

#3018378

5201 Rainier Ave S **EARLY DESIGN GUIDANCE**

5201 RAINIER LLC **S+HWorks**
ARCHITECTURE & DESIGN

ADDRESS

5201 RAINIER AVE S
DPD# 30183789

PROJECT TEAM

OWNER5201 Rainier LLC

ARCHITECTS+H Works, LLC

SURVEYOREmerald Land Surveying, Inc.

PROJECT INFO

ZONINGNC2-40 (NC3-65 PENDING)

LOT SIZE24,408 sf

FAR4.75

ALLOWABLE GFA115,938 sf

PROPOSED GFA109,548 sf

PROPOSED UNITS121

PROPOSED LIVE/WORK3

COMMERCIAL SQ.FT.1,888 sf

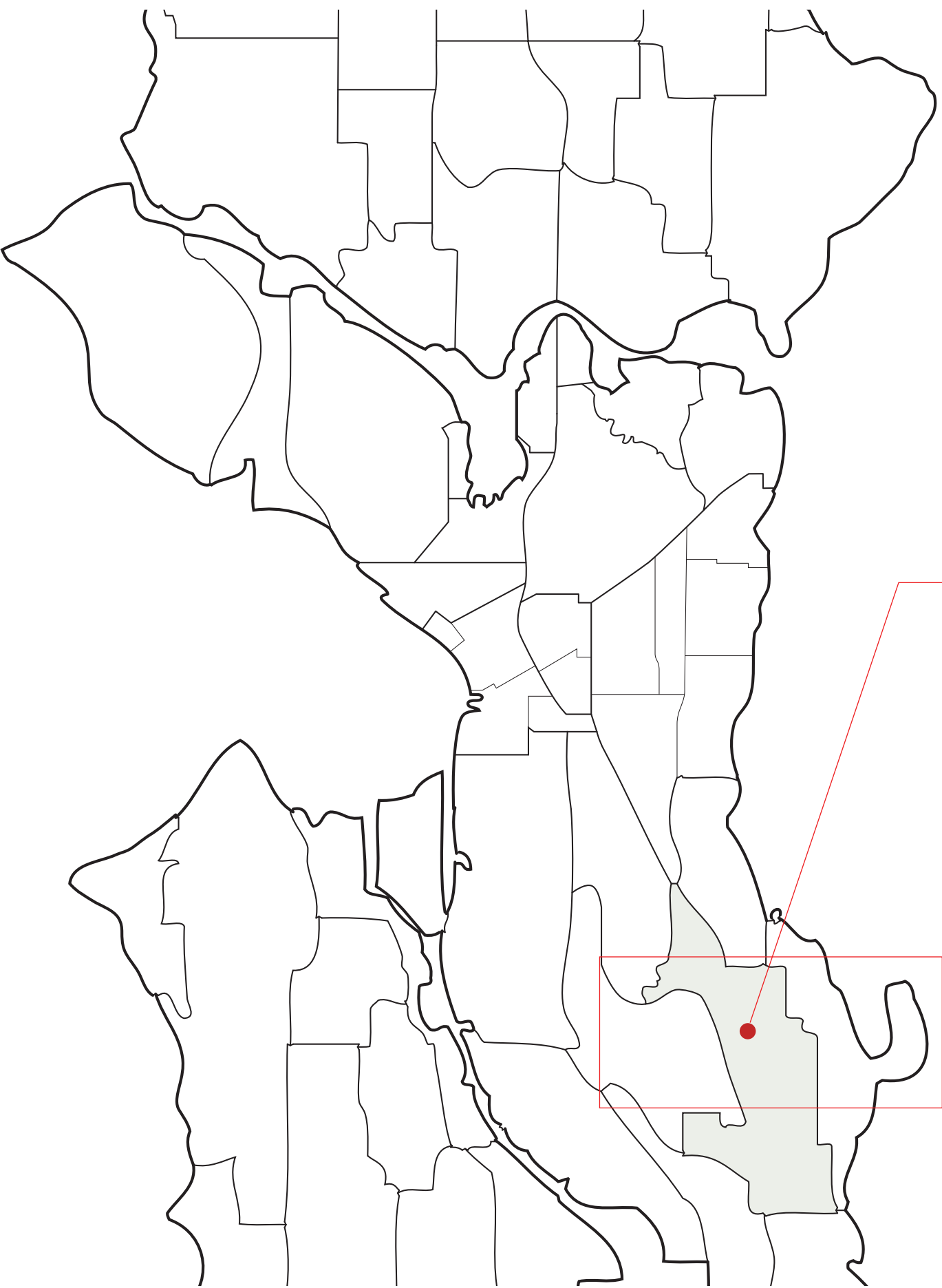
PARKING STALLS58

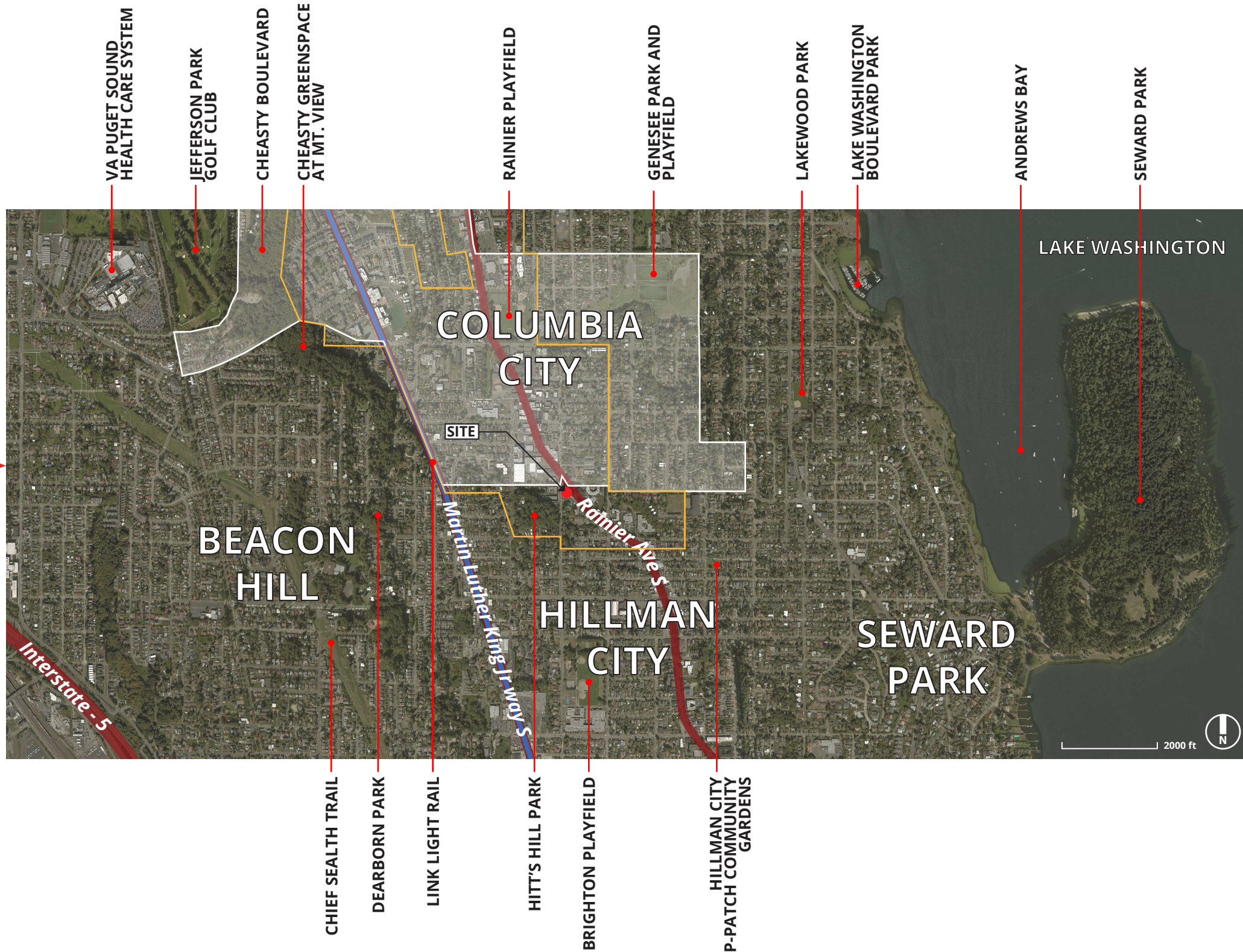
PROJECT DESCRIPTION

The proposed development is a mixed use apartment building containing 121 residential units and 1,588 sf of commercial space. 58 parking stalls will be provided in a below grade parking garage.

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COLUMBIA CITY

Columbia City is a growing and vibrant urban area within Seattle which offers a unique combination of parks and public amenities. Following the recent addition of the light rail train, there has been a significant amount of development within the area consisting primarily of multi-family townhouse and mixed use projects.

DEVELOPMENT GOALS

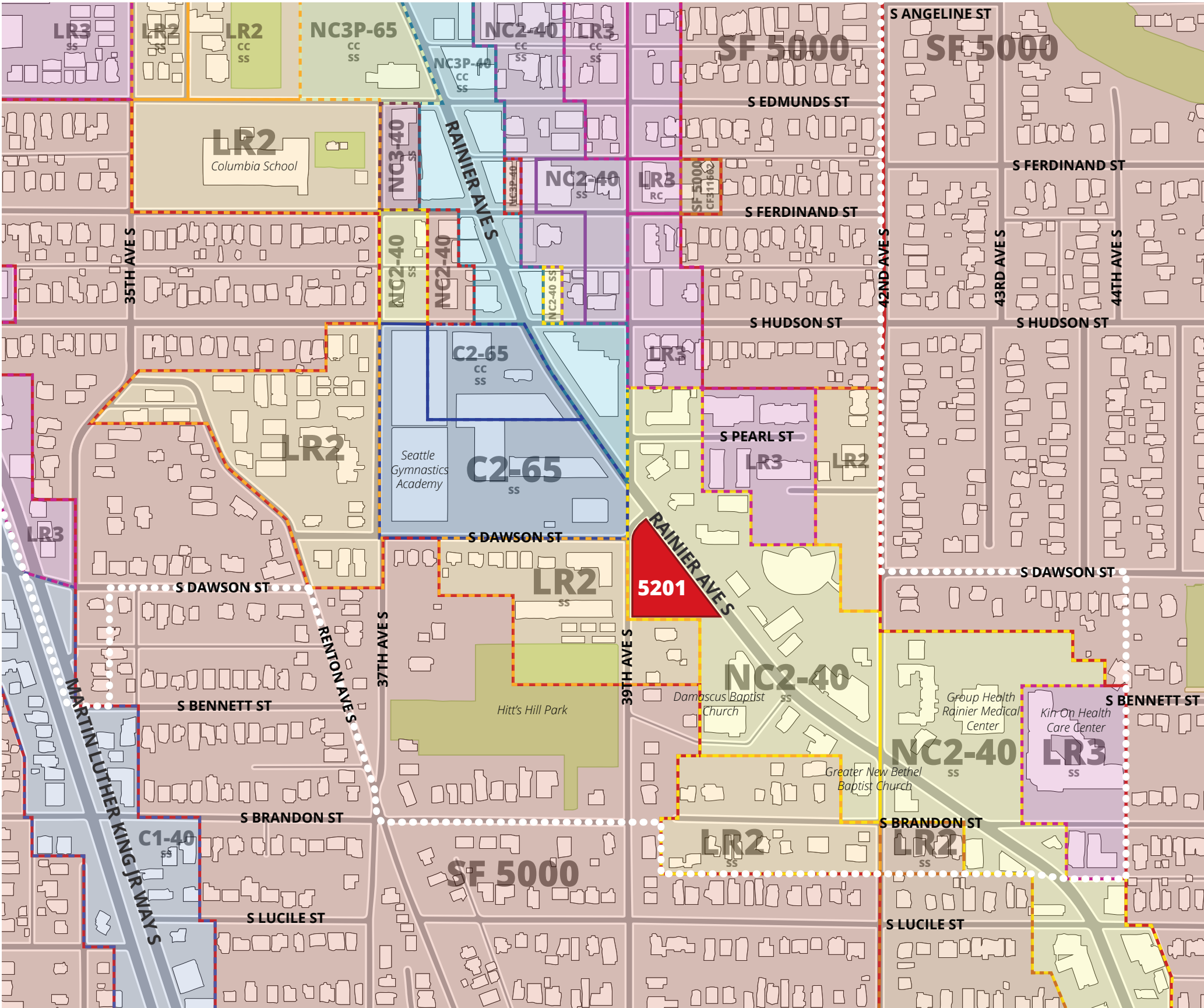
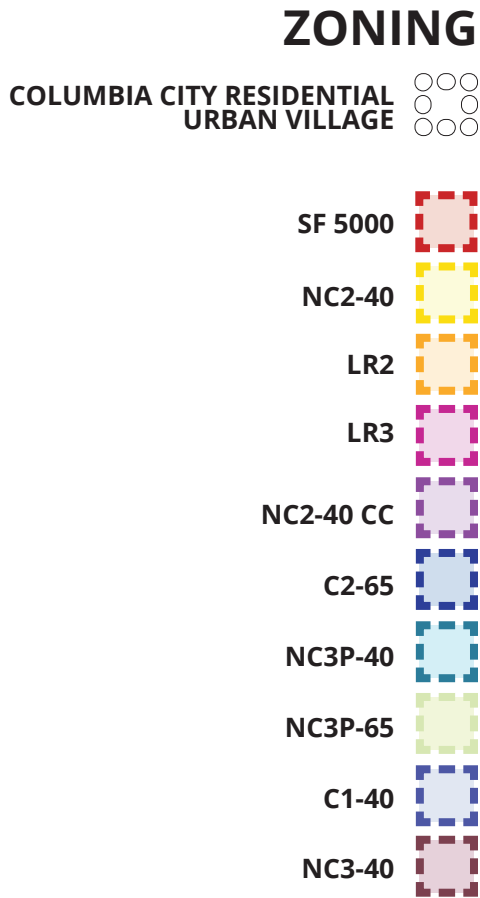
- Design a project that respects both the commercial and residential character of the surrounding area.
- Create an attractive project that is welcoming and pedestrian friendly.
- Respond appropriately to neighboring residential uses and create a transition between Rainier Ave and the surrounding residential streets.
- Create high quality living units to further serve the growing neighborhood of Columbia City.

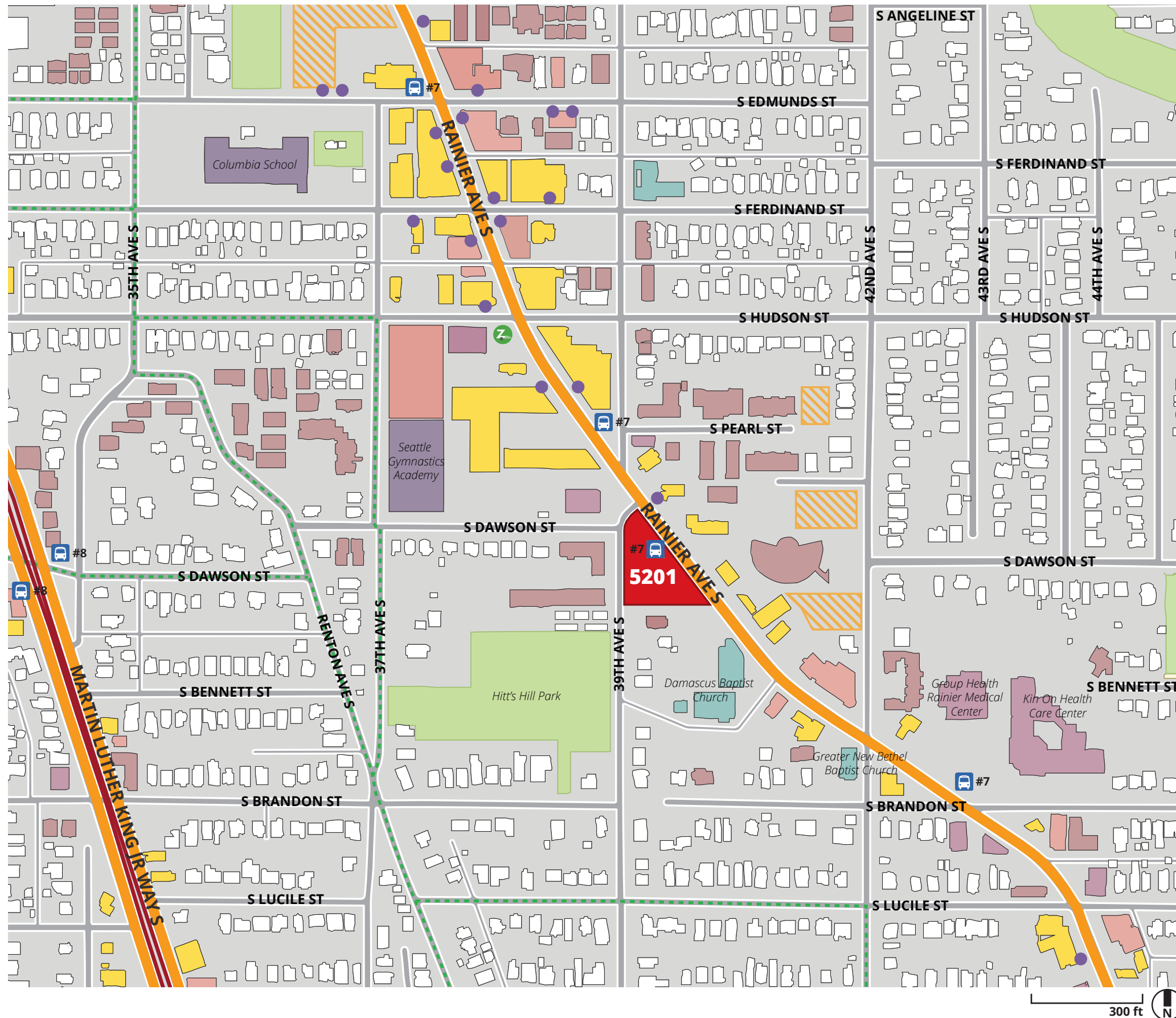
-  COLUMBIA CITY RESIDENTIAL URBAN VILLAGE
-  COLUMBIA CITY
-  SOUND TRANSIT LINK LIGHT RAIL
-  SITE

SITE ANALYSIS

The project site is zoned NC2-40 (NC3-65 pending) and is flanked by NC2-40 across Rainier Ave S, C2-65 directly to the northwest, and LR2 to the west across 39th Ave S. Rainier Ave S is a busy 4 lane arterial corridor with public transit and provides direct access to downtown Seattle. The project site is currently home to a used car lot and is surrounded by chain link fencing. There are several small structures on the lot that serve the car dealership.

A variety of small businesses are located near the site, with the primary commercial corridor of Columbia City a few blocks to the north. The area is characterized by a diverse range of businesses and housing, creating an engaging and unique urban environment with great potential.





CIRCULATION

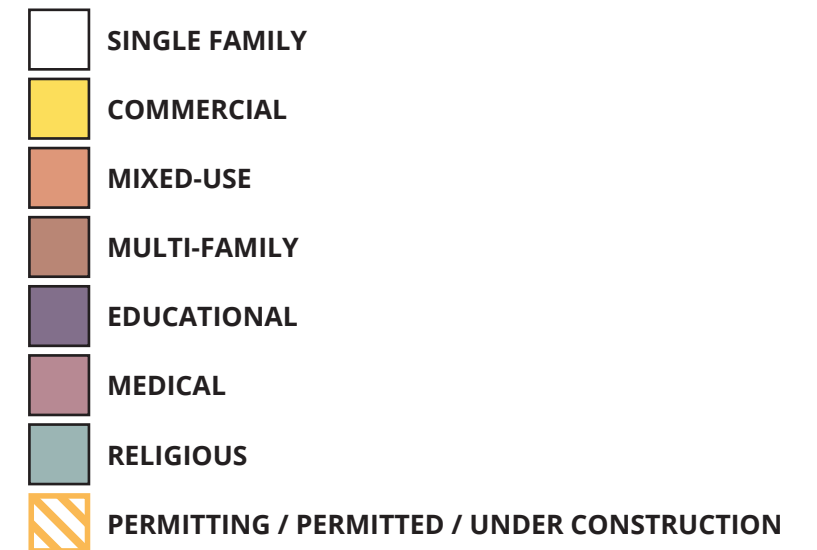


Rainier Ave South is a four lane arterial street that connects Columbia City to Central Seattle, and continues south to Renton. Route 7 of King County Metro runs along Rainier Ave at regular intervals, with a bus stop located directly in front of the project site. Bike friendly routes are located one block west of the site, and a Zipcar site is one block to the northwest. The light rail is located approximately half a mile to the west along Martin Luther King Jr Way.

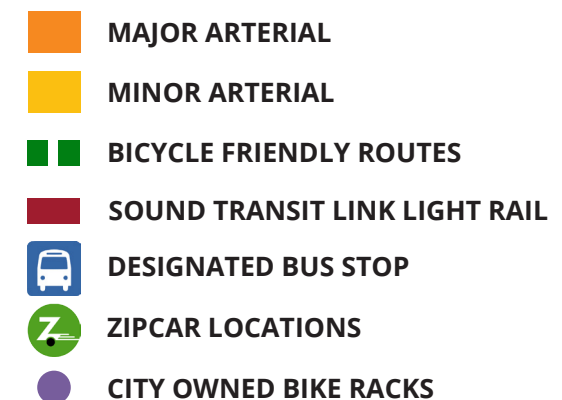
TPOLOGY / ADJACENCIES

The project site slopes upward gently toward the southwest, rising approximately 12 feet. The surrounding area slopes the same direction to form a small hill which crests at South Lucille Street. Small commercial and various storage buildings are located near the lot, and three story apartment buildings are located to the west.

BUILDING TYPES



CIRCULATION



DOWNTOWN COLUMBIA CITY



COLUMBIA PARK



FARMERS MARKET



COLUMBIA BRANCH LIBRARY



COLUMBIA SCHOOL



HITT'S HILL PARK



COLUMBIA CITY THEATER

Photo: columbiacitytheater.com



RETAIL + CULTURAL CORRIDOR



RECENT / PROPOSED DEVELOPMENTS



35TH AVE S / TOWNHOMES

Image: NK Architects



3829 S EDMUNDS ST / LIVE/WORK

RECENT / PROPOSED DEVELOPMENTS



C 3717 S ALASKA ST / MIXED-USE Image: Johnston Architects PLLC



D 4730 32ND AVE S / MIXED-USE Image: BAR Architects



E 3701 S HUDSON ST / MIXED-USE



F 5290 RAINIER AVE S / MIXED-USE / ONE BLOCK SOUTH



G RAINIER AVE S & S EDMUNDS ST / MIXED-USE



H 3903 S FERDINAND ST / MULTI-FAMILY



I 3820 S HUDSON ST / MULTI-FAMILY

1 39TH AVE S LOOKING EAST



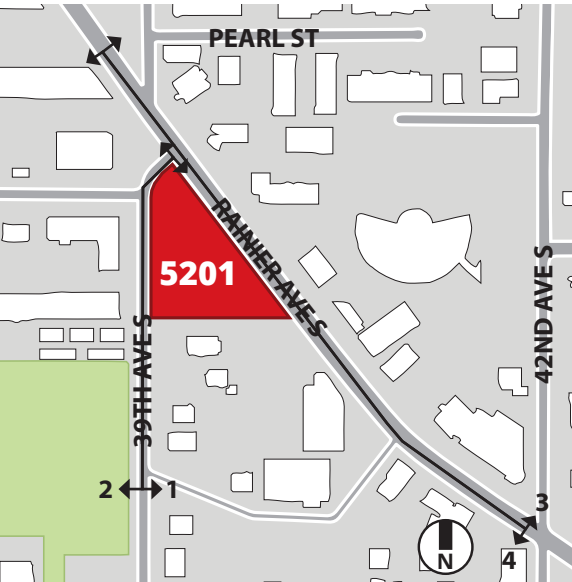
2 39TH AVE S LOOKING WEST



4 RAINIER AVE S LOOKING WEST



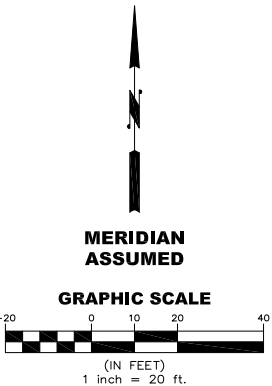
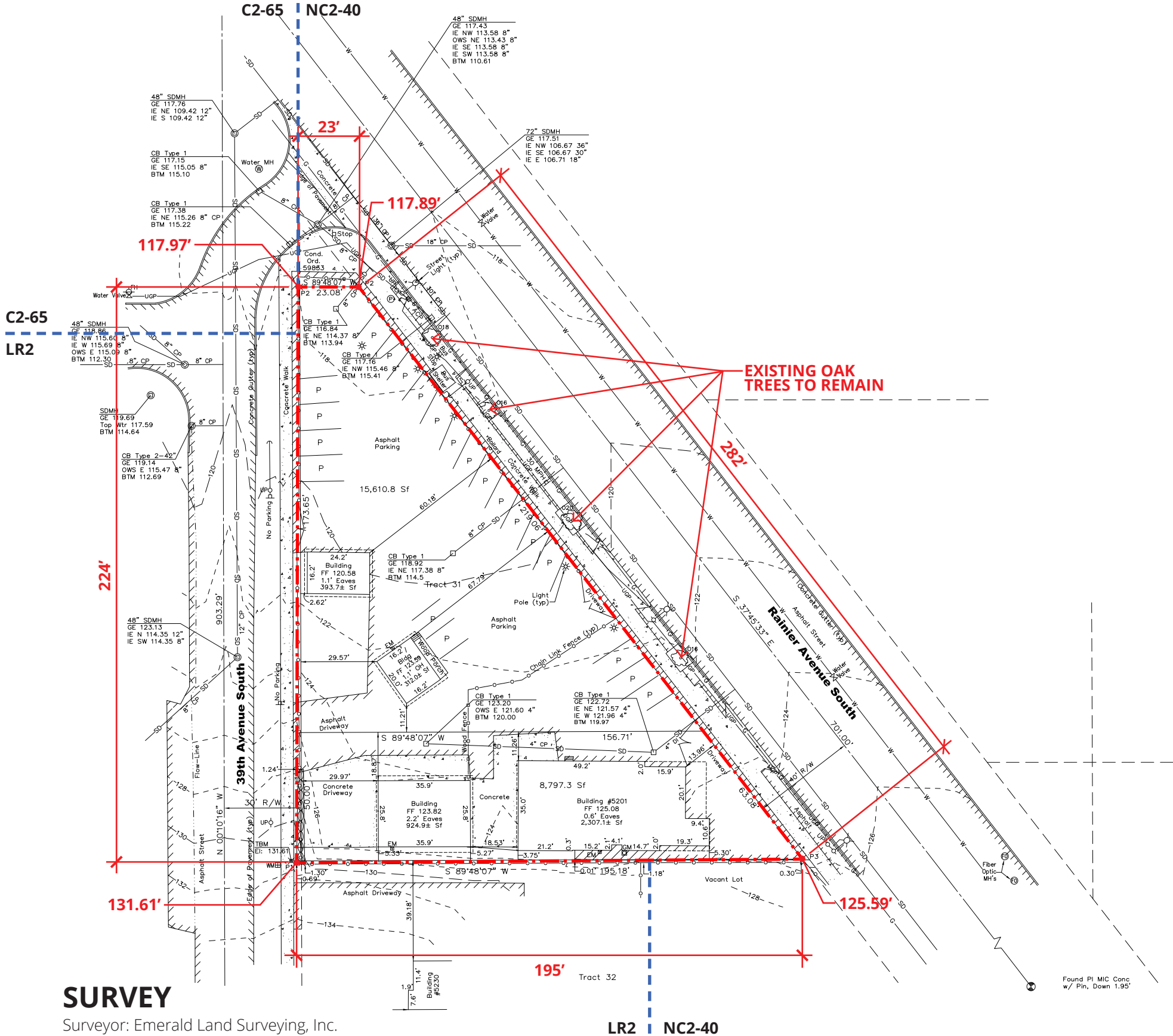
3 RAINIER AVE S LOOKING EAST



*NOTE:
Street Elevations are not to scale and vary
between 39th Ave S and Rainier Ave S

SURVEY

Surveyor: Emerald Land Surveying, Inc.
Date: 01/23/2007



LEGAL DESCRIPTION

THE NORTH 50 FEET OF TRACT 32, MORNINGSIDE ACRE TRACTS, ACCORDING TO THE PLAT THEREOF, RECORDED IN VOLUME 9 OF PLATS, PAGE 64 RECORDS OF KING COUNTY, WASHINGTON;

AND

TRACT 31, MORNINGSIDE ACRE TRACTS, ACCORDING TO THE PLAT THEREOF, RECORDED IN VOLUME 9 OF PLATS, PAGE 64 RECORDS OF KING COUNTY, WASHINGTON;

EXCEPT THAT PORTION CONVEYED TO THE CITY OF SEATTLE FOR ROAD UNDER CITY ORDINANCE NUMBER 59833.

TREE DESCRIPTIONS

O Oak (Quercus)

PROPERTY CORNERS

- P1 Set Tack in Lead w/ Tag, LS 30581, 4' Offset
P2 Set Mag & Tag, LS 30581
P3 Set Rebar & Cap, LS 30581

SURVEY NOTES

INSTRUMENT USED: SOKKIA SET 5 EDM
METHOD USED: FIELD TRAVERSE

APPROXIMATE POINT ACCURACY: $\pm 0.05'$

SURVEY MEETS OR EXCEEDS STATE STANDARDS PER WAC 332-130-090.

MONUMENTS SHOWN HEREON WERE VISITED ON OCTOBER 17, 2006.

THE INFORMATION SHOWN ON THIS MAP REPRESENTS THE RESULTS OF A SURVEY MADE ON THE INDICATED DATE AND CAN ONLY BE CONSIDERED AS THE GENERAL EXISTING CONDITION AT THAT TIME.

NO EASEMENTS, RESTRICTIONS OR RESERVATION OF RECORD WHICH WOULD BE DISCLOSED BY A TITLE REPORT ARE SHOWN.

DATUM - NAVD 88
CONTOUR INTERVAL - 2 FOOT

BENCH MARK: POINT NAME: SNV-2516 "Brass Cap 10' S & 0.5' W of NW cor bldg at the int bkcv in the SE cor int S. Hudson St & Rainier Ave S" Elev: 114.873.

SURVEY IN THE:
S.E. 1/4, N.W. 1/4 SEC. 22 TWP. 24N., RGE. 4E., W.M.

INTENTIONALLY BLANK

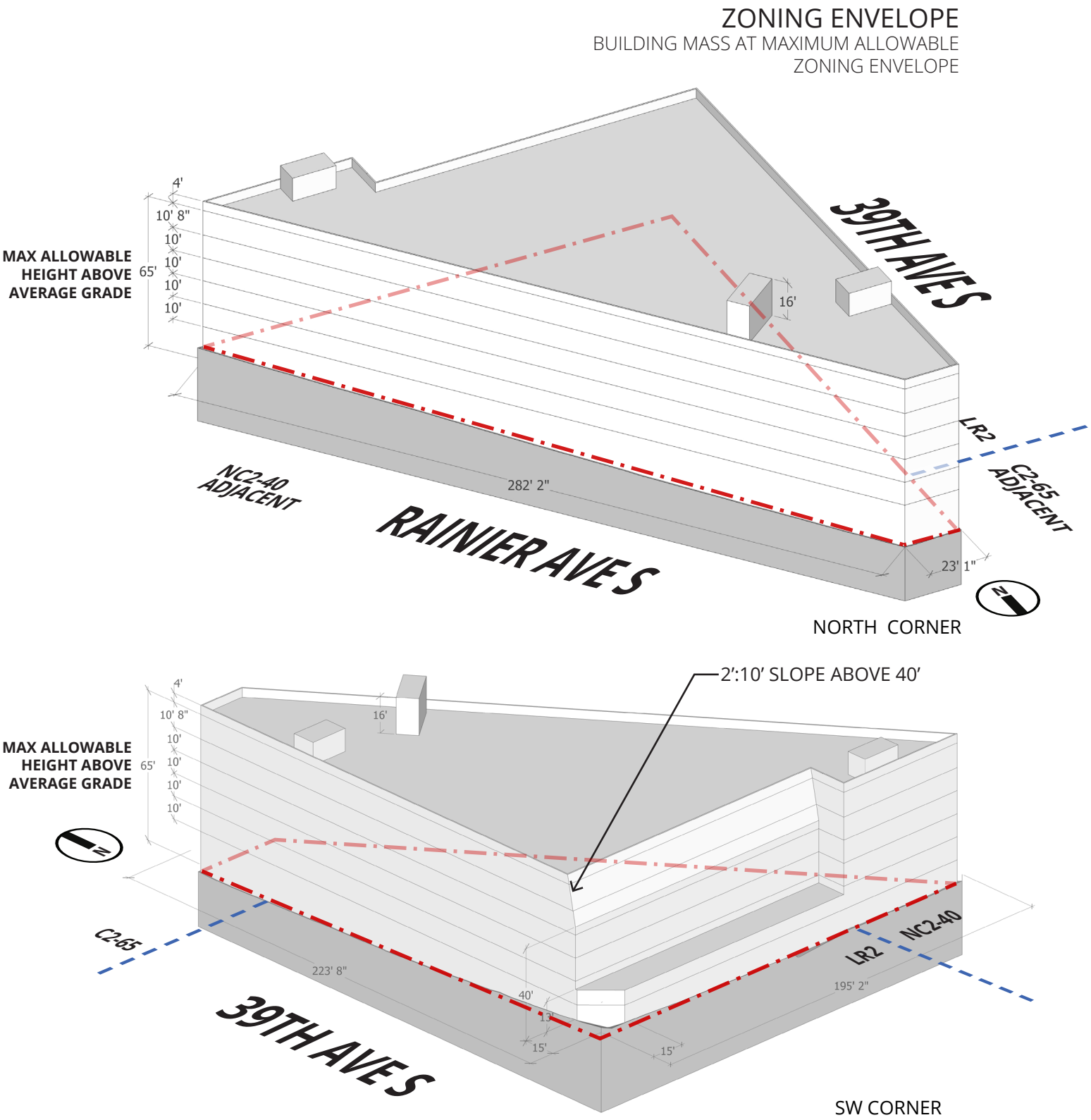


EXISTING SITE CONDITIONS

The project site is home to a used car lot and auto repair shop. The grounds are poorly maintained and the site is surrounded in chain link fencing. The site has ample light exposure. The topography gradually slopes up to the south, climbing approximately 7'-9" along Rainier Ave S and 13'-6" along 39th Ave S. The ROW along Rainier Ave is in very good shape with a new sidewalk and large healthy street trees along the majority of its length. There is a generous planting strip along 39th Ave S.



| ZONING: | NC2-40 (NC3-65 PENDING) | |
|----------------|---|--|
| OVERLAYS: | COLUMBIA CITY RESIDENTIAL URBAN VILLAGE / SOUTHEAST SEATTLE REINVESTMENT AREA AND RAINIER/GENESEE BUSINESS DISTRICT / FREQUENT TRANSIT | |
| SMC | Code | DEFINITION |
| SMC 23.47A.004 | | PERMITTED USES -Residential, live-work, and commercial permitted outright |
| SMC 23.47A.005 | | STREET-LEVEL USES -Residential uses are limited to 20% of the ground floor street-level facade when facing an arterial |
| SMC 23.47A.008 | | STREET-LEVEL DEVELOPMENT STANDARDS - Blank facades may not exceed 20' in width or 40% of the facade length within 2' and 8' above sidewalk grade. - 60% transparency required between 2' and 8' above sidewalk grade. - 15' minimum and 30' average commercial depth required. - Commercial at grade shall have a minimum 13' floor-to-floor height. - Street facing facades shall be within 10' of the lot line. |
| SMC 23.47A.012 | | HEIGHT - Base height limit of 65' above average grade (pending). - Parapets, railings etc. may extend 4' above the allowed height limit. - Stair and elevator penthouses may extend 16' above the allowed height limit. |
| SMC 23.47A.013 | | FLOOR AREA RATIO -4.75 FAR allowed for mixed-use development |
| SMC 23.47A.014 | | SETBACKS - 15'x15' triangular setback required where a structure with a residential use abuts the lot line of a lot in a residential zone. - 15' rear setback above 13' required where abutting a lot in a residential zone. - Additional setback of 2' horizontal for every 10' vertical above 40' is required . |
| SMC 23.47A.016 | | LANDSCAPING - 0.30 Green Factor Required |
| SMC 23.47A.024 | | AMENITY AREA - 5% of total floor area of residential use shall be provided as amenity area. - All residents shall have access to a common or private amenity area. - Common Amenity: minimum 250 sf, 10' minimum dimension. Private decks & balconies shall be 60 sf minimum, 6' minimum dimension. |
| SMC 23.54.015 | | REQUIRED PARKING - No vehicular parking required (Urban Village + Frequent Transit) - Bicycle Parking: - Residential, Long Term: 1 space per 4 units - Commercial, Long Term 1 per 12,000 SF, Short Term, 1 per 4,000 sf. - Residential driveways: >30 parking spaces = 20' for two-way traffic, slope 15% maximum. - Sight triangle: < 22' wide = 10' triangle required from sidewalk on both sides of driveway. |
| SMC 23.54.040 | | SOLID WASTE & RECYCLING - Residential: more than 100 dwelling units: 575 sf + 4 sf per each additional unit above 100 (21x4) = 84 (+575) = 659 sf required - Non-residential: 5,001 - 15,000 GFA = 125 sf x 50% = 62.5 sf - 12' minimum horizontal dimension |



PRIORITY GUIDELINES CITY WIDE GUIDELINES

CS1: NATURAL SYSTEMS AND SITE FEATURES

B. SUNLIGHT AND NATURAL VENTILATION

2. Daylight and shading: The preferred scheme transitions scale from more urban multi-story apartments along Rainier Ave S, to townhouses at the southwest of the site on 39th Ave S. This allows for daylight and ventilation to reach the residential units more effectively.

C. TOPOGRAPHY

The project utilizes the existing sloped site topography by burying parking and service uses into the site. On the preferred scheme, the roof of the parking garage acts as an outdoor amenity area between the apartment building and townhouses. The townhouses are orientated towards the upslope part of the site, helping the project transition scale from Rainier Ave S to the surrounding residential streets.

CS2: URBAN PATTERN AND FORM

A. LOCATION IN THE CITY AND NEIGHBORHOOD

The proposed structure has a high degree of visibility along Rainier Ave S and is located along a portion that is under-developed. The project will help establish a standard in this area of Columbia City for design and open space.

B. ADJACENT SITES, STREETS, AND OPEN SPACES

The main façade along Rainier Ave S is the most visible and will have a high degree of transparency both at the street and residential levels. On the preferred scheme, the bulk of the massing will be pushed up against Rainier Ave S and will feature an open space area between the main structure and scaled-down townhouses at the southwest portion of the site towards 39th Ave S.

C. RELATIONSHIP TO THE BLOCK

1. Corner sites: The wedge shaped project site allows the design to feature a prominent corner at the northwest portion of the parcel facing Rainier Ave S. The proposed massing provides a strong urban edge for the block.

D. HEIGHT, BULK, AND SCALE

The preferred scheme transitions in scale from the NC zone along Rainier Ave S to the LR2 zone along 39th Ave S by pushing the bulk of the massing along Rainier and providing smaller scale townhouses along 39th Ave S.

CS3: ARCHITECTURAL CONTEXT AND CHARACTER

A. EMPHASIZING POSITIVE NEIGHBORHOOD ATTRIBUTES

4. Evolving neighborhoods: The surrounding area along Rainier Ave S is below its development potential and is primarily composed of single story commercial and light industrial buildings. This project will help set a precedent for future pedestrian and transit oriented development.

B. LOCAL HISTORY AND CULTURE

1. Placemaking: Columbia City is a growing and vibrant urban area within Seattle. Rainier Ave S is undergoing significant development, growing out of its core historical district. The project will have a retail and residential character that compliments the area's historical precedent.

PL1: ARCHITECTURAL CONTEXT AND CHARACTER

A. NETWORK OF OPEN SPACES

2. Adding to public life: Along Rainier Ave S, a variety of massing articulation, signage, and transparent elements animate the street and building façade. An existing bus stop and street trees further scale the site to the pedestrian environment.

B. WALKWAYS AND CONNECTIONS

3. Pedestrian amenities: Landscaping, seating, and other amenities will activate the space and promote interaction between tenants, as well as a sense of community.

C. OUTDOOR USES AND ACTIVITIES

1. Selecting activity areas: The plaza on the preferred scheme is located on the south end of the parcel and has sightlines through the property and adjacent sites. A built in bus shelter helps the building integrate with the street, along with a landscaped commercial plaza located at the corner.

PL2: ARCHITECTURAL CONTEXT AND CHARACTER

A. ACCESSIBILITY

1. Access for all: The commercial spaces and building entrance will be located on Rainier Ave S at grade to provide easy access to the project.

B. SAFETY AND SECURITY

On Rainier Ave S, the building façade will be highly transparent, allowing for a high degree of street surveillance by residents and visitors. The retail/commercial space will activate the corner of Rainier Ave S and 39th Ave S with pedestrian activity, and the live-work units will allow for more eyes on the street and increased public safety at all hours. The plaza toward the south end of the site will be secured for protection, yet have an open and inviting quality rather than an opaque barrier to the street.

C. WEATHER PROTECTION

1. Locations and coverage: Overhead canopies will be provided at the commercial spaces and individual entries for the live-work spaces.

PL3: STREET LEVEL INTERACTION

A. ENTRIES

1&2. Design objectives & ensemble of elements: The main building lobby and commercial space are located on Rainier Ave S and are visually prominent at the corner of 39th Ave. The commercial space will be the most prominent at the intersection, with the main lobby located just to the east on Rainier Ave S.

B. RESIDENTIAL EDGES

On Rainier Ave S, the main entry lobby will front the street between the live-work units and the commercial space. The live-work units themselves will be terraced to follow the grade on Rainier Ave, yet will be recessed and vertically modulated to create a buffer and to provide a sense of protection to the units. On the west and south sides of the project, entry staircases are secured, yet transparent edges help transition the site from the busy Rainier Ave S corridor to the residential scale of the neighborhood.

C. RETAIL EDGES

The preferred scheme has regularly spaced entries that establish a rhythm of transparency and massing to the facade. The transparency creates a strong visual connection between the sidewalk and commercial activity.

PL4: ACTIVE TRANSPORTATION

A.2 ENTRY LOCATIONS AND RELATIONSHIPS – CONNECTIONS TO ALL MODES

The preferred scheme's lobby is located within eyesight of a transit stop. The entry will be designed to not interfere with the queuing or waiting areas for riders. The project will provide a mix of indoor bike storage for tenants and exterior bike parking for guests.

DC1: PROJECT USES AND ACTIVITIES

A. ARRANGEMENT OF INTERIOR USES

1. Visibility: The proposed scheme has commercial, a residential lobby/entry, and live-work uses located in a visually prominent way along Rainier Ave S.

B. VEHICULAR ACCESS AND CIRCULATION

1. Access location and design: Access to the partially below grade parking garage will be off 39th Ave S and will be located behind the commercial space and additional circulation areas. The driveway will be minimized in width and visual prominence to the greatest extent possible.

C.1&2 PARKING AND SERVICE USES

1&2. Below-grade parking & visual impacts: The proposed structure will have partially below grade parking that takes advantage of the site topography to minimize the visual impact of the garage.

DC2: ARCHITECTURAL CONCEPT

A. MASSING

The preferred scheme uses the site's special characteristics, such as the wedge shaped lot, the slope up from Rainier Ave S, and the visual prominence along the street to inform the massing. The residential units occupying the upper levels vary in size and shape to create modulation on the building façade along Rainier Ave S. The ground floor commercial and overhead canopies create a transition between the lower and upper portions of the building, similar to some of the historic buildings located nearby.

B. ARCHITECTURAL AND FAÇADE COMPOSITION

1. Façade composition: The proposed massing is modulated to create a rhythm along Rainier Ave S and to break up the façade into smaller elements.

C. SECONDARY ARCHITECTURAL FEATURES

1. Visual depth and interest: Recesses in the building façade add visual depth and create shadowed relief.

DC3: OPEN SPACE CONCEPT

A BUILDING-OPEN SPACE RELATIONSHIP

1. Interior/exterior fit: The preferred scheme has the main massing configured along Rainier Ave S, opening up the south and west portion of the lot for open space and smaller scale townhouses. This allows the more residential nature of 39th Ave S to be preserved while also adding activity and density to Rainier Ave S.

C. DESIGN

2. Amenities and features: The preferred scheme's plaza will have a variety of attractive paving, screening, landscape planters, and seating elements which will activate the space and create a pleasant environment.

DC4: EXTERIOR ELEMENTS AND FINISHES

A. BUILDING MATERIALS

The project will utilize a simple material palette. The materials will be used to differentiate the massing and reduce the perceived mass of the building.

B. SIGNAGE & C. LIGHTING

Signage and lighting will be designed to be integrated into the street-front experience. Signage will be planned for at the commercial entries. Landscape lighting will be provided at the plaza area.

D. TREES, LANDSCAPE AND HARDSCAPE MATERIALS

Landscape will be chosen using a modern palette that compliments the building and provides year round interest.

CONCEPT A: L-SHAPE

117,063 GSF
115 UNITS
7 LIVE/WORK UNITS, 700-1530 SF EACH
2,057 COMMERCIAL SF
25 PARKING STALLS PROVIDED
4,417 SF- TOTAL COMMON AMENITY
2,185 SF - COURTYARD
2,479 SF - ROOF DECK
4.75 FAR - 115,938 GFA

POSITIVE

- BREAKS UP FACADE MASSING ALONG RAINIER AVE S
- RESIDENTIAL COURTYARD FACES RAINIER AVE S
- MAXIMIZES BUILDABLE AREA

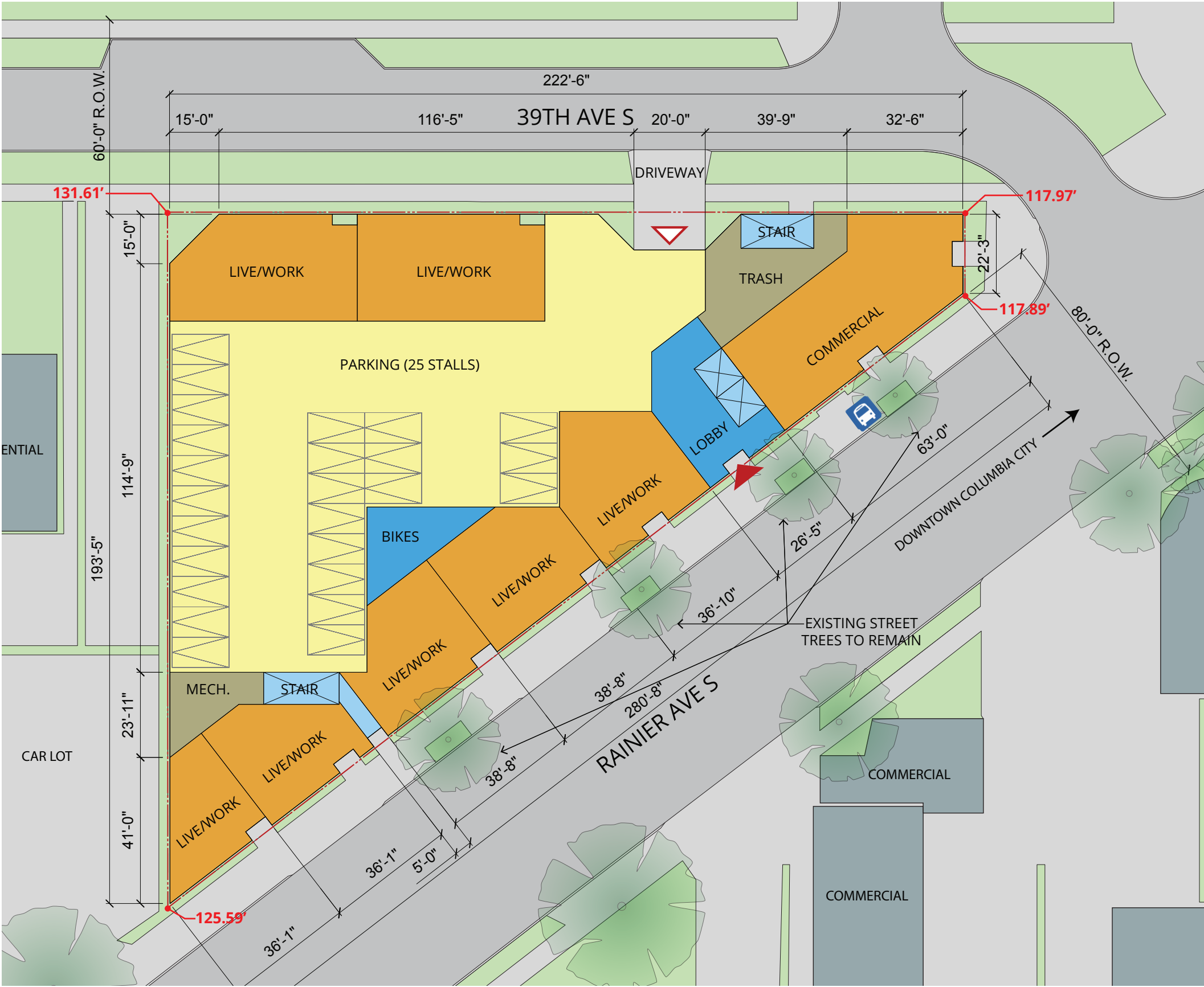
NEGATIVE

- LONG CONTINUOUS FACADE ALONG 39TH AVE S & SOUTH LOT LINE
- ABRUPT TRANSITION TO LR2 ZONE
- NO OPEN SPACE ON 39TH AVE S

- ▶ PRIMARY RESIDENTIAL ENTRANCE
- ▷ PARKING GARAGE ENTRANCE
- 🚌 BUS STOP (ROUTE #7)
- 🏠 BUILDING SERVICE
- 🏠 RESIDENTIAL
- 🏠 COMMERCIAL
- 🚶 CIRCULATION
- 🚶 COMMON
- 🚶 PARKING

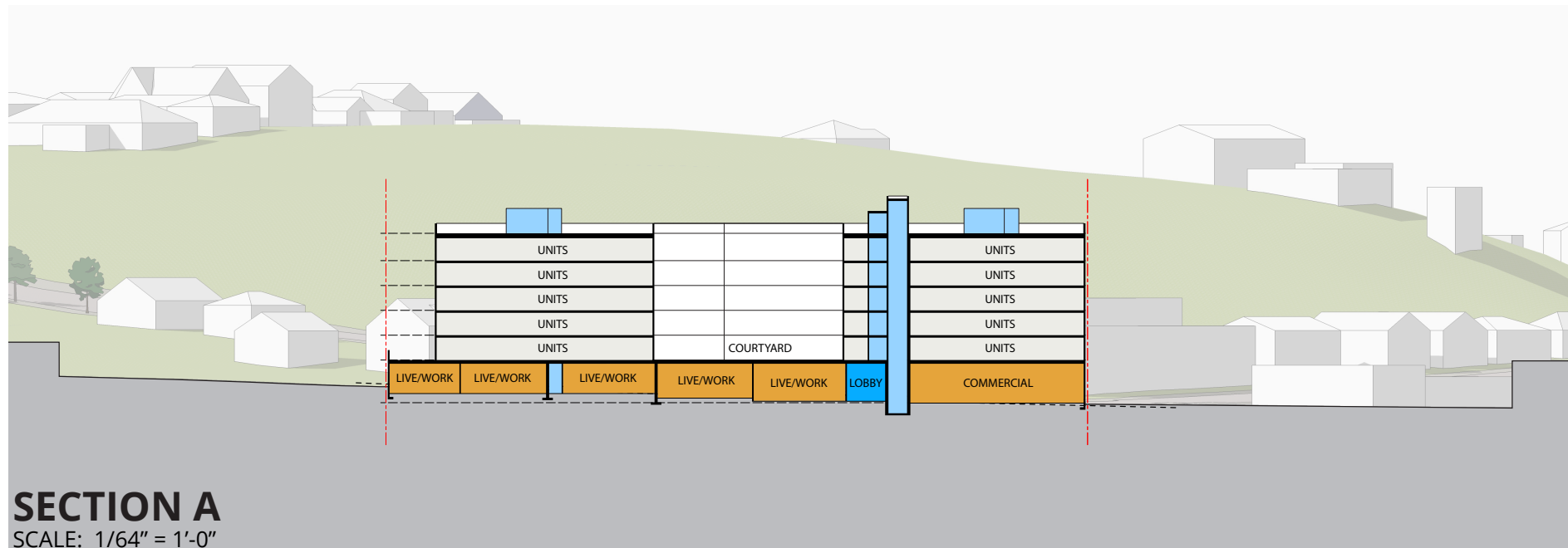
SITE / STREET LEVEL PLAN

SCALE: 1/32" = 1'-0"

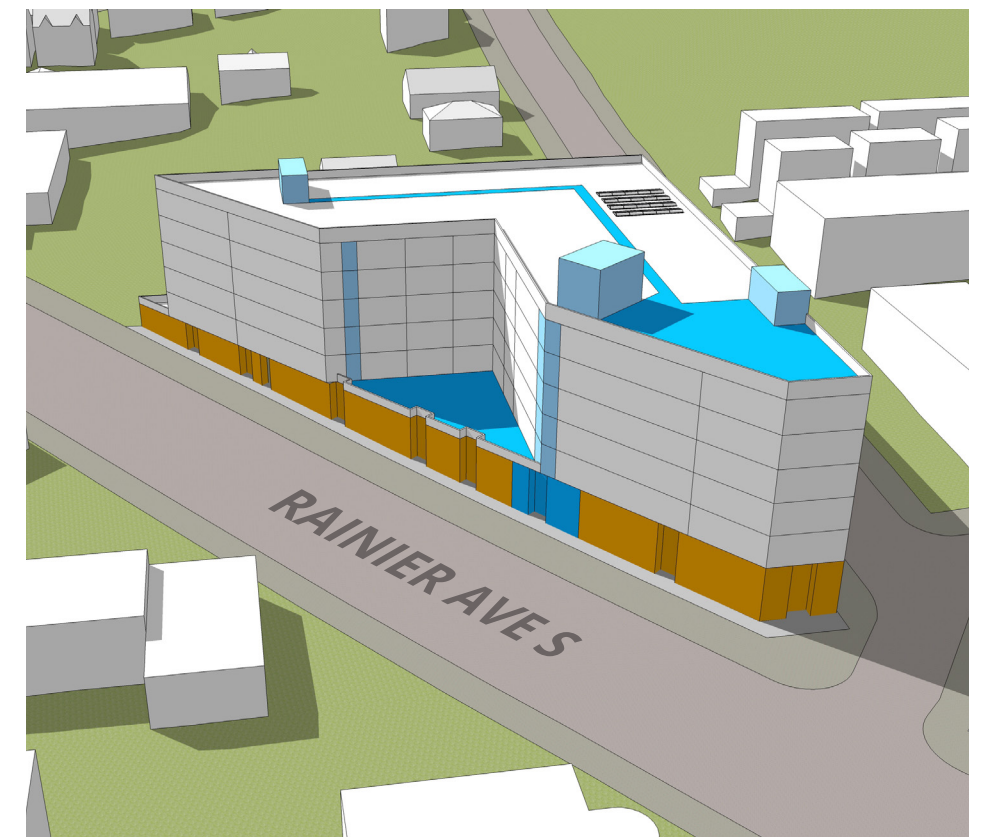


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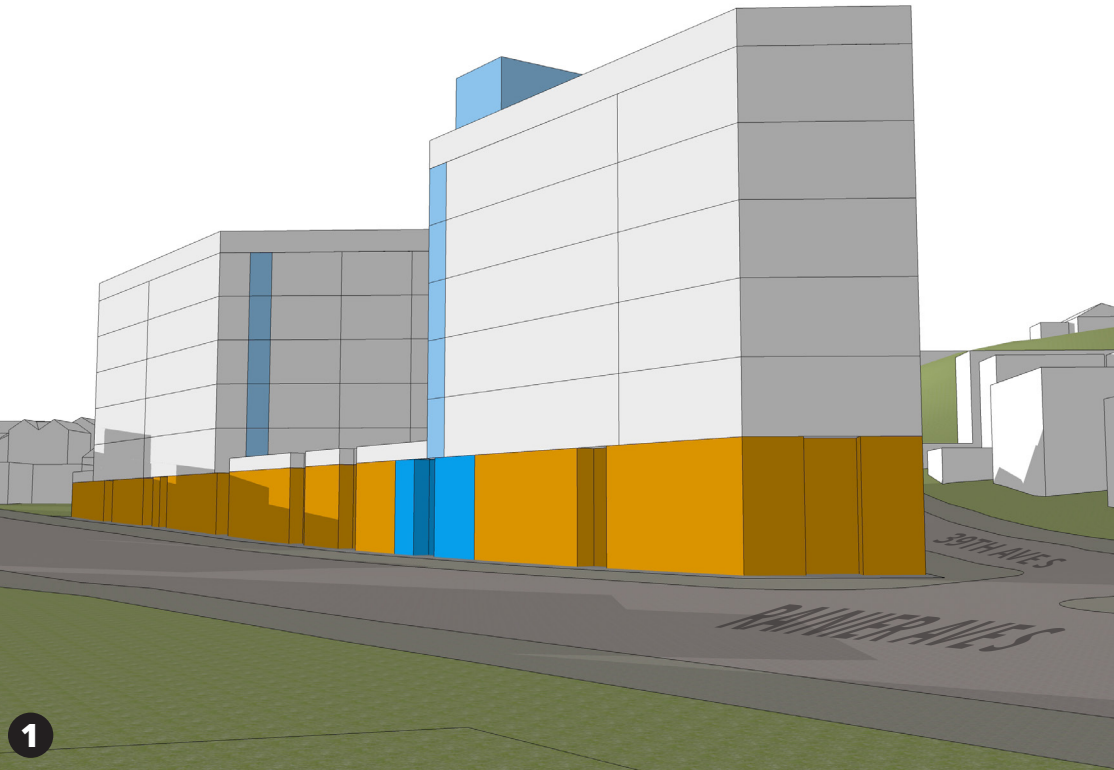
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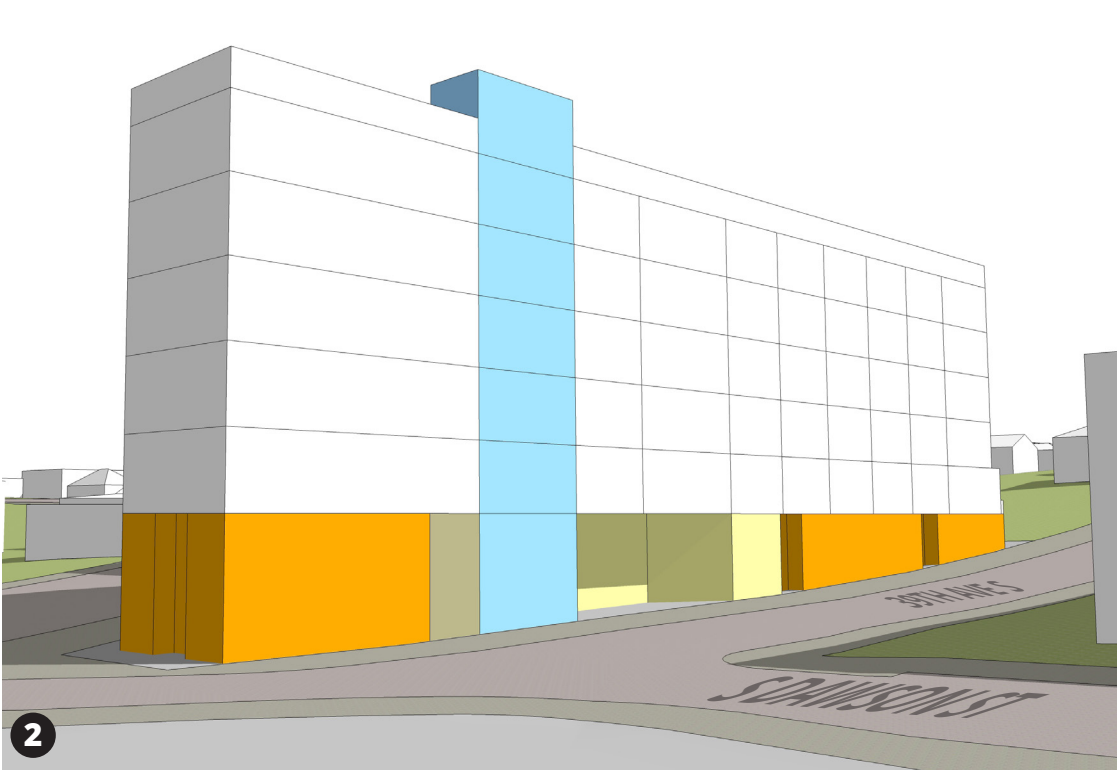
SCALE: 1/64" = 1'-0"

5201 RAINIER LLC **S+HWorks**
ARCHITECTURE & DESIGN

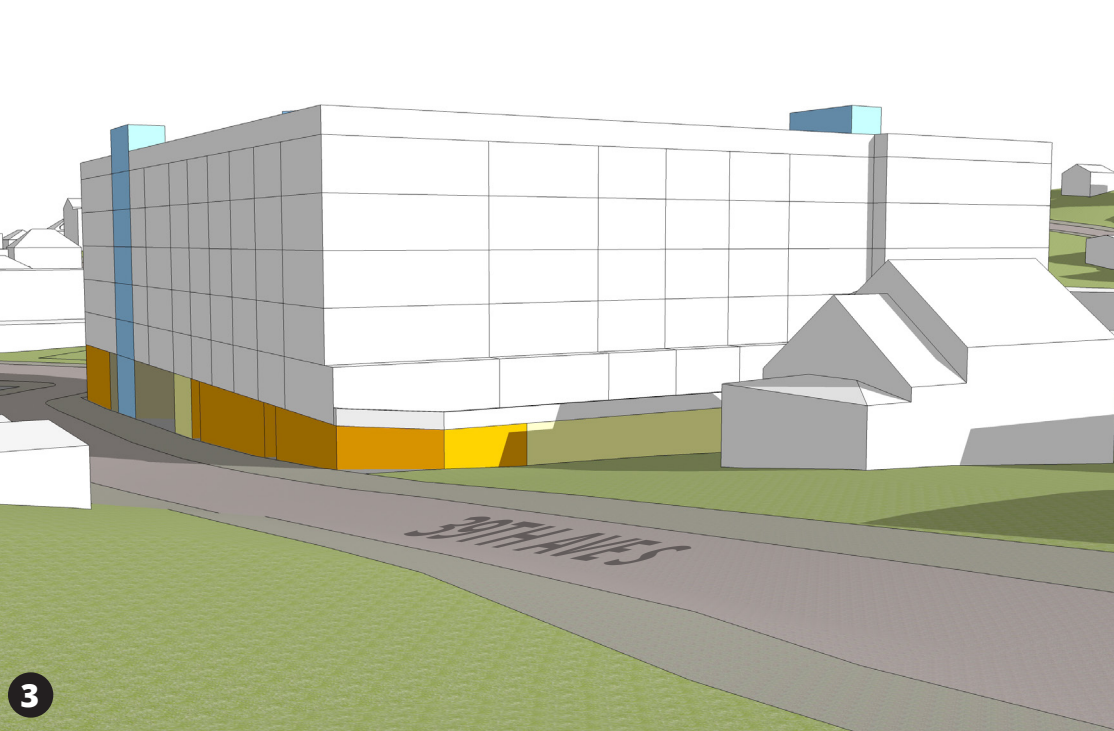
CONCEPT A: L-SHAPE



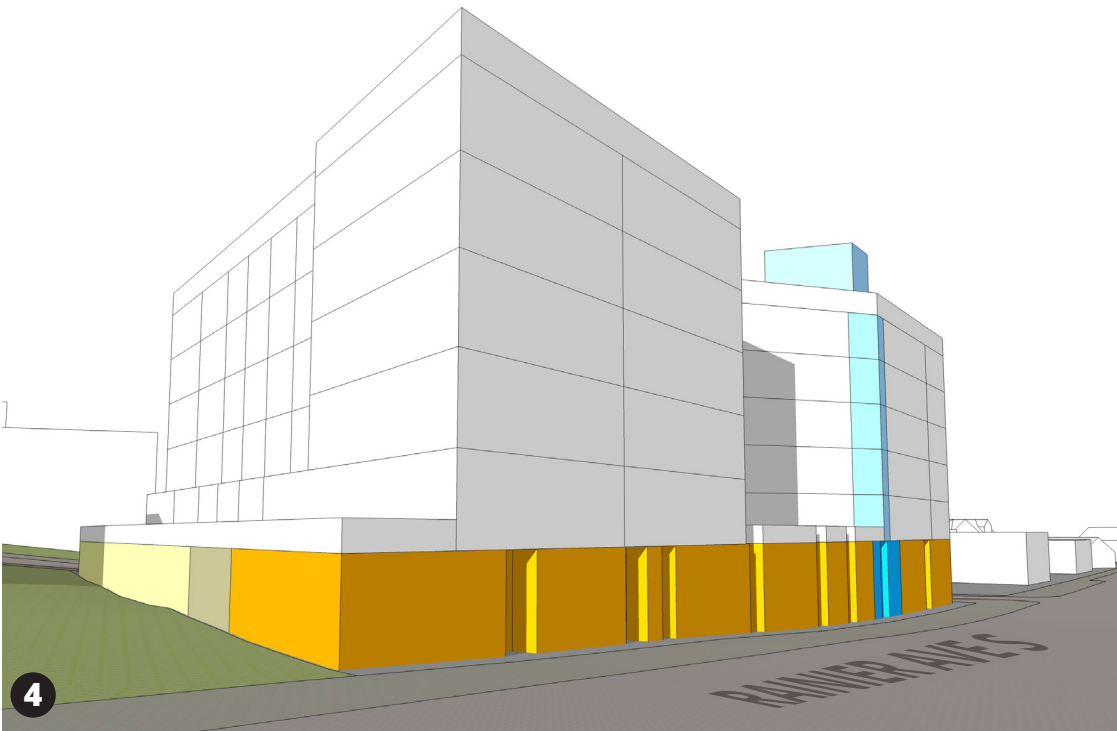
PERSPECTIVE LOOKING AT NORTH CORNER OF BUILDING



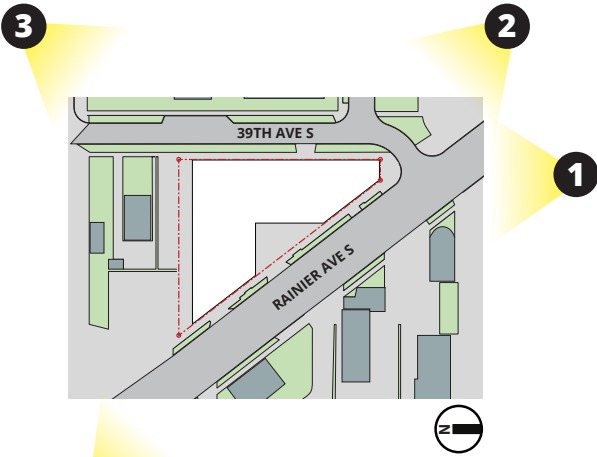
PERSPECTIVE LOOKING AT NORTHWEST CORNER OF BUILDING



PERSPECTIVE LOOKING AT SOUTHWEST CORNER OF BUILDING



PERSPECTIVE LOOKING AT SOUTHEAST CORNER OF THE BUILDING

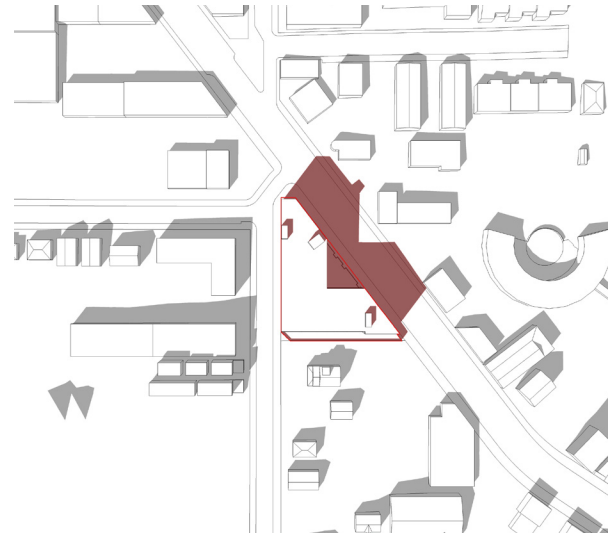
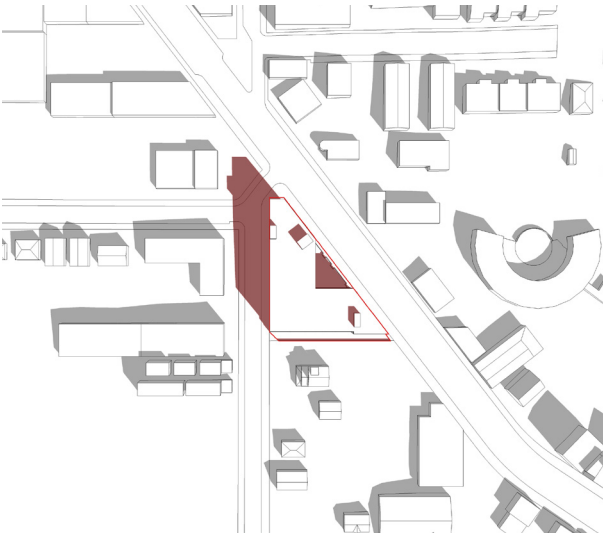


NOTE: TREES NOT SHOWN FOR CLARITY

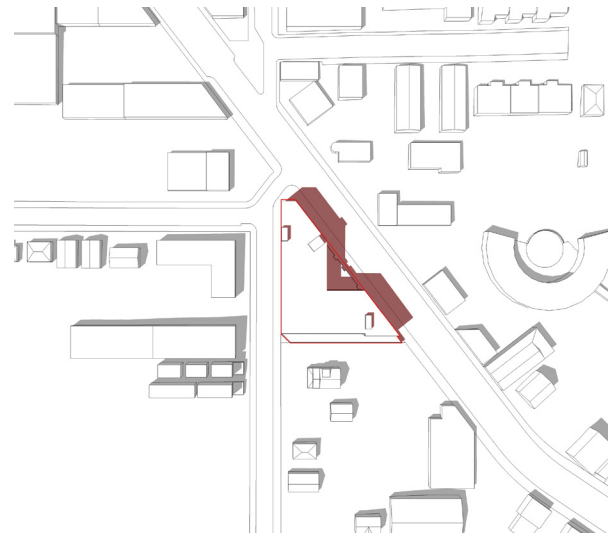
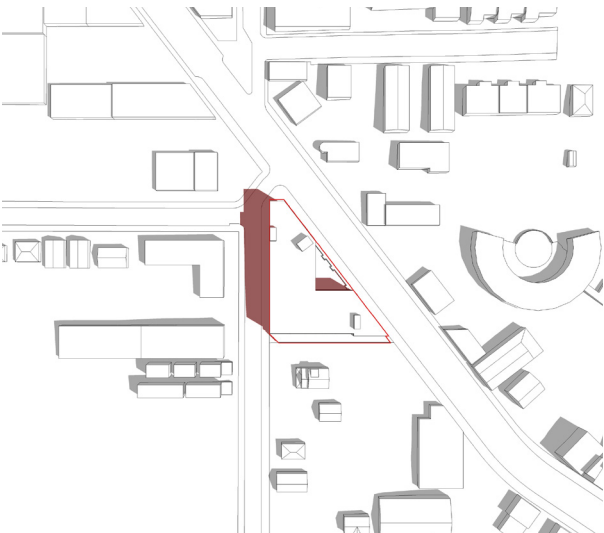
DEPARTURES
NONE REQUESTED

SUN PATH / SHADOW STUDY: CONCEPT A

SPRING / FALL EQUINOX



SUMMER SOLSTICE



WINTER SOLSTICE



10:00 A.M.

12:00 P.M.

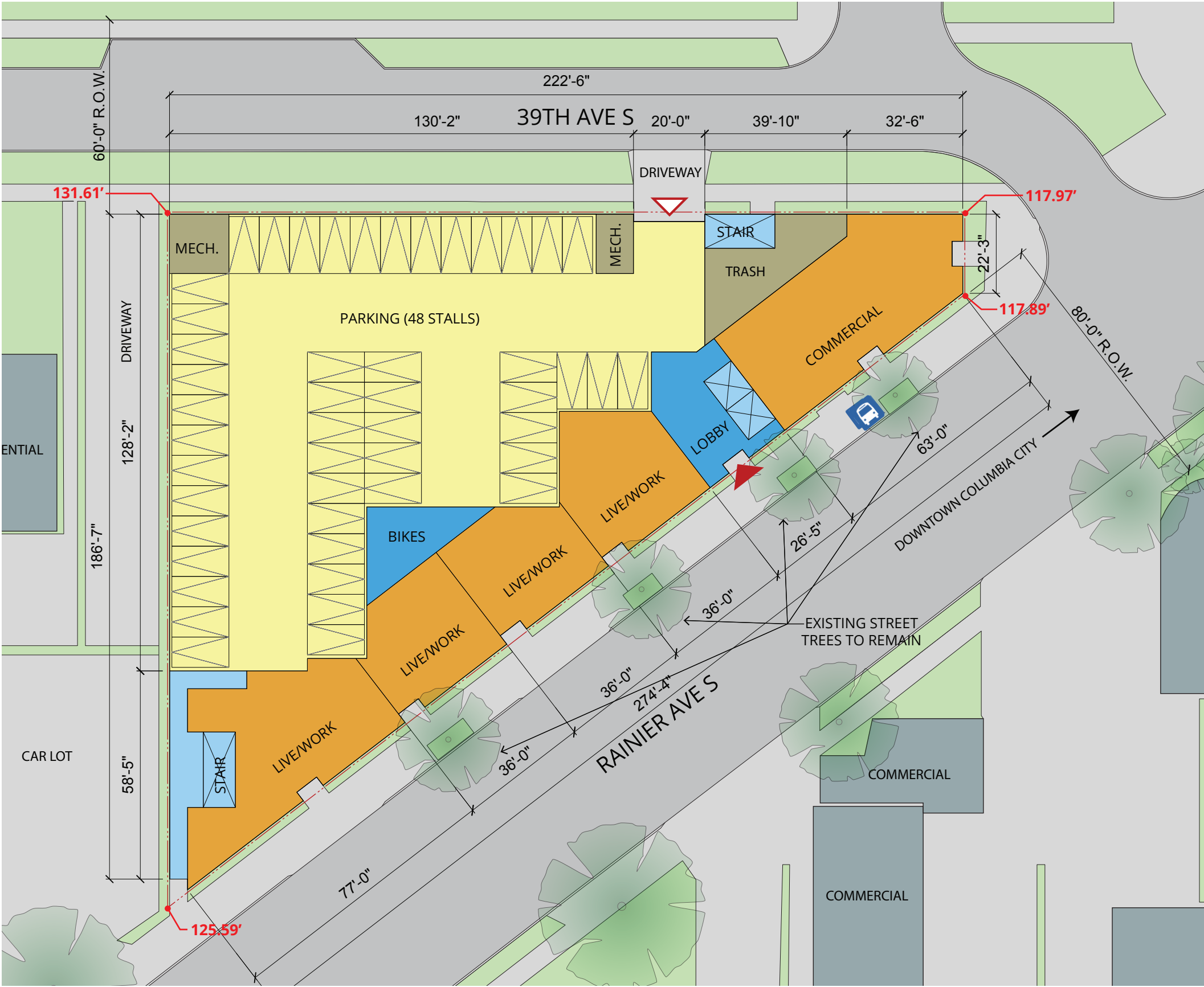
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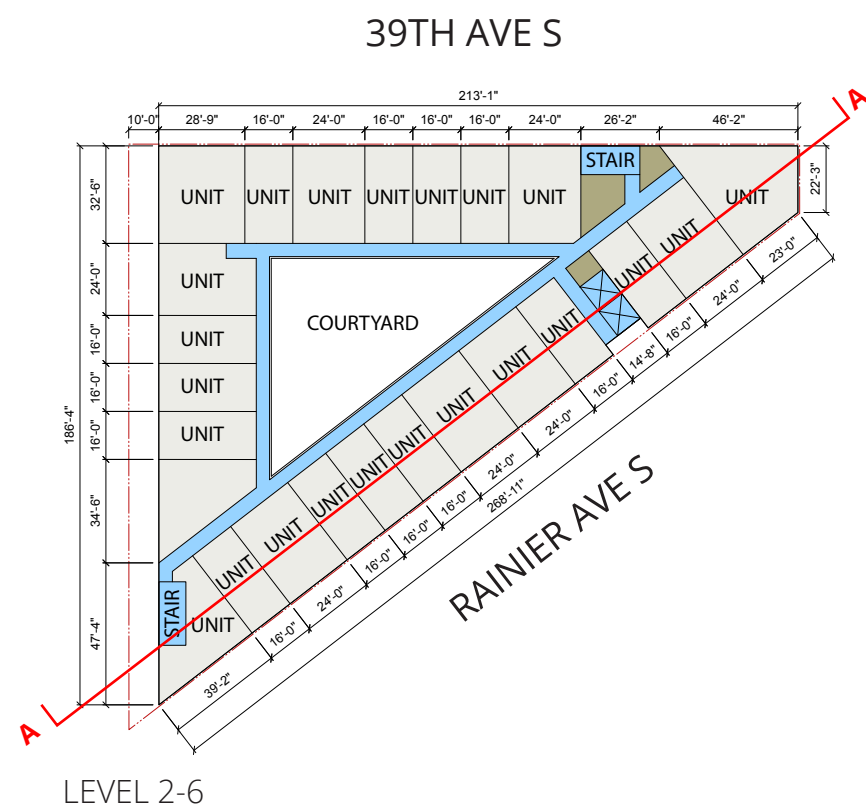
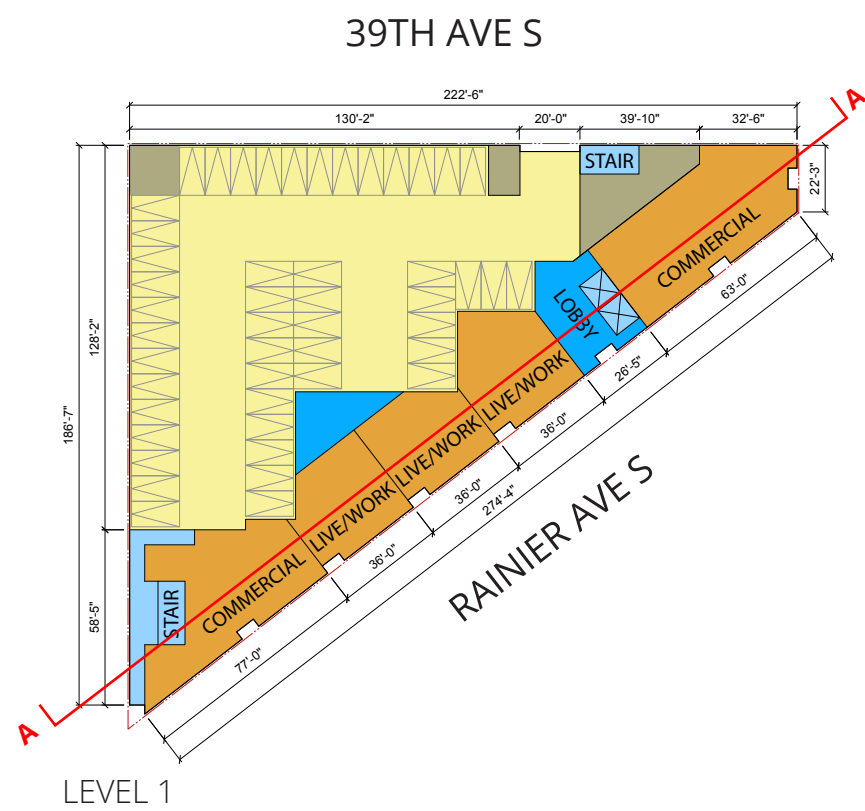
**CONCEPT B: INTERIOR
COURTYARD**
118,238 GSF
120 UNITS
4 LIVE/WORK UNITS, 1037-1818 SF EACH
2,057 COMMERCIAL SF
48 PARKING STALLS PROVIDED
4,307 SF- TOTAL COMMON AMENITY
3,423 SF - AT COURTYARD
2,832 SF - ROOF DECK
4.75 FAR - 115,938 GFA

- POSITIVE**
- INTERIOR RESIDENTIAL COURTYARD
 - MAXIMIZES SITE FOOTPRINT
- NEGATIVE**
- LONG CONTINUOUS FACADES ON ALL SIDES
 - ABRUPT TRANSITION TO LR2 ZONE
 - REQUIRES DEPARTURES
 - LESS ACTIVATED FACADE ALONG 39TH

- ▶ PRIMARY RESIDENTIAL ENTRANCE
- ▷ PARKING GARAGE ENTRANCE
- 🚌 BUS STOP (ROUTE #7)
- 🏠 BUILDING SERVICE
- 🏠 RESIDENTIAL
- 🏠 COMMERCIAL
- 🚶 CIRCULATION
- 🚶 COMMON
- 🚶 PARKING

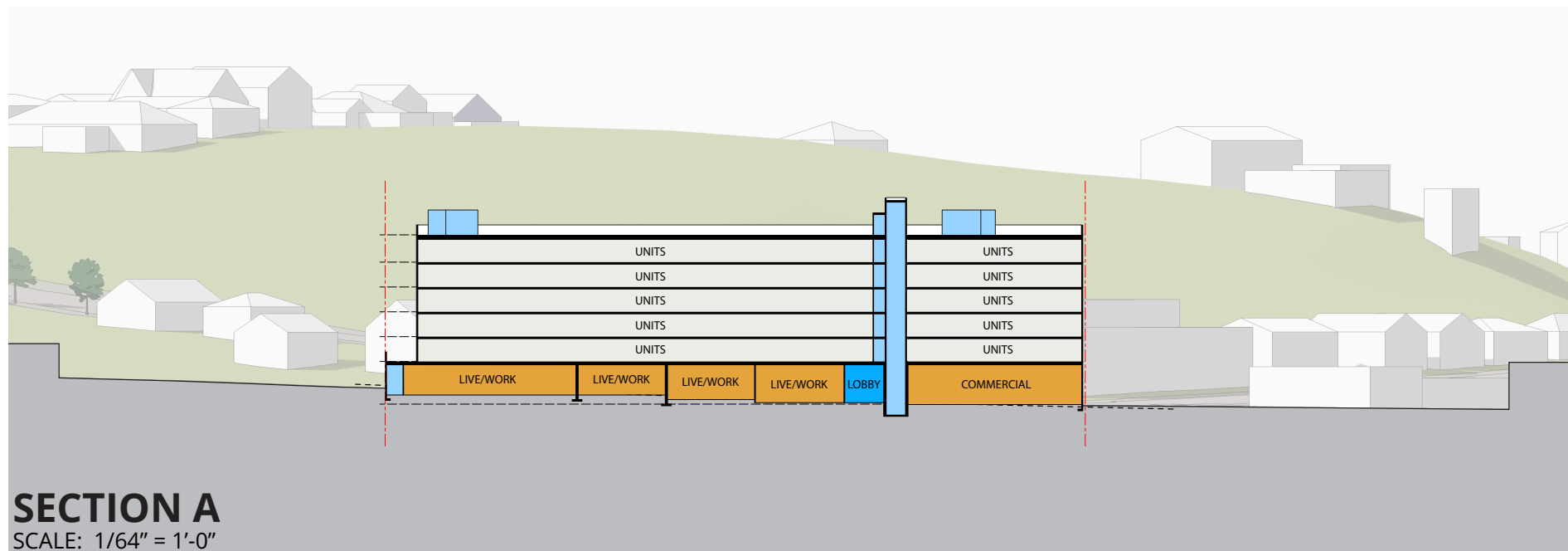
SITE / STREET LEVEL PLAN
SCALE: 1/32" = 1'-0"





FLOOR PLANS

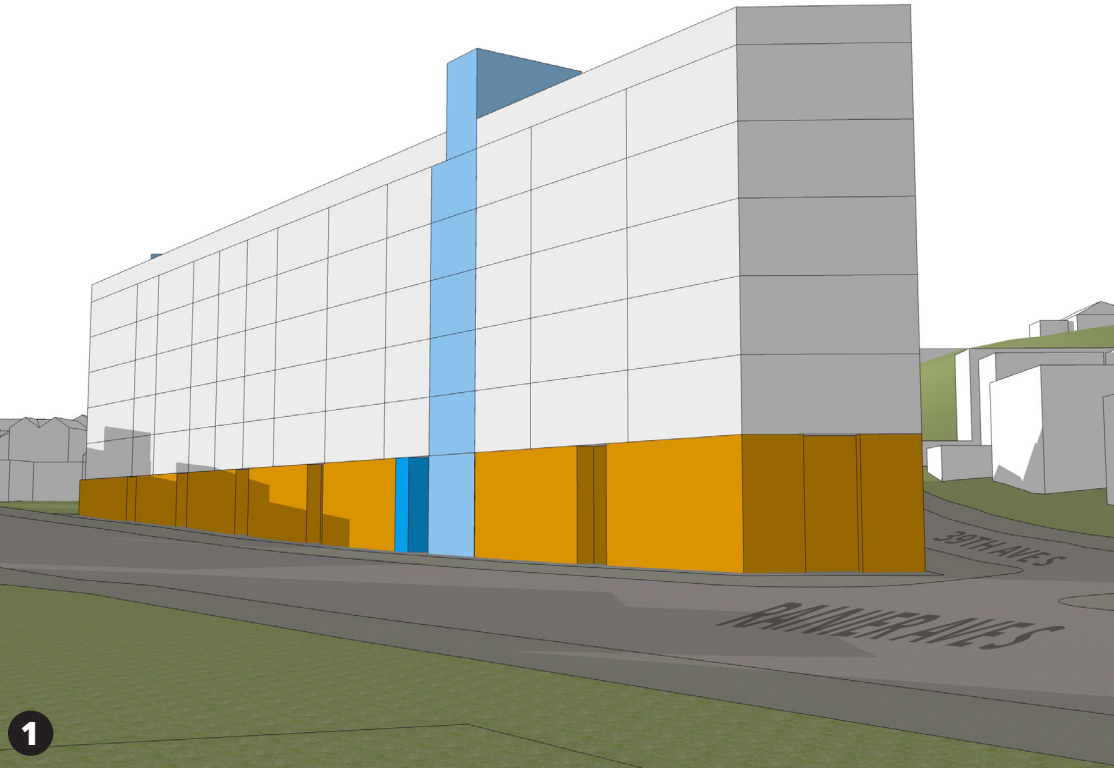
SCALE: 1/64" = 1'-0"



SCALE: 1/64" = 1'-0"

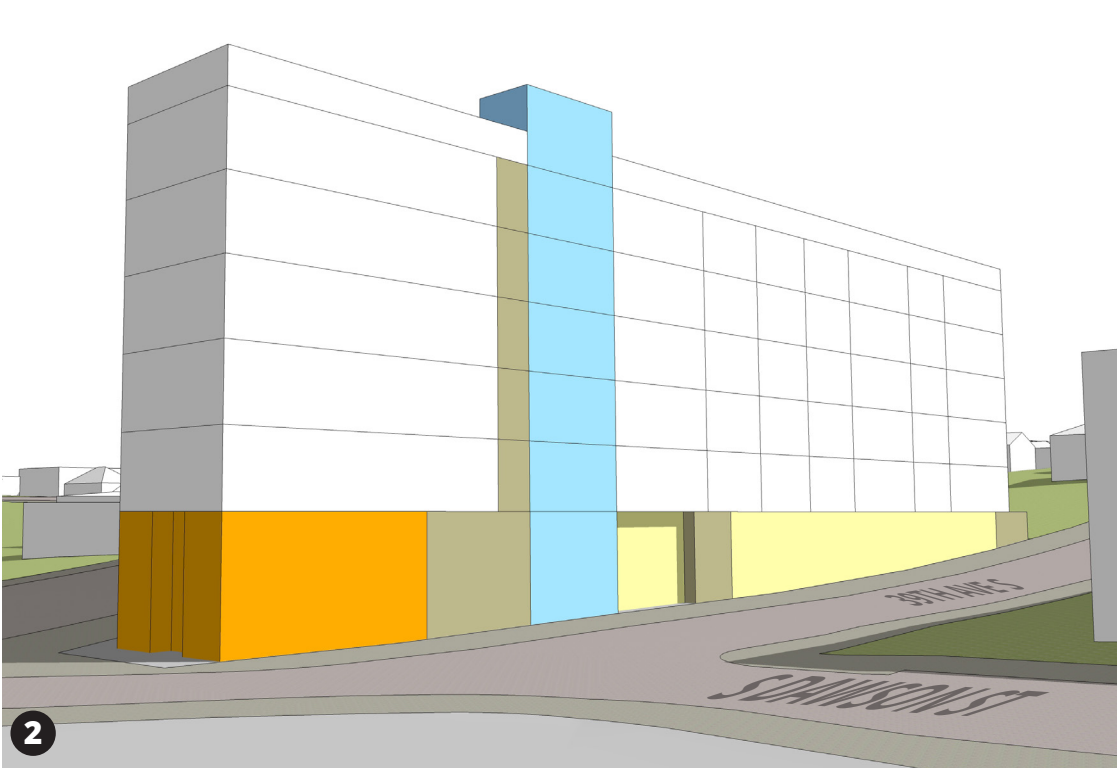


CONCEPT B: INTERIOR COURTYARD



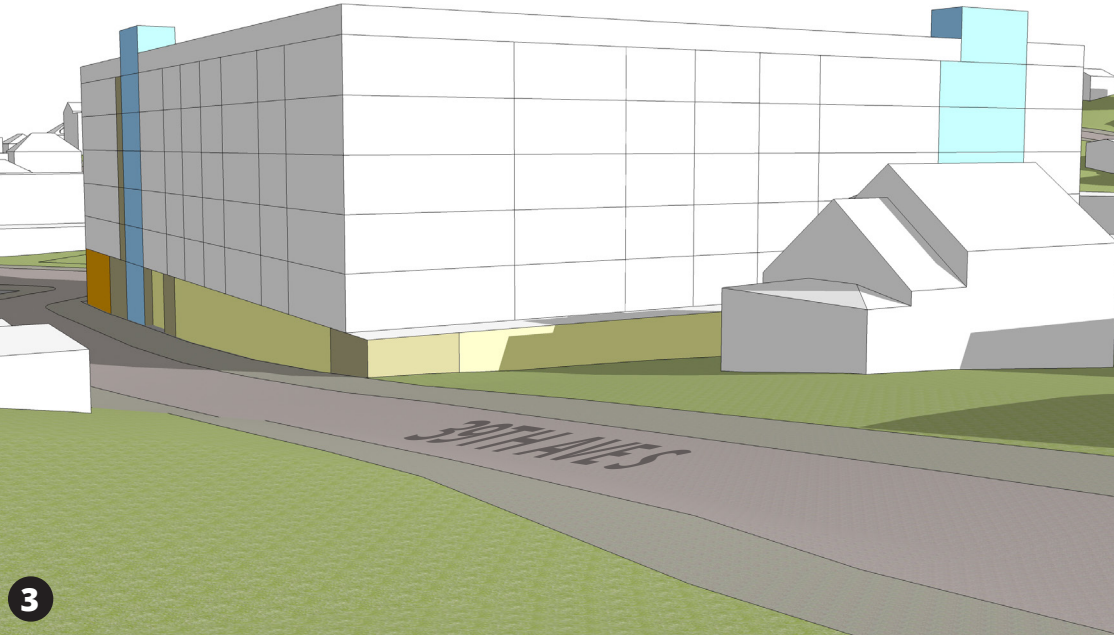
1

PERSPECTIVE LOOKING AT NORTH CORNER OF BUILDING



2

PERSPECTIVE LOOKING AT NORTHWEST CORNER OF BUILDING



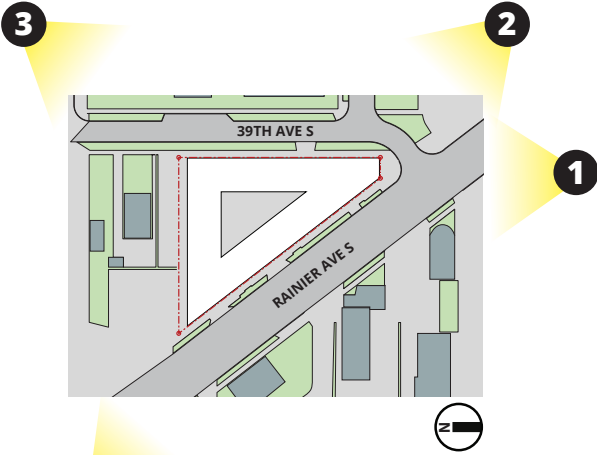
3

PERSPECTIVE LOOKING AT SOUTHWEST CORNER OF BUILDING



4

PERSPECTIVE LOOKING AT SOUTHEAST CORNER OF THE BUILDING



NOTE: TREES NOT SHOWN FOR CLARITY

DEPARTURES

1) SMC 23.47A.014.B.1 - SETBACK FOR LOTS ABUTTING RESIDENTIAL LOTS

REQUIRED: 15' TRIANGULAR SETBACK ABUTTING RESIDENTIAL LOTS

PROPOSED: 15' REDUCTION OF TRIANGULAR SETBACK

GUIDELINES & JUSTIFICATION:

-DC2/A MASSING: THE REDUCED SETBACK ALLOWS THE MASSING TO COMPLETE ITSELF AT THE SOUTHWEST CORNER AND FORM A CLEAR EDGE FROM THE GROUND TO THE SKY

-CS1/C TOPOGRAPHY: THE SETBACK REDUCTION ALLOWS THE PARKING GARAGE TO BE MORE EFFICIENTLY LOCATED

2) SMC 23.47A.014.B.3 - REAR SETBACK ABUTTING RESIDENTIAL ZONES

REQUIRED: 15' BETWEEN 13' TO 40' HEIGHT, PLUS 2' FOR EVERY 10' ABOVE 40' HEIGHT

PROPOSED: 10' SETBACK BETWEEN 5' TO 65' HEIGHT.

GUIDELINES & JUSTIFICATION:

-DC3/A.1 BUILDING OPEN SPACE RELATIONSHIP: THE SETBACK REDUCTION ALLOWS THE CENTRAL COURTYARD CONCEPT TO WORK BY ALLOWING ENOUGH ROOM FOR THE UNITS TO WRAP AROUND THE SPACE.

3) SMC 23.47A.008.B.2 - NON-RESIDENTIAL STREET-LEVEL REQUIREMENTS

REQUIRED: 60% STREET FACING FACADE 2'-8" ABOVE SIDEWALK TRANSPARENT

PROPOSED: NONE @ 39TH AVE S. ONLY

GUIDELINES & JUSTIFICATION:

-CS1/C TOPOGRAPHY: IN ORDER TO UTILIZE THE SLOPING SITE TOPOGRAPHY, AND BURY THE GARAGE, A PORTION OF THE FACADE ALONG 39TH AVE S WILL BE OPAQUE

4) SMC 23.47A.032.B.1.b

REQUIRED: STREET-LEVEL PARKING WITHIN STRUCTURE MUST BE SEPARATED FROM STREET-FACING FACADES BY ANOTHER PERMITTED USE

PROPOSED: 130'-2" OF PARKING ALONG 39TH AVE S FACADE

GUIDELINES & JUSTIFICATION:

-DC1/A+C: ARRANGEMENT OF INTERIOR USES + PARKING AND SERVICE USES: THE DEPARTURE ALLOWS THE RETAIL/COMMERCIAL SPACES TO BE LOCATED ALONG RAINIER AVE S RATHER THAN 39TH AVE S, WHICH IS NOT A VIABLE COMMERCIAL FRONTAGE.

5) SMC 23.54.030.G.1 - SIGHT TRIANGLE

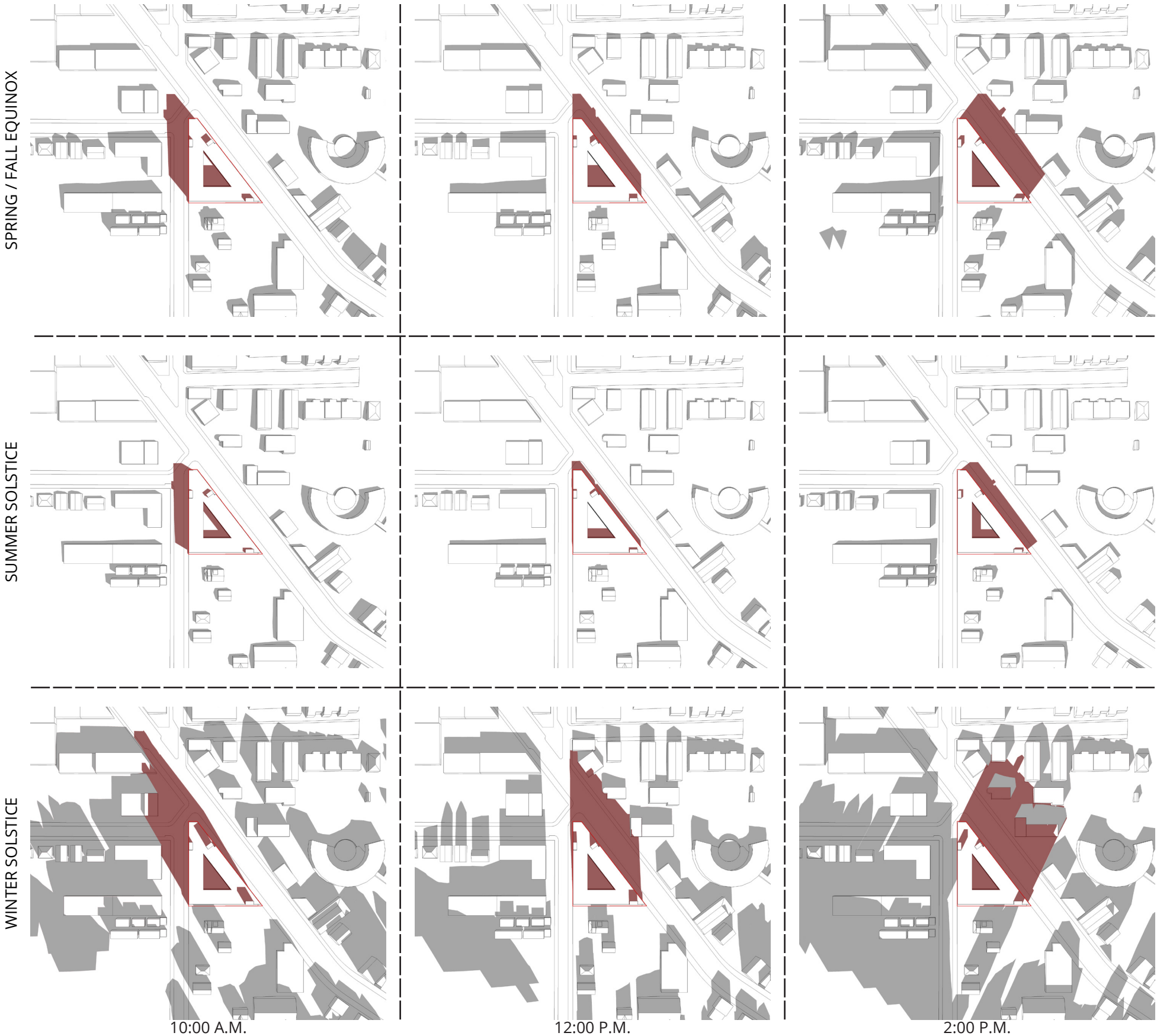
REQUIRED: 10' TRIANGLE FROM INTERSECTION OF DRIVEWAY AND SIDEWALK

PROPOSED: 3'-7" TRIANGLE @ NORTH AND SOUTH WITH MIRRORS AND RUMBLE PAVING AT DRIVEWAY

GUIDELINES & JUSTIFICATION:

DC1/B: VEHICULAR ACCESS AND CIRCULATION: THE SIGHT TRIANGLE REDUCTION MINIMIZES THE WIDTH AND VISUAL PROMINENCE OF THE GARAGE ENTRY ALONG 39TH AVE S.

SUN PATH / SHADOW STUDY: CONCEPT B



CONCEPT C: PREFERRED

112,994 GSF
121 UNITS
3 LIVE/WORK UNITS, 1404-1652 SF EACH
1,888 COMMERCIAL SF
55 PARKING STALLS PROVIDED
3,771 SF- TOTAL COMMON AMENITY
3,537 SF - COURTYARD
2,531 SF - ROOF DECK
4.75 FAR - 115,938 GFA

POSITIVE

- FACADE SCALE IS REDUCED ON 39TH AVE S
- TRANSITIONS SCALE TO THE LR2 ZONE
- TOWNHOMES ADD VARIETY TO THE PROJECT AND ACTIVATE 39TH AVE S
- PLAZA PROVIDES AMENITY + VIEWS THROUGH SITE
- TWO STORY LIVE/WORK UNITS

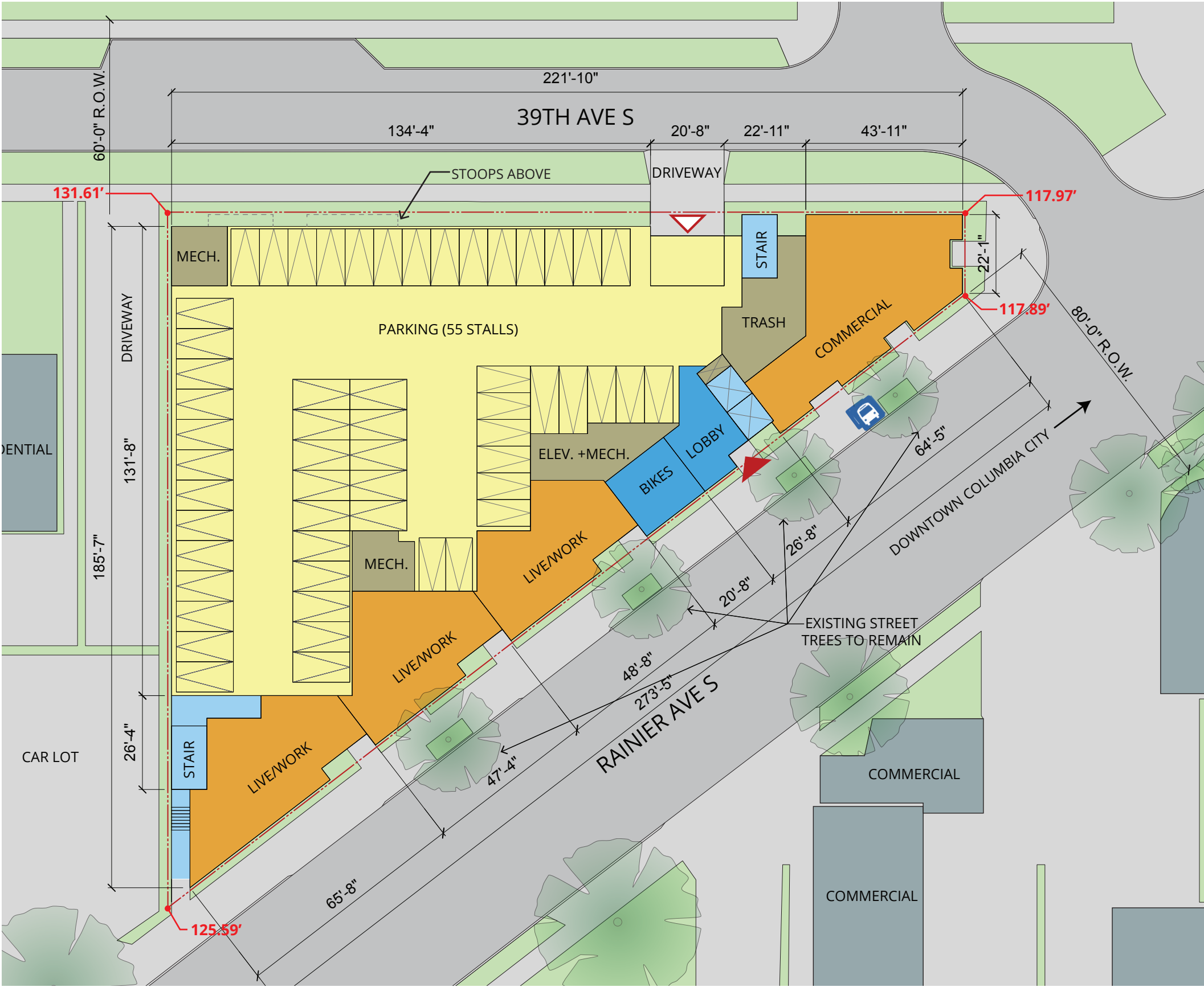
NEGATIVE

- REQUIRES DEPARTURES
- LONG FACADE ALONG RAINIER AVE S

- ▶ PRIMARY RESIDENTIAL ENTRANCE
- ▷ PARKING GARAGE ENTRANCE
- 🚌 BUS STOP (ROUTE #7)
- BUILDING SERVICE
- RESIDENTIAL
- COMMERCIAL
- CIRCULATION
- COMMON
- PARKING

SITE / STREET LEVEL PLAN

SCALE: 1/32" = 1'-0"



39TH AVE S



SCALE: 1/64" = 1'-0"



39TH AVE S



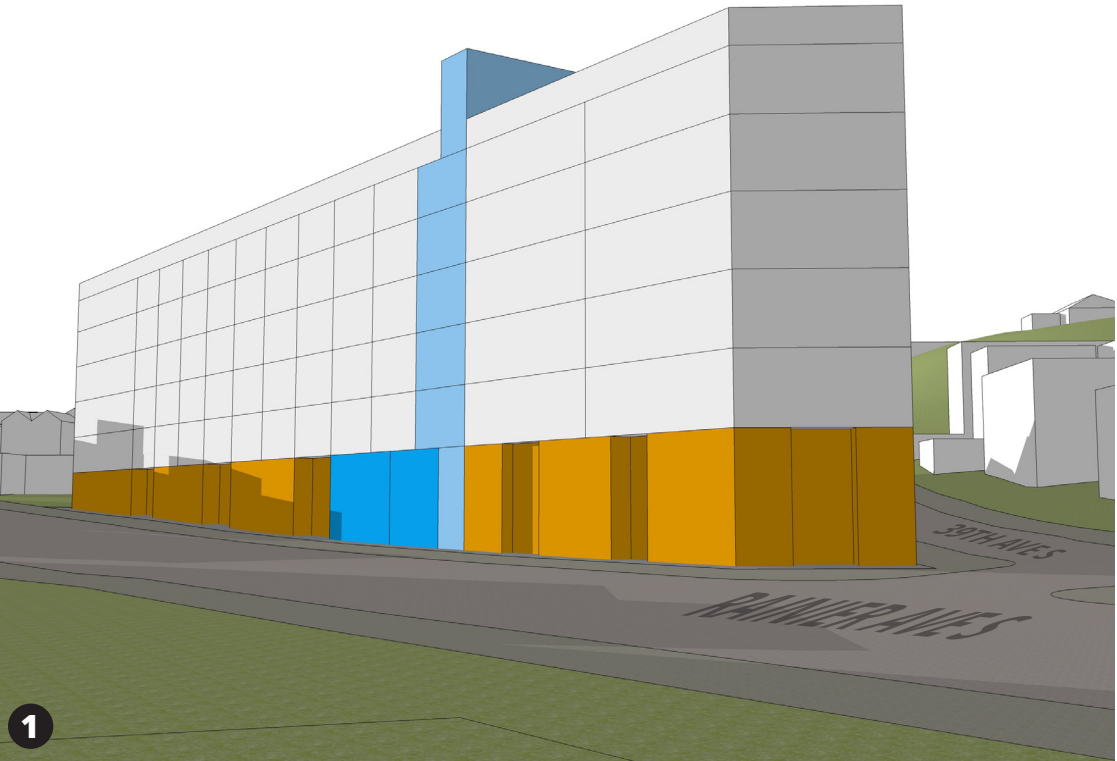
39TH AVE S



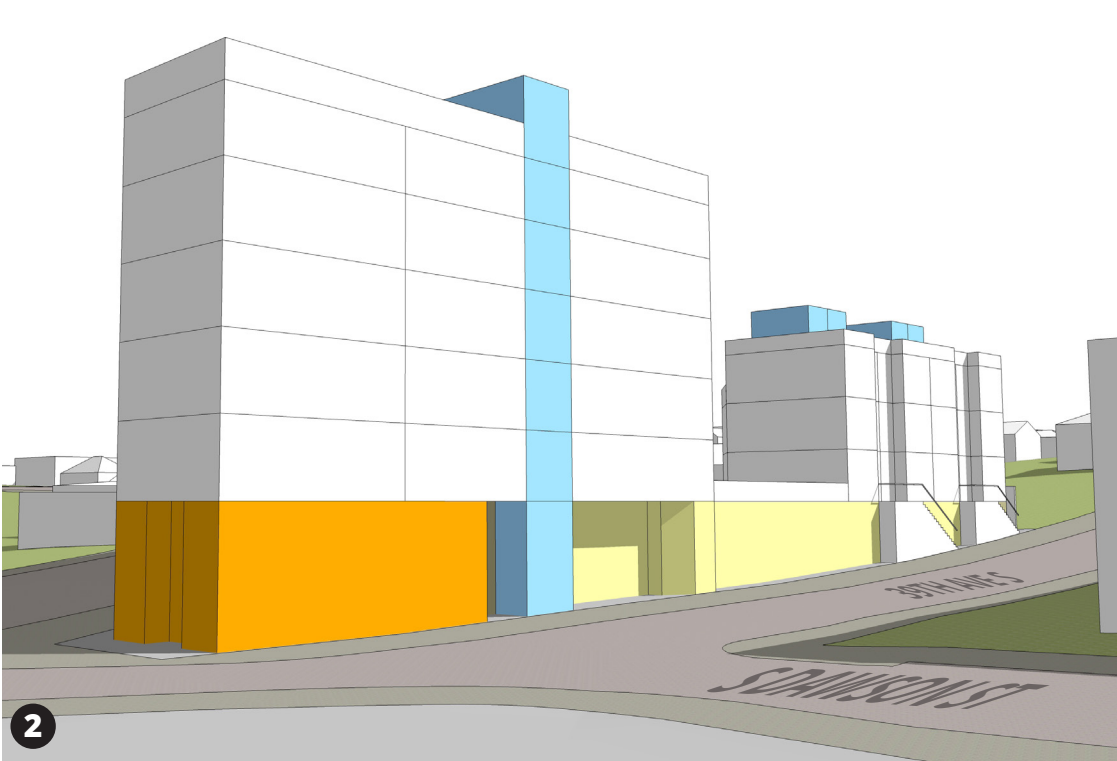
SCALE: 1/64" = 1'-0"



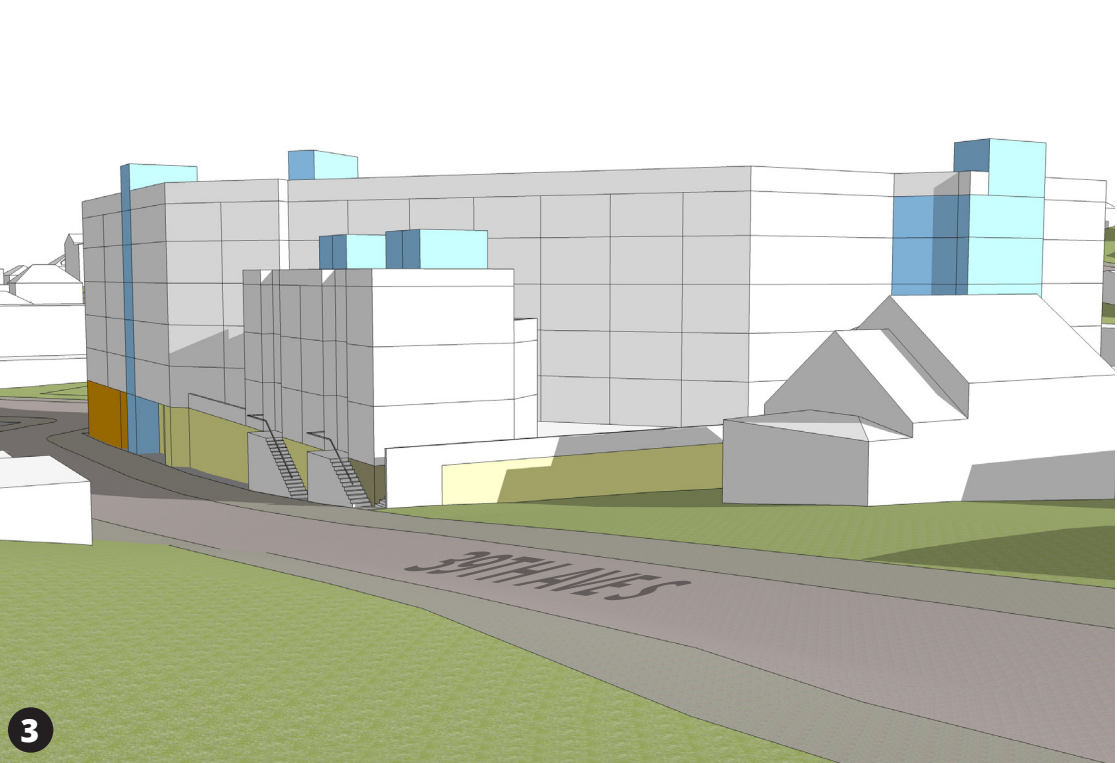
CONCEPT C: PREFERRED



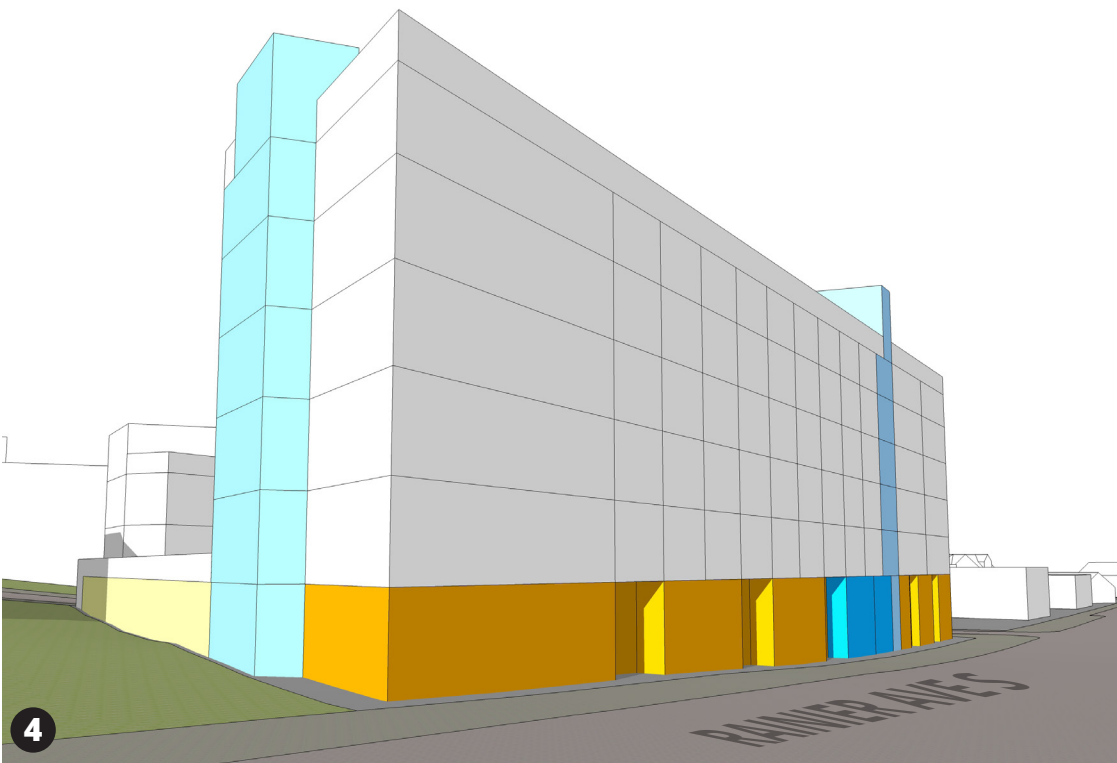
PERSPECTIVE LOOKING AT NORTH CORNER OF BUILDING



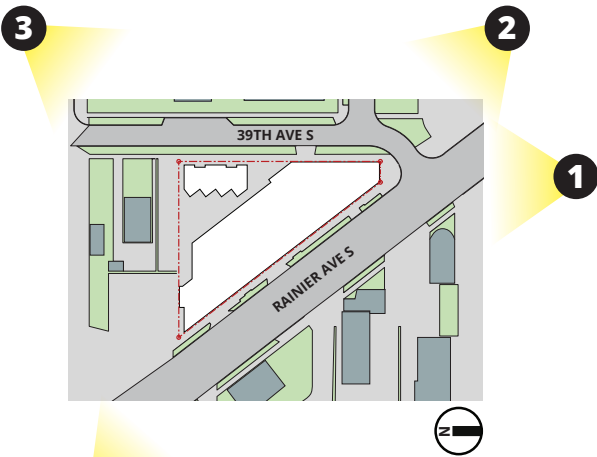
PERSPECTIVE LOOKING AT NORTHWEST CORNER OF BUILDING



PERSPECTIVE LOOKING AT SOUTHWEST CORNER OF BUILDING



PERSPECTIVE LOOKING AT SOUTHEAST CORNER OF THE BUILDING



NOTE: TREES NOT SHOWN FOR CLARITY

DEPARTURES

1) SMC 23.47A.014.B.1 - SETBACK FOR LOTS ABUTTING RESIDENTIAL LOTS

REQUIRED: 15' TRIANGULAR SETBACK ABUTTING RESIDENTIAL LOTS

PROPOSED: 9' REDUCTION OF TRIANGULAR SETBACK

GUIDELINES & JUSTIFICATION:

-DC2/A+C MASSING, BUILDING OPEN SPACE RELATIONSHIP: TO PROVIDE THE TOWNHOUSE UNITS AND THE PLAZA, THE TOWNHOUSES AT THE SOUTHWEST CORNER REQUIRE A REDUCED SETBACK

2) SMC 23.47A.014.B.3 - REAR SETBACK ABUTTING RESIDENTIAL ZONES

REQUIRED: 15' BETWEEN 13' TO 40' HEIGHT, PLUS 2' FOR EVERY 10' ABOVE 40' HEIGHT

PROPOSED: 6' SETBACK BETWEEN 13' TO 36' HEIGHT.

GUIDELINES & JUSTIFICATION:

- CS2 B ADJACENT SITES, STREETS AND OPEN SPACES: THE REDUCTION HELPS THE PROPOSED/ PREFERRED SCHEME TRANSITION SCALE FROM RAINIER AVE S TO THE MORE RESIDENTIAL SCALE OF 39TH AVE S

-PL3.B RESIDENTIAL EDGES: THE SETBACK REDUCTION ALLOWS THE TOWNHOMES TO FIT WITH THE ADJACENT LR2 ZONE.

3) SMC 23.47A.008.B.2 - NON-RESIDENTIAL STREET-LEVEL REQUIREMENTS

REQUIRED: 60% STREET FACING FACADE 2'-8" ABOVE SIDEWALK TRANSPARENT

PROPOSED: NONE @ 39TH AVE S. ONLY

GUIDELINES & JUSTIFICATION:

-CS1/C TOPOGRAPHY: IN ORDER TO UTILIZE THE SLOPING SITE TOPOGRAPHY, AND BURY THE GARAGE, A PORTION OF THE FACADE ALONG 39TH AVE S WILL BE OPAQUE

4) SMC 23.47A.032.B.1.b

REQUIRED: STREET-LEVEL PARKING WITHIN STRUCTURE MUST BE SEPARATED FROM STREET-FACING FACADES BY ANOTHER PERMITTED USE

PROPOSED: 134'-4" OF PARKING ALONG 39TH AVE S FACADE

GUIDELINES & JUSTIFICATION:

-DC1/A+C: ARRANGEMENT OF INTERIOR USES + PARKING AND SERVICE USES: THE DEPARTURE ALLOWS THE RETAIL/COMMERCIAL SPACES TO BE LOCATED ALONG RAINIER AVE S RATHER THAN 39TH AVE S, WHICH IS NOT A VIABLE COMMERCIAL FRONTAGE.

5) SMC 23.54.030.G.1 - SIGHT TRIANGLE

REQUIRED: 10' TRIANGLE FROM INTERSECTION OF DRIVEWAY AND SIDEWALK

PROPOSED: 7'-1" TRIANGLE @ SOUTH, 0'-9" TRIANGLE @ NORTH WITH MIRRORS AND RUMBLE PAVING AT DRIVEWAY

GUIDELINES & JUSTIFICATION:

-DC1/B: VEHICULAR ACCESS AND CIRCULATION: THE SIGHT TRIANGLE REDUCTION MINIMIZES THE WIDTH AND VISUAL PROMINENCE OF THE GARAGE ENTRY ALONG 39TH AVE S.

6) SMC 23.47A.008.B.3 - NON-RESIDENTIAL STREET-LEVEL REQUIREMENTS

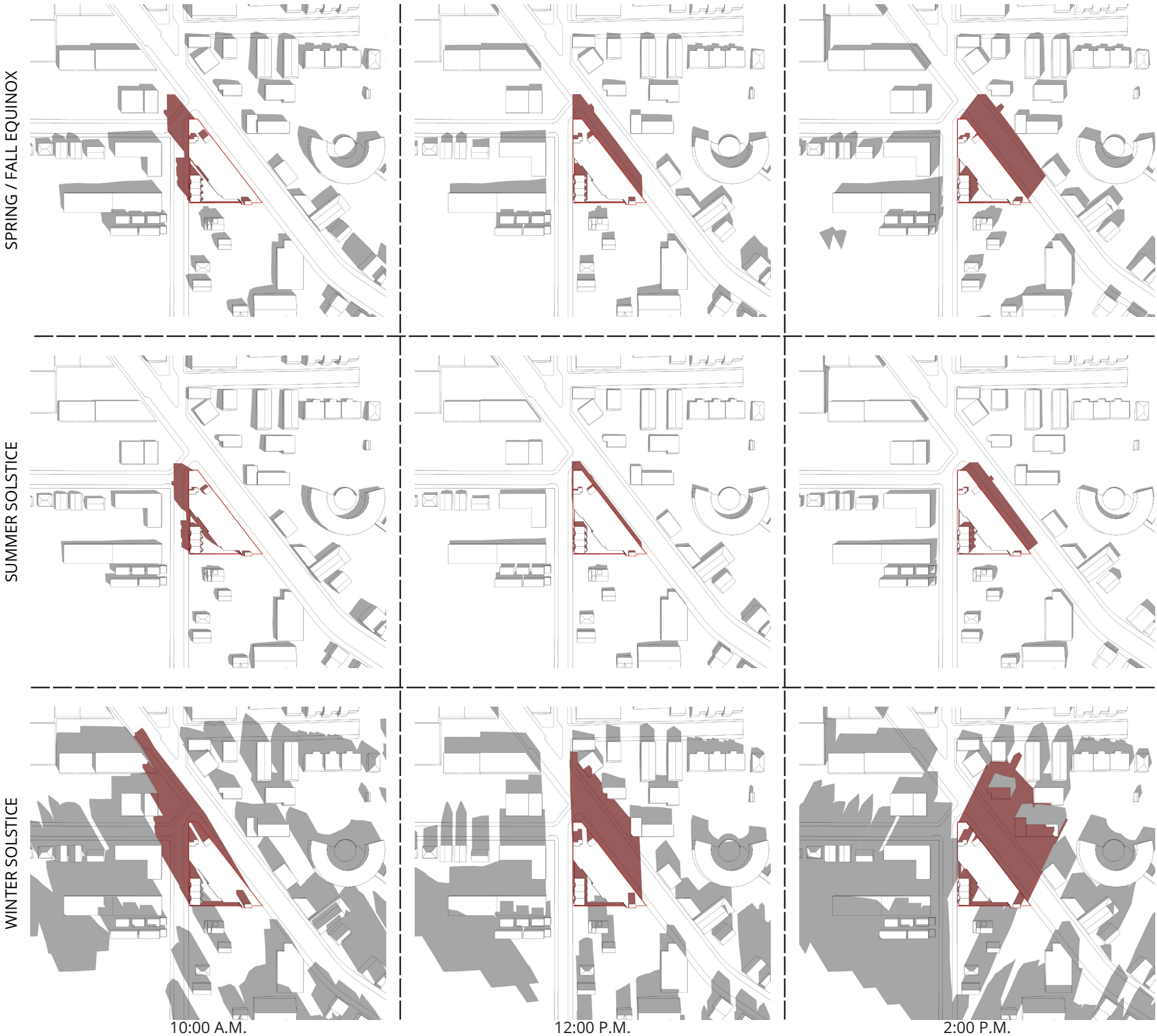
REQUIRED: AVG. 30', 15' MINIMUM DEPTH FOR NON-RESIDENTIAL USES

PROPOSED: SEE DIAGRAM

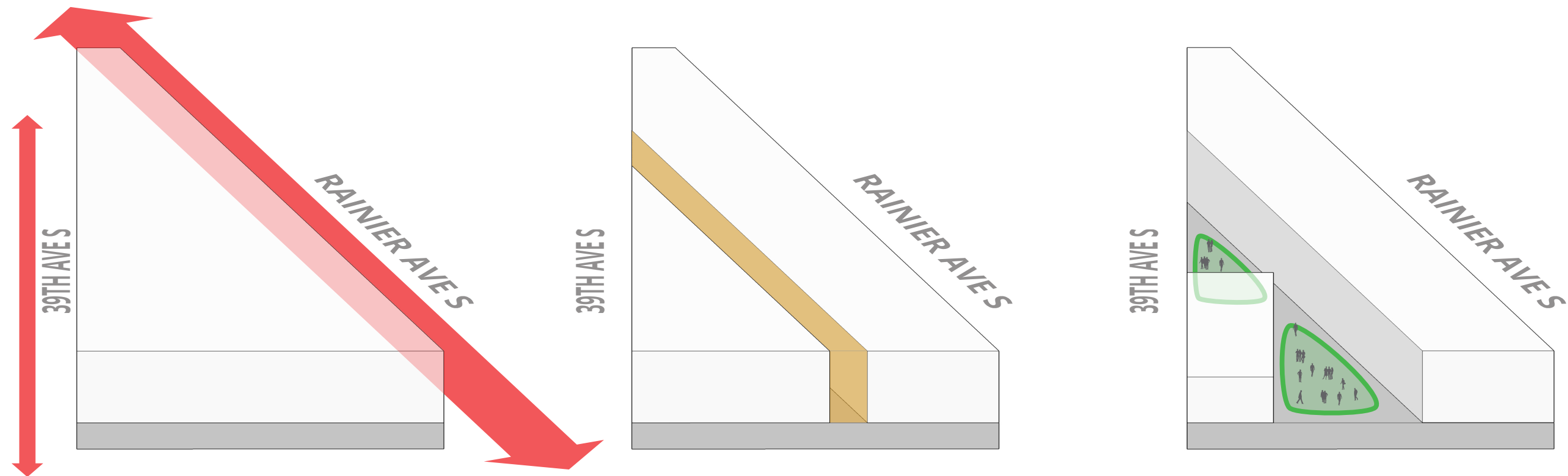
GUIDELINES & JUSTIFICATION:

- DC1/A ARRANGEMENT OF INTERIOR USES: THE DEPARTURE ALLOWS THE PARKING TO BE EFFICIENTLY PLANNED AND FOR THE ANGLED BUILDING GEOMETRY TO BE RESOLVED

SUN PATH / SHADOW STUDY: CONCEPT C



MASSING CONCEPT



CONTEXT TRANSITIONS

This site sits at the cross section of multiple zoning typologies and that vary in scale. The project aims to capture and combine the qualities of each.

39TH AVE S

- SLOW
- LOW-RISE
- RESIDENTIAL
- QUIETER

RAINIER AVE S

- FAST
- TRANSIT
- COMMERCIAL
- MID-RISE
- BUSY

STITCH / SPLIT

Create separation between large scale of Rainier Ave S and the smaller scale of 39th Ave S.

SCALE TRANSLATION

Minimize massing along 39th Ave S, maximize massing along Rainier Ave S. Create transitional amenity space between separation of typologies.

39TH AVE S

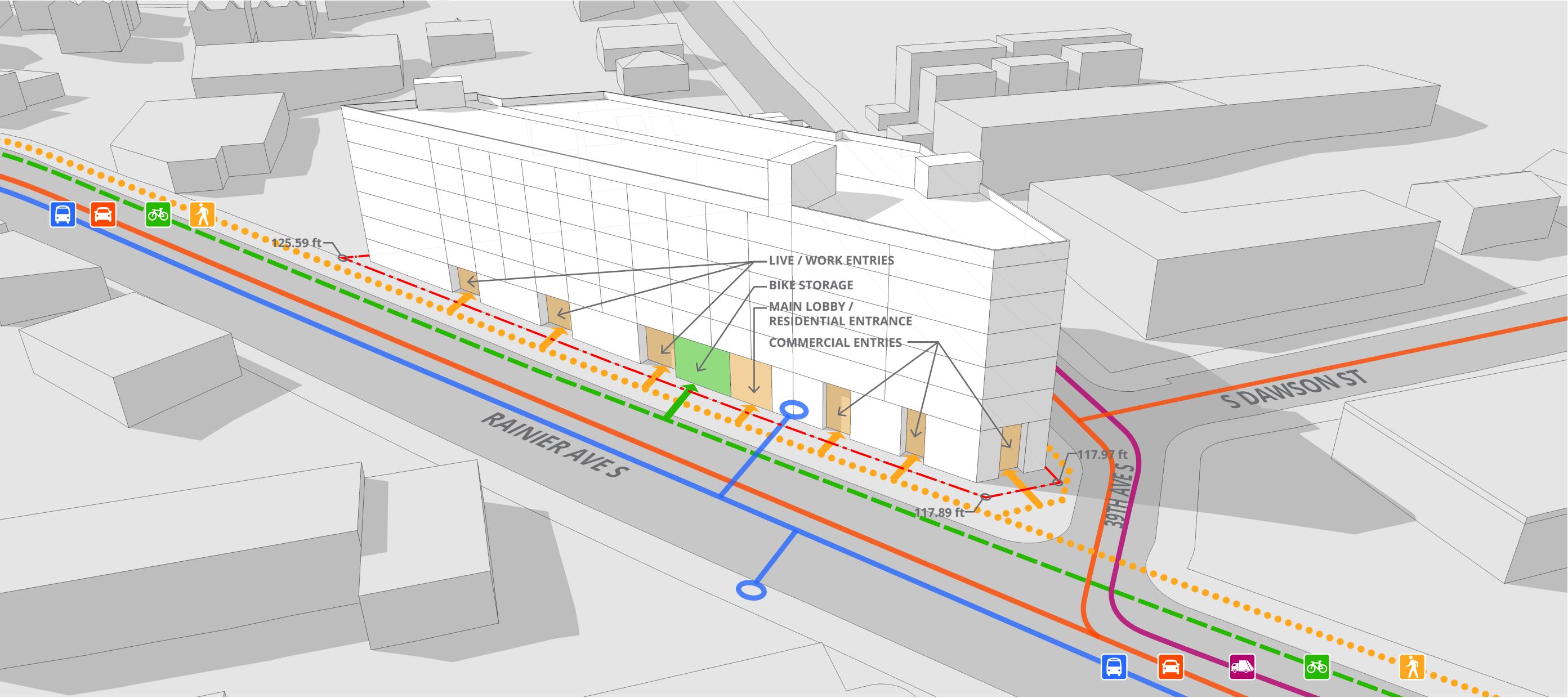
- 3-STORIES
- TOWNHOUSE/RESIDENTIAL
- SLOWER
- QUIETER

RAINIER AVE S

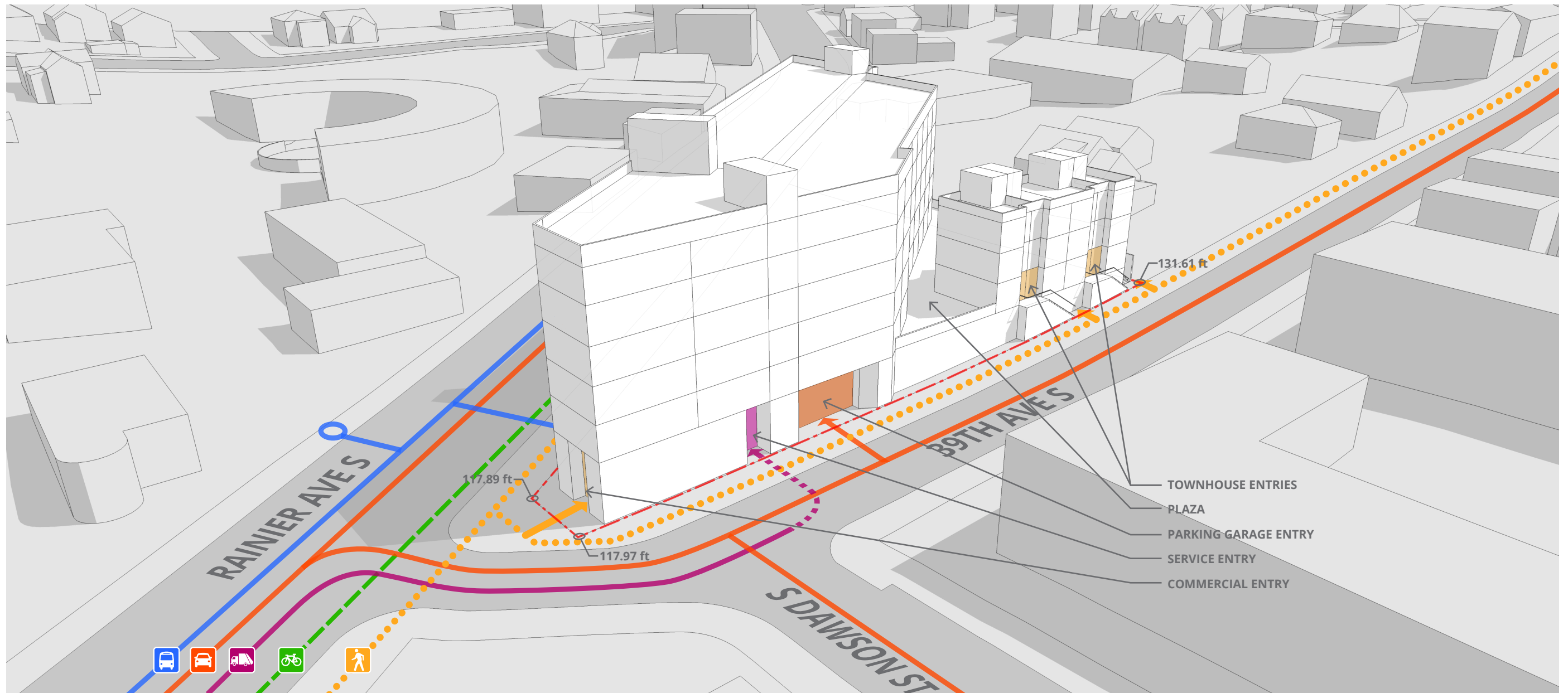
- 6-STORIES
- URBAN/MIXED-USE
- BUSY
- FAST

INTENTIONALLY BLANK

CONCEPT DEVELOPMENT: SITE CIRCULATION

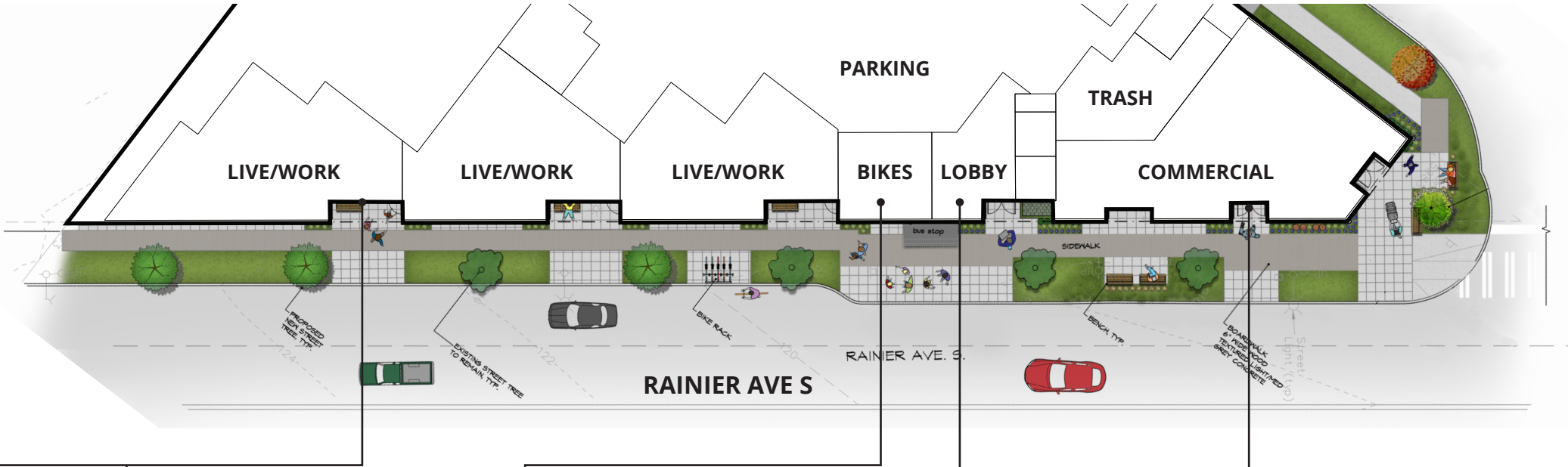


-  TRANSIT
-  VEHICULAR
-  TRASH / SERVICE
-  BICYCLE
-  PEDESTRIAN



SITE ELEMENTS: RAINIER AVE S

- COMMERCIAL CORNER
- MEDIUM TRAFFIC
 - COMMERCIAL PLAZA
 - STOREFRONTS, CANOPY, SIGNAGE
- LIVE-WORKS
- 2-STORY SPACES
 - TRANSPARENT ENTRIES
- RESIDENTIAL LOBBY
- DISTINGUISH FROM COMMERCIAL
 - SIGNAGE / MATERIALS
 - BIKE STORAGE
- BUS STOP



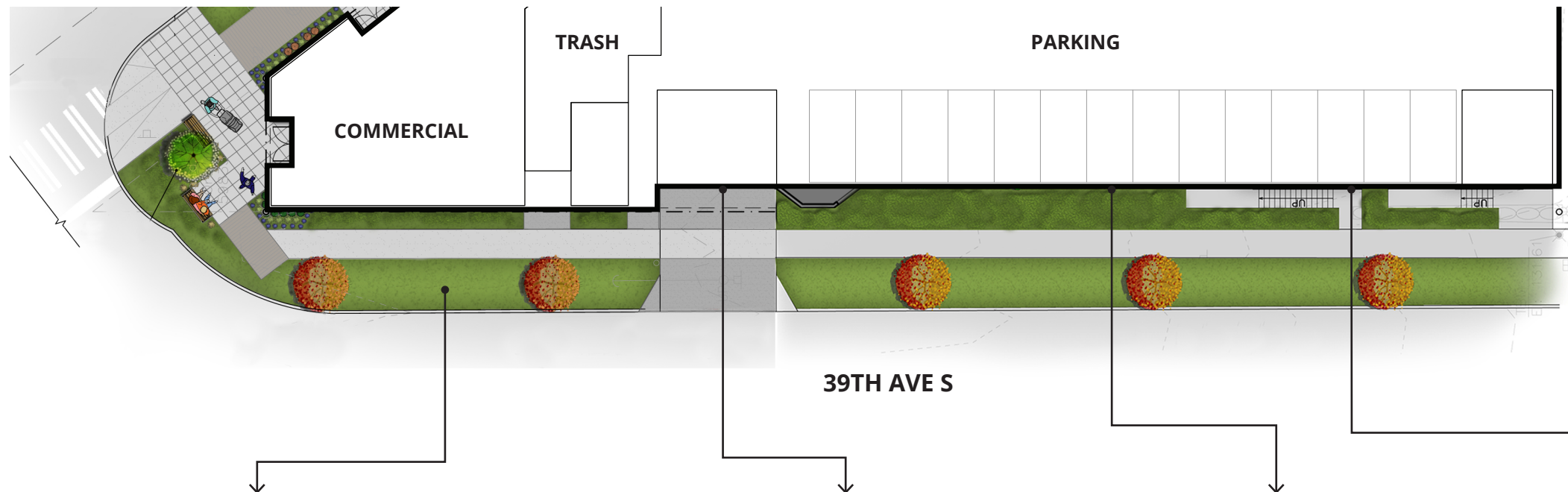
LIVE / WORK



BIKE STORAGE + INTEGRATED BUS STOP



COMMERCIAL FRONT



SITE ELEMENTS: 39TH AVE S

GARAGE ENTRY

- MINIMAL, UNOBTRUSIVE
- COMMERCIAL TRANSITION
- ELEVATED TOWNHOME ENTRIES
- BLANK WALL WITH PLAZA ABOVE



R.O.W. PLANTING



GARAGE ENTRANCE



GREEN WALL

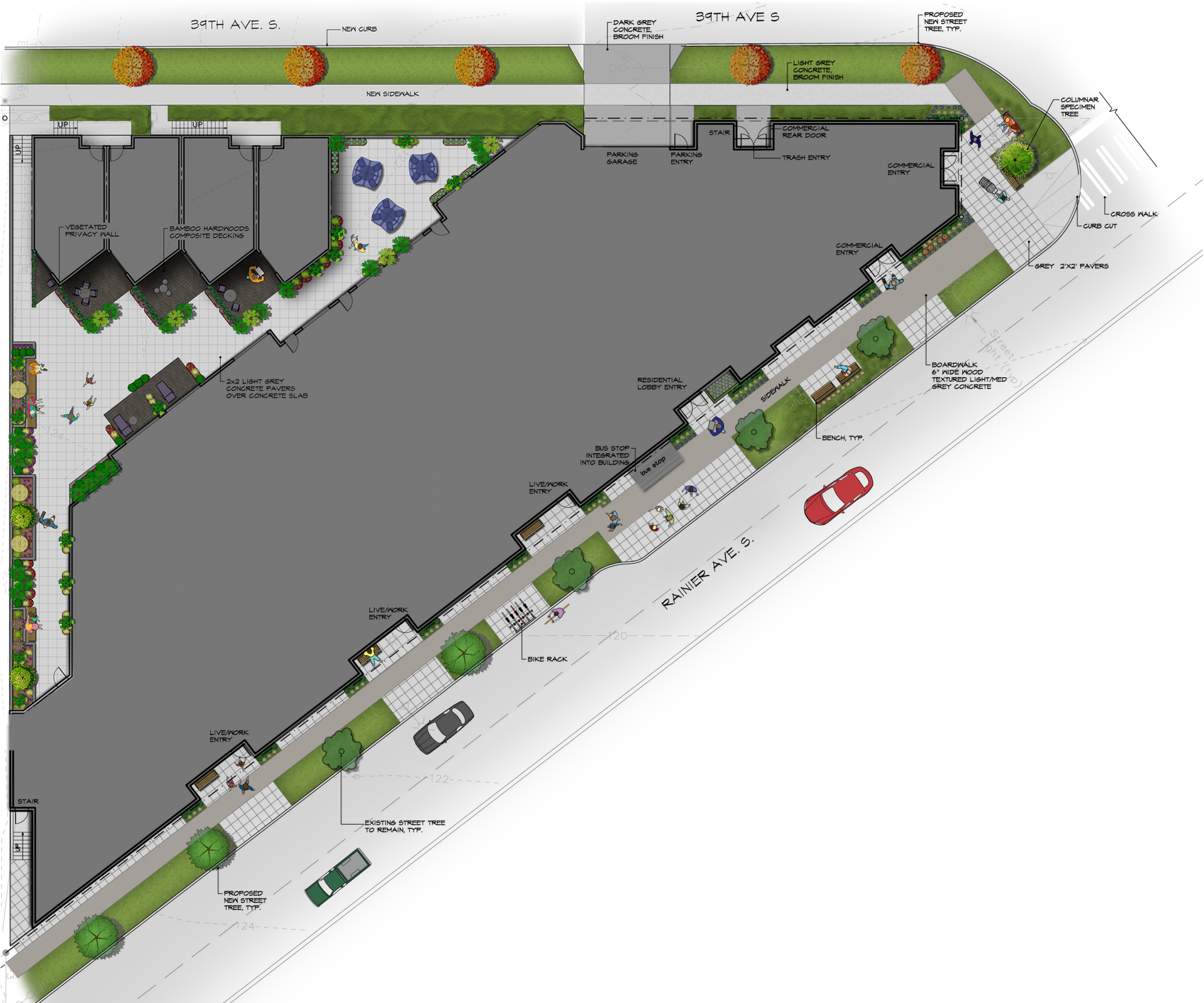


TOWNHOUSES



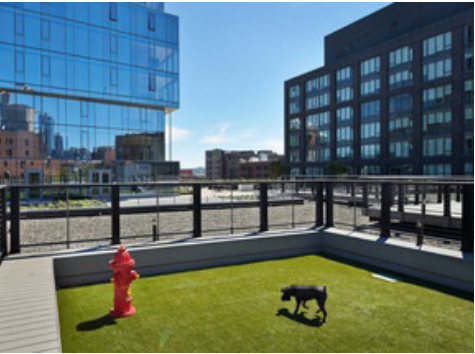
LANDSCAPE CONCEPT- SITE

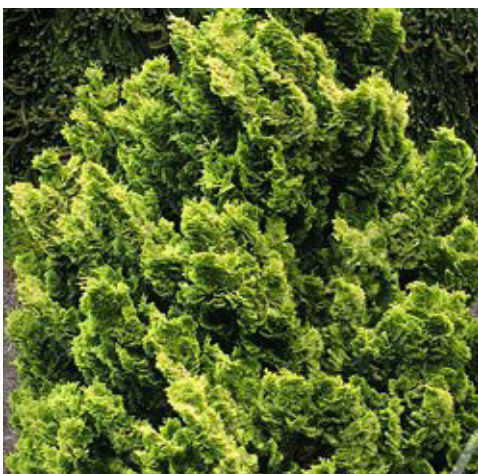
N.T.S.



SITE ELEMENTS

The landscape materials are selected to complement the modern aesthetic of the architecture, and arranged to control and buffer the streetscape. The landscaping will create year-round interest and will be durable and easily maintainable.





LANDSCAPE ELEMENTS

(left to right)

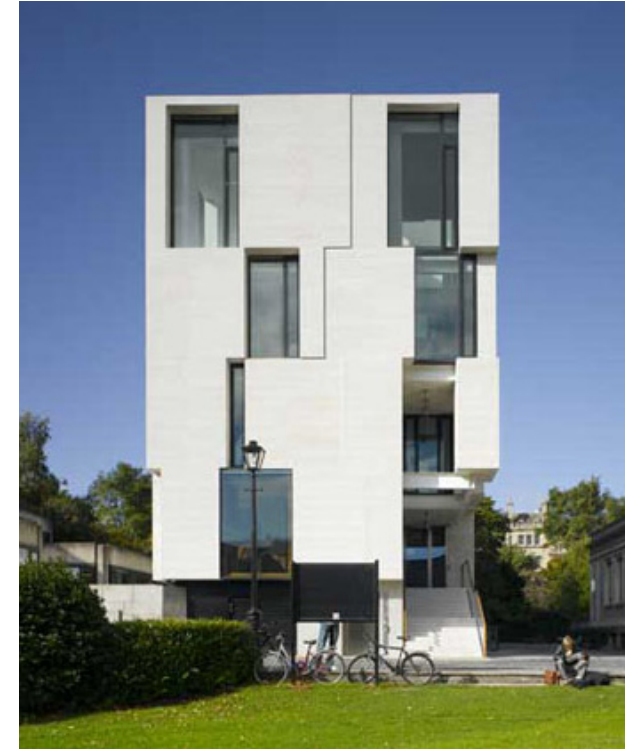
1. Firedance Sedge 'Uncinia rubra'
2. Horsetail 'Equisetum'
3. Katsura Japanese Maple
4. Japanese Painted Fern 'Athyrum niponicum'

5. Fuchsia Double Otto 'Fuchsia'
6. Black Bamboo 'Phyllostachys nigra'
7. Fiveleaf Akebia 'Akebia quinata'
8. Horsetail planters

9. Green Spire Little Leaf Linden 'Tilia cordata'
10. Golden Hinoki Cypress 'Chamaecyparis obtusa'
11. Green Spire Euonymus 'Euonymus japonicus'
12. Fatsia 'Fatsia japonica'

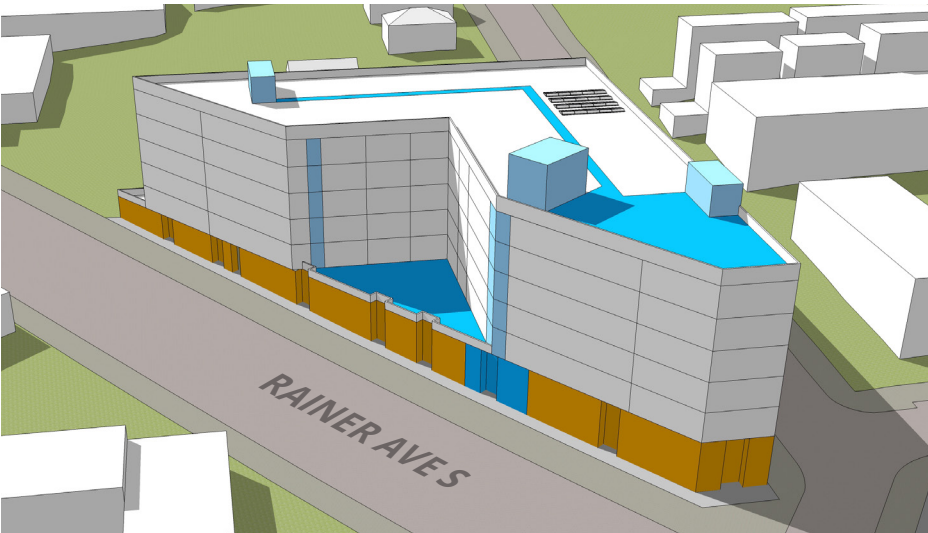
ARCHITECTURAL CONCEPT

The design team looked to high quality modern precedents, both at home and abroad, for inspiration. The building massing has been modulated to create a scale that is more sympathetic to both the Rainier Ave corridor as well as the pedestrian. High quality modern materials are used throughout.

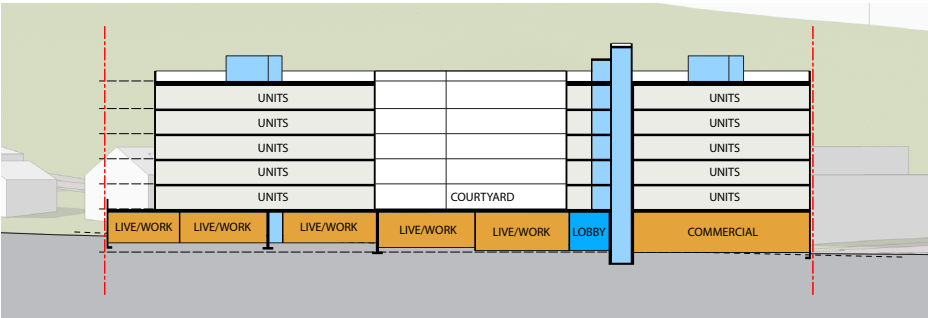


CONCEPT A: L-SHAPE

115 Units
7 Live/work units, 700-1530 sf (each)
2,057 Commercial sf
25 Parking stalls provided



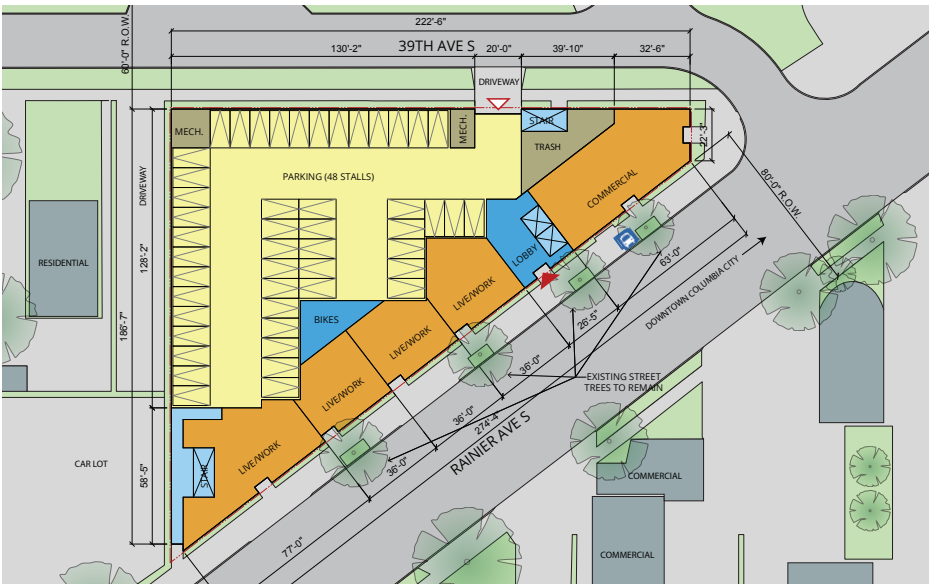
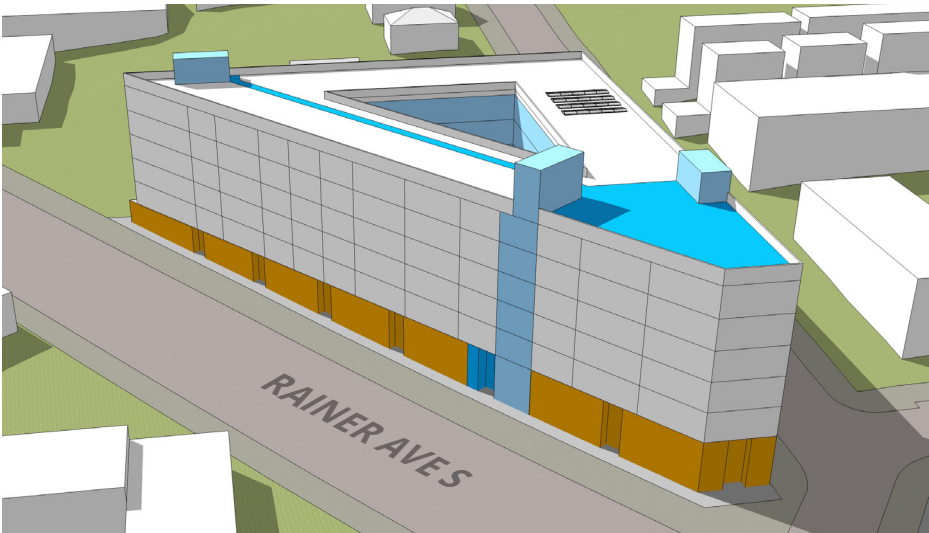
SITE PLAN
SCALE: N.T.S.



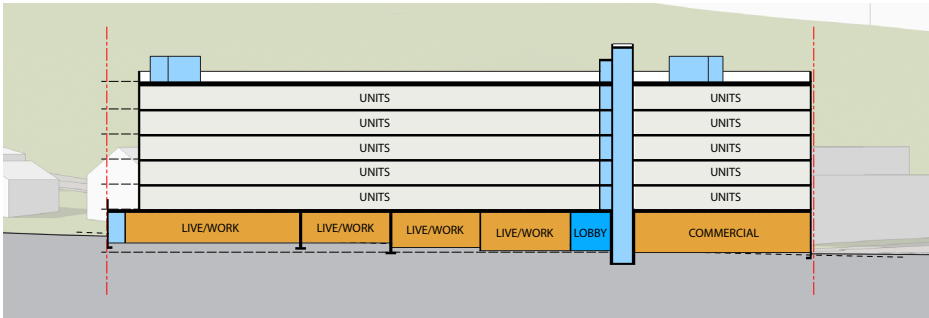
SECTION LOOKING EAST
SCALE: N.T.S.

CONCEPT B: INTERIOR COURTYARD

120 Units
4 Live/work units, 1037-1818 sf (each)
2,057 Commercial sf
48 Parking stalls provided



SITE PLAN
SCALE: N.T.S.



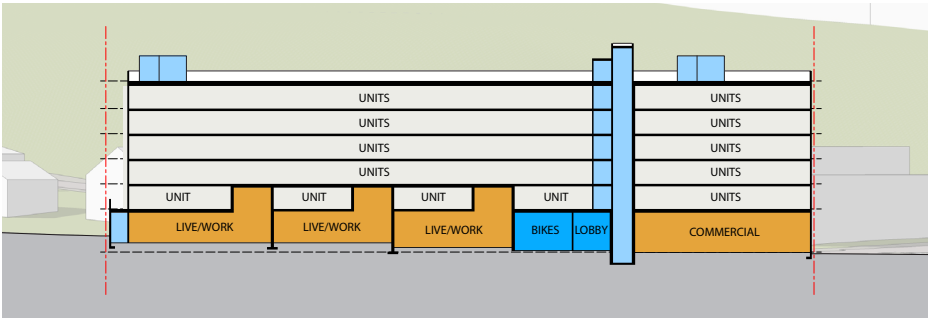
SECTION LOOKING EAST
SCALE: N.T.S.

CONCEPT C: PREFERRED

121 Units
3 Live/work units, 1404-1652 sf (each)
1,888 Commercial sf
55 Parking stalls provided



SITE PLAN
SCALE: N.T.S.



SECTION LOOKING EAST
SCALE: N.T.S.

DEPARTURES

CONCEPT B: INTERIOR COURTYARD

1) SMC 23.47A.014.B.1 - SETBACK FOR LOTS ABUTTING RESIDENTIAL LOTS

REQUIRED: 15' TRIANGULAR SETBACK ABUTTING RESIDENTIAL LOTS
PROPOSED: 15' REDUCTION OF TRIANGULAR SETBACK
GUIDELINES & JUSTIFICATION:
-DC2/A MASSING: THE REDUCED SETBACK ALLOWS THE MASSING TO COMPLETE ITSELF AT THE SOUTHWEST CORNER AND FORM A CLEAR EDGE FROM THE GROUND TO THE SKY
-CS1/C TOPOGRAPHY: THE SETBACK REDUCTION ALLOWS THE PARKING GARAGE TO BE MORE EFFICIENTLY LOCATED

2) SMC 23.47A.014.B.3 - REAR SETBACK ABUTTING RESIDENTIAL ZONES

REQUIRED: 15' BETWEEN 13' TO 40' HEIGHT, PLUS 2' FOR EVERY 10' ABOVE 40' HEIGHT
PROPOSED: 10' SETBACK BETWEEN 5' TO 65' HEIGHT.
GUIDELINES & JUSTIFICATION:
-DC3/A.1 BUILDING OPEN SPACE RELATIONSHIP: THE SETBACK REDUCTION ALLOWS THE CENTRAL COURTYARD CONCEPT TO WORK BY ALLOWING ENOUGH ROOM FOR THE UNITS TO WRAP AROUND THE SPACE.

3) SMC 23.47A.008.B.2 - NON-RESIDENTIAL STREET-LEVEL REQUIREMENTS

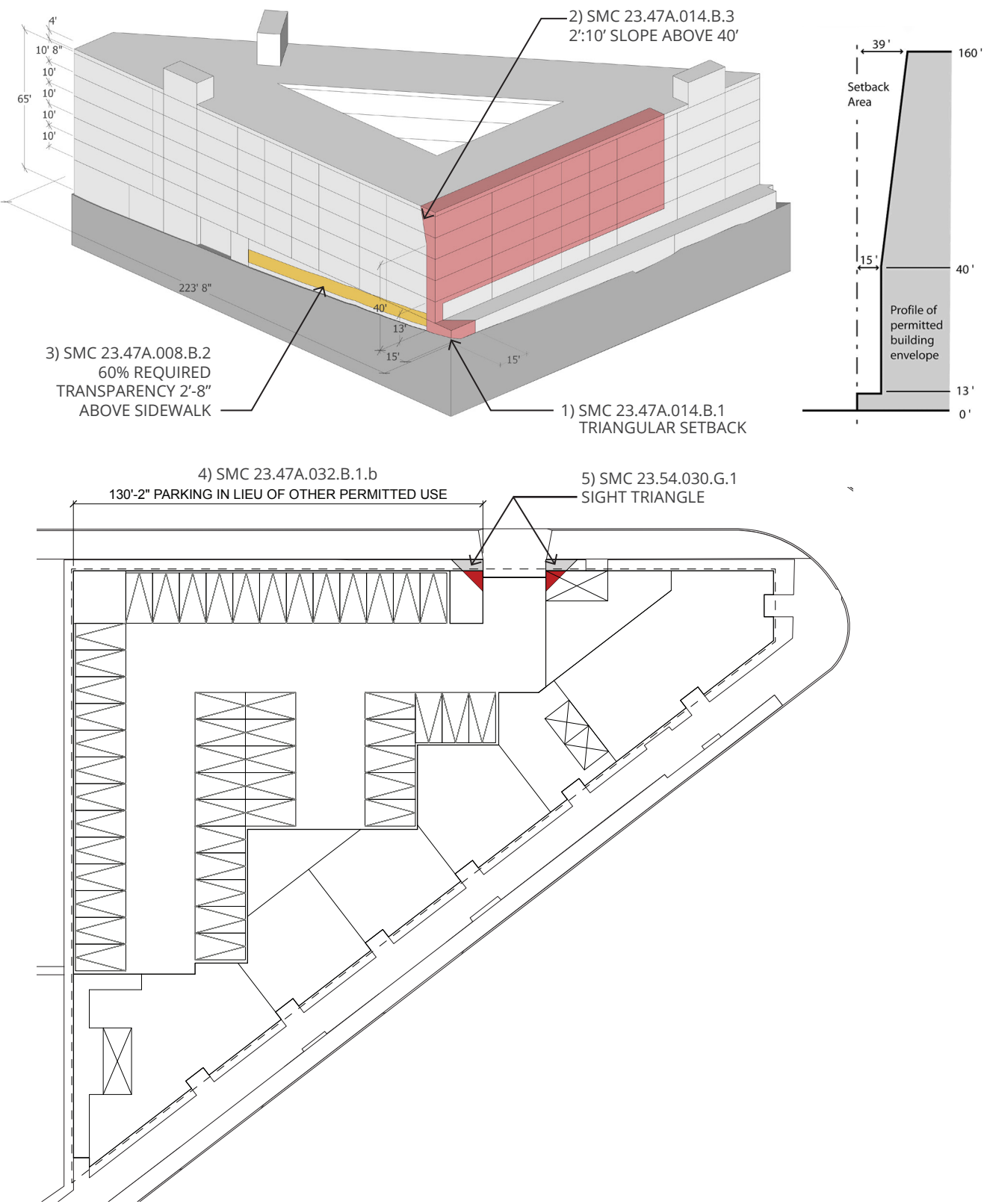
REQUIRED: 60% STREET FACING FACADE 2'-8" ABOVE SIDEWALK TRANSPARENT
PROPOSED: NONE @ 39TH AVE S. ONLY
GUIDELINES & JUSTIFICATION:
-CS1/C TOPOGRAPHY: IN ORDER TO UTILIZE THE SLOPING SITE TOPOGRAPHY, AND BURY THE GARAGE, A PORTION OF THE FACADE ALONG 39TH AVE S WILL BE OPAQUE

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GUIDELINES & JUSTIFICATION:
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5) SMC 23.54.030.G.1 - SIGHT TRIANGLE

REQUIRED: 10' TRIANGLE FROM INTERSECTION OF DRIVEWAY AND SIDEWALK
PROPOSED: 3'-7" TRIANGLE @ NORTH AND SOUTH WITH MIRRORS AND RUMBLE PAVING AT DRIVEWAY
GUIDELINES & JUSTIFICATION:
DC1/B: VEHICULAR ACCESS AND CIRCULATION: THE SIGHT TRIANGLE REDUCTION MINIMIZES THE WIDTH AND VISUAL PROMINENCE OF THE GARAGE ENTRY ALONG 39TH AVE S.



DEPARTURES

CONCEPT C: PREFERRED

1) SMC 23.47A.014.B.1 - SETBACK FOR LOTS ABUTTING RESIDENTIAL LOTS

REQUIRED: 15' TRIANGULAR SETBACK ABUTTING RESIDENTIAL LOTS

PROPOSED: 9' REDUCTION OF TRIANGULAR SETBACK

GUIDELINES & JUSTIFICATION:

-DC2/A+C MASSING, BUILDING OPEN SPACE RELATIONSHIP: TO PROVIDE THE TOWNHOUSE UNITS AND THE PLAZA, THE TOWNHOUSES AT THE SOUTHWEST CORNER REQUIRE A REDUCED SETBACK

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REQUIRED: 15' BETWEEN 13' TO 40' HEIGHT, PLUS 2' FOR EVERY 10' ABOVE 40' HEIGHT

PROPOSED: 6' SETBACK BETWEEN 13' TO 36' HEIGHT.

GUIDELINES & JUSTIFICATION:

- CS2 B ADJACENT SITES, STREETS AND OPEN SPACES: THE REDUCTION HELPS THE PROPOSED/ PREFERRED SCHEME TRANSITION SCALE FROM RAINIER AVE S TO THE MORE RESIDENTIAL SCALE OF 39TH AVE S

-PL3.B RESIDENTIAL EDGES: THE SETBACK REDUCTION ALLOWS THE TOWNHOMES TO FIT WITH THE ADJACENT LR2 ZONE.

3) SMC 23.47A.008.B.2 - NON-RESIDENTIAL STREET-LEVEL REQUIREMENTS

REQUIRED: 60% STREET FACING FACADE 2'-8" ABOVE SIDEWALK TRANSPARENT

PROPOSED: NONE @ 39TH AVE S. ONLY

GUIDELINES & JUSTIFICATION:

-CS1/C TOPOGRAPHY: IN ORDER TO UTILIZE THE SLOPING SITE TOPOGRAPHY, AND BURY THE GARAGE, A PORTION OF THE FACADE ALONG 39TH AVE S WILL BE OPAQUE

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PROPOSED: 7'-1" TRIANGLE @ SOUTH, 0'-9" TRIANGLE @ NORTH WITH MIRRORS AND RUMBLE PAVING AT DRIVEWAY

GUIDELINES & JUSTIFICATION:

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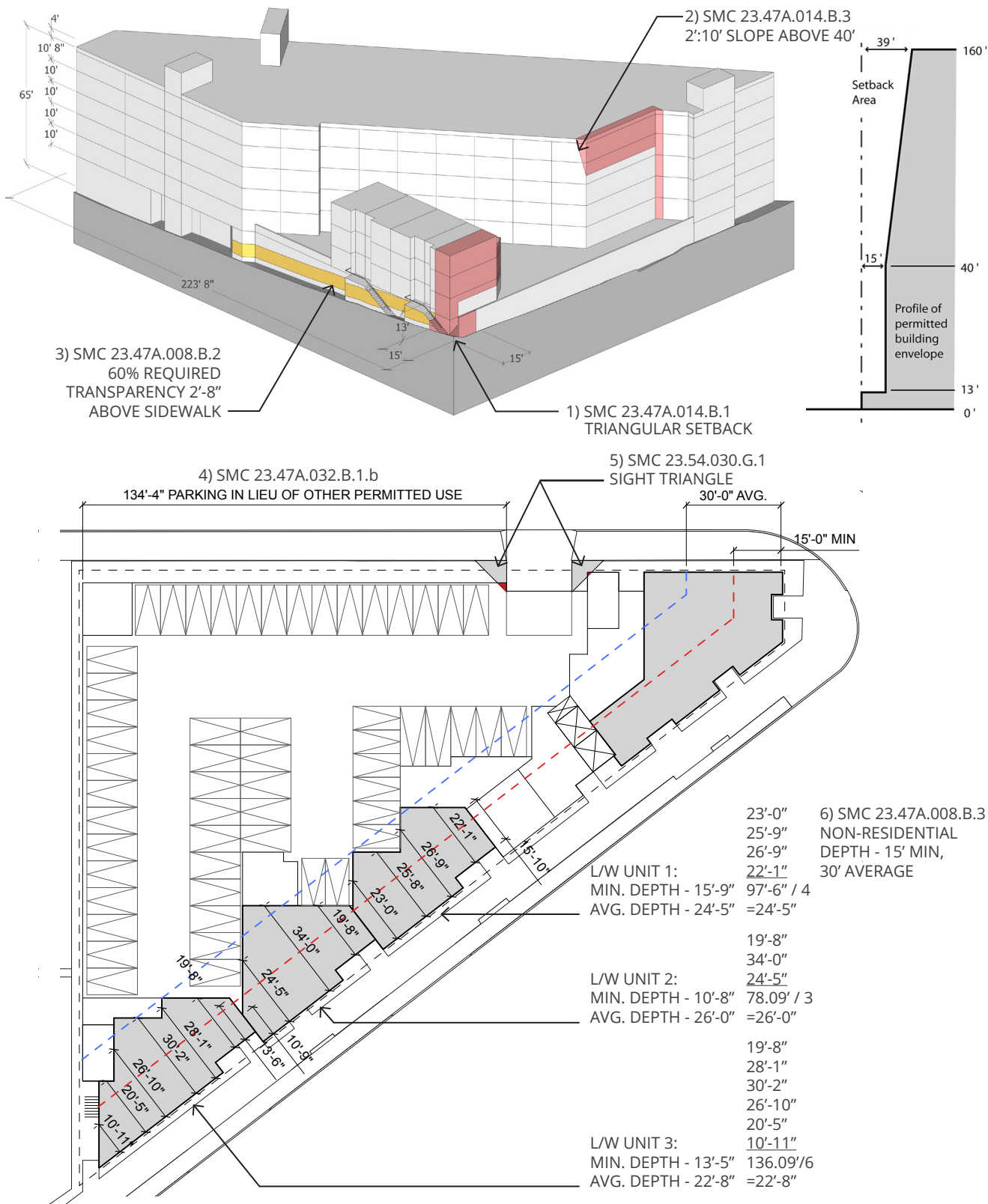
6) SMC 23.47A.008.B.3 - NON-RESIDENTIAL STREET-LEVEL REQUIREMENTS

REQUIRED: AVG. 30', 15' MINIMUM DEPTH FOR NON-RESIDENTIAL USES

PROPOSED: SEE DIAGRAM

GUIDELINES & JUSTIFICATION:

- DC1/A ARRANGEMENT OF INTERIOR USES: THE DEPARTURE ALLOWS THE PARKING TO BE EFFICIENTLY PLANNED AND FOR THE ANGLED BUILDING GEOMETRY TO BE RESOLVED



RECENT WORK



5902 22ND AVE NW (UNDER CONSTRUCTION)



116 13TH AVE E



1728 12TH AVE



2418 NW 58TH ST (UNDER CONSTRUCTION)



1806 23RD AVE.



6301 15TH AVE NW