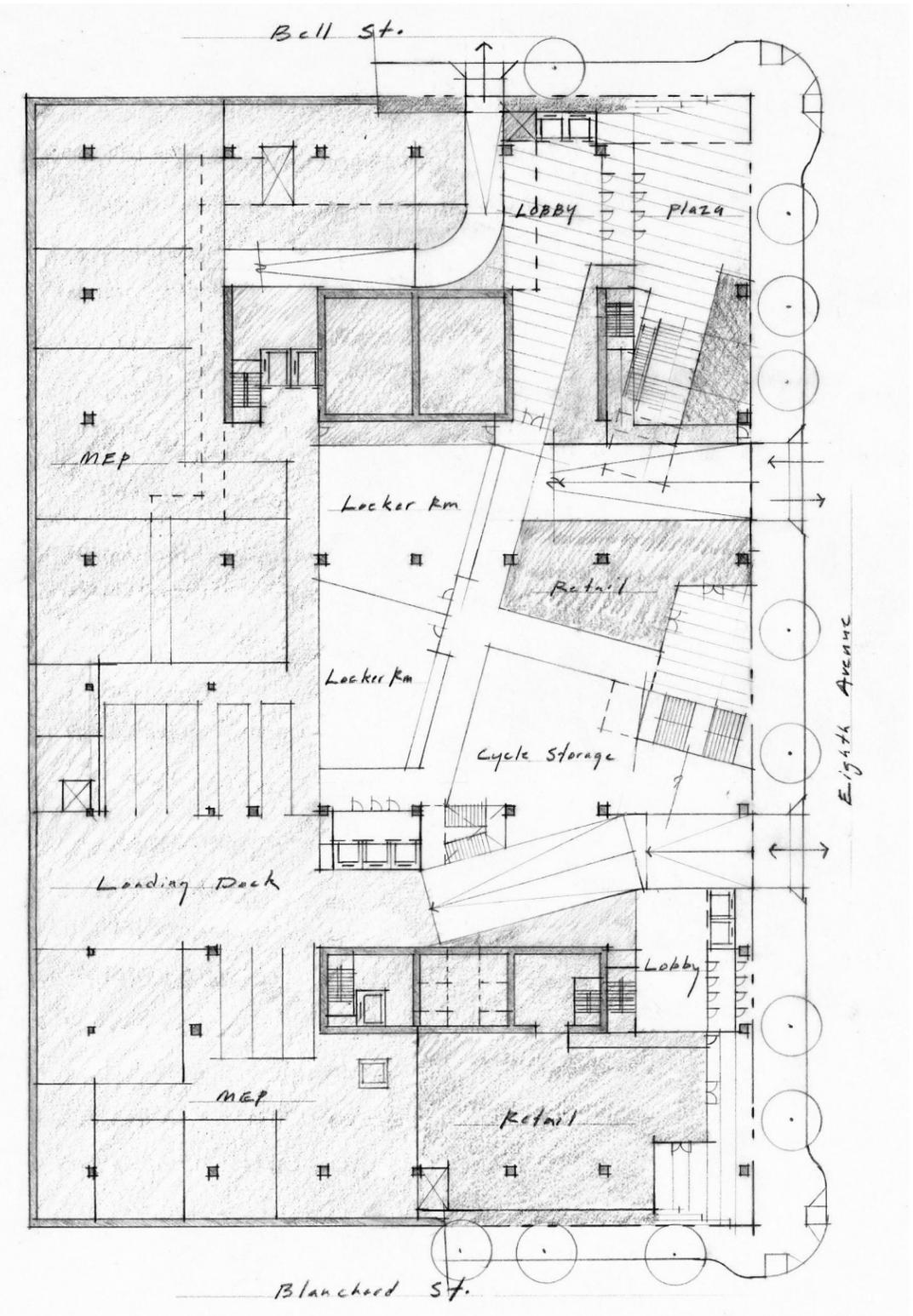
SITE + MASSING CONCEPTS



SITE + MASSING CONCEPTS



Seventh Avenue Site Plan



Eighth Avenue Site Plan





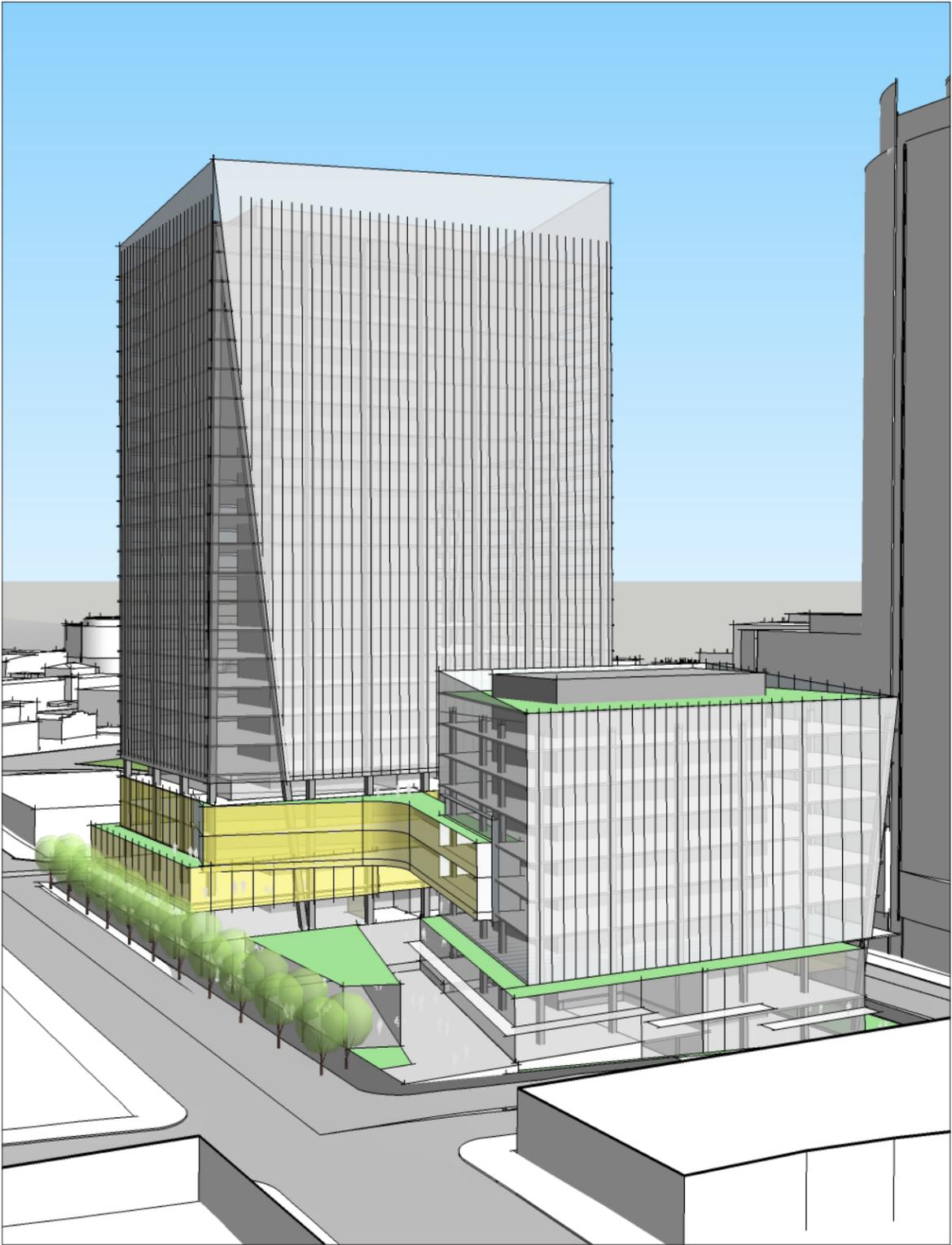
Looking SW



Looking NW



Through-block



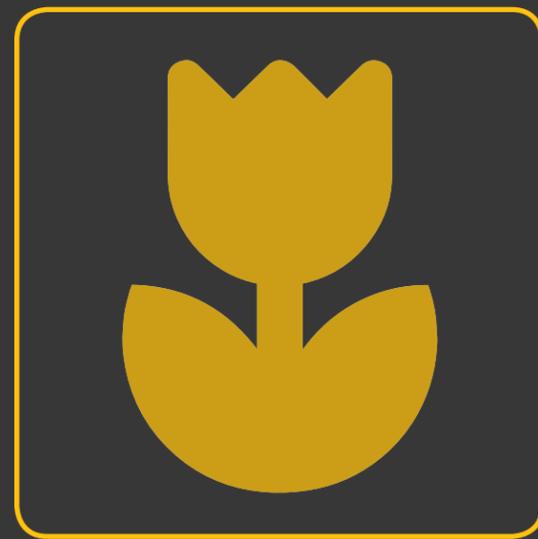
Looking north



Rather than a straight line, the through-block connection takes a meandering path as pedestrians pass through diverse, linked spaces.



LANDSCAPE CONCEPT



Projects in Development/Construction

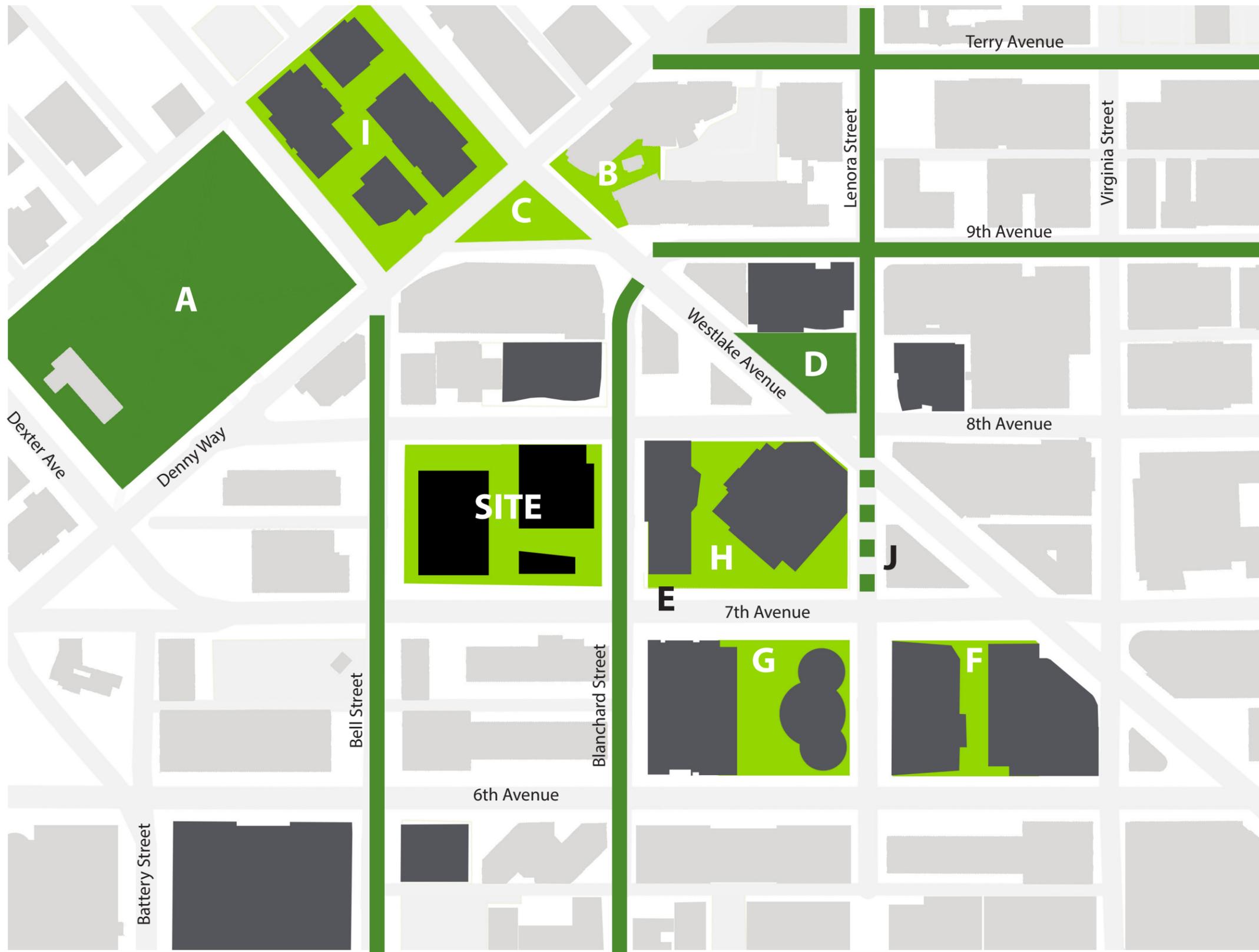


NEIGHBORHOOD CONTEXT

Connections



- Streetcar
- Streetcar Stop
- Green Street
- Cycle Track
- Shared-use Street



NEIGHBORHOOD CONTEXT

Open Space



A - Denny Park



B - 2200 Westlake



C - 2201 Westlake Corner



D - Westlake + Lenora Park



E - 7th Avenue Cycle Track



F - Block 14



G - Block 19



H - Block 20



I - Block 48



J - Lenora Shared-Use Street

Landscape Concept Plan



Belltown | Bell Street
Green Street, Retail
SLU | Denny Park

7th Avenue
Tree Double Row,
Cycle Track, Retail



Downtown | Belltown

Blanchard Street
Green Street, Retail

Westlake

Overlook
Pause, Views

8th Avenue
Canopy Trees,
Office, Vehicular Access



BLOCK 21

Street Section Studies

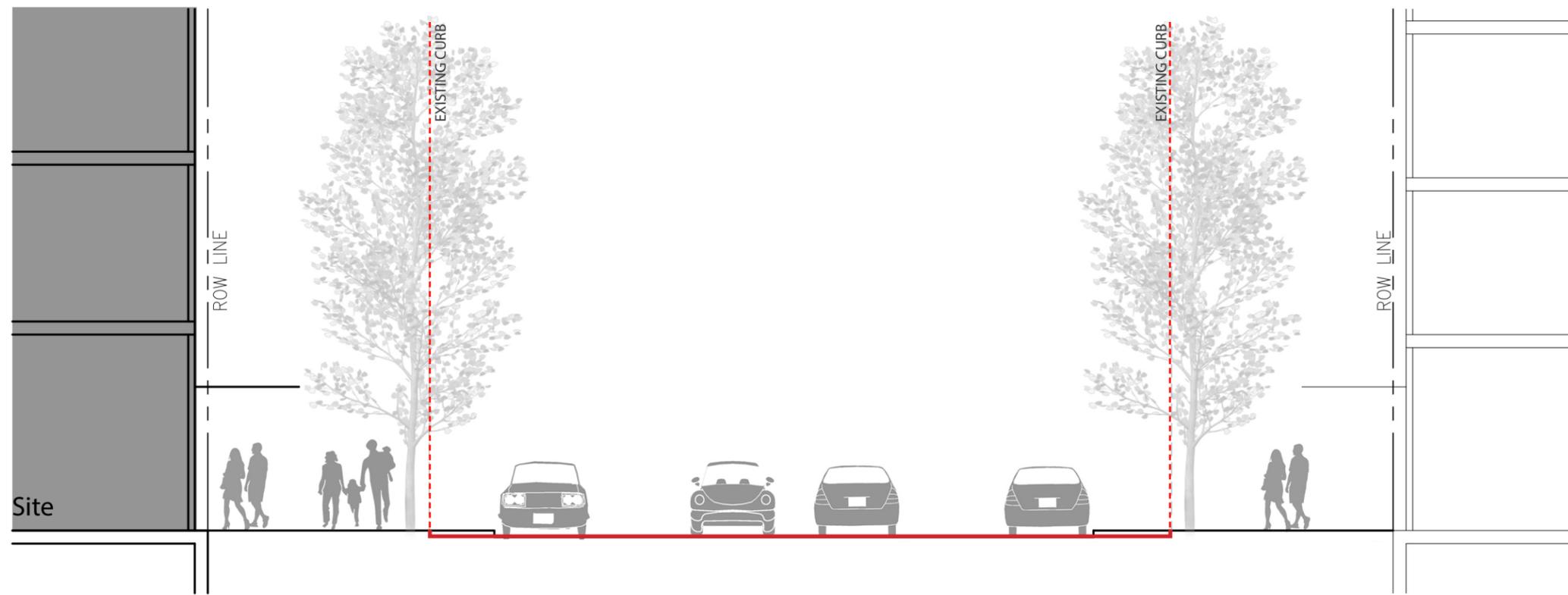
7th Avenue

90' Right-of-Way
Class II Pedestrian Street



8th Avenue

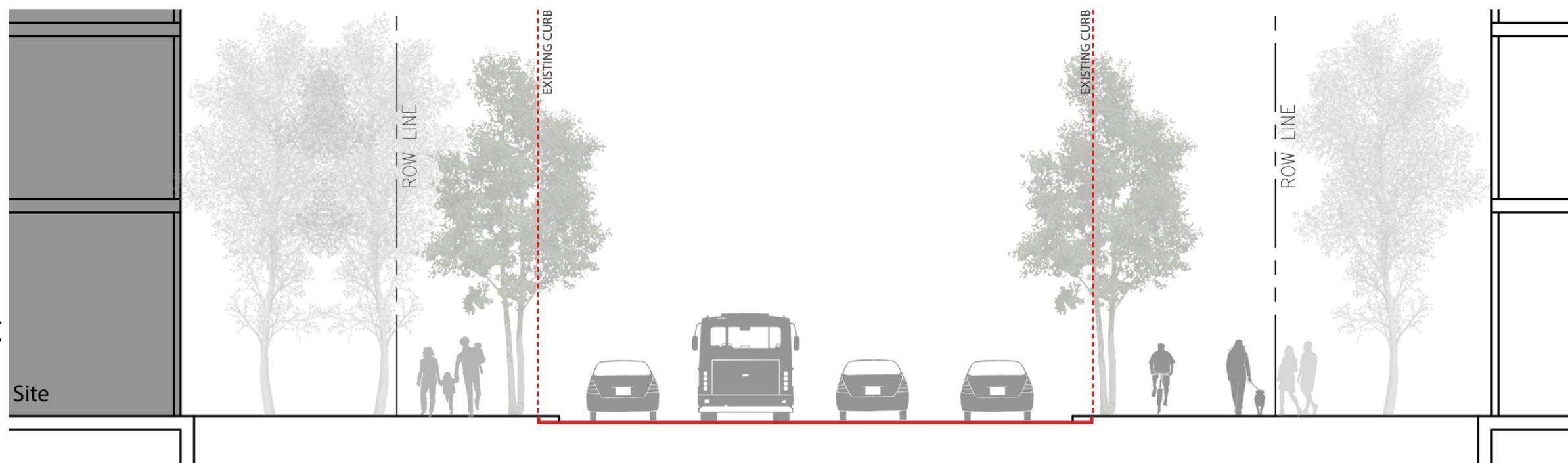
90' Right-of-Way
Class II Pedestrian Street



Bell Street
66' Right-of-Way
Green Street



Blanchard Street
66' Right-of-Way
Green Street



8th & BLANCHARD LANDSCAPE PLAN



Illustrative Site Plan

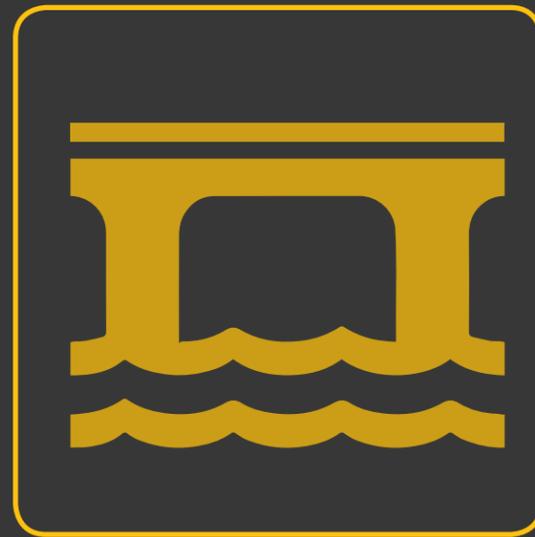
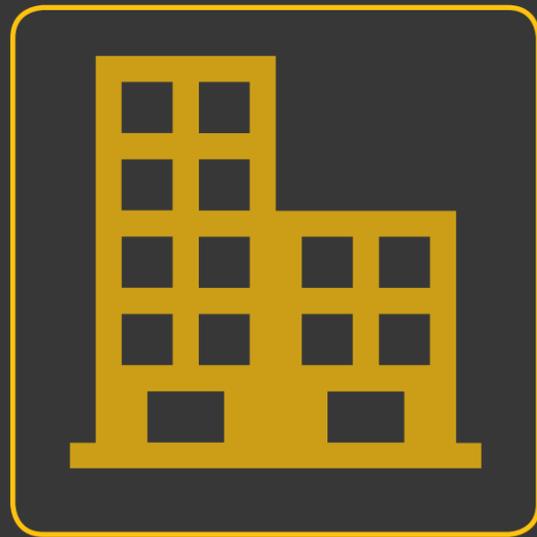


Vicinity Plan



Aerial View

DEVELOPMENT DEPARTURES



DEVELOPMENT DEPARTURES OPTION 2

Additional Setback for Green Street Departures

Item #	Development Standard	Requirement	Departure Amount Required	Rationale	Design Guidelines Reinforced	Reference
1	23.49.056 Street Facade, Landscaping, and Street Setbacks	B. Facade Setback Limits 2. General Setback Limits. The following setback limits apply on streets not requiring property line facades, as shown on Map 1H. d. The maximum setback of the facade from the street lot lines at intersections is 10 feet. The minimum distance the facade must conform to this limit is 20 feet along each street.	The setback at the corner of 8th Ave (& Bell St) exceeds the maximum setback limit by 10 feet.	The entry at this corner is enhanced and supported by a more gracious zone between the public right-of-way and the building.	A-1 Respond to physical environment B-2 Create a transition in bulk & scale B-4 Design a well-proportioned & unified building C-4 Reinforce building entries	Diagram 1
2	Same as (1) above	Same as (1) above	The setback at the corner of 7th Ave (& Blanchard St) exceeds the maximum setback limit by 10 feet.	The entry at this corner is enhanced and supported by a more gracious zone between the public right-of-way and the building.	A-1 Respond to physical environment B-2 Create a transition in bulk & scale B-4 Design a well-proportioned & unified building C-4 Reinforce building entries	Diagram 1

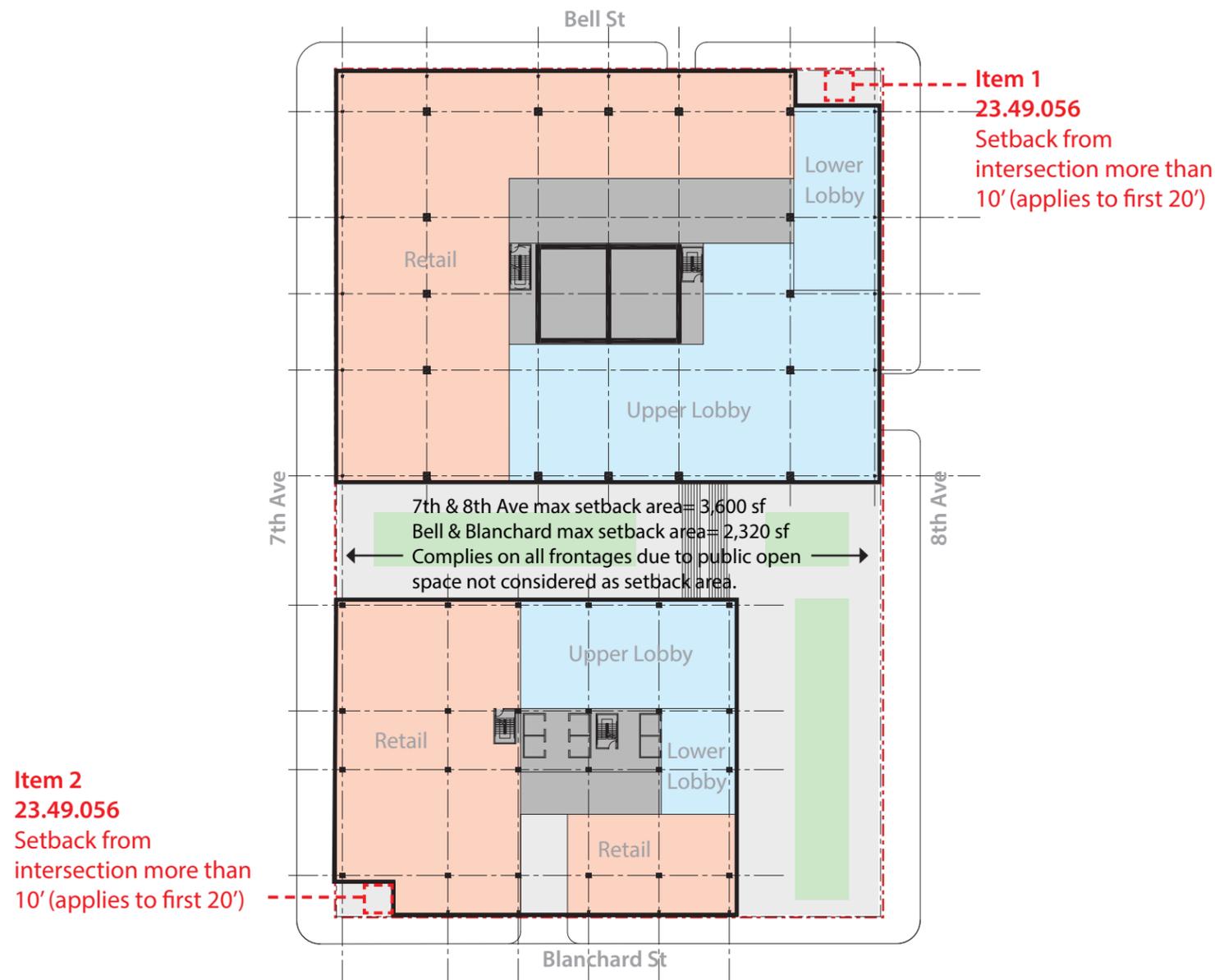


Diagram 1-
Level 1 Plan



Diagram 2-
Upper Level Plan

DEVELOPMENT DEPARTURES OPTION 3 (PREFERRED)

Additional Setback for Green Street Departures

Item #	Development Standard	Requirement	Departure Amount Required	Rationale	Design Guidelines Reinforced	Reference
1	23.49.056 Street Facade, Landscaping, and Street Setbacks	B. Facade Setback Limits 2. General Setback Limits. The following setback limits apply on streets not requiring property line facades, as shown on Map 1H. d. The maximum setback of the facade from the street lot lines at intersections is 10 feet. The minimum distance the facade must conform to this limit is 20 feet along each street.	The setback at the corner of 8th Ave (& Bell St) exceeds the maximum setback limit by 10 feet.	The entry at this corner is enhanced and supported by a more gracious zone between the public right-of-way and the building.	A-1 Respond to physical environment B-2 Create a transition in bulk & scale B-4 Design a well-proportioned & unified building C-4 Reinforce building entries	Diagram 1
2	Same as (1) above	Same as (1) above	The setback at the corner of Bell St (& 8th Ave) exceeds the maximum setback limit by 10 feet.	The entry at this corner is enhanced and supported by a more gracious zone between the public right-of-way and the building.	A-1 Respond to physical environment B-2 Create a transition in bulk & scale B-4 Design a well-proportioned & unified building C-4 Reinforce building entries	Diagram 1
3	Same as (1) above	Same as (1) above	The setback at the corner of 8th Ave (& Blanchard St) exceeds the maximum setback limit by 10 feet.	The entry at this corner is enhanced and supported by a more gracious zone between the public right-of-way and the building.	A-1 Respond to physical environment B-2 Create a transition in bulk & scale B-4 Design a well-proportioned & unified building C-4 Reinforce building entries	Diagram 1

Facade Modulation Departure

Item #	Development Standard	Requirement	Departure Amount Required	Rationale	Design Guidelines Reinforced	Reference						
4	23.49.058 Upper-Level Development Standards	B. Facade Modulation 2. The maximum length of a facade without modulation is prescribed in Table 23.49.058A... <table border="1"> <tr> <td>Elevation</td> <td>Max length un-modulated facade w/in 15' of prop line</td> </tr> <tr> <td>161-240'</td> <td>125'</td> </tr> <tr> <td>241-500'</td> <td>100'</td> </tr> </table>	Elevation	Max length un-modulated facade w/in 15' of prop line	161-240'	125'	241-500'	100'	At the eastern facade of the large building the maximum facade length without modulation is greater than the dimensions in table 23.49.058A by 10' at elevation 161-240 feet and by 35' at elevation 241-340 feet.	The overall massing of the tower is intended to be shaped at it's lower levels to allow access to daylight and views at the critical 8th and Bell Green Street intersection. This form also results in a tower with more vertical, elegant proportions and better responds to the neighborhood context than a code-compliant tower with facade modulation at mid-block or setbacks at upper corners where impact on pedestrians is less perceivable.	A-2 Enhance the skyline B-1 Respond to neighborhood context B-4 Design a well-proportioned & unified building	Diagram 2
Elevation	Max length un-modulated facade w/in 15' of prop line											
161-240'	125'											
241-500'	100'											

Upper Level Setback Departure

Item #	Development Standard	Requirement	Departure Amount Required	Rationale	Design Guidelines Reinforced	Reference
5	23.49.058 Upper-Level Development Standards	F. Upper Level Setbacks 2. When a lot in a DMC or DOC2 zone is located on a designated green street, a continuous upper-level setback of fifteen (15) feet shall be provided on the street frontage abutting the green street at a height of forty-five (45) feet.	At Bell street an elevator penthouse/overrun encroaches in the green street designated setback by approximately 10 feet.	The elevators provide required handicap access from the lower grade level at 8th and Bell to the upper public plaza and the upper lobby on level 3. The elevators are located here to function better for building visitors and the public by giving a clear visual cue as to how to ascend to the upper lobby and plaza. The elevators are intended to be glass-enclosed (transparent), so their functionality also adds to the sense of active street-level use.	A-1 Respond to physical environment B-2 Create a transition in bulk & scale B-4 Design a well-proportioned & unified building	Diagram 1

DEVELOPMENT DEPARTURES OPTION 3 (PREFERRED)

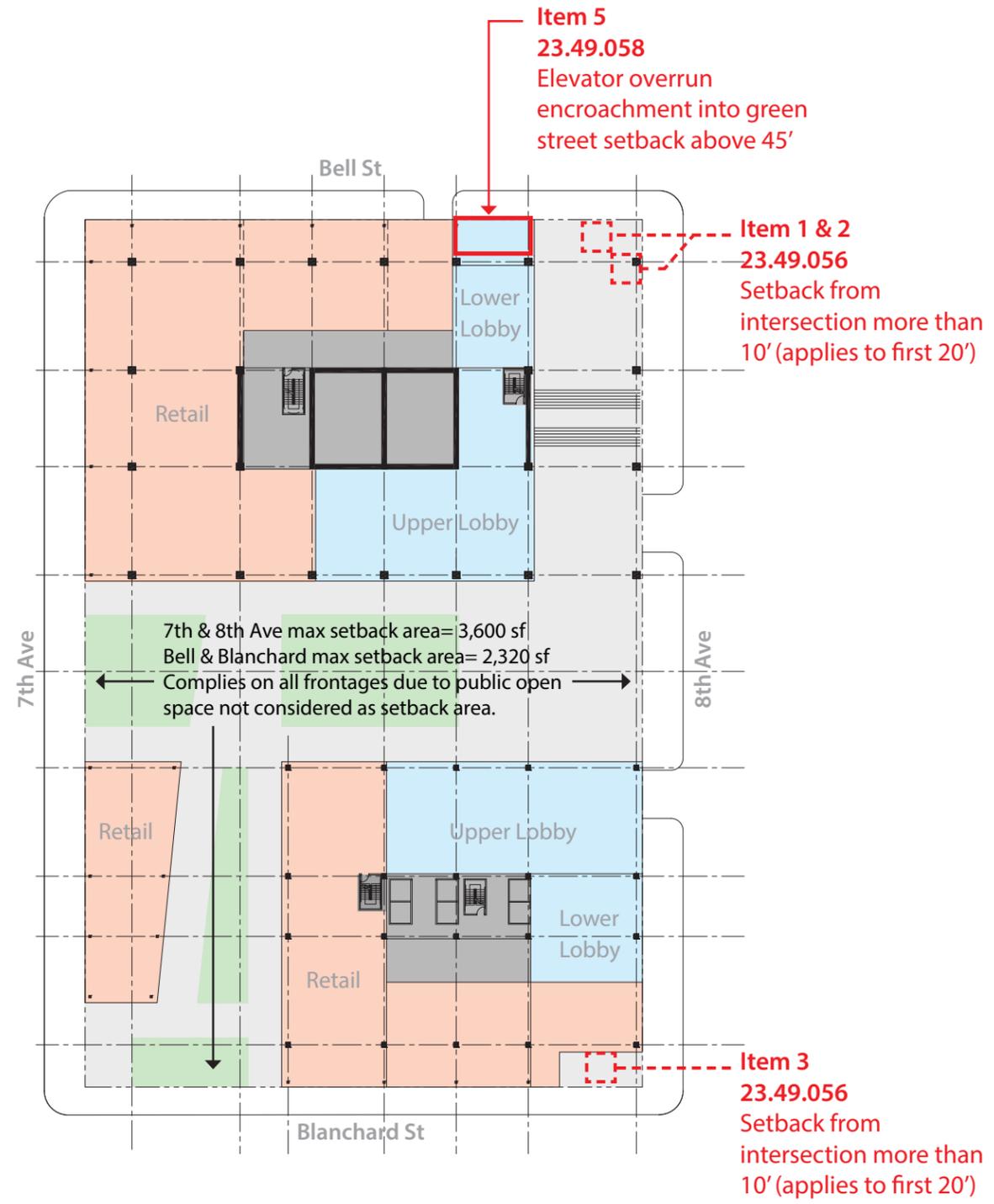


Diagram 1- Level 1 Plan

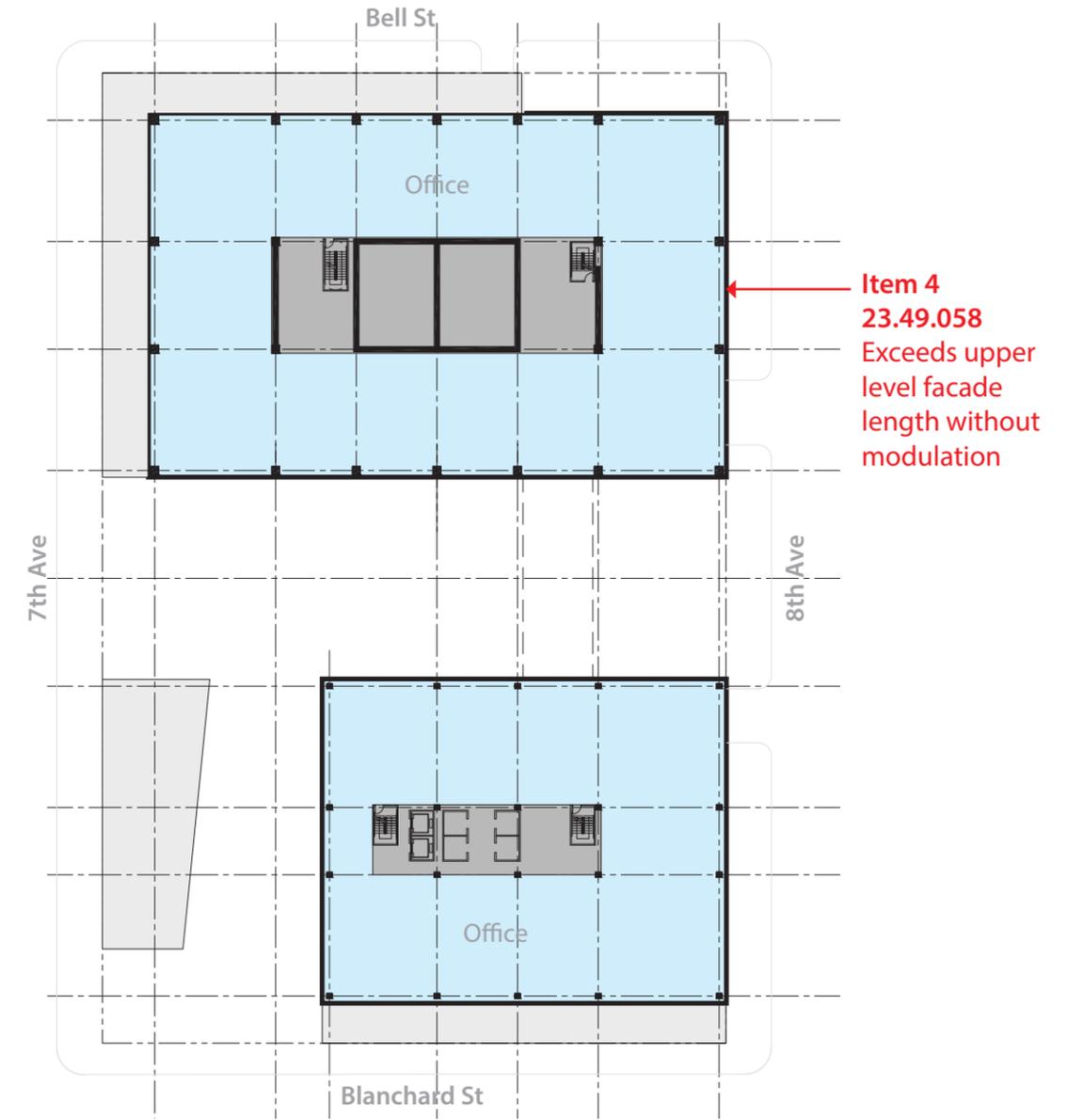


Diagram 2- Upper Level Plan