

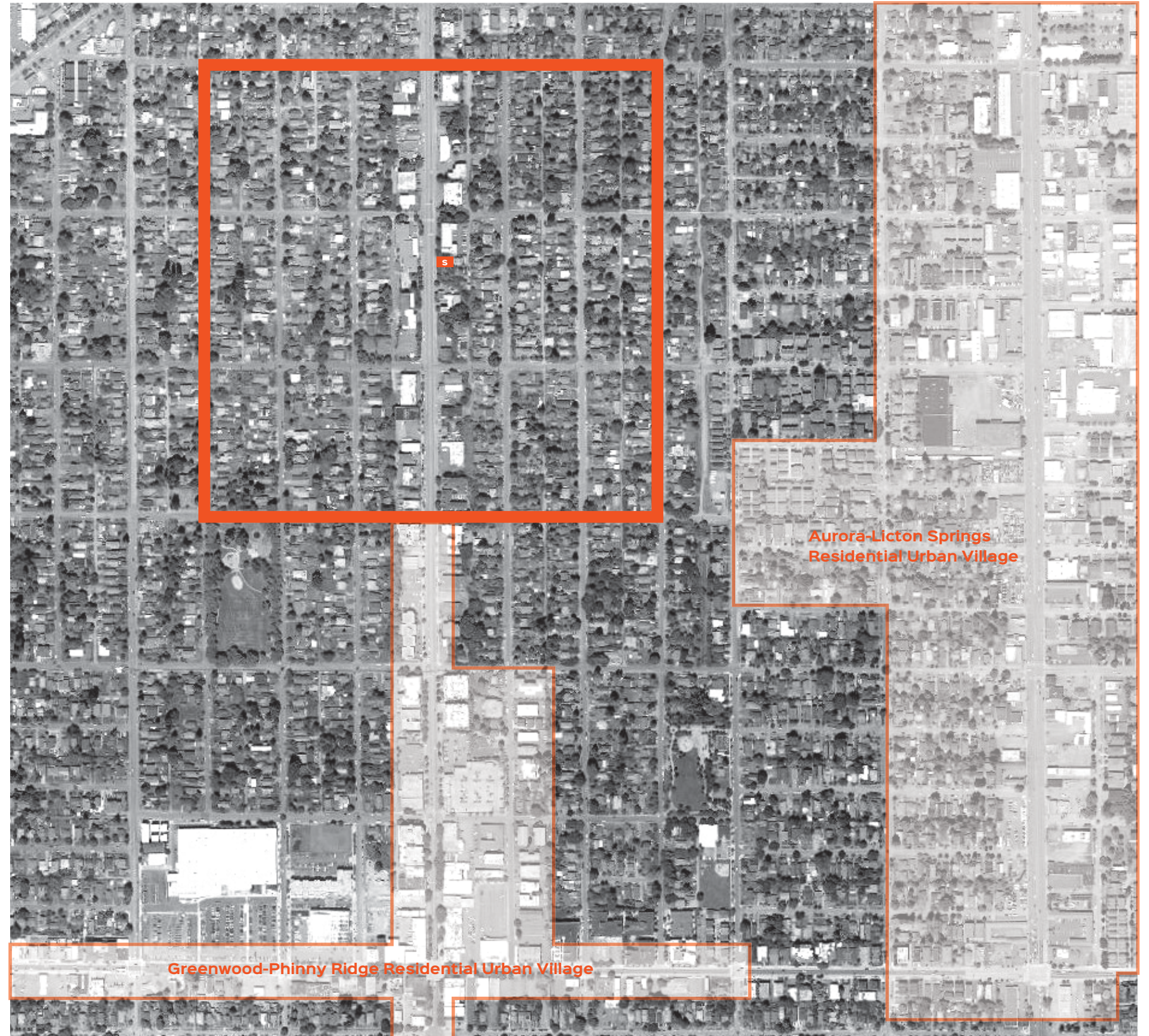
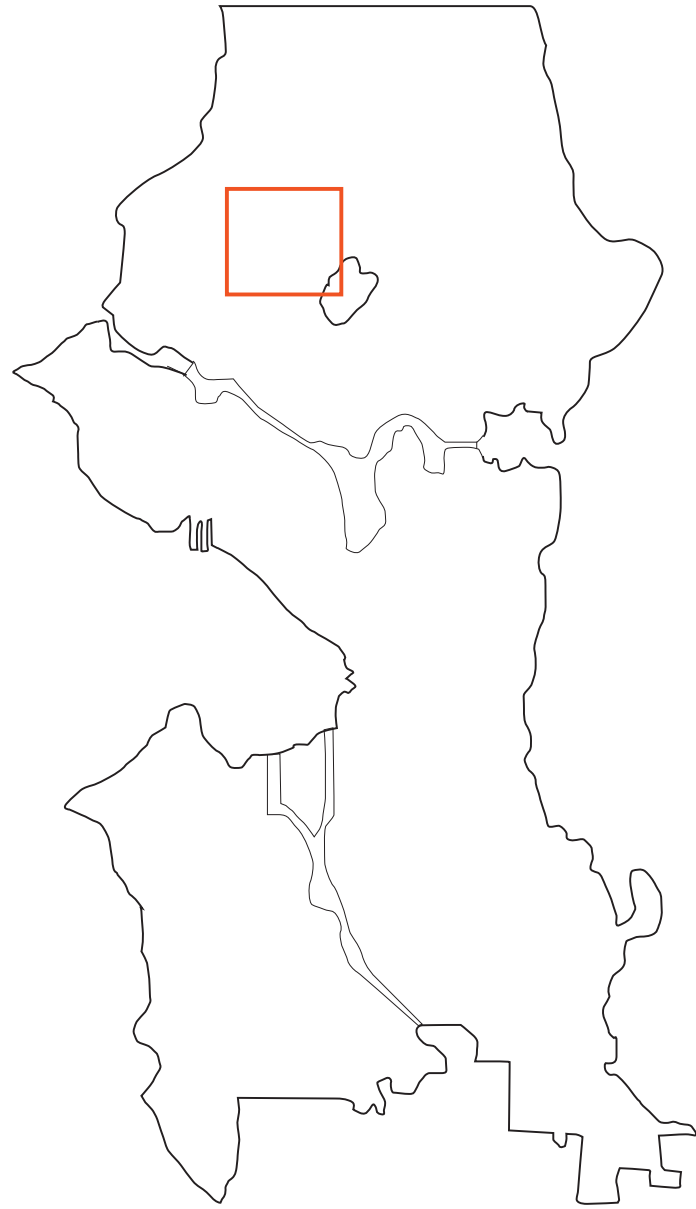


9532 GREENWOOD AVE N

EARLY DESIGN GUIDANCE PACKET • OCTOBER 2015 • DPD# 3018436

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PROPOSAL

Project Summary:

The proposed project is a seven story mixed use building containing two ground level live work units and 14 residential apartments. Parking for 8 vehicles will be provided onsite (one of which is dedicated to the existing duplex).

Total Unit Count: 14

Live/Work Units: 2 - 1530 SF total
(No Parking Required)

Apartment Units: 12 - 9257 SF apartments

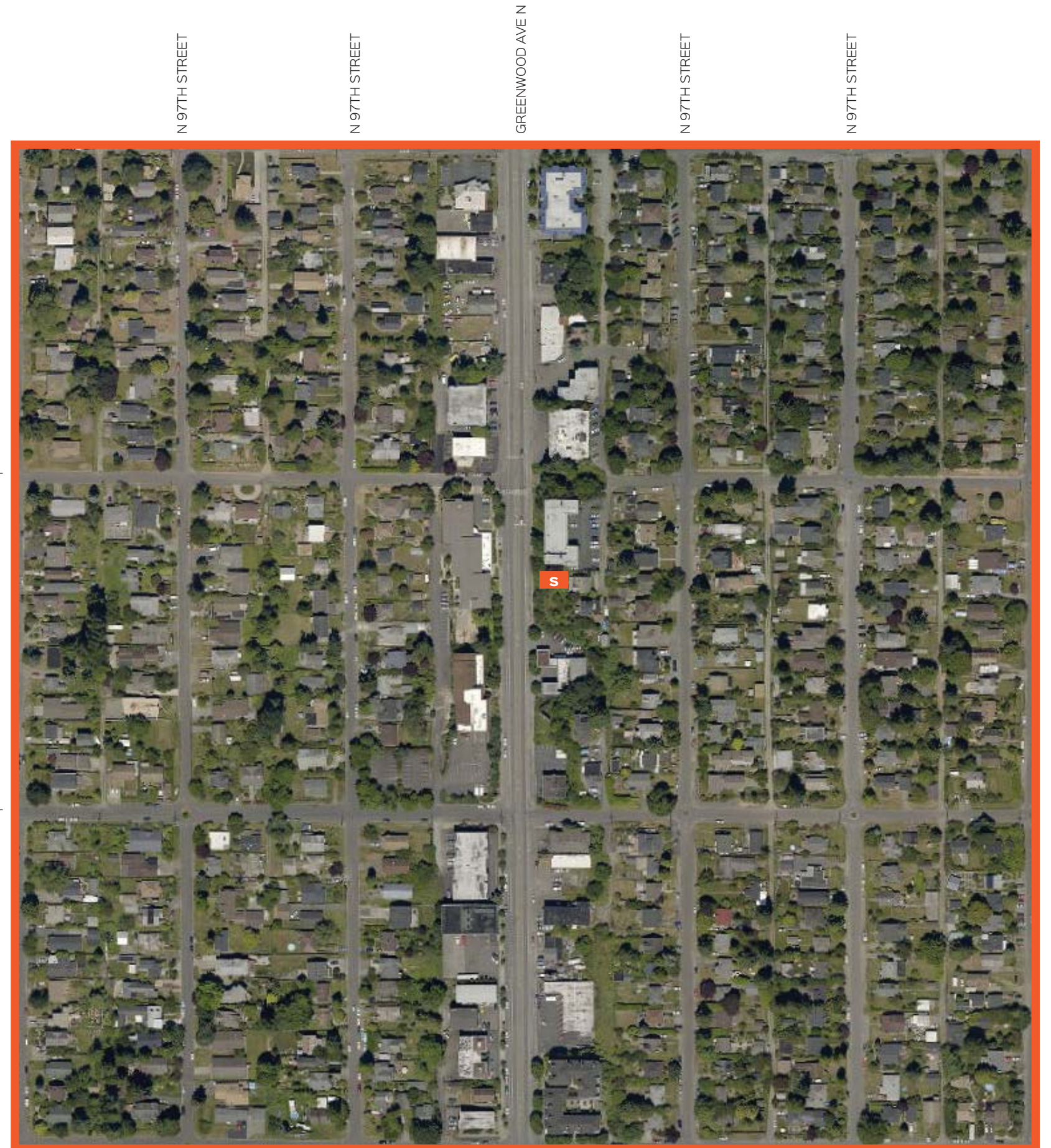
Parking Stall Count: 5 Covered Parking
(Including Accessible Van Parking), 3 Surface
Parking at Alley

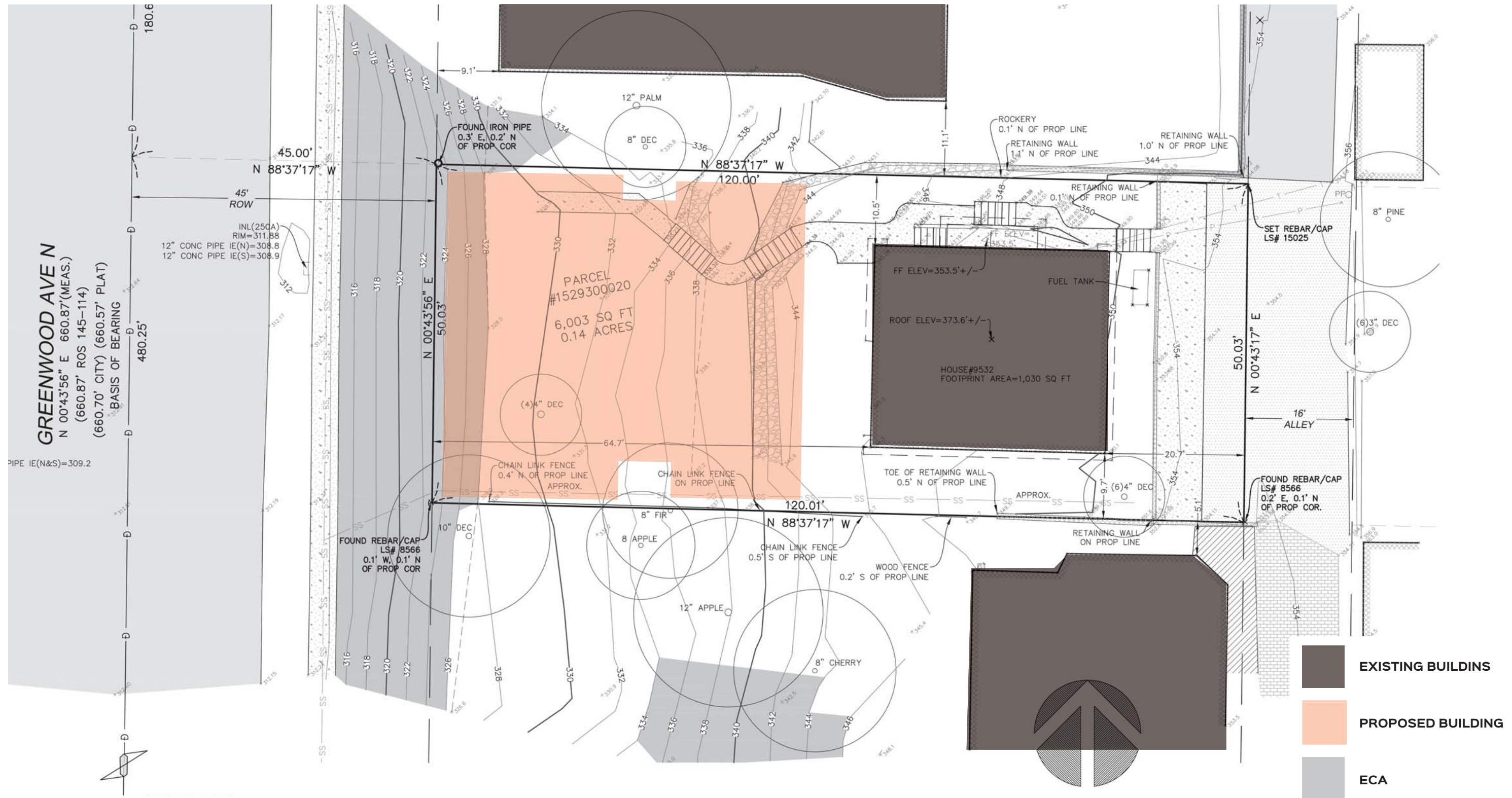
Legal Description:

Lot 4, block 1, Chateau Thiery, according to the plat thereof recorded in volume 22 of plats, page 88, records of King County, Washington.

N 97TH STREET

N 95TH STREET





ZONING DATA

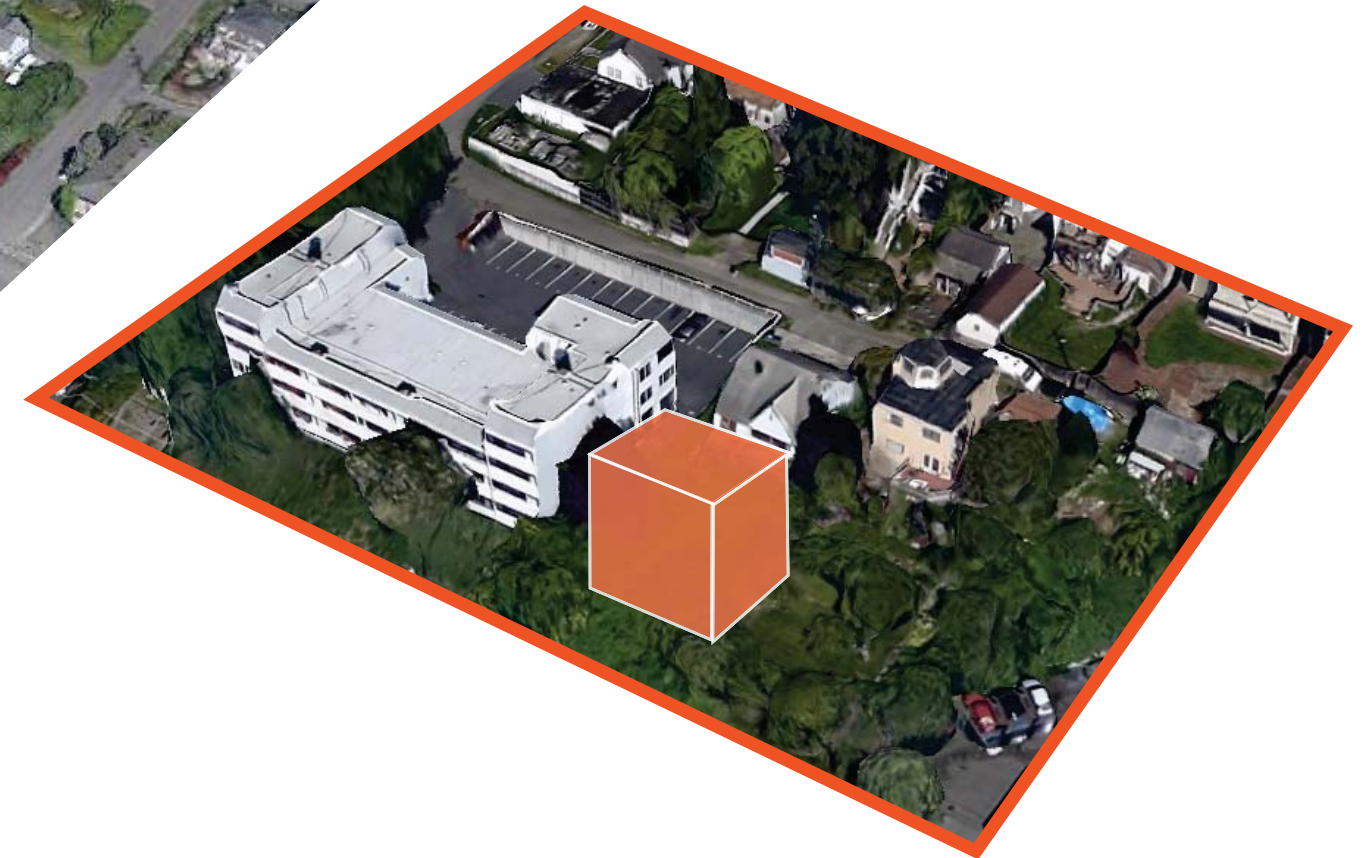
PRELIMINARY SEATTLE ZONING DATA

DEVELOPMENT STANDARD	COMPLIANCE
23.47A.004 Table A. Permitted and prohibited uses	All options compliant
23.47A.008.A Basic street-level requirements	All options compliant
23.47A.008.B.3 Non-residential street-level requirements	Departure Requested #1 to allow an average commercial depth that is less than 30' in depth. This would require significant excavation, shoring, tiebacks into neighboring properties and structure for this specific site.
23.47A.008.E Live-work units	All options compliant
23.47A.010 Maximum size of nonresidential use	All options compliant
23.47A.012.A Structure Height	All options compliant and include 4' height bonus pursuant to 23.47A.012.A.1.a. See view analysis at page 26.
23.47A.012.C. Rooftop Features	All options compliant
23.47A.013 Floor Area Ratio	All options compliant
23.47A.016 Landscaping and Screening Standards	All options compliant
23.47A.022 Light and Glare Standards	All options compliant
23.47A.024 Amenity Area	Departure Requested #2 to allow a common amenity area at the south yard. This location is 9'-9" in width which means it is 3" shy of the 10' width requirement for common amenity areas.
23.47A.030 Required Parking and Loading	All options compliant
23.54.015.K Bicycle Parking	All options compliant
23.47A.032.A Parking location and access	All options compliant
23.54.020.G Sight Triangles	Departure Requested #3 to allow a mirror to be used to provide visibility into the alley from the driveway in lieu of the required sight triangle.
23.54.030 Parking Space Requirements	All options compliant
23.54.030.C Backing Distances and Moving Other Vehicles	All options compliant
23.54.040 Solid waste and recyclable materials storage and access	All options compliant

CONTEXT ANALYSIS: AERIAL PHOTOGRAPH

COMMUNITY NODES AND LANDMARKS

The project vicinity is a transitioning mixed use district outside two nearby residential urban villages: Greenwood-Phinney Ridge to the south and Aurora-Licton Springs to the east. Though Greenwood Avenue forms a major commercial node 10 blocks to the south at N 85th Street, there is no major activity center between there and the N 105th Street and Holman Road commercial area.



CONTEXT ANALYSIS: LAND USE







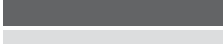
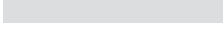
Uses

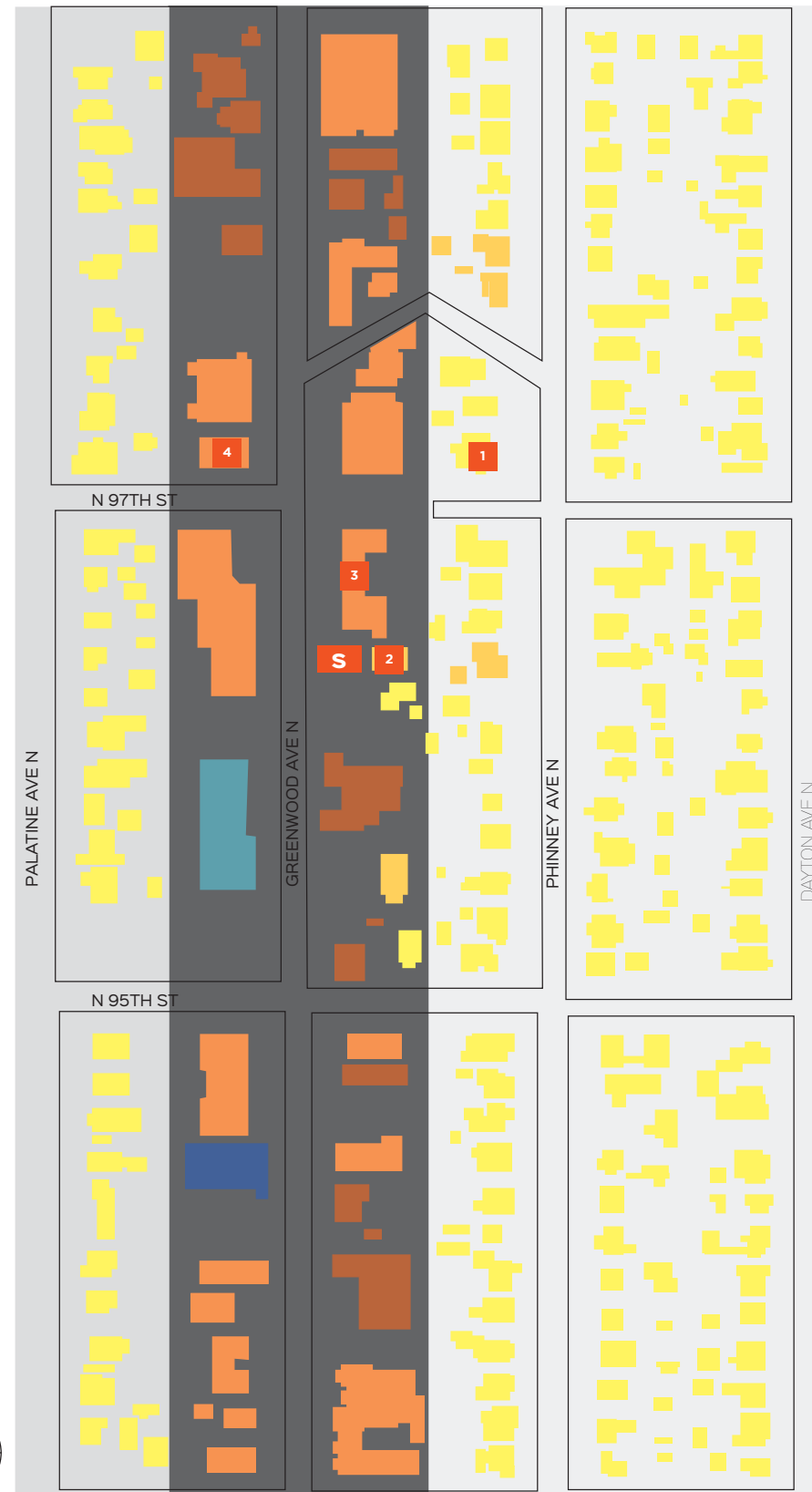
The area surrounding the project site is developed with 2-4 story residential buildings, single-story commercial uses, a few single family dwellings, and a Salvation Army residence with a preschool and childcare facility.

Zoning

Zoning along Greenwood Avenue is Commercial 1 (C1) with Single Family 5000 (SF 5000) beginning one parcel back to the east and west.

Legend

	SF Residential
	Duplex
	Multi Family
	Retail and Commercial
	Public Facilities
	Industrial Uses
	Commercial Zone C1
	Single Family Zone



CONTEXT ANALYSIS: NOTABLE ARCHITECTURE

Notable Architecture & Siting Patterns

Windows patterns are predictable and establish a rhythm indicative of the mid-century era of which much of the surrounding context was built

Surface parking is prominent and not concealed.

Balconies and exterior stairs are frequent.



CONTEXT ANALYSIS: GREENWOOD AVE

PROJECT SITE

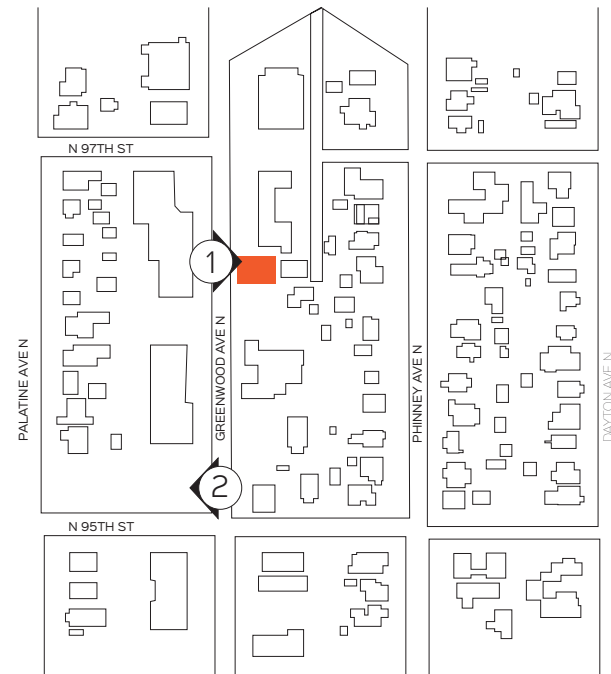


1 - Looking East

OPPOSITE PROJECT SITE



2 - Looking West



PROJECT SITE

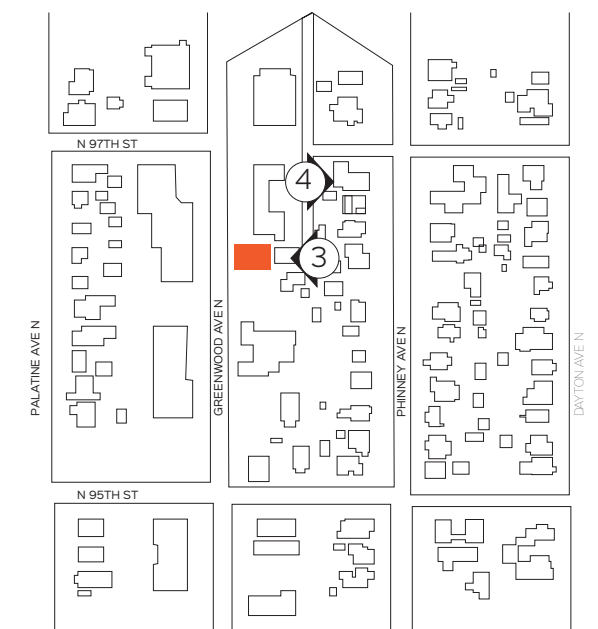


3 - Looking West

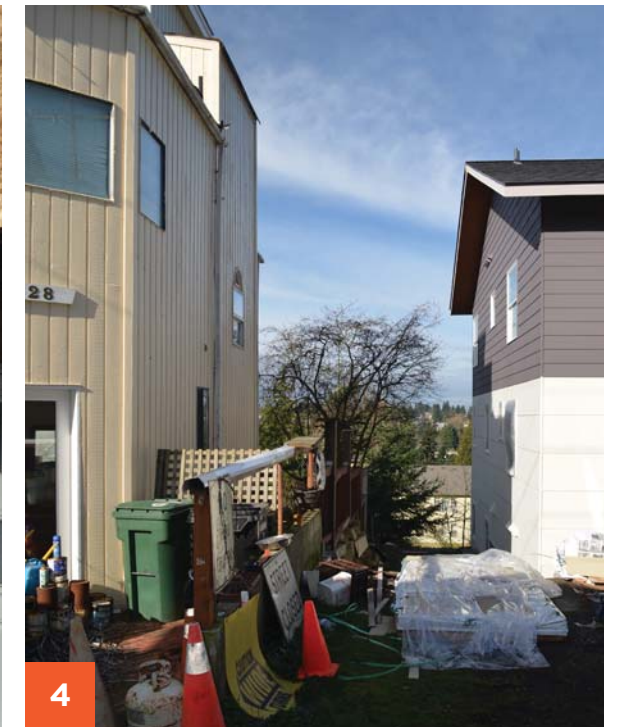
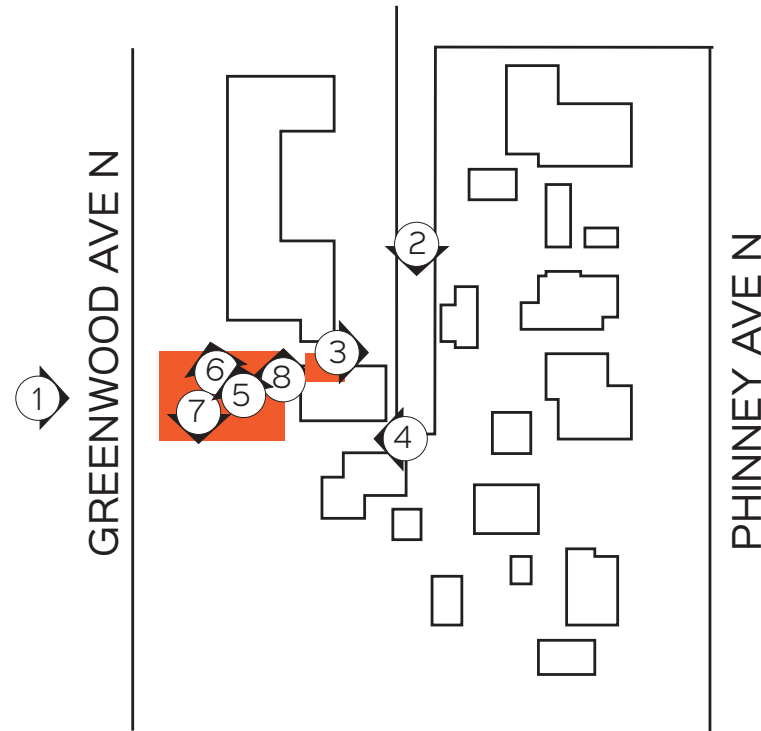


4 - Looking East

OPPOSITE PROJECT SITE



CONTEXT ANALYSIS: PHOTOS OF SITE



CONTEXT ANALYSIS: PHOTOS FROM SITE



NEIGHBORHOOD DESIGN CUES

Site Stair / Hill Climb

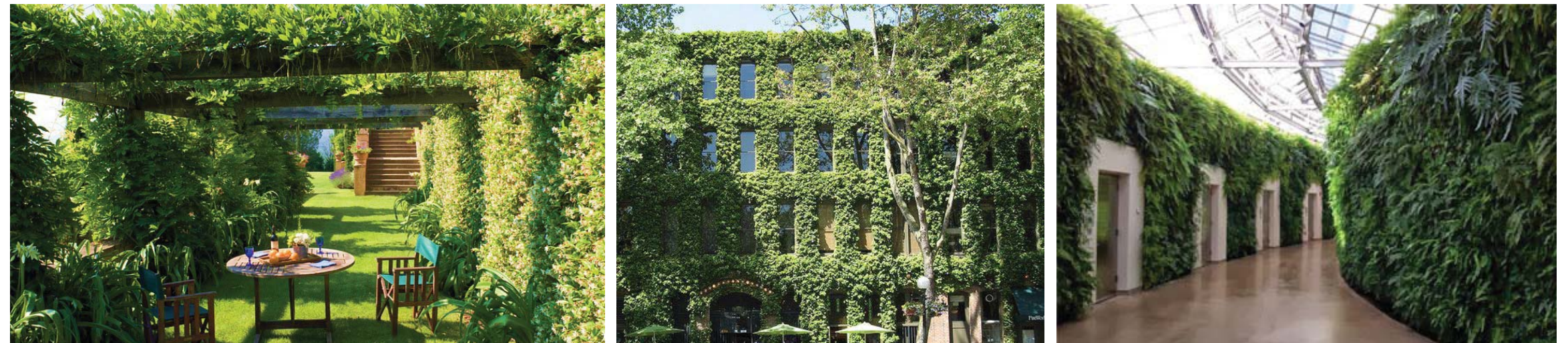
- The lot to south provides easy pedestrian access between the alley at the rear of the lot and Greenwood. Without this stair, residents would have to walk a significant distance because the alley at the rear of the lot is a dead end. With a preponderance of steep slopes in the area, site stairs are common.

Green Room

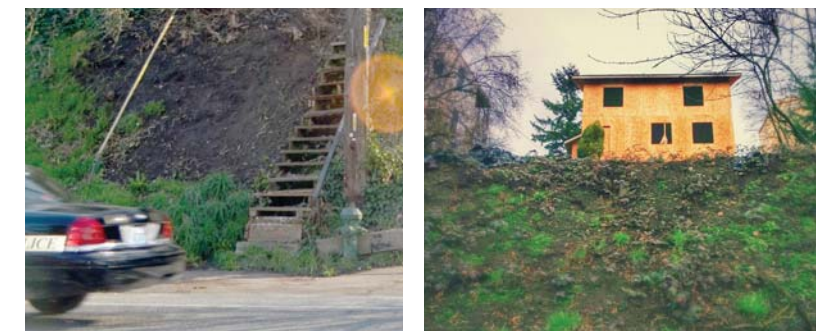
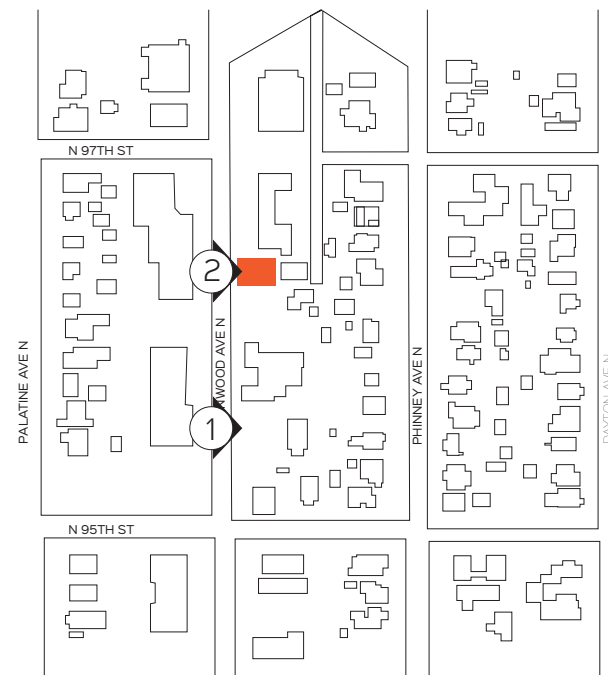
- The right of way is currently sloping at roughly 1:1 and provides a vegetated edge to Greenwood Ave. Right of way improvements for this project require excavating down to the elevation of Greenwood, and it is likely that it will be many years before the adjacent properties will have to excavate down as well. With this in mind, the newly excavated "room" provides an opportunity to reintroduce natural vegetation into the experience along Greenwood Ave.



Site Stair / Hill Climb Inspiration



Green Room Inspiration



Site Stair / Hill Climb Cue

Green Room Cue

NEIGHBORHOOD DESIGN CUES



Window / Aperture Rhythm

- Many apartments in the area employ a predictable, window rhythm. This is particularly pronounced at the condo building to the north of the project site. This project will not replicate that pattern, but will still endeavor to establish a predictable and simple window rhythm.

Disengage Ground Plane

- The condo building to the north of the property floats over the landscape rather than resting solidly on the earth. The earth appears to “peel away” from the building. This creates a very interesting tension to play off of with this new project.

Window / Aperture Rhythm Inspiration



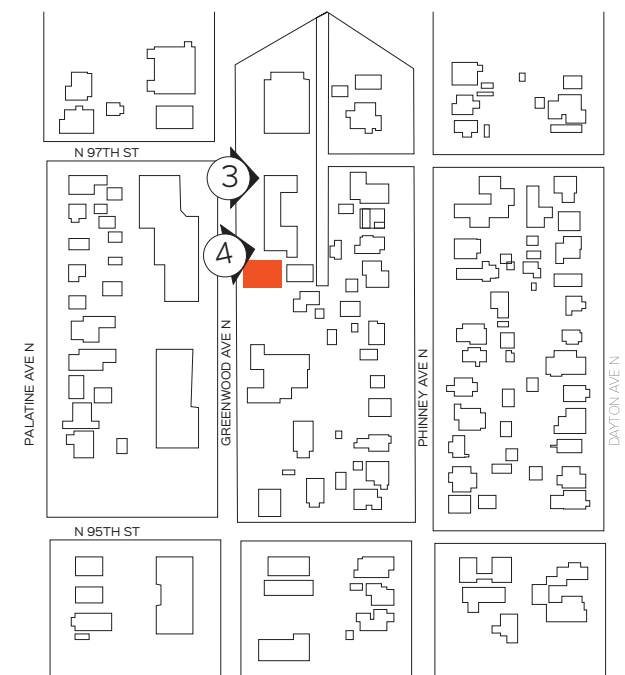
Disengage Ground Plane Inspiration



Window / Aperture Rhythm Cue



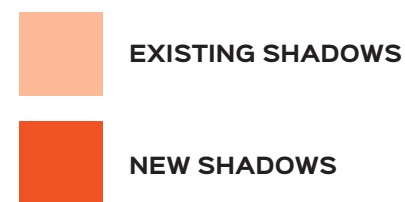
Disengage Ground Plane Cue



ADDITIONAL INFORMATION: SHADOW STUDIES

Uses

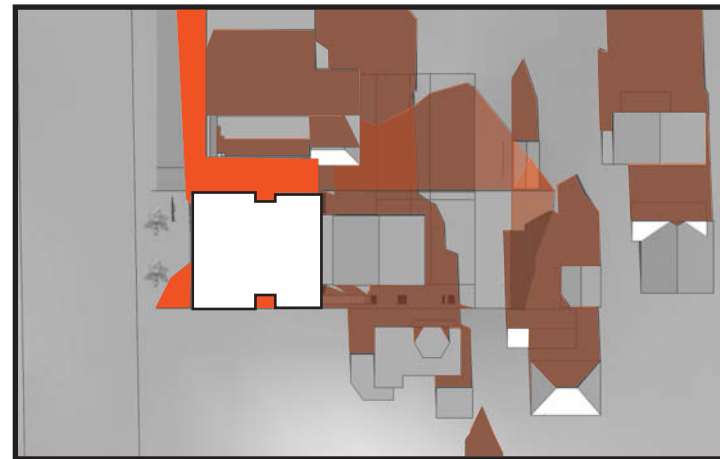
The proposed building predominately creates shadows on the western portion of the south condo wall (to the north). Note that there are few windows located on that portion of the building. Secondly, there will be large shadows cast on the existing duplex to the east on the same property, since this duplex owned by the same owner as the proposed building, it was an anticipated consequence that was deemed acceptable.



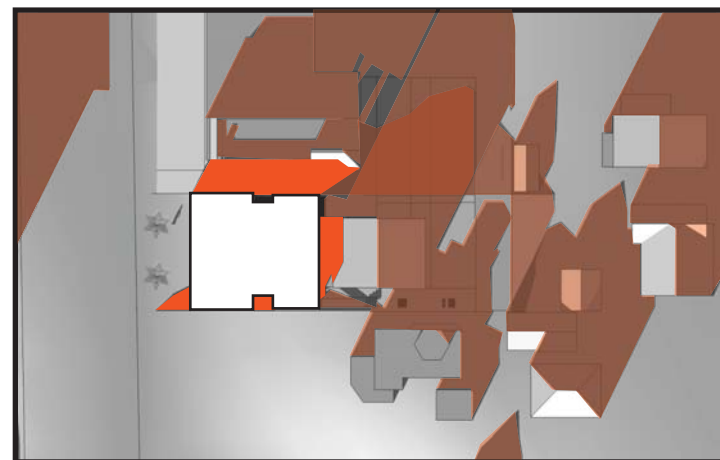
DECEMBER 21



10:00 AM



12:00 NOON

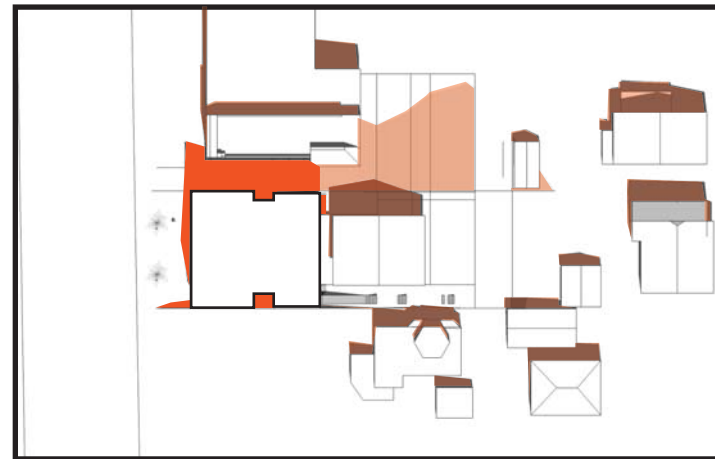


2:00 PM

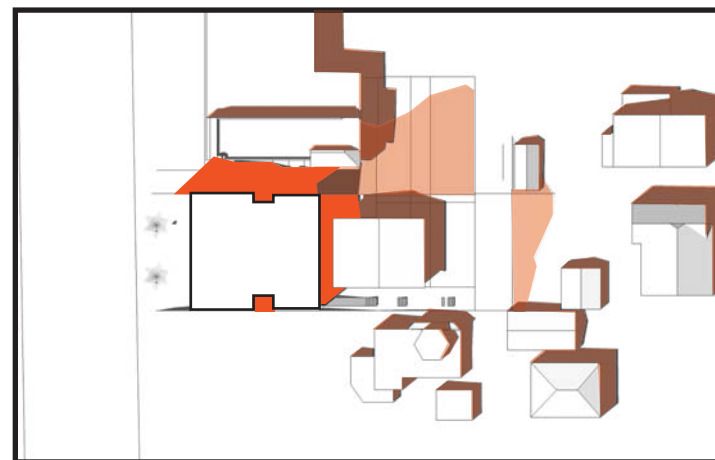
JUNE 21



10:00 AM



12:00 NOON

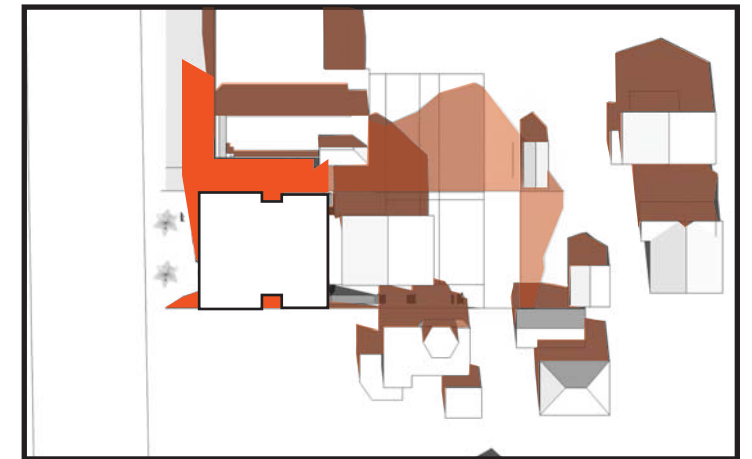


2:00 PM

MARCH 21



10:00 AM



12:00 NOON



2:00 PM

8.0 DESIGN GUIDELINES

CS1 Natural Systems and Site Features



C. TOPOGRAPHY 1. Land Form: Use the natural topography and/or other desirable land forms or features to inform the project design. 2. Elevation Changes: Use the existing site topography when locating structures and open spaces on the site. Consider “stepping up or down” hillsides to accommodate significant changes in elevation.

Response: The building steps up to respond to the existing topography. Right of way improvements require excavating within the right of way, down to the elevation of Greenwood Ave N. This provides an opportunity to create an excavated "green room" at this zone between the street and the building.

CS2 Urban Pattern and Form

D. HEIGHT, BULK, AND SCALE

1. Existing Development and Zoning: Review the height, bulk, and scale of neighboring buildings as well as the scale of development anticipated by zoning for the area to determine an appropriate complement and/or transition. Note that existing buildings may or may not reflect the density allowed by zoning or anticipated by applicable policies.

Response: Greenwood Avenue at the project site still has a significant unbuilt potential based on current zoning. This proposal makes efficient use of an underdeveloped site, while maintaining the existing building therefore creating an appropriate scale in a transitioning area. All three design options rely on landscape, material, and massing to break down the volume of the building. The result is a building that reads as having a base, a middle, and a top that aligns with the elevation of the condo building to the north.

CS3 Architectural Context and Character

A. EMPHASIZING POSITIVE NEIGHBORHOOD ATTRIBUTES

4. Evolving Neighborhoods: In neighborhoods where architectural character is evolving or otherwise in transition, explore ways for new development to establish a positive and desirable context for others to build upon in the future

Response: Building will be a good example for high quality design

PL1 Connectivity



B. WALKWAYS AND CONNECTIONS

1. Pedestrian Infrastructure: Connect on-site pedestrian walkways with existing public and private pedestrian infrastructure, thereby supporting pedestrian connections within and outside the project.

Response: The project's pedestrian pathway/hillclimb will provide a connection from the right of way to the alley behind the site. This connection will provide greater site circulation and matches a similar stairway on a neighboring parcel.

PL2 Walkability



B. SAFETY AND SECURITY

1. Eyes on the Street: Create a safe environment by providing lines of sight and encouraging natural surveillance through strategic placement of doors, windows, balconies and street-level uses.

Response: The live-work uses at the ground level of the proposed structure will have large windows to provide eyes on the street, in addition to windows on the upper stories providing additional surveillance onto both Greenwood Avenue and the pedestrian hillclimb to the south of the site.

PL3 Street-Level



B. RESIDENTIAL EDGES

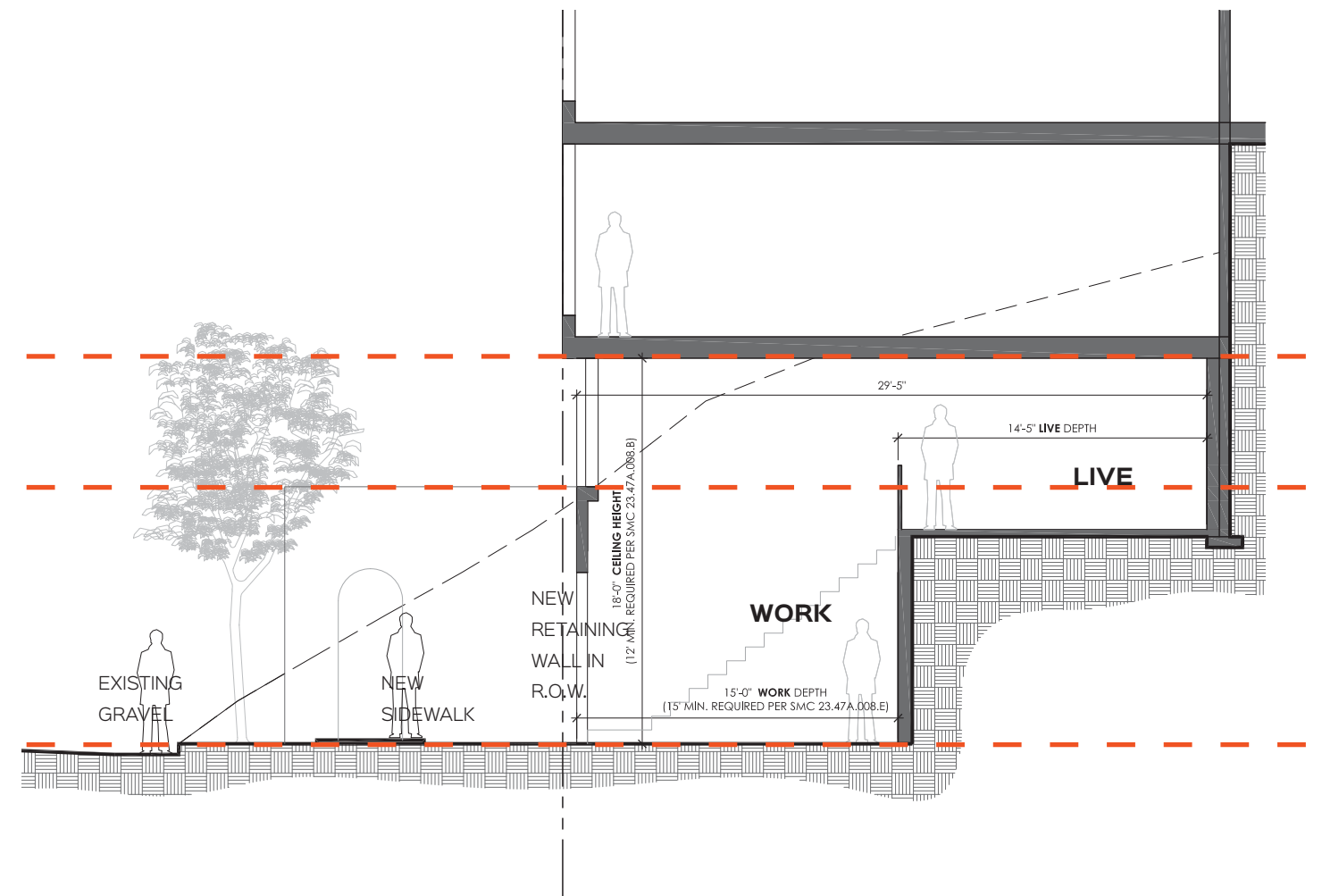
3. Buildings with Live/Work Uses: Maintain active and transparent facades in the design of live/work residences that are required to orient the nonresidential portions of the unit toward the street. Design the first floor so it can be adapted to other commercial use as needed in the future.

Response: The live-work units will maintain transparent facades oriented toward Greenwood Avenue. The nonresidential spaces are designed to be flexible to accommodate a variety of small commercial uses.

SITE DIAGRAM: TOPOGRAPHY

The architecture is experienced as a series of spaces as a person navigates up and down the topography of the site via a stairclimb. The mass of the building is broken down into a series of horizontal divisions that hearken back to existing topography and neighboring buildings.

The site stair provides a sequential connection through the site. As a person ascends they move through Greenroom, LiveWork, Living, Amenity, and finally Alley at the top of the site.



SITE DIAGRAM: GREEN ROOM



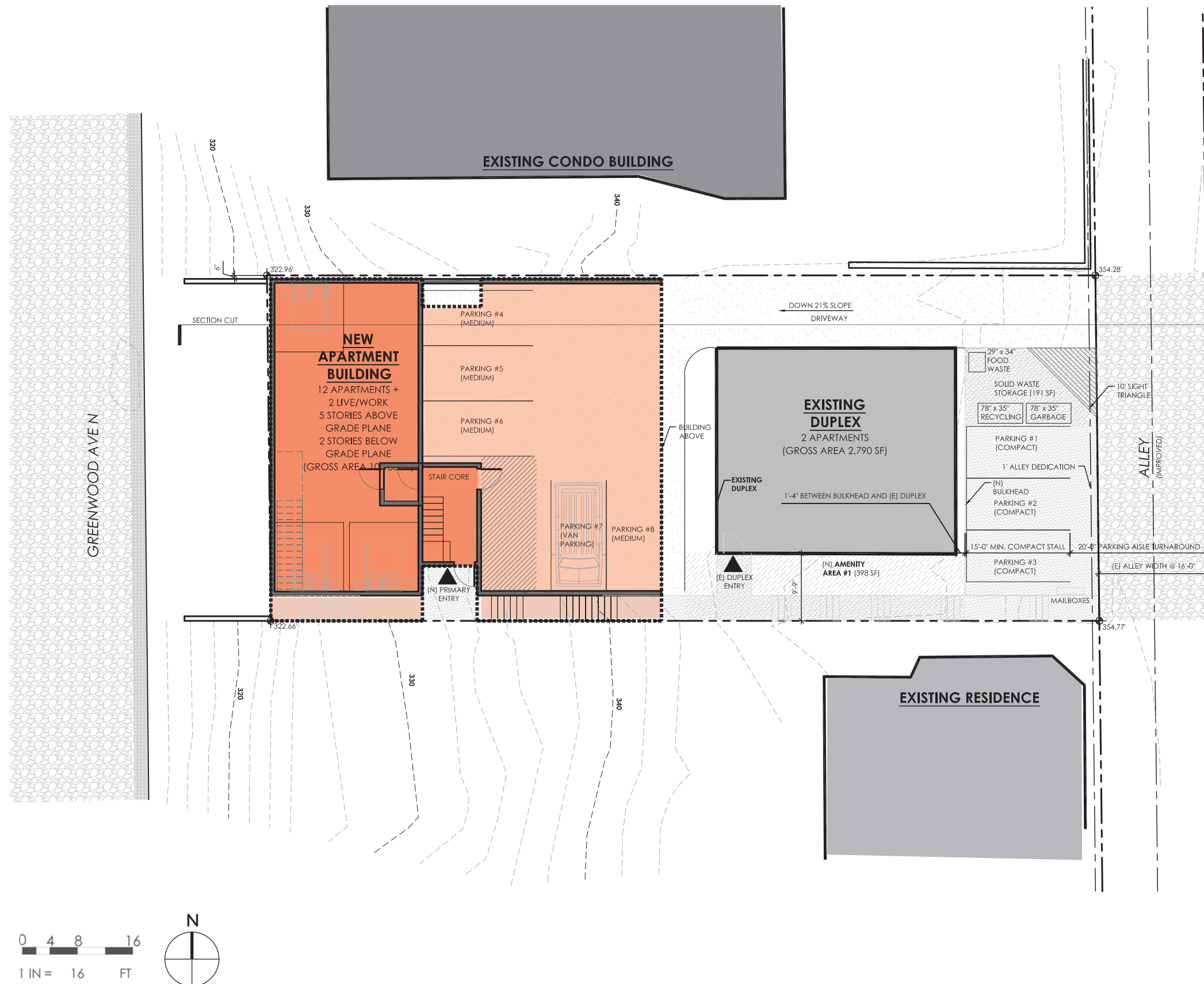
The right of way is currently sloping at roughly 1:1 and provides a vegetated edge to Greenwood.

Right of way improvements for this project require excavating down to the elevation of Greenwood. It is likely that it will be many years before the adjacent properties will have to excavate down as well.

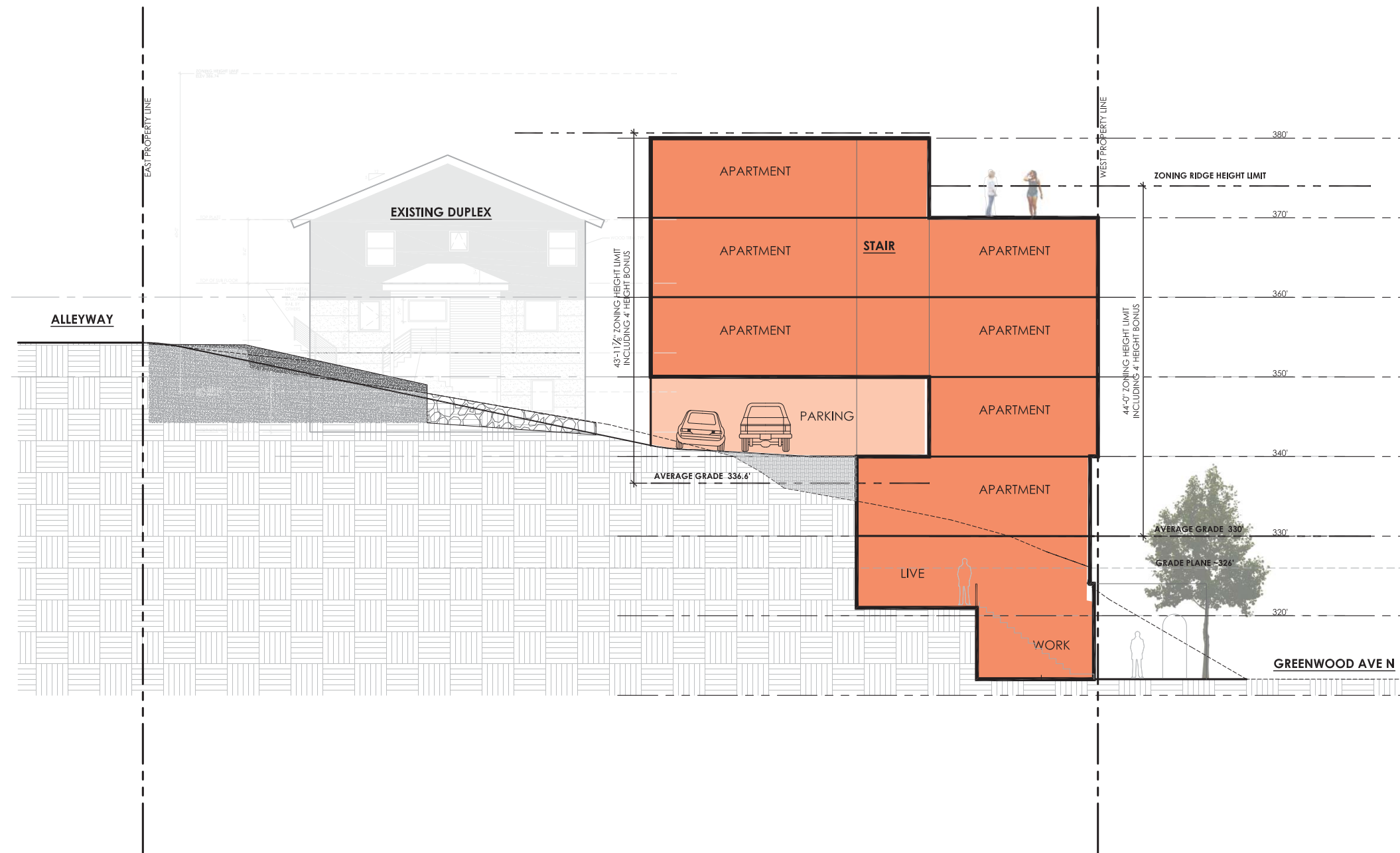
With this in mind, the newly excavated "room" provides an opportunity to reintroduce green & vegetation into the experience along Greenwood Ave.



SITE PLAN: AT PARKING LEVEL



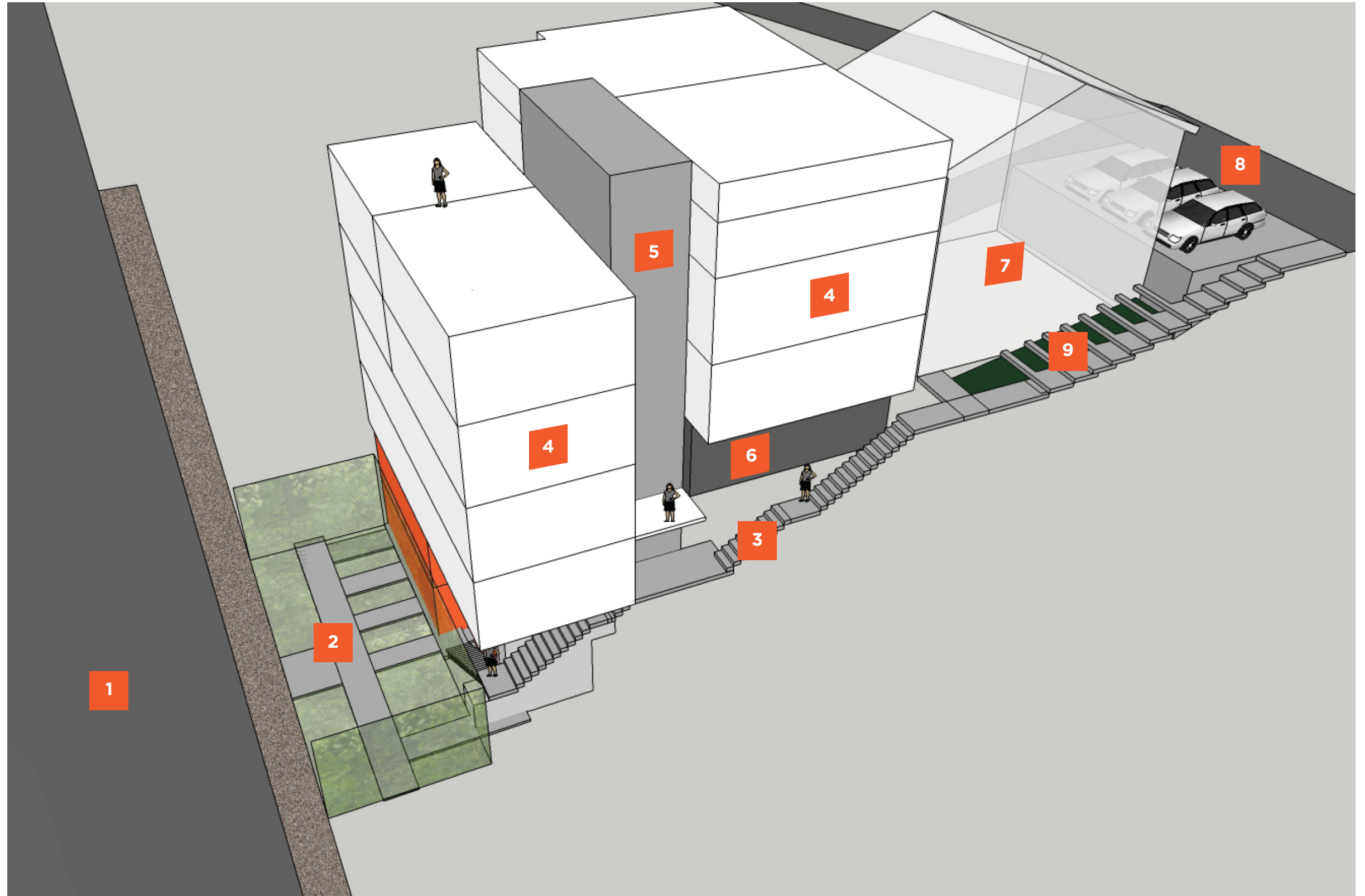
SITE SECTION: THROUGH DRIVEWAY LOOKING SOUTH



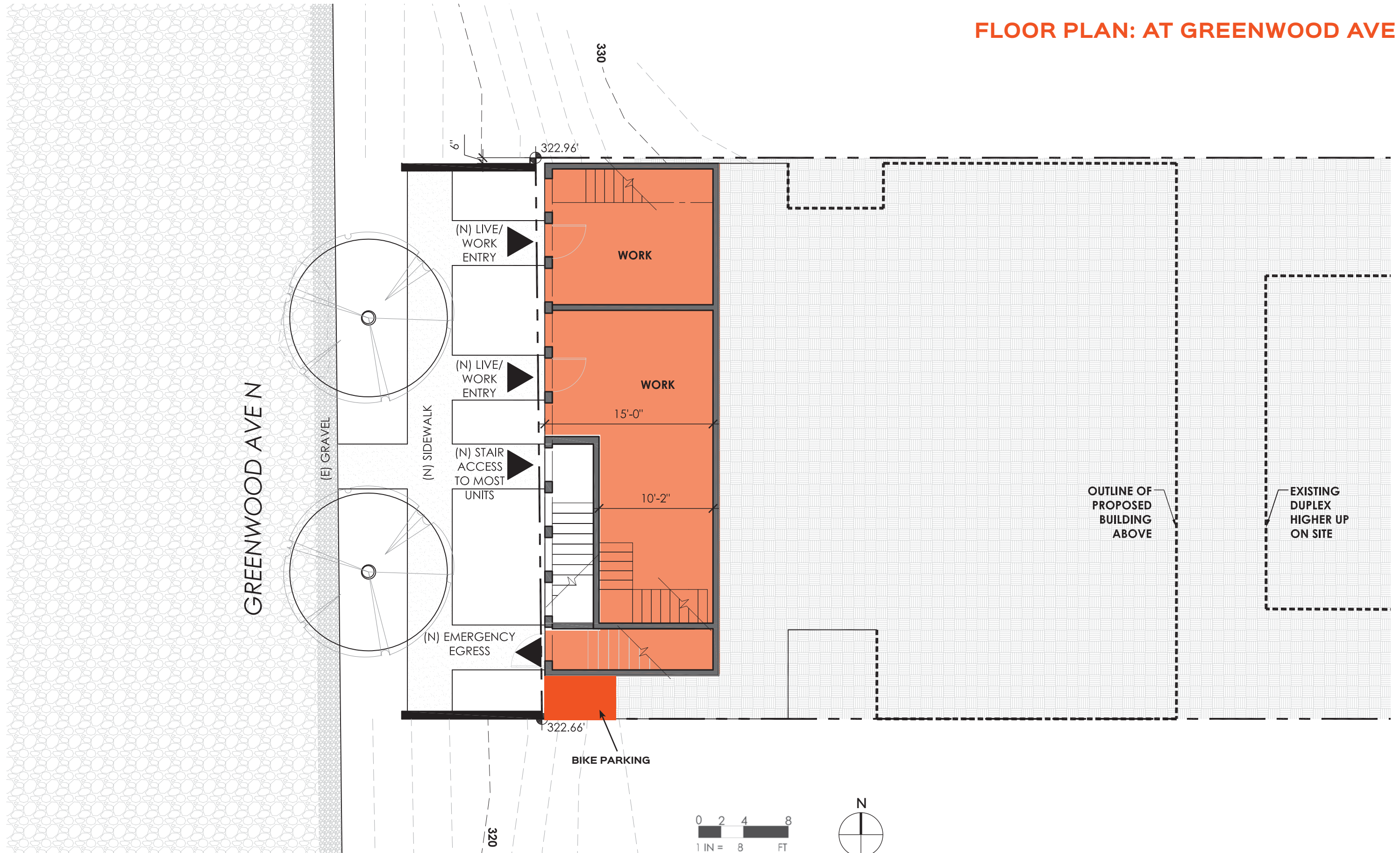
PROGRAM & USES

LEGEND

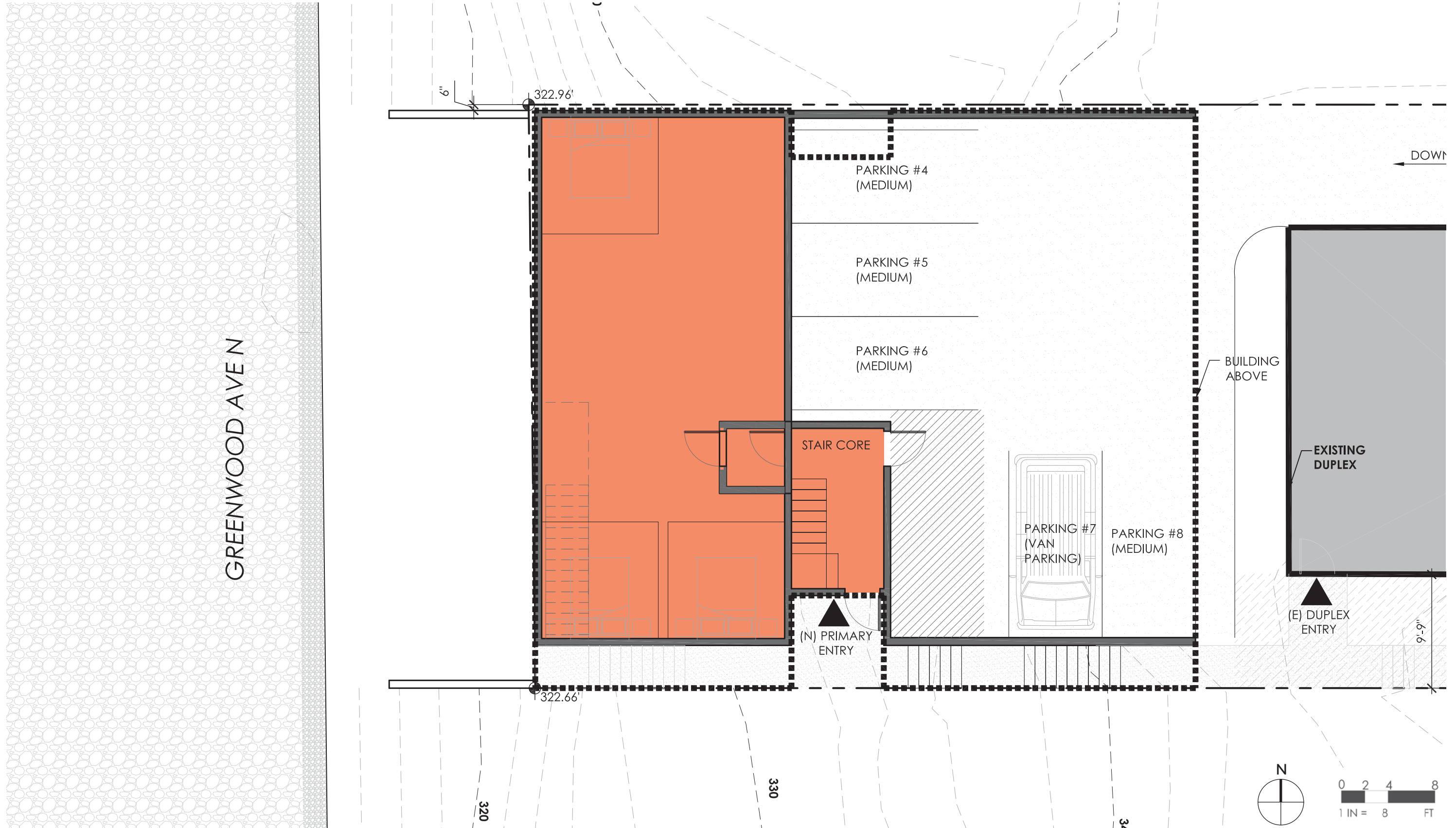
- 1 - GREENWOOD AVE
- 2 - OUTDOOR ROOM
- 3 - ACCESS STAIR
- 4 - APARTMENTS
- 5 - STAIR CORE
- 6 - COVERED PARKING LEVEL
- 7 - EXISTING DUPLEX
- 8 - ALLEY
- 9 - AMENITY



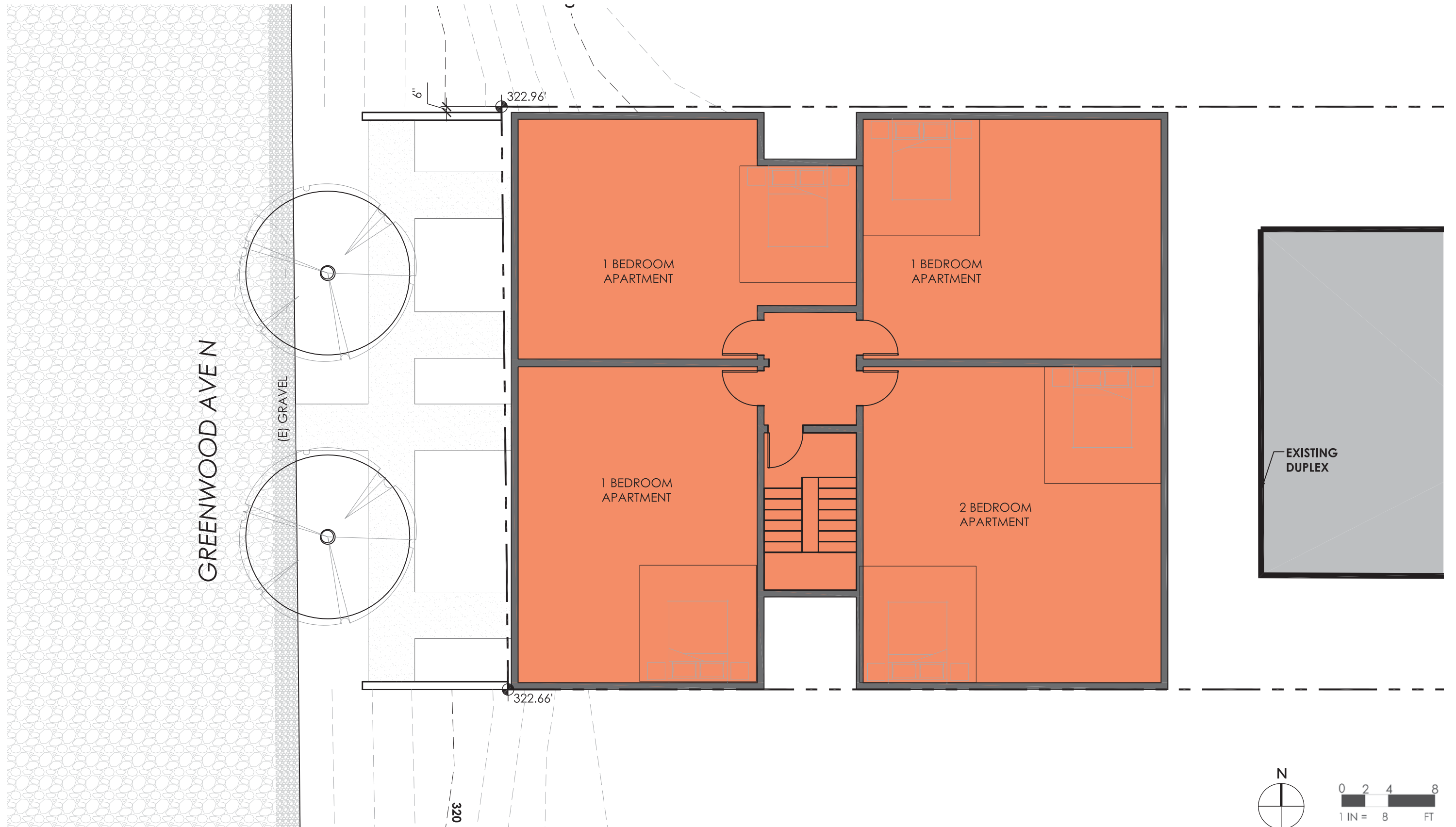
FLOOR PLAN: AT GREENWOOD AVE



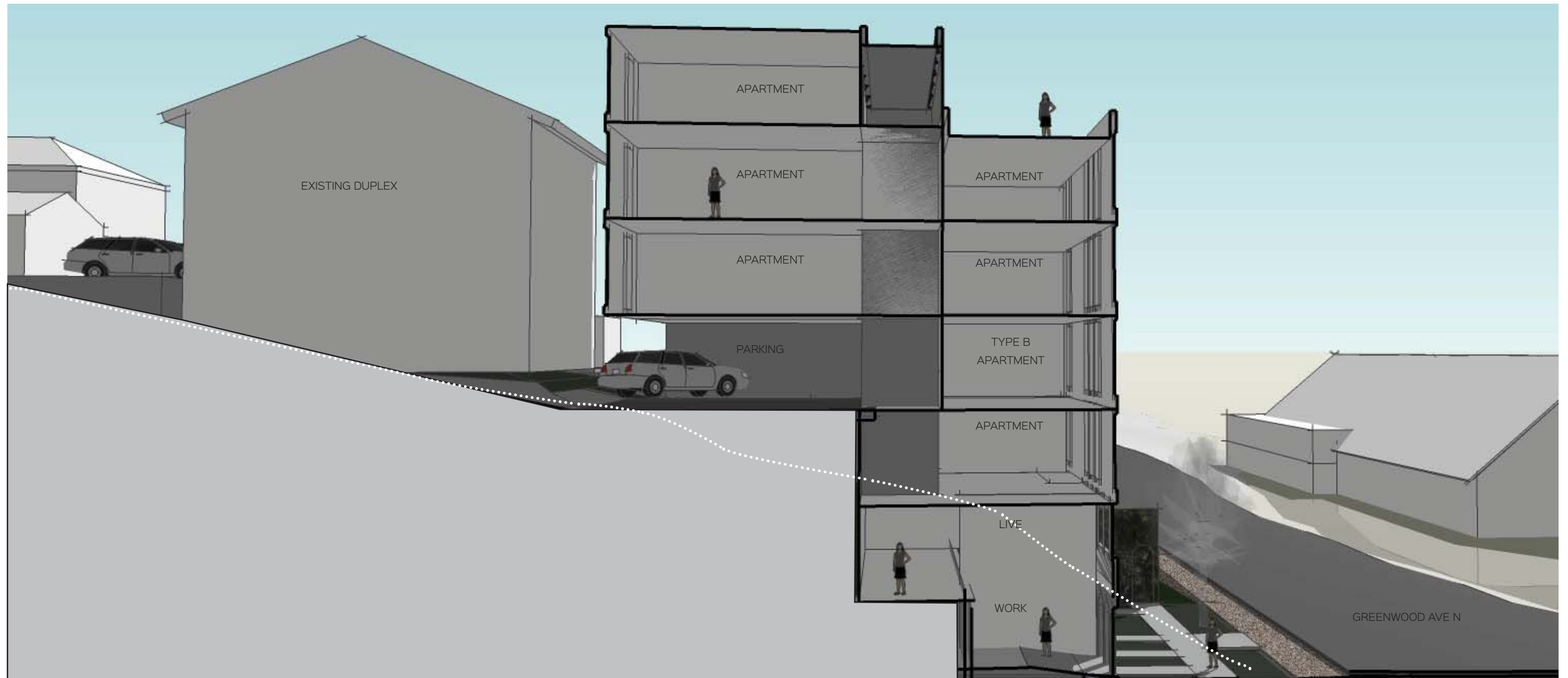
FLOOR PLAN: PARKING LEVEL



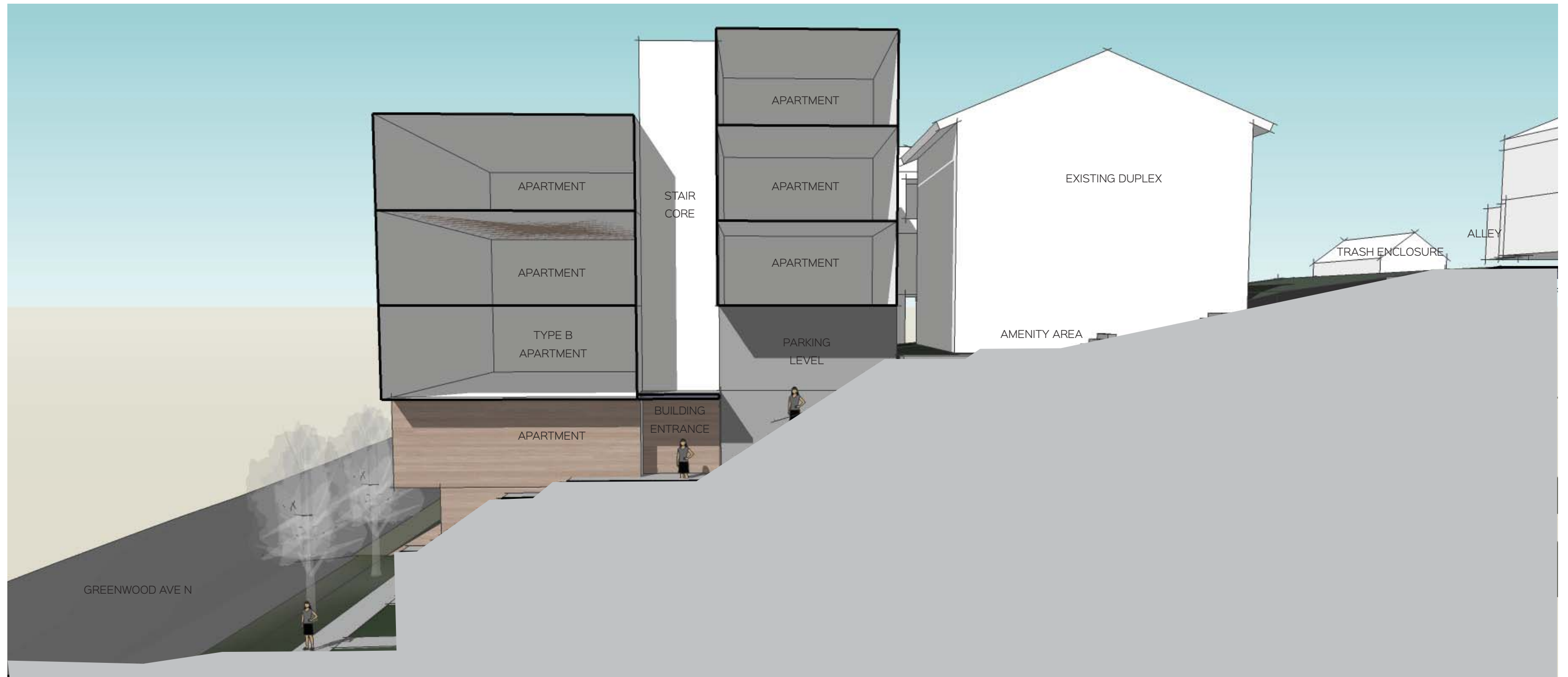
FLOOR PLAN: TYPICAL



SITE SECTION PERSPECTIVE: LOOKING SOUTH



SITE SECTION PERSPECTIVE: LOOKING NORTH



VEHICULAR ACCESS

LEGEND

- 1 - ALLEY
- 2 - SLOPED DRIVEWAY
- 3 - COVERED PARKING LEVEL
- 4 - GREENWOOD AVE
- 5 - SURFACE PARKING OFF OF ALLEY



VIEW CORRIDORS - GREENWOOD AVE. SOUTH VIEW



4' HEIGHT BONUS



RIGHT OF WAY TO BE EXCAVATED DOWN TO ELEVATION OF GREENWOOD AVE N, PER SDOT

OPTION A - CODE COMPLIANT / NO COMMERCIAL

PROGRAM:

16 apartments.
No Live/Work or Commercial Space

STREET LEVEL EXPERIENCE:

A Green Room provides a transition between greenwood and the apartment building.

Residential units are set 4' above street grade.

Rain protection is provided along the site stair through the use of building overhangs.

The site stair ascends gradually.

MASSING:

The building's massing is greatest at the top four stories and gradually steps down to street level. The white massing of the building aligns with the base for the existing condo building to the north.

The proposal also takes cues from the condo building to the North, by disengaging the building massing from the ground.





OPTION B - LIVE/WORK AT STREET LEVEL / OVERHANG

PROGRAM:

14 Apartments.
2 Live/Work Units

STREET LEVEL EXPERIENCE:

A Green Room provides a transition between greenwood and the apartment building. Tall retaining walls act as greenwalls to break down the massing of the building, and create a sense of entering a space rather than a dug out hole. These walls also reinforce the appearance of a building perched atop a hill rather than grounded into the site.

Rain protection is provided along the site stair through the use of building overhangs.

MASSING:

The building's massing is greatest at the top four stories and gradually steps down to street level. The white massing of the building aligns with the base for the existing condo building to the north.

The proposal also takes cues from the condo building to the North, by disengaging the building massing from the ground.

A 3' X 15' overhang projects into the right of way



GREENWOOD AVE N



OPTION C - LIVE/WORK AT STREET LEVEL (PREFERRED)

PROGRAM:

14 Apartments.
2 Live/Work Units

STREET LEVEL EXPERIENCE:

A Green Room provides a transition between greenwood and the apartment building. Tall retaining walls act as greenwalls to break down the massing of the building, and create a sense of entering a space rather than a dug out hole. These walls also reinforce the appearance of a building perched atop a hill rather than grounded into the site.

Rain protection is provided along the site stair through the use of building overhangs.

MASSING:

The building's massing is greatest at the top four stories and gradually steps down to street level. The white massing of the building aligns with the base for the existing condo building to the north.

The proposal also takes cues from the condo building to the North, by disengaging the building massing from the ground.

No overhang into the right of way.





OPTIONS COMPARISON

A. FULLY COMPLIANT / NO COMMERCIAL



PROGRAM:
16 apartments
No Live/Work or Commercial Space

STREET LEVEL EXPERIENCE:
Residential Units are set 4' above street grade.

MASSING:
10' gap between new apartment building and existing duplex.

AMENITY:
Common Amenity is located between new apartment building and existing duplex.

B. LIVE/WORK AT STREET LEVEL + OVERHANG



PROGRAM:
14 apartments
2 Live/Work Units at Street Level (departure needed)

STREET LEVEL EXPERIENCE:
Live/Work Units activate Greenwood at Street Level. Taller retaining walls create a sense of entering a space, rather than an excavated hole. These walls are overgrown to reintroduce green within the right of way and match neighboring properties.

MASSING:
8' gap between new apartment building and existing duplex.
3'x15' wide overhang projects into right of way.

AMENITY:
Common Amenity is located at South Sideyard (departure needed for width)

C. LIVE/WORK AT STREET LEVEL (PREFERRED)



PROGRAM:
14 apartments
2 Live/Work Units at Street Level (departure needed)

STREET LEVEL EXPERIENCE:
Live/Work Units activate Greenwood at Street Level. Taller retaining walls create a sense of entering a space, rather than an excavated hole. These walls are overgrown to reintroduce green within the right of way and match neighboring properties.

MASSING:
8' gap between new apartment building and existing duplex.

AMENITY:
Common Amenity is located at South Sideyard (departure needed for width)

10.0 DEPARTURES REQUESTED

CODE CITATION	CODE REQUIREMENT	OPTION A	OPTION B	OPTION C
#1 23.47A.008.B Nonresidential street-level requirements	3. Height and depth provisions for new structures or new additions to existing structures. Non-residential uses shall extend <u>an average depth of at least 30 feet and a minimum depth of 15 feet</u> from the street-level street-facing facade. If the combination of the requirements of Sections 23.47A.005 or 23.47A.008 and this depth requirement would result in a requirement that an area greater than 50 percent of the structure's footprint be dedicated to non-residential use, the Director may modify the street-facing facade or depth requirements, or both, so that no more than 50 percent of the structure's footprint is required to be non-residential. Non-residential uses at street level shall have a floor-to-floor height of at least 13 feet.	<u>Departure request</u> to allow a minimum depth of 12 feet from the street-level street-facing façade.	Compliant	<u>Departure request</u> to allow a minimum depth of 12 feet from the street-level street-facing façade.
#2 23.47A.024 Amenity Area	B Required amenity areas shall meet the following standards, as applicable: 1. All residents shall have access to at least one common or private amenity area; 2. Amenity areas shall not be enclosed; 3. Parking areas, vehicular access easements, and driveways do not count as amenity areas, except that a woonerf may provide a maximum of 50 percent of the amenity area if the design of the woonerf is approved through a design review process pursuant to Chapter 23.41 4. Common amenity areas shall have a minimum horizontal dimension of 10 feet, and no common amenity area shall be less than 250 square feet in size; [...]	<u>Departure request</u> to allow south side yard to count as a public amenity area, although the minimum horizontal dimension is 9' 9" instead of the required 10'. Private balcony for accessible unit and roof deck will also be provided.	Compliant Private balcony for accessible unit and large roof deck will also be provided.	<u>Departure request</u> to allow south side yard to count as a public amenity area, although the minimum horizontal dimension is 9' 9" instead of the required 10'. Private balcony for accessible unit and roof deck will also be provided.
#3 23.54.020.G Sight Triangles	For exit-only driveways and easements, and two way driveways and easements less than 22 feet wide, a sight triangle on both sides of the driveway or easement shall be provided, and shall be kept clear of any obstruction for a distance of 10 feet from the intersection of the driveway or easement with a driveway, easement, sidewalk or curb intersection if there is not sidewalk, as depicted in Exhibit E for 23.54.030.	<u>Departure requested</u> for mirrors to be used in lieu of a site triangle to provide visibility to cars and pedestrians accessing the alley. <u>Note:</u> we have received advanced approval of the driveway slope (see letter dated 3/19/2015). This departure is made outside of the typical design standards and is necessary for all design options.	<u>Departure requested</u> for mirrors to be used in lieu of a site triangle to provide visibility to cars and pedestrians accessing the alley.	<u>Departure requested</u> for mirrors to be used in lieu of a site triangle to provide visibility to cars and pedestrians accessing the alley.

DEPARTURE 1: STREET LEVEL FACING FACADE - DEPTH OF FLOORPLATE

Options B & C provide Live/Work Units at Greenwood. The design intent is to activate Greenwood at the street level through work spaces at the ground floor. The “Live” portion of the unit is located 1 story above the street and are accessed from stairs that are internal to the unit.

Minimum Live/Work Depth: A significant portion of the workspace adheres to the 15’ minimum depth requirement prescribed by code section.

DEPARTURE REQUEST

Average Depth for Commercial Spaces: The average depth for this commercial space does not meet the 30’ depth requirement prescribed by code section 23.47A.008.B.3. A 30’ average depth for commercial space fronting Greenwood would require significant excavation, tiebacks, and shoring because of the dramatic elevation changes over the length of the site, which is why the foundation walls step up periodically to respond to topography.

Adhering to a 30’ average commercial depth would be very cost prohibitive and unfortunately would result in removal of all Live/Work units from the project proposal.

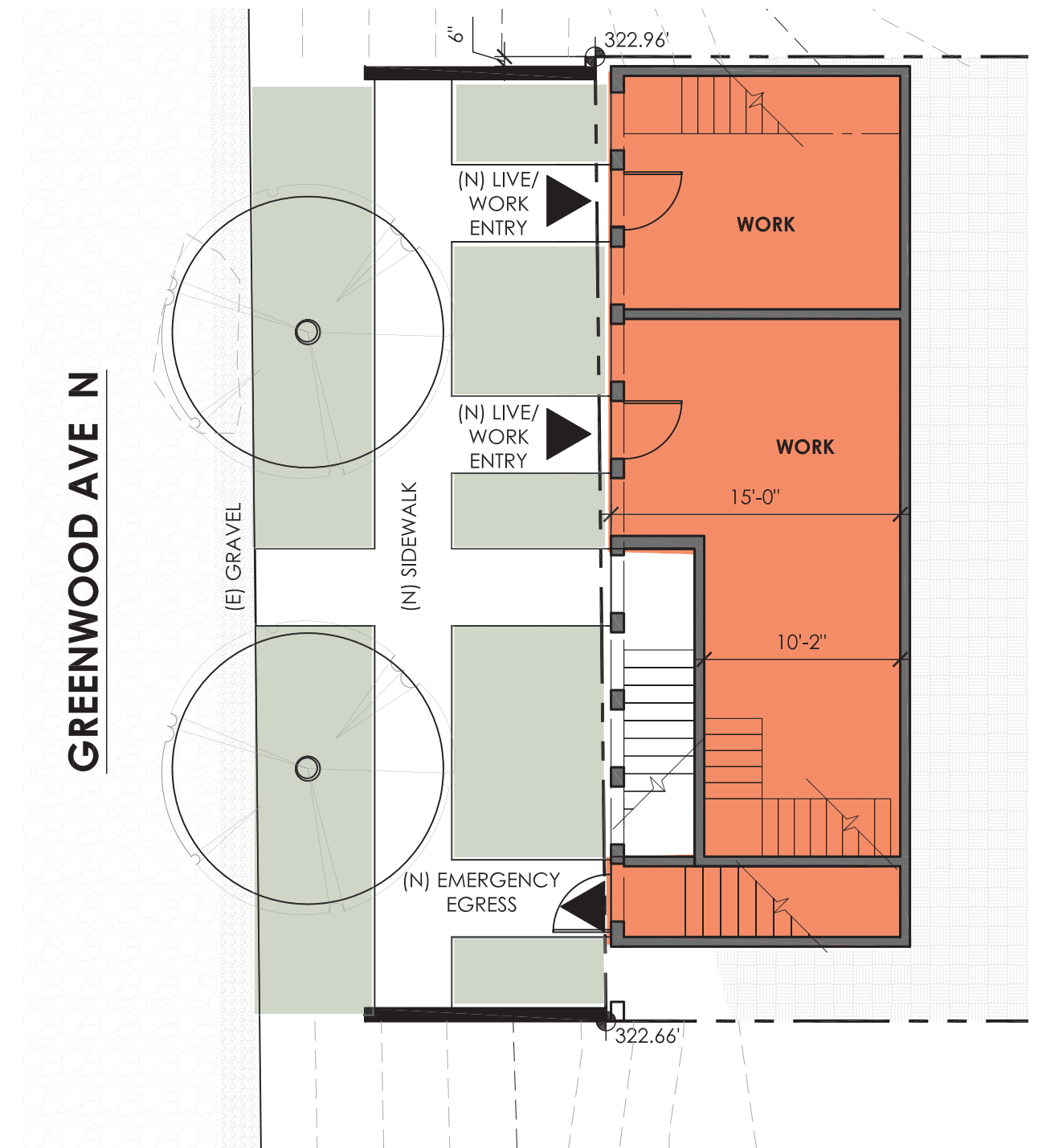
A 15’ deep space still provides functional workspace for a live/work unit and would be a welcome addition to pedestrians along Greenwood. The depth is not optimal (30’ average), but it is still beneficial to the urban experience and is preferable to an entirely residential building in a commercial zone.

Right of way excavation down to Greenwood elevation

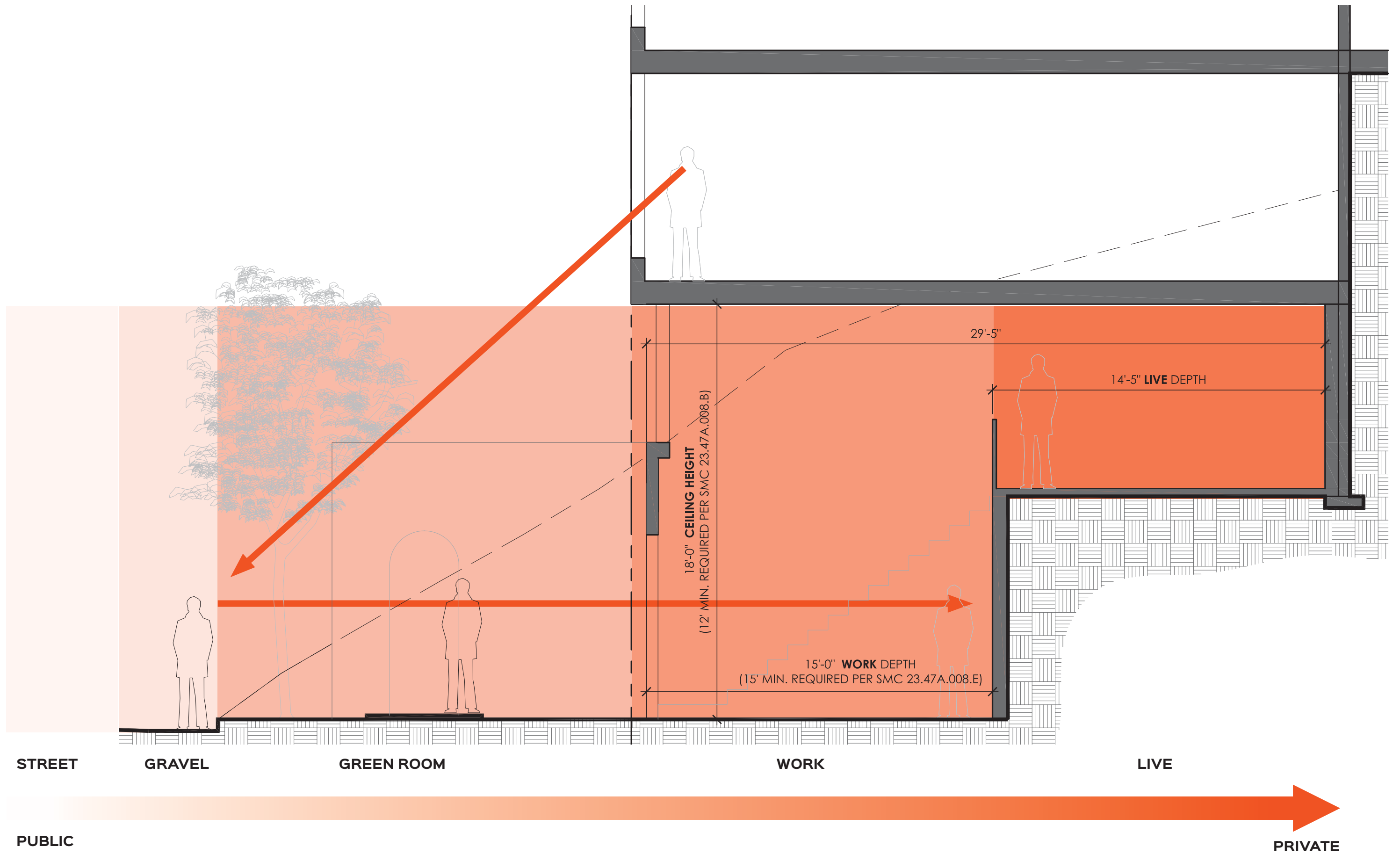
Revokable street use permit for retaining wall required per SDOT



GREEN ROOM STREET EXPERIENCE



STREET LEVEL COMMERCIAL



DEPARTURE 2: COMMON AMENITY AREA WIDTH & AREA

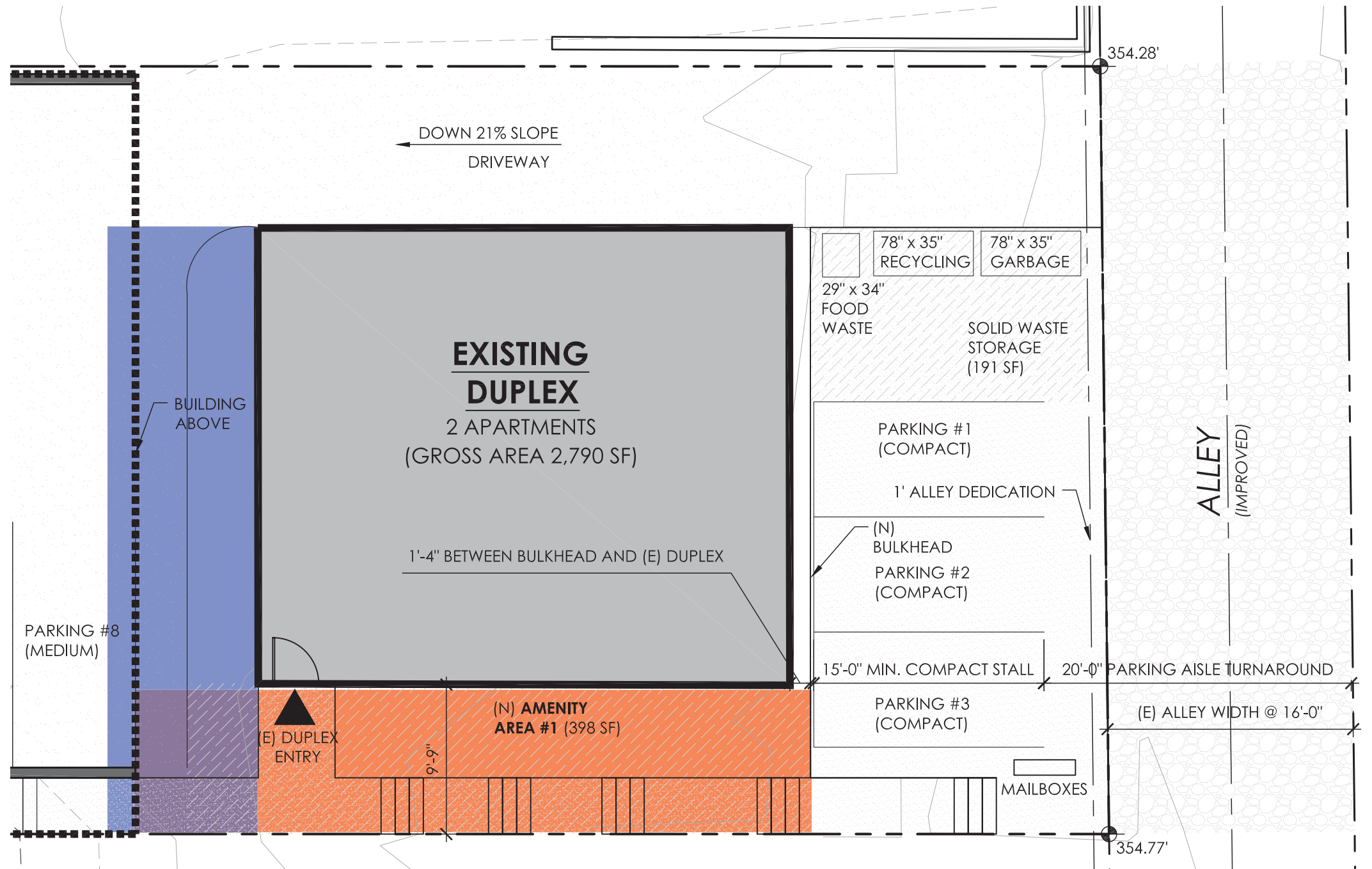
DEPARTURE REQUEST

The South Sideyard is the preferred location for a larger Common Amenity Area because it has the best access to direct sunlight.

Per 23.47A.024, Common Amenity areas are to be 10' in width minimum. Unfortunately the south sideyard is only 9'-9" in width (see orange area below), and isn't compliant.

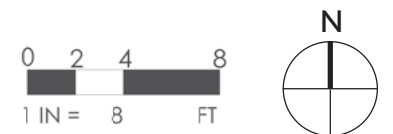
An alternate location is indicated with a blue box, and although this would provide the necessary width, it is between two buildings and has less desirable access to sunlight.

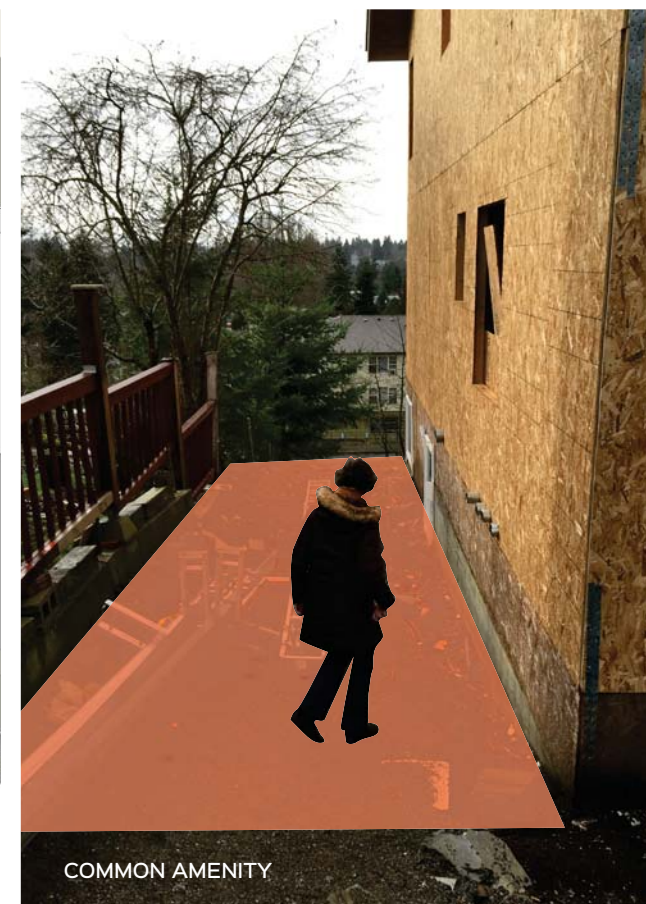
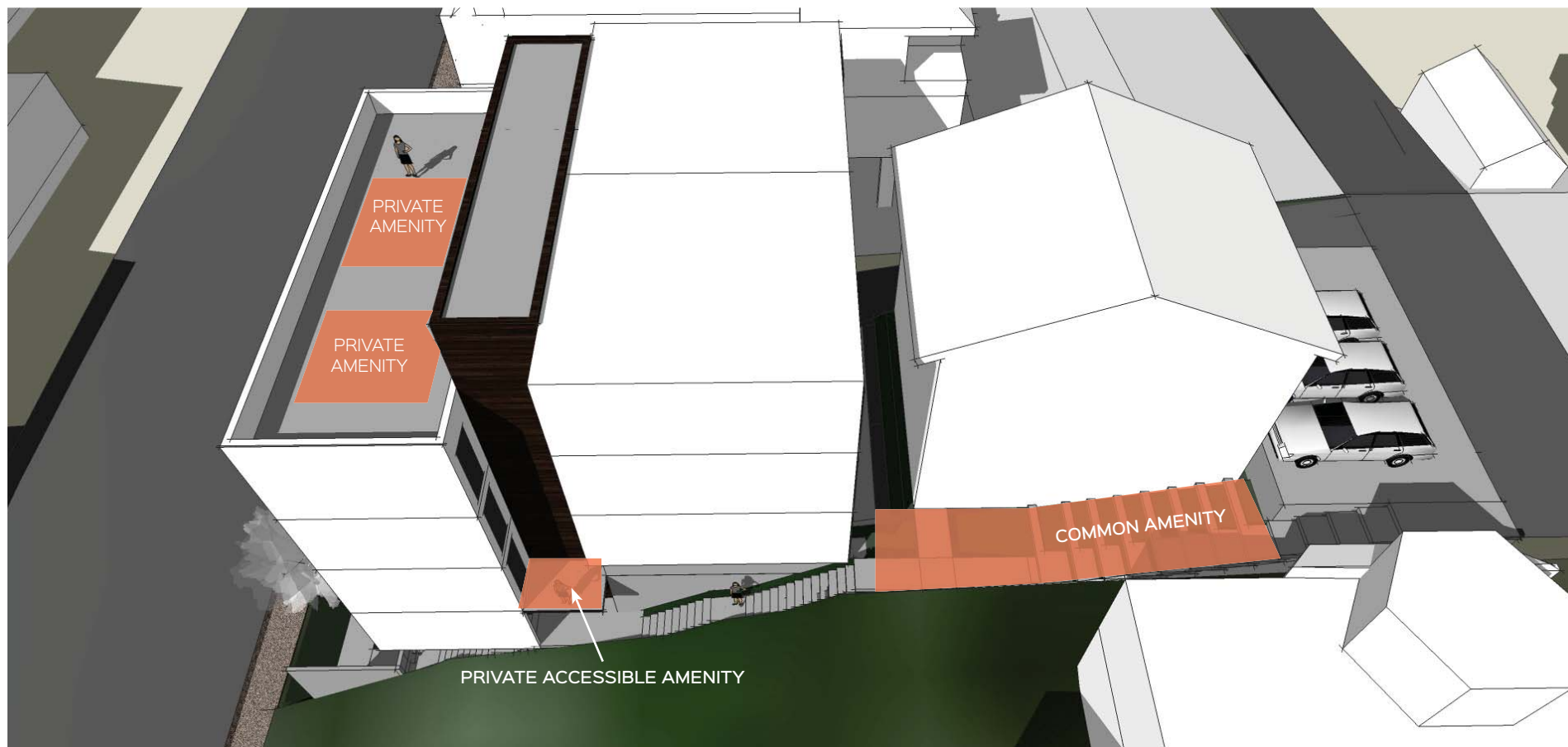
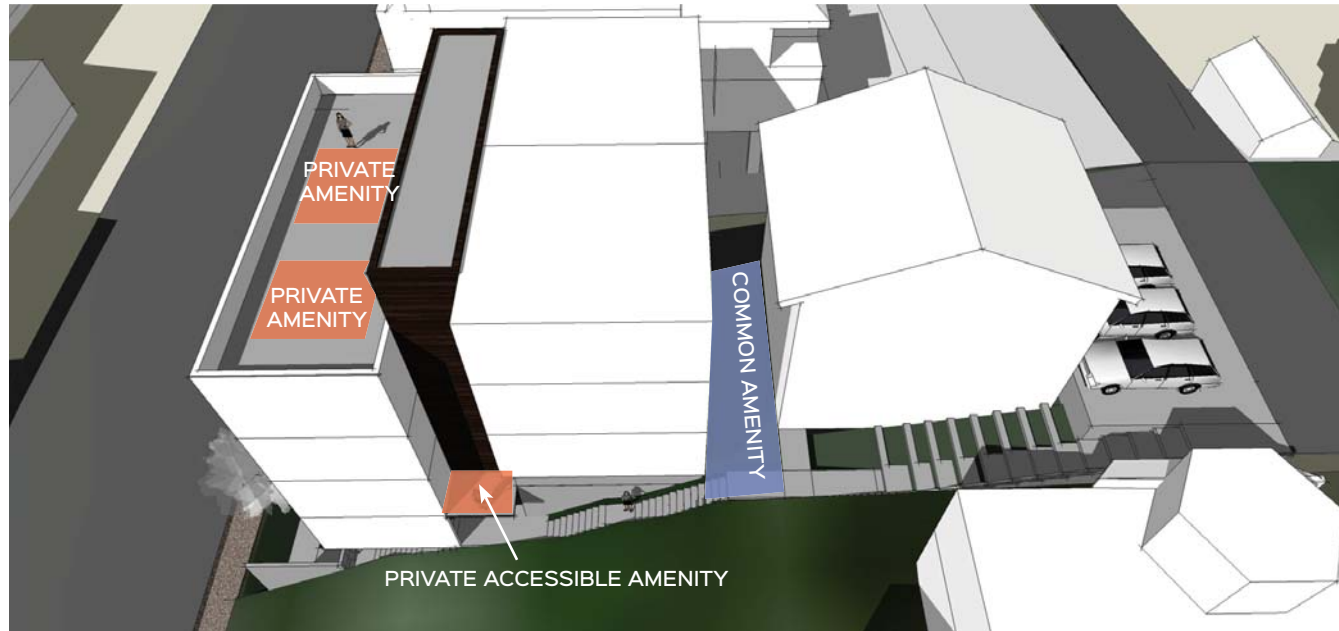
A departure is requested to allow for a 9'-9" wide common amenity area at the South Sideyard.



PREFERRED LAYOUT

LESS PREFERRED LAYOUT



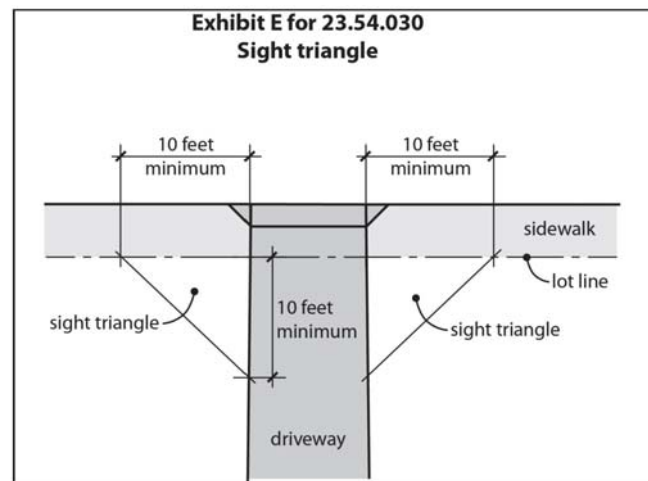


DEPARTURE 3: SIGHT TRIANGLE AT DRIVEWAY

DESIGN STANDARD

SMC 23.54.030.G Sight Triangles

For exit-only driveways and easements, and two way driveways and easements less than 22 feet wide, a sight triangle on both sides of the driveway or easement shall be provided, and shall be kept clear of any obstruction for a distance of 10 feet from the intersection of the driveway or easement with a driveway, easement, sidewalk or curb intersection if there is no sidewalk, as depicted in Exhibit E for 23.54.030.



DEPARTURE REQUEST

Currently, the guard rail along the north property line affords views of the alleyway because the gaurdrail is largely open. The property owners to the North could however install an opaque gaurd system in the future.

To address this potential change in visibility, mirrors and or warning lights/sounds could be employed to make drivers aware of approaching cars or pedestrians.

It is anticipated that pedestrian and car traffic will be minimal because this alleyway dead-ends and serves only two properties.

DPD MISC SERVICES REQUEST

Early in the design process the following request was applied for to determine project feasibility. A 21% driveway slope was approved. A consequence of the increased driveway slope is diminished sight lines. Therefore we are requesting a sight triangle



City of Seattle
Department of Planning & Development
Land Use Review

LEAH C MARTIN
 1429 12th Ave Suite C
 Seattle, WA 98122

Re: Project# 3018436

Early Determination Letter

Review Type	ZONING	Date	March 19, 2015
Project Address	9532 Greenwood Ave N	Contact Phone	(206) 383-7274
Contact Email	leah@allied8.com	Contact Fax	(206) 374-2379
DPD Reviewer	Naomi Henry	Address	Department of Planning & Development 700 5th Ave Suite 2000 PO Box 34019 Seattle, WA 98124-4019
Reviewer Phone	(206) 684-0711		
Reviewer Fax			
Reviewer Email	naomi.henry@seattle.gov		
Owner	SACHIN KUKREJA		

Dear Ms. Martin,

This letter is in response to your request for preliminary zoning analysis of driveway slope and width standards found in SMC 23.54.030.D.1 and 3.

Driveway Slope

SMC 23.54.030.D.3 allows a maximum driveway slope of 15%. A modification to allow 21% is being requested due to the site's topography and the location of the existing duplex at the rear of the lot. SMC 23.54.030.D.3.a-c allows a driveway slope to exceed 15% if it is found that:

- a. The topography or other special characteristic of the lot makes a 15 percent maximum driveway slope infeasible;
- b. The additional amount of slope permitted is the least amount necessary to accommodate the conditions of the lot; and
- c. The driveway is still useable as access to the lot.

Based on the information provided, the site's existing slope, and the location of the existing duplex and its windows, it is clear that the above criteria are met, and that a 21% slope is needed to allow access to the lot. DPD will support this request.

Driveway Width

Per SMC 23.54.030.D.1.a, driveways less than 100 feet in length that serve 30 or fewer parking spaces shall be a minimum of 10 feet in width for one-way or two-way traffic. Your request to allow a 9.7 foot driveway is not code compliant, and there is no modification in this section that would allow the driveway to be anything less than 10 feet in width. A variance will be required to have a driveway that is less than 10 feet wide. Please refer to SMC 23.40.020 and TIP 210 for information on the variance process.

Sincerely,

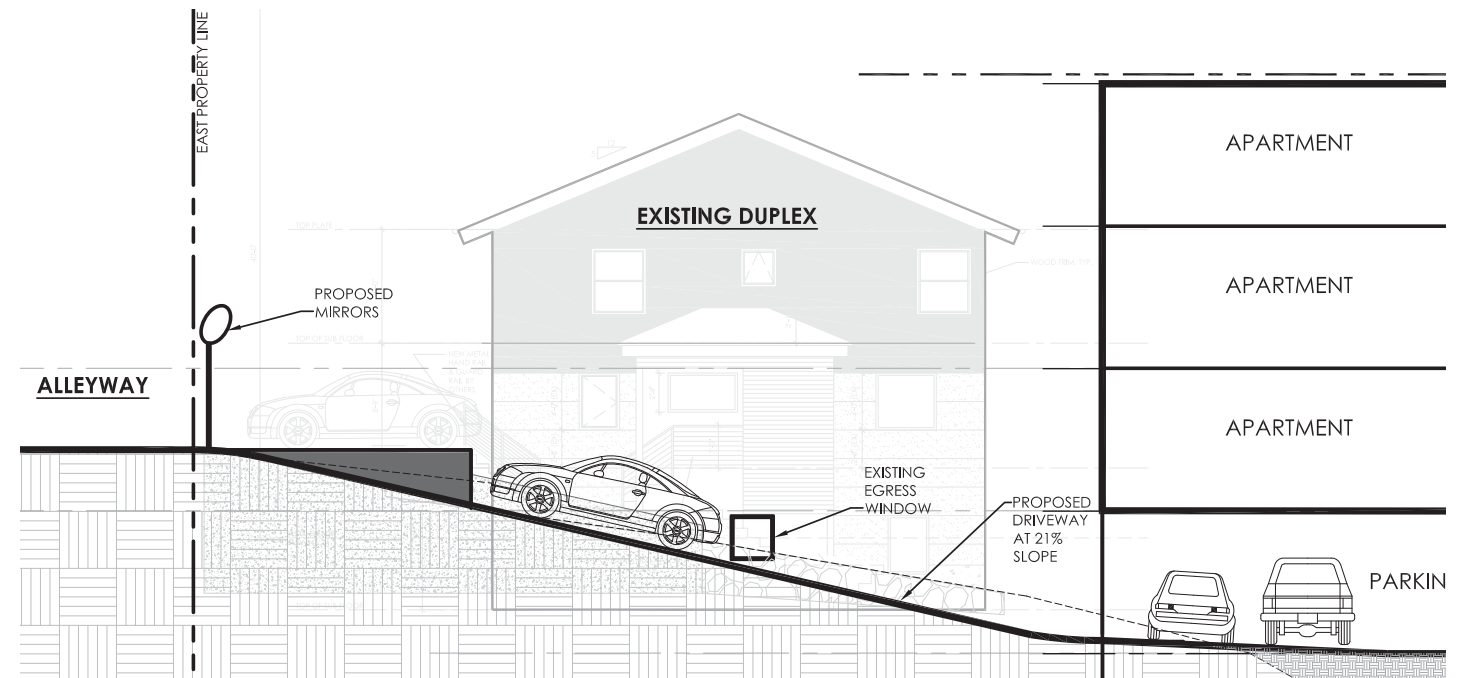
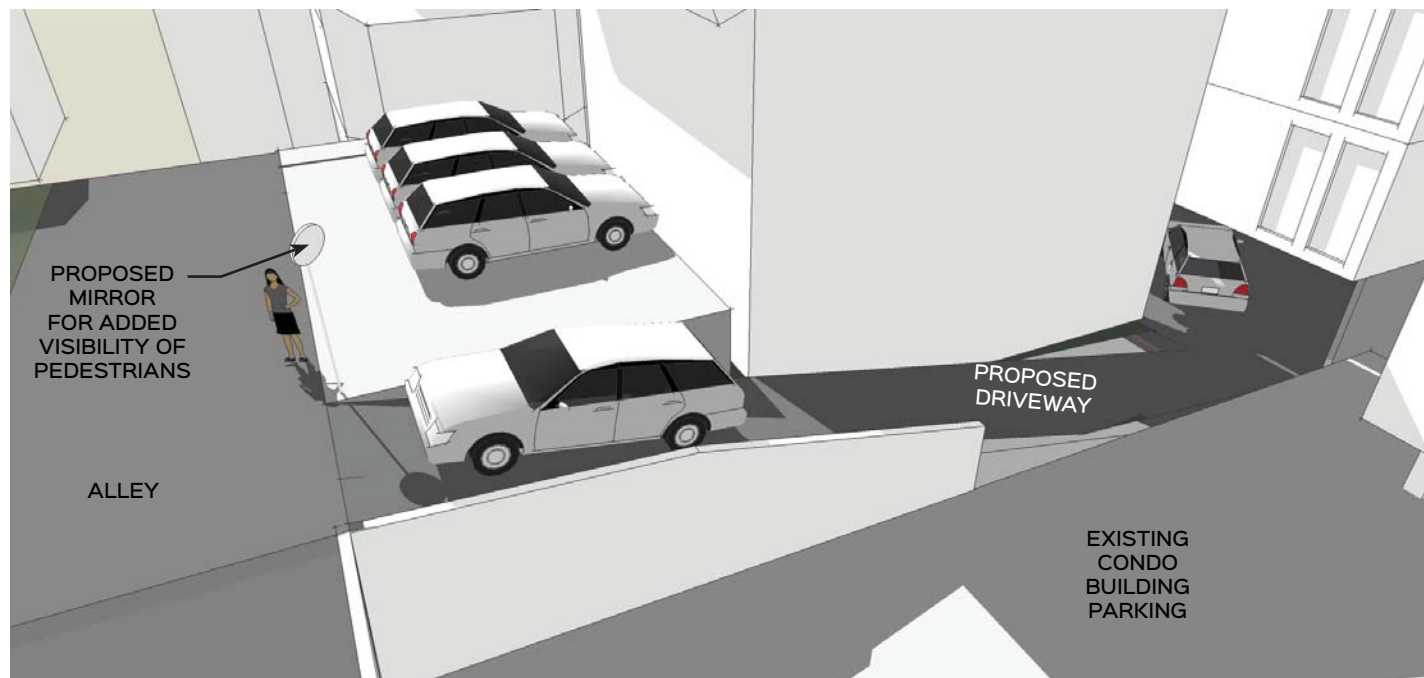
Naomi Henry, Senior Land Use Planner
 City of Seattle, DPD
 206-684-0711



VIEW TO ALLEYWAY FROM LOCATION OF PROPOSED DRIVEWAY



VIEW FROM ALLEYWAY LOOKING SOUTH. ALLEYWAY IS VISIBLE FROM DRIVEWAY BECAUSE OF EXISTING OPEN GUARDRAIL ON TOP OF CONDO RETAINING WALL.



SITE SECTION THROUGH DRIVEWAY - LOOKING SOUTH